

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	1
		ILLINOIS	CONTRACT NO. 62R93	

FOR INDEX OF SHEETS AND HIGHWAY STANDARDS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE CITIES OF CREST HILL AND JOLIET

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 112: IL ROUTE 7 / 53 (BROADWAY STREET)
SOUTH OF IL ROUTE 7 (THEODORE STREET) TO
NORTH OF CATON FARM ROAD
SECTION: FAP-0112-11-BJ
PROJECT: NHPP-W07U(134)
DESIGNED OVERLAY, BRIDGE OVERLAY, ADA IMPROVEMENTS
WILL COUNTY

C-91-263-22

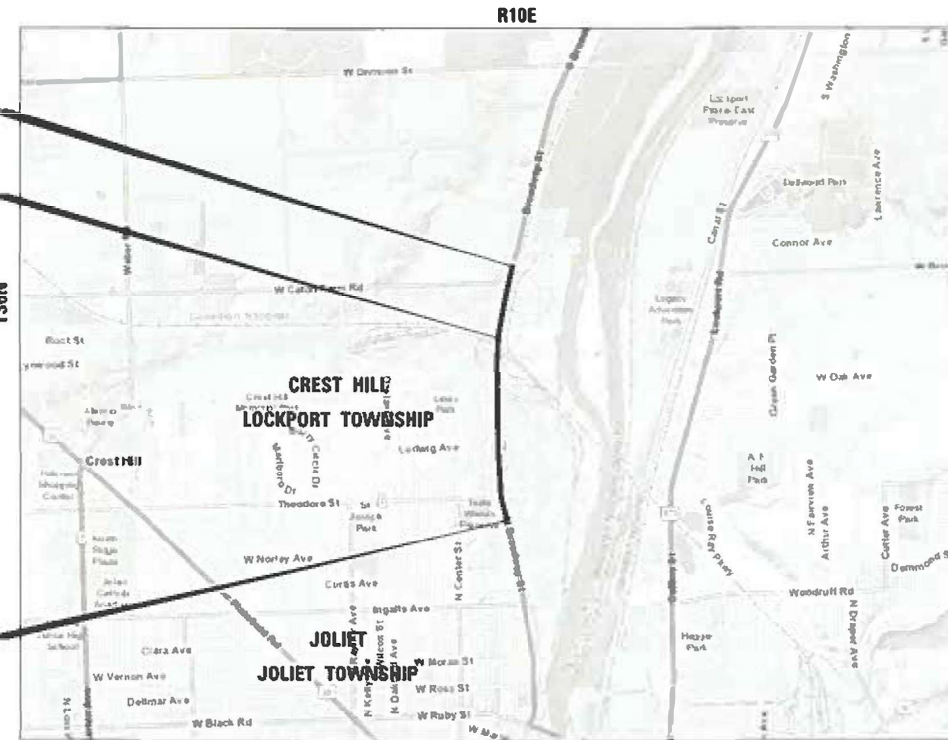
TRAFFIC DATA

2021 ADT = 17,300 VPD
POSTED SPEED LIMIT = 40 MPH
PRINCIPAL ARTERIAL

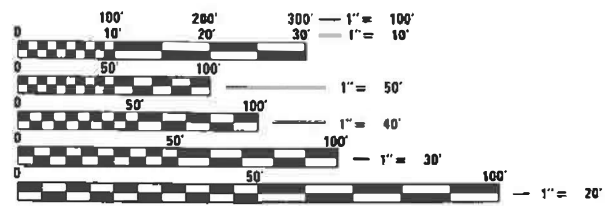


PROJECT ENDS
STA 147 + 52
STRUCTURE NO. 099-0266
STA 127 + 91 TO STA 128 + 92

PROJECT BEGINS
STA 84 + 07



GROSS LENGTH = 6,345 FT. = 1.20 MILE
NET LENGTH = 6,345 FT. = 1.20 MILE

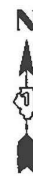


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62R93



Alexander Lane
ALEXANDER CARL LANE, P.E.
IL. LIC. NO. 062-063261
EXP: 11/30/2023
DATE: 1/20/2023

SURFACING OMISSION FROM
STA. 127 + 91 TO STA. 128 + 92

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED: January 31 20 23
Alexander Lane
REGIONAL ENGINEER
March 24, 2023

Steph M. Smith
ENGINEER OF DESIGN AND ENVIRONMENT
March 24, 2023
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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CONTACT: ALEXANDER LANE (312) 477-0620

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
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36	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
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38	ARTERIAL ROAD INFORMATION SIGN (TC-22)
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40	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

STATE STANDARDS

STANDARD NO.	DRAWING NAME
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
482001-02	HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
630001-12	STEEL PLATE BEAM GUARDRAIL
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≤ 40 MPH
701601-10	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES

- FRAME AND GRATE ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS, UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- WHEN MILLED PAVEMENT OPENS TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- THE RESIDENT ENGINEER SHALL CONTACT ERIC CAMPOS, AREA TRAFFIC FIELD ENGINEER, AT ERIC.CAMPOS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS.
- FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT. ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS-RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTENT)" SHOWN IN PLANS.
- THE CENTERLINE IS FOR INFORMATION ONLY.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- WHEN EXISTING SIDEWALK IS TO BE REMOVED WITHOUT PROPOSED SIDEWALK REPLACEMENT, IT SHALL BE REPLACED WITH TOPSOIL AND SOD.
- THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
- LANDSCAPED AREAS AFFECTED BY SIDEWALK CONSTRUCTION SHALL BE RESTORED WITH 18-INCH WIDE STRIP OF "SODDING, SALT TOLERANT" AND "TOPSOIL FURNISH AND PLACE, 4-INCH" INSTALLED FROM THE BACK OF THE SIDEWALK. OR AS DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT THE TIMES DURING CONSTRUCTION.
- ALL CAST OPEN LIDS FOR FRAMES, TYPE 1, WITHIN CURB RAMPS FOR SIDEWALK, SHALL BE "ADA COMPLIANT" CAST OPEN LIDS PER HIGHWAY STANDARD 604001.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF THE CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
- THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM TO CONTINUOUSLY MONITOR FOR WORKER SAFETY AND SOIL CONTAMINATION AT SEVERAL LOCATIONS. SEE SPECIAL PROVISION AND SUPPLEMENTAL SPECIFICATIONS FOR DETAILS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
- ALL MILLED SURFACES SHALL BE AT A UNIFORM CROSS SLOPE PER LANE AND FREE OF RIDGES BETWEEN PASSES. ANY DEVIATION SHALL BE CORRECTED AT NO COST TO THE DEPARTMENT.
- THE CONTRACTOR SHALL USE EXTRA CARE AND PRECAUTION WHEN WORKING UNDER THE RAILROAD TRACKS.
- THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1.

MODEL Path: J:\PROJECTS\2023\04\04-00 IDOT Urban Phase 2 (FTR) 106 Item: 161110 28 62993\DC\DC\000_Sheet1D161903-SHT-GENERAL-01.dgn

INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9594 www.infrastructure-eng.com	USER NAME = ALane PLOT SCALE = 20,0000 * / in. PLOT DATE = 1/25/2023	DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 01/25/2023	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES IL-753 (THEODORE ST TO CATON FARM RD)	F.A.P. RTE. = 112 SECTION = FAP-0112-22-BJ COUNTY = WILL TOTAL SHEETS = 40 SHEET NO. = 2	CONTRACT NO. 62R93
	SCALE:	STA. TO STA.	ILLINOIS FED. AID PROJECT				

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				URBAN		100% STATE
				0059 80% FEDERAL 20% STATE	0005 80% FEDERAL 20% STATE	
20200100	EARTH EXCAVATION	CU YD	225		225	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	27		27	
25200110	SODDING, SALT TOLERANT	SQ YD	27		27	
25200200	SUPPLEMENTAL WATERING	UNIT	1		1	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	8		8	
28000510	INLET FILTERS	EACH	14		14	
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	309		309	
35101400	AGGREGATE BASE COURSE, TYPE B	TON	15		15	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	24,178		24,178	
40600370	LONGITUDINAL JOINT SEALANT	FOOT	20,871		20,871	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	54		54	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	591		591	
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	36		36	
40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	4,009		4,009	
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, Mix "F", N80	TON	3,508		3,508	
42001300	PROTECTIVE COAT	SQ YD	5,635		5,635	
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1,644		1,644	
42400800	DETECTABLE WARNINGS	SQ FT	240		240	
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	83		83	
44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	35,119		35,119	
44000600	SIDEWALK REMOVAL	SQ FT	1,663		1,663	
44002215	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 3 3/4"	SQ YD	422		422	
44004250	PAVED SHOULDER REMOVAL	SQ YD	94		94	

* = SPECIALTY ITEM

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				URBAN		100% STATE
				0059 80% FEDERAL 20% STATE	0005 80% FEDERAL 20% STATE	
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	26		26	
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	51		51	
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	346		346	
44213200	SAW CUTS	FOOT	643		643	
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	49		49	
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	238		238	
50300260	BRIDGE DECK GROOVING	SQ YD	635	635		
50300300	PROTECTIVE COAT	SQ YD	644	644		
60255500	MANHOLES TO BE ADJUSTED	EACH	3		3	
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	14		14	
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	6		6	
63100169	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED	EACH	2		2	
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	250		250	
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2		2	
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1		1	
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1		1	
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	2		2	
67100100	MOBILIZATION	L SUM	1		1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10		10	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	8,430		8,430	
70300211	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - PAINT	SQ FT	146		146	
70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	25,293		25,293	
70300241	TEMPORARY PAVEMENT MARKING - LINE 6"- PAINT	FOOT	570		570	
70300251	TEMPORARY PAVEMENT MARKING - LINE 8"- PAINT	FOOT	118		118	

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PLOT DATE = 1/26/2023	CHECKED - ACL	REVISED -
	DATE - 01/25/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
IL-753 (THEODORE ST TO CATON FARM RD)**

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 3
CONTRACT NO. 62R93			ILLINOIS FED. AID PROJECT	

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				URBAN		100% STATE
				0059 80% FEDERAL 20% STATE	0005 80% FEDERAL 20% STATE	
70300261	TEMPORARY PAVEMENT MARKING - LINE 12"- PAINT	FOOT	1,174		1,174	
70300261	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	324		324	
70306120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE III TAPE	FOOT	1,249		1,249	
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	7,249		7,249	
* 70001100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	146		146	
* 70002200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	24,700		24,700	
* 70004400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	570		570	
* 70005500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	118		118	
* 70006600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,060		1,060	
* 70006650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	297		297	
* 70009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	514		514	
* 70009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	114		114	
* 70009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	28		28	
* 70100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	652		652	
70300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	652		652	
70300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1,474		1,474	
* 10202200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	20		20	
* 50002200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2		2	
* 7301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	617		617	
* 7301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	20		20	
* 79002200	DRILL EXISTING HANDHOLE	EACH	2		2	
00600100	DETECTOR LOOP, TYPE 1	FOOT	290		290	
* 00600600	DETECTOR LOOP REPLACEMENT	FOOT	920		920	
* 95002200	RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	6		6	
* 9502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	169		169	

* = SPECIALTY ITEM

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE		
				URBAN		100% STATE
				0059 80% FEDERAL 20% STATE	0005 80% FEDERAL 20% STATE	
* 9502376	REBUILD EXISTING HANDHOLE	EACH	1		1	
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1		1	
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	129	129		
* X1400367	PEDESTRIAN SIGNAL POST, 10 FT.	EACH	2		2	
* X1400423	REMOVE EXISTING PEDESTRIAN PUSH BUTTON	EACH	4		4	
X4060995	TEMPORARY RAMP, SPECIAL	SQ YD	160		160	
X4400501	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT LESS THAN OR EQUAL TO 10 FEET	FOOT	1,120		1,120	
X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	876		876	
X5537000	STORM SEWERS TO BE CLEANED 12"	FOOT	500			500
X6030205	FRAMES AND GRATES TO BE ADJUSTED (SPECIAL)	EACH	18		18	
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12		12	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1		1	
X0760200	ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8		8	
X0700012	CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	8		8	
Z0006014	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	644	644		
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	644	644		
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	13		13	
Z0018100	DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL)	EACH	1		1	
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	33			33
Z0030050	TEMPORARY INFORMATION SIGNING	SQ FT	122		122	
Z0041895	POLYMER CONCRETE	CU FT	5	5		
Z0048665	RAILROAD PROTECTION LIABILITY INSURANCE	L SUM	1	0.5	0.5	
Z0073500	TEMPORARY SUPPORT SYSTEM	L SUM	1	1		
Ø Z0076600	TRAINEES	HOUR	500	500		
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	500	500		

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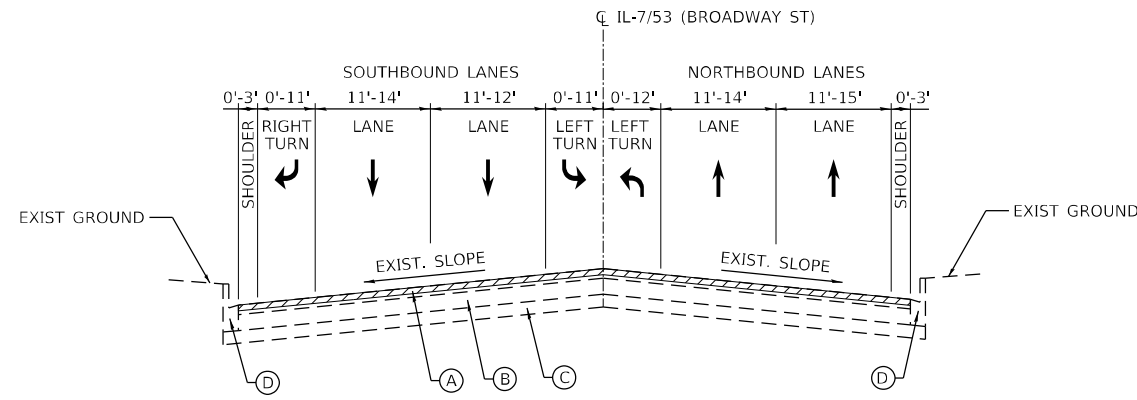
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCALE:		STA. TO STA.	
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SUMMARY OF QUANTITIES
IL-753 (THEODORE ST TO CATON FARM RD)

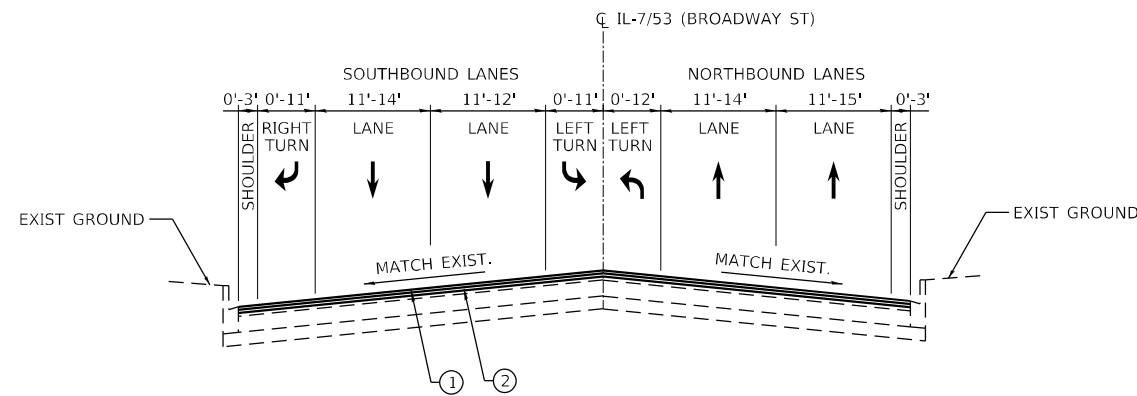
F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R93	

Ø 0042



LEGEND

- (A) EXISTING HOT-MIX ASPHALT PAVEMENT 4"-14"
- (B) EXISTING P.C.C. BASE COURSE 0"-16"
- (C) EXISTING SUBBASE GRANULAR MATERIAL, 8"
- (D) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1-3/4"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"



LEGEND:

- 3-3/4" HMA SURFACE REMOVAL

NOTES:

1. THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE.
2. THE CONTRACTOR SHALL PATCH FIRST THEN MILL.
3. PAVEMENT CORING WAS PERFORMED AT 7 LOCATIONS ALONG THE ROUTE TO DETERMINE THE EXISTING PAVEMENT DEPTHS.

HOT-MIX ASPHALT MIXTURE REQUIRMENTS			QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes		
PAVEMENT RESURFACING - DESIGNED OVERLAY			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	3.5% @ 80 GYR		QCP
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	4% @ 70 GYR		QCP
PATCHING			
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR		QC/QA
HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR		QC/QA
SHOULDER WIDENING			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70	4% @ 70 GYR		QCP
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	4% @ 70 GYR		QC/QA
QMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC/QA): QUALITY CONTROL FOR PERFORMANCE (QCP): PAY FOR PERFORMANCE (PFP)			

MIXTURE REQUIREMENT NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

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	DATE - 01/25/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
IL-7/53 (THEODORE ST TO CATON FARM RD)**

SCALE: NTS

STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	5
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES

- ALL MAINTENANCE OF TRAFFIC (MOT) LANE CLOSURES CAN BE COMPLETED WITH IDOT HIGHWAY STANDARDS. THE MOT PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY IMPROVE OR MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
- THE CONTRACTOR SHALL CONTACT THE IDOT DISTRICT 1 ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE IL ROUTE 7/53 (BROADWAY STREET) WORK ZONE SPEED LIMIT SHALL BE 40 MPH.
- EXISTING REFLECTORS IN RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH STAGED TRAFFIC PATTERNS SHALL BE REMOVED UNDER THIS CONTRACT. THESE SHALL BE REPLACED WITHIN THE LIMITS OF THIS CONTRACT'S MAINTENANCE OF TRAFFIC, ALONG WITH ANY OTHER MARKERS THAT WERE MISSING REFLECTORS PRIOR TO REOPENING THE LANES TO TRAFFIC.
- ALL SIGNS MUST BE POST MOUNTED, IN THE GROUND WHEREVER POSSIBLE, AND WITH A 7 FEET CLEARANCE WHICH IS MEASURED FROM THE TOP OF PAVEMENT TO THE BOTTOM OF THE SIGN.
- CONTRACTOR SHALL REMOVE OR COVER ALL W21-1 (WORKERS PRESENT) AND W20-7 (FLAGGER) SIGNS, WITHIN AN HOUR, WHEN WORKERS ARE NOT PRESENT OR IF THE SIGN IS NOT APPLICABLE.
- CONTRACTOR TO INSTALL CHANGEABLE MESSAGE SIGNS AT LEAST TWO WEEK PRIOR TO STAGE 1. CONTRACTOR TO INFORM THE PUBLIC OF THE UPCOMING TRAFFIC CONTROL SHIFT.

IL 7/53 (BROADWAY ST.) CONSTRUCTION STAGING

MAINTENANCE OF TRAFFIC - STAGE 1

CONSTRUCTION:

- COMPLETE BRIDGE DECK AND JOINT REPAIRS AND OVERLAY OF THE EAST SIDE (NORTHBOUND DIRECTION) OF THE UNDERPASS AND BRIDGE.

MAINTENANCE OF TRAFFIC:

- CLOSE THE NORTHBOUND LANES OF TRAFFIC AND SHIFT TRAFFIC TO THE SOUTHBOUND LANES. 1 LANE IN EACH DIRECTION SHALL BE MAINTAINED.

MAINTENANCE OF TRAFFIC - STAGE 2

CONSTRUCTION:

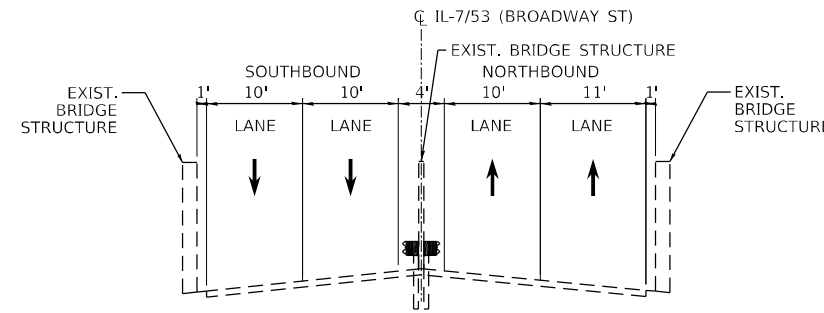
- COMPLETE BRIDGE DECK AND JOINT REPAIRS AND OVERLAY OF THE WEST SIDE (SOUTHBOUND DIRECTION) OF THE UNDERPASS AND BRIDGE.

MAINTENANCE OF TRAFFIC:

- CLOSE THE SOUTHBOUND LANES OF TRAFFIC AND SHIFT TRAFFIC TO THE NORTHBOUND LANES. 1 LANE IN EACH DIRECTION SHALL BE MAINTAINED.

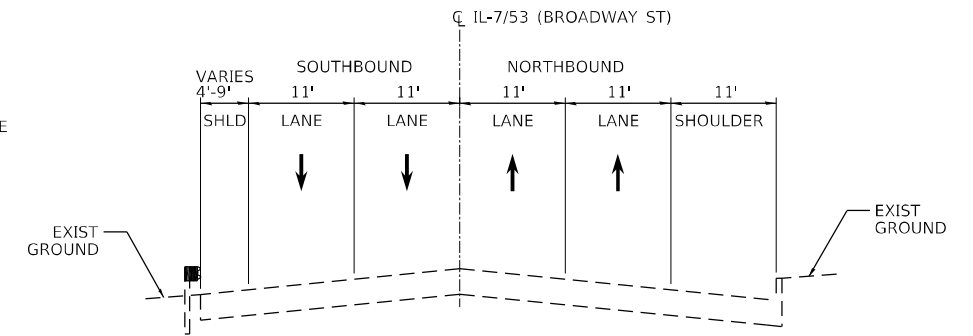
WORK ZONE SPEED LIMIT:

	EXISTING POSTED SPEED	PROPOSED POSTED SPEED
STAGE 1	40 MPH	40 MPH
STAGE 2	40 MPH	40 MPH



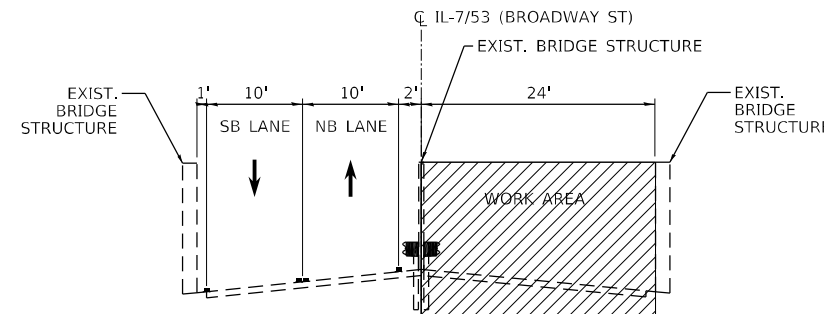
EXISTING TYPICAL CROSS SECTION

UNDER CN RAILROAD BRIDGE



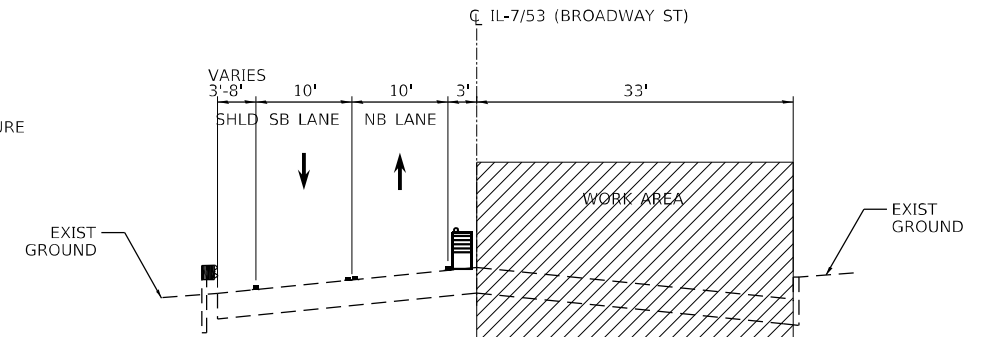
EXISTING TYPICAL CROSS SECTION

ON SN 099-0266



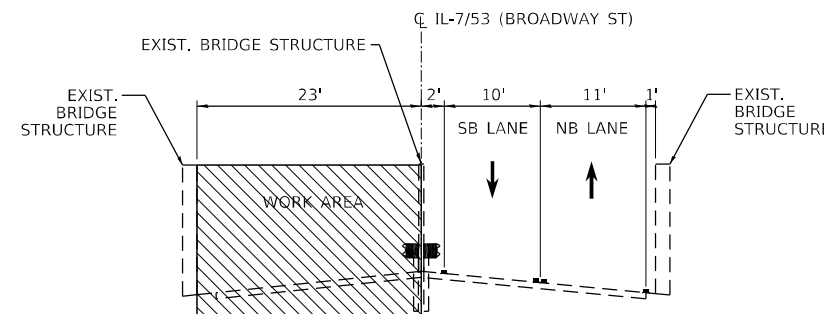
STAGE 1 CONSTRUCTION

UNDER CN RAILROAD BRIDGE



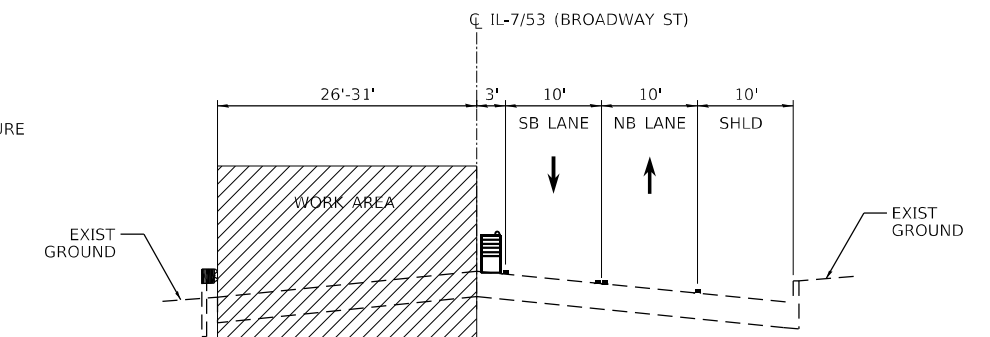
STAGE 1 CONSTRUCTION

ON SN 099-0266



STAGE 2 CONSTRUCTION

UNDER CN RAILROAD BRIDGE



STAGE 2 CONSTRUCTION

ON SN 099-0266

MOT TYPICAL LEGEND:

