

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

FAP ROUTE 344: 127TH STREET OVER RAILROADS
(0.5 MILES WEST OF I-57)
SECTION: FAP 344 22 BR
PROJECT: NHPP-5UNY(318)
BRIDGE JOINT REPAIR, BRIDGE DECK SEALING,
BRIDGE REPAIR
COOK COUNTY

C-91-065-23

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP 344 22 BR	COOK	23	1
ILLINOIS CONTRACT NO.			62U49	

* 23 + 5 = 28 TOTAL SHEETS

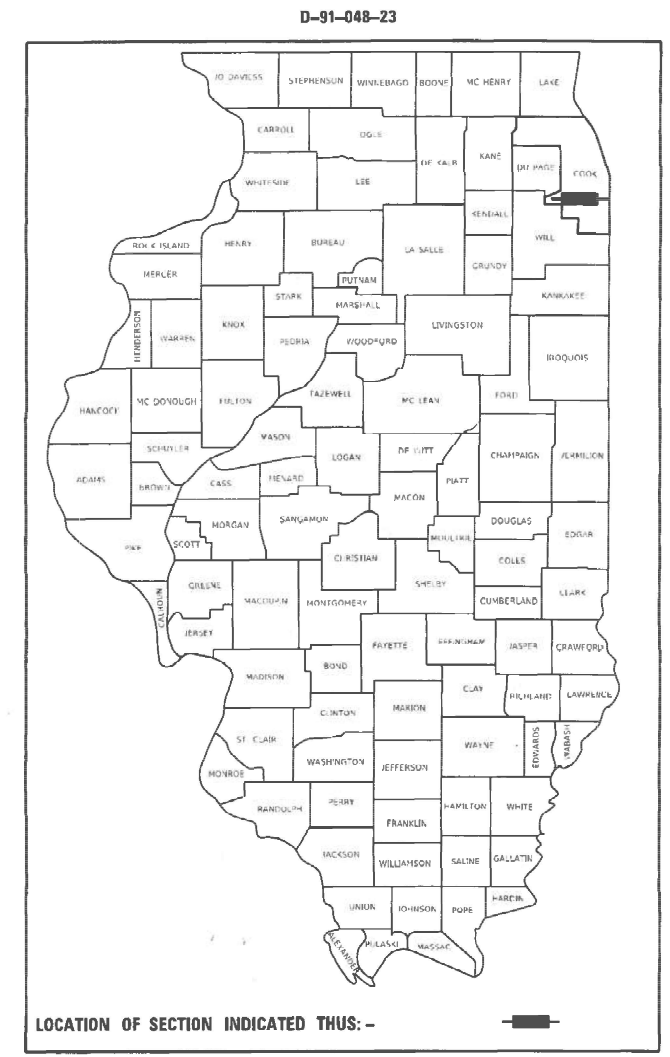
FOR INDEX OF SHEETS AND HIGHWAY STANDARDS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE CITY OF BLUE ISLAND

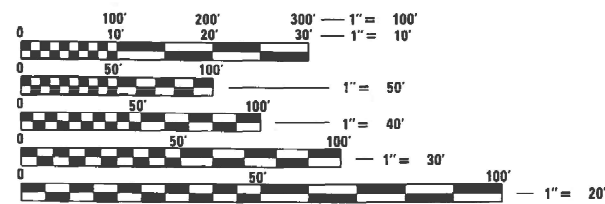
TRAFFIC DATA

2018 ADT = 27,200 VPD
POSTED SPEED LIMIT = 30 MPH

PROJECT LOCATION
127TH ST OVER RAILROADS
STRUCTURE NO. 016-0908



LOCATION OF SECTION INDICATED THUS: -



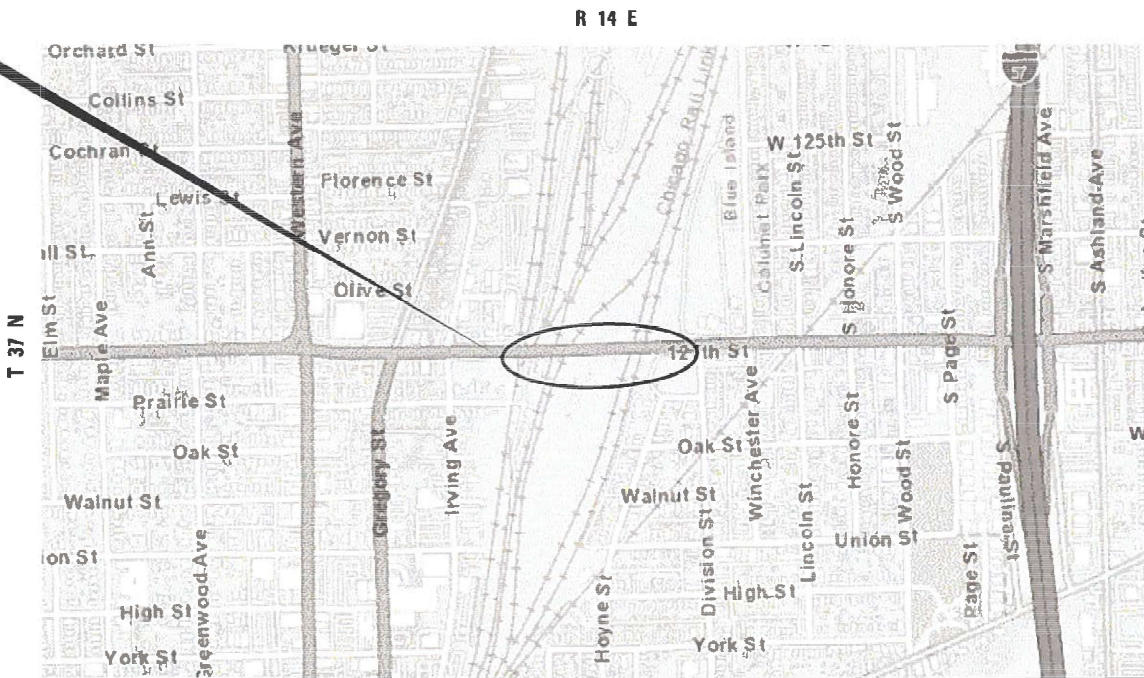
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123 OR 811

MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE
CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-287-7672

PROJECT ENGINEER: LUKASZ POCIECHA (847) 705-4255
PROJECT MANAGER: FAWAD AQUEEL

CONTRACT NO. 62U49



CALUMET TOWNSHIP

GROSS LENGTH = 1,932 FT. = 0.37 MILES
NET LENGTH = 1,932 FT. = 0.37 MILES



Alexander Lane
ALEXANDER CARL LANE, P.E.
IL. LIC. NO. 062-063261
EXP: 11/30/2023
DATE: 1/9/2023

THIS SEAL AND SIGNATURE PERTAINS TO SHEETS 1 TO 9, 19 TO 20, 39 TO 51

INFRASTRUCTURE ENGINEERING | INCORPORATED
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CONTACT: ALEXANDER LANE (312) 477-0620

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 27 20 23
Jose Pina
REGION 1 ENGINEER

March 24, 2023
Steve A. Etk
ENGINEER OF DESIGN AND ENVIRONMENT

March 24, 2023
Stephen M. Smith
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
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8	ROADWAY PLAN
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22	TYPICAL PAVEMENT MARKINGS (TC-13)
22A	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
23	ARTERIAL ROAD INFORMATION SIGN (TC-22)

STATE STANDARDS

STANDARD NO.	DRAWING NAME
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
630001-12	STEEL PLATE BEAM GUARDRAIL
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

- THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.
- MEADE ELECTRIC COMPANY, THE IDOT DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR, LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES. CALL 773-287-7672 FOR THE INITIAL LOCATE. REQUEST FOR LOCATES OF PREVIOUSLY MARKED FACILITIES MAY BE AT THE CONTRACTOR'S EXPENSE.
- IN ADDITION TO FIELD REVIEW AND AERIAL DATA, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE BID PRICE FOR THE WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO BEGINNING CONSTRUCTION AND ORDERING MATERIALS.
- THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
- THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION 11.G.1. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN, AT PATRICE.HARRIS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A USACE PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USACE PERMIT, IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USACE PERMITS. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO SECURE AND COMPLY WITH A USACE PERMIT FOR CONTRACTOR'S ACTIVITIES WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS.
- THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT. ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS-RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTENT)" SHOWN IN PLANS.
- THE CENTERLINE IS FOR INFORMATION ONLY.
- THE CONTRACTOR SHALL NOTIFY AIRLITE TOWING COMPANY, WHO USE THE GROUND UNDER THE BRIDGE FOR VEHICLE STORAGE, AT LEAST TWO WEEKS BEFORE WORK BEGINS, TO MOVE THE VEHICLES IN ORDER TO ACCESS THE WORK ZONE.
- THE CONTRACTOR MUST CONTACT LENNY WILFINGER FROM THE CHICAGO RAIL LINK RAILROAD (CRL) AT (312) 718-6562 OR (773) 530-6151 EXT. 103, PRIOR TO THE START OF CONSTRUCTION TO SCHEDULE A RR FLAGGER FOR ANY WORK TAKING PLACE NEAR PIER 2.

MODEL Path: \\p01\infrastructure\p01\2020\4094-00 IDOT Urban Phase 2 (ITB) 106 Item 161110 33 62U49\CADD\Sheet\62U49-23-GenNotes.dgn
FILE NAME: P:\2020\4094-00 IDOT Urban Phase 2 (ITB) 106 Item 161110 33 62U49\CADD\Sheet\62U49-23-GenNotes.dgn

<p>INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312-425-9598 F 312-425-9594 www.infrastructure-eng.com</p>	USER NAME = Alane PLOT SCALE = 20,0000 * / in. PLOT DATE = 3/17/2023	DESIGNED - CW DRAWN - CW CHECKED - ACL DATE - 03/16/2023	REVISED - REVISED - REVISED -	<p align="center">STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</p>	<p align="center">INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES 127TH STREET OVER RAILROADS</p>	F.A.P. RTE. 344 SECTION FAP 344 22 BR COUNTY COOK TOTAL SHEETS 23 SHEET NO. 2 CONTRACT NO. 62U49	ILLINOIS FED. AID PROJECT
	SCALE:	SHEET OF SHEETS STA. TO STA.					

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	
				80% FEDERAL 20% STATE	100% STATE
50300300	PROTECTIVE COAT	SQ YD	7,850	7,850	
50102400	CONCRETE REMOVAL	CU YD	25.7	25.7	
50157300	PROTECTIVE SHIELD	SQ YD	1,974	1,974	
50300225	CONCRETE STRUCTURES	CU YD	26.5	26.5	
50800515	BAR SPLICERS	EACH	24	24	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	3,070	3,070	
52000600	FABRIC REINFORCED ELASTOMERIC TROUGH	FOOT	66	66	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	131	131	
67100100	MOBILIZATION	L SUM	1	1	
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	10	10	
X7010238	CHANGEABLE MESSAGE SIGN (SPECIAL)	CAL MO	3	3	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	677	677	
70307100	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE	SQ FT	73	73	
70307120	TEMPORARY PAVEMENT MARKING - LINE 4"- TYPE IV TAPE	FOOT	6,212	6,212	
70307130	TEMPORARY PAVEMENT MARKING - LINE 6"- TYPE IV TAPE	FOOT	260	260	
70307160	TEMPORARY PAVEMENT MARKING - LINE 12"- TYPE IV TAPE	FOOT	88	88	
70306120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE III TAPE	FOOT	387	387	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,212.5	1,212.5	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1,207	1,207	
70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	2	2	
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	73	73	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	6,212	6,212	
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	260	260	
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	88	88	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	234	234	

* = SPECIALTY ITEM

PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				URBAN	
				80% FEDERAL 20% STATE	100% STATE
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	234	234	
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	2,340	2,340	
X0325749	FIBER WRAP	SQ FT	225	225	
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	131	131	
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	500		500
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12	
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1	
Z0007122	REMOVING AND RE-ERECTING EXISTING RAILING	FOOT	9	9	
Z0010605	CLEANING DRAINAGE SYSTEM	L SUM	1		1
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	220	220	
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	60	60	
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52	
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1	
Z0073200	TEMPORARY SHORING AND CRIBBING	EACH	2	2	

MODEL: Default
FILE NAME: P:\2023\4694-30 IDOT Various Phase 2 (ITB) 196 Item 16\WD 33 62U49DCHCADD Sheets\62U49-ITB-500-01.dgn

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USER NAME = ALane	DESIGNED - CW	REVISED -
PLOT SCALE = 20.0000' / in.	DRAWN - CW	REVISED -
PLOT DATE = 1/27/2023	CHECKED - ACL	REVISED -
	DATE - 01/27/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
127TH STREET OVER RAILROADS**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP 344 22 BR	COOK	23	3
			CONTRACT NO. 62U49	
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES

- ALL MAINTENANCE OF TRAFFIC (MOT) LANE CLOSURES CAN BE COMPLETED WITH IDOT HIGHWAY STANDARDS. THE MOT PLANS SHALL SERVE AS A GUIDE FOR SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. HOWEVER, THE CONTRACTOR MAY IMPROVE OR MODIFY THE MOT PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE MOT PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
- THE CONTRACTOR SHALL CONTACT THE IDOT DISTRICT 1 ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE WEST 127TH STREET WORK ZONE SPEED LIMIT SHALL BE 30 MPH.
- EXISTING REFLECTORS IN RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH STAGED TRAFFIC PATTERNS SHALL BE REMOVED UNDER THIS CONTRACT. THESE SHALL BE REPLACED WITHIN THE LIMITS OF THIS CONTRACT'S MAINTENANCE OF TRAFFIC, ALONG WITH ANY OTHER MARKERS THAT WERE MISSING REFLECTORS PRIOR TO REOPENING THE LANES TO TRAFFIC.
- ALL SIGNS MUST BE POST MOUNTED, IN THE GROUND WHEREVER POSSIBLE, AND WITH A 7 FEET CLEARANCE WHICH IS MEASURED FROM THE TOP OF PAVEMENT TO THE BOTTOM OF THE SIGN.
- CONTRACTOR SHALL REMOVE OR COVER ALL W21-1 (WORKERS PRESENT) AND W20-7 (FLAGGER) SIGNS, WITHIN AN HOUR, WHEN WORKERS ARE NOT PRESENT OR IF THE SIGN IS NOT APPLICABLE.

127TH STREET CONSTRUCTION STAGING

MAINTENANCE OF TRAFFIC - STAGE 1

CONSTRUCTION:

- COMPLETE BRIDGE DECK AND JOINT REPAIRS OF THE SOUTH SIDE (EASTBOUND DIRECTION) OF THE BRIDGE.

MAINTENANCE OF TRAFFIC:

- CLOSE THE EASTBOUND LANES OF TRAFFIC AND SHIFT TRAFFIC TO THE WESTBOUND LANES. 1 LANE IN EACH DIRECTION SHALL BE MAINTAINED.

MAINTENANCE OF TRAFFIC - STAGE 2

CONSTRUCTION:

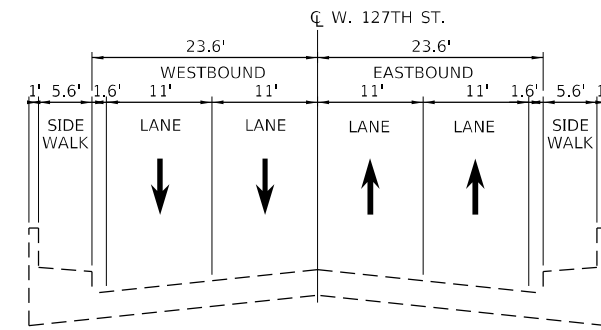
- COMPLETE BRIDGE DECK AND JOINT REPAIRS AND OVERLAY OF THE NORTH SIDE (WESTBOUND DIRECTION) OF THE BRIDGE.

MAINTENANCE OF TRAFFIC:

- CLOSE THE WESTBOUND LANES OF TRAFFIC AND SHIFT TRAFFIC TO THE EASTBOUND LANES. 1 LANE IN EACH DIRECTION SHALL BE MAINTAINED.

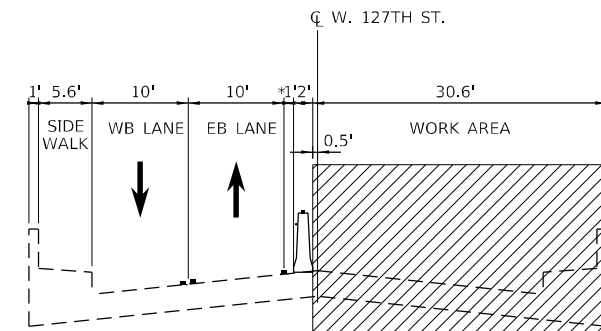
WORK ZONE SPEED LIMIT:

	EXISTING POSTED SPEED	PROPOSED POSTED SPEED
STAGE 1	30 MPH	30 MPH
STAGE 2	30 MPH	30 MPH



EXISTING TYPICAL CROSS SECTION

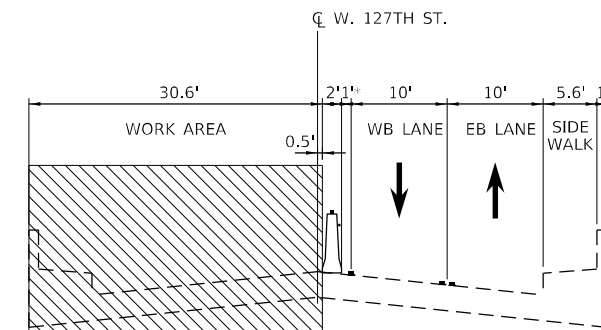
ON SN-016-0908



STAGE 1 CONSTRUCTION

ON SN-016-0908

*BUFFER REDUCED WITH 8:1 BARRIER TAPER AT RECONSTRUCTED JOINT







STAGE 2 CONSTRUCTION

ON SN-016-0908

*BUFFER REDUCED WITH 8:1 BARRIER TAPER AT RECONSTRUCTED JOINT

MOT TYPICAL LEGEND:

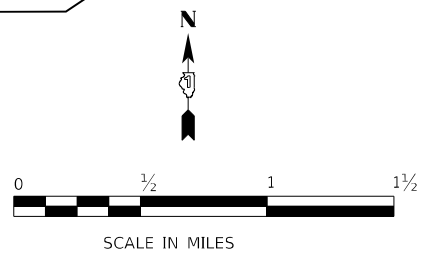
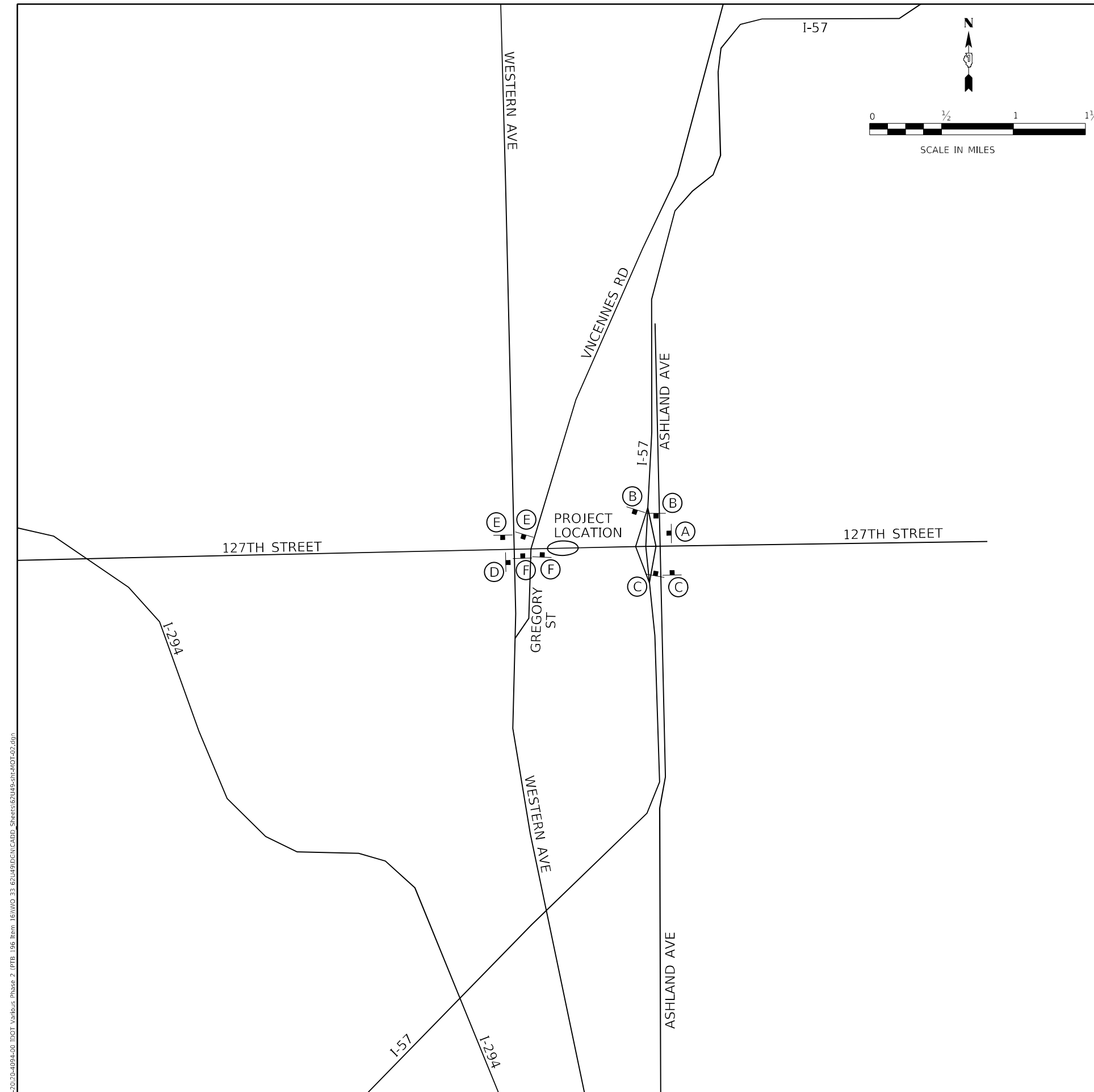
-  WORK AREA
-  DIRECTION OF TRAVEL
-  TEMPORARY CONCRETE BARRIER WITH TYPE C, BI-DIRECTIONAL CRYSTAL REFLECTORS PER STD 704001 AND 782006.
-  TEMPORARY PAVEMENT MARKING

MODEL Path: \\... INFRASTRUCTURE ENGINEERING INCORPORATED

USER NAME = Alane	DESIGNED - CW	REVISED -
DRAWN - CW	REVISIONS -	
PLOT SCALE = 20,0000 * / in.	CHECKED - ACL	REVISED -
PLOT DATE = 3/14/2023	DATE - 02/15/2023	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP 344 22 BR	COOK	23	4
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				

MODEL Path: \\... \PUB\2023\4094-00_IDOT_VnHous_Phase 2 (ITB)_106 Item: 161NVD_33_62U49\DC\CADD_Sheets\62U49-48-Plan\T-02.dgn



(A)

MAX WIDTH
8' - 6"
0.3 MILES AHEAD

W12-1103-4848

(E)

← 8'-6" →
←

W12-1102(O)-48
M6-1L(O)-2115

(B)

MAX WIDTH
8' - 6"
0.3 MILES AHEAD
WEST
127th Street
→

W12-1103-4848
M3-4(O)-2412
SPECIAL 24"x18"
6" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND
M6-1R(O)-2115

