

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE. 3887	SECTION A-R-RS-3	COUNTY KANE	TOTAL SHEETS 24	SHEET NO. 1
ILLINOIS			CONTRACT NO. 62C69	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED HIGHWAY PLANS

F.A.U. ROUTE 3887: IL-31 (Batavia Ave.)
FROM MAIN ST. TO MAPLE AVE.

SECTION: A-R-RS-3
PROJECT STP-3887(011)
RESURFACING (3P), PEDESTRIAN RAMPS
KANE COUNTY

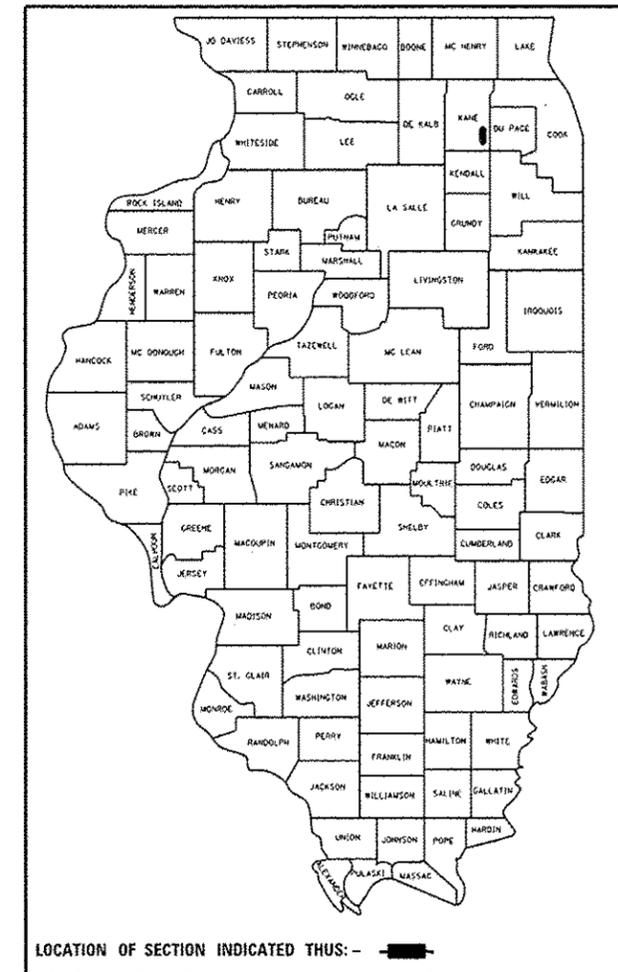
THIS IMPROVEMENT IS LOCATED IN
THE VILLAGE OF BATAVIA AND NORTH AURORA

TRAFFIC DATA

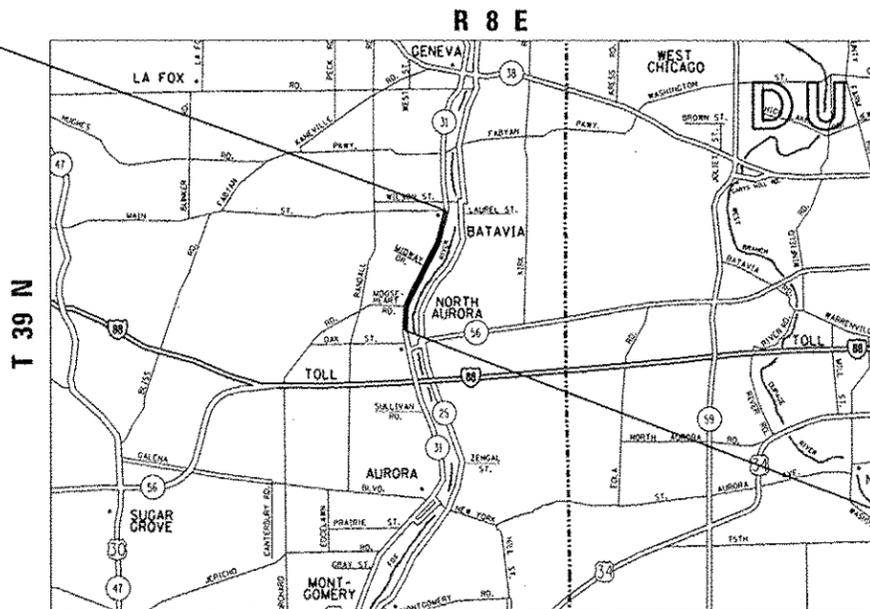
2016 ADT = 13900
SPEED LIMIT = 30 - 45 MPH

C-91-376-16

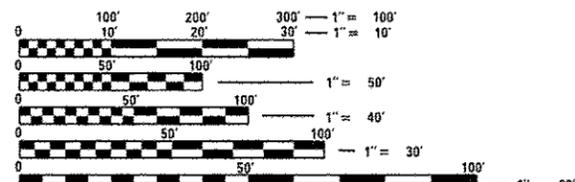
D-91-376-16



PROJECT ENDS
STA. 169 + 22.40



PROJECT BEGINS
STA. 24 + 22.12



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: ALAIN MIDY (847) 221-3056
PROJECT MANAGER: ISSAM RAYYAN (847) 705-4178

GROSS & NET LENGTH = 14500 FT. = 2.746 MILE

CONTRACT NO. 62C69

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED February 6, 2017
Amiriy J. R...
REGIONAL ENGINEER

Mar 24, 2017
Maureen M. Addis, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Mar 24, 2017
Amelle...
DIRECTOR OF PROGRAM DEVELOPMENT

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

STATE STANDARDS

GENERAL NOTES

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34	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
00001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-09	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-02	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-03	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-03	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS, TYPE 1
606001-06	COMBINATION CONCRETE CURB AND GUTTER
606201-03	TYPE B GUTTER (INLET, OUTLET & ENTRANCE)
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS ± 45 MPH
701427-05	LANE CLOSURE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS ≤ 40 MPH
701606-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-06	TRAFFIC CONTROL DEVICES

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES. THE CITY OF BATAVIA AND THE VILLAGE OF NORTH AURORA.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED)

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

THE CONTRACTOR SHALL CONTACT THE IDOT ARTERIAL DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, TRAFFIC FIELD ENGINEER, AT (847) 741-8857, OR AT DON.CHIARUGI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH. WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.

ALL PAVEMENT PATCHING LOCATIONS WILL DETERMINED IN THE FIELD BY THE ENGINEER.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

THE COST OF SURFACE REMOVAL IN THE GUTTER FLAG SHALL BE INCLUDED IN THE COST OF OF "HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4" PAY ITEM.

ALL COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

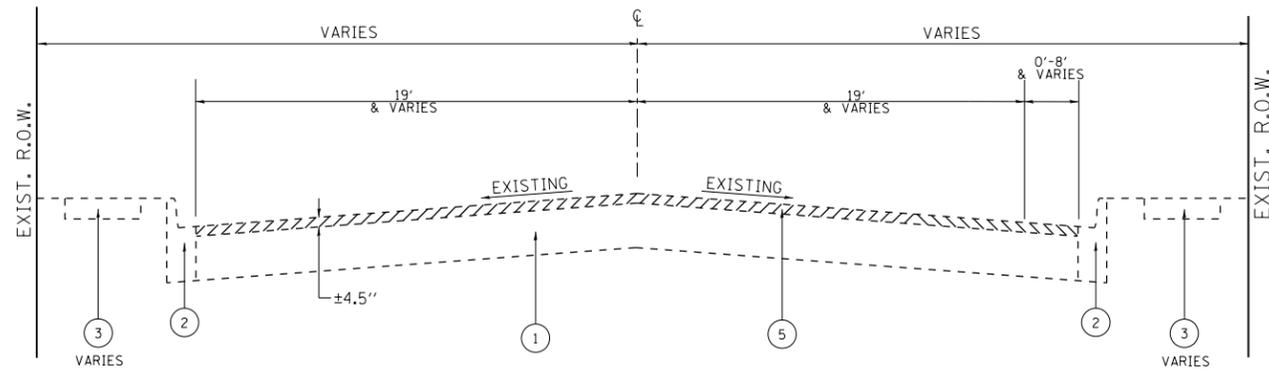
SPECIALTY
ITEM
NON
PARTICIPATING
ITEM

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80/20 FED/STATE 0005				
20101000	TEMPORARY FENCE	FOOT	75	75				
20200100	EARTH EXCAVATION	CU YD	32	32				
20800150	TRENCH BACKFILL	CU YD	248	248				
21101625	TOPSOIL FURNISH AND PLACE, 6"	SO YD	692	692				
21400100	GRADING AND SHAPING DITCHES	FOOT	60	60				
25200110	SODDING, SALT TOLERANT	SQ YD	692	692				
25200200	SUPPLEMENTAL WATERING	UNIT	1	1				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	39000	39000				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	90	90				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	2400	2400				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	340	340				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	4875	4875				
42001300	PROTECTIVE COAT	SO YD	932	932				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	20	20				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2762	2762				
42400800	DETECTABLE WARNINGS	SO FT	251	251				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80/20 FED/STATE 0005				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	57700	57700				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	25	25				
44000600	SIDEWALK REMOVAL	SO FT	2971	2971				
44201777	CLASS D PATCHES, TYPE II, 11 INCH	SO YD	195	195				
44201781	CLASS D PATCHES, TYPE III, 11 INCH	SO YD	100	100				
44201783	CLASS D PATCHES, TYPE IV, 11 INCH	SO YD	3000	3000				
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	72	72				
550A0360	STORM SEWERS, CLASS A, TYPE 2 15"	FOOT	41	41				
550A0380	STORM SEWERS, CLASS A, TYPE 2 18"	FOOT	172	172				
55100400	STORM SEWER REMOVAL 10"	FOOT	11	11				
60201340	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 24 FRAME AND GRATE	EACH	4	4				
60208240	CATCH BASINS, TYPE C, TYPE 24 FRAME AND GRATE	EACH	3	3				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	30	30				
60500050	REMOVING CATCH BASINS	EACH	1	1				
60600095	CLASS S1 CONCRETE (OUTLET)	CU YD	23	23				

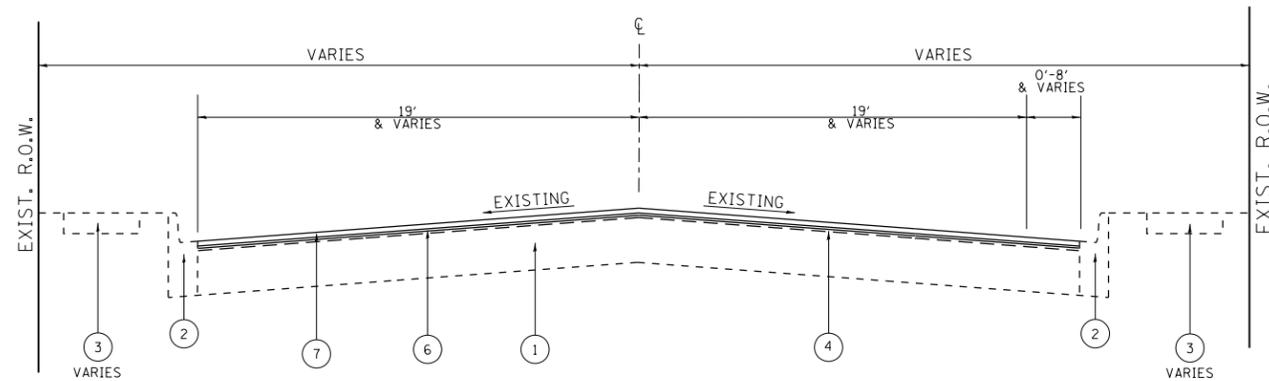
16

FILE NAME =	USER NAME = nissam	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE. SUMMARY OF QUANTITIES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
OFFICE OF STATE HIGHWAY DESIGN	OFFICE OF STATE HIGHWAY DESIGN	CHECKED -	REVISED -			3887	A-R-RS-3	KANE	34	3
PLOT SCALE = 1/8" = 1'	PLOT DATE = 2/10/2017	DATE -	REVISED -			CONTRACT NO. 62C69				
						SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	



**IL-31 (BATAVIA AVE.)
EXISTING TYPICAL SECTION**

STA. 24+22.12 TO STA. 169+22.40
OMMISSION STA 49+05 TO 60+52
OMMISSION STA 70+52.54 TO 71+54.54



**IL-31 (BATAVIA AVE.)
PROPOSED TYPICAL SECTION**

STA. 24+22.12 TO STA. 169+22.40
OMMISSION STA 49+05 TO 60+52
OMMISSION STA 70+52.54 TO 71+54.54

LEGEND

- ① EXIST. P.C.C. PAVEMENT ±7"
- ② EXIST. COMBINATION CONCRETE CURB AND GUTTER
- ③ EXIST. P.C.C. SIDEWALK
- ④ EXIST. HOT-MIX ASPHALT PAVEMENT AFTER MILLING ±2"
- ⑤ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑥ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑦ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"

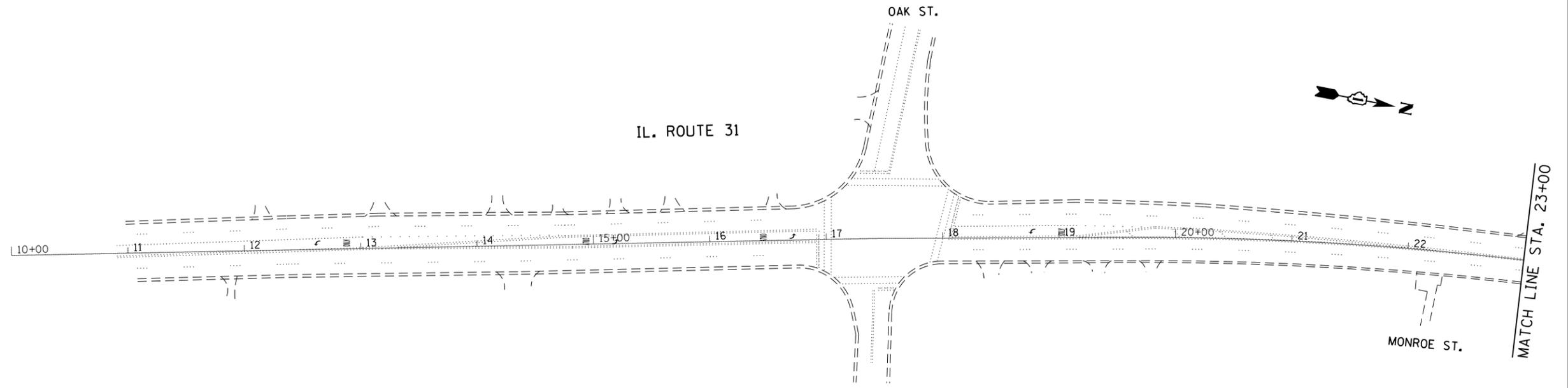
MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE USES	VOIDS @ Ndes	
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	4% AT 70 GYR.	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER, IL-19.0 mm)	4% AT 70 GYR.	QC/QA
QMP Designation: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP)		

NOTE:

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SOYD/IN
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.

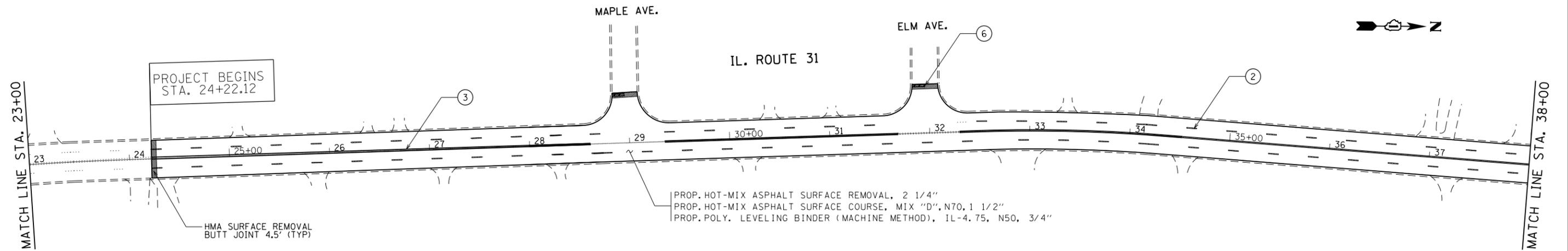
FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING & PROPOSED TYPICAL SECTIONS IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\IL084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI376\BROWns\Design\DI37616-sh-typical.dgn		REVISION	REVISED -			3887	A-R-RS-3	KANE	34	5	
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -			CONTRACT NO. 62C69					
	PLOT DATE = 2/10/2017	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



- | | |
|---|---|
| ① PROP. THERMOPLASTIC PVMT MARKING 6",
CROSSWALK 2 @ 6' C-C, WHITE (TYP.) | ⑤ PROP. THERMOPLASTIC PVMT. MARKING 4"
SHOULDER, WHITE (TYP.) |
| ② PROP. THERMOPLASTIC PVMT MARKING 4"
30' SKIP 10' DASH, WHITE (TYP.) | ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24"
STOPBAR, WHITE (TYP.) |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4",
DOUBLE YELLOW @ 11" C-C (TYP.) | ⑦ PROP. THERMOPLASTIC PVMT. MARKING 12"
SHOULDER @ 45' 75' C-C, WHITE (TYP.) |
| ④ PROP. THERMOPLASTIC PVMT MARKING LINE 12",
CROSSWALK 6' C-C 2', WHITE (TYP.) | |

ROADWAY LEGEND

- Ⓐ PROP. SIDEWALK REMOVAL
 PROP. PCC SIDEWALK, 5"
 PROP. DETECTABLE WARNINGS
 PROP. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL



FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/10/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

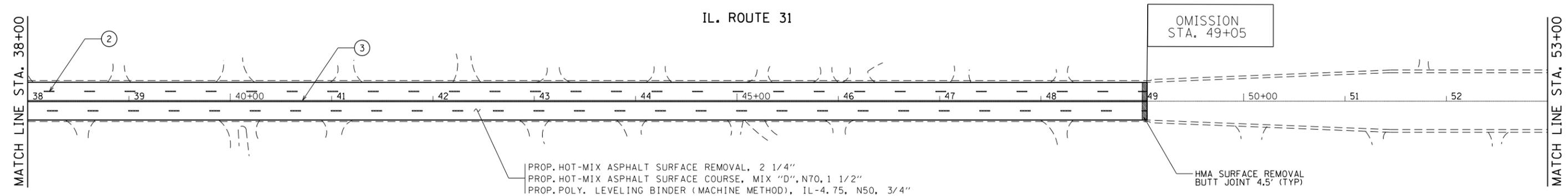
**ROADWAY PLAN
IL 31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

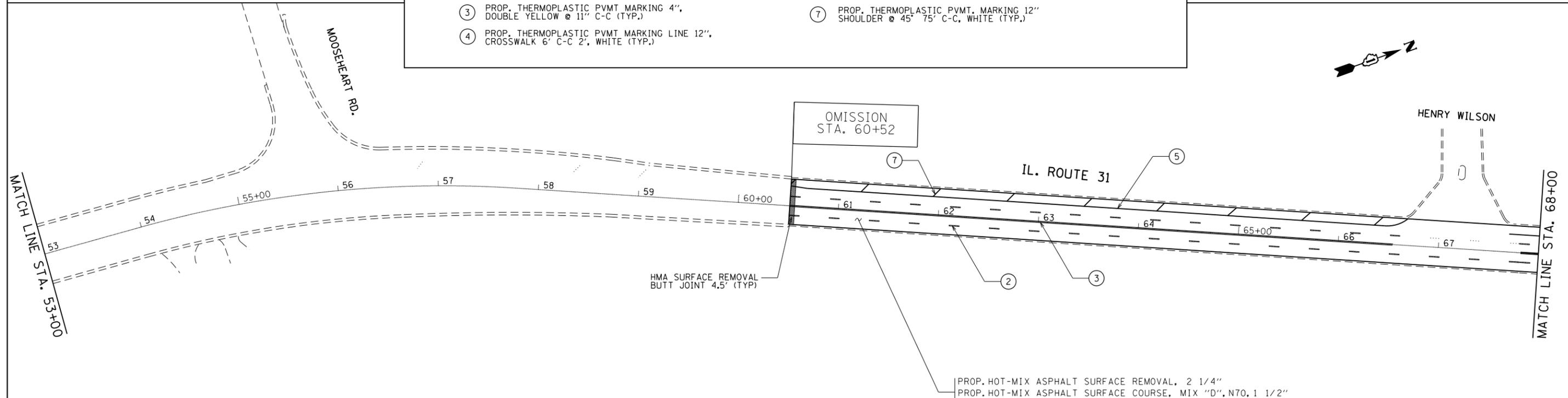
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	6
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				

