

# CITY OF AURORA AURORA, ILLINOIS

## CONSTRUCTION PLANS FOR AURORA MUNICIPAL AIRPORT OVERLAY SOUTHEAST QUADRANT PERIMETER ROADWAYS

ILLINOIS PROJECT: ARR-4636

MARCH 2, 2018

### INDEX TO SHEETS

1. COVER SHEET
2. SUMMARY OF QUANTITIES AND MISCELLANEOUS DETAILS/NOTES
3. SITE PLAN AND PROJECT CONTROL PLAN
4. SEQUENCE OF CONSTRUCTION
5. SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS
6. TYPICAL SECTIONS
7. EXISTING CONDITIONS AND PROPOSED REMOVALS - SHEET 1
8. EXISTING CONDITIONS AND PROPOSED REMOVALS - SHEET 2
9. STAKING AND MARKING PLAN



*Kris*  
 02-28-16  
 Exp 11-30-19

**811** Know what's below. Call before you dig.

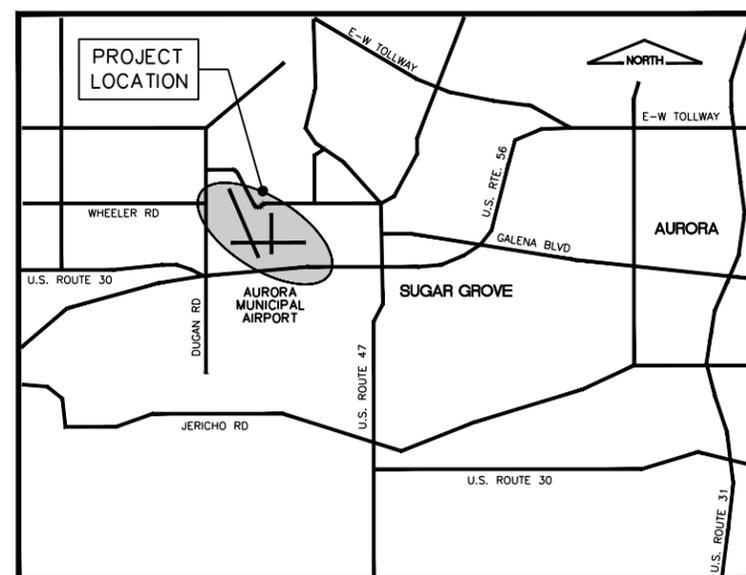
J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS [www.illinois1call.com](http://www.illinois1call.com)

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

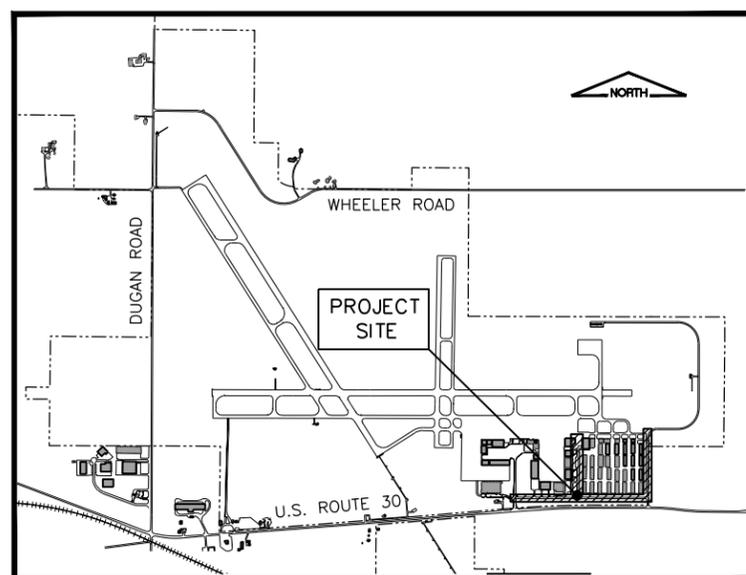
CALL J.U.L.I.E BEFORE EXCAVATING AT 811

AURORA MUNICIPAL AIRPORT

SECTION: 17 AND 18	COUNTY: KANE
RANGE: 7 EAST	U.S. ROUTE 30
TOWNSHIP: 38 NORTH	SUGAR GROVE TOWNSHIP



LOCATION MAP



SITE PLAN

**CITY OF AURORA, ILLINOIS**

APPROVED BY *Richard C. Irvin* MAYOR  
 RICHARD C. IRVIN

DATE MARCH 2 2018

APPROVED BY *Stephen K. Andras* AIRPORT MANAGER  
 STEPHEN K. ANDRAS, P.E.

DATE 3-1- 2018

**CMT** 17285-03  
 CRAWFORD MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS

SUBMITTED BY *Kris*  
 KRIS SALVATERA, P.E.  
 02-28-16

DATE \_\_\_\_\_

# STORM WATER POLLUTION PREVENTION NOTES

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

## SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING AN OVERLAY, PAVEMENT PATCHING, PAVEMENT MILLING AND CRACK REPAIR AT AURORA MUNICIPAL AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

- UNCLASSIFIED EXCAVATION
- 4" AGGREGATE BASE FOR SHOULDERS.

## AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

## DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO STORM SEWERS THAT OUTLET INTO THE WELCH CREEK.

## SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

## DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

- WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
- EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
- AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
  - PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
  - EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.

- CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

- SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.

- THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

## DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

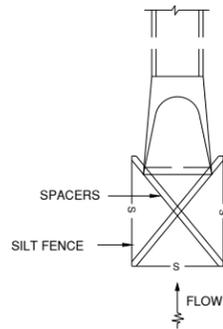
TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

## MAINTENANCE AFTER CONSTRUCTION

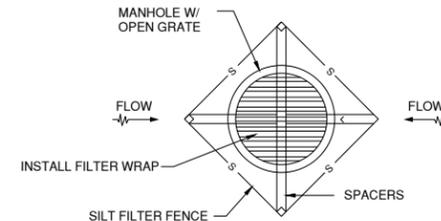
CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

SUMMARY OF QUANTITIES				
BASE BID - OVERLAY SOUTHEAST QUADRANT PERIMETER ROADWAYS				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEERS FIELD OFFICE	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	525	
AR152540	SOIL STABILIZATION FABRIC	SY	455	
AR156520	INLET PROTECTION	EACH	6	
AR162570	DETECTOR LOOP	LS	1	
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	3550	
AR201663	SAND MIX CRACK REPAIR	LF	970	
AR208515	POROUS GRANULAR EMBANKMENT	CY	152	
AR208604	4" AGGREGATE BASE COURSE	SY	1440	
AR208610	10" AGGREGATE BASE COURSE	SY	755	
AR401610	BITUMINOUS SURFACE COURSE	TON	670	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	6050	
AR401900	REMOVE BITUMINOUS PAVEMENT	SY	610	
AR401915	REM & REP BIT PAVEMENT - TYPE A	SY	320	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SY	455	
AR403610	BITUMINOUS BASE COURSE	TON	140	
AR602510	BITUMINOUS PRIME COAT	GAL	200	
AR603510	BITUMINOUS TACK COAT	GAL	965	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	275	
ADDITIVE ALTERNATE 1 - PAVEMENT RECONSTRUCTION OF A PORTION OF EAST ACCESS ROAD				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AS152410	UNCLASSIFIED EXCAVATION	CY	411	
AS152540	SOIL STABILIZATION FABRIC	SY	672	
AS208515	POROUS GRANULAR EMBANKMENT	CY	224	
AS208610	10" AGGREGATE BASE COURSE	SY	672	
AS401610	BITUMINOUS SURFACE COURSE	TON	70	
AS401900	REMOVE BITUMINOUS PAVEMENT	SY	610	
AS403610	BITUMINOUS BASE COURSE	TON	140	
AS602510	BITUMINOUS PRIME COAT	GAL	200	
AS603510	BITUMINOUS TACK COAT	GAL	185	



