

FAU RTE. 1446	SECTION 22-00053-00-RS	COUNTY DUPAGE	TOTAL SHEETS 29	SHEET NO. 1
ILLINOIS CONTRACT NO. 61J22				

04-28-2023 LETTING ITEM 077

STATE OF ILLINOIS

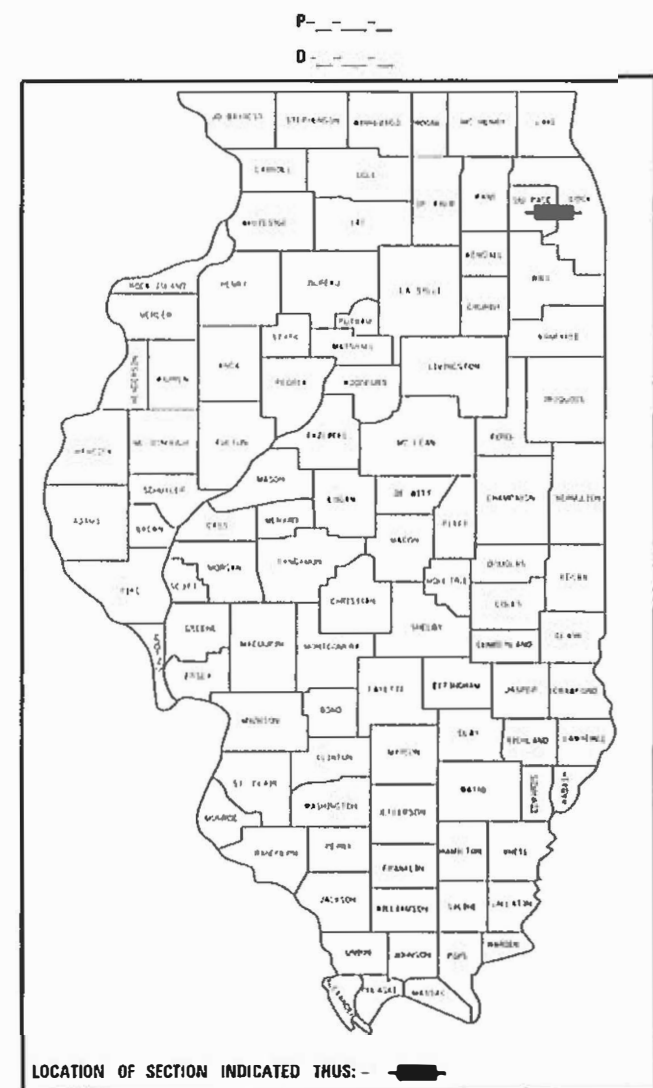
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR A LIST OF APPLICABLE HIGHWAY STANDARDS, SEE SHEET NO. 2

FAU 1446 (HARGER ROAD)
 SPRING ROAD TO
 WB I-88 RAMP IMPROVEMENTS
 RESURFACING
 SECTION NO.: 22-00053-00-RS
 PROJECT NO.: JG5E(926)
 VILLAGE OF OAK BROOK
 DUPAGE COUNTY
 JOB NO. C-91-196-22



FUNCTIONAL CLASSIFICATION
 MINOR COLLECTOR
 2020 ADT = 1150
 POSTED SPEED LIMIT: 30MPH

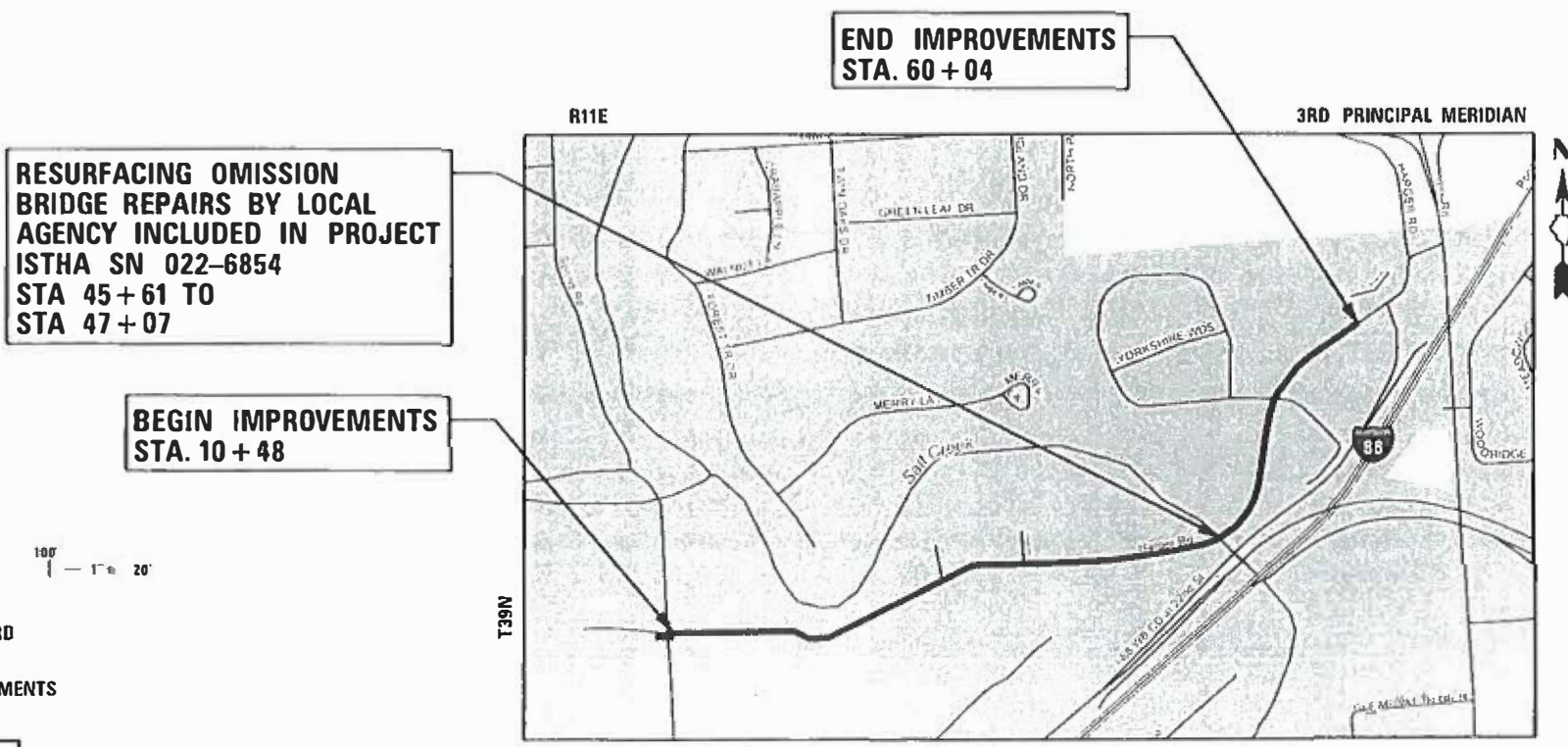
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

APPROVED DATE 1/17/23
 [Signature]
 VILLAGE OF OAK BROOK PUBLIC WORKS DIRECTOR

PASSED March 1, 2023
 [Signature]
 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW
 March 1, 2023
 [Signature]
 REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

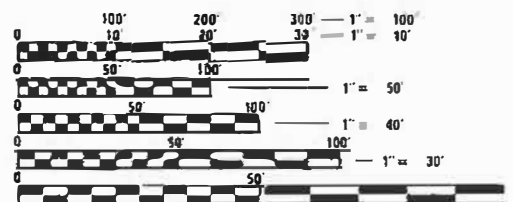


RESURFACING OMISSION
 BRIDGE REPAIRS BY LOCAL
 AGENCY INCLUDED IN PROJECT
 ISTHA SN 022-6854
 STA 45+61 TO
 STA 47+07

BEGIN IMPROVEMENTS
 STA. 10 + 48

END IMPROVEMENTS
 STA. 60 + 04

YORK TOWNSHIP LOCATION MAP N.T.S.
 GROSS LENGTH = 4,956 FT. = 0.94 MILE
 NET LENGTH = 4,956 FT. = 0.94 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 DR 811

ORION C. GALEY
 ILLINOIS REGISTRATION NO. 062-060873
 EXPIRATION DATE: 11/2023

CHRISTOPHER B. BURKE ENGINEERING LTD
 16221 W 159th Street, Suite 201
 Lockport, Illinois 60441
 (815) 770-2850
 PROFESSIONAL DESIGN FIRM NO. 184-001175
 EXPIRATION DATE: 04/30/23

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAMBURG, IL

CONTRACT NO. 61J22

INDEX OF SHEETS

1	COVER
2	GENERAL NOTES
3 – 6	SUMMARY OF QUANTITIES
7	TYPICAL SECTIONS
8	SCHEDULE OF QUANTITIES – SIGN REPLACEMENT
9 – 13	PROPOSED PLANS
14	DETOUR PLAN
15 - 21	BRIDGE REPAIRS – PLANS AND DETAILS
22	CONSTRUCTION DETAILS
23-29	IDOT DISTRICT 1 DETAILS

HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604051-04	FRAME AND GRATE TYPE 11
606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-12	STEEL PLATE BEAM GUARDRAIL
631006-08	TRAFFIC BARRIER TERMINAL, TYPE 1B
631011-10	TRAFFIC BARRIER TERMINAL, TYPE 2
635001-02	DELINEATORS
701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATION – DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE CURRENT EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATION"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS."
- UTILITY LOCATIONS HAVE NOT ALL BEEN SHOWN ON THESE PLANS. THE CONTRACTOR SHALL HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER OR VILLAGE.
- THE CONTRACTOR SHALL NOTIFY THE VILLAGE PUBLIC WORKS AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
- MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IF IN THE JUDGEMENT OF THE VILLAGE, IT SHOULD BE NECESSARY TO REMOVE SUCH MATERIALS, THE VILLAGE WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS OR BUSINESSES AND THE ENGINEER WHEN ACCESS TO DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY/SIDEWALK REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES, INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED. AT RESIDENCES WITH TWO DRIVEWAY APRONS, THE CONTRACTOR SHALL ONLY REMOVE AND REPLACE ONE APRON AT A TIME. THE SECOND APRON SHALL NOT BE REMOVED UNTIL THE FIRST APRON HAS BEEN REPLACED AND IS ACCESSIBLE.
- THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, THEIR AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED ¼" ABOVE THE GUTTER FLAG.
- THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
- CONTRACTOR SHALL TAKE PRECAUTION BY PRESERVING EXISTING TREES WITHIN THE RIGHT OF WAY. IF ANY DAMAGE OCCURS, TREES SHALL BE REPLACED IN KIND PER ARTICLE 201.07 REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL REQUIREMENTS STATED HEREIN.
- ALL NEW STORM STRUCTURES SHALL BE PROVIDED WITH 4" OF CONCRETE RINGS TO ALLOW ADJUSTMENT IN THE FIELD.
- DURING INSTALLATION OF STORM SEWERS, THE CONTRACTOR SHALL MAKE EVERY EFFORT TO AVOID RIDING THE TIRE (TRACK OF THE MACHINE) OVER THE CURB AND GUTTER. ANY DAMAGE TO THE CURB AND GUTTER RESULTING FROM THIS ACTION SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE VILLAGE.
- ALL STORM SEWER STRUCTURE HOLES FOR SEWERS SHALL BE CUT APPROPRIATELY. EXCESSIVE HOLES SHALL RESULT IN REJECTION OF THE STORM STRUCTURE.
- NO ARTIFICIAL KICK-BACKS IN THE CURB AND GUTTER AROUND STREET INLET GRATES UNLESS INDICATED BY THE VILLAGE.
- ALL REPLACED FRAMES AND GRATES SHALL BE SALVAGED AND DELIVERED TO THE VILLAGE PUBLIC WORKS DEPARTMENT.

16. CONTRACTOR IS EXPECTED TO BEGIN STORM SEWER INSTALLATION PRIOR TO ANY PAVING WORK, INCLUDING MILLING. ANY ALTERNATIVE SEQUENCING MUST BE PRE-APPROVED BY THE VILLAGE.

17. THE CONTRACTOR MUST GET APPROVAL FROM THE VILLAGE PRIOR TO BREAKING AWAY FROM AN UNCOMPLETED AREA TO START ON A NEW PROJECT AREA. GENERALLY, THE CONTRACTOR SHALL BE SUBSTANTIALLY COMPLETE WITH AN AREA BEFORE MOVING TO ANOTHER AREA. NO ADDITIONAL COMPENSATION WILL BE MADE FOR ADDITIONAL MOBILIZATIONS. THIS PERTAINS TO MOBILIZATIONS FOR THE GENERAL CONTRACTOR AS WELL AS ALL SUBCONTRACTORS. A SEQUENCE OF THE AREAS TO BE REHABILITATED IN THE CONTRACT SHALL BE PROVIDED BY THE CONTRACTOR AT THE PRECONSTRUCTION MEETING ALONG WITH AN ANTICIPATED CONSTRUCTION SCHEDULE FOR EACH AREA.

18. CONTRACTOR TO VERIFY MINIMUM PIPE SLOPE OF POSITIVE DRAINAGE IS ACHIEVED ON ALL PROPOSED STORM SEWERS.

19. NO INSTREAM WORK WILL OCCUR FOR BRIDGE MAINTENANCE WORK AND SHALL BE PERFORMED FROM THE SUPERSTRUCTURE

20. THE CONTRACTOR MUST PROVIDE MEASURES TO PREVENT CONSTRUCTION DEBRIS FROM FALLING INTO SALT CREEK.

COMMITMENTS

- EXECUTE AN IGA BETWEEN THE ISTHA AND VILLAGE DURING PHASE II FOR BRIDGE MAINTENANCE WORK FOR THE HARGER ROAD BRIDGE OVER SALT CREEK (UNDER ISTHA JURISDICTION).
- THE LENGTH OF REPLACED GUARDRAIL WILL BE CONFIRMED IN PHASE II.
- NO INSTREAM WORK WILL OCCUR FOR BRIDGE MAINTENANCE WORK AND SHALL BE PERFORMED FROM THE SUPERSTRUCTURE AND WRITTEN INTO THE SPECIFICATION ACCORDINGLY
- A POST ASSESSMENT (BRIDGE BAT ASSESSMENT) DISCOVERY OF BATS AT BRIDGE/CULVERT OR STRUCTURE FORM SHOULD BE SUBMITTED WITHIN 2 WORKING DAYS IF THE PRESENCE OF BATS IS DETECTED DURING PHASE II ENGINEERING OR CONSTRUCTION.
- ALL APPLICABLE SPECIAL WASTE PAY ITEMS SHOULD BE INCLUDED IN THE FINAL PS&E AS NEEDED
- IF NECESSARY, A LOCAL PSI WILL BE COMPLETED IN PHASE II
- APPROPRIATE NOTES SHALL BE ADDED TO THE PHASE II PS&E THAT THE CONTRACTOR MUST PROVIDE MEASURES TO PREVENT CONSTRUCTION DEBRIS FROM FALLING INTO SALT CREEK.

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD STP RESURFACING PROJECT GENERAL NOTES			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\OAKBROOK\160597.00044\Civil\NOT.160597.00044.sht	DRAWN - DJS	REVISED -	1446					22-00053-00-RS	DUPAGE	29	2	
Default	PLOT SCALE = 40'	CHECKED - OCG	REVISED -		CONTRACT NO. 61J22			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 3/6/2023	DATE - AUGUST 2022	REVISED -		SCALE: 40'	SHEET 1 OF 1 SHEETS	STA. N/A	TO STA. N/A				

FUNDING SOURCE					STP	
					80% FED 20% LOCAL	100% LOCAL
CONSTRUCTION TYPE CODE					0005	0013
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE REHAB	
20800150	TRENCH BACKFILL	CU YD	5	5		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	796	796		
* 21400100	GRADING AND SHAPING DITCHES	FOOT	777	777		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	10	10		
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	10	10		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	10	10		
25200110	SODDING, SALT TOLERANT	SQ YD	796	796		
25200200	SUPPLEMENTAL WATERING	UNIT	42	42		
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	25	25		
28000510	INLET FILTERS	EACH	12	12		
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	10037	10037		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	87	87		
40602978	HOT-MIX ASPHALT BINDER COURSE, IL- 9.5, N50	TON	1275	1275		
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	1275	1275		
42400800	DETECTABLE WARNINGS	SQ FT	25	25		
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	14729	14729		
44201690	CLASS D PATCHES, TYPE I, 4 INCH	SQ YD	30	30		
44201692	CLASS D PATCHES, TYPE II, 4 INCH	SQ YD	45	45		

* INDICATES A SPECIAL PROVISION
 ~ INDICATES A SPECIALTY ITEM

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD STP RESURFACING PROJECT SUMMARY OF QUANTITIES			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\QA\BROOK\160597.00044\Civil\5001.1605	7.00244.dwt	DRAWN - DJS	REVISED -					1446	22-00053-00-RS	DUPAGE	29	3
Default	PLOT SCALE = 40'	CHECKED - OCG	REVISED -		SCALE: 40'	SHEET 1 OF 4 SHEETS	STA. N/A	TD STA. N/A	CONTRACT NO. 61J22			
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -		[ILLINOIS] FED. AID PROJECT							

FUNDING SOURCE					STP	
					80% FED 20% LOCAL	100% LOCAL
CONSTRUCTION TYPE CODE					0005	0013
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE REHAB	
44201694	CLASS D PATCHES, TYPE III, 4 INCH	SQ YD	74	74		
44201696	CLASS D PATCHES, TYPE IV, 4 INCH	SQ YD	148	148		
44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD	74	74		
44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD	148	148		
44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD	295	295		
44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD	663	663		
48101200	AGGREGATE SHOULDERS, TYPE B	TON	174	174		
~ 50102400	CONCRETE REMOVAL	CU YD	15		15	
50105220	PIPE CULVERT REMOVAL	FOOT	65	65		
~ 50300255	CONCRETE SUPERSTRUCTURE	CU YD	15		15	
~ 50300260	BRIDGE DECK GROOVING	SQ YD	25		25	
~ 50300300	PROTECTIVE COAT	SQ YD	35		35	
~ 50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2250		2250	
~ 52000110	PREFORMED JOINT STRIP SEAL	FOOT	35		35	
54262712	METAL FLARED END SECTIONS 12"	EACH	2	2		
542C0217	PIPE CULVERTS, CLASS C, TYPE 1 12"	FOOT	65	65		
60404800	FRAMES AND GRATES, TYPE 11	EACH	1	1		
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	3	3		
~ 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	173	173		

* INDICATES A SPECIAL PROVISION
~ INDICATES A SPECIALTY ITEM

FUNDING SOURCE					STP	
					80% FED 20% LOCAL	100% LOCAL
CONSTRUCTION TYPE CODE					0005	0013
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE REHAB
~	63100041	TRAFFIC BARRIER TERMINAL, TYPE 1B	EACH	1	1	
~	63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1	1	
	63200310	GUARDRAIL REMOVAL	FOOT	210	210	
	67100100	MOBILIZATION	L SUM	1	0.6	0.4
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	56	28	28
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	2071	2071	
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	691	691	
	70300221	TEMPORARY PAVEMENT MARKING - LINE 4"- PAINT	FOOT	19066	19066	
	70300281	TEMPORARY PAVEMENT MARKING - LINE 24"- PAINT	FOOT	164	164	
~	72000100	SIGN PANEL - TYPE 1	SQ FT	104	104	
	72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	17	17	
~	72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	249	249	
~	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	200	200	
~	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17236	17236	
~	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	577	577	
~	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	281	281	
~	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	166	166	
~	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	82	82	

