

DUPAGE AIRPORT AUTHORITY

WEST CHICAGO, ILLINOIS

CONSTRUCTION PLANS FOR DUPAGE AIRPORT

CONSTRUCT NEW AUTOMOBILE PARKING LOT

ILLINOIS PROJECT: DPA-4980

ISSUED FOR BID

MARCH 3, 2023

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DUPAGE AIRPORT

TOWNSHIP: 40 NORTH WAYNE TOWNSHIP
 RANGE: 9 EAST (SECTIONS: 31)
 DUPAGE COUNTY



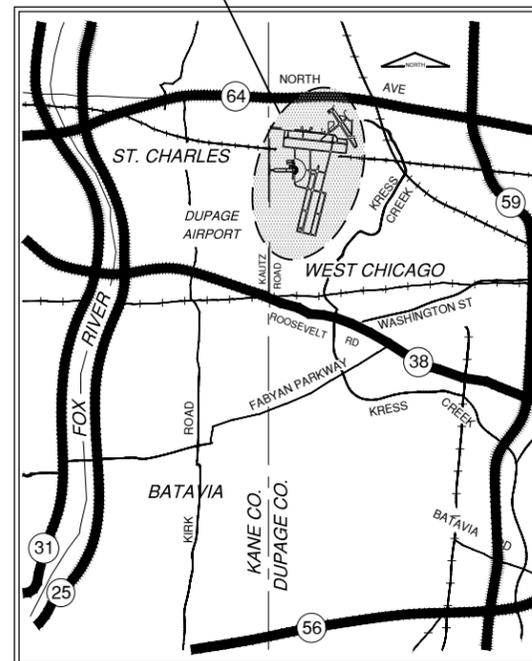
Know what's below.
 Call before you dig.

J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

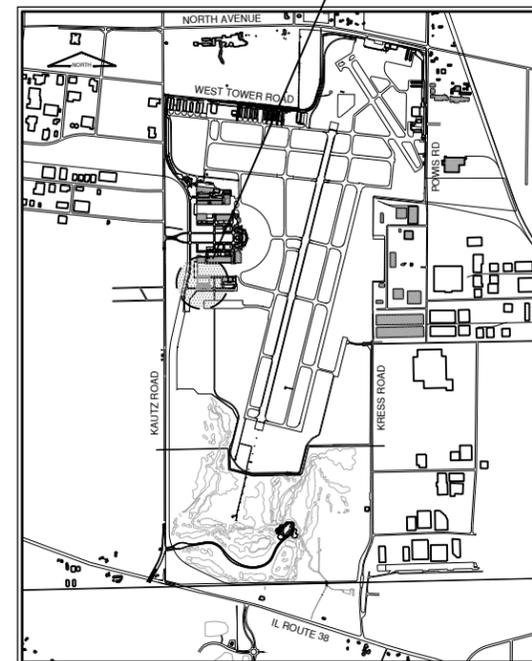
CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

PROJECT
 LOCATION



LOCATION MAP

PROJECT
 LOCATION



SITE PLAN



CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613

21002032-00



SUBMITTED BY Daniel L. Pape

DATE 3/10/23



DuPage Airport

2700 INTERNATIONAL DRIVE
 SUITE 200
 WEST CHICAGO, IL. 60185

APPROVED BY Mark Doles

MARK DOLES
 EXECUTIVE DIRECTOR

DATE 03/09/2023

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
BASE BID				
AR106905	REMOVE LIGHT POLE AND FIXTURE	EACH	3	
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AR150520	MOBILIZATION	L SUM	1	
AR152410	UNCLASSIFIED EXCAVATION	CU YD	2,755	
AR152540	SOIL STABILIZATION FABRIC	SQ YD	2,430	
AR156510	SILT FENCE	FOOT	410	
AR156520	INLET PROTECTION	EACH	7	
AR208515	POROUS GRANULAR EMBANKMENT	CU YD	540	
AR208608	8" AGGREGATE BASE COURSE	SQ YD	2,426	
AR401610	BITUMINOUS SURFACE COURSE	TON	270	
AR401900	REMOVE BITUMINOUS PAVEMENT	SQ YD	18	
AR403610	BITUMINOUS BASE COURSE	TON	540	
AR602510	BITUMINOUS PRIME COAT	GALLON	1,150	
AR603510	BITUMINOUS TACK COAT	GALLON	690	
AR701512	12" RCP, CLASS IV	FOOT	100	
AR751411	INLET - TYPE A	EACH	2	
AR751943	ADJUST MANHOLE	EACH	4	
AR751983	RECONSTRUCT MANHOLE	EACH	1	
AR754410	COMB CONCRETE CURB AND GUTTER	FOOT	660	
AR760000	WATER MAIN CONNECTIONS	EACH	2	
AR760312	12" WATER MAIN	FOOT	330	
AR760800	FIRE HYDRANT	EACH	2	
AR760900	REMOVE WATER MAIN	FOOT	320	
AR760905	REMOVE FIRE HYDRANT	EACH	2	
AR760947	ADJUST WATER VALVE	EACH	1	
AR770945	ADJUST SANITARY MANHOLE	EACH	2	
AR800152	EXPLORATORY TRENCH	FOOT	80	
AR800153	POTHOLING EXISTING UTILITIES	CAL DAY	4	
AR800171	PAINT PAVEMENT MARKING - LETTERS & SY	SQ FT	91	
AR800172	PAINT PAVEMENT MARKING - LINE 4"	FOOT	1,480	
AR800185	CONCRETE PARKING BLOCK	EACH	3	
AR901510	SEEDING	ACRE	1	
AR908510	MULCHING	ACRE	1	
AR910201	SIGN PANEL	SQ FT	24.5	
AR910246	SIGN SUPPORT	FOOT	63.5	

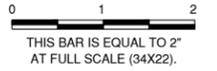
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
ADDITIVE ALTERNATE NO. 1 - PARKING LOT LIGHTING				
AS106960	RELOCATE LIGHT POLE	EACH	3	
AS110610	ELECTRICAL HANDHOLE	EACH	1	
AS800031	NEW DOUBLE MAST ARM - 4 FOOT	EACH	3	
AS800108	2-1/2" #8 XLP-USE WITH 1-1/2" #8 GND IN 1-1/4" UD	FOOT	432	
AS910120	ROADWAY LIGHT FIXTURE	EACH	6	

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
ADDITIVE ALTERNATE NO. 2 - EAST AUTOMOBILE PARKING LOT				
AT152410	UNCLASSIFIED EXCAVATION	CU YD	1,591	
AT152540	SOIL STABILIZATION FABRIC	SQ YD	1,160	
AT156520	INLET PROTECTION	EACH	1	
AT208515	POROUS GRANULAR EMBANKMENT	CU YD	270	
AT208606	6" AGGREGATE BASE COURSE	SQ YD	46	
AT208608	8" AGGREGATE BASE COURSE	SQ YD	1,156	
AT401610	BITUMINOUS SURFACE COURSE	TON	120	
AT403610	BITUMINOUS BASE COURSE	TON	240	
AT501506	6" PCC PAVEMENT	SQ YD	46	
AT602510	BITUMINOUS PRIME COAT	GALLON	504	
AT603510	BITUMINOUS TACK COAT	GALLON	152	
AT620510	PAVEMENT MARKING	SQ FT	200	
AT701515	15" RCP, CLASS IV	FOOT	118	
AT751540	MANHOLE 4'	EACH	1	
AT751940	ADJUST INLET	EACH	1	
AT751927	REPLACE FRAME AND GRATE	EACH	1	
AT754410	COMB CONCRETE CURB AND GUTTER	FOOT	572	
AT800060	DUMPSTER ENCLOSURE	L SUM	1	
AT901510	SEEDING	ACRE	0.50	
AT908510	MULCHING	ACRE	0.50	
AT910230	HANDICAP SIGN	EACH	2	
AT910243	SIGN SUPPORT	EACH	1	
AT910420	BOLLARD	EACH	5	

IL CONTRACT: **DU091**
IL LETTING ITEM: **07A**
IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT

SUMMARY OF QUANTITIES


CMT
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 184-000613


 DUPAGE AIRPORT
 AUTHORITY

DESIGN BY: JUM
 DRAWN BY: JMM
 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 03/03/2023
 JOB No: 21002032-00

FINAL

CALL J.U.L.I.E. BEFORE EXCAVATING
1-800-892-0123

IDOT STANDARD DRAWINGS

701901-08 TRAFFIC CONTROL DEVICES

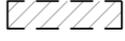
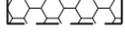
IDOT DISTRICT 1 STANDARD DRAWINGS

TC10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
2. THE CONTRACTOR WILL BE REQUIRED TO FURNISH AND PLACE CONSTRUCTION LAYOUT STAKES FOR THIS PROJECT. CONSTRUCTION LAYOUT SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM FOR WHICH THE LAYOUT IS REQUIRED.

LEGEND

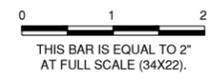
-  BASE BID (PARKING LOT)
-  ADDITIVE ALTERNATE NO. 1 (PARKING LOT LIGHTING)
-  ADDITIVE ALTERNATE NO. 2 (ADDITIONAL PARKING)
-  CONTRACTOR STAGING AREA
-  ENGINEER'S FIELD OFFICE
-  TRAFFIC DIRECTION



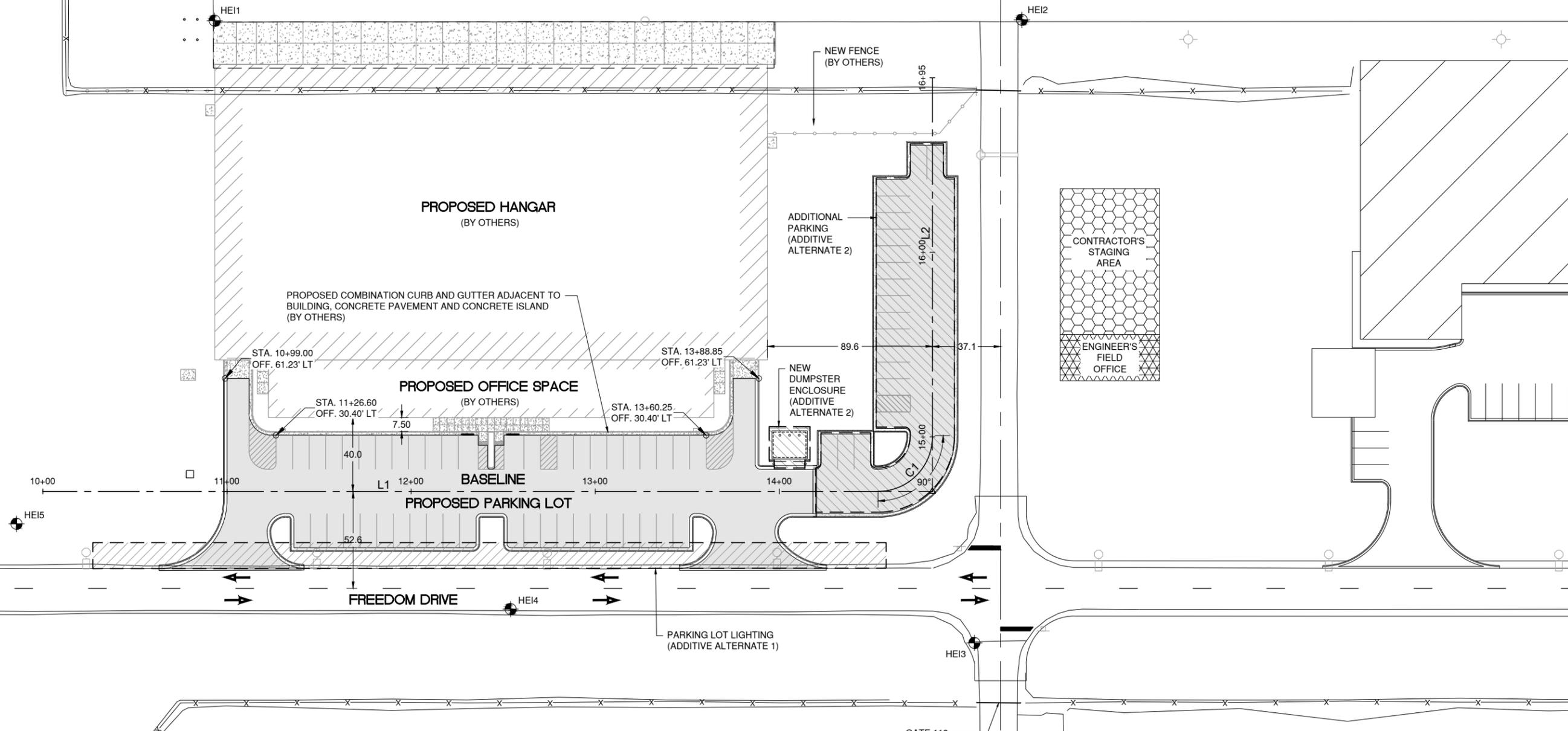
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SOUTH HIGHTAIL RAMP



**DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT
SITE PLAN AND PROJECT CONTROL PLAN**

SOUTH PARKING LOT									
NUMBER	LENGTH	RADIUS	LINE/CHORD DIRECTION	START STATION	START NORTHING	START EASTING	END STATION	END NORTHING	END EASTING
L1	453.586		N89° 59' 59.86"E	10+00.00	1907404.3869	1004289.6986	14+53.59	1907404.3872	1004743.2842
L2	194.649		N00° 00' 00.00"E	14+99.92	1907433.8872	1004772.7842	16+94.57	1907628.5361	1004772.7842
C1	46.338	29.500	N44° 59' 59.93"E	14+53.59	1907404.3872	1004743.2842	14+99.92	1907433.8872	1004772.7842

PROJECT CONTROL				
POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTOR
HEI1	1907659.4790	1004383.0000	754.50	X PCC HEI 1
HEI2	1907659.9250	1004821.3680	754.49	X PCC HEI 2
HEI3	1907322.2950	1004795.6040	756.34	X PCC HEI 3
HEI4	1907340.6130	1004543.7110	754.56	MAG NAIL HEI 4
HEI5	1907386.8260	1004275.3520	750.98	X PCC CULVERT HEI 5

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**DUPAGE AIRPORT
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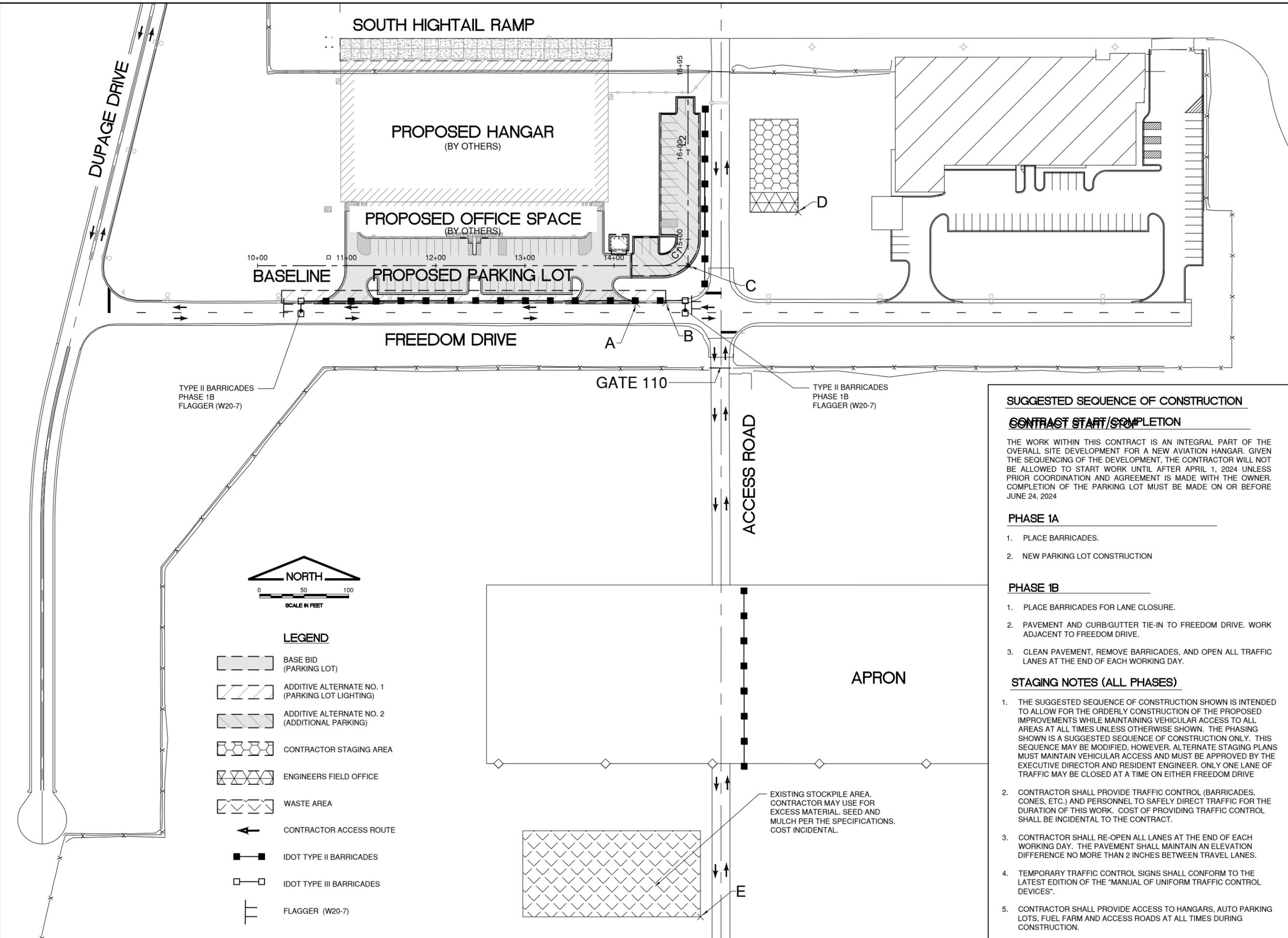
DESIGN BY: JUM
DRAWN BY: JMM
CHECKED BY: DKP
APPROVED BY: DLP
DATE: 03/03/2023
JOB No: 21002032-00

FINAL

UPDATE BY: Tony Meirin
LAYOUT: CONSTRUCTION AND STAGING PLAN

DATE: Friday, March 10, 2023 5:37:22 PM
FILE: K:\dupage\h2102032-00_SouthAutoParkingLot\Draw\Sheets\2102032-C-103.dwg

DATE: Friday, March 10, 2023 5:37:37 PM
 FILE: K:\Dodge\h2102032-00_SouthHullPermit\Draw\Sheets\2102032-SE0.dwg
 UPDATE BY: Tony Meirn
 LAYOUT: CONSTRUCTION AND STAGING PLAN



IL CONTRACT: **DU091**
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SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

SEQUENCE OF CONSTRUCTION

DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT

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DESIGN BY: JUM
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 JOB No: 21002032-00

FINAL

SHEET 4 OF 27 SHEETS

SUGGESTED SEQUENCE OF CONSTRUCTION

CONTRACT START/COMPLETION

THE WORK WITHIN THIS CONTRACT IS AN INTEGRAL PART OF THE OVERALL SITE DEVELOPMENT FOR A NEW AVIATION HANGAR. GIVEN THE SEQUENCING OF THE DEVELOPMENT, THE CONTRACTOR WILL NOT BE ALLOWED TO START WORK UNTIL AFTER APRIL 1, 2024 UNLESS PRIOR COORDINATION AND AGREEMENT IS MADE WITH THE OWNER. COMPLETION OF THE PARKING LOT MUST BE MADE ON OR BEFORE JUNE 24, 2024

PHASE 1A

1. PLACE BARRICADES.
2. NEW PARKING LOT CONSTRUCTION

PHASE 1B

1. PLACE BARRICADES FOR LANE CLOSURE.
2. PAVEMENT AND CURB/GUTTER TIE-IN TO FREEDOM DRIVE. WORK ADJACENT TO FREEDOM DRIVE.
3. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

STAGING NOTES (ALL PHASES)

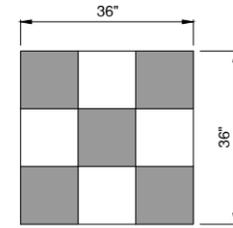
1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING VEHICULAR ACCESS TO ALL AREAS AT ALL TIMES UNLESS OTHERWISE SHOWN. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED, HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN VEHICULAR ACCESS AND MUST BE APPROVED BY THE EXECUTIVE DIRECTOR AND RESIDENT ENGINEER. ONLY ONE LANE OF TRAFFIC MAY BE CLOSED AT A TIME ON EITHER FREEDOM DRIVE
2. CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL (BARRICADES, CONES, ETC.) AND PERSONNEL TO SAFELY DIRECT TRAFFIC FOR THE DURATION OF THIS WORK. COST OF PROVIDING TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.
3. CONTRACTOR SHALL RE-OPEN ALL LANES AT THE END OF EACH WORKING DAY. THE PAVEMENT SHALL MAINTAIN AN ELEVATION DIFFERENCE NO MORE THAN 2 INCHES BETWEEN TRAVEL LANES.
4. TEMPORARY TRAFFIC CONTROL SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
5. CONTRACTOR SHALL PROVIDE ACCESS TO HANGARS, AUTO PARKING LOTS, FUEL FARM AND ACCESS ROADS AT ALL TIMES DURING CONSTRUCTION.