INLET PROTECTION (END SECTION)

NOT TO SCALE  
IDOT STANDARD 280001-07

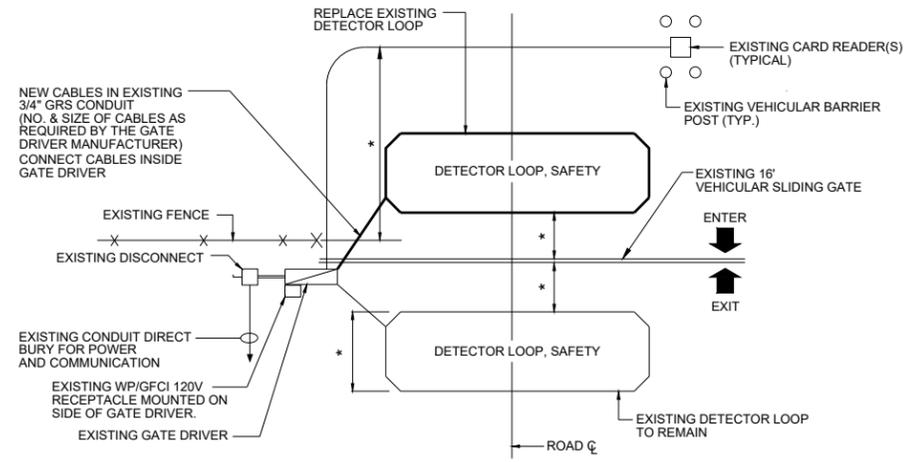


INLET PROTECTION (INLET/MANHOLES - IN TURF)

NOT TO SCALE  
IDOT STANDARD 280001-07

## NOTES FOR INLET PROTECTION DETAILS

- FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
- FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2016.
- FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
- CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
- FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
- COST OF FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



## REPLACE EXISTING DETECTOR LOOP LAYOUT

NOT TO SCALE

\* PER MANUFACTURERS RECOMENDATION CONTRACTOR SHALL COORDINATE THIS WORK WITH ENGINEER.

### NOTES:

- THE LOCATION OF THE NEW DETECTOR LOOPS ARE FOR INFORMATION ONLY AND SHALL BE FIELD ADJUSTED PER THE MANUFACTURER RECOMMENDATION.

### NOTES:

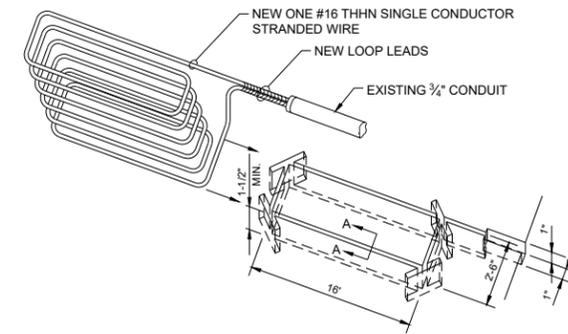
- LOOP LEADS ARE LIMITED TO 100 FEET.
- LOOP LEADS MUST HAVE FOUR (4) TWISTS PER FOOT.
- LOOP AND LOOP LEADS MUST BE LOCATED, AT LEAST, 18" FROM ANY ELECTRICAL POWER SERVICE OR RUN, OR STEEL REINFORCEMENT.
- LOOP LEADS MUST BE IN SEPARATE CONDUIT BETWEEN LOOP AND DETECTOR. THEY MUST NOT SHARE CONDUIT WITH OTHER WIRING OR LEADS FROM OTHER LOOPS.
- WIRE SHALL BE #16 THHN SINGLE CONDUCTOR STRANDED WIRE.
- ALL WIRE SHALL BE CONTINUOUS WITHOUT SPLICING.

### CAUTION:

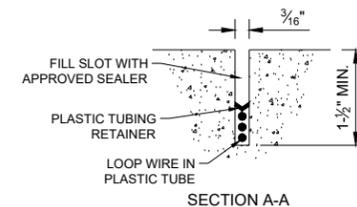
- DO NOT SPLICE WIRE.
- DO NOT FRACTURE WIRE INSULATION. LOOPS SHORTED TO GROUND WILL CAUSE DETECTOR MALFUNCTION. WHEN PLACING WIRE IN THE SLOT, DO NOT USE SCREWDRIVER OR OTHER SHARP TOOLS.

### TYPICAL LAYOUT FOR LOOP:

- SAW SLOT 3/16" WIDE x 1-1/2" DEEP. MAKE RECTANGULAR SHAPE TO SPECIFIED LOOP DIMENSIONS PLUS SLOT FOR LEAD CONDUIT.



PERSPECTIVE VIEW OF DETECTOR LOOP SAW SLOT



DETECTOR LOOP DETAILS

NOT TO SCALE

IL CONTRACT: AU068

IL LETTING ITEM: 06A

IL PROJECT: ARR-4636

S.B.G. PROJECT: N/A

SURVEY BOOK #

## REVISIONS

NUMBER	BY	DATE

0 1 2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

AURORA MUNICIPAL AIRPORT  
 AURORA, ILLINOIS  
 OVERLAY SOUTHEAST QUADRANT PERIMETER ROADWAYS

## SUMMARY OF QUANTITIES AND MISCELLANEOUS DETAILS/NOTES

© Copyright CMT, Inc.