* INDICATES A SPECIAL PROVISION

~ INDICATES A SPECIALTY ITEM

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD STP RESURFACING PROJECT SUMMARY OF QUANTITIES				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\DAKBRDDK\168597.00244\Civil\5001.1605	7.00244.sht	DRAWN - DJS	REVISED -		1446	22-00053-00-RS	DUPAGE	29	5				
Default	PLOT SCALE = 40'	CHECKED - OCG	REVISED -		SCALE: 40'	SHEET 3 OF 4 SHEETS	STA. N/A	TO STA. N/A	CONTRACT NO. 61J22				
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -		ILLINOIS FED. AID PROJECT								

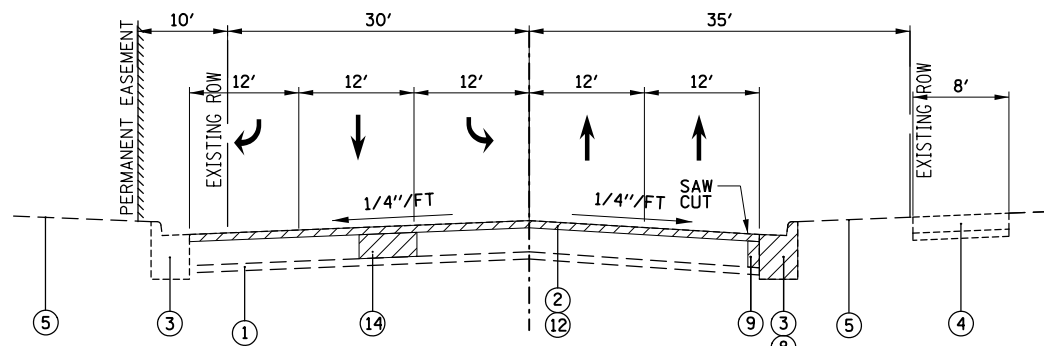
FUNDING SOURCE						STP	
						80% FED 20% LOCAL	100% LOCAL
CONSTRUCTION TYPE CODE						0005	0013
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE REHAB	
~	78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	5	5		
~	87900200	DRILL EXISTING HANDHOLE	EACH	2	2		
~	* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	240	240		
	* X0326806	WASHOUT BASIN	L SUM	1	0.1	0.9	
	* X0326862	STRUCTURES TO BE ADJUSTED	EACH	1	1		
	* X0327036	BIKE PATH REMOVAL	SQ YD	17	17		
	* X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	5	5		
	* X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	9	9		
	* X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT	151	151		
	* X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	6	6		
	* X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	0.4	0.6	
	* XX009049	REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)	FOOT	62	62		
~	* Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	420		420	
~	* Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	80		80	
~	* Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	70		70	
	* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52		
#	* Z0076600	TRAINEES	HOUR	500	500		
#	* Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500		

* INDICATES A SPECIAL PROVISION

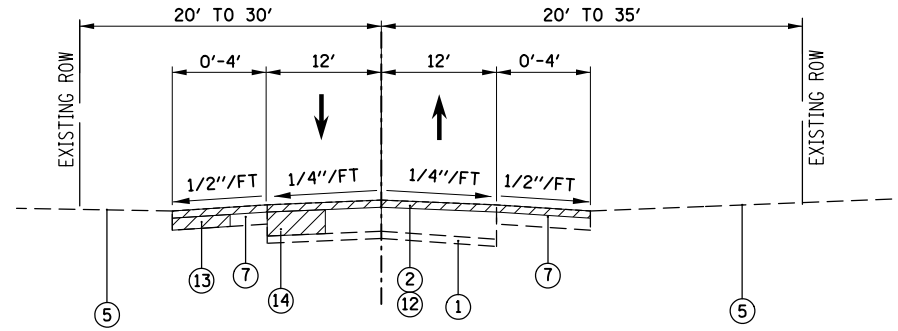
~ INDICATES A SPECIALTY ITEM

COA2

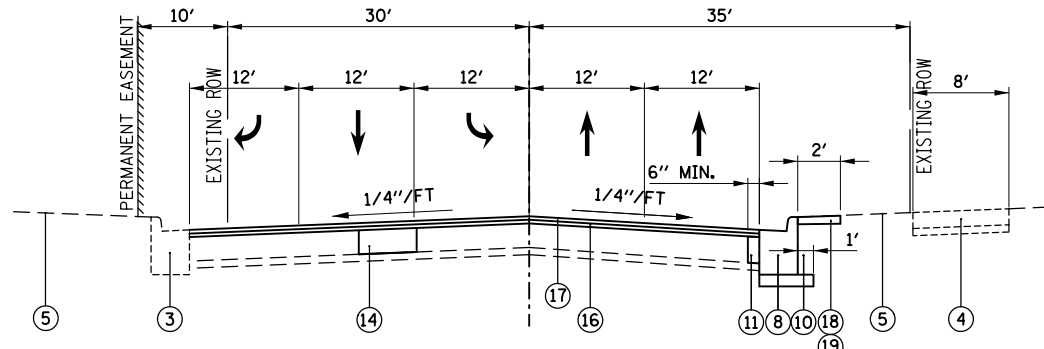
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N:\D&B\BROOK\160597.00244\C\1\1\5001.160597.00244.sht	PLOT SCALE = 40'	DRAWN - DJS	REVISED -					1446	22-00053-00-RS	DUPAGE	29	6
Default	PLOT DATE = 2/21/2023	CHECKED - DCC	REVISED -		SCALE: 40'	SHEET 4 OF 4 SHEETS	STA. N/A	TO STA. N/A	CONTRACT NO. 61J22			
		DATE - AUGUST 2022	REVISED -						ILLINOIS FED. AID PROJECT			



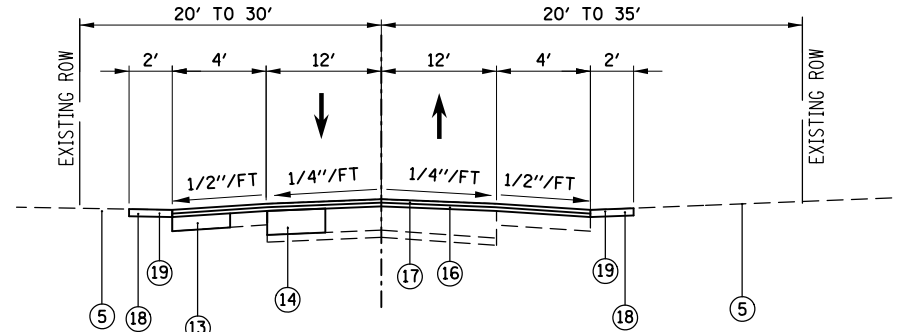
EXISTING TYPICAL SECTION
HARGER ROAD
STA 10+48 TO 14+50
SPRING ROAD APPROACH (CURB/GUTTER SECTION)



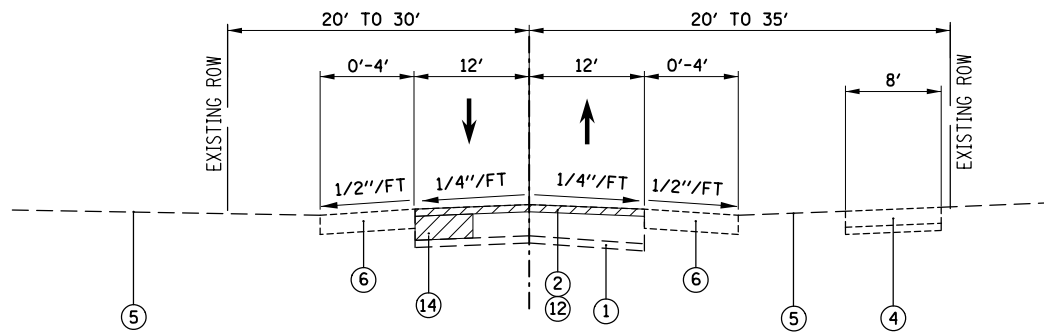
EXISTING TYPICAL SECTION
HARGER ROAD
STA 47+07 TO 56+00
TWO-LANE RURAL SECTION



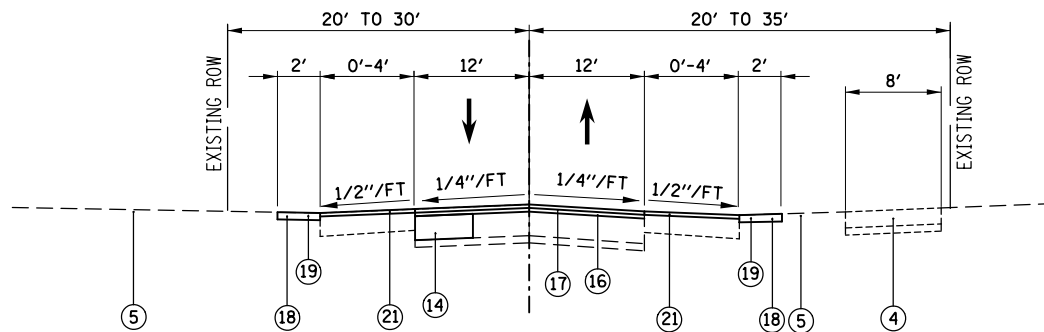
PROPOSED TYPICAL SECTION
HARGER ROAD
STA 10+48 TO 14+50
SPRING ROAD APPROACH (CURB/GUTTER SECTION)



PROPOSED TYPICAL SECTION
HARGER ROAD
STA 47+07 TO 56+00
TWO-LANE RURAL SECTION



EXISTING TYPICAL SECTION
HARGER ROAD
STA 14+50 TO 45+61
STA 56+00 TO 60+04
TWO-LANE RURAL SECTION



PROPOSED TYPICAL SECTION
HARGER ROAD
STA 14+50 TO 45+61
STA 56+00 TO 60+04
TWO-LANE RURAL SECTION

LEGEND

- ① EXISTING AGGREGATE SUBGRADE (APPROX 6" - UNVERIFIED)
 - ② EXISTING HMA PAVEMENT (APPROX. 10" - UNVERIFIED)
 - ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER (TYPE B-6.12)
 - ④ EXISTING HMA MULTI-USE PATH
 - ⑤ EXISTING SOIL AND GROUND COVER
 - ⑥ EXISTING AGGREGATE SHOULDER
 - ⑦ EXISTING HMA SHOULDER
 - *⑧ REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)
 - *⑨ EXISTING PAVEMENT REMOVAL (INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL))
 - *⑩ SUBBASE GRANULAR MATERIAL, TYPE B, 4" (INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL))
 - *⑪ PORTLAND CEMENT CONCRETE BASE COURSE (INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL))
 - ⑫ HOT MIX ASPHALT SURFACE REMOVAL, 3"
 - *⑬ CLASS D PATCHES, VARIOUS, 4"
 - *⑭ CLASS D PATCHES, VARIOUS, 7"
 - ⑮ THIS ITEM HAS BEEN INTENTIONALLY LEFT BLANK
 - ⑯ HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50 - 1.5"
 - ⑰ HOT MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.5"
 - ⑱ FURNISH AND PLACE TOPSOIL, 4"
 - ⑲ SODDING, SALT TOLERANT
 - ⑳ THIS ITEM HAS BEEN INTENTIONALLY LEFT BLANK
 - *㉑ AGGREGATE SHOULDERS, TYPE B
- * INDICATES ITEM AS DIRECTED BY THE ENGINEER IN THE FIELD

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE ITEM	AIR VOIDS @ Ndes	QMP TYPE
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N50; 1.5"	4% @ 50 GYR	LR 1030-2
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50; 1.5"	4% @ 50 GYR	LR 1030-2
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19mm)	4% @ 70 GYR	LR 1030-2
QMP Designations: Quality Control/Quality Assurance (QC/QA) per LR 1030-2		

NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIAL PROVISIONS

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		DATE - AUGUST 2022	REVISED -

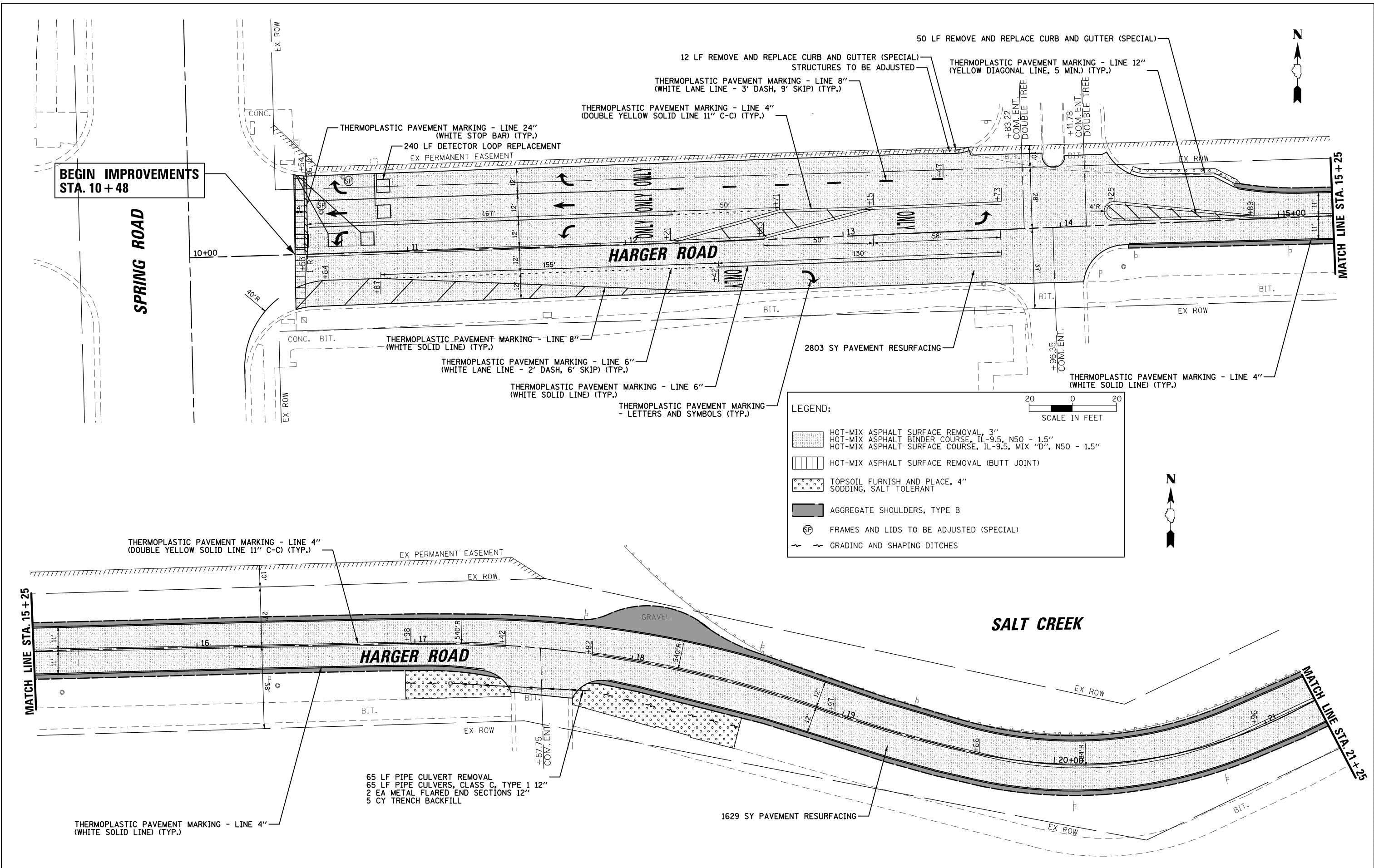
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HARGER ROAD STP RESURFACING PROJECT
TYPICAL SECTIONS

SCALE: 40' SHEET 1 OF 1 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	7
			CONTRACT NO. 61J22	
ILLINOIS FED. AID PROJECT				

Sign Description	MUTCD # or Standard	Color	Dimension (inches)	Area (SF)	Station/Location	LT/RT	Mounting Instructions	Remove Sign Panel Assembly	Telescoping Steel Sign Support
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00	14+30	RT	Post	1	14
HORIZONTAL ALIGNMENT	W1-4R	Black on Yellow	30 x 30	6.25	17+15	RT	Post	1	16
ADVISORY SPEED PLAQUE	W13-1P	Black on Yellow	18 x 18	2.25					
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00	17+65	LT	Post	1	14
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00					
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00	18+50	LT	Post	1	14
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00					
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00	19+35	LT	Post	1	14
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00					
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00	19+55	RT	Post	1	14
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00					
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00	20+35	RT	Post	1	14
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00					
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00	21+00	RT	Post	1	14
CHEVRON ALIGNMENT	W1-8	Black on Yellow	18 x 24	3.00					
HORIZONTAL ALIGNMENT	W1-4R	Black on Yellow	30 x 30	6.25	23+25	LT	Post	1	16
ADVISORY SPEED PLAQUE	W13-1P	Black on Yellow	18 x 18	2.25					
SPEED LIMIT	R2-1	Black on White	24 x 30	5.00	24+75	LT	Post	1	14.5
PASS WITH CARE	R4-2	Black on White	24 x 30	5.00	29+25	RT	Post	1	14.5
SPEED LIMIT	R2-1	Black on White	24 x 30	5.00	37+25	RT	Post	1	14.5
SPEED LIMIT	R2-1	Black on White	24 x 30	5.00	42+30	RT	Post	1	14.5
SPEED LIMIT	R2-1	Black on White	24 x 30	5.00	47+30	RT	Post	1	14.5
SPEED LIMIT	R2-1	Black on White	24 x 30	5.00					
SPEED LIMIT	R2-1	Black on White	24 x 30	5.00	48+25	LT	Post	1	17
DO NOT PASS	R4-1	Black on White	24 x 30	5.00					
STOP	R1-1	White on Red	30 x 30	6.25	56+00	RT	Post	1	14.5
STOP	R1-1	White on Red	30 x 30	6.25	56+65	LT	Post	1	14.5
Totals:				103.50				17	248.5



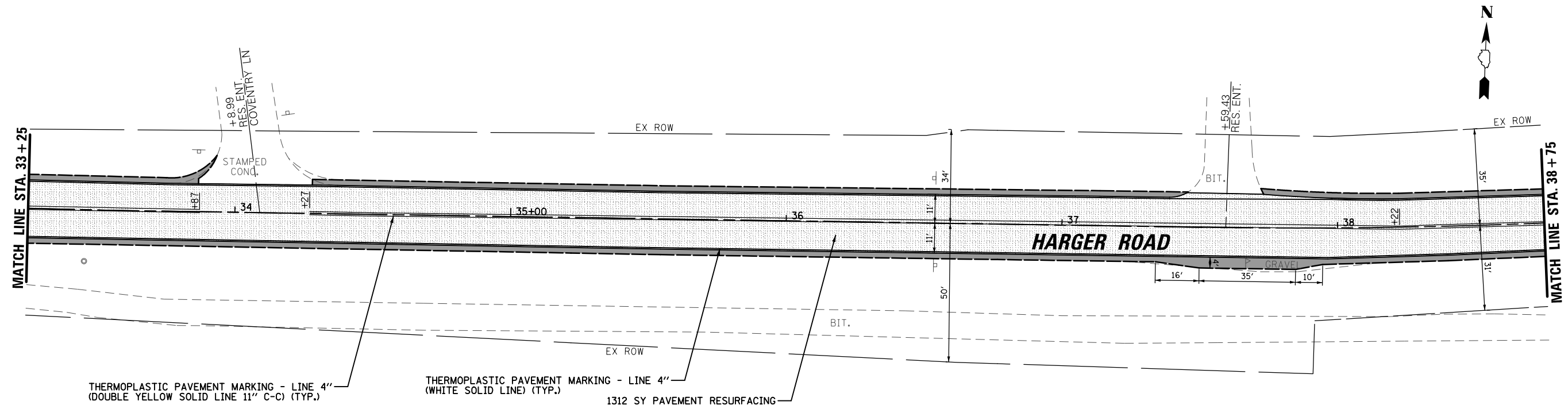
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Default		CHECKED -	OCG	REVISED -	
	PLOT DATE = 2/21/2023	DATE -	AUGUST 2022	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**HARGER ROAD STP RESURFACING PROJECT
PROPOSED PLAN**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	9
			CONTRACT NO. 61J22	
ILLINOIS FED. AID PROJECT				

SCALE: 40' SHEET 1 OF 5 SHEETS STA. 10+48 TO STA. 21+25



THERMOPLASTIC PAVEMENT MARKING - LINE 4"
(DOUBLE YELLOW SOLID LINE 11" C-C) (TYP.)