EXISTING STOCKPILE AREA. CONTRACTOR MAY USE FOR EXCESS MATERIAL. SEED AND MULCH PER THE SPECIFICATIONS. COST INCIDENTAL.

GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING VEHICLE ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT EXECUTIVE DIRECTOR AND RESIDENT ENGINEER.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370 (LATEST EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS. NO EXCEPTIONS FOR SLOW MOVING EQUIPMENT SHALL BE ALLOWED
- THE AIRPORT EXECUTIVE DIRECTOR SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING PHASING AND SEQUENCE AS IT RELATES TO PEDESTRIAN AND VEHICULAR SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT EXECUTIVE DIRECTOR.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE TENANT PARKING LOTS AND DRIVEWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER AND AIRPORT.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, CONSTRUCTION FENCE, SIGNING, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES SHALL HAVE FLASHING YELLOW LIGHT(S) AND CONFORM TO IDOT STANDARD 701901-08, TYPE II AND TYPE III. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT EXECUTIVE DIRECTOR THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT EXECUTIVE DIRECTOR AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- DUPAGE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO PREVIOUS EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT EXECUTIVE DIRECTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS OR A WORKING BEACON LIGHT ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT. THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DETAILS HOW THE CONTRACTOR WILL COMPLY WITH THE CSPP. ALSO, IT WILL NOT BE POSSIBLE TO DETERMINE ALL SAFETY PLAN DETAILS (FOR EXAMPLE SPECIFIC HAZARD EQUIPMENT AND LIGHTING, CONTRACTOR'S POINTS OF CONTACT, CONSTRUCTION EQUIPMENT HEIGHTS) DURING THE DEVELOPMENT OF THE CSPP. THE SUCCESSFUL CONTRACTOR MUST DEFINE SUCH DETAILS BY PREPARING AND SPCD THAT THE AIRPORT OPERATOR REVIEWS FOR APPROVAL PRIOR TO ISSUANCE OF A NOTICE-TO-PROCEED. THE SPCD IS A SUBSET OF THE CSPP, SIMILAR TO HOW A SHOP DRAWING REVIEW IS A SUBSET TO THE TECHNICAL SPECIFICATIONS.



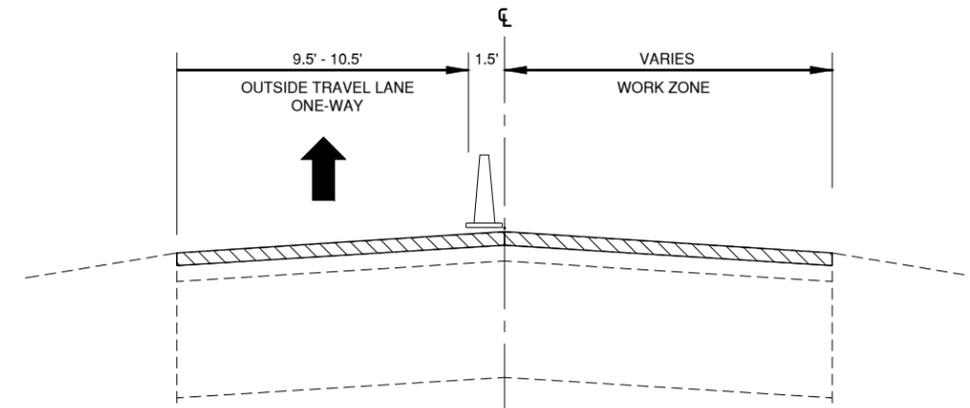
CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS. THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS AND SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE SITE DUE TO ADJACENT CONSTRUCTION

- NEW AVIATION HANGAR
- WEST TOWER ROAD AND DUPAGE ROAD RESURFACING
- RECONFIGURE TAXIWAY E

CRITICAL POINTS					
POINT	DESCRIPTION	GROUND ELEVATION	EQUIPMENT HEIGHT	LATITUDE	LONGITUDE
A	BASE BID	756	25'	N041° 54' 11.88"	W088° 15' 29.36"
B	ADD ALT 1	755	25'	N041° 54' 11.89"	W088° 15' 28.93"
C	ADD ALT 2	755	25'	N041° 54' 12.30"	W088° 15' 28.59"
D	STAGING AREA	754	25'	N041° 54' 12.90"	W088° 15' 26.97"
E	WASTE AREA	754	25'	N041° 54' 05.10"	W088° 15' 28.42"



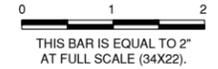
LANE CLOSURE DETAIL

NOT TO SCALE

IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT

SEQUENCE OF CONSTRUCTION
GENERAL NOTES AND DETAILS

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DUPAGE AIRPORT
AUTHORITY

DESIGN BY:	JJM
DRAWN BY:	JMM
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21002032-00

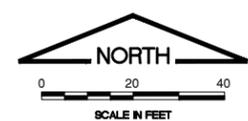
FINAL

IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



CARD READER

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT
EXISTING CONDITIONS PLAN

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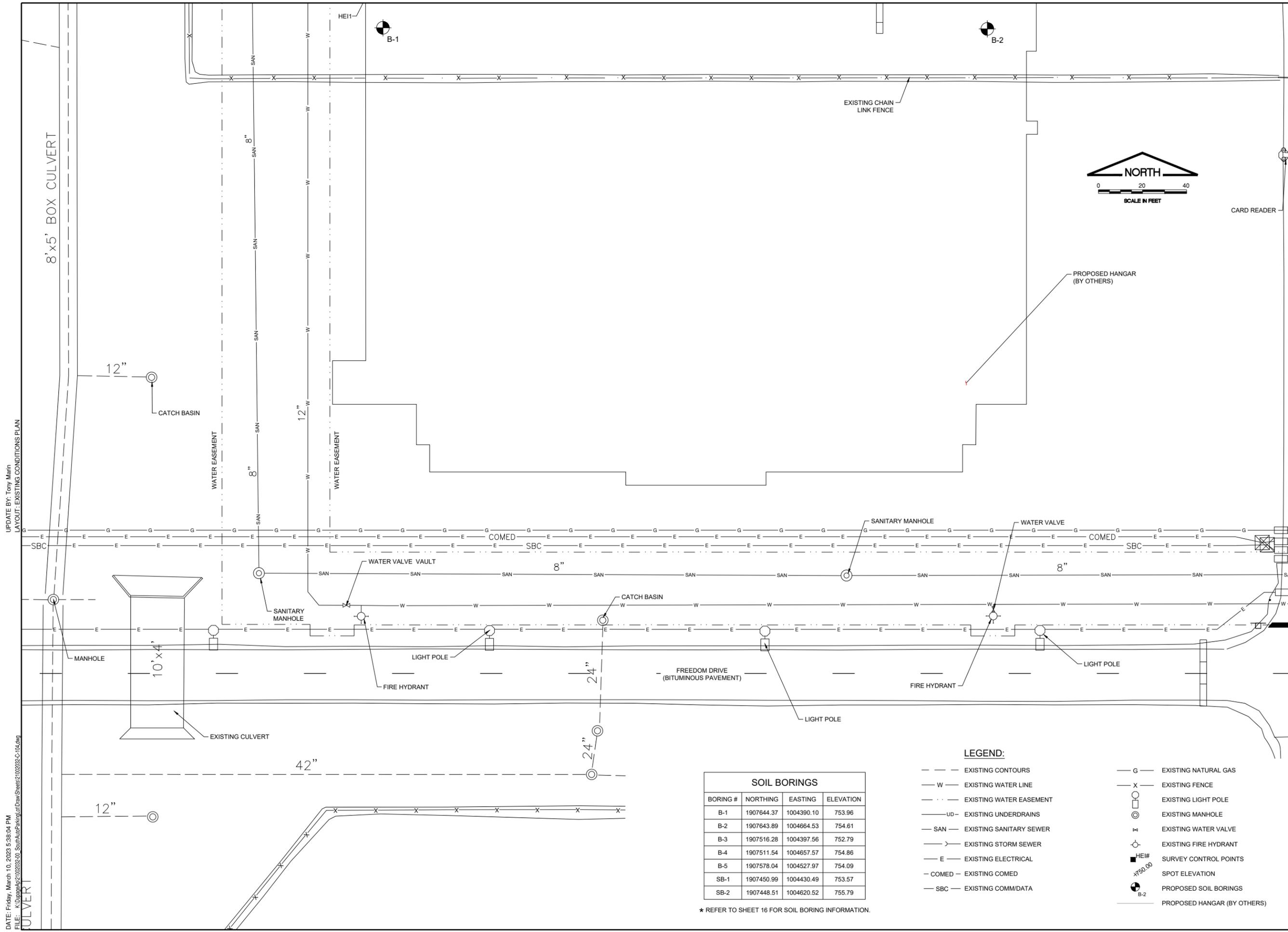
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 DRAWN BY: JMM
 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 03/03/2023
 JOB No: 21002032-00

FINAL

SHEET 6 OF 27 SHEETS



SOIL BORINGS			
BORING #	NORTHING	EASTING	ELEVATION
B-1	1907644.37	1004390.10	753.96
B-2	1907643.89	1004664.53	754.61
B-3	1907516.28	1004397.56	752.79
B-4	1907511.54	1004657.57	754.86
B-5	1907578.04	1004527.97	754.09
SB-1	1907450.99	1004430.49	753.57
SB-2	1907448.51	1004620.52	755.79

* REFER TO SHEET 16 FOR SOIL BORING INFORMATION.

LEGEND:

- EXISTING CONTOURS
- W --- EXISTING WATER LINE
- EXISTING WATER EASEMENT
- UD --- EXISTING UNDERDRAINS
- SAN --- EXISTING SANITARY SEWER
- EXISTING STORM SEWER
- E --- EXISTING ELECTRICAL
- COMED --- EXISTING COMED
- SBC --- EXISTING COMM/DATA
- G --- EXISTING NATURAL GAS
- X --- EXISTING FENCE
- EXISTING LIGHT POLE
- ⊙ EXISTING MANHOLE
- ⊕ EXISTING WATER VALVE
- ⊕ EXISTING FIRE HYDRANT
- HEI# SURVEY CONTROL POINTS
- ⊕ SPOT ELEVATION
- ⊕ PROPOSED SOIL BORINGS
- ⊕ PROPOSED HANGAR (BY OTHERS)

UPDATE BY: Tony Meirn
 LAYOUT: EXISTING CONDITIONS PLAN

DATE: Friday, March 10, 2023 5:38:04 PM
 FILE: K:\Dupage\21002032-00_SouthwestParkingLot\Draw\Sheets\21002032-C-04.dwg

CULVERT

NOTES:

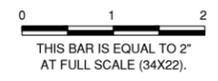
1. PROTECT UTILITIES THAT ARE EXISTING TO REMAIN, EXPOSED OR CONCEALED. VERIFY PROPER IDENTIFICATION/ LABELING, AND WHERE TERMINATION OCCUR, CAP AS REQUIRED.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ALL REQUIRED LOCAL PERMITS FROM LOCAL AGENCIES.
3. THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.
4. THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
5. CONTRACTOR SHALL TAKE MEASURES TO PROTECT EXISTING PAVEMENTS, ANY PAVEMENT DAMAGED BY CONTRACTOR'S EQUIPMENT SHALL BE SAWCUT PER RESIDENT ENGINEER LAYOUT AND REPLACED IN KIND AT NO ADDITIONAL COST TO CONTRACT.
6. CONTRACTOR TO TAKE MEASURES TO PROTECT ALL UNDERGROUND UTILITIES INCLUDING, BUT NOT LIMITED TO, NEW AND EXISTING SHALLOW STORM SEWER PIPE FROM DAMAGE DUE TO CONSTRUCTION EQUIPMENT.



IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT
DEMOLITION PLAN

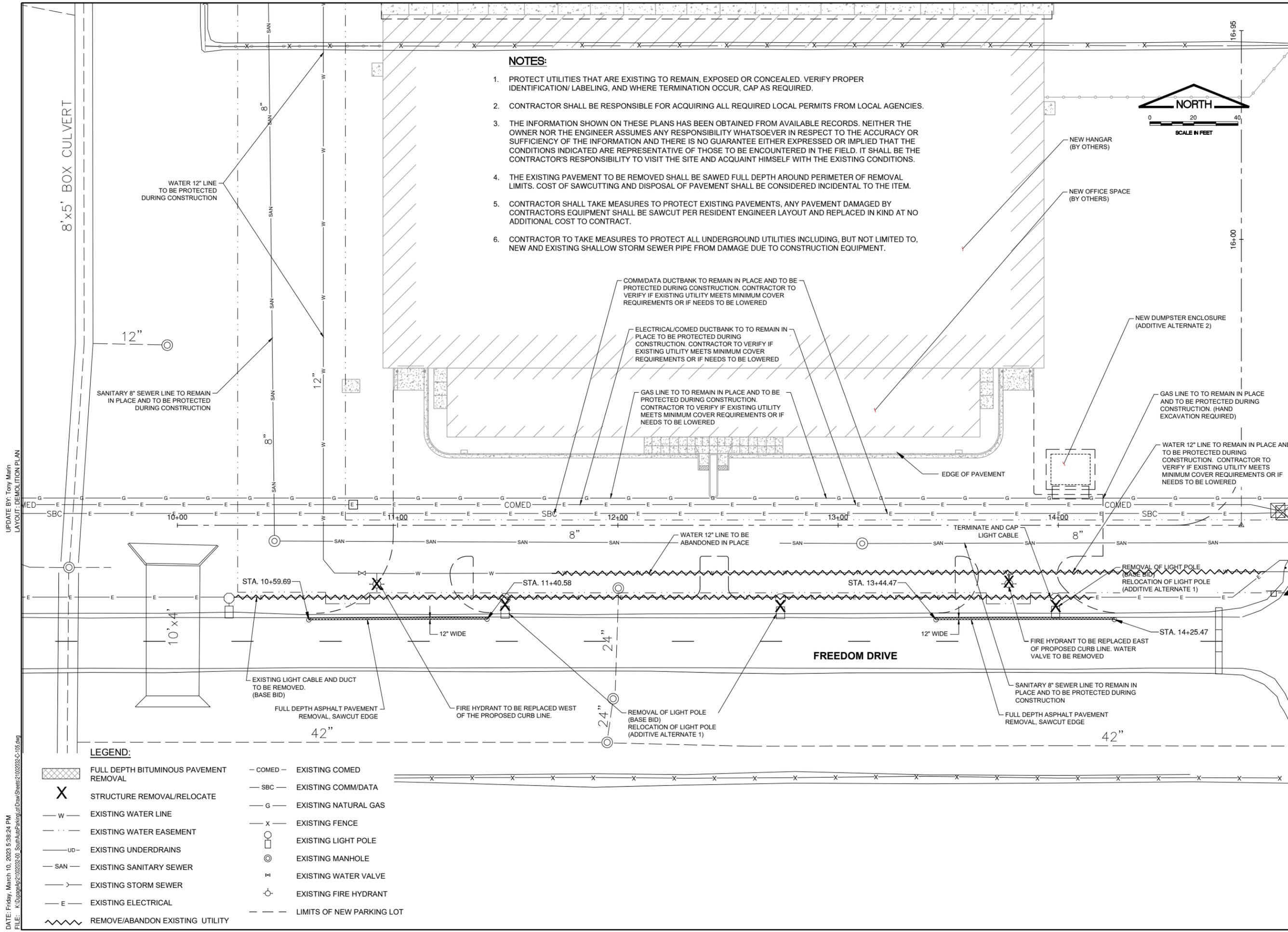
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DUPAGE AIRPORT
AUTHORITY

DESIGN BY: JUM
 DRAWN BY: JMM
 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 03/03/2023
 JOB No: 21002032-00

FINAL



8'x5' BOX CULVERT

UPDATE BY: Tony Meirn
LAYOUT: DEMOLITION PLAN

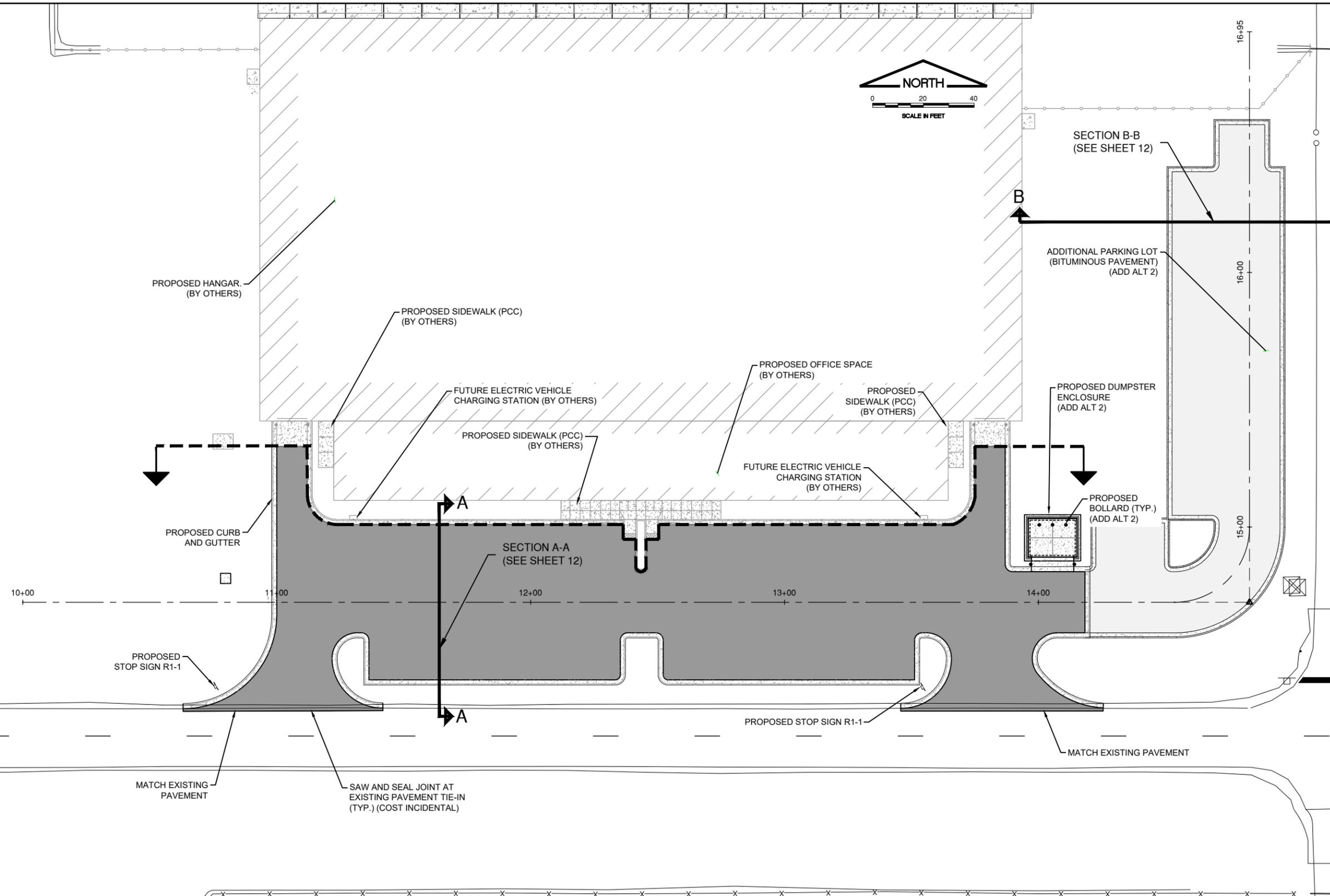
DATE: Friday, March 10, 2023 5:38:24 PM
FILE: K:\dupage\21002032-00_SouthsideParkingLot\DrawSheets\21002032-C-105.dwg

LEGEND:

- | | | | | |
|--|--|--|-------|---------------------------|
| | FULL DEPTH BITUMINOUS PAVEMENT REMOVAL | | COMED | EXISTING COMED |
| | STRUCTURE REMOVAL/RELOCATE | | SBC | EXISTING COMM/DATA |
| | EXISTING WATER LINE | | G | EXISTING NATURAL GAS |
| | EXISTING WATER EASEMENT | | X | EXISTING FENCE |
| | EXISTING UNDERDRAINS | | | EXISTING LIGHT POLE |
| | EXISTING SANITARY SEWER | | | EXISTING MANHOLE |
| | EXISTING STORM SEWER | | | EXISTING WATER VALVE |
| | EXISTING ELECTRICAL | | | EXISTING FIRE HYDRANT |
| | REMOVE/ABANDON EXISTING UTILITY | | | LIMITS OF NEW PARKING LOT |

UPDATE BY: Tony Meirin
LAYOUT: PROPOSED GEOMETRY PLAN

DATE: Friday, March 10, 2023 5:38:38 PM
FILE: K:\dupage\h2102032-00_SouthsideParkingLot\Draw\Sheets\2102032-C-106.dwg



- LEGEND:**
- CONTRACT LIMITS
 - PROPOSED PCC
 - PROPOSED ASPHALT (BASE BID)
 - PROPOSED ASPHALT (ADD ALT 2)
 - PROPOSED FENCE (BY OTHERS)
 - PROPOSED BOLLARD