**CMT**  
CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 184-000613

**AURORA**  
MUNICIPAL AIRPORT

DESIGN BY: KWS

DRAWN BY: JRO

CHECKED BY: KWS

APPROVED BY: DKP

DATE: 03/02/2018

JOB No: 17285-03-00

**FINAL**

SHEET 2 OF 9 SHEETS

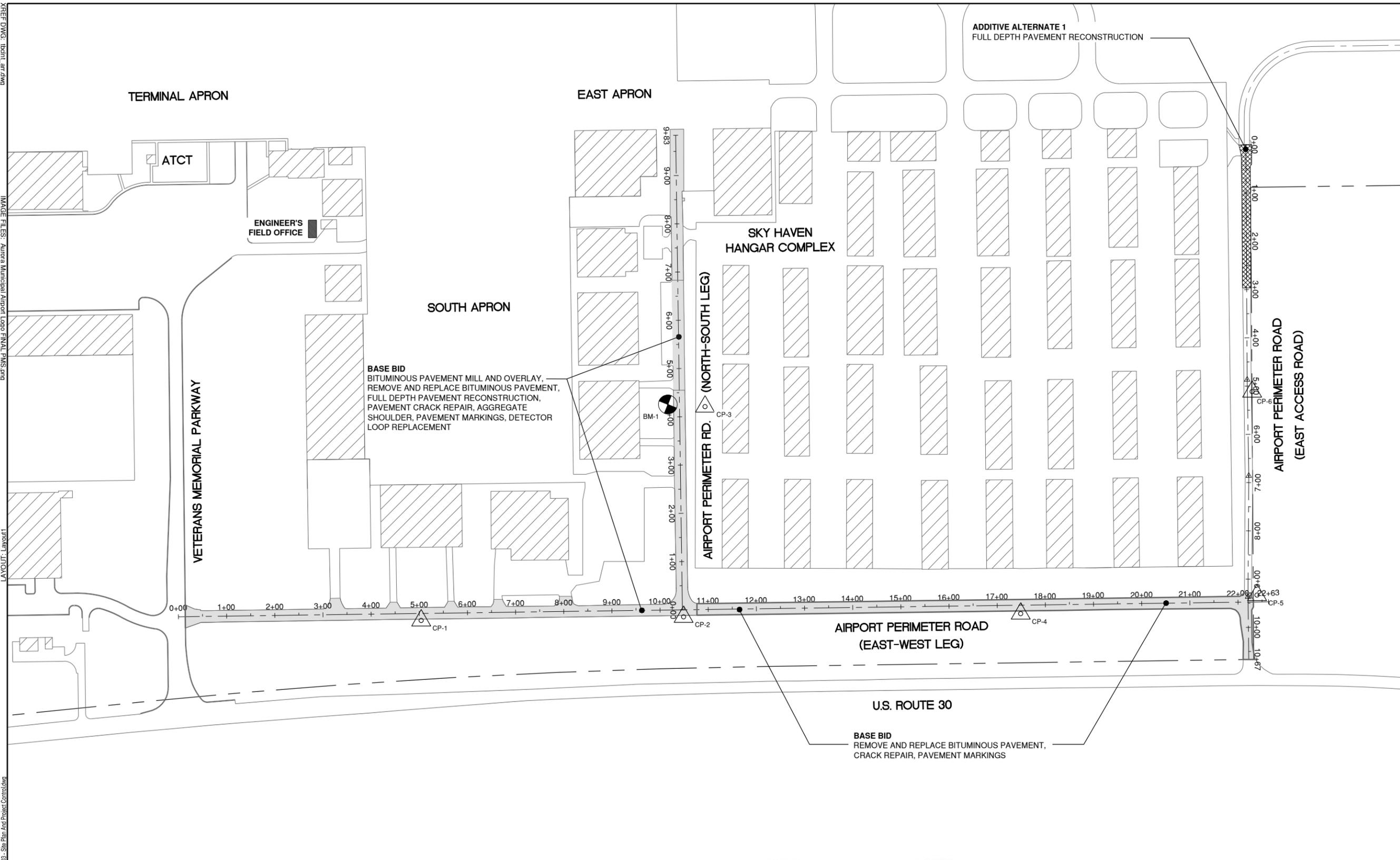
XREF DWG: 18041.dwg

IMAGE FILE: Aurora Municipal Airport Logo FINAL PMS.png

UPDATE BY: Kris Savaterra  
LAYOUT: Layout1

DATE: Monday, March 26, 2018 12:22:31 PM  
FILE: K:\Aurora\17285-03\_Schematic\Draw\DrawSheets\02 - Index of Sheets - Summary Quantities.dwg

DATE: Monday, March 26, 2018 12:22:44 PM  
 FILE: K:\Aurora\Aurora\Draw\Draw\Sheets\03 - Site Plan And Project Control.dwg  
 UPDATE BY: Kris Salvatera  
 LAYOUT: Layout1  
 MANAGE FILES: Aurora Municipal Airport Logo FINAL PMS.png  
 REF DWG: recent air.dwg  
 DWG: base bid base.dwg



HORIZONTAL CONTROL			
POINT	DESCRIPTION	NORTHING	EASTING
CP-1	IRON ROD WITH ORANGE CAP	1857180.050	948350.911
CP-2	IRON ROD WITH ORANGE CAP	1857186.899	948895.891
CP-3	IRON ROD WITH ORANGE CAP	1857708.345	948905.240
CP-4	IRON ROD WITH ORANGE CAP	1857194.235	949595.562
CP-5	IRON ROD WITH ORANGE CAP	1857234.173	950079.307
CP-6	IRON ROD WITH ORANGE CAP	1857656.924	950078.210

HORIZONTAL CONTROL COORDINATES EXPRESSED IN NAD83

VERTICAL CONTROL		
POINT	DESCRIPTION	ELEVATION
BM-1	N.W. BOLT ON HYDRANT	702.06

VERTICAL CONTROL COORDINATES EXPRESSED IN NAVD88

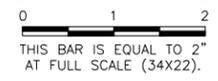
- LEGEND**
- BASE BID
  - ADDITIVE ALTERNATE #1
  - AIRPORT PROPERTY LINE
  - CONTROL POINT
  - BENCHMARK

- NOTES**
- AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
  - THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO THE CONTRACT

IL. CONTRACT: **AU068**  
 IL. LETTING ITEM: **06A**  
 IL. PROJECT: **ARR-4636**  
 S.B.G. PROJECT: **N/A**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**OVERLAY SOUTHEAST QUADRANT PERIMETER ROADWAYS**  
**SITE PLAN AND PROJECT CONTROL PLAN**