ROADWAY LEGEND

- (A) PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL



- ① PROP. THERMOPLASTIC PVMT MARKING 6", CROSSWALK 2 @ 6' C-C, WHITE (TYP.)
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- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 4" SHOULDER, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.)
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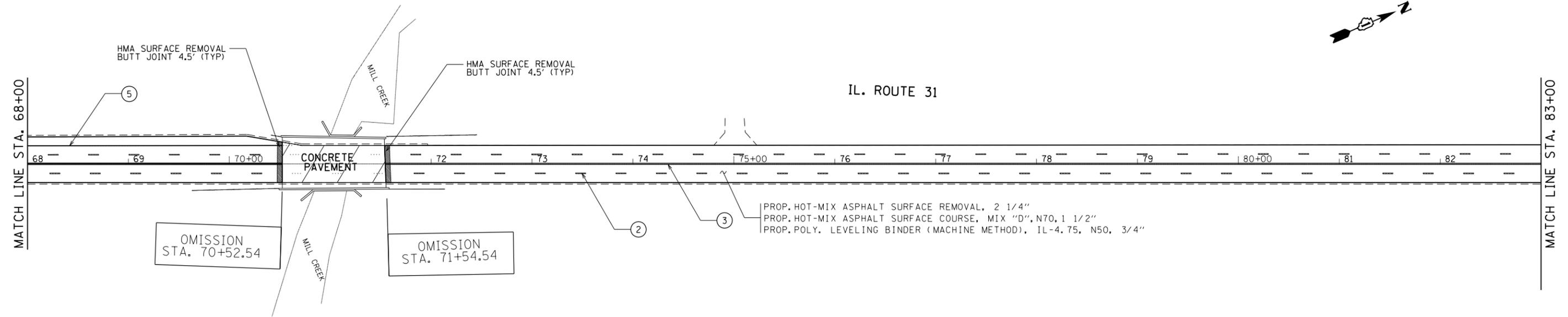
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FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN IL 31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI37616\BATAVIA\Design\DI37616-shr-pln.dgn		CHECKED -	REVISED -			3887	A-R-RS-3	KANE	34	7	
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62C69					
	PLOT DATE = 2/10/2017					ILLINOIS FED. AID PROJECT					

ROADWAY LEGEND

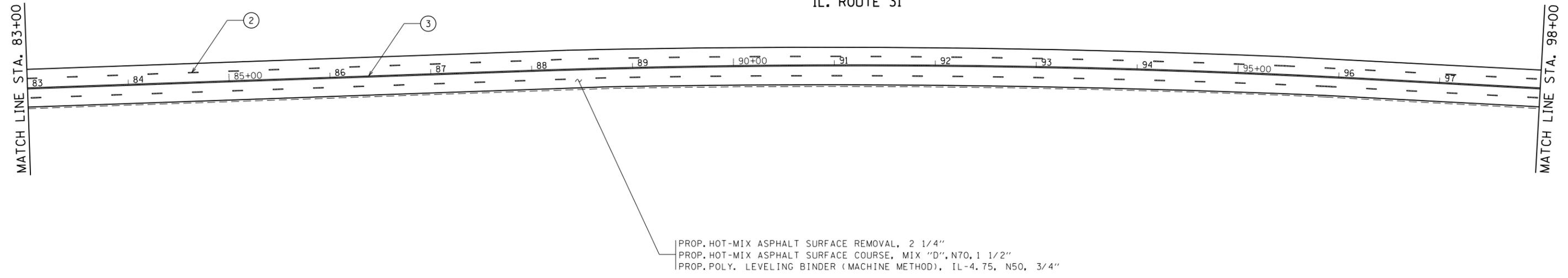
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/10/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

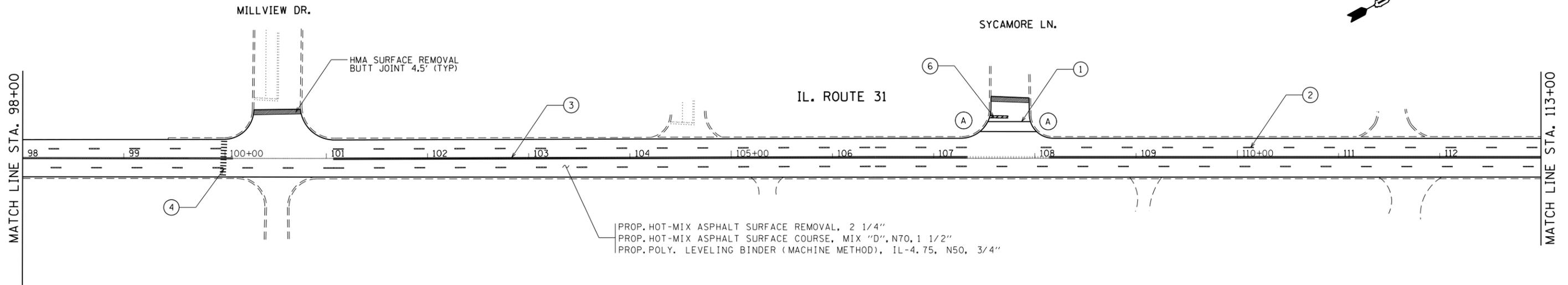
ROADWAY PLAN
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SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	8
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				

ROADWAY LEGEND

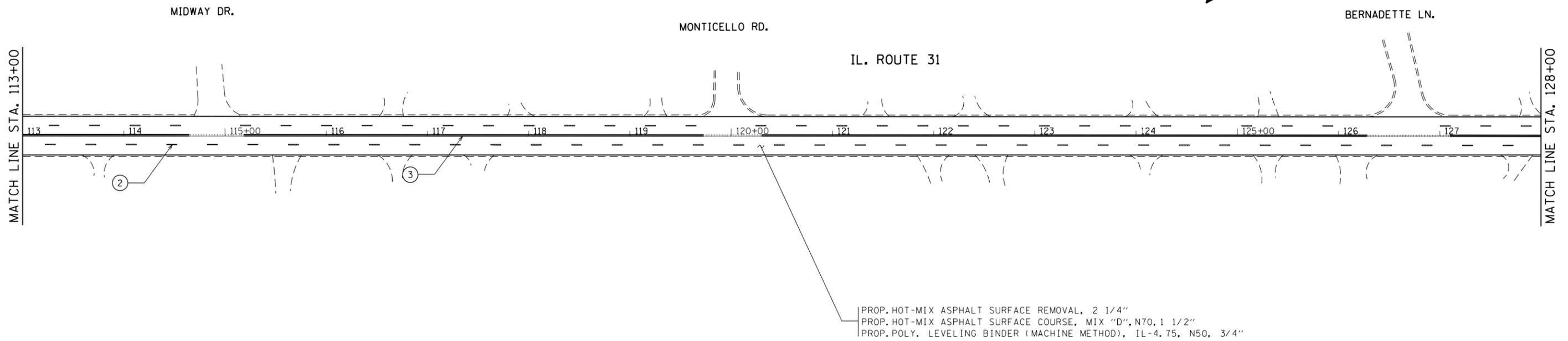
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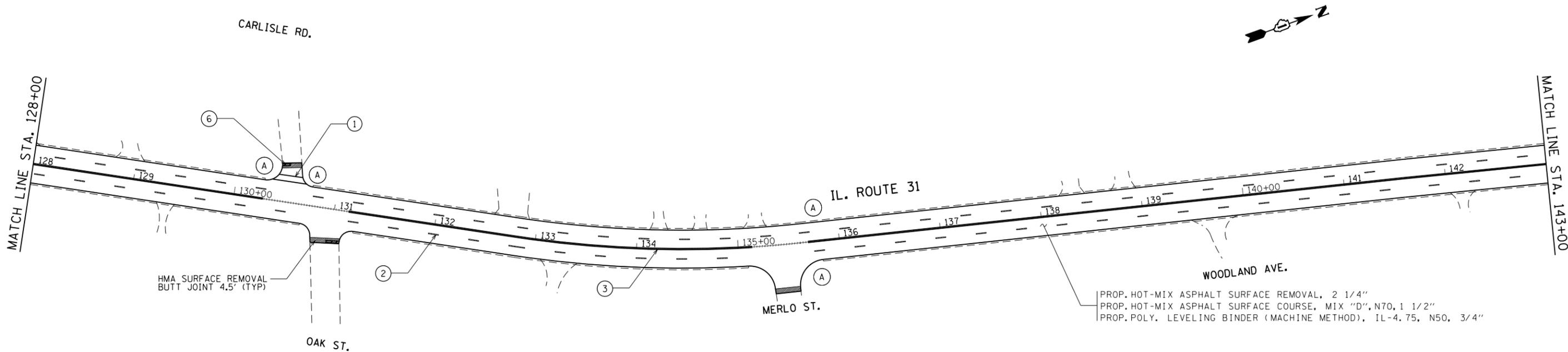
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- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 4" SHOULDER, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT, MARKING 12" SHOULDER @ 45' 75' C-C, WHITE (TYP.)



FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLAN IL 31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI37616\BATAVIA\Design\DI37616-shr-plan.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			3887	A-R-RS-3	KANE	34	9	
Default	PLOT DATE = 2/10/2017	DATE -	REVISED -			CONTRACT NO. 62C69					
						ILLINOIS FED. AID PROJECT					

ROADWAY LEGEND

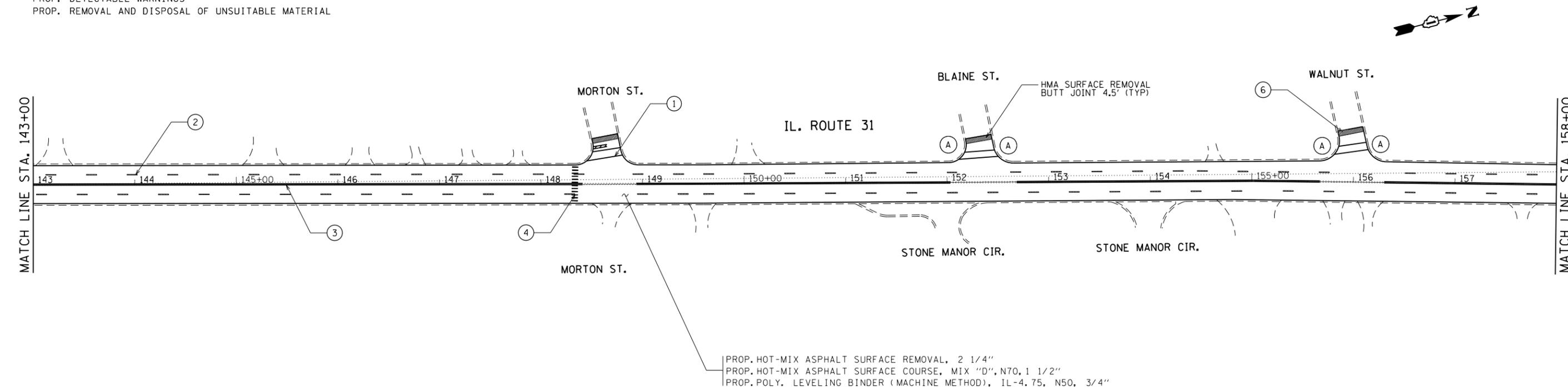
- (A) PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL



- ① PROP. THERMOPLASTIC PVMT MARKING 6", CROSSWALK 2 @ 6' C-C, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT MARKING 4" 30' SKIP 10' DASH, WHITE (TYP.)
- ③ PROP. THERMOPLASTIC PVMT MARKING 4", DOUBLE YELLOW @ 11" C-C (TYP.)
- ④ PROP. THERMOPLASTIC PVMT MARKING LINE 12", CROSSWALK 6' C-C 2', WHITE (TYP.)
- ⑤ PROP. THERMOPLASTIC PVMT. MARKING 4" SHOULDER, WHITE (TYP.)
- ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT, MARKING 12" SHOULDER @ 45' 75' C-C, WHITE (TYP.)

ROADWAY LEGEND

- (A) PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
 PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
 PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI37616\Browns\Design\DI37616-shr-plan.dgn		REVISED -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
Default	PLOT DATE = 2/10/2017	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

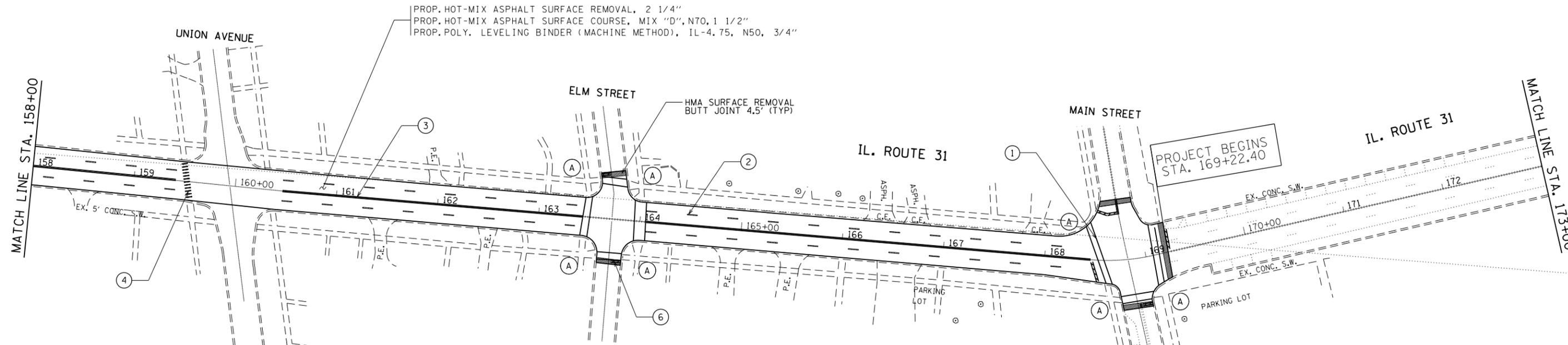
**ROADWAY PLAN
 IL 31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

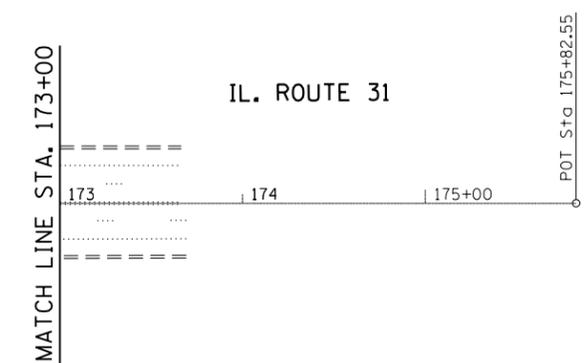
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	10
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				

ROADWAY LEGEND

- (A) PROP. SIDEWALK REMOVAL
- PROP. PCC SIDEWALK, 5"
- PROP. DETECTABLE WARNINGS
- PROP. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL



- ① PROP. THERMOPLASTIC PVMT MARKING 6", CROSSWALK 2 @ 6' C-C, WHITE (TYP.)
- ② PROP. THERMOPLASTIC PVMT MARKING 4" 30' SKIP 10' DASH, WHITE (TYP.)
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- ④ PROP. THERMOPLASTIC PVMT MARKING LINE 12", CROSSWALK 6' C-C 2', WHITE (TYP.)
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- ⑥ PROP. THERMOPLASTIC PVMT MARKING LINE, 24" STOPBAR, WHITE (TYP.)
- ⑦ PROP. THERMOPLASTIC PVMT, MARKING 12" SHOULDER @ 45' 75' C-C, WHITE (TYP.)



FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	DATE = 2/10/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
IL 31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	11
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING AND PATCHING OPERATIONS)."
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

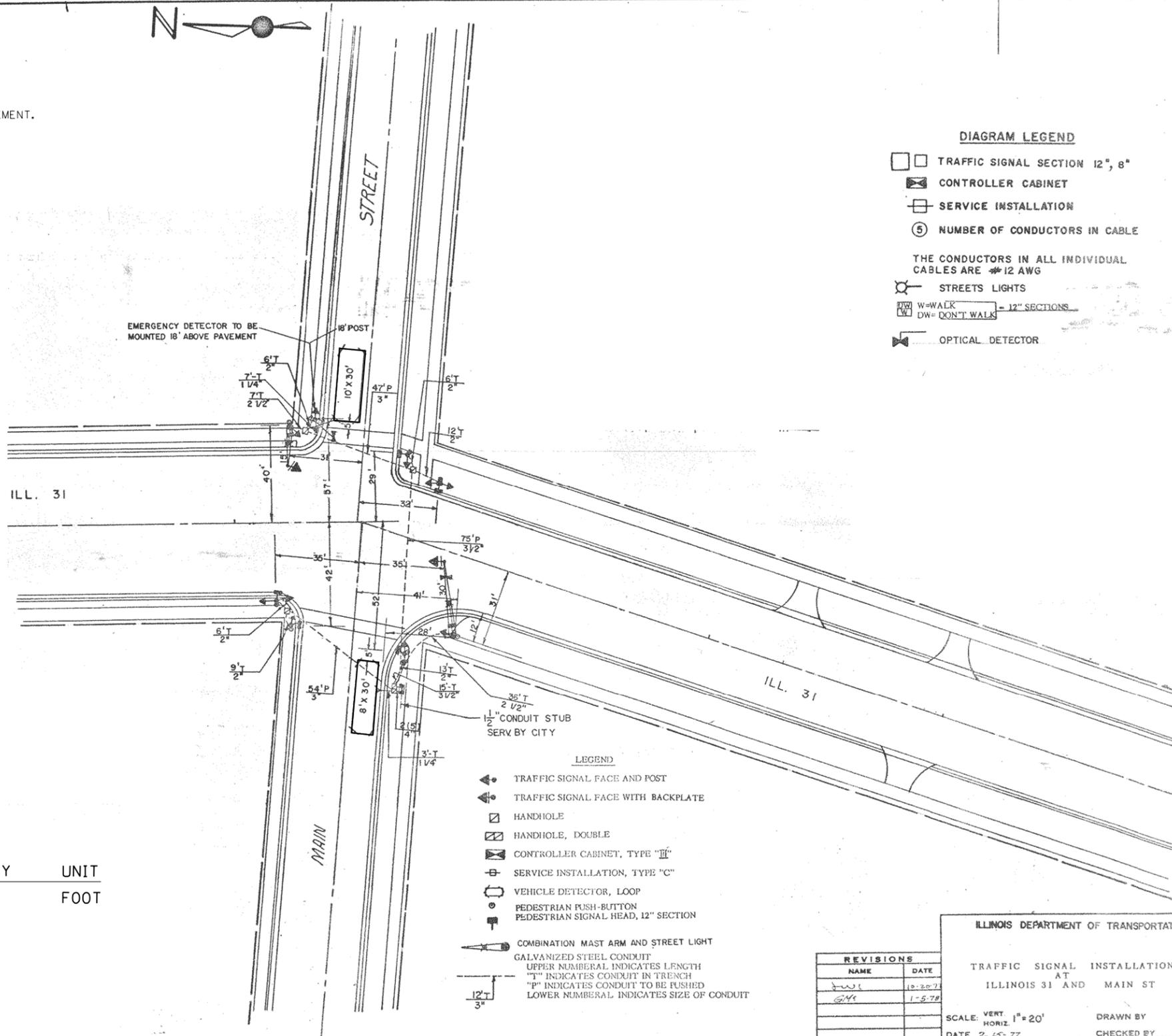


DIAGRAM LEGEND

- TRAFFIC SIGNAL SECTION 12", 8"
 - ⊠ CONTROLLER CABINET
 - ⊞ SERVICE INSTALLATION
 - ⑤ NUMBER OF CONDUCTORS IN CABLE
- THE CONDUCTORS IN ALL INDIVIDUAL CABLES ARE #12 AWG
- STREETS LIGHTS
 - W= WALK
 - DW= DON'T WALK
 - 12" SECTIONS
 - ▶ OPTICAL DETECTOR

REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	175	FOOT

- LEGEND**
- ▶ TRAFFIC SIGNAL FACE AND POST
 - ▶ TRAFFIC SIGNAL FACE WITH BACKPLATE
 - ⊞ HANDHOLE
 - ⊞ HANDHOLE, DOUBLE
 - ⊠ CONTROLLER CABINET, TYPE "III"
 - ⊞ SERVICE INSTALLATION, TYPE "C"
 - VEHICLE DETECTOR, LOOP
 - PEDESTRIAN PUSH-BUTTON
 - PEDESTRIAN SIGNAL HEAD, 12" SECTION
 - ▶ COMBINATION MAST ARM AND STREET LIGHT
- GALVANIZED STEEL CONDUIT
UPPER NUMERAL INDICATES LENGTH
"T" INDICATES CONDUIT IN TRENCH
"P" INDICATES CONDUIT TO BE PUSHED
LOWER NUMERAL INDICATES SIZE OF CONDUIT

