

CONSTRUCTION PLANS - ISSUED MARCH 2, 2018

PHASE 1- RECONSTRUCT MAIN PCC APRON

SALEM-LECKRONE AIRPORT  
SALEM, MARION COUNTY, ILLINOIS

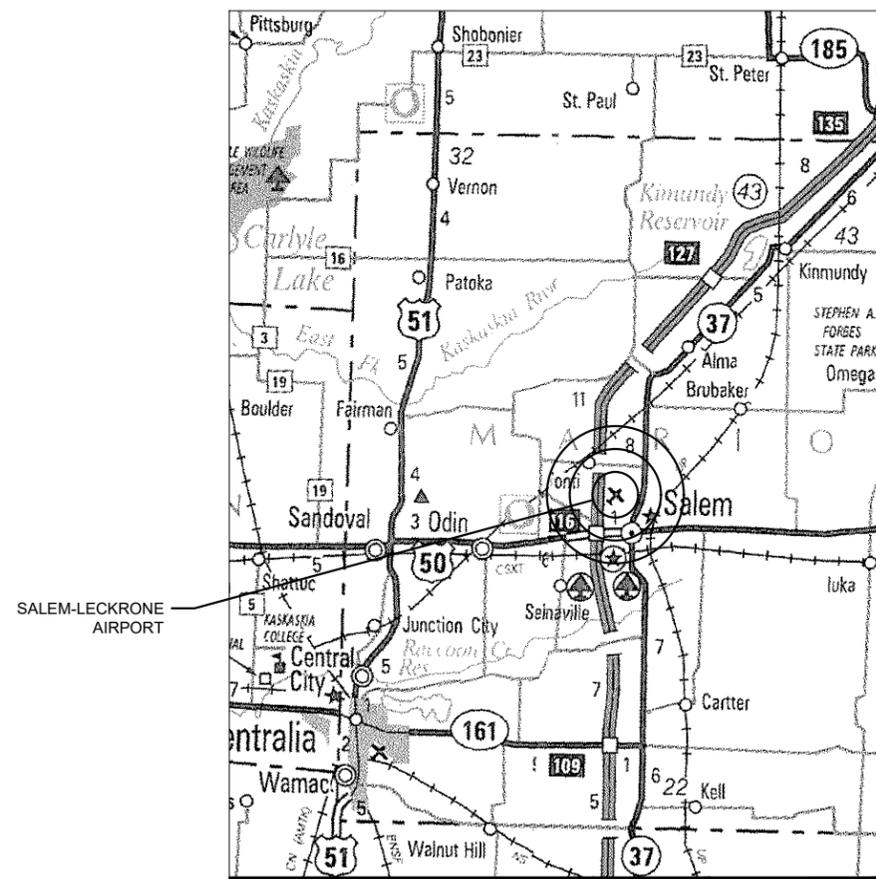
IDA PROJECT NO. SLO-4581  
SBG PROJECT NO. 3-17-SBGP-120/133/139

SCOPE OF WORK:

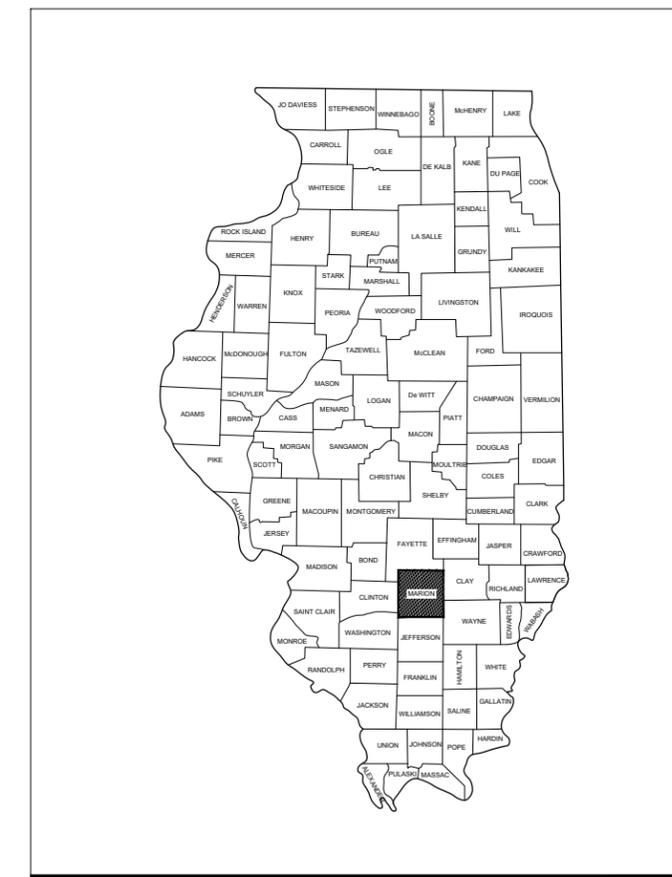
THIS PROJECT CONSIST OF RECONSTRUCTION OF A PORTION OF THE MAIN PCC AIRCRAFT APRON, INCLUDING REMOVAL OF AN ABANDONED UNDERGROUND FUEL STORAGE TANK. THIS PROJECT INCLUDES PAVEMENT REMOVAL, TANK REMOVAL, CONSTRUCTION OF NEW PCC PAVEMENT, PAVEMENT MARKING AND ASSOCIATED ITEMS.

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.



VICINITY MAP



LOCATION MAP

No.	Issue/Description	Sheets Changed	Date	By

  
 Barry Stolz, P.E.  
 Project Manager

  
 BARRY S. STOLZ  
 062-057281  
 LICENSED PROFESSIONAL ENGINEER  
 EXP 11/30/19  
 3/2/18 Date

  
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 Telephone: 217.788.2450  
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Salem-Leckrone Airport  


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 SALEM AIRPORT AUTHORITY  
 1100 Airport Rd.  
 Salem, IL 62881  
 Telephone: 618.548.2411

  
 3-2-18  
 Date

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Salem-Leckrone Airport

SALEM AIRPORT AUTHORITY  
1100 AIRPORT RD.  
SALEM, IL 62881

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L.S.	1	
AR150520	MOBILIZATION	L.S.	1	
AR150540	HAUL ROUTE	L.S.	1	
AR152480	SHOULDER ADJUSTMENT	S.Y.	350	
AR209510	CRUSHED AGGREGATE BASE COURSE	TON	900	
AR401911	REMOVE & REPLACE BIT. SURFACE	S.Y.	391	
AR501506	6" PCC PAVEMENT	S.Y.	3,830	
AR501530	PCC TEST BATCH	EACH	1	
AR501900	REMOVE PCC PAVEMENT	S.Y.	3,830	
AR510510	TIE DOWN	EACH	15	
AR620520	PAVEMENT MARKING-WATERBORNE	S.F.	155	
AR620525	PAVEMENT MARKING-BLACK BORDER	S.F.	92	
AR800554	REMOVE UNDERGROUND FUEL TANK	L.S.	1	
AR800555	CONTAMINATED SOIL REMOVAL	C.Y.	50	

INDEX TO SHEETS	
Sheet Number	Sheet Title
1	COVER SHEET
2	SUMMARY OF QUANTITIES AND INDEX TO SHEETS
3	SCOPE OF WORK
4	PROPOSED SAFETY PLAN
5	CONSTRUCTION SAFETY DETAILS AND NOTES
6	PROPOSED TYPICAL CROSS SECTIONS
7	PROPOSED CONSTRUCTION PLAN
8	PROPOSED JOINTING PLAN
9	PROPOSED STAKING PLAN
10	PROPOSED JOINT DETAILS
11	PROPOSED MARKING PLAN



PHASE 1 -  
RECONSTRUCT MAIN  
PCC APRON

IDA No: SLO-4581  
SBG Project No:  
3-17-SBGP-120/133/139  
Contract No. SA031

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 2, 2018  
PROJECT NO: 17A0017D  
CAD FILE: G-002-FLP.DWG  
DESIGN BY: JRH 02/02/2018  
DRAWN BY: JRH 02/02/2018  
REVIEWED BY: BSS 03/02/2018

SHEET TITLE

SUMMARY OF  
QUANTITIES AND  
INDEX TO SHEETS

**GENERAL NOTES:**

QUANTITIES

PAYMENT WILL BE MADE UNDER THE ITEM NUMBERS, DESCRIPTIONS AND UNITS NOTED IN THE ABOVE TABLE IN ACCORDANCE WITH THE BASIS OF PAYMENT FOR EACH RESPECTIVE WORK ITEM COMPLETED AND ACCEPTED BY THE ENGINEER.