IL. CONTRACT: **DU091**
IL. LETTING ITEM: **07A**
IL. PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT**

GEOMETRY PLAN 1

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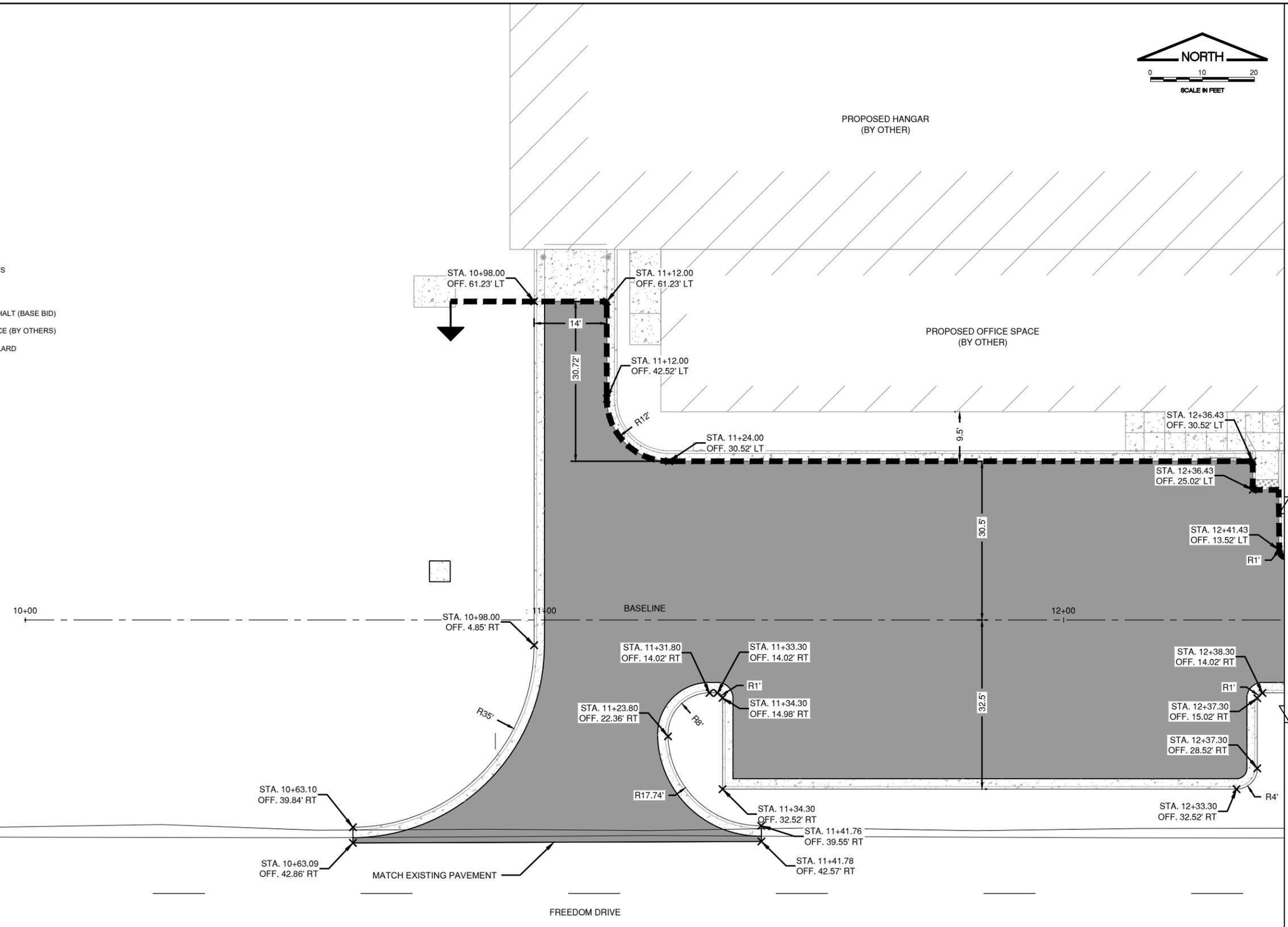
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**DUPAGE AIRPORT
AUTHORITY**

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CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21002032-00
FINAL	
SHEET	8 OF 27 SHEETS

UPDATE BY: Tony Meiri
 LAYOUT: PROPOSED GEOMETRY PLAN 2
 DATE: Friday, March 10, 2023 5:38:57 PM
 FILE: K:\Data\h2\02032-00_South\Auto\Plan\DrawSheets\2\02032-C-106-2.dwg

- LEGEND:**
- CONTRACT LIMITS
 - PROPOSED PCC
 - PROPOSED ASPHALT (BASE BID)
 - PROPOSED FENCE (BY OTHERS)
 - PROPOSED BOLLARD



MATCHLINE SEE GEOMETRY PLAN 3

IL. CONTRACT: **DU091**
 IL. LETTING ITEM: **07A**
 IL. PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
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DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT

GEOMETRY PLAN 2

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DESIGN BY:	JJM
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CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21002032-00

FINAL

SHEET 9 OF 27 SHEETS

LEGEND:

- CONTRACT LIMITS
- PROPOSED PCC
- PROPOSED ASPHALT (BASE BID)
- PROPOSED FENCE (BY OTHERS)
- PROPOSED BOLLARD



PROPOSED HANGAR
(BY OTHER)

PROPOSED OFFICE SPACE
(BY OTHER)

BASELINE

MATCH EXISTING PAVEMENT

ACCESS ROAD

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT

GEOMETRY PLAN 3

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DATE: 03/03/2023
JOB No: 21002032-00

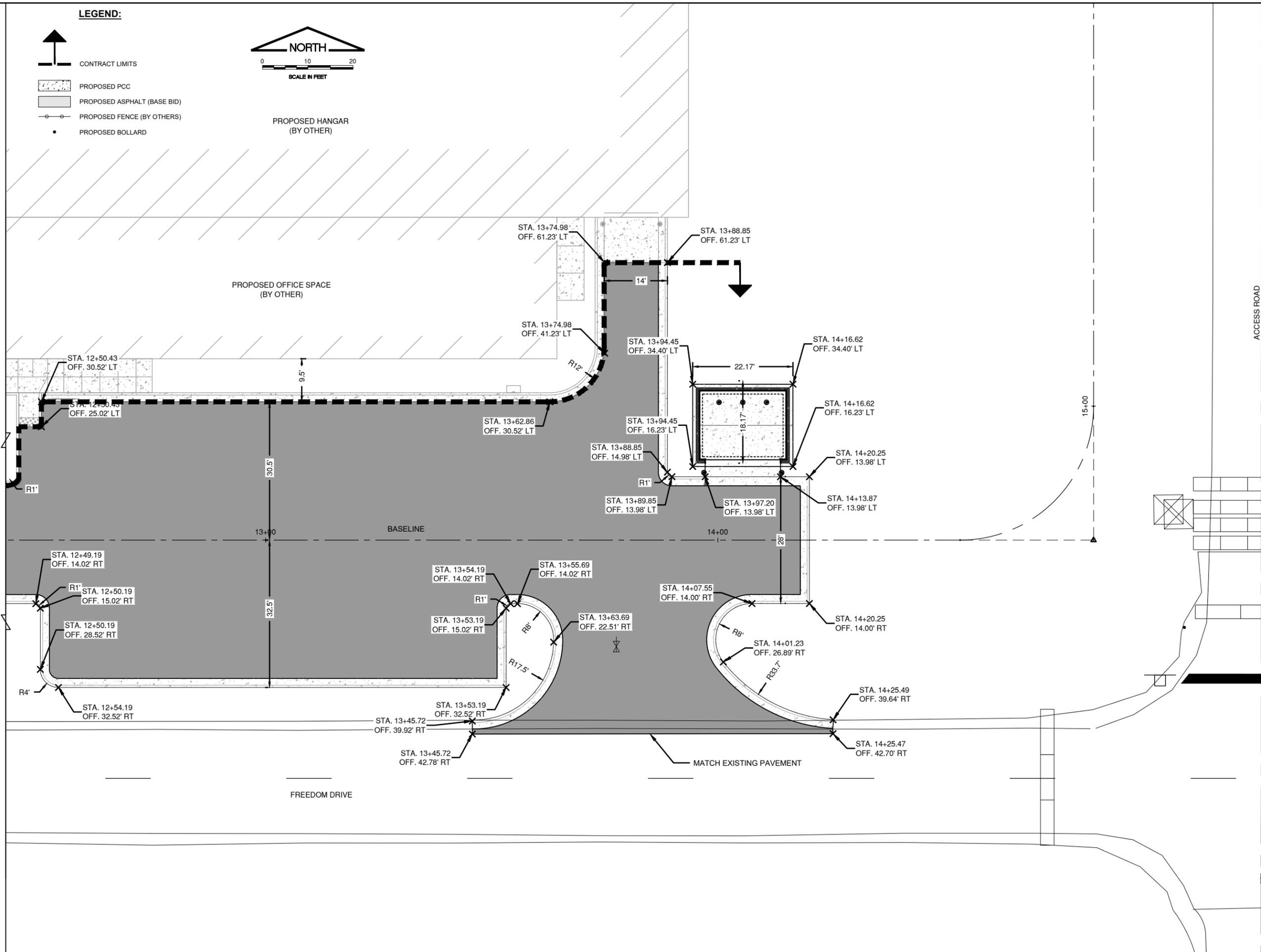
FINAL

SHEET 10 OF 27 SHEETS

UPDATE BY: Tony Meirin
LAYOUT: PROPOSED GEOMETRY PLAN 3

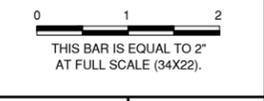
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MATCHLINE SEE GEOMETRY PLAN 2



SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



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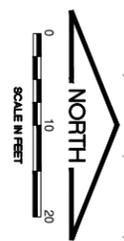
UPDATE BY: Tony Meirin
 LAYOUT: PROPOSED GEOMETRY PLAN 4

FREEDOM DRIVE

PROPOSED OFFICE SPACE
(BY OTHER)

PROPOSED HANGAR
(BY OTHER)

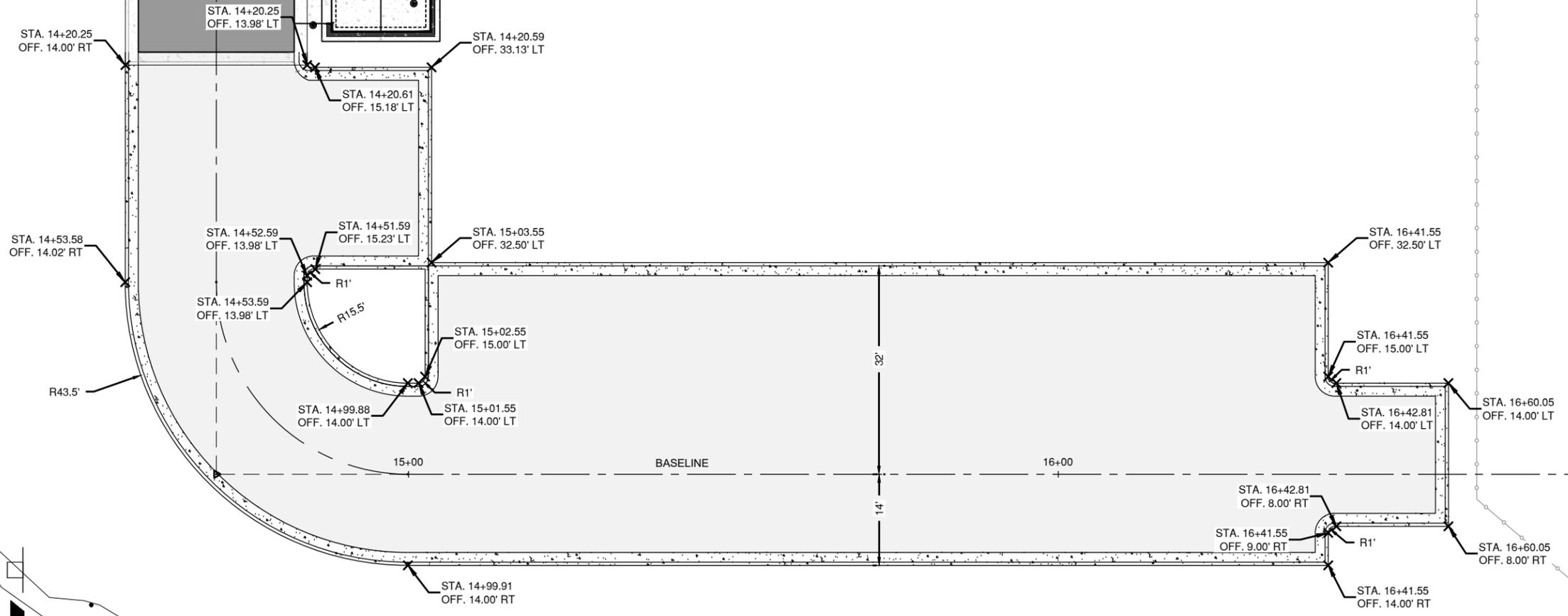
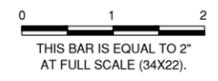
- LEGEND:**
- CONTRACT LIMITS
 - PROPOSED PCC
 - PROPOSED ASPHALT (BASE BID)
 - PROPOSED ASPHALT (ADD ALT 2)
 - PROPOSED FENCE (BY OTHERS)
 - PROPOSED BOLLARD



IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



ACCESS ROAD

DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT

GEOMETRY PLAN 4

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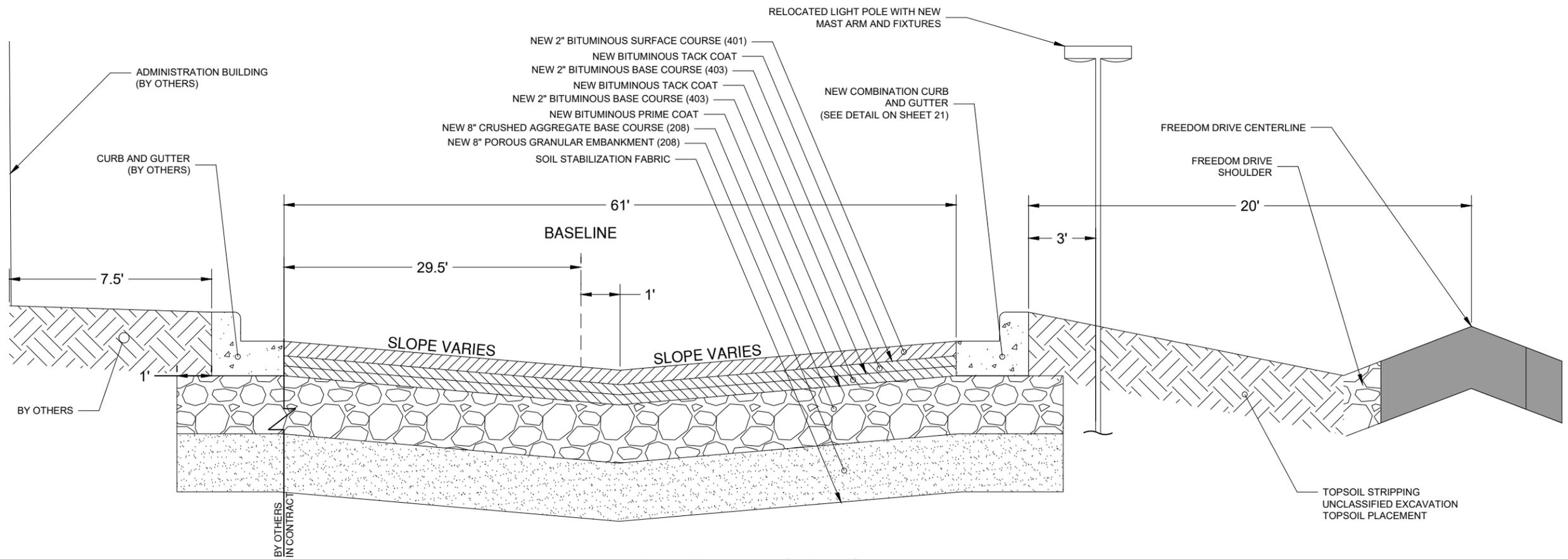
DESIGN BY: JUM
 DRAWN BY: JMM
 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 03/03/2023
 JOB No: 21002032-00

ADD ALT NO 2

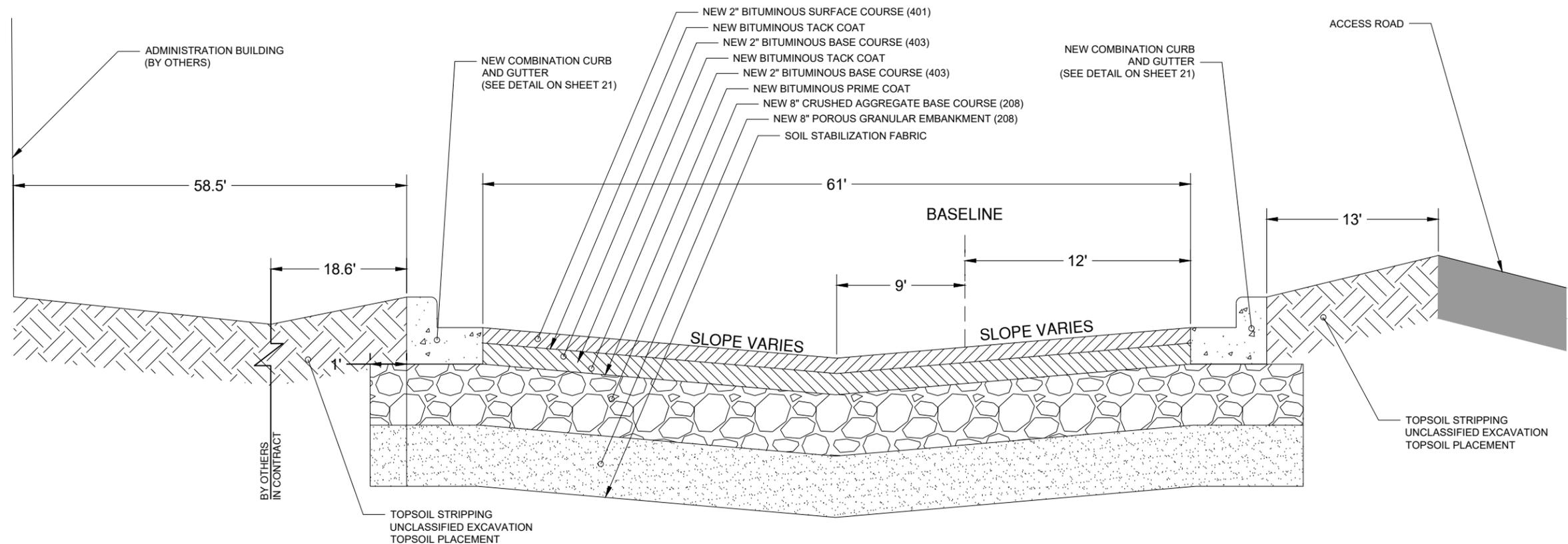
FINAL

UPDATE BY: Tony Meirin
LAYOUT: TYPICAL SECTIONS

DATE: Friday, March 10, 2023 5:39:29 PM
FILE: K:\Dodge\h2102032-00_SouthsideParkingLot\Drawn\Sheets\2102032-C-301-5302.dwg



TYPICAL SECTION A-A (BASE BID)
NOT TO SCALE



TYPICAL SECTION B-B (ADDITIVE ALTERNATIVE 2)
NOT TO SCALE

IL. CONTRACT: **DU091**
IL. LETTING ITEM: **07A**
IL. PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
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DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT

TYPICAL SECTIONS

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JOB No: 21002032-00

FINAL

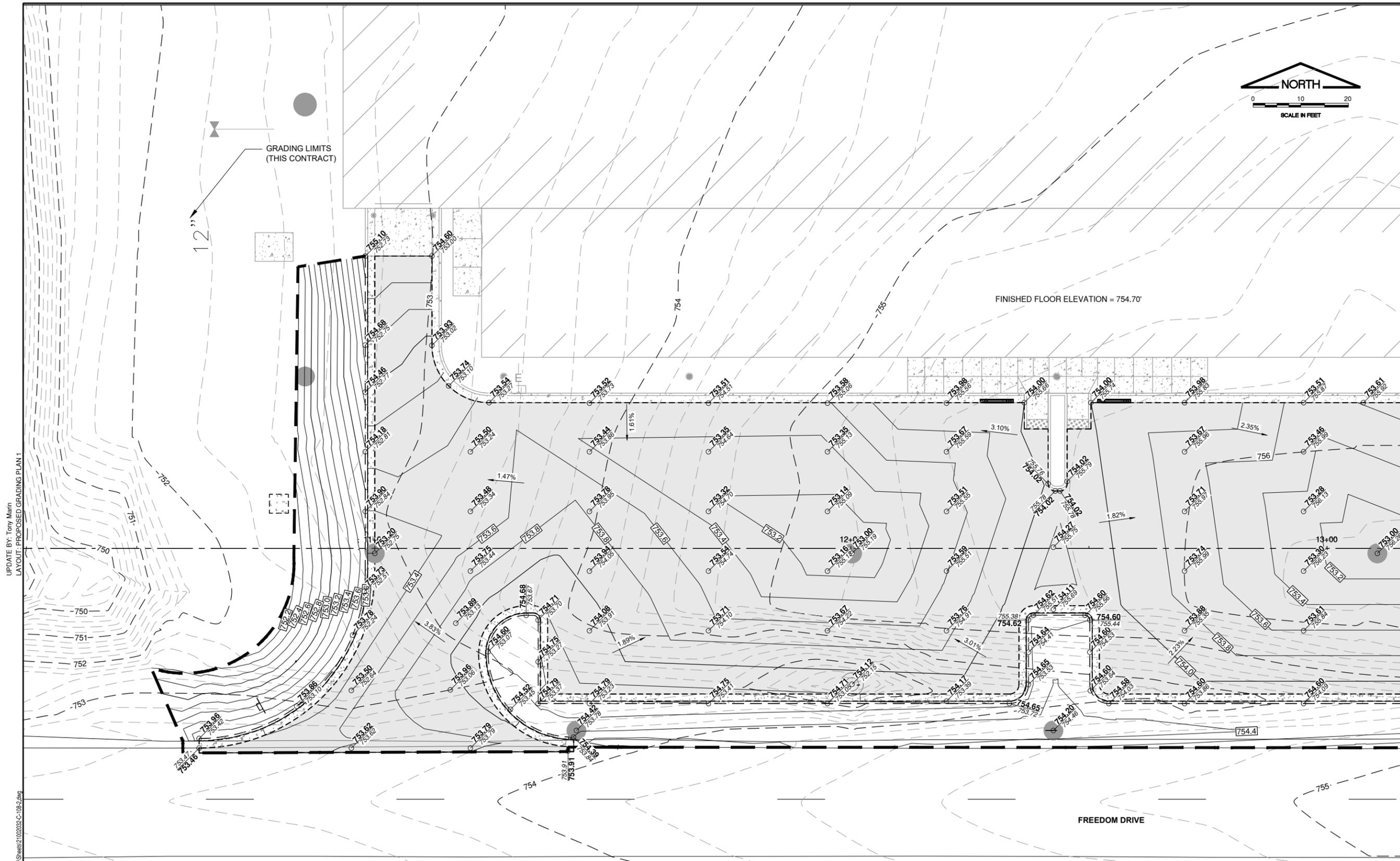
IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2
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DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT
GRADING PLAN 1

EARTHWORK			
	CUT (CYD)	FILL (CYD)	TOTAL (CYD)
BASE BID	2,833	78	2,755 CUT
WITH ADD ALT 2	4,419	78	4,346 CUT

- LEGEND:**
- PROPOSED SITE LAYOUT
 - 754.0 — PROPOSED CONTOURS
 - 754.0 - EXISTING CONTOURS
 - - - GRADING LIMITS

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DUPAGE AIRPORT AUTHORITY

DESIGN BY: JUM
 DRAWN BY: JMM
 CHECKED BY: DJK
 APPROVED BY: DLP
 DATE: 03/03/2023
 JOB No: 21002032-00

FINAL

UPDATE BY: Tony Meirin
 LAYOUT: PROPOSED GRADING PLAN 1

DATE: Friday, March 10, 2023 5:39:54 PM
 FILE: K:\Dupage\21002032-00_South\Auto\Permit\1\Draw\Sheets\21002032-C-108-2.dwg

IL CONTRACT: DU091

IL LETTING ITEM: 07A

IL PROJECT: DPA-4980

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

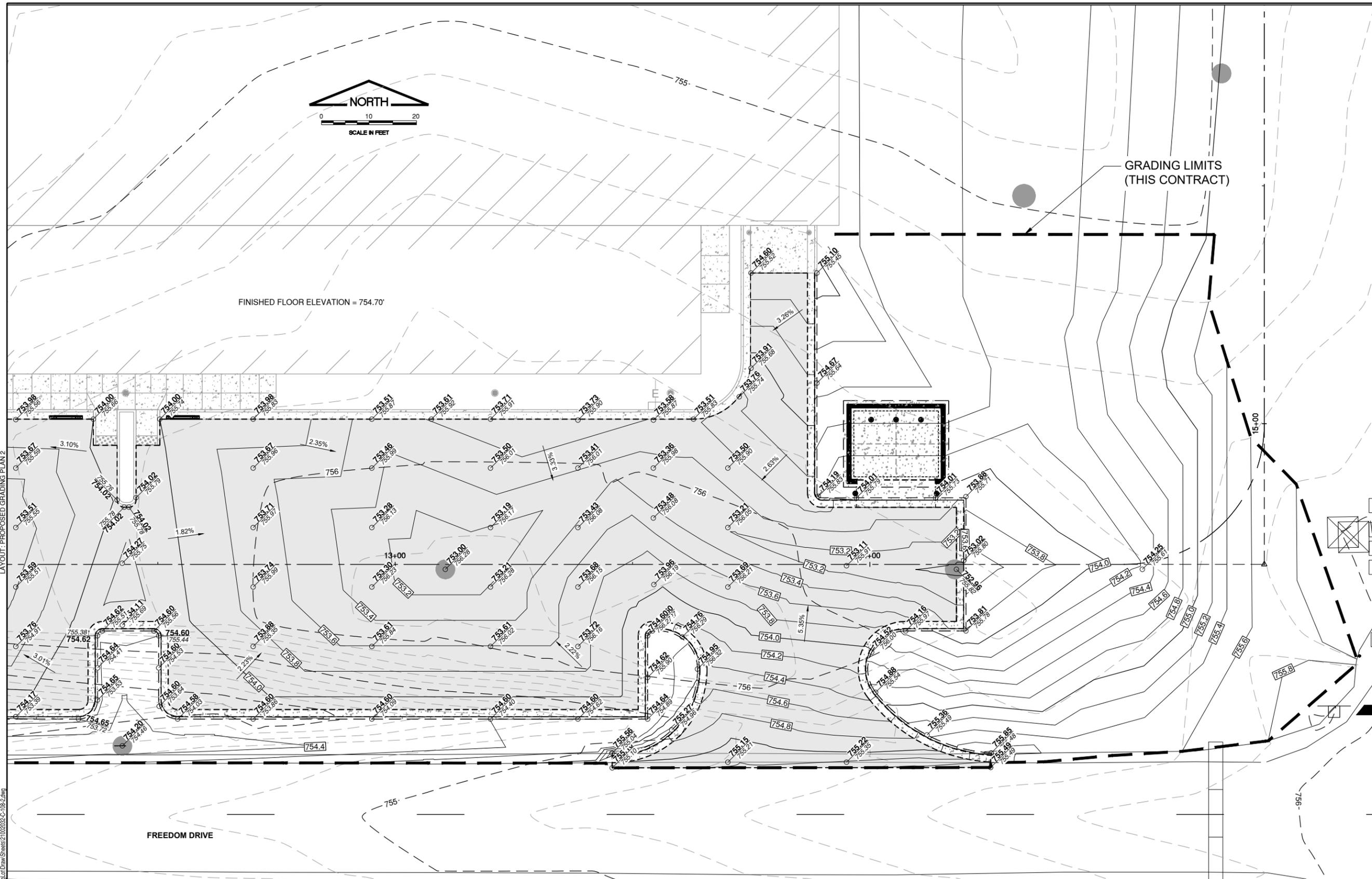


GRADING LIMITS
(THIS CONTRACT)

FINISHED FLOOR ELEVATION = 754.70'

UPDATE BY: Tony Meirin
LAYOUT: PROPOSED GRADING PLAN 2

DATE: Friday, March 10, 2023 5:40:05 PM
FILE: K:\Dpa\4980\202303-03_South\Auto\Permit\Draw\Sheets\21020302-C-108-2.dwg



DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT

GRADING PLAN 2

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NOTES:
FOR ADDITIVE PARKING GRADING, SEE GRADING PLAN 3

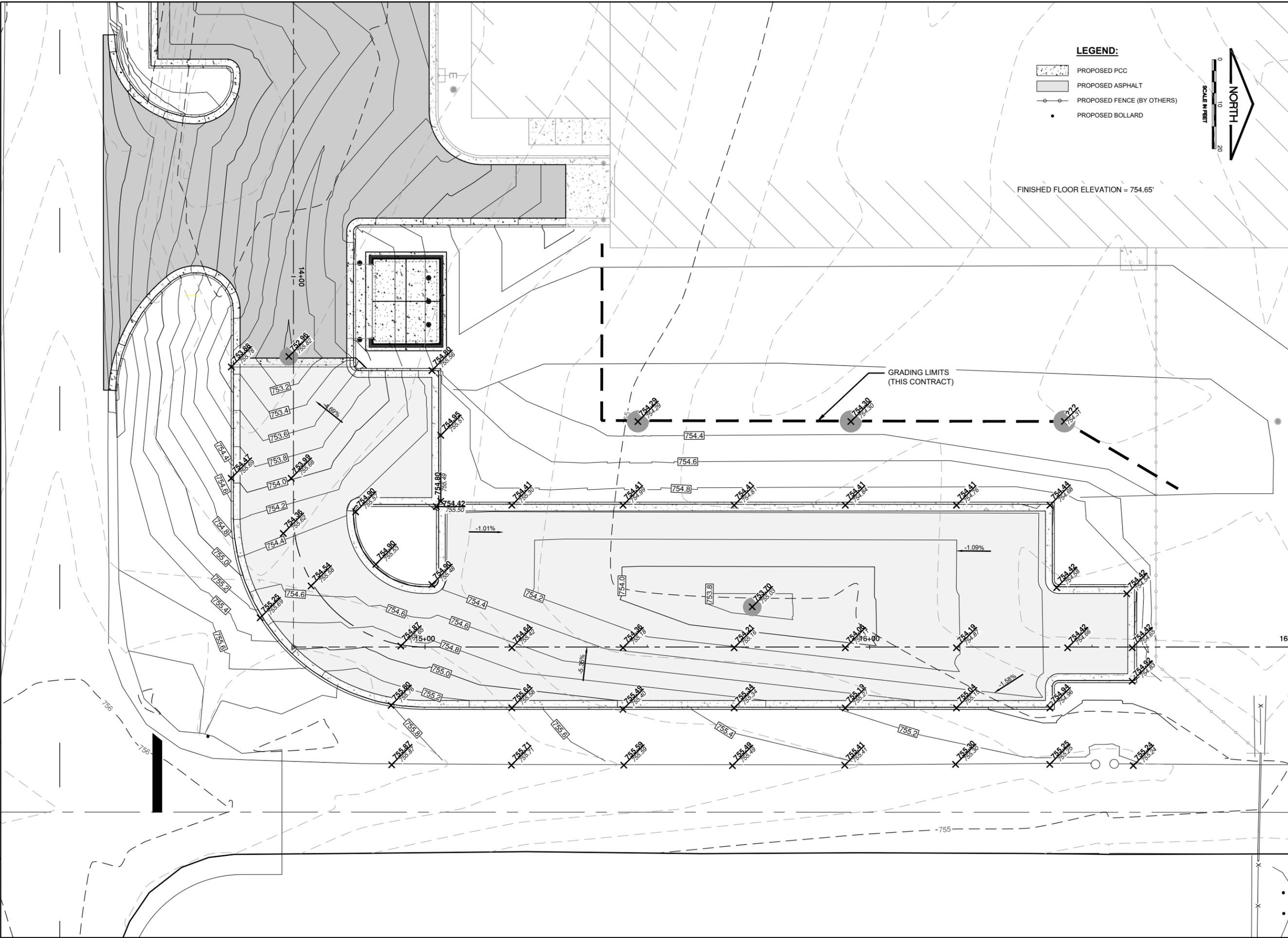
- LEGEND:**
- PROPOSED SITE LAYOUT
 - PROPOSED CONTOURS
 - 754.0 - EXISTING CONTOURS
 - CONSTRUCTION LIMITS

DESIGN BY:	JUM
DRAWN BY:	JMM
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21002032-00

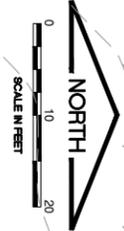
FINAL

UPDATE BY: Tony Meirin
LAYOUT: PROPOSED GRADING PLAN 3

DATE: Friday, March 10, 2023 5:40:26 PM
FILE: K:\Dugage\21002032-00_South\Auto\Permit\Draw\Sheets\21002032-C-112.dwg



- LEGEND:**
- PROPOSED PCC
 - PROPOSED ASPHALT
 - PROPOSED FENCE (BY OTHERS)
 - PROPOSED BOLLARD



FINISHED FLOOR ELEVATION = 754.65'

IL CONTRACT: **DU091**
IL LETTING ITEM: **07A**
IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

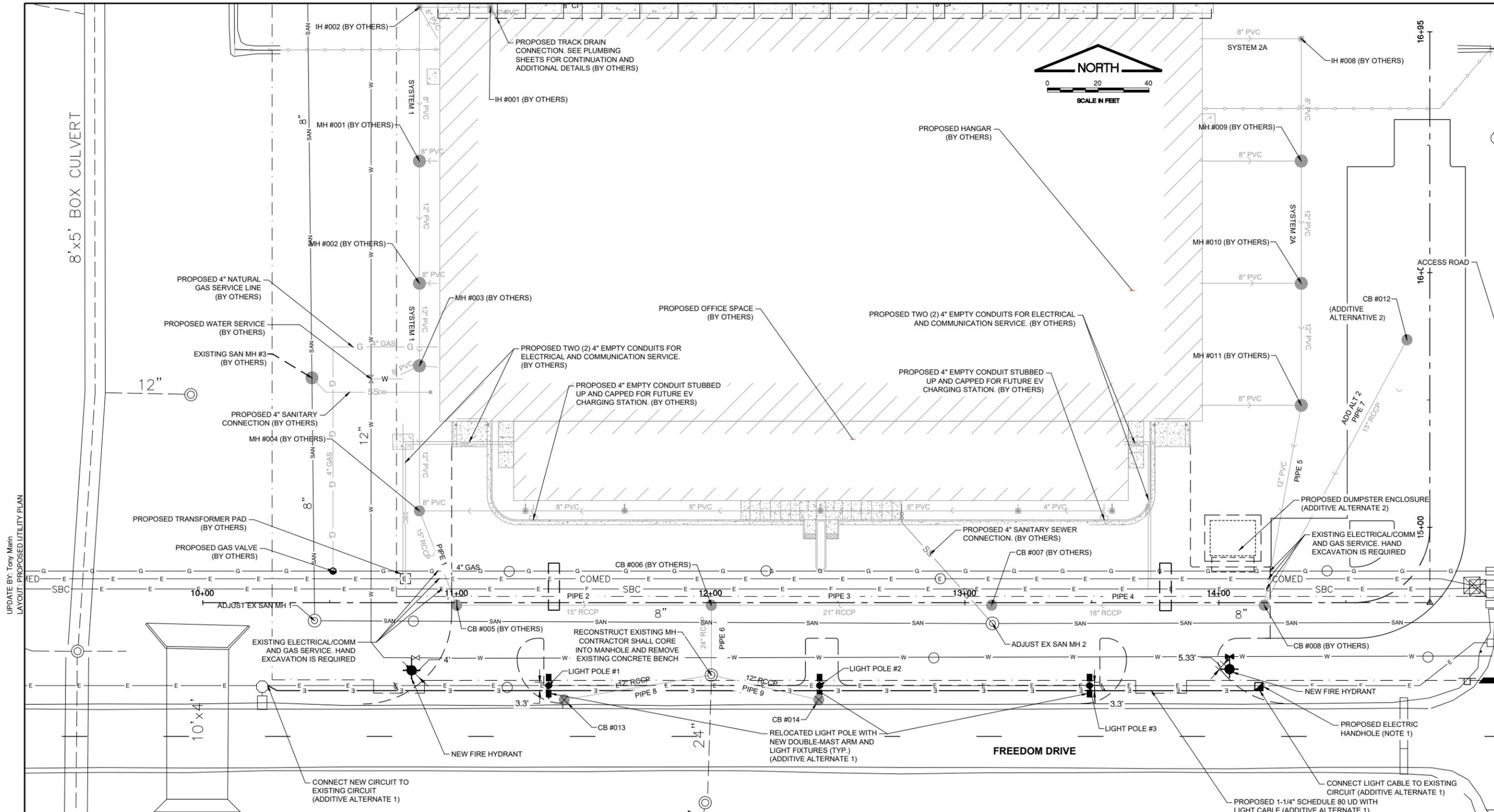
**DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT
GRADING PLAN 3**

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CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21002032-00
FINAL	
SHEET	15 OF 27 SHEETS



IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT
 UTILITY PLAN

NOTES:

- CONTRACTOR TO LOCATE EXISTING CIRCUIT AND SPLICE NEW CIRCUIT INSIDE NEW HANDHOLE.

LEGEND:

— W —	PROPOSED WATER LINE	— E —	PROPOSED ELECTRICAL - 2 - 1/C #8 XLP-USE WITH 1-1/C #8 GND IN 1 1/4" UD, DIRECT BURY CONDUIT	⊗	PROPOSED WATER VALVE
— W —	EXISTING WATER LINE	— E —	EXISTING ELECTRICAL/ COMMUNICATIONS	⊗	EXISTING WATER VALVE
— G —	PROPOSED NATURAL GAS	(4)	EXISTING CONC. ENCASED DUCT (# OF WAYS NOTED)	●	PROPOSED FIRE HYDRANT
— G —	EXISTING NATURAL GAS	■	RELOCATED LIGHT POLE FIXTURE WITH NEW MAST ARM AND TWO NEW FIXTURES	○	EXISTING FIRE HYDRANT
— SAN —	PROPOSED SANITARY SEWER	○	EXISTING LIGHT POLE	●	PROPOSED GAS VALVE
— SAN —	EXISTING SANITARY SEWER	○	PROPOSED MANHOLE	○	EXPLORATORY TRENCH (APPROX LOCATION)
— S —	PROPOSED STORM SEWER	○	EXISTING MANHOLE	○	UTILITY POTHOLE (APPROX LOCATION)
— S —	EXISTING STORM SEWER	○	PROPOSED INSPECTION HOLE	□	
— UD —	EXISTING UNDERDRAINS	■	PROPOSED ELECTRIC HANDHOLE		