REVISIONS	
NAME	DATE
JW	10-20-77
GH	1-5-78

ILLINOIS DEPARTMENT OF TRANSPORTATION

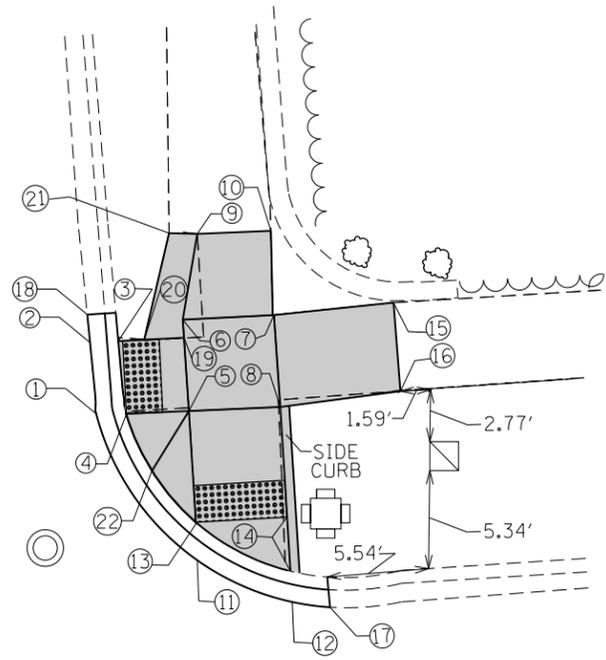
TRAFFIC SIGNAL INSTALLATION
AT
ILLINOIS 31 AND MAIN ST

SCALE: VERT. 1"=20'
HORIZ. 1"=20'

DATE: 2-15-77

DRAWN BY: _____
CHECKED BY: _____

IL RTE 31 (BATAVIA AVENUE)



MAIN STREET

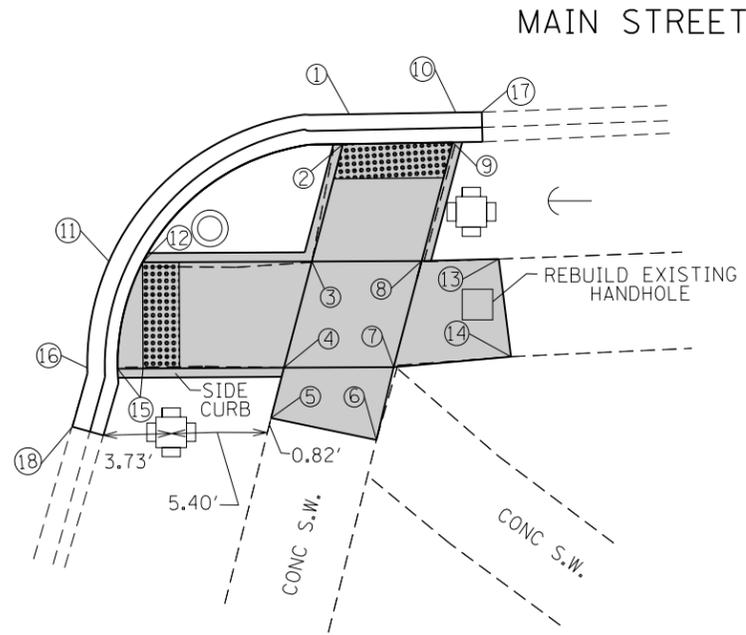
(WV)

ADA RAMP NORTHEAST CORNER IL-31 AT MAIN ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	715.13	715.13
2	715.19	715.19
3	NA	715.19
4	NA	715.19
5	NA	715.22
6	NA	715.22
7	NA	715.22
8	NA	715.22
9	715.42	Meet Existing
10	715.38	Meet Existing
11	714.83	Meet Existing
12	714.72	714.78
13	NA	714.83
14	NA	714.75
15	715.07	Meet Existing
16	715.01	Meet Existing
17	714.61	Meet Existing
18	715.22	Meet Existing
19	NA	715.22
20	NA	715.2
21	715.42	Meet Existing
22	NA	715.69

POINT NO.	DISTANCE (FEET)	GRADES
15-16	4.85	1.2%
7-15	6.58	2.3%
8-16	6.63	3.2%
6-7	5.00	0.0%
7-8	5.00	0.0%
5-8	5.00	0.0%
5-6	5.00	0.0%
9-10	4.00	1.0%
10-7	4.60	3.4%
9-6	4.74	4.2%
3-19	3.6	0.8%
19-5	4.00	0.0%
19-6	1.00	0.0%
19-20	2.18	0.9%
20-21	5.98	3.6%
21-9	1.54	0.0%
3-19	3.60	0.8%
5-4	3.43	0.9%
5-13	6.09	6.4%
8-14	6.00	7.8%
2-3	1.50	0.0%
4-1	1.50	4.0%
2-1	4.00	1.5%
13-11	1.50	0.3%
11-12	5.00	0.9%
12-14	1.50	2.0%
17-18	21.40	2.83%

IL RTE 31 (BATAVIA AVENUE)



ADA RAMP SOUTHEAST CORNER IL-31 AT MAIN ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	714.44	Meet Existing
2	NA	714.44
3	NA	714.73
4	NA	714.73
5	714.62	Meet Existing
6	714.79	Meet Existing
7	NA	714.73
8	NA	714.73
9	NA	714.35
10	714.35	Meet Existing
11	714.64	Meet Existing
12	NA	714.64
13	714.54	Meet Existing
14	714.72	Meet Existing
15	NA	714.60
16	714.60	Meet Existing
17	714.33	Meet Existing
18	714.49	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.50	0.0%
2-3	6.62	4.3%
3-4	6.00	0.0%
4-5	2.83	3.9%
5-6	5.90	2.9%
6-7	4.08	1.4%
7-8	6.00	0.0%
8-9	6.69	5.6%
9-10	1.50	0.0%
11-12	1.50	0.0%
12-3	9.27	0.9%
3-8	6.00	0.0%
8-13	4.22	4.6%
13-14	5.37	3.3%
14-7	6.45	0.2%
7-4	6.00	0.0%
4-15	7.70	1.7%
15-16	1.50	0.0%
17-18	31.71	0.5%
12-15	6.00	0.7%
11-16	6.10	0.7%
2-9	6.00	1.5%
1-10	6.00	1.5%

REFERENCE BENCHMARK ELEV 714.990
 BENCHMARK : TOP OF CATCH BASIN
 LOCATION : SOUTH WEST CORNER IL-31 & MAIN ST.

LEGEND

xx.xx'

EXISTING LENGTH

=====

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
 REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 714.990
 BENCHMARK : TOP OF CATCH BASIN
 LOCATION : SOUTH WEST CORNER IL-31 & MAIN ST.

FILE NAME =	USER NAME = hassan	DESIGNED -	REVISED -
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/10/2017	DATE -	REVISED -

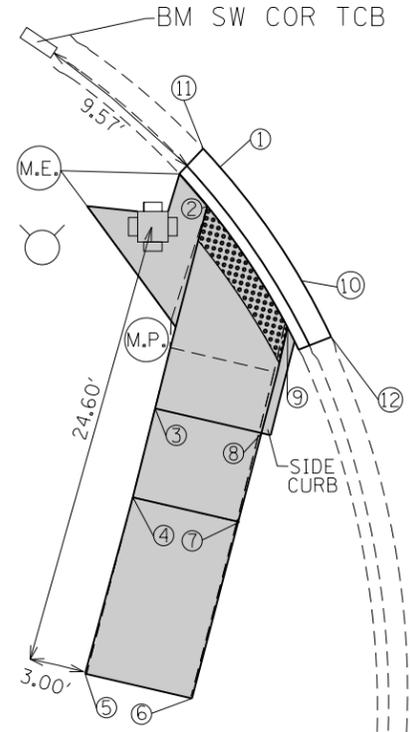
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA RAMPS
 IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	13
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				

MAIN STREET

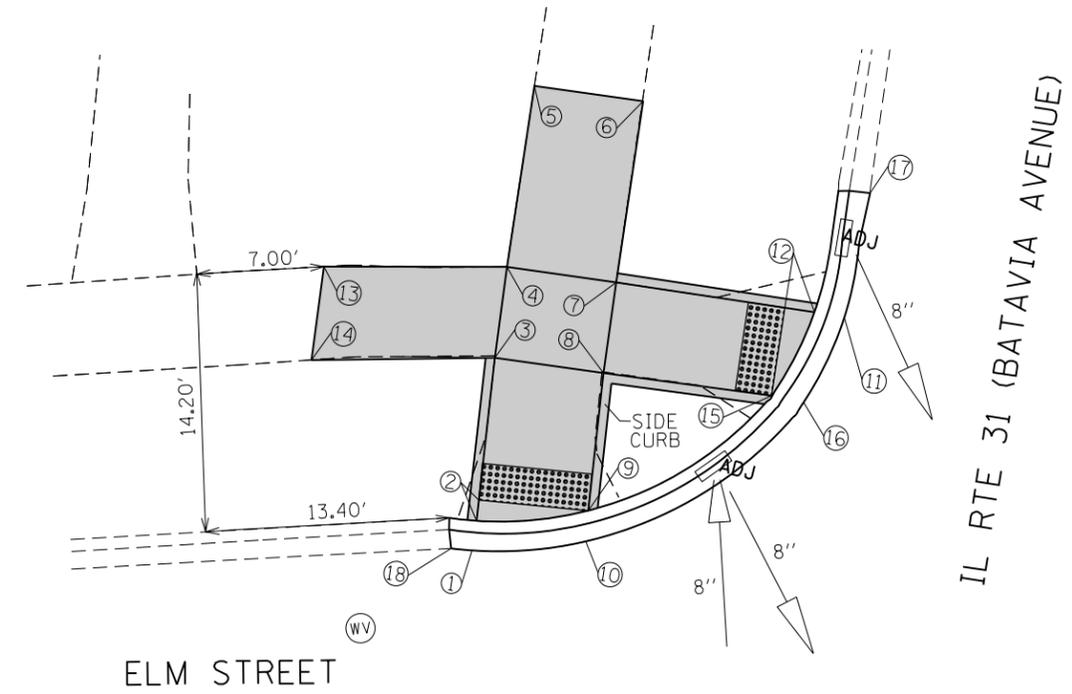


IL RTE 31 (BATAVIA AVENUE)

ADA RAMP SOUTHWEST CORNER IL-31 AT MAIN ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	714.87	Meet Existing
2	NA	714.87
3	NA	714.87
4	NA	714.87
5	715.53	Meet Existing
6	715.44	Meet Existing
7	NA	714.87
8	NA	714.87
9	NA	714.88
10	714.88	Meet Existing
11	714.92	Meet Existing
12	714.90	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.50	0.0%
2-3	11.41	0.0%
3-4	5.00	0.0%
3-8	6.00	0.0%
4-5	10.00	6.6%
5-6	6.00	1.4%
6-7	10.00	5.7%
4-7	6.00	0.0%
7-8	5.00	0.0%
8-9	5.95	0.1%
9-2	6.00	0.2%
9-10	1.50	0.0%
1-10	7.60	0.1%
11-12	11.90	0.1%



ELM STREET

IL RTE 31 (BATAVIA AVENUE)

ADA RAMP NORTHWEST CORNER IL-31 AT ELM ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	713.16	713.14
2	NA	713.12
3	NA	713.10
4	NA	713.10
5	713.85	Meet Existing
6	713.73	Meet Existing
7	NA	713.10
8	NA	713.10
9	NA	713.00
10	712.97	Meet Existing
11	712.58	Meet Existing
12	NA	712.60
13	713.84	Meet Existing
14	713.69	Meet Existing
15	NA	712.66
16	712.64	Meet Existing
17	712.40	Meet Existing
18	713.13	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.50	1.3%
2-3	7.80	0.3%
3-4	5.00	0.0%
4-5	10.00	7.5%
5-6	6.00	2.1%
6-7	10.00	6.3%
7-8	5.00	0.0%
8-9	7.55	1.3%
9-10	1.50	1.8%
11-12	1.50	1.5%
12-7	9.30	5.4%
7-4	6.00	0.0%
4-13	10.00	7.4%
13-14	5.00	2.9%
14-3	10.00	5.9%
3-8	6.00	0.0%
8-15	9.30	4.7%
15-16	1.50	1.1%
17-18	32.70	2.2%
1-10	6.10	3.00%
2-9	6.00	2.00%
16-11	5.10	1.30%
15-12	5.00	1.20%

REFERENCE BENCHMARK ELEV 714.990
 BENCHMARK : TOP OF CATCH BASIN
 LOCATION : SOUTH WEST CORNER IL-31 & MAIN ST.

LEGEND

xx.xx'

EXISTING LENGTH

==

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
 REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 714.335
 BENCHMARK : NORTHEAST BOLT OF HYDRANT
 LOCATION : ON NORTH EAST CORNER OF IL-31 & ELM STREET

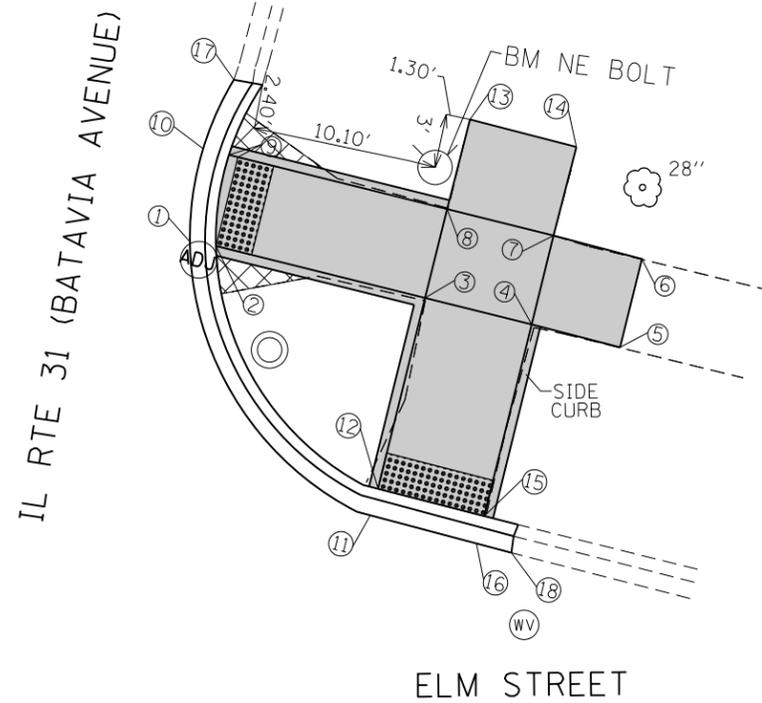
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Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/10/2017	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ADA RAMPS
 IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.

SCALE: SHEET OF SHEETS STA. TO STA.

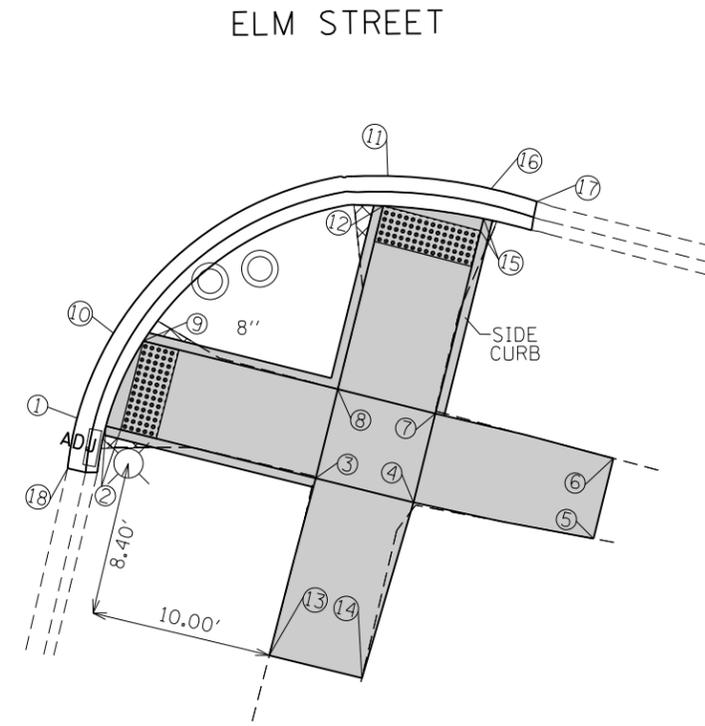
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	14
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				



ADA RAMP NORTHEAST CORNER IL-31 AT ELM ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	712.44	Meet Existing
2	NA	712.51
3	NA	713.45
4	NA	713.45
5	713.71	Meet Existing
6	713.77	Meet Existing
7	NA	713.45
8	NA	713.39
9	NA	712.45
10	712.43	Meet Existing
11	712.67	Meet Existing
12	NA	712.64
13	713.61	Meet Existing
14	713.81	Meet Existing
15	NA	712.68
16	712.70	Meet Existing
17	712.55	Meet Existing
18	713.70	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.50	4.7%
2-3	11.80	8.0%
3-4	6.00	0.0%
4-5	5.00	5.2%
5-6	5.00	1.2%
6-7	5.00	6.4%
7-8	6.00	0.0%
8-9	12.30	7.6%
9-10	1.50	1.3%
11-12	1.50	2.0%
12-3	10.70	7.6%
3-8	5.00	0.0%
8-13	5.00	4.4%
13-14	6.00	3.3%
14-7	5.00	7.2%
7-4	5.00	0.0%
4-15	10.80	7.1%
15-16	1.50	1.3%
17-18	35.00	3.3%
9-2	5.00	1.2%
10-1	5.10	0.2%
12-15	6.00	0.7%
11-16	6.00	0.5%



ADA RAMP SOUTHEAST CORNER IL-31 AT ELM ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	712.60	Meet Existing
2	NA	712.61
3	NA	713.50
4	NA	713.50
5	714.07	Meet Existing
6	714.01	Meet Existing
7	NA	713.50
8	NA	713.50
9	NA	712.70
10	712.67	Meet Existing
11	712.92	Meet Existing
12	NA	712.90
13	714.14	Meet Existing
14	714.09	Meet Existing
15	NA	712.95
16	712.98	Meet Existing
17	712.99	Meet Existing
18	712.51	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.5	1.0%
2-3	11.8	7.5%
3-4	5.5	0.0%
4-5	10	5.7%
5-6	5	1.2%
6-7	10	5.1%
7-8	5.5	0.0%
8-9	11	7.3%
9-10	1.5	1.8%
11-12	1.5	1.5%
12-8	10.3	5.8%
8-3	5	0.0%
3-13	10	6.4%
13-14	5.1	1.1%
14-4	10	5.9%
4-7	5	0.0%
7-15	11	5.0%
15-16	1.5	2.0%
17-18	32.7	1.5%
1-10	5	1.6%
2-9	5	1.8%
16-11	5.5	1.1%
15-12	5.5	0.9%

REFERENCE BENCHMARK ELEV 714.335
 BENCHMARK : NORTHEAST BOLT OF HYDRANT
 LOCATION : ON NORTH EAST CORNER OF IL-31 & ELM STREET

LEGEND

xx.xx'

EXISTING LENGTH

====

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 714.335
 BENCHMARK : NORTHEAST BOLT OF HYDRANT
 LOCATION : ON NORTH EAST CORNER OF IL-31 & ELM STREET

FILE NAME =	USER NAME = hassan	DESIGNED -	REVISED -
p:\IL\084EBID\INTEG\illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI37616\Design\DI37616-shr-details.dgn		REVISED -	
Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/10/2017	DATE -	REVISED -

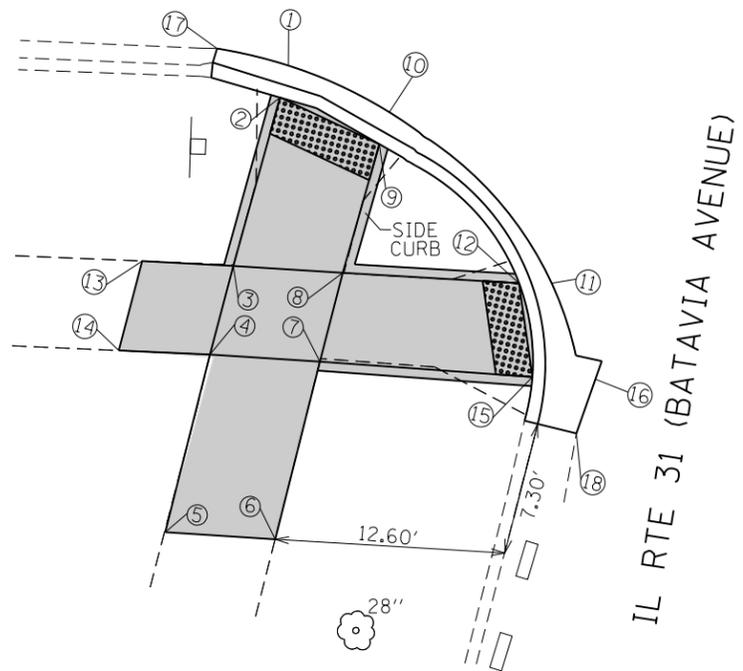
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS
IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	15
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				