(F)

← 8'-6" →
→

W12-1102(O)-48
M6-1R(O)-2115

(C)

MAX WIDTH
8' - 6"
0.3 MILES AHEAD
WEST
127th Street
←

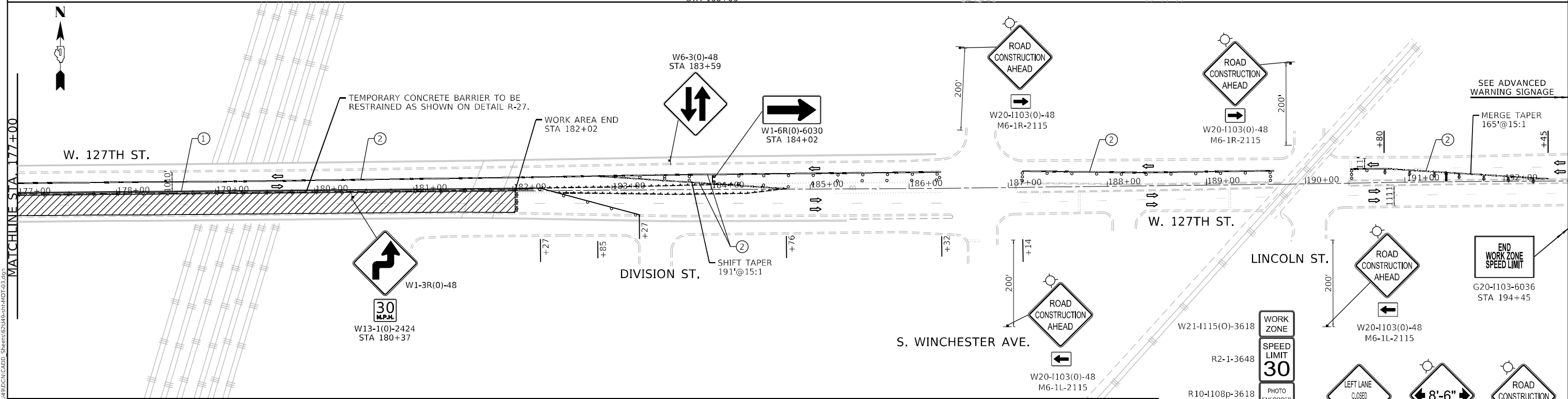
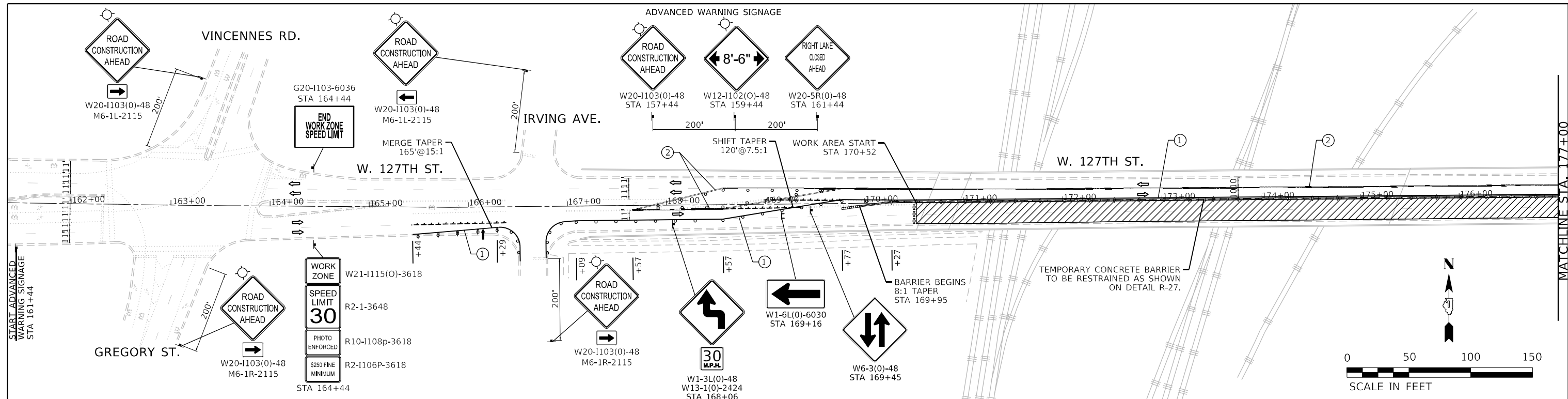
W12-1103-4848
M3-4(O)-2412
SPECIAL 24"x18"
6" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND
M6-1L(O)-2115

(D)

MAX WIDTH
8' - 6"
0.2 MILES AHEAD

W12-1103-4848

- NOTES**
1. PLACE SIGNS 500 FEET BEFORE THE INTERSECTIONS OR 300 FEET THE RAMP GORES / EXITS.
 2. ENGINEER MAY ADJUST THE SIGN LOCATION BASED ON FIELD CONDITIONS.
 3. THE WIDTH RESTRICTION SIGNING IS REQUIRED WHEN THE STAGED CONSTRUCTION IS IN EFFECT.



MOT SYMBOL LEGEND		MOT PROPOSED LEGEND	
	CONE, DRUM OR BARRICADE		① TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (DOUBLE YELLOW) (70300904)
	WORK AREA		② TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE) (70300904)
	TYPE III BARRICADE		
	DIRECTION OF TRAFFIC		
	SIGN ON PORTABLE OR PERMANENT SUPPORT		
	ARROW BOARD		
	BARRICADE OR DRUM WITH FLASHING LIGHT		
	DIRECTION INDICATOR BARRICADE		
	TEMPORARY CONCRETE BARRIER		
	IMPACT ATTENUATOR		
	CRYSTAL, BIDIRECTIONAL BARRIER WALL REFLECTOR		
	PAVEMENT MARKING REMOVAL		

NOTE: TRAFFIC CONTROL BASED ON STANDARD 701611-01.
REMOVE CONFLICTING PAVEMENT MARKING USING PAVEMENT MARKING REMOVAL - WATERBLASTING

INFRASTRUCTURE ENGINEERING INCORPORATED
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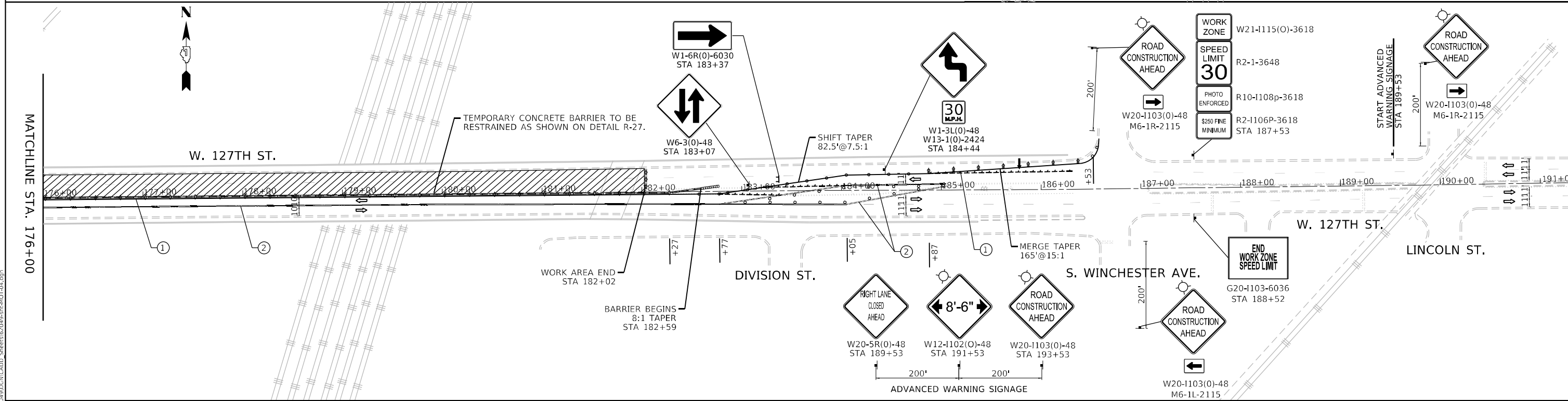
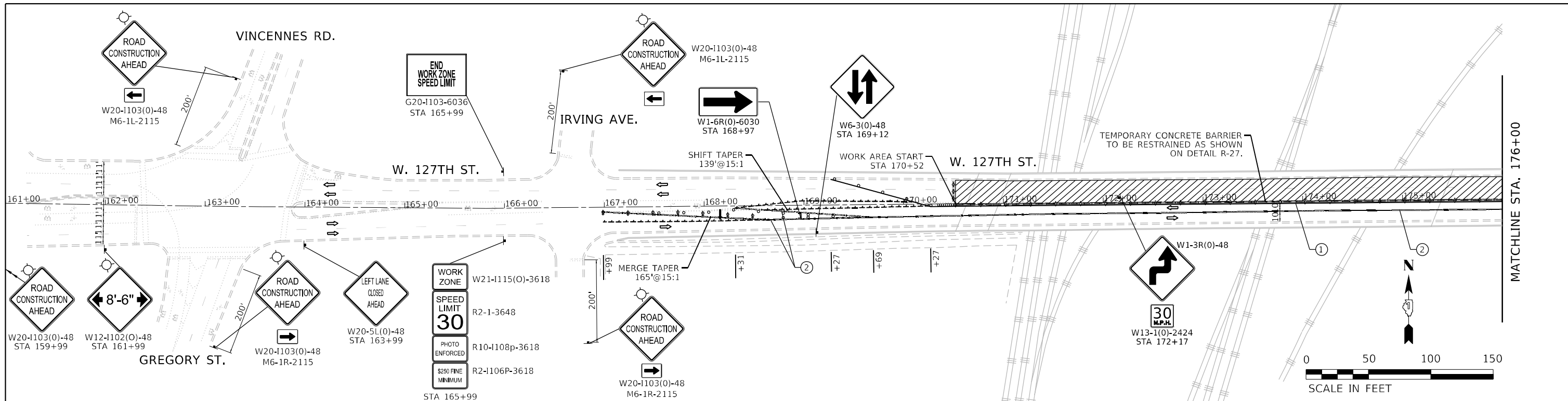
USER NAME = Alane	DESIGNED - CW	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN - CW	REVISED -
PLOT DATE = 3/16/2023	CHECKED - ACL	REVISED -
	DATE - 02/15/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TRAFFIC CONTROL PLANS STAGE 1			
SCALE: 1" = 50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE. 344	SECTION FAP 344 22 BR	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 6
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				

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- MOT SYMBOL LEGEND**
- CONE, DRUM OR BARRICADE
 - ▭ WORK AREA
 - ▭ TYPE III BARRICADE
 - ↔ DIRECTION OF TRAFFIC
 - ⊥ SIGN ON PORTABLE OR PERMANENT SUPPORT
 - ⇄ ARROW BOARD
 - ⊙ BARRICADE OR DRUM WITH FLASHING LIGHT
 - ⊙ DIRECTION INDICATOR BARRICADE
 - ▬ TEMPORARY CONCRETE BARRIER
 - ▬ IMPACT ATTENUATOR
 - ◇ CRYSTAL, BIDIRECTIONAL BARRIER WALL REFLECTOR
 - PAVEMENT MARKING REMOVAL

- MOT PROPOSED LEGEND**
- ① TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (DOUBLE YELLOW) (70300904)
 - ② TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE) (70300904)

NOTE: TRAFFIC CONTROL BASED ON STANDARD 701611-01.
REMOVE CONFLICTING PAVEMENT MARKING USING PAVEMENT MARKING REMOVAL - WATERBLASTING

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DRAWN - CW
CHECKED - ACL
DATE - 02/15/2023

PLOT SCALE = 100,0000' / in.
PLOT DATE = 3/16/2023

DESIGNED - CW	REVISD -
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CHECKED - ACL	REVISD -
DATE - 02/15/2023	

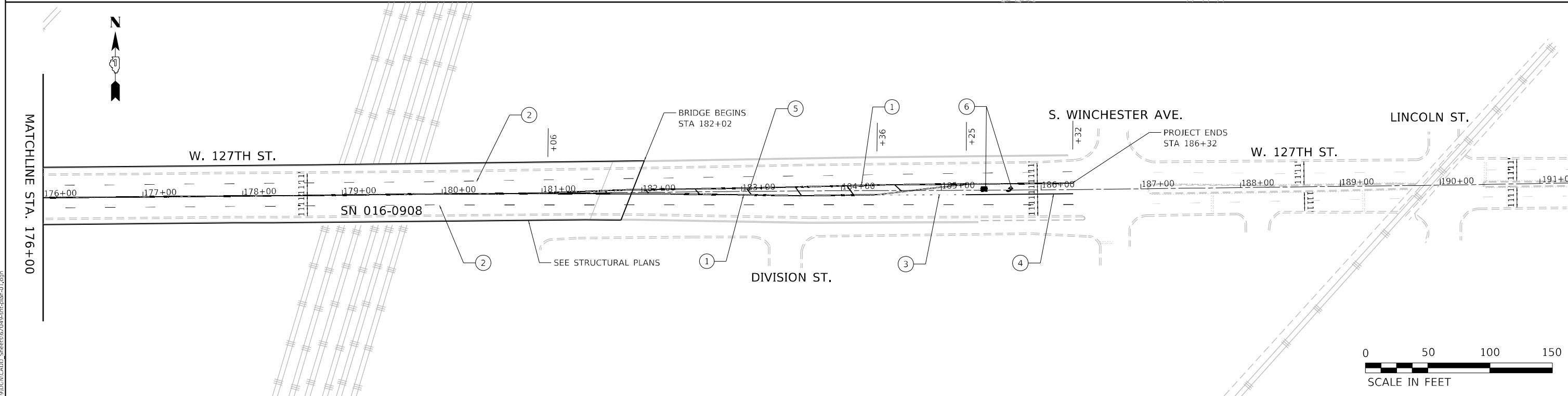
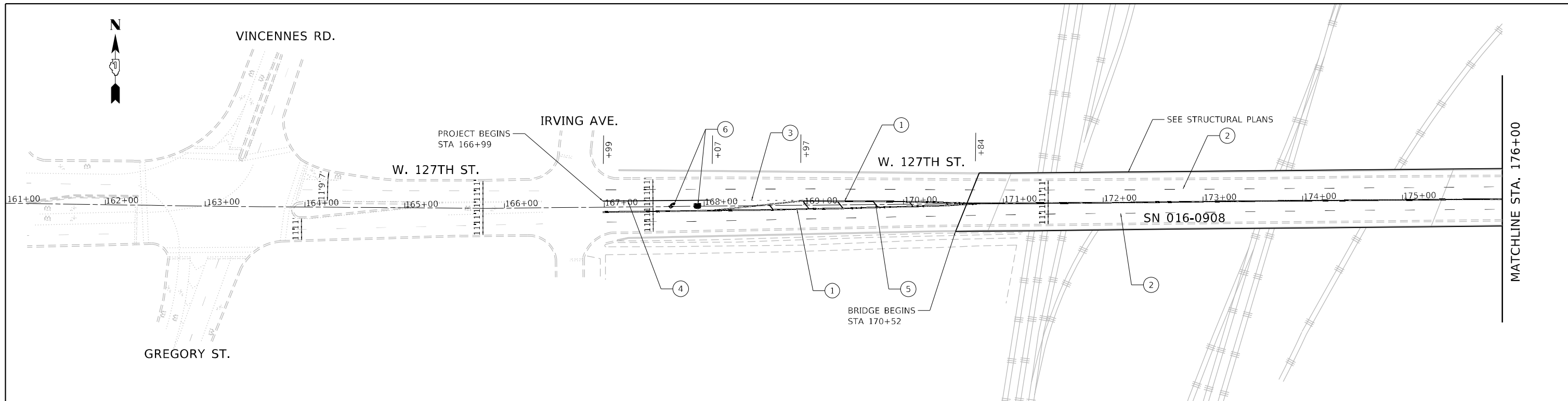
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLANS
STAGE 2**

SCALE: 1" = 50'
SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 344	SECTION FAP 344 22 BR	COUNTY COOK	TOTAL SHEETS 23	SHEET NO. 7
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				

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PAVEMENT MARKING LEGEND

- ① LINE 2 @ 4" SOLID YELLOW, 11" C-C - CENTERLINE (TYP)
- ② LINE 4", SKIP-DASH, WHITE (10' LINE - 30' SPACE) - LANE LINES
- ③ LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES (TYP)
- ④ LINE 6", SOLID WHITE - TURN LANE MARKING (TYP)
- ⑤ LINE 12", SOLID, YELLOW - PAINTED MEDIANS DIAGONALS (TYP)
(SEE DISTRICT DETAIL TC-13 FOR SPACING)
- ⑥ LETTERS & SYMBOLS, SOLID, WHITE - TURN LANE MARKINGS (TYP)

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 FILE NAME: P:\d\p\11...
 MODEL: P:\d\p\11...
 FILE NAME: P:\d\p\11...

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 PLOT DATE = 1/27/2023

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 DATE - 01/27/2023

REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS
 127TH STREET OVER RAILROADS**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP 344 22 BR	COOK	23	8
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				

Benchmark: Disk set at southeast corner of 119th Street and Ashland Ave., Elev. 601.07

Existing Structure: Structure No. 016-0908, originally built in 1939 under Sec. 180-A-0101.1-15D-BR. 13 span, 1830 built-up steel girders supported on column bent piers and closed abutments with pile foundations. Existing girders made composite with deck in a 1957 rehabilitation. Outer 3 spans on each side were filled and supported with retaining wall systems in 2003 rehab. 1058'-9³/₈" back-back of abutments with a total out-out width of 60'-4¹/₈"

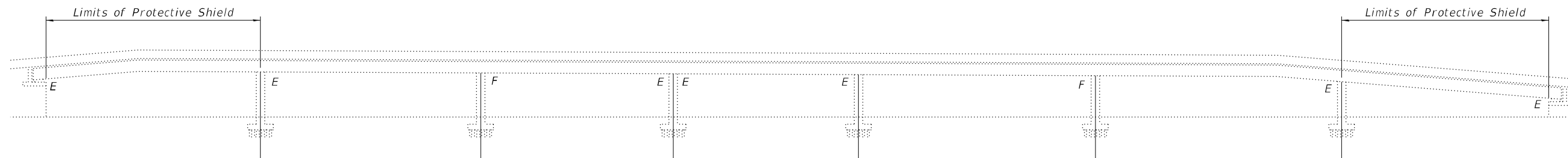
Traffic will be maintained using Stage Construction.

No salvage.

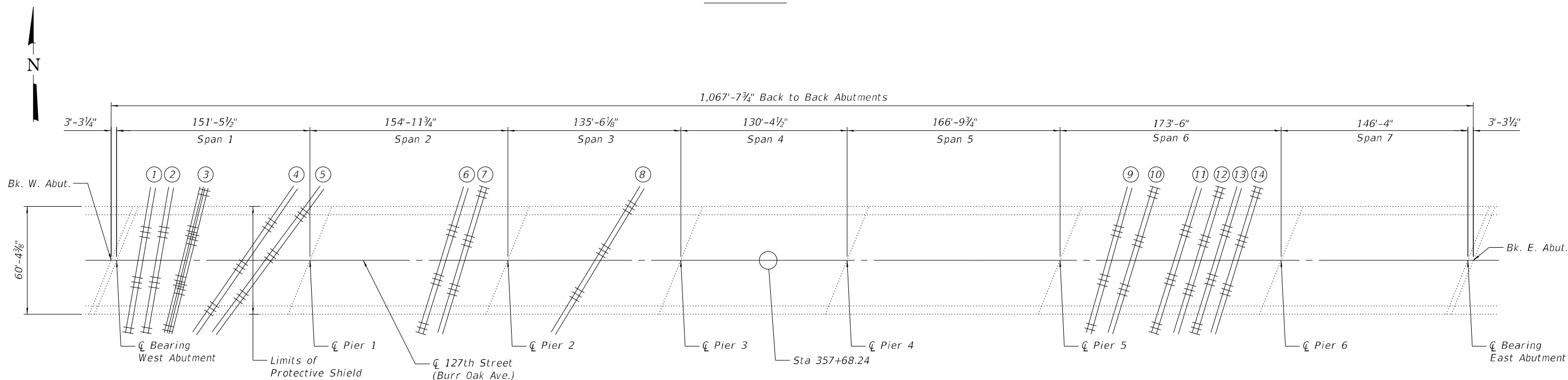


Kenneth P. Smorynski

License No. 081-005763
Expires 11/30/2024



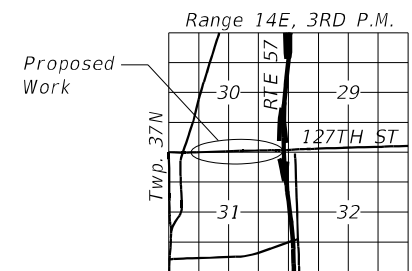
ELEVATION



PLAN

① Denotes railroad track number

Note: Tracks 1-3 are Metra Railroad tracks.
Tracks 4-14 are Iowa Interstate Railroad tracks.



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
127TH ST. (BURR OAK AVE.)
OVER METRA/IOWA INTERSTATE RAILROAD
F.A.P. RTE 344 SECTION FAP-344-22-BR
COOK COUNTY
STATION 357+68.24
STRUCTURE NUMBER 016-0908

MODEL: Default
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PLOT DATE = 3/23/2023	DRAWN - TY	REVISED -
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SHEET 1 OF 13 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP-344-22-BR	COOK	23	9
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

All expansion joint structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

If the Contractor elects to use cantilever forming brackets on the exterior girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06(b) of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior girder at each of these additional bracket locations.

INDEX OF SHEETS

1. General Plan And Elevation
2. General Notes, Scope Of Work, & Bill Of Materials
3. Stage Construction Details
4. Temporary Concrete Barrier For Stage Construction
5. Deck Repair Plan
6. Abutment and Deck Concrete Removal
- 6A. Abutment and Deck Concrete Removal Sections
7. Abutment and Deck Alterations
8. Abutment and Deck Details
9. Preformed Joint Strip Seal - Sidewalk (Sheet 1 of 2)
- 9A. Preformed Joint Strip Seal - Sidewalk (Sheet 2 of 2)
10. Trough Replacement at Pier 3
11. Pier 2 Repairs
12. Pier 3 Repairs
13. Bar Splicer Assembly And Mechanical Splicer Details

SCOPE OF WORK

1. Remove and replace trough, plates, and nuts and bolts on Pier 3.
2. Repair structural concrete on Piers 2 and 3. Install shoring and cribbing to support Girders 8 and 9 on the east side of Pier 2. Fiber wrap column in Pier 2.
3. Remove neoprene expansion joints at abutments.
4. Install preformed joint strip seal at abutments.
5. Apply protective coat to deck, abutments, sidewalks and parapets.
6. Clean and reseal relief joints.
7. Clean deck drainage system.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	25.7		25.7
Protective Shield	Sq Yd	1,974		1,974
Concrete Superstructure	Cu Yd	25.7		25.7
* Protective Coat	Sq Yd	7,850		7,850
Reinforcement Bars, Epoxy Coated	Pound	3,070		3,070
Bar Splicers	Each	24		24
Preformed Joint Strip Seal	Foot	131		131
Fabric Reinforced Elastomeric Trough	Foot		66	66
Fiber Wrap	Sq Ft		225	225
Clean & Reseal Relief Joint	Foot	131		131
Cleaning Drainage System	L Sum	1		1
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft		220	220
Structural Repair Of Concrete (Depth Greater Than 5 Inches)	Sq Ft		60	60
Railroad Protective Liability Insurance	L Sum			1
Temporary Shoring and Cribbing	Each	2		2

* Quantity is for the top of deck, sidewalks and parapets and the roadway face of sidewalks and parapets. Quantity is also for the top of new concrete on both abutment backwalls.