**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

DESIGN BY: KWS  
 DRAWN BY: JRO  
 CHECKED BY: KWS  
 APPROVED BY: DKP  
 DATE: 03/02/2018  
 JOB No: 17285-03-00

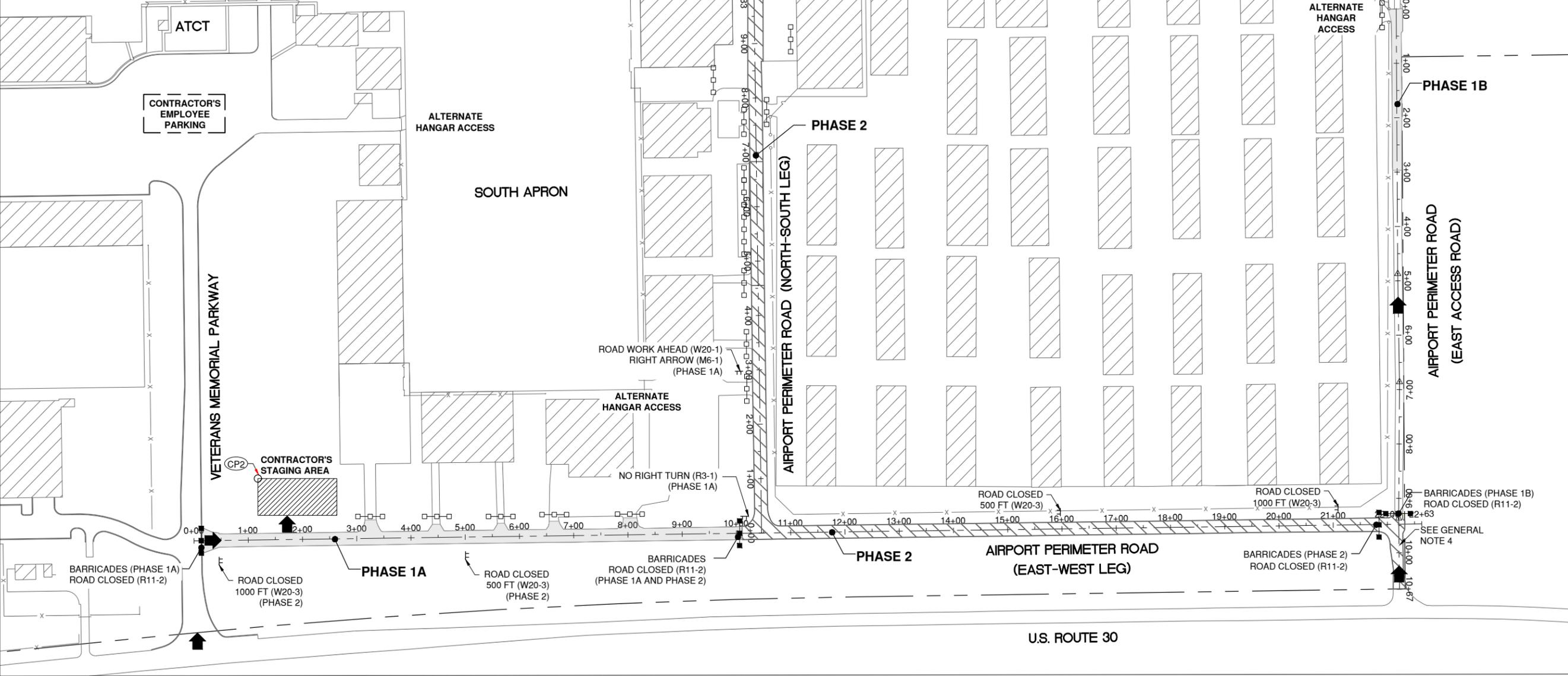
FINAL

SHEET 3 OF 9 SHEETS

DATE: Monday, March 26, 2018 12:22:50 PM  
 FILE: K:\Aurora\17285-03 - Sequence of Construction Plan.dwg  
 UPDATE BY: Kris Salvatera  
 LAYOUT: Layout1  
 MANAGE FILES: Aurora Municipal Airport Logo FINAL PMS.png  
 XREF DWG: 17285-03 - Sequence of Construction Plan.dwg

**GENERAL NOTES**

1. PHASE 1A CAN BE CONSTRUCTED CONCURRENTLY WITH PHASE 1B.
2. COORDINATE ALL ROAD CLOSURES WITH THE RESIDENT ENGINEER AND AIRPORT MANAGER 14 DAYS PRIOR TO THE START OF CLOSURES AT EACH PHASE.
3. AT THE CONTRACTOR'S OPTION, THE PAVEMENT MARKING AND AGGREGATE SHOULDERS CAN BE CONSTRUCTED AT THE END OF THE PROJECT WHILE MAINTAINING TRAFFIC CONTROL WITH NO ROAD CLOSURES.
4. CONTRACTOR SHALL EXPEDITE WORK IN THIS AREA UNDER ONE (1) DAY TIME CLOSURE. TRAFFIC CONTROL SHALL BE MAINTAINED TO ALLOW VEHICULAR ACCESS TO THE EAST ACCESS ROAD DURING PHASE 2.



IL CONTRACT: **AU068**  
 IL LETTING ITEM: **06A**  
 IL PROJECT: **ARR-4636**  
 S.B.G. PROJECT: **N/A**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**OVERLAY SOUTHEAST QUADRANT PERIMETER ROADWAYS**  
**SEQUENCE OF CONSTRUCTION**

**LEGEND**

	PHASE 1A AND PHASE 1B (ADDITIVE ALTERNATE 1)
	PHASE 2
	CONTRACTOR'S EMPLOYEE PARKING, EQUIPMENT AND MATERIAL STORAGE LOCATION
	CONTRACTOR ACCESS ROUTE
	CRITICAL POINT AND NUMBER
	TEMPORARY TYPE III BARRICADES WITH FLASHING LIGHT
	TEMPORARY TYPE II BARRICADE WITH FLASHING LIGHT OR REFLECTORIZED CONE
	TEMPORARY TRAFFIC CONTROL SIGN
	AIRPORT PROPERTY LINE

- SUGGESTED SEQUENCE OF CONSTRUCTION**
- PHASE 1A**
1. PLACE TYPE III BARRICADE AND APPLICABLE TEMPORARY TRAFFIC CONTROL SIGNS FOR ROAD CLOSURES.
  2. BITUMINOUS PAVEMENT MILLING.
  3. AS DIRECTED BY THE RESIDENT ENGINEER, CLEAN AND SEAL BITUMINOUS CRACKS AND SAND MIX CRACK REPAIR.
  4. AS DIRECTED BY THE RESIDENT ENGINEER, REMOVE AND REPLACE BITUMINOUS PAVEMENT UP TO THE MILLED SURFACE ELEVATION. PRIOR TO BITUMINOUS PLACEMENT, IF REQUIRED, REMOVE AGGREGATE BASE AND UNCLASSIFIED EXCAVATION. CONSTRUCT POROUS GRANULAR EMBANKMENT AND AGGREGATE BASE.
  5. CONSTRUCT 2" BITUMINOUS SURFACE COURSE.
  6. CONSTRUCT REMAINING IMPROVEMENTS.
  7. RE-OPEN ROADWAY PRIOR TO PHASE 2 CONSTRUCTION.

- PHASE 1B (ADDITIVE ALTERNATE 1)**
1. PLACE TYPE III BARRICADE AND APPLICABLE TEMPORARY TRAFFIC CONTROL SIGNS FOR ROAD CLOSURES.
  2. REMOVE BITUMINOUS PAVEMENT.
  3. AS DIRECTED BY THE RESIDENT ENGINEER, REMOVE AGGREGATE BASE AND UNCLASSIFIED EXCAVATION. CONSTRUCT POROUS GRANULAR EMBANKMENT AND AGGREGATE BASE AND BITUMINOUS PAVEMENT.
  4. CONSTRUCT REMAINING IMPROVEMENTS.
  5. RE-OPEN ROADWAY PRIOR TO PHASE 2 CONSTRUCTION.

- PHASE 2**
1. PLACE TYPE III BARRICADE AND APPLICABLE TEMPORARY TRAFFIC CONTROL SIGNS FOR ROAD CLOSURES.
  2. BITUMINOUS PAVEMENT MILLING, NORTH-SOUTH LEG ONLY.
  3. AS DIRECTED BY THE RESIDENT ENGINEER, CLEAN AND SEAL BITUMINOUS CRACKS AND SAND MIX CRACK REPAIR.
  4. AS DIRECTED BY THE RESIDENT ENGINEER, REMOVE AND REPLACE BITUMINOUS PAVEMENT UP TO THE MILLED SURFACE ELEVATION. PRIOR TO BITUMINOUS PLACEMENT, IF REQUIRED, REMOVE AGGREGATE BASE AND UNCLASSIFIED EXCAVATION. CONSTRUCT POROUS GRANULAR EMBANKMENT AND AGGREGATE BASE.
  5. CONSTRUCT 2" BITUMINOUS SURFACE COURSE, NORTH-SOUTH LEG ONLY.
  6. CONSTRUCT REMAINING IMPROVEMENTS.
  7. RE-OPEN ROADWAY.

© Copyright CMT, Inc.

**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 184-000613

**AURORA**  
 MUNICIPAL AIRPORT

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	03/02/2018
JOB No:	17285-03-00

**FINAL**

SHEET 4 OF 9 SHEETS

**GENERAL NOTES**

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING VEHICLE ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY IDOT AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2 (LATEST EDITION) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS. NO EXCEPTIONS FOR SLOW MOVING EQUIPMENT SHALL BE ALLOWED.
- THE AIRPORT MANAGER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING PHASING AND SEQUENCE AS IT RELATES TO PEDESTRIAN AND VEHICULAR SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE TENANT PARKING LOTS AND DRIVEWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER AND AIRPORT.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, CONSTRUCTION FENCE, SIGNING, , ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES SHALL HAVE FLASHING LIGHT(S) AND CONFORM TO IDOT STANDARD 701901-07, TYPE II AND TYPE III SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- AURORA MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.
- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS. OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO PREVIOUS EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDENT, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 50-17 OF THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT MANAGER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER.

