

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	1
ILLINOIS		CONTRACT NO. 62C43		

\*51+4: 55 total pages

D-91-350-16



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

FOR INDEX OF SHEETS, SEE SHEET NO. 2

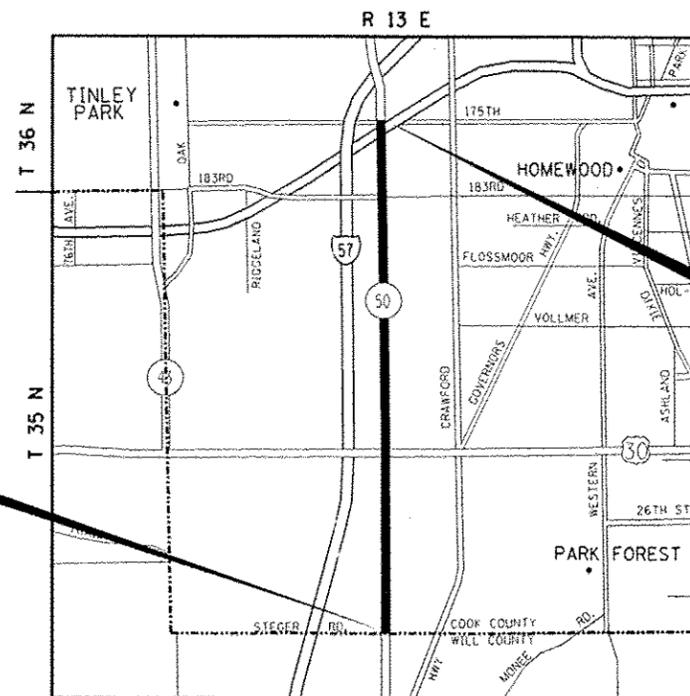
THE IMPROVEMENT IS LOCATED IN THE VILLAGES OF RICHTON PARK, MATTESON AND COUNTRY CLUB HILLS.

# PROPOSED HIGHWAY PLANS

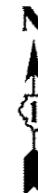
FAP 350: IL ROUTE 50 (CICERO AVE.)  
 175TH STREET TO STEGER ROAD  
 SECTION: (101-EXT.) RS-3  
 RESURFACING (3P); ADA RAMP  
 PROJECT NHPP-0350(041)

COOK COUNTY  
 C-91-350-16

TRAFFIC DATA:  
 2013 ADT = 20,800  
 POSTED SPEED LIMIT=45-50 MPH



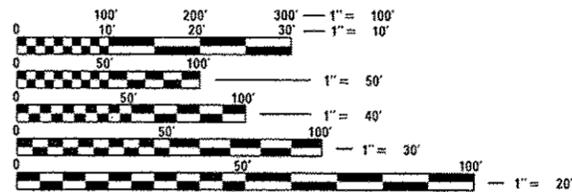
RICH AND BREMEN TOWNSHIPS



IMPROVEMENT ENDS  
 STA. 388 + 05

OMISSION:  
 STA. 227 + 78 TO 239 + 72

IMPROVEMENT BEGINS  
 STA. 17 + 74



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT MANAGER FAWAD AQUEEL (847) 705-4247  
 PROJECT ENGINEER JENPAI CHANG (847) 705-4432

CONTRACT NO. 62C43

GROSS LENGTH OF IMPROVEMENT = 37,031 LINEAL FT. = 7.0 MILES  
 NET LENGTH OF IMPROVEMENT = 35,837 LINEAL FT. = 6.79 MILES

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUBMITTED February 9, 2017  
*Anthony J. Borlase*  
 REGIONAL ENGINEER

Mar 24, 2017  
*Mareen M. Addis*  
 ENGINEER OF DESIGN AND ENVIRONMENT

Mar 24, 2017  
*Amelia*  
 DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY  
 OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

**HIGHWAY STANDARDS**

**GENERAL NOTES**

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES
3,3A,-5	SUMMARY OF QUANTITIES
6-10	TYPICAL SECTIONS
11-23	ROADWAY AND PAVEMENT MARKING PLANS
* 24-29	DETECTOR LOOP REPLACEMENT PLANS
30-33	SIGNAL MODIFICATION PLAN
34	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
35	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
36	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
37	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
38	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
39	TYPICAL APPLICATION FOR RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)
40	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
41	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
42	PAVEMENT MARKING LETTERS & SYMBOLS FOR TRAFFIC STAGING (TC-16)
43	ARTERIAL ROAD INFORMATION SIGN (TC-22)
44	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)
45-51	STANDARD TRAFFIC SIGNAL DESING DETAILS (TC-05)

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-03	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
604001-04	FRAMES AND LIDS TYPE 1
606001-06	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 2' FROM PAVEMENT EDGE
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS >= 45 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-08	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "G.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF RICHTON PARK, MATTESON AND COUNTRY CLUB HILLS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS. ALL DAMAGE TO F&L, REPLACING STRUCTURES IF DAMAGE OR IF SPECEFIED BY ENGINEER SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, IDOT'S AREA TRAFFIC FIELD ENGINEER FOR SOUTH COUNTY, VIA E-MAIL AT PATRICE.HARRIS@ILLINOIS.GOV AND AT (847) 705-4412, A MINIMUM, OF TWO (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

ALL SIDEWALK RAMPS WITHIN THE LIMITS OF THE PROJECT SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND THE APPLICABLE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.

ALL PROPOSED SIDE CURB QUANTITIES SHALL BE PAID FOR AS PCC SIDEWALK 5", UNLESS OTHERWISE NOTED ON PLANS.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION

\* includes 24A-C

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES  
IL ROUTE 50 (175TH STREET TO STEGER ROAD)

FILE NAME :	USER NAME : FUENTESJA	DESIGNED -	REVISED -	SCALE: 1" = 50'	SHEET OF SHEETS STA. TO STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
Default		CHECKED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	2	
		DATE -	REVISED -			CONTRACT NO. 62C43					
						ILLINOIS FED. AID PROJECT					

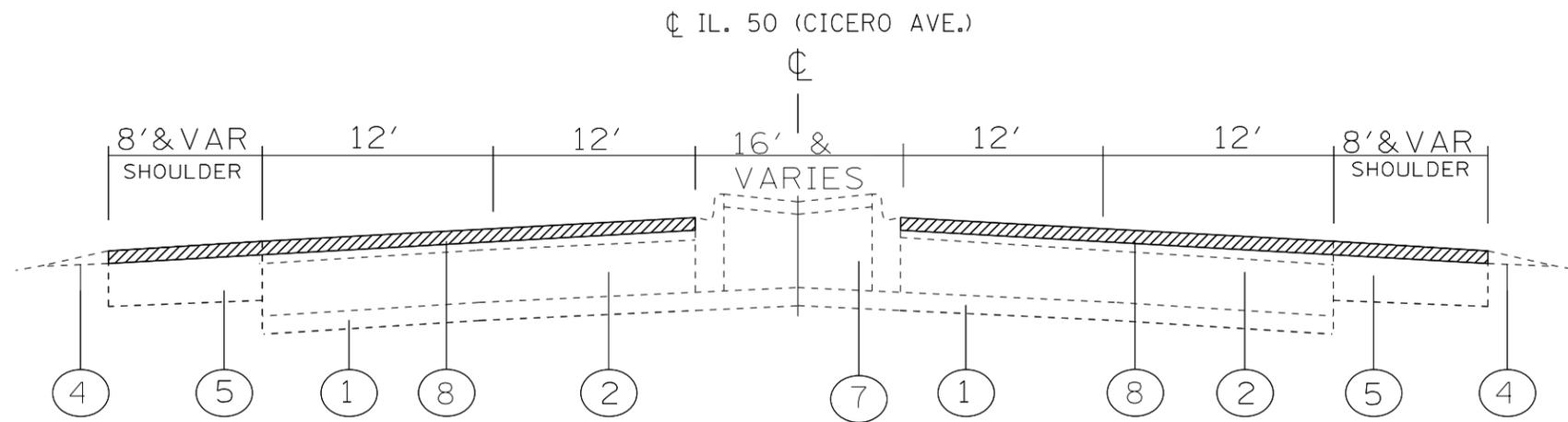




SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005	80% FED 20% STATE 0021					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005	80% FED 20% STATE 0021				
* 81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	419		419					* 87900200	DRILL EXISTING HANDHOLE	EACH	9		9				
* 81400100	HANDHOLE	EACH	1		1					* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4		4				
* 81400200	HEAVY-DUTY HANDHOLE	EACH	4		4					* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	6696		6696				
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1		1					* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	8		8				
* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1035		1035					* 89500200	RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2		2				
* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1370		1370					* 89502200	MODIFY EXISTING CONTROLLER	EACH	1		1				
* 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1330		1330					* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	3420		3420				
* 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1370		1370					* 89502350	REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	2390		2390				
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	875		875					* 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1		1				
* 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	500		500					* 89502376	REBUILD EXISTING HANDHOLE	EACH	2		2				
										* 89502380	REMOVE EXISTING HANDHOLE	EACH	4		4				
										X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1					
										X2020110	GRADING AND SHAPING SHOULDERS	UNIT	618	618					
																			* SPECIALTY ITEMS

10





**EXISTING TYPICAL CROSS SECTION**

STA. 17+74 TO STA. 72+13  
 STA. 89+08 TO STA. 105+18  
 STA. 141+00 TO STA. 152+83  
 STA. 215+36 TO STA. 283+63  
 STA. 384+00 TO STA. 388+05

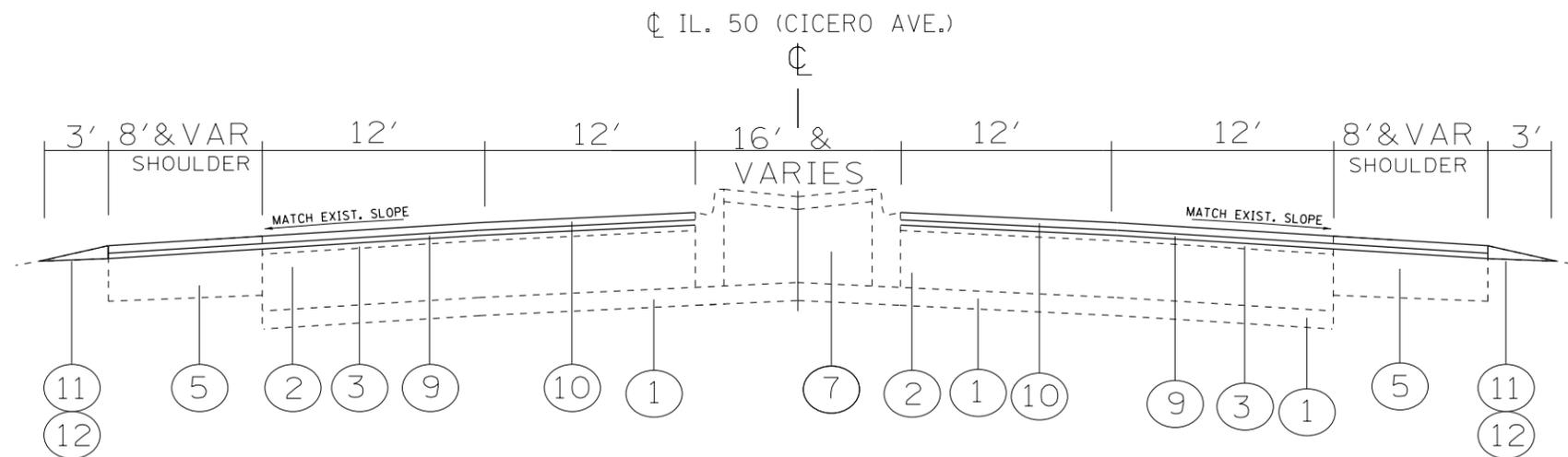
**LEGEND**

- ① EXISTING STABILIZED SUB-BASE
- ② EXISTING P.C.C PAVEMENT, ± 10"
- ③ EXISTING HMA SURFACE OVERLAY AFTER MILLING, ± 2"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING HMA SHOULDER
- ⑥ EXISTING COMB. CONCRETE CURB AND GUTTER
- ⑦ EXISTING CONCRETE BARRIER MEDIAN
- ⑧ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑩ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑫ PROPOSED GRADING AND SHAPING SHOULDERS

**NOTES:**

FOR THE EXISTING HMA SURFACE, THE CONTRACTOR SHALL MILL THE PAVEMENT FIRST, THEN PATCHING PER BD-22 DETAIL.

LOCATION OF COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.



**PROPOSED TYPICAL CROSS SECTION**

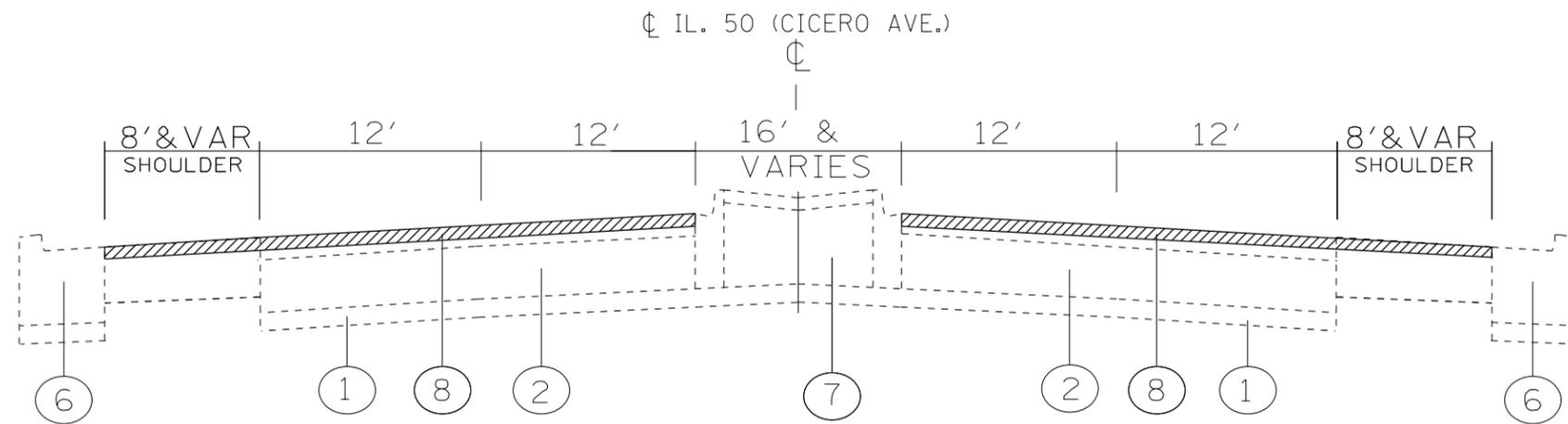
STA. 17+74 TO STA. 72+13  
 STA. 89+08 TO STA. 105+18  
 STA. 141+00 TO STA. 152+83  
 STA. 215+36 TO STA. 283+63  
 STA. 384+00 TO STA. 388+05

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE USES	DESIGN AIR VOIDS @ N <sub>DES</sub>	QMP
<b>MAINLINE PAVEMENT &amp; SHOULDER RESURFACING</b>		
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP
POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, (IL-9.5 mm)	4% AT 70 GYR.	PPF
<b>PAVEMENT PATCHING</b>		
CLASS D PATCH (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PPF)		

**NOTES:**

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURES QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.  
 FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.  
 QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

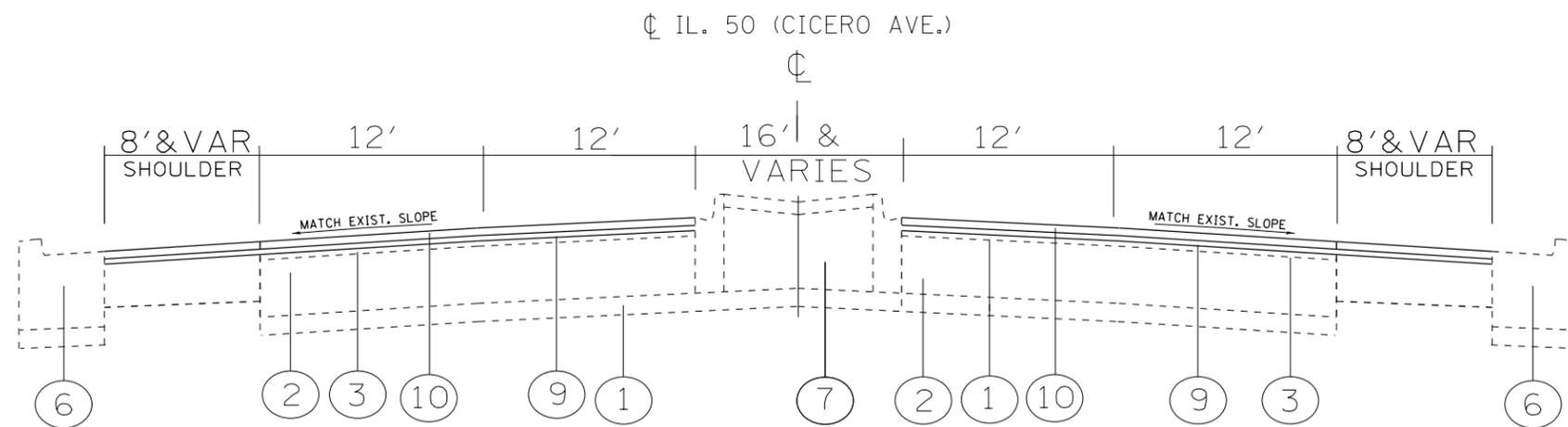


**EXISTING TYPICAL CROSS SECTION**

STA. 72+13 TO STA. 78+52  
 STA. 168+00 TO STA. 197+60

**LEGEND**

- ① EXISTING STABILIZED SUB-BASE
- ② EXISTING P.C.C PAVEMENT, ± 10"
- ③ EXISTING HMA SURFACE OVERLAY AFTER MILLING, ± 2"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING HMA SHOULDER
- ⑥ EXISTING COMB. CONCRETE CURB AND GUTTER
- ⑦ EXISTING CONCRETE BARRIER MEDIAN
- ⑧ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑩ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑫ PROPOSED GRADING AND SHAPING SHOULDERS



**PROPOSED TYPICAL CROSS SECTION**

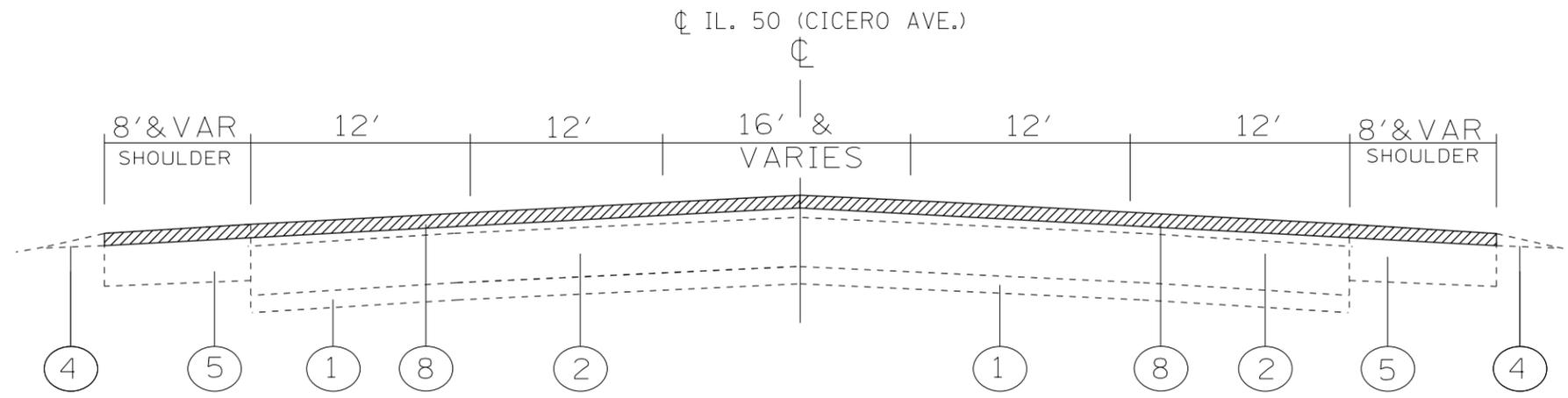
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**NOTES:**

FOR THE EXISTING HMA SURFACE, THE CONTRACTOR SHALL MILL THE PAVEMENT FIRST, THEN PATCHING PER BD-22 DETAIL.

LOCATION OF COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS IL ROUTE 50 (STEGER RD. TO 175TH ST.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI350\BROWNS\Design\DI35016-sh-t-plan.dgn		REVISIONS	REVISIONS			350	(101-EXT.) RS-3	COOK	51	7
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISIONS	REVISIONS			CONTRACT NO. 62C43				
PLOT DATE = 2/16/2017	DATE -	REVISIONS	REVISIONS			ILLINOIS FED. AID PROJECT				
					SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.



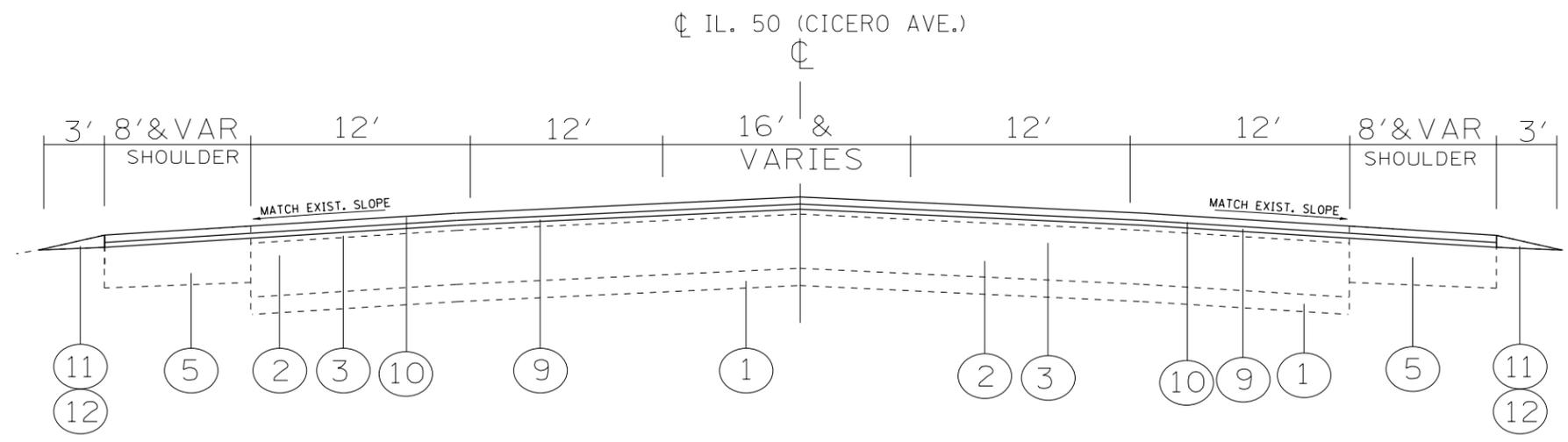
**EXISTING TYPICAL CROSS SECTION**  
STA. 283+63 TO STA. 384+00

**LEGEND**

- ① EXISTING STABILIZED SUB-BASE
- ② EXISTING P.C.C PAVEMENT, ± 10"
- ③ EXISTING HMA SURFACE OVERLAY AFTER MILLING, ± 2"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING HMA SHOULDER
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- ⑦ EXISTING CONCRETE BARRIER MEDIAN
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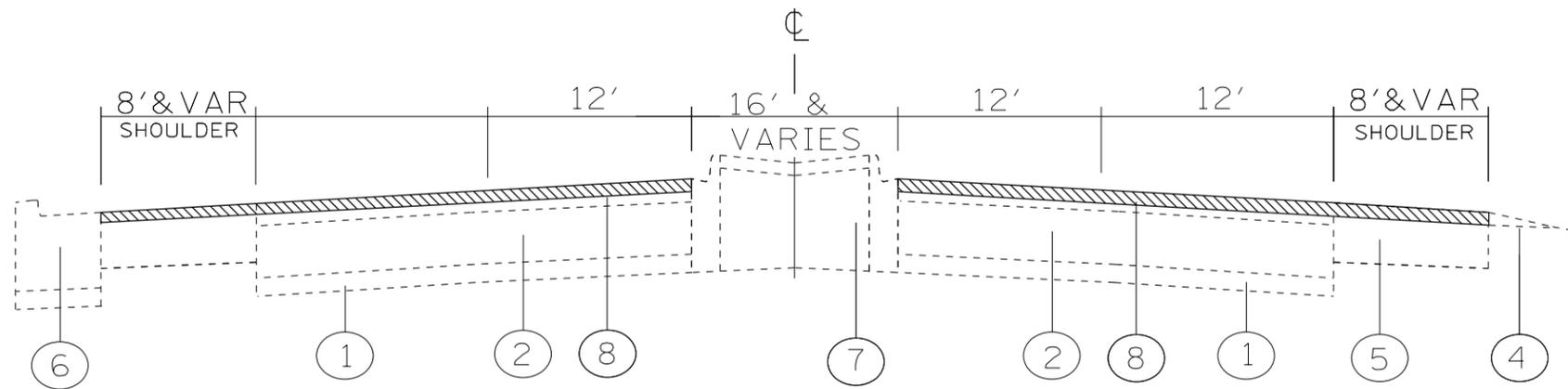
**NOTES:**

FOR THE EXISTING HMA SURFACE, THE CONTRACTOR SHALL MILL THE PAVEMENT FIRST, THEN PATCHING PER BD-22 DETAIL.  
LOCATION OF COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.



**PROPOSED TYPICAL CROSS SECTION**  
STA. 283+63 TO STA. 384+00

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p:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI350\Drawings\Design\DI35016-sh-t-plan.dgn		CHECKED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	8
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**EXISTING TYPICAL CROSS SECTION**

STA. 105+18 TO STA. 127+73  
 STA. 162+00 TO STA. 168+00

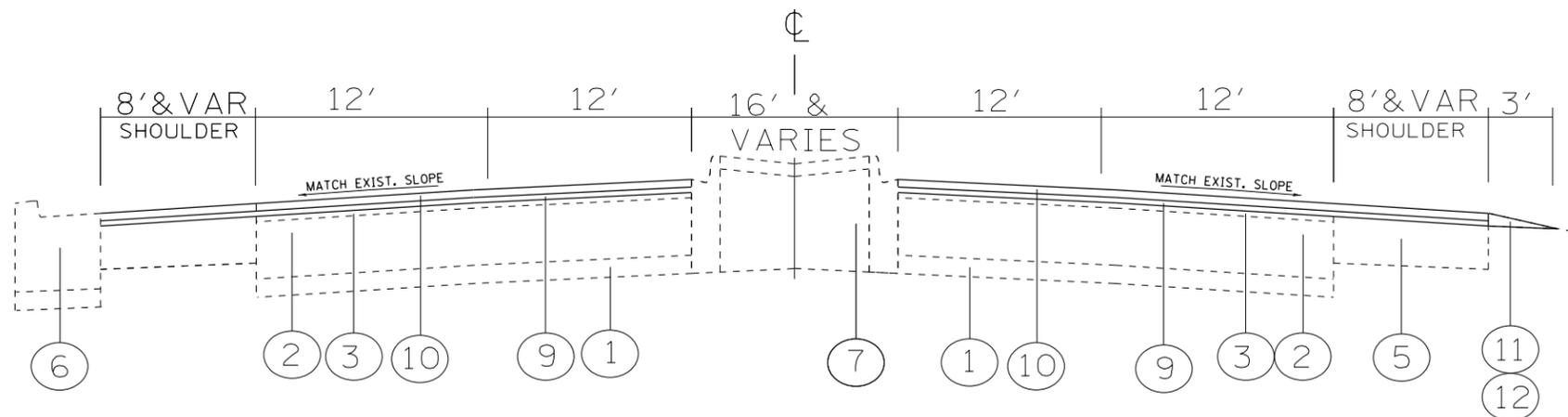
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**NOTES:**

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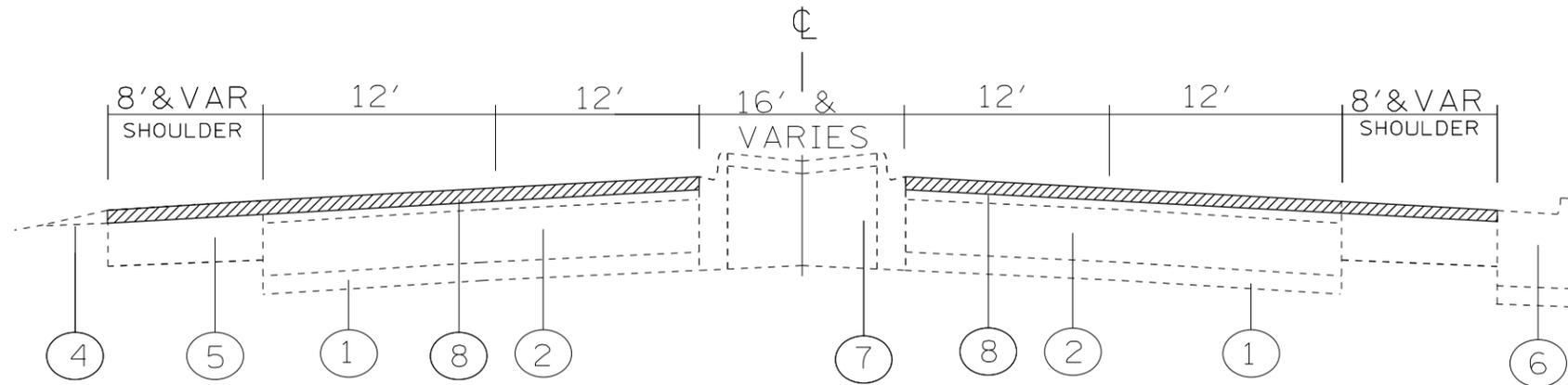
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**PROPOSED TYPICAL CROSS SECTION**

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PLOT DATE = 2/16/2017						ILLINOIS FED. AID PROJECT				
						SCALE:	SHEET NO. OF SHEETS	STA. TO STA.		