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway Bridges (17th Edition)

LOADING HS20-44

Allow 50 psf for future wearing surface

DESIGN STRESSES

New Construction

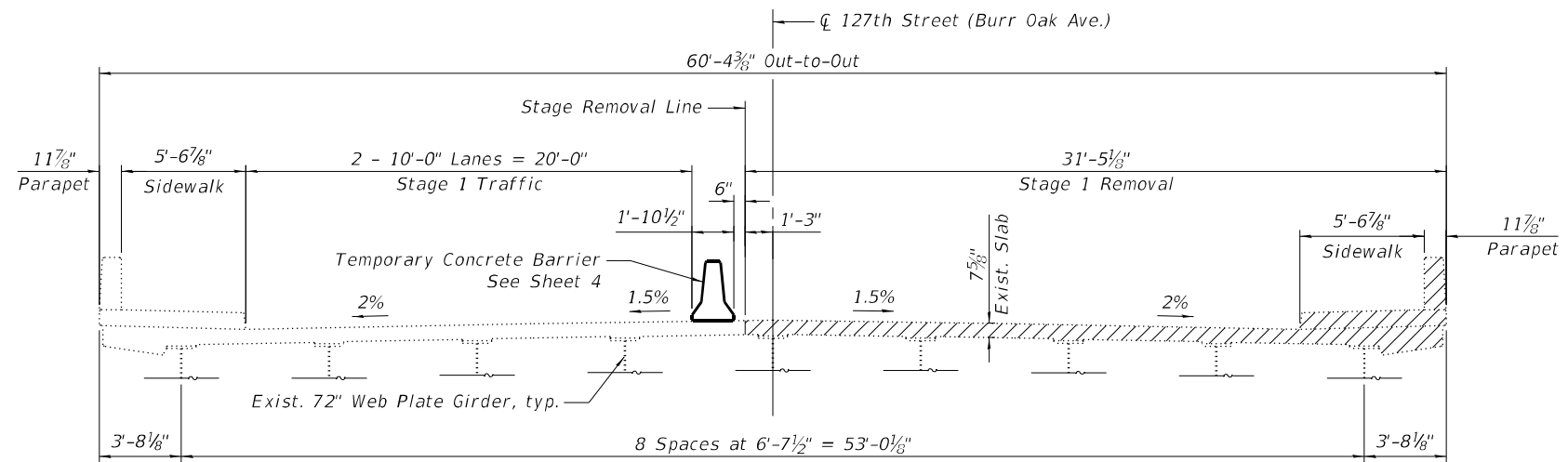
f'c = 4,000 psi (Superstructure Concrete)
fy = 60,000 psi (Reinforcement)

Existing Construction

f'c = 3,500 psi (Concrete)
fy = 60,000 psi (Reinforcement after 2003)
fy = 33,000 psi (Reinforcement before 2003)
fy = 50,000 psi (Structural Steel - M270 Gr. 50)

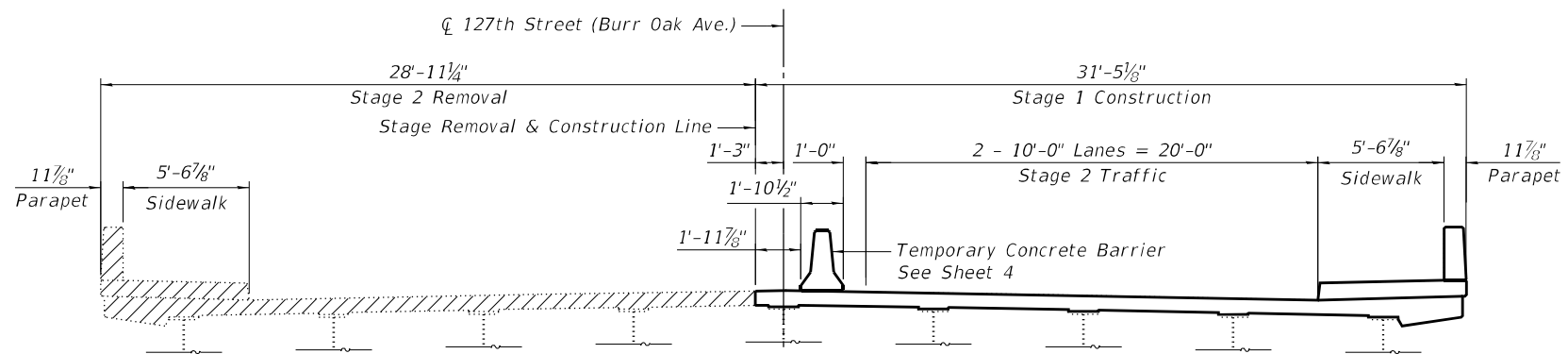
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	PLOT SCALE = 0.1667' / in. PLOT DATE = 3/24/2023	SHEET 2 OF 13 SHEETS			ILLINOIS FED. AID PROJECT	



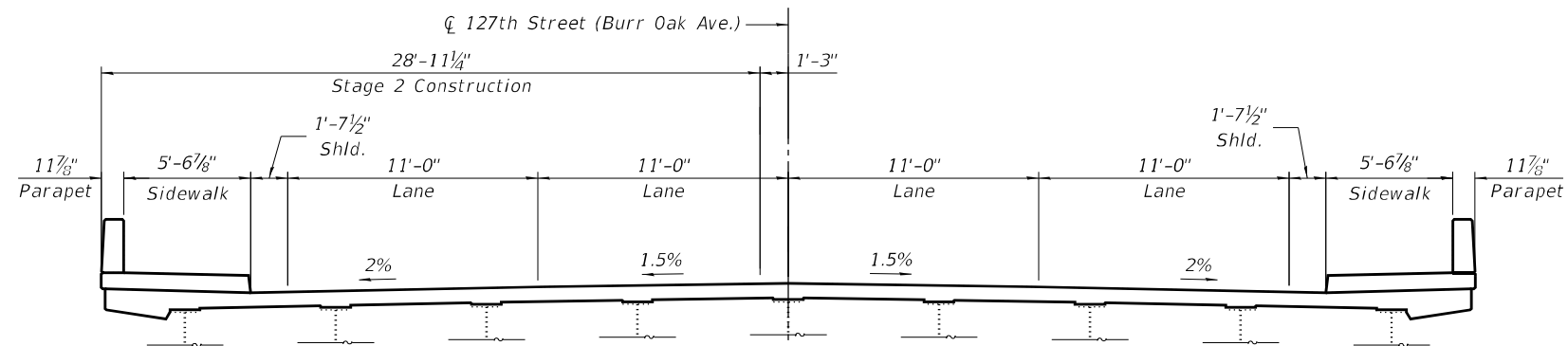
STAGE 1 REMOVAL

(Looking East)



**STAGE 1 CONSTRUCTION
& STAGE 2 REMOVAL**

(Looking East)



STAGE 2 CONSTRUCTION

(Looking East)

Note: Stage Construction Details are shown for the expansion joint replacement at the west and east ends of the deck.

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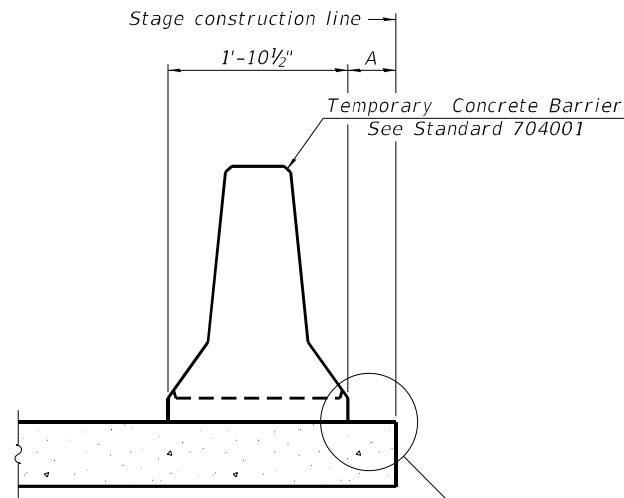
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DEPARTMENT OF TRANSPORTATION**

**STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 016-0908**

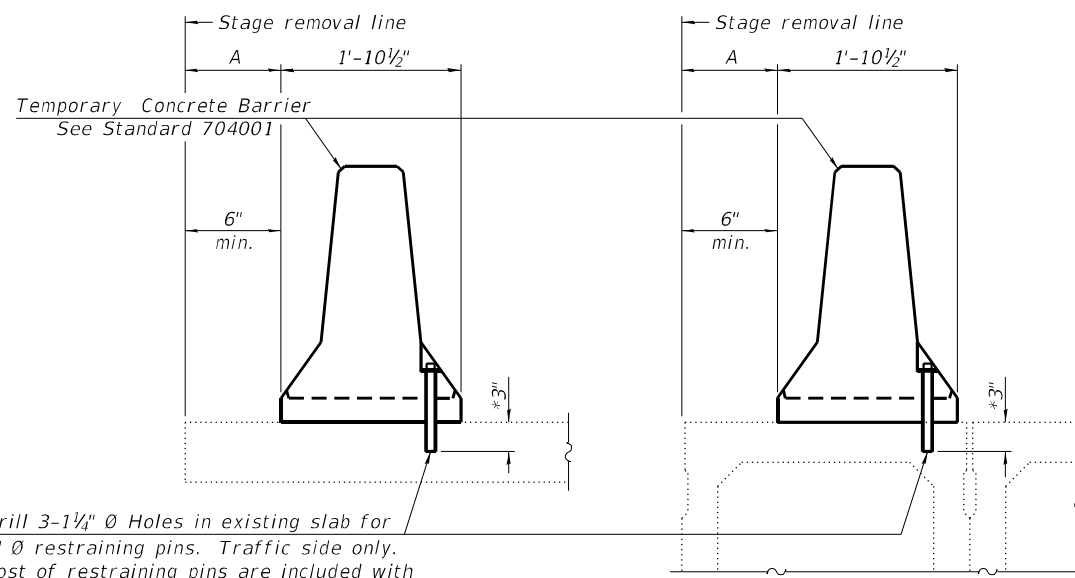
SHEET 3 OF 13 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP-344-22-BR	COOK	23	11
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

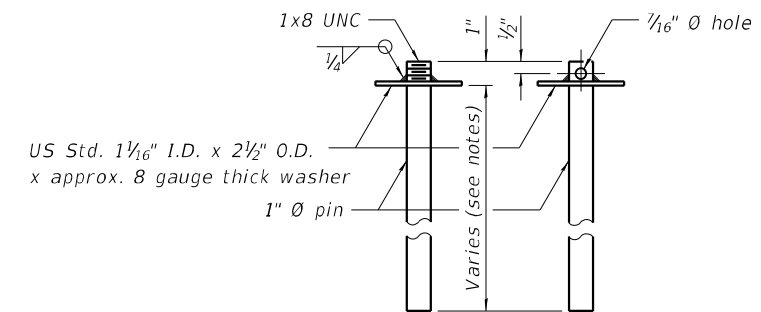


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

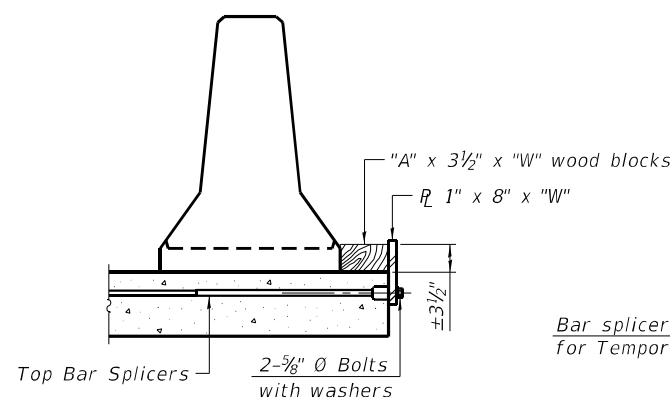
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

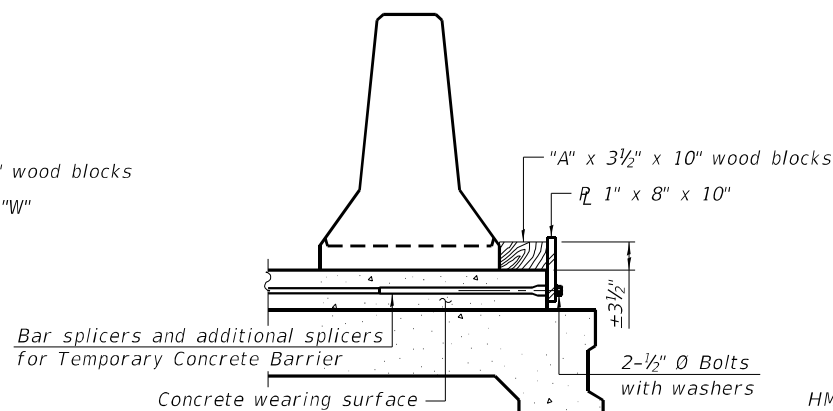


RESTRAINING PIN

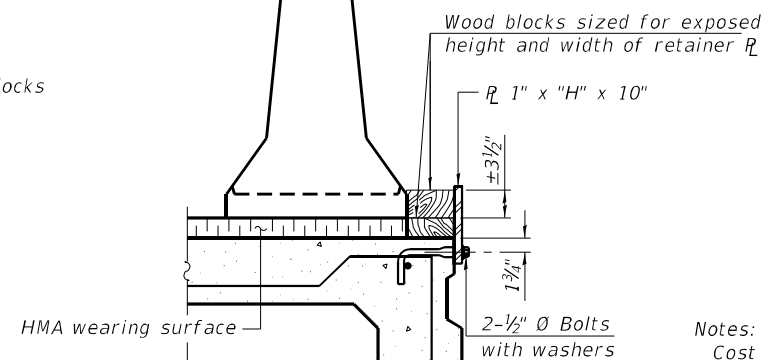
SECTIONS THRU SLAB OR DECK BEAM



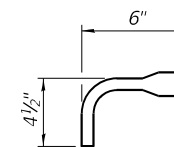
DETAIL I



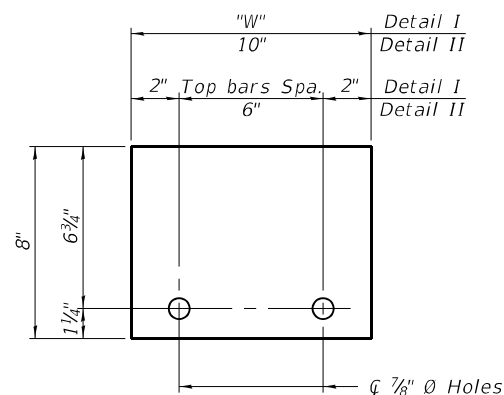
DETAIL II



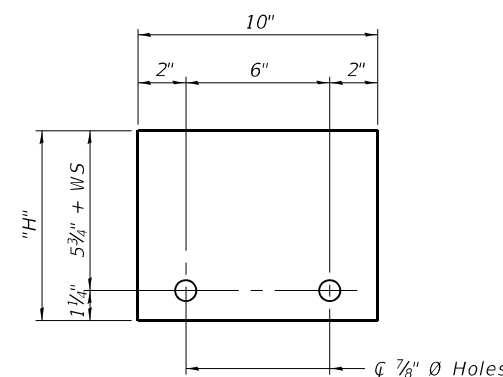
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W" (Detail I and II)



STEEL RETAINER R 1" x "H" x 10" (Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate $\frac{1}{2}$ of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6' to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 10-12-2021

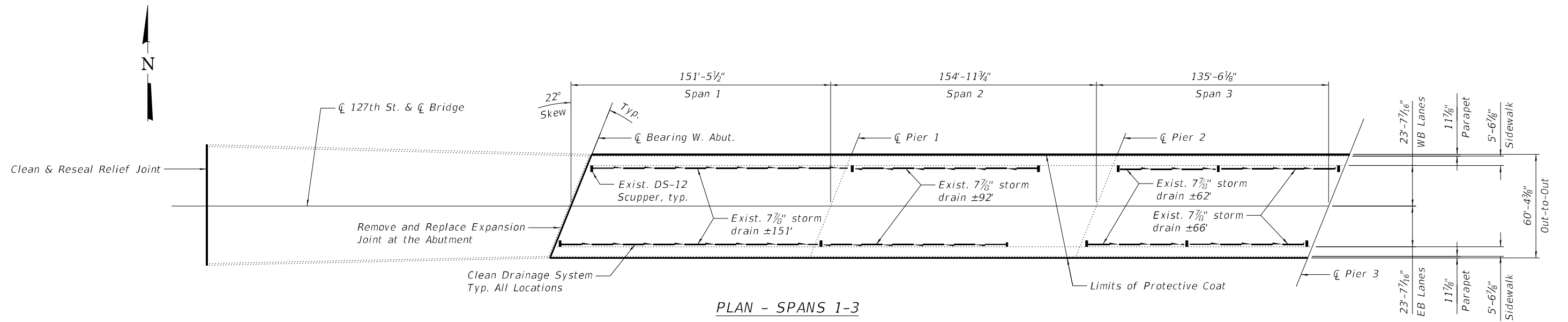
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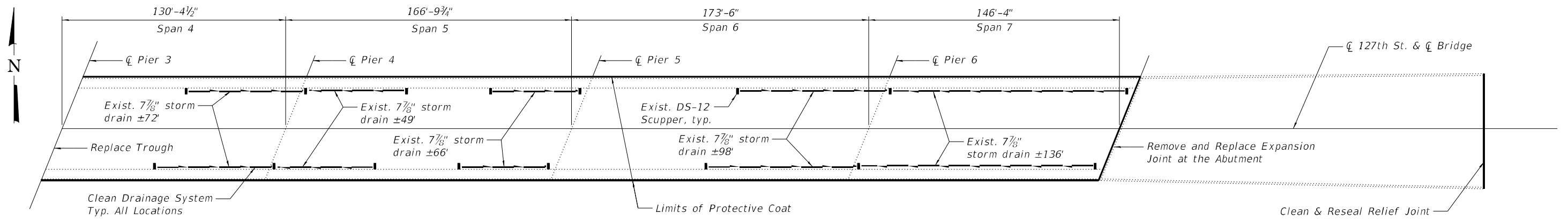
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 DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 STRUCTURE NO. 016-0908

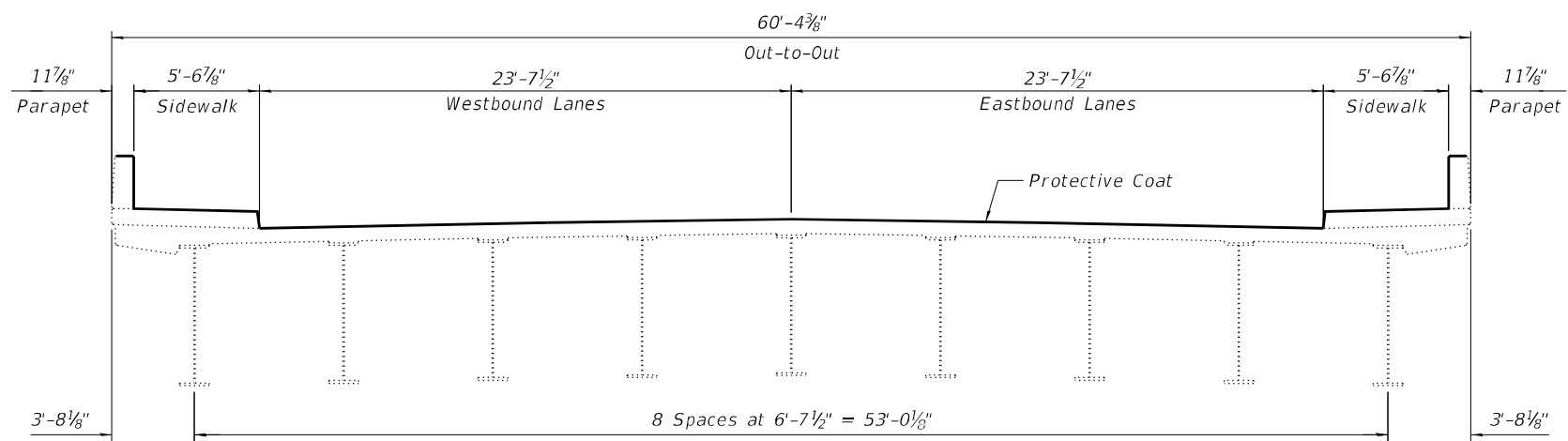
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP-344-22-BR	COOK	23	12
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				



PLAN - SPANS 1-3



PLAN - SPANS 4-7



TYPICAL SECTION
(Looking East)

NOTES

1. For Trough Replacement Details, see Sheet 10.
2. For Abutment Expansion Joint Replacement Details, see Sheets 6 through 9A.