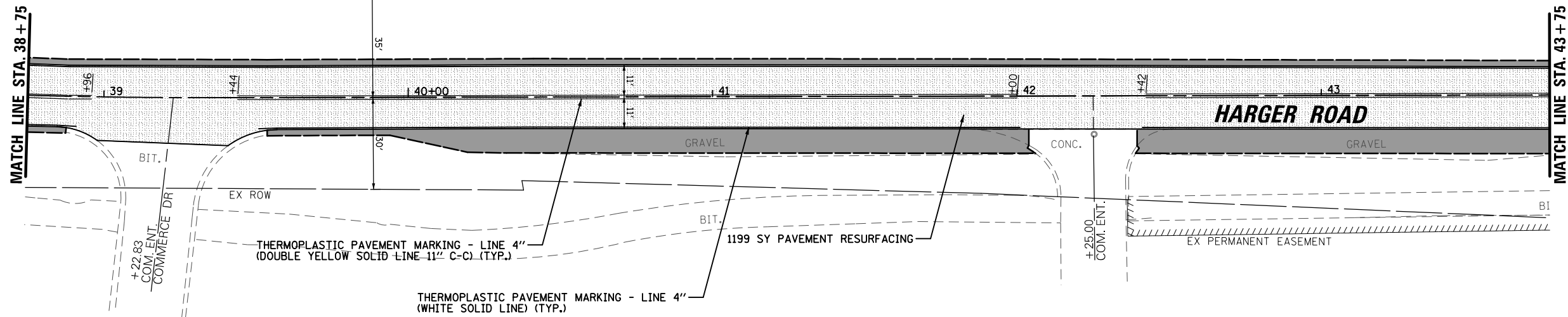
THERMOPLASTIC PAVEMENT MARKING - LINE 4"
(WHITE SOLID LINE) (TYP.)

1312 SY PAVEMENT RESURFACING

LEGEND:

SCALE IN FEET

HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50 - 1.5"
 HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.5"
 HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)
 TOPSOIL FURNISH AND PLACE, 4"
 SODDING, SALT TOLERANT
 AGGREGATE SHOULDERS, TYPE B
 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
 GRADING AND SHAPING DITCHES

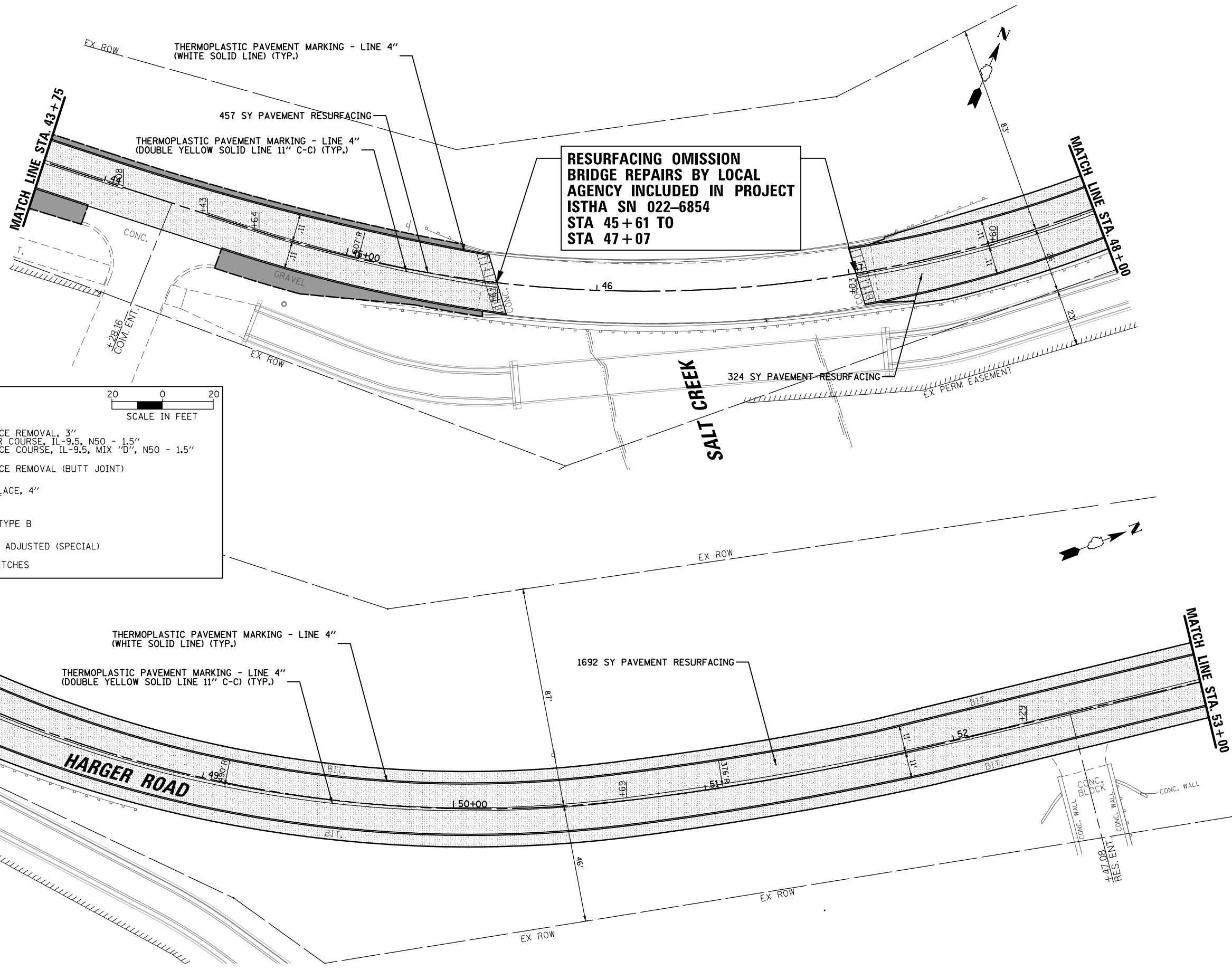


THERMOPLASTIC PAVEMENT MARKING - LINE 4"
(DOUBLE YELLOW SOLID LINE 11" C-C) (TYP.)

THERMOPLASTIC PAVEMENT MARKING - LINE 4"
(WHITE SOLID LINE) (TYP.)

1199 SY PAVEMENT RESURFACING

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD STP RESURFACING PROJECT PROPOSED PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\OAKBROOK\160597.00044\Civil\RPP.160597.00044.03.sht	DRAWN - DJS	REVISED -	1446					22-00053-00-R5	DUPAGE	29	11	
Default	PLOT SCALE = 40'	CHECKED - OCG	REVISED -		SCALE: 40' SHEET 3 OF 5 SHEETS STA. 33+25 TO STA. 43+75			CONTRACT NO. 61J22		ILLINOIS FED. AID PROJECT		
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -									



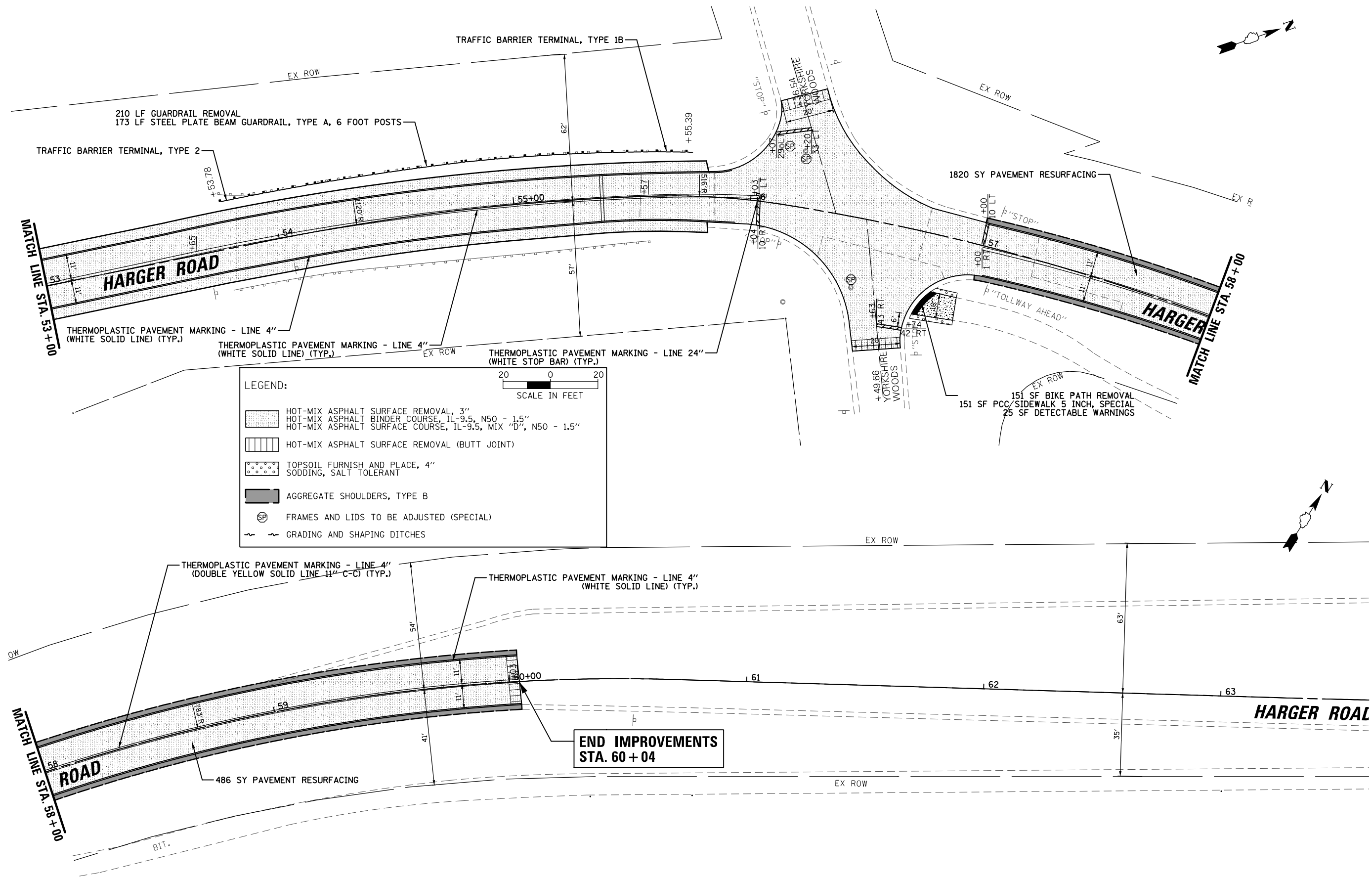
**RESURFACING OMISSION
BRIDGE REPAIRS BY LOCAL
AGENCY INCLUDED IN PROJECT
ISTHA SN 022-6854
STA 45+61 TO
STA 47+07**

LEGEND:

SCALE IN FEET: 0, 20

- HOT-MIX ASPHALT SURFACE REMOVAL, 3"
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50 - 1.5"
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.5"
- HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)
- TOPSOIL FURNISH AND PLACE, 4"
SODDING, SALT TOLERANT
- AGGREGATE SHOULDERS, TYPE B
- FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)
- GRADING AND SHAPING DITCHES

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD STP RESURFACING PROJECT PROPOSED PLAN			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\OAKBROOK\160597.00044\Civil\RPP\160597.00044.04.sht	DRAWN - DJS	REVISED -	1446					22-00053-00-R5	DUPAGE	29	12	
Default	PLOT SCALE = 48'	CHECKED - OCG	REVISED -		SCALE: 40' SHEET 4 OF 5 SHEETS STA. 43+75 TO STA. 53+00			CONTRACT NO. 61J22				
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -		ILLINOIS FED. AID PROJECT							



LEGEND:

HOT-MIX ASPHALT SURFACE REMOVAL, 3"
 HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50 - 1.5"
 HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50 - 1.5"

HOT-MIX ASPHALT SURFACE REMOVAL (BUTT JOINT)

TOPSOIL FURNISH AND PLACE, 4"
 SODDING, SALT TOLERANT

AGGREGATE SHOULDERS, TYPE B

FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)

GRADING AND SHAPING DITCHES

20 0 20
 SCALE IN FEET

FILE NAME = N:\OAKBROOK\160597.00044\Civil\RPP.160597.00044.05.sht	USER NAME = dschroeder	DESIGNED - DJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD STP RESURFACING PROJECT PROPOSED PLAN			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 48'	CHECKED - OCG	REVISED -					1446	22-00053-00-RS	DUPAGE	29	13
Default	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -	SCALE: 40'	SHEET 5 OF 5 SHEETS	STA. 53+00 TO STA. 60+04	CONTRACT NO. 61J22 ILLINOIS FED. AID PROJECT					



SCHEDULE OF SIGNS

SIGN NO.	1	2	3	4	5	6	7	8
SIGN TYPE	Harger Road 8" UC LETTERS 48X18	ROAD CLOSED R-11-2-48X18	DETOUR M4-9 R (O) 30X24	DETOUR M4-9 L (O) 30X24	DETOUR M4-9 (O) 30X21	ROAD CLOSED TO THRU TRAFFIC R-11-4-60X30	EAST M3-2 (O) 24X12	WEST M3-4 (O) 24X12
SIGN NO.	9	10	3A	4A	11	12	13	
SIGN TYPE	DETOUR M4-9 R (O) 30X24	ROAD CLOSED 500 FT W20-3 (O) 48X48	DETOUR M4-9 R (O) 30X24	DETOUR M4-9 L (O) 30X24	END DETOUR M4-8A 18x24	DETOUR AHEAD W20-2 (O) 48X48	DETOUR M4-10 L 48x18	

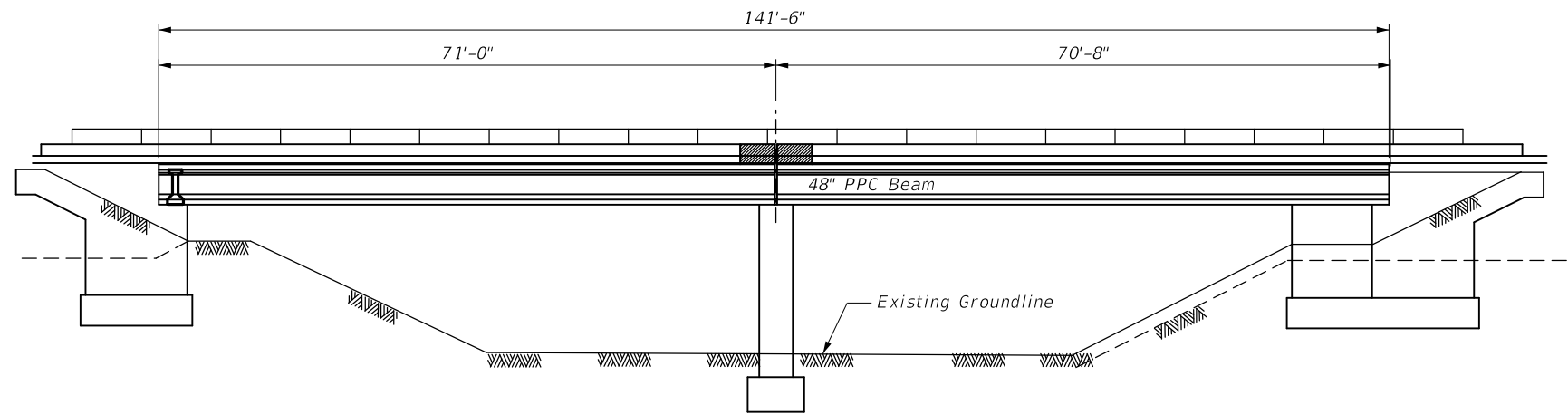
MAINTENANCE OF TRAFFIC GENERAL NOTES

- ALL SIGNAGE TO BE IN ACCORDANCE WITH MUTCD. SUGGESTED MAINTENANCE OF TRAFFIC SHOWN IS MINIMUM REQUIRED; CONTRACTOR SHALL PROVIDE ADDITIONAL TRAFFIC CONTROL MEASURES AS DIRECTED BY RESIDENT ENGINEER. THIS WORK WILL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (DETOUR).
- THE DETOUR SHALL REMAIN IN PLACE FOR A MAXIMUM OF 21 CALENDAR DAYS.
- TWO CHANGABLE MESSAGE SIGNS SHALL BE PLACED ON HARGER ROAD AT THE DIRECTION OF THE ENGINEER.

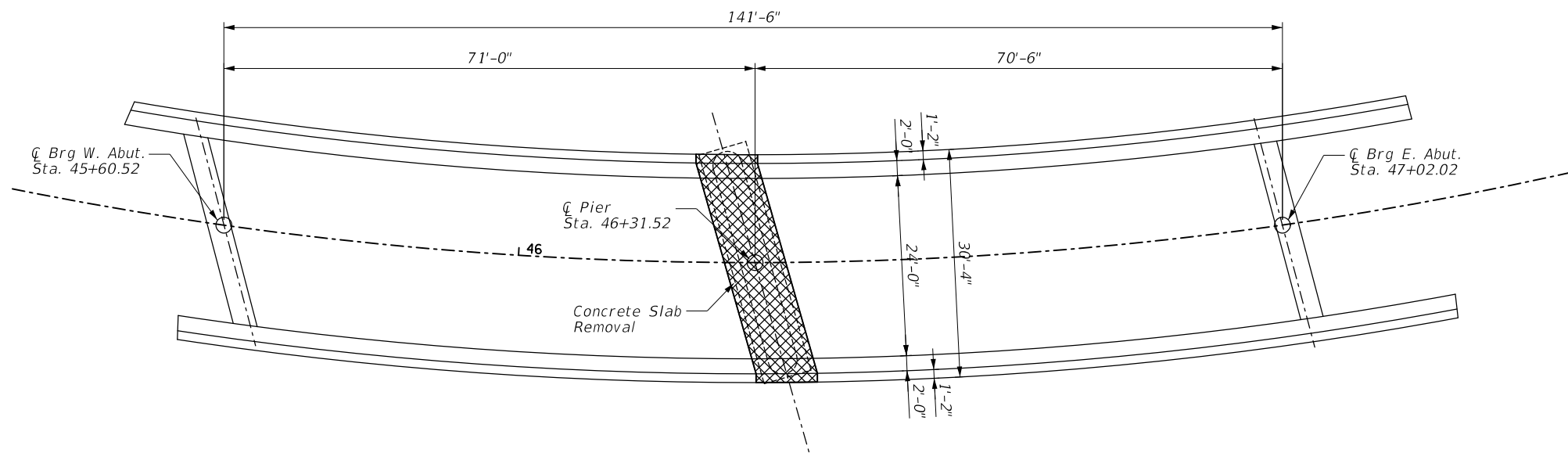
Bench Mark: Square cut on northeast corner wall of Bridge over Salt Creek over Harger Road.
Elevation 665.10

Existing Structure: Existing structure is 141'-6" long, two-span 48" PPC Beams with an 8 inch concrete deck supported by cast in place abutments and pier on steel piles. The out to out width of the deck is 30'-4" with a 24'-0" wide roadway, 2'-1'-9" wide walks, and a 1'-2" wide parapet along each end.

Traffic: See sheet 14 for detour plan required to complete bridge maintenance work.