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER/TECHNICIAN CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATIONS

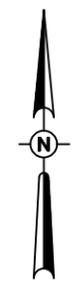
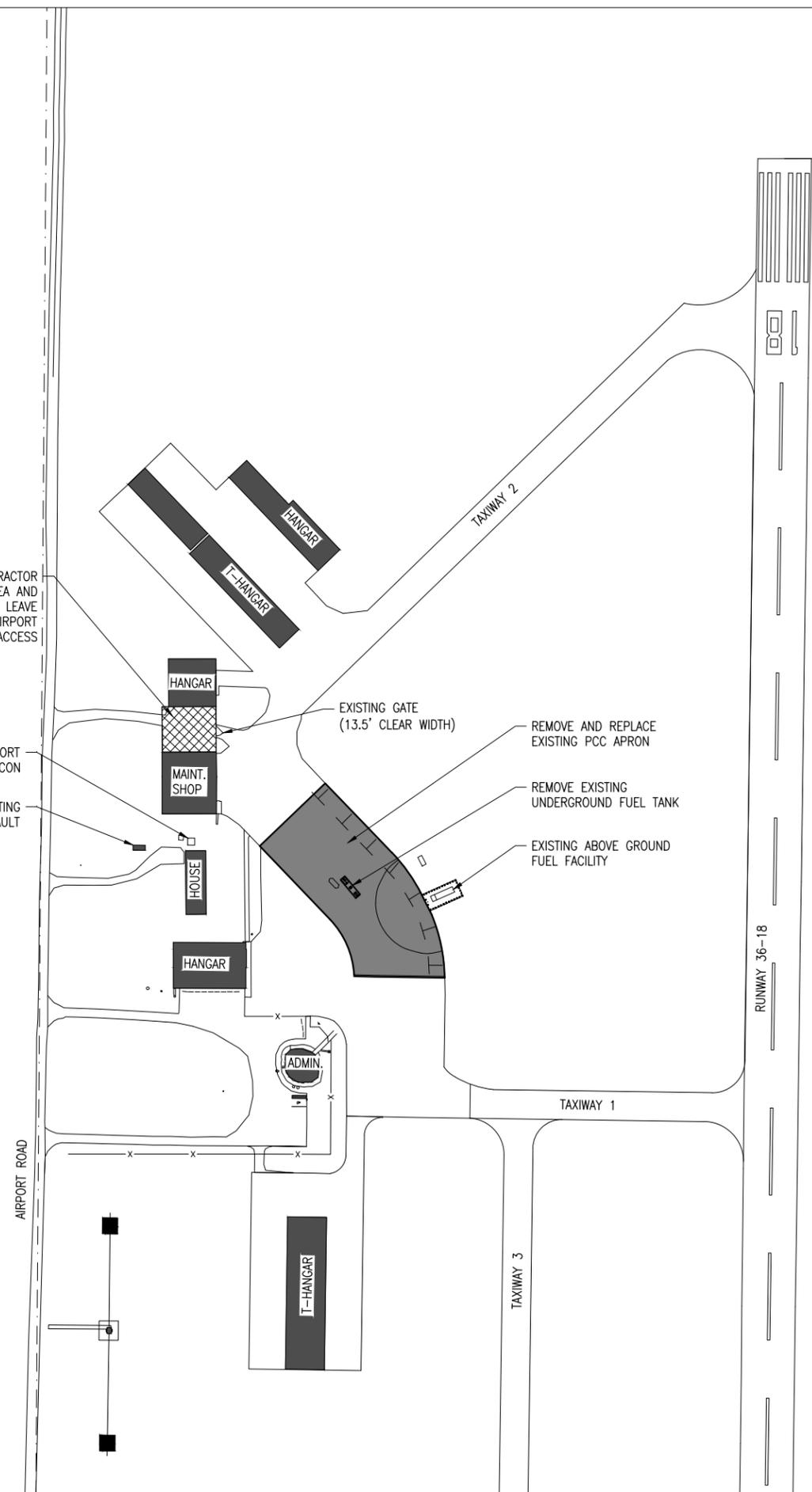
MATERIALS TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL. ALL MATERIALS TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. USE OF MATERIALS WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.

**GENERAL NOTES**

1. THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS AND THEIR APPROXIMATE LOCATIONS AND LIMITS, FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. THIS SHEET SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE FOLLOWING PLAN SHEETS FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
2. WORK FOR THIS PROJECT CONSIST OF RECONSTRUCTION OF A PORTION OF THE MAIN PCC AIRCRAFT APRON, INCLUDING REMOVAL OF AN ABANDONED UNDERGROUND FUEL STORAGE TANK. THIS PROJECT INCLUDES PAVEMENT REMOVAL, TANK REMOVAL, CONSTRUCTION OF NEW PCC PAVEMENT, PAVEMENT MARKING AND ASSOCIATED ITEMS.
3. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
4. THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN.
5. THE CONTRACTOR IS NOT PERMITTED TO USE THE AIRPORT ENTRANCE DRIVE AND AUTO PARKING LOT ADJACENT TO THE ADMINISTRATION BUILDING FOR MATERIAL AND EQUIPMENT HAULING OR STORAGE.
6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT.
7. NO EQUIPMENT SHALL BE PERMITTED TO CROSS OR USE ANY EXISTING AIRPORT PAVEMENT OUTSIDE THE CONSTRUCTION LIMITS, GENERAL PROJECT AREA OR HAUL ROUTE.
8. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
9. UNLESS OTHERWISE NOTED, ALL DISTURBED AREAS OUTSIDE OF THE PROPOSED CONSTRUCTION LIMITS SHALL BE GRADED, SEEDING AND/OR HYDROMULCH SEEDING IN ACCORDANCE WITH ITEM 901 AT NO ADDITIONAL COST TO THE CONTRACT.
10. ALL WASTE MATERIAL SHALL BE HAULED FROM THE AIRPORT AND PROPERLY DISPOSED OF UNLESS OTHERWISE SPECIFIED HEREIN.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
12. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
13. THE CONTRACTOR SHALL COORDINATE WITH THE RESIDENT ENGINEER/TECHNICIAN SO THEY MAY DEVELOP ONE SET OF REDLINED AS-BUILT RECORD DRAWINGS AT THE COMPLETION OF THE PROJECT.
14. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO BEGINNING ANY WORK.
15. CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS TO BE REPAIRED, GRADED, AND MULCHED SEEDING IN ACCORDANCE WITH ITEMS 901 AND 908, UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF THE HAUL ROUTE.
16. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER/TECHNICIAN IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS.
17. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER PERSONS FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
18. THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE FOR ALL AREAS AFFECTED BY HIS WORK.
19. NO CONSTRUCTION EQUIPMENT GREATER THAN 25' TALL WILL BE PERMITTED ON THE PROJECT. HOWEVER OTHER EQUIPMENT TALLER THAN 25' MAY BE PERMITTED WITH THE APPROVAL OF THE AIRPORT MANAGER AND AIRSPACE APPROVAL BY THE FAA.

PROPOSED CONTRACTOR ACCESS, STAGING AREA AND EQUIPMENT PARKING. LEAVE OPENING FOR AIRPORT VEHICLE ACCESS

EXISTING AIRPORT ROTATING BEACON  
EXISTING ELECTRICAL VAULT



0' 100' 200' 400'  
HALF SIZE SCALE: 1" = 400'  
FULL SIZE SCALE: 1" = 200'

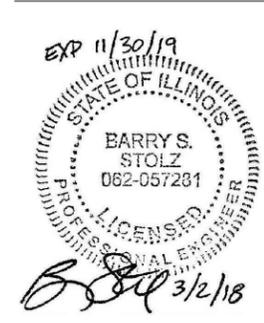
**LEGEND**

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED IMPROVEMENTS
- PROPOSED CONTRACTOR STAGING AREA
- X  EXISTING FENCE
- AIRPORT PROPERTY LINE



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Illinois Licensed  
Professional Service Corporation  
#184-001084

**Salem-Leckrone Airport**  
SALEM AIRPORT AUTHORITY  
1100 AIRPORT RD.  
SALEM, IL 62881



**PHASE 1 - RECONSTRUCT MAIN PCC APRON**

IDA No: SLO-4581  
SBG Project No: 3-17-SBGP-120/133/139  
Contract No. SA031


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 2, 2018  
PROJECT NO: 17A0017D  
CAD FILE: G-003-SOW.DWG  
DESIGN BY: JRH 02/02/2018  
DRAWN BY: JRH 02/02/2018  
REVIEWED BY: BSS 03/02/2018

**SCOPE OF WORK**

MAR 02 2018 1:27 PM SCHUB01446 I:\17\JOBS\17A0017D\CAD\AIRPORT\ISHEET\G-003-SOW.DWG

**UTILITY NOTE**

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

**J.U.L.I.E. INFORMATION**

COUNTY            MARION  
 CITY            SALEM  
 TOWNSHIP        SALEM  
 SECTION NO.        T.2N. - R.2E.  
 ADDRESS            SALEM-LECKRONE AIRPORT  
                   1100 AIRPORT ROAD  
                   SALEM, IL 62881

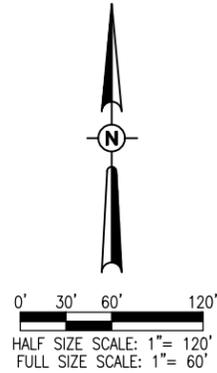
**SAFETY PLAN NOTES**

1. BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION.
2. THE CONTRACTOR MAY NOT WORK IN BOTH PHASES AT THE SAME TIME WITH THE EXCEPTION OF PAVEMENT MARKING WORK AND THE JOINT WIDENING & SEALING WORK. THE INITIAL JOINT SAW-CUT SHALL BE DONE SEPARATELY WITH EACH PHASE. THE PHASE 1 CONCRETE SHALL REACH SPECIFIED STRENGTH TO OPEN PAVEMENT TO AIRCRAFT TRAFFIC PRIOR TO BEGINNING PHASE 2 WORK. FOLLOWING THE FINAL CONCRETE POUR IN EACH PHASE, IF NO WORK ITEMS ARE IN PROGRESS DURING THE CONCRETE CURING PERIOD, THE CONTRACT TIME WILL BE TEMPORARILY STOPPED UNTIL THE CONCRETE REACHES SPECIFIED STRENGTH AND WORK RESUMES.
3. THE CONTRACTOR MAY PROPOSE ALTERNATE PHASE LIMITS FOR THE PROJECT, HOWEVER THE ABOVE GROUND FUEL FACILITY SHALL REMAIN ACCESSIBLE AT ALL TIMES DURING CONSTRUCTION.
4. THE CONTRACTOR SHALL BE AWARE THERE IS A SEPARATE PROJECT IN THE AREA OF THE AIRPORT PARKING LOT AND ENTRANCE ROADS THAT MAY BE OCCURRING AT THE SAME TIME AS THIS CONTRACT. COORDINATION BETWEEN THE CONTRACTS IS MANDATORY. NO TIME EXTENSIONS OR CHANGE ORDERS WILL BE PROCESSED DUE TO LACK OF COORDINATION BETWEEN CONTRACTS. ANY POTENTIAL DELAYS OR CONFLICTS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE RESIDENT ENGINEER/TECHNICIAN.
5. FLAGPERSONS AND/OR ESCORTS MAY BE NECESSARY TO ENSURE PROPER CONTROL OF CONSTRUCTION VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. PAVEMENTS ARE TO BE KEPT FREE OF DEBRIS AT ALL TIMES. COSTS TO MAINTAIN ACTIVE/CLEAN PAVEMENTS SHALL BE INCLUDED IN ITEM AR150540 HAUL ROUTE.
6. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE DISRUPTION TO AIRCRAFT OPERATIONS.
7. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROUTE AND CONSTRUCTION EQUIPMENT PARKING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS PER THE SPECIFICATIONS.
8. THE COSTS FOR PROVISION, PLACEMENT, MAINTENANCE AND REMOVAL OF BARRICADES ON THE AIRFIELD AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.
9. THE COSTS FOR CONSTRUCTION/MAINTENANCE OF HAUL ROUTE AND EQUIPMENT STAGING AREA, TEMPORARY SIGNAGE AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150540 HAUL ROUTE.