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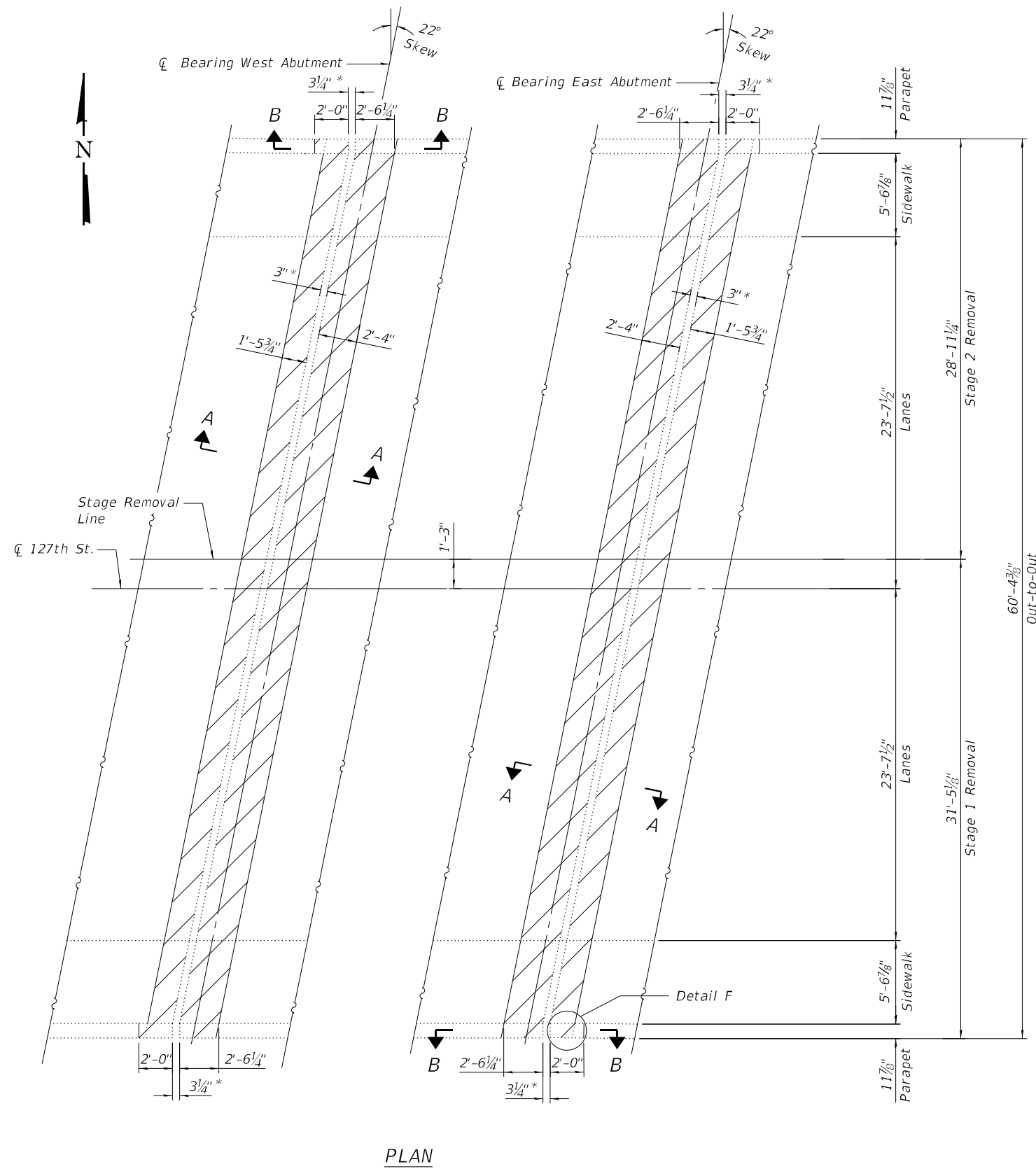
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DEPARTMENT OF TRANSPORTATION**

**DECK REPAIR PLAN
STRUCTURE NO. 016-0908**

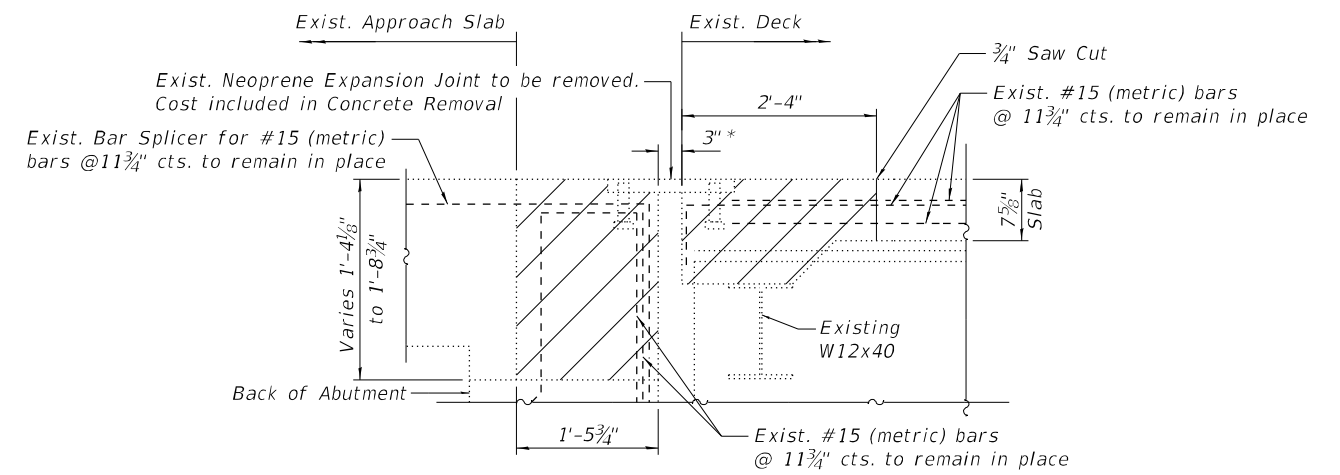
SHEET 5 OF 13 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT				

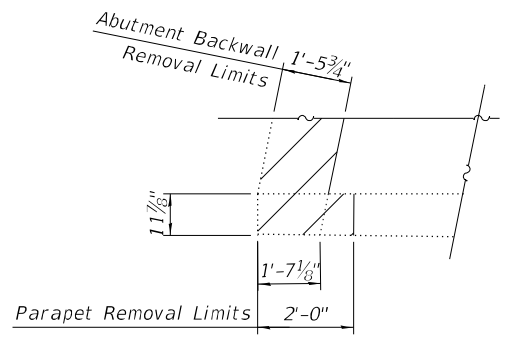
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PLAN



SECTION A-A
 (Dimensions are at right angle)



Detail F

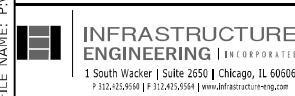
NOTES

- Hatched areas denote Concrete Removal.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Cut the end of existing longitudinal reinforcement bars in the parapet, deck and abutment as necessary to clear the new skewed joint at the edges of the deck. Coat the cut end of the existing reinforcement bars with an anti-rust compound. Cost included with Concrete Removal.
- See Sheet 6A, for Section B-B

* At 50° F

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	27.2



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PLOT DATE = 3/23/2023	DRAWN - TY	REVISED -
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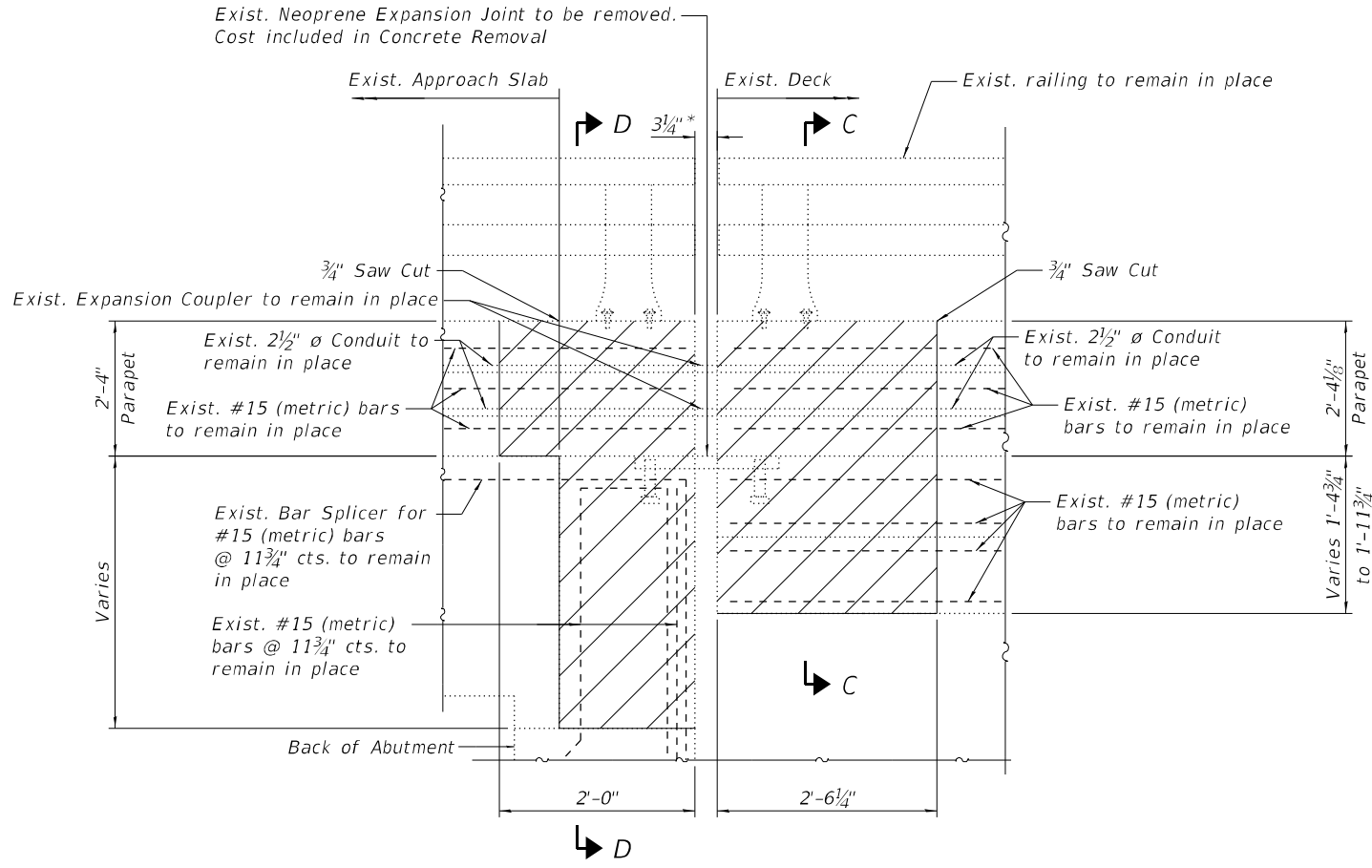
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ABUTMENT AND DECK CONCRETE REMOVAL
 STRUCTURE NO. 016-0908

SHEET 6 OF 13 SHEETS

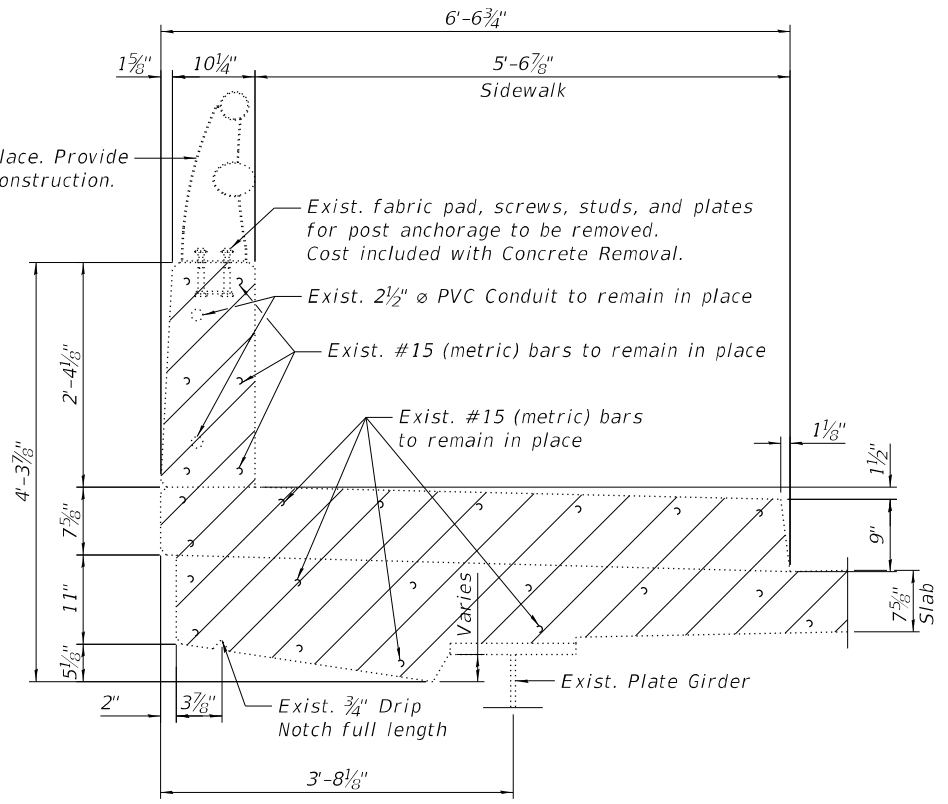
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344	FAP-344-22-BR	COOK	23	14
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				

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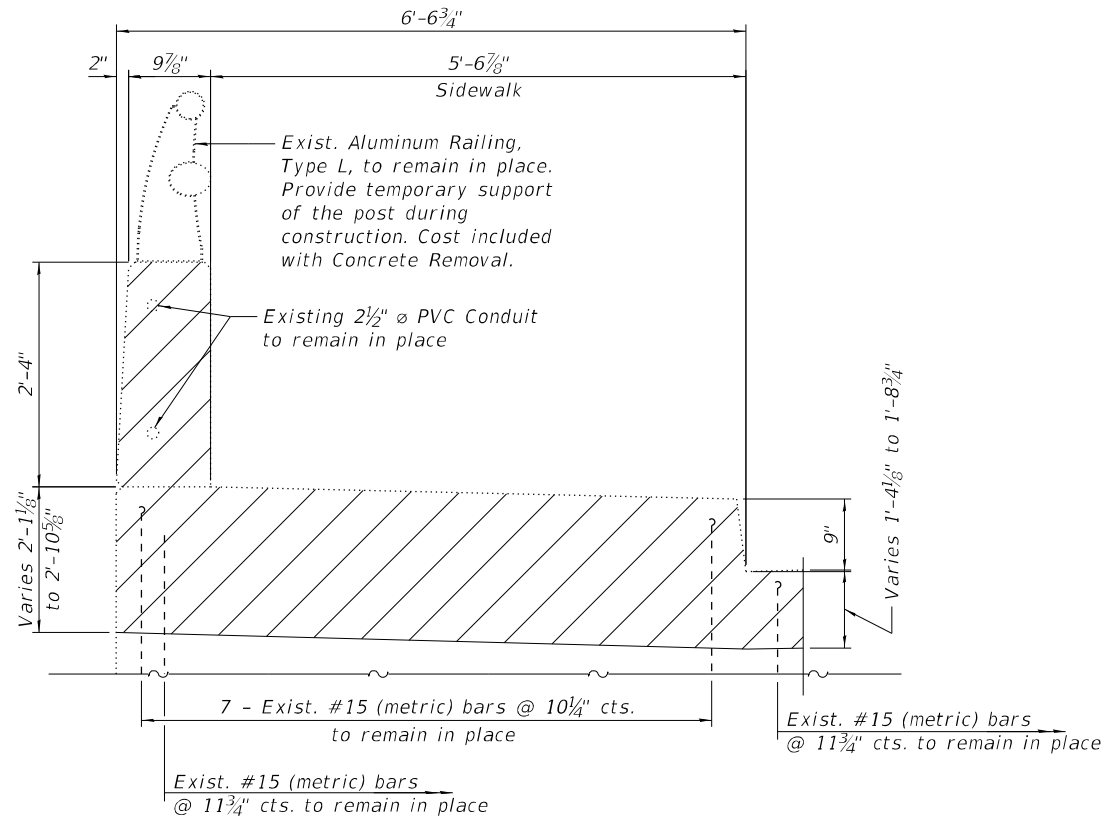


SECTION B-B

Aluminum Railing, Type L to remain in place. Provide temporary support of the post during construction. Cost included with Concrete Removal.

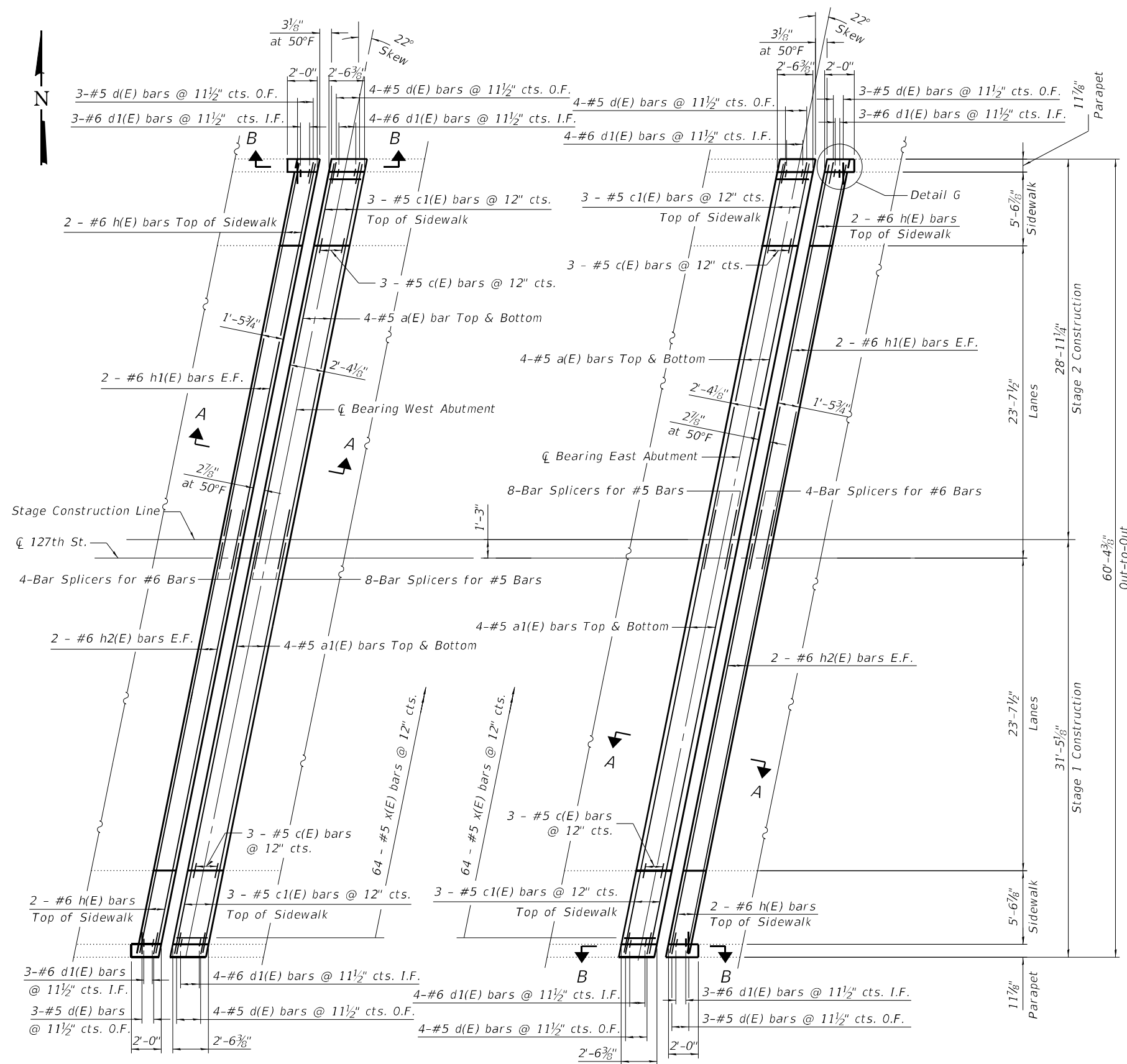


SECTION C-C

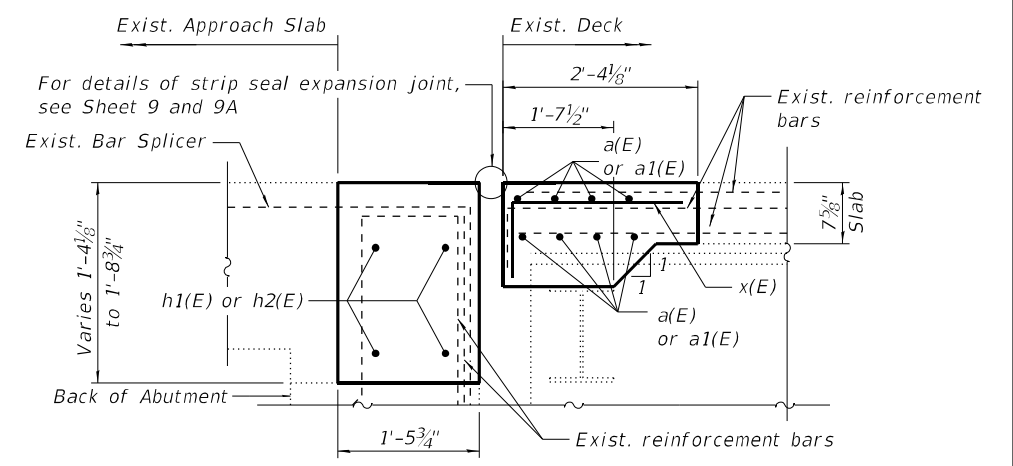


SECTION D-D

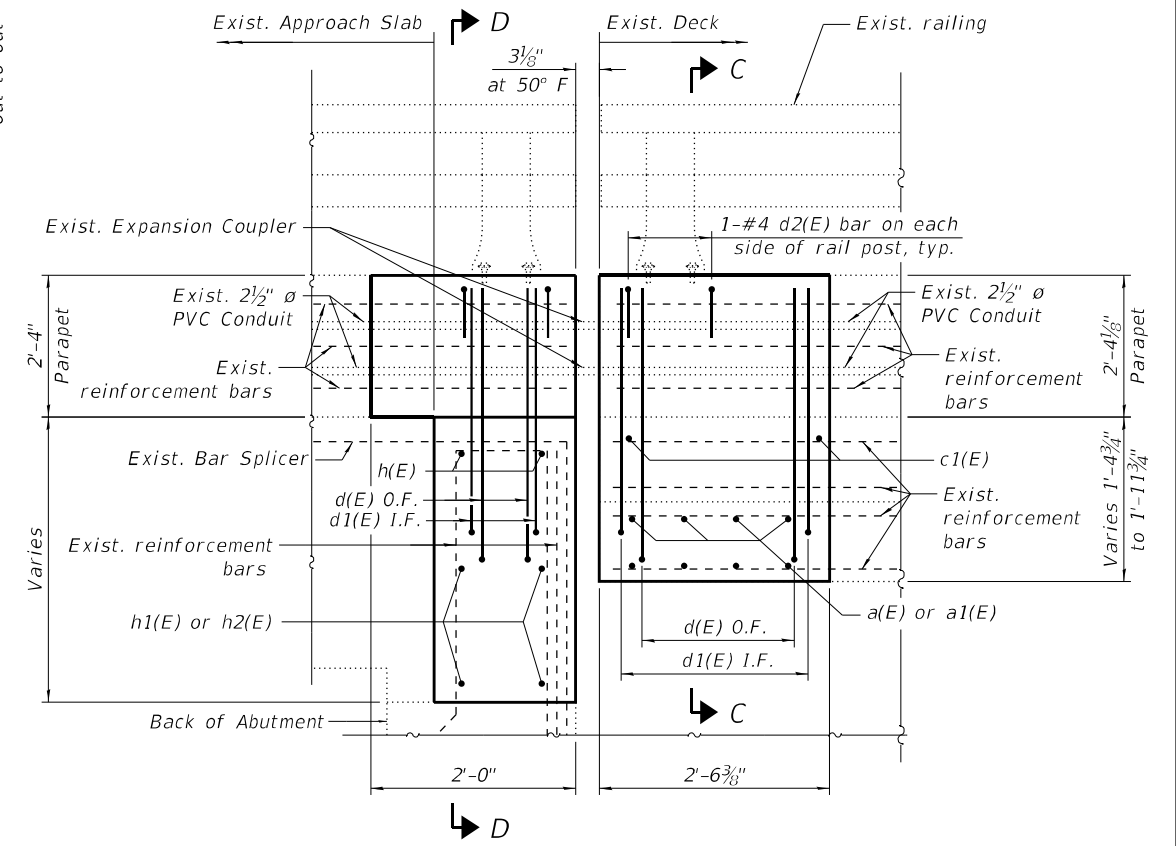
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PLAN



SECTION A-A
 (Dimensions are at right angles)



SECTION B-B

NOTES

1. For Sections C-C, D-D and Detail G, see Sheet 8.
2. For Bill of Material, see Sheet 8.
3. Cut a2(E) bars as required to fit at the stage construction line. Cut end to be coated with an approved anti-rust compound. Cost included with Reinforcement Bars, Epoxy Coated.