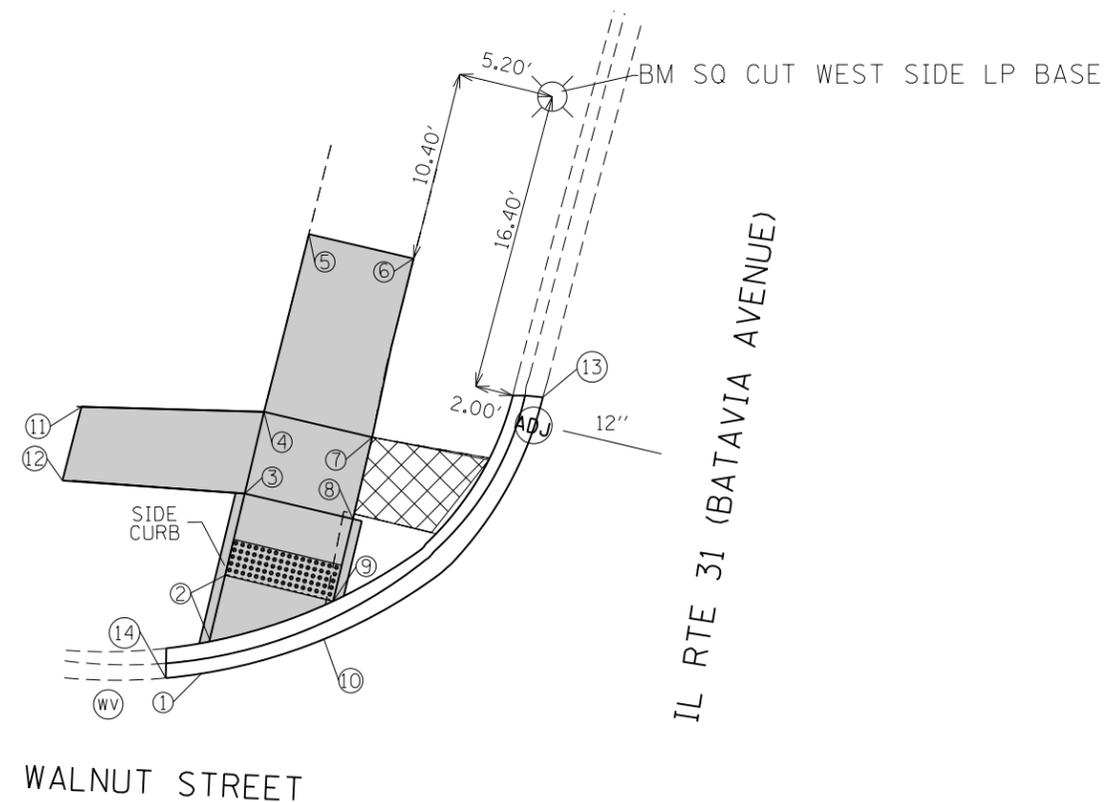
ELM STREET



ADA RAMP SOUTHWEST CORNER IL-31 AT ELM ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	712.90	Meet Existing
2	NA	712.92
3	NA	713.30
4	NA	713.30
5	713.87	Meet Existing
6	713.76	Meet Existing
7	NA	713.30
8	NA	713.30
9	NA	712.80
10	712.78	712.79
11	712.65	Meet Existing
12	NA	712.63
13	713.69	Meet Existing
14	713.81	Meet Existing
15	NA	712.60
16	712.64	Meet Existing
17	712.79	Meet Existing
18	712.52	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.50	1.2%
2-3	9.50	4.0%
3-4	5.00	0.0%
4-5	10.00	5.7%
5-6	6.00	1.9%
6-7	10.00	4.6%
7-8	5.00	0.0%
8-9	7.30	6.8%
9-10	1.50	0.7%
11-12	1.50	1.6%
12-8	9.60	7.0%
8-3	6.00	0.0%
3-13	10.00	3.9%
13-14	5.00	2.4%
14-4	10.00	5.1%
4-7	6.00	0.0%
7-15	11.65	6.0%
15-16	1.50	2.5%
17-18	29.60	0.9%
1-10	6.10	1.8%
2-9	6.00	2.0%
16-11	5.00	0.3%
15-12	5.00	0.6%



ADA RAMP NORTHWEST CORNER IL-31 AT WALNUT ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	713.45	Meet Existing
2	NA	713.47
3	NA	713.75
4	NA	713.75
5	714.49	Meet Existing
6	714.35	Meet Existing
7	NA	713.75
8	NA	713.75
9	NA	713.50
10	713.49	Meet Existing
11	714.38	Meet Existing
12	714.29	Meet Existing
13	713.45	Meet Existing
14	713.43	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.5	1.7%
2-3	4.6	6.1%
3-4	4.6	0.0%
4-5	10	7.4%
5-6	6	2.4%
6-7	10	6.0%
7-8	4.6	0.0%
8-9	4.6	5.4%
9-10	1.5	0.8%
11-12	4.2	2.2%
12-3	10	5.4%
11-4	10	6.3%
3-8	6	0.0%
4-7	6	0.0%
2-9	6	0.5%
1-10	7.3	0.6%
13-14	26.22	0.1%

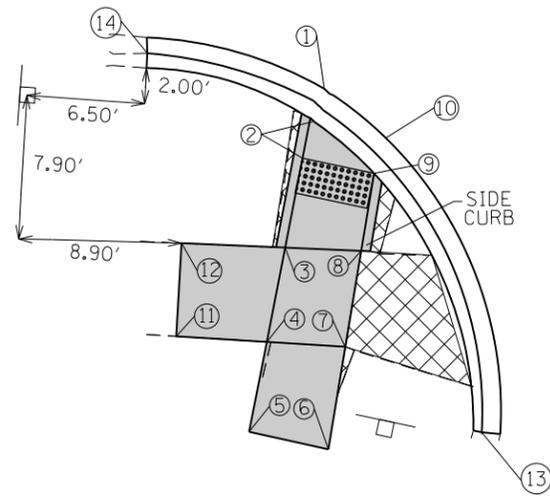
REFERENCE BENCHMARK ELEV 714.335
 BENCHMARK : NORTHEAST BOLT OF HYDRANT
 LOCATION : ON NORTH EAST CORNER OF IL-31 & ELM STREET

LEGEND

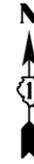
- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 713.972
 BENCHMARK : SQ CUT WEST SIDE OF LP BASE
 LOCATION : ON NORTH WEST CORNER OF IL-31 & WALNUT STREET

WALNUT STREET



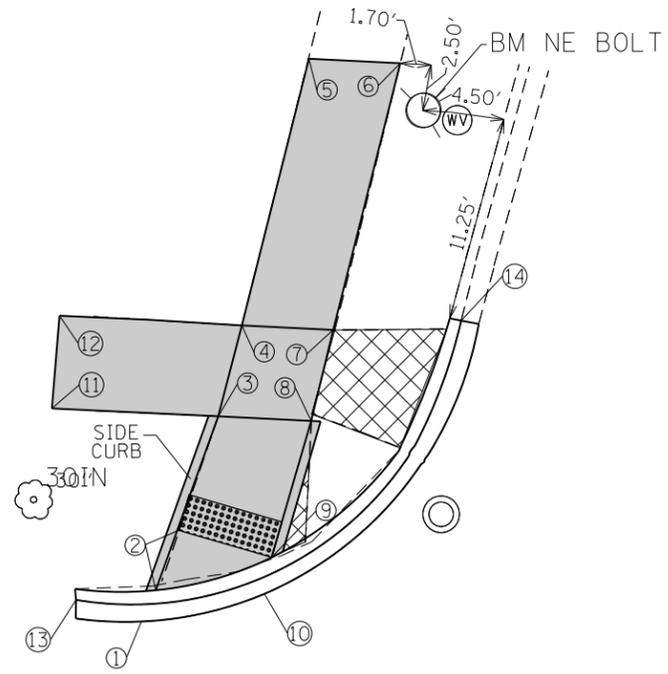
IL RTE 31 (BATAVIA AVENUE)



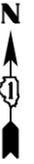
ADA RAMP SOUTHWEST CORNER IL-31 AT WALNUT ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	713.28	Meet Existing
2	NA	713.30
3	NA	713.50
4	NA	713.50
5	713.70	Meet Existing
6	713.58	Meet Existing
7	NA	713.50
8	NA	713.50
9	NA	713.30
10	713.29	Meet Existing
11	713.70	Meet Existing
12	713.54	Meet Existing
13	713.29	Meet Existing
14	713.12	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.5	1.4%
2-3	5	4.0%
3-4	5	0.0%
4-5	5	4.0%
5-6	4.4	2.7%
6-7	5.7	1.4%
7-8	5	0.0%
8-9	4.3	4.7%
9-10	1.5	0.9%
11-12	5.1	3.1%
12-3	5.7	0.7%
11-4	5	4.0%
3-8	4	0.0%
4-7	4	0.0%
2-9	4	0.0%
1-10	4	0.2%
13-14	30.75	0.6%



IL RTE 31 (BATAVIA AVENUE)



BLAINE STREET

ADA RAMP NORTHWEST CORNER IL-31 AT BLAINE ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	706.26	Meet Existing
2	NA	706.26
3	NA	706.65
4	NA	706.65
5	707.48	Meet Existing
6	707.46	Meet Existing
7	NA	706.65
8	NA	706.65
9	NA	706.26
10	706.53	706.29
11	706.96	Meet Existing
12	706.88	Meet Existing
13	706.10	Meet Existing
14	706.92	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.5	0.0%
2-3	6.5	6.0%
3-4	5	0.0%
4-5	15	5.5%
5-6	5	0.4%
6-7	15	5.4%
7-8	5	0.0%
8-9	8	4.9%
9-10	1.5	2.0%
11-12	5	1.6%
12-4	10	2.3%
11-3	9	3.4%
13-14	28.9	2.8%
3-8	5	0.0%
4-7	5	0.0%
1-10	6.7	0.5%
2-9	5	0.0%

REFERENCE BENCHMARK ELEV 713.972

BENCHMARK : SQ CUT WEST SIDE OF LP BASE

LOCATION : ON NORTH WEST CORNER OF IL-31 & WALNUT STREET

LEGEND

xx.xx'

EXISTING LENGTH

==

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 710.039

BENCHMARK : NORTH EAST BOLT OF HYDRANT

LOCATION : ON NORTH WEST CORNER OF IL-31 & BLAINE STREET

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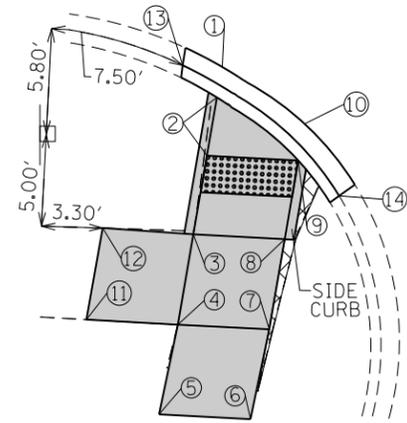
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS
IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.

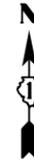
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	17
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				

BLAINE STREET



IL RTE 31 (BATAVIA AVENUE)

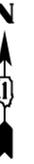
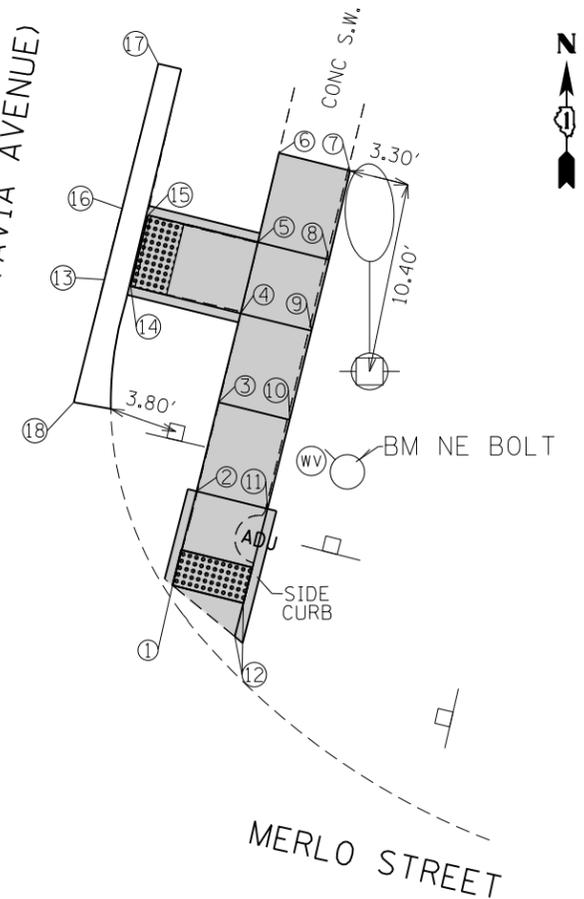


ADA RAMP SOUTHWEST CORNER IL-31 AT BLAINE ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	705.28	Meet Existing
2	NA	705.30
3	NA	705.60
4	NA	705.68
5	706.07	Meet Existing
6	705.99	Meet Existing
7	NA	705.60
8	NA	705.60
9	NA	705.37
10	705.35	Meet Existing
11	705.93	Meet Existing
12	705.89	Meet Existing
13	705.30	Meet Existing
14	705.39	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.5	1.6%
2-3	4.4	6.8%
3-4	5	1.6%
4-5	5	7.8%
5-6	5	1.7%
6-7	5	7.7%
7-8	5	0.0%
8-9	4.4	5.2%
9-10	1.5	1.0%
11-12	5	0.6%
12-3	5	5.9%
11-4	5	4.9%
3-8	5	0.0%
4-7	5	1.6%
2-9	5	1.4%
1-10	5.1	1.5%
13-14	11.3	0.9%

IL RTE 31 (BATAVIA AVENUE)



ADA RAMP NORTHEAST CORNER IL-31 AT MERLO ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	714.06	Meet Existing
2	NA	714.00
3	NA	714.20
4	NA	714.20
5	NA	714.20
6	714.58	Meet Existing
7	714.62	Meet Existing
8	NA	714.22
9	NA	714.20
10	NA	714.20
11	NA	713.98
12	713.99	Meet Existing
13	714.13	Meet Existing
14	NA	714.15
15	NA	714.15
16	714.14	Meet Existing
17	714.15	Meet Existing
18	714.17	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	5.3	1.05%
2-3	5	4.00%
3-4	5	0.00%
4-5	4	0.00%
5-6	5	7.56%
6-7	4	0.97%
7-8	5	7.93%
8-9	4	0.50%
9-10	5	0.00%
10-11	5	4.40%
11-12	7.2	0.08%
13-14	1.5	1.31%
14-4	6.2	0.81%
15-5	6.2	0.81%
15-16	1.5	0.82%
5-8	4	0.50%
4-9	4	0.00%
3-10	4	0.00%
2-11	4	0.50%
1-12	4.42	1.60%
13-16	4	0.18%
17-18	19.1	0.13%

REFERENCE BENCHMARK ELEV 710.039

BENCHMARK : NORTH EAST BOLT OF HYDRANT

LOCATION : ON NORTH WEST CORNER OF IL-31 & BLAINE STREET

LEGEND



EXISTING LENGTH



PROPOSED SIDE CURB



EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 715.935

BENCHMARK : NORTH EAST BOLT OF HYDRANT

LOCATION : ON NORTH EAST CORNER OF IL-31 & MERLO STREET

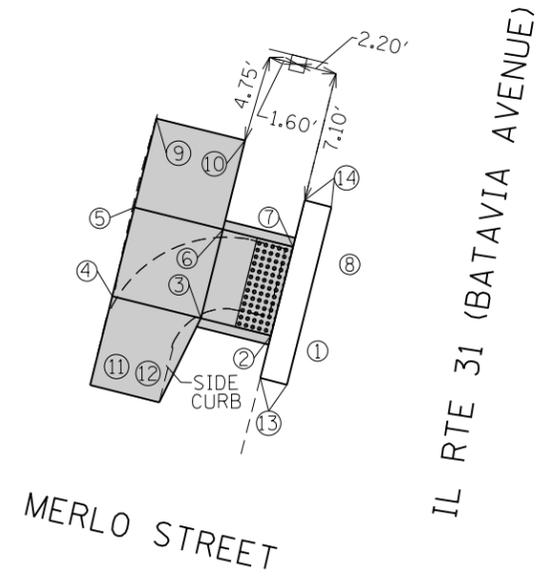
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	PLOT DATE = 2/10/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS
IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.

SCALE: SHEET OF SHEETS STA. TO STA.

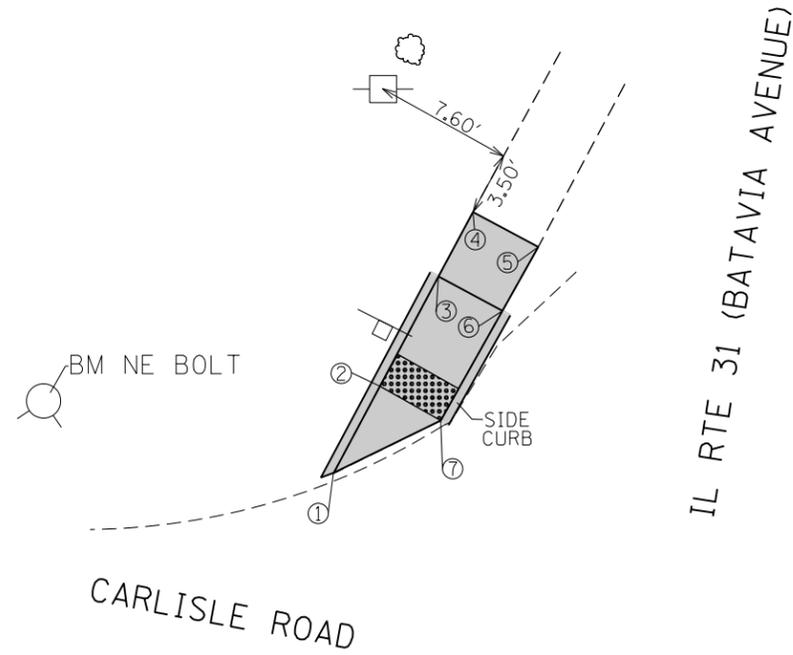
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	18
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				



ADA RAMP NORTHWEST CORNER IL-31 AT MERLO ST.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	713.47	Meet Existing
2	NA	713.50
3	NA	713.80
4	NA	713.80
5	NA	713.82
6	NA	713.80
7	NA	713.54
8	713.53	Meet Existing
9	714.20	Meet Existing
10	714.04	Meet Existing
11	713.96	Meet Existing
12	713.80	Meet Existing
13	713.40	Meet Existing
14	713.54	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.5	1.8%
2-3	4	7.5%
3-4	5	0.0%
4-5	5	0.4%
5-9	5	7.7%
9-10	5	3.3%
10-6	5	4.8%
5-6	5	0.4%
6-7	4	6.5%
7-8	1.5	0.7%
11-4	5	3.2%
11-12	4	3.9%
12-3	5.1	0.1%
13-14	10	1.4%
2-7	5	0.8%
3-6	5	0.0%
1-8	5	1.1%



ADA RAMP NORTHWEST CORNER IL-31 AT CARLISLE RD.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	710.18	Meet Existing
2	NA	710.18
3	NA	710.54
4	710.54	Meet Existing
5	710.54	Meet Existing
6	NA	710.54
7	710.36	710.25

POINT NO.	DISTANCE (FEET)	GRADES
1-2	5.4	0.07%
2-3	7.8	4.67%
3-4	4	0.00%
4-5	4	0.02%
5-6	4	0.00%
6-7	6.9	2.74%
2-7	3.8	1.84%
3-6	4	0.02%
1-7	6.6	1.00%

REFERENCE BENCHMARK ELEV 715.935
 BENCHMARK : NORTH EAST BOLT OF HYDRANT
 LOCATION : ON NORTH EAST CORNER OF IL-31 & MERLO STREET

LEGEND

xx.xx'

EXISTING LENGTH

====

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 711.962
 BENCHMARK : NORTH EAST BOLT OF HYDRANT
 LOCATION : ON NORTH WEST CORNER OF IL-31 & CARLISLE ROAD

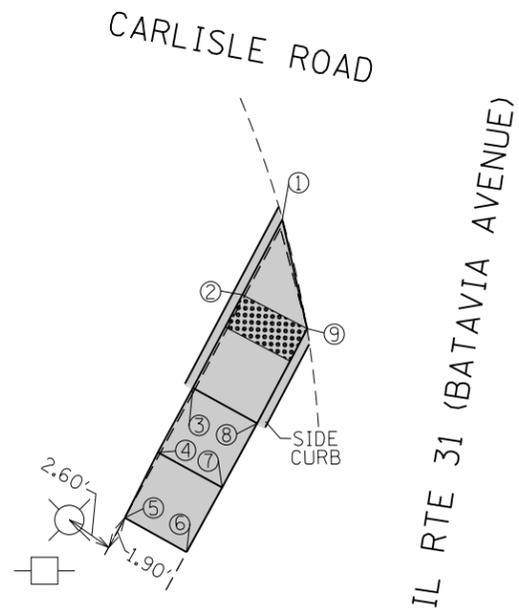
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	PLOT DATE = 2/10/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS
IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.