UPDATE BY: Tony Meirin
 LAYOUT: PROPOSED UTILITY PLAN

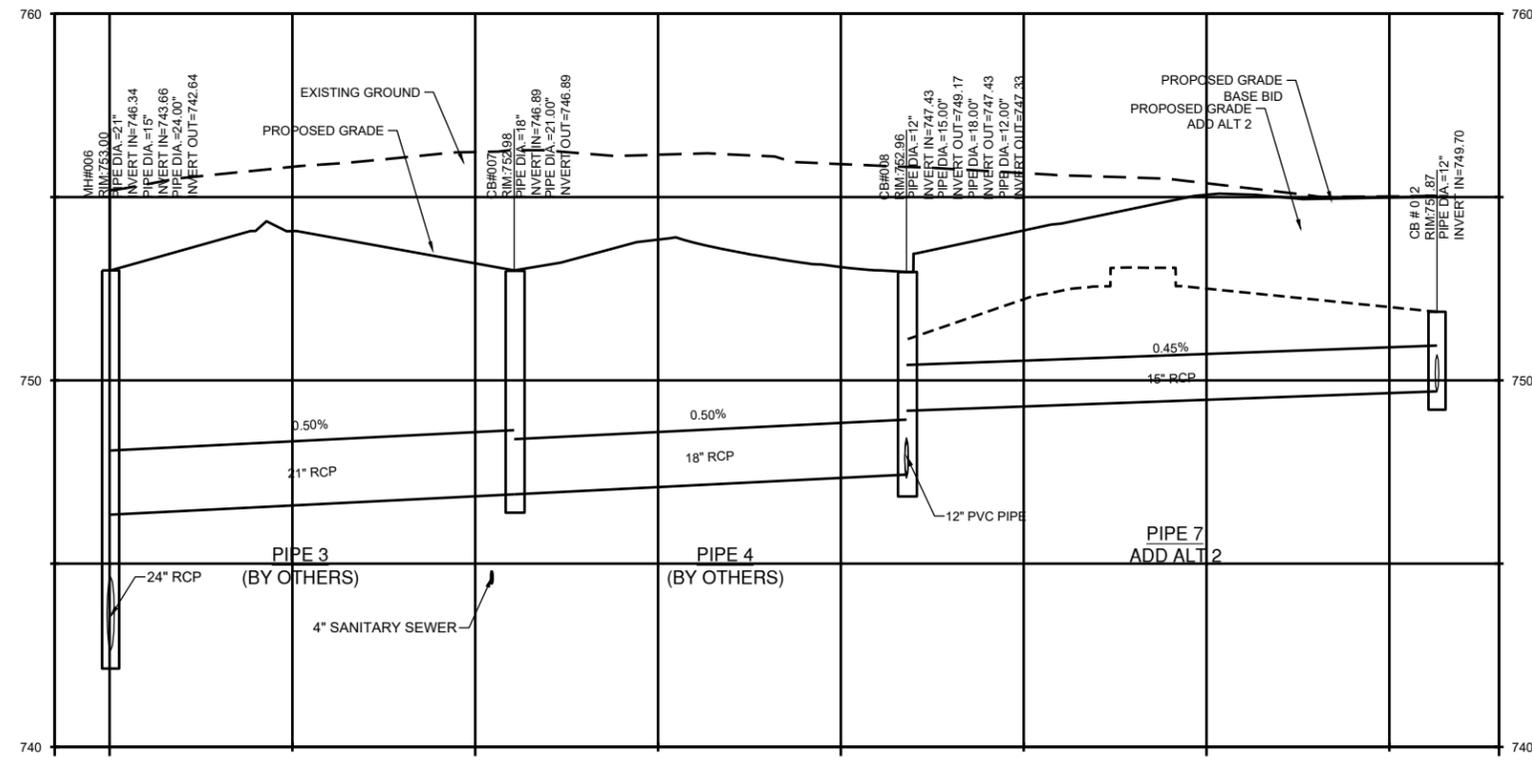
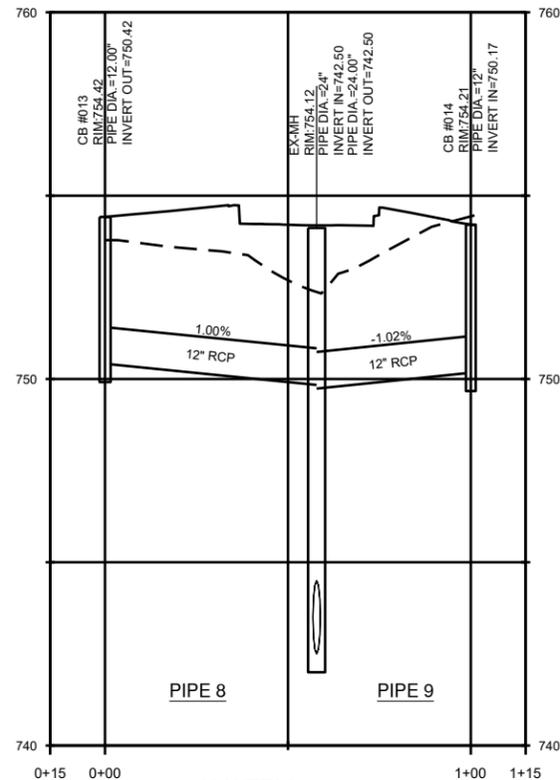
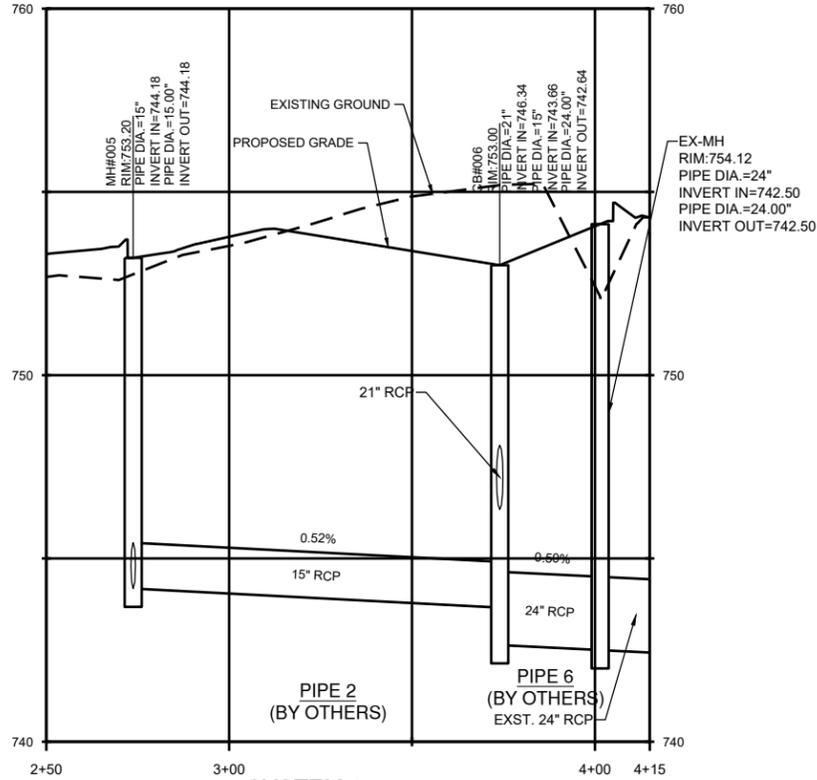
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DRAINAGE NOTES

- DURING CONSTRUCTION OPERATIONS THE CONTRACTOR SHALL ENSURE POSITIVE SITE DRAINAGE AT THE CONCLUSION OF EACH DAY. SITE DRAINAGE MAY BE ACHIEVED BY DITCHING, PUMPING OR ANY OTHER METHOD ACCEPTABLE TO THE ENGINEER.
- FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE DRAINAGE STRUCTURE COST.
- CEMENT BRICKS AND NON-SHRINK MORTAR SHALL BE USED IN ALL STORM STRUCTURES.
- BEFORE ORDERING PIPE CULVERTS, STORM SEWER, INLETS OR MANHOLES, THE CONTRACTOR SHALL VERIFY THE DEPTH OF EXISTING DOWNSTREAM STRUCTURES OR PIPES FOR CONNECTION AND EXISTING UTILITY CROSSINGS.
- DURING CONSTRUCTION OPERATIONS, WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES SO THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY.
- THE COST OF CONNECTING EXISTING STORM SEWERS TO THE PROPOSED DRAINAGE SYSTEM SHALL BE INCLUDED IN THE ORIGINAL DRAINAGE COST.
- WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS, OR CATCH BASINS. HE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS. HE SHALL BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL PERMANENT CONNECTIONS WITH THE SEWERS ARE BUILT AND IN SERVICE. DISCHARGED WATER MUST BE FILTERED TO THE SATISFACTION OF THE ENGINEER AND SHALL BE VISIBLY FREE OF SEDIMENT. THIS WORK SHALL NOT BE PAID FOR DIRECTLY, BUT WILL BE INCLUDED IN THE COST OF STORM SEWER ITEMS BEING INSTALLED.
- DRAINAGE STRUCTURE GRADES SHALL BE VERIFIED BY THE CONTRACTOR IN THE FIELD PRIOR TO INSTALLATION OF DRAINAGE ITEMS. GRADES OF EXISTING SEWER LINES WERE DETERMINED FROM AVAILABLE PLANS AND SURVEY. THE INVERTS OF THE PROPOSED DRAINAGE MAY REQUIRE REVISIONS TO MEET THE EXISTING FIELD CONDITIONS. ANY ADJUSTMENTS SHALL BE DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE OF THE PROPOSED STORM SEWER AND STRUCTURES.
- BEFORE FINAL ACCEPTANCE OF THE PROJECT, ALL PROPOSED AND EXISTING STORM SEWER LINES AND STRUCTURES AFFECTED BY PROJECT LIMIT SHALL BE CLEANED AS DIRECTED BY THE ENGINEER. CLEANING OF THE PROPOSED AND EXISTING STORM SEWER LINES AND STRUCTURES IS CONSIDERED TO BE INCLUDED IN THE COST OF THE DRAINAGE ITEM.
- WHEN STORM SEWER IS CONSTRUCTED NEAR WATER MAIN, SEPARATION REQUIREMENTS SHALL BE MET IN ACCORDANCE WITH THE ILLINOIS STANDARD SPECIFICATIONS FOR WATER AND SEWER CONSTRUCTION.
- THE CONTRACTOR SHALL VERIFY ALL ELEVATIONS AND FLOWLINES PRIOR TO ORDERING NEW MANHOLE AND INLET STRUCTURES.
- IF DURING MANHOLE ADJUSTMENTS, THE CONTRACTOR DAMAGES EITHER THE FRAME OR LID, THEY SHALL FURNISH A NEW FRAME AND LID, SAME OR EQUAL, AT NO ADDITIONAL COST. ALL NEW MANHOLES AND INLETS SHALL BE CONSTRUCTED WITH NEW FRAMES AND GRATES.
- NEW MANHOLE AND INLET LIDS ON THIS PROJECT SHALL HAVE THE WORD "STORM" ON THE LID. CITY OF ROCKFORD LOGO LIDS SHALL BE USED ON ALL CASTINGS. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN THE APPROPRIATE TYPE OF LID. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK.
- THE CONTRACTOR SHALL CALL JULIE AT 811 OR 1-800-892-0123 48 HOURS PRIOR TO CONSTRUCTION FOR CONFIRMATION OF CURRENT UTILITY LOCATIONS AND FOR ALL NON-EMERGENCY WORK. THESE ARE THE KNOWN UTILITIES LOCATED WITHIN THE PROJECT LIMITS OR IMMEDIATELY ADJACENT TO THE PROJECT CONSTRUCTION LIMITS. UTILITIES WHICH ARE NOT MEMBERS OF JULIE SHOULD BE NOTIFIED INDIVIDUALLY BY THE CONTRACTOR.
- ALL TESTING, FITTINGS, BEDDING AND GRANULAR CRADLE WHERE NECESSARY, SHALL BE INCLUDED IN THE INSTALLATION OF UNDERGROUND FACILITIES. TRENCH BACKFILL IS REQUIRED WHEREVER UNDERGROUND PIPING AND UTILITIES PASS BENEATH OR WITHIN 2 FEET OF THE PAVEMENT, SIDEWALK OR CURB.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRS TO ANY UTILITY LINES AND EXISTING IMPROVEMENTS TO REMAIN THAT ARE DAMAGED AS A RESULT OF THE WORK.

IL. CONTRACT: **DU091**
 IL. LETTING ITEM: **07A**
 IL. PROJECT: **DPA-4980**

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DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT
STORM SEWER PROFILES

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UPDATE BY: Tony Meirn
LAYOUT: STORM AND LIGHTING SCHEDULE

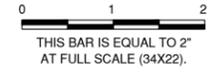
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IL CONTRACT: **DU091**
IL LETTING ITEM: **07A**
IL PROJECT: **DPA-4980**

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NUMBER	BY	DATE



STRUCTURE SCHEDULE										
STRUCTURE	STATION	OFFSET	NORTHING	EASTING	RIM ELEVATION	INVERT ELEVATION	TYPE	SIZE	FRAME AND GRATE LID BASE BID	FRAME AND GRATE LID ADD ALT 2
ADJUST EX SAN MH 1	10+44.10	7.122	1907397.2650	1004333.7970	751.03	INV (N) = 739.37 INV (E) = 739.38	EXISTING	EXISTING	EXISTING	EXISTING *
ADJUST EX SAN MH 2	13+10.92	8.213	1907396.1740	1004600.6170	753.73	INV (E) = 740.49 INV (W) = 740.47	EXISTING	EXISTING	EXISTING	EXISTING *
CB #005 (BY OTHERS)	11+00	1.000	1907403.3373	1004389.6985	0.00	INV (E) = 744.18 INV (N) = 744.18	EXISTING	EXISTING	TYPE 23 FRAME OPEN LID	TYPE 23 FRAME OPEN LID *
CB #006 (BY OTHERS)	12+00	1.000	1907403.3307	1004489.9414	753.50	INV (E) = 746.34 INV (W) = 743.66 INV (S) = 742.64	EXISTING	EXISTING	TYPE 1 FRAME OPEN LID	TYPE 1 FRAME OPEN LID *
CB #007 (BY OTHERS)	13+10.58	1.000	1907403.3434	1004600.2793	753.50	INV (E) = 746.89 INV (W) = 746.89	EXISTING	EXISTING	TYPE 1 FRAME OPEN LID	TYPE 1 FRAME OPEN LID *
CB #008 (BY OTHERS)	14+17.90	1.000	1907403.3432	1004707.5949	753.92	INV (E) = 748.47 INV (W) = 747.43 INV (N) = 747.43	EXISTING	EXISTING	TYPE 23 FRAME OPEN LID	TYPE 1 FRAME OPEN LID *
CB #012	15+73.60	-9.000	1907507.5661	1004763.7847	753.70	INV (SW) = 749.17	A	4	N/A	TYPE 1 FRAME OPEN LID
CB #013	11+42.35	38.152	1907366.2352	1004432.0482	754.42	INV (E) = 750.42	A	2	TYPE 8 GRATE	TYPE 8 GRATE
CB #014	12+42.53	18.152	1907366.2352	1004532.2330	754.20	INV (W) = 750.17	A	2	TYPE 8 GRATE	TYPE 8 GRATE
EXISTING SAN MH #3	12+42.53	18.152	1907492.6344	1004332.7119	752.96	INV (SE) = 748.67 INV (S) = 738.95 INV (N) = 738.95	EXISTING	EXISTING	EXISTING	EXISTING *
MH #001 (BY OTHERS)	10+85.35	-173.382	1907577.7687	1004375.0446	754.54	INV (N) = 749.44 INV S = 749.44 INV (E) = 749.82	EXISTING	EXISTING	EXISTING	EXISTING *
MH #002 (BY OTHERS)	10+85.35	-125.359	1907529.7463	1004375.0446	754.52	INV (N) = 749.20 INV (S) = 746.52 INV (E) = 749.82	EXISTING	EXISTING	EXISTING	EXISTING *
MH #003 (BY OTHERS)	10+85.35	-92.977	1907497.3644	1004375.0446	754.58	INV (N) = 746.36 INV (S) 746.36 INV (E) = 749.89 INV (SW) = 749.89	EXISTING	EXISTING	EXISTING	EXISTING *
MH #004 (BY OTHERS)	10+85.35	-36.000	1907440.3885	1004375.0446	754.13	INV (N) = 746.07 INV (E) = 748.92 INV (S) = 744.37	EXISTING	EXISTING	EXISTING	EXISTING *
MH #009 (BY OTHERS)	16+43.81	-50.616	1907577.7687	1004722.1685	754.31	INV (N) = 749.42 INV (S) = 749.42 INV (W) 749.63	EXISTING	EXISTING	EXISTING	EXISTING *
MH #010 (BY OTHERS)	15+95.78	-50.614	1907529.7490	1004722.1717	754.30	INV (N) = 749.18 INV (S) = 749.18 INV (W) = 749.62	EXISTING	EXISTING	EXISTING	EXISTING *
MH #011 (BY OTHERS)	15+47.89	-50.611	1907481.8491	1004722.1721	754.29	INV (N) = 748.94 INV (S) = 747.83 INV (W) = 749.60	EXISTING	EXISTING	EXISTING	EXISTING *

SCHEDULE NOTES:

- NORTHING AND EASTING FOR MANHOLES IS TO CENTER OF STRUCTURE.
 - D/S: DOWNSTREAM.
 - U/S: UPSTREAM.
 - LENGTH OF PIPE FROM MANHOLE TO MANHOLE IS FROM CENTER OF STRUCTURE.
 - RCCP: REINFORCED CONCRETE CIRCULAR PIPE, CLASS IV.
 - PVC: POLYVINYL CHLORIDE PIPE, SDR 26.
 - MANHOLES SHALL BE IDOT STANDARD 602401-07.
 - CONTRACTOR SHALL VERIFY RIM AND INVERT ELEVATIONS ON EXISTING DRAINAGE STRUCTURES THAT ARE TO BE CONNECTED TO, ADJUSTED OR RECONSTRUCTED BEFORE ORDERING MATERIAL (INCIDENTAL TO CONTRACT).
 - FRAME AND LIDS SHALL BE IDOT STANDARD 604001-05, 604036-03 AND 604086-05.
 - EXISTING STRUCTURES TO BE ADJUSTED OR RECONSTRUCTED SHALL SALVAGE AND REUSE EXISTING FRAMES AND GRATES OR PROVIDE NEW FRAMES AS INDICATED.
- * INSTALLED BY OTHERS. ADJUST AS NECESSARY TO MEET FINAL GRADES

PIPE SCHEDULE					
PIPE NAME	UPSTREAM INLET	DOWNSTREAM INLET	PIPE TYPE	LENGTH	SLOPE
PIPE 1	MH #004	CB #005	15" RCCP	40	0.48% *
PIPE 2	CB #006	CB #005	15" RCCP	105	0.50% *
PIPE 3	CB #007	CB #006	21" RCCP	110	0.50% *
PIPE 4	CB #008	CB #007	18" RCCP	107	0.50% *
PIPE 5	MH #011	CB #008	12" PVC	80	0.49% *
PIPE 6	CB #006	EX MH	24" RCCP	25	0.56% *
PIPE 7	CB #012	CB #008	15" RCCP	118	0.50% **
PIPE 8	CB #013	EX MH	12" RCCP	58	1.00%
PIPE 9	CB #014	EX MH	12" RCCP	42	1.02%

NOTE: *BY OTHERS
**ADD ALT 2

LIGHTING SCHEDULE (ADD ALT 1)					
STRUCTURE	STATION	OFFSET	NORTHING	EASTING	GROUND ELEVATION
LIGHT POLE #1	11+41.18	36.5	1907371.8029	1004425.9252	754.55
LIGHT POLE #2	12+47.81	36.5	1907371.7984	1004532.5574	754.85
LIGHT POLE #3	13+54.05	36.5	1907371.7921	1004638.7943	754.64

NOTE: CENTER OF POLE 3.3' OFF BACK OF CURB

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT

STORM AND LIGHTING SCHEDULE

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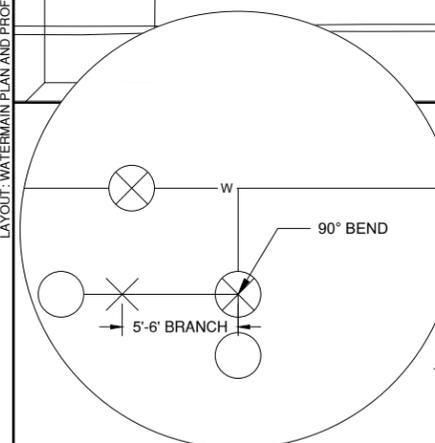
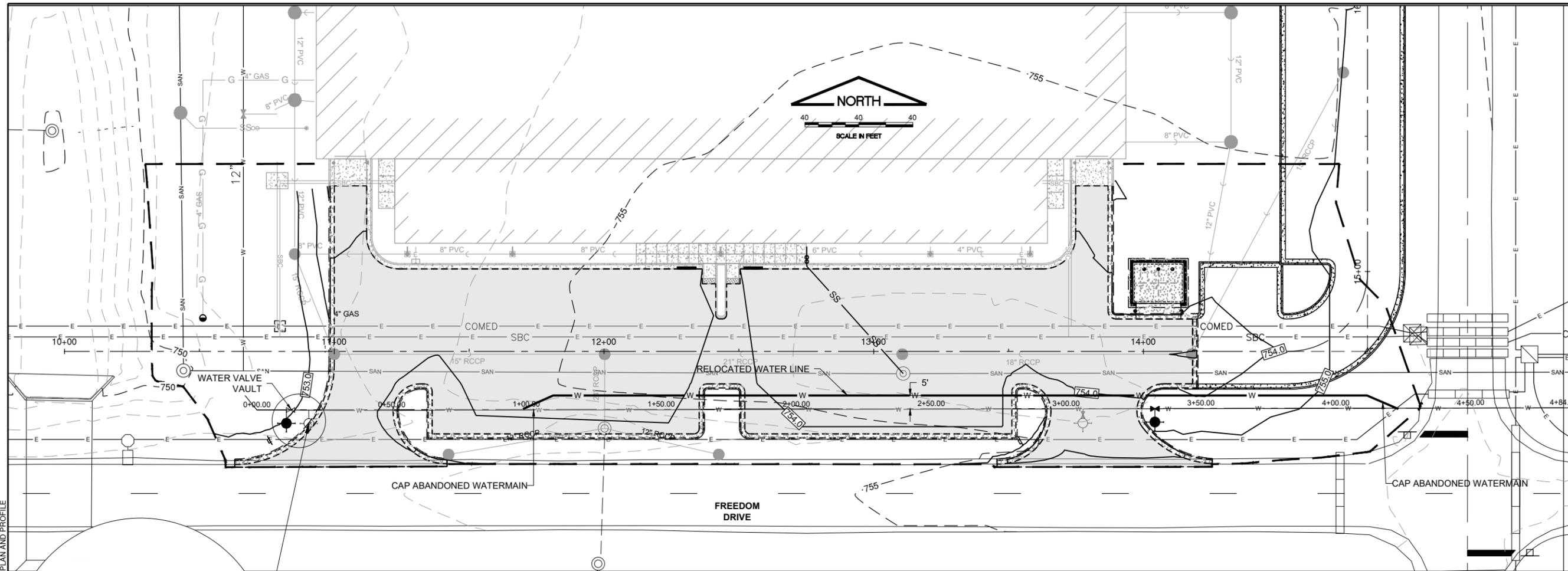
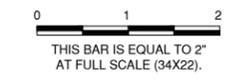
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IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DPA-4980**