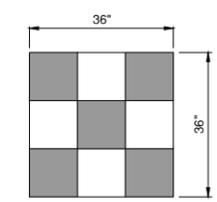
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS OR A WORKING BEACON LIGHT ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- PREPARE A SAFETY PLAN COMPLIANCE DOCUMENT. THE SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) DETAILS HOW THE CONTRACTOR WILL COMPLY WITH THE CSPP. ALSO, IT WILL NOT BE POSSIBLE TO DETERMINE ALL SAFETY PLAN DETAILS (FOR EXAMPLE SPECIFIC HAZARD EQUIPMENT AND LIGHTING, CONTRACTOR'S POINTS OF CONTACT, CONSTRUCTION EQUIPMENT HEIGHTS) DURING THE DEVELOPMENT OF THE CSPP. THE SUCCESSFUL CONTRACTOR MUST DEFINE SUCH DETAILS BY PREPARING AND SPCD THAT THE AIRPORT OPERATOR REVIEWS FOR APPROVAL PRIOR TO ISSUANCE OF A NOTICE-TO-PROCEED. THE SPCD IS A SUBSET OF THE CSPP, SIMILAR TO HOW A SHOP DRAWING REVIEW IS A SUBSET TO THE TECHNICAL SPECIFICATIONS.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS. THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS THE SITE DUE TO ADJACENT CONSTRUCTION

- INSTALL AIRFIELD LIGHTING CONTROL AND MONITORING SYSTEM (ALCMS) INCLUDING REMOVING EXISTING SYSTEM
- REHABILITATE SOUTHWEST AUTO PARKING LOT AT SOUTH APRON

**TABLE 1 - CRITICAL POINTS**

POINT	APPROXIMATE ELEVATION OF GROUND (NAV 83)	ANTICIPATED EQUIPMENT AND HEIGHT	APPROXIMATE ELEVATION OF EQUIPMENT (NAV 83)	LATITUDE (NAD 83)	LONGITUDE (NAD 83)
CP1	704.4	SEMI/DUMP TRUCK - 25'	729.4	41°46'06.03"	88°27'46.90"
CP2	699.9	SEMI/DUMP TRUCK - 25'	724.9	41°45'57.17"	88°27'58.80"



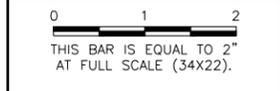
**CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG**  
 NOT TO SCALE

IL CONTRACT: **AU068**  
 IL LETTING ITEM: **06A**  
 IL PROJECT: **ARR-4636**  
 S.B.G. PROJECT: **N/A**

SURVEY BOOK #

**REVISIONS**

NUMBER	BY	DATE



**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**OVERLAY SOUTHEAST QUADRANT PERIMETER ROADWAYS**  
**SEQUENCE OF CONSTRUCTION**  
**GENERAL NOTES AND DETAILS**

© Copyright CMT, Inc.

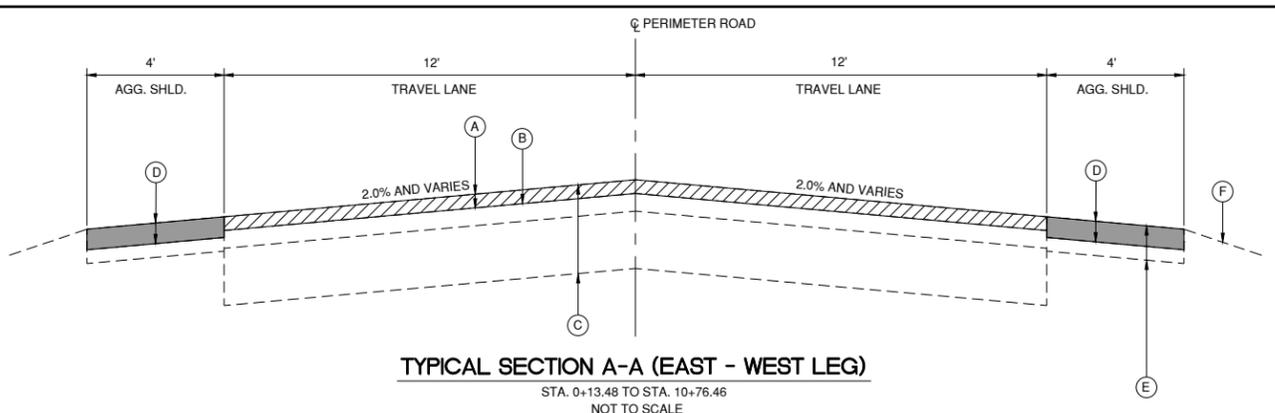
**CMT**  
 CRAWFORD, MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS  
 License No. 164-000613

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	03/02/2018
JOB No:	17285-03-00

**FINAL**

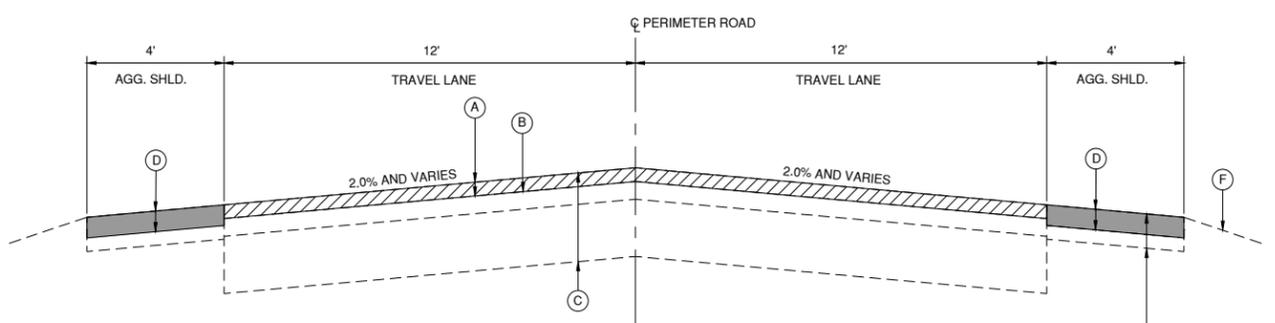
SHEET 5 OF 9 SHEETS

REF: DWG: 180117.dwg  
 IMAGE FILES: Aurora Municipal Airport Logo FINAL PMS.smg  
 UPDATE BY: Kris Salvatera  
 LAYOUT: Layout1  
 DATE: Monday, March 26, 2018 12:23:16 PM  
 FILE: K:\Aurora\17285-03\_ScenarioOverlay\Draw\Sheet06 - Typical Sections.dwg



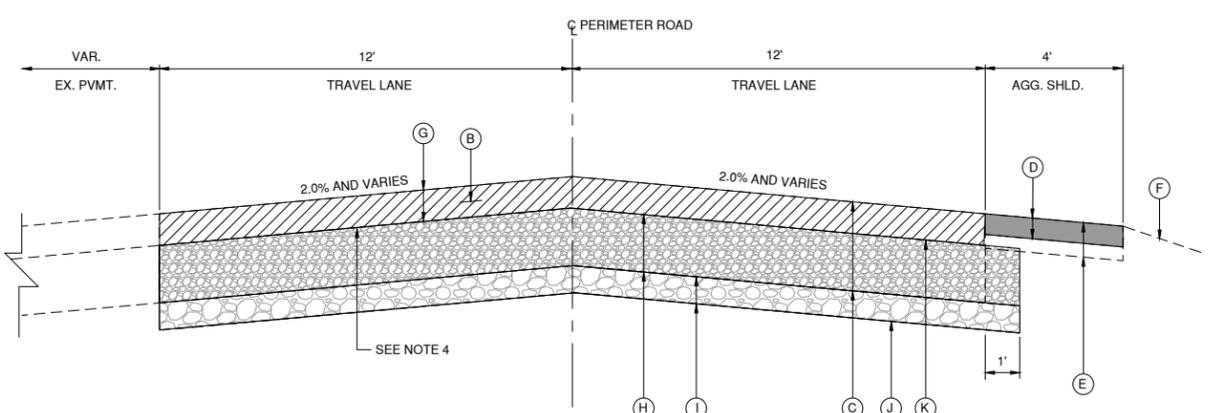
**TYPICAL SECTION A-A (EAST - WEST LEG)**