ELEVATION



PLAN

INDEX OF SHEETS

- 1 General Plan and Elevation-Bridge
- 2 General Notes and Details
- 3 Abutment Details
- 4 Pier Details
- 5 Joint Replacement Plan
- 6 Joint Replacement Details
- 7 Deck Repair

SCOPE OF WORK

- 1 Remove existing joint over center pier
- 2 Replace joint over pier
- 3 Concrete repairs to sidewalk and parapet on deck
- 4 Concrete Repairs to abutments and pier of substructure

LOADING HS-20

DESIGN SPECIFICATIONS

2020 AASHTO Bridge Design Specifications, 9th Edition with Interims

DESIGN STRESSES

FIELD UNITS (PROPOSED)

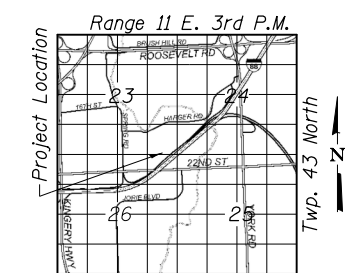
$f'_c = 3,500$ psi (Concrete Structure)
 $f'_c = 4,000$ psi (Concrete Superstructure)
 $f_y = 60,000$ psi (Reinforcement)

FIELD UNITS (EXISTING)

$f'_c = 3,000$ psi (Concrete Structures)
 $f_y = 40,000$ psi (Reinforcement)

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (S_{D1}) = 0.067g
Design Spectral Acceleration at 0.2 sec. (S_{D5}) = 0.126g
Soil Site Class = C



LOCATION SKETCH

I Certify That To The Best Of My Knowledge, Information And Belief, This Bridge Design Is Structurally Adequate For The Design Loading Shown On The Plans, The Design Is An Economical One For The Style Of Structure And Complies With Requirements Of The Current "AASHTO LRFD Bridge Design Specifications".



Majid Mobasseri 2/16/23
MAJID MOBASSERI

ILLINOIS REGISTRATION No. 081-005058 STRUCTURAL ENGINEER
EXPIRATION DATE: 11/30/24

GENERAL PLAN & ELEVATION
HARGER ROAD OVER SALT CREEK
SN 022-6854
DuPAGE COUNTY
STATION 45+460.52 TO 47+02.02

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED - _____
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Default	PLOT SCALE = 28"	CHECKED - OCG	REVISED - _____
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED - _____

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HARGER ROAD
GENERAL PLAN AND ELEVATION

SCALE: 20' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	15
			CONTRACT NO. 61J22	
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A706 GR 60. SEE SPECIAL PROVISIONS.
2. REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.
3. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK; HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.
4. EXISTING REINFORCEMENT SHALL BE CLEANED AND INCORPORATED INTO THE NEW CONSTRUCTION. COST INCLUDED WITH CONCRETE REMOVAL.
5. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2022; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD), THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS" (SSTCI), THE "DETAILS" IN THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.
6. THE CONTRACTOR SHALL LIMIT HIS/HER CONSTRUCTION ACTIVITIES TO THE WORK AREAS DESIGNATED ON THE PLANS. ANY DAMAGE TO AREAS OUTSIDE OF THESE LIMITS SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.
7. DURING THE CONSTRUCTION OPERATIONS WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES SO THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF THE CONSTRUCTION OPERATIONS ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS CAUSED BY THE CONSTRUCTION. ALL CONSTRUCTION DEBRIS SHALL BE KEPT OUT OF THE RIVER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PAY ITEM TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
8. THE CONTRACTOR SHALL GIVE NOTICES AND COMPLY WITH APPLICABLE LAWS, ORDINANCES, RULES, REGULATIONS AND LAWFUL ORDERS OF ALL PUBLIC AUTHORITIES BEARING ON SAFETY OF PERSONS OR PROPERTY OR THEIR PROTECTION FROM DAMAGE, INJURY OR LOSS.
9. **UTILITIES**
 - (A) ALL UNDERGROUND UTILITY LOCATIONS, INCLUDING BUT NOT LIMITED TO SANITARY AND STORM SEWERS, WATER MAINS AND THEIR RESPECTIVE SERVICE LINES, SHOWN ON THE PLANS ARE APPROXIMATE ONLY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL UTILITIES AS NECESSARY, PRIOR TO STARTING CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. AT (800) 892-0123, (OR 811) AND ALL PUBLIC AND PRIVATE UTILITIES BEFORE STARTING CONSTRUCTION.
 - (B) THE CONTRACTOR SHALL TEMPORARILY BRACE/SUPPORT EXISTING UTILITIES DURING THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE ITEM BEING CONSTRUCTED. CONTRACTOR SHALL COORDINATE WITH UTILITY OWNER PRIOR TO CONSTRUCTION OF ANY TEMPORARY WORK TO UTILITY.
 - (C) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES

EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.

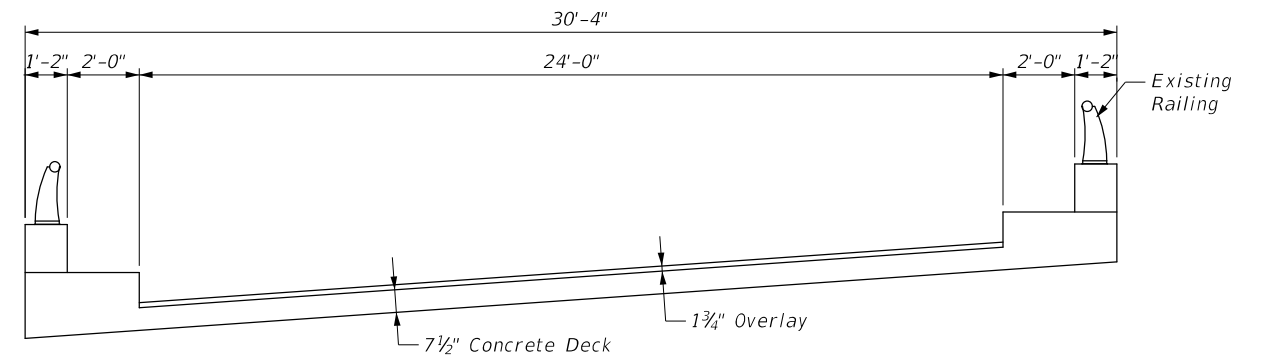
10. MISCELLANEOUS

- (A) ALL SAWCUTTING SHALL BE INCLUDED IN THE COST OF THE REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.
- (B) NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER SIGNS AND BARRICADES HAVE BEEN INSTALLED.
- (C) ANY REPAIRS FOR DISTURBANCE OUTSIDE OF THE LIMITS OF WORK SHALL BE INCIDENTAL TO THE CONTRACT

11. THE LOCATION AND EXTENT OF REPAIR ITEMS ARE BASED ON VISUAL OBSERVATION MADE DURING FIELD INSPECTIONS AND ARE NOT GUARENTEED TO BE ACCURATE OR ALL INCLUSIVE. THE LOCATION, EXTENT OF REPAIR, AND QUANTITY OF REPAIR SHALL BE FIELD VERIFIED BY THE FIELD ENGINEER.

12. A HANDS-ON INSPECTION WAS NOT COMPLETED ON THE PIER DUE TO ACCESSIBILITY. ESTIMATES WERE CREATED BASED ON VISUAL INSPECTION. FOR ESTIMATING PURPOSES AN ADDITIONAL QUANTITY OF STRUCTURAL REPAIR OF CONCRETE WAS INCLUDED TO ACCOUNT FOR ADDITIONAL QUANTITY IDENTIFIED DURING CONSTRUCTION. THIS ADDITIOINAL QUANTITY CAN BE USED AT THE ENGINEER'S DISCRETION.

13. DEBRIS REMOVAL AT THE PIER SHAL BE COMPLETED PRIOR TO BEGINNING STRUTURAL REPAIR OF CONCRETE. THIS REMOVAL SHALL BE DONE BY OTHERS (COUNTY) BUT SHALL BE CORRDNATED BY THE CONTRACTOR TO ENSURE ADDITIONAL DEBRIS WILL NOT ACCUMULATE BY THE TIME THE REPAIR WORK BEGINS.



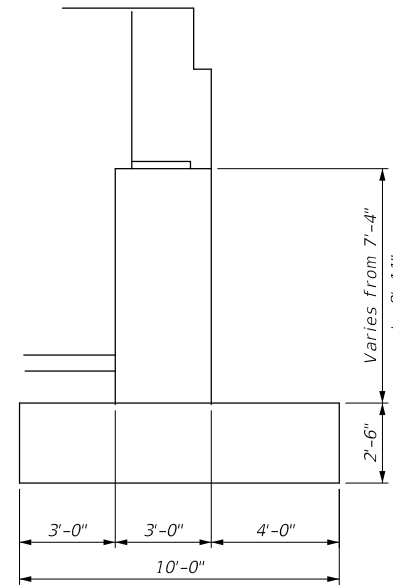
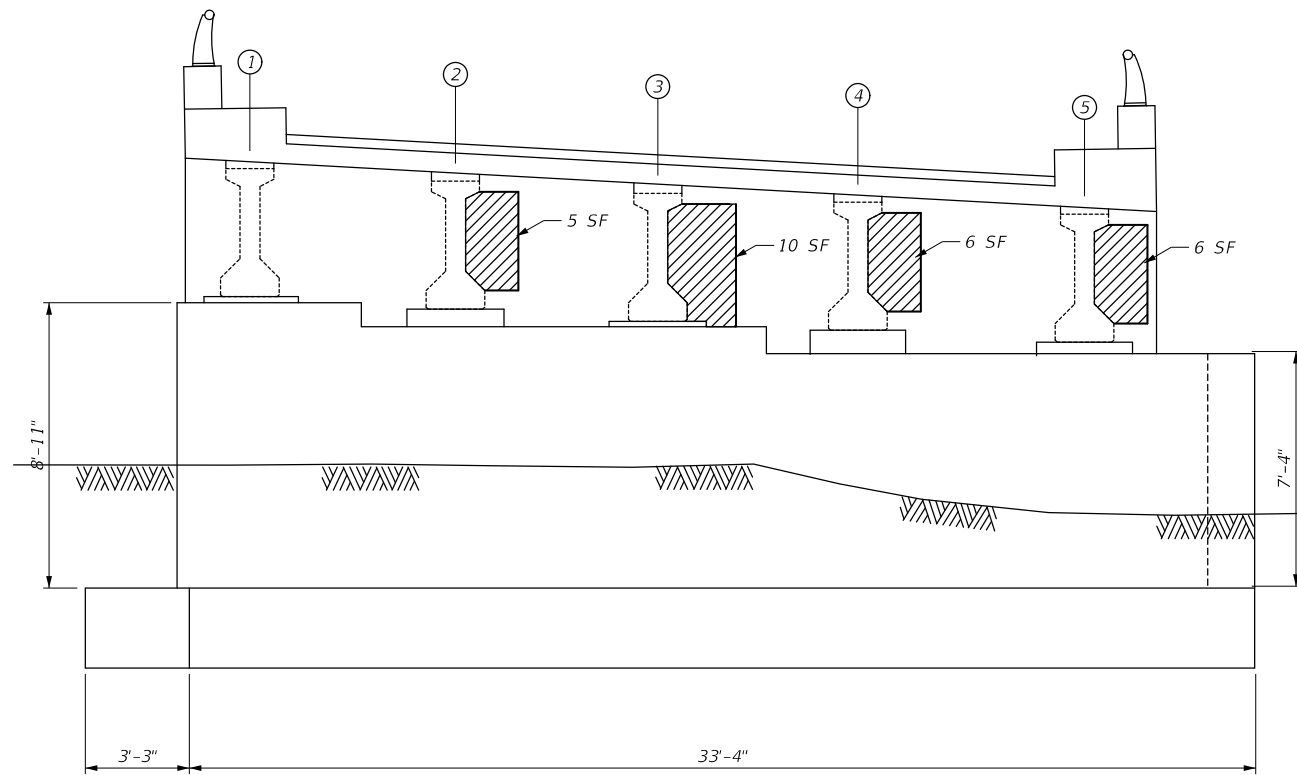
EXISTING BRIDGE CROSS SECTION

SUMMARY OF QUANTITIES

PAY ITEM	ITEM	UNIT	QUANTITY
50102400	Concrete Removal	Cu. Yd.	15.0
50300255	Concrete Superstructure	Cu. Yd.	15.0
50300260	Bridge Deck Grooving	Sq. Yd.	25
50300300	Protective Coat	Sq. Yd.	35
50800205	Reinforcement Bars, Epoxy Coated	Pound	2,250
52000110	Preformed Joint Strip Seal	Foot	35
* Z0012754	Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	420
* Z0012755	Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq. Ft.	80
* Z0016200	Deck Slab Repair (Partial)	Sq. Yd.	70

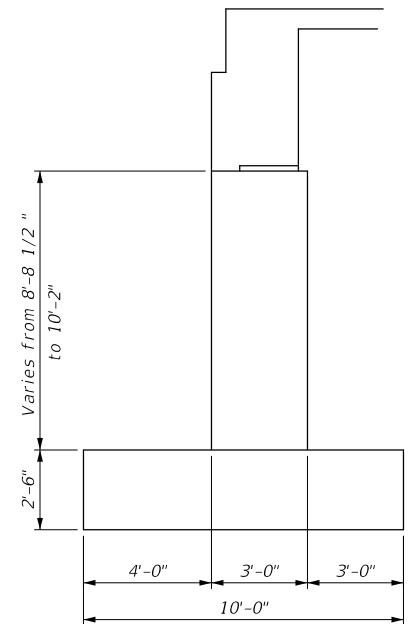
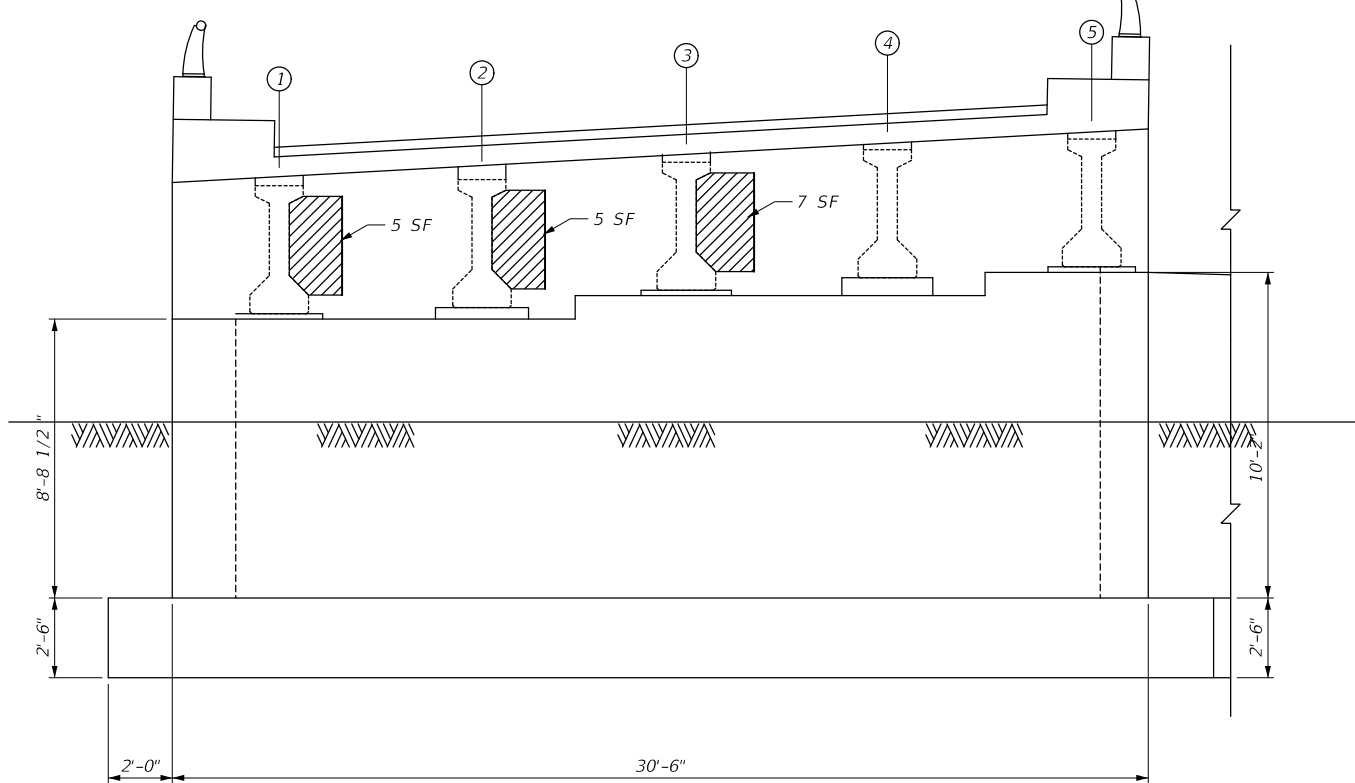
* See Special Provisions

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD GENERAL NOTES AND SUMMARY OF QUANTITIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
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Default	PLOT SCALE = 2'	CHECKED - OCG	REVISIED -			SCALE: 2'	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISIED -									



 Structural Repair of Concrete

ELEVATION-WEST ABUTMENT



ELEVATION-EAST ABUTMENT

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete Equal To or Less Than 5"	SF	45

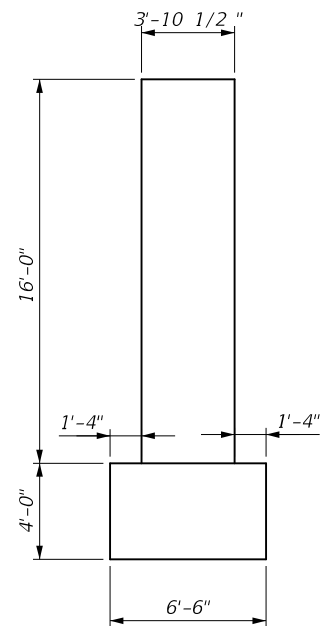
FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED - _____
N:\OAKBROOK\160597.00044\Struct\160597.00044-03-Abutment.sht		DRAWN - DJS	REVISED - _____
Default	PLOT SCALE = 2"	CHECKED - OCG	REVISED - _____
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED - _____

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

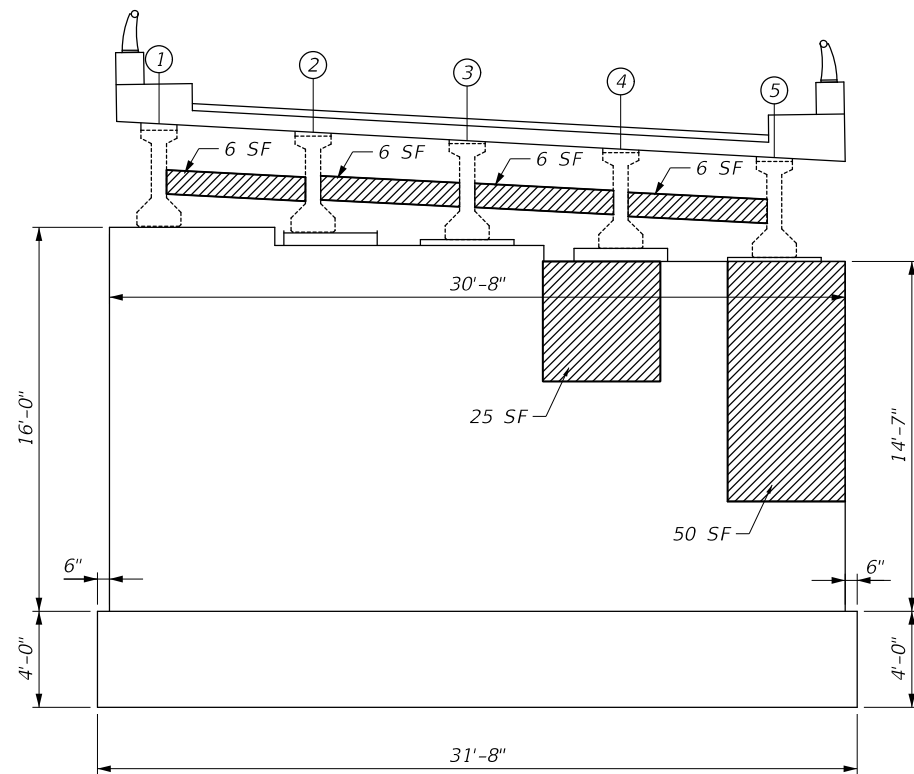
**HARGER ROAD
ABUTMENT REPAIRS**

SCALE: 2" _____ SHEET OF _____ SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	17
			CONTRACT NO. 61J22	
ILLINOIS FED. AID PROJECT				