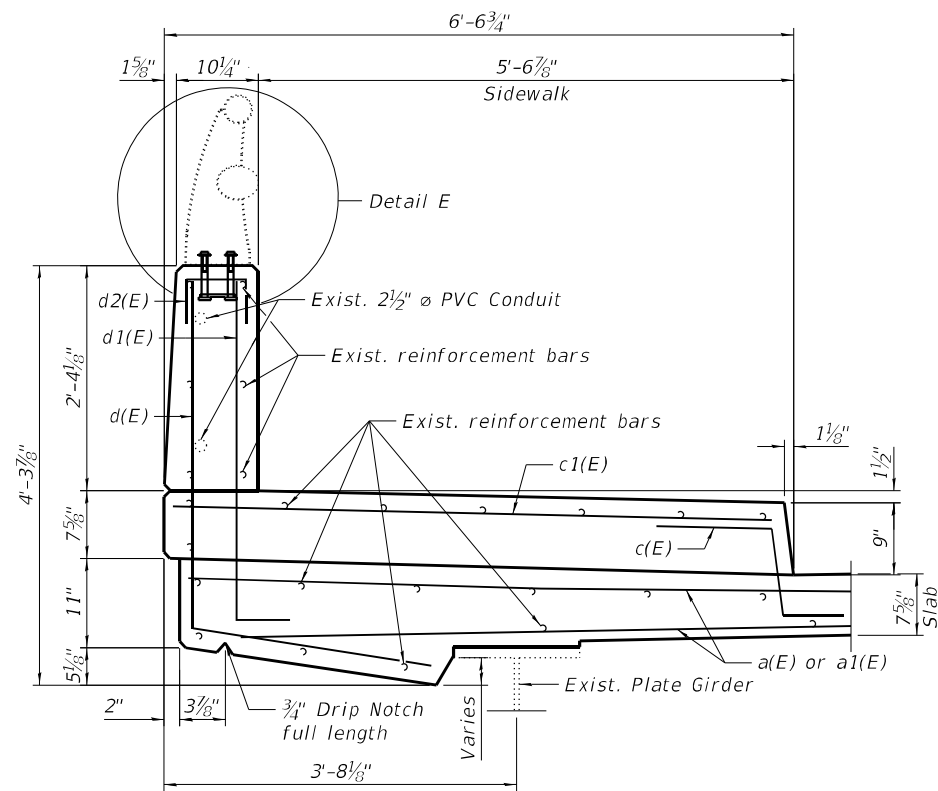


USER NAME = Alane	DESIGNED - TY	REVISED -
PLOT SCALE = 8,000' / in.	CHECKED - KPS	REVISED -
PLOT DATE = 3/24/2023	DRAWN - TY	REVISED -
	CHECKED - KPS	REVISED -

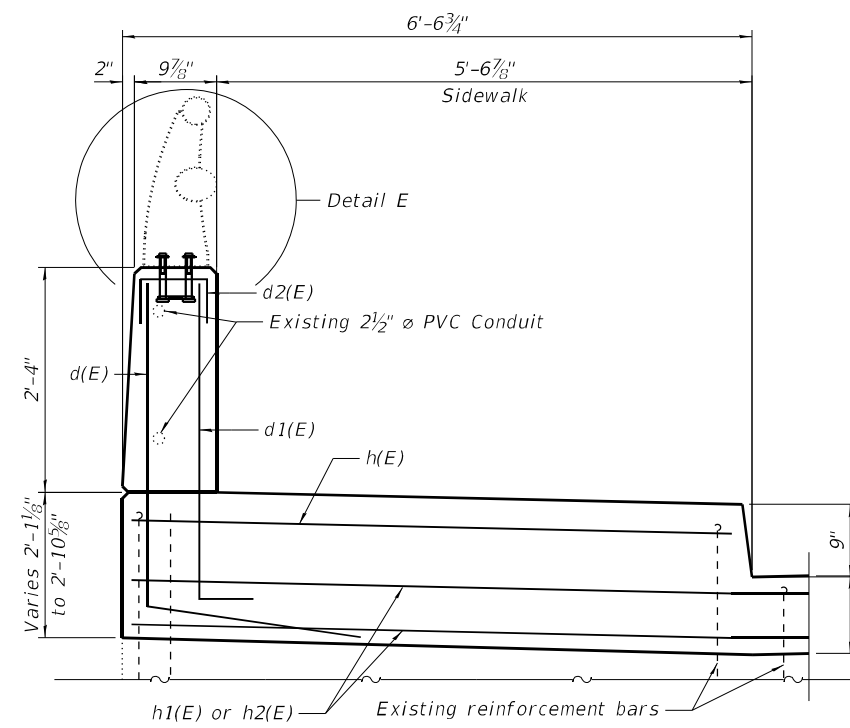
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ABUTMENT AND DECK ALTERATIONS
 STRUCTURE NO. 016-0908
 SHEET 7 OF 13 SHEETS

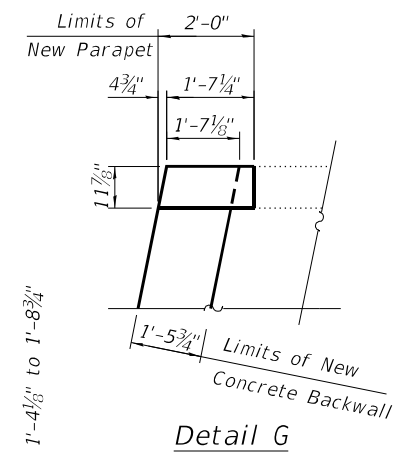
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP-344-22-BR	COOK	23	15
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				



SECTION C-C



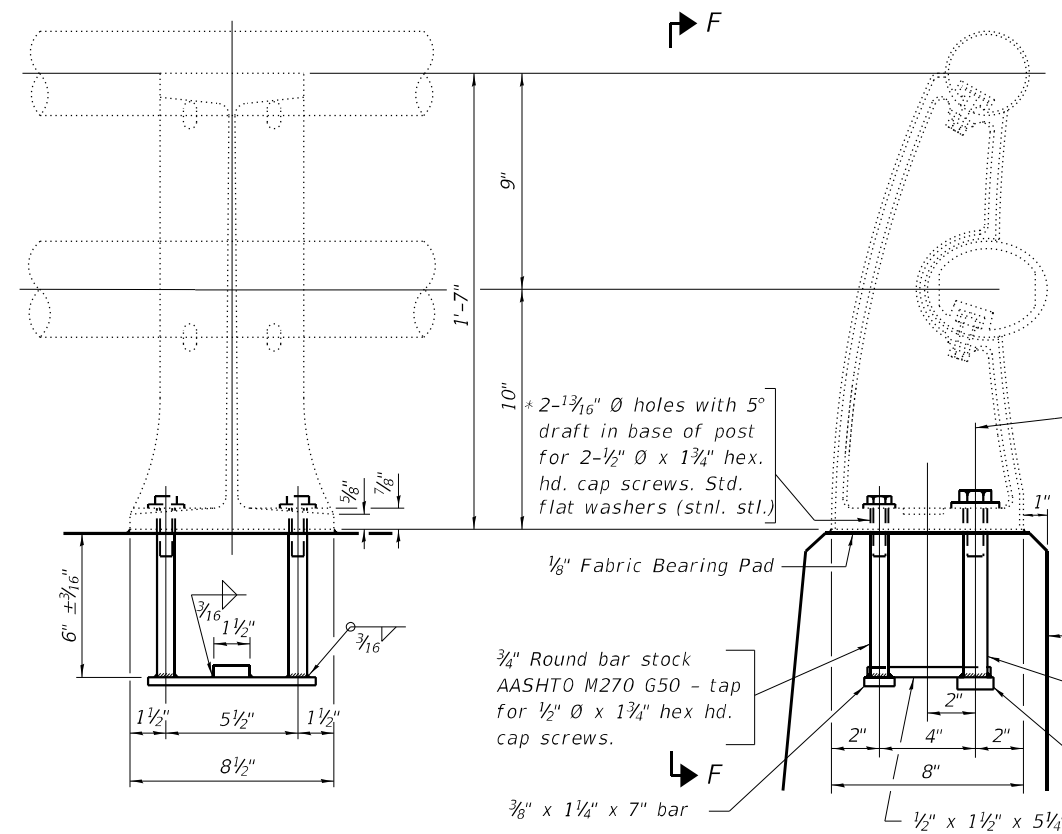
SECTION D-D



Detail G

BILL OF MATERIAL

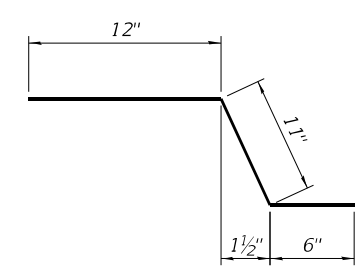
Bar	No.	Size	Length	Shape
a(E)	16	#5	30'-8"	—
a1(E)	16	#5	33'-5"	—
c(E)	12	#5	2'-5"	—
c1(E)	12	#5	6'-9"	—
d(E)	28	#5	5'-11"	—
d1(E)	28	#6	4'-7"	—
d2(E)	16	#4	2'-0"	—
h(E)	8	#6	6'-8"	—
h1(E)	8	#6	30'-11"	—
h2(E)	8	#6	33'-7"	—
x(E)	128	#5	2'-9"	—
Concrete Superstructure			Cu Yd	25.7
Reinforcement Bars, Epoxy Coated			Pound	3,070



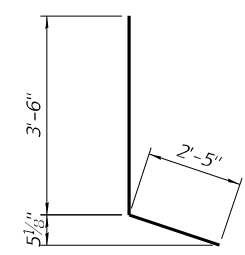
RAIL POST DETAILS

Note: Cost of furnishing and installing the rail post anchor device and fabric bearing pad is included with Concrete Superstructure.

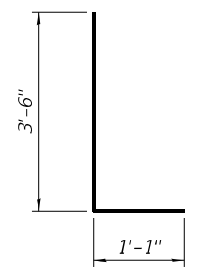
* In lieu of the cast-in-place anchor device shown, the Contractor has the option of drilling and setting stainless steel anchor rods of the same diameter and grade as the specified cap screws according to Article 509.06 of the Standard Specifications. Embedment shall be according to the manufacturer's specifications.



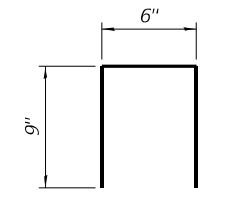
BAR c(E)



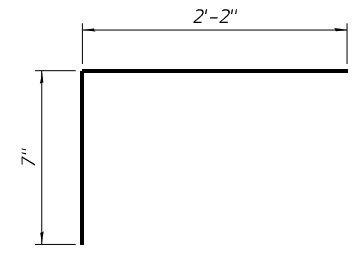
BAR d(E)



BAR d1(E)



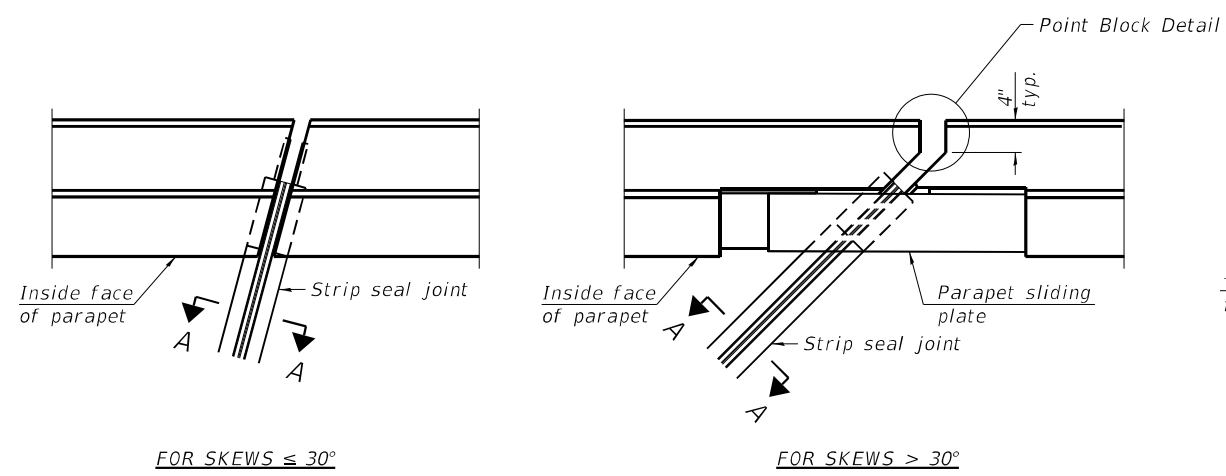
BAR d2(E)



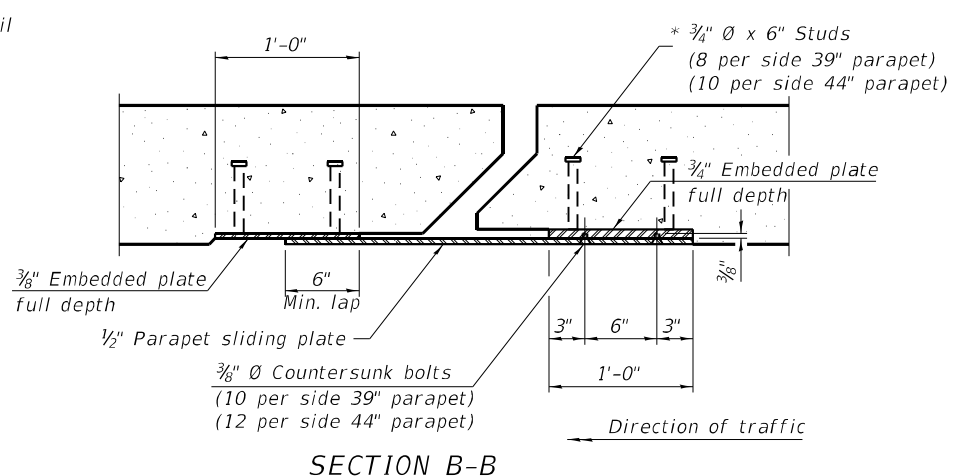
BAR x(E)

MODEL: Default; FILE NAME: P:\P-20-20-4094-00 IDOT Various Phase 2 (PTB 196 Item 16)\WO 33 62U49\DGNCADD_Sheets\62U49-SHT-5TR-07_ExplRep - A.dgn

MODEL: Default
 FILE NAME: P:\P-20-20-4094-00 IDOT Various Phase 2, (PTB 196 Item 16)\WO 33 62U49\IGNICADD_Sheets\62U49-SHT-STR-08_ExplDet.dgn
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P. 312.425.9500 | F. 312.425.9594 | www.infrastructurercs.com

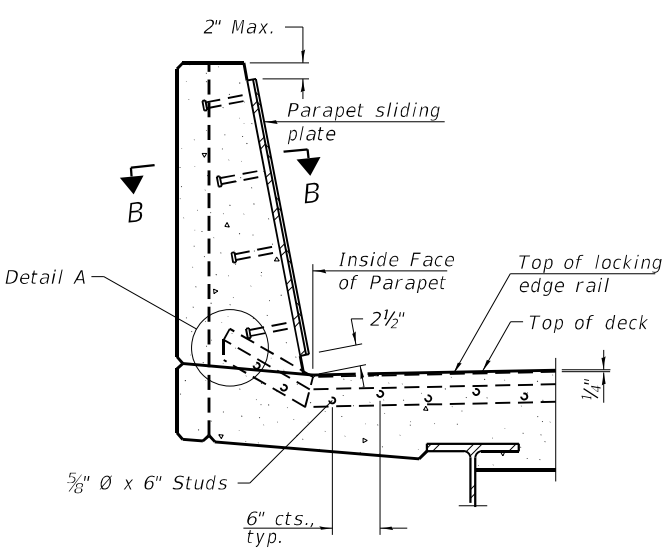


PLAN AT PARAPET



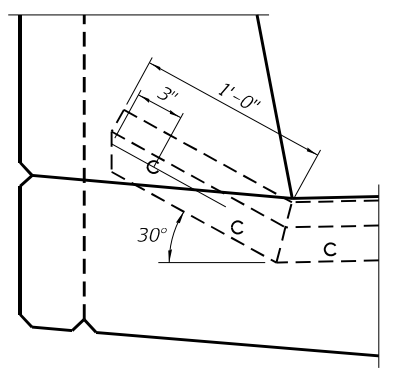
SECTION B-B

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
 The manufacturer's recommended installation methods shall be followed.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.
 The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.
 Cost of parapet sliding plates, sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.
 39" constant slope barrier shown, 44" constant slope barrier similar as noted.
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

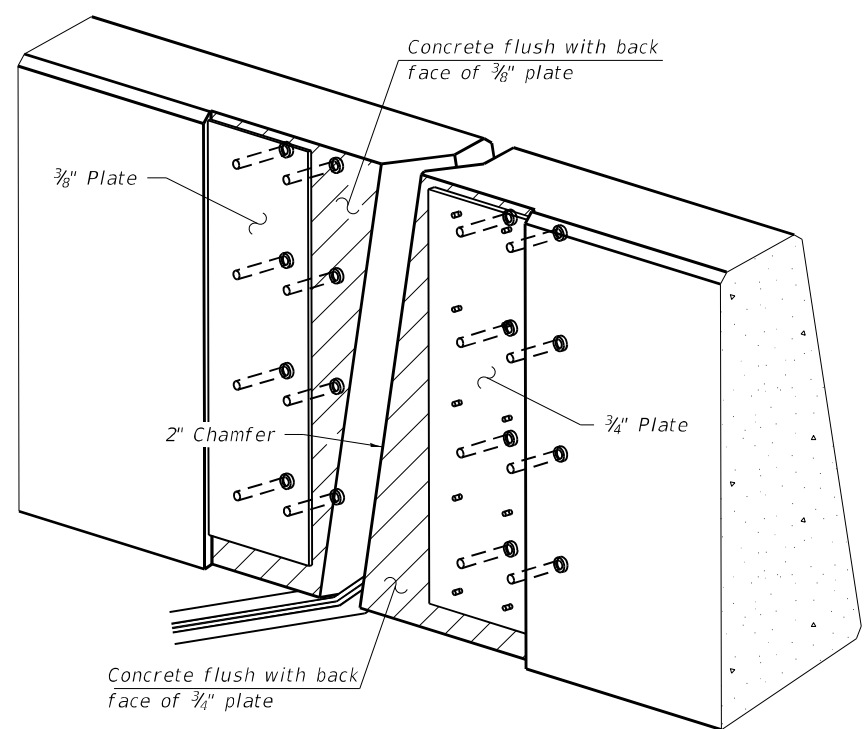


SECTION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

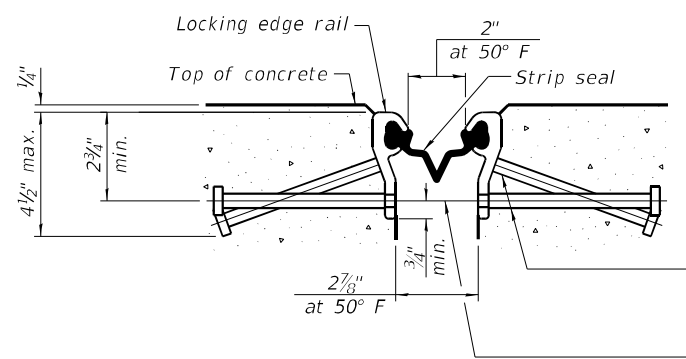


DETAIL A



TRIMETRIC VIEW

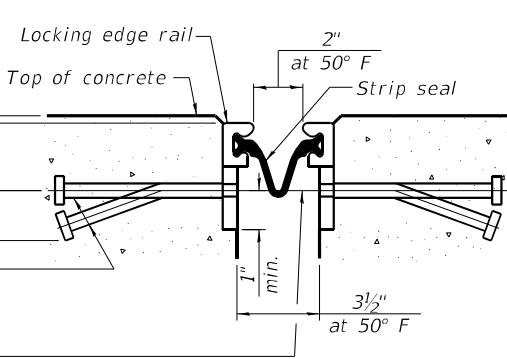
(Showing embedded plates only)



SHOWING ROLLED RAIL JOINT

* 3/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

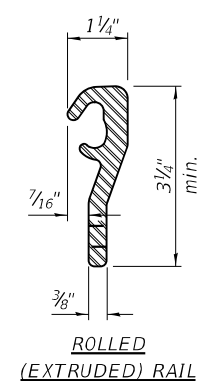
3/8" Ø threaded rods in 1/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.