STA. 0+13.48 TO STA. 10+76.46  
NOT TO SCALE



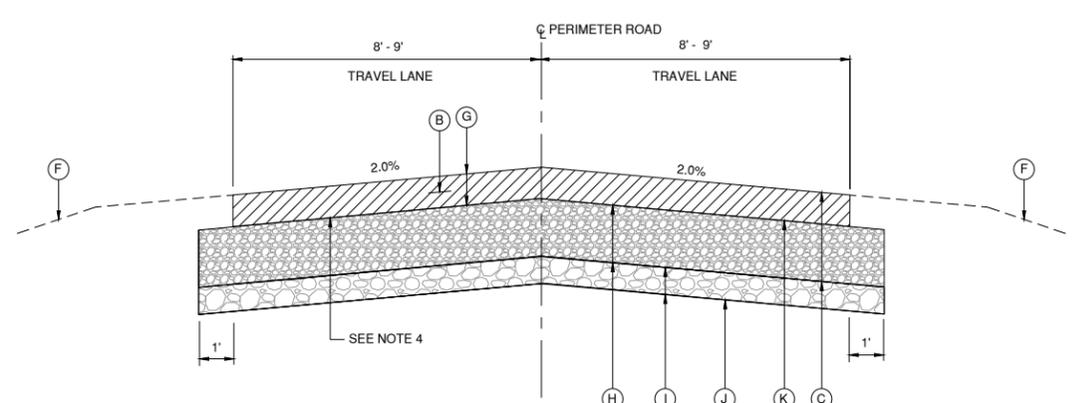
**TYPICAL SECTION B-B (NORTH - SOUTH LEG)**

STA. 0+00.00 TO STA. 4+55.29 AND  
STA. 6+83.05 TO STA. 9+95.87  
NOT TO SCALE



**TYPICAL SECTION C-C (NORTH - SOUTH LEG)**

STA. 4+55.29 TO STA. 6+83.05  
NOT TO SCALE



**TYPICAL SECTION D-D (EAST ACCESS ROAD)  
(ADDITIVE ALTERNATE NO. 1)**

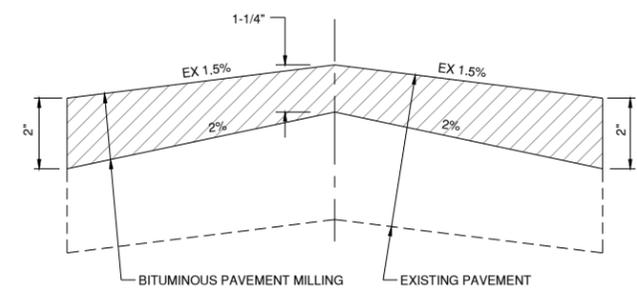
STA. 0+00.00 TO STA. 2+96.53  
NOT TO SCALE

**LEGEND**

- (A) NEW 2" BITUMINOUS SURFACE MILLING (SEE NOTE 1)  
NEW 2" BITUMINOUS SURFACE COURSE
- (B) NEW BITUMINOUS TACK COAT BETWEEN LIFTS
- (C) EXISTING APPROX. 5-1/2" BITUMINOUS PAVEMENT  
EXISTING APPROX. 10" CRUSHED AGGREGATE BASE COURSE
- (D) NEW UNCLASSIFIED EXCAVATION  
NEW 4" CRUSHED AGGREGATE SHOULDER
- (E) EXISTING 6" AGGREGATE SHOULDER
- (F) EXISTING GROUND LINE
- (G) BITUMINOUS PAVEMENT REMOVAL  
NEW 2" BITUMINOUS SURFACE COURSE  
NEW 4" BITUMINOUS BASE COURSE (403)  
NEW 2-2" LIFTS AND TACK COAT BETWEEN LIFTS
- (H) AGGREGATE BASE REMOVAL (SEE NOTE 2)  
NEW 10" AGGREGATE BASE COURSE  
(AS DIRECTED BY THE RESIDENT ENGINEER)
- (I) NEW UNCLASSIFIED EXCAVATION  
NEW POROUS GRANULAR EMBANKMENT.  
(AS DIRECTED BY THE RESIDENT ENGINEER)
- (J) NEW STABILIZATION FABRIC  
(AS DIRECTED BY RESIDENT ENGINEER)
- (K) NEW PRIME COAT

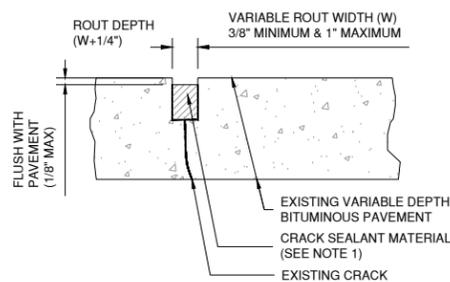
**NOTES**

1. ROADWAY MILLING SHALL BE 1-1/4" DEPTH AT THE ROADWAY CENTERLINE AND 2" DEPTH AT THE EDGE OF PAVEMENT. ADJACENT DRIVEWAY MILLING SHALL BE 2" DEPTH.
2. AGGREGATE BASE IS PAID FOR UNDER UNCLASSIFIED EXCAVATION.
3. SEE SHEET 9 FOR PLAN VIEW LOCATIONS OF TYPICAL SECTIONS.
4. THE CONTRACTOR SHALL REGRADE AND RECOMPACT EXISTING CRUSHED AGGREGATE BASE COURSE, INCLUDING ANY REQUIRED BASE PLACEMENT OR REMOVAL TO GRADE TO 6" SECTION FOR AREAS PER SPECIFICATION 208. FOR TYPICAL SECTION C-C AND D-D (INCIDENTAL TO THE CONTRACT).



**VARIABLE BITUMINOUS SURFACE MILLING DETAIL**

(SEE NOTE 1)  
NOT TO SCALE

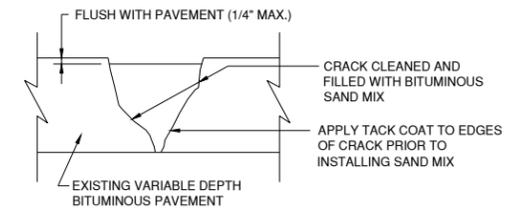


**CLEAN AND SEAL BITUMINOUS CRACKS**

NOT TO SCALE

**CRACK SEALING NOTES:**

1. THE CRACK ROUTING DIMENSIONS WILL PROVIDE A WIDTH TO DEPTH RATIO OF 1:1 FOR SEALANT MATERIAL.
2. CRACKS TO BE ROUTED, CLEANED AND SEALED AT LOCATIONS DESIGNATED BY THE RESIDENT ENGINEER.