- WORK AREA
- DIRECTION OF TRAVEL
- TEMPORARY PAVEMENT MARKING
- DRUMS SPACED AT 25' CENTERS

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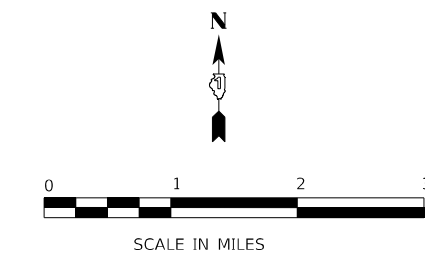
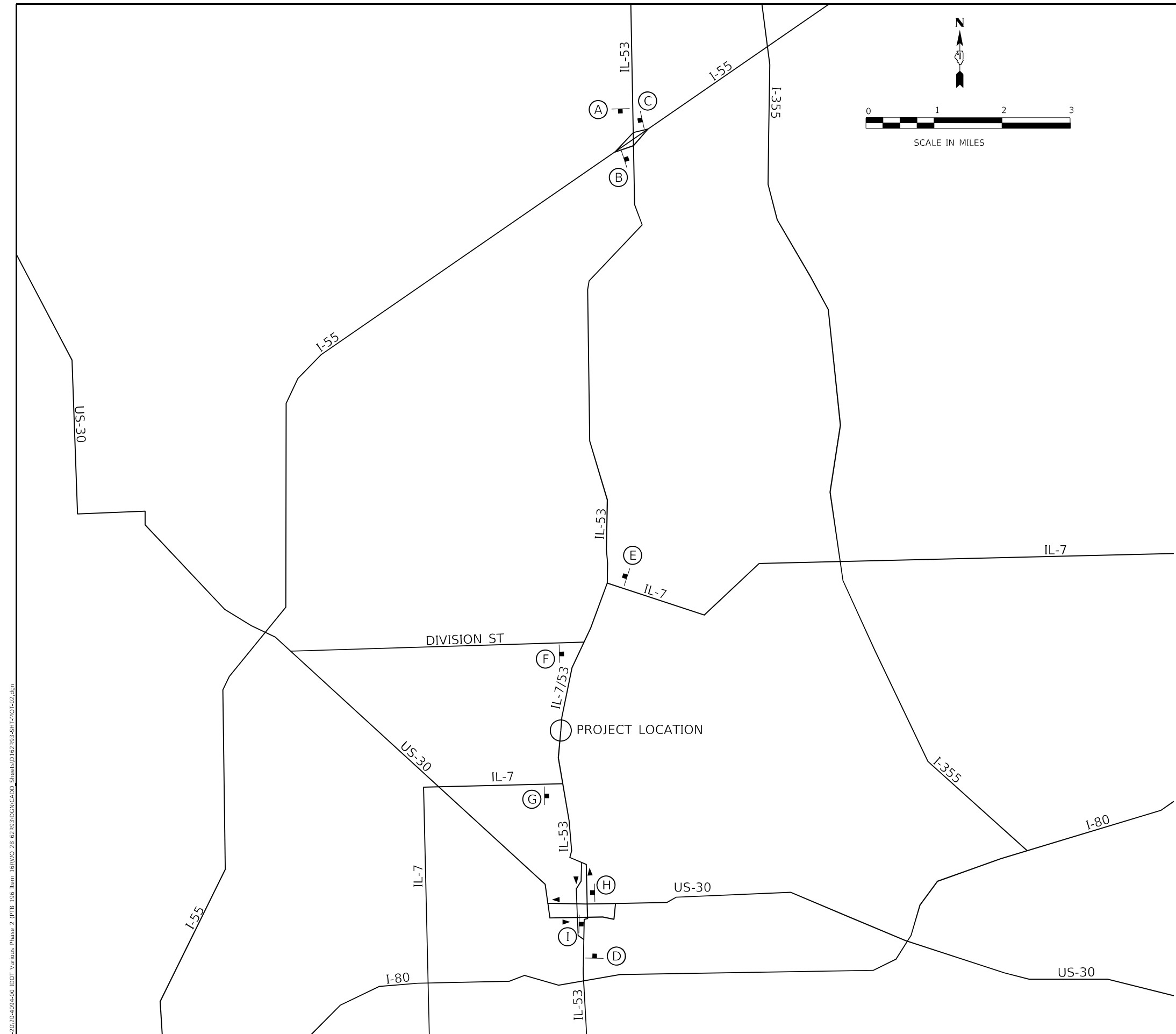
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DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLANS
GENERAL NOTES AND TYPICAL SECTIONS**

SCALE: NTS

STA. TO STA.

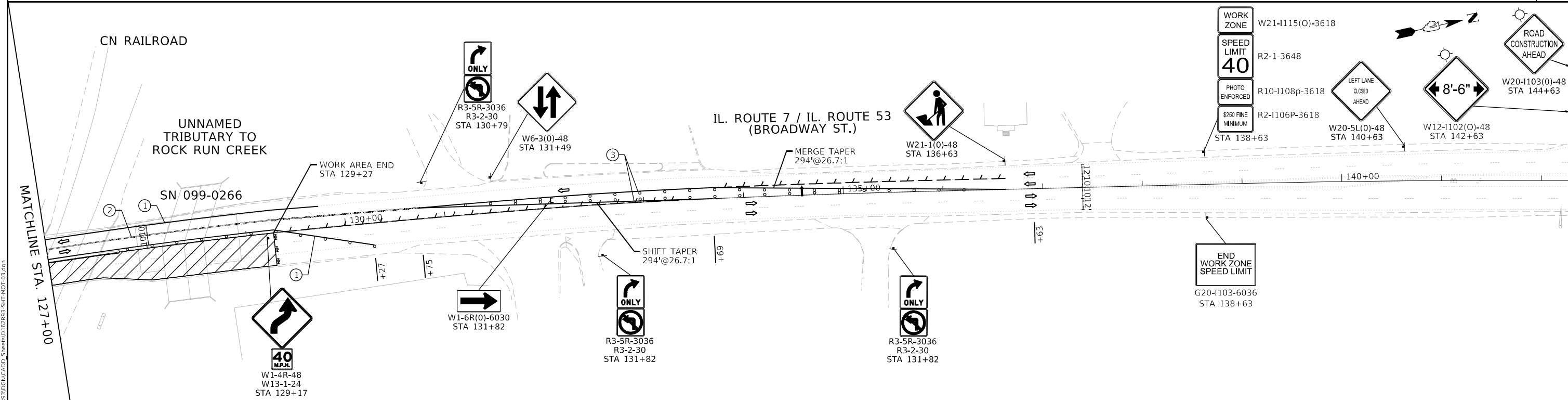
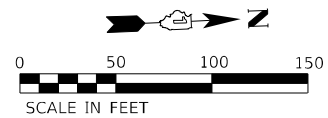
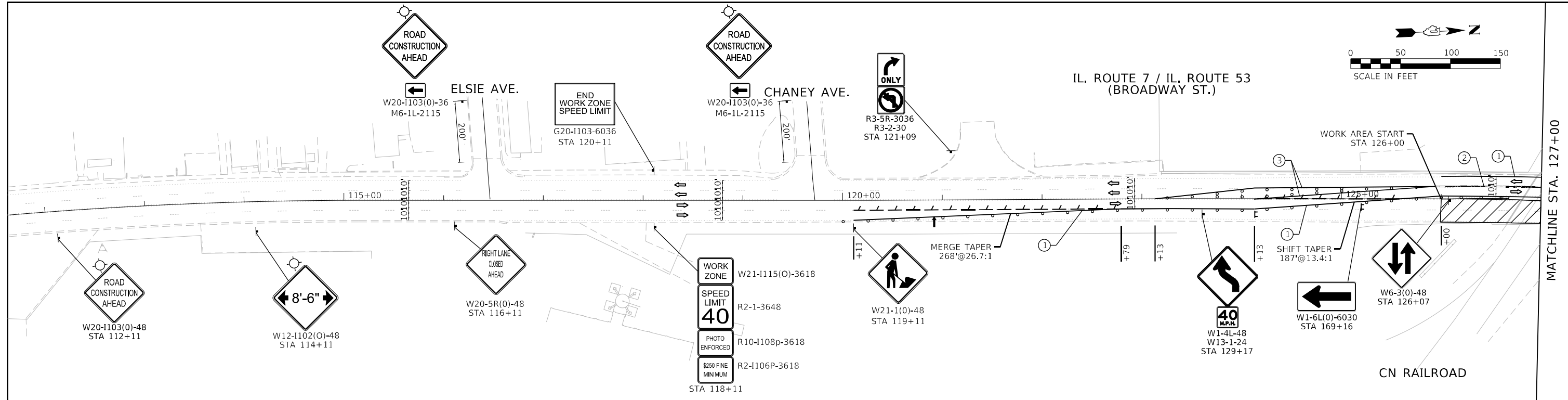
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	7
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



(A)		W12-1103-4848	(E)		W12-1103-4848
					M6-1L(O)-2115
(B)		W12-1103-4848	(F)		W12-1103-4848
		M3-3(O)-2412			M6-1R(O)-2115
		M1-5-2424			
		M6-1R(O)-2115			
(C)		W12-1103-4848	(G)		W12-1103-4848
		M3-3(O)-2412			M6-1L(O)-2115
		M1-5-2424			
		M6-1L(O)-2115			
(D)		W12-1103-4848	(H)		W12-1103-4848
					M3-1(O)-2412
					M1-5-2424
					M6-1R(O)-2115
			(I)		W12-1103-4848
					M3-1(O)-2412
					M1-5-2424
					M6-1L(O)-2115

NOTE: SIGNS SHOULD BE PLACED 500 FEET IN ADVANCE OF THE INTERSECTIONS, BUT MAY BE ADJUSTED DUE TO FIELD CONDITIONS.

MODEL: P:\d\h\h...
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 IDOT: V:\h\h...
 SHEET: 112 OF 112



- MOT SYMBOL LEGEND**
- CONE, DRUM OR BARRICADE
 - ◻ WORK AREA
 - ┌ TYPE III BARRICADE
 - ⇄ DIRECTION OF TRAFFIC
 - ┌ SIGN ON PORTABLE OR PERMANENT SUPPORT
 - ⇄ ARROW BOARD
 - ☑ BARRICADE OR DRUM WITH FLASHING LIGHT
 - ┌ PAVEMENT MARKING REMOVAL

- MOT PROPOSED LEGEND**
- ① TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE) (70300904)
 - ② TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (DOUBLE YELLOW) (70300904)
 - ③ TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (YELLOW) (70300904)

NOTE: TRAFFIC CONTROL BASED ON STANDARD 701611-01. REMOVE CONFLICTING PAVEMENT MARKING USING PAVEMENT MARKING REMOVAL - WATERBLASTING.

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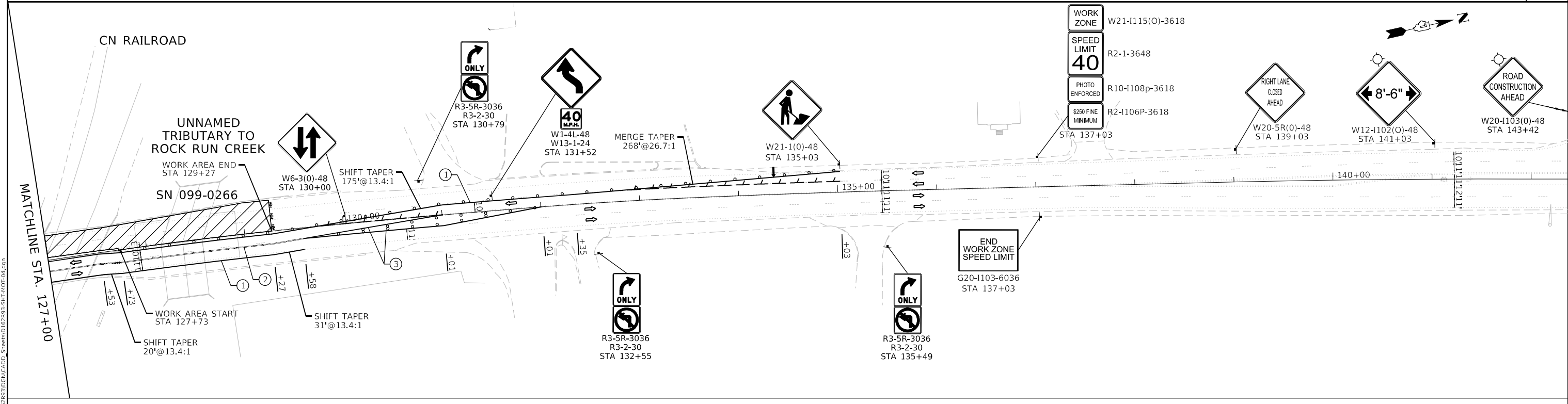
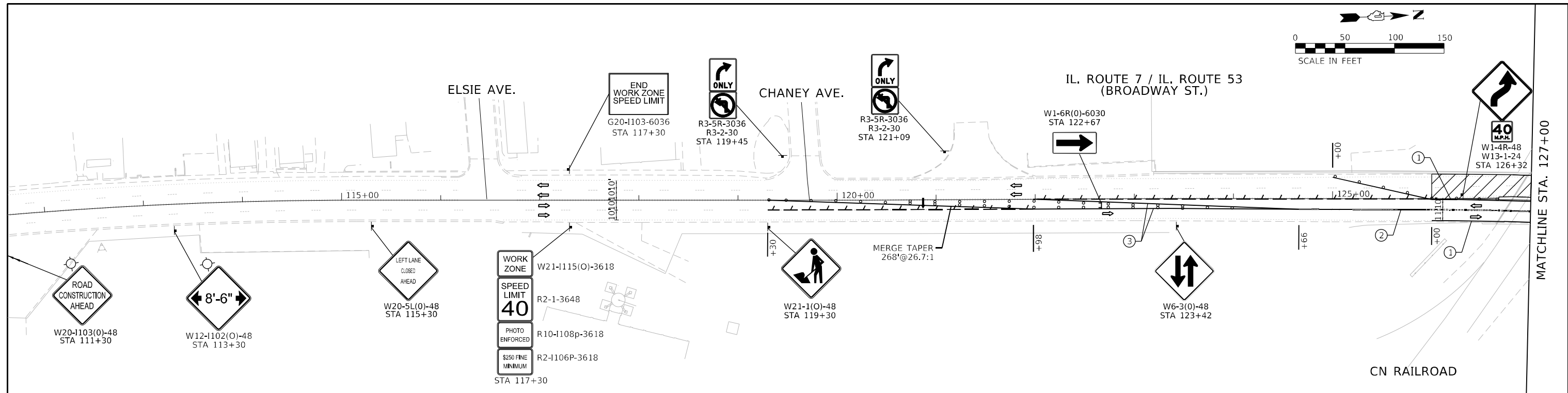
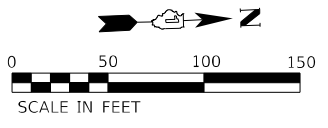
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		DATE - 01/26/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLANS
STAGE 1**

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 9
CONTRACT NO. 62R93			ILLINOIS FED. AID PROJECT	

SCALE: 1" = 50' STA. TO STA.



MOT SYMBOL LEGEND

- CONE, DRUM OR BARRICADE
- WORK AREA
- TYPE III BARRICADE
- DIRECTION OF TRAFFIC
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- ARROW BOARD
- BARRICADE OR DRUM WITH FLASHING LIGHT
- PAVEMENT MARKING REMOVAL

NOTE: TRAFFIC CONTROL BASED ON STANDARD 701611-01. REMOVE CONFLICTING PAVEMENT MARKING USING PAVEMENT MARKING REMOVAL - WATERBLASTING.

MOT PROPOSED LEGEND

- ① TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE) (70300904)
- ② TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (DOUBLE YELLOW) (70300904)
- ③ TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (YELLOW) (70300904)

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		DATE - 01/26/2023	

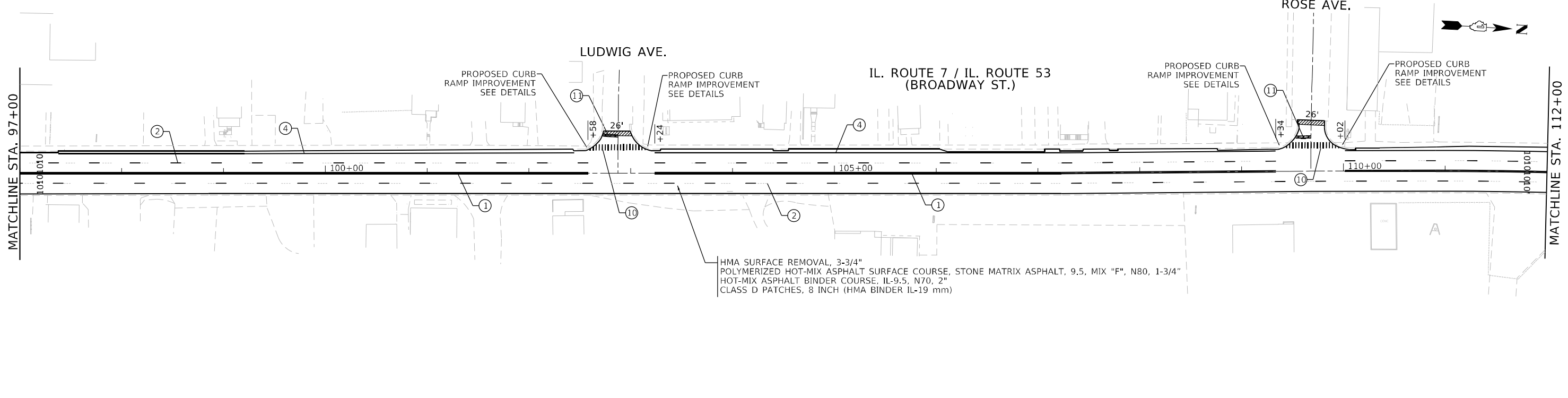
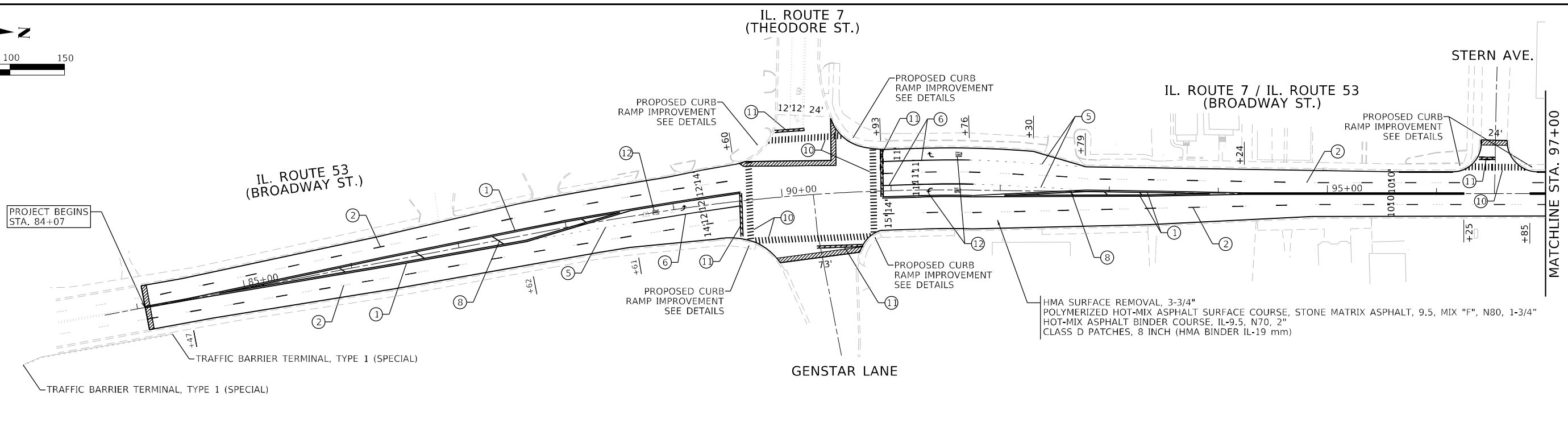
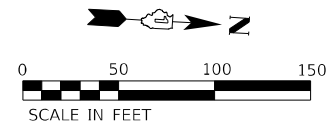
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DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLANS
STAGE 2**

SCALE: 1" = 50'
STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	10
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				

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PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP)
- ⑪ LINE 24", SOLID, WHITE - STOP LINES (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES
- ⑦ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)
- ⑫ LETTERS & SYMBOLS, SOLID, WHITE - TURN LANE MARKINGS (TYP)
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
- ⑧ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP)
(SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑬ LINE 2 @ 6" SKIP-DASH, YELLOW (2' LINE - 6' SPACE) - DOTTED LINES (TYP)
- ④ LINE 4", SOLID WHITE - EDGE LINE (TYP)
- ⑨ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP)
(SEE DISTRICT TC-13 FOR SPACING)
- ⑩ LINE 12", SOLID, WHITE - CONTINENTAL CROSSWALK (TYP)
- ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)

LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

1. LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
2. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE) ON HMA PAVEMENT AND MODIFIED URETHANE ON CONCRETE PAVEMENT
3. ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
4. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

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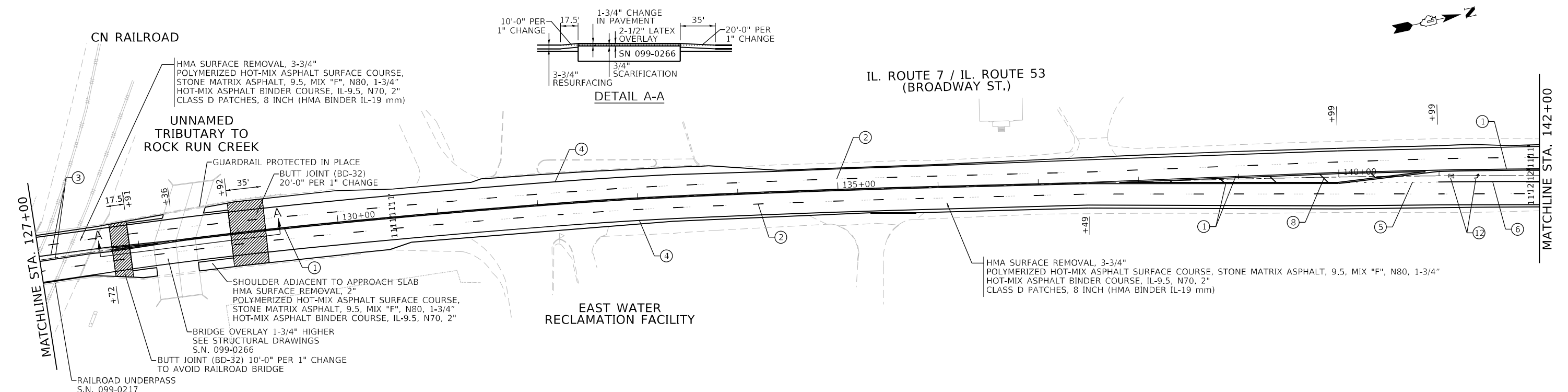
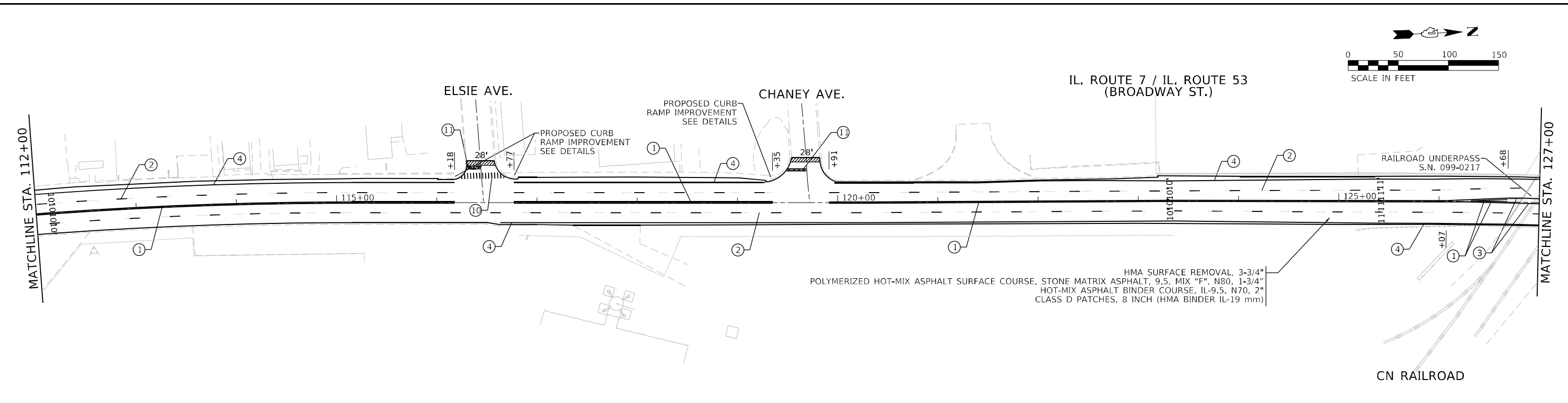
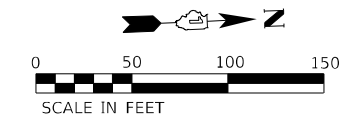
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	DATE - 01/26/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
IL-7/53 (THEODORE ST TO CATON FARM RD)**