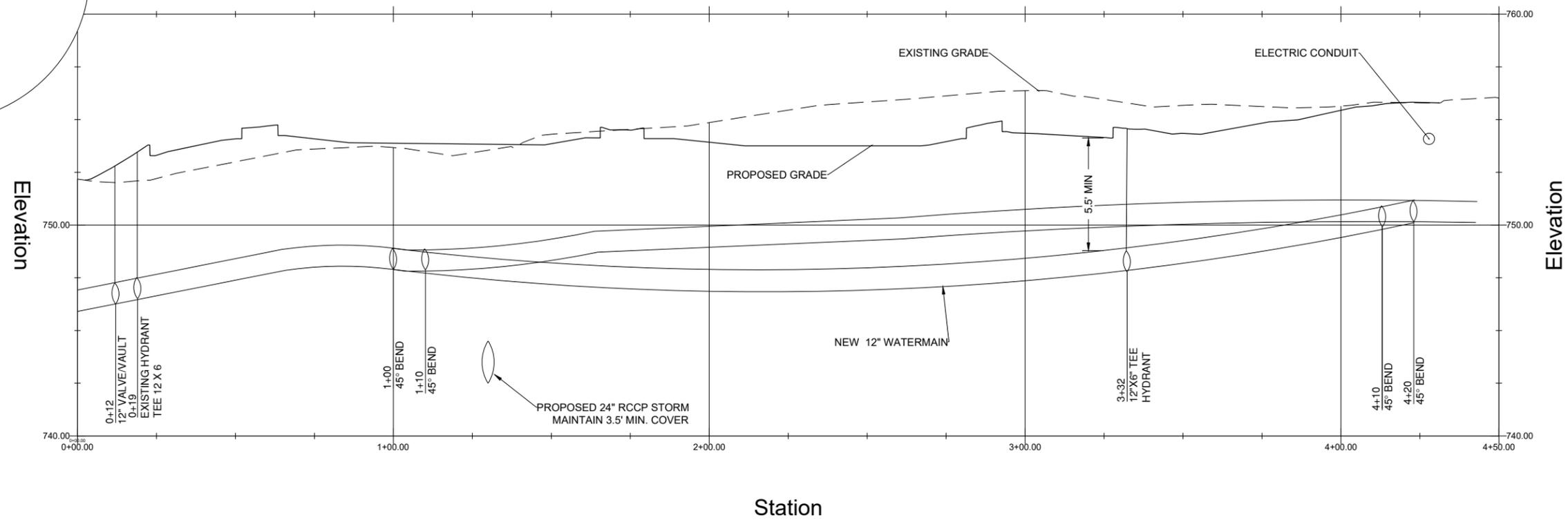
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- CLOSE 6" VALVE AUX VALVE
- REMOVE HYDRANT AND BRANCH FROM AUX VALVE
- EXTEND A NEW 6" BRANCH FROM THE EXISTING AUX VALVE AND INSTALL A NEW HYDRANT AND 6" AUX VALVE

- NOTE:
1. EXISTING WATERMAIN CANNOT BE SHUT DOWN FOR EXTENDED PERIOD
 2. INSTALL NEW WATER MAIN, PRESSURE TEST AND CHLORINATE PRIOR TO CONNECTING TO EXISTING WATERMAIN
 3. AFTER PASSING PRESSURE TEST AND OBTAINING SAFE SAMPLE. CONNECT NEW WATERMAIN TO EXISTING WATERMAIN
 4. CAP ABANDONED WATERMAIN



UPDATE BY: Tony Meiri
 LAYOUT: WATERMAIN PLAN AND PROFILE

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DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT

WATERMAIN PROFILE

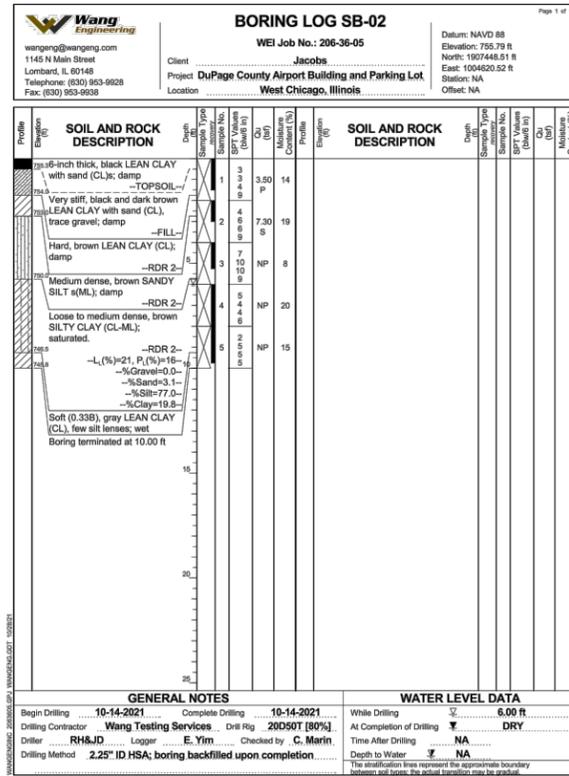
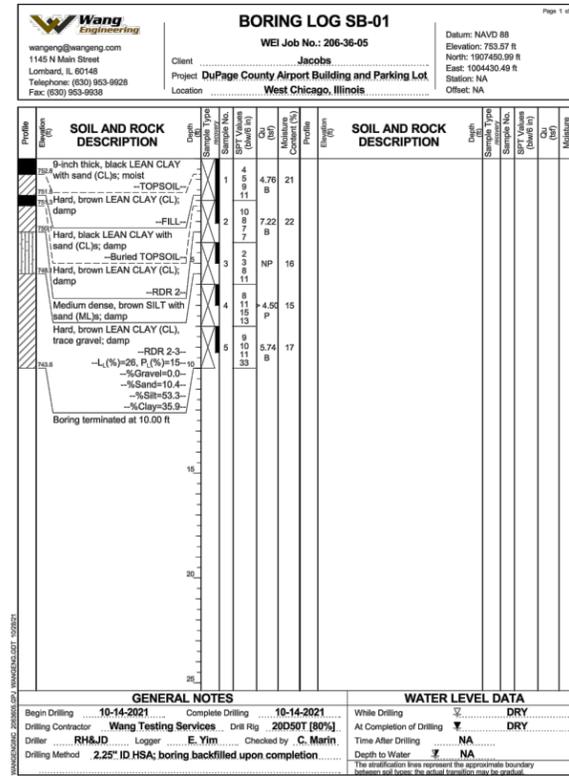
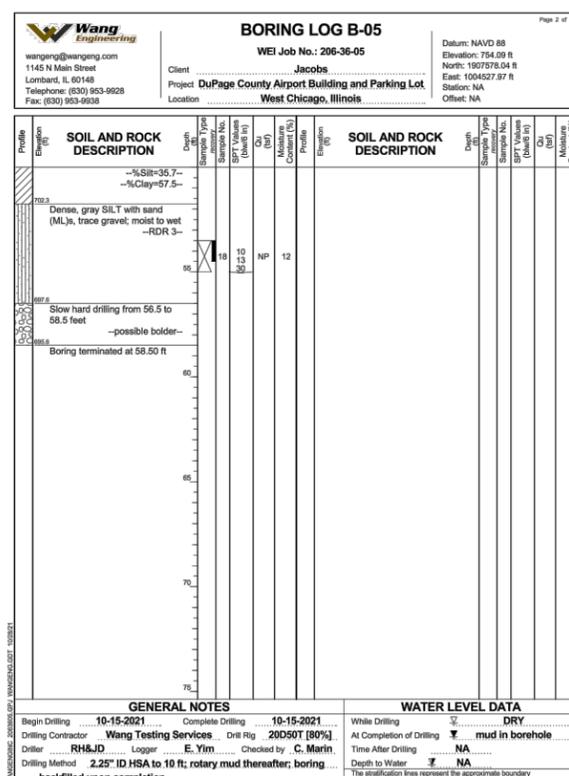
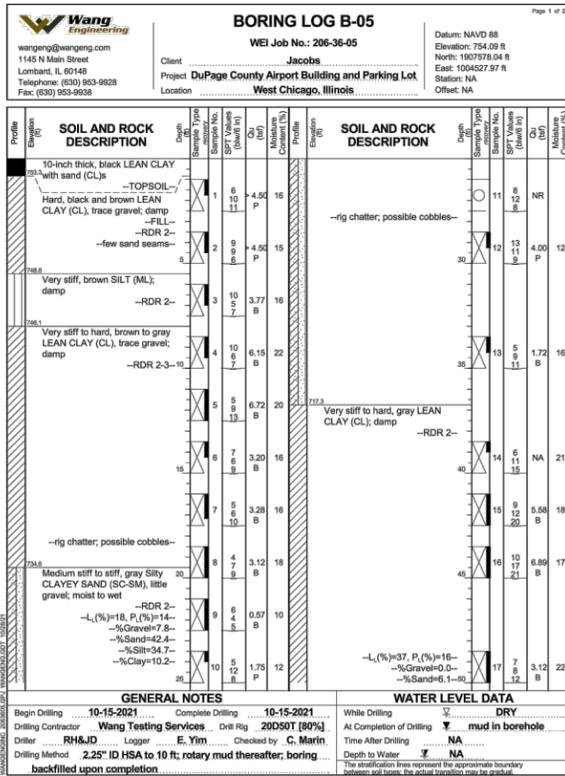
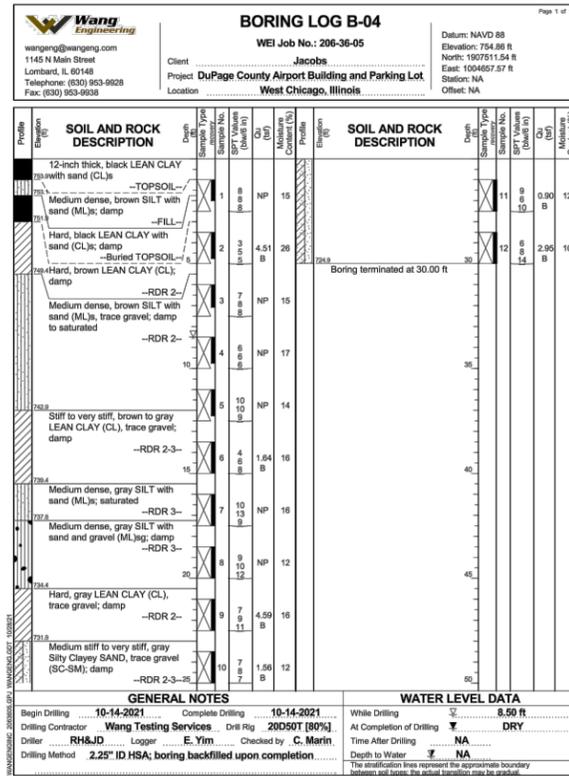
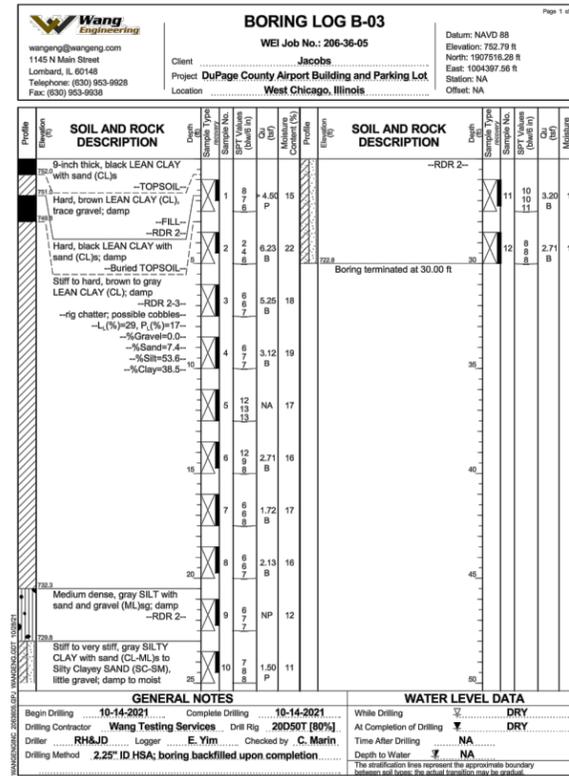
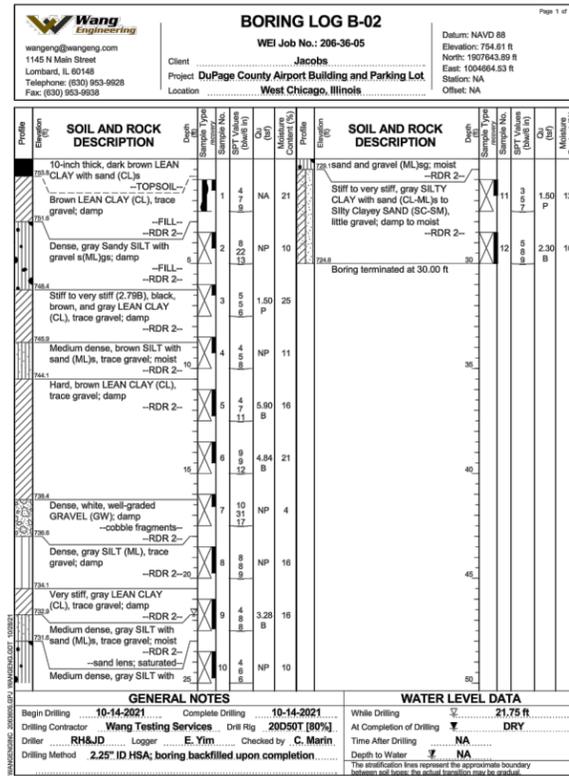
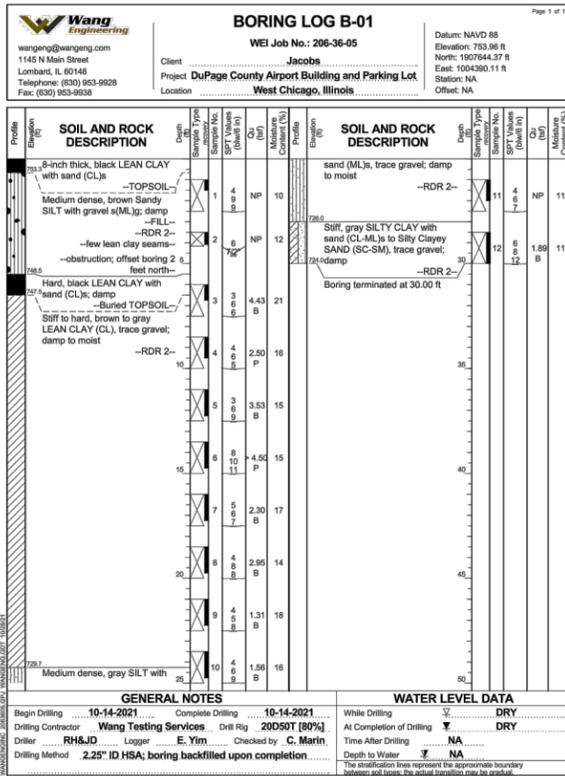
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IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DP A-4980**

SURVEY BOOK #

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0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT
 BORING LOGS

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 SHEET 20 OF 27 SHEETS

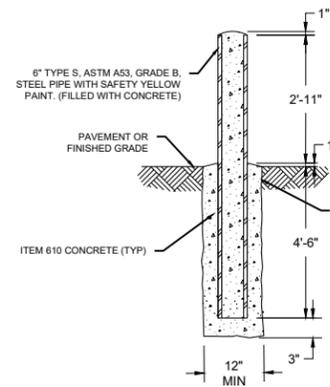
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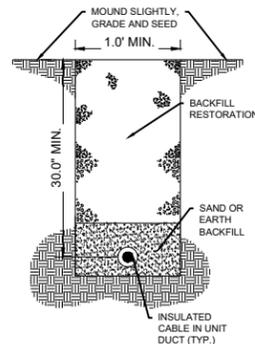
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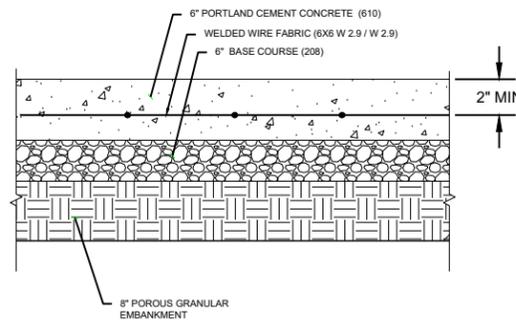


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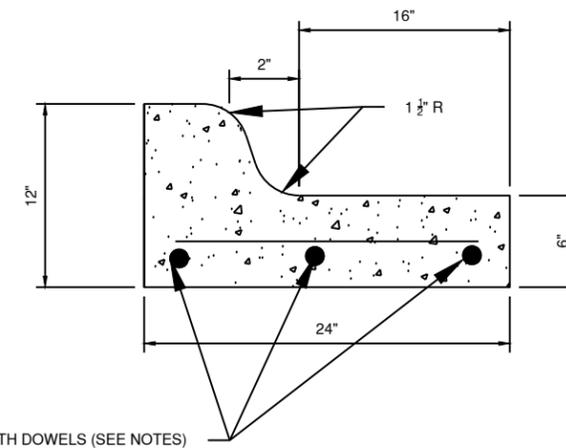


CABLE TRENCH DETAIL
NOT TO SCALE

NOTE:
1. TRENCHES WITH MORE THAN 2 CABLES SHALL BE INCREASED 3" IN WIDTH FOR EACH ADDITIONAL CABLE.

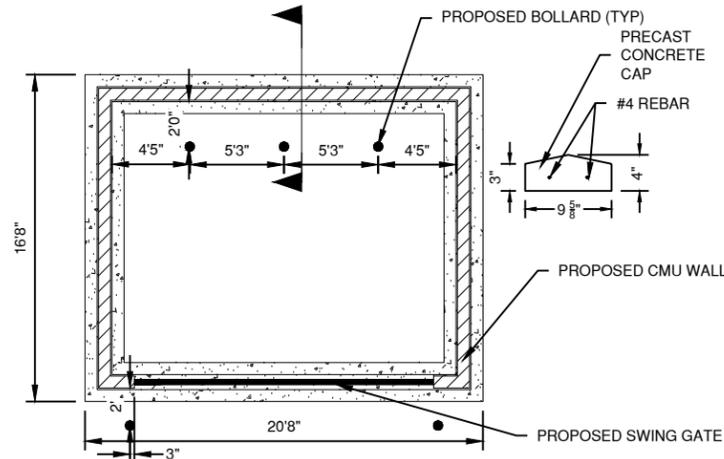


DUMPSTER PAD PAVEMENT (PPC)
NOT TO SCALE

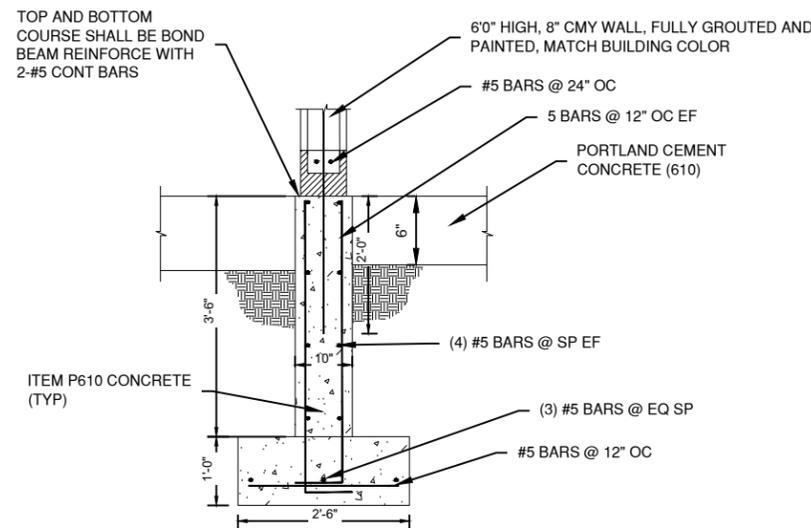


#5 X 24" SMOOTH DOWELS (SEE NOTES)

CURB AND GUTTER
NOT TO SCALE

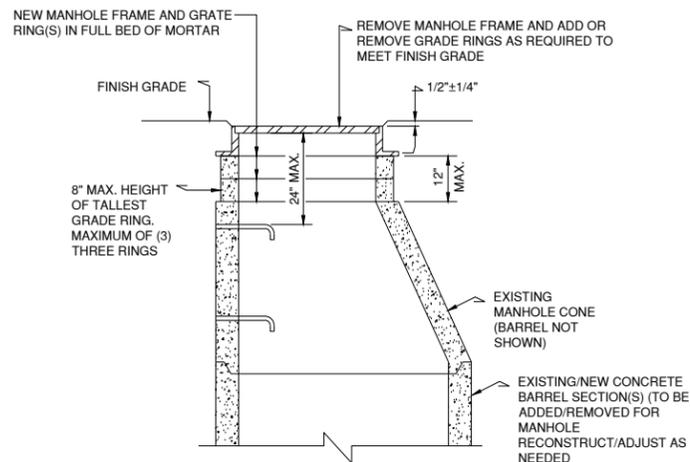


DUMPSTER ENCLOSURE PLAN
NOT TO SCALE



DUMPSTER FOUNDATION SECTION
NOT TO SCALE

NOTES:
1. SEE SHEET 14 FOR ELEVATIONS AT TOP OF PAVEMENT AT DUMPSTER AREA.