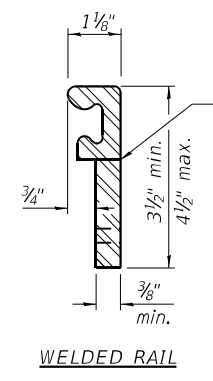
SHOWING WELDED RAIL JOINT

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



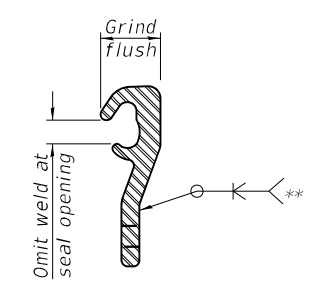
ROLLED (EXTRUDED) RAIL



WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

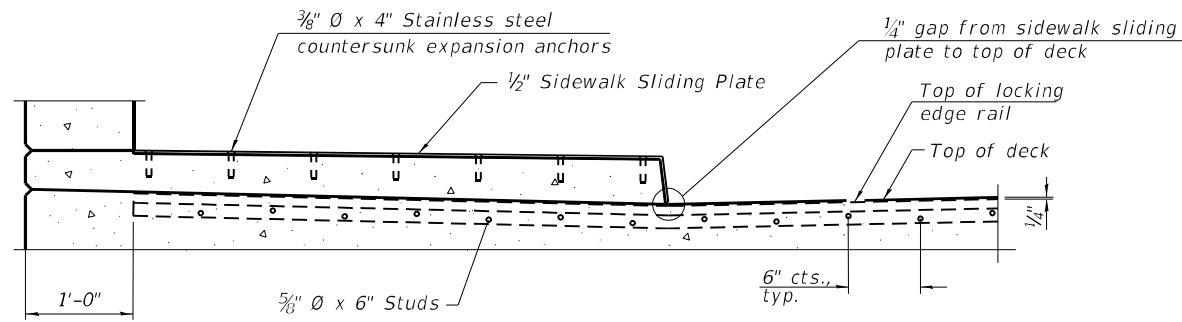
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

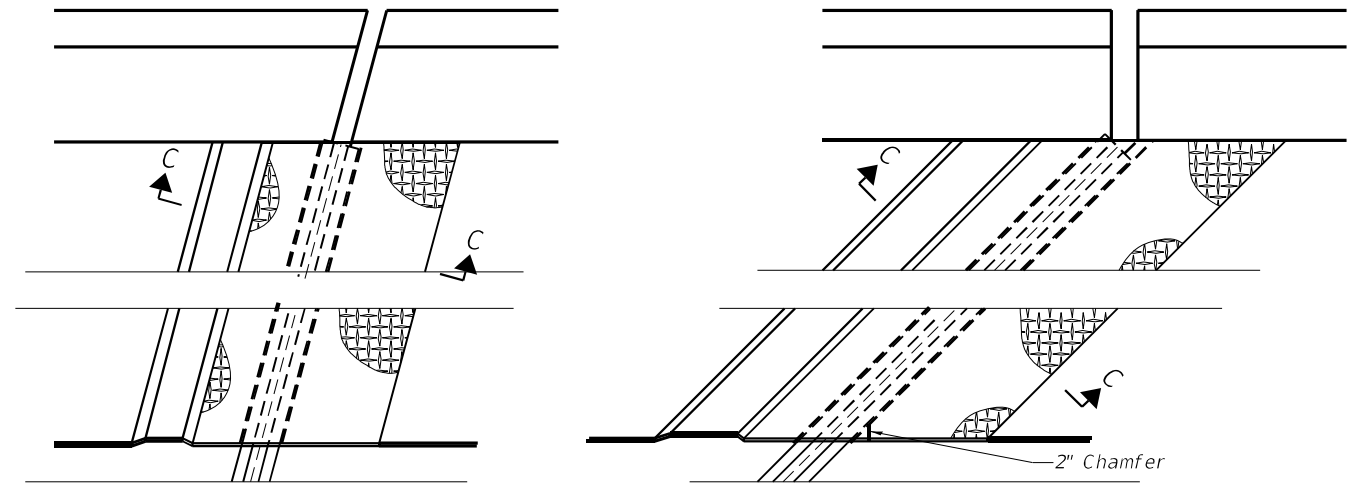
Item	Unit	Total
Preformed Joint Strip Seal	Foot	131

EJ-SS-S

1-1-2020



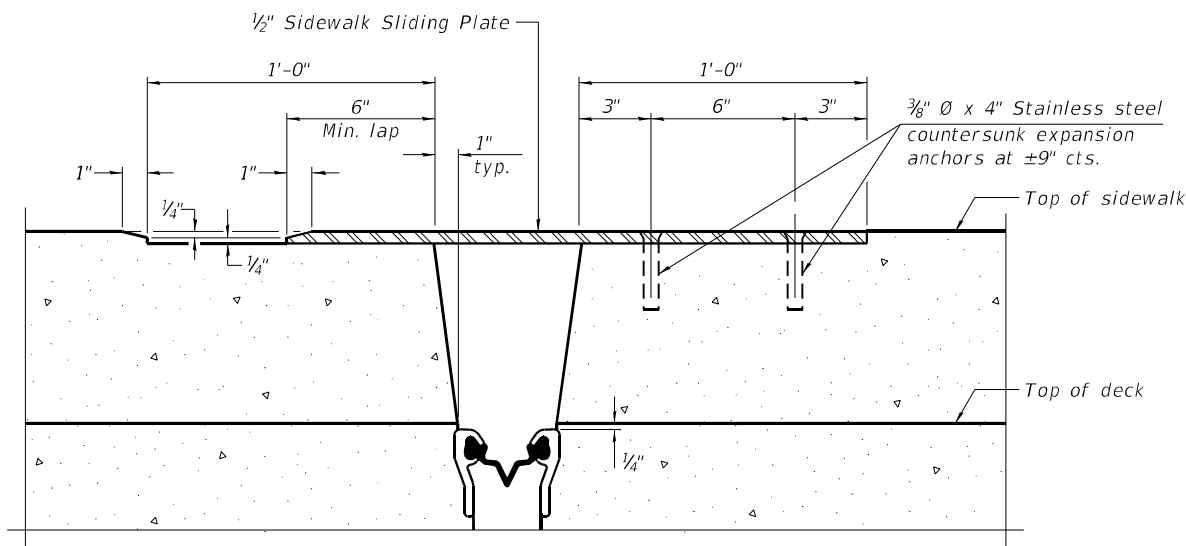
SECTION AT RAISED SIDEWALK



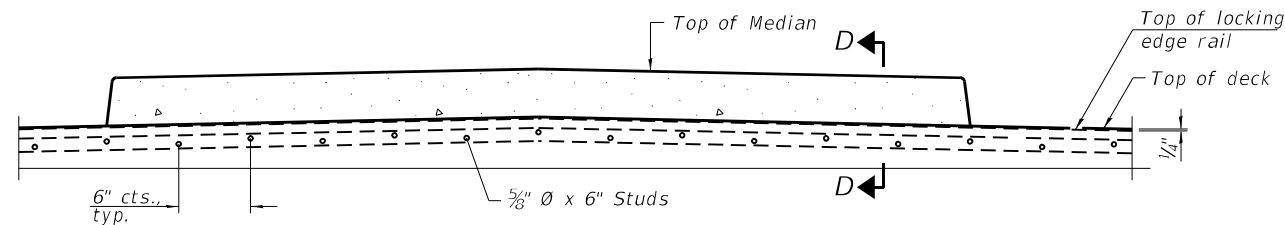
(FOR SKEWS ≤ 30°)

(FOR SKEWS > 30°)

PLAN AT RAISED SIDEWALK

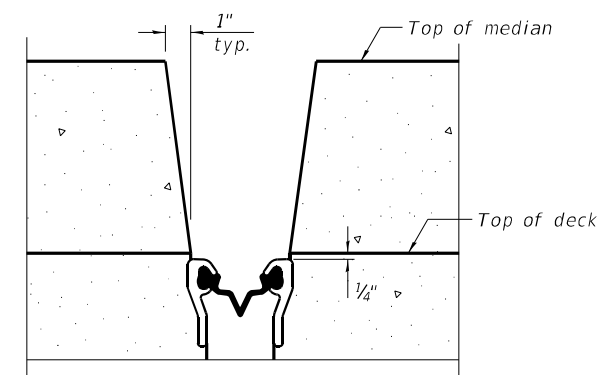


SECTION C-C

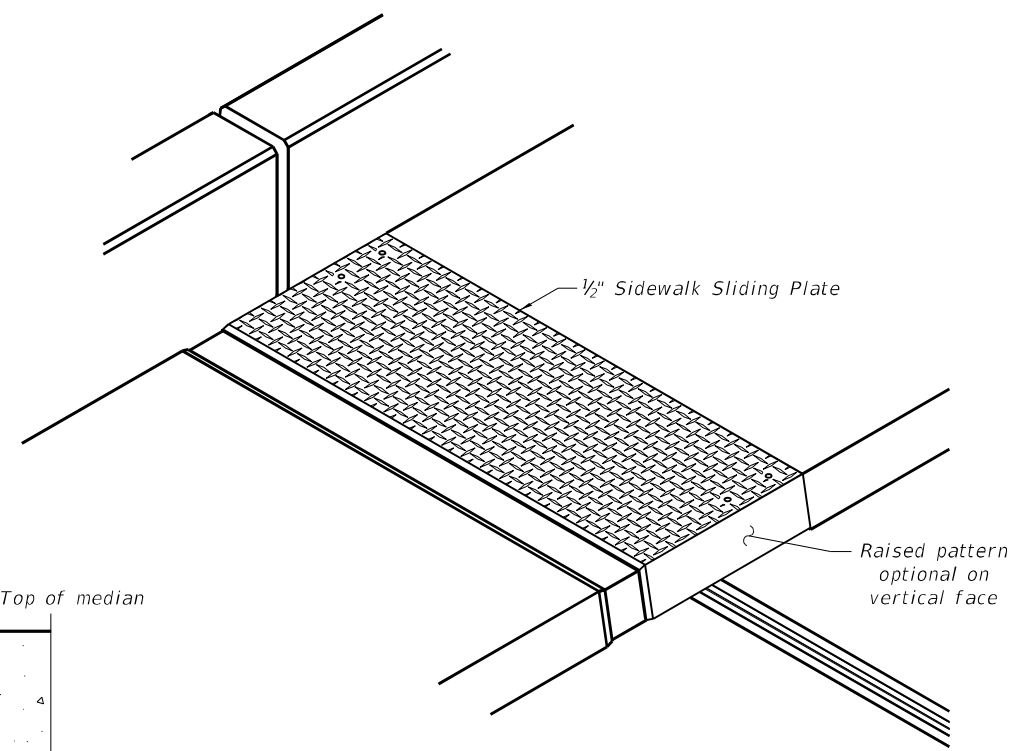


SECTION AT MEDIAN

For skews > 30°, chamfer acute corners 2" similar to sidewalk.



SECTION D-D
(at Rt. L's)



TRIMETRIC VIEW

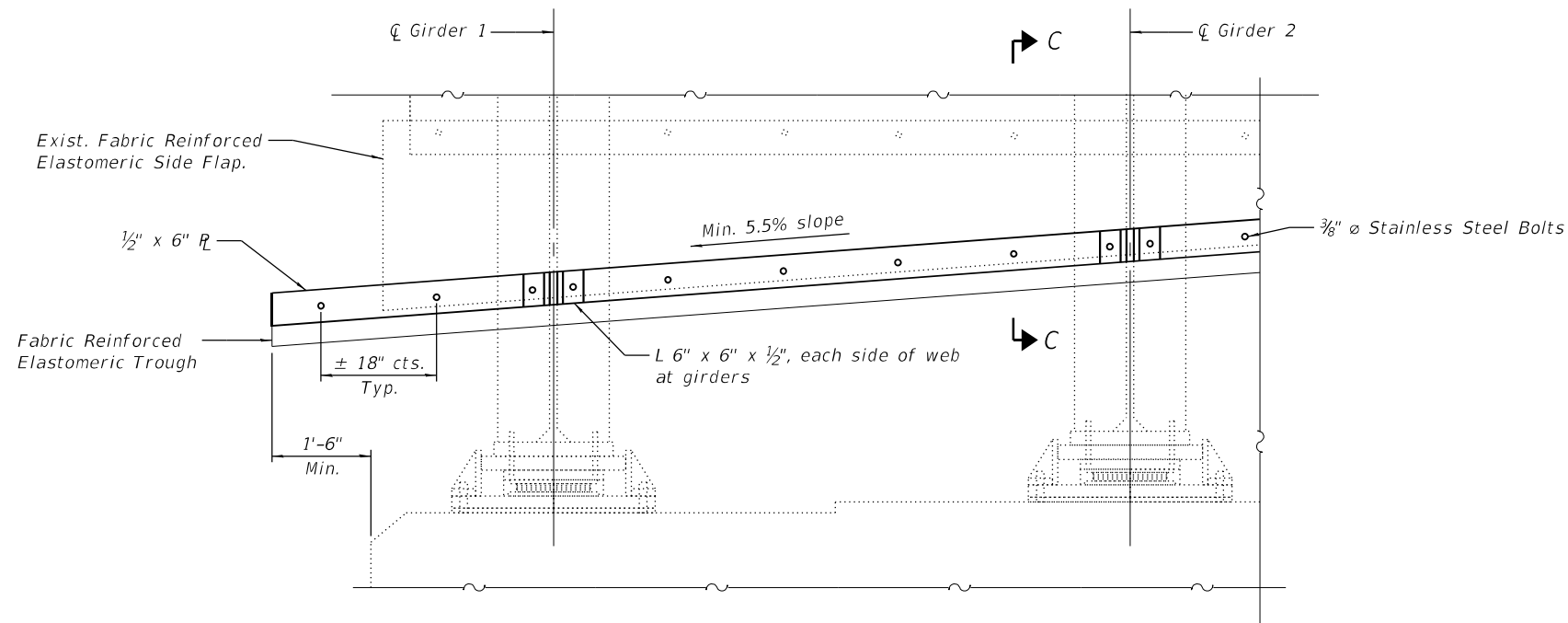
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EJ-SS-S

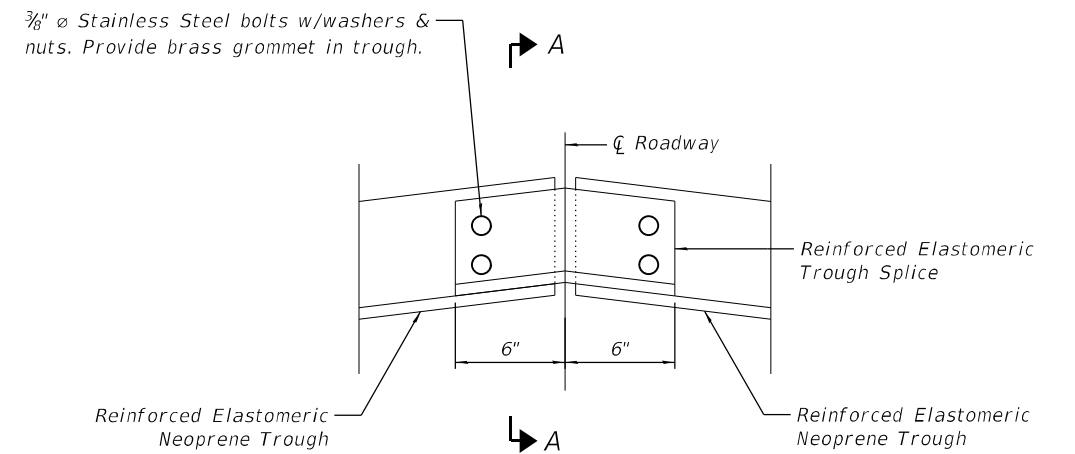
11-1-2022

INFRASTRUCTURE ENGINEERING <small>1 South Wacker Suite 2650 Chicago, IL 60606 P. 312.425.8500 F. 312.425.8594 www.infrastructre-eng.com</small>	USER NAME = TYU	DESIGNED - TY	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PREFORMED JOINT STRIP SEAL – SIDEWALK (SHEET 2 OF 2) STRUCTURE NO. 016-0908	F.A. RTE. = 344	SECTION = FAP-344-22-BR	COUNTY = COOK	TOTAL SHEETS = 23	SHEET NO. = 17A
	PLOT SCALE = 0.1667' / in.	DRAWN - TY	REVISED -			SHEET 9A OF 13 SHEETS	CONTRACT NO. 62U49			
	PLOT DATE = 3/23/2023	CHECKED - KPS	REVISED -			ILLINOIS FED. AID PROJECT				

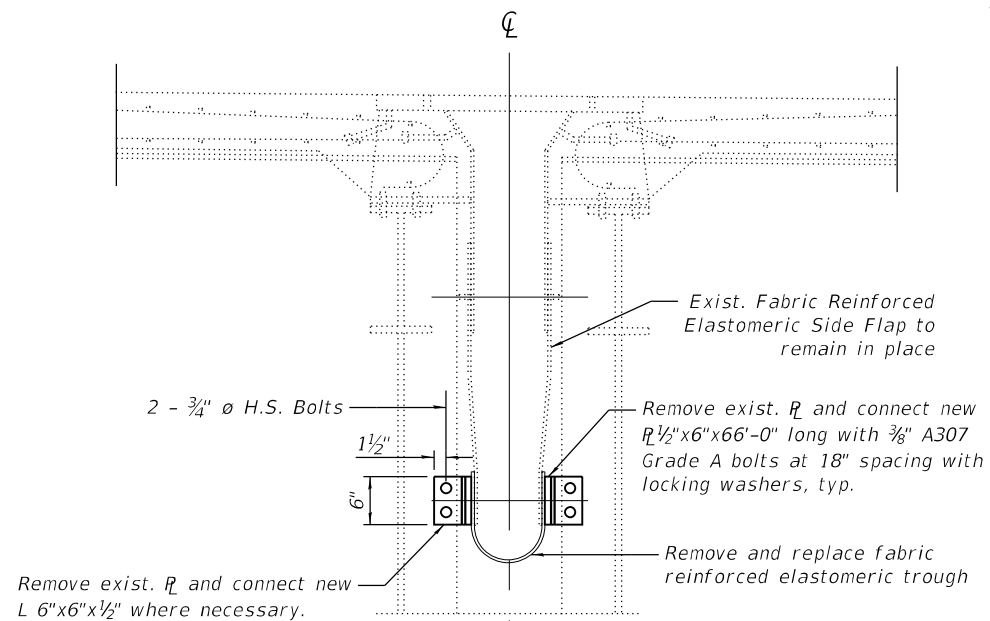
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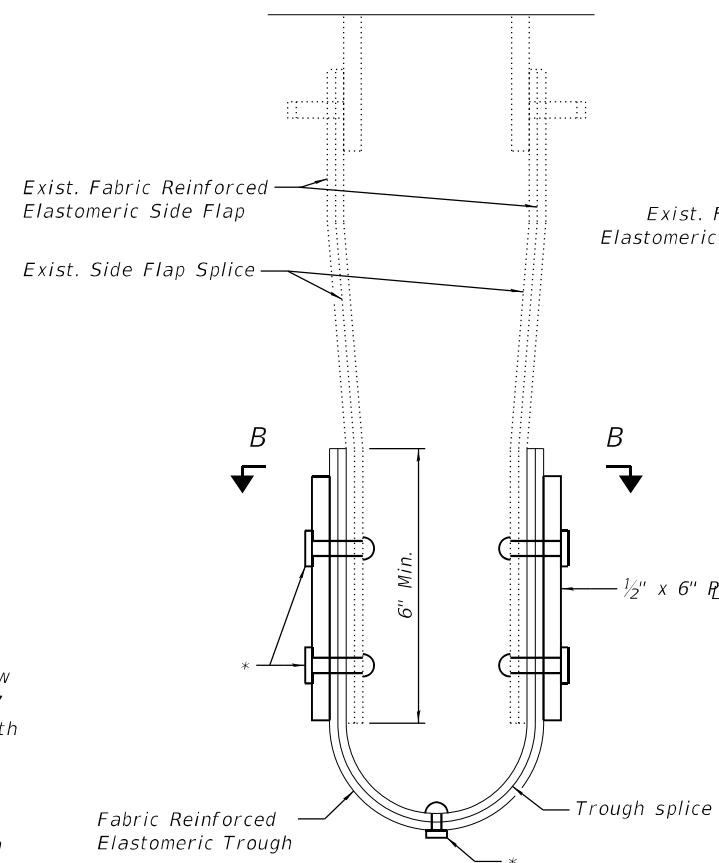
TROUGH ELEVATION VIEW



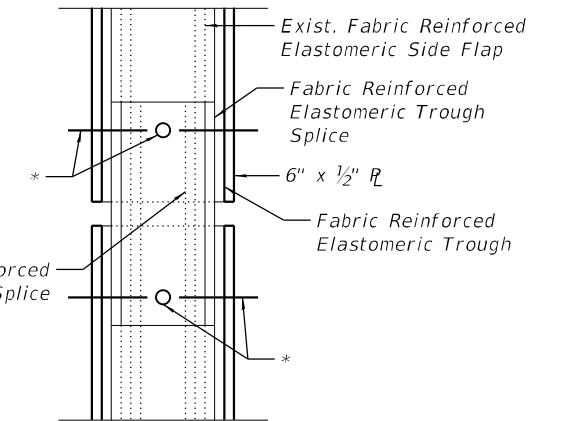
TROUGH SPLICE DETAIL



SECTION C-C



SECTION A-A



SECTION B-B

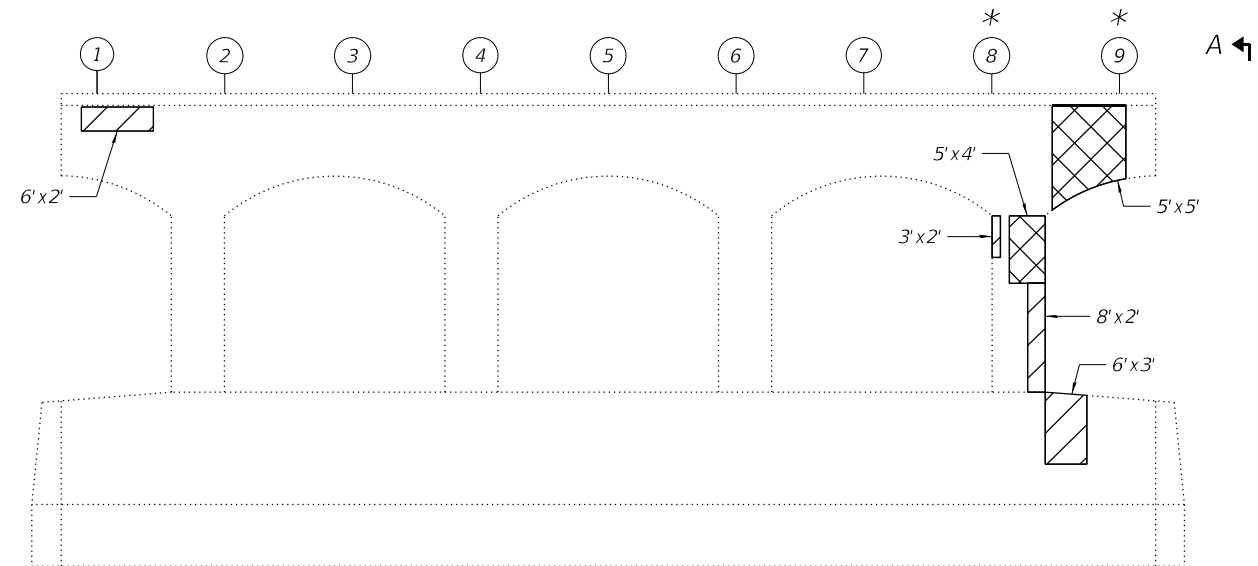
* 3/8" Stainless Steel bolts w/washers & nuts. Provide brass grommet in trough.

BILL OF MATERIAL

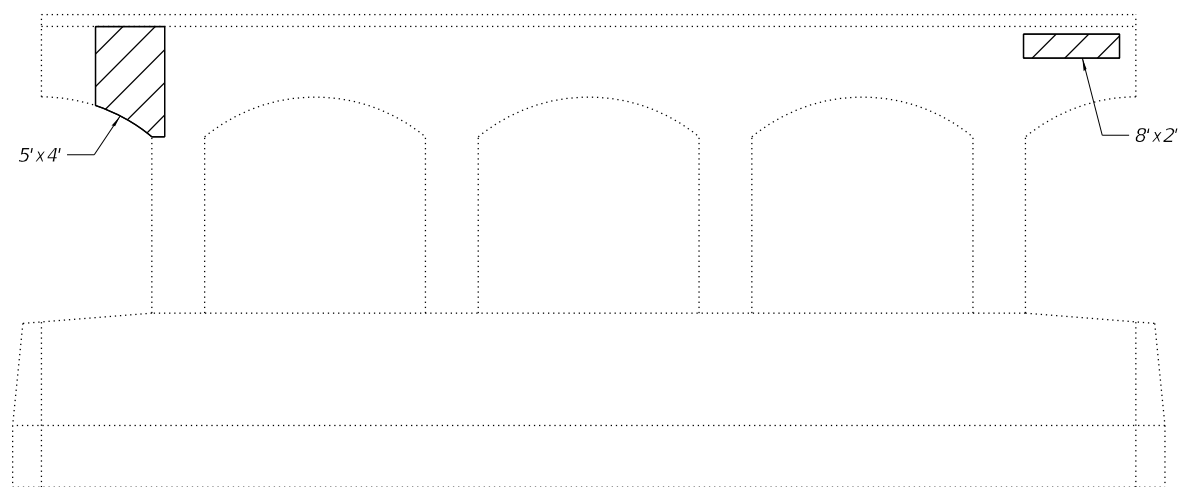
ITEM	UNIT	TOTAL
Fabric Reinforced Elastomeric Trough	Foot	66

NOTES

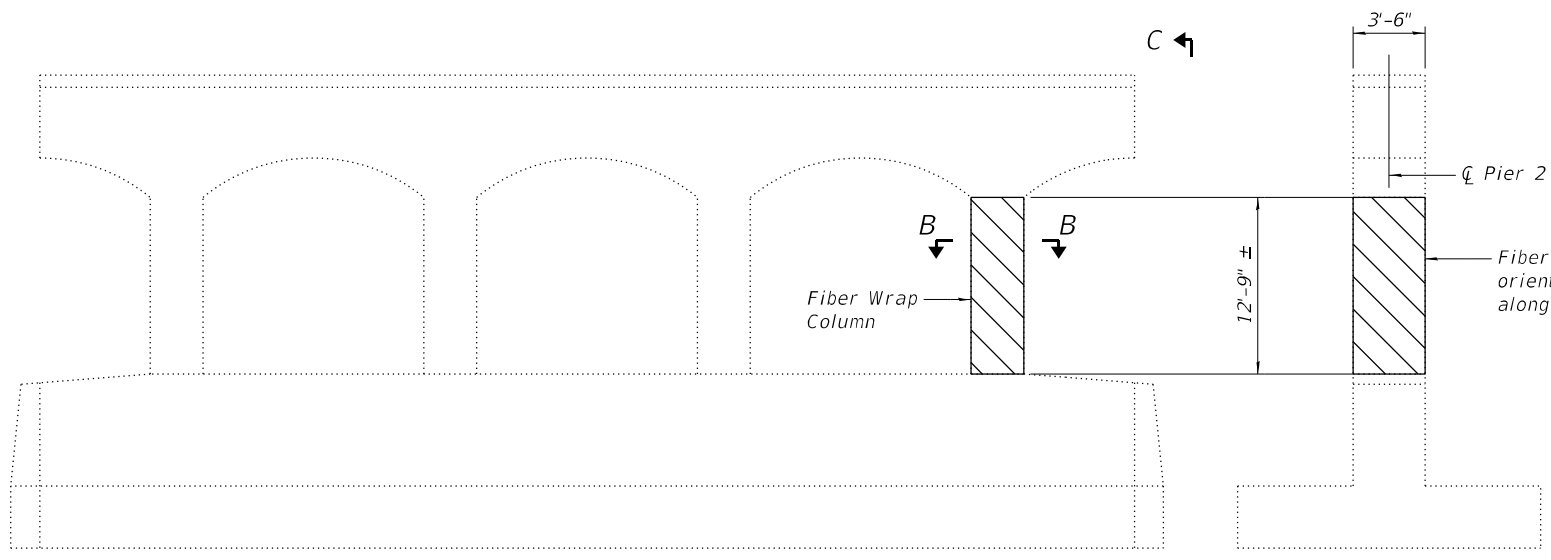
1. Cost of removal of all existing material as required to install the new fabric reinforced elastomeric trough is included with the cost for fabric reinforced elastomeric trough
2. Contractor to field drill holes and new material to match holes in the existing material.