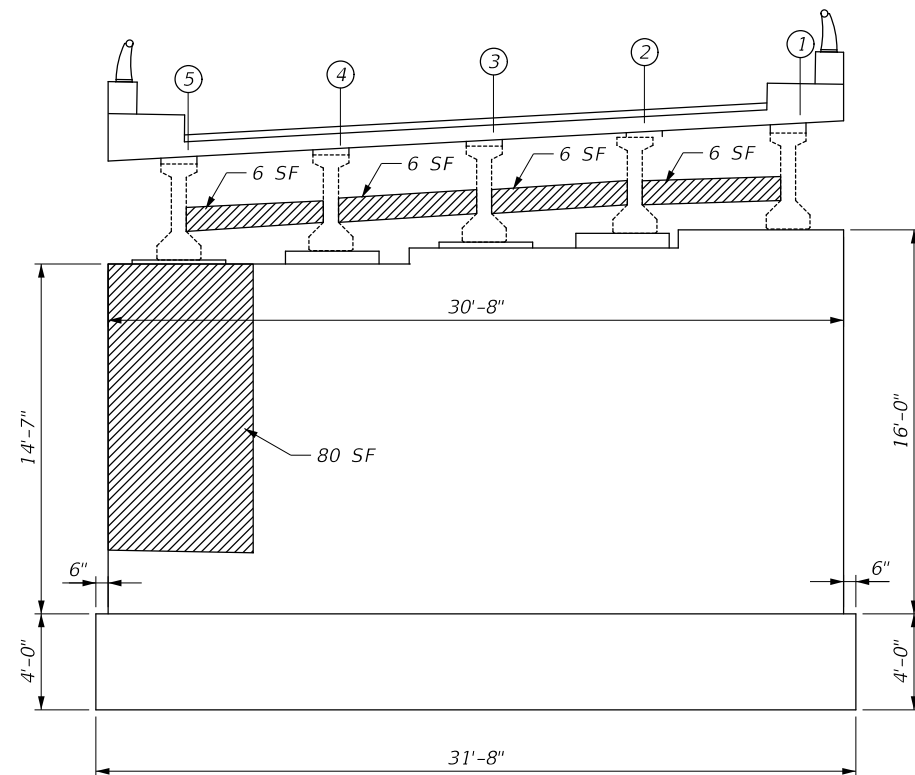


PIER ELEVATION
(SOUTH FACE)

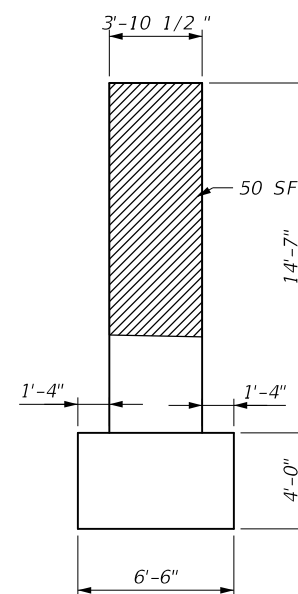


PIER ELEVATION
(EAST FACE)

 Structural Repair of Concrete



PIER ELEVATION
(WEST FACE)



PIER ELEVATION
(NORTH FACE)

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair of Concrete Equal To or Less Than 5"	SF	375
Structural Repair of Concrete Greater Than 5"	SF	80

Note: During repairs below Beams 4 and 5, if it is determined that beam pedestal will need concrete structure repairs, Contractor will address need for potential temporary shoring or cribbing with the Engineer at that time. Payment will be as stated in Special Provision for Structural Repair of Concrete.

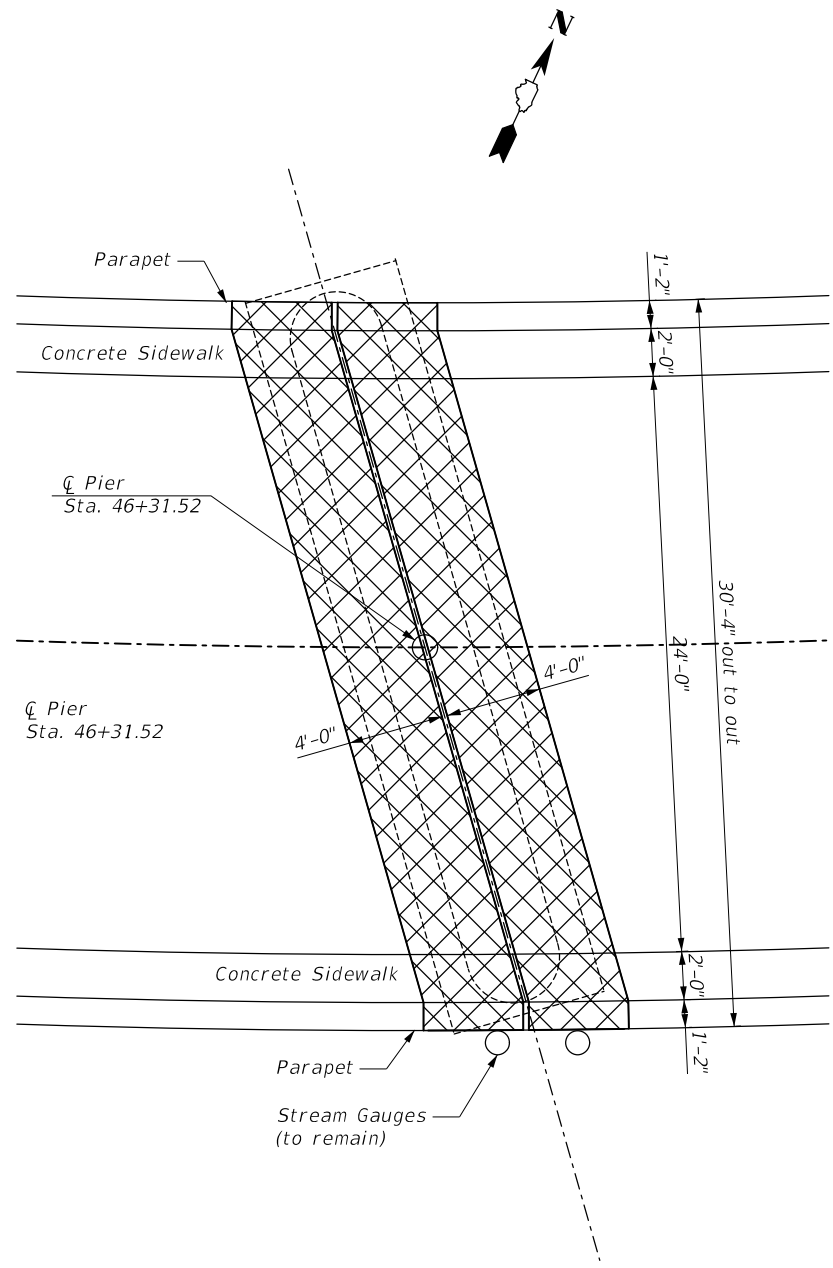
FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED - _____
N:\OAKBROOK\160597.00044\Struct\160597.00044-04-Pier.sht		DRAWN - DJS	REVISED - _____
Default	PLOT SCALE = 2"	CHECKED - OCG	REVISED - _____
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED - _____

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

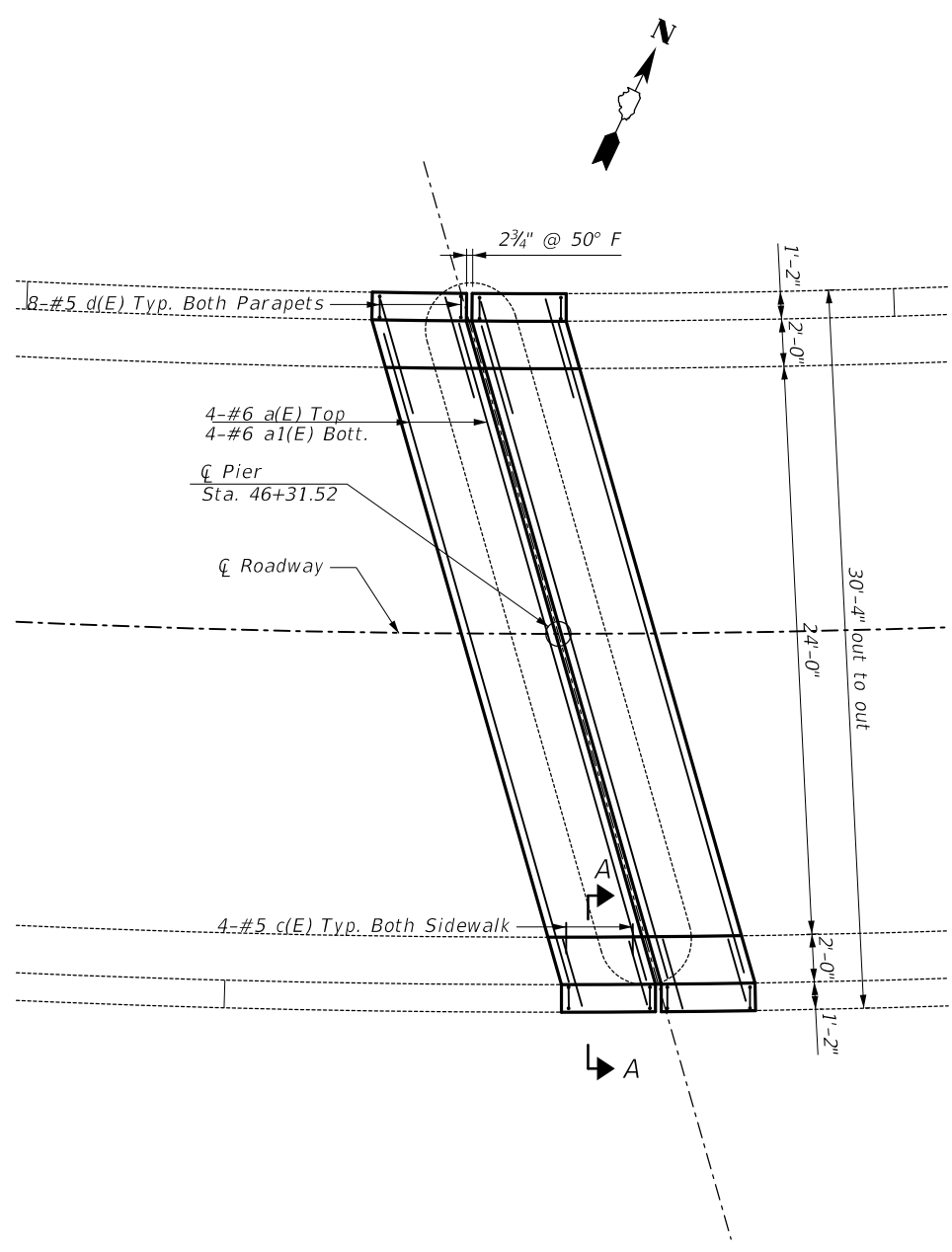
HARGER ROAD
PIER REPAIRS

SCALE: 2" SHEET OF _____ SHEETS STA. TO STA.

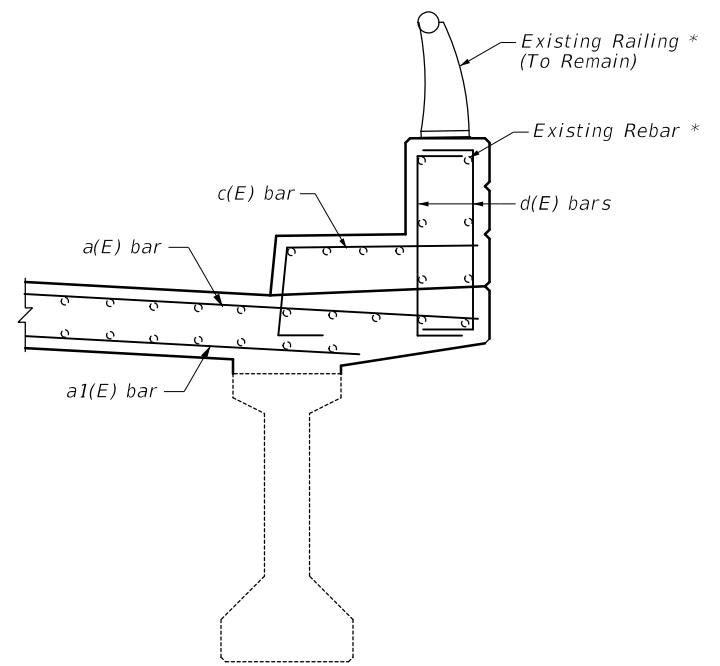
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	18
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61J22	



PLAN - CONCRETE REMOVAL



PLAN - CONCRETE REPLACEMENT

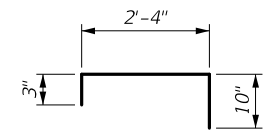


SECTION A-A

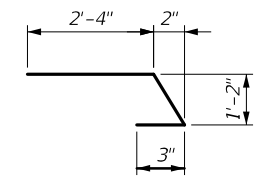
* Existing reinforcement to be cleaned and incorporated into new construction. Rail post removal & reinstallation including any anchor bolts will be included in the cost of Concrete Superstructure.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	8	#6	30'-0"	—
a1(E)	8	#6	30'-0"	—
c(E)	16	#5	4'-5"	└
d(E)	32	#5	3'-7"	└
Concrete Removal			Cu. Yd.	15.0
Concrete Superstructure			Cu. Yd.	15.0
Reinforcement Bars, Epoxy Coated			Pound	2,250



BAR d(E)



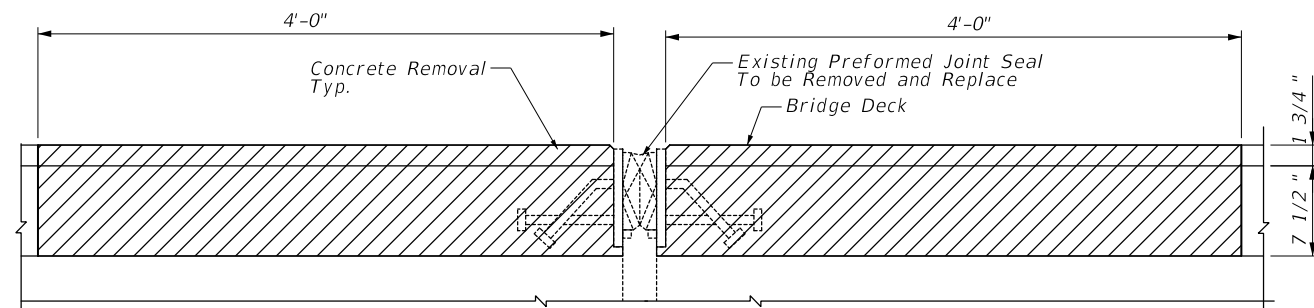
BAR c(E)

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED - _____
N:\OAKBROOK\160597.00044\Struct\160597.00044-05-Joint.sht		DRAWN - DJS	REVISED - _____
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	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED - _____

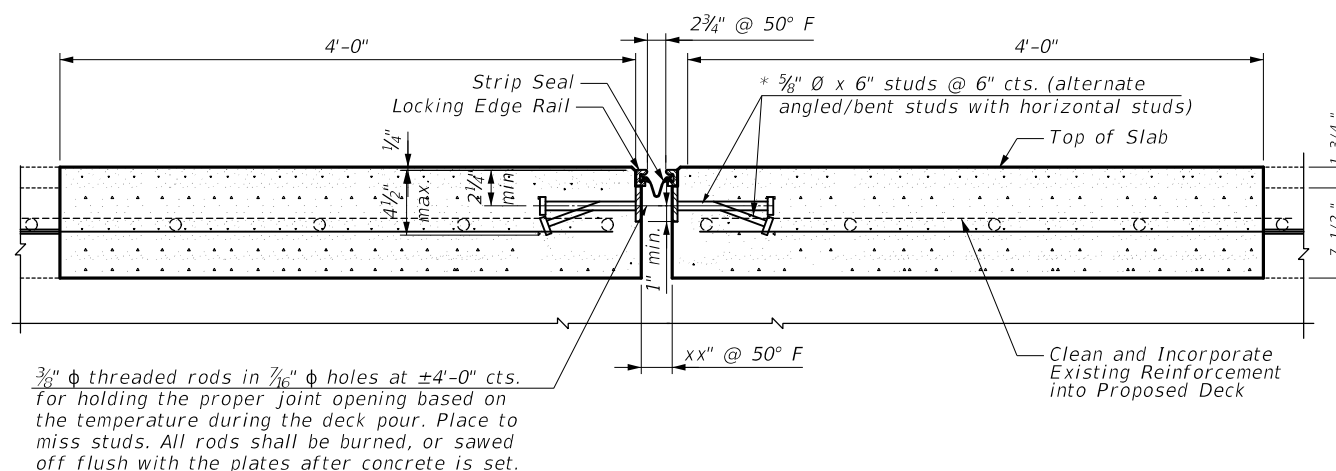
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HARGER ROAD
PLAN-REMOVAL AND CONCRETE REPLACEMENT
SCALE: 8" _____ SHEET OF _____ SHEETS STA. _____ TO STA. _____

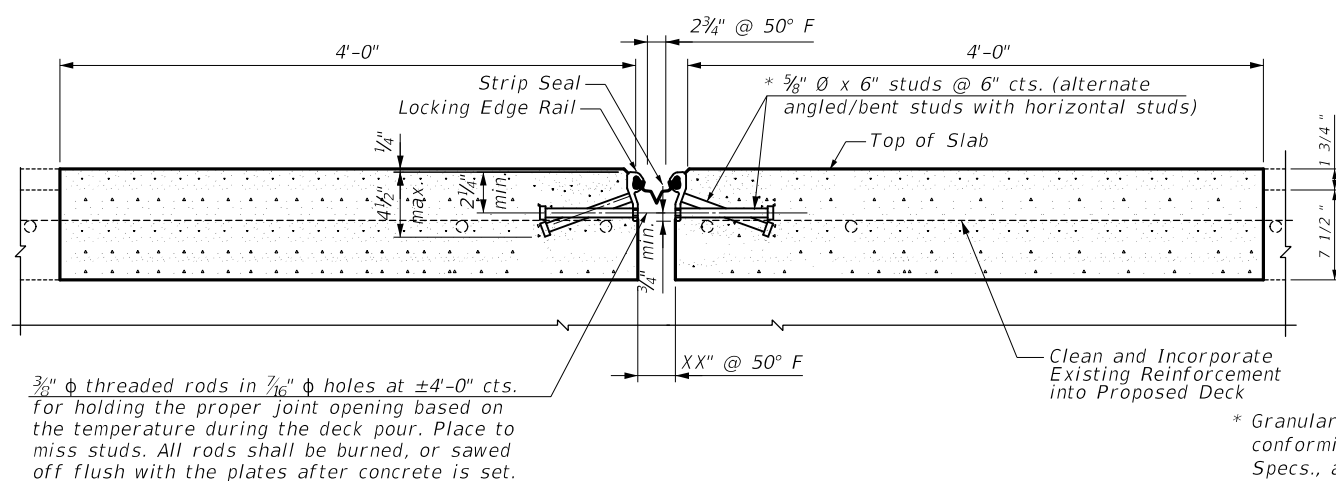
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	19
CONTRACT NO. 61J22				
ILLINOIS FED. AID PROJECT				