ADJUST/RECONSTRUCT MANHOLE
NOT TO SCALE

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT

CIVIL DETAILS 1

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CIVIL DETAILS 3

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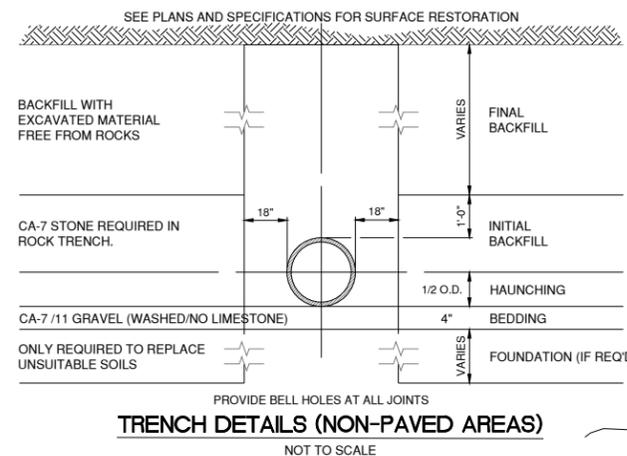
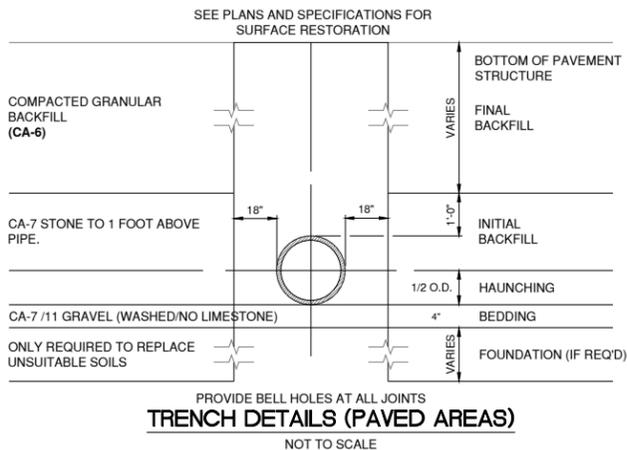


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DESIGN BY:	JJM
DRAWN BY:	JMM
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21002032-00

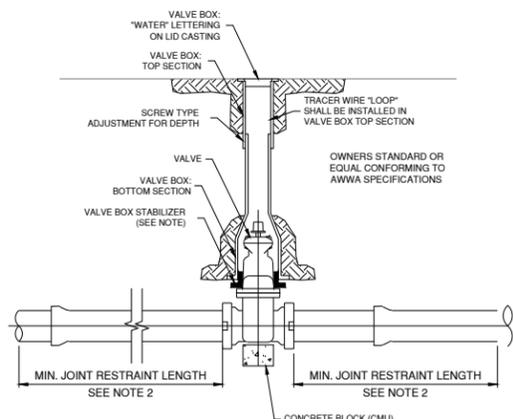
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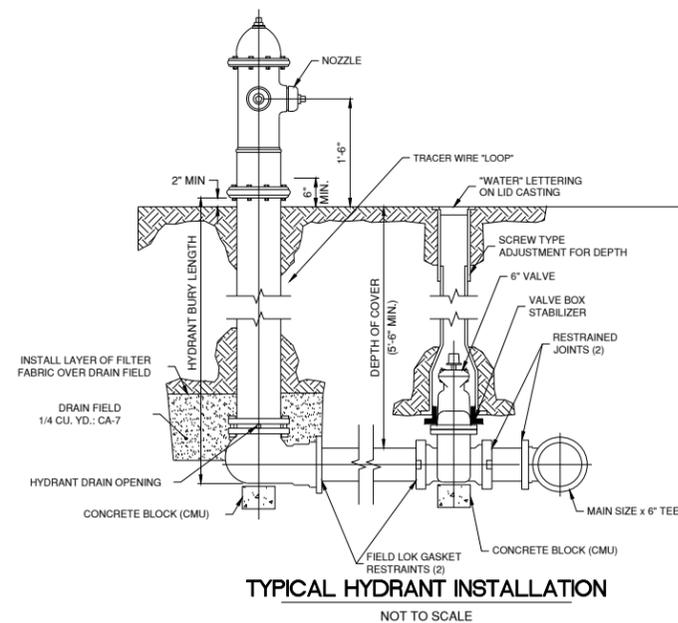
INSIDE DIAMETER OF CONDUIT IN INCHES "D"	MAXIMUM TRENCH WIDTH IN FEET FOR PAYMENT	MAXIMUM PAVEMENT REMOVAL WIDTH IN FEET FOR PAYMENT
6	3.58	5.58
8	3.78	5.78
10	3.97	5.97
12	4.17	6.17
14	4.36	6.36
15	4.46	6.46
16	4.56	6.56
18	4.75	6.75
20	4.94	6.94
21	5.04	7.04
24	5.33	7.33

FROM STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS 5TH EDITION

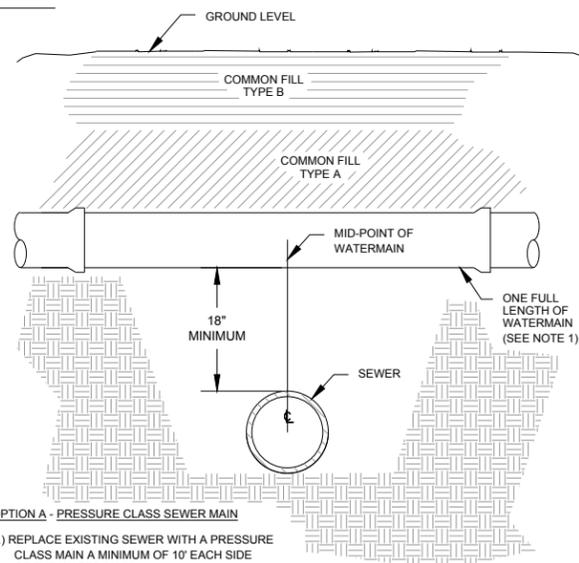
- NOTES:
- 1.) TRACER WIRE INTEGRAL TO PIPING OR MECHANICALLY FASTENED TO PIPING TERMINATED AT VALVE BOX.
 - 2.) TRENCH DETAIL (PAVED AREAS) SHALL APPLY TO WATERMAIN CONSTRUCTION WITHIN 5' OF STREETS, CURBS, GUTTERS, DRIVEWAYS, AND SIDEWALKS.



- NOTES:
- 1) USE SPECIFIC VALVE BOX STABILIZER/ADAPTER DESIGNED FOR VALVE TYPE, MANUFACTURER, AND SIZE.
 - 2) MINIMUM RESTRAINED LENGTH ON EITHER SIDE OF VALVE SHALL BE 30'.



- NOTES:
- 1) CONCRETE BASE & BLOCKING MATERIAL SHALL NOT OBSTRUCT HYDRANT DRAIN OPENINGS.
 - 2) SEE SPECIFICATIONS FOR MANUFACTURER-MODEL OF HYDRANT.
 - 3) SEE SPECIFICATIONS FOR NOZZLE/CONNECTION SIZES.
 - 4) USE BACKFILL METHOD FOR PAVED LOCATIONS WHERE EXCAVATION IS WITHIN 2' OF ROAD.
 - 5) HEIGHT OF SAFETY FLANGE SHALL BE ACCORDING TO MANUFACTURER INSTRUCTIONS.



OPTION A - PRESSURE CLASS SEWER MAIN

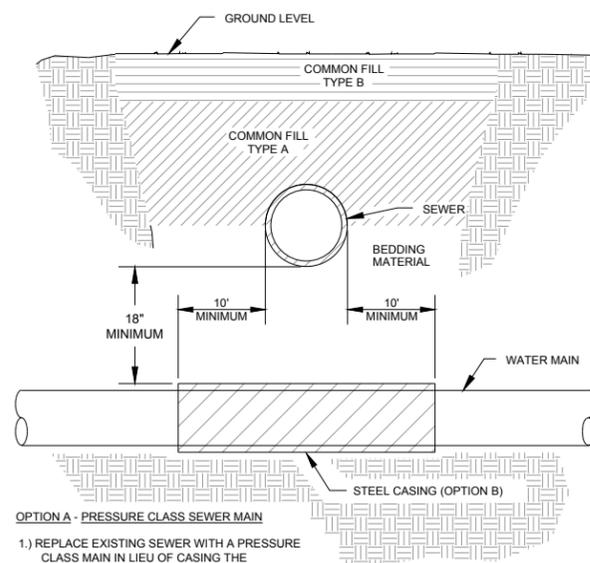
- 1.) REPLACE EXISTING SEWER WITH A PRESSURE CLASS MAIN A MINIMUM OF 10' EACH SIDE OF THE WATER MAIN.

OPTION B - WATER MAIN INSTALLATION

- 1.) MID-POINT OF ONE FULL LENGTH OF WATER MAIN IS TO BE CENTERED ABOVE SEWER LINE.
- 2.) PROVIDE ADEQUATE SUPPORT FOR THE WATER MAIN TO PREVENT DAMAGE DUE TO SETTLEMENT.

NOTES:

- 1.) FOLLOW TRENCH MATERIAL DETAIL WHEN BACK-FILLING WATER MAIN.
- 2.) THE SELECTED OPTION MUST BE APPROVED BY AN ENGINEERS REPRESENTATIVE.



OPTION A - PRESSURE CLASS SEWER MAIN

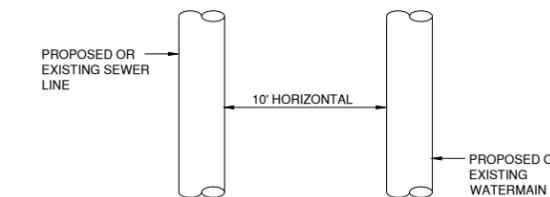
- 1.) REPLACE EXISTING SEWER WITH A PRESSURE CLASS MAIN IN LIEU OF CASING THE WATER MAIN.

OPTION B - WATER MAIN CASING INSTALLATION

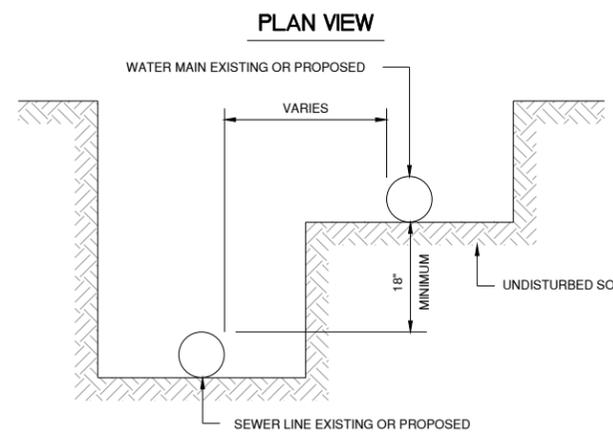
- 1.) PROVIDE ADEQUATE SUPPORT FOR THE EXISTING SEWER TO PREVENT DAMAGE DUE TO SETTLEMENT.

NOTES:

- 1.) FOLLOW TRENCH MATERIAL DETAIL WHEN BACK-FILLING WATER MAIN.
- 2.) THE SELECTED OPTION MUST BE APPROVED BY AN ENGINEERS REPRESENTATIVE.



WHEN PROPOSED SEWER (OR WATER) IS LOCATED 10 FEET OR MORE FROM EXISTING WATER (OR SEWER), NO SPECIAL CONSTRUCTION REQUIRED. SEE SECTION 41-2.01B (1) OF STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS - 7TH EDITION..



WHEN PROPOSED SEWER (OR WATER) IS LOCATED LESS THAN 10 FEET FROM EXISTING WATER (OR SEWER), DETAILS BELOW SHALL APPLY. SEE SECTION 41-2.01B (2) OF STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS - 7TH EDITION.

WATER AND SEWER SEPARATION REQUIREMENTS

HORIZONTAL SEPARATION
NOT TO SCALE
DIV. V-STANDARD DRAWING NO. 18

SURVEY BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT

CIVIL DETAILS 3

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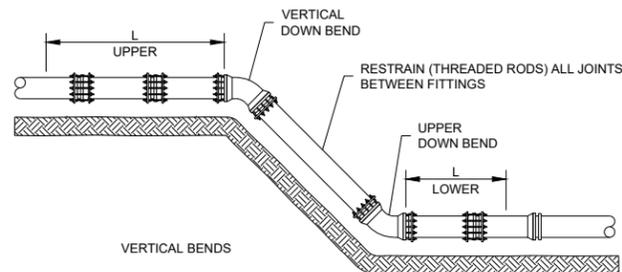
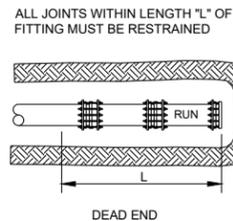
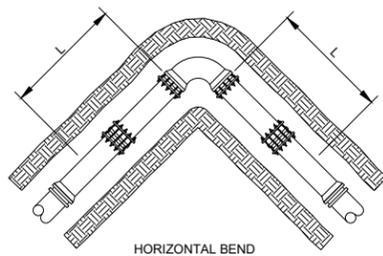
APPROVED BY: DLP

DATE: 03/03/2023

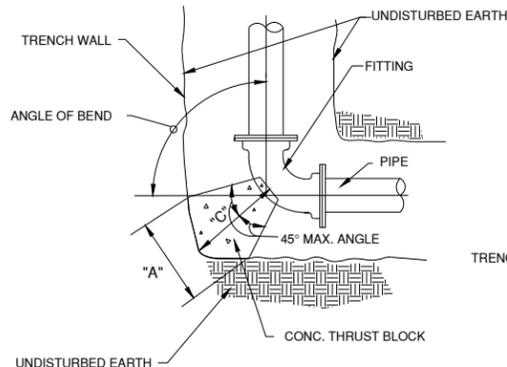
JOB No: 21002032-00

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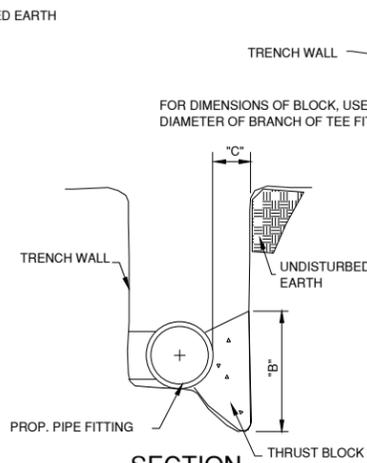
D/P diameter (inches)	L (feet)							
	Horizontal bends				Vertical Down Bends	Vertical Up Bends	Dead Ends	
	11.25'	22.5'	45'	90'	45'	45'		
4 to 8	1	3	6	14	15	6	36	
10 to 12	2	4	8	20	21	8	51	
14 to 16	3	5	11	25	27	11	66	
18 to 20	3	6	13	31	33	13	80	
24	4	7	15	36	39	15	94	



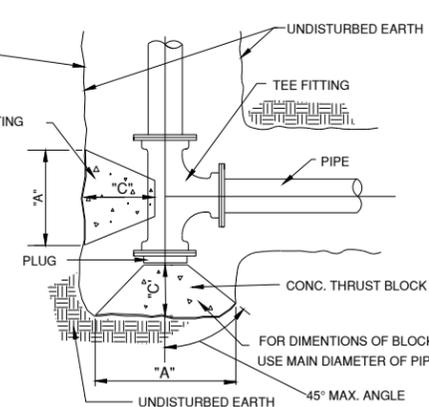
RESTRAINING GLAND RESTRAINT
NOT TO SCALE



PLAN OF BEND
N.T.S.



SECTION
N.T.S.



PLAN OF TEE
N.T.S.

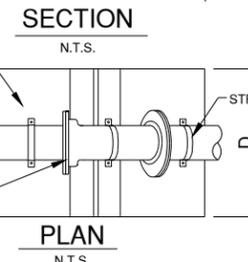
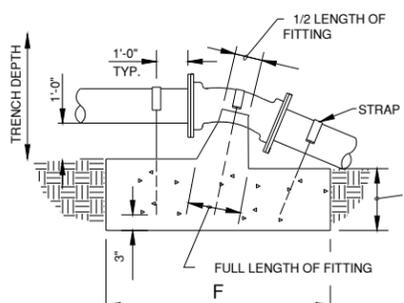
SIZE	90° BEND			45° BEND			22-1/2° BEND			11-1/4° BEND			TEE OR PLUG		
	A	B	C	A	B	C	A	B	C	A	B	C	A	B	C
4" & 6"	2'-0"	1'-4"	9"	1'-6"	1'-0"	6"	1'-1"	0'-8"	6"	0'-9"	0'-6"	6"	2'-5"	1'-7"	1'-0"
8"	2'-8"	1'-10"	1'-0"	2'-0"	1'-4"	8"	1'-5"	1'-0"	8"	1'-0"	0'-8"	8"	3'-2"	2'-2"	1'-3"
10"	3'-4"	2'-3"	1'-3"	2'-6"	1'-8"	10"	1'-9"	1'-2"	8"	1'-3"	0'-10"	8"	4'-0"	2'-8"	1'-7"
12"	4'-0"	2'-8"	1'-6"	3'-0"	2'-0"	1'-0"	2'-2"	1'-5"	8"	1'-6"	1'-0"	8"	4'-10"	3'-2"	1'-11"
14"	4'-8"	3'-2"	1'-9"	3'-6"	2'-4"	1'-2"	2'-6"	1'-8"	8"	1'-9"	1'-2"	8"	5'-7"	3'-9"	2'-3"
16"	5'-4"	3'-7"	2'-0"	3'-11"	2'-8"	1'-4"	2'-10"	1'-11"	9"	2'-0"	1'-4"	8"	6'-4"	4'-3"	2'-6"
18"	6'-0"	4'-0"	2'-3"	4'-5"	3'-0"	1'-6"	3'-2"	2'-2"	10"	2'-3"	1'-6"	8"	7'-2"	4'-9"	2'-10"
20"	6'-8"	4'-5"	2'-6"	4'-11"	3'-4"	1'-8"	3'-6"	2'-4"	11"	2'-6"	1'-8"	8"	7'-11"	5'-4"	3'-2"
24"	8'-0"	5'-4"	3'-0"	5'-11"	3'-11"	2'-0"	4'-3"	2'-10"	1'-1"	3'-0"	2'-0"	8"	9'-6"	6'-4"	3'-9"
30"															
36"															

100 P.S.I. TABLE

NOTES:

- ALL BENDS, TEES, PLUGS, FITTINGS OR OTHER SIGNIFICANT CHANGES IN ALIGNMENT SHALL BE BRACED WITH POURED CONCRETE THRUST BLOCKS FITTINGS WITH RETAINING GLANDS WILL NOT BE ALLOWED.
- "C" DIMENSION SHALL BE AS REQUIRED TO REACH UNDISTURBED EARTH BUT NOT LESS THAN VALUE LISTED IN TABLE.
- DIMENSIONS "A" AND "B" ARE BASED ON INTERNAL PIPE PRESSURE OF 100 P.S.I. AND BEARING ON THE UNDISTURBED SOIL OF 1500 P.S.F.
- "B" = HEIGHT OF THRUST BLOCK

THRUST BLOCK DETAIL
NOT TO SCALE



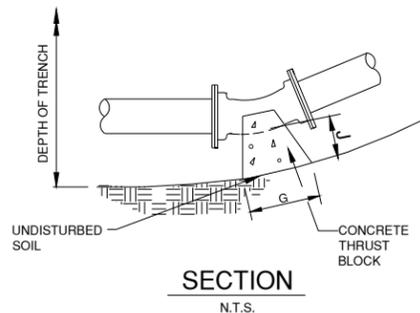
THRUST BLOCK DETAIL
FOR VERTICAL ALIGNMENT CONCAVE DOWNWARD
NOT TO SCALE

TABLE

BEND	SIZE									
	6"	8"	10"	12"	16"	20"	24"	30"	36"	
11 1/4°	D	1'-6"	1'-9"	2'-0"	3'-0"	3'-6"	4'-0"	4'-3"	4'-6"	5'-0"
	E	1'-3"	1'-6"	1'-9"	2'-0"	2'-3"	2'-6"	2'-9"	3'-0"	4'-0"
	F	2'-0"	2'-6"	2'-9"	3'-0"	4'-0"	5'-0"	5'-9"	6'-6"	7'-0"
22 1/2°	D	2'-0"	3'-4"	3'-8"	4'-0"	4'-4"	4'-8"	5'-0"	5'-4"	6'-0"
	E	1'-9"	2'-3"	2'-6"	2'-6"	2'-6"	2'-8"	3'-8"	4'-6"	5'-6"
	F	2'-6"	2'-8"	3'-10"	4'-0"	5'-6"	7'-6"	8'-6"	8'-6"	10'-0"
45°	D	2'-6"	3'-0"	4'-0"	4'-6"	5'-2"	5'-6"	6'-0"	6'-6"	7'-6"
	E	2'-6"	2'-9"	3'-0"	3'-6"	4'-0"	4'-6"	5'-6"	6'-0"	6'-0"
	F	3'-0"	4'-0"	4'-6"	4'-9"	6'-6"	8'-0"	9'-6"	11'-0"	11'-6"

NOTES:

- ALL POURED CONCRETE SHALL BE LAID ON UNDISTURBED EARTH AFTER EXCAVATION ACCORDING TO DIMENSIONS INDICATED ON THRUST BLOCK DIMENSION TABLE OR IT SHALL BE LAID THE FULL WIDTH OF TRENCH FROM UNDISTURBED WALL TO UNDISTURBED WALL BUT NOT LESS THAN LISTED ON THRUST BLOCK TABLE.
- ALL POURED CONCRETE SHALL BE 3000 P.S.I. @ 28 DAYS.
- ALL STRAPS TO BE 3"W. X 1/2" WROUGHT IRON. APPLY TWO COATS OF APPROVED BITUMINOUS PAINT TO EXPOSED STRAP.
- ALL RODS TO BE WROUGHT IRON AS SHOWN ON TABLE, THREADED ONE END AND BENT THE OTHER. APPLY TWO COATS OF APPROVED BITUMINOUS PAINT TO EXPOSED AREAS.