CRITICAL POINT DATA					
DESCRIPTION	LATITUDE	LONGITUDE	GROUND ELEV.	EQUIP. HEIGHT	EQUIP. ELEV.
CRITICAL POINT 1	N038°38'47.06"	W088°58'00.79"	569.0'	25'	594.0'
CRITICAL POINT 2	N038°38'46.02"	W088°57'58.88"	568.0'	25'	593.0'
CRITICAL POINT 3	N038°38'43.38"	W088°57'56.77"	569.0'	25'	594.0'
CRITICAL POINT 4	N038°38'44.45"	W088°57'57.14"	568.0'	25'	593.0'

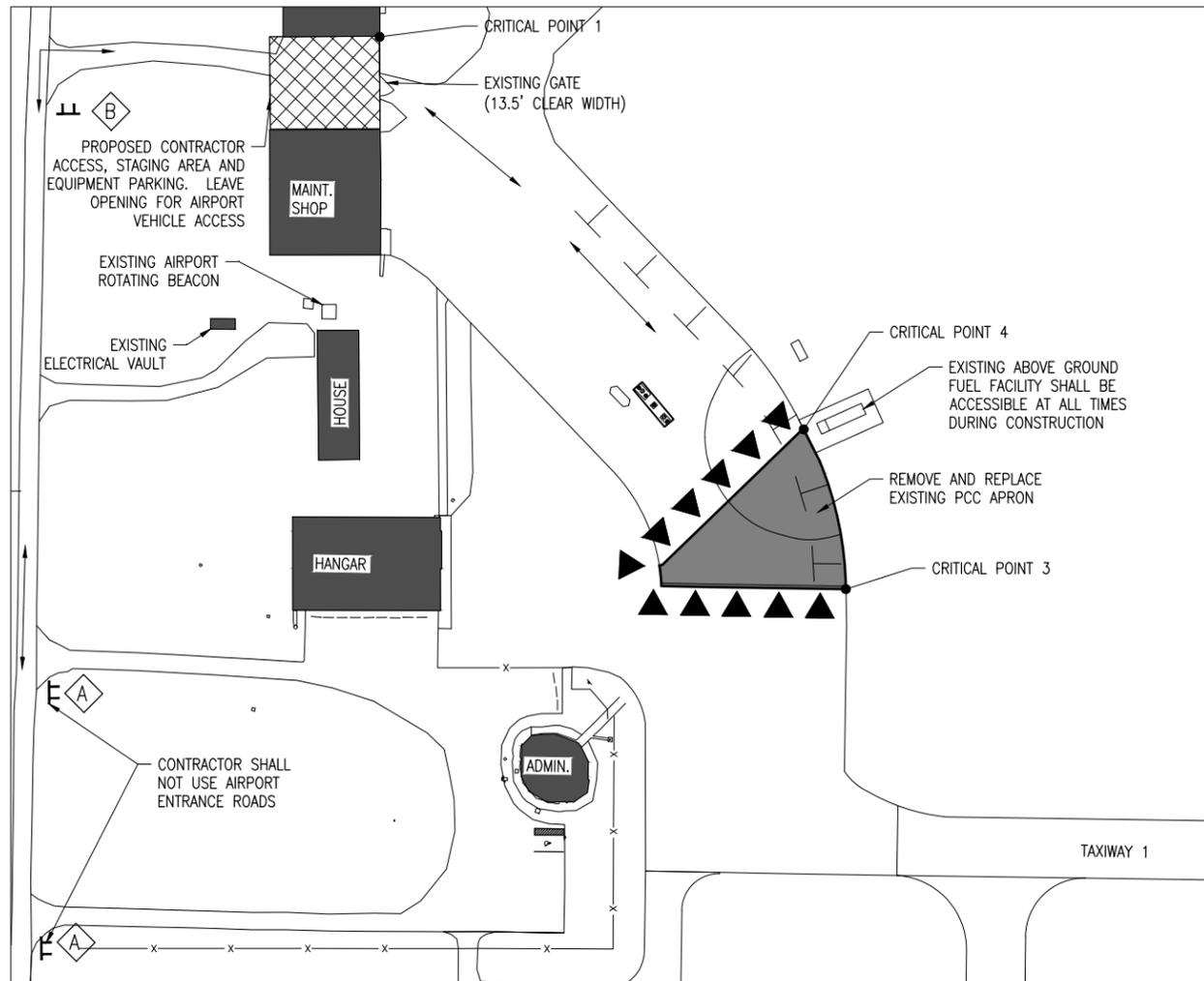
**SAFETY PLAN COMPLIANCE DOCUMENT**

PRIOR TO THE ISSUANCE OF A CONSTRUCTION NOTICE-TO-PROCEED (NTP), THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING AND SUBMITTING A SAFETY PLAN COMPLIANCE DOCUMENT IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G, PARAGRAPH 2.4.2, OR EQUIVALENT SECTION IN SUBSEQUENT/CURRENT ISSUE. THE AIRPORT DIRECTOR SHALL APPROVE THIS DOCUMENT AND SUBMIT TO THE DIVISION OF AERONAUTICS FOR APPROVAL PRIOR TO THE NTP ISSUANCE.

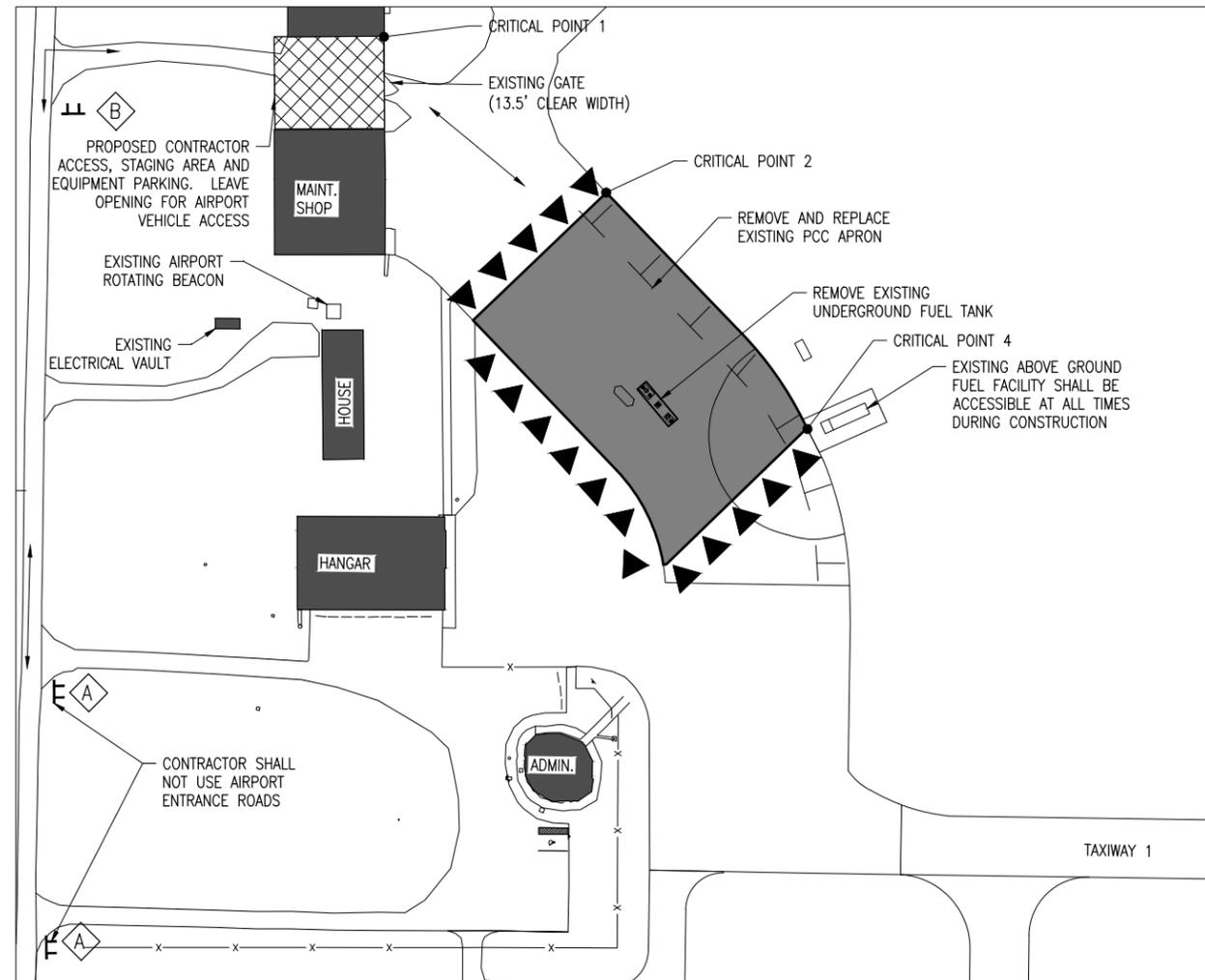


**LEGEND**

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED IMPROVEMENTS
- PROPOSED CONTRACTOR STAGING AREA
- EXISTING FENCE
- PROPOSED HAUL ROUTE
- PROPOSED BARRICADES
- CONSTRUCTION SIGN