**EXISTING TYPICAL CROSS SECTION**

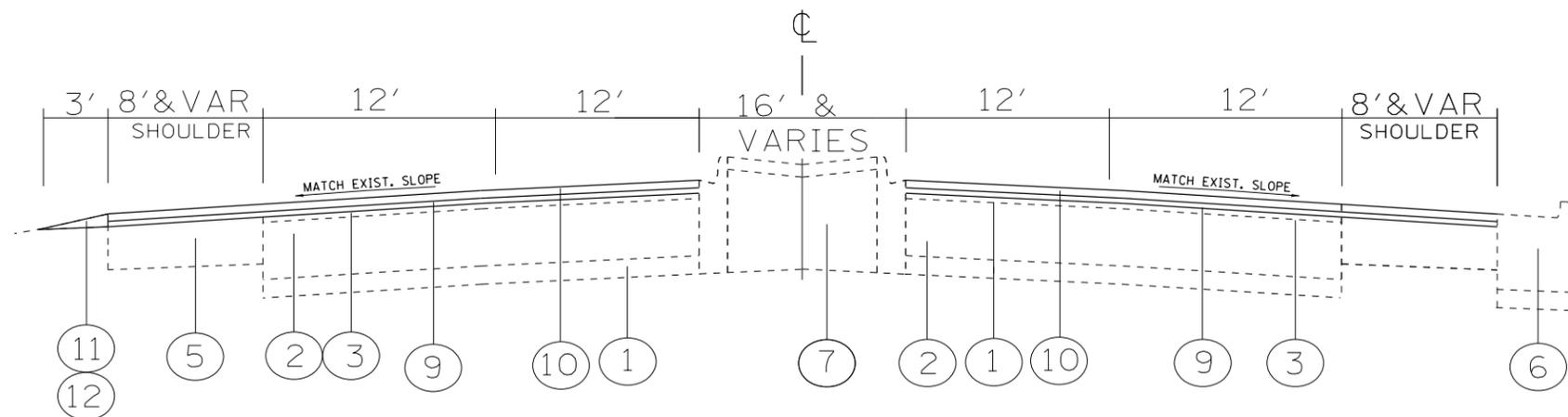
STA. 78+52 TO STA. 89+08  
 STA. 127+73 TO STA. 141+00  
 STA. 152+83 TO STA. 162+00  
 STA. 197+60 TO STA. 215+36

**LEGEND**

- ① EXISTING STABILIZED SUB-BASE
- ② EXISTING P.C.C PAVEMENT, ± 10"
- ③ EXISTING HMA SURFACE OVERLAY AFTER MILLING, ± 2"
- ④ EXISTING AGGREGATE SHOULDER
- ⑤ EXISTING HMA SHOULDER
- ⑥ EXISTING COMB. CONCRETE CURB AND GUTTER
- ⑦ EXISTING CONCRETE BARRIER MEDIAN
- ⑧ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑨ PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑩ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- ⑪ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑫ PROPOSED GRADING AND SHAPING SHOULDERS

**NOTES:**

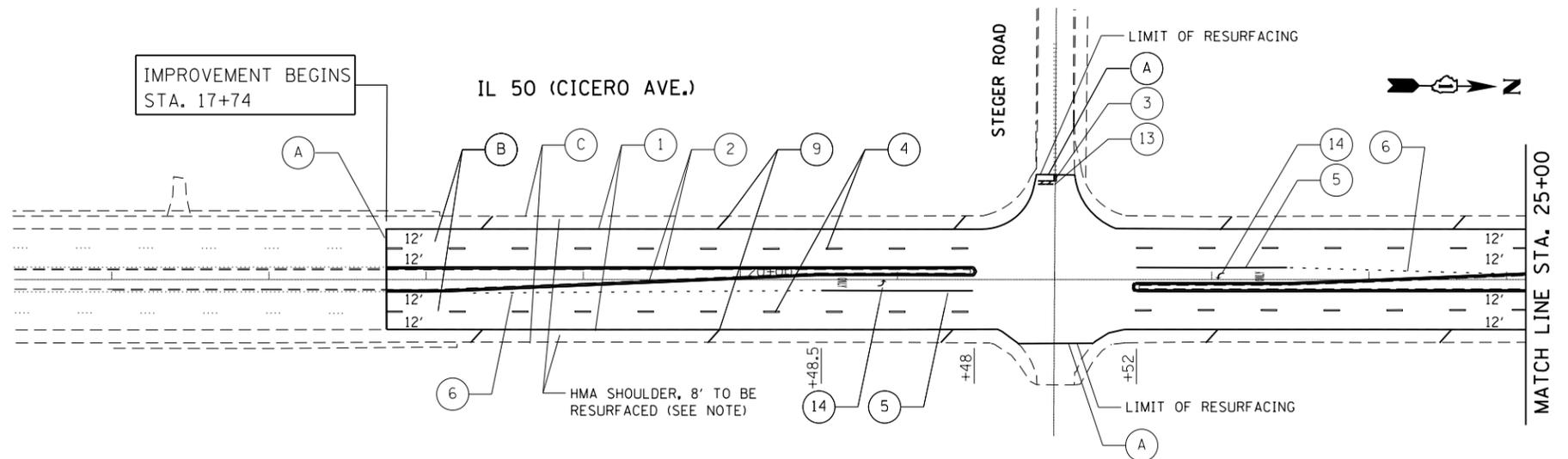
FOR THE EXISTING HMA SURFACE, THE CONTRACTOR SHALL MILL THE PAVEMENT FIRST, THEN PATCHING PER BD-22 DETAIL.  
 LOCATION OF COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER.



**PROPOSED TYPICAL CROSS SECTION**

STA. 78+52 TO STA. 89+08  
 STA. 127+73 TO STA. 141+00  
 STA. 152+83 TO STA. 162+00  
 STA. 197+60 TO STA. 215+36

FILE NAME = p:\IL\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI350\Drawings\Design\DI35016-sht-plen.dgn	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS IL ROUTE 50 (STEGER RD. TO 175TH ST.)</b>	F.A.P. RTE. = 350	SECTION = (101-EXT.) RS-3	COUNTY = COOK	TOTAL SHEETS = 51	SHEET NO. = 10
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 62C43	
PLOT DATE = 2/16/2017	DATE -	REVISED -	REVISED -							



**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
| ② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIANS LINE, YELLOW (TYP.)           | ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SHOULDER DIAGONALS @ 45°, WHITE (TYP.), 150' SPACING |
| ③ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW (TYP.)          | ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" MEDIAN DIAGONALS @ 45°, YELLOW (TYP.)                |
| ④ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" LANE LINE @ 30' SKIP & 10' DASH, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" LONGITUDINAL BARS (SCHOOL) @ 90°, WHITE (TYP.)       |
| ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" TURN LANE, WHITE (TYP.)                       | ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL (BIKE & EQUESTRIAN) @ 45°, WHITE (TYP.)     |
| ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED @ 6' SKIP & 2' DASH, WHITE (TYP.)      | ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" STOP BAR, WHITE (TYP.)                               |
| ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, WHITE (TYP.)                                |

**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5" (TYP.)
- Ⓑ PROP. HMA SURFACE REMOVAL, 2 1/2"  
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- Ⓒ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B  
PROP. GRADING AND SHAPING SHOULDERS

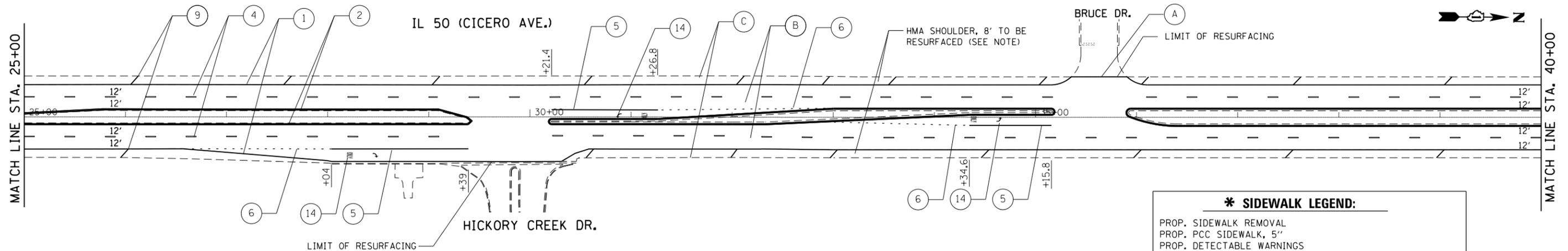
**PLAN NOTES**

ALL PAVEMENT MARKINGS OBLITERATED BY MILLING WITHIN PROJECT LIMITS ARE TO BE REPLACED ACCORDING TO THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL (TC-13).

RAISED REFLECTIVE PAVEMENT MARKERS ARE TO BE INSTALLED THROUGHOUT PROJECT LIMITS ACCORDING TO "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) DETAIL (TC-11).

**NOTE FOR RESURFACING OF HMA SHOULDERS:**

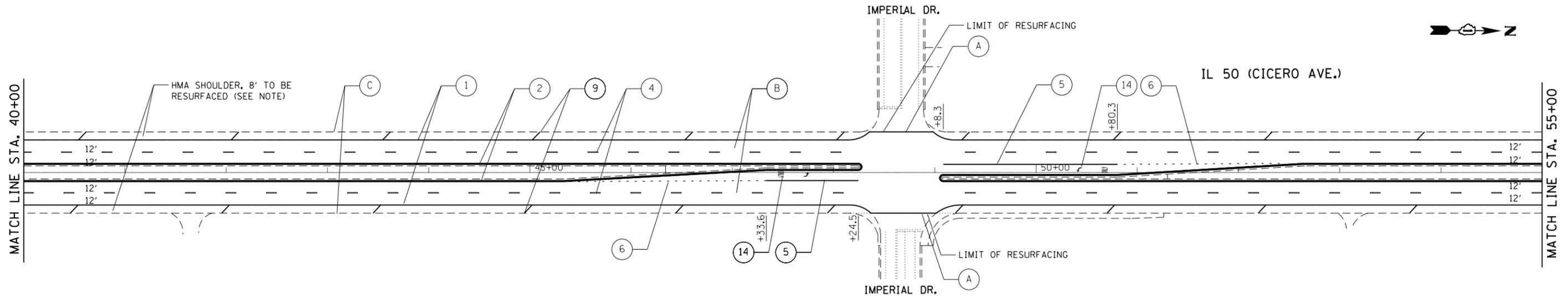
PROP. HMA SURFACE REMOVAL, 2 1/2"  
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"



**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
- PROP. SODDING, SALT TOLERANT
- PROP. NITROGEN FERTILIZER NUTRIENT
- PROP. POTASSIUM FERTILIZER NUTRIENT
- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI350\BROWNS\Design\DI35016-sh1-plan.dgn	PLotted SCALE = 100.0000' / in.	CHECKED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	11	
Default	PLOT DATE = 2/16/2017	DATE -	REVISED -			CONTRACT NO. 62C43					
						SCALE:	SHEET OF SHEETS		STA. TO STA.	ILLINOIS FED. AID PROJECT	



**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
| ② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIANS LINE, YELLOW (TYP.)           | ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SHOULDER DIAGONALS @ 45°, WHITE (TYP.), 150' SPACING |
| ③ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW (TYP.)          | ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" MEDIAN DIAGONALS @ 45°, YELLOW (TYP.)                |
| ④ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" LANE LINE @ 30' SKIP & 10' DASH, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" LONGITUDINAL BARS (SCHOOL) @ 90°, WHITE (TYP.)       |
| ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" TURN LANE, WHITE (TYP.)                       | ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL (BIKE & EQUESTRIAN) @ 45°, WHITE (TYP.)     |
| ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED @ 6' SKIP & 2' DASH, WHITE (TYP.)      | ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" STOP BAR, WHITE (TYP.)                               |
| ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, WHITE (TYP.)                                |

**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
- Ⓑ PROP. HMA SURFACE REMOVAL, 2 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- Ⓒ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B PROP. GRADING AND SHAPING SHOULDERS

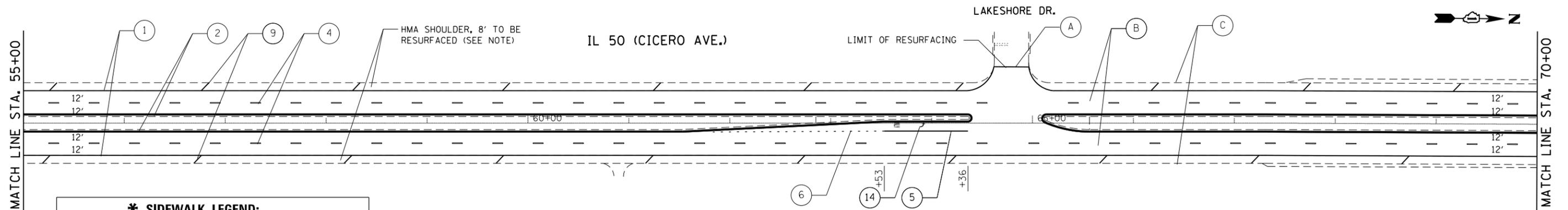
**PLAN NOTES**

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**NOTE FOR RESURFACING OF HMA SHOULDERS:**

PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"



**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
- PROP. SODDING, SALT TOLERANT
- PROP. NITROGEN FERTILIZER NUTRIENT
- PROP. POTASSIUM FERTILIZER NUTRIENT
- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B

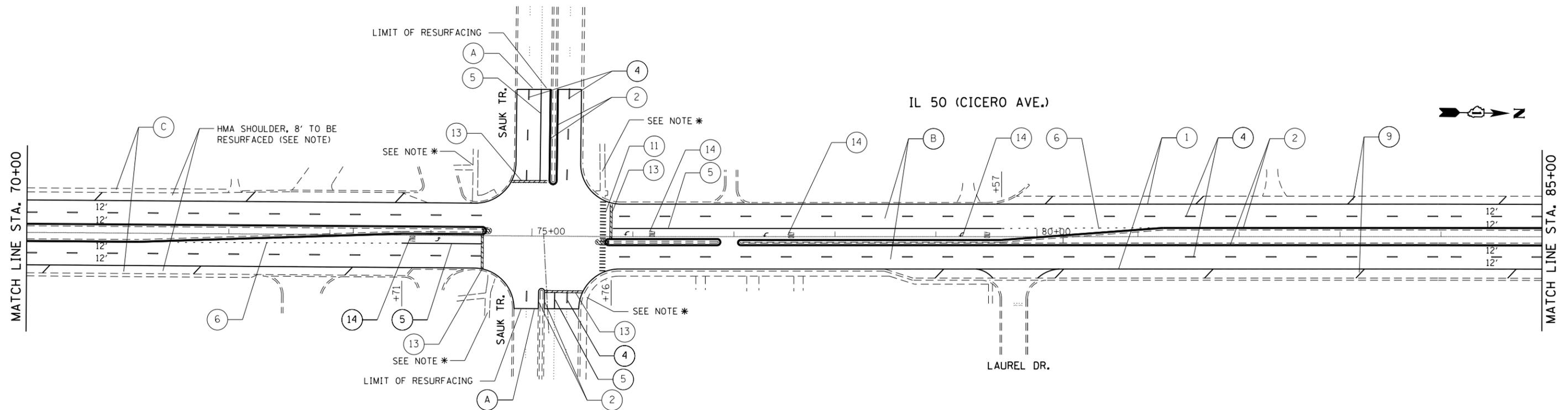
FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/16/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
 ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	12
CONTRACT NO. 62C43				
ILLINOIS FED. AID PROJECT				



**\* SIDEWALK LEGEND:**

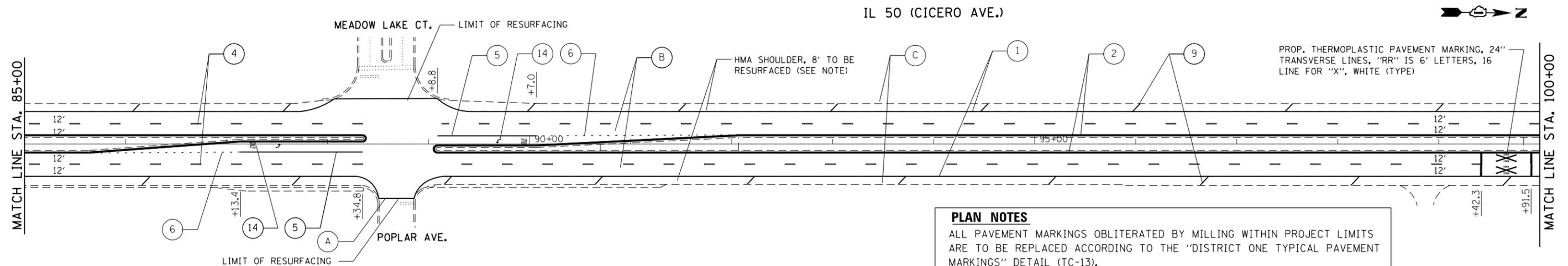
- PROP. SIDEWALK REMOVAL
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- PROP. EARTH EXCAVATION
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- PROP. POTASSIUM FERTILIZER NUTRIENT
- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B

**PAVEMENT MARKING LEGEND**

- ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIANS LINE, YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" LANE LINE @ 30' SKIP & 10' DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED @ 6' SKIP & 2' DASH, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.)
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**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
- Ⓑ PROP. HMA SURFACE REMOVAL, 2 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- Ⓒ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B PROP. GRADING AND SHAPING SHOULDERS



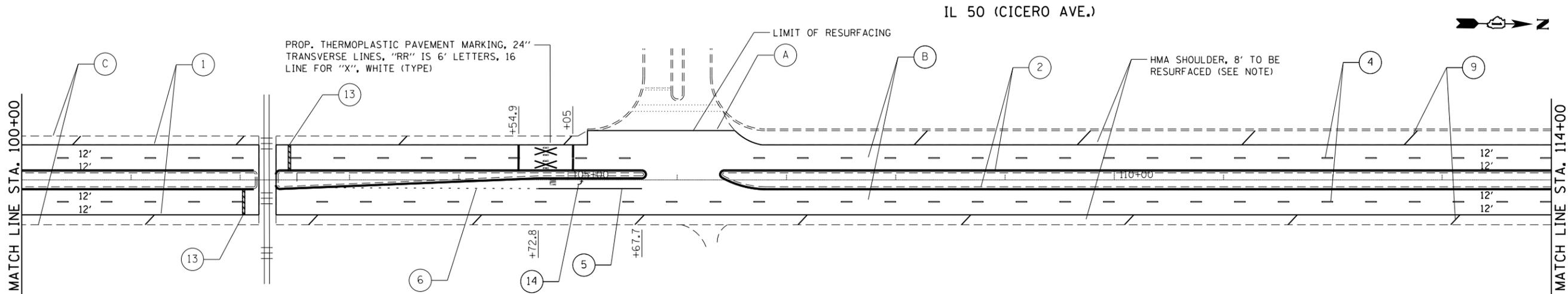
**PLAN NOTES**

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**NOTE FOR RESURFACING OF HMA SHOULDERS:**

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 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
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FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI35016\Design\DI35016-shr-plan.dgn		CHECKED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	13	
Default		DATE -	REVISED -			CONTRACT NO. 62C43					
	PLOT DATE = 2/16/2017					ILLINOIS FED. AID PROJECT					



**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
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**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
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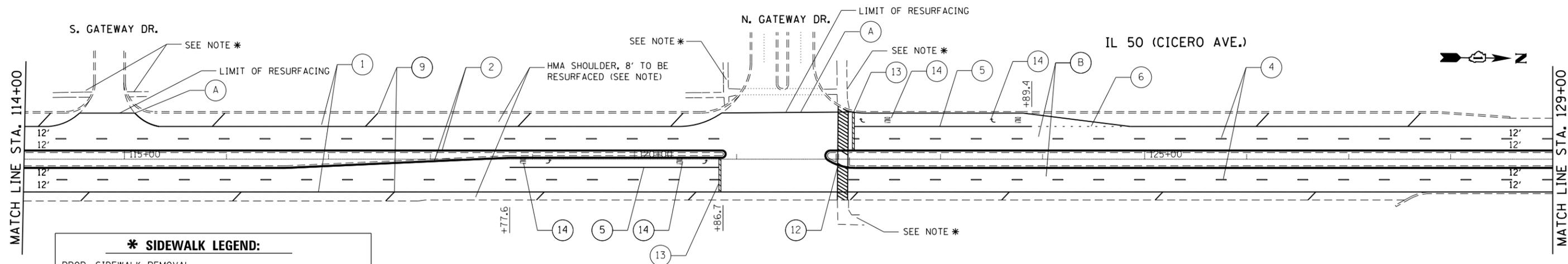
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**NOTE FOR RESURFACING OF HMA SHOULDERS:**

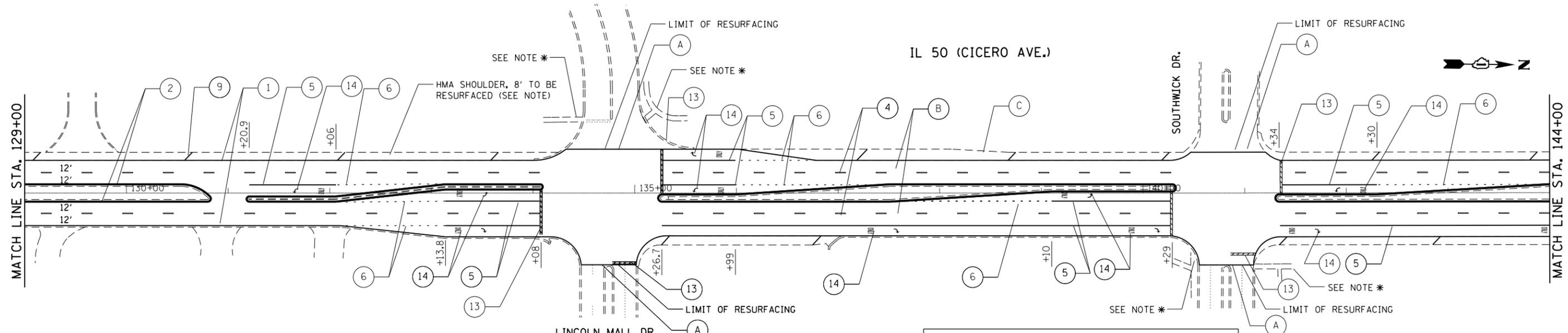
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**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
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- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B

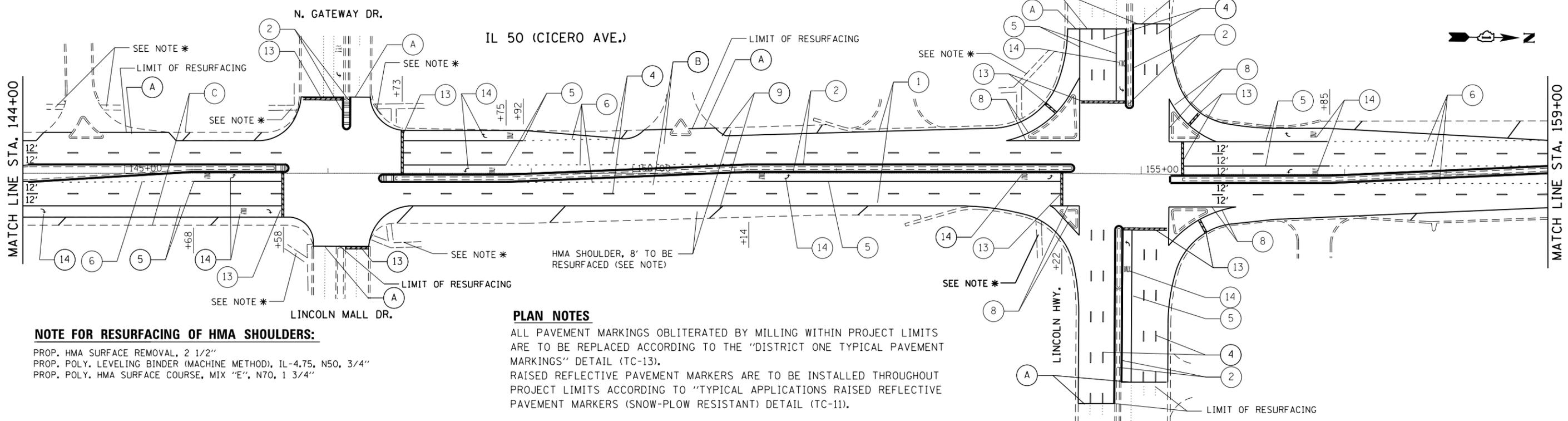
FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBID\INTEG\illinois.gov\PIW00T\Documents\DOT Offices\District 1\Projects\DI350\Design\DI35016-sh1-plan.dgn	PLotted SCALE = 100.0000' / 1" =	CHECKED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	14	
Default	PLOT DATE = 2/16/2017	DATE -	REVISED -			CONTRACT NO. 62C43					
						SCALE:	SHEET OF SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT



- PAVEMENT MARKING LEGEND**
- ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)
  - ② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIANS LINE, YELLOW (TYP.)
  - ③ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
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  - ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED @ 6" SKIP & 2" DASH, WHITE (TYP.)
  - ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.)
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  - ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" MEDIAN DIAGONALS @ 45°, YELLOW (TYP.)
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  - ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, WHITE (TYP.)

- \* SIDEWALK LEGEND:**
- PROP. SIDEWALK REMOVAL
  - PROP. PCC SIDEWALK, 5"
  - PROP. DETECTABLE WARNINGS
  - PROP. EARTH EXCAVATION
  - PROP. TOPSOIL FURNISH AND PLACE, 4"
  - PROP. SODDING, SALT TOLERANT
  - PROP. NITROGEN FERTILIZER NUTRIENT
  - PROP. POTASSIUM FERTILIZER NUTRIENT
  - PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
  - PROP. CONCRETE CURB, TYPE B

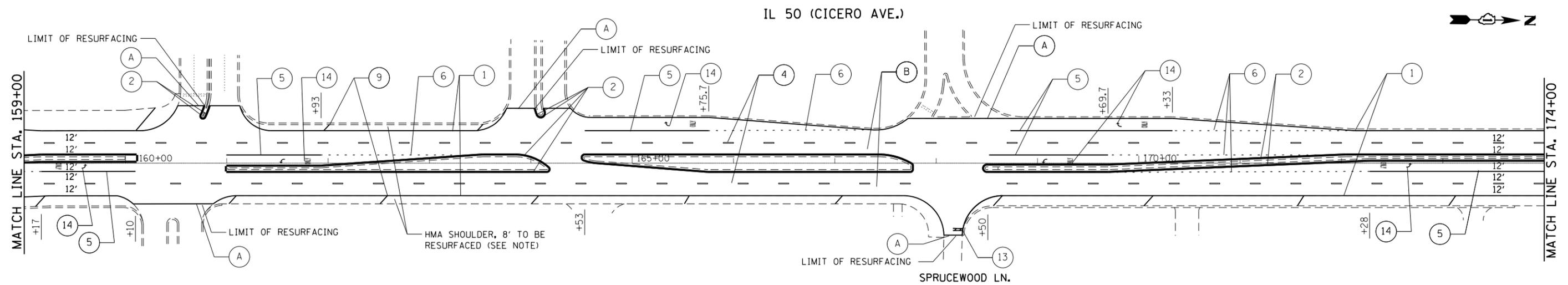
- ROADWAY LEGEND**
- (A) HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
  - (B) PROP. HMA SURFACE REMOVAL, 2 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
  - (C) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B PROP. GRADING AND SHAPING SHOULDERS



**NOTE FOR RESURFACING OF HMA SHOULDERS:**  
 PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
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**PLAN NOTES**  
 ALL PAVEMENT MARKINGS OBLITERATED BY MILLING WITHIN PROJECT LIMITS ARE TO BE REPLACED ACCORDING TO THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL (TC-13).  
 RAISED REFLECTIVE PAVEMENT MARKERS ARE TO BE INSTALLED THROUGHOUT PROJECT LIMITS ACCORDING TO "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) DETAIL (TC-11).