SCALE: 1" = 50'

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	11
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
- ④ LINE 4", SOLID WHITE - EDGE LINE (TYP)
- ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)
- ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP)
- ⑦ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)
- ⑧ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP) (SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑨ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP) (SEE DISTRICT TC-13 FOR SPACING)
- ⑩ LINE 12", SOLID, WHITE - CONTINENTAL CROSSWALK (TYP)
- ⑪ LINE 24", SOLID, WHITE - STOP LINES (TYP)
- ⑫ LETTERS & SYMBOLS, SOLID, WHITE - TURN LANE MARKINGS (TYP)
- ⑬ LINE 2 @ 6" SKIP-DASH, YELLOW (2' LINE - 6' SPACE) - DOTTED LINES (TYP)

LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

1. LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
2. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE) ON HMA PAVEMENT AND MODIFIED URETHANE ON CONCRETE PAVEMENT
3. ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
4. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

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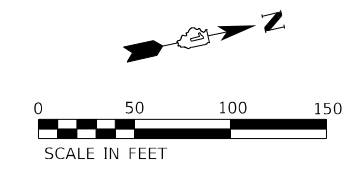
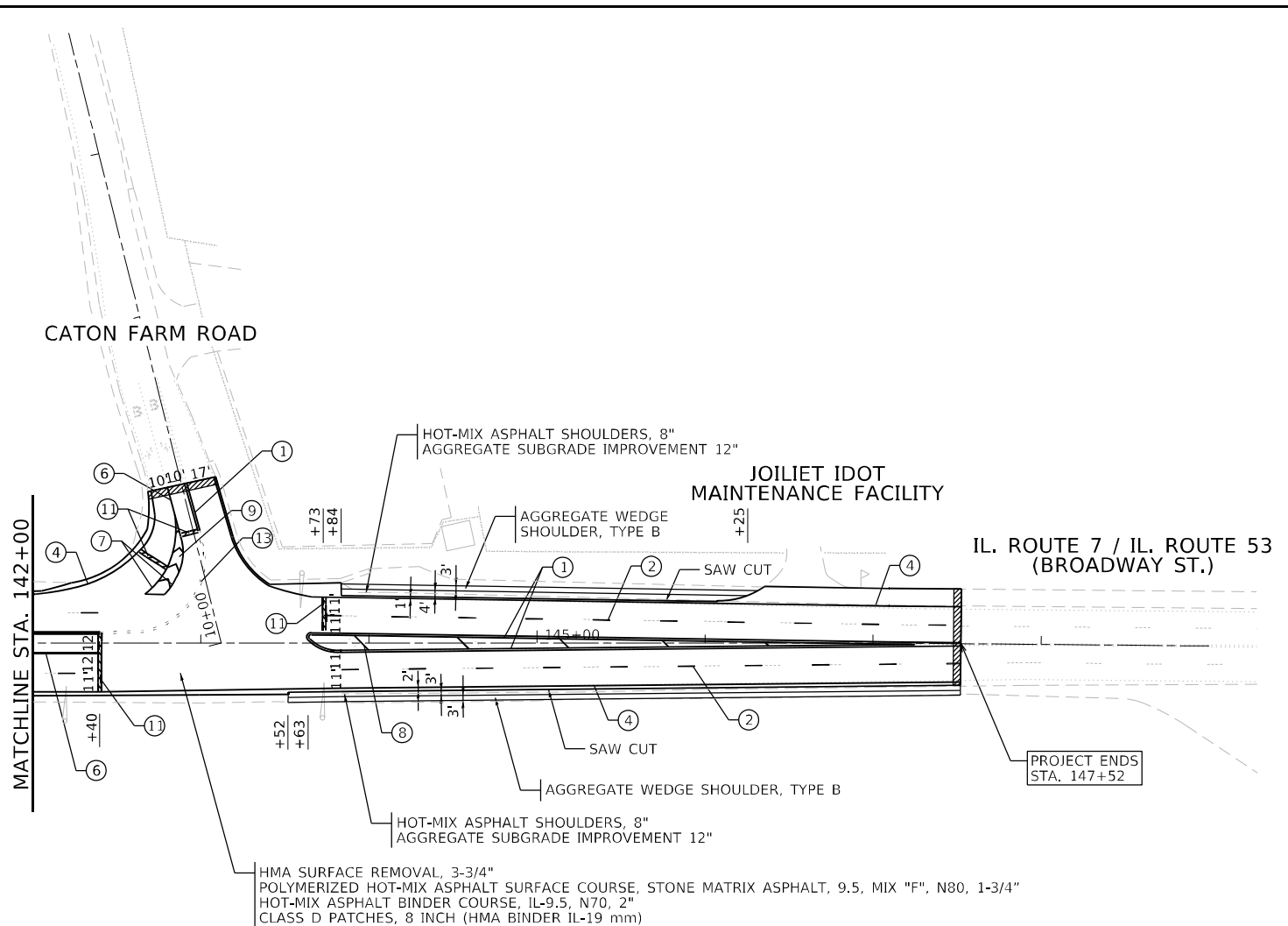
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PLOT DATE = 1/25/2023	CHECKED - ACL	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
IL-7/53 (THEODORE ST TO CATON FARM RD)**

SCALE: 1" = 50'

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	12
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES
- ③ LINE 4", SOLID, YELLOW - EDGE LINE (TYP)
- ④ LINE 4", SOLID WHITE - EDGE LINE (TYP)
- ⑤ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)
- ⑥ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP)
- ⑦ LINE 8", SOLID, WHITE - GORE MARKING AND CHANNELIZING LINES (TYP)
- ⑧ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP)
(SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑨ LINE 12", SOLID, WHITE - CHANNELIZATION LINES 45° (TYP)
(SEE DISTRICT TC-13 FOR SPACING)
- ⑩ LINE 12", SOLID, WHITE - CONTINENTAL CROSSWALK (TYP)
- ⑪ LINE 24", SOLID, WHITE - STOP LINES (TYP)
- ⑫ LETTERS & SYMBOLS, SOLID, WHITE - TURN LANE MARKINGS (TYP)
- ⑬ LINE 2 @ 6" SKIP-DASH, YELLOW (2' LINE - 6' SPACE) - DOTTED LINES (TYP)

LEGEND

- HMA SURFACE REMOVAL BUTT JOINT, 4.5'

NOTES:

1. LIMIT OF RESURFACING ON THE SIDE STREET THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
2. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE) ON HMA PAVEMENT AND MODIFIED URETHANE ON CONCRETE PAVEMENT
3. ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
4. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.

MODEL Path: \\...
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 1 South Wacker | Suite 2650 | Chicago, IL 60606
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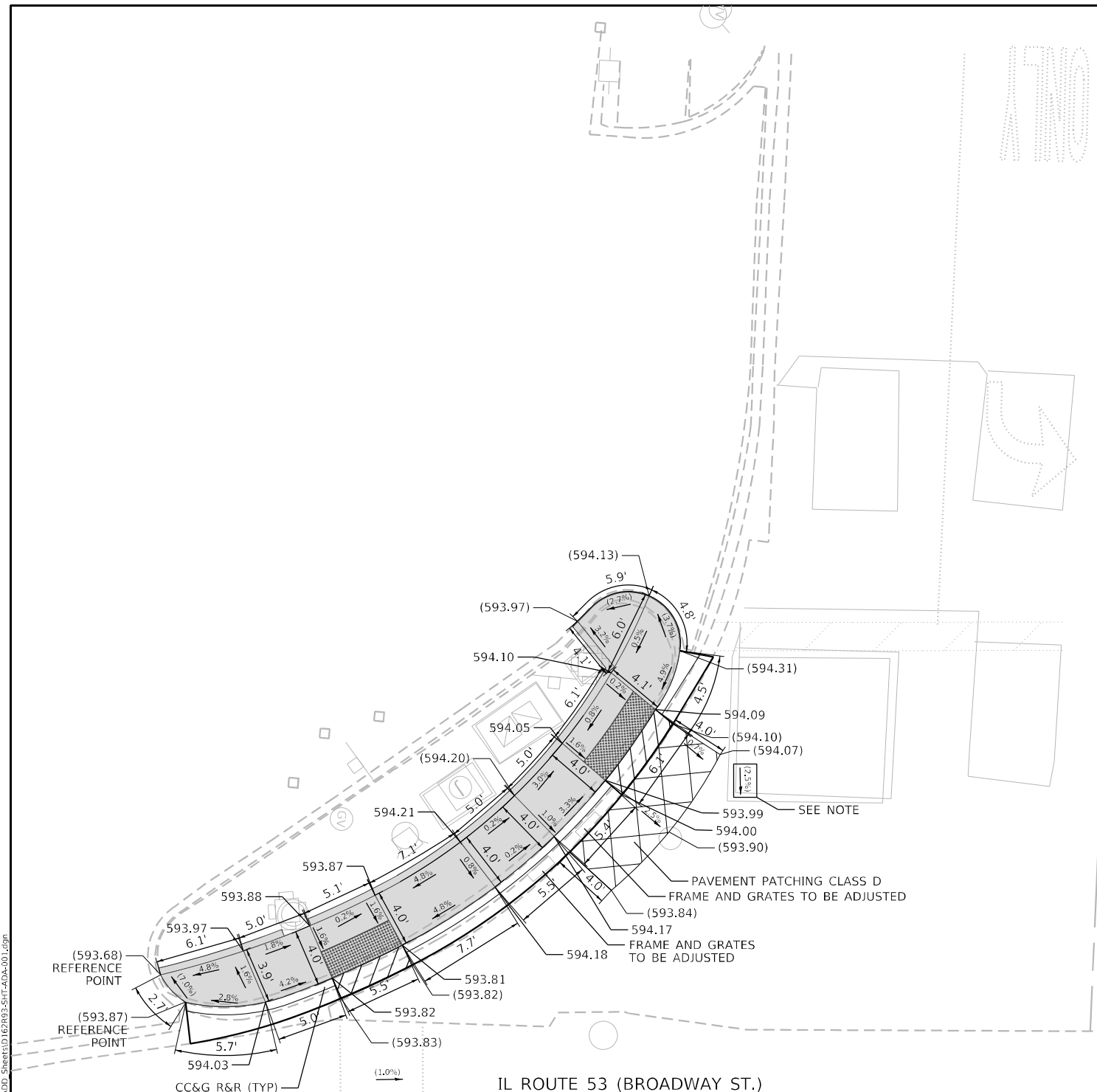
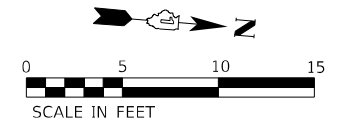
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	DATE - 01/25/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

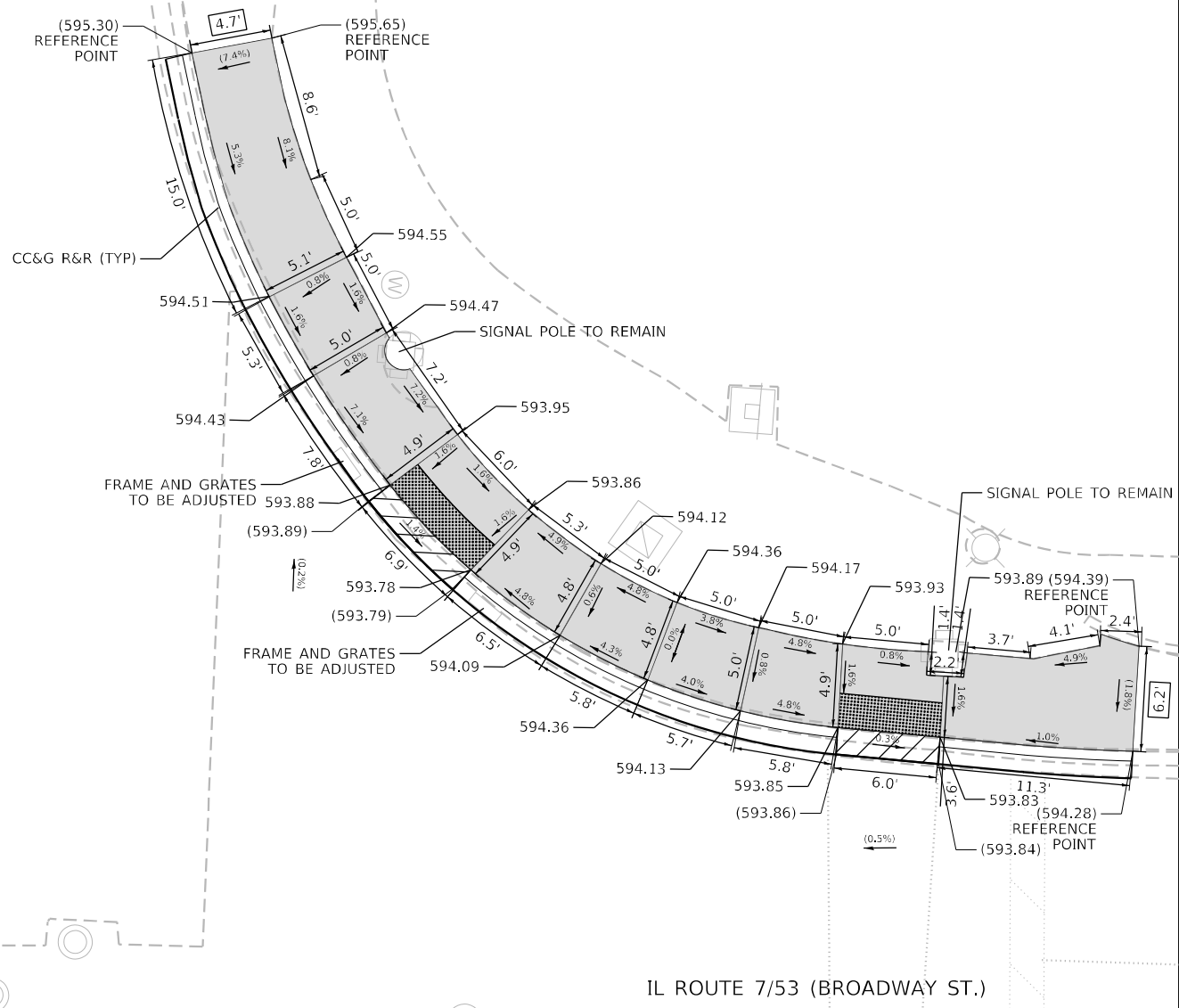
**ROADWAY AND PAVEMENT MARKING PLANS
IL-7/53 (THEODORE ST TO CATON FARM RD)**

SCALE: 1" = 50' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	13
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



IL ROUTE 7 (THEODORE ST.)



IL ROUTE 7/53 (BROADWAY ST.)

REFERENCE BENCHMARK: #1 EL. 594.21
 BENCHMARK: SET "X" IN SIDEWALK
 LOCATION: SW CORNER OF ROUTE 7 AND BROADWAY ST.

NOTE:
 1. REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS. CROSSWALK WIDTH SHALL BE A MINIMUM OF 6 FT AND SHALL BE OFFSET A MINIMUM OF 4 FT FROM THE STOP BAR.
 2. CONTRACTOR TO CORRECT CROSS SLOPE TO LESS THAN 2% WITHIN THE CROSSWALK WITH THE MILLING MACHINE.

LEGEND

xx.xx'	EXISTING LENGTH
()	EXISTING ELEVATION / SLOPE
[Solid Gray Box]	PROPOSED SIDEWALK
[Grid Pattern Box]	DETECTABLE WARNINGS
[Diagonal Line Pattern Box]	DEPRESSED CURB AND GUTTER
[Cross-hatch Pattern Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD

MODEL: D:\p\inf\... FILE NAME: P:\2020\4094-00 IDOT_Various Phase 2 (ITB)_106 Item 16\NW0_28_62993\DC\CADD_Sheet\1162993-517-ADA-01.dwg

 INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9594 www.infrastructure-eng.com	USER NAME = Alane	DESIGNED - KEK	REVISED -
	PLOT SCALE = 10,0000' / in.	DRAWN - KEK	REVISED -
	PLOT DATE = 1/26/2023	CHECKED - ACL	REVISED -
		DATE - 01/25/2023	

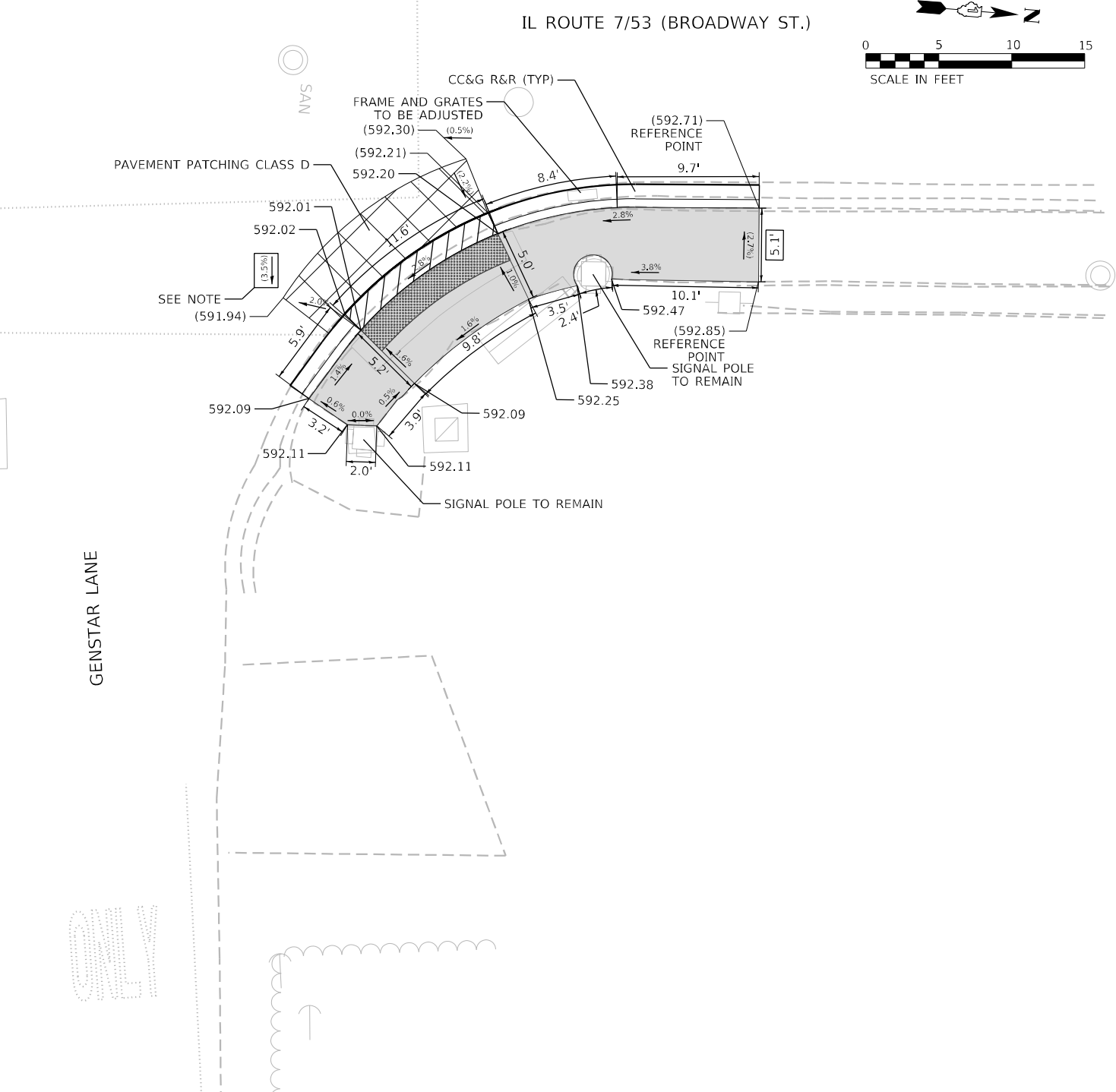
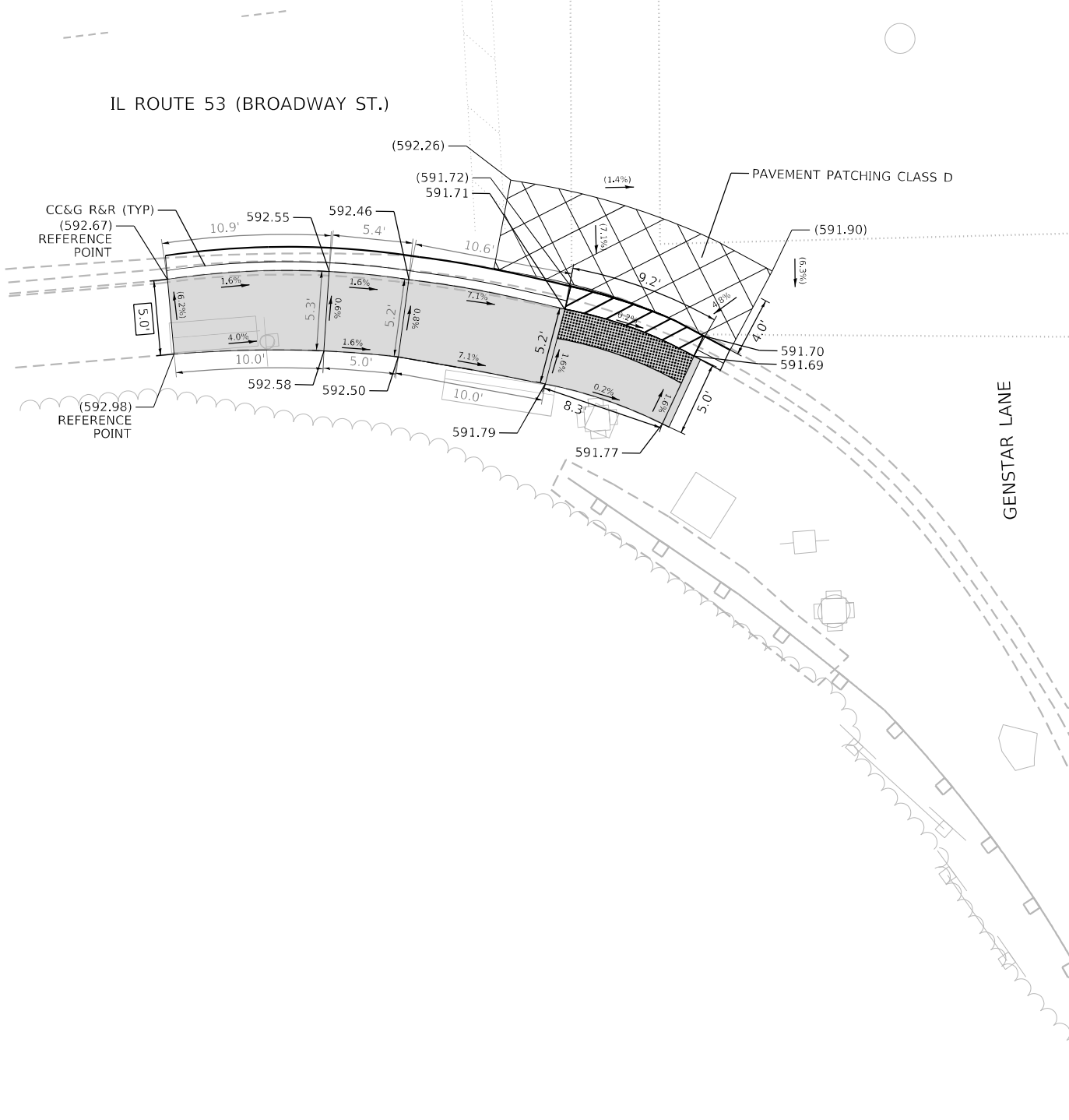
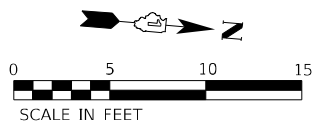
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
 BROADWAY ST AT THEODORE ST**

SCALE: 1" = 5'

STA. TO STA.

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 14
CONTRACT NO. 62R93			ILLINOIS FED. AID PROJECT	



REFERENCE BENCHMARK: #1 EL. 594.21
 BENCHMARK: SET "X" IN SIDEWALK
 LOCATION: SW CORNER OF ROUTE 7 AND BROADWAY ST.

- NOTE:
- REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS. CROSSWALK WIDTH SHALL BE A MINIMUM OF 6 FT AND SHALL BE OFFSET A MINIMUM OF 4 FT FROM THE STOP BAR.
 - CONTRACTOR TO CORRECT CROSS SLOPE TO LESS THAN 2% WITHIN THE CROSSWALK WITH THE MILLING MACHINE.