**WEST ELEVATION
SHOWING CONCRETE REPAIR**
Looking East



**EAST ELEVATION
SHOWING CONCRETE REPAIR**
Looking West



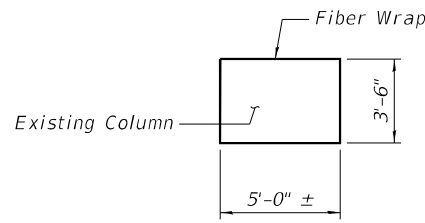
**WEST ELEVATION
SHOWING FIBER WRAP REPAIR**
Looking East

SECTION C-C

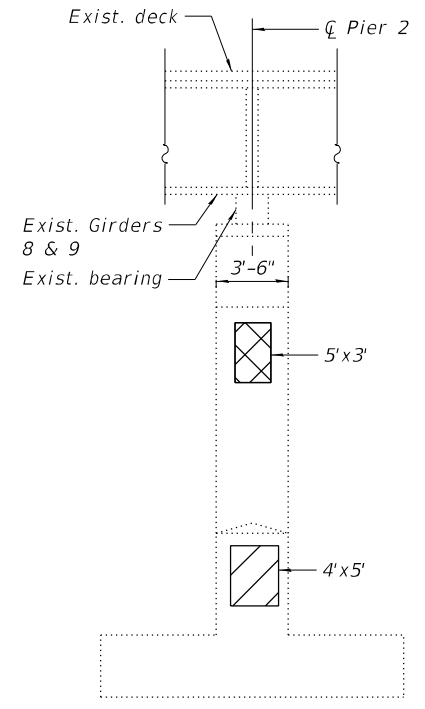
PIER 2 GIRDER REACTION TABLE

R_DL(k)	259.9
R_LL(k)	81.4
Imp(k)	9.9
R_Total(k)	351.2

Note: Quantities and limits of Structural Repair of Concrete shown herein are estimated from a survey completed by the District. Actual limits shall be determined in the field by the Engineer and noted in the As-Built Plans.



SECTION B-B



SECTION A-A

LEGEND

- Indicates Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)
- Indicates Structural Repair Of Concrete (Depth Greater Than 5 Inches)
- Indicates limits of Fiber Wrap
- ① Indicates Girder Number
- * Beam requires temporary shoring

FIBER WRAP REPAIR NOTES

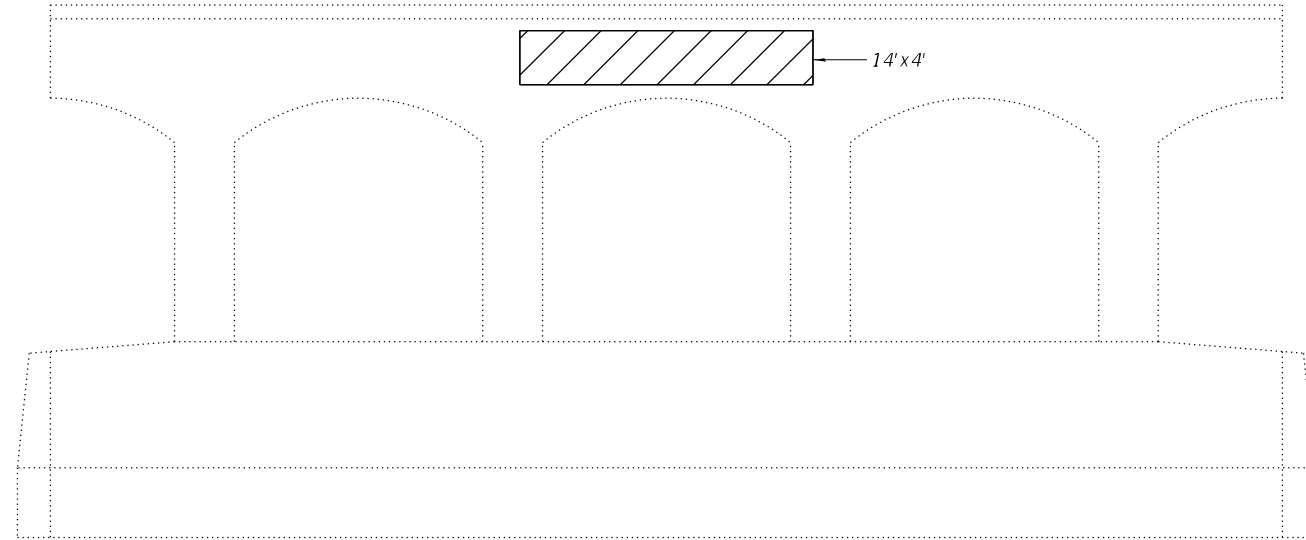
1. Existing uneven surfaces must be filled with an appropriate repair mortar.
2. Cracks with width greater than 0.012 inches must be stabilized using epoxy injection methods. Use manufacturers data sheets for information on mixing epoxy resin.
3. System is a vapor barrier. Do not encapsulate concrete if any surface moisture is present. Carbon fabric is non-reactive. However, caution must be used when handling, since a fine "carbon dust" may be present on the surface.
4. Gloves and protective face masks must be worn to protect against any respiratory problems and skin irritation. Wrap the identified piers with the specified number of wraps as indicated on the plans or as directed by the engineer.
5. Concrete edges shall be rounded to at least 3/4" radius and smoothed to a surface finish prior to application of fiber wrap.
6. Acrylic coating shall be applied to all surfaces of the fiber wrap.

BILL OF MATERIAL

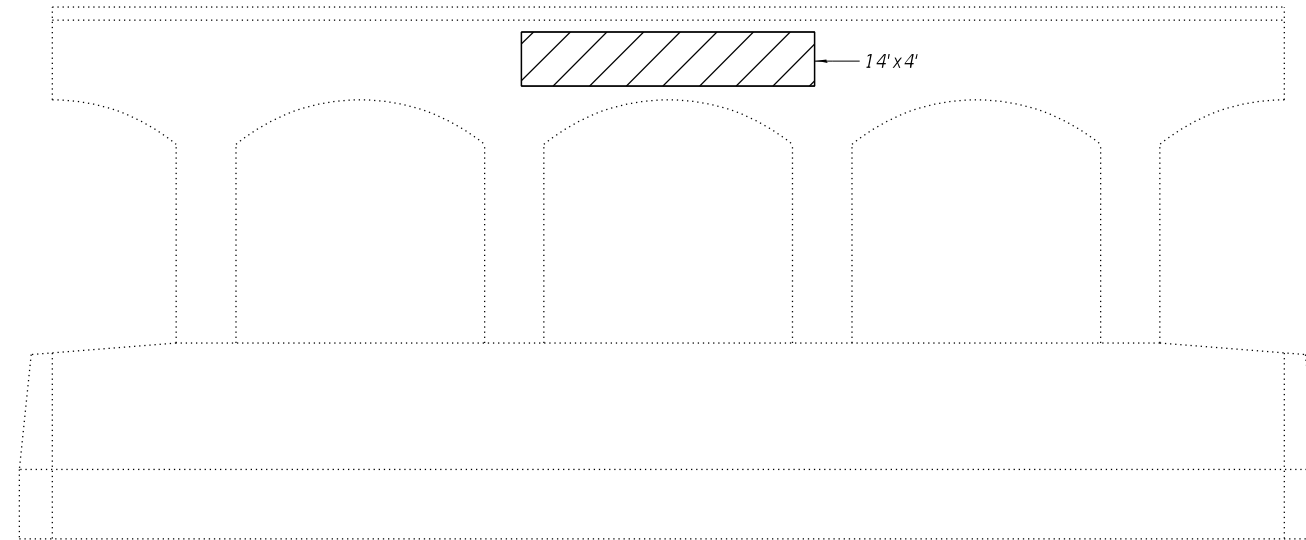
ITEM	UNIT	TOTAL
Fiber Wrap	Sq Ft	225
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft	108
Structural Repair Of Concrete (Depth Greater Than 5 Inches)	Sq Ft	60
Temporary Shoring and Cribbing	Each	2

MODEL: Default
FILE NAME: P:\P-20-20-4094-00_IDOT_Various Phase 2 (PTB 196 Item 16)\WO 33 62U49\DGNCADD_Sheets\62U49-SHT-STR-12_Pier2.dgn

MODEL: Default
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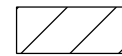


WEST ELEVATION
 Looking East



EAST ELEVATION
 Looking West

LEGEND



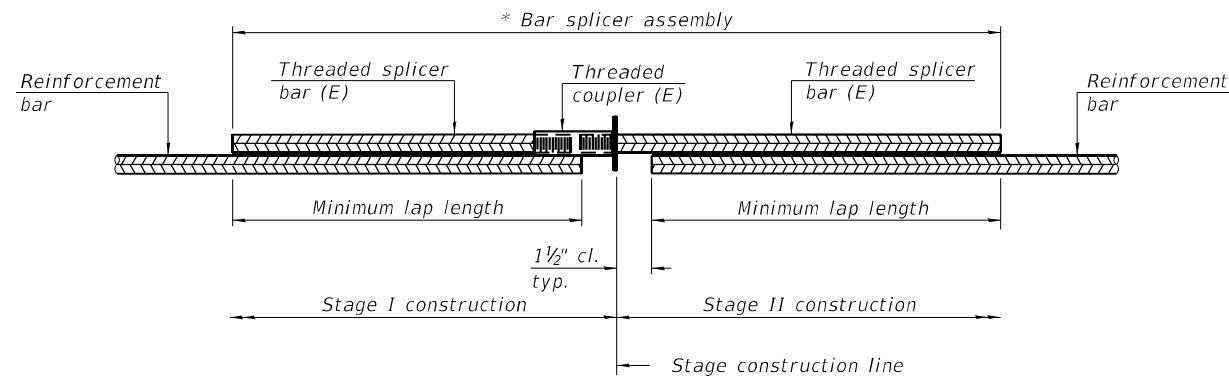
Indicates Structural Repair Of Concrete
 (Depth Equal To Or Less Than 5 Inches)

NOTE

- Quantities and limits of Structural Repair of Concrete shown herein are estimated from a survey completed by the District. Actual limits shall be determined in the field by the Engineer and noted in the As-Built Plans.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft	112

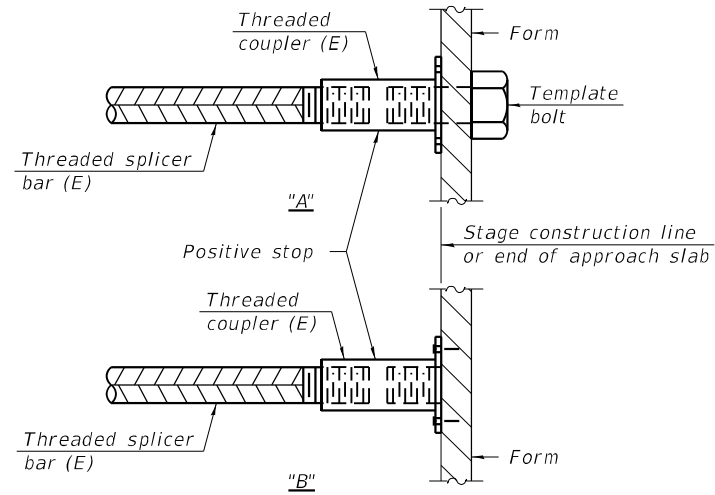


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

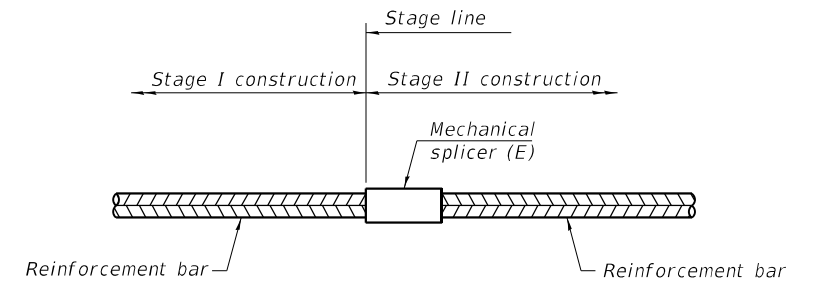
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Abutments	#6	8	4'-0"
Deck	#5	16	3'-6"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

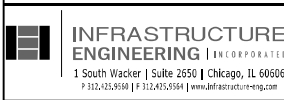
Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default
 FILE NAME: P:\P-20-20-4094-00 IDOT Various Phase 2 (PTB 196 Item 16)\WO 33 62U49\DGNCADD_Sheets\62U49-SHT-11_Spllices.dgn

BSD-1

1-1-2020



INFRASTRUCTURE
 ENGINEERING INCORPORATED
 1 South Wacker | Suite 2650 | Chicago, IL 60606
 P 312.425.9598 | F 312.425.9598 | www.infrastructure-eng.com

USER NAME = TYU
 DESIGNED - TY
 CHECKED - KPS
 PLOT SCALE = 0.1667' / in.
 DRAWN - TY
 PLOT DATE = 3/23/2023
 CHECKED - KPS

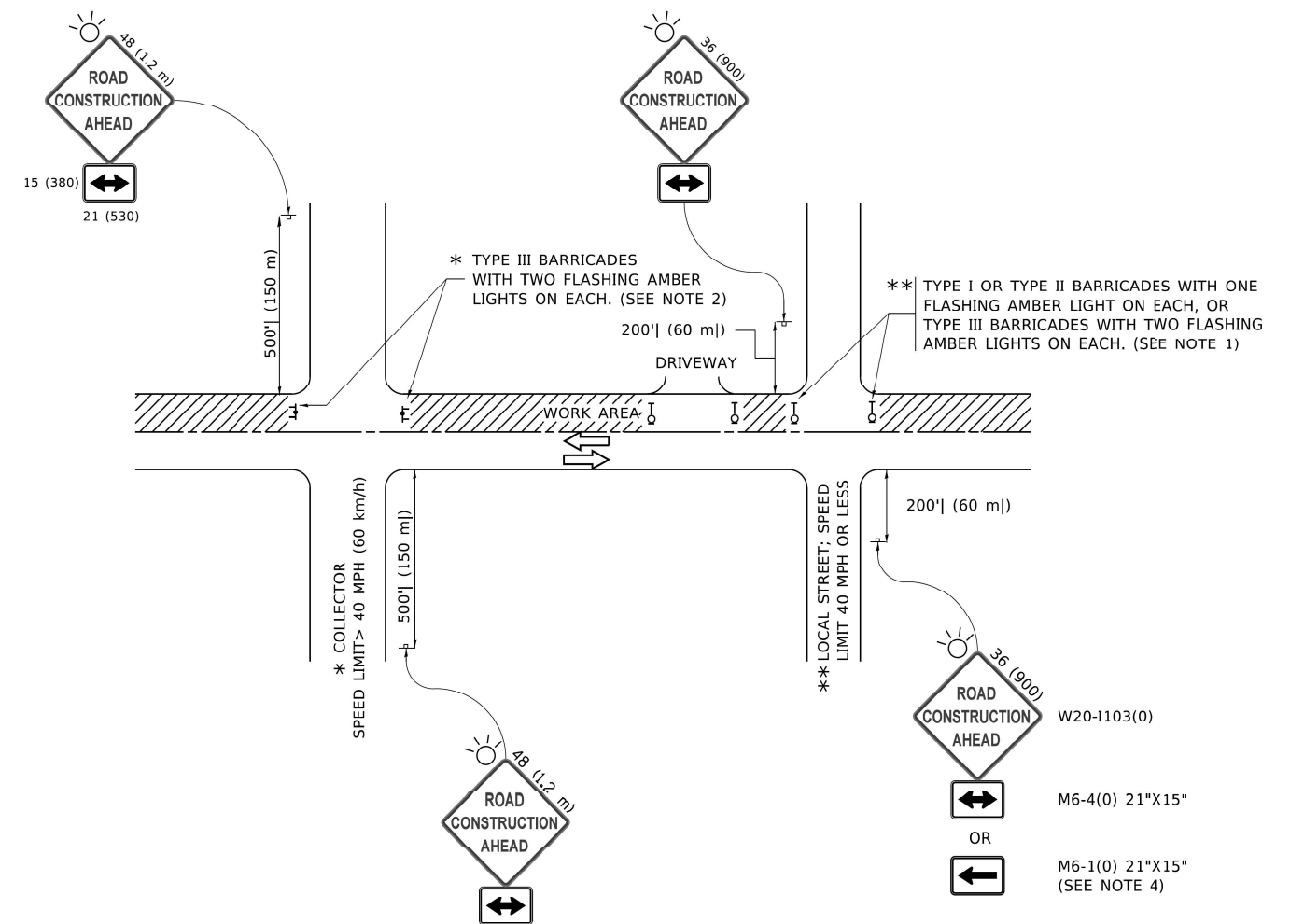
REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 STRUCTURE NO. 016-0908

SHEET 13 OF 13 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP-344-22-BR	COOK	23	21
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER.
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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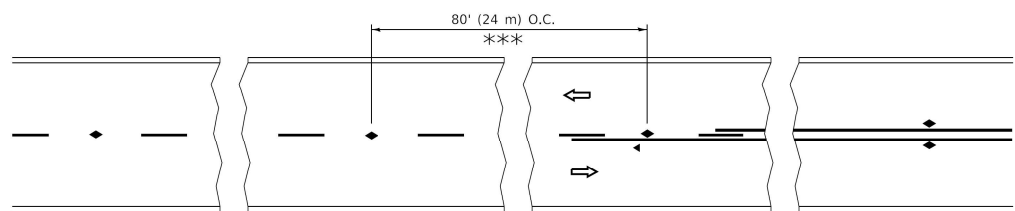
INFRASTRUCTURE ENGINEERING INCORPORATED 1 South Wacker Suite 2650 Chicago, IL 60606 P 312.425.9500 F 312.425.9594 www.infrastructure-eng.com	USER NAME = ALane	DESIGNED - CW	REVISED -
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	PLOT DATE = 3/16/2023	CHECKED - ACL	REVISED -
		DATE - 03/16/2023	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)

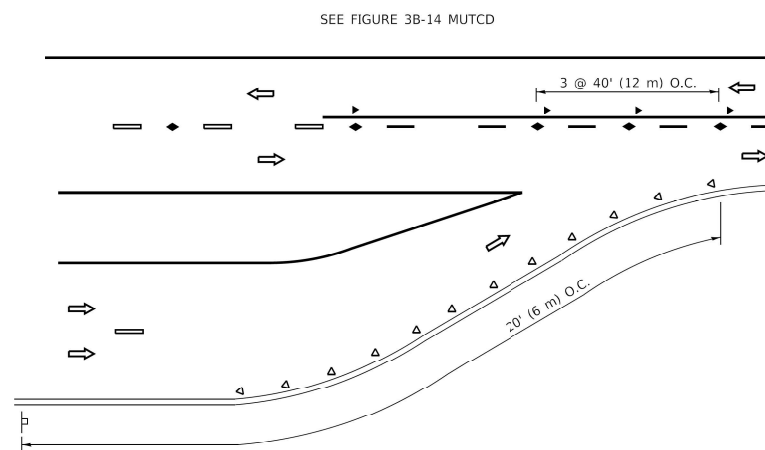
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CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				

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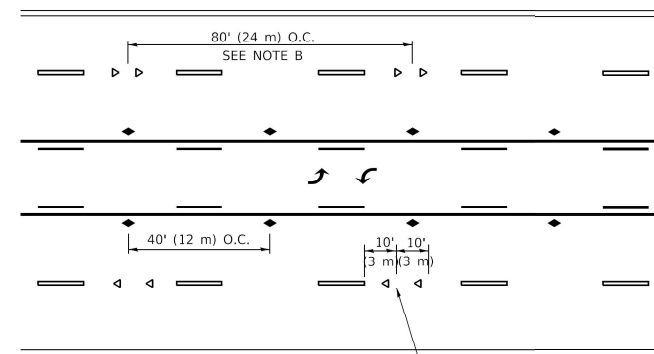


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

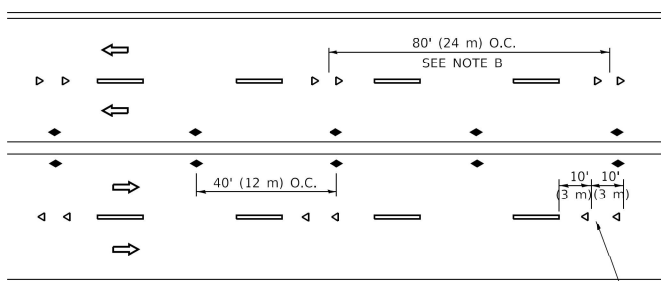
TWO-LANE/TWO-WAY



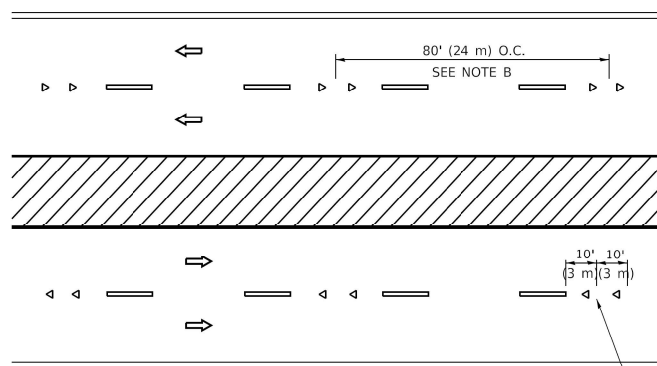
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