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG.illinois.gov\PIWID\Documents\DOT Offices\District 1\Projects\DI350\Design\DI35016-sh-t-plan.dgn		CHECKED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	15
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62C43				
	PLOT DATE = 2/16/2017					ILLINOIS FED. AID PROJECT				



**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
| ② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIAN LINE, YELLOW (TYP.)            | ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SHOULDER DIAGONALS @ 45°, WHITE (TYP.), 150' SPACING |
| ③ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW (TYP.)          | ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" MEDIAN DIAGONALS @ 45°, YELLOW (TYP.)                |
| ④ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" LANE LINE @ 30' SKIP & 10' DASH, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" LONGITUDINAL BARS (SCHOOL) @ 90°, WHITE (TYP.)       |
| ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" TURN LANE, WHITE (TYP.)                       | ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL (BIKE & EQUESTRIAN) @ 45°, WHITE (TYP.)     |
| ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED @ 6' SKIP & 2' DASH, WHITE (TYP.)      | ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" STOP BAR, WHITE (TYP.)                               |
| ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, WHITE (TYP.)                                |

**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5" (TYP.)
- Ⓑ PROP. HMA SURFACE REMOVAL, 2 1/2"  
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- Ⓒ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B  
PROP. GRADING AND SHAPING SHOULDERS

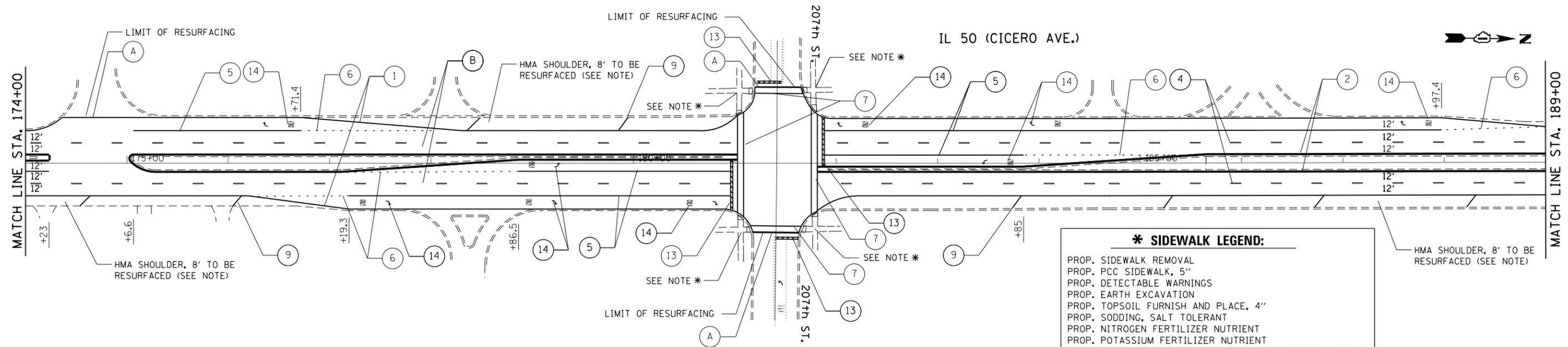
**PLAN NOTES**

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**NOTE FOR RESURFACING OF HMA SHOULDERS:**

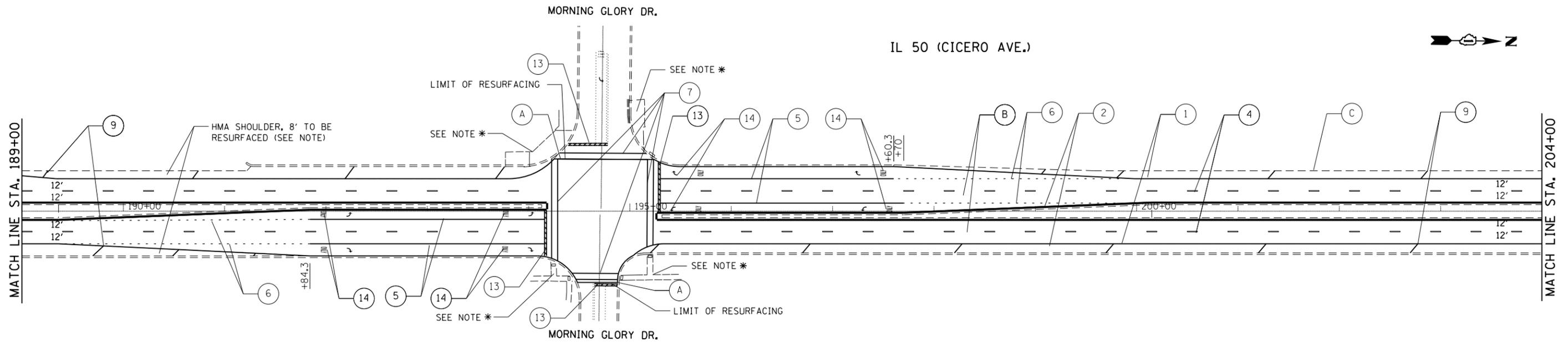
PROP. HMA SURFACE REMOVAL, 2 1/2"  
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"



**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
- PROP. SODDING, SALT TOLERANT
- PROP. NITROGEN FERTILIZER NUTRIENT
- PROP. POTASSIUM FERTILIZER NUTRIENT
- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI35016\Design\DI35016-shr-plan.dgn		CHECKED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	16
Default		DATE -	REVISED -			CONTRACT NO. 62C43			ILLINOIS FED. AID PROJECT	
						SCALE:	SHEET	OF	SHEETS	STA.



**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
| ② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIANS LINE, YELLOW (TYP.)           | ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SHOULDER DIAGONALS @ 45°, WHITE (TYP.), 150' SPACING |
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| ④ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" LANE LINE @ 30' SKIP & 10' DASH, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" LONGITUDINAL BARS (SCHOOL) @ 90°, WHITE (TYP.)       |
| ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" TURN LANE, WHITE (TYP.)                       | ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL (BIKE & EQUESTRIAN) @ 45°, WHITE (TYP.)     |
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| ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, WHITE (TYP.)                                |

**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
- Ⓑ PROP. HMA SURFACE REMOVAL, 2 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- Ⓒ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B PROP. GRADING AND SHAPING SHOULDERS

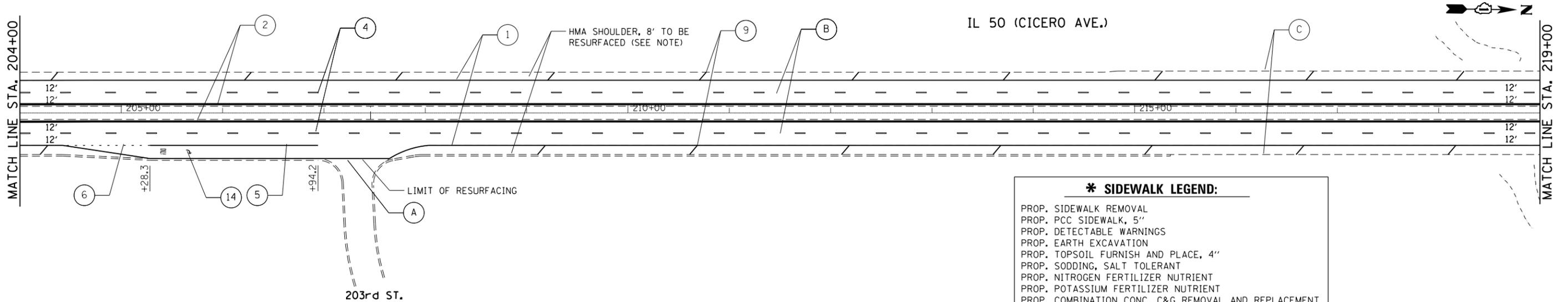
**NOTE FOR RESURFACING OF HMA SHOULDERS:**

PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
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**PLAN NOTES**

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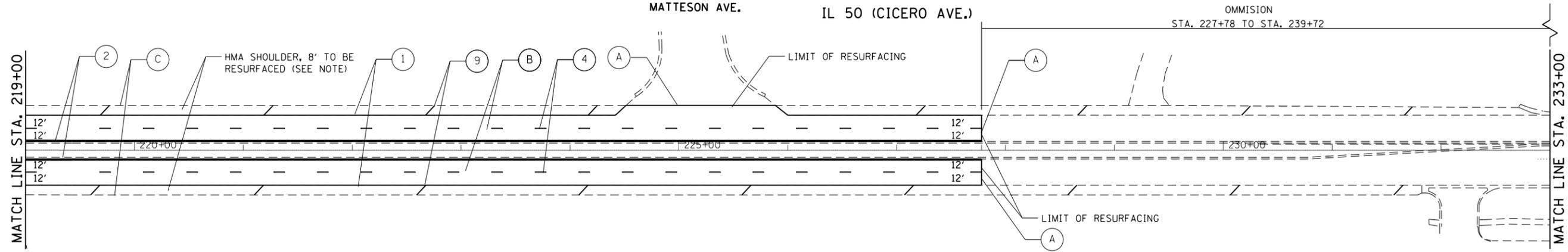
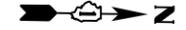
RAISED REFLECTIVE PAVEMENT MARKERS ARE TO BE INSTALLED THROUGHOUT PROJECT LIMITS ACCORDING TO "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) DETAIL (TC-11).



**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
- PROP. SODDING, SALT TOLERANT
- PROP. NITROGEN FERTILIZER NUTRIENT
- PROP. POTASSIUM FERTILIZER NUTRIENT
- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI35016\Design\DI35016-sh1-plan.dgn		REVISOR	REVISION			350	(101-EXT.) RS-3	COOK	51	17	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISOR			CONTRACT NO. 62C43					
	PLOT DATE = 2/16/2017	DATE -	REVISION			ILLINOIS FED. AID PROJECT					



**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
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**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
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- Ⓒ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B PROP. GRADING AND SHAPING SHOULDERS

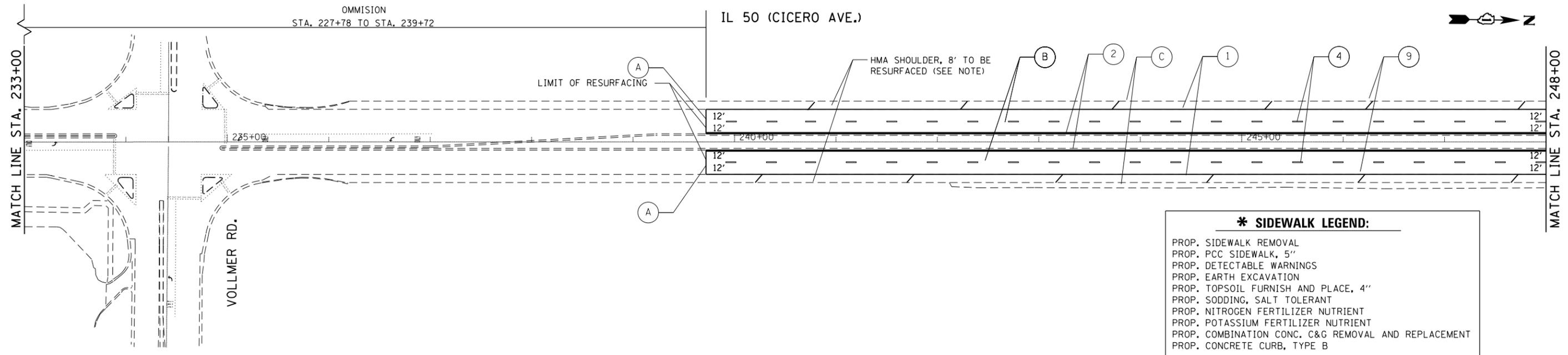
**PLAN NOTES**

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**\* SIDEWALK LEGEND:**

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FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -
p:\11084EBID\INTEG.illinois.gov\PIWDDT\Documents\DOT Offices\District 1\Projects\DI35016\Design\DI35016-sh1-plan.dgn		REVISED -	REVISED -
Default	PLOT DATE = 2/16/2017	CHECKED -	REVISED -
		DATE -	REVISED -

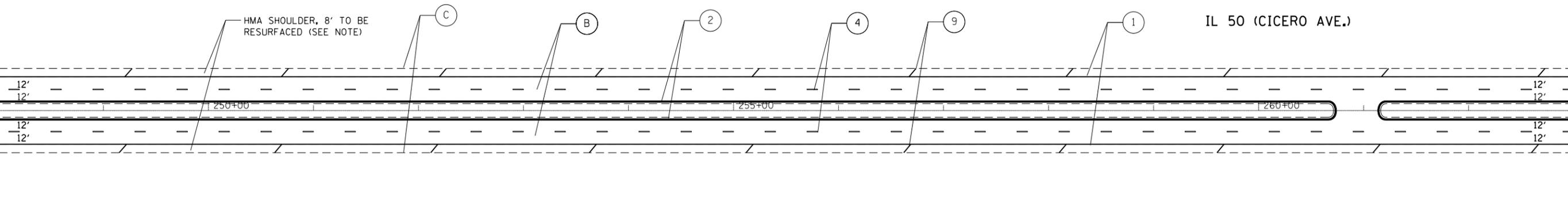
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
 ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	18
			CONTRACT NO. 62C43	
ILLINOIS FED. AID PROJECT				

MATCH LINE STA. 248+00



MATCH LINE STA. 263+00

**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
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**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
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- Ⓒ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B PROP. GRADING AND SHAPING SHOULDERS

**PLAN NOTES**

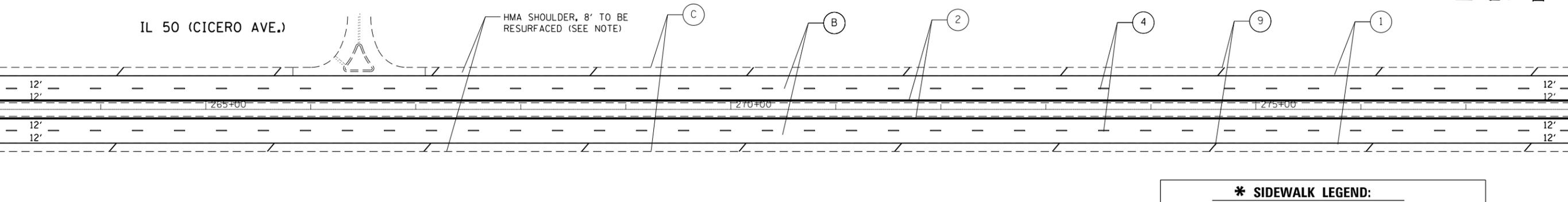
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**NOTE FOR RESURFACING OF HMA SHOULDERS:**

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 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"

MATCH LINE STA. 263+00



MATCH LINE STA. 278+00

**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
- PROP. SODDING, SALT TOLERANT
- PROP. NITROGEN FERTILIZER NUTRIENT
- PROP. POTASSIUM FERTILIZER NUTRIENT
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- PROP. CONCRETE CURB, TYPE B

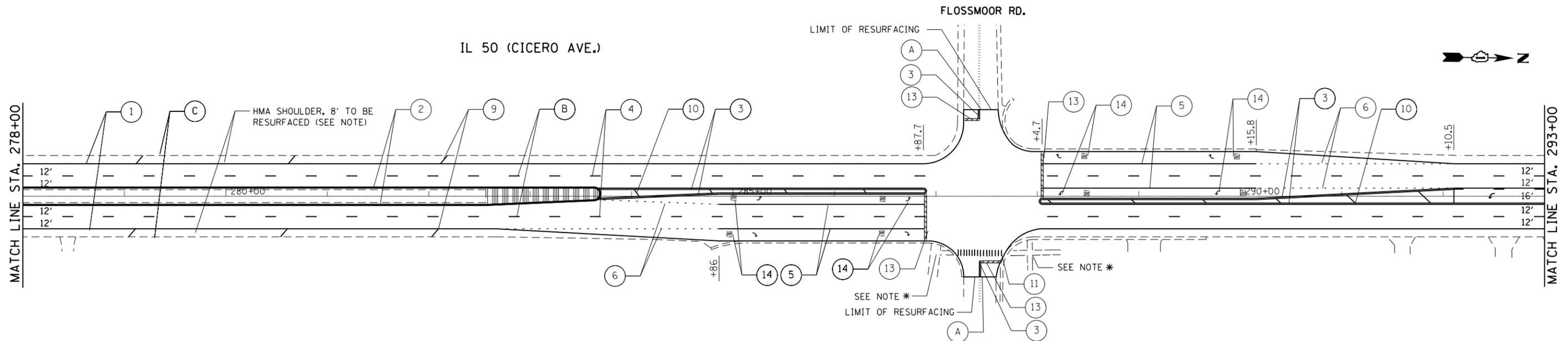
FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/16/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN  
 ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	19
			CONTRACT NO. 62C43	
ILLINOIS FED. AID PROJECT				



**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
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**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
- Ⓑ PROP. HMA SURFACE REMOVAL, 2 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
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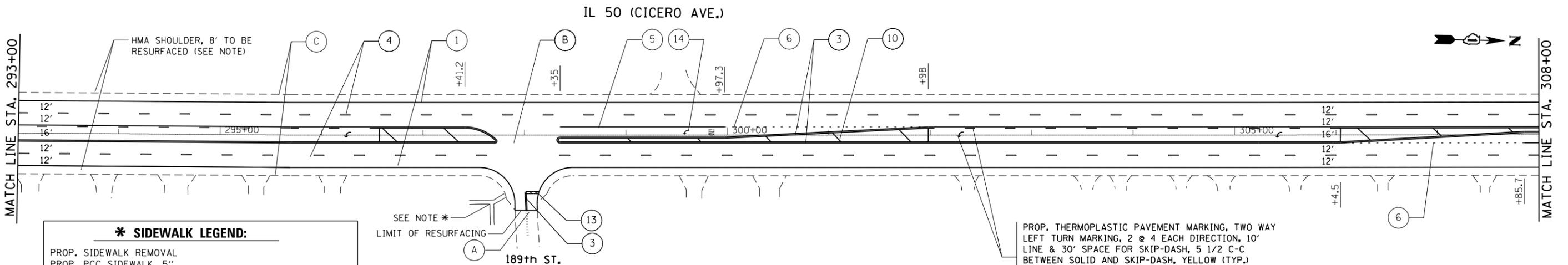
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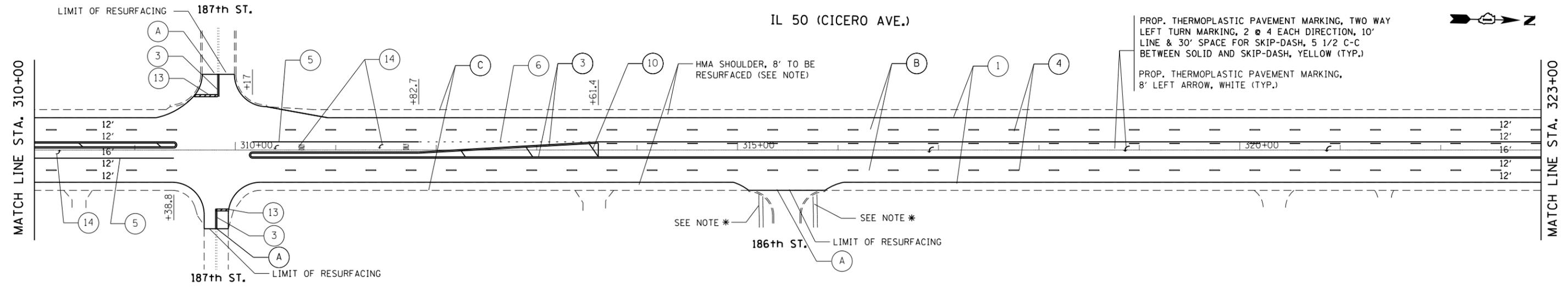
**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
- PROP. SODDING, SALT TOLERANT
- PROP. NITROGEN FERTILIZER NUTRIENT
- PROP. POTASSIUM FERTILIZER NUTRIENT
- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B

PROP. THERMOPLASTIC PAVEMENT MARKING, TWO WAY LEFT TURN MARKING, 2 @ 4 EACH DIRECTION, 10' LINE & 30' SPACE FOR SKIP-DASH, 5 1/2 C-C BETWEEN SOLID AND SKIP-DASH, YELLOW (TYP.)

PROP. THERMOPLASTIC PAVEMENT MARKING, 8' LEFT ARROW, WHITE (TYP.)

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI350\Drawings\Design\DI35016-shr-plan.dgn	DRWN	REVISED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	20	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62C43					
	PLOT DATE = 2/16/2017	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



**PAVEMENT MARKING LEGEND**

- |   |   |
|---|---|
| ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)                       | ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)                      |
| ② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIANS LINE, YELLOW (TYP.)           | ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SHOULDER DIAGONALS @ 45°, WHITE (TYP.), 150" SPACING |
| ③ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW (TYP.)          | ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" MEDIAN DIAGONALS @ 45°, YELLOW (TYP.)                |
| ④ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" LANE LINE @ 30' SKIP & 10' DASH, WHITE (TYP.) | ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" LONGITUDINAL BARS (SCHOOL) @ 90°, WHITE (TYP.)       |
| ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" TURN LANE, WHITE (TYP.)                       | ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL (BIKE & EQUESTRIAN) @ 45°, WHITE (TYP.)     |
| ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED @ 6' SKIP & 2' DASH, WHITE (TYP.)      | ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" STOP BAR, WHITE (TYP.)                               |
| ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.) | ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, WHITE (TYP.)                                |

**ROADWAY LEGEND**

- Ⓐ HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
- Ⓑ PROP. HMA SURFACE REMOVAL, 2 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- Ⓒ PROP. AGGREGATE WEDGE SHOULDERS, TYPE B PROP. GRADING AND SHAPING SHOULDERS

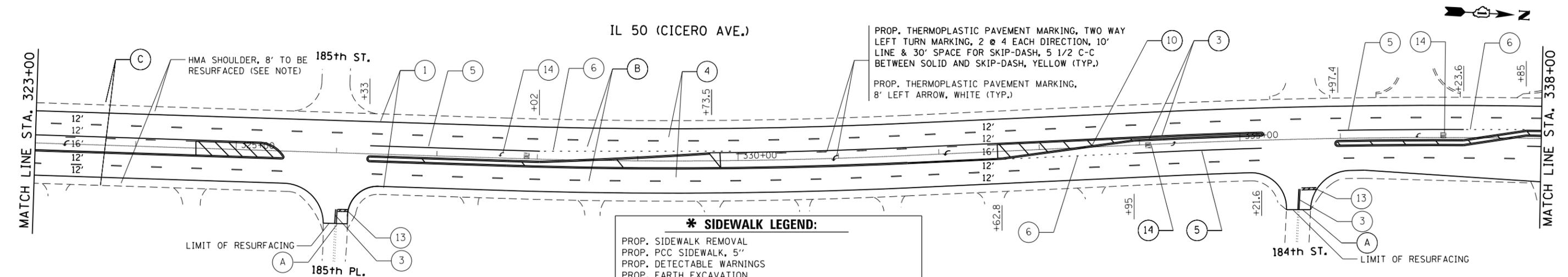
**PLAN NOTES**

ALL PAVEMENT MARKINGS OBLITERATED BY MILLING WITHIN PROJECT LIMITS ARE TO BE REPLACED ACCORDING TO THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL (TC-13).

RAISED REFLECTIVE PAVEMENT MARKERS ARE TO BE INSTALLED THROUGHOUT PROJECT LIMITS ACCORDING TO "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) DETAIL (TC-11).

**NOTE FOR RESURFACING OF HMA SHOULDERS:**

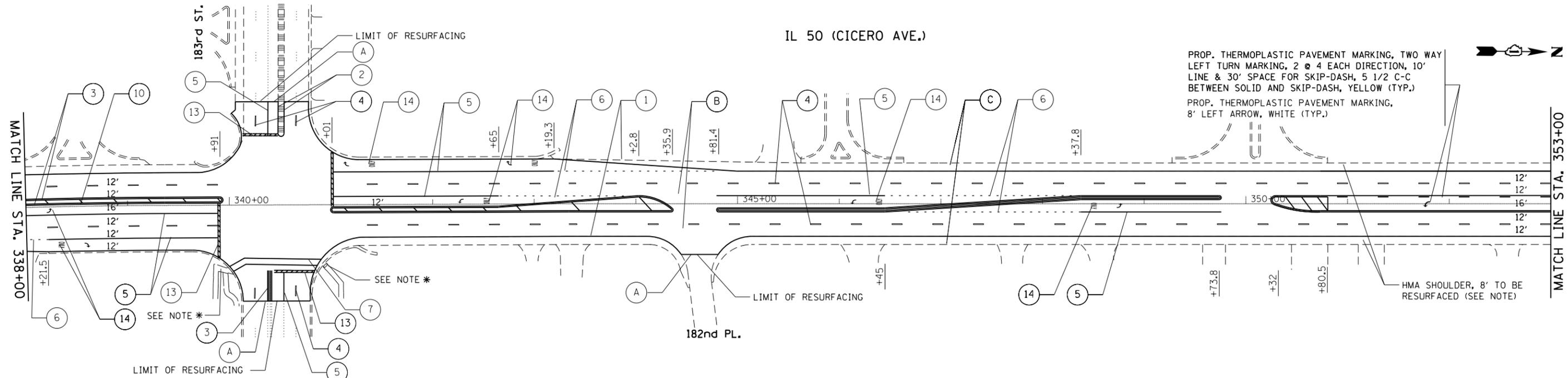
PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"



**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
- PROP. SODDING, SALT TOLERANT
- PROP. NITROGEN FERTILIZER NUTRIENT
- PROP. POTASSIUM FERTILIZER NUTRIENT
- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\1\084EBID\INTEG\Illinois.gov\PIWID\Documents\DOT Offices\District 1\Projects\DI350\Drawings\Design\DI35016-shr-plan.dgn	PLotted SCALE = 100.0000' / in.	CHECKED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	21	
Default	PLOT DATE = 2/16/2017	DATE -	REVISED -			CONTRACT NO. 62C43					
						ILLINOIS FED. AID PROJECT					



PROP. THERMOPLASTIC PAVEMENT MARKING, TWO WAY LEFT TURN MARKING, 2 @ 4 EACH DIRECTION, 10' LINE & 30' SPACE FOR SKIP-DASH, 5 1/2 C-C BETWEEN SOLID AND SKIP-DASH, YELLOW (TYP.)  
 PROP. THERMOPLASTIC PAVEMENT MARKING, 8' LEFT ARROW, WHITE (TYP.)

**PAVEMENT MARKING LEGEND**

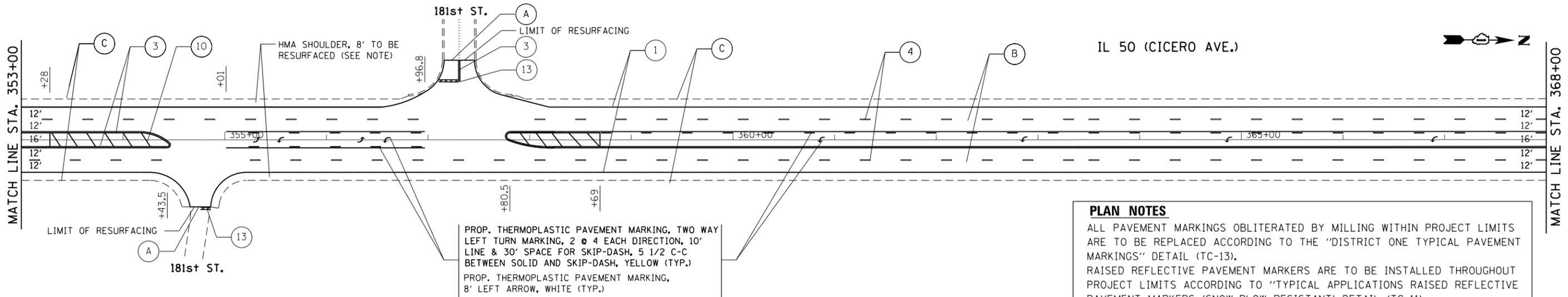
- ① PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIANS LINE, YELLOW (TYP.)
- ③ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
- ④ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" LANE LINE @ 30' SKIP & 10' DASH, WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" TURN LANE, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED @ 6' SKIP & 2' DASH, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.)
- ⑧ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)
- ⑨ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SHOULDER DIAGONALS @ 45°, WHITE (TYP.), 150' SPACING
- ⑩ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" MEDIAN DIAGONALS @ 45°, WHITE (TYP.)
- ⑪ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" LONGITUDINAL BARS (SCHOOL) @ 90°, WHITE (TYP.)
- ⑫ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL (BIKE & EQUESTRIAN) @ 45°, WHITE (TYP.)
- ⑬ PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" STOP BAR, WHITE (TYP.)
- ⑭ PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, WHITE (TYP.)

**ROADWAY LEGEND**

- (A) HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
- (B) PROP. HMA SURFACE REMOVAL, 2 1/2" PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
- (C) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B PROP. GRADING AND SHAPING SHOULDERS

**\* SIDEWALK LEGEND:**

- PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. EARTH EXCAVATION
- PROP. TOPSOIL FURNISH AND PLACE, 4"
- PROP. SODDING, SALT TOLERANT
- PROP. NITROGEN FERTILIZER NUTRIENT
- PROP. POTASSIUM FERTILIZER NUTRIENT
- PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT
- PROP. CONCRETE CURB, TYPE B



PROP. THERMOPLASTIC PAVEMENT MARKING, TWO WAY LEFT TURN MARKING, 2 @ 4 EACH DIRECTION, 10' LINE & 30' SPACE FOR SKIP-DASH, 5 1/2 C-C BETWEEN SOLID AND SKIP-DASH, YELLOW (TYP.)  
 PROP. THERMOPLASTIC PAVEMENT MARKING, 8' LEFT ARROW, WHITE (TYP.)