**PHASE 1**



**PHASE 2**



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Illinois Licensed  
 Professional Service Corporation  
 #184-001084

**Salem-Leckrone Airport**

SALEM AIRPORT AUTHORITY  
 1100 AIRPORT RD.  
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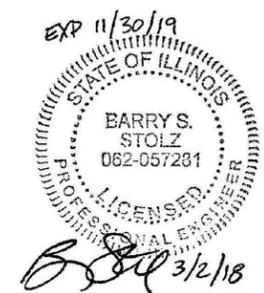
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 2, 2018  
 PROJECT NO: 17A0017D  
 CAD FILE: C-004-SFY.DWG  
 DESIGN BY: JRH 02/02/2018  
 DRAWN BY: JRH 02/02/2018  
 REVIEWED BY: BSS 03/02/2018

SHEET TITLE

**PROPOSED SAFETY PLAN**

MAR 02 2018 5:00 PM STOLZ01547 I:\17\JOBS\17A0017\17A0017D\CAD\AIRPORT\SHSEET1C-004-SFY.DWG



**PHASE 1 -  
RECONSTRUCT MAIN  
PCC APRON**

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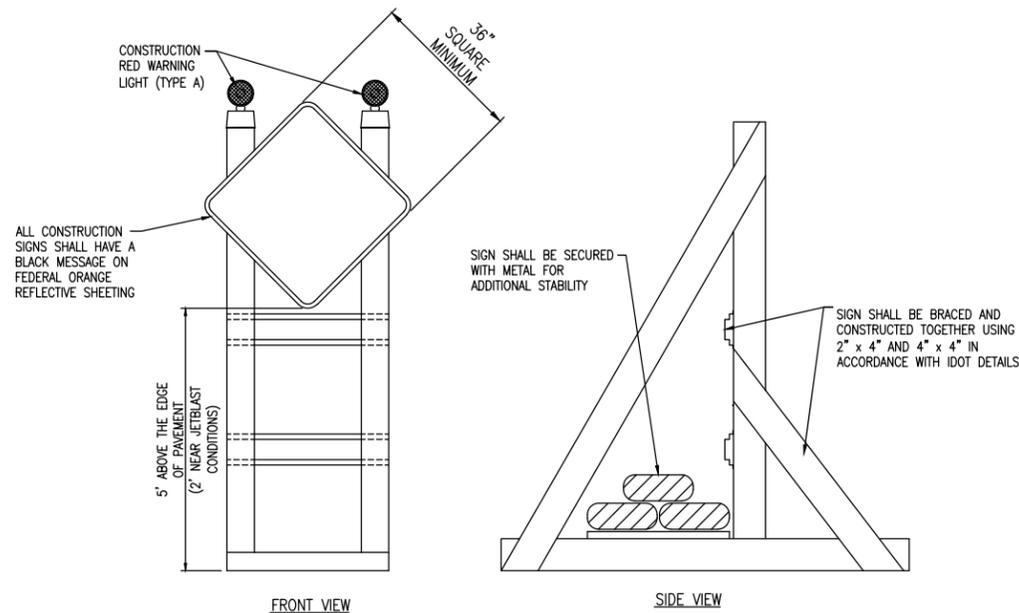
ISSUE: MARCH 2, 2018  
PROJECT NO: 17A0017D  
CAD FILE: C-501-SFY.DWG  
DESIGN BY: JRH 02/02/2018  
DRAWN BY: JRH 02/02/2018  
REVIEWED BY: BSS 03/02/2018

SHEET TITLE

**CONSTRUCTION  
SAFETY DETAILS  
AND NOTES**

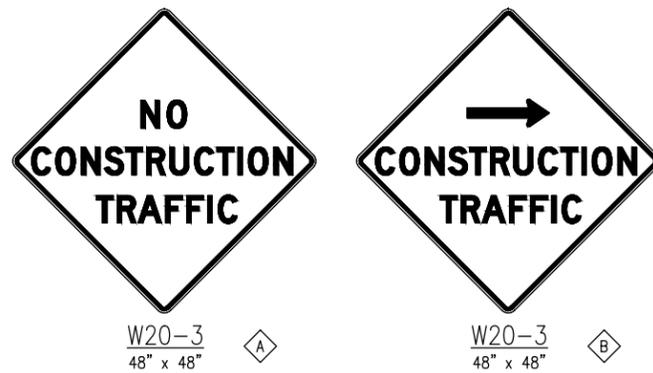
SAFETY NOTES

- FOLLOWING ARE THE CONSTRUCTION SAFETY PROCEDURES THAT THE CONTRACTOR SHALL FOLLOW THROUGHOUT THIS PROJECT. ADDITIONAL REQUIREMENTS ARE SHOWN ON THE SAFETY AND PHASING NOTES AND DETAILS SHEET.
- ALL PROVISIONS OF THE LATEST EDITION OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION", APPLY TO THIS CONTRACT, EXCEPT AS MODIFIED BY THIS SAFETY PLAN, OR AS MODIFIED BY THE OWNER THROUGH THE RESIDENT ENGINEER AT THE PRECONSTRUCTION CONFERENCE, OR DURING THE COURSE OF THE CONTRACT.
- THE CONTRACTORS SHALL MINIMIZE DISRUPTION OF STANDARD OPERATING PROCEDURES FOR AERONAUTICAL ACTIVITY BY REMAINING WITHIN THE PRESCRIBED STAGING, CONSTRUCTION, AND PHASING AREAS PRESENTED ON THE PROJECT SAFETY AND PHASING PLANS.
- NO UNAUTHORIZED PERSONNEL SHALL ENTER ANY AREA OF THE AIRPORT THAT COULD POTENTIALLY BE HAZARDOUS. THE AIRPORT MANAGER RESERVES THE RIGHT TO SUSPEND OPERATIONS IN ORDER TO MAINTAIN SAFETY AT THE AIRPORT.
- CONTRACTOR EQUIPMENT, VEHICLES, AND PROJECT MATERIALS SHALL BE STORED AT THE STAGING AREA SHOWN ON THE PLAN VIEW, EXCEPT AS OTHERWISE PROVIDED FOR AT THE PRECONSTRUCTION CONFERENCE.
- ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" LATEST EDITION.
- NO CONSTRUCTION MATERIAL STOCKPILES SHALL BE LOCATED WITHIN 250' OF ANY ACTIVE RUNWAY, WITHIN 93' OF ANY OTHER ACTIVE AIRPORT OPERATIONS AREA, OR PENETRATE A PART 77 IMAGINARY SURFACE (PROVIDED BY THE RESIDENT ENGINEER) EXTENDING OUT AND UPWARDS FROM ALL SIDES OF AN ACTIVE RUNWAY.
- NO OPEN TRENCHES WITHIN 250' OF AN ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY AIRPORT OPERATIONS AREA WILL BE PERMITTED UNLESS PROPERLY MARKED. OTHER TRENCHES SHALL BE MAINTAINED SAFE, I.E., BARRICADED OR COVERED WITH STEEL PLATES IN ALL OTHER AREAS.
- OPEN TRENCHES, EXCAVATIONS, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHOULD BE PROMINENTLY MARKED WITH ORANGE FLAGS AND LIGHTED WITH FLASHING YELLOW LIGHTS DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS.
- THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET, WHICH IS EXPECTED TO BE A CONCRETE TRUCK, DUMP TRUCK, OR BACKHOE.
- NO OPEN FLAME WELDING OR TORCH CUTTING OPERATION IS PERMITTED UNLESS ADEQUATE FIRE AND SAFETY PRECAUTIONS ARE PROVIDED AND HAVE BEEN APPROVED BY THE AIRPORT MANAGER NO FLARE POTS ARE ALLOWED ON THE PROJECT.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEEPED, PICKED UP AND REMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND RESIDENT ENGINEER. HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS. STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- CONTRACTOR SHALL PLACE, SECURE, AND MAINTAIN LIGHTED BARRICADES AND CLOSURE CROSSES WHEN A RUNWAY/TAXIWAY/APRON IS CLOSED OR AS REQUIRED BY THE PLANS AND DESIGNATED BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MARK HAZARDOUS AREA WITH STEADY-BURNING OR FLASHING RED LIGHTS DURING PERIODS OF LOW VISIBILITY AS REQUIRED.
- THE CONTRACTOR SHALL PERIODICALLY PERFORM ONSITE INSPECTIONS THROUGHOUT THE DURATION OF THE PROJECT WITH THE IMMEDIATE REMEDY OF ANY DIFFERENCES, WHETHER CAUSED BY NEGLIGENCE, OVERSIGHT, OR PROJECT SCOPE CHANGE.
- CONTRACTOR SHALL MOVE MAINTENANCE OF TRAFFIC COMPONENTS AT THE WRITTEN DIRECTION OF THE RESIDENT ENGINEER/TECHNICIAN AT NO ADDITIONAL COST.
- CONTRACTOR SHALL NOT REMOVE THE BARRICADES WITHOUT THE APPROVAL BY THE RESIDENT ENGINEER/TECHNICIAN.
- CONTRACTOR SHALL MAINTAIN FLASHERS, SIGNS AND/OR BARRICADES AS REQUIRED BY THE PLANS, CITY OR COUNTY REGULATIONS OR CONTRACTOR ACTIVITIES. CONTRACTOR SHALL OBTAIN ANY AND ALL REQUIRED LOCAL PERMITS UNLESS SPECIFIED OTHERWISE.
- THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
- NO CONSTRUCTION VEHICLES SHALL BE DRIVEN ACROSS ANY ACTIVE RUNWAY, INCLUDING TURF RUNWAYS. CONSTRUCTION EQUIPMENT OR CONSTRUCTION ACTIVITY WILL NOT BE PERMITTED WITHIN 250' OF ANY ACTIVE RUNWAY CENTERLINE OR WITHIN 93' OF ANY OTHER ACTIVE AIRPORT TAXIWAY OR APRON. HOWEVER, CONSTRUCTION MAY BE PERMITTED IN THESE AREAS IF THE CONTRACTOR HAS GAINED APPROVAL FROM THE AIRPORT MANAGER AT LEAST 7 DAYS IN ADVANCE OF THE SCHEDULED CONSTRUCTION PERIOD AND THE OPERATIONAL AREA IS CLOSED TO TRAFFIC AND PROPER NOTAMS ARE ISSUED BY THE AIRPORT MANAGER TO THE APPROPRIATE FLIGHT SERVICE STATION.
- UNLESS SPECIFIED OTHERWISE, COST FOR THE ABOVE IS TO BE CONSIDERED INCIDENTAL TO THE PROJECT. SEPARATE PAYMENT SHALL NOT BE MADE.