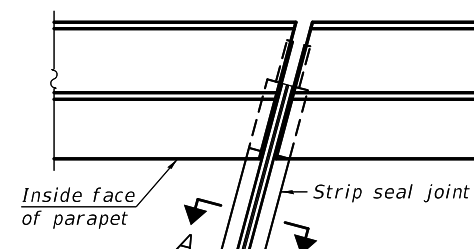
CONCRETE DECK AND JOINT SEAL REMOVAL



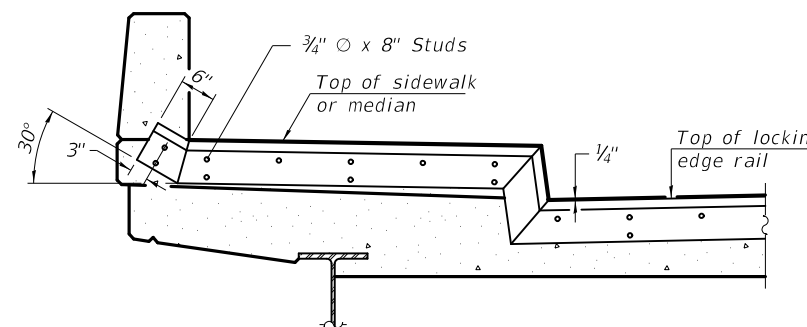
DETAIL-NEW CONCRETE DECK AND WELDED RAIL JOINT



DETAIL-NEW CONCRETE DECK AND ROLLED RAIL JOINT

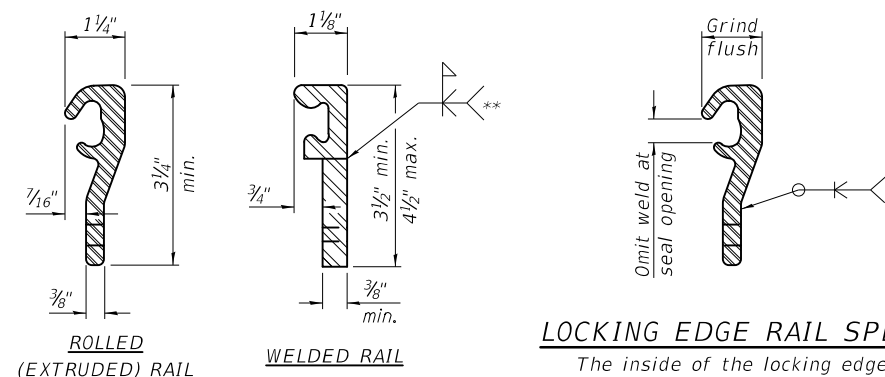


FOR SKEWS ≤ 30°



TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.

LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	35

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.

Cost of parapet sliding plates, sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.

39" constant slope barrier shown, 44" constant slope barrier similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

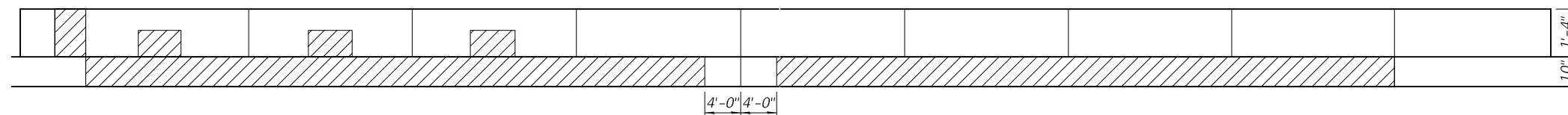
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Default	PLOT SCALE = 2"	CHECKED - OCG	REVISED -
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**HARGER ROAD
PREFORMED JOINT SEAL DETAIL**

SCALE: 2" SHEET OF SHEETS STA. TO STA.

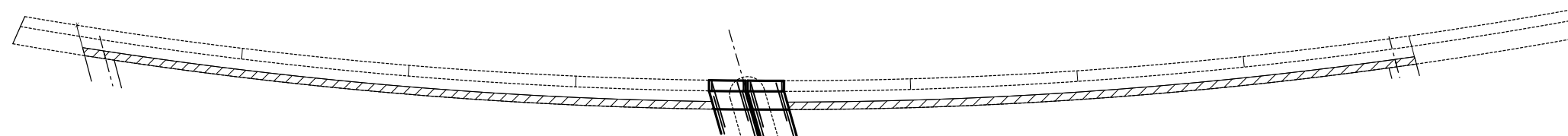
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	20
CONTRACT NO. 61J22				
ILLINOIS FED. AID PROJECT				



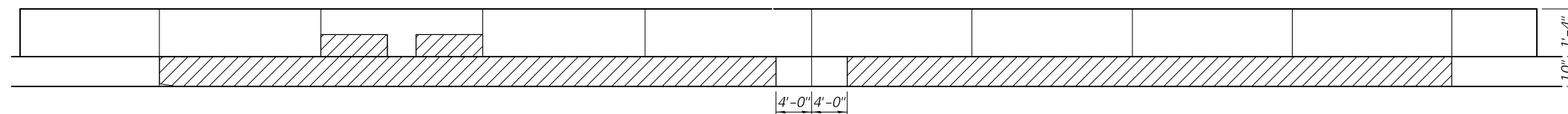
ELEVATION NORTH



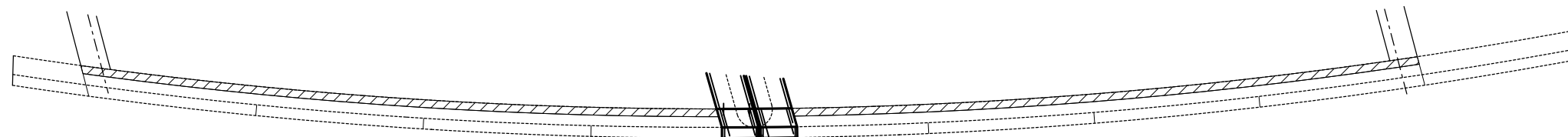
Deck Slab Repair (Partial)



PLAN NORTH



ELEVATION SOUTH



PLAN SOUTH

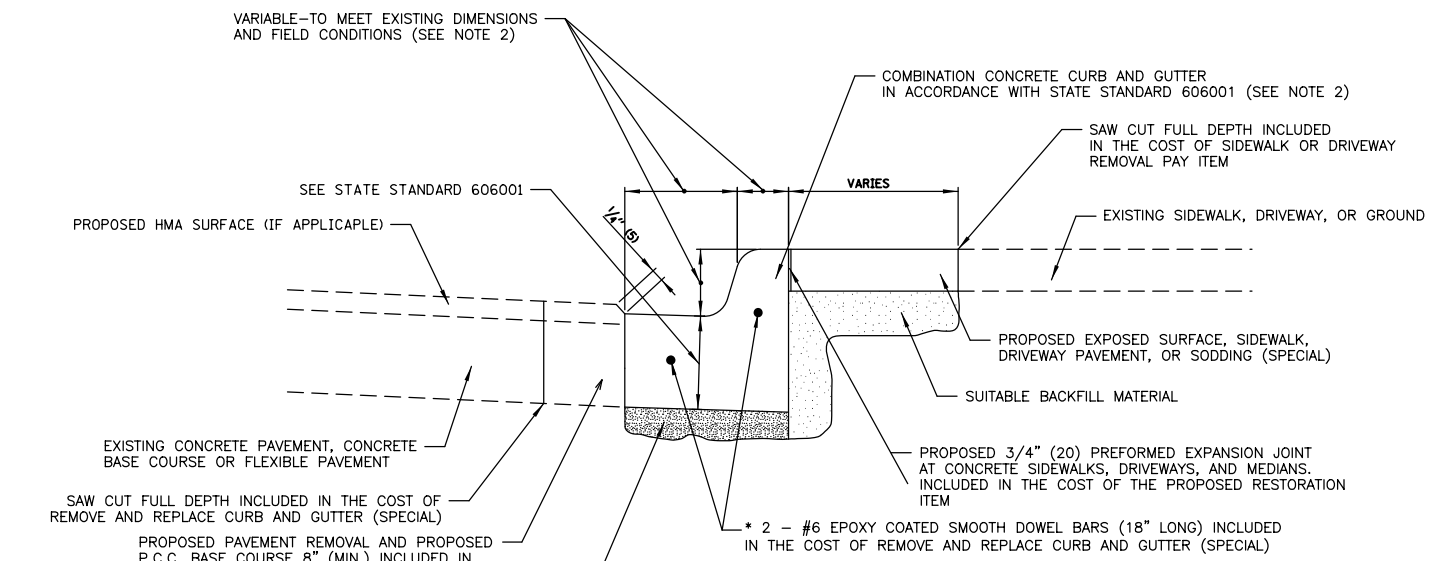
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Default	PLOT SCALE = 16'	CHECKED - OCG	REVISED - _____
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED - _____

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**HARGER ROAD
EXISTING JOINTS PLAN AND ELEVATION**

SCALE: NTS SHEET OF _____ SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	21
			CONTRACT NO. 61J22	
ILLINOIS FED. AID PROJECT				



SEE STATE STANDARD 606001

PROPOSED HMA SURFACE (IF APPLICABLE)

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

SAW CUT FULL DEPTH INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)

PROPOSED PAVEMENT REMOVAL AND PROPOSED P.C.C. BASE COURSE 8" (MIN.) INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL) WHEN ABUTTING EXISTING PAVEMENT

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH THE ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

- NOTE:**
1. SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
 2. CURB OR CURB AND GUTTER SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE DIRECTED BY THE ENGINEER IN THE FIELD
 4. LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)
 5. THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)
 6. THE REMOVAL EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 OF THE STANDARD SPECIFICATIONS AND PAID FOR REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)
 7. THE INSTALLATION OF CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 606 OF THE STANDARD SPECIFICATIONS AND PAID FOR AS REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)
 8. THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

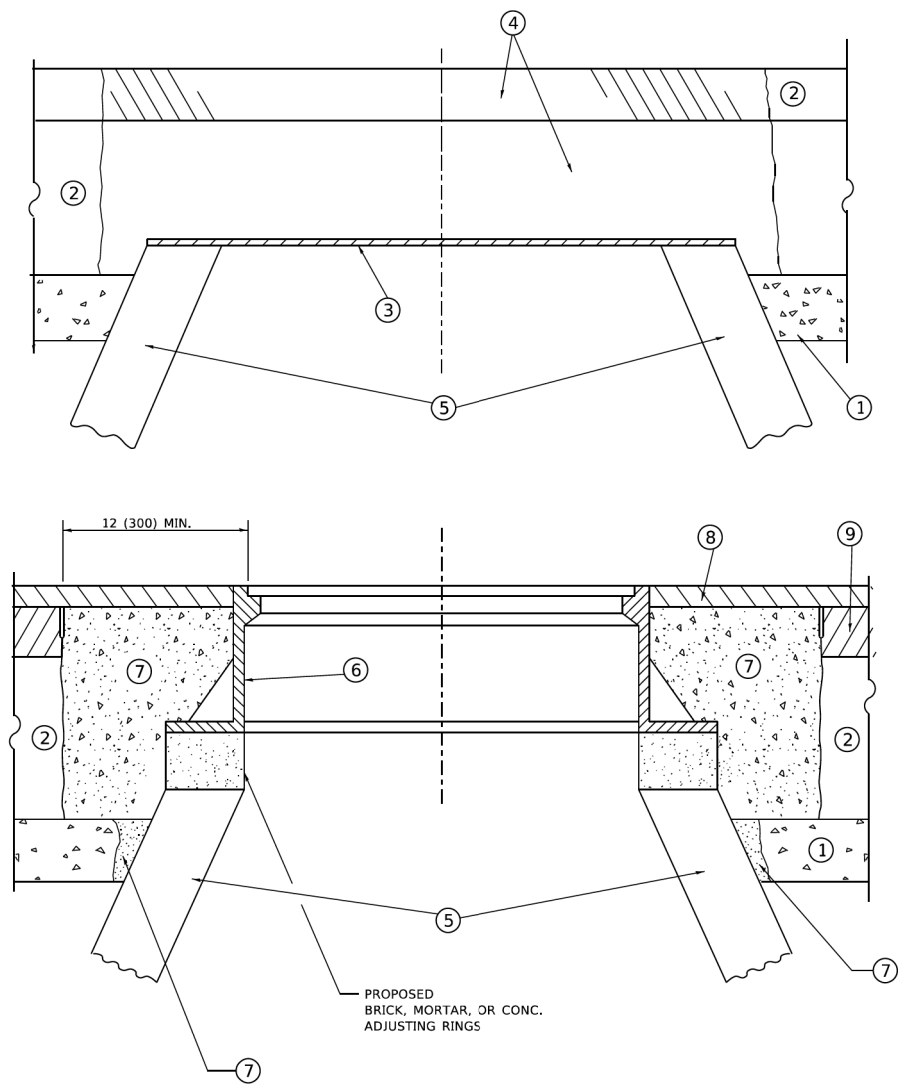
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)"

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REMOVE AND REPLACE CURB AND GUTTER (SPECIAL)

FILE NAME =	USER NAME = dschroeder	DESIGNED - DJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD STP RESURFACING PROJECT CONSTRUCTION DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\OAKBROOK\160597.00044\Civi1\DETT1.160597.00044.shx	DRAWN - DJS	REVISIED -	1446			22-00053-00-R5	DUPAGE	29	22	
Default	PLOT SCALE = 48"	CHECKED - OCG	REVISED -			CONTRACT NO. 61J22				
	PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -			SCALE: 40'	SHEET 1 OF 1 SHEETS	STA. N/A	TO STA. N/A	ILLINOIS FED. AID PROJECT



DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

CONSTRUCTION PROCEDURES

- STAGE 1 (BEFORE PAVEMENT MILLING)**
- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
 - REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
 - COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
 - BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 1 1/2 (40) HMA TO REMAIN AFTER MILLING).
- STAGE 2 (AFTER PAVEMENT MILLING)**
- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS*PP-1 CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- *UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS*PP-1 CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