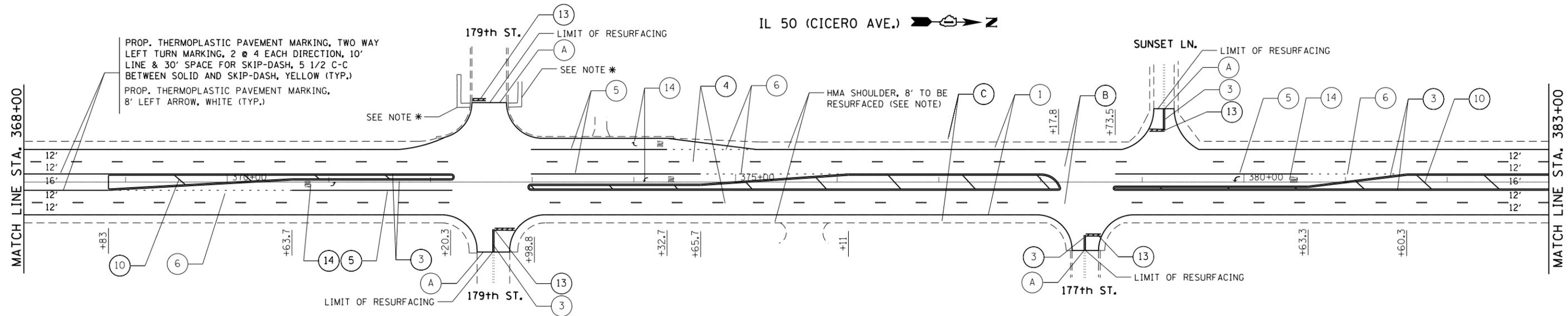
**PLAN NOTES**

ALL PAVEMENT MARKINGS OBLITERATED BY MILLING WITHIN PROJECT LIMITS ARE TO BE REPLACED ACCORDING TO THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL (TC-13).  
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**NOTE FOR RESURFACING OF HMA SHOULDERS:**

PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI350\Drawings\Design\DI35016-shr-plan.dgn	DRWN	REVISED -	REVISED -			350	(101-EXT.) RS-3	COOK	51	22	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62C43					
	PLOT DATE = 2/16/2017	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



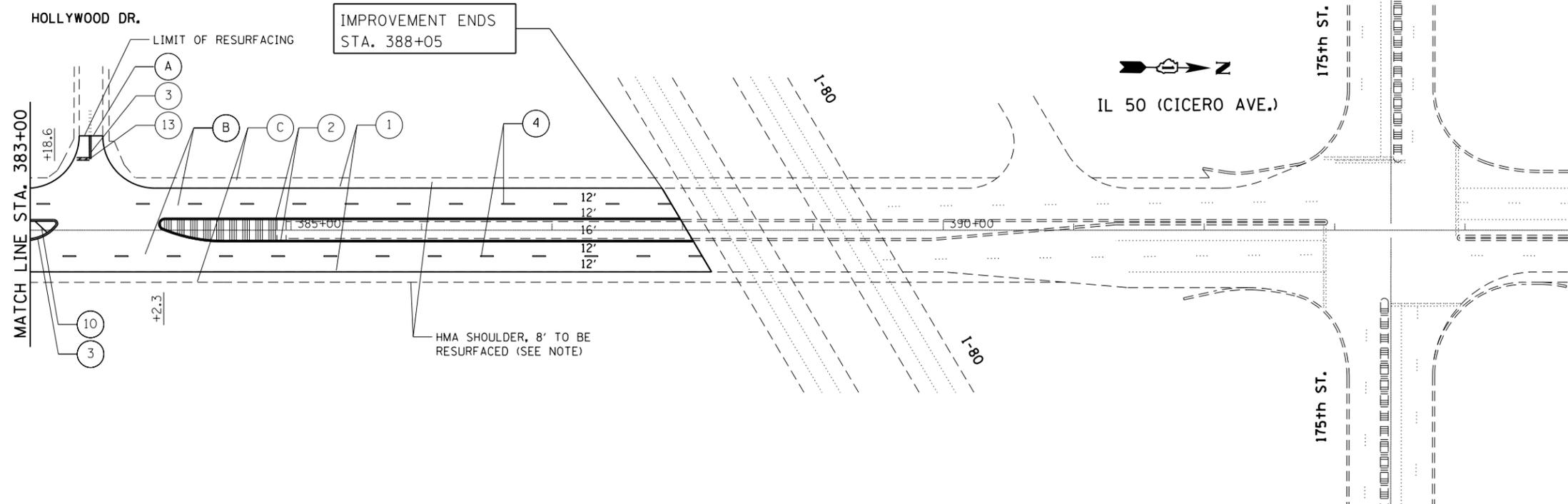
**\* SIDEWALK LEGEND:**  
 PROP. SIDEWALK REMOVAL  
 PROP. PCC SIDEWALK, 5"  
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 PROP. EARTH EXCAVATION  
 PROP. TOPSOIL FURNISH AND PLACE, 4"  
 PROP. SODDING, SALT TOLERANT  
 PROP. NITROGEN FERTILIZER NUTRIENT  
 PROP. POTASSIUM FERTILIZER NUTRIENT  
 PROP. COMBINATION CONC. C&G REMOVAL AND REPLACEMENT  
 PROP. CONCRETE CURB, TYPE B

**PLAN NOTES**  
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**NOTE FOR RESURFACING OF HMA SHOULDERS:**  
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 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"

- ROADWAY LEGEND**
- (A) HMA SURFACE REMOVAL BUTT JOINT, 4.5' (TYP.)
  - (B) PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"  
 PROP. POLY. HMA SURFACE COURSE, MIX "E", N70, 1 3/4"
  - (C) PROP. AGGREGATE WEDGE SHOULDERS, TYPE B  
 PROP. GRADING AND SHAPING SHOULDERS

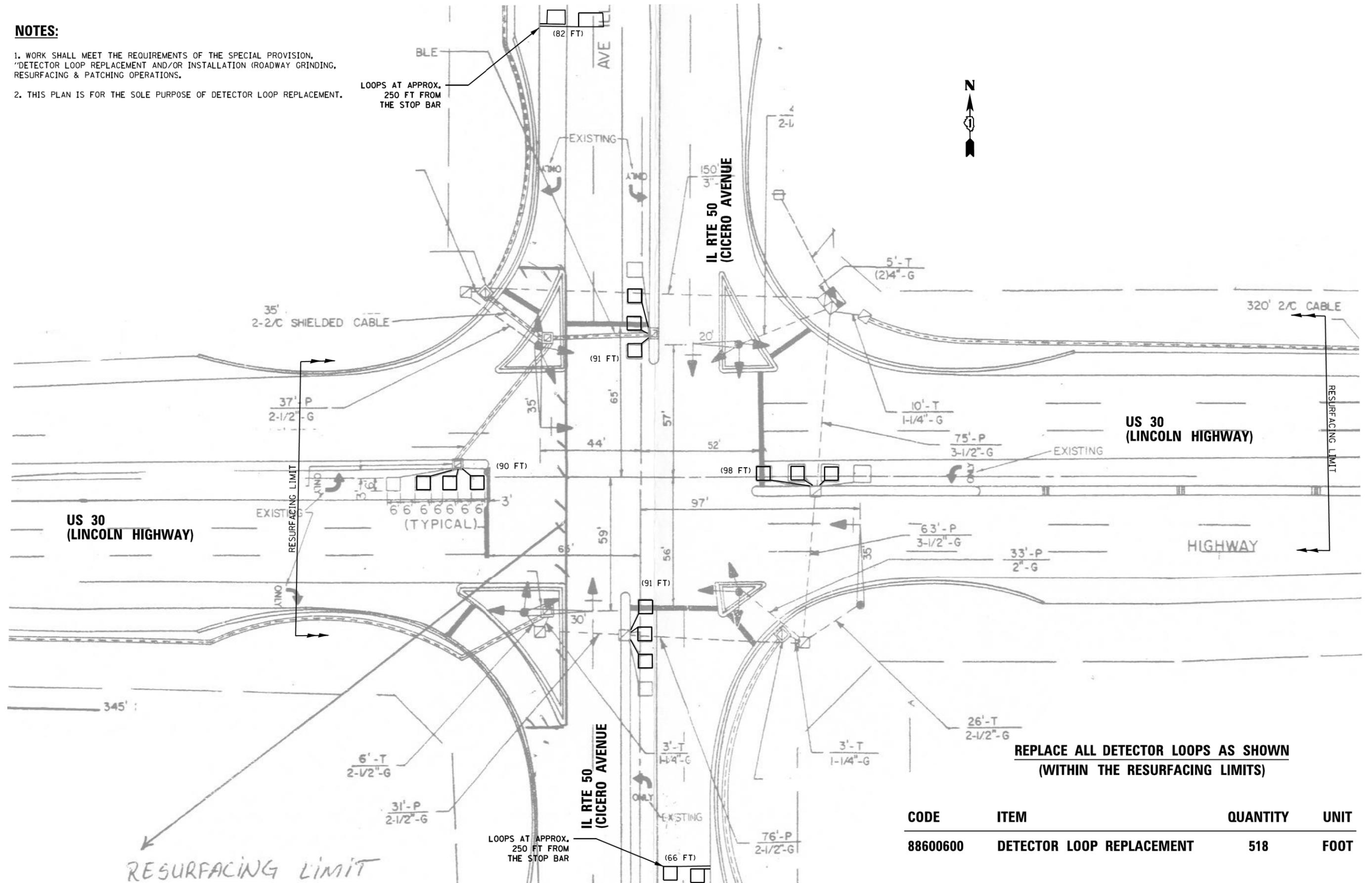
- PAVEMENT MARKING LEGEND**
- (1) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" EDGE LINE, WHITE (TYP.)
  - (2) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" OUTLINE MEDIANS LINE, YELLOW (TYP.)
  - (3) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" DOUBLE LINE @ 11" C-C, YELLOW (TYP.)
  - (4) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 4" LANE LINE @ 30' SKIP & 10' DASH, WHITE (TYP.)
  - (5) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" TURN LANE, WHITE (TYP.)
  - (6) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" DOTTED @ 6' SKIP & 2' DASH, WHITE (TYP.)
  - (7) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 6" CROSSWALK LINES SPACED @ 6' C-C, WHITE (TYP.)
  - (8) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 8" CHANNELIZING LINES, WHITE (TYP.)
  - (9) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" SHOULDER DIAGONALS @ 45°, WHITE (TYP.), 150' SPACING
  - (10) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" MEDIAN DIAGONALS @ 45°, YELLOW (TYP.)
  - (11) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" LONGITUDINAL BARS (SCHOOL) @ 90°, WHITE (TYP.)
  - (12) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 12" DIAGONAL (BIKE & EQUESTRIAN) @ 45°, WHITE (TYP.)
  - (13) PROP. THERMOPLASTIC PAVEMENT MARKING LINE, 24" STOP BAR, WHITE (TYP.)
  - (14) PROP. THERMOPLASTIC PAVEMENT MARKING LETTERS & SYMBOLS, WHITE (TYP.)



FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN ILLINOIS ROUTE 50 (175th ST. TO STEGER RD.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI350\Drawings\Design\DI35016-shr-plan.dgn		REVISIONS	REVISIONS			350	(101-EXT.) RS-3	COOK	51	23
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISIONS	REVISIONS			CONTRACT NO. 62C43				
Default	DATE -	REVISIONS	REVISIONS			ILLINOIS FED. AID PROJECT				

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	518	FOOT

FILE NAME =	USER NAME = jovanmerdazg	DESIGNED - ZGJ	REVISED -
TS 1395 ILL RTE 50 & LINCOLN HWY.11292016.dgn		DRAWN - ZGJ	REVISED -
Default		CHECKED - LP	REVISED -
		DATE - 11/29/2016	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

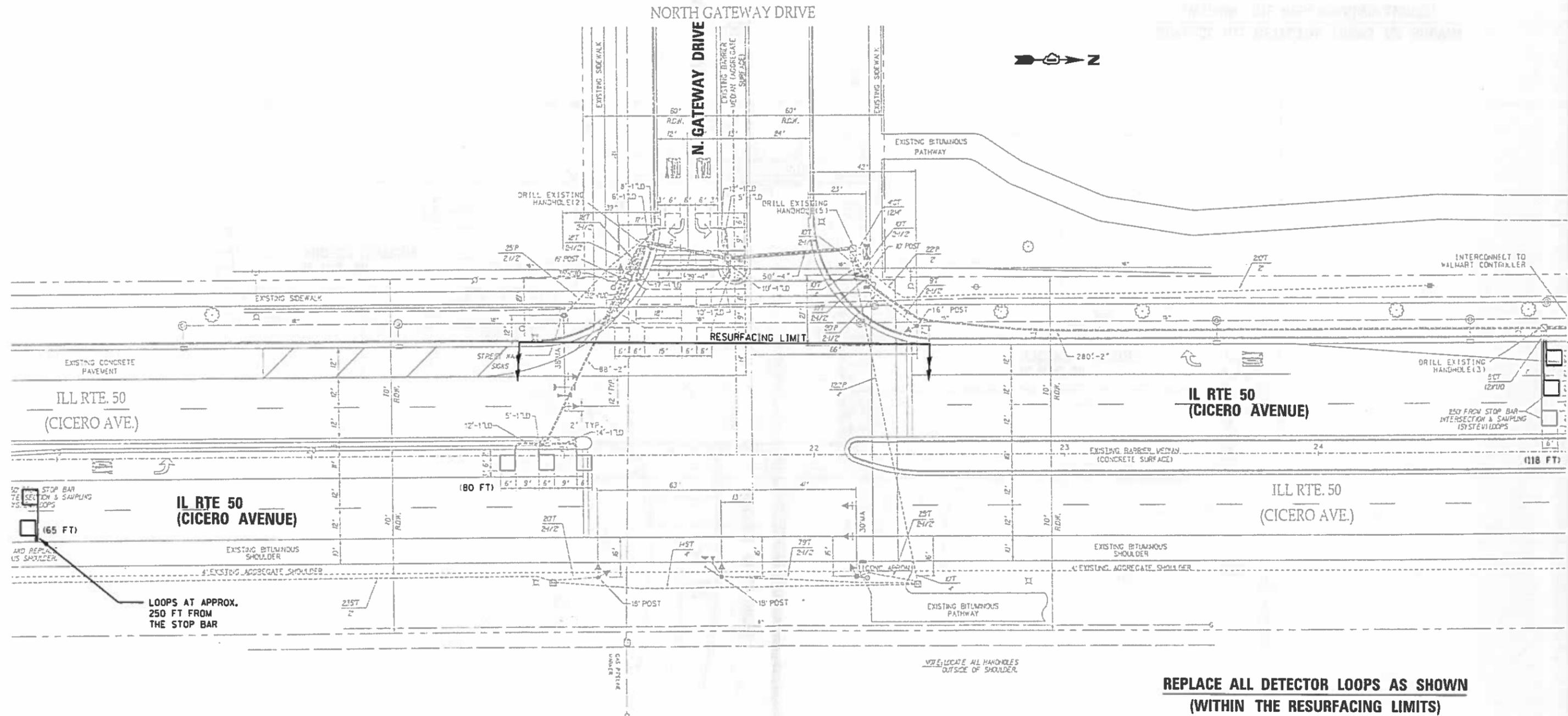
**DETECTOR LOOP REPLACEMENT PLAN  
IL RTE 50 (CICERO AVENUE) AT US 30 (LINCOLN HIGHWAY)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT,RS-3)	COOK	51	24
CONTRACT NO. 62C43				
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, 'DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

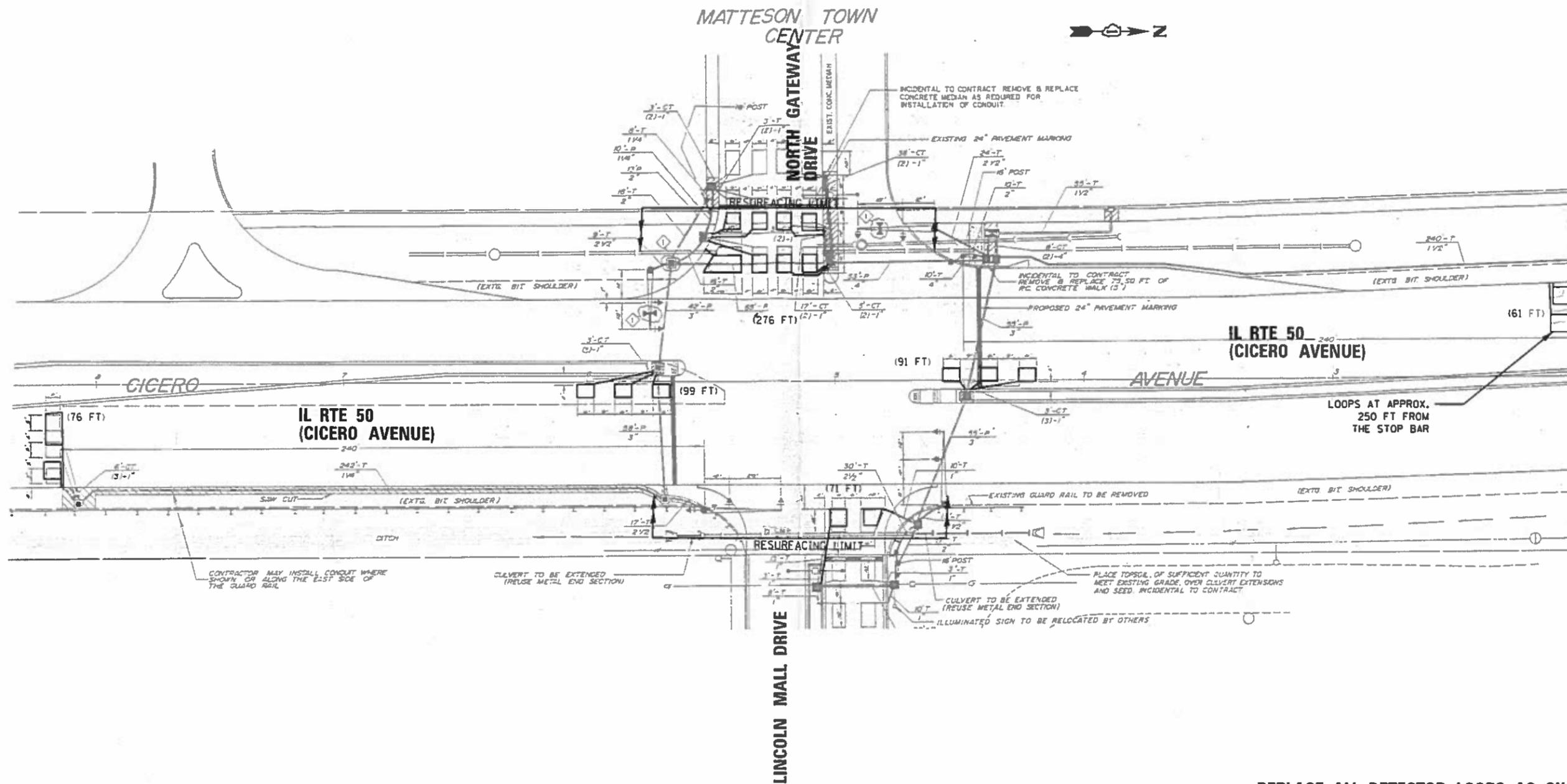


CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	263	FOOT

FILE NAME = TS 12404 ILL RTE 50 & N GATEWAY DRIVE	USER NAME = jveinhardt	DESIGNED = ZGJ	REVISED =	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN ILL RTE 50 (CICERO AVENUE) AT NORTH GATEWAY DRIVE</b>	F.A.P. RTE. = 350	SECTION = 1101-EXT.JRS-3	COUNTY = COOK	TOTAL SHEETS = 51	SHEET NO. = 24B		
Default	PLOT SCALE = 40,0000 / 10.	CHECKED = LP	REVISED =			SCALE:	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 62C43			
	PLOT DATE = 11/29/2016	DATE = 11/29/2016	REVISED =			ILLINOIS FED. AID PROJECT						

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



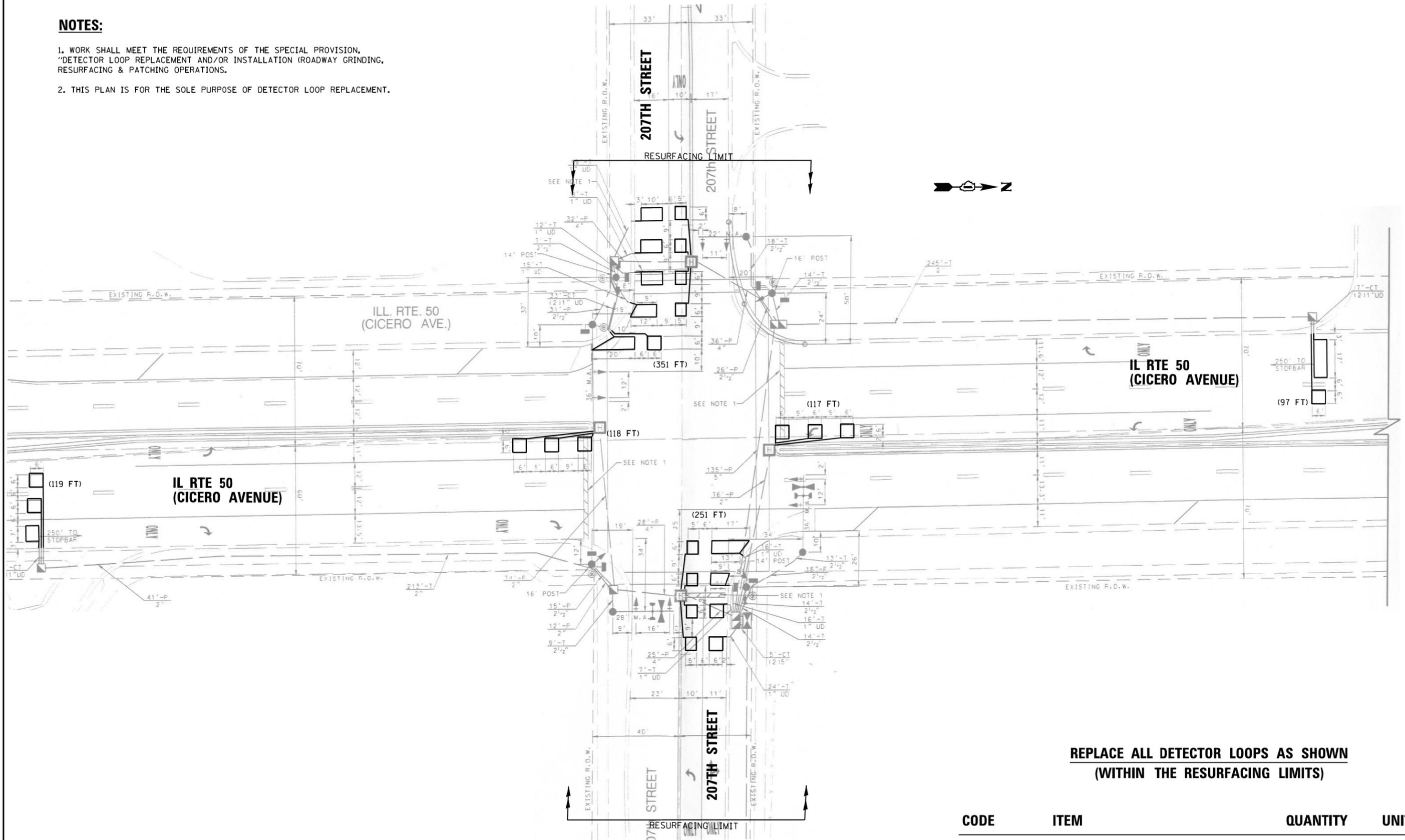
**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	674	FOOT

FILE NAME : TS 2630 ILL RTE 50 & N.GATEWAY DRIVE	USER NAME : javanmrdzq LINCOLN MALL DRIVE.11292016.dgn	DESIGNED - ZGJ DRAWN - ZGJ	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN ILL RTE 50 (CICERO AVE) AT N.GATEWAY DRIVE (LINCOLN MALL DRIVE)</b>		F.A.P. RTE. 350	SECTION (101-EXT-IRS-3)	COUNTY COOK	TOTAL SHEETS 51	SHEET NO. 242
Default	PLOT SCALE : 48.0000 1/8" in.	CHECKED - LP	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 62C43
	PLOT DATE : 12/2/2016	DATE : 11/29/2016	REVISED -								(ILLINOIS) FED. AID PROJECT

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	956	FOOT

FILE NAME =	USER NAME = javanmrdzsg	DESIGNED - ZGJ	REVISED -
TS 1357 ILL RTE 50 & 207TH STREET.11172016.dgn		DRAWN - ZGJ	REVISED -
Default	PLOT SCALE = 48,00000 ' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 11/29/2016	DATE - 11/29/2016	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
ILL RTE 50 (CICERO AVENUE) AT 207TH STREET**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 350	SECTION (101-EXT,RS-3)	COUNTY COOK	TOTAL SHEETS 51	SHEET NO. 25
			CONTRACT NO. 62C43	
ILLINOIS FED. AID PROJECT				

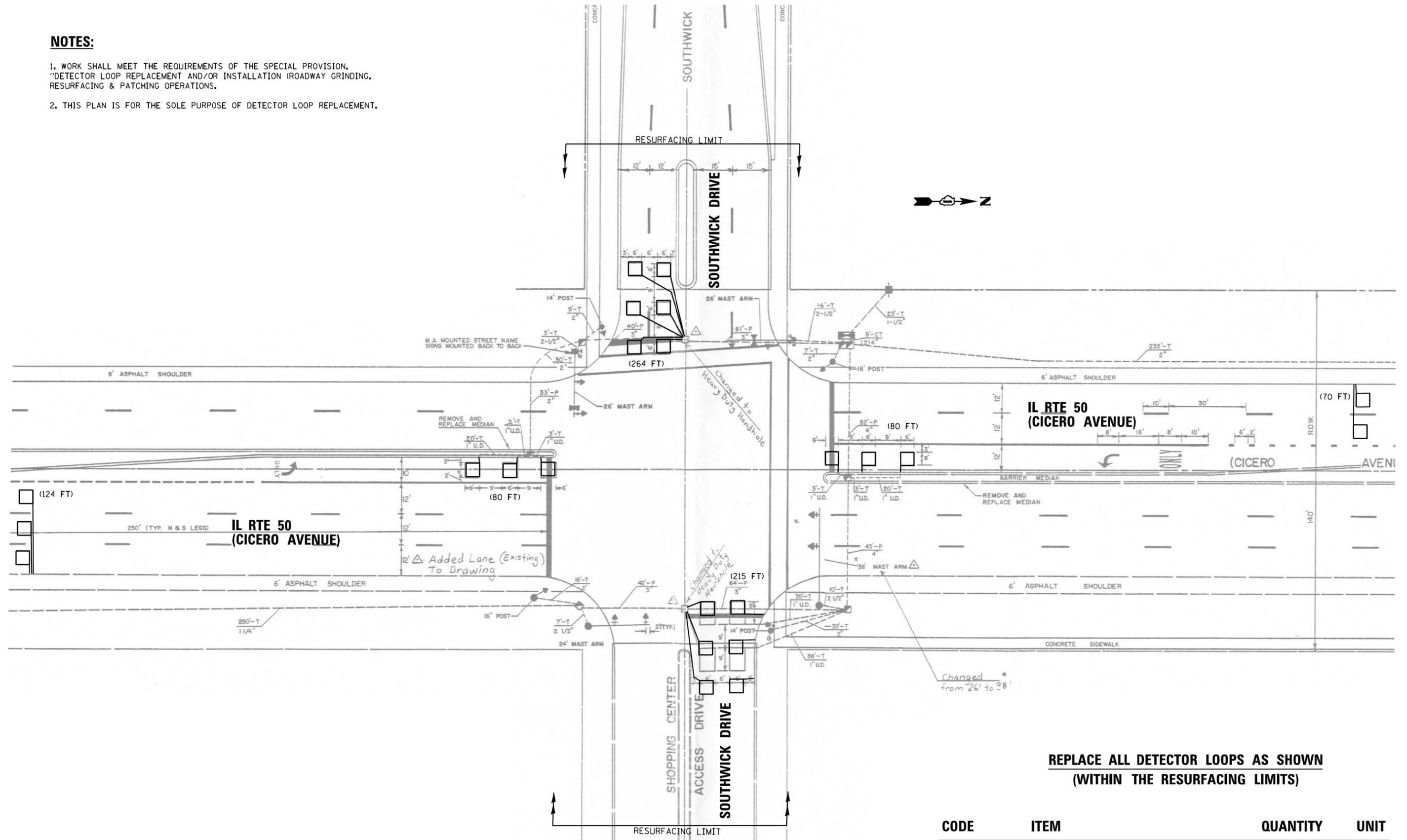






**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	813	FOOT

FILE NAME = TS 12400 ILL RTE 50 & SOUTHWICK DRIVER	USER NAME = jvanmerdztg	DESIGNED - ZGJ	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN IL RTE 50 (CICERO AVENUE) AT SOUTHWICK DRIVE</b>	F.A.P. RTE. 350	SECTION (101-EXT,RS-3)	COUNTY COOK	TOTAL SHEETS 51	SHEET NO. 29		
Default	PLOT SCALE = 48,0000' / in.	CHECKED - LP	REVISED -			CONTRACT NO. 62C43		ILLINOIS FED. AID PROJECT				
	PLOT DATE = 11/29/2016	DATE - 11/29/2016	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.				