LEGEND

xx.xx	EXISTING LENGTH
()	EXISTING ELEVATION / SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	DEPRESSED CURB AND GUTTER
	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD

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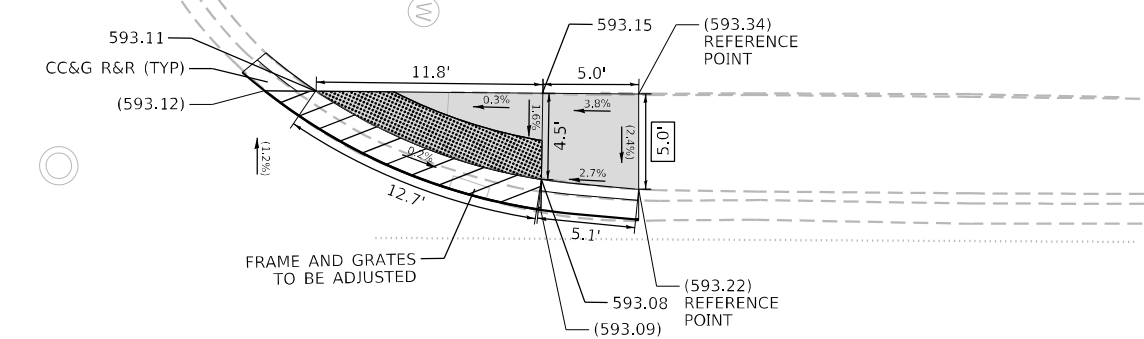
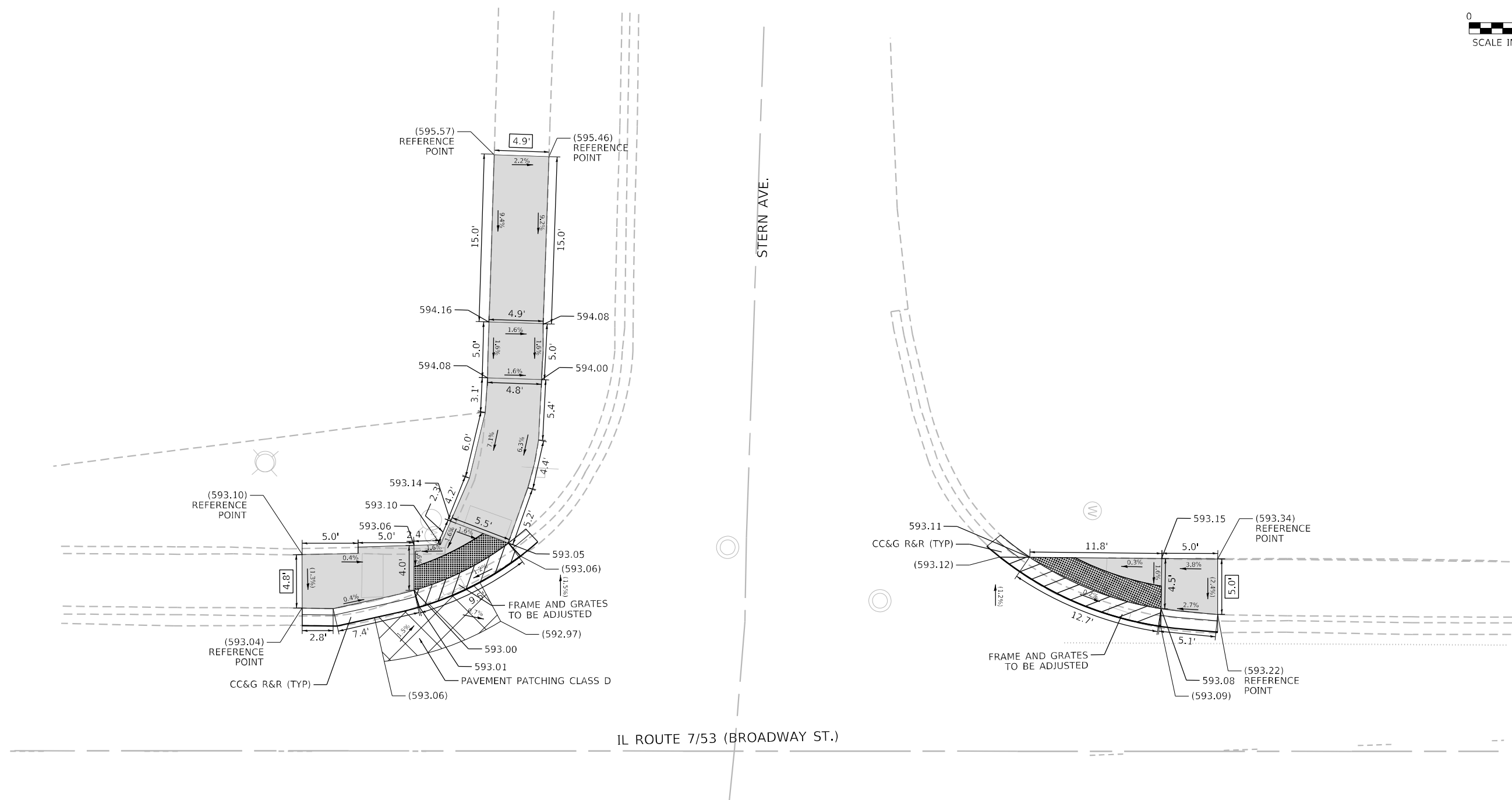
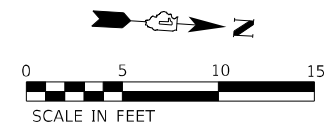
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PLOT DATE = 1/26/2023	CHECKED - ACL	REVISED -
	DATE - 01/25/2023	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
 BROADWAY ST AT GENSTAR LANE**

SCALE: 1" = 5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	15
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



REFERENCE BENCHMARK: #24 EL. 593.91
 BENCHMARK: BOX CUT IN NORTH END OF DOOR SILL TO 1702 BROADWAY ST.
 LOCATION: NE CORNER OF STERN AVE. AND BROADWAY ST.

NOTE:

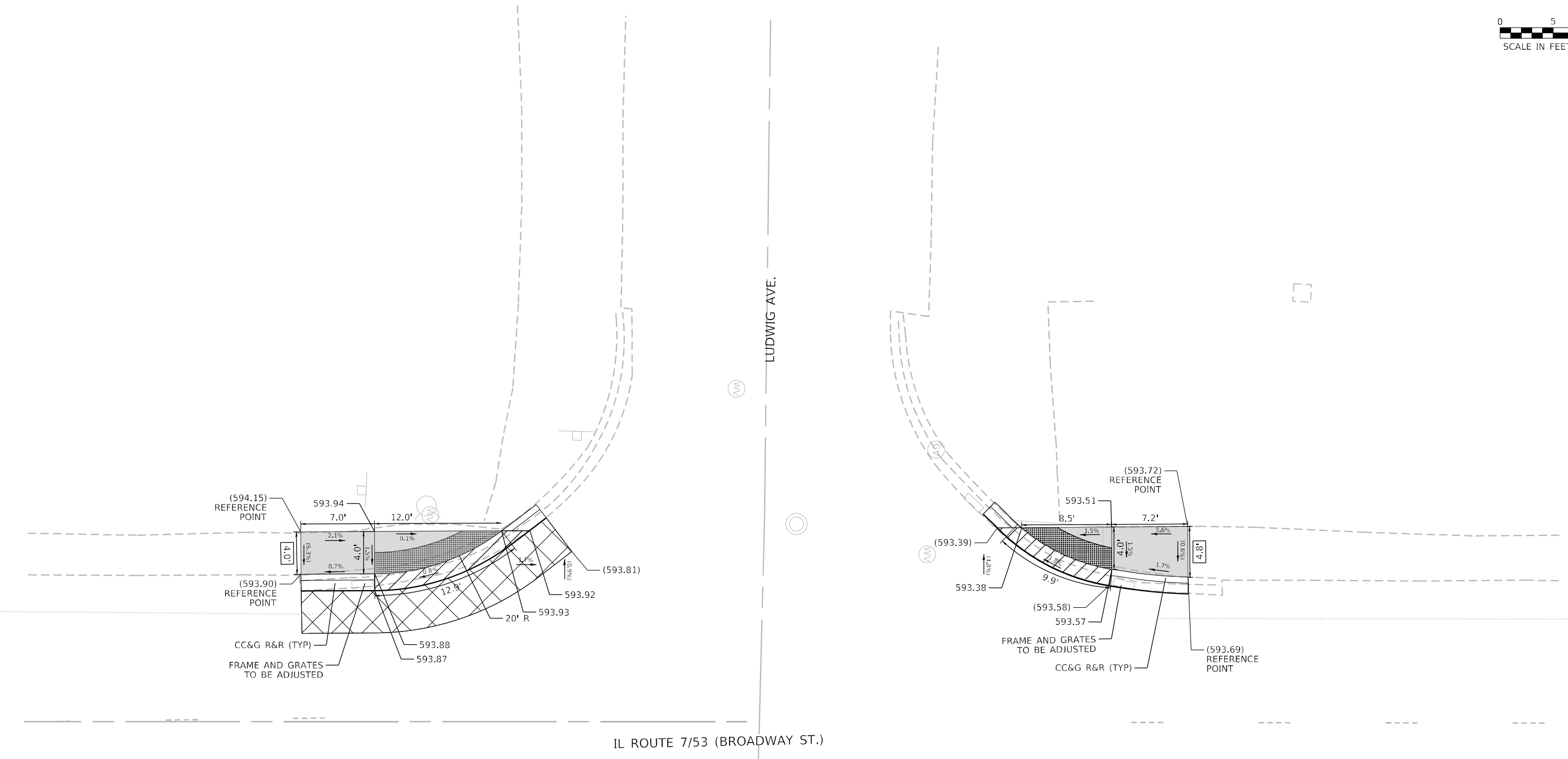
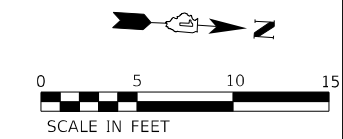
1. REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS. CROSSWALK WIDTH SHALL BE A MINIMUM OF 6 FT AND SHALL BE OFFSET A MINIMUM OF 4 FT FROM THE STOP BAR.

LEGEND

xx.xx'	EXISTING LENGTH
()	EXISTING ELEVATION / SLOPE
[Solid Gray Box]	PROPOSED SIDEWALK
[Cross-hatched Box]	DETECTABLE WARNINGS
[Diagonal Line Box]	DEPRESSED CURB AND GUTTER
[X-pattern Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD

MODEL: P:\dxf\h1 FILE NAME: P:\2023\04\09\400 IDOT\Unbld Phase 2 (ITR) 106 Item 161\W0_28 62R93\DWG\CADD_Sheet1D162R93-SHT-ADA-003.dwg

INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P: 312.425.9599 F: 312.425.9598 www.infrastructure-eng.com	USER NAME = Alane DESIGNED - KEK DRAWN - KEK CHECKED - ACL DATE - 01/25/2023	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ADA RAMP DETAILS BROADWAY ST AT STERN AVE		F.A.P. RTE. 112 SECTION FAP-0112-22-BJ COUNTY WILL TOTAL SHEETS 40 SHEET NO. 16	CONTRACT NO. 62R93 ILLINOIS FED. AID PROJECT
	PLOT SCALE = 10,0000' / in. PLOT DATE = 1/26/2023			SCALE: 1" = 5' STA. TO STA.			



IL ROUTE 7/53 (BROADWAY ST.)

LUDWIG AVE.

REFERENCE BENCHMARK: #23 EL. 593.94
 BENCHMARK: BOX CUT AT EAST END OF CONC. WALK IN FRONT OF 1801 BROADWAY ST
 LOCATION: NW COR. OF LUDWIG AVE, AND BROADWAY ST.

NOTE:
 1. REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS. CROSSWALK WIDTH SHALL BE A MINIMUM OF 6 FT AND SHALL BE OFFSET A MINIMUM OF 4 FT FROM THE STOP BAR.

LEGEND

xx.xx'	EXISTING LENGTH
()	EXISTING ELEVATION / SLOPE
[Solid Gray]	PROPOSED SIDEWALK
[Grid Pattern]	DETECTABLE WARNINGS
[Diagonal Lines]	DEPRESSED CURB AND GUTTER
[Cross-hatch Pattern]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD

MODEL: P:\d\p\111\111.dwg
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 1 South Wacker | Suite 2650 | Chicago, IL 60606
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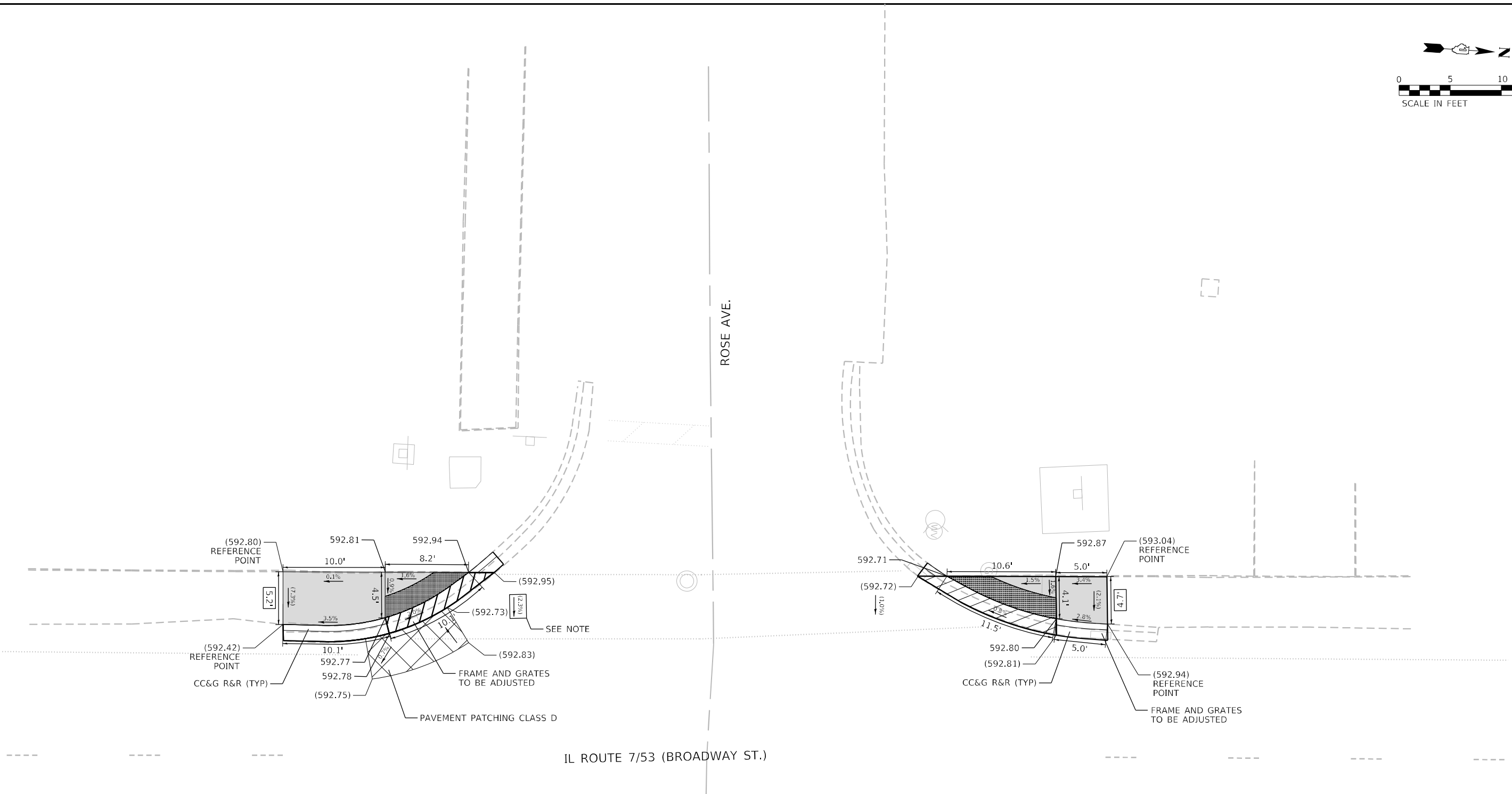
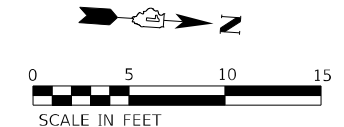
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PLOT DATE = 1/26/2023	CHECKED - ACL	REVISED -
	DATE - 01/25/2023	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
 BROADWAY ST AT LUDWIG AVE**

SCALE: 1" = 5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



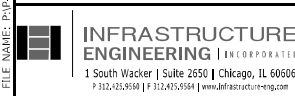
REFERENCE BENCHMARK: #22 EL. 595.69
 BENCHMARK: BOX CUT AT NE CORNER OF CONC. SLAB NEAR DOOR TO 1901 BROADWAY ST.
 LOCATION: NW CORNER OF ROSE AVE AND BROADWAY ST.

- NOTE:
- REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS. CROSSWALK WIDTH SHALL BE A MINIMUM OF 6 FT AND SHALL BE OFFSET A MINIMUM OF 4 FT FROM THE STOP BAR.
 - CONTRACTOR TO CORRECT CROSS SLOPE TO LESS THAN 2% WITHIN THE CROSSWALK WITH THE MILLING MACHINE.

LEGEND

xx.xx'	EXISTING LENGTH
()	EXISTING ELEVATION / SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	DEPRESSED CURB AND GUTTER
	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD

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	DATE - 01/25/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

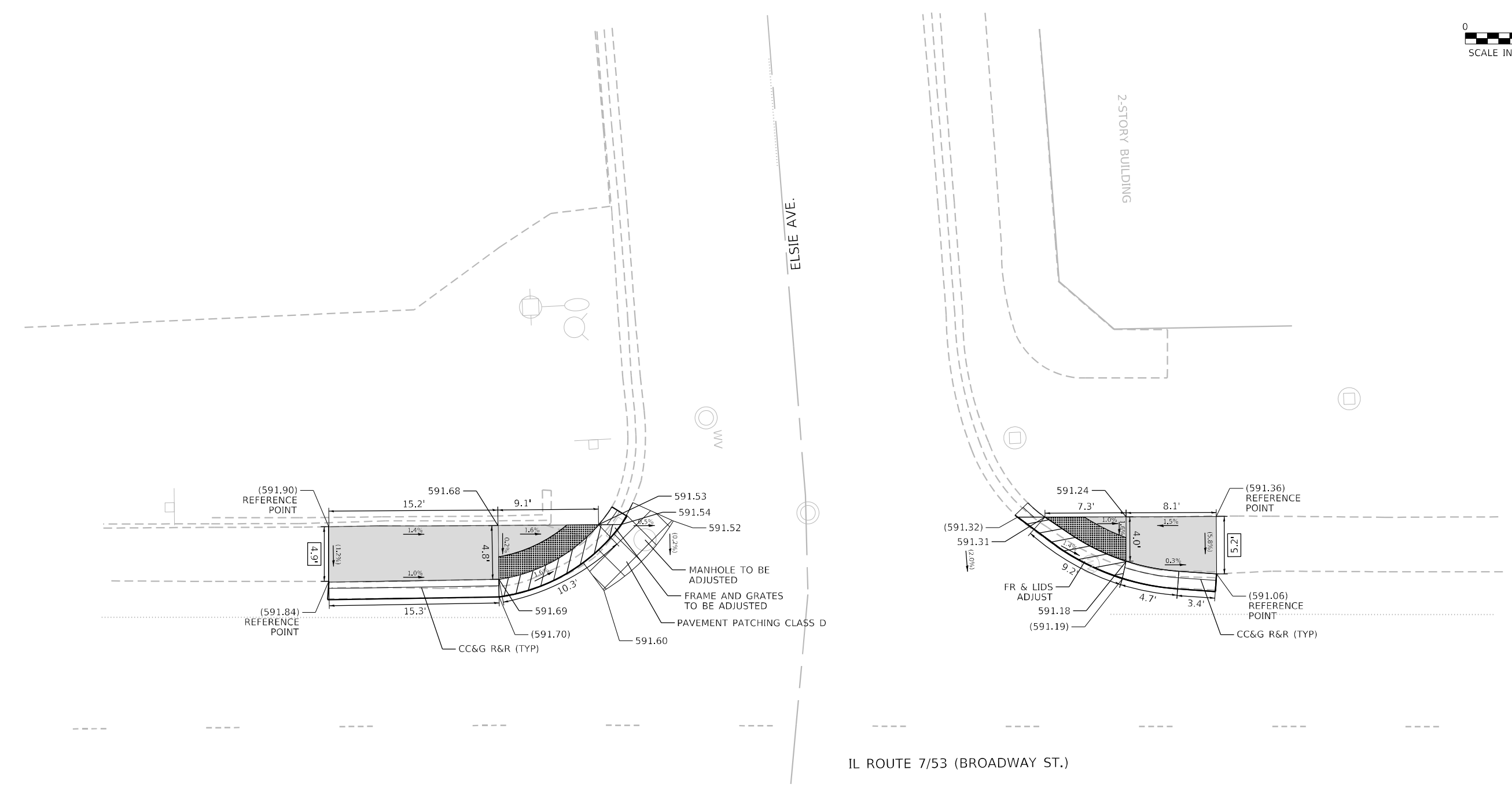
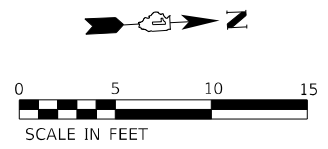
**ADA RAMP DETAILS
BROADWAY ST AT ROSE AVE**

SCALE: 1" = 5'

STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	18
CONTRACT NO. 62R93				

ILLINOIS FED. AID PROJECT



IL ROUTE 7/53 (BROADWAY ST.)

REFERENCE BENCHMARK: #21A 596.30
 BENCHMARK: BOX CUT AT S.E. CORNER OF FOUND STAIR
 LOCATION: NW CORNER OF IL-53 AND MARQUETTE DR

NOTE:
 1. REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS. CROSSWALK WIDTH SHALL BE A MINIMUM OF 6 FT AND SHALL BE OFFSET A MINIMUM OF 4 FT FROM THE STOP BAR.

LEGEND

xx.xx'	EXISTING LENGTH
()	EXISTING ELEVATION / SLOPE
[Solid Grey]	PROPOSED SIDEWALK
[Grid Pattern]	DETECTABLE WARNINGS
[Diagonal Lines]	DEPRESSED CURB AND GUTTER
[Cross-hatch]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD

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	DATE - 01/25/2023	

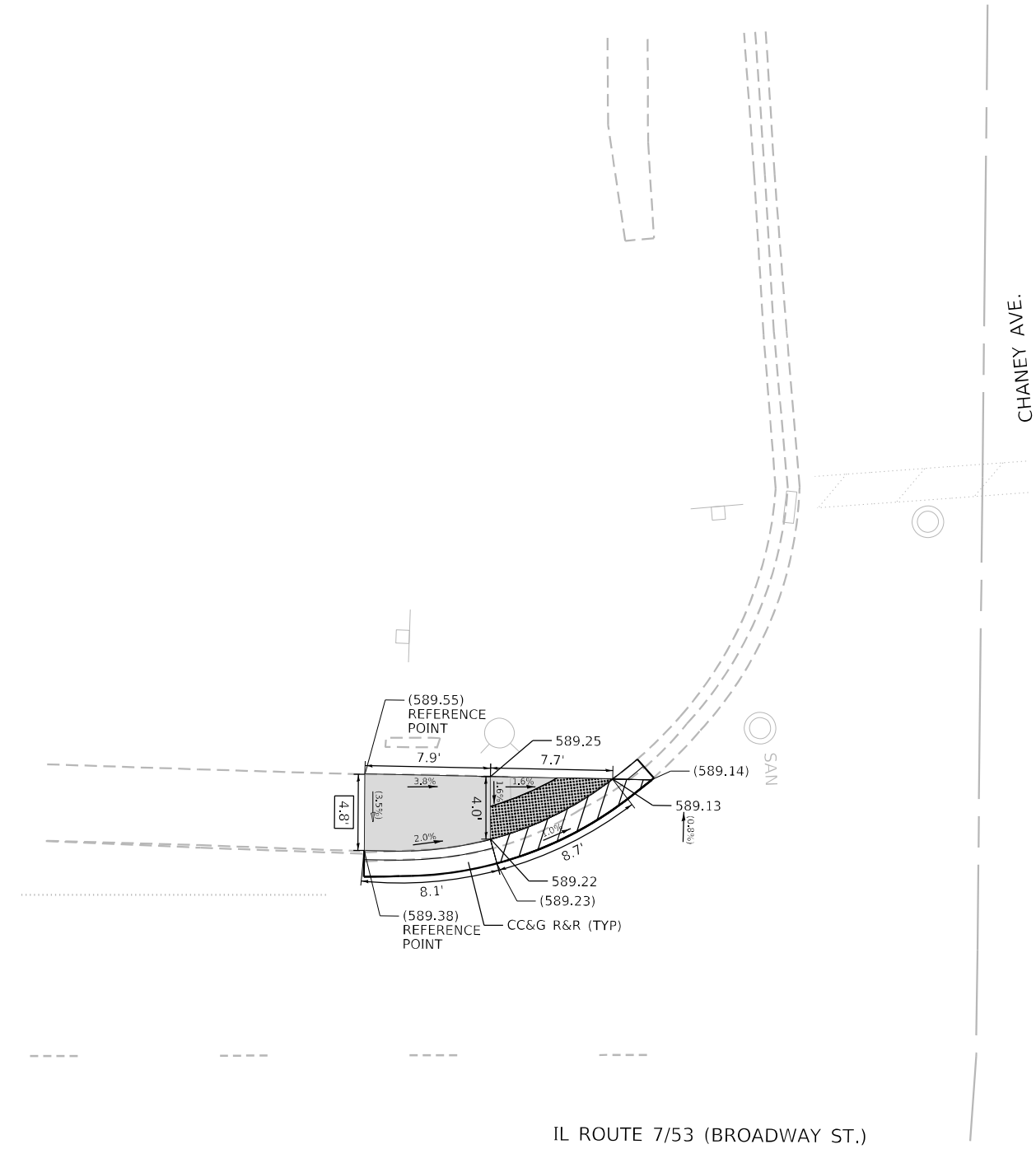
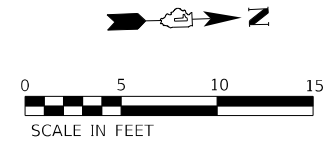
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS
 BROADWAY ST AT ELSIE AVE**

SCALE: 1" = 5' STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	19
CONTRACT NO. 62R93				

ILLINOIS FED. AID PROJECT



REFERENCE BENCHMARK: #20 EL. 594.52
 BENCHMARK: BOX CUT ON EAST CURB OF FIRST DRIVEWAY WEST OF BROADWAY STREET
 LOCATION: NW COR. OF BROADWAY AND CHANEY AVE.

