

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: Illinois Professional Design Firm
 DATE PLOTTED: 3/23/2018 11:59:45 AM
 FILE NAME: 86140100-sh1-1gp-01.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOTLABEL.tbl



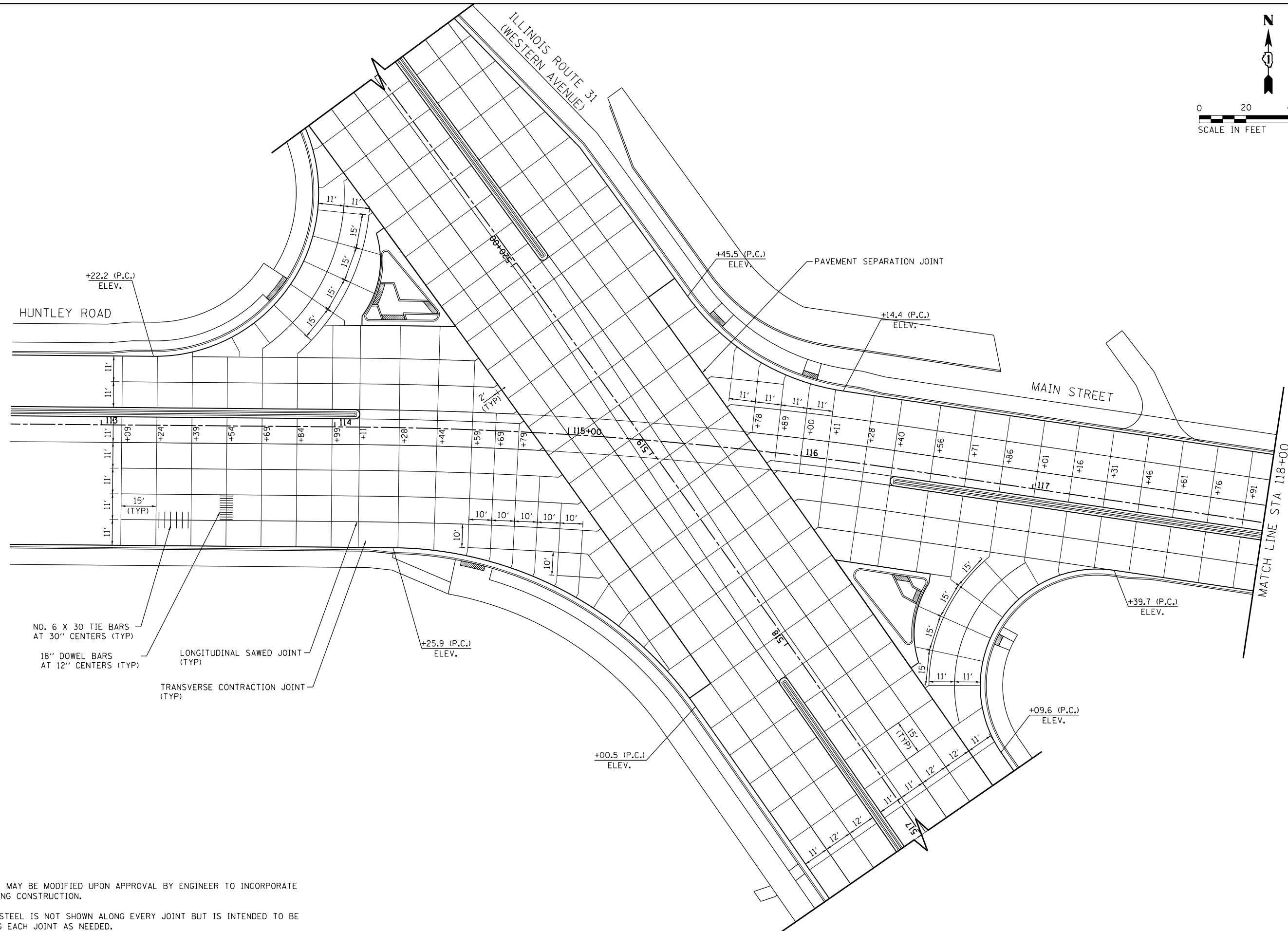
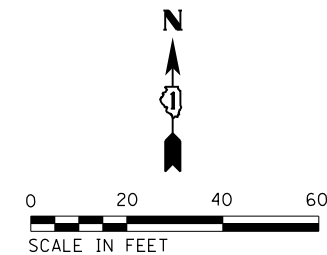
USER NAME = jmelhu	DESIGNED - JRM	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=10'	CHECKED - TH	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INTERSECTION GRADING PLAN
MAIN STREET AND LINCOLN AVENUE