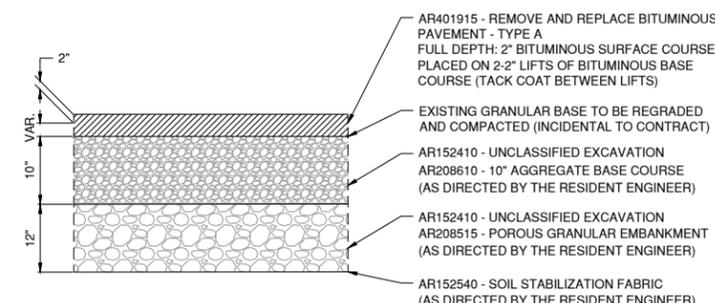


**HMA SAND MIX CRACK REPAIR DETAIL**

NOT TO SCALE

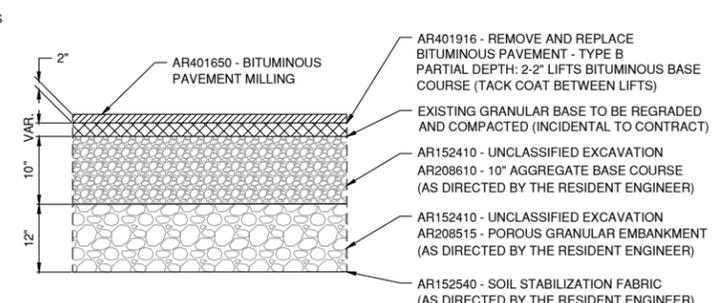
**CRACK REPAIR NOTES:**

1. CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
2. SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1" WIDE AS DESIGNATED BY THE RESIDENT ENGINEER.
3. CONTRACTOR SHALL ROUTE, CRACK PLOW AND REMOVE LOOSE FILLER, LOOSE AGGREGATE AND LAITANCE WITH COMPRESSED AIR AS SPECIFIED PRIOR TO SEALING.



**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A DETAIL**

NOT TO SCALE



**REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B DETAIL**

NOT TO SCALE

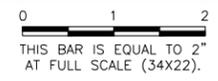
1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS AND PAVEMENT CORES. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. EXISTING MEDIUM TO HIGH SEVERITY TRAVERSE CRACKS ARE SPACED AT VARIABLE INTERVALS. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.
3. AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE COURSE SHALL BE DONE AT SECTIONS WHERE THE REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B IS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES.

IL CONTRACT: **AU068**  
 IL LETTING ITEM: **06A**  
 IL PROJECT: **ARR-4636**  
 S.B.G. PROJECT: **N/A**

SURVEY BOOK #

**REVISIONS**

NUMBER	BY	DATE



**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**OVERLAY SOUTHEAST QUADRANT PERIMETER ROADWAYS**

**TYPICAL SECTIONS**

© copyright CMT, Inc.



DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	03/02/2018
JOB No:	17285-03-00

**FINAL**

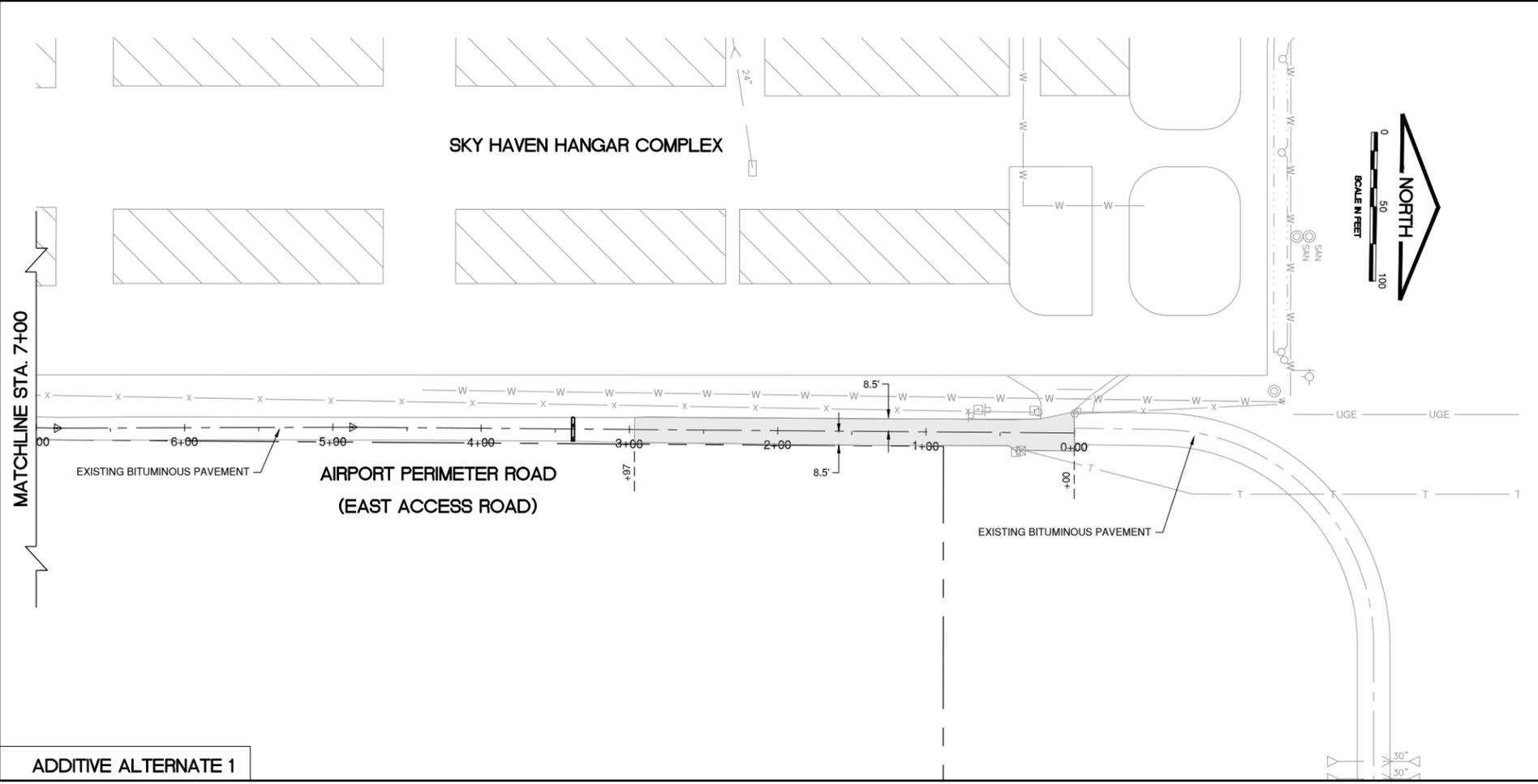
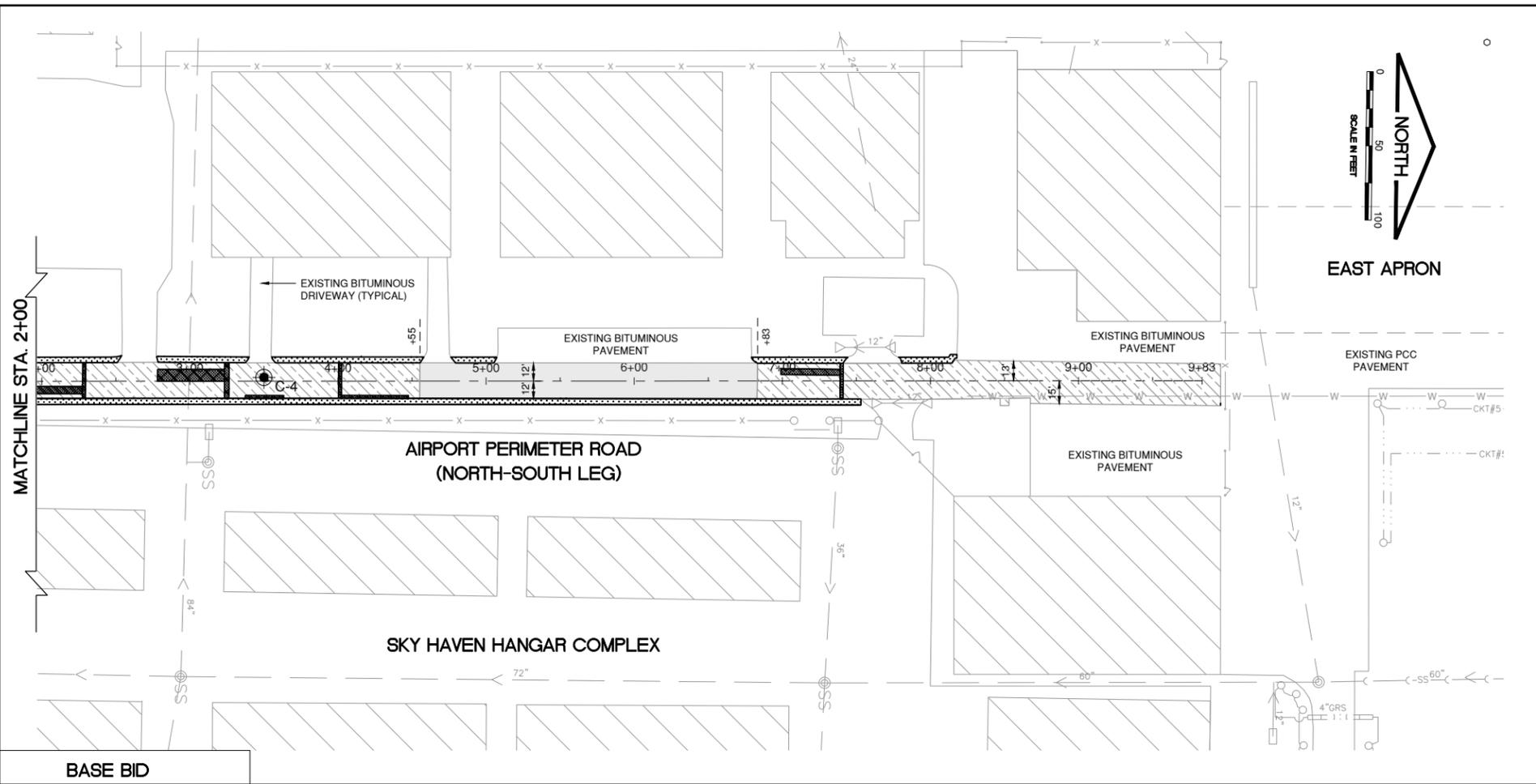