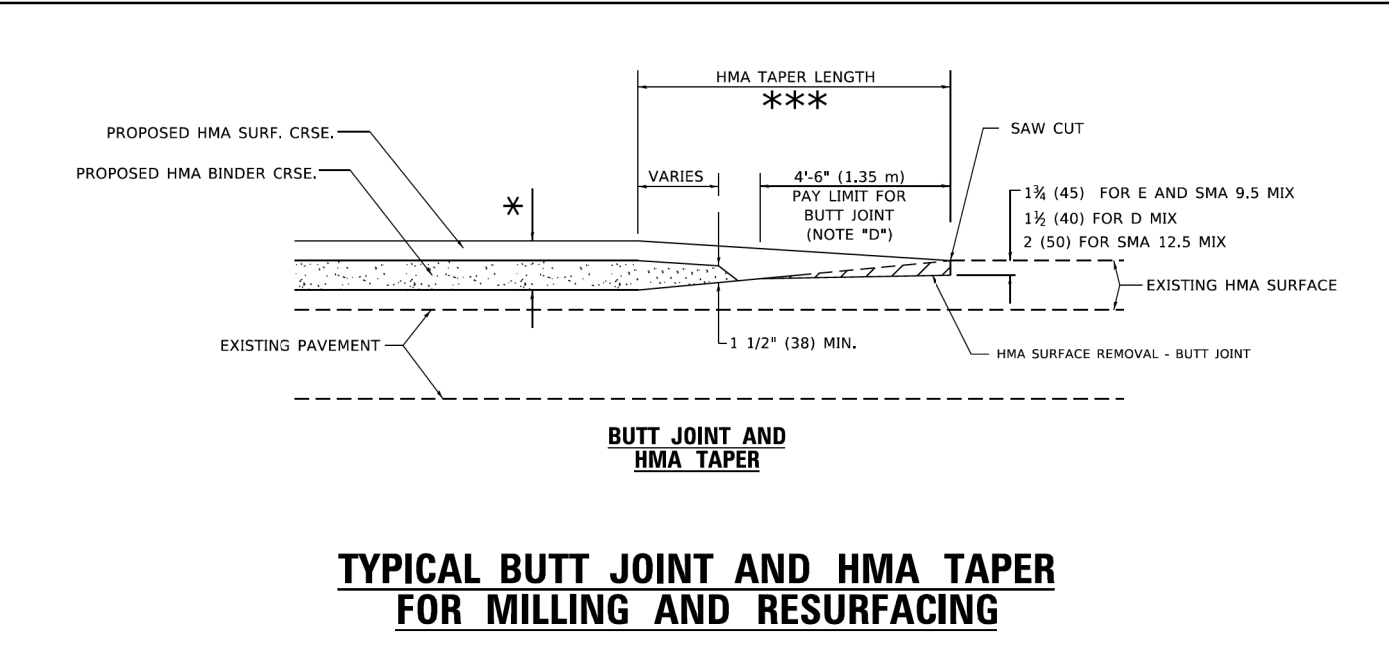
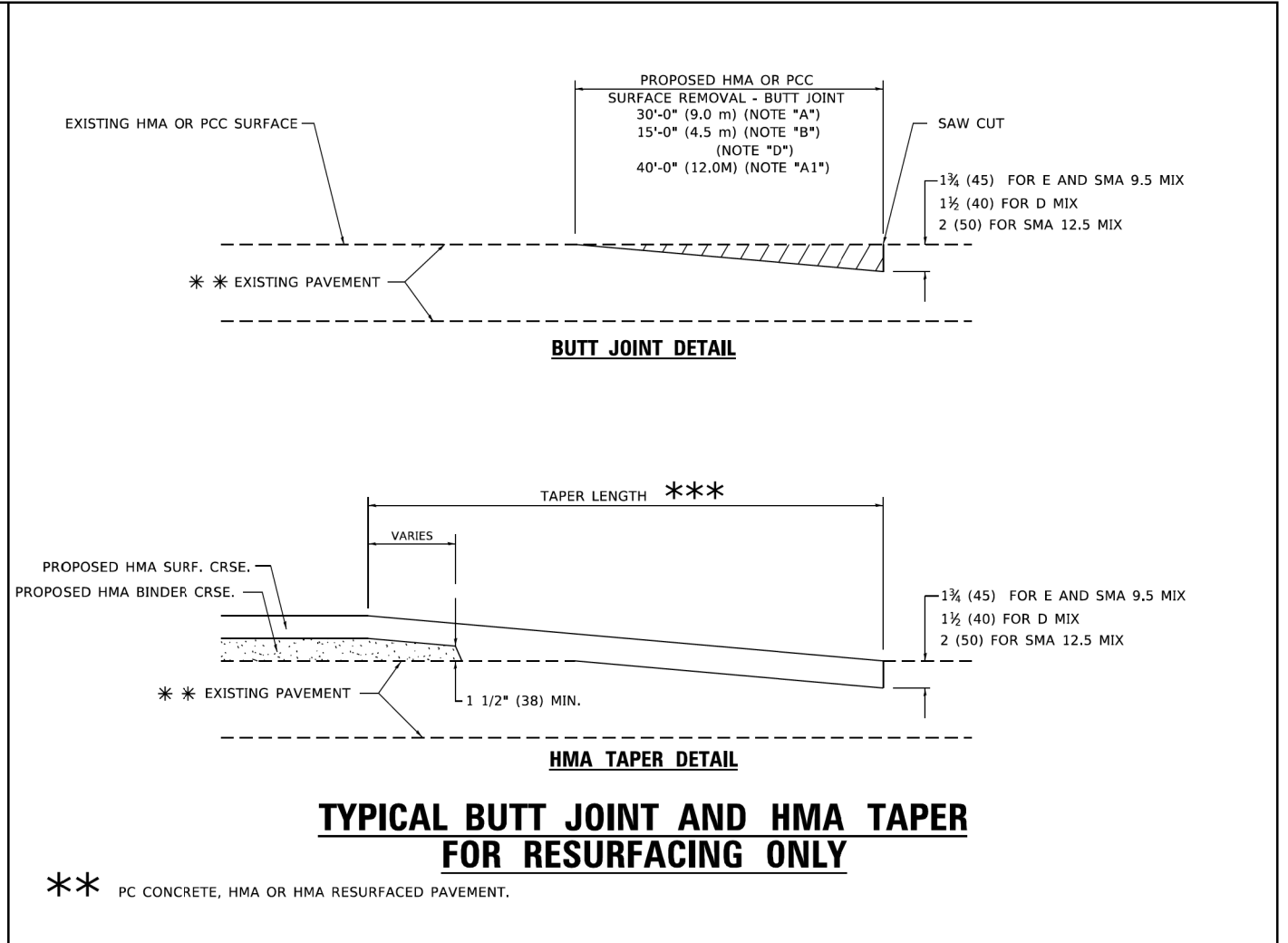
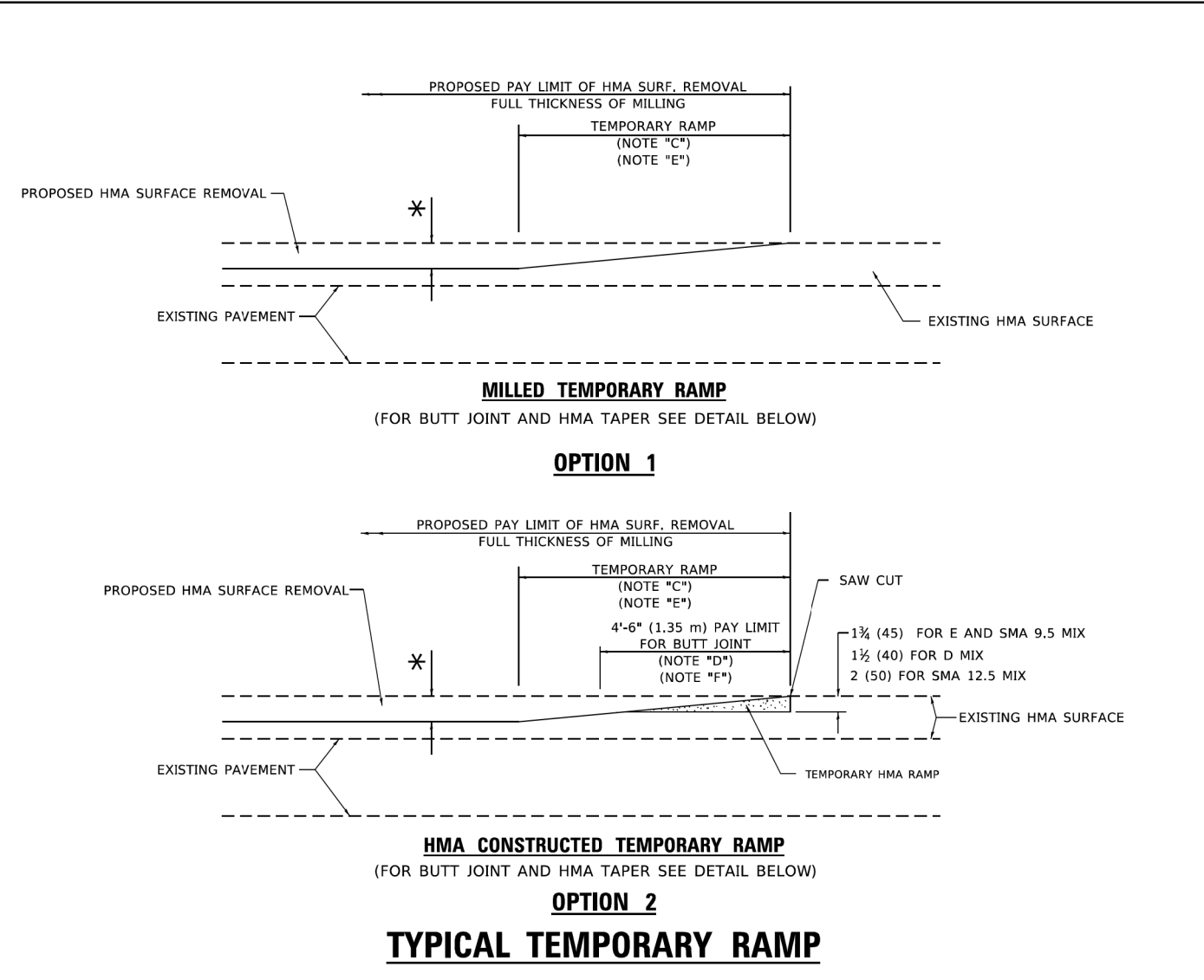
THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME = Lawrence,DeManche DESIGNED - R. SHAH DRAWN - CHECKED - DATE - 10-25-94		REVISED - R. BORO 01-01-07 REVISED - R. BORO 03-09-11 REVISED - R. BORO 12-06-11 REVISED - K. SMITH 11-18-22		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 100.0000' / in. PLOT DATE = 11/18/2022		CHECKED - DATE - 10-25-94		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				HARGER ROAD STP RESURFACING PROJECT IDOT DISTRICT 1 DETAILS				BD600-03 (BD-08)				
PLOT SCALE = 48" PLOT DATE = 2/21/2023		CHECKED - OCG DATE - AUGUST 2022										SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.				ILLINOIS FED. AID PROJECT
USER NAME = dschroeder DRAWN - DJS CHECKED - DATE -		DESIGNED - DJS DRAWN - DJS CHECKED - OCG DATE - AUGUST 2022		STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				HARGER ROAD STP RESURFACING PROJECT IDOT DISTRICT 1 DETAILS				22-00053-00-R5				
PLOT SCALE = 48" PLOT DATE = 2/21/2023		CHECKED - OCG DATE - AUGUST 2022										SCALE: 40' SHEET 1 OF 7 SHEETS STA. N/A TO STA. N/A				ILLINOIS FED. AID PROJECT



- GENERAL NOTES**
- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
 - A1. INTERSTATES
 - B. MINOR SIDE ROADS.
 - C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
 - F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
 - *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 - 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

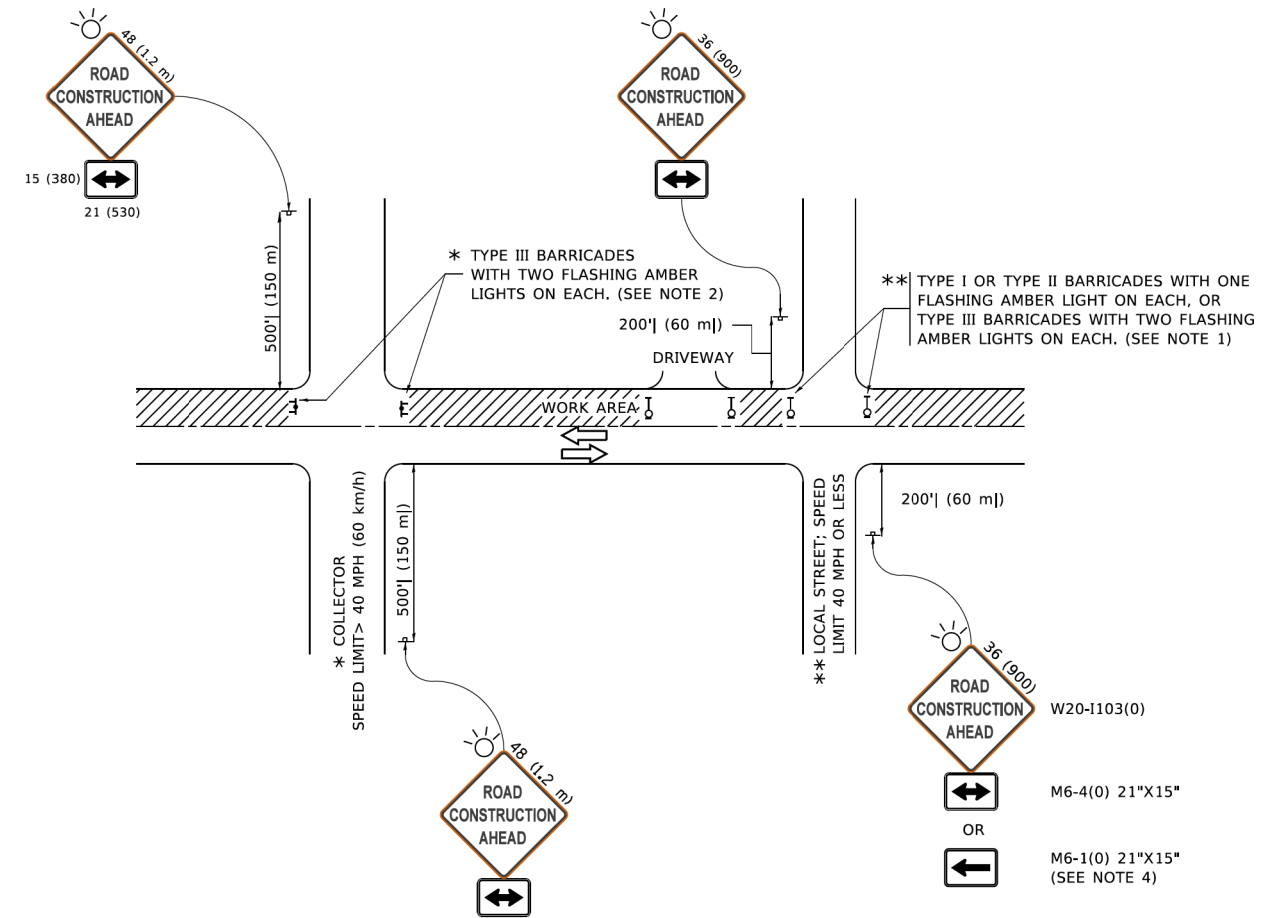
- BASIS OF PAYMENT**
1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = Lawrence,DeManche DESIGNED - M. DE YONG DRAWN - CHECKED - DATE - 06-13-90 REVISIONS: REVISED - A. ABBAS 03-21-97 REVISED - M. GOMEZ 04-06-01 REVISED - R. BORO 01-01-07 REVISED - K. SMITH 11-18-22	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		BUTT JOINT AND HMA TAPER DETAILS		F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.		
	SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.		BD400-05 BD-32 CONTRACT NO. ILLINOIS FED. AID PROJECT				
FILE NAME = N:\OAKBROOK\160597.00044\Civi1\DET2.160597.00044.sht Default	USER NAME = dschroeder DESIGNED - DJJ DRAWN - DJJ CHECKED - OCG DATE - AUGUST 2022	DESIGNED - DJJ DRAWN - DJJ CHECKED - OCG DATE - AUGUST 2022	REVISIONS: REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		HARGER ROAD STP RESURFACING PROJECT IDOT DISTRICT 1 DETAILS	F.A.U. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
SCALE: 40' SHEET 2 OF 7 SHEETS STA. N/A TO STA. N/A				1446 22-00053-00-RS DUPAGE 29 24 CONTRACT NO. 61J22 ILLINOIS FED. AID PROJECT			

MODEL: Default
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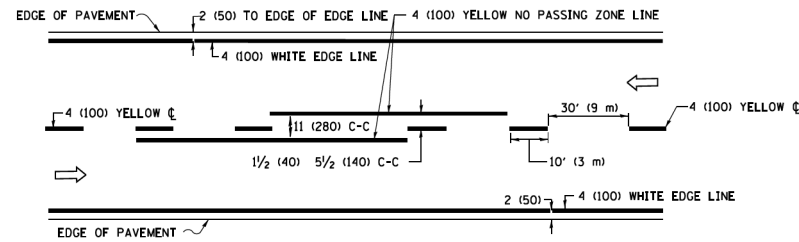


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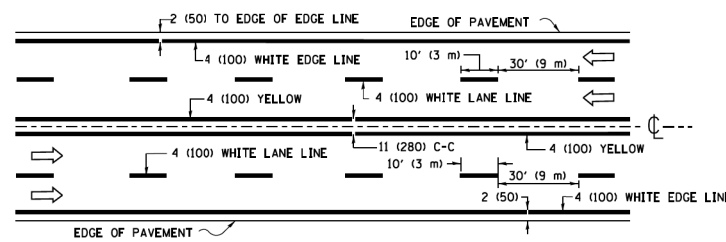
1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

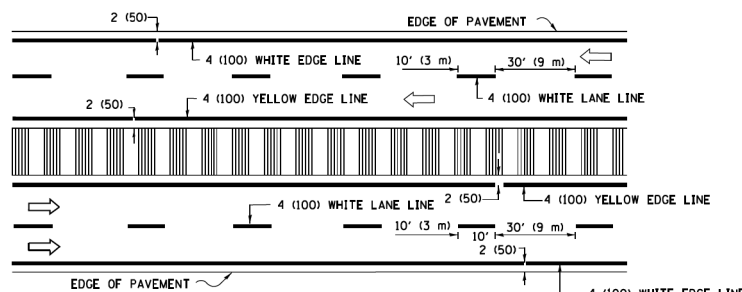
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96					TC-10					
DRAWN -	DRAWN -	REVISED - T. RAMMACHER 01-06-00					CONTRACT NO.					
PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13					ILLINOIS FED. AID PROJECT					
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16					SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.	
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				HARGER ROAD STP RESURFACING PROJECT IDOT DISTRICT 1 DETAILS				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
USER NAME = dschroeder	DESIGNED - DJS	REVISED -					SCALE: 40'		SHEET 3 OF 7 SHEETS		STA. N/A TO STA. N/A	
DRAWN - DJS	DRAWN -	REVISED -					ILLINOIS FED. AID PROJECT					
PLOT SCALE = 48"	CHECKED - OCG	REVISED -					CONTRACT NO. 61J22					
PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -					ILLINOIS FED. AID PROJECT					



2-LANE ROADWAY

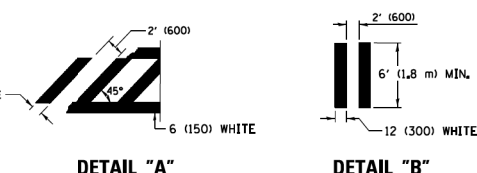
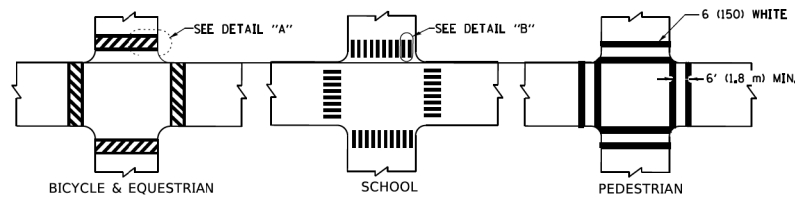


MULTI-LANE UNDIVIDED



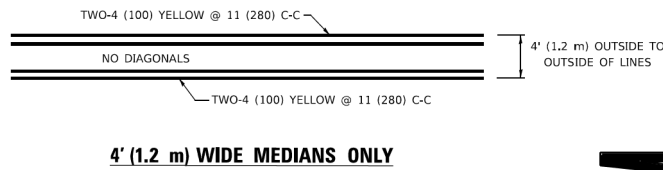
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

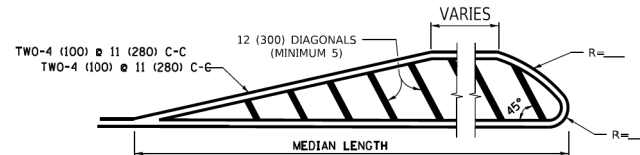


TYPICAL CROSSWALK MARKING

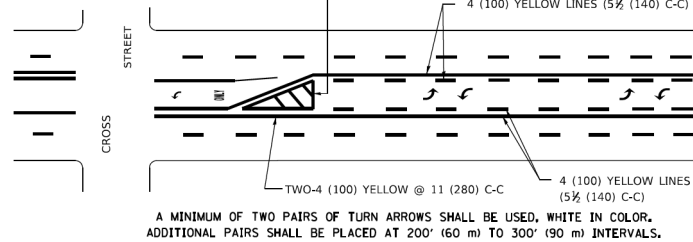
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



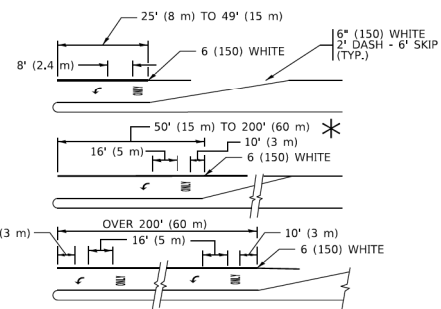
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

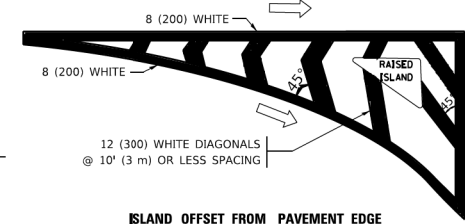


**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

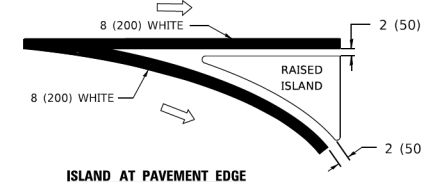


TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

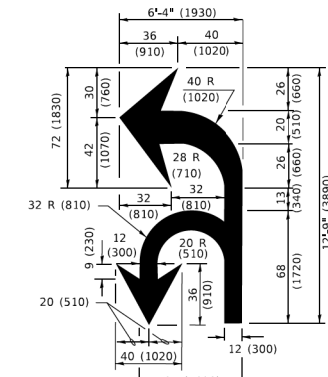


ISLAND OFFSET FROM PAVEMENT EDGE

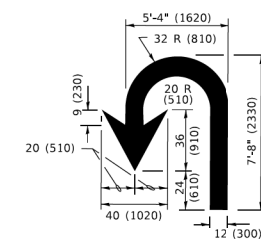


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TC-13			CONTRACT NO.	
ILLINOIS			FED. AID PROJECT	

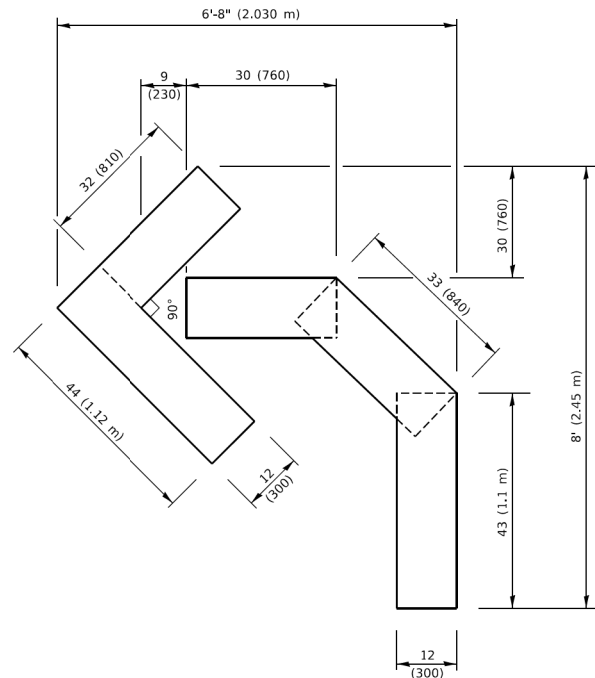
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