SCALE: 1"=10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	102
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				61E73



NO. 6 X 30 TIE BARS
AT 30" CENTERS (TYP)

18" DOWEL BARS
AT 12" CENTERS (TYP)

LONGITUDINAL SAWED JOINT
(TYP)

TRANSVERSE CONTRACTION JOINT
(TYP)

- NOTES:**
1. JOINT LAYOUT MAY BE MODIFIED UPON APPROVAL BY ENGINEER TO INCORPORATE CHANGES DURING CONSTRUCTION.
 2. REINFORCING STEEL IS NOT SHOWN ALONG EVERY JOINT BUT IS INTENDED TO BE PLACED ALONG EACH JOINT AS NEEDED.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:58:17 AM
 FILE NAME: 86140100-sh1-pv1-joint-01.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOTLABEL.tbl



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

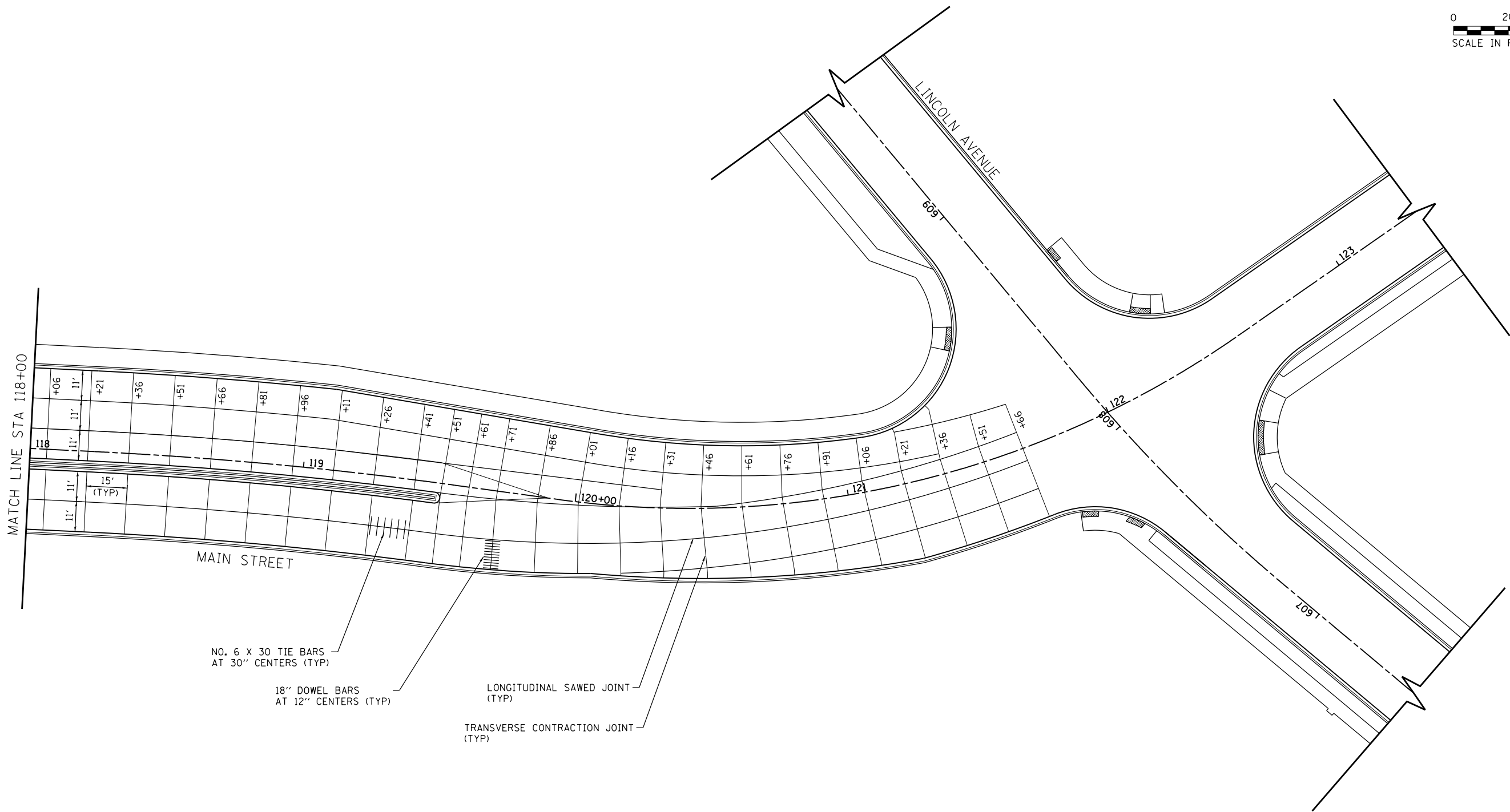
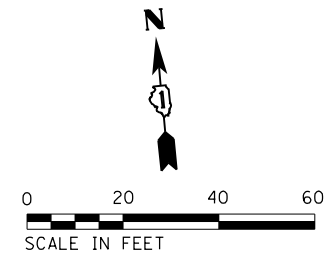
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT JOINTING PLAN
HUNTLEY ROAD/MAIN STREET AT IL RTE 31**