SIGNAGE NOTES

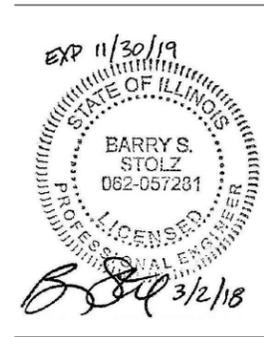
- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- UNLESS OTHERWISE SPECIFIED, CONSTRUCTION SIGNS SHALL BE MOUNTED ON PORTABLE OR NON-PORTABLE SUPPORTS. A PORTABLE SUPPORT IS DEFINED AS A TYPICAL SIGN STANDARD AS SHOWN ON THIS SHEET, OR A SMALL LIGHT WEIGHT TRAILER. A NON-PORTABLE SUPPORT IS DEFINED AS DRIVEN METAL OR WOOD POST. ALL SIGNS, REGARDLESS OF THE TYPE OF SUPPORTS USED, SHALL BE MOUNTED SUCH THAT THE MESSAGE ON THE SIGN IS LEVEL IN THE HORIZONTAL PLANE AFTER PLACEMENT. THE COST OF CONSTRUCTION WARNING LIGHTS SHALL BE INCLUDED IN THE COST OF THE CONSTRUCTION SIGNS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY ARE TO BE USED IN A LOW INTENSITY FLASHING MODE (TYPE A).
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING SIGNS SHALL BE INCLUDED IN ITEM AR150540 HAUL ROUTE.



CONSTRUCTION SIGNS  
NOT TO SCALE

BARRICADE NOTES

- CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND STOCKPILED MATERIALS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION, LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY LIGHTS AND FLAGS) ON THE TAXIWAYS AND COMPLY WITH ADVISORY CIRCULAR 150/5370-2, LATEST EDITION. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION.
- BARRICADES SHALL BE SPACED END TO END THE WIDTH OF THE PAVEMENT IN 4' INCREMENTS. BARRICADES ARE TO BE SET BACK 66' FROM THE ACTIVE TAXIWAY CENTERLINE OR AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR BEFORE SUNSET AND 1/2 HOUR AFTER SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, AND REMOVING BARRICADES SHALL BE INCLUDED IN ITEM AR150520 MOBILIZATION.



**PHASE 1 -  
RECONSTRUCT MAIN  
PCC APRON**

IDA No: SLO-4581

SBG Project No:  
3-17-SBGP-120/133/139

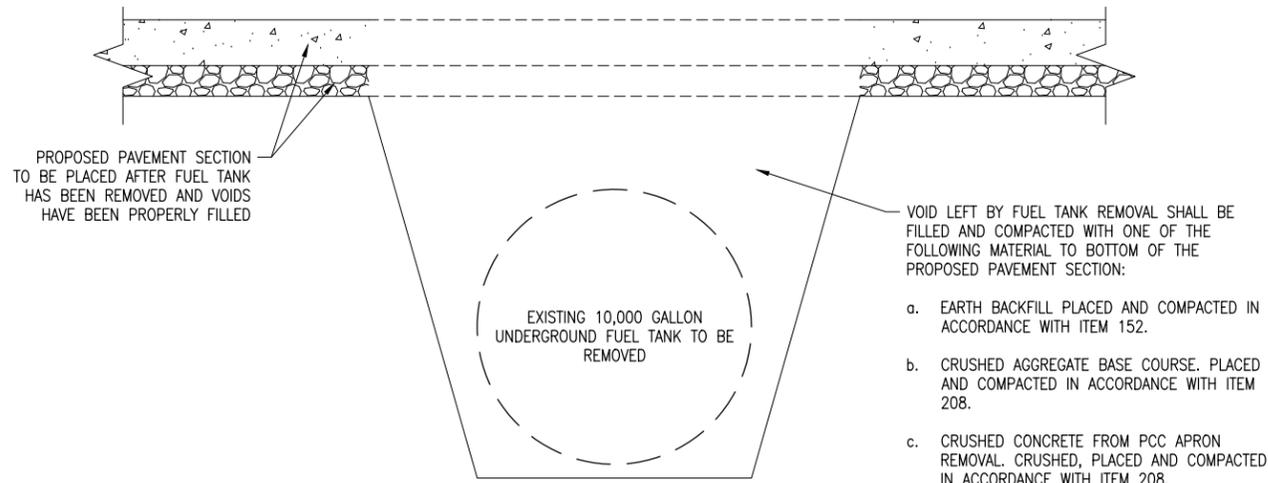
Contract No. SA031

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 2, 2018  
PROJECT NO: 17A0017D  
CAD FILE: C-302-TYP.DWG  
DESIGN BY: JRH 02/02/2018  
DRAWN BY: JRH 02/02/2018  
REVIEWED BY: BSS 03/02/2018

SHEET TITLE

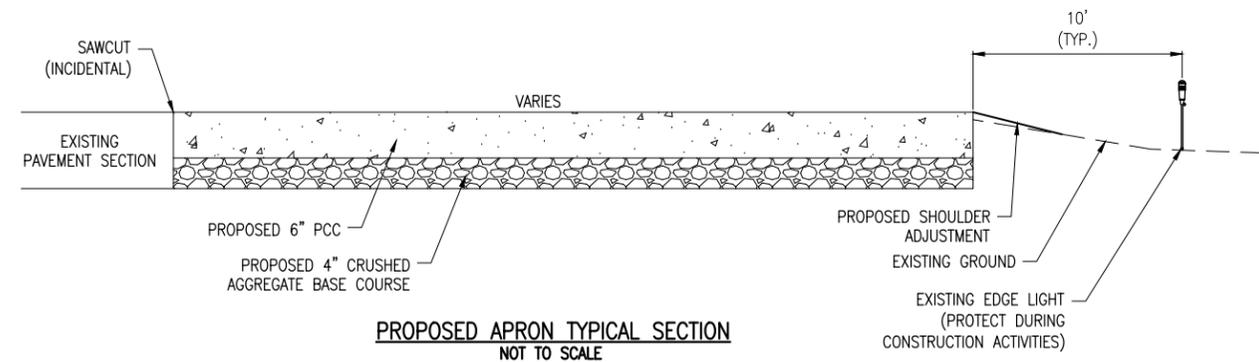
**PROPOSED TYPICAL  
CROSS SECTIONS**

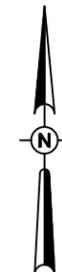


**PROPOSED UNDERGROUND FUEL TANK REMOVAL**  
NOT TO SCALE

**REMOVE UNDERGROUND FUEL STORAGE TANK NOTES**

- THE EXISTING 10,000 GALLON UNDERGROUND FUEL STORAGE TANK SHALL BE REMOVED AND DISPOSED OF OFF THE AIRPORT SITE, IN A LEGAL MANNER. IF THE UNDERGROUND FUEL TANK RESTS ON A CONCRETE PAD, THEN THAT CONCRETE PAD WILL BE LEFT IN PLACE. THE UNDERGROUND TANK SHALL BE REMOVED AND DISPOSED OF IN ACCORDANCE WITH ALL CURRENT LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS AND CODES, INCLUDING BUT NOT LIMITED TO: SECTION 175.830, REMOVAL OF USTs, OF THE ILLINOIS ADMINISTRATIVE CODE, TITLE 41, CHAPTER I, PART 175.
- IF NECESSARY, THE CONTRACTOR WILL SAW THE ADJACENT BITUMINOUS OR CONCRETE PAVEMENT TO INSURE PROPER SEPARATION OF THE CONCRETE PAVEMENT (TO BE REMOVED) FROM THE ADJACENT PAVEMENT. ANY DISPLACEMENT OF THE ADJACENT PAVEMENT WILL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN ALL PERTINENT PERMITS REQUIRED FOR THE REMOVAL, HAULING AND DISPOSAL OF THE STORAGE TANK.
- THE STATE FIRE MARSHAL'S OFFICE WILL BE CONTACTED PRIOR TO REMOVAL OF THE TANK, AND THE TANK SHALL NOT BE REMOVED FROM THE SITE WITHOUT PRIOR APPROVAL FROM THE STATE FIRE MARSHAL'S OFFICE.
- THE CONTRACTOR SHALL FURNISH MATERIAL TO FILL IN THE VOID LEFT FROM THE REMOVAL OF THE UNDERGROUND FUEL STORAGE TANK. THE MATERIAL PLACED IN THE HOLE WILL BE PLACED IN MAXIMUM 12" LIFTS AND EACH LIFT WILL BE COMPACTED TO PREVENT FUTURE SETTLEMENT.
- THE REMOVAL AND DISPOSAL OF THE UNDERGROUND TANK, ASSOCIATED EQUIPMENT, PIPING AND ELECTRICAL; BACKFILLING OF THE VOIDS MADE FROM THE REMOVAL WILL BE PAID FOR UNDER:  
ITEM AR800554, REMOVE UNDERGROUND FUEL TANK, PER LUMP SUM.
- CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS.
- ABANDONED CONDUIT FOR THE PREVIOUSLY REMOVED FUEL SYSTEM EQUIPMENT AND/OR OTHER EQUIPMENT ASSOCIATED WITH THE FUEL STORAGE TANK SHALL BE REMOVED.
- CONTRACTOR SHALL COORDINATE WORK AND ANY POWER OUTAGES WITH THE AIRPORT MANAGER AND/OR THE AIRPORT REPRESENTATIVE. ANY SHUTDOWN OF EXISTING SYSTEMS SHALL BE SCHEDULED WITH AND APPROVED BY THE AIRPORT MANAGER AND/OR THE AIRPORT REPRESENTATIVE PRIOR TO SHUTDOWN. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY AND HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES, INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E - STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.
- THE AIRPORT DID MAINTAIN A MONITORING SYSTEM FOR THE UNDERGROUND FUEL TANK, AND THERE HAVE BEEN NO PRIOR INDICATIONS OF ANY LEAKS OR RELATED ISSUES WITH THE TANK. NO PRELIMINARY SOIL TESTING HAS BEEN PERFORMED IN THIS AREA. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY SOIL TESTING AND ANALYSIS. A PAY ITEM FOR CONTAMINATED SOIL REMOVAL HAS BEEN INCLUDED WITH A SMALL ESTIMATED QUANTITY IN THE EVENT SUCH CONDITIONS ARE ENCOUNTERED. ANY CONTAMINATED SOIL REMOVED FROM THE PROJECT SITE MAY BE PLACED AT A LOCATION ON AIRPORT PROPERTY TO BE DETERMINED AS NEEDED.