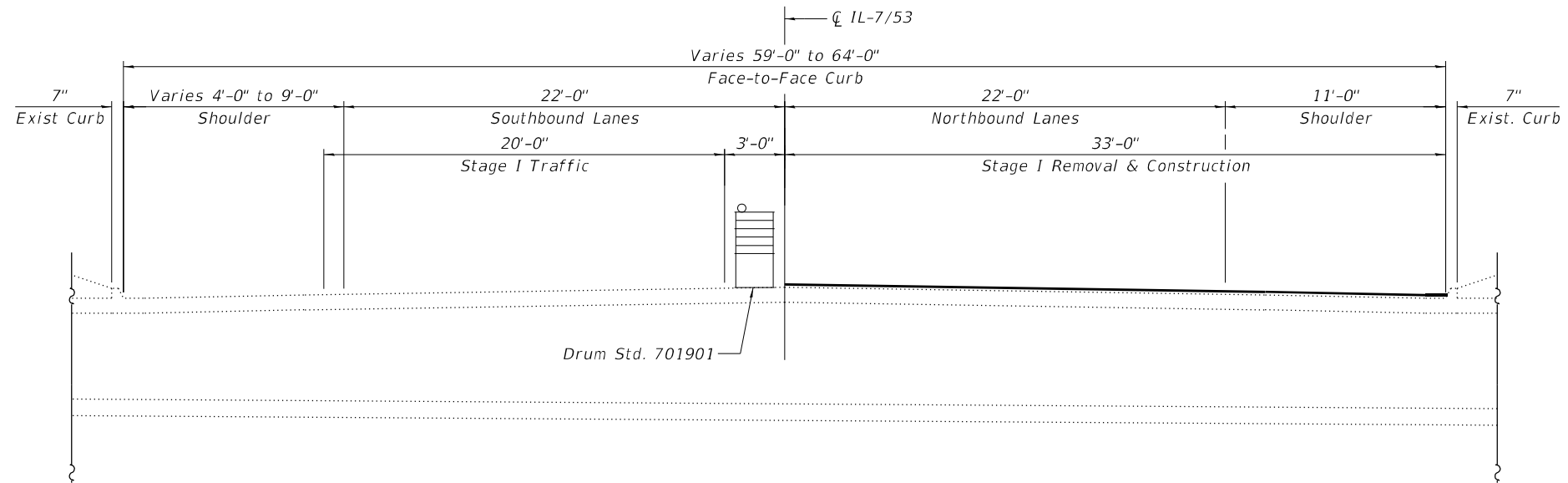
NOTE:
 1. REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS. CROSSWALK WIDTH SHALL BE A MINIMUM OF 6 FT AND SHALL BE OFFSET A MINIMUM OF 4 FT FROM THE STOP BAR.

LEGEND

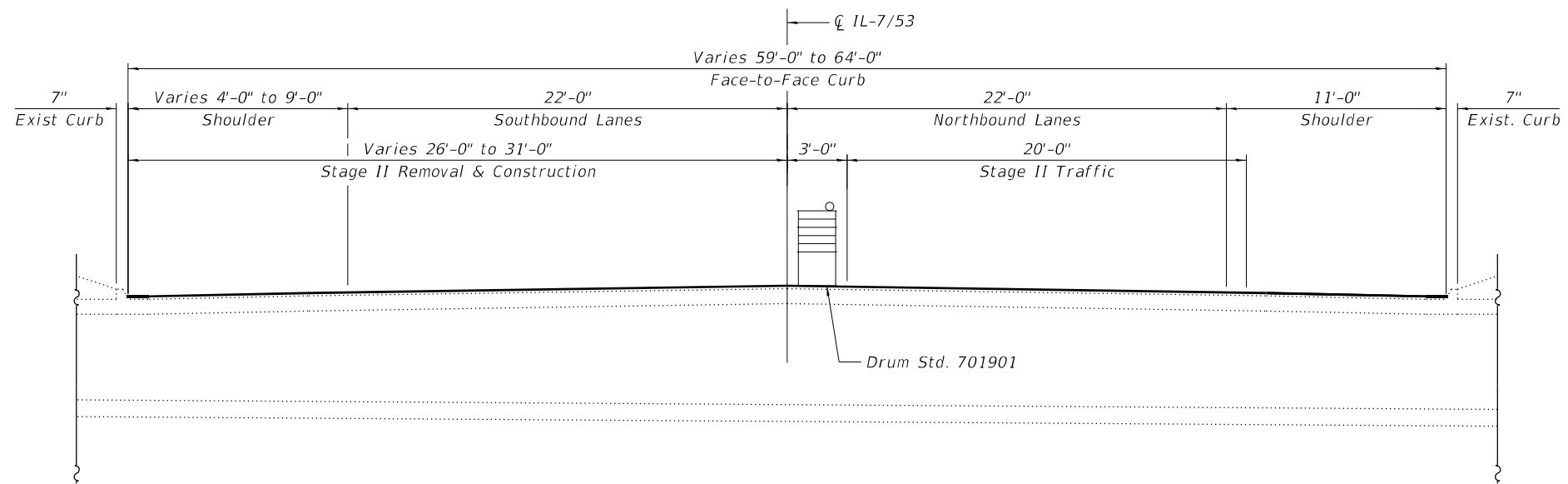
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	EXISTING ELEVATION / SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	DEPRESSED CURB AND GUTTER
	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD

MODEL: D:\p\h\h\... INFRASTRUCTURE ENGINEERING INCORPORATED

INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9598 www.infrastructure-eng.com	USER NAME = Alane PLOT SCALE = 10,0000 * / in. PLOT DATE = 1/26/2023	DESIGNED - KEK DRAWN - KEK CHECKED - ACL DATE - 01/25/2023	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ADA RAMP DETAILS BROADWAY ST AT CHANEY AVE	F.A.P. RTE. 112 SECTION FAP-0112-22-BJ COUNTY WILL TOTAL SHEETS 40 SHEET NO. 20 CONTRACT NO. 62R93	SCALE: 1" = 5' STA. TO STA.	ILLINOIS FED. AID PROJECT



STAGE I REMOVAL AND CONSTRUCTION
(Looking North)



STAGE II REMOVAL AND CONSTRUCTION
(Looking North)

MODEL Path: I:\Projects\020-04094-00_IDOT_Various Phase 2 (ITB)_106 Item: 161110_28_62R93\DC\CAD\Drawings\Sheet\161110-28-62R93-511-STR-02.dgn

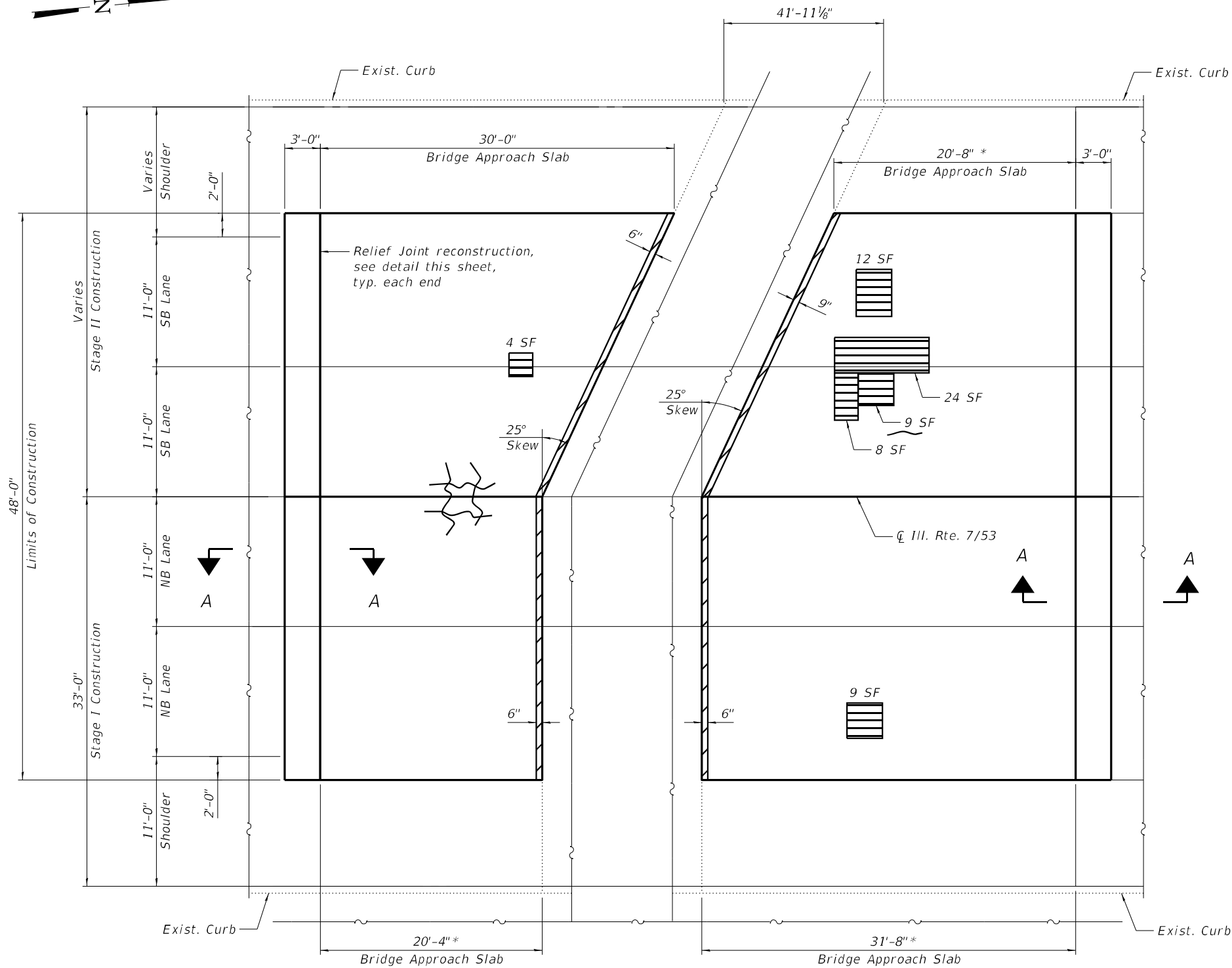
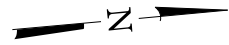
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USER NAME = Alane	DESIGNED - MDA	REVISED -
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PLOT DATE = 3/16/2023		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

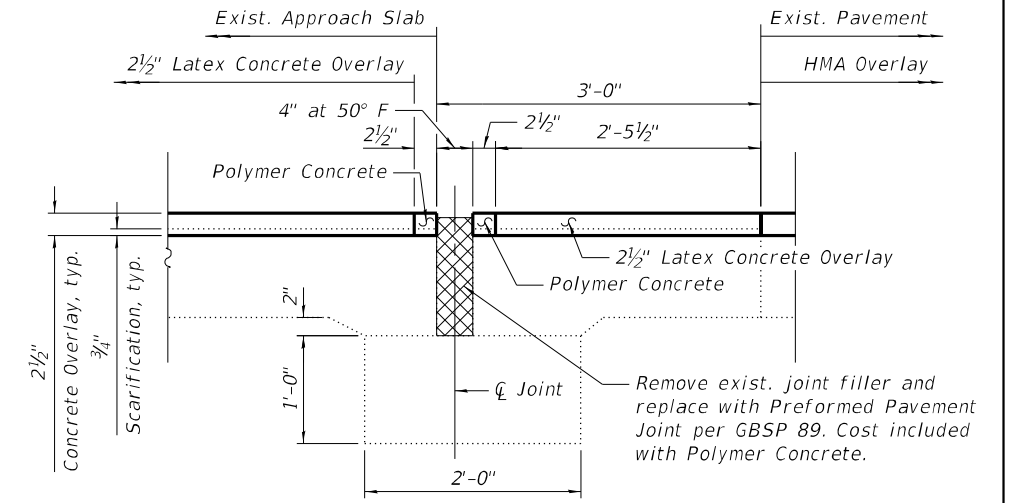
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SCALE:	SHEET 2 OF 4 SHEETS	STA. 2	TO STA. 4

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 22
CONTRACT NO. 62R93			ILLINOIS FED. AID PROJECT	



SOUTH BRIDGE APPROACH SLAB PLAN

NORTH BRIDGE APPROACH SLAB PLAN



SECTION A-A
(At the end of approach slabs)

LEGEND

- Deck Slab Repair (Partial Depth)
- Approach Slab Repair (Partial Depth)
- Crack (For Record Only)

NOTES

1. Quantities and limits of Approach Slab Repairs shown herein are estimated from a survey completed by the District. Actual limits shall be determined in the field by the Engineer and noted in the As-Built Plans.
2. Approach Slab Repair locations and quantities shown herein are for information only and will not be measured for payment but shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 1/2 Inches.
3. Protective Coat shall be applied to the top of new concrete overlay areas.

* Field Measurement

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Bridge Deck Grooving	Sq Yd	340
Protective Coat	Sq Yd	340
Bridge Deck Latex Concrete Overlay, 2 1/2 Inches	Sq Yd	340
Bridge Deck Scarification 3/4"	Sq Yd	340
Polymer Concrete	Cu Ft	5

MODEL Path: \\... INFRASTRUCTURE Phase 2 (FPEL 196 Item 16)WVD 28 62R93(2)DCM\CADD_Sheet\1162R93-5115-STR-04.dgn

INFRASTRUCTURE ENGINEERING
 1 South Wacker | Suite 2650 | Chicago, IL 60606
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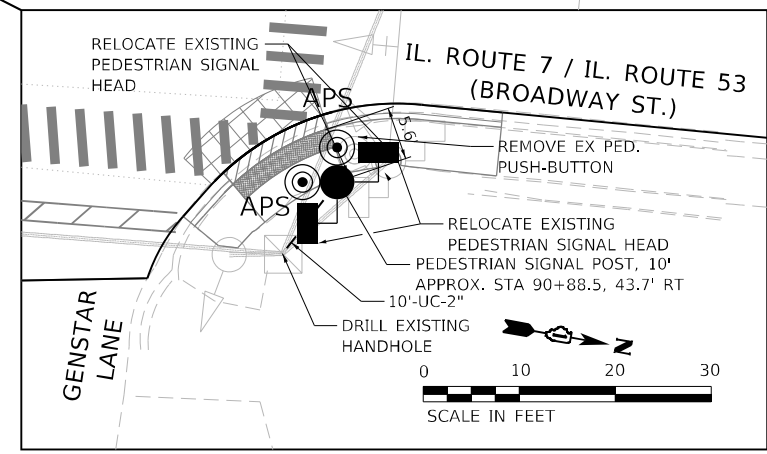
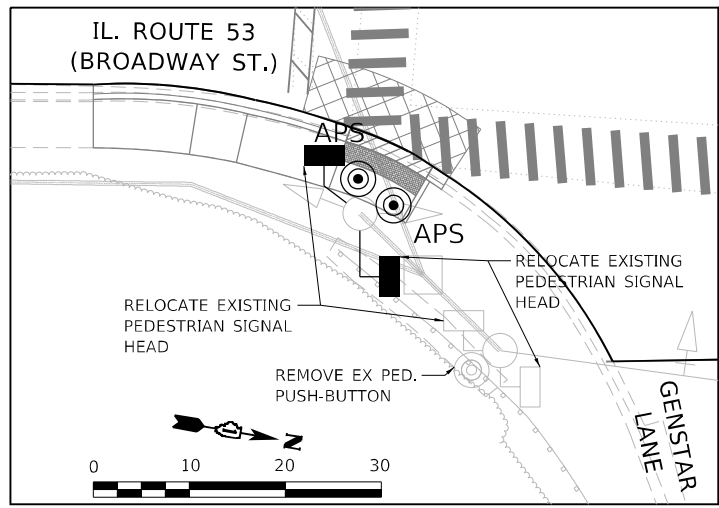
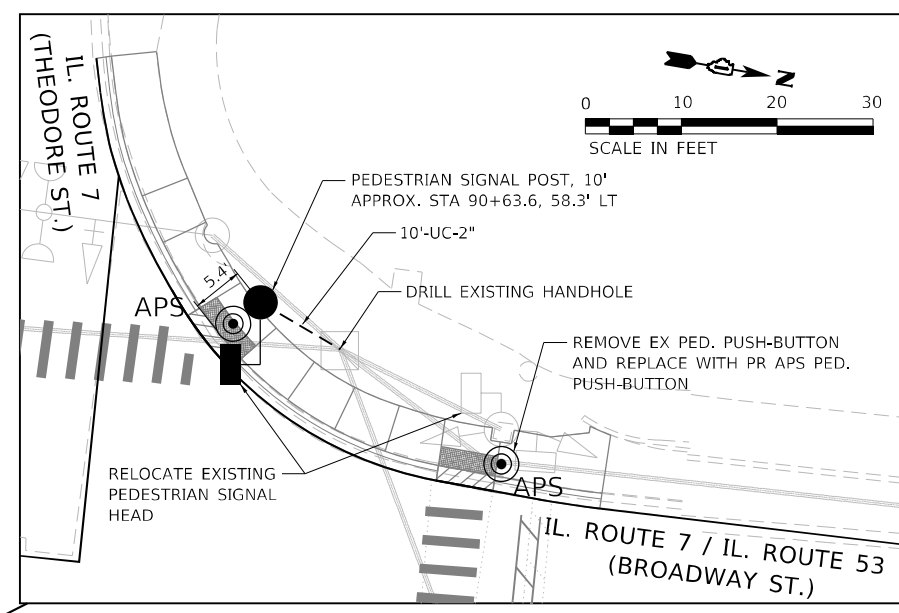
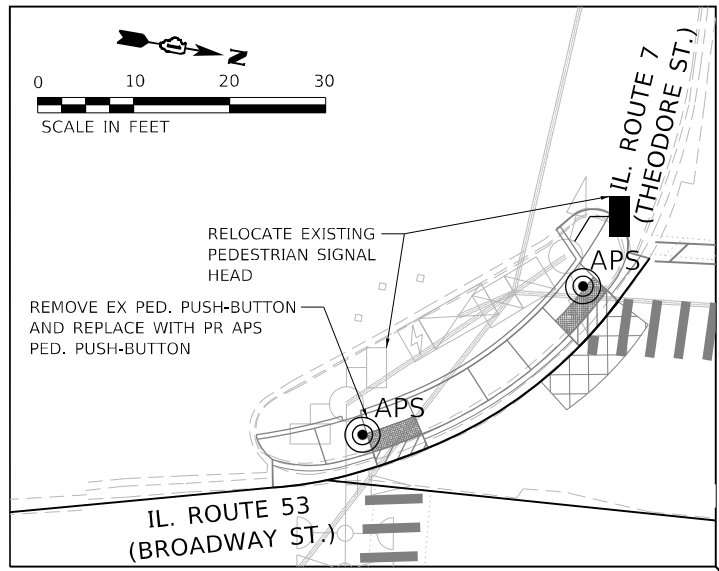
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PLOT DATE = 3/16/2023	CHECKED - PK	REVISED -
	DATE - 01/26/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRIDGE APPROACH SLABS
STRUCTURE NO. 099-0266**

SCALE: SHEET 4 OF 4 SHEETS STA. 4 TO STA. 4

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 24
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



REMOVAL AND RELOCATION NOTES:

1. THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR OWN EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.
4 EACH PEDESTRIAN PUSH-BUTTON
2. ALL EXISTING PEDESTRIAN PUSH-BUTTONS SHALL BE REMOVED.
3. CONTRACTOR SHALL PERFORM WORK IN SUCH A WAY AS TO MINIMIZE THE TIME THE PEDESTRIAN EQUIPMENT IS NOT OPERATIONAL.
4. CONTRACTOR SHALL CONFIRM THE FINAL LOCATION OF THE APS EQUIPMENT WITH THE TRAFFIC SIGNAL AREA ENGINEER BEFORE INSTALLATION.
5. APS BUTTONS SHALL BE PLACED PARALLEL TO THE CORRESPONDING CROSSWALK.

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL INSTALLATION PLAN
IL-7/53 AT THEODORE STREET/GENSTAR LANE**

TS 7515

MODEL: P:\dtd\11... FILE NAME: P:\2023\04\04\00 IDOT_Various Phase 2 (TRF) 106 Item: 161110_28_62992\DOT\CAD\DOT_Sheet\1162992-5117-15-01.dwg

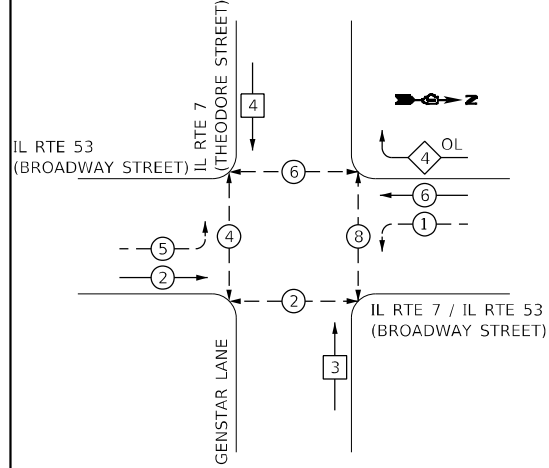
INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9594 www.infrastructure-eng.com	USER NAME = notte	DESIGNED - NFO	REVISIONS
PLOT SCALE = 40,0000 * / in.	CHECKED - MPK	DATE - 03/17/2023	REVISIONS
PLOT DATE = 3/17/2023			

DESIGNED - NFO	REVISIONS
DRAWN - NFO	REVISIONS
CHECKED - MPK	REVISIONS
DATE - 03/17/2023	

SCALE: 1" = 20'	STA. TO STA.
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F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 25
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				

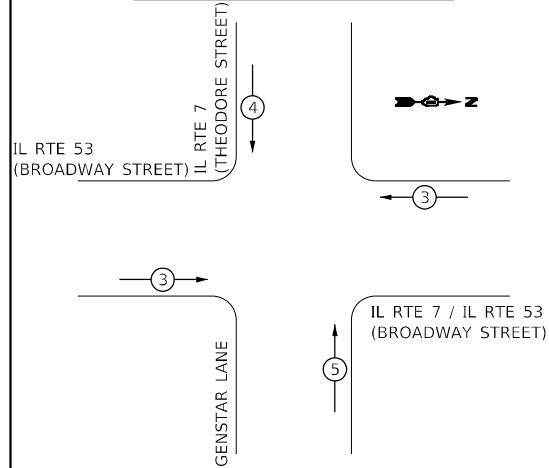
EXISTING CONTROLLER SEQUENCE



LEGEND:

- ← ⊕ → DUAL ENTRY PHASE
- ← ⊠ → SINGLE ENTRY PHASE
- ← ⊕ → PROTECTED/PERMITTED DUAL ENTRY PHASE
- ← ⊙ → PEDESTRIAN PHASE
- ← ⊠ OL → OVERLAP
- * NUMBER REFERS TO ASSOCIATED PHASE

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



EMERGENCY VEHICLE PREEMPTORS			
EMERGENCY VEHICLE PREEMPTOR	3	4	5
MOVEMENT	↔	↓	↑

TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)		17	0.50	119.0
(YELLOW)		25	0.25	87.5
(GREEN)		15	0.25	52.5
PERMISSIVE ARROW		12	0.10	19.2
PED. SIGNAL		25	1.00	200.0
CONTROLLER		100	1.00	100.0
UPS		25	1.00	25.0
VIDEO SYSTEM		150	1.00	
BLANK-OUT SIGN		25	0.05	
FLASHER			0.50	
STREET NAME SIGN		120	0.50	
LUMINAIRE				
TOTAL =				603.2

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAY/DISTRICT 1
201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096

ENERGY SUPPLY: CONTACT: _____
PHONE: _____
COMPANY: COMED _____
ACCOUNT NUMBER: _____

<p>INFRASTRUCTURE ENGINEERING 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.475.9598 F 312.475.9599 www.infrastructure-eng.com</p>	USER NAME = Alane	DESIGNED - NFO	REVISED -
	PLOT SCALE = 2,000' / in.	DRAWN - NFO	REVISED -
	PLOT DATE = 3/16/2023	CHECKED - MPK	REVISED -
		DATE - 01/26/2023	

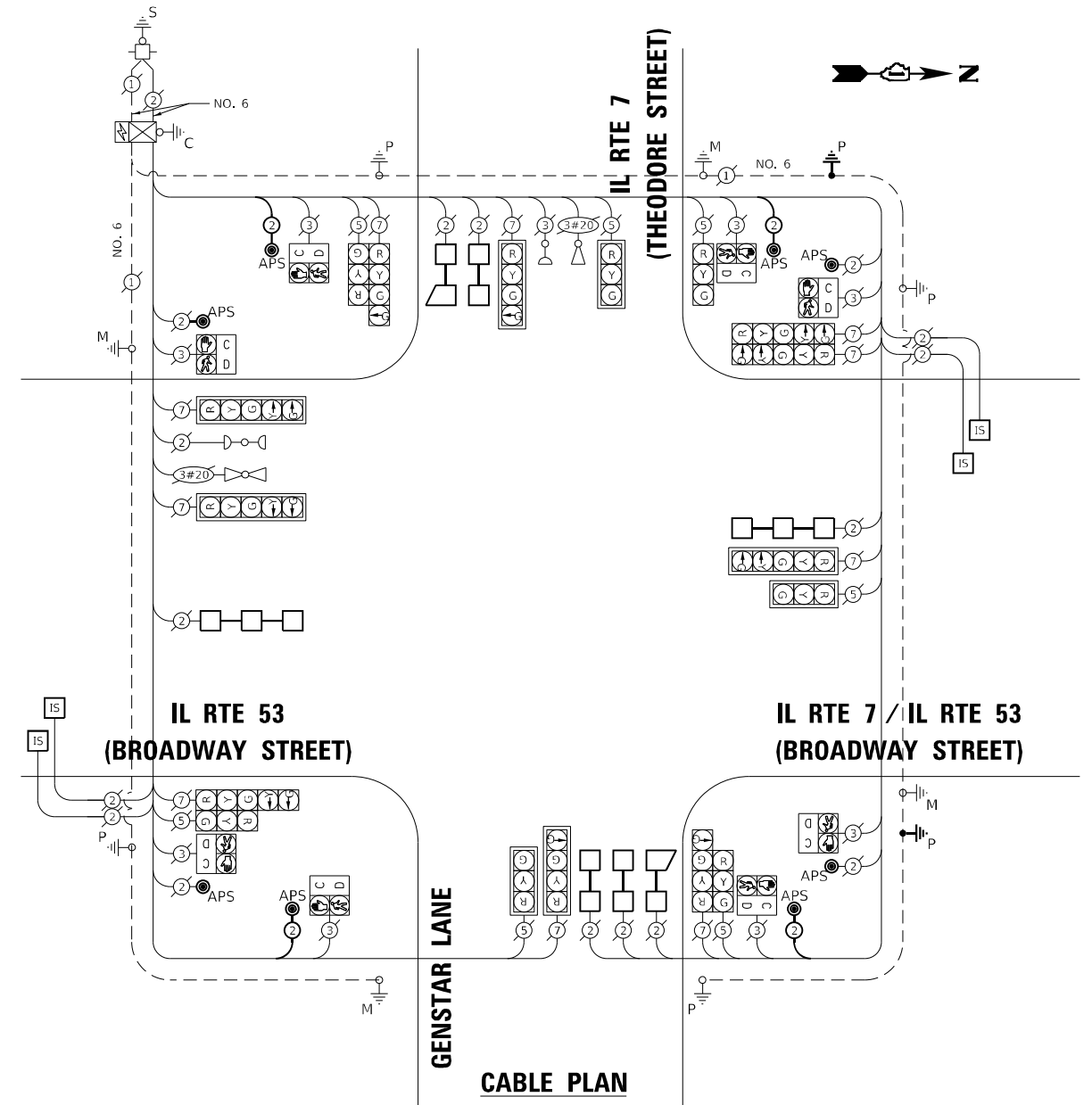
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DIAGRAM, AND EVP SEQUENCE | IL-7/53 AT THEODORE STREET/GENSTAR LANE

SCALE: NTS

STA. TO STA.