**REMOVAL NOTES:**

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

3 EACH PEDESTRIAN PUSH-BUTTON

**NOTES:**

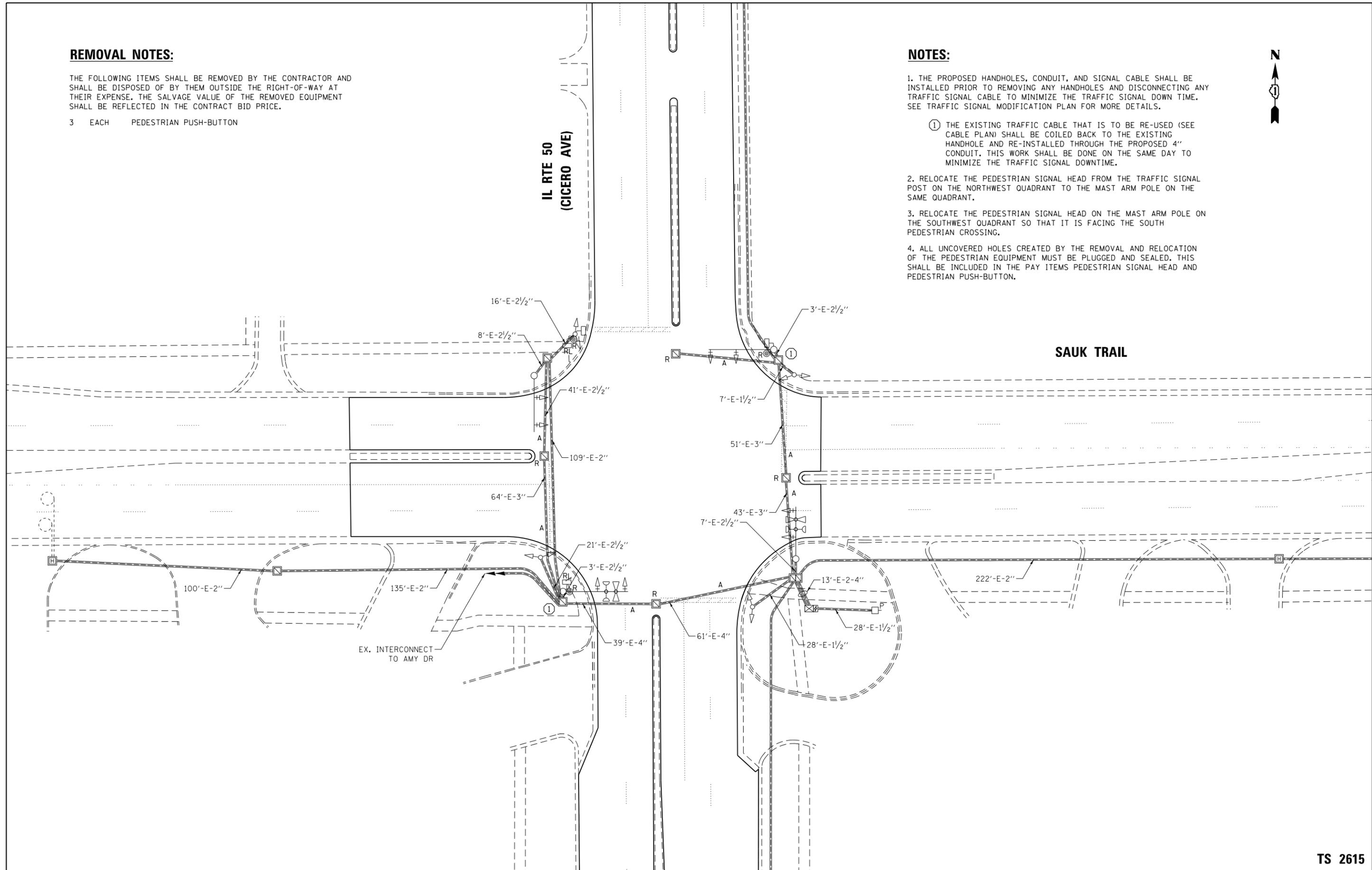
1. THE PROPOSED HANDHOLES, CONDUIT, AND SIGNAL CABLE SHALL BE INSTALLED PRIOR TO REMOVING ANY HANDHOLES AND DISCONNECTING ANY TRAFFIC SIGNAL CABLE TO MINIMIZE THE TRAFFIC SIGNAL DOWN TIME. SEE TRAFFIC SIGNAL MODIFICATION PLAN FOR MORE DETAILS.

① THE EXISTING TRAFFIC CABLE THAT IS TO BE RE-USED (SEE CABLE PLAN) SHALL BE COILED BACK TO THE EXISTING HANDHOLE AND RE-INSTALLED THROUGH THE PROPOSED 4" CONDUIT. THIS WORK SHALL BE DONE ON THE SAME DAY TO MINIMIZE THE TRAFFIC SIGNAL DOWNTIME.

2. RELOCATE THE PEDESTRIAN SIGNAL HEAD FROM THE TRAFFIC SIGNAL POST ON THE NORTHWEST QUADRANT TO THE MAST ARM POLE ON THE SAME QUADRANT.

3. RELOCATE THE PEDESTRIAN SIGNAL HEAD ON THE MAST ARM POLE ON THE SOUTHWEST QUADRANT SO THAT IT IS FACING THE SOUTH PEDESTRIAN CROSSING.

4. ALL UNCOVERED HOLES CREATED BY THE REMOVAL AND RELOCATION OF THE PEDESTRIAN EQUIPMENT MUST BE PLUGGED AND SEALED. THIS SHALL BE INCLUDED IN THE PAY ITEMS PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSH-BUTTON.



TS SHT NO. 1

TS 2615

FILE NAME =	USER NAME = plascencia	DESIGNED - IP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC SIGNAL REMOVAL PLAN IL RTE 50 (CICERO AVE) AT SAUK TRAIL</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\135016\Traffic\135016-sht-ts.dgn		CHECKED - AK	REVISED -			350	(101-EXT.) RS-3	COOK	51	30	
PLOT SCALE = 40.0000' / in.		DATE - 02/08/2017	REVISED -			CONTRACT NO. 62C43					
Default			REVISED -			ILLINOIS FED. AID PROJECT					

**NOTES:**

1. THE PROPOSED HANDHOLES, CONDUIT, AND SIGNAL CABLE SHALL BE INSTALLED PRIOR TO REMOVING ANY HANDHOLES AND DISCONNECTING ANY TRAFFIC SIGNAL CABLE TO MINIMIZE THE TRAFFIC SIGNAL DOWN TIME.

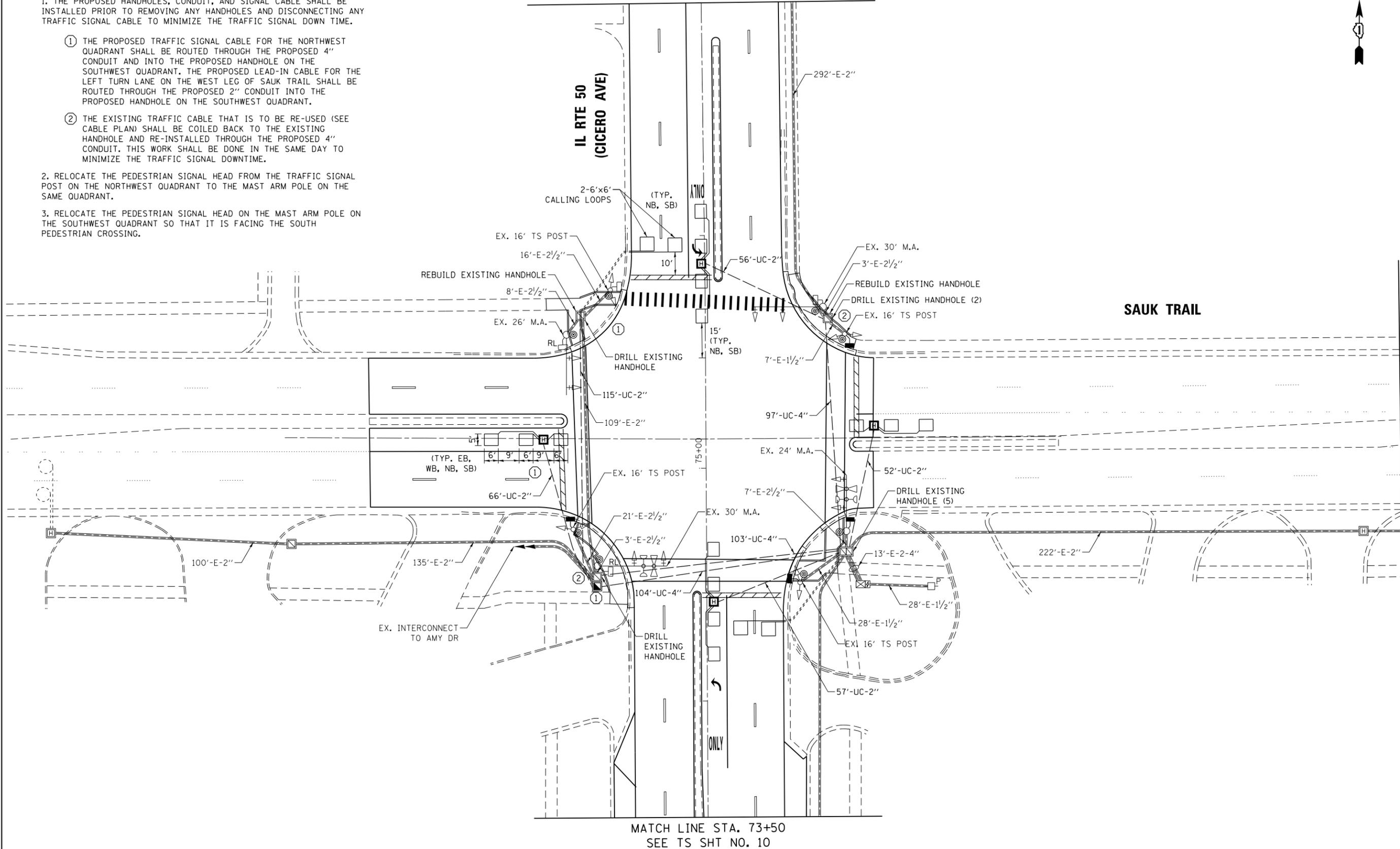
① THE PROPOSED TRAFFIC SIGNAL CABLE FOR THE NORTHWEST QUADRANT SHALL BE ROUTED THROUGH THE PROPOSED 4" CONDUIT AND INTO THE PROPOSED HANDHOLE ON THE SOUTHWEST QUADRANT. THE PROPOSED LEAD-IN CABLE FOR THE LEFT TURN LANE ON THE WEST LEG OF SAUK TRAIL SHALL BE ROUTED THROUGH THE PROPOSED 2" CONDUIT INTO THE PROPOSED HANDHOLE ON THE SOUTHWEST QUADRANT.

② THE EXISTING TRAFFIC CABLE THAT IS TO BE RE-USED (SEE CABLE PLAN) SHALL BE COILED BACK TO THE EXISTING HANDHOLE AND RE-INSTALLED THROUGH THE PROPOSED 4" CONDUIT. THIS WORK SHALL BE DONE IN THE SAME DAY TO MINIMIZE THE TRAFFIC SIGNAL DOWNTIME.

2. RELOCATE THE PEDESTRIAN SIGNAL HEAD FROM THE TRAFFIC SIGNAL POST ON THE NORTHWEST QUADRANT TO THE MAST ARM POLE ON THE SAME QUADRANT.

3. RELOCATE THE PEDESTRIAN SIGNAL HEAD ON THE MAST ARM POLE ON THE SOUTHWEST QUADRANT SO THAT IT IS FACING THE SOUTH PEDESTRIAN CROSSING.

SEE TS SHT NO. 10  
MATCH LINE STA. 77+00



TS SHT NO. 2

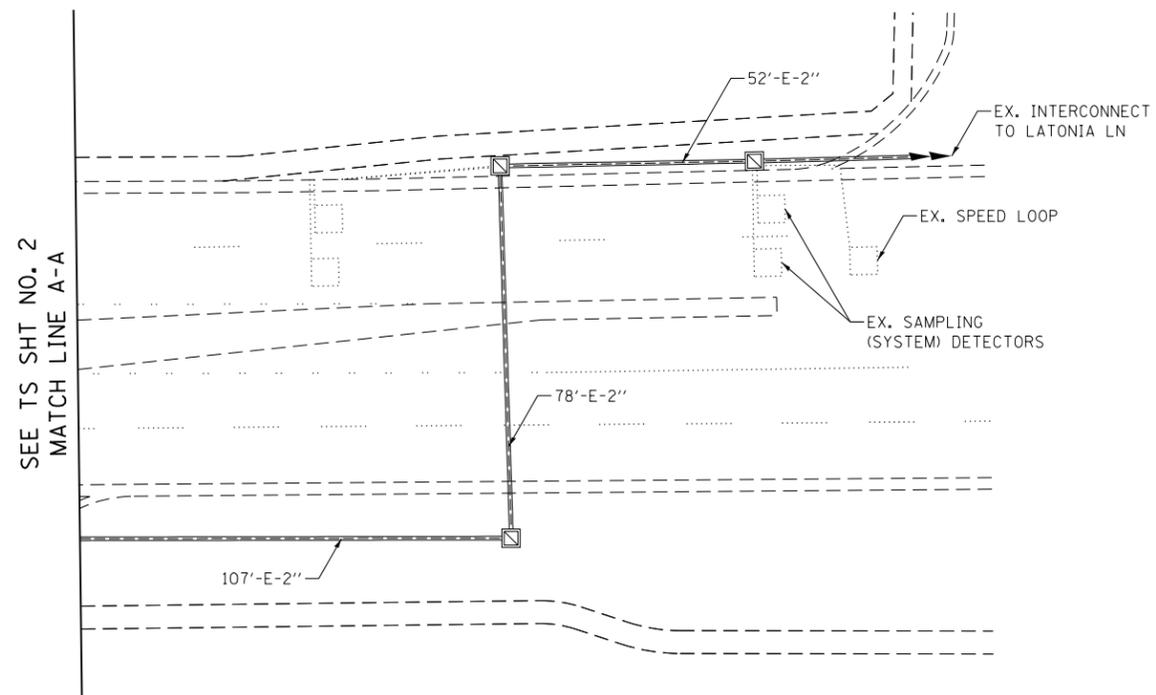
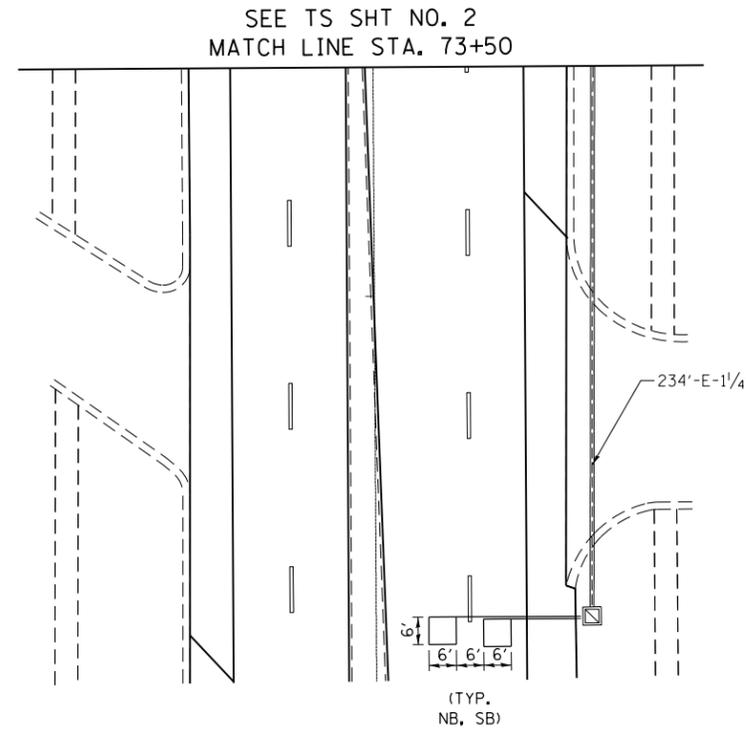
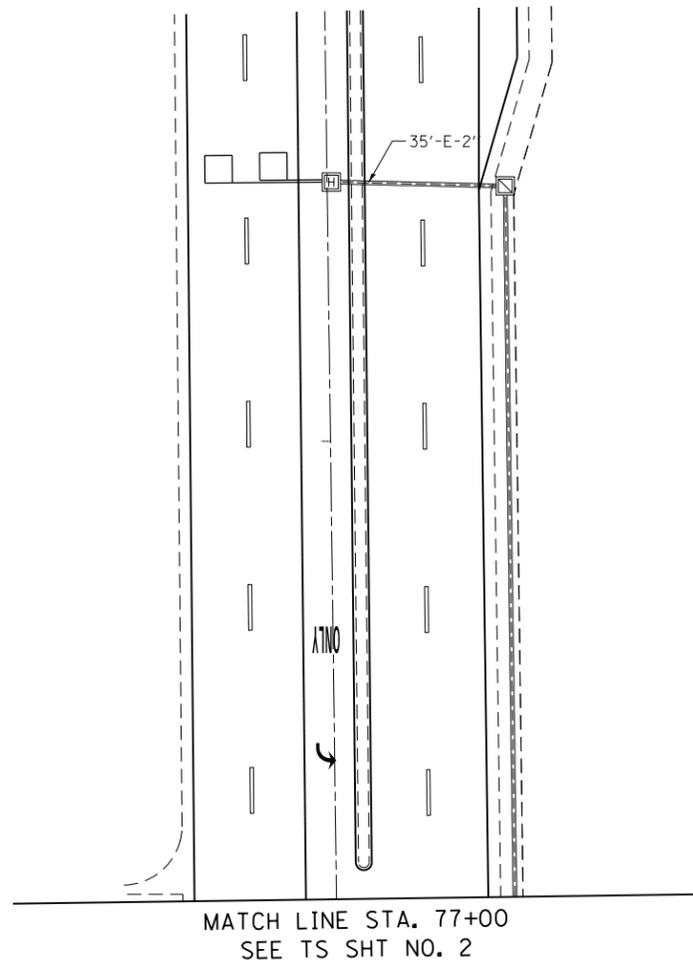
MATCH LINE A-A  
SEE TS SHT NO. 3

MATCH LINE STA. 73+50  
SEE TS SHT NO. 10

**TS 2615**

FILE NAME = p:\11\084EBID\INTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\135016\Traffic\135016-sht-ts.dgn	USER NAME = plascencia	DESIGNED - IP	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC SIGNAL MODIFICATION PLAN (SHEET 1 OF 2) IL RTE 50 (CICERO AVE) AT SAUK TRAIL</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 40.0000' / in.	CHECKED - AK	REVISED -			350	(101-EXT.) RS-3	COOK	51	31
Default	PLOT DATE = 2/17/2017	DATE - 02/08/2017	REVISED -	SCALE: SHEET OF SHEETS STA. TO STA.		CONTRACT NO. 62C43 ILLINOIS FED. AID PROJECT				

TS SHT NO. 3



FILE NAME =	USER NAME = plascencia	DESIGNED - IP	REVISED -
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\135016\135016-sht-ts.dgn		DRAWN	REVISED -
Default	PLOT SCALE = 40.0000' / in.	CHECKED - AK	REVISED -
	PLOT DATE = 2/16/2017	DATE - 02/08/2017	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

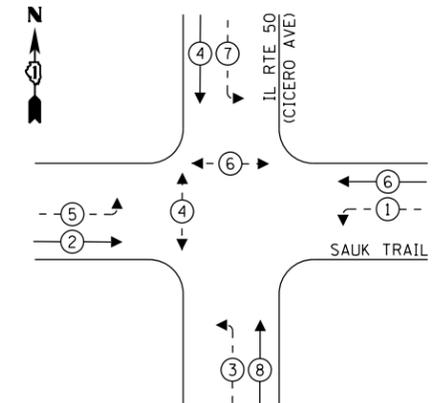
TRAFFIC SIGNAL MODIFICATION PLAN (SHEET 2 OF 2)  
IL RTE 50 (CICERO AVE) AT SAUK TRAIL

SCALE: SHEET OF SHEETS STA. TO STA.

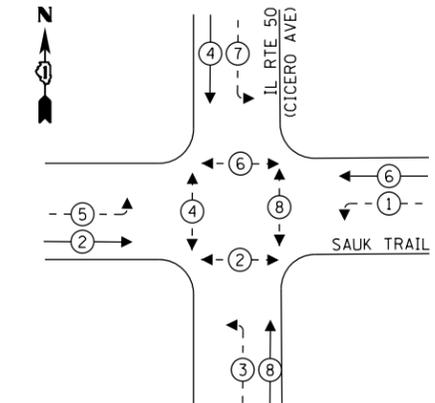
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	32
CONTRACT NO. 62C43				
ILLINOIS FED. AID PROJECT				

TS 2615

**EXISTING CONTROLLER SEQUENCE**

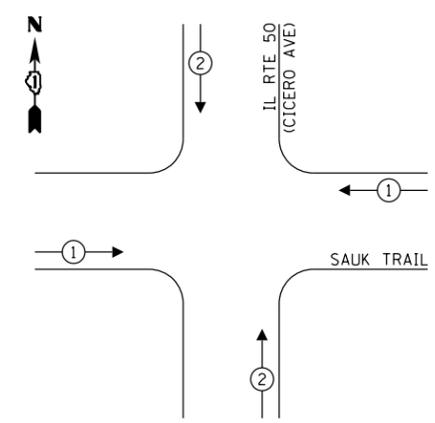


**PROPOSED CONTROLLER SEQUENCE**



- LEGEND:**
- ← \* → PROTECTED PHASE
  - ← \* - - PROTECTED/PERMITTED PHASE
  - ← \* → PEDESTRIAN PHASE
  - ← \* OL → OVERLAP

**EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE**



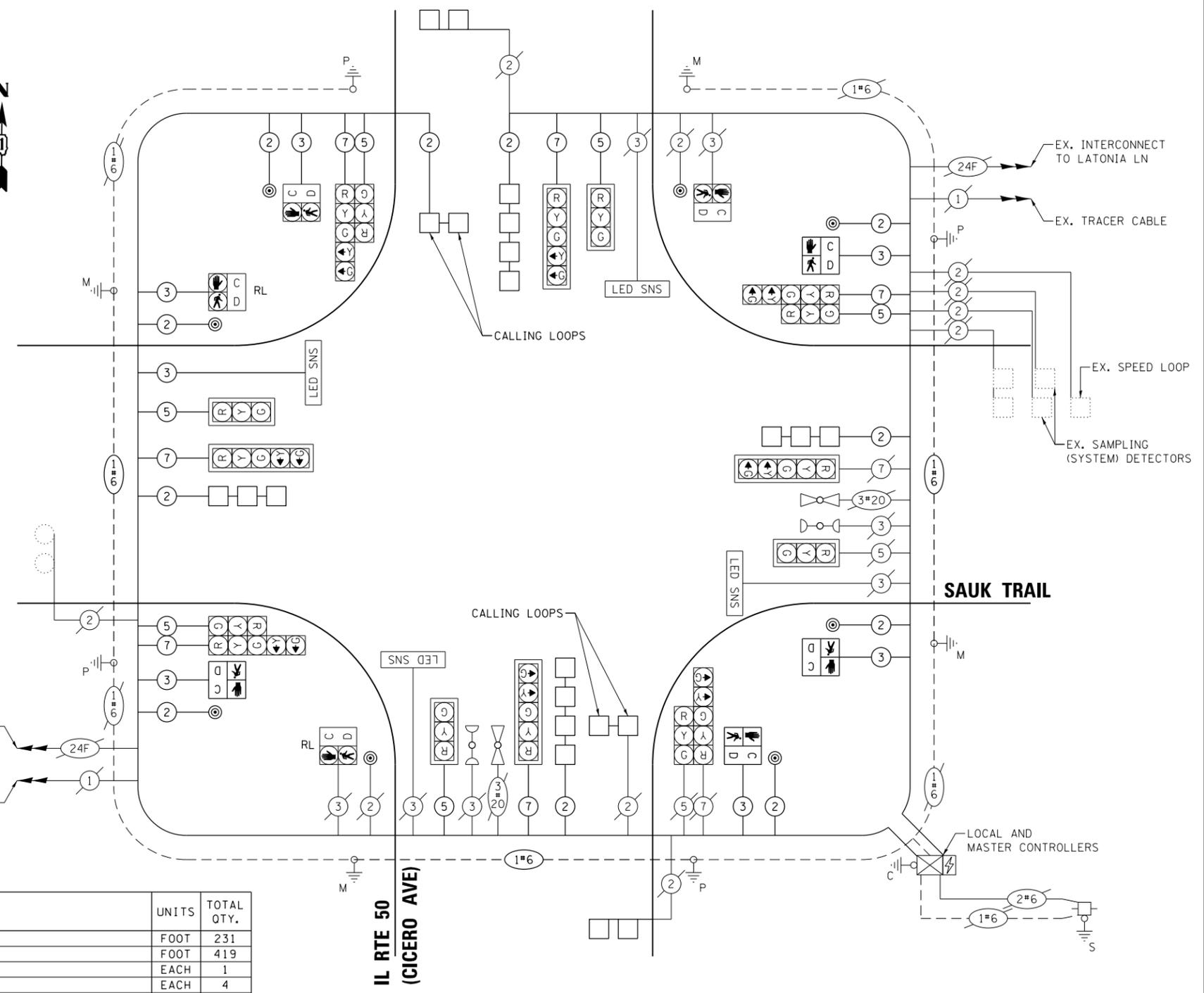
**SCHEDULE OF QUANTITIES**

**TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	16	11	50	88.0
(YELLOW)	16	20	5	21.0
(GREEN)	16	12	45	113.4
PERMISSIVE ARROW	16	10	10	24.0
PED. SIGNAL	8	20	100	40.0
CONTROLLER	2	100	100	200.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	4	65	50	130.0
LUMINAIRE	-	-	-	-
<b>TOTAL =</b>				<b>641.4</b>

ITEM DESCRIPTION	UNITS	TOTAL QTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	231
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	419
HANDHOLE	EACH	1
HEAVY-DUTY HANDHOLE	EACH	4
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,035
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,370
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	1,330
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1,370
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	875
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	500
DRILL EXISTING HANDHOLE	EACH	9
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
DETECTOR LOOP REPLACEMENT	FOOT	750
PEDESTRIAN PUSH-BUTTON	EACH	8
RELOCATE EXISTING PEDESTRIAN SIGNAL HEAD	EACH	2
MODIFY EXISTING CONTROLLER	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	3,420
REMOVE AND REINSTALL ELECTRIC CABLE FROM CONDUIT	FOOT	2,390
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REBUILD EXISTING HANDHOLE	EACH	2
REMOVE EXISTING HANDHOLE	EACH	4
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

ENERGY COSTS TO:  
 VILLAGE OF RICHTON PARK  
 4455 SAUK TRAIL  
 RICHTON PARK, IL 60471  
 ENERGY SUPPLY: CONTACT: ILYAS MOHIUDDIN  
 PHONE: (708) 235-2692  
 COMPANY: COMMONWEALTH EDISON  
 ACCOUNT NUMBER: ---



**CABLE PLAN**  
(NOT TO SCALE)

TS SHT NO. 4

FILE NAME = ...  
 USER NAME = plascencia  
 DESIGNED - IP  
 REVISIONS:  
 REVISIONS -  
 CHECKED - AK  
 REVISIONS -  
 DATE - 02/08/2017  
 REVISIONS -

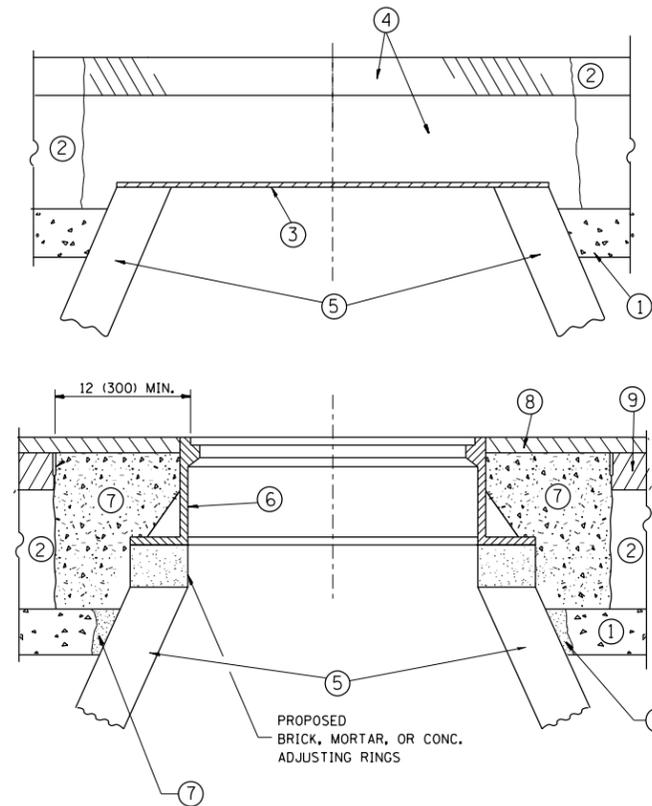
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION SEQUENCE, AND SCHEDULE OF QUANTITIES  
 IL RTE 50 (CICERO AVE) AT SAUK TRAIL

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 350 SECTION (101-EXT.) RS-3 COUNTY COOK TOTAL SHEETS 51 SHEET NO. 33 CONTRACT NO. 62C43 ILLINOIS FED. AID PROJECT