XREF DWG: 180101.dwg  
 AAS: 180101.dwg  
 Proposed improvements: dwp

IMAGE FILE: Aurora Municipal Airport Logo FINAL.PMS.png  
 UPDATE BY: Kris Savaterra  
 LAYOUT: Layout1

DATE: Monday, March 26, 2018 12:29:51 PM  
 FILE: K:\Aurora\17285-03\_SCD\Aurora\17285-03\_Existing Conditions.dwg

DATE: Monday, March 26, 2018 12:29:51 PM  
 FILE: K:\Aurora\17285-03\_SCD\Aurora\17285-03\_Existing Conditions.dwg



**LEGEND**

- EXISTING ELECTRICAL/STORM/SANITARY/TELEPHONE MANHOLE OR EXISTING WATER VALVE VAULT
- EXISTING ELECTRICAL HANDHOLE
- EXISTING STORM INLET
- EXISTING SLOPE BOX
- EXISTING FLARED END SECTION
- EXISTING HEADWALL
- EXISTING FLUSH MOUNTED FIRE HYDRANT
- EXISTING WATER VALVE
- EXISTING UTILITY PEDESTAL
- EXISTING CONDUIT/DUCT BANK
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING WATERMAIN
- EXISTING SANITARY SEWER
- EXISTING CRACK
- PAVEMENT CORE/BORE AND NUMBER
- BITUMINOUS PAVEMENT MILLING (AR401650)
- REMOVE BITUMINOUS PAVEMENT (AR401900)
- REM & REP BIT PAVEMENT - TYPE A (AR401915) AND TYPE B (AR401916)
- UNCLASSIFIED EXCAVATION (AR152410)

**NOTES**

1. THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.
2. THE EXISTING BITUMINOUS PAVEMENT SHALL BE SAWCUT AT THE MILLING LIMITS TO PROVIDE A CLEAN VERTICAL FACE. COST FOR SAWCUT SHALL BE INCLUDED IN THE CONTRACT.
3. PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE SWEEPED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED. ALL CRACKS TO BE WIDER THAN 1" SHALL BE REPAIRED UNDER ITEM AR201663, SAND MIX CRACK REPAIR. OTHER CRACKS AS NOTED DURING THE INSPECTION SHALL BE SEALED UNDER ITEM AR201661, CLEAN AND SEAL BITUMINOUS CRACKS.
4. EXACT LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A AND TYPE B SHALL BE DETERMINED IN THE FIELD WITH THE APPROVAL OF THE RESIDENT ENGINEER. MINIMUM WIDTH OF REPAIRS SHALL BE NO LESS THAN 3'.
5. REMOVE AND REPLACE AGGREGATE BASE SHALL BE INSTALLED ONLY AS DIRECTED BY THE ENGINEER FOR CONSTRUCTION OF REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A AND TYPE B.

**PAVEMENT THICKNESS INFORMATION FROM CORES COMPLETED NOVEMBER 2017**

<b>BORING B-1</b>	
7"	BITUMINOUS MATERIALS (201)
8"	AGGREGATE BASE COURSE (CRUSHED STONE)
12"	CLAYEY SANDY SILT
30"	SILT
18"	SANDY SILT
<b>CORE C-1</b>	
5"	BITUMINOUS MATERIALS (201)
10"	AGGREGATE BASE COURSE (CRUSHED STONE)
9"	CLAYEY SANDY SILT
<b>CORE C-2</b>	
6"	BITUMINOUS MATERIALS (201)
8"	AGGREGATE BASE COURSE (CRUSHED STONE)
0"	CLAYEY SANDY SILT
<b>CORE C-3</b>	
6.75"	BITUMINOUS MATERIALS (201)
7.25"	AGGREGATE BASE COURSE (CRUSHED STONE)
10"	CLAYEY SANDY SILT
<b>CORE C-4</b>	
4.25"	BITUMINOUS MATERIAL
7.25"	AGGREGATE BASE COURSE (CRUSHED STONE)
12"	CLAYEY SANDY SILT

IL CONTRACT: **AU068**  
 IL LETTING ITEM: **06A**  
 IL PROJECT: **ARR-4636**  
 S.B.G. PROJECT: **N/A**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**OVERLAY SOUTHEAST QUADRANT PERIMETER ROADWAYS**  
**EXISTING CONDITIONS AND PROPOSED REMOVALS**  
**SHEET 2**

© Copyright CMT, Inc.

CMT

CRAWFORD, MURPHY & TILLY, INC.  
CONSULTING ENGINEERS  
License No. 164-000613

DESIGN BY:	KWS
DRAWN BY:	JRO
CHECKED BY:	KWS
APPROVED BY:	DKP
DATE:	03/02/2018
JOB No:	17285-03-00