BEND	SIZE									
	6"	8"	10"	12"	16"	20"	24"	30"	36"	
11 1/4°	G	6"	8"	10"	1'-0"	1'-4"	1'-8"	2'-0"	2'-6"	3'-0"
	H	1'-2"	1'-4"	1'-6"	1'-8"	2'-0"	2'-4"	2'-8"	3'-2"	3'-10"
	J	7"	7"	8"	8"	9"	10"	1'-0"	1'-1"	1'-2"
22 1/2°	G	9"	1'-0"	1'-6"	1'-4"	2'-3"	3'-0"	3'-6"	4'-2"	5'-4"
	H	1'-2"	1'-4"	1'-6"	1'-8"	2'-0"	2'-4"	2'-8"	3'-2"	4'-0"
	J	8"	9"	10"	11"	1'-2"	1'-4"	1'-6"	1'-9"	2'-0"
45°	G	1'-3"	1'-8"	2'-1"	2'-6"	3'-4"	4'-2"	5'-0"	6'-3"	7'-6"
	H	1'-2"	1'-4"	1'-6"	1'-10"	2'-6"	3'-0"	3'-4"	4'-0"	5'-0"
	J	8"	9"	10"	11"	1'-2"	1'-4"	1'-9"	2'-3"	2'-8"

NOTES:

- ALL H & J DIMENSIONS TO BE AS REQ'D. TO REACH UNDISTURBED EARTH BUT NOT LESS THAN LISTED ON THRUST BLOCK TABLE.
- ALL POURED CONCRETE SHALL BE 3000 P.S.I. @ 28 DAYS.
- ALL POURED CONCRETE SHALL BE LAID THE FULL WIDTH OF TRENCH FROM UNDISTURBED WALL TO UNDISTURBED WALL.

PLAN
N.T.S.

THRUST BLOCK DETAIL
FOR VERTICAL ALIGNMENT CONCAVE UPWARD
NOT TO SCALE

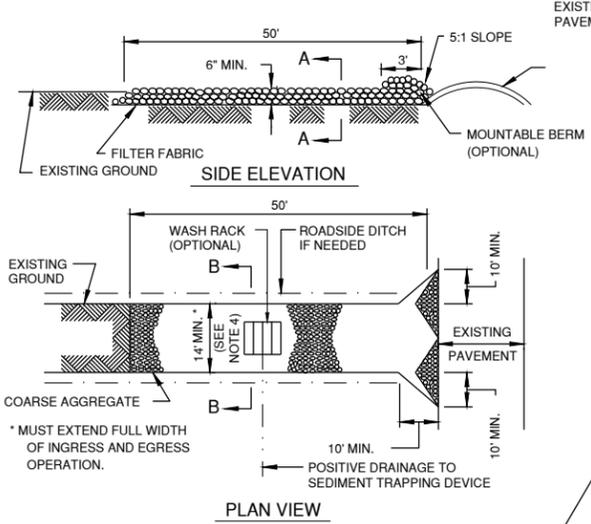
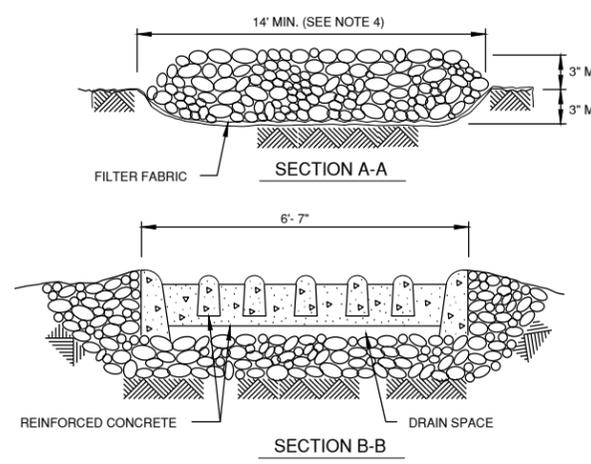
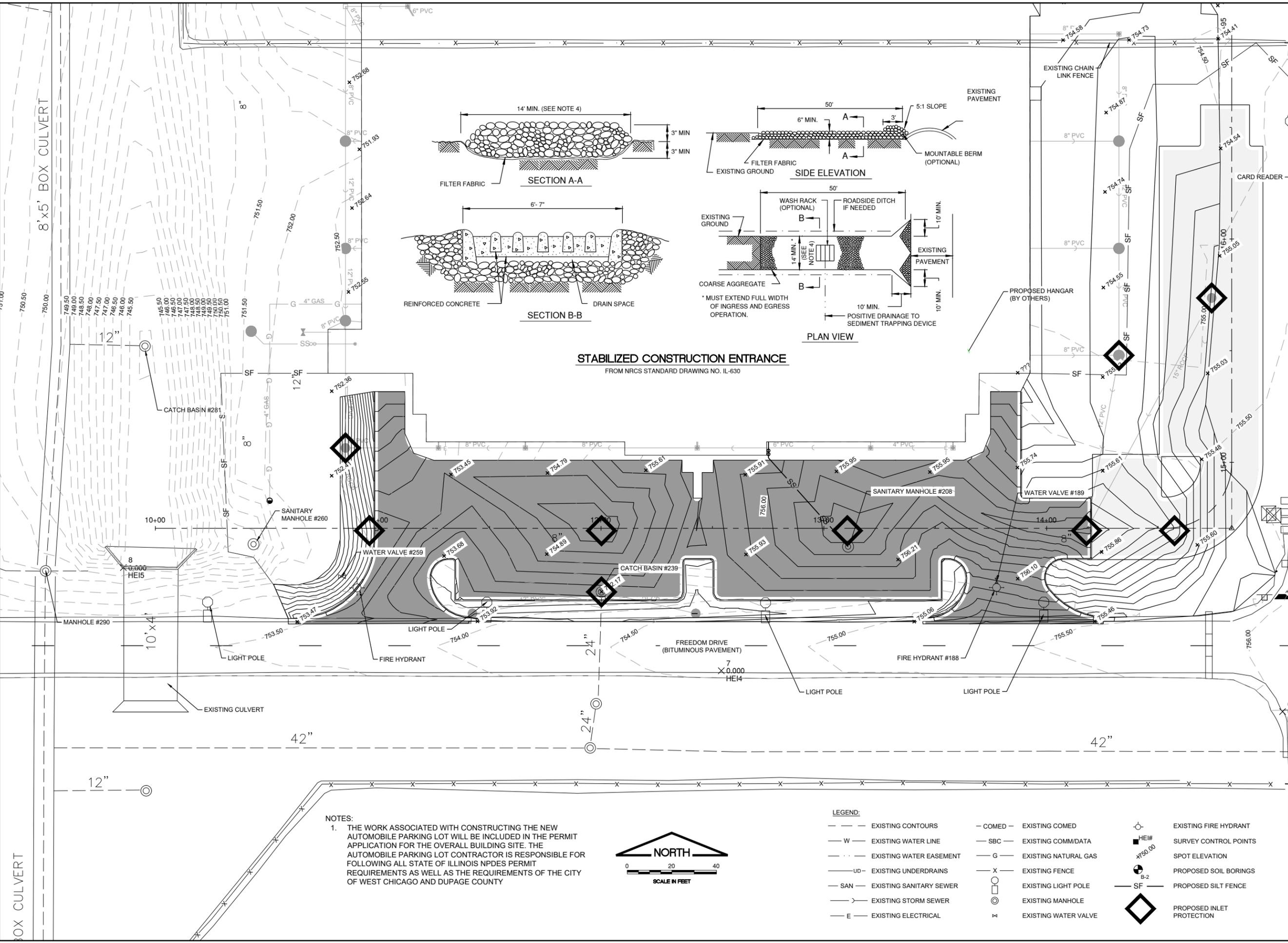
UPDATE BY: Tony Meirn
LAYOUT: CIVIL DETAILS 4

DATE: Friday, March 10, 2023 5:42:30 PM
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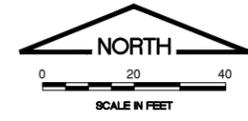
UPDATE BY: Tony Meiri
 LAYOUT: EXISTING CONDITIONS PLAN

8'x5' BOX CULVERT



STABILIZED CONSTRUCTION ENTRANCE
 FROM NRCS STANDARD DRAWING NO. IL-630

NOTES:
 1. THE WORK ASSOCIATED WITH CONSTRUCTING THE NEW AUTOMOBILE PARKING LOT WILL BE INCLUDED IN THE PERMIT APPLICATION FOR THE OVERALL BUILDING SITE. THE AUTOMOBILE PARKING LOT CONTRACTOR IS RESPONSIBLE FOR FOLLOWING ALL STATE OF ILLINOIS NPDES PERMIT REQUIREMENTS AS WELL AS THE REQUIREMENTS OF THE CITY OF WEST CHICAGO AND DUPAGE COUNTY



LEGEND:

--- EXISTING CONTOURS	- COMED - EXISTING COMED	HEI# EXISTING FIRE HYDRANT
- W - EXISTING WATER LINE	- SBC - EXISTING COMM/DATA	HEI# SURVEY CONTROL POINTS
- · - EXISTING WATER EASEMENT	- G - EXISTING NATURAL GAS	750.00 SPOT ELEVATION
- UD - EXISTING UNDERDRAINS	- X - EXISTING FENCE	B-2 PROPOSED SOIL BORINGS
- SAN - EXISTING SANITARY SEWER	EXISTING LIGHT POLE	- SF - PROPOSED SILT FENCE
- - - EXISTING STORM SEWER	EXISTING MANHOLE	PROPOSED INLET PROTECTION
- E - EXISTING ELECTRICAL	EXISTING WATER VALVE	

IL CONTRACT: **DU091**
 IL LETTING ITEM: **07A**
 IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT
 WEST CHICAGO, ILLINOIS
 CONSTRUCT NEW AUTOMOBILE PARKING LOT

STORM WATER POLLUTION PREVENTION PLAN

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DUPAGE AIRPORT AUTHORITY

DESIGN BY: JUM
 DRAWN BY: JMM
 CHECKED BY: DKP
 APPROVED BY: DLP
 DATE: 03/03/2023
 JOB No: 21002032-00

FINAL

SHEET 25 OF 27 SHEETS

STORM WATER POLLUTION PREVENTION NOTES:

THE FOLLOWING IS THE ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING A NEW PARKING LOT AT DUPAGE AIRPORT. THE PROJECT INCLUDES PAVEMENT DEMOLITION, DRAINAGE SYSTEMS INSTALLATION, SITE GRADING, ASPHALT PAVEMENT, CONCRETE CURB AND GUTTER, AND PAVEMENT MARKING.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES:

1. INSTALL TEMPORARY EROSION CONTROL MEASURES.
2. DRAINAGE SYSTEM AND SITEWORK.
3. CONSTRUCT NEW PARKING LOT.
4. REMOVAL AND DISPOSAL OF TEMPORARY EROSION CONTROL MEASURES.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE IS APPROXIMATELY 0.69 ACRES OF WHICH 0.69 ACRES WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO STORM SEWER SYSTEMS THAT OUTLET INTO KRESS CREEK.

SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
5. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEDED.

MAINTENANCE AFTER CONSTRUCTION

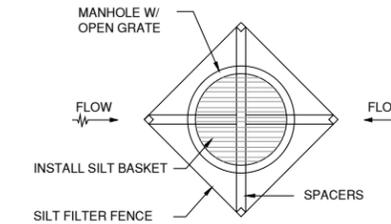
CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

SOIL EROSION AND SEDIMENT CONTROL NOTES:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, DUPAGE COUNTY, DUPAGE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

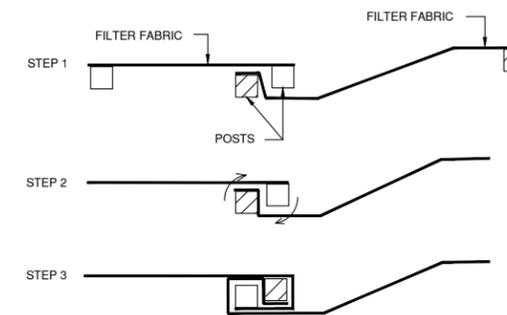
STAGING AND PROTECTION NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.
4. ALL EQUIPMENT FUELING AND GREASING SHALL BE COMPLETED AT THE STAGING AREA. SPILLS SHALL BE IMMEDIATELY CONTAINED AND THE AREA CLEANED AT NO ADDITIONAL COST THE CONTRACT.



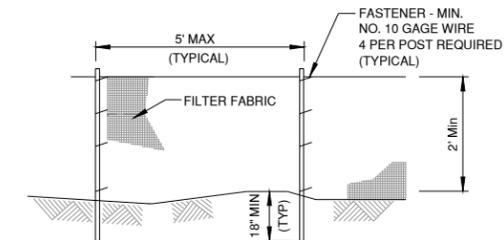
INLET PROTECTION (INLET/MANHOLES IN TURF)

NOT TO SCALE
IDOT STANDARD 280001-07

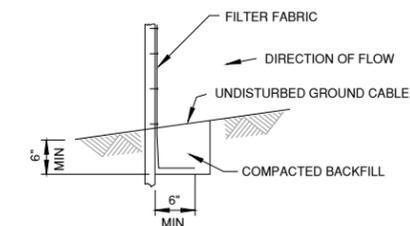


ATTACHING TWO SILT FENCES

NOT TO SCALE
FROM NRCS STANDARD DRAWING NO. IL-ENG-49



ELEVATION



FABRIC ANCHOR DETAIL

NOT TO SCALE
FROM NRCS STANDARD DRAWING NO. IL-ENG-49

NOTES:

1. TEMPORARY SEDIMENT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. THEY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD AND REMOVED IN CONJUNCTION WITH THE FINAL GRADING AND SITE STABILIZATION.
2. FILTER FABRIC SHALL MEET THE REQUIREMENTS OF MATERIAL SPECIFIED FOR ITEM 102-2.5 SILT FENCE IN THE SPECIAL PROVISIONS.
3. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE. MAINTENANCE, WHICH INCLUDES THE REPLACEMENT OF DAMAGED FENCE, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE SILT FENCE.
4. SILT FENCE SHALL BE INSTALLED PER STORM WATER POLLUTION PREVENTION PLAN OR AS DIRECTED BY THE ENGINEER.
5. FENCE POSTS SHALL BE EITHER STANDARD STEEL POSTS OR WOOD POSTS WITH A MINIMUM CROSS-SECTIONAL AREA OF 3.0 SQ. IN..
6. PLACE THE END POST OF THE SECOND FENCE INSIDE THE END POST OF THE FIRST FENCE.
7. ROTATE BOTH POSTS AT LEAST 180 DEGREES IN A CLOCKWISE DIRECTION TO CREATE A TIGHT SEAL WITH THE FABRIC MATERIAL.
8. DRIVE BOTH POSTS A MINIMUM OF 18 INCHES INTO THE GROUND AND BURY THE FLAP.

IL. CONTRACT: **DU091**

IL. LETTING ITEM: **07A**

IL. PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS

NUMBER	BY	DATE

0 1 2

THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT
STORM WATER POLLUTION PREVENTION PLAN
NOTES AND DETAILS

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DUPAGE AIRPORT
AUTHORITY

DESIGN BY: JUM

DRAWN BY: JMM

CHECKED BY: DKP

APPROVED BY: DLP

DATE: 03/03/2023

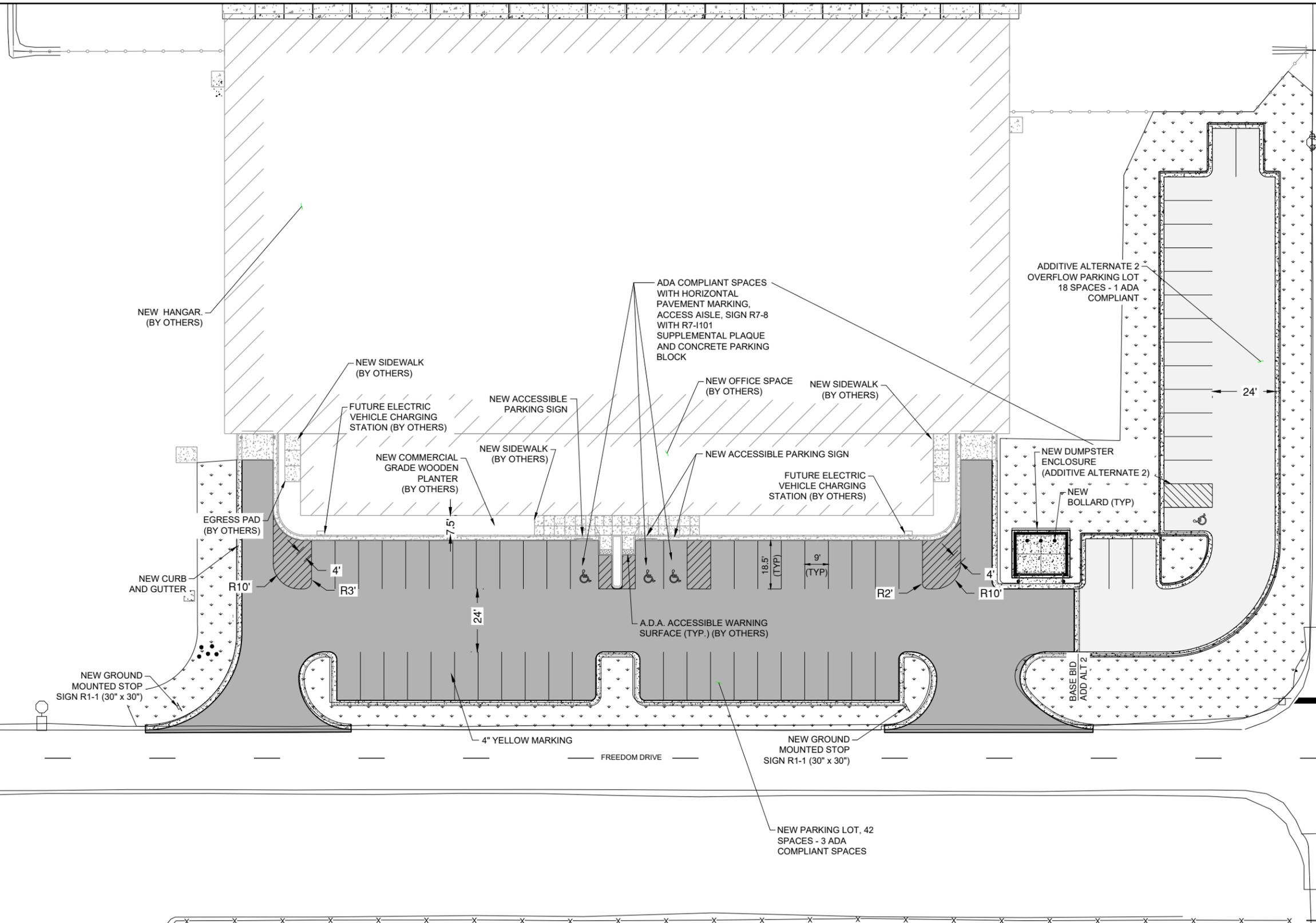
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FINAL

SHEET 26 OF 27 SHEETS

UPDATE BY: Tony Meirn
LAYOUT: PROPOSED GEOMETRY PLAN

DATE: Friday, March 10, 2023 5:43:17 PM
FILE: K:\dupage\h2102032-00_SouthHubParkingLot\Draw\Sheets\2102032-C-111.dwg



- LEGEND:**
- SEEDING AND MULCHING
 - PROPOSED PCC
 - PROPOSED ASPHALT
 - PROPOSED FENCE (BY OTHERS)
 - PROPOSED BOLLARD
 - FLOWER/GRASS (BY OTHERS)
 - SHRUB - 18" DIA. (DECIDUOUS)(BY OTHERS)
 - TREE - 4" MIN. TRUNK DIA. (DECIDUOUS)(BY OTHERS)

IL CONTRACT: **DU091**
IL LETTING ITEM: **07A**
IL PROJECT: **DPA-4980**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

DUPAGE AIRPORT
WEST CHICAGO, ILLINOIS
CONSTRUCT NEW AUTOMOBILE PARKING LOT

**PROPOSED MARKING, SIGNING
AND RESTORATION PLAN**

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**DUPAGE AIRPORT
AUTHORITY**

DESIGN BY:	JUM
DRAWN BY:	JMM
CHECKED BY:	DKP
APPROVED BY:	DLP
DATE:	03/03/2023
JOB No:	21002032-00

FINAL

SHEET 27 OF 27 SHEETS