SCALE: SHEET OF SHEETS STA. TO STA.

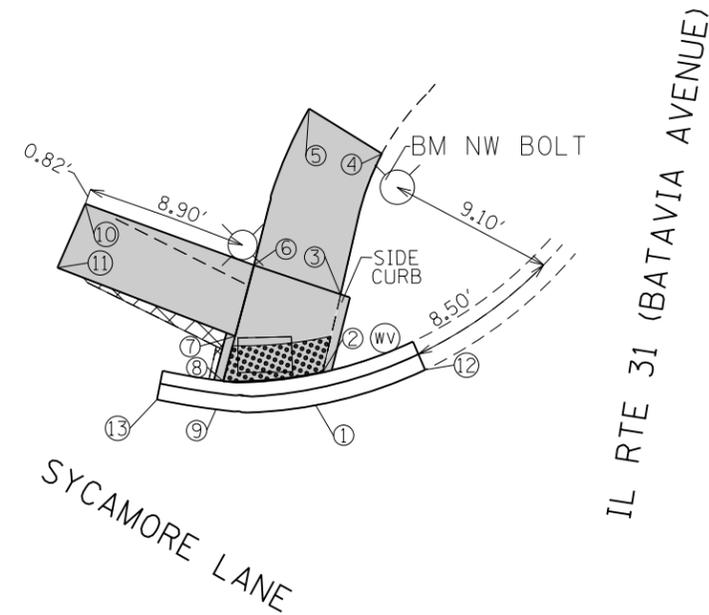
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	19
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				



ADA RAMP SOUTHWEST CORNER IL-31 AT CARLISLE RD.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	710.05	Meet Existing
2	NA	710.05
3	NA	710.06
4	NA	710.06
5	710.06	Meet Existing
6	710.15	Meet Existing
7	NA	710.10
8	NA	710.10
9	710.04	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	4.6	0.10%
2-3	5.8	0.17%
3-4	4	0.00%
4-5	4	0.07%
5-6	3.9	2.30%
6-7	4	1.31%
7-8	4	0.00%
8-9	5.8	1.06%
1-9	6.1	0.27%
2-9	4	0.29%
3-8	4	1.00%
4-7	4	1.00%



ADA RAMP NORTHWEST CORNER IL-31 AT SYCAMORE LN.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	696.39	Meet Existing
2	NA	696.40
3	NA	696.40
4	696.91	Meet Existing
5	696.18	Meet Existing
6	NA	696.40
7	NA	696.40
8	NA	696.40
9	696.46	696.43
10	696.40	Meet Existing
11	696.29	Meet Existing
12	696.32	Meet Existing
13	696.52	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.50	0.9%
2-3	4.34	0.0%
3-4	8.00	6.4%
4-5	4.70	15.6%
5-6	8.00	2.8%
6-7	4.00	0.0%
7-8	2.50	0.0%
8-9	1.50	2.0%
10-11	4.00	2.6%
10-6	9.80	0.0%
12-13	14.60	1.4%
1-9	5.50	0.8%
2-8	5.50	0.0%
3-6	5.00	0.0%
11-7	10.4	1.0%

REFERENCE BENCHMARK ELEV 711.962

BENCHMARK : NORTH EAST BOLT OF HYDRANT

LOCATION : ON NORTH WEST CORNER OF IL-31 & CARLISLE ROAD

LEGEND



EXISTING LENGTH



PROPOSED SIDE CURB



EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 697.658

BENCHMARK : NORTH WEST BOLT OF HYDRANT

LOCATION : ON NORTH WEST CORNER OF IL-31 AND SYCAMORE LANE

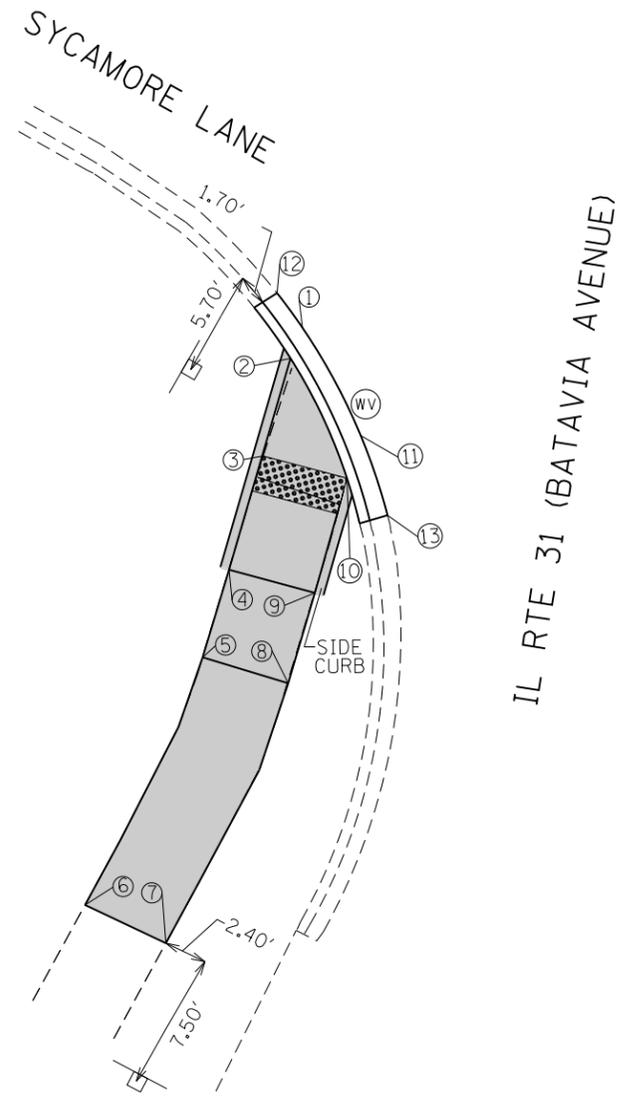
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	PLOT DATE = 2/10/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMPS
IL-31 (BATAVIA AVE.) FROM MAIN ST. TO MAPLE AVE.

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	20
CONTRACT NO. 62C69				
ILLINOIS FED. AID PROJECT				



ADA RAMP SOUTHWEST CORNER IL-31 AT SYCAMORE LN.

POINT NO.:	EX. ELEVATION	PR. ELEVATION
1	696.12	696.27
2	NA	696.30
3	NA	696.40
4	NA	696.80
5	NA	696.80
6	697.97	Meet Existing
7	697.92	Meet Existing
8	NA	696.80
9	NA	696.80
10	NA	696.40
11	696.41	Meet Existing
12	696.12	Meet Existing
13	696.56	Meet Existing

POINT NO.	DISTANCE (FEET)	GRADES
1-2	1.50	2.0%
2-3	5.50	1.8%
3-4	6.50	6.2%
4-5	5.00	0.0%
5-6	15.00	7.8%
6-7	5.00	1.0%
7-8	15.00	7.5%
8-9	5.00	0.0%
9-10	6.50	6.2%
12-13	13.70	3.2%
1-11	6.70	2.0%
2-10	7.30	1.4%
3-10	5.00	0.0%
4-9	5.00	0.0%
5-8	5	0.0%
10-11	1.5	0.4%

REFERENCE BENCHMARK ELEV 697.658
 BENCHMARK : NORTH WEST BOLT OF HYDRANT
 LOCATION : ON NORTH WEST CORNER OF IL-31 AND SYCAMORE LANE

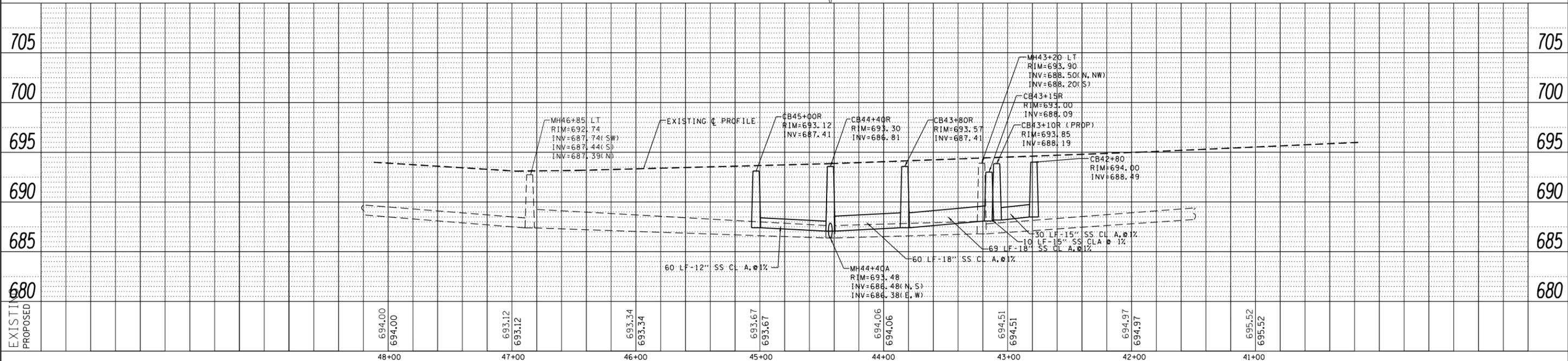
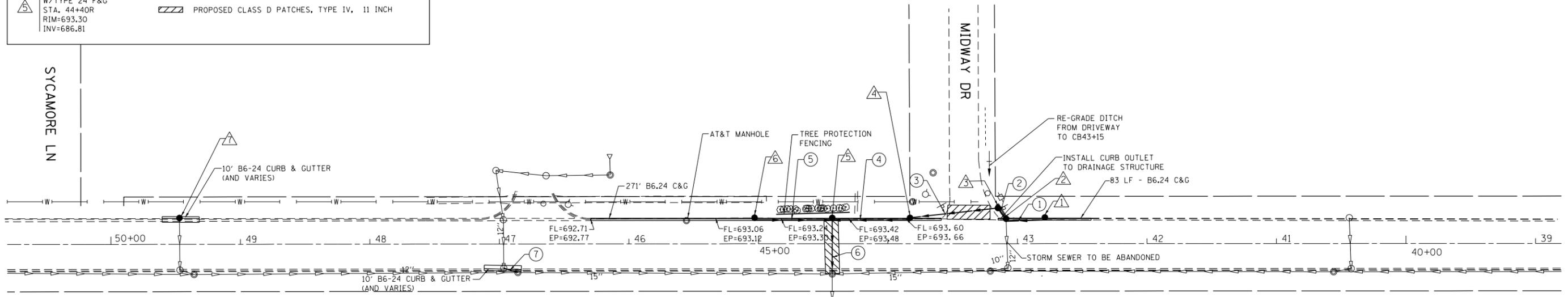
LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

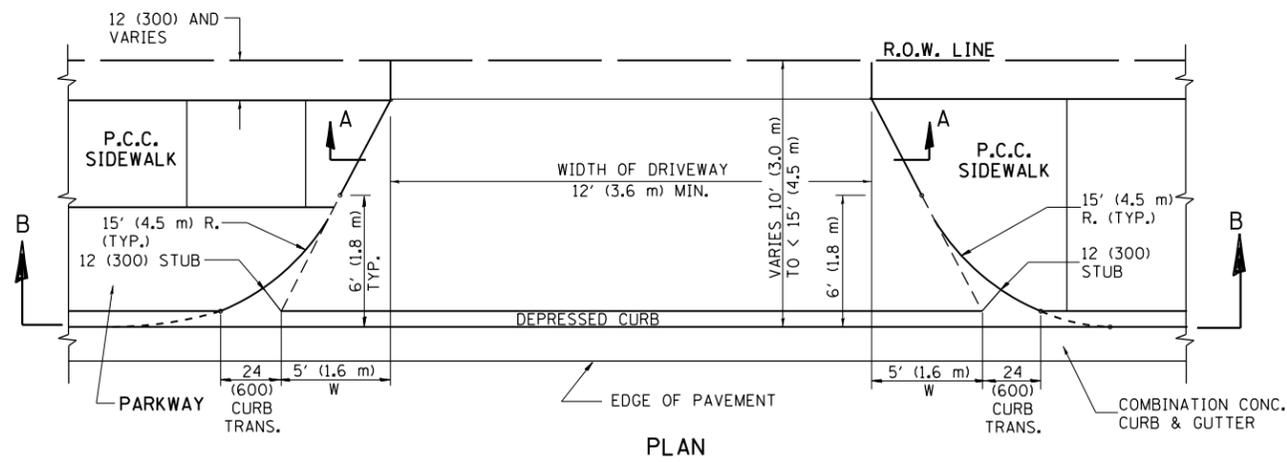
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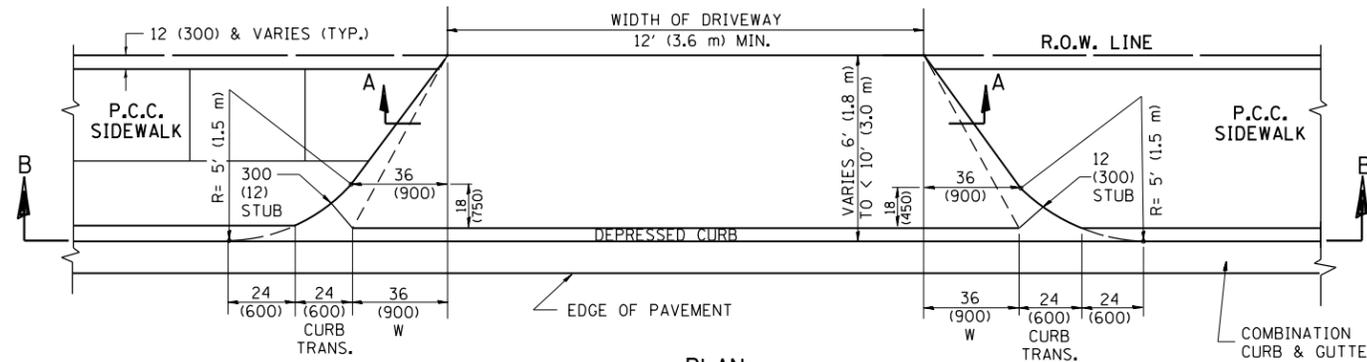
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	CB TYPE A, 4' DIA W/TYPE 24 F&G STA. 43+15R RIM=693.00 INV=688.09
	CB TYPE A, 4' DIA W/TYPE 24 F&G STA. 43+80R RIM=693.57 INV=687.41
	CB TYPE A, 4' DIA W/TYPE 24 F&G STA. 44+40R RIM=693.30 INV=686.81
	CB TYPE C, 4' DIA W/TYPE 24 F&G STA. 45+00R RIM=693.12 INV=687.41
	REMOVING CATCH BASINS CB TYPE C, 4' DIA W/TYPE 24 F&G STA. 49+47R INV= MEET EXISTING
	① STORM SEWERS, CLASS A, TYPE 2 15", 30'
	② STORM SEWERS, CLASS A, TYPE 2 15", 10'
	③ STORM SEWERS, CLASS A, TYPE 2 18", 69'
	④ STORM SEWERS, CLASS A, TYPE 2 18", 60'
	⑤ STORM SEWERS, CLASS A, TYPE 2 12", 60'
	⑥ STORM SEWERS, CLASS A, TYPE 2 18", 43'
	⑦ REMOVE EXISTING 10" STORM SEWER, 11' PROPOSED STORM SEWERS, CLASS A, TYPE 2 12", 12'
	PROPOSED CLASS D PATCHES, TYPE IV, 11 INCH



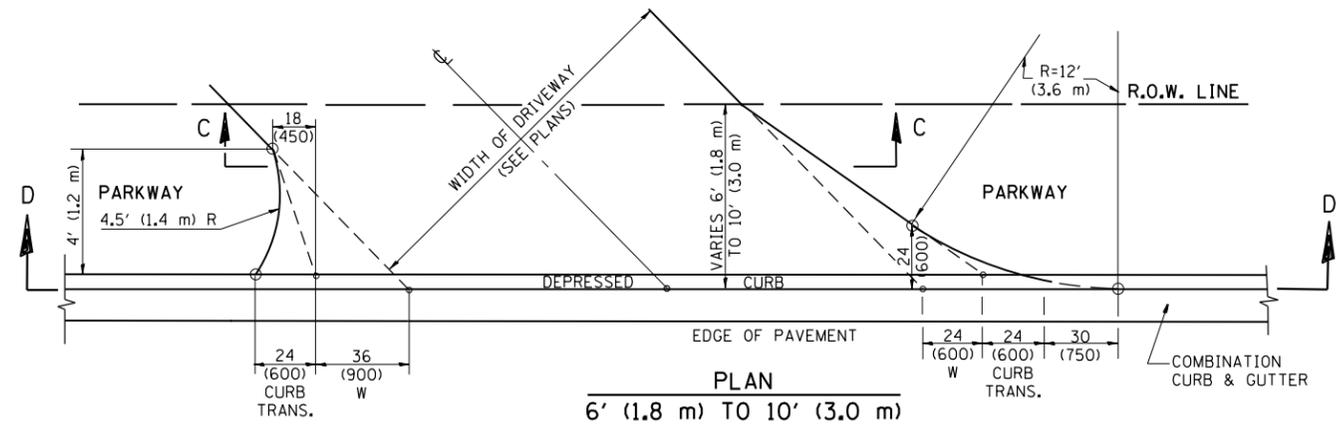
FILE NAME =	USER NAME = hassan	DESIGNED	REVISED	ILLINOIS ROUTE 31 PROPOSED DRAINAGE PLAN		F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
						3887	A-R-RS-3	KANE	34	22
						CONTRACT NO. 62C69				
						ILLINOIS FED. AID PROJECT				



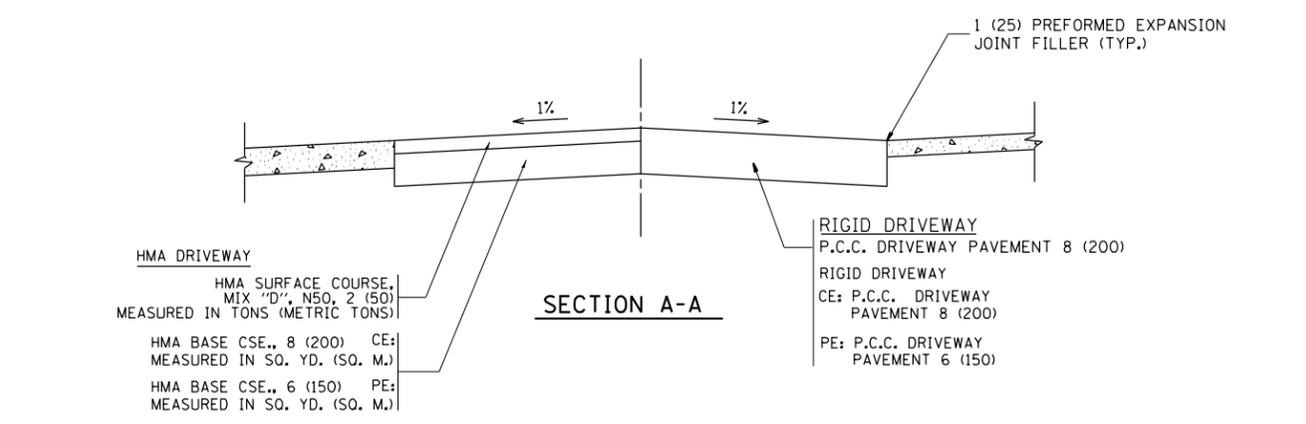
PLAN
10' (3.0 m) TO < 15' (4.5 m)



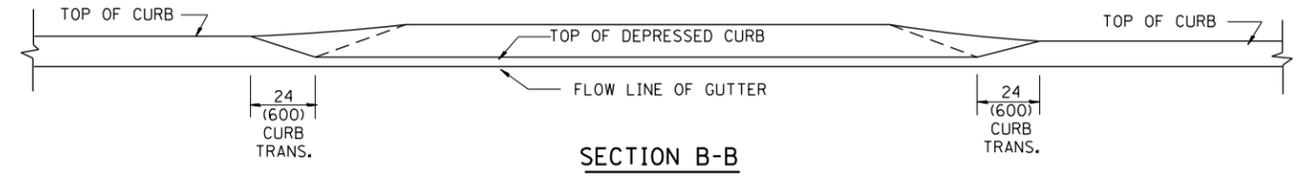
PLAN
6' (1.8 m) TO < 10' (3.0 m)