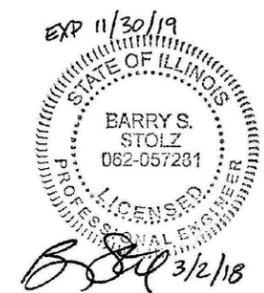
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FULL SIZE SCALE: 1"= 20'

**LEGEND**

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED IMPROVEMENTS
- EXISTING FENCE
- PROPOSED SAWCUT
- PROPOSED BUTT JOINT
- PROPOSED UNDERGROUND FUEL TANK REMOVAL
- PROPOSED SHOULDER ADJUSTMENT

**NOTES**

1. VOID LEFT BY TANK REMOVAL SHALL BE FILLED TO THE BOTTOM OF THE PROPOSED PAVEMENT SECTION IN ACCORDANCE WITH THE TYPICAL SECTIONS SHEET.
2. DURING THE PCC PAVEMENT RECONSTRUCTION, THE CONTRACTOR SHALL REMOVE TO A SUFFICIENT DEPTH IN ORDER TO ACCOMMODATE THE CONSTRUCTION OF THE PROPOSED PCC PAVEMENT TO PROPOSED GRADES. IF FOR ANY REASON THE CONTRACTOR REMOVES A GREATER DEPTH, NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR EXTRA MATERIAL.
3. EXISTING TIE-DOWN ANCHORS (20) ARE TO BE REMOVED AND PROPERLY DISPOSED OF AS INCIDENTAL TO THE EXISTING PCC PAVEMENT REMOVAL.
4. THE CONTRACTOR SHALL PROTECT EXISTING PAVEMENT, FUEL DEPOT, HANGARS AND BUILDINGS AT ALL TIMES DURING CONSTRUCTION.
5. EXISTING PCC PAVEMENT SECTION IS APPROX. 6" PCC ON 4" CRUSHED AGGREGATE.
6. DURING THE BITUMINOUS SURFACE REMOVAL AND REPLACEMENT WORK, THE CONTRACTOR SHALL PROTECT THE PROPOSED PCC PAVEMENT AREA AND EXISTING PAVEMENT TO REMAIN. THE INTENT IS TO REMOVE A SUFFICIENT DEPTH OF PAVEMENT IN ORDER TO PAVE A 2" DEPTH OF BITUMINOUS SURFACE TO PROPOSED GRADES. IF FOR ANY REASON THE CONTRACTOR REMOVES A GREATER DEPTH, NO ADDITIONAL COMPENSATION WILL BE ALLOWED TO PAVE BACK TO THE PROPOSED GRADES.



**PHASE 1 - RECONSTRUCT MAIN PCC APRON**

IDA No: SLO-4581  
SBG Project No: 3-17-SBGP-120/133/139  
Contract No. SA031


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

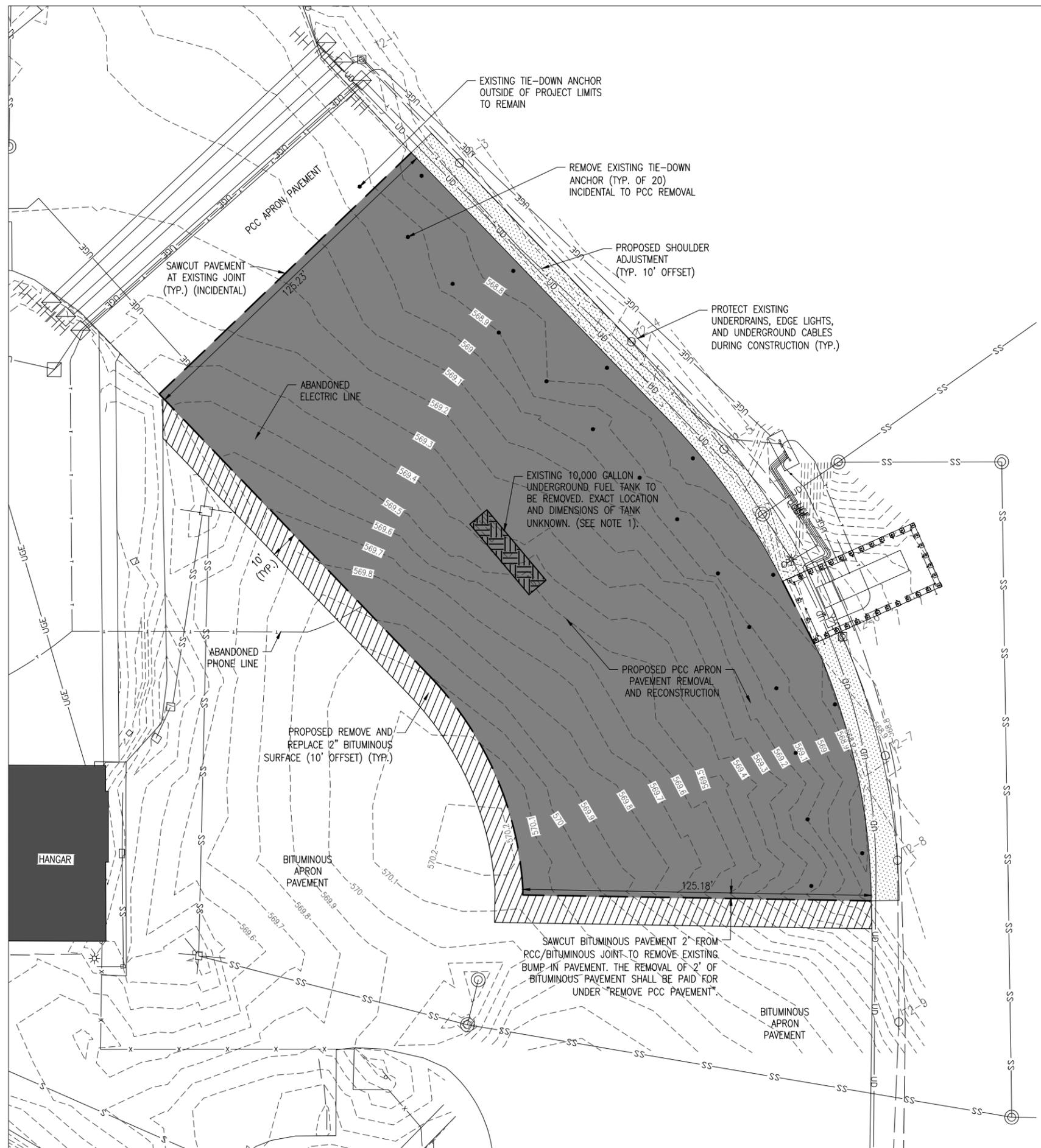
ISSUE: MARCH 2, 2018

PROJECT NO: 17A0017D  
CAD FILE: C-141-CON.DWG  
DESIGN BY: JRH 02/02/2018  
DRAWN BY: JRH 02/02/2018  
REVIEWED BY: BSS 03/02/2018

SHEET TITLE

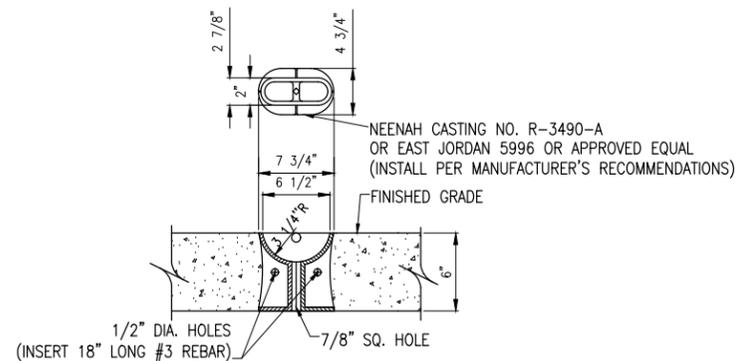
**PROPOSED CONSTRUCTION PLAN**

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FULL SIZE SCALE: 1" = 20'

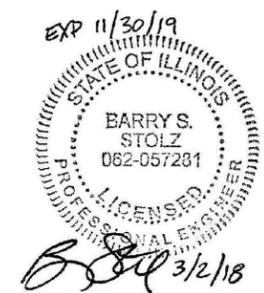


**TIE-DOWN ANCHOR IN CONCRETE APRON DETAIL**  
NOT TO SCALE



PROPOSED TIE DOWN ANCHORS (TYP. OF 15 TOTAL) SEE PROPOSED MARKING PLAN SHEET FOR DIMENSIONS/LAYOUT

- LEGEND**
- EXISTING IMPROVEMENTS
  - PROPOSED IMPROVEMENTS
  - EXISTING FENCE
  - TYPE A-MODIFIED, ISOLATION JOINT
  - TYPE B, CONTRACTION JOINT
  - TYPE C, CONTRACTION JOINT
  - TYPE D, CONTRACTION JOINT
  - TYPE E, CONTRACTION JOINT
  - PROPOSED TIE DOWN ANCHOR
  - PROPOSED REINFORCED PANEL