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 26
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R93	

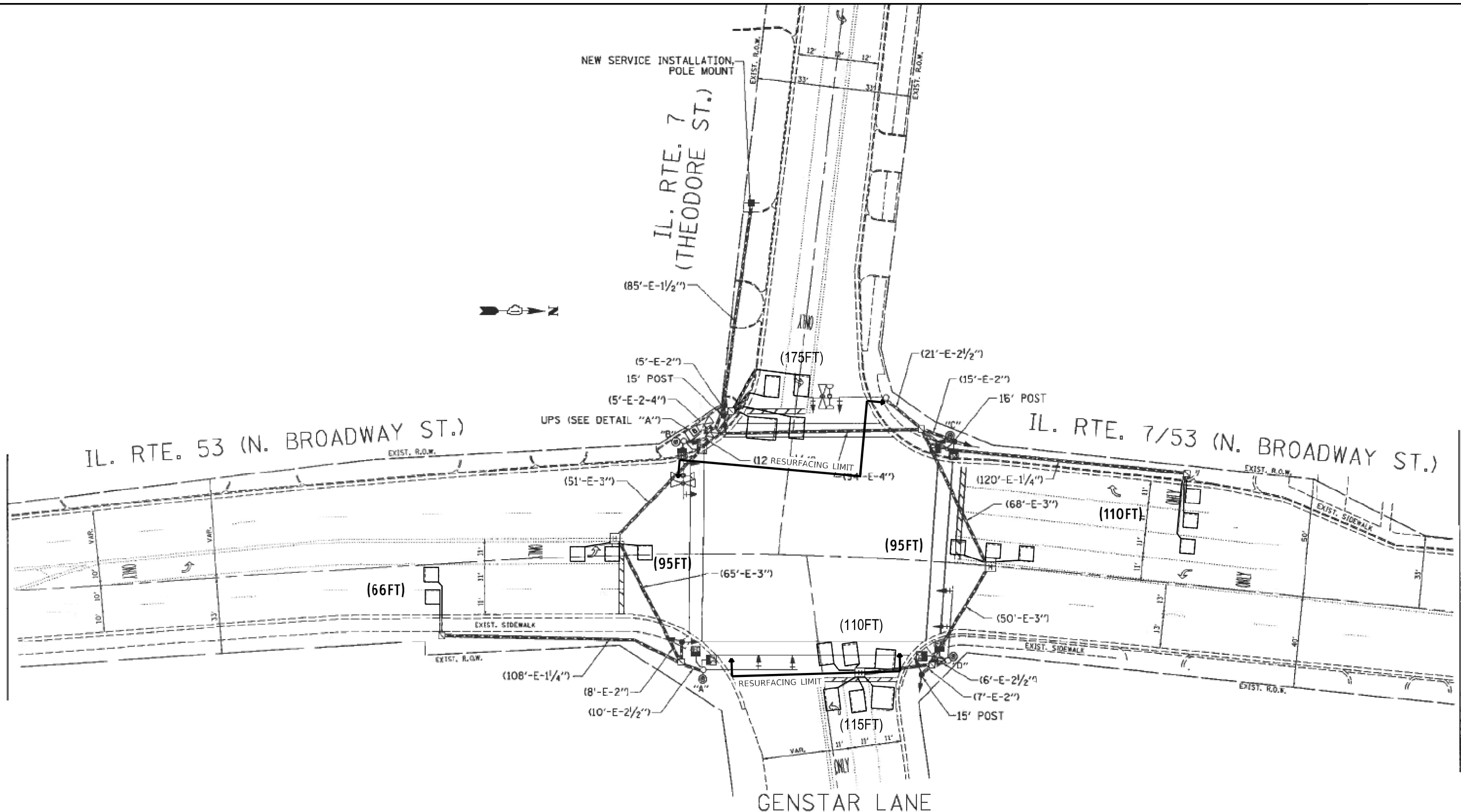


SCHEDULE OF QUANTITIES

DESCRIPTION	UNIT	TOTAL
PEDESTRIAN SIGNAL POST, 10 FT.	EACH	2
REMOVE EXISTING PEDESTRIAN PUSH BUTTON	EACH	4
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	20
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	617
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	20
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	FOOT	8
DRILL EXISTING HANDHOLE	EACH	2
DETECTOR LOOP, TYPE I	FOOT	290
DETECTOR LOOP REPLACEMENT	FOOT	920
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	6
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	169

TS 7515

MODEL: P:\ts\ts7515.dwg FILE NAME: P:\ts\ts7515.dwg DATE: 01/26/2023 10:40:00 IDOT Vehicle Phase 2 (RTE 106 Item 16) NO. 28 62R93(1)DCU\CD00_Sheet1(1)62R93-5417-TS-02.dwg



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600100	DETECTOR LOOP TYPE 1	290	FOOT
88600600	DETECTOR LOOP REPLACEMENT	476	FOOT

NOTES:

- 1.- WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
- 2.- THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

MODEL: D:\p\h\h
 FILE NAME: P:\2020\4094-00_IDOT_Vehic\Phase 2 (ITB)_106 Item 161110_28_62993\DOT\CADD_Sheet\1162993-5111-LOGS-01.dwg

USER NAME = gonzo.o.meza	DESIGNED - Steven M. Nguyen	REVISED -
	DRAWN - Gonzalo Meza	REVISED -
PLOT SCALE = 40.0000' / in.	CHECKED - Steven M. Nguyen	REVISED -
PLOT DATE = 11/28/2022	DATE - 10/24/2022	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

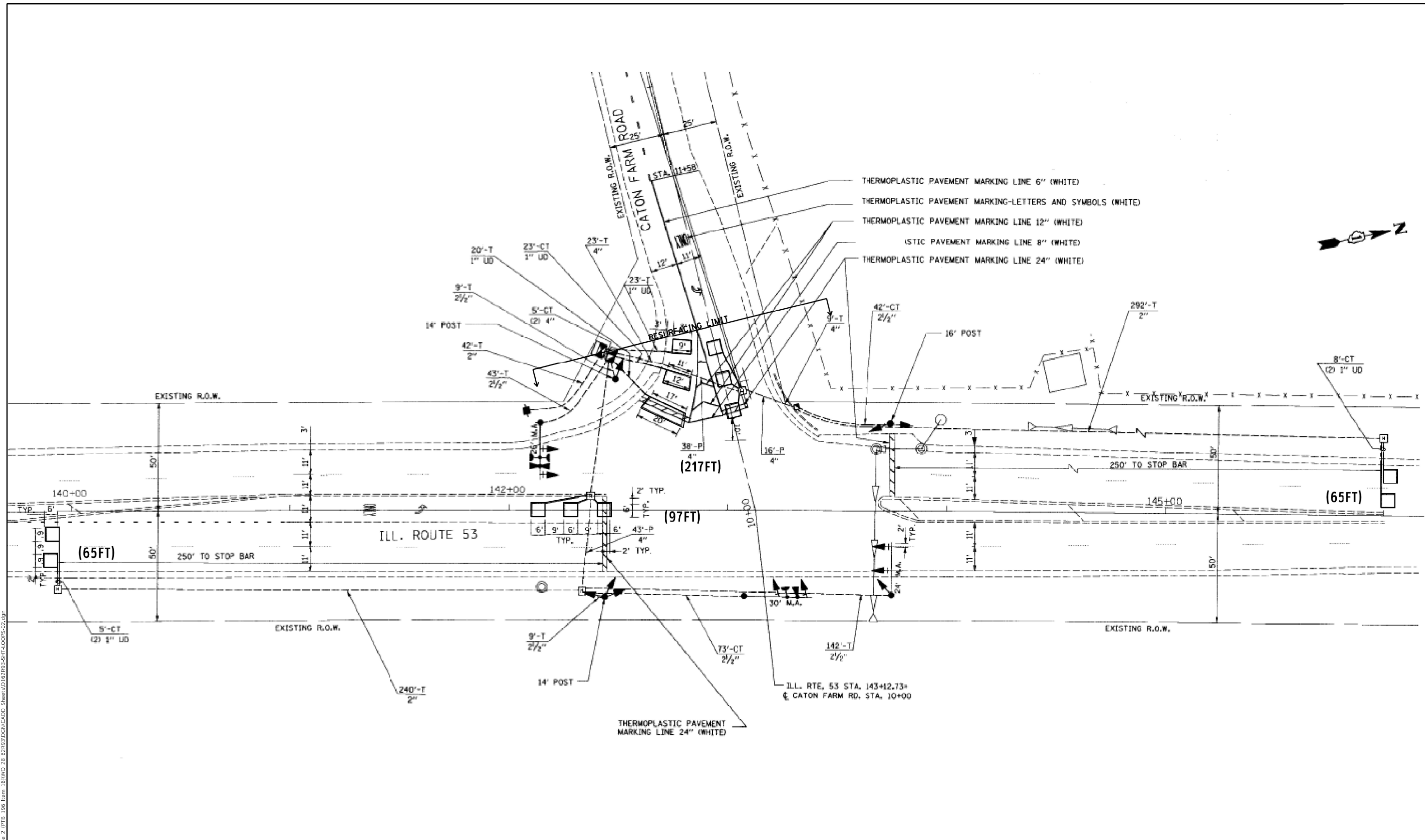
**DETECTOR LOOP REPLACEMENT PLAN
IL-753 AT THEODORE STREET/GENSTAR LANE**

SCALE: TO STA.

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 27
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R93	

TS#7515

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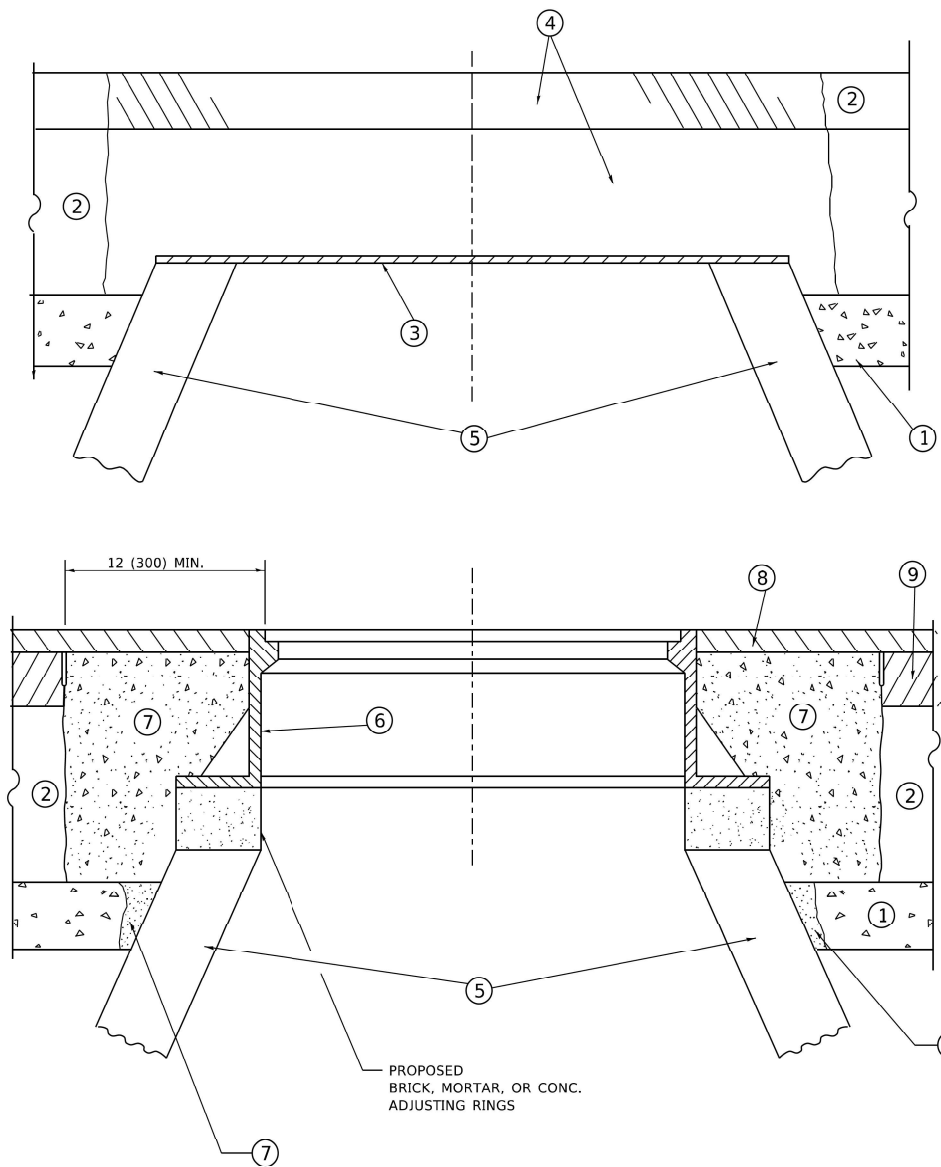
- 1.- WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
- 2.- THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	444	FOOT

TS#20561

<p>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.475.9598 F 312.475.9594 www.infrastructure-eng.com</p>	USER NAME = Alane PLOT SCALE = 20,0000 * / in. PLOT DATE = 1/25/2023	DESIGNED - Steven M. Nguy DRAWN - Gonzalo Me CHECKED - Steven M. Nguy DATE - 10/24/2022	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL-753 AT CATON FARM ROAD	F.A.P. RTE. 112 SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40 SHEET NO. 28	CONTRACT NO. 62R93	
	SCALE: _____		STA. _____ TO STA. _____			ILLINOIS FED. AID PROJECT				



**DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING**

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 1 1/2 (40) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS*PP 1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS*PP-1 CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL Path: \\p1\pub\2023\2023-04-09-400 IDOT\Info\Phase 2\ITB\106 Item 161\NO. 28 62R93\DC\DC\000_Sheet1\16162R93-SHT-STD\ARDS-01.dwg
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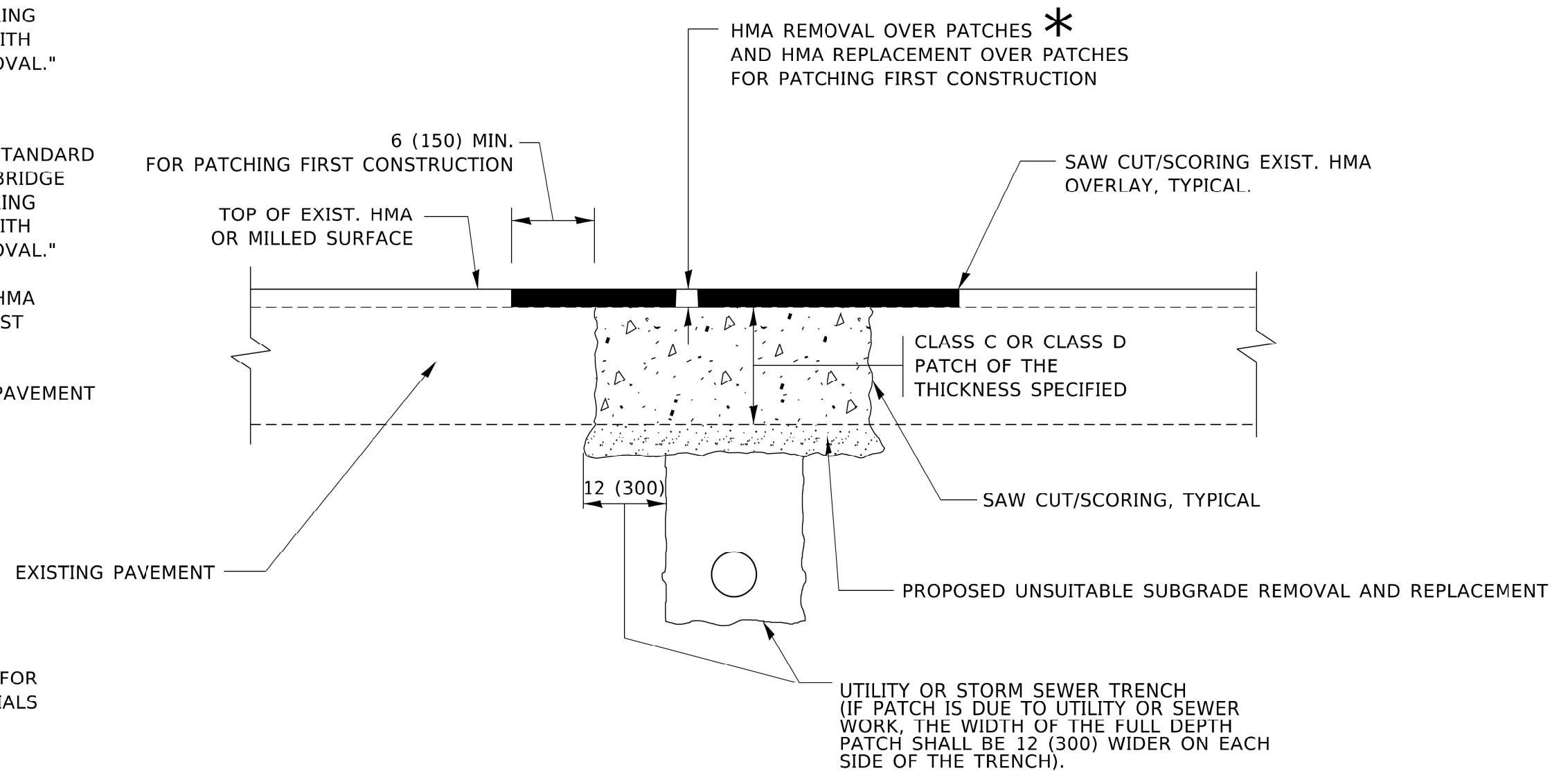
<p>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9598 www.infrastructure-eng.com</p>	USER NAME = Alane PLOT SCALE = 20,0000 * / in. PLOT DATE = 1/25/2023	DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 01/25/2023	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)	F.A.P. RTE. 112 SECTION FAP-0112-22-BJ COUNTY WILL TOTAL SHEETS 40 SHEET NO. 29 CONTRACT NO. 62R93	ILLINOIS FED. AID PROJECT
	SCALE:	STA.	TO STA.				

METHOD OF MEASUREMENT

REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."

BASIS OF PAYMENT

1. REFER TO SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND THE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL."
2. SAW CUT/SCORING OF EXISTING HMA OVERLAY IS INCLUDED IN THE COST OF PAVEMENT PATCHING.
3. SAW CUT/SCORING OF EXISTING PAVEMENT IS INCLUDED IN THE COST OF PAVEMENT PATCHING.



SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

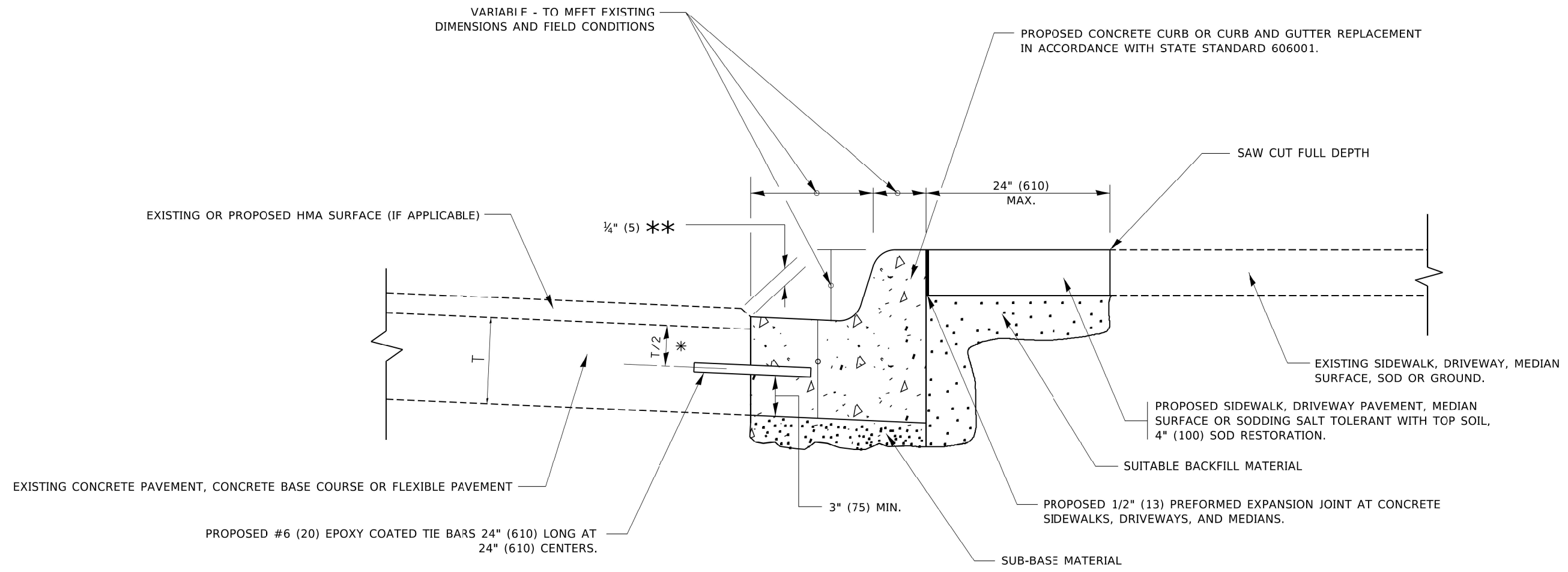
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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<p>INFRASTRUCTURE ENGINEERING 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.475.9598 F 312.475.9598 www.infrastructure-eng.com</p>	USER NAME = Alane PLOT SCALE = 20,0000 * / in. PLOT DATE = 1/25/2023	DESIGNED - MLD DRAWN - MLD CHECKED - ACL DATE - 01/25/2023	REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE – PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)	F.A.P. RTE. 112 SECTION FAP-0112-22-BJ COUNTY WILL TOTAL SHEETS 40 SHEET NO. 30 CONTRACT NO. 62R93	SCALE: STA. TO STA.	ILLINOIS FED. AID PROJECT



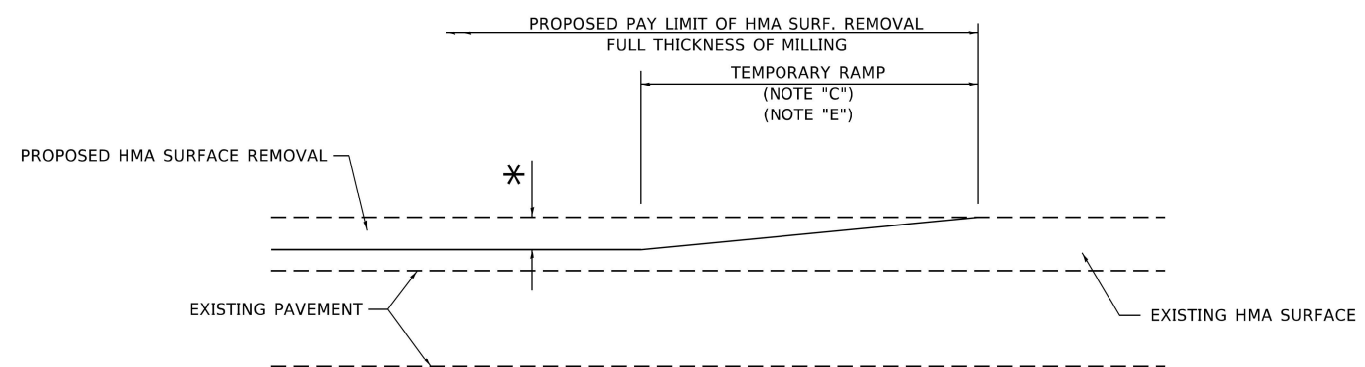
- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

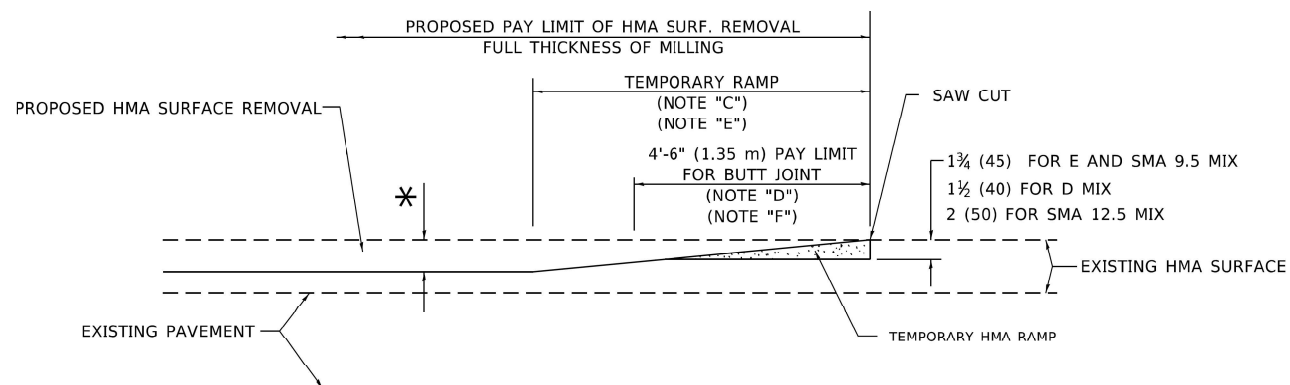
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INFRASTRUCTURE ENGINEERING <small>1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9594 www.infrastructure-eng.com</small>	USER NAME = Alane	DESIGNED - MLD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)	F.A.P. RTE. = 112	SECTION = FAP-0112-22-BJ	COUNTY = WILL	TOTAL SHEETS = 40	SHEET NO. = 31
	PLOT SCALE = 20,0000 * / in.	CHECKED - ACL	REVISED -			CONTRACT NO. 62R93				
	PLOT DATE = 1/25/2023	DATE = 01/25/2023	REVISED -			ILLINOIS FED. AID PROJECT				
				SCALE:	STA.	TO STA.				



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

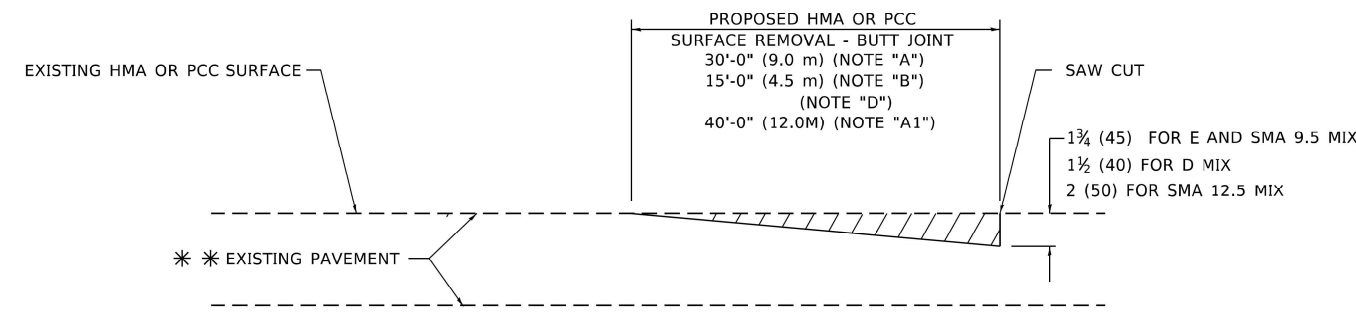
OPTION 1



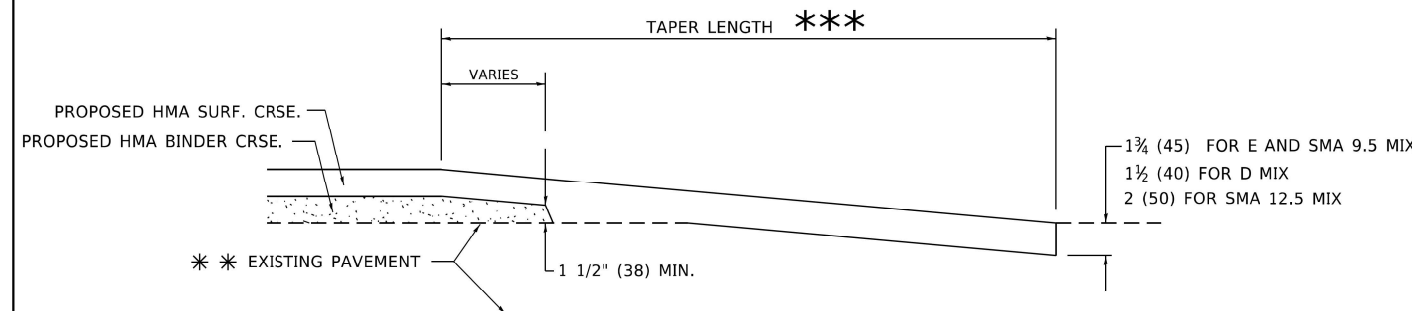
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

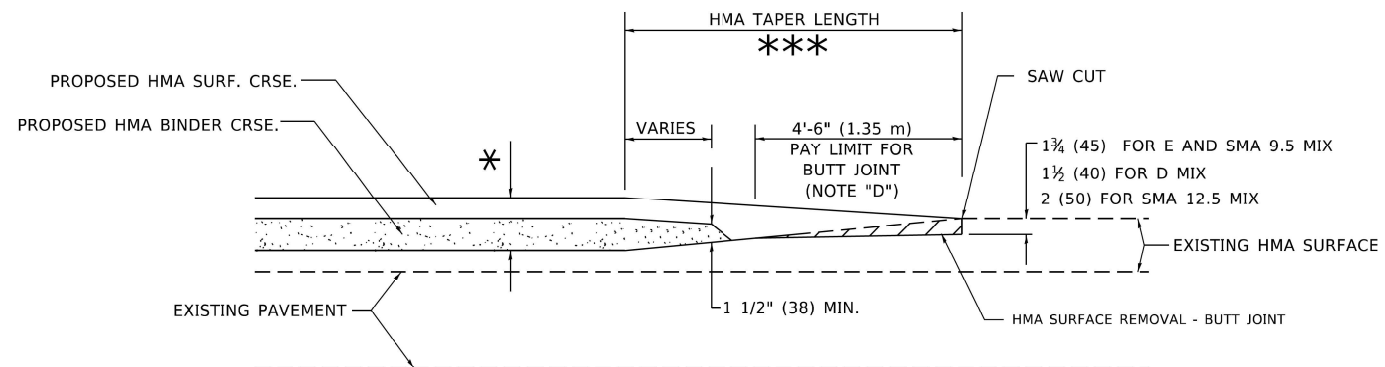
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL Path: \\... \FAP-0112-22-BJ\DOT_Various_Phase 2 (ITB) 106 Item: 161110_28_62R93\DOT\CADD_Sheet\161110_28_62R93\STANDARD-04.dwg

INFRASTRUCTURE ENGINEERING INCORPORATED
1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

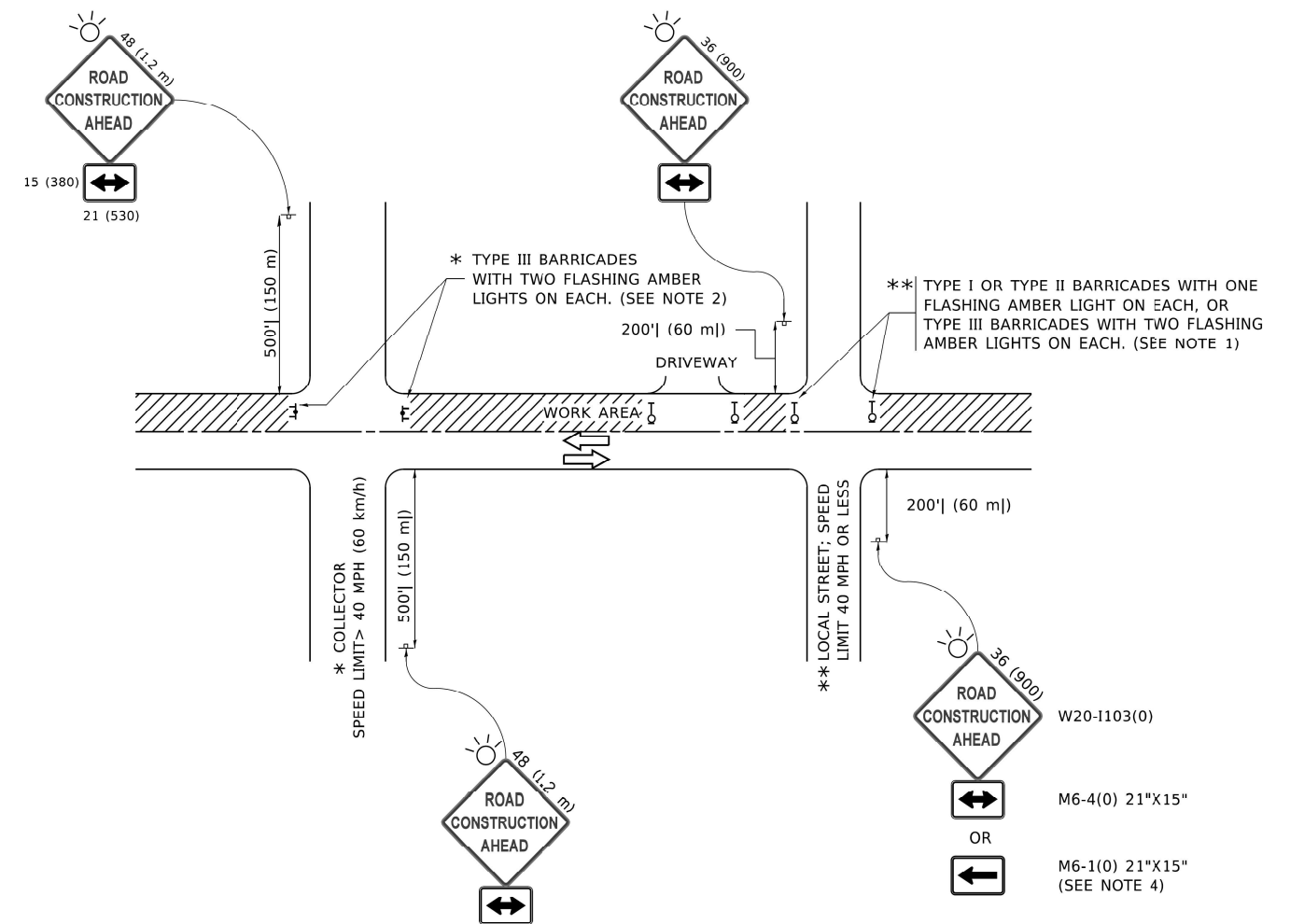
USER NAME = Alane	DESIGNED - MLD	REVISED -
PLOT SCALE = 20,0000 * / in.	DRAWN - MLD	REVISED -
PLOT DATE = 1/25/2023	CHECKED - ACL	REVISED -
	DATE - 01/25/2023	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - BUTT JOINT AND HMA TAPER DETAILS (BD-32)

SCALE: TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	32
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: P:\d\h\h\... INFRASTRUCTURE ENGINEERING INCORPORATED

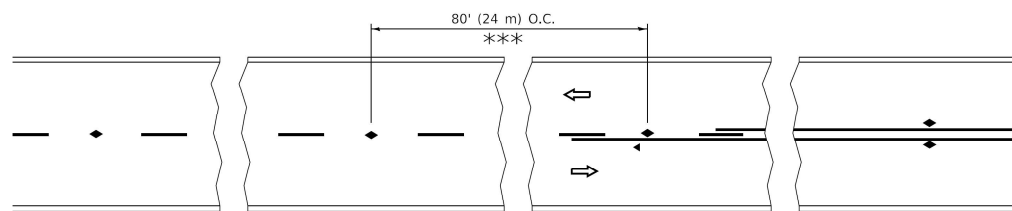
<p>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9598 F 312.425.9598 www.infrastructure-eng.com</p>	USER NAME = Alane	DESIGNED - MLD	REVISED -
	PLOT SCALE = 20,0000 * / in.	DRAWN - MLD	REVISED -
	PLOT DATE = 1/25/2023	CHECKED - ACL	REVISED -
		DATE - 01/25/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE – TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)**

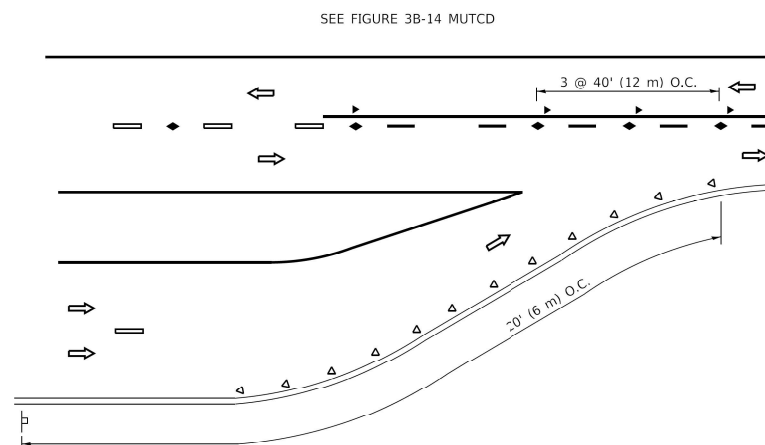
SCALE: _____ STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	33
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				

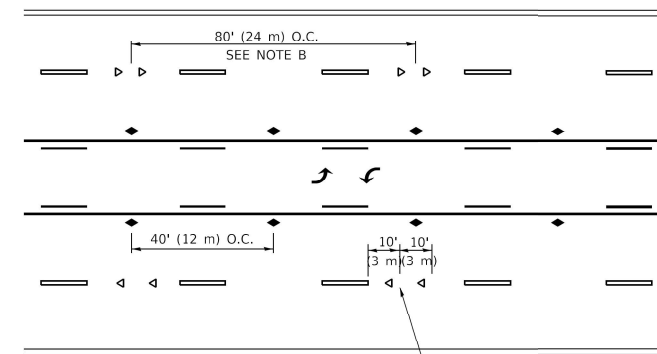


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

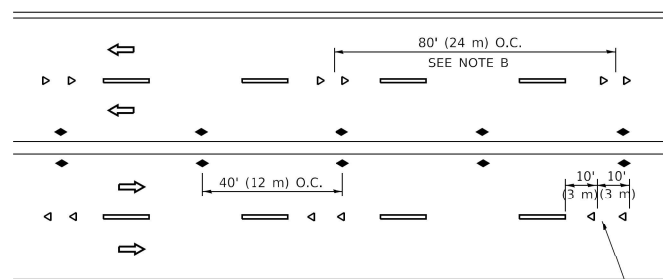
TWO-LANE/TWO-WAY



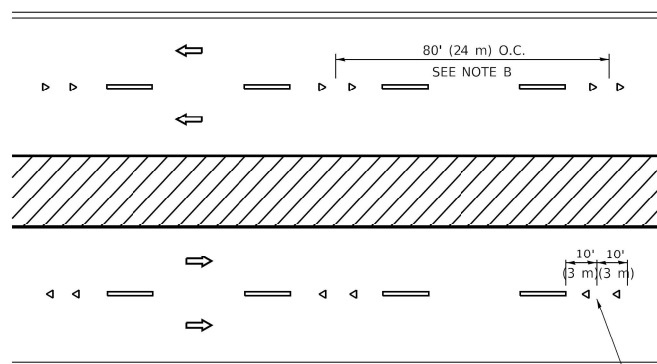
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

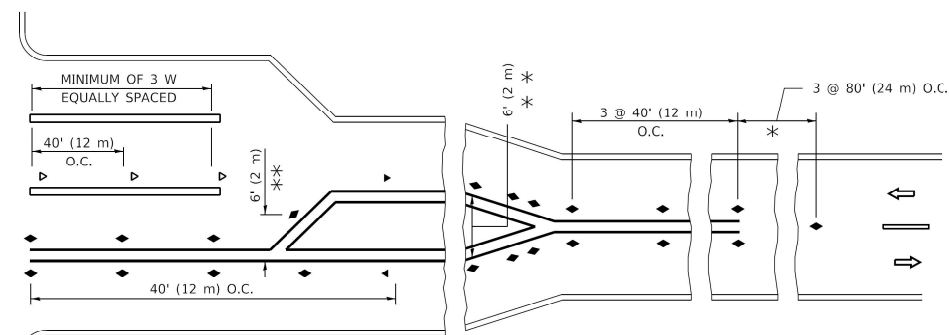
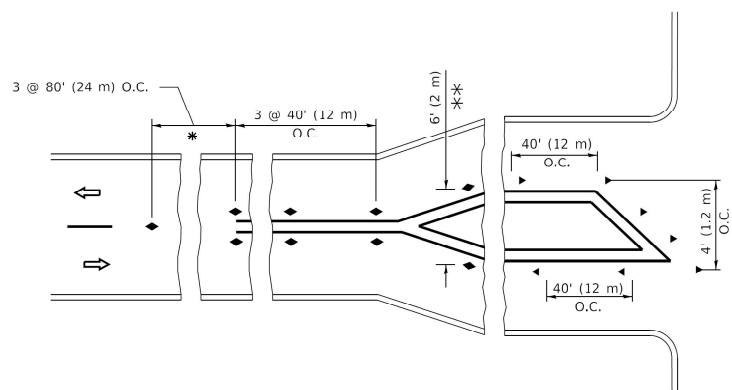
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



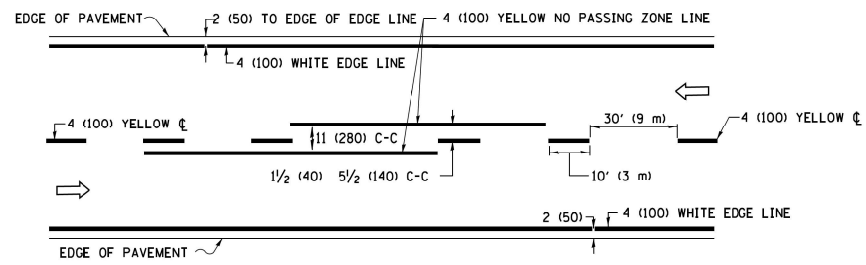
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 *** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

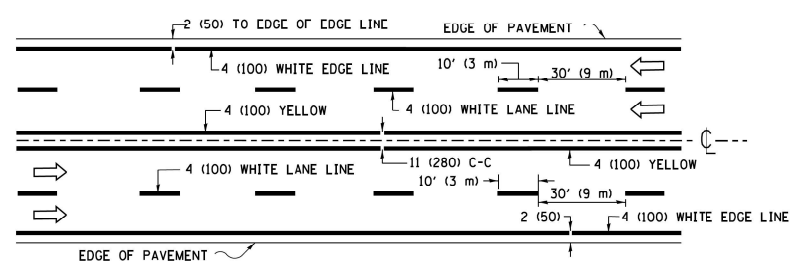
All dimensions are in inches (millimeters) unless otherwise shown.

MODEL Path: \\... \PUB\2020\2020-4094-00_IDOT_Vehicles_Phase 2 (ITB)_106 Item: 1611010_28_629R93\DOT\CLAD\SheetID162933-5\IT-STANDARDS-05.dgn

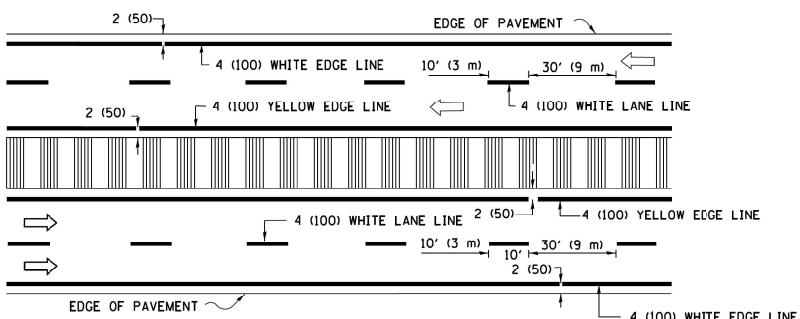
<p>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.8588 F 312.425.9598 www.infrastructure-eng.com</p>	USER NAME = Alane	DESIGNED - MLD	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) (TC-11)	F.A.P. RTE. = 112	SECTION = FAP-0112-22-BJ	COUNTY = WILL	TOTAL SHEETS = 40	SHEET NO. = 34
	PLOT SCALE = 20,0000 * / in.	CHECKED - ACL	REVISED -			CONTRACT NO. 62R93				
	PLOT DATE = 1/25/2023	DATE = 01/25/2023	REVISED -			ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

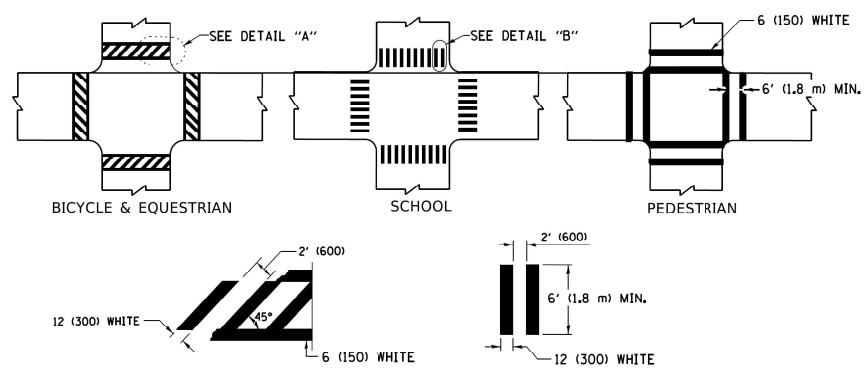


MULTI-LANE UNDIVIDED



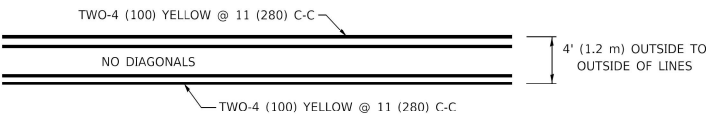
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

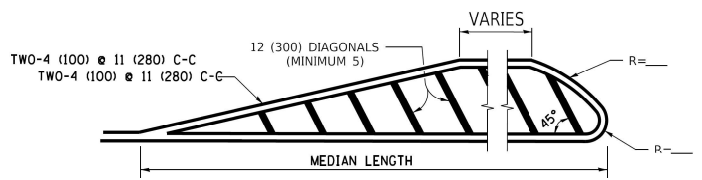


TYPICAL CROSSWALK MARKING

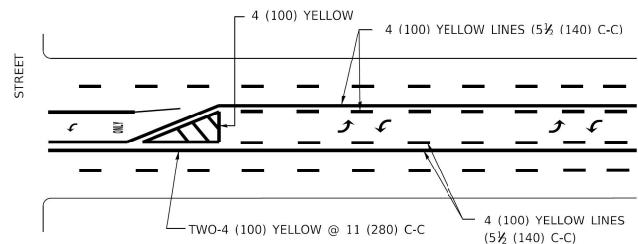
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



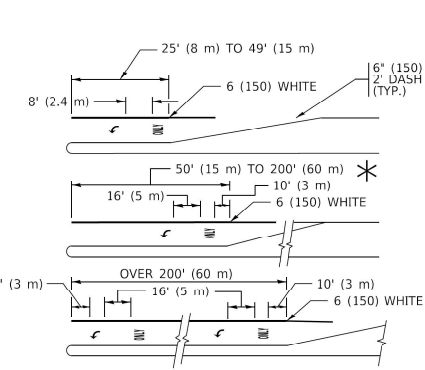
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



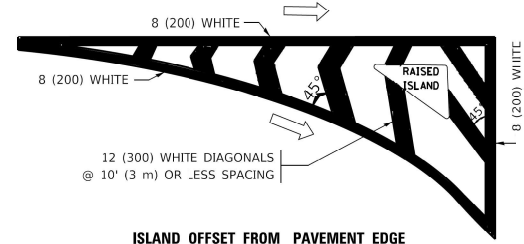
TYPICAL PAINTED MEDIAN MARKING



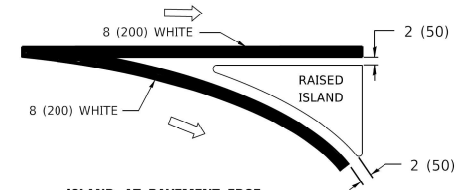
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - *ONLY* INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - *ONLY*.