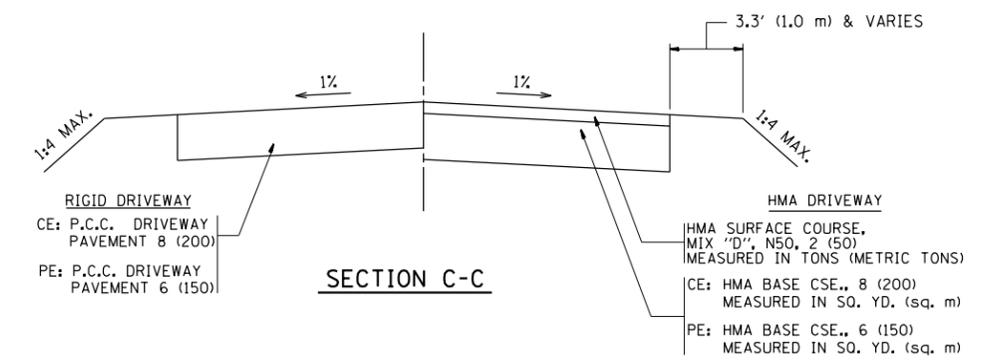
PLAN
6' (1.8 m) TO 10' (3.0 m)



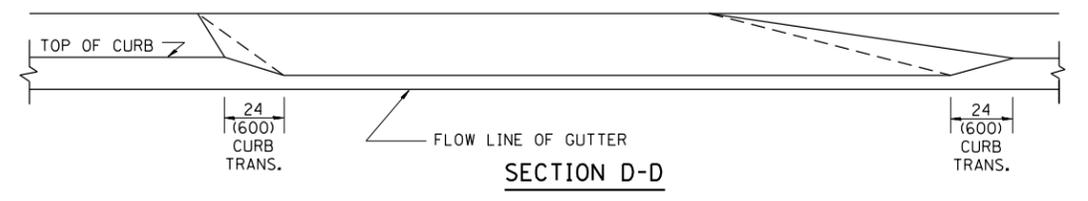
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

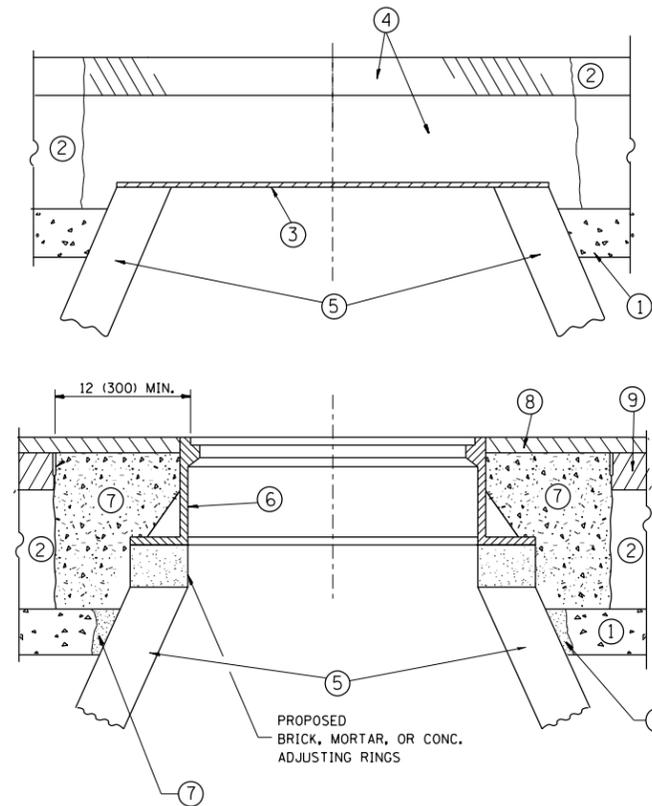
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = hassann	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
pw\1\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\01376\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - P. LaFLEUR 04-15-03
		DATE - 11-06-95	REVISED - R. BORO 01-01-07
			REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	23
BD400-02 (BD-02)			CONTRACT NO. 62C69	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

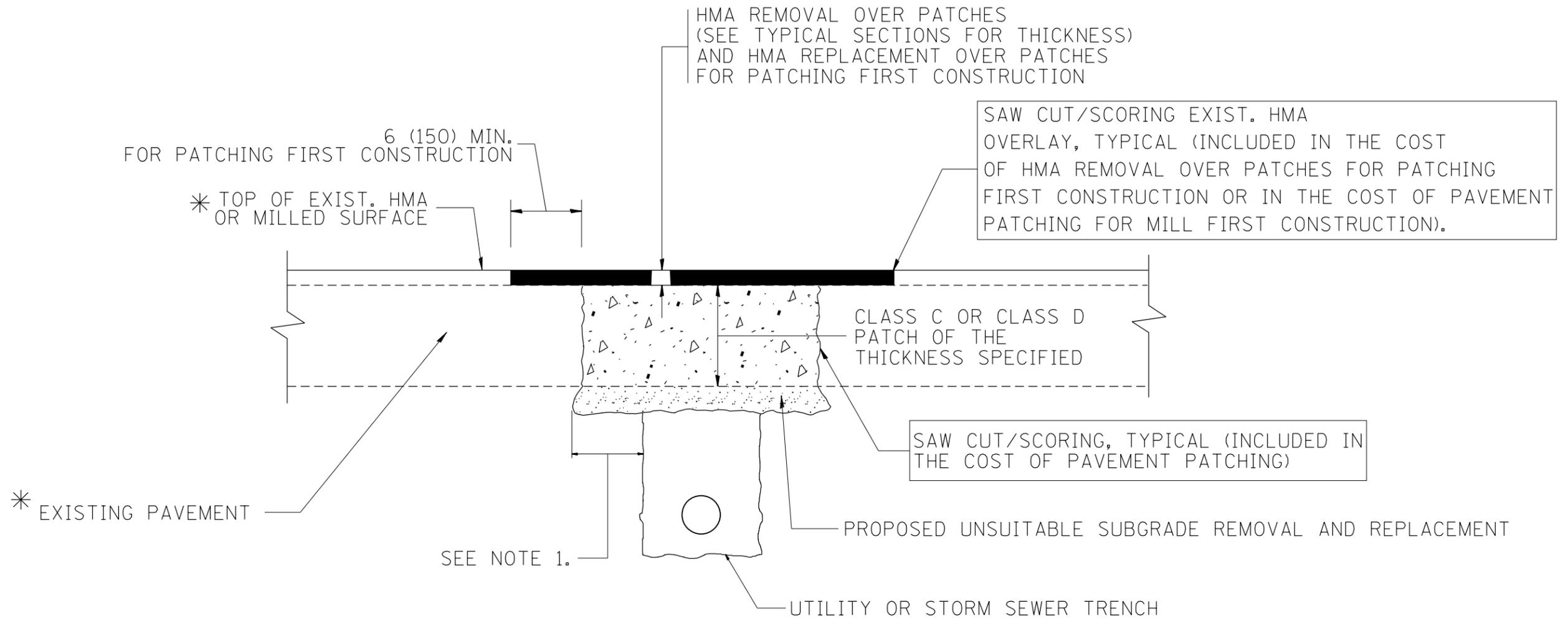
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = hassann	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI376\BROWNS\Design\Diststd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 2/10/2017	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	24
BD600-03 (BD-8)		CONTRACT NO.	62C69	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

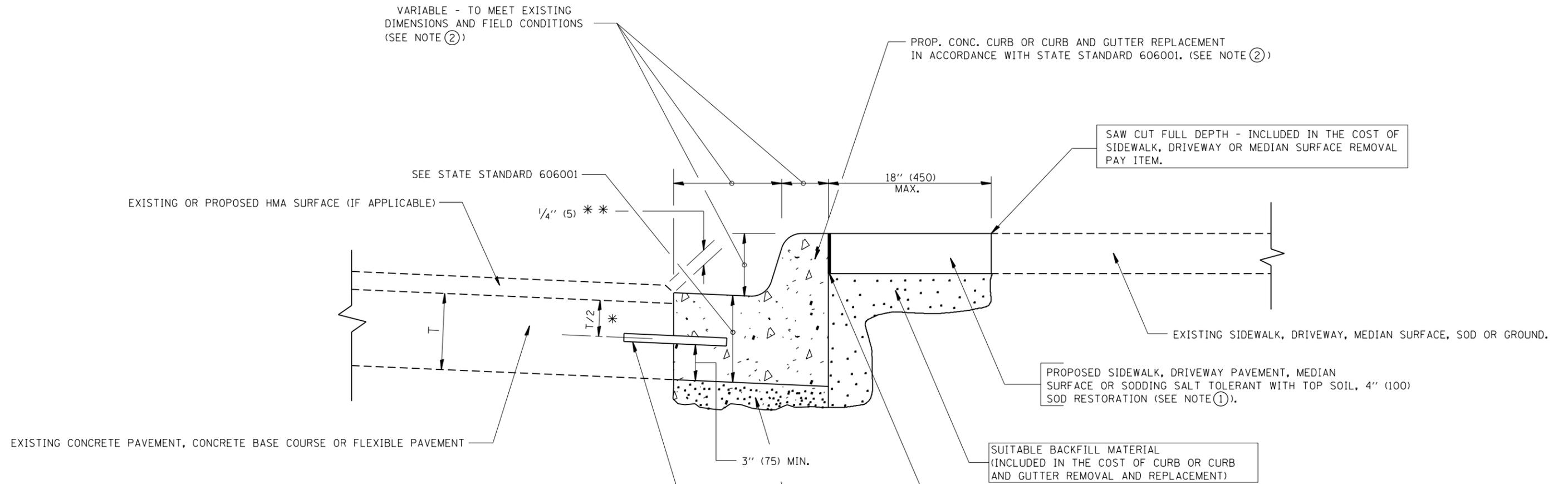
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hassann	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI376\BROWNS\Design\Diststd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					3887	A-R-RS-3	KANE	34	25
	PLOT DATE = 2/10/2017	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62C69				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
 - ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
 - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
 - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
 - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
 - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
 - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

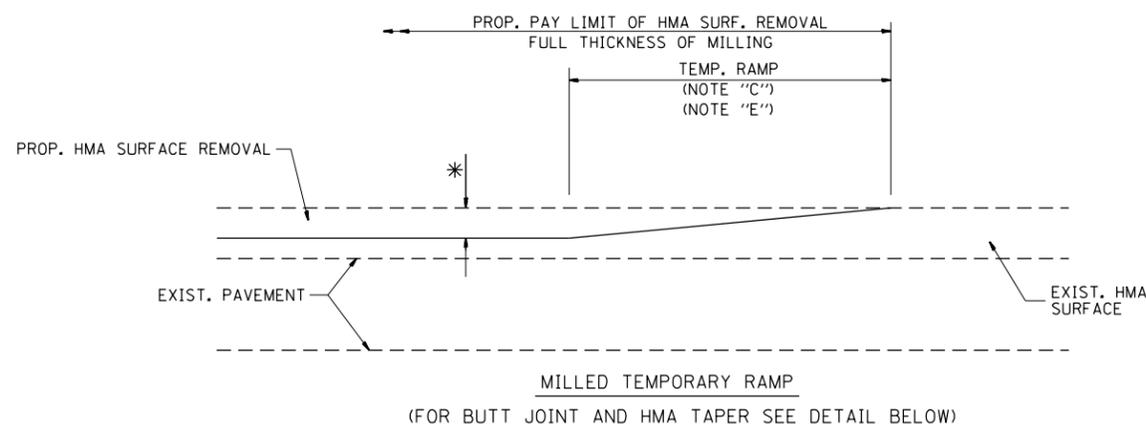
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

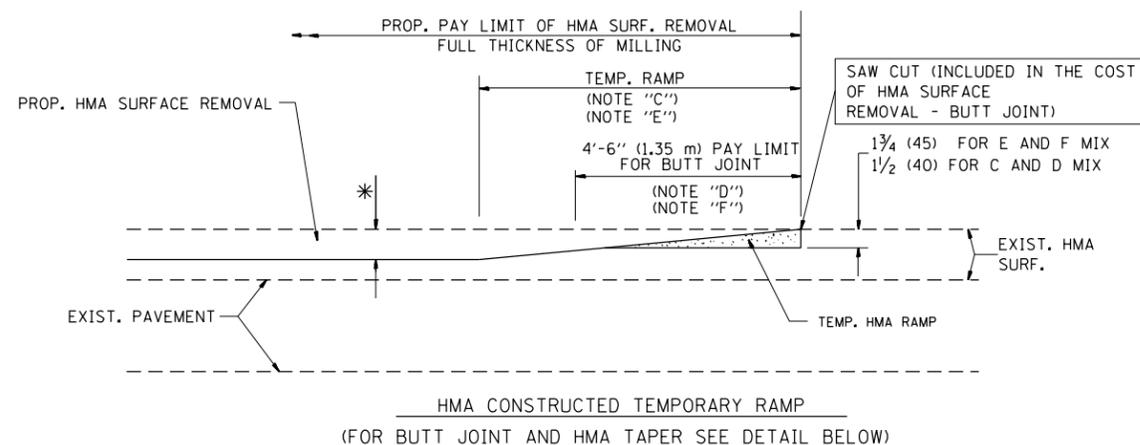
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hassan	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			REVISED - A. ABBAS 03-21-97			3887	A-R-RS-3	KANE	34	26
			REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 62C69		
			REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
PLOT SCALE = 100.0000' / 1in.		CHECKED -	DATE - 03-11-94	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.			

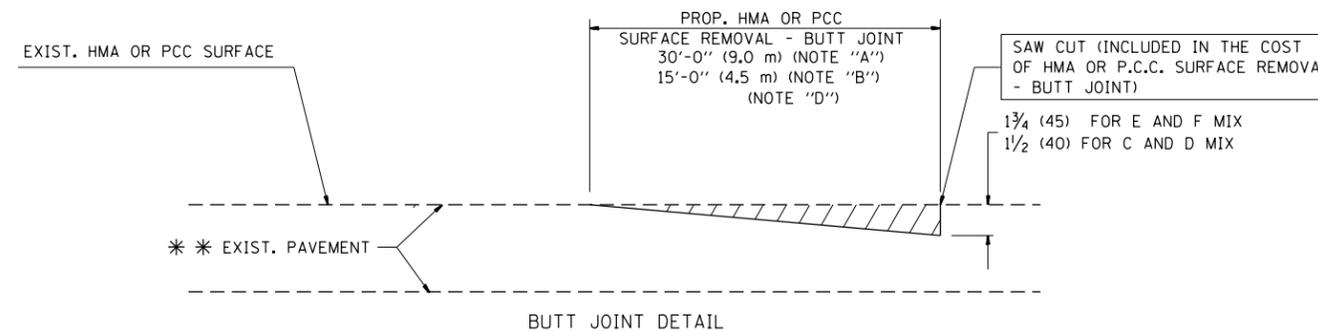


OPTION 1

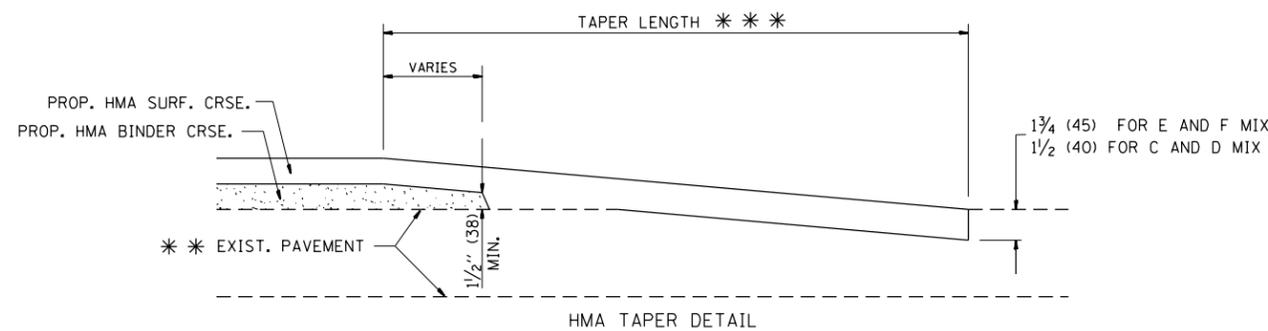


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

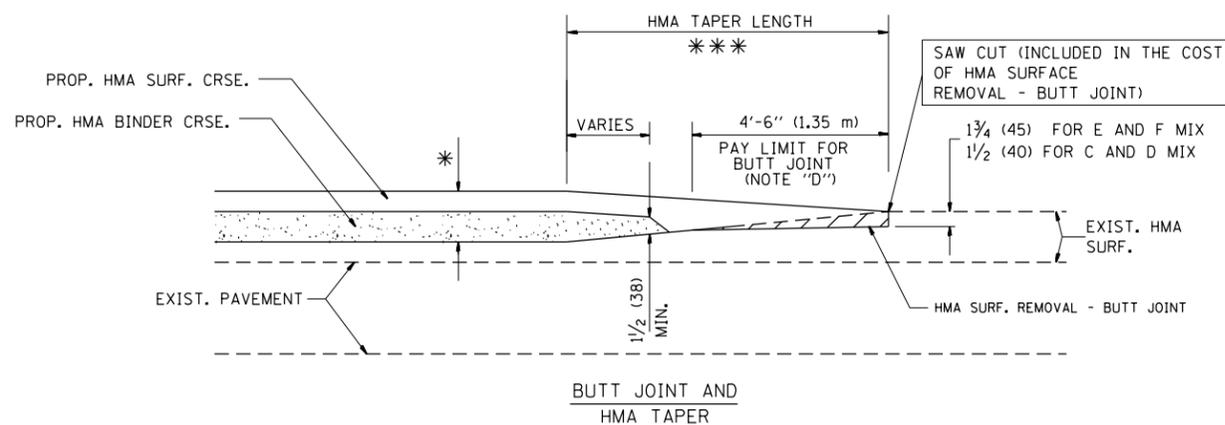
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

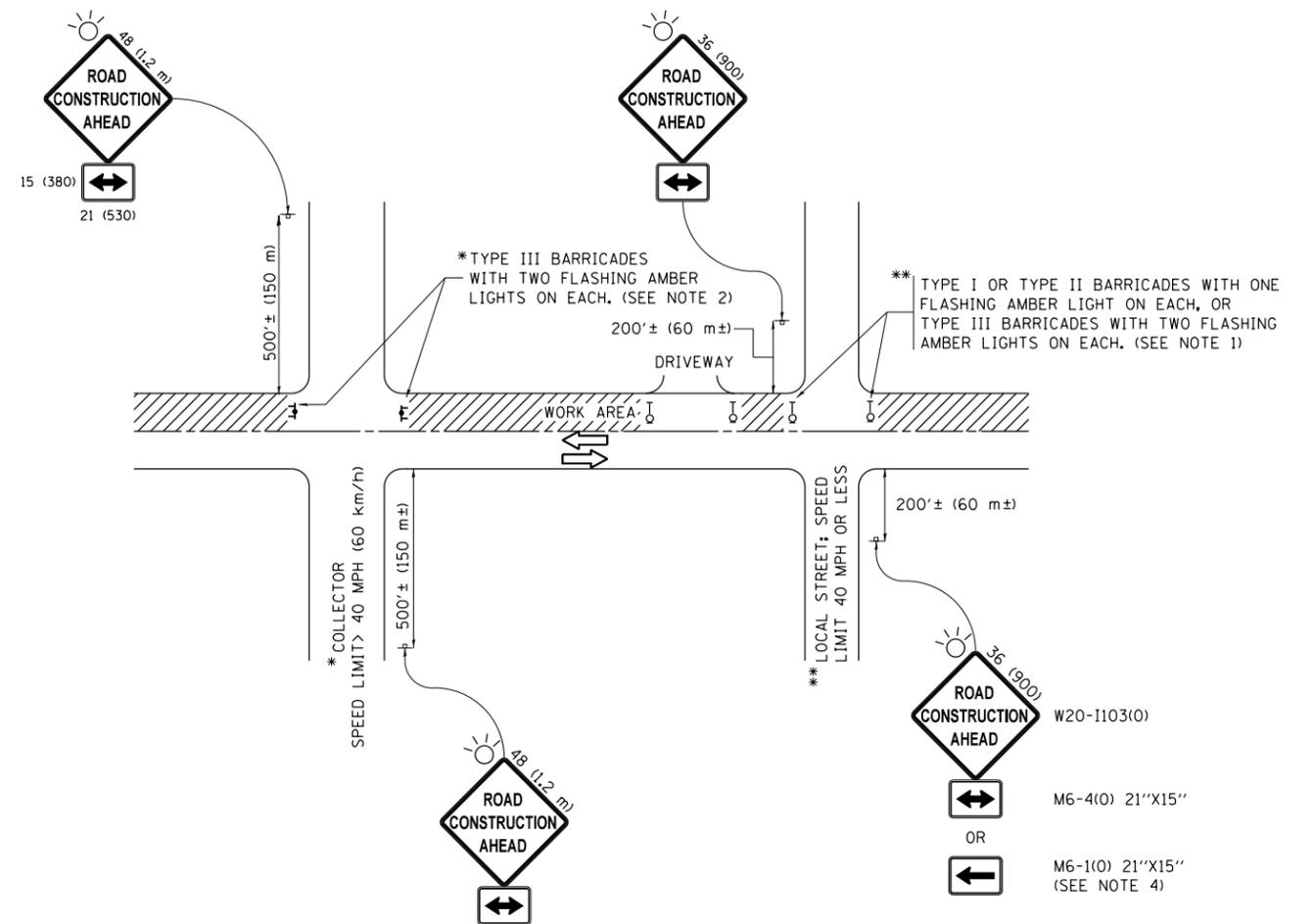
FILE NAME =	USER NAME = hassan	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
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	PLOT SCALE = 100.0000' / 1in.	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 2/10/2017		REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	27
BD400-05 BD32		CONTRACT NO. 62C69		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