**PHASE 1 - RECONSTRUCT MAIN PCC APRON**

IDA No: SLO-4581  
SBG Project No: 3-17-SBGP-120/133/139  
Contract No. SA031

**JOINTING NOTES**

1. ALL JOINT EDGES SHALL BE SAWCUT TO PRODUCE THE 1/4" CHAMFER.
2. ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED.
3. ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL ENSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO INSTALLATION.
4. DOWEL BARS FOR 6 IN. THICK PAVEMENT SHALL BE 3/4 IN. DIAMETER, 18 IN. LONG AT 12 IN. SPACING.
5. ALL TIE BARS SHALL BE HELD IN PLACE BY SUPPORT PINS OR OTHER METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.
6. ALL TIE BARS SHALL BE PLACED AT A POINT NOT LESS THAN 6" OR MORE THAN 15" FROM A TRANSVERSE OR EXPANSION JOINT.
7. DOWELS IN TRANSVERSE CONTRACTION AND LONGITUDINAL CONSTRUCTION JOINTS SHALL BE COATED WITH A RUSTPROOFING COMPOUND AND HALF THE LENGTH GREASED WITH A HEAVY GREASE.
8. ALLOWABLE TOLERANCES FOR GROOVE DEPTH WILL BE ±1/8" FOR CONSTRUCTION JOINTS AND ±1/4" FOR CONTRACTION JOINTS.
9. THE CONTRACTOR IS REQUIRED TO DRILL AND EPOXY THE PROPOSED DOWELS IN ACCORDANCE WITH THE DETAILS AND SPECIFICATIONS. THE EPOXY MATERIAL MUST BE APPROVED BY THE DIVISION OF AERONAUTICS PRIOR TO USE.
10. THE COST OF ALL DOWEL BARS, TIE-BARS, SAWING AND SEALING SHALL BE INCLUDED IN THE COST OF THE PCC PAVEMENT.
11. WHEN CONSTRUCTING "FILL-IN" PAVEMENT LANES THE CONTRACTOR SHALL USE BELTING OR OTHER PROTECTIVE MATERIAL FOR THE PAVING MACHINE TO TRAVEL ON AND WILL PROTECT THE TRANSVERSE JOINTS.
12. JOINT SEALANT SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS, ITEM 605.
13. CURING COMPOUND SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS, ITEM 501, SECTION 501-2.9, AND SHALL BE APPROVED PRIOR TO THE PAVING OPERATION BY THE RESIDENT ENGINEER/TECHNICIAN.
14. ALL NON-ALIGNED EDGES WILL BE SAWED FULL DEPTH.


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 2, 2018

PROJECT NO: 17A0017D  
CAD FILE: C-161-JNT.DWG

DESIGN BY: JRH 02/02/2018

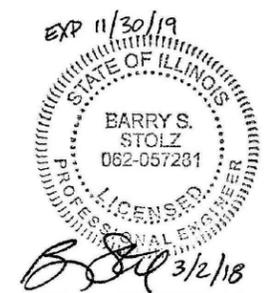
DRAWN BY: JRH 02/02/2018

REVIEWED BY: BSS 03/02/2018

SHEET TITLE

**PROPOSED JOINTING PLAN**





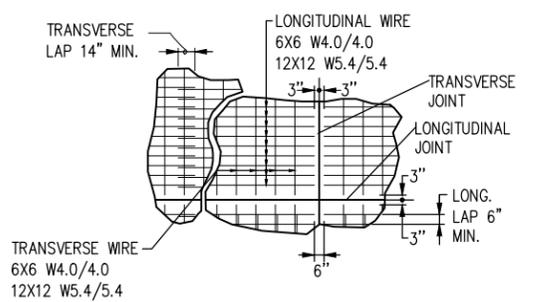
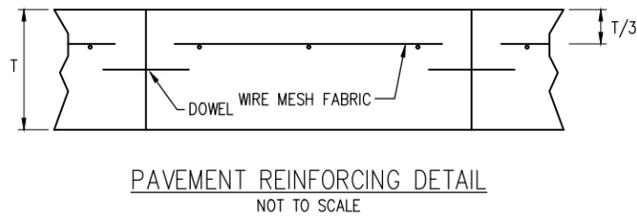
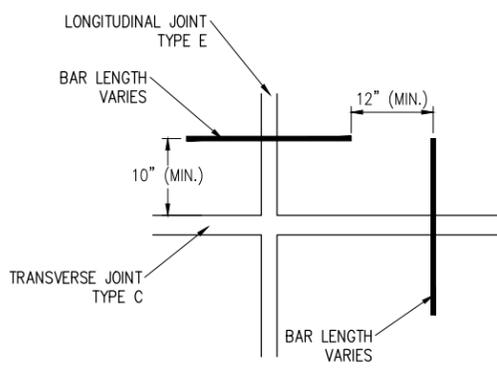
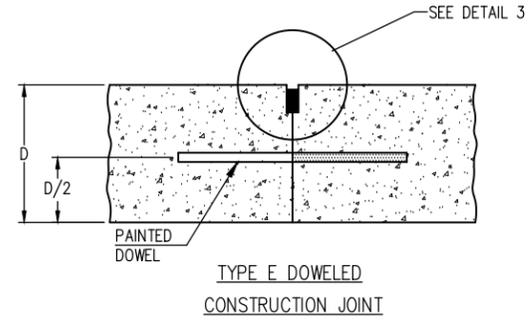
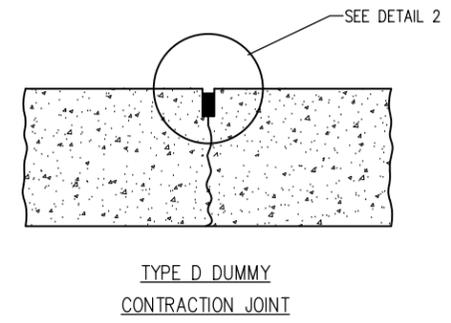
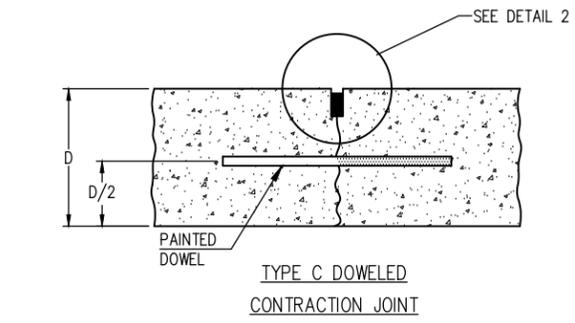
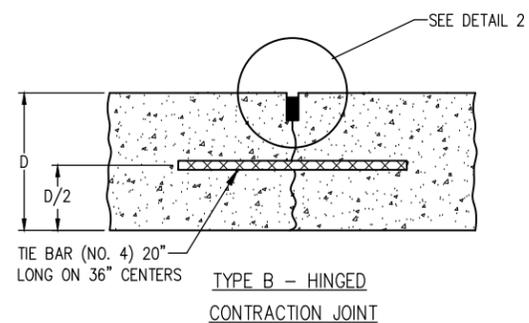
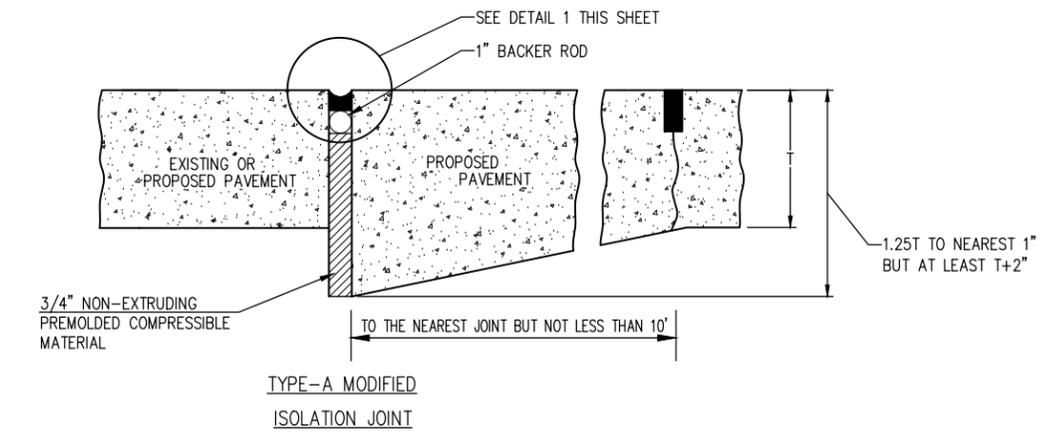
PHASE 1 -  
RECONSTRUCT MAIN  
PCC APRON