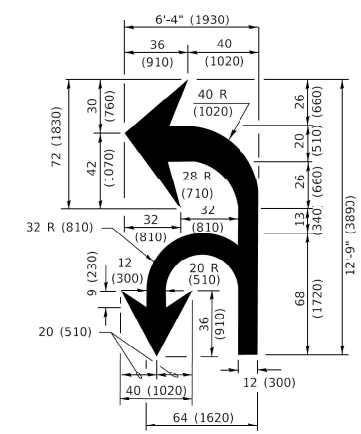


ISLAND OFFSET FROM PAVEMENT EDGE

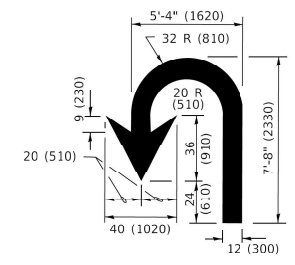


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C C FROM SKIP DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ² EACH) "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 5F
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 5F

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Paf64h; FILE NAME: P:\2020\2020-4094-00 IDOT Virobus Phase 2 (ITR) 106 Item: 161WV0_28_62993\DOT\Urobus_Phase 2_SHEET\162993-SHT-STD\ARDS-07.dwg



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 DRAWN - MLD
 CHECKED - ACL
 DATE - 01/25/2023
 PLOT SCALE = 20,0000 * / in.
 PLOT DATE = 1/25/2023

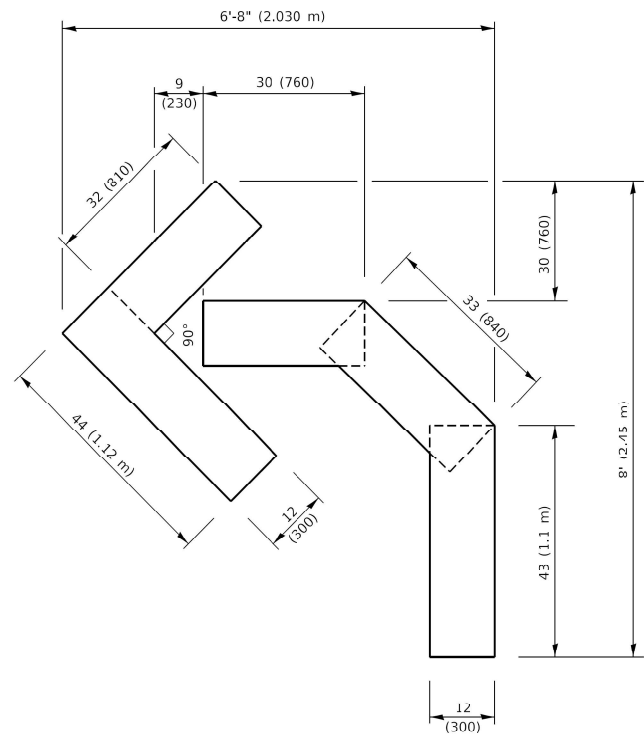
REVISED -
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE - TYPICAL PAVEMENT MARKINGS (TC-13)

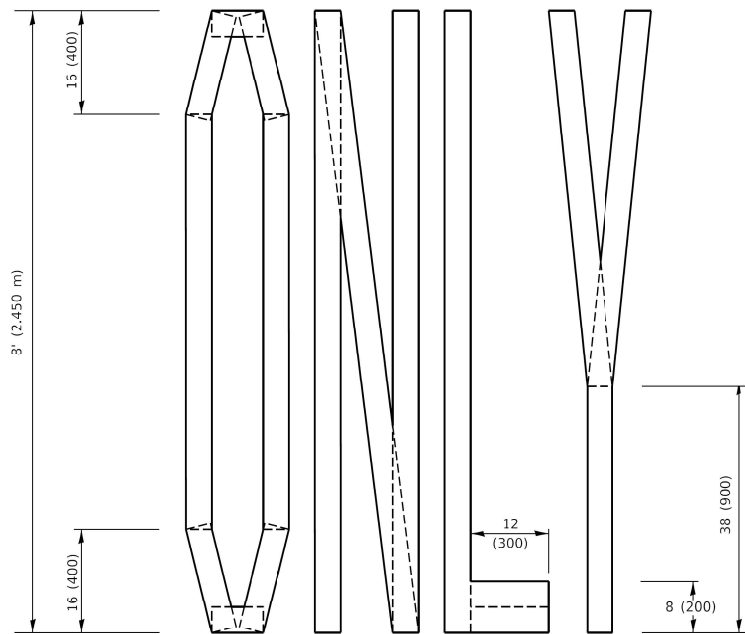
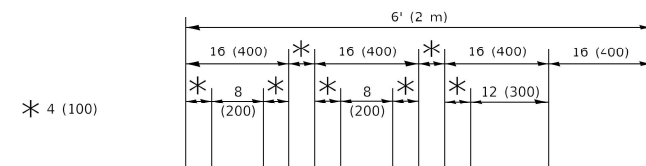
SCALE: STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	35
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				



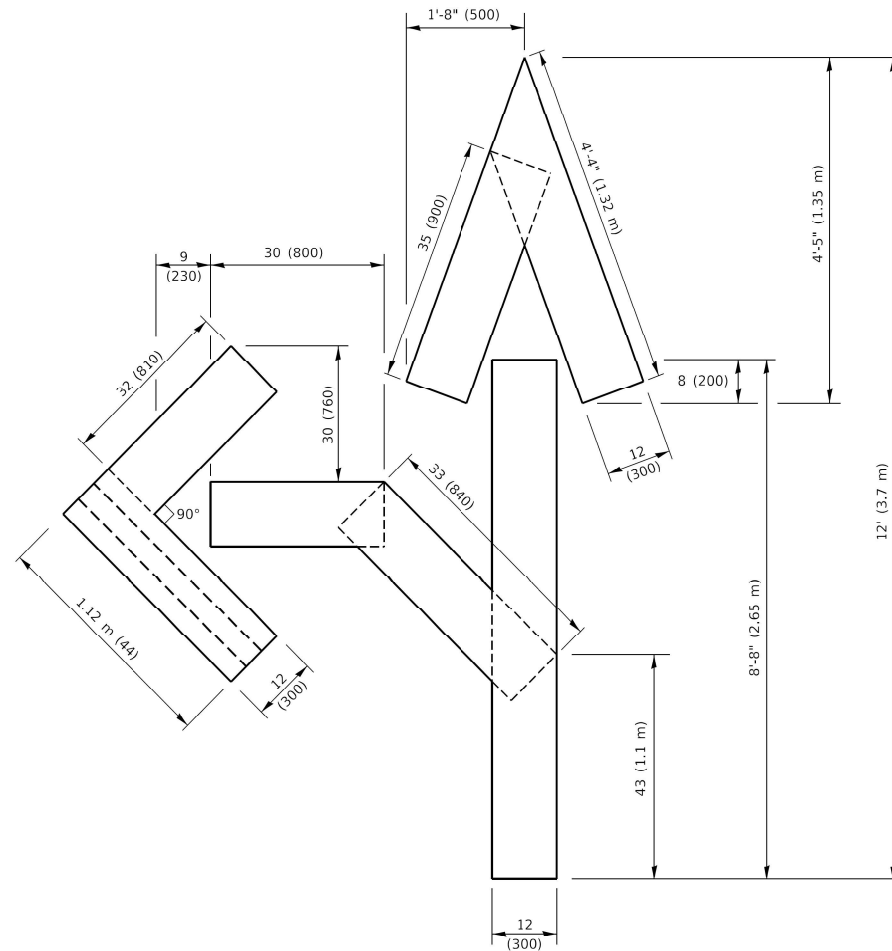
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

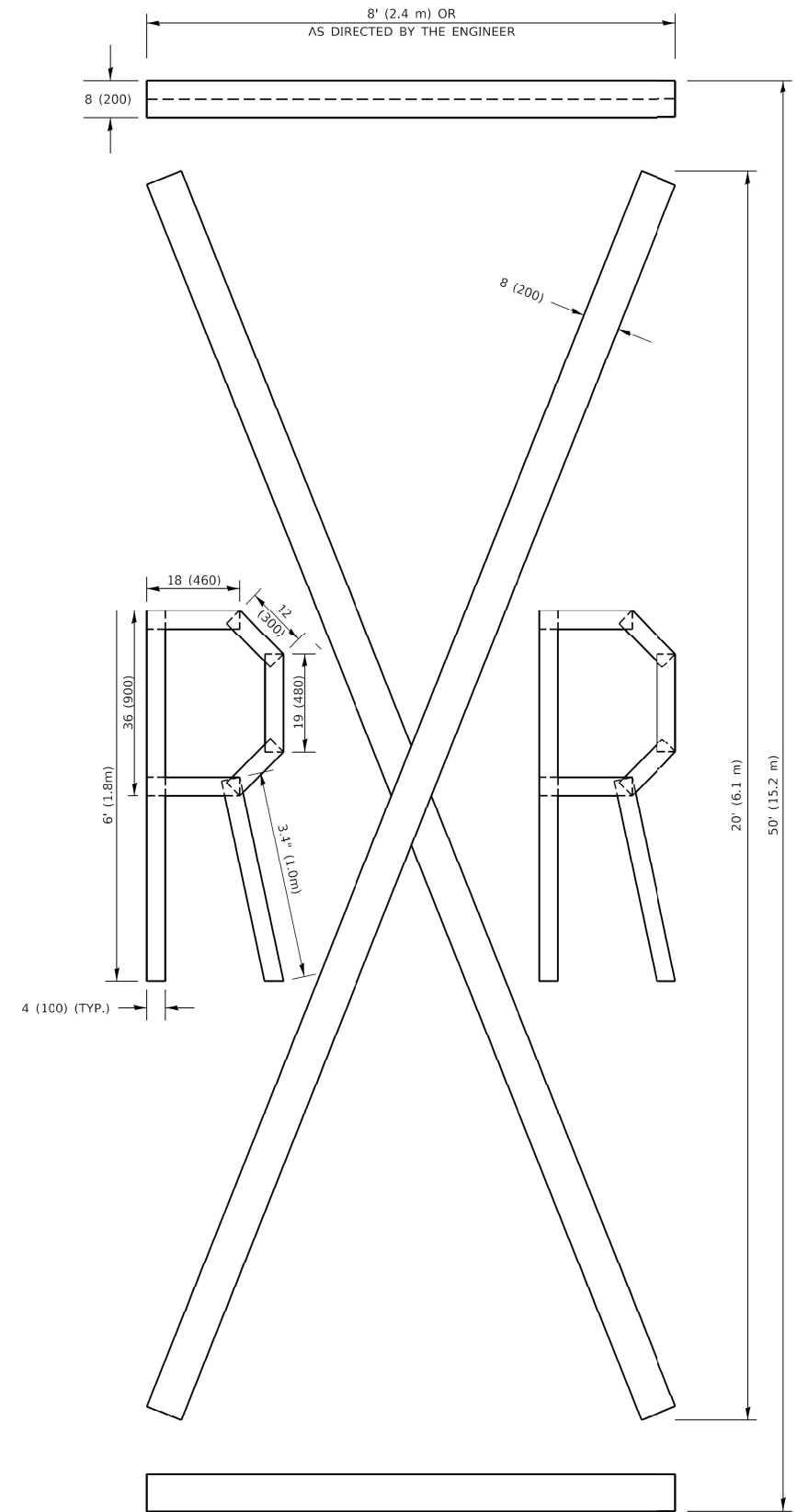


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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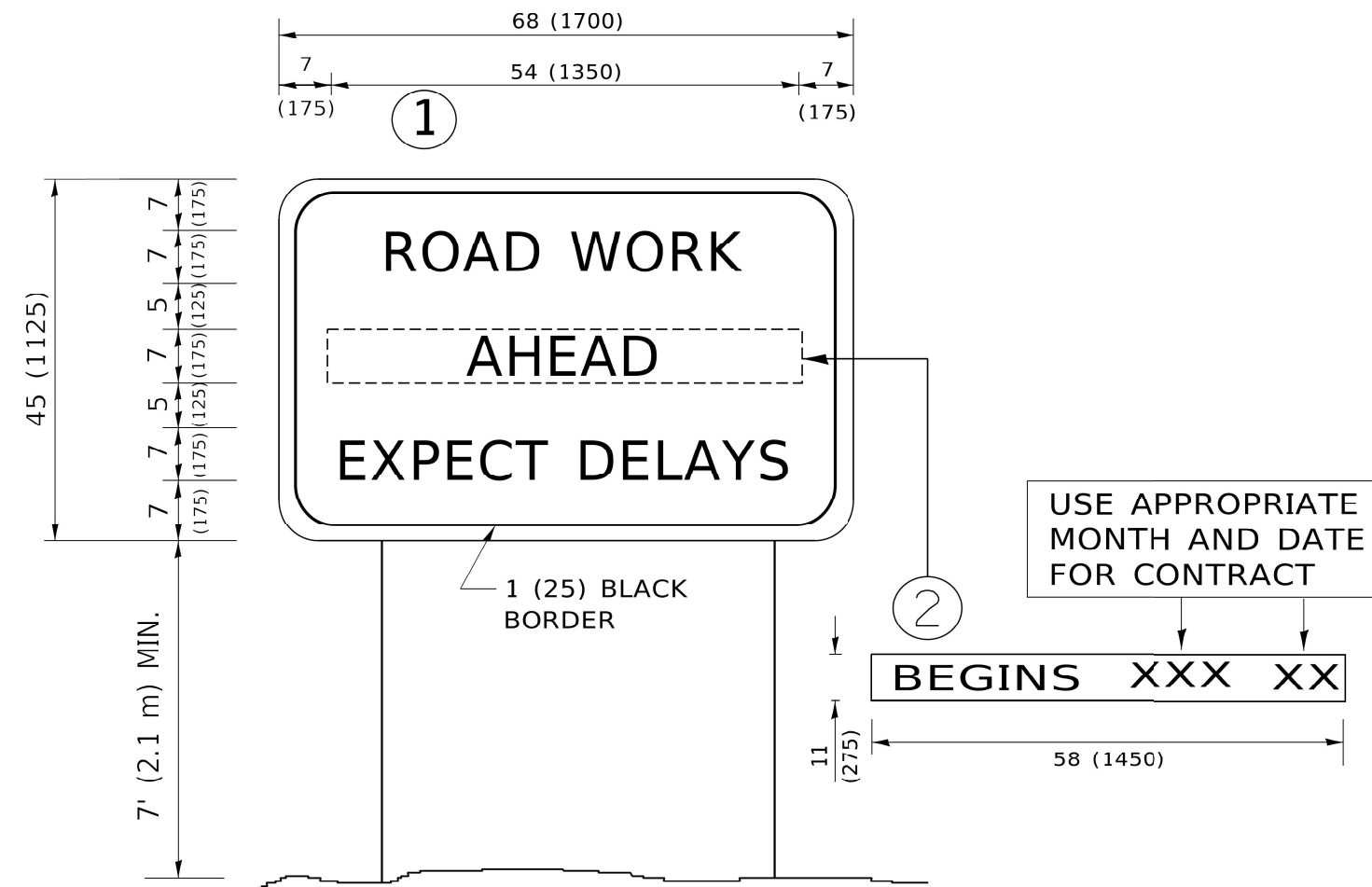
INFRASTRUCTURE ENGINEERING
1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

USER NAME = Alane	DESIGNED - MLD	REVISED -
PLOT SCALE = 20,0000 * / in.	DRAWN - MLD	REVISED -
PLOT DATE = 1/25/2023	CHECKED - ACL	REVISED -
	DATE - 01/25/2023	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - SHORT TERM PAVEMENT MARKING
LETTERS AND SYMBOLS (TC-16)
SCALE: _____
STA. _____ TO STA. _____

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 37
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				

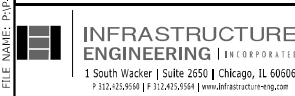


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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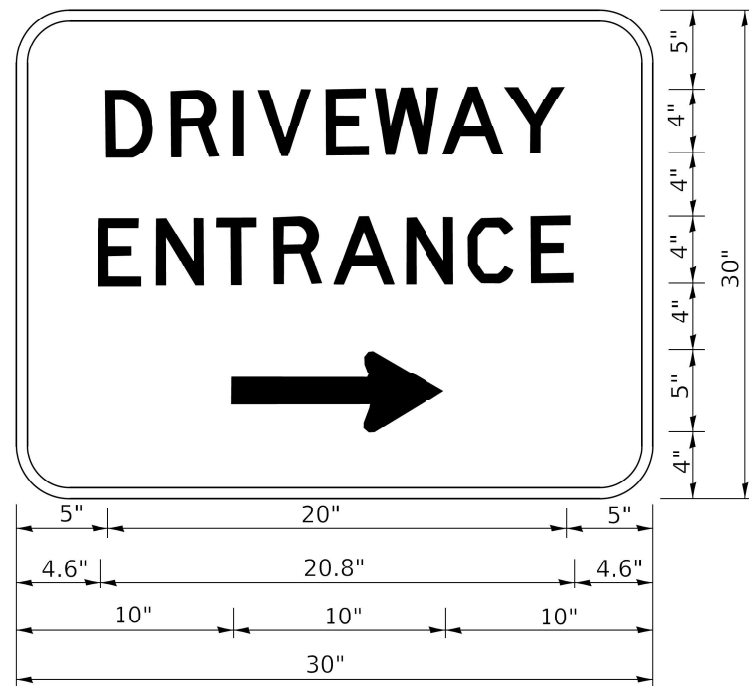
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	DRAWN - MLD	REVISED -
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PLOT DATE = 1/25/2023	DATE - 01/25/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE – ARTERIAL ROAD INFORMATION SIGN (TC-22)

SCALE: _____ STA. _____ TO STA. _____

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
112	FAP-0112-22-BJ	WILL	40	38
CONTRACT NO. 62R93				
		ILLINOIS	FED. AID PROJECT	



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

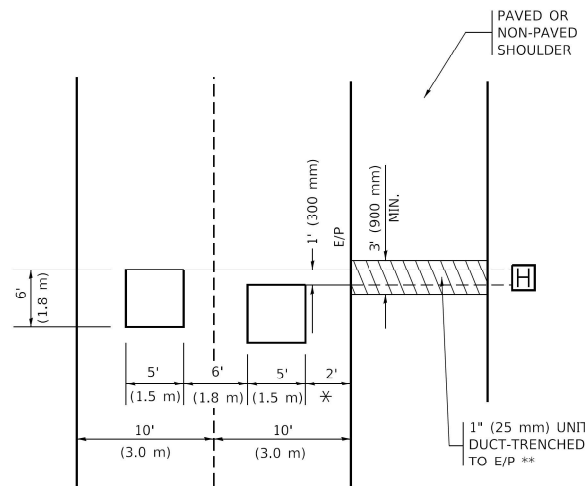
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

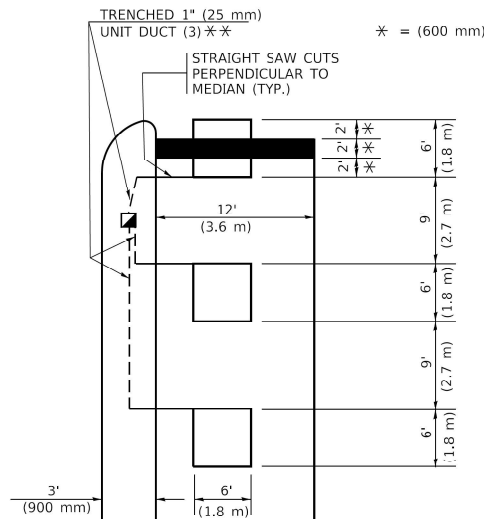


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

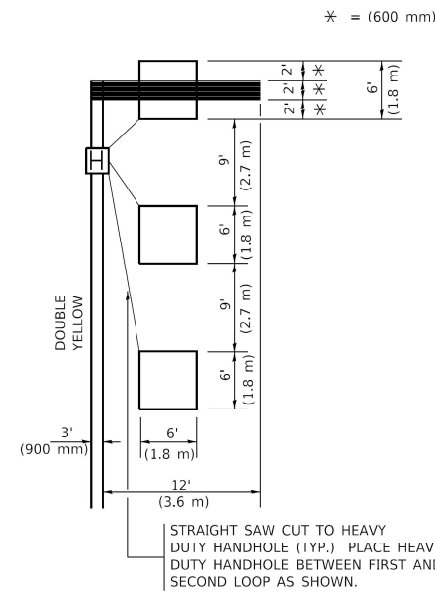


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT. THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

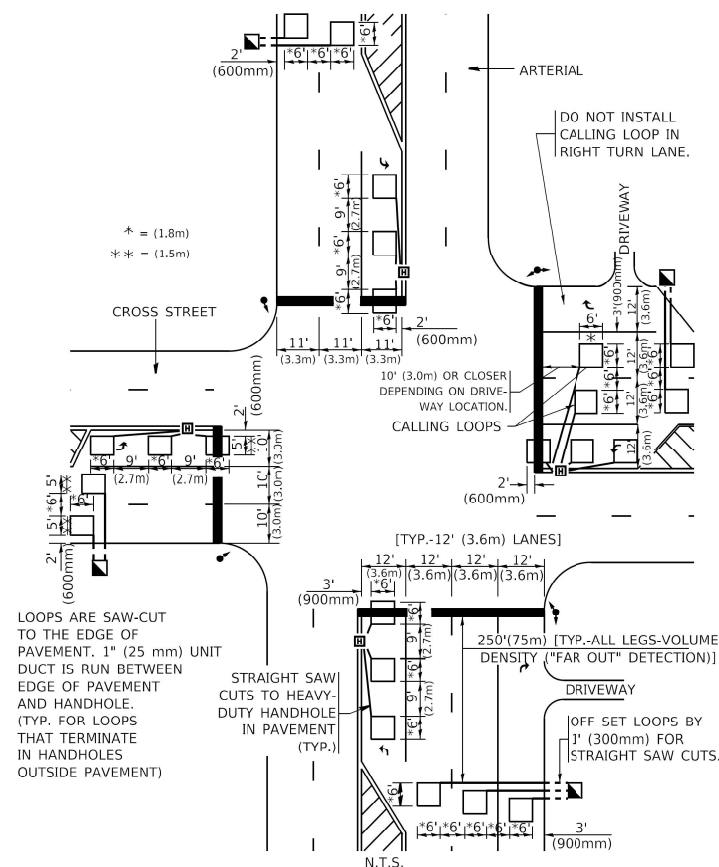
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

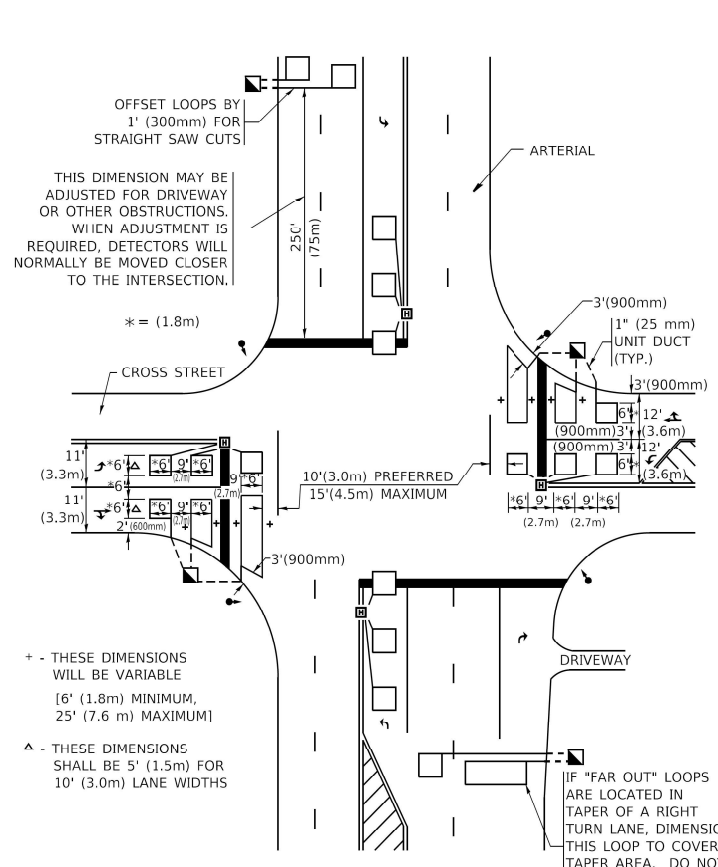
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

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USER NAME = Alane	DESIGNED - MLD	REVISED -
PLOT SCALE = 20,0000 * / in.	DRAWN - MLD	REVISED -
PLOT DATE = 1/25/2023	CHECKED - ACL	REVISED -
	DATE - 01/25/2023	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING (TS-07)

F.A.P. RTE. 112	SECTION FAP-0112-22-BJ	COUNTY WILL	TOTAL SHEETS 40	SHEET NO. 40
CONTRACT NO. 62R93				
ILLINOIS FED. AID PROJECT				