TS 2615



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

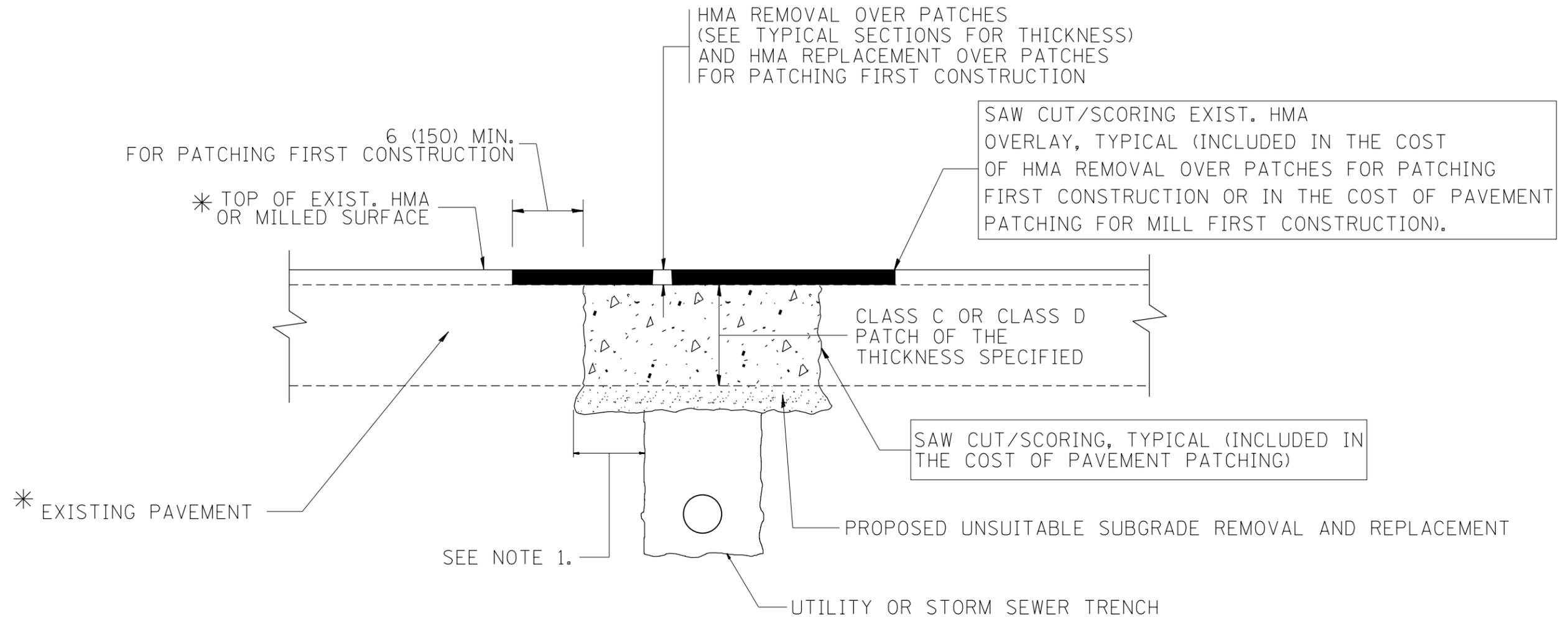
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = FUENTESJA	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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		PLOT SCALE = 100.0000' / 1in.	REVISED - R. BORO 03-09-11
		PLOT DATE = 2/16/2017	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	34
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 62C43</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

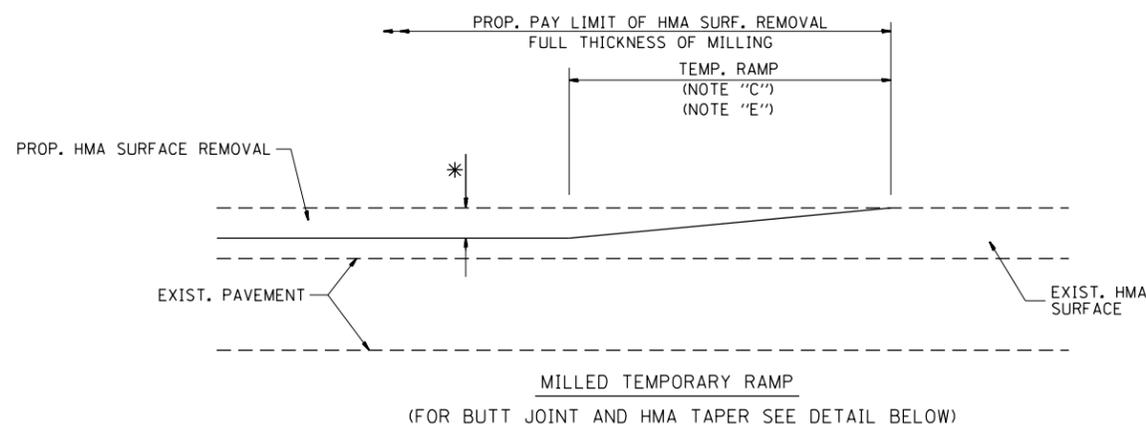
SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

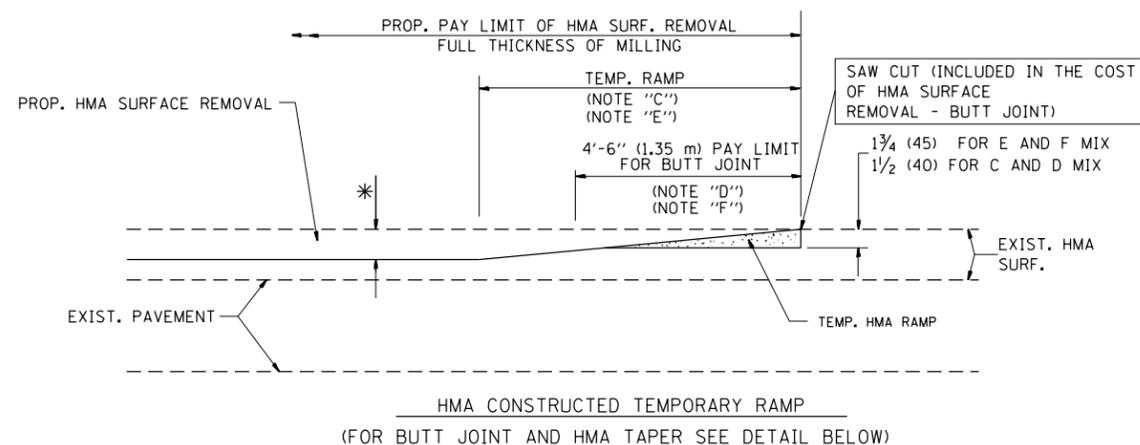
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = FUENTESJA	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI350\BROWNS\Design\Diststd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					350	(101-EXT.) RS-3	COOK	51	35
	PLOT DATE = 2/16/2017	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62C43				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



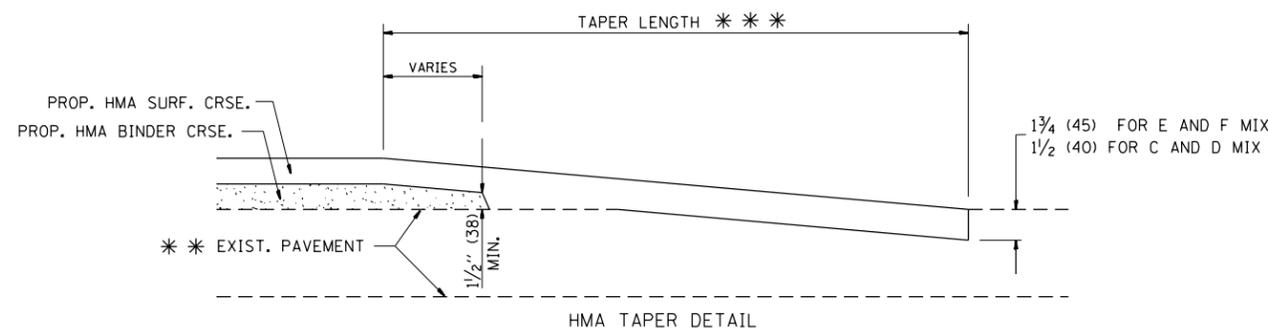
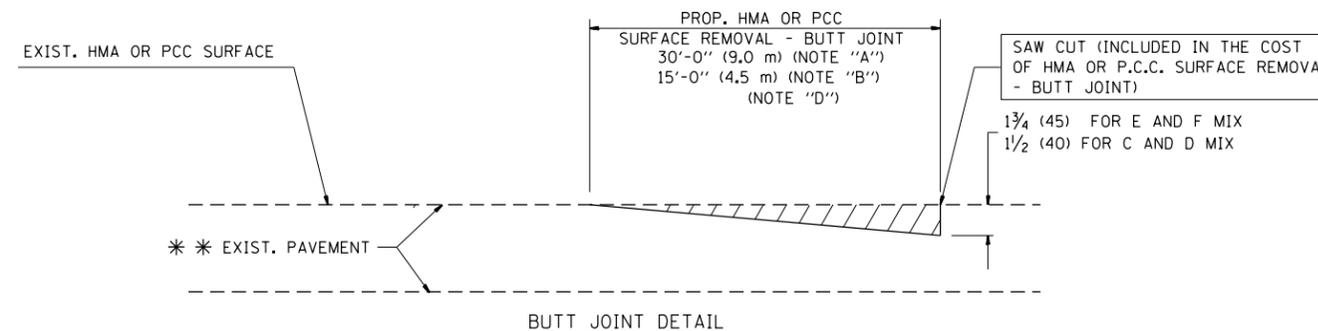


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

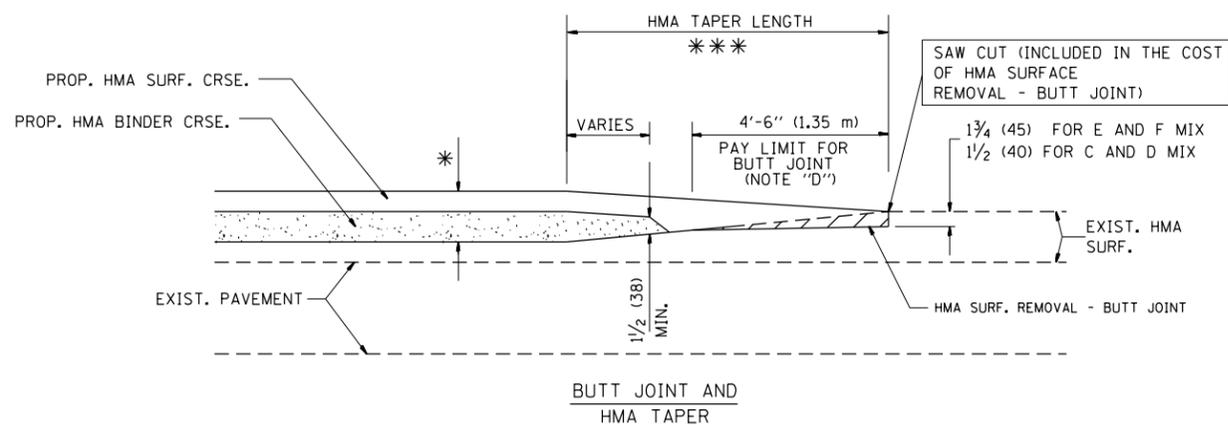
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

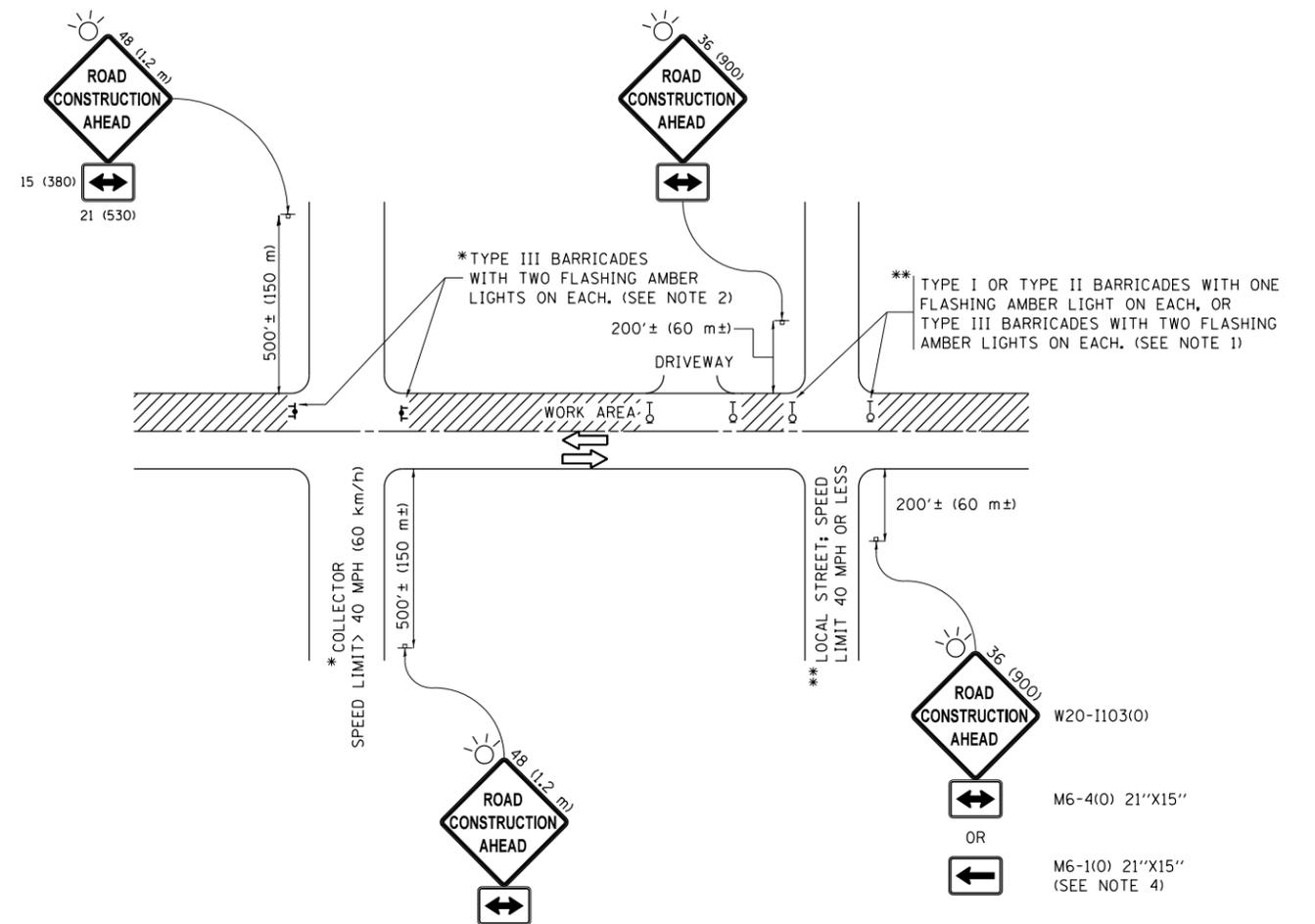
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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 2/16/2017	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	37
BD400-05 BD32		CONTRACT NO. 62C43		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

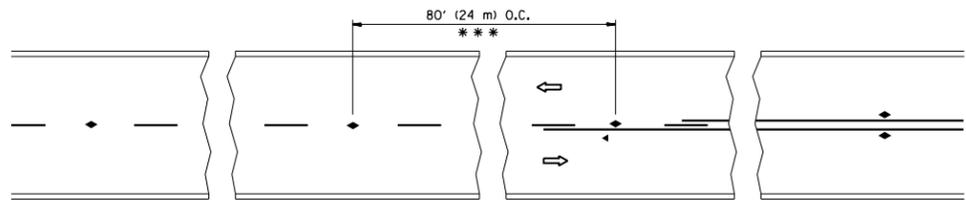
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p:\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI350\Drawings\Design\Diststd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 2/16/2017	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

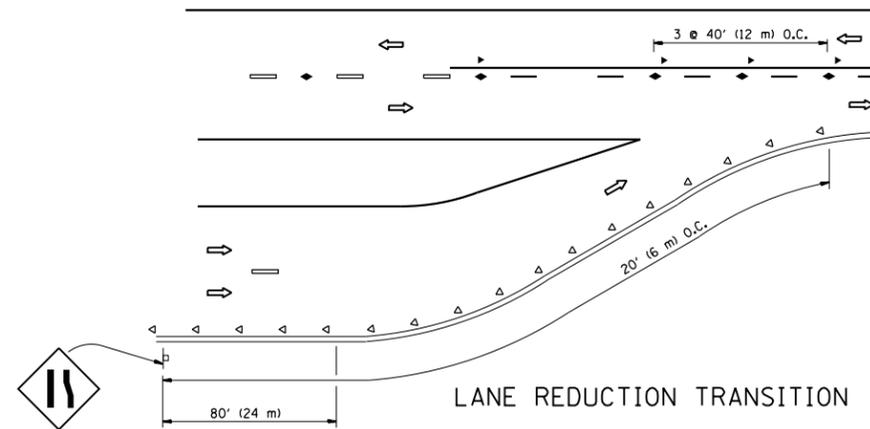
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	38
<b>TC-10</b>			<b>CONTRACT NO. 62C43</b>	
ILLINOIS FED. AID PROJECT				

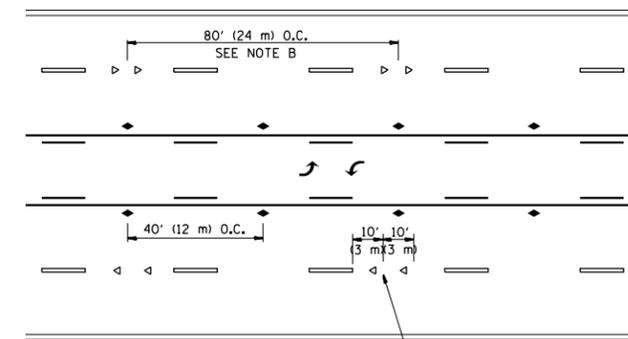


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

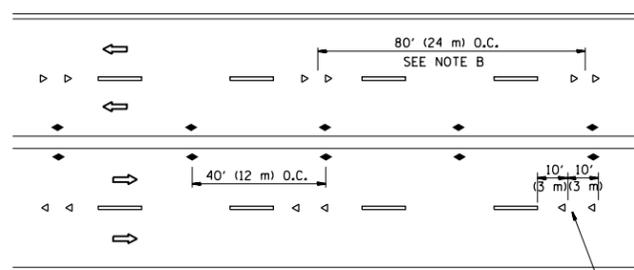
TWO-LANE/TWO-WAY



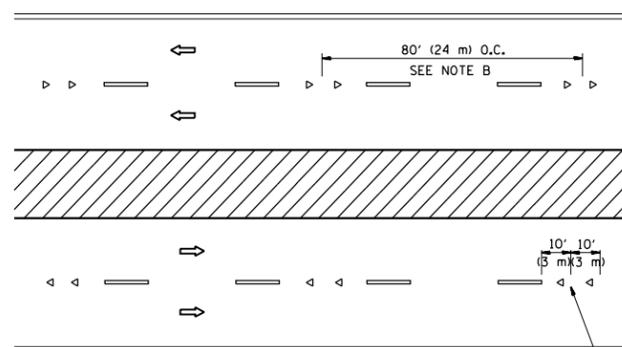
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

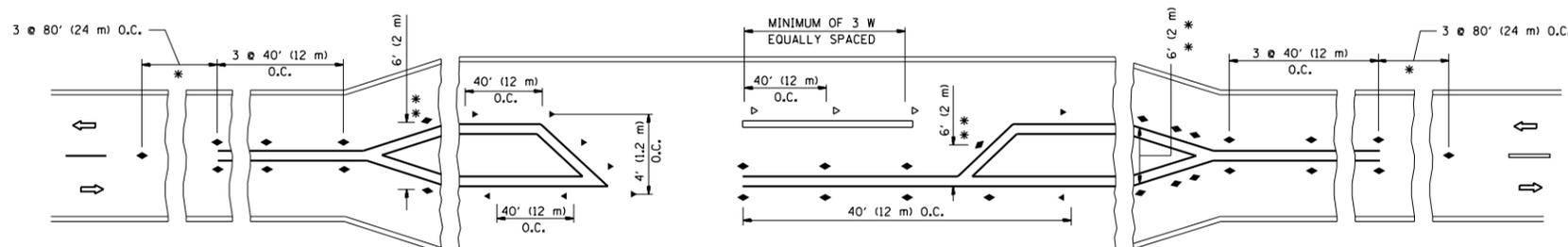
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

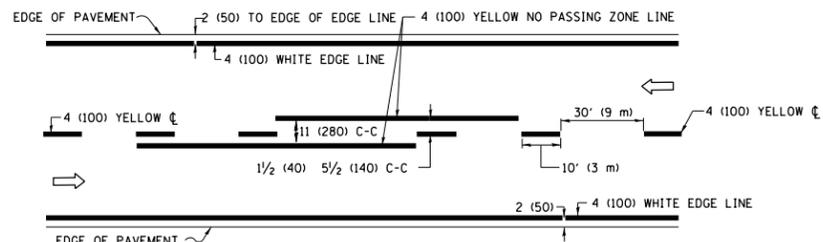
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw:\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI350\Drawings\Design\Diststd.dgn		REVISED - T. RAMMACHER 03-12-99	REVISED - T. RAMMACHER 01-06-00
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - C. JUCIUS 09-09-09
	PLOT DATE = 2/16/2017	DATE -	

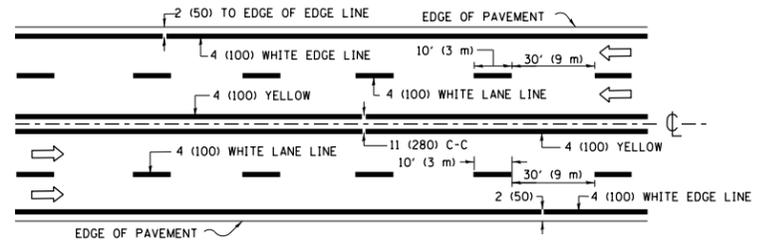
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

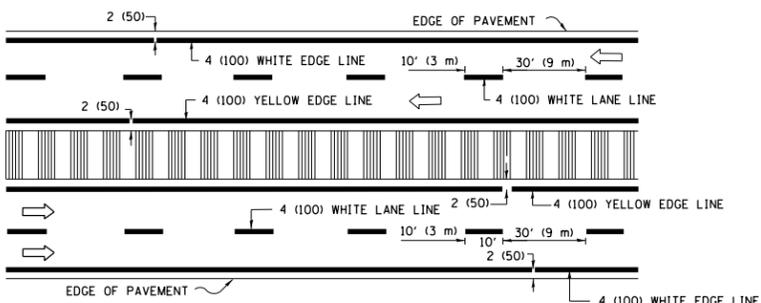
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	39
TC-11			CONTRACT NO. 62C43	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

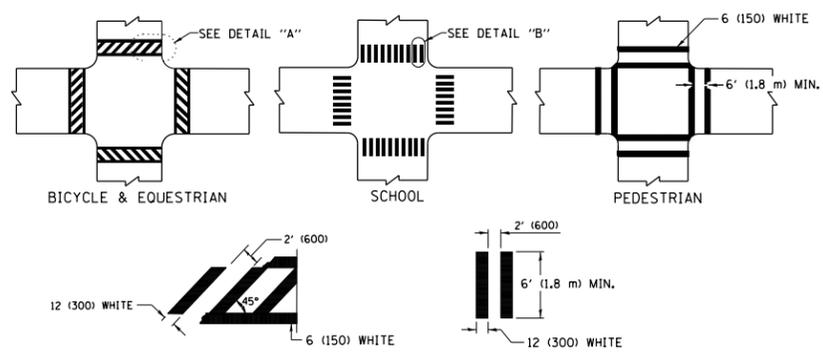


**MULTI-LANE UNDIVIDED**



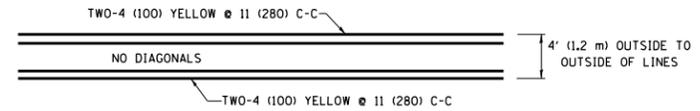
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

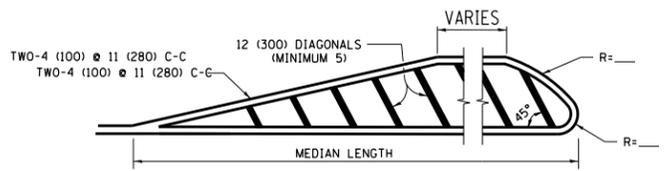


**TYPICAL CROSSWALK MARKING**

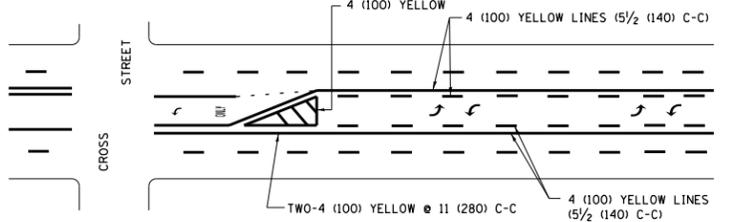
\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



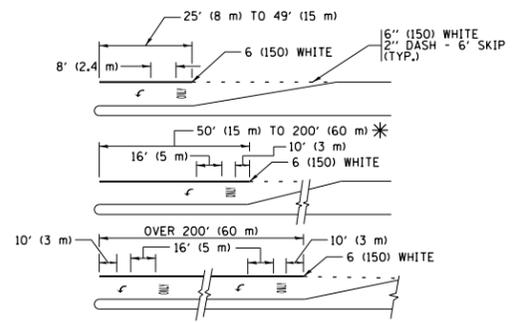
**4' (1.2 m) WIDE MEDIANS ONLY**



**MEDIANS OVER 4' (1.2 m) WIDE**



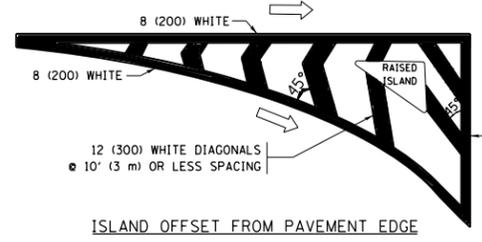
**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**



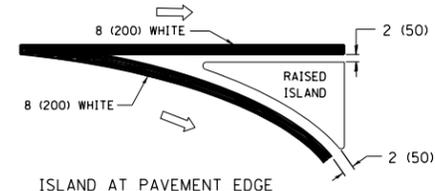
FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

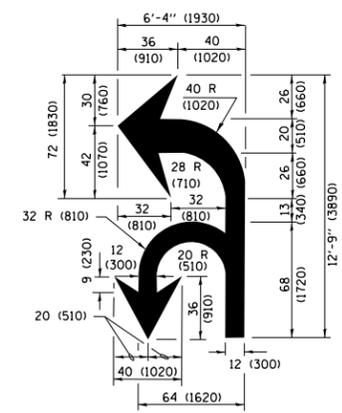
**TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING**



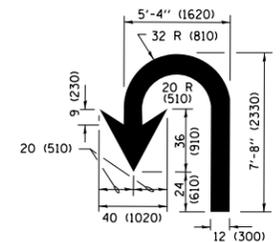
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (22.5 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = FUENTESJA	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01350\01350\Design\Diststd.dgn			REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 2/16/2017	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

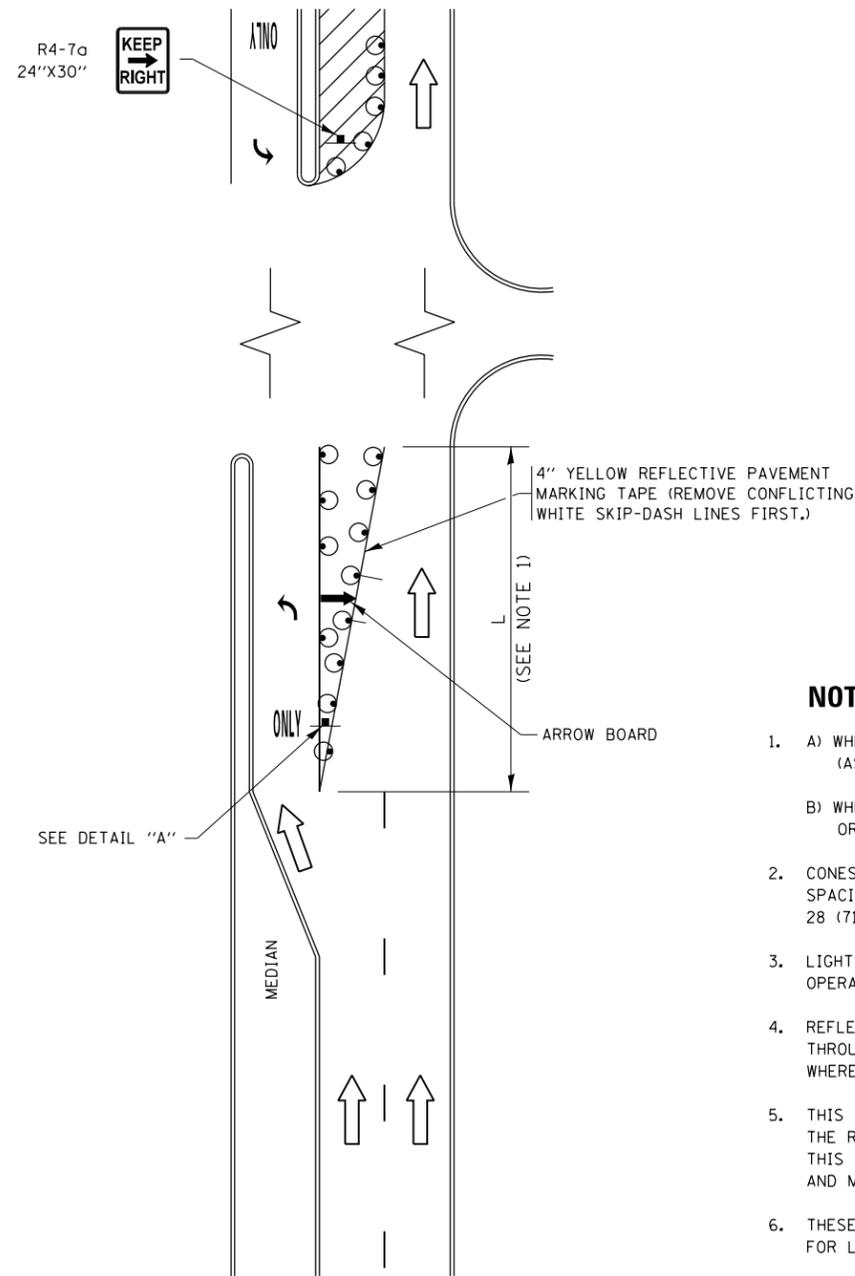
**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.
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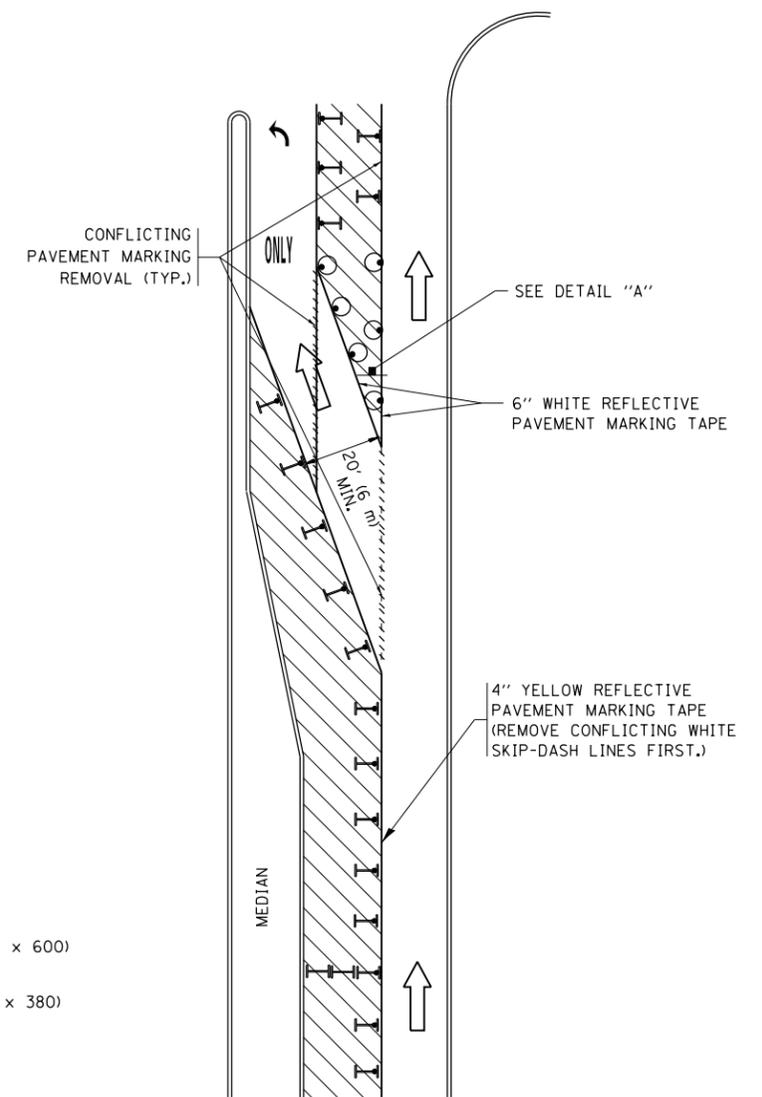
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	40
TC-13		CONTRACT NO. 62C43		
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