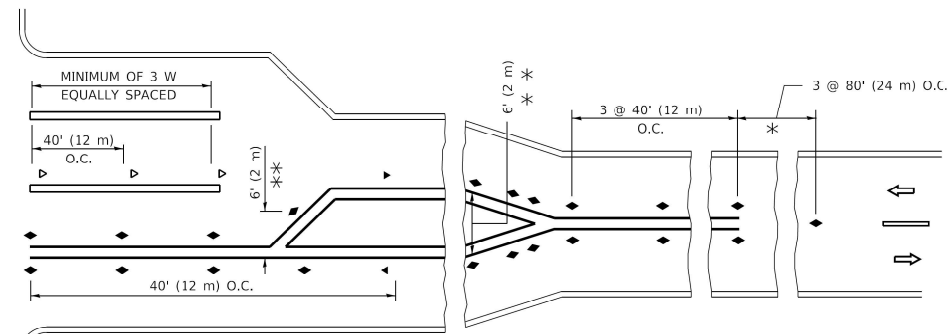
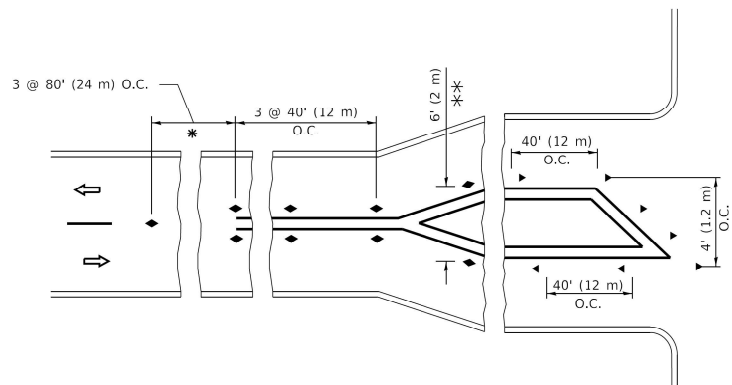
- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



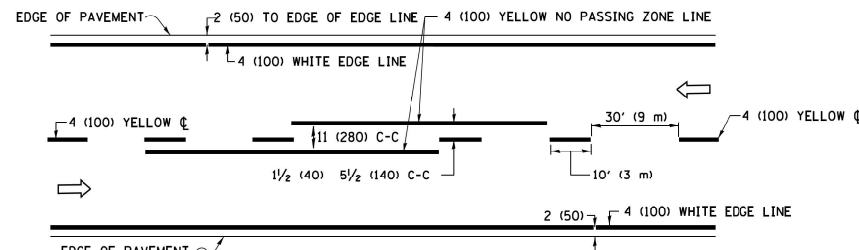
TURN LANES

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 *** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

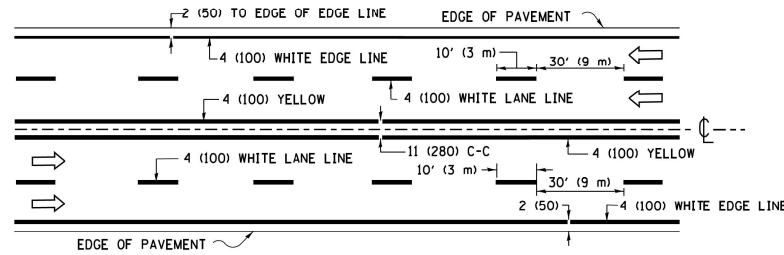
All dimensions are in inches (millimeters) unless otherwise shown.

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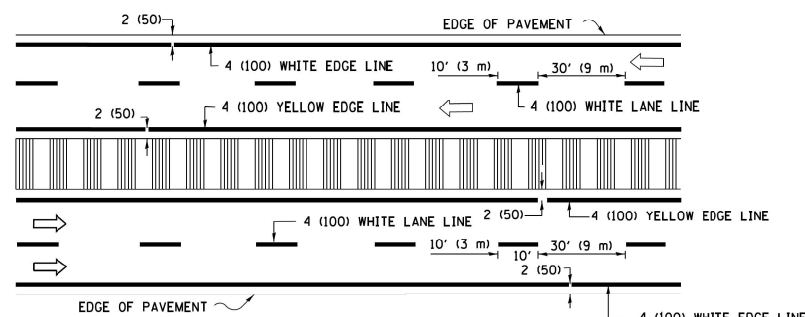
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2-LANE ROADWAY

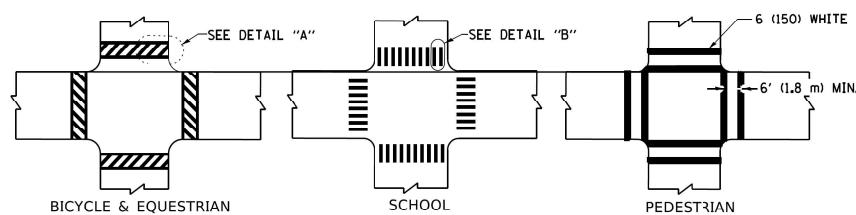


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

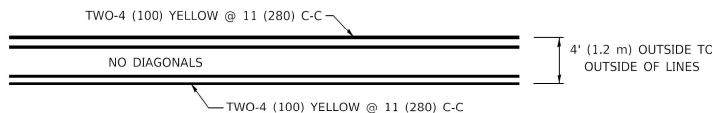


DETAIL "A"

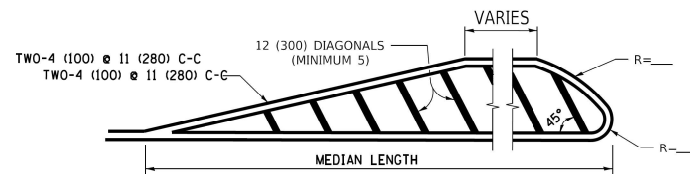
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

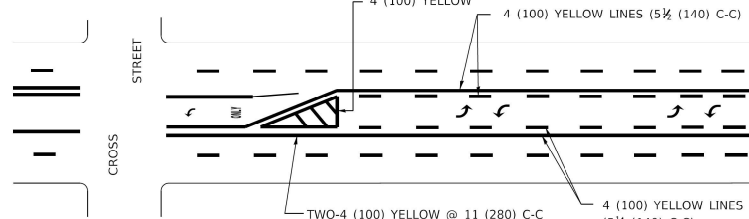


4' (1.2 m) WIDE MEDIANS ONLY



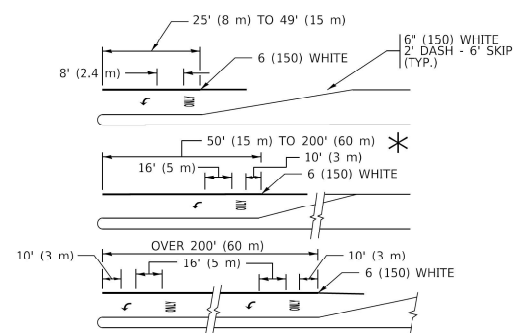
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



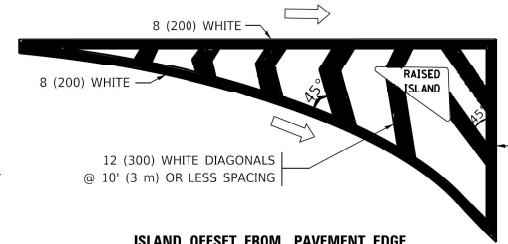
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

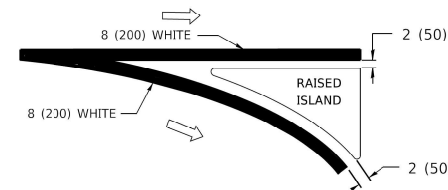


TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

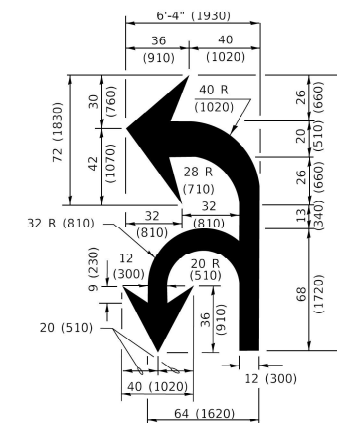


ISLAND OFFSET FROM PAVEMENT EDGE

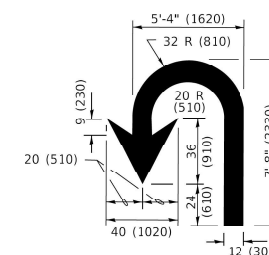


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK; IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R=3.6 SQ. FT. (0.33 m²) EACH *X=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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INFRASTRUCTURE ENGINEERING
1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.425.9598 | F 312.425.9594 | www.infrastructure-eng.com

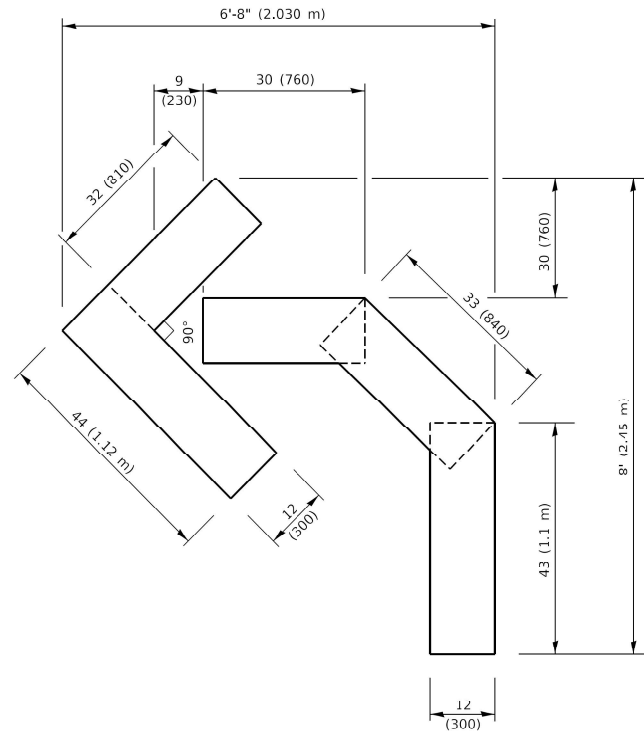
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	DATE - 01/27/2023	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE - TYPICAL PAVEMENT MARKINGS (TC-13)

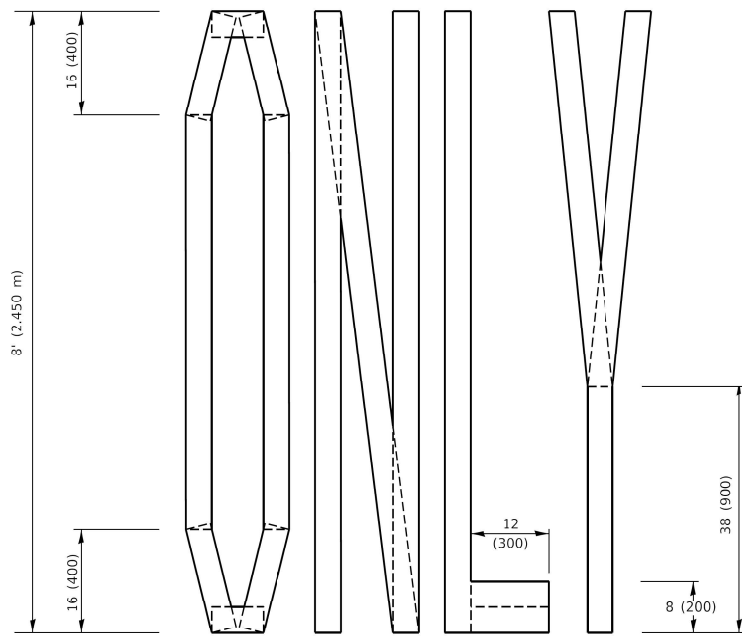
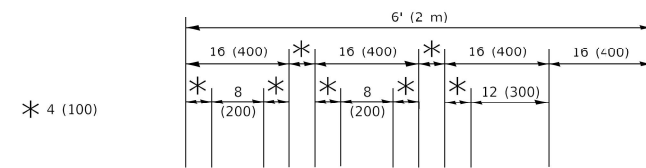
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	FAP 344 22 BR	COOK	23	22
CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				



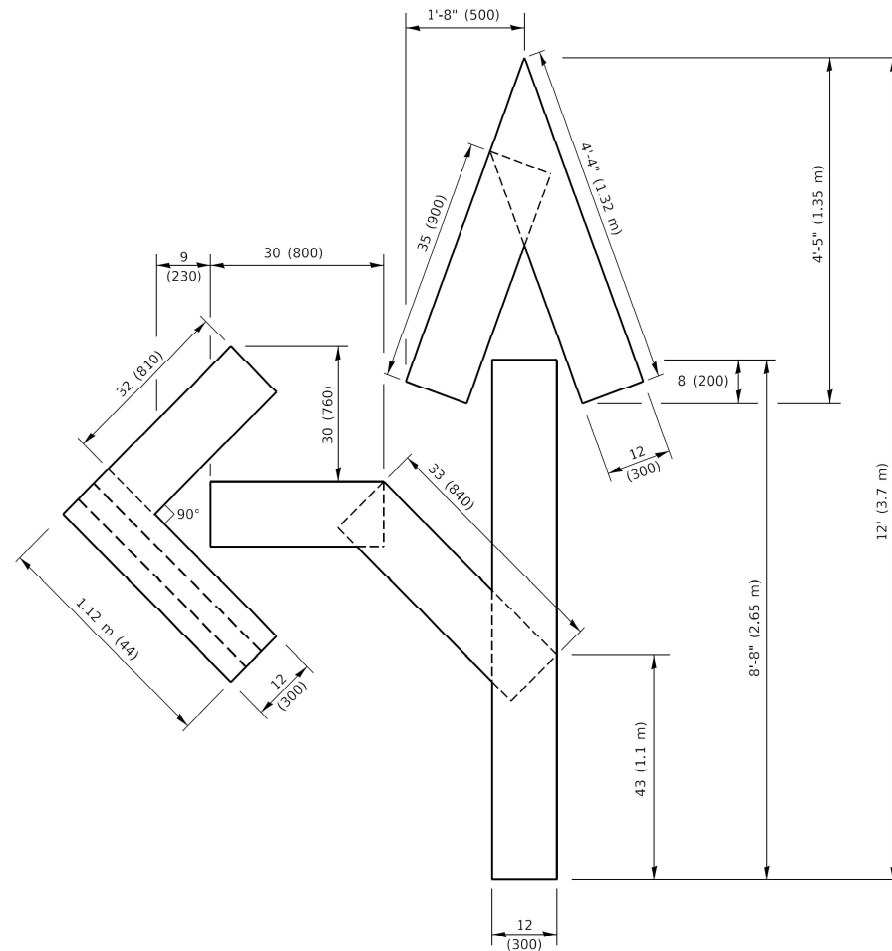
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

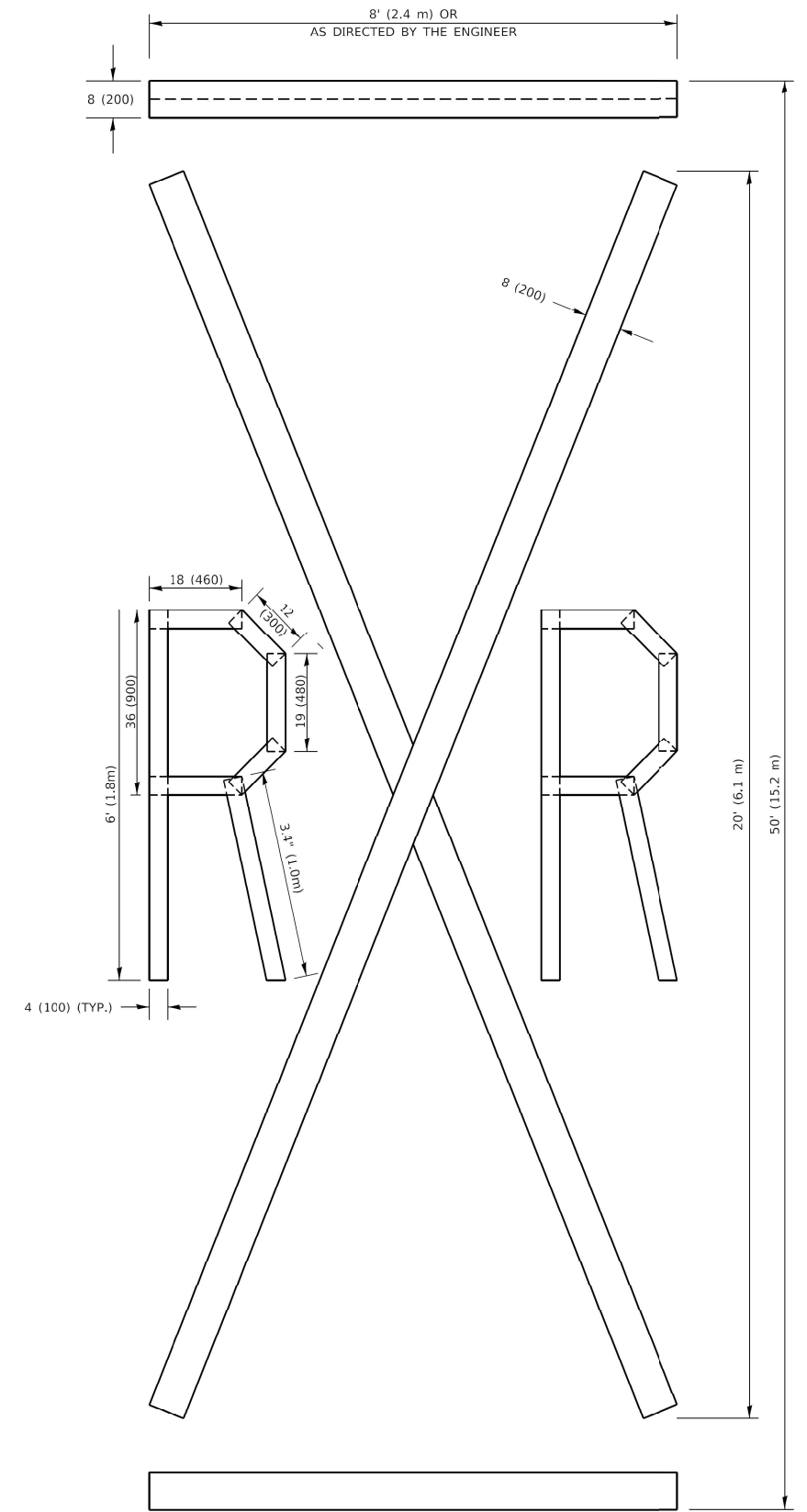


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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INFRASTRUCTURE ENGINEERING INCORPORATED
1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.425.9500 | F 312.425.9594 | www.infrastructure-eng.com

USER NAME = ALane	DESIGNED - CW	REVISD -
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	DATE - 03/24/2023	

DESIGNED - CW	REVISD -
DRAWN - CW	REVISD -
CHECKED - ACL	REVISD -
DATE - 03/24/2023	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

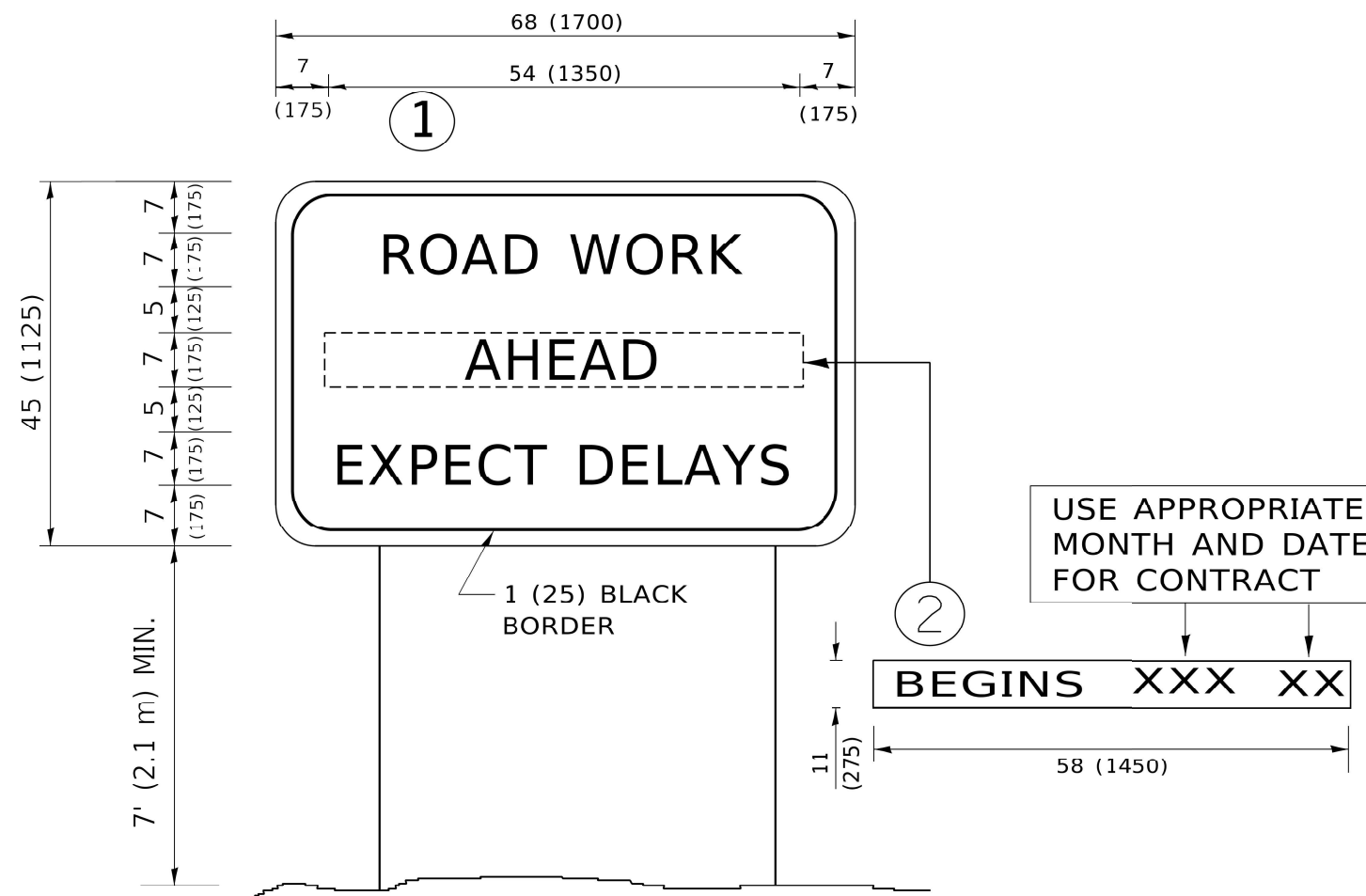
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CONTRACT NO. 62U49				
ILLINOIS FED. AID PROJECT				

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344	FAP 344 22 BR	COOK	23	23
ILLINOIS			FED. AID PROJECT	

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.