IDA No: SLO-4581  
SBG Project No:  
3-17-SBGP-120/133/139  
Contract No. SA031

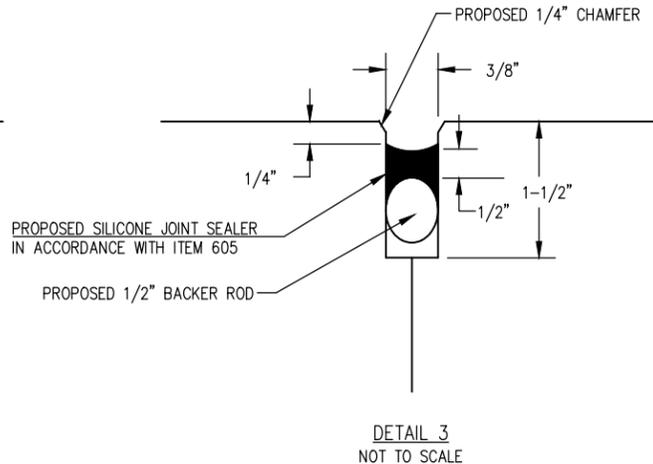
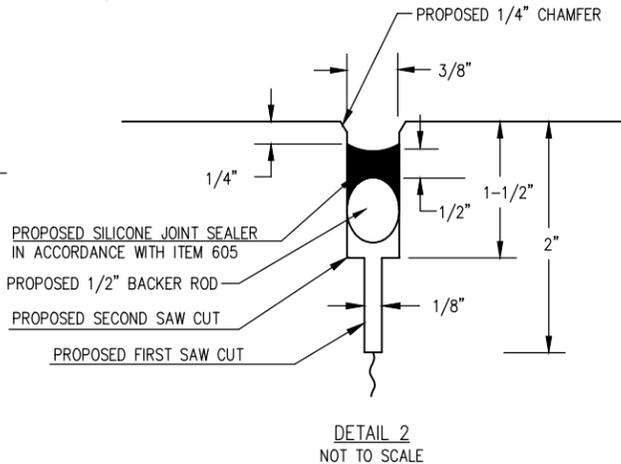
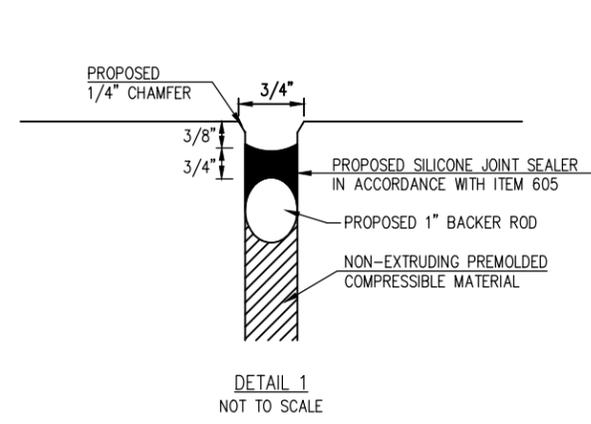

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 2, 2018  
PROJECT NO: 17A0017D  
CAD FILE: C-561-JNT.DWG  
DESIGN BY: JRH 02/02/2018  
DRAWN BY: JRH 02/02/2018  
REVIEWED BY: BSS 03/02/2018

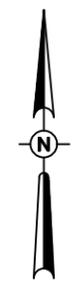
PROPOSED JOINT  
DETAILS



- REINFORCING NOTES:**
1. END LAPS SHALL BE A MINIMUM OF 12", BUT NOT LESS THAN 30 TIMES THE DIAMETER OF THE LONGITUDINAL WIRE OR BAR.
  2. SIDE LAPS SHALL BE A MINIMUM OF 6", BUT NOT LESS THAN 20 TIMES THE DIAMETER OF TRANSVERSE WIRE OR BAR.
  3. END AND SIDE CLEARANCES SHALL BE A MAXIMUM OF 6" AND A MINIMUM OF 2".
  4. LONGITUDINAL MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 12" APART.
  5. TRANSVERSE MEMBERS SHALL BE SPACED NOT LESS THAN 4" NOR MORE THAN 24" APART.
  6. REINFORCING SHALL CONSIST OF WELDED STEEL WIRE FABRIC CONFORMING TO THE REQUIREMENTS OF ASTM A 185.
  7. PAVEMENT REINFORCING SHALL BE INCIDENTAL TO ITEM 501 PCC PAVEMENT.

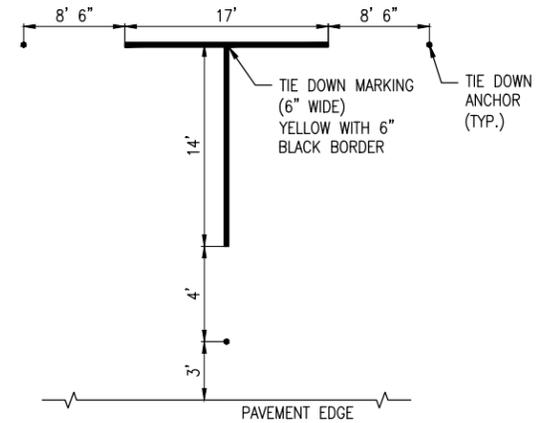
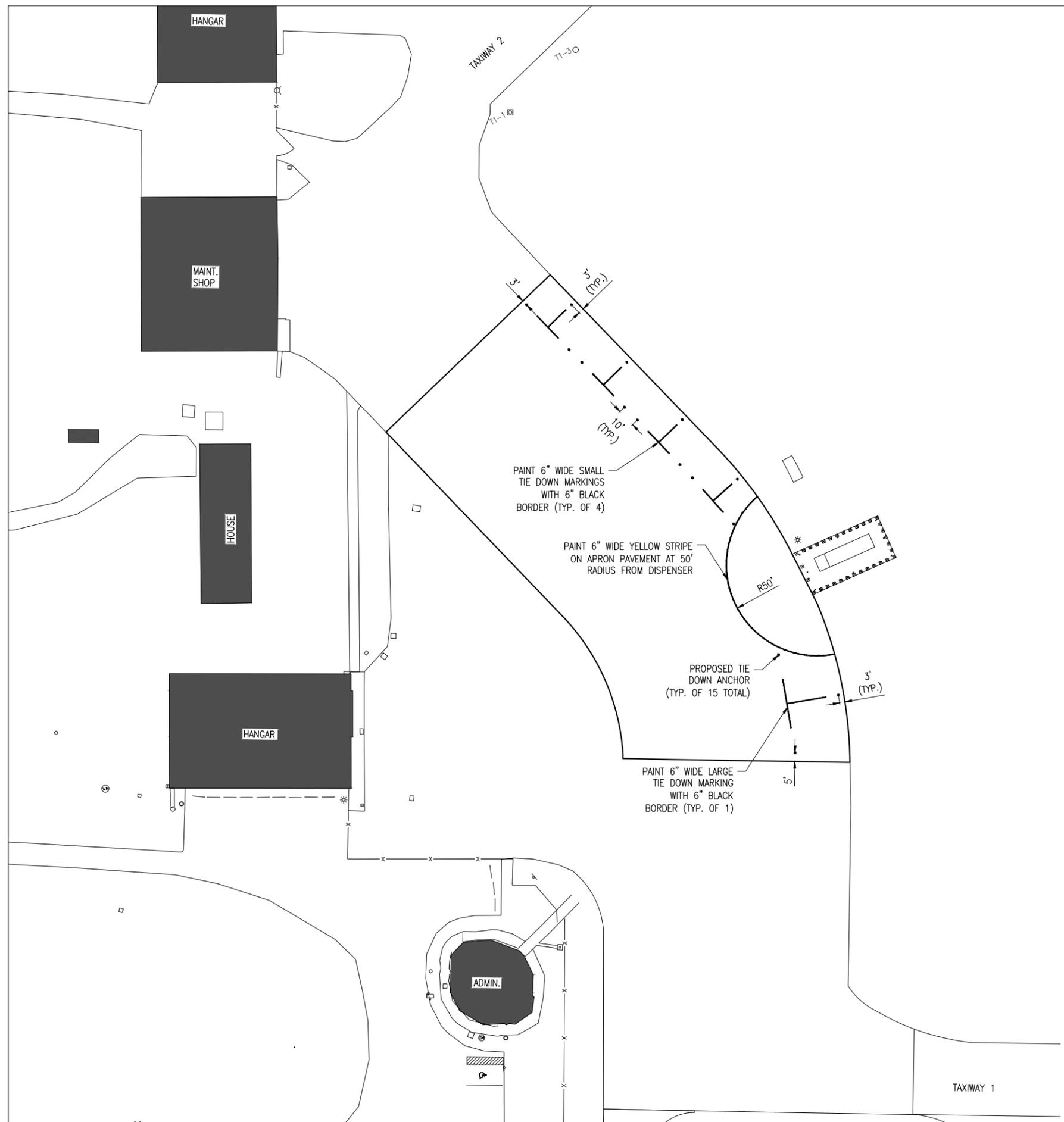


JOINT SEALING DETAILS  
NOT TO SCALE

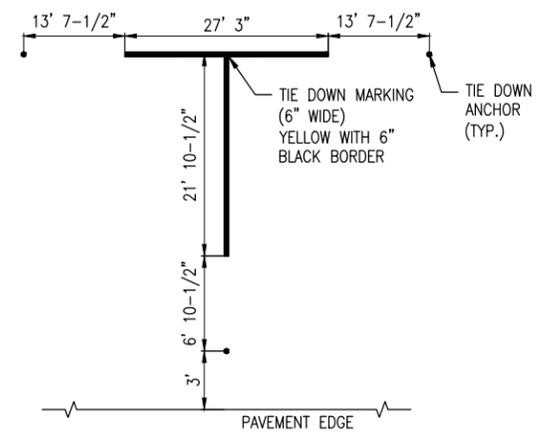


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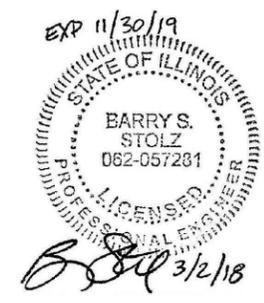
- LEGEND**
- EXISTING IMPROVEMENTS
  - EXISTING BUILDINGS
  - PROPOSED IMPROVEMENTS
  - EXISTING FENCE
  - PROPOSED PAVEMENT MARKING



**SMALL TIE DOWN DETAIL - TYP. OF 4**  
NOT TO SCALE



**LARGE TIE DOWN DETAIL - TYP. OF 1**  
NOT TO SCALE



**PHASE 1 - RECONSTRUCT MAIN PCC APRON**

IDA No: SLO-4581  
SBG Project No: 3-17-SBGP-120/133/139  
Contract No. SA031

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: MARCH 2, 2018  
PROJECT NO: 17A0017D  
CAD FILE: C-151-MRK.DWG  
DESIGN BY: JRH 02/02/2018  
DRAWN BY: JRH 02/02/2018  
REVIEWED BY: BSS 03/02/2018

SHEET TITLE

**PROPOSED MARKING PLAN**