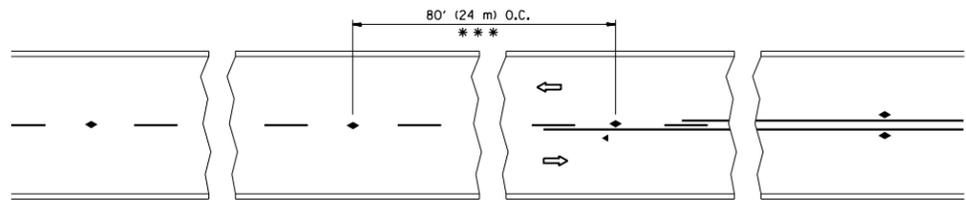
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p:\11\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI376\Drawings\Design\Diststd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 2/10/2017	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

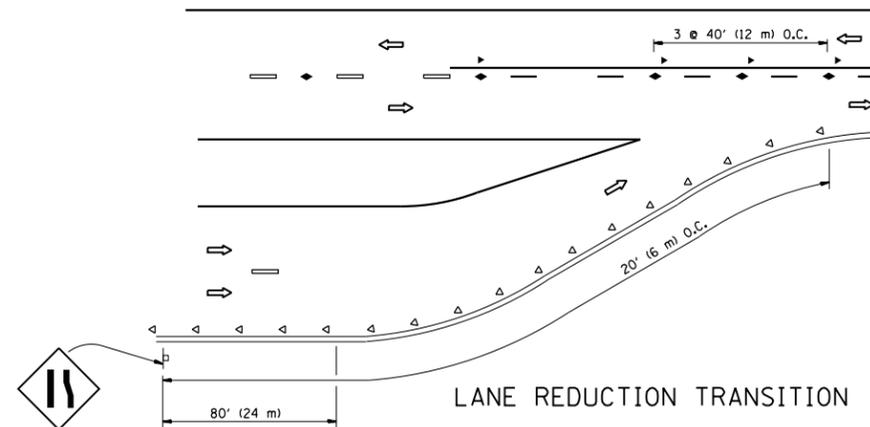
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	28
TC-10			CONTRACT NO.	62C69
ILLINOIS FED. AID PROJECT				

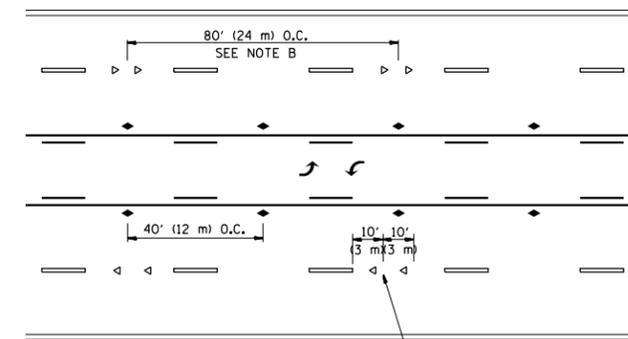


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

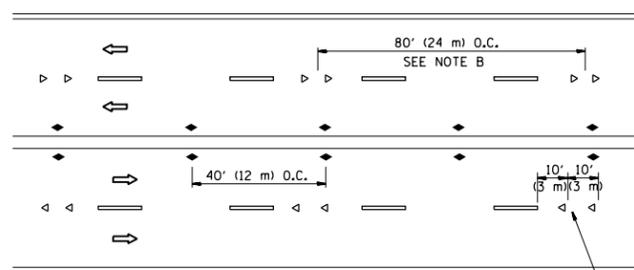
TWO-LANE/TWO-WAY



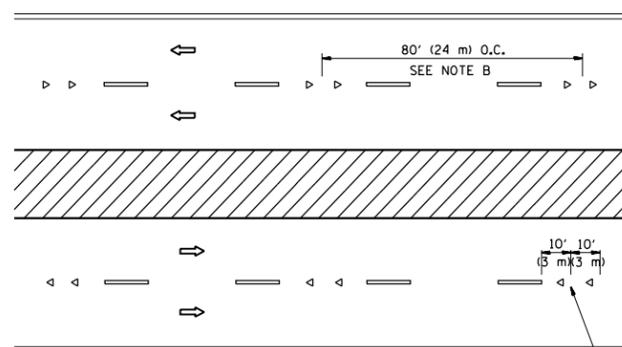
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

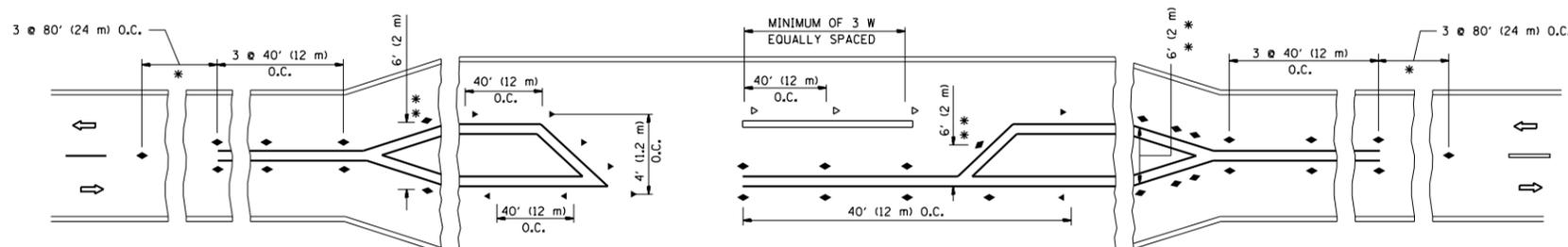
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

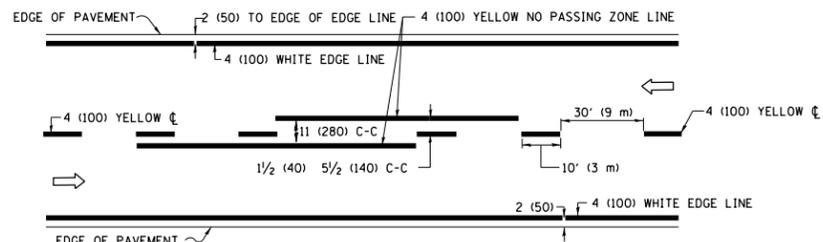
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw:\IL\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI376\BROW\Meta\Design\Diststd.dgn		CHECKED -	REVISED - T. RAMMACHER 03-12-99
		DATE -	REVISED - T. RAMMACHER 01-06-00
			REVISED - C. JUCIUS 09-09-09

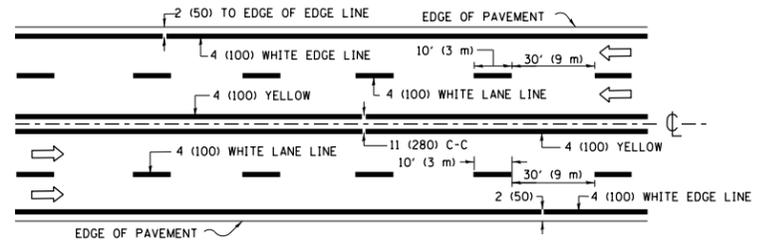
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

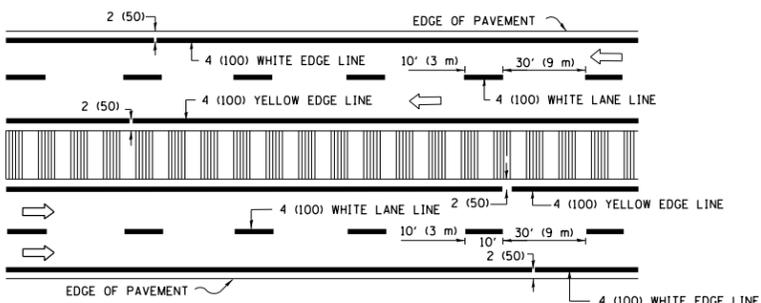
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	29
TC-11			CONTRACT NO. 62C69	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

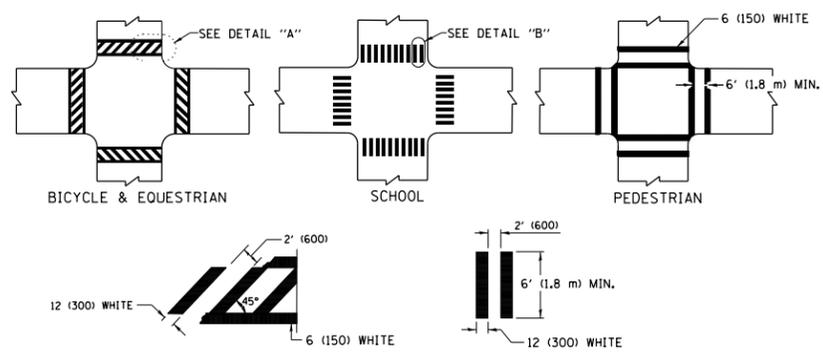


MULTI-LANE UNDIVIDED



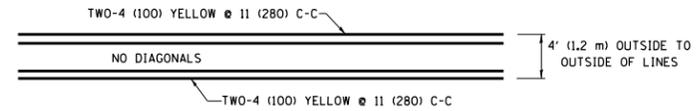
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

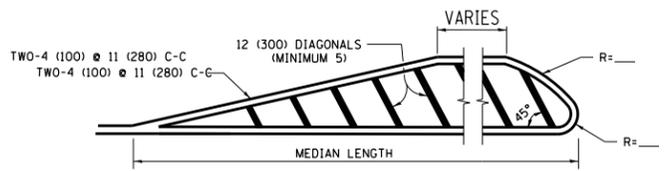


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

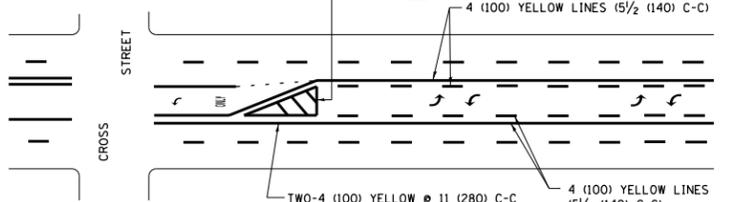


4' (1.2 m) WIDE MEDIANS ONLY



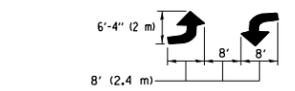
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

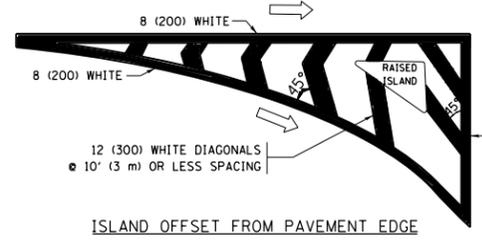
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



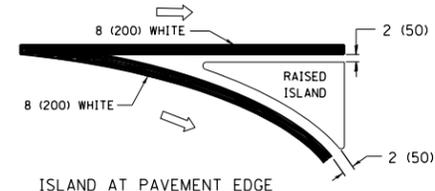
TYPICAL LEFT (OR RIGHT) TURN LANE

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

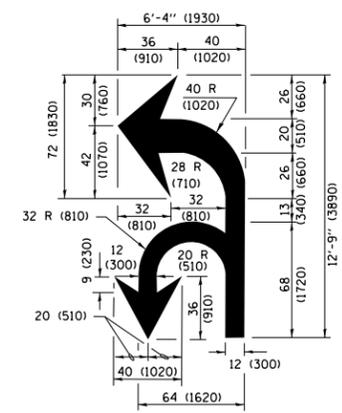
TYPICAL TURN LANE MARKING



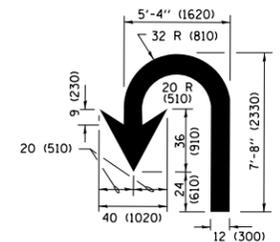
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = hassann	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01376\BROWN\Meta\Design\Diststd.dgn			REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 2/10/2017	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	30
TC-13			CONTRACT NO.	62C69
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

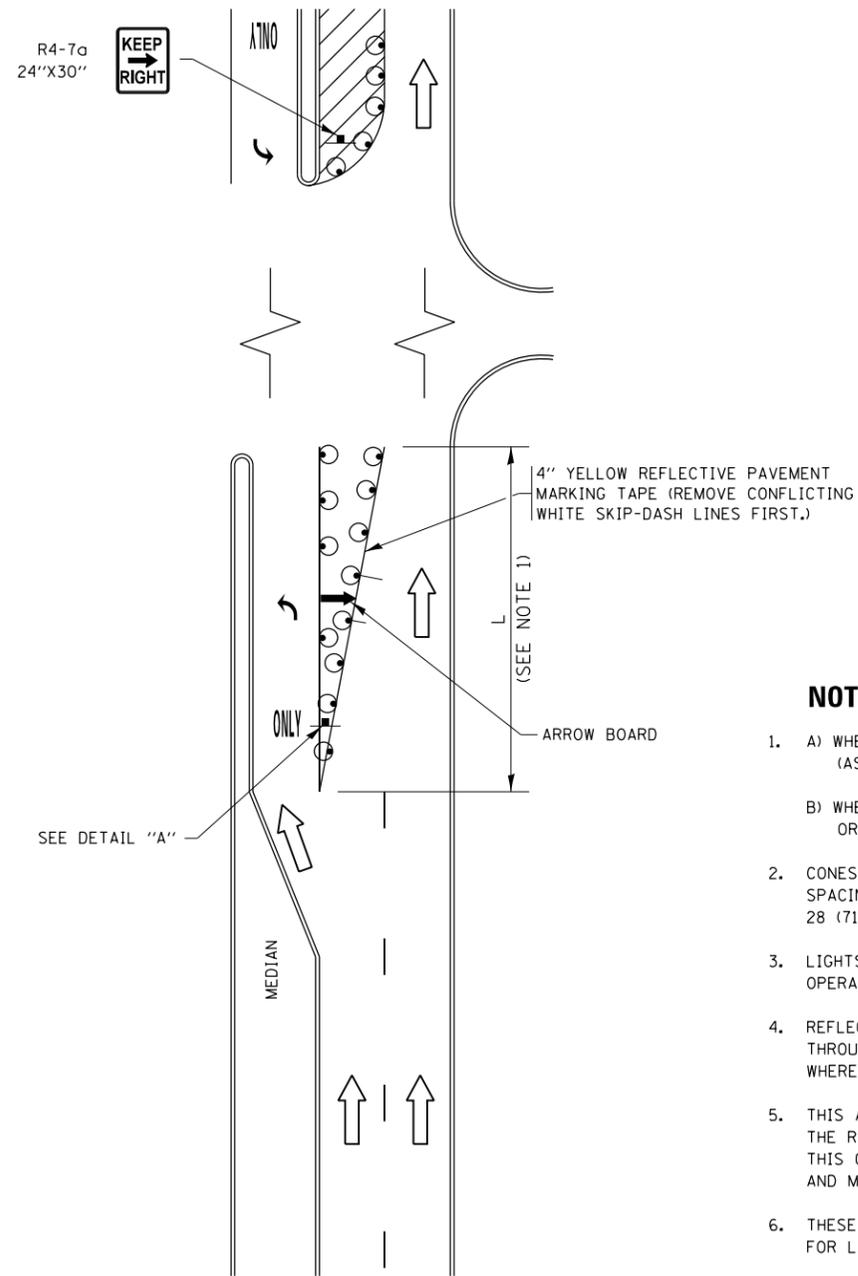


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

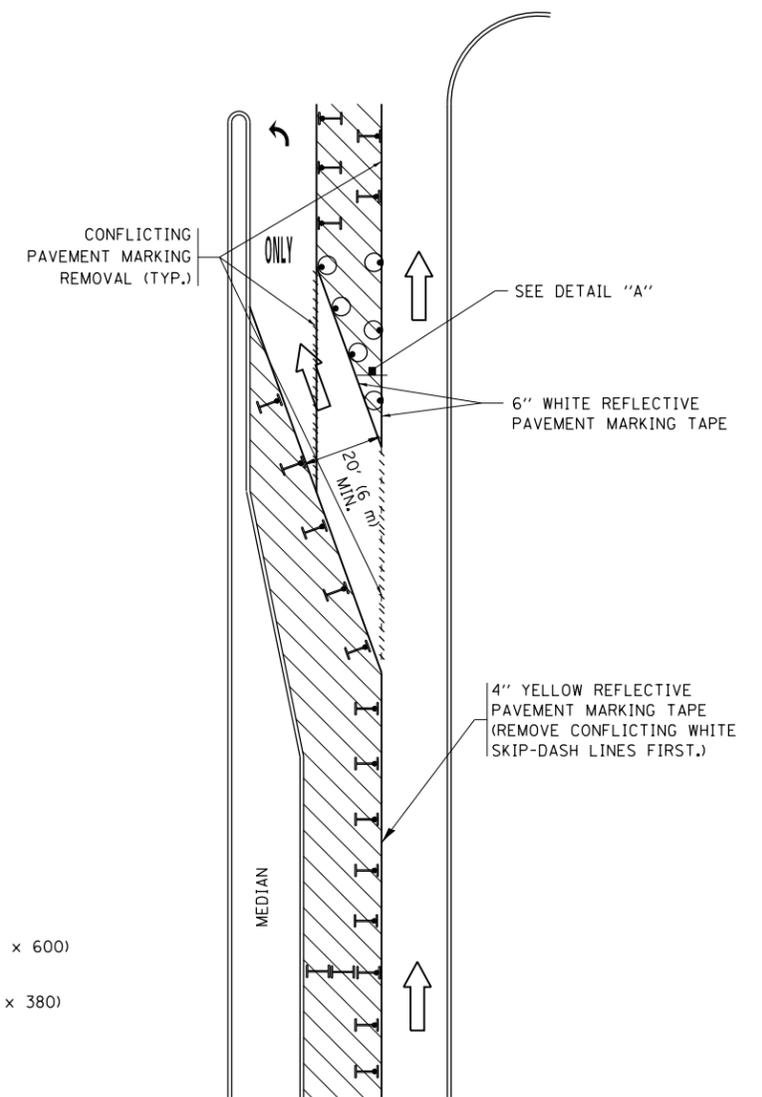
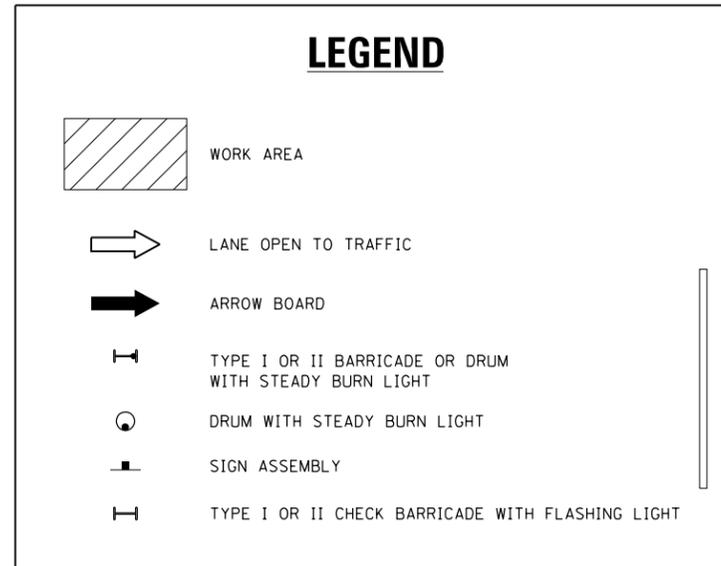
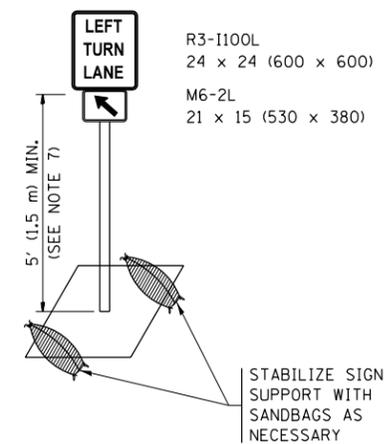