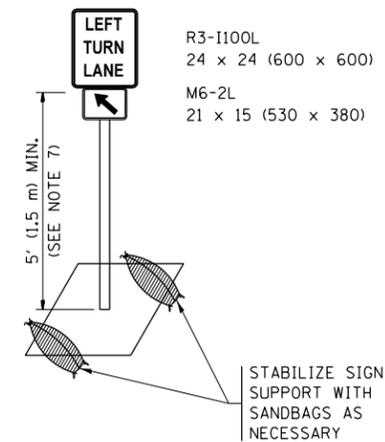
**FIGURE 2**

### LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

### NOTES:

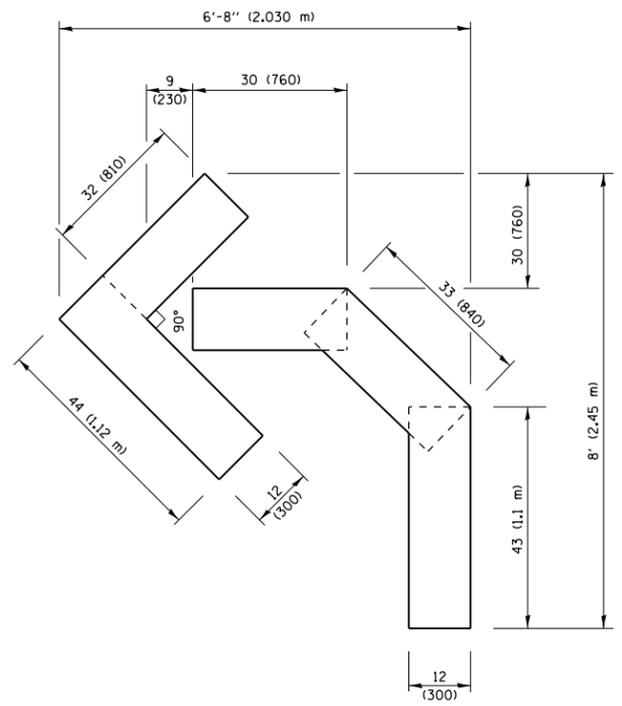
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



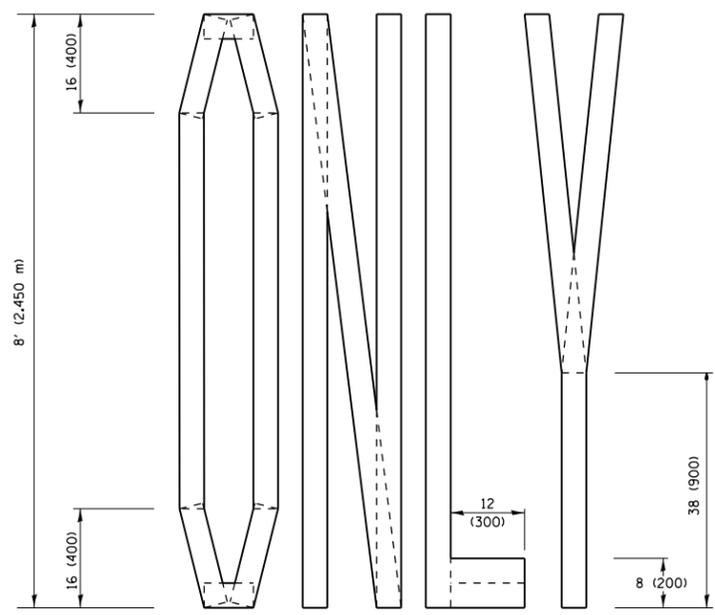
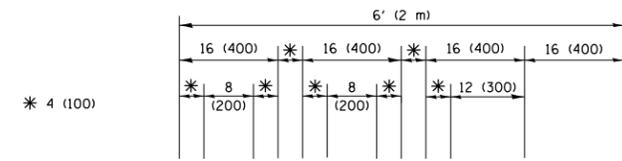
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

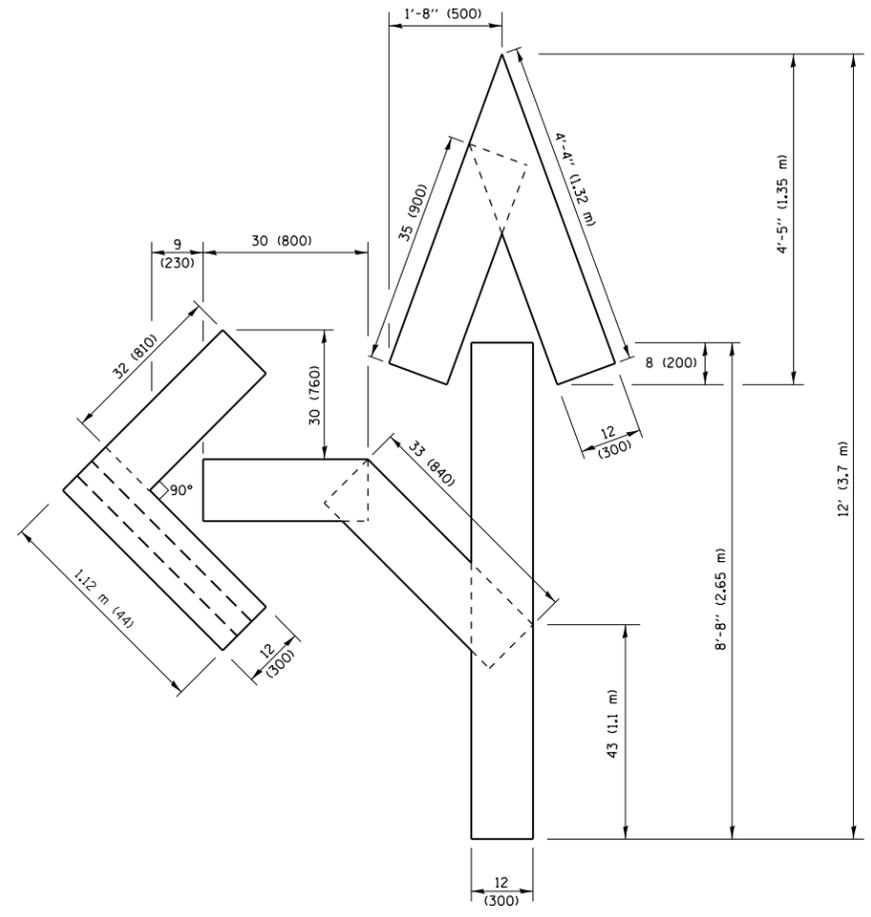
FILE NAME =	USER NAME = FUENTESJA	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI350\REVISED Design\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			350	(101-EXT.) RS-3	COOK	51	41	
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			<b>TC-14</b>		<b>CONTRACT NO. 62C43</b>			
	PLOT DATE = 2/16/2017	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.	



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

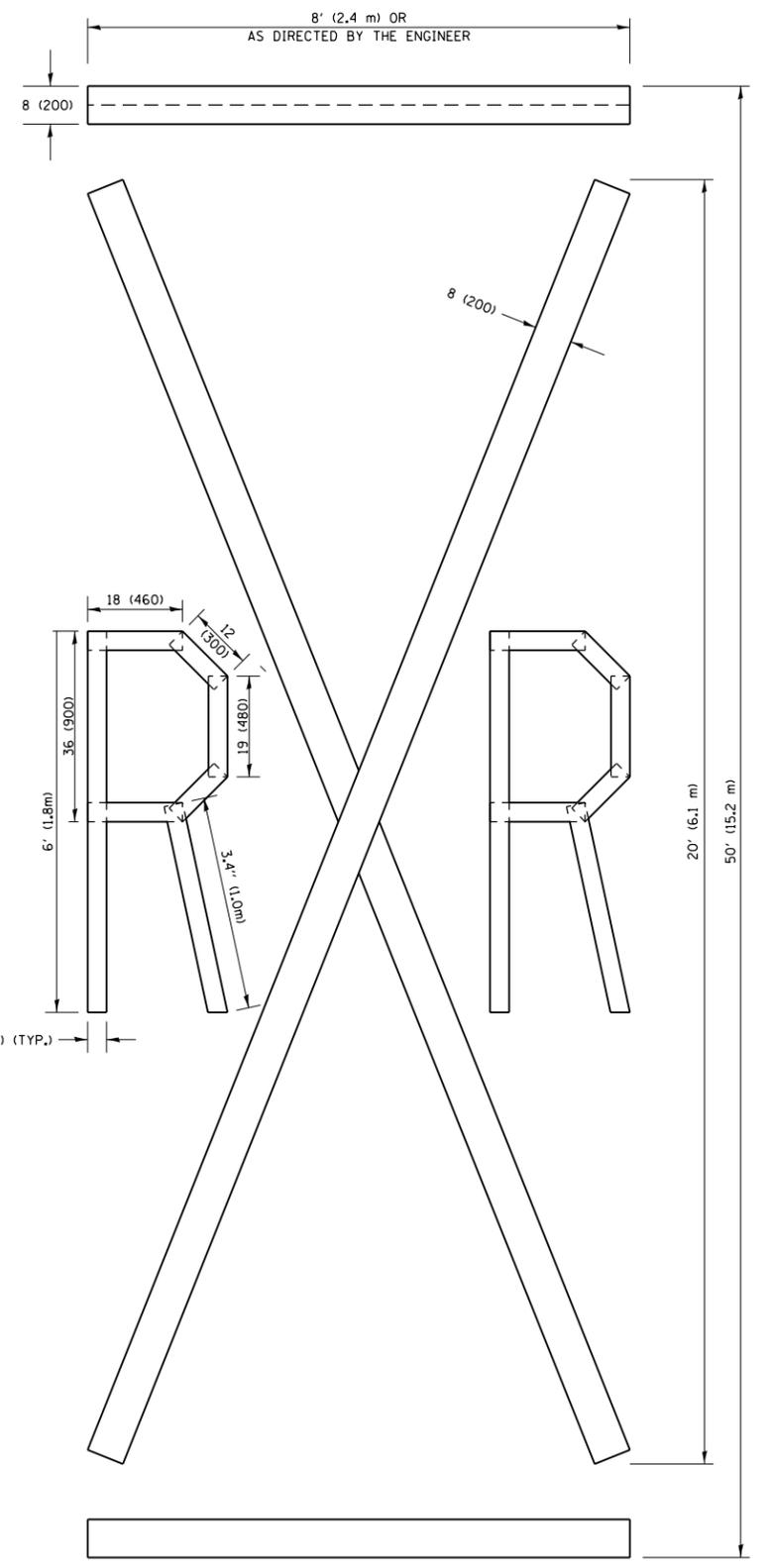


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED  
 IN LINEAR FEET OF 4" LINES TO MATCH THE  
 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS  
 THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

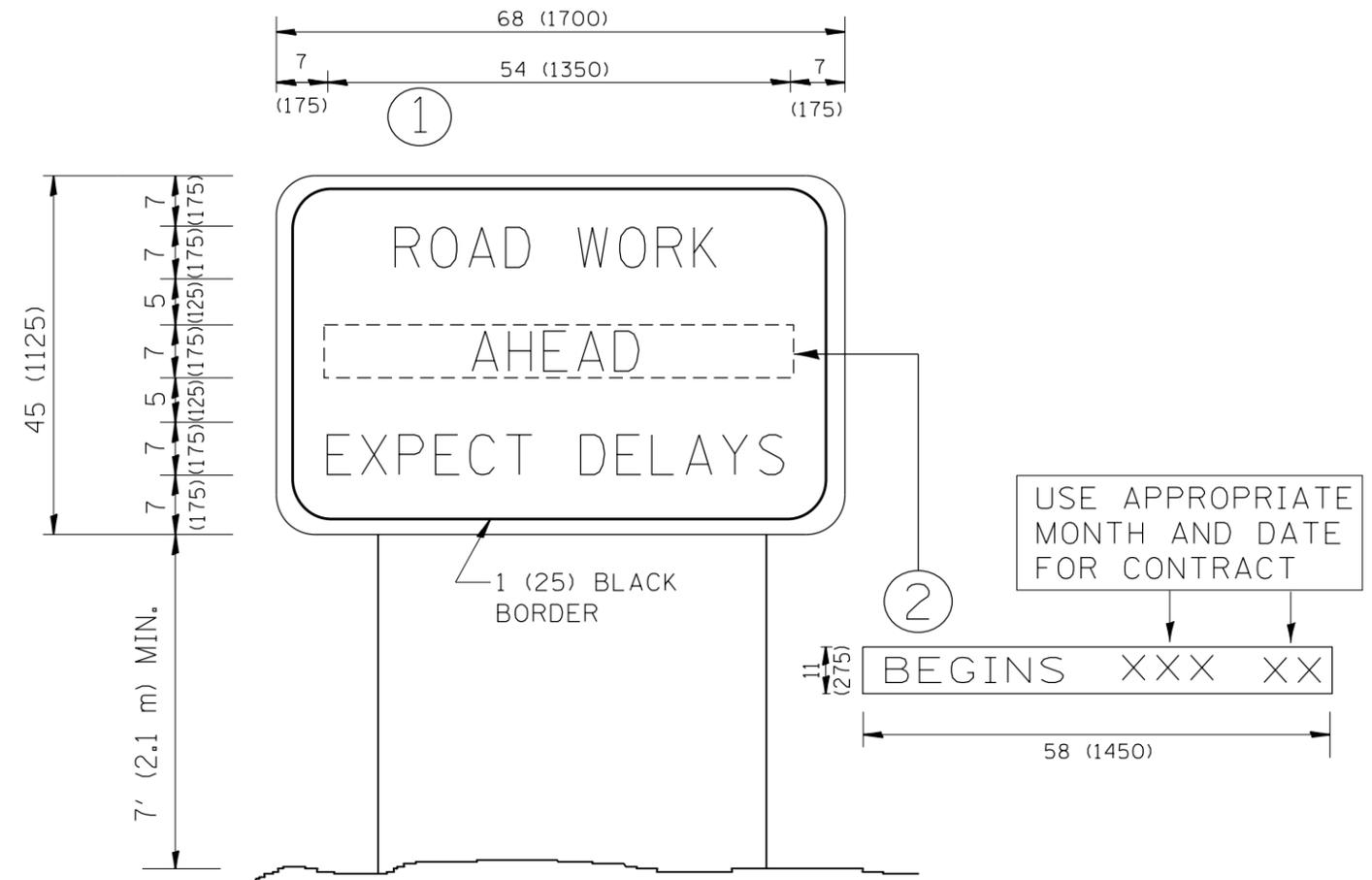
All dimensions are in inches (millimeters)  
 unless otherwise shown.

FILE NAME =	USER NAME = FUINTESJA	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
p:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01350\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	42
<b>TC-16</b>		<b>CONTRACT NO. 62C43</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

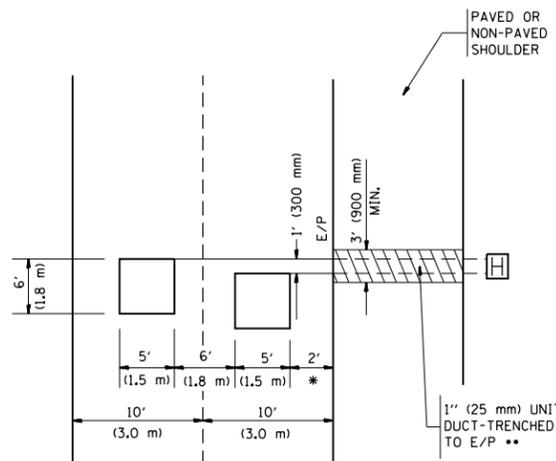
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = FUENTESJA	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI350\Drawings\Design\Diststd.dgn			REVISED - R. MIRS 12-11-97			350	(101-EXT.) RS-3	COOK	51	43
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99				<b>TC-22</b>		<b>CONTRACT NO. 62C43</b>		
PLOT DATE = 2/16/2017	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT	

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



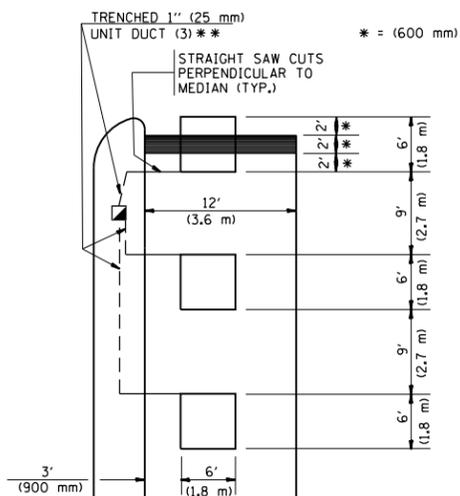
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



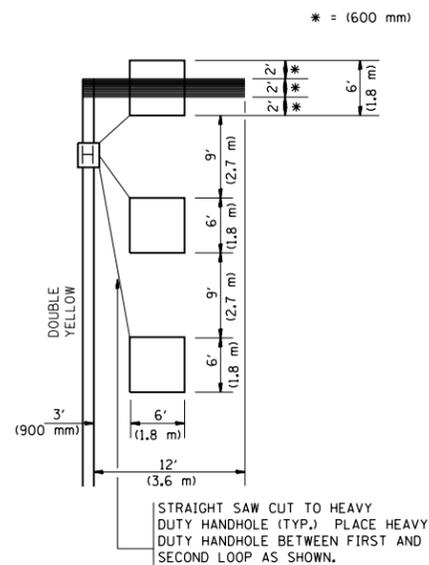
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



\* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

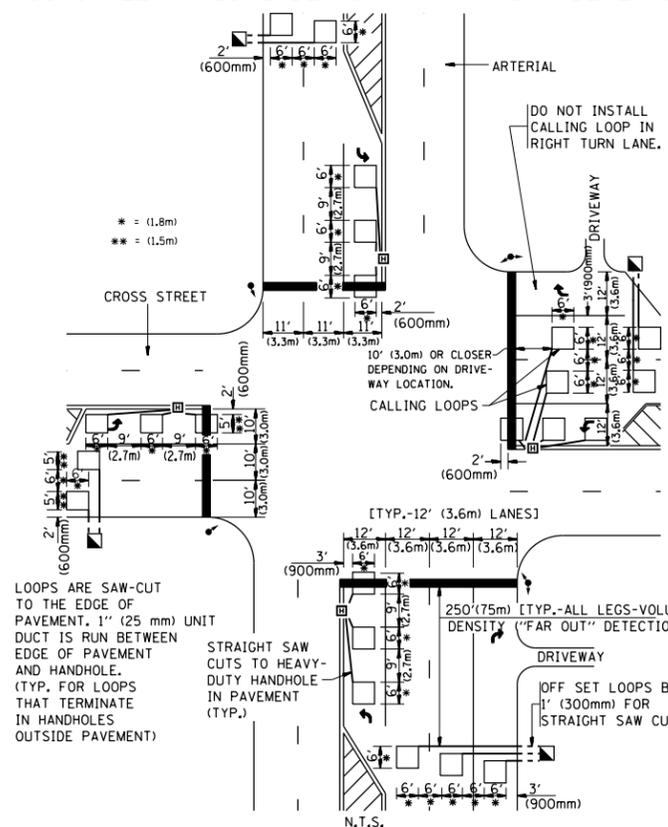
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

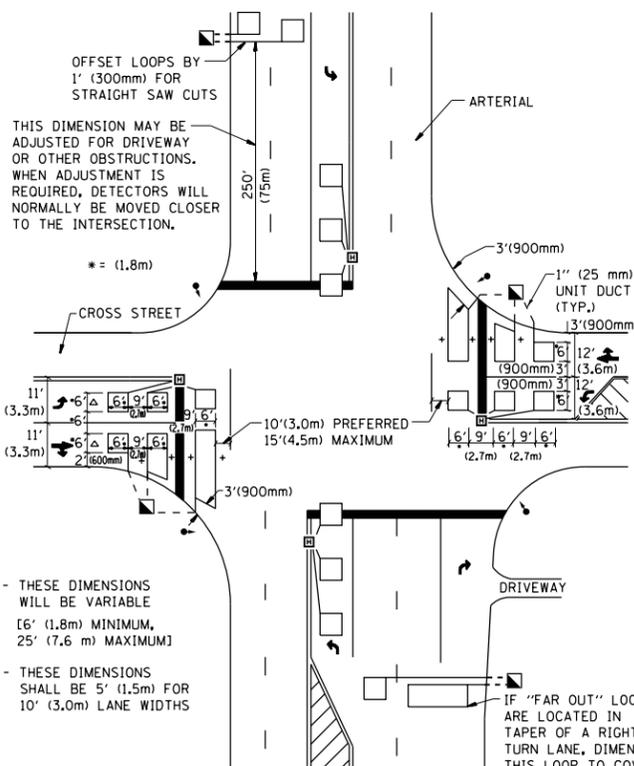


LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

DETAIL 1  
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2  
N.T.S.

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PLOT DATE = 2/16/2017	DATE -	REVISED -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

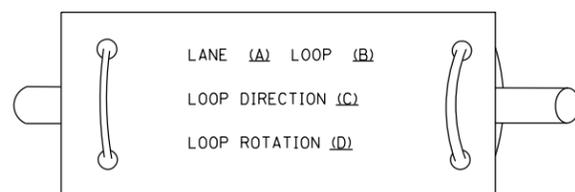
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	44
TS-07		CONTRACT NO. 62C43		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



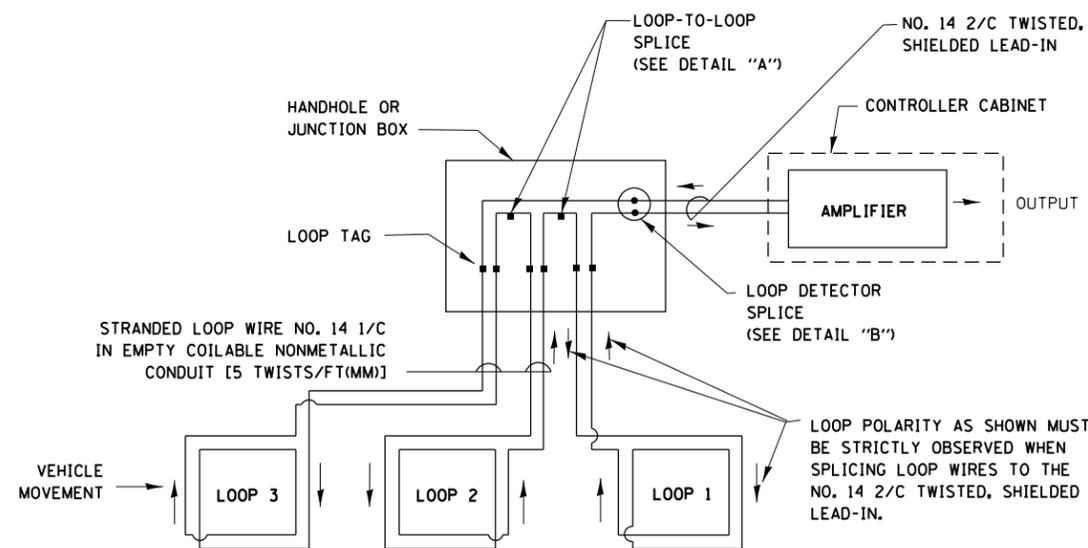
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

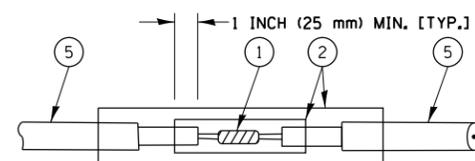


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

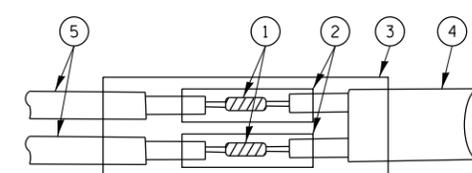


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

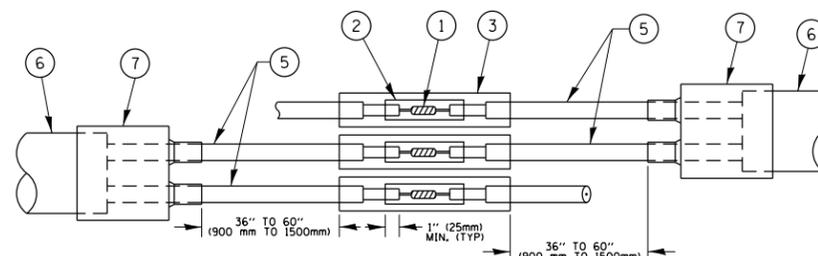


DETAIL "A"  
LOOP-TO-LOOP SPLICE

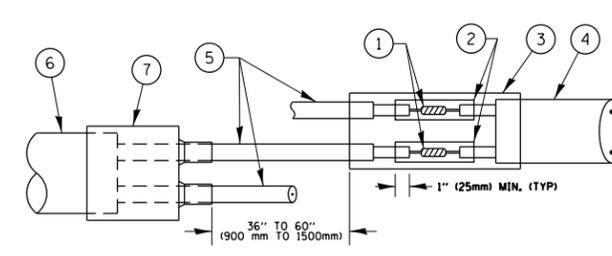


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**PRE-FORMED LOOP**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

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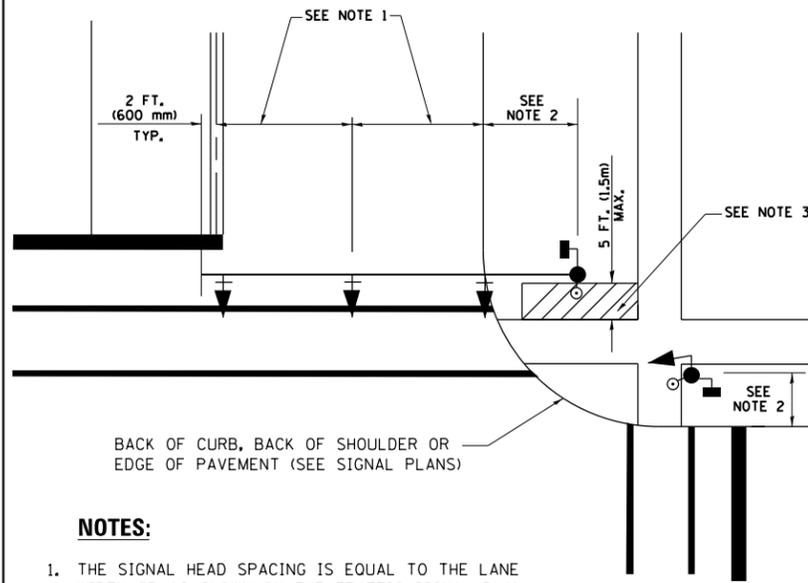
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	46
<b>TS-05</b>		<b>CONTRACT NO. 62C43</b>		
ILLINOIS FED. AID PROJECT				