SCALE: 1"=20' SHEET NO. 1 OF 6 SHEETS STA. 113+00.00 TO STA. 118+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	103
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				61E73

86140100
 11 Border
 86140100-sh1-pv1-joint-01.dgn



NOTES:

1. JOINT LAYOUT MAY BE MODIFIED UPON APPROVAL BY ENGINEER TO INCORPORATE CHANGES DURING CONSTRUCTION.
2. REINFORCING STEEL IS NOT SHOWN ALONG EVERY JOINT BUT IS INTENDED TO BE PLACED ALONG EACH JOINT AS NEEDED.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 DATE PLOTTED: 3/23/2018 11:59:20 AM
 FILE NAME: 86140100-sh1-pv1joint-02.dgn
 PLOT DRIVER: ILPlotDriver
 PLOT TABLE: PLOTTABLE.tbl



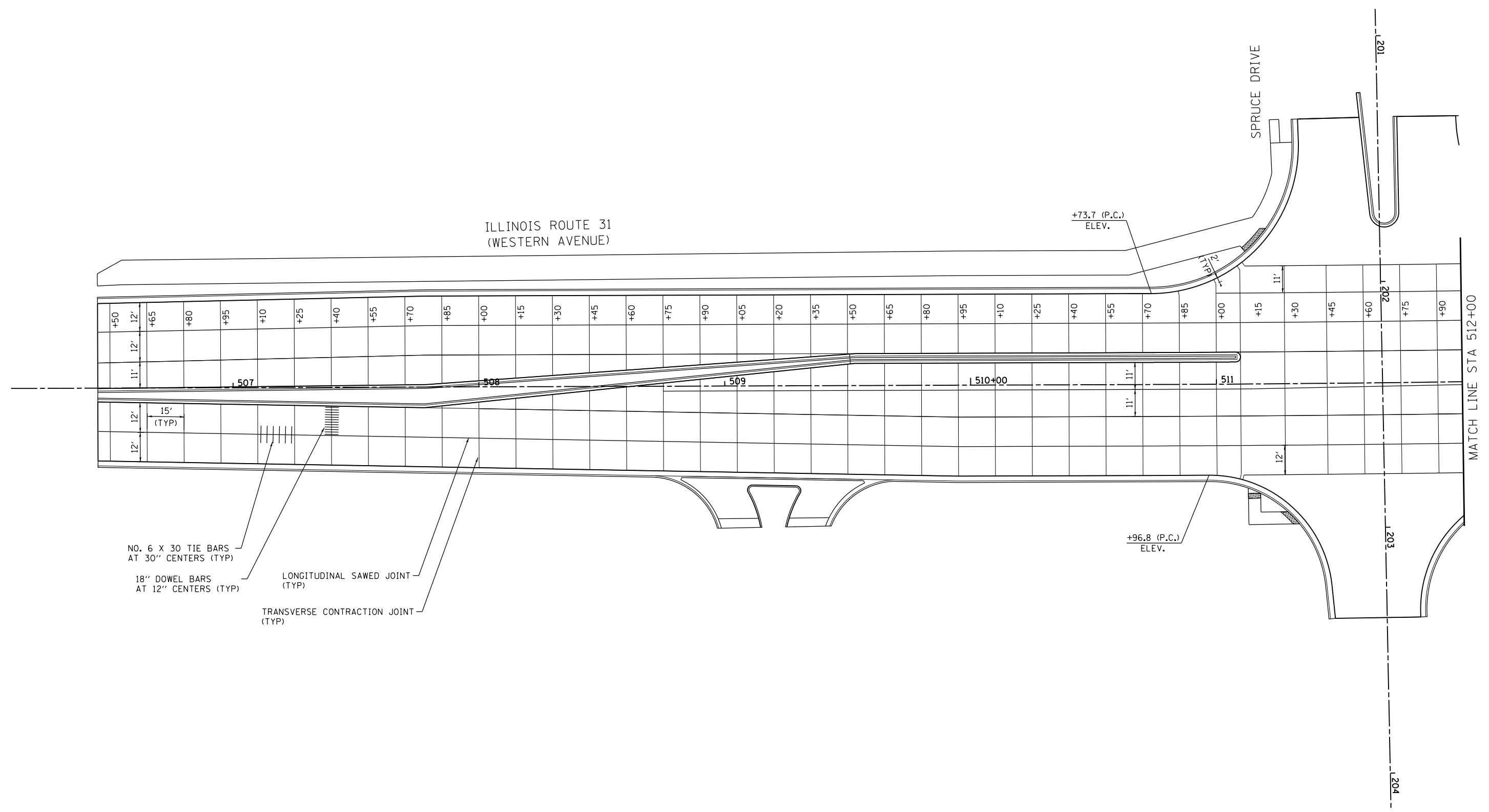
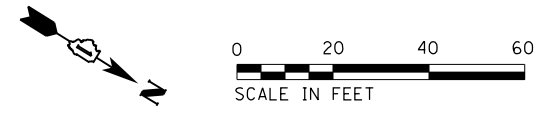
USER NAME = jmelhus	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISIONS -	
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISIONS -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISIONS -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT JOINTING PLAN
HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 2 OF 6 SHEETS STA. 118+00.00 TO STA. 121+66.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	104
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				61E73



- NO. 6 X 30 TIE BARS AT 30" CENTERS (TYP)
- 18" DOWEL BARS AT 12" CENTERS (TYP)
- LONGITUDINAL SAWED JOINT (TYP)
- TRANSVERSE CONTRACTION JOINT (TYP)

NOTES:

1. JOINT LAYOUT MAY BE MODIFIED UPON APPROVAL BY ENGINEER TO INCORPORATE CHANGES DURING CONSTRUCTION.
2. REINFORCING STEEL IS NOT SHOWN ALONG EVERY JOINT BUT IS INTENDED TO BE PLACED ALONG EACH JOINT AS NEEDED.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:59:22 AM
 FILE NAME: 86140100-sh1-pv1joint-03.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOTTABLE.tbl



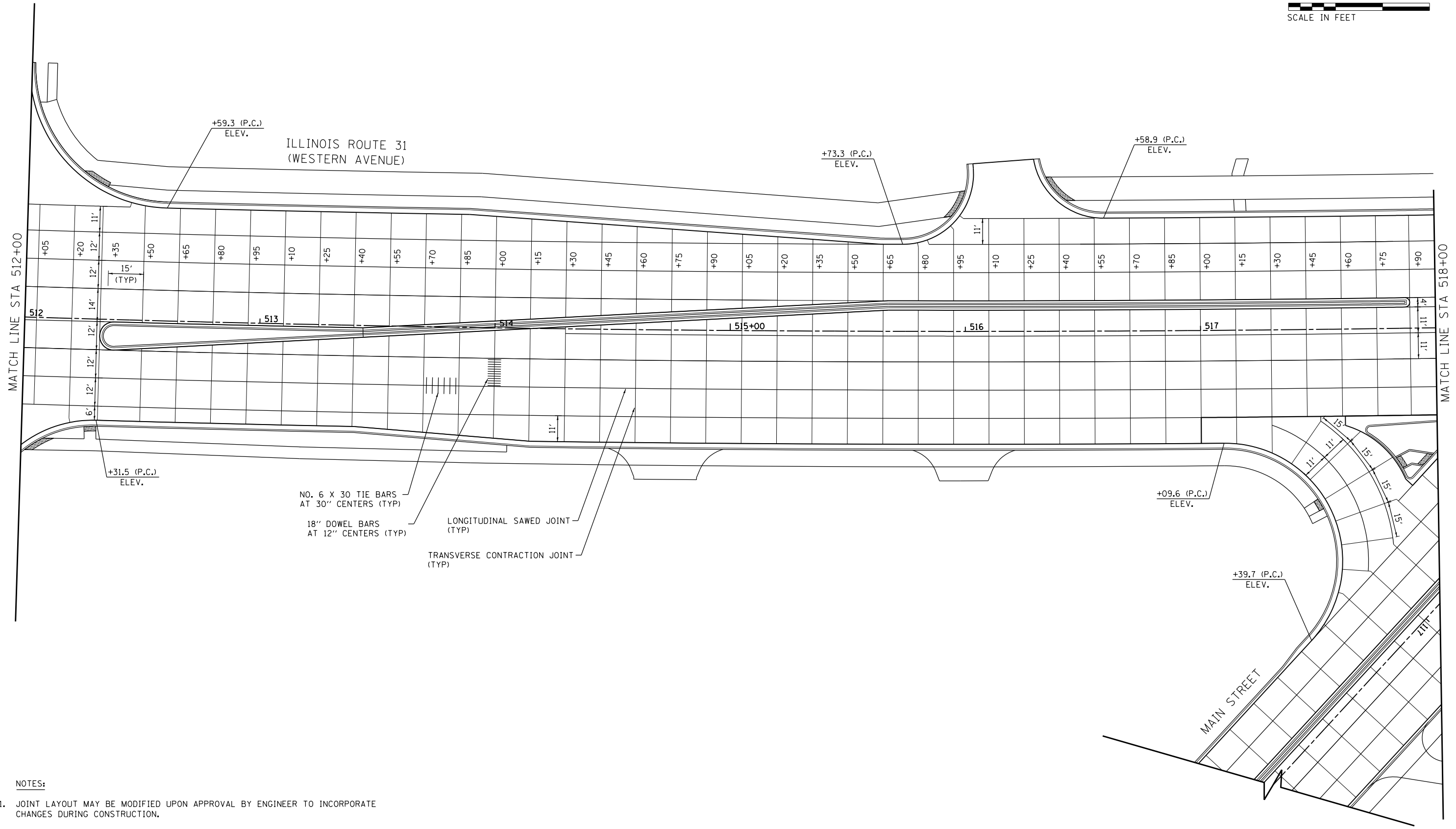
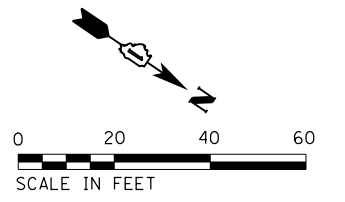
USER NAME = jmelhus	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISIONS -	
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT JOINTING PLAN
ILLINOIS ROUTE 31**

SCALE: 1"=20' SHEET NO. 3 OF 6 SHEETS STA. 506+45.00 TO STA. 512+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	105
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				CONTRACT NO. 61E73



NOTES:

1. JOINT LAYOUT MAY BE MODIFIED UPON APPROVAL BY ENGINEER TO INCORPORATE CHANGES DURING CONSTRUCTION.
2. REINFORCING STEEL IS NOT SHOWN ALONG EVERY JOINT BUT IS INTENDED TO BE PLACED ALONG EACH JOINT AS NEEDED.

COMPANY NAME: #COMPANY NAME#
 PROJECT CONTACT: #PROJECT CONTACT#
 DATE PLOTTED: 3/23/2018 11:59:25 AM
 FILE NAME: 86140100-sh1-pv1joint-04.dgn
 PLOT DRIVER: IL_paf.dwt
 PEN TABLE: PLOTLABEL.tbl



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISIONS -	
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISIONS -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISIONS -

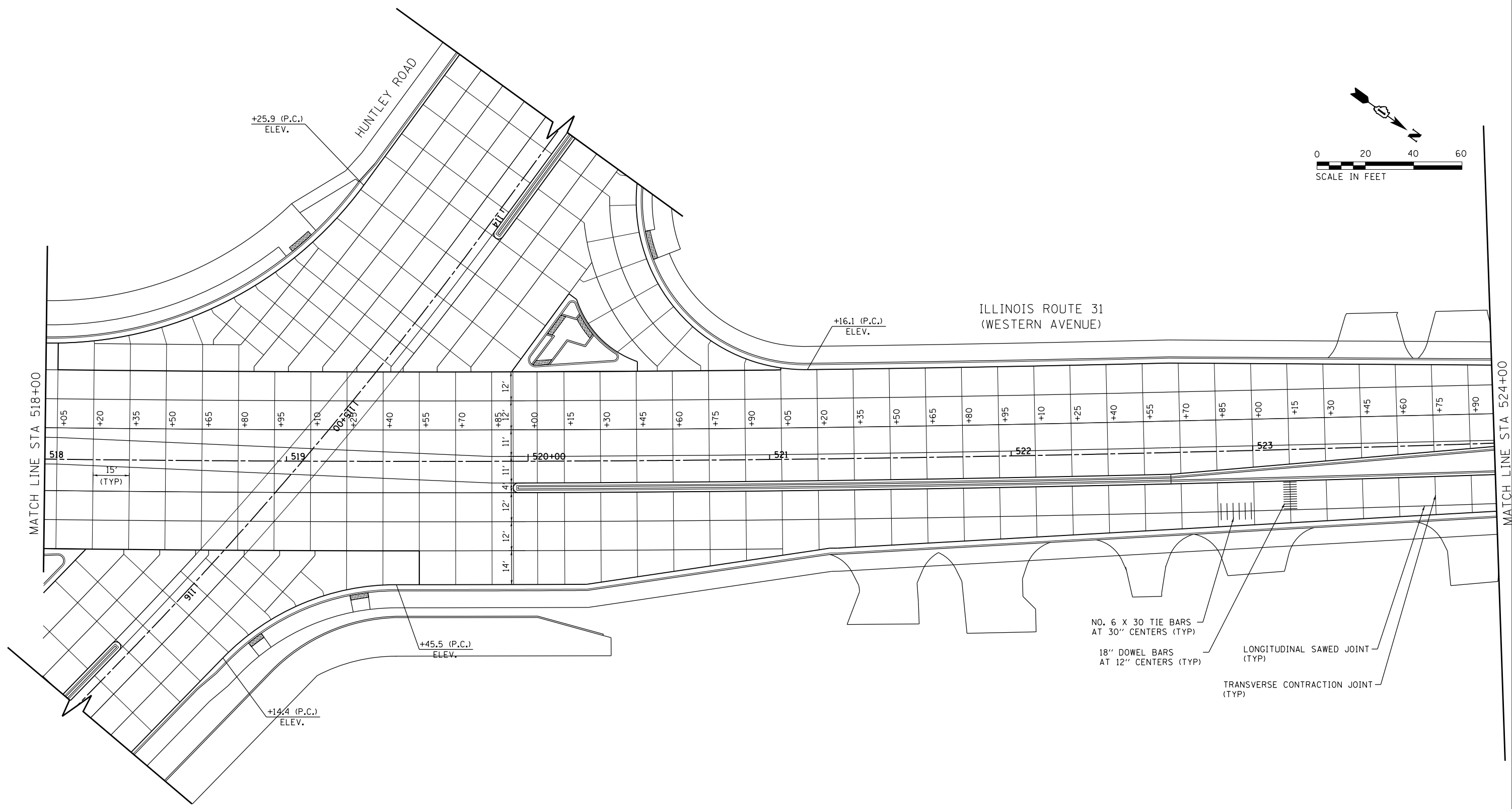
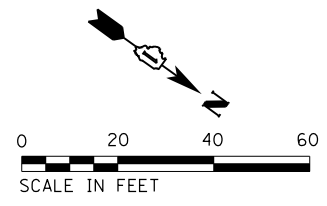
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT JOINTING PLAN
 ILLINOIS ROUTE 31**

SCALE: 1"=20' SHEET NO. 4 OF 6 SHEETS STA. 512+00.00 TO STA. 518+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	106
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				61E73

CONTRACT NO. 61E73



NO. 6 X 30 TIE BARS
AT 30" CENTERS (TYP)

18" DOWEL BARS
AT 12" CENTERS (TYP)

LONGITUDINAL SAWED JOINT
(TYP)

TRANSVERSE CONTRACTION JOINT
(TYP)

NOTES:

- JOINT LAYOUT MAY BE MODIFIED UPON APPROVAL BY ENGINEER TO INCORPORATE CHANGES DURING CONSTRUCTION.
- REINFORCING STEEL IS NOT SHOWN ALONG EVERY JOINT BUT IS INTENDED TO BE PLACED ALONG EACH JOINT AS NEEDED.

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:59:28 AM
 FILE NAME: 86140100-sh1-pv1joint-05.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOT LABEL.tbl