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

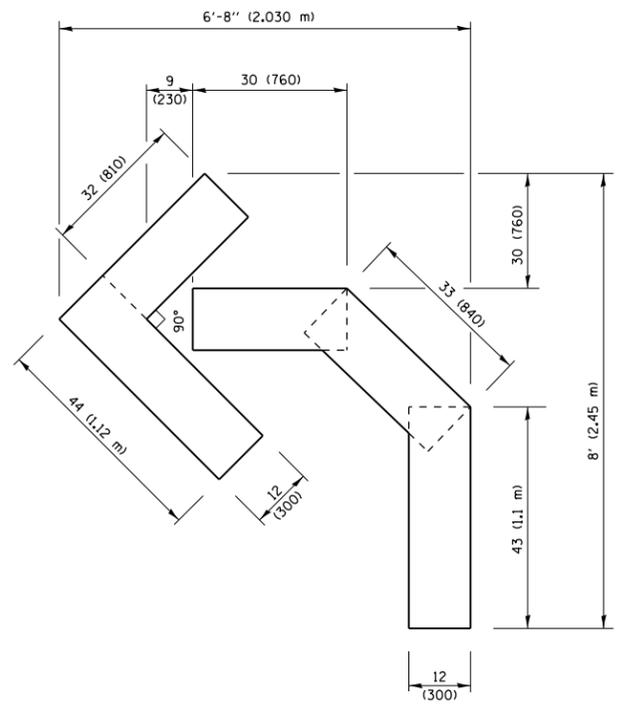


DETAIL A

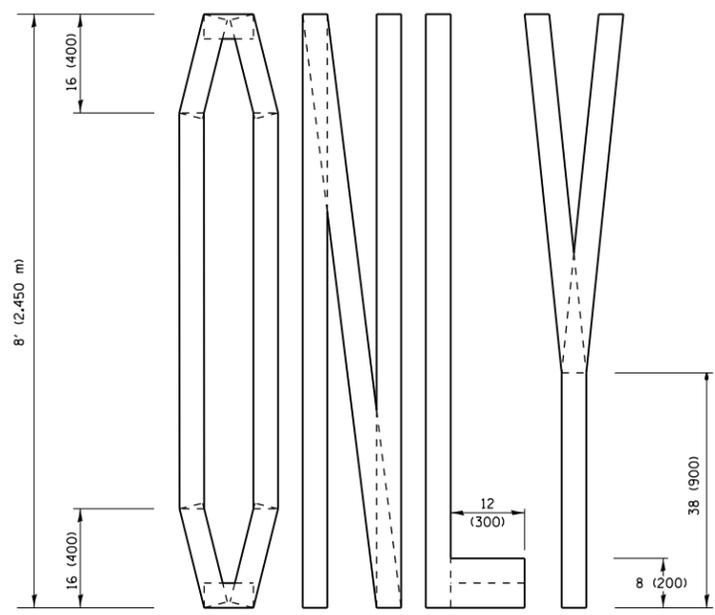
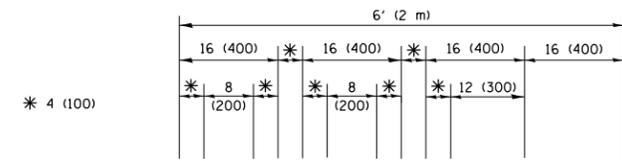
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = hassann	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.U. R.T.E. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\01376\REVISED Design\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			3887	A-R-RS-3	KANE	34	31
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62C69		
	PLOT DATE = 2/10/2017	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.

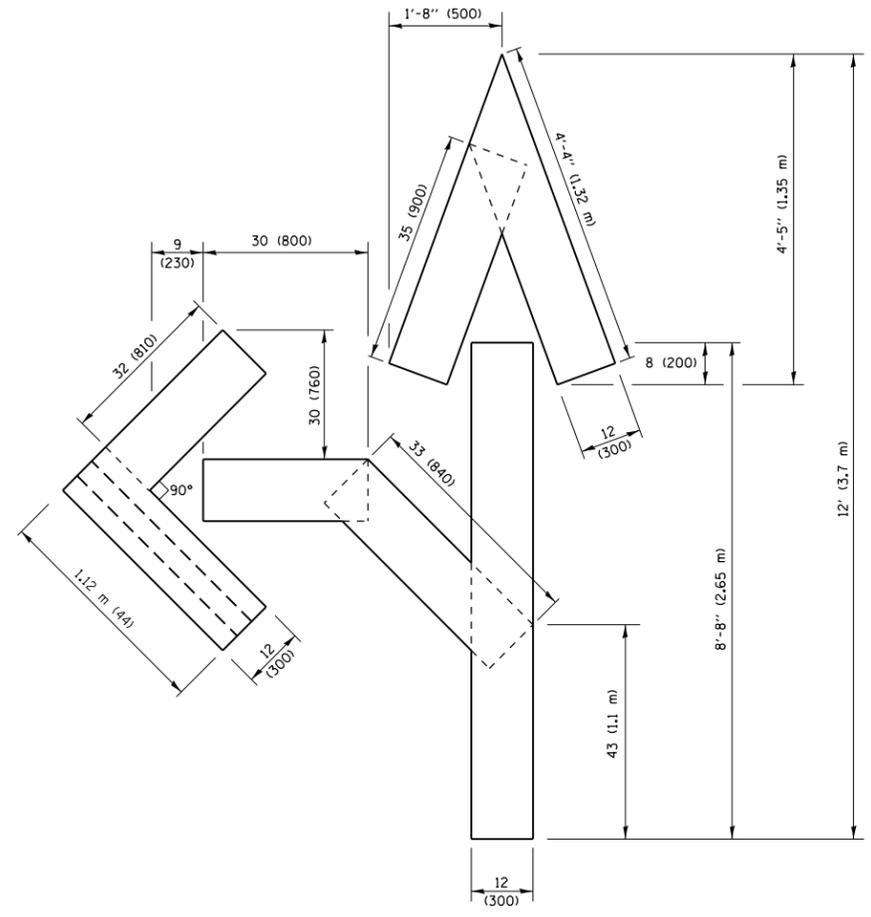
ILLINOIS FED. AID PROJECT



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

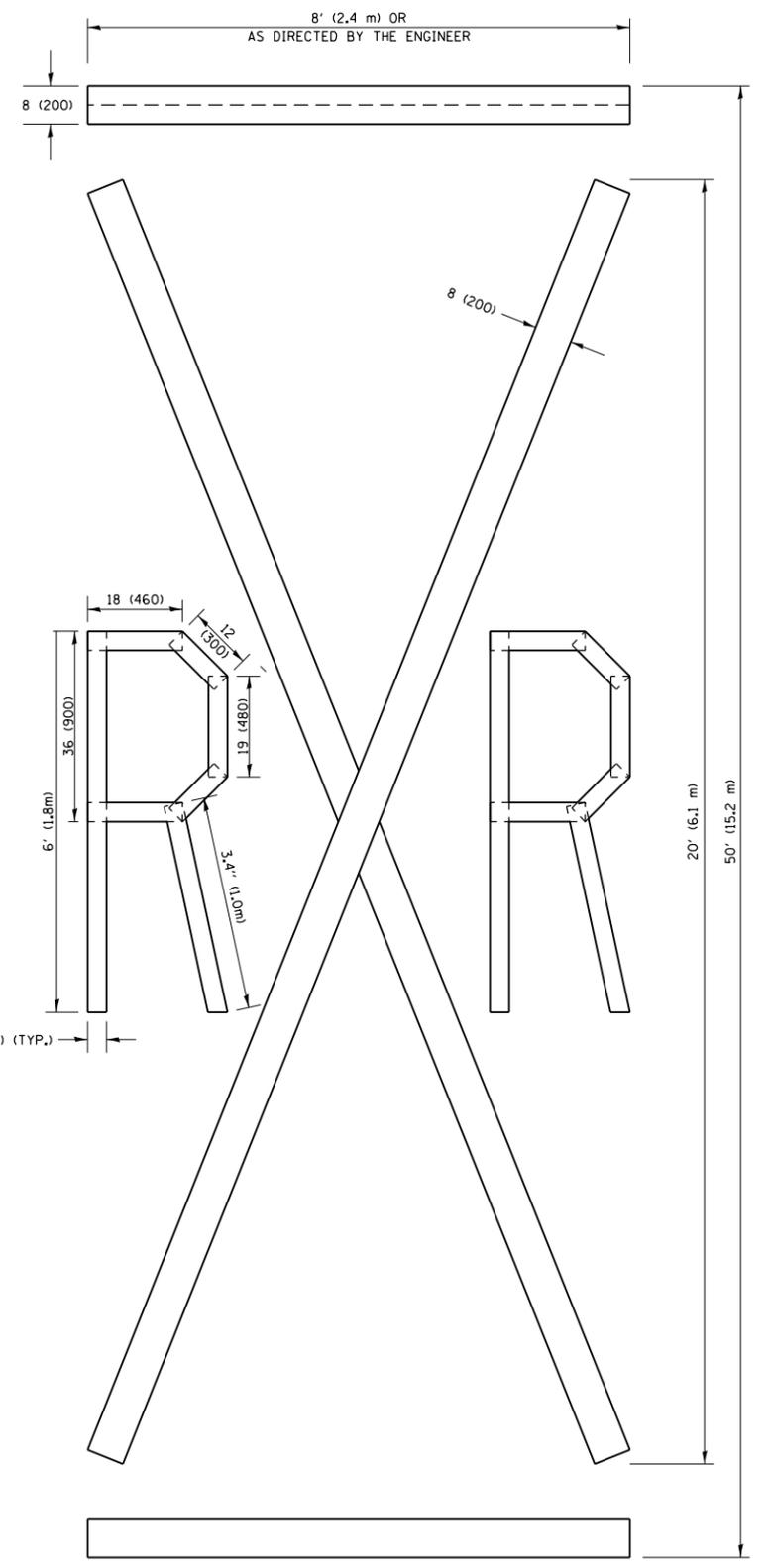


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

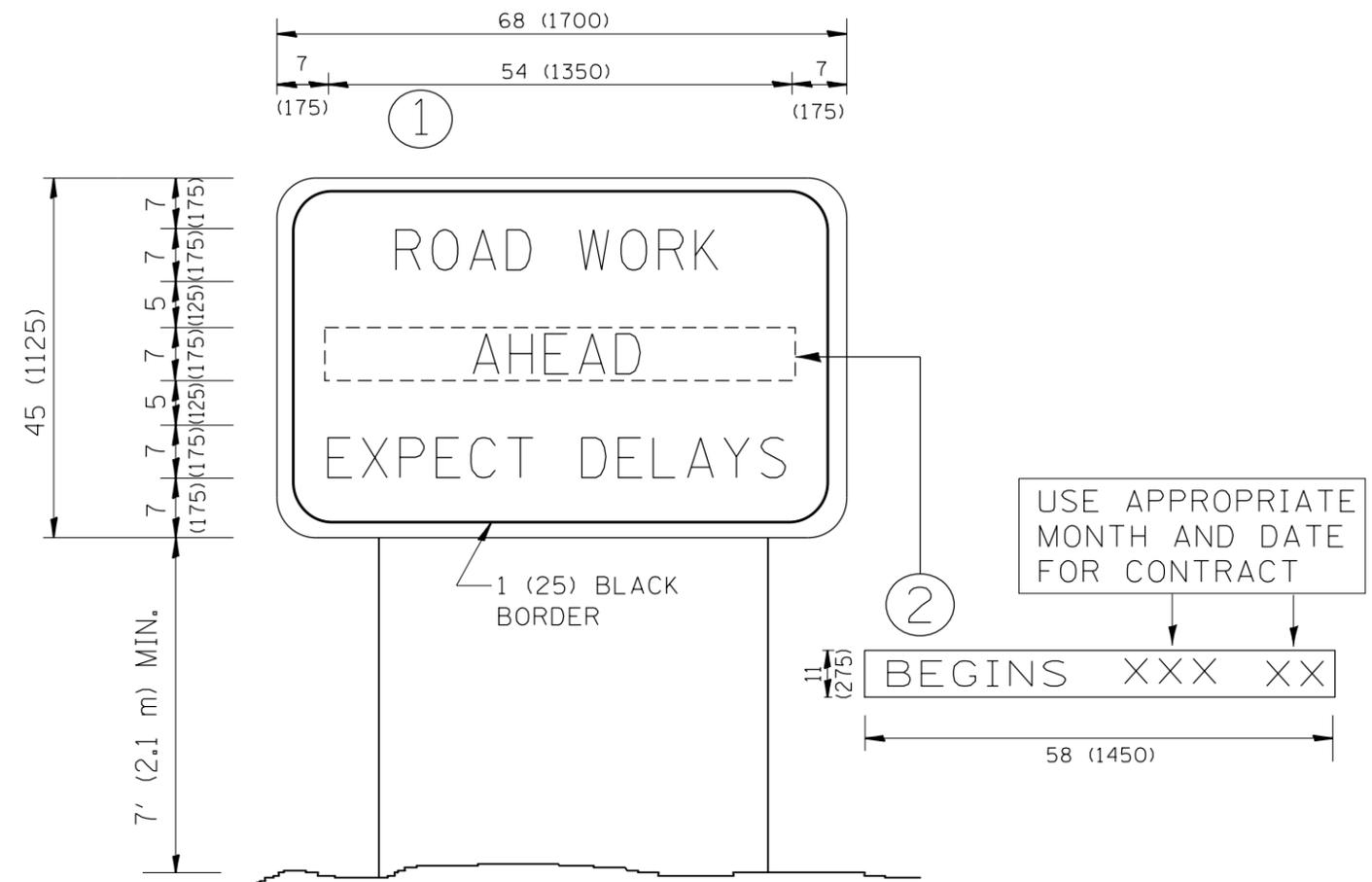
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01376\Drawings\Design\Diststd.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE -	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	32
TC-16			CONTRACT NO. 62C69	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED - R. MIRS 09-15-97
p:\11\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI376\BROWNS\Design\Diststd.dgn		CHECKED -	REVISED - R. MIRS 12-11-97
		PLOT SCALE = 100.0000' / 1in.	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

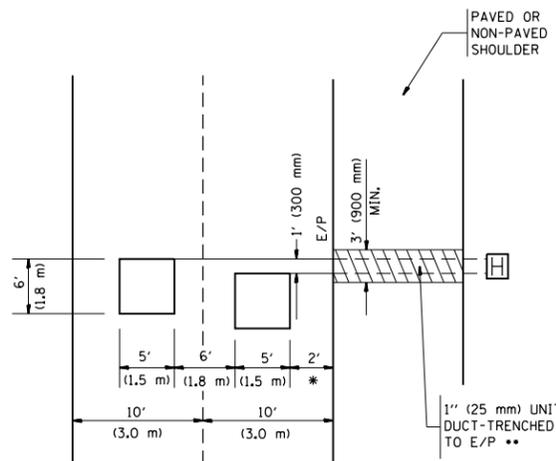
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	A-R-RS-3	KANE	34	33
TC-22		CONTRACT NO. 62C69		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

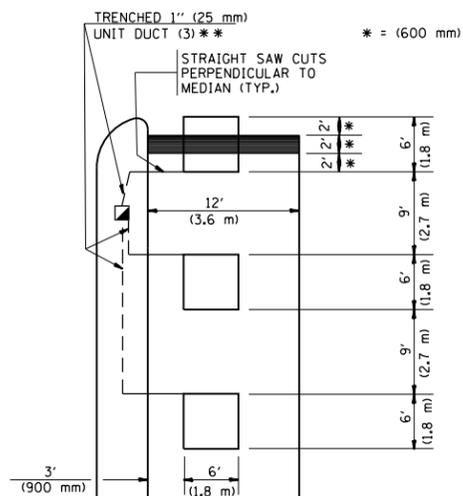


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

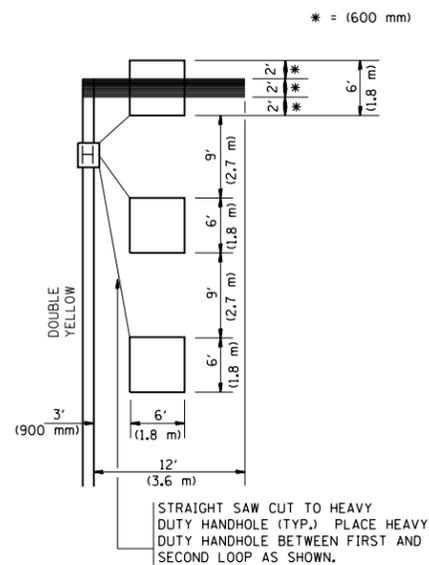
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

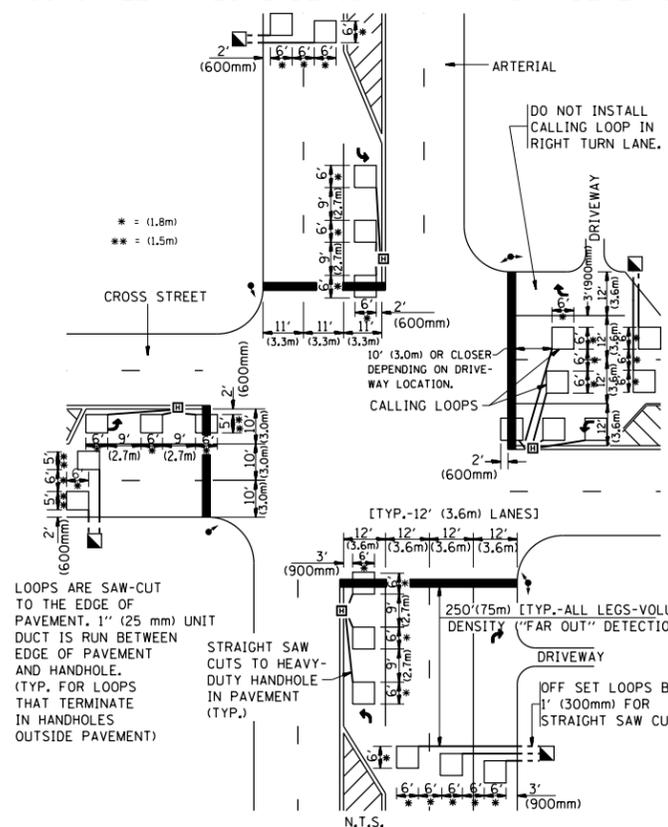
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



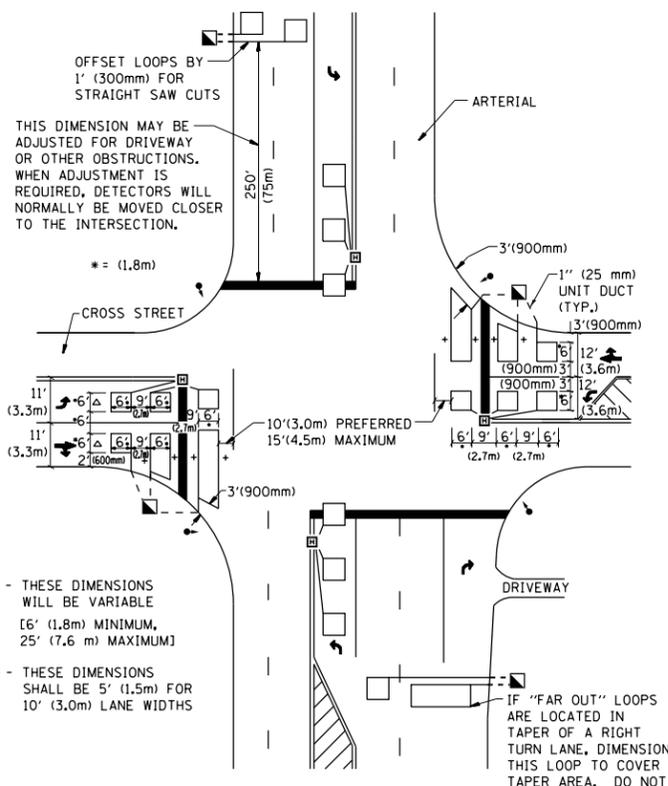
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = hassann	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -		TS-07		CONTRACT NO.		62C69		
PLOT DATE = 2/10/2017					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		