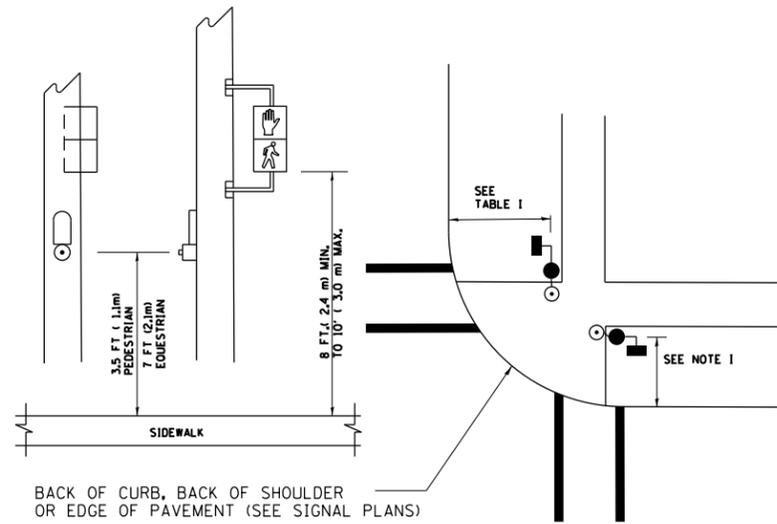
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST  
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR  
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN  
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

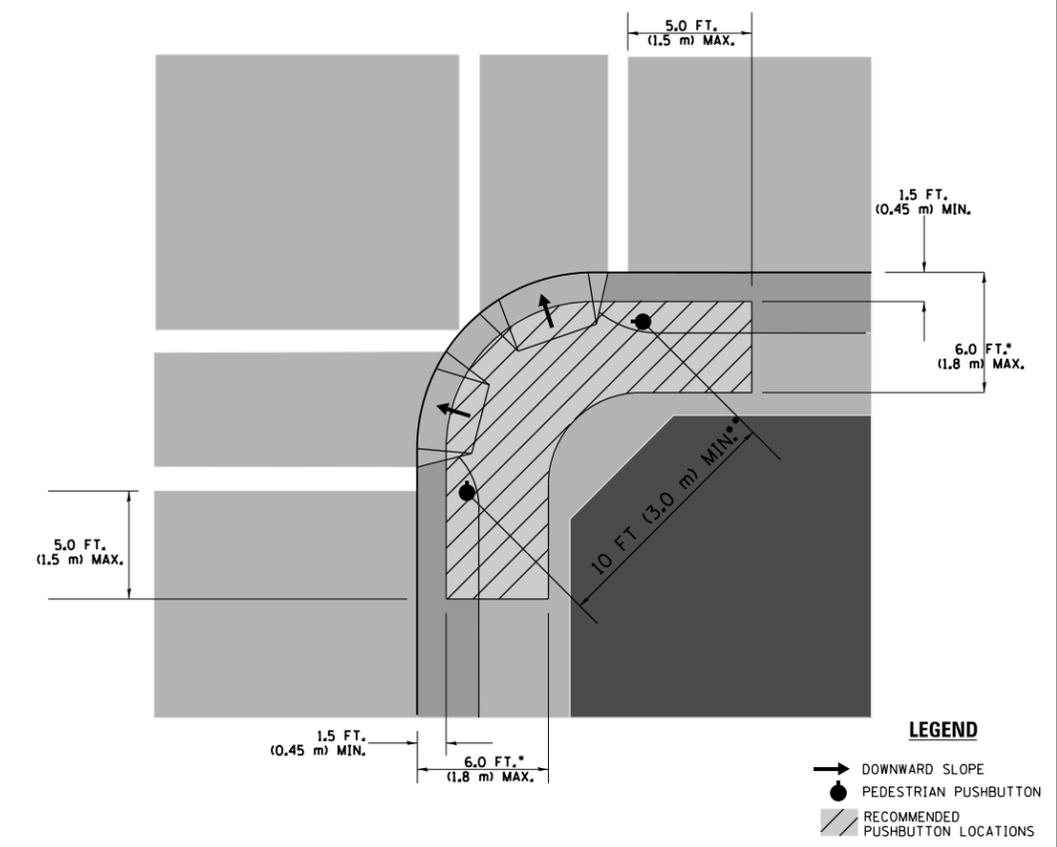
**PEDESTRIAN SIGNAL POST  
AND  
PEDESTRIAN PUSH BUTTON POST**



**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



**LEGEND**

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

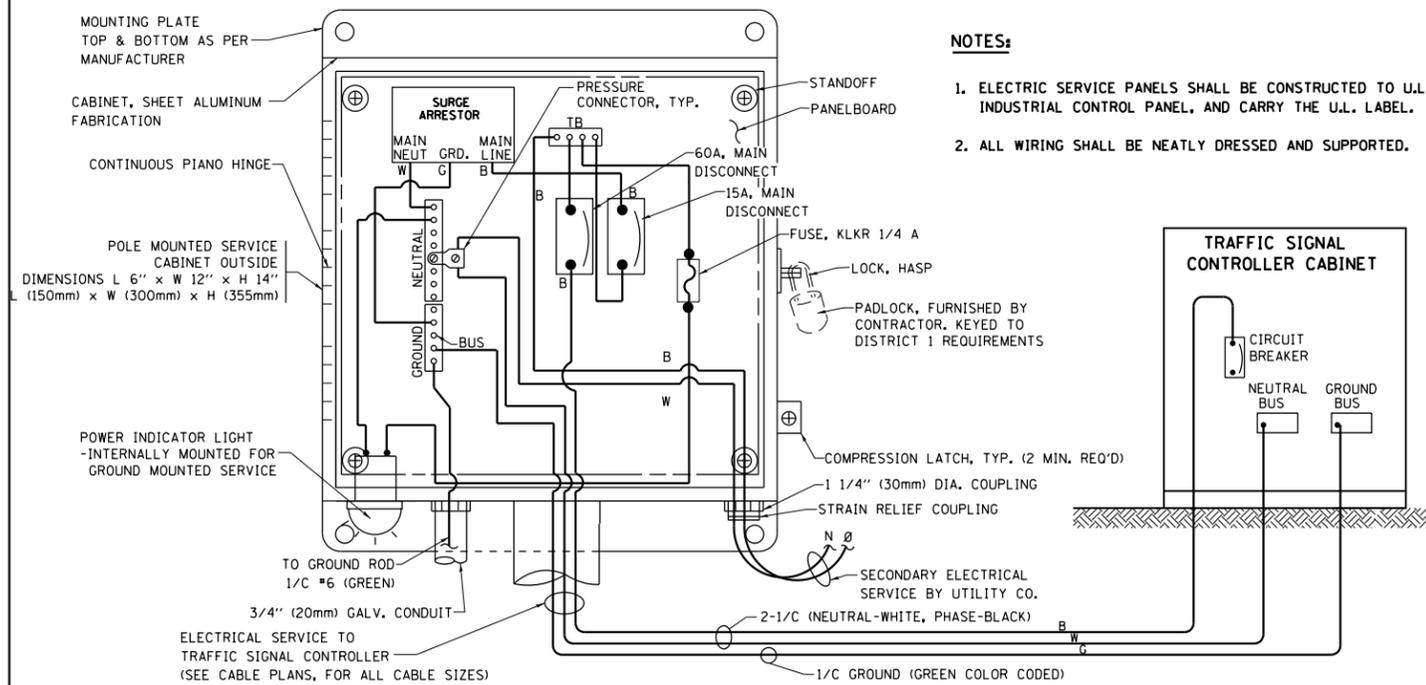
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

**TRAFFIC SIGNAL EQUIPMENT OFFSET**

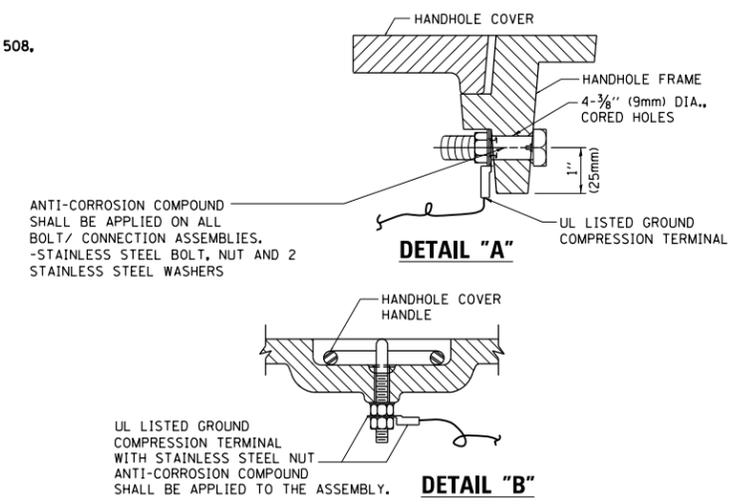
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

**NOTES:**

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

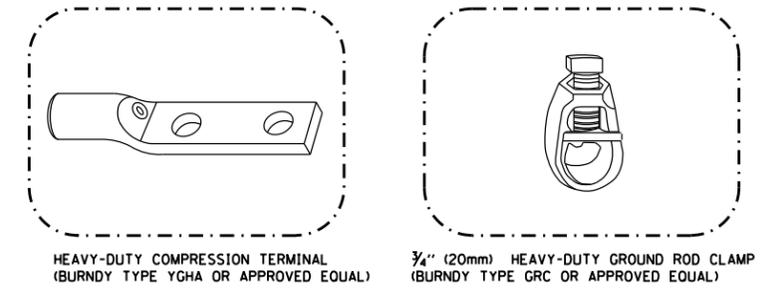
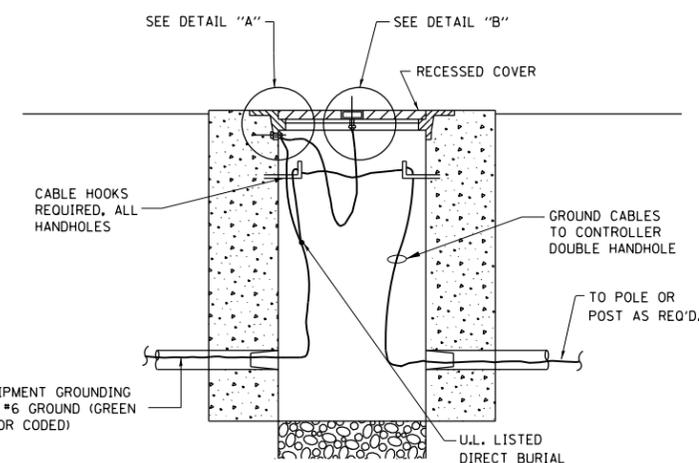


**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE) SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)**

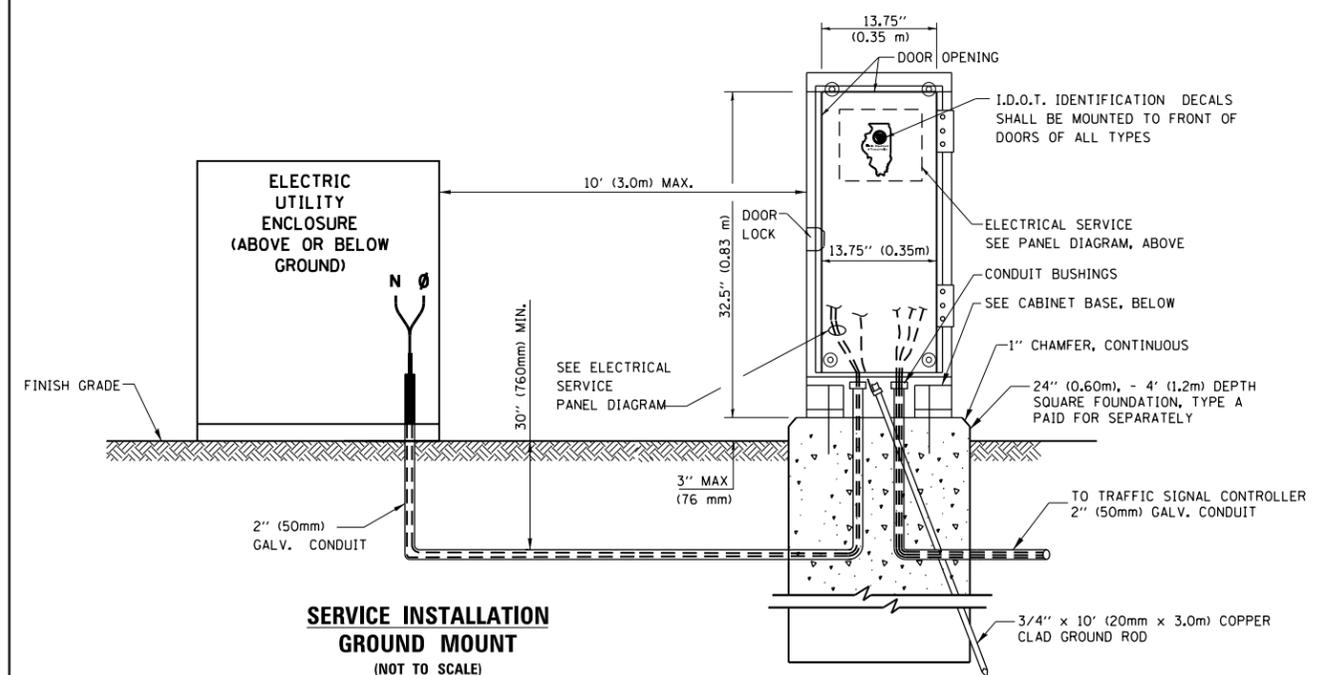
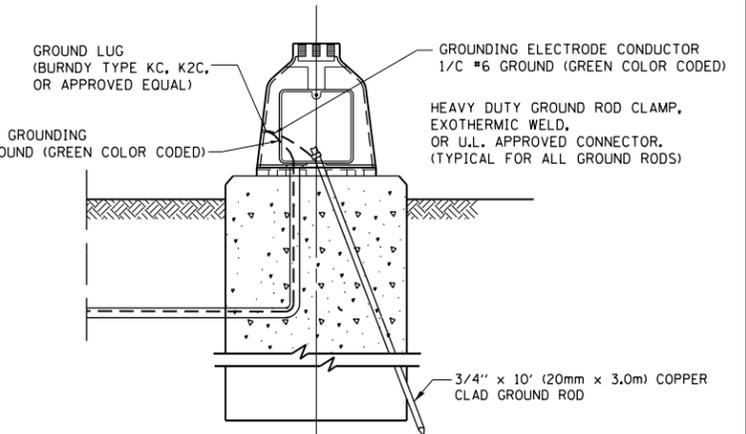
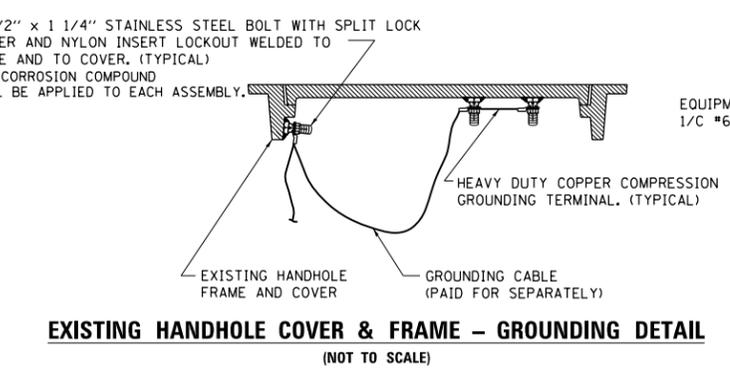


**NOTES:**  
**GROUNDING SYSTEM**

- THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN ENCLOSED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
- THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
- ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
- THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

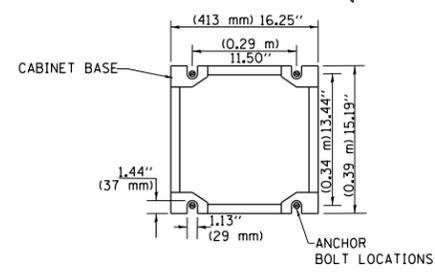


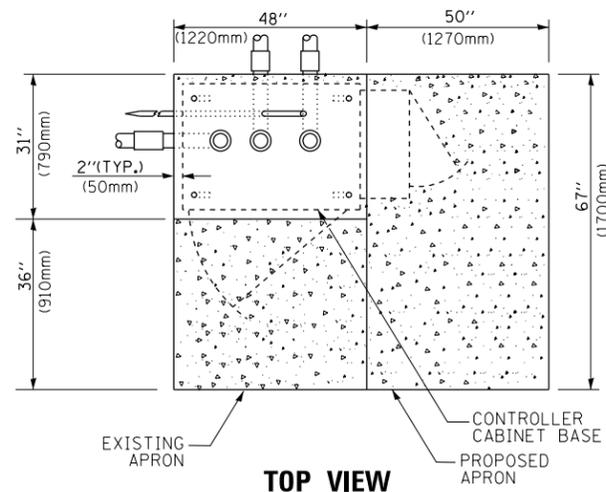
- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
  - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



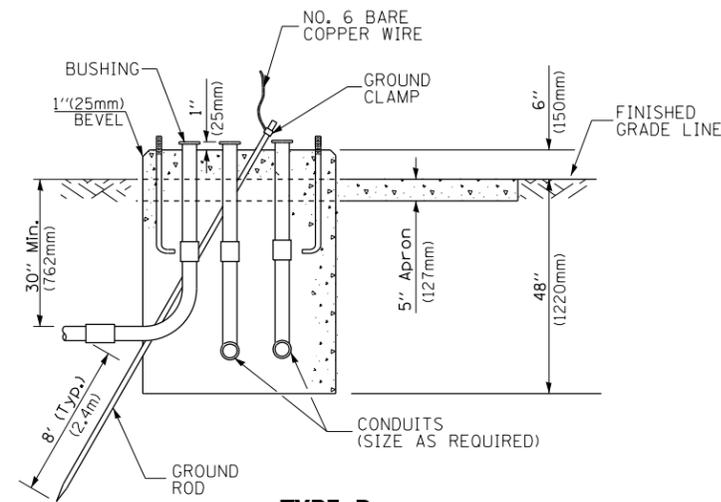
**SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)**

**CABINET - BASE BOLT PATTERN (NOT TO SCALE)**

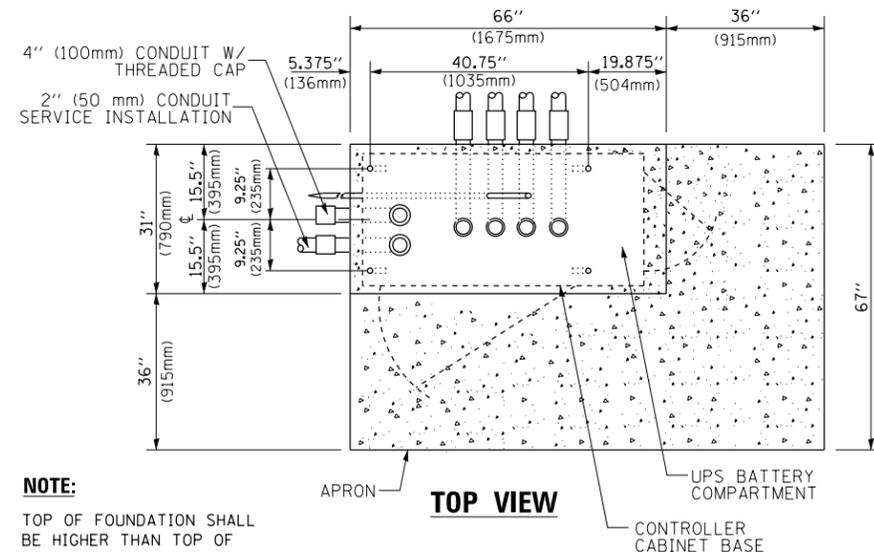




**TOP VIEW**



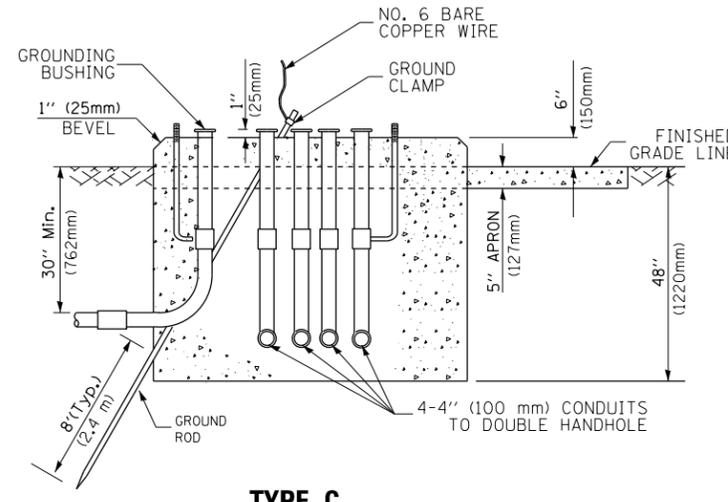
**TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**



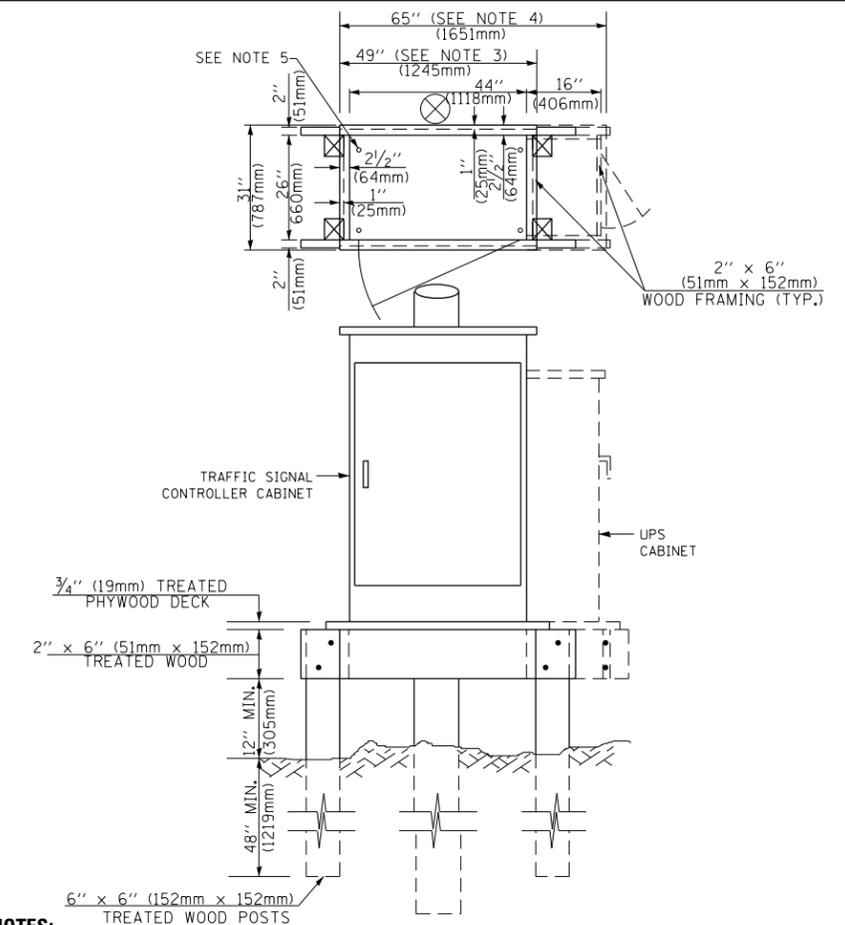
**TOP VIEW**

**NOTE:**

TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C  
FOR GROUND MOUNTED  
SUPER P (TYPE IV) AND SUPER R (TYPE V)  
CONTROLLER CABINETS**



**NOTES:**

- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
- PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
- DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
- FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

**DEPTH OF FOUNDATION**

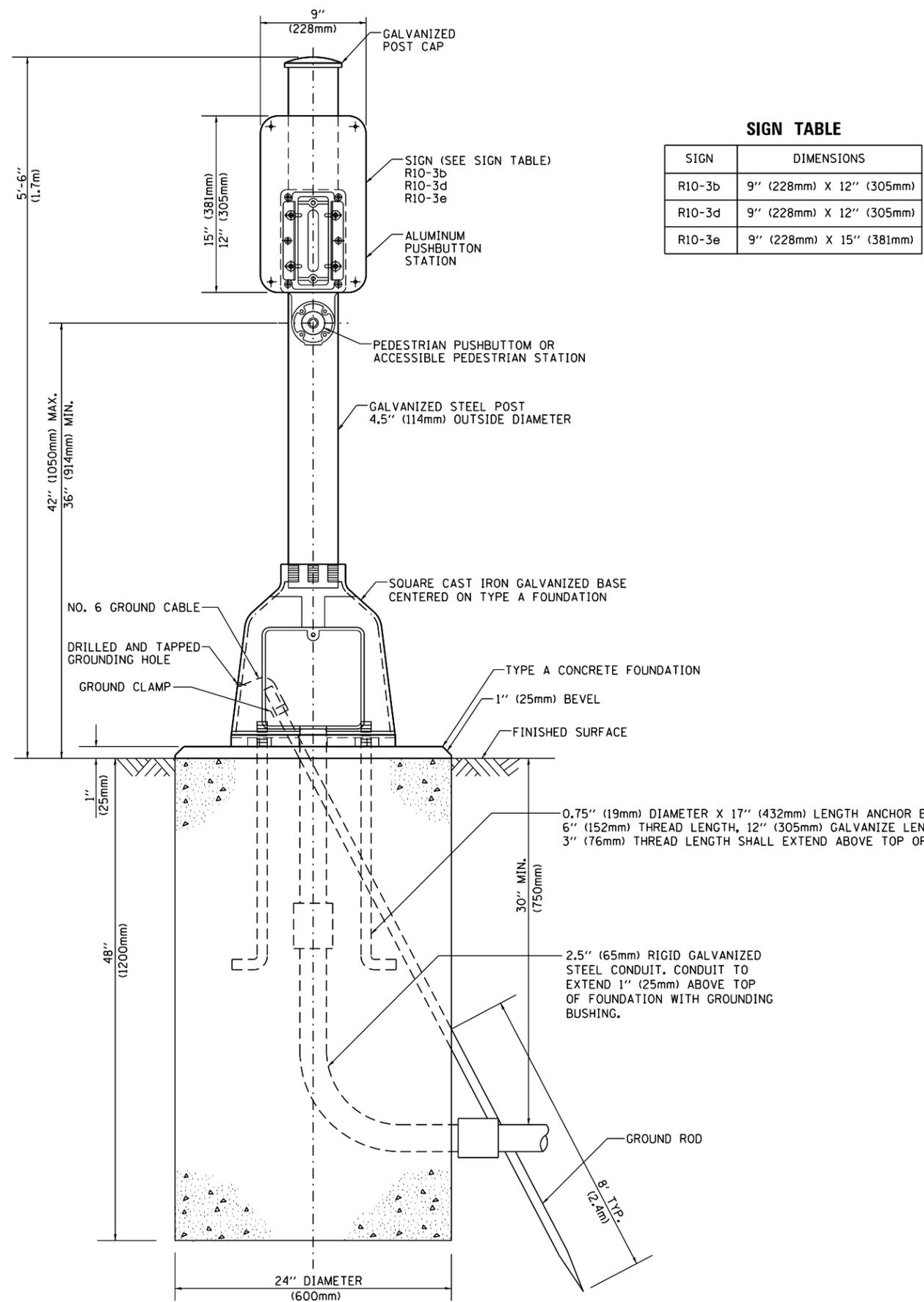
MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m) and up to 85' (25.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

**NOTES:**

- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
- Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
- Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
- For mast arm assemblies with dual arms refer to state standard 878001..

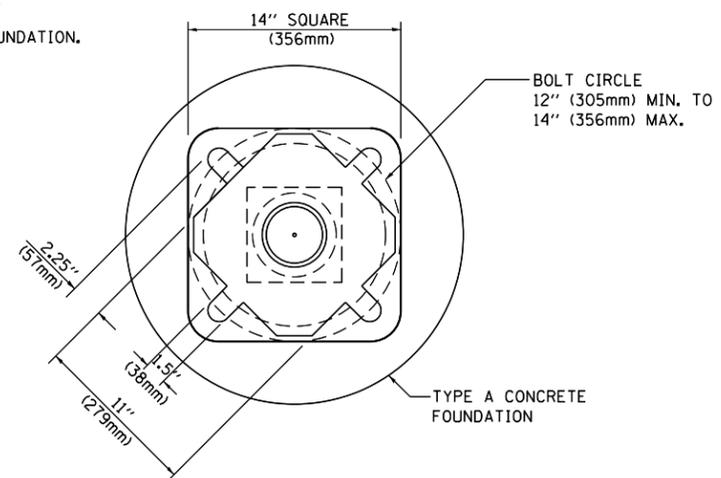
**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**





**SIGN TABLE**

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



**BOLT PATTERN**

**PEDESTRIAN PUSH BUTTON POST, TYPE A**

FILE NAME =	USER NAME = plascencia	DESIGNED -	REVISED -
p:\11\084EBID\INTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\11350\Drawings\Traffic\1135016-sht-ts.dgn		DRWN -	REVISED -
Default	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
	PLOT DATE = 2/16/2017	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE    SHEET 7 OF 7 SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
350	(101-EXT.) RS-3	COOK	51	51
<b>TS-05</b>			<b>CONTRACT NO. 62C43</b>	
ILLINOIS FED. AID PROJECT				