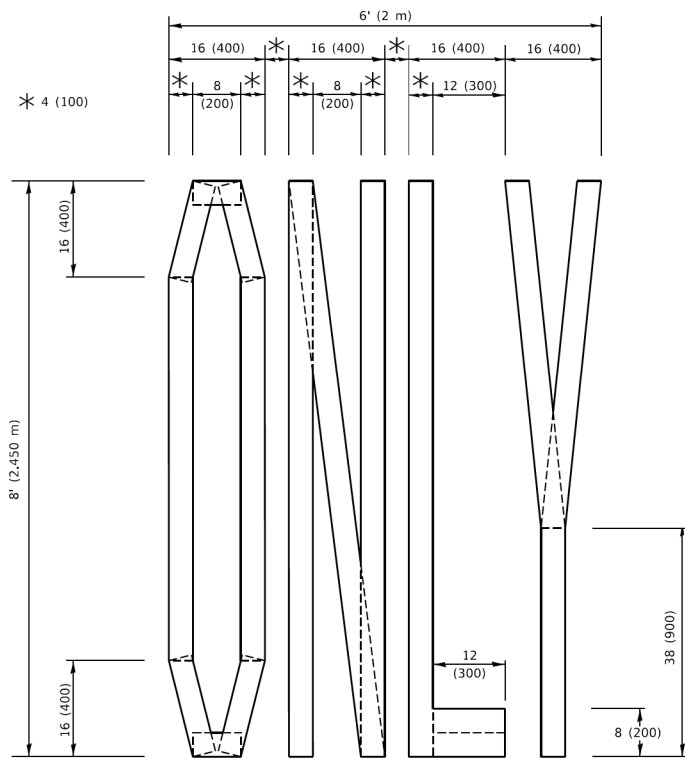
**HARGER ROAD STP RESURFACING PROJECT
IDOT DISTRICT 1 DETAILS**

SCALE: 40'	SHEET 4 OF 7 SHEETS	STA. N/A	TO STA. N/A
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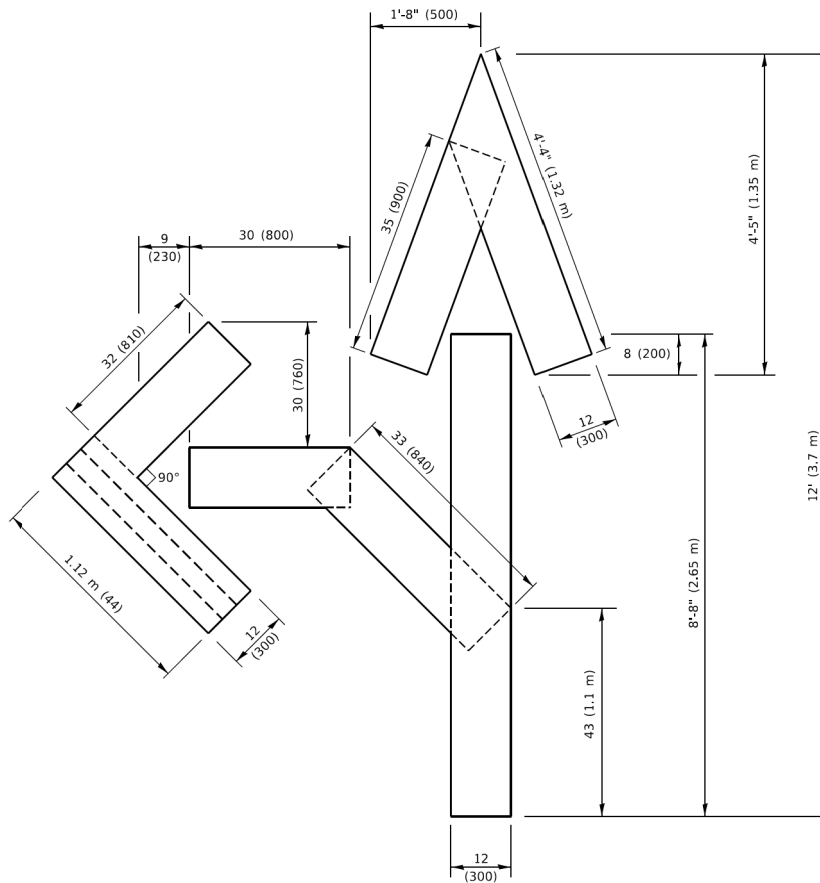
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-RS	DUPAGE	29	26
ILLINOIS			FED. AID PROJECT	
			CONTRACT NO. 61J22	



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

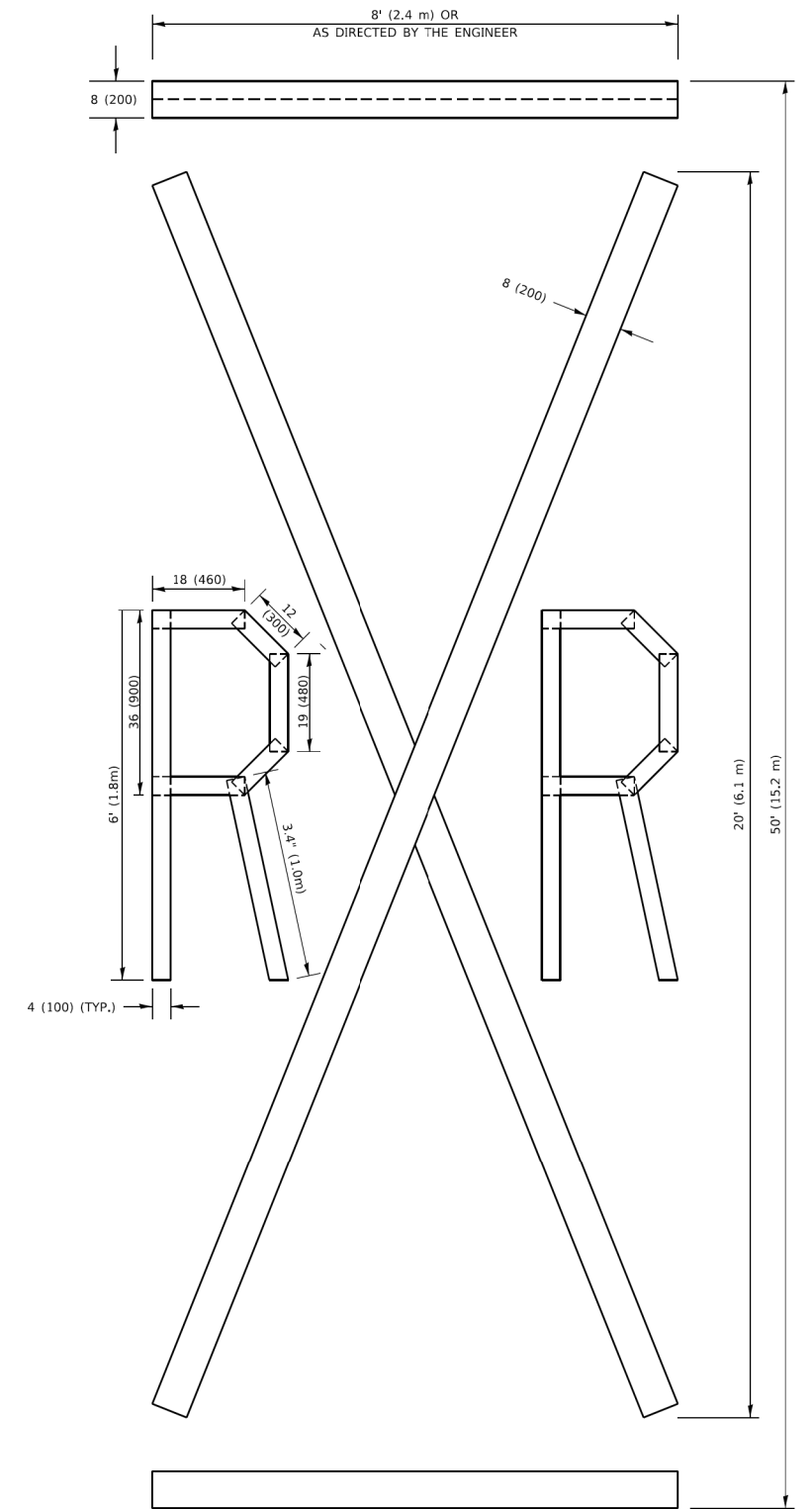


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-16			
ILLINOIS FED. AID PROJECT			CONTRACT NO.	

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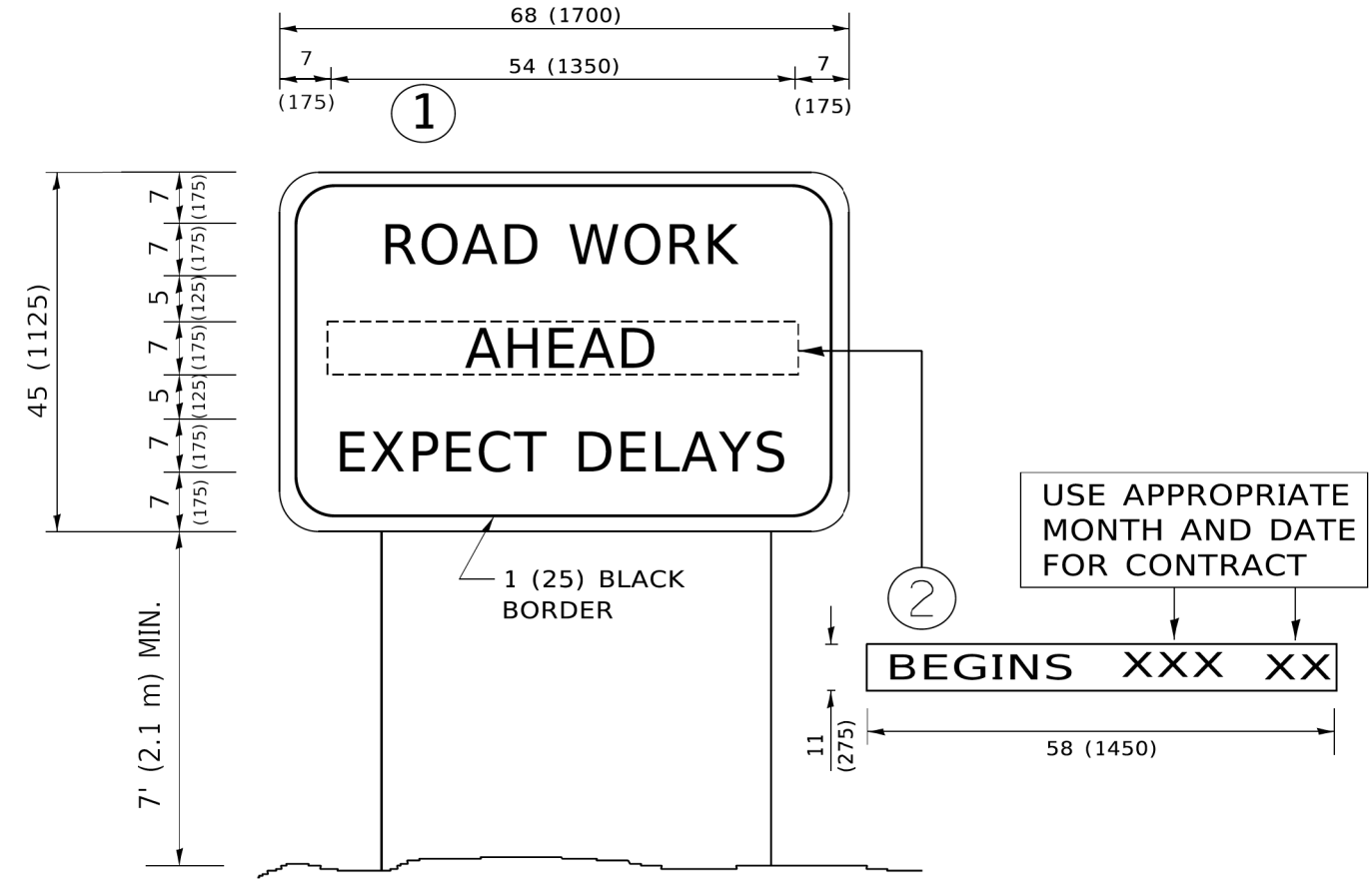
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PLOT SCALE = 48"	CHECKED - OCG	REVISED -
PLOT DATE = 2/21/2023	DATE - AUGUST 2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**HARGER ROAD STP RESURFACING PROJECT
 IDOT DISTRICT 1 DETAILS**

SCALE: 40' SHEET 5 OF 7 SHEETS STA. N/A TO STA. N/A

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-R5	DUPAGE	29	27
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61J22	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

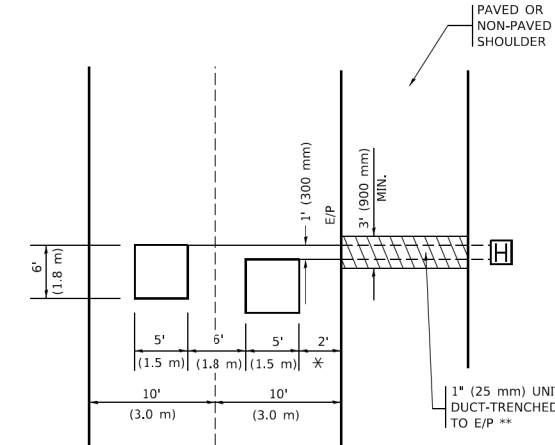
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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				STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	HARGER ROAD STP RESURFACING PROJECT IDOT DISTRICT 1 DETAILS	F.A. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.	1446 22-00053-00-RS DUPAGE 29 28	ILLINOIS FED. AID PROJECT
				SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.	SCALE: 40' SHEET 6 OF 7 SHEETS STA. N/A TO STA. N/A			

LOOPS NEXT TO SHOULDERS

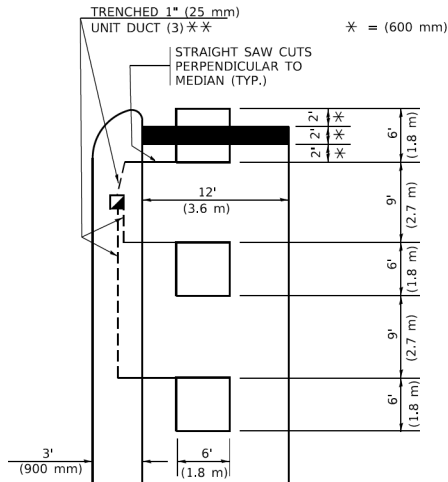
PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



* = (600 mm)
 ** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

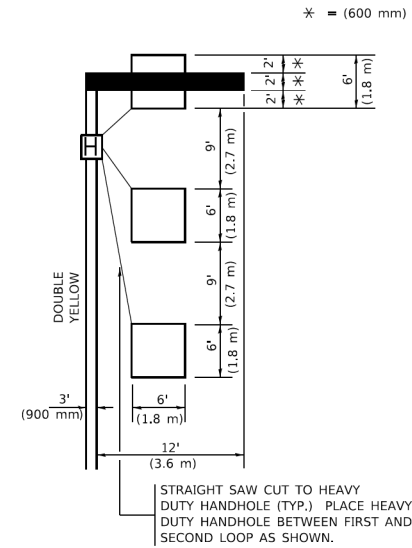
LEFT TURN LANES WITH MEDIANS
 VOLUME DENSITY ("FAR OUT" DETECTION)
 ON SAME APPROACH
 (PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.
 NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
 VOLUME DENSITY ("FAR OUT" DETECTION)
 ON SAME APPROACH
 (PROTECTED / PERMITTED LEFT TURN PHASING)



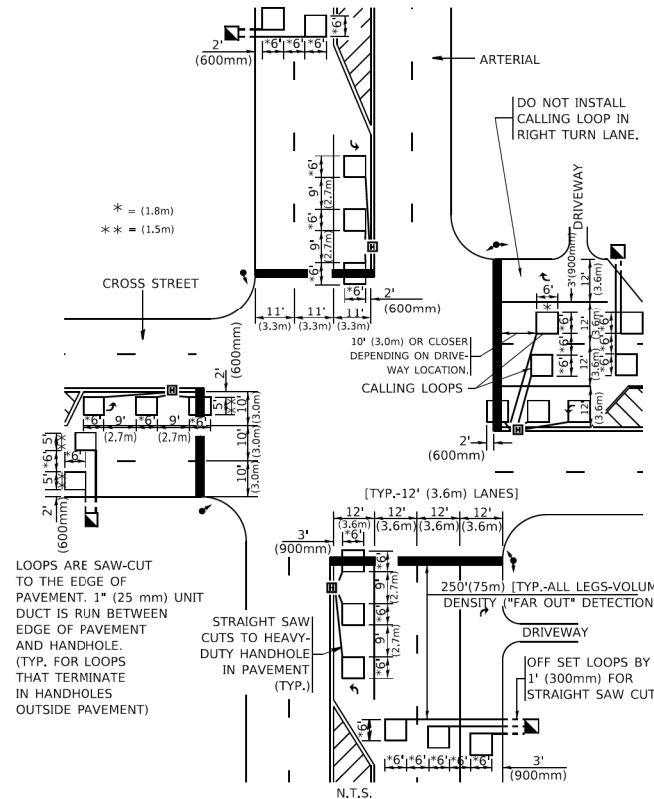
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

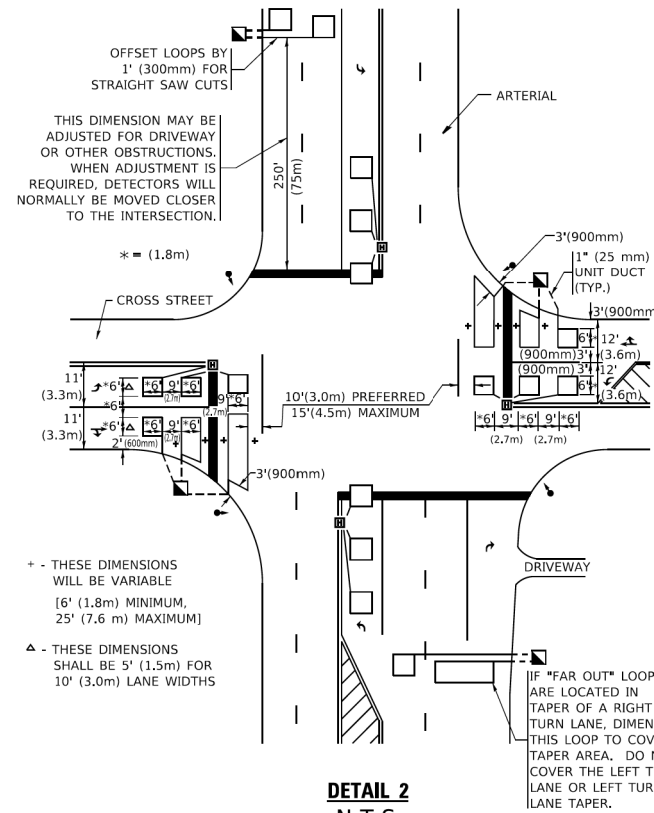
- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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USER NAME = footemj	DESIGNED -	REVISED -
PLOT SCALE = 50.0000' / in.	CHECKED - R.K.F.	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TS-07			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

FILE NAME =	USER NAME = dschroeder	DESIGNED -	REVISED -
N:\OAKBROOK\160597.00044\Civil\DET2.160597.00044.sht		DJS	DJS
Default	PLOT SCALE = 48'	OCG	
	PLOT DATE = 2/21/2023	DATE -	REVISED -
		AUGUST 2022	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HARGER ROAD STP RESURFACING PROJECT
IDOT DISTRICT 1 DETAILS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1446	22-00053-00-R5	DUPAGE	29	29
			CONTRACT NO. 61J22	
ILLINOIS FED. AID PROJECT				

SCALE: 40' SHEET 7 OF 7 SHEETS STA. N/A TO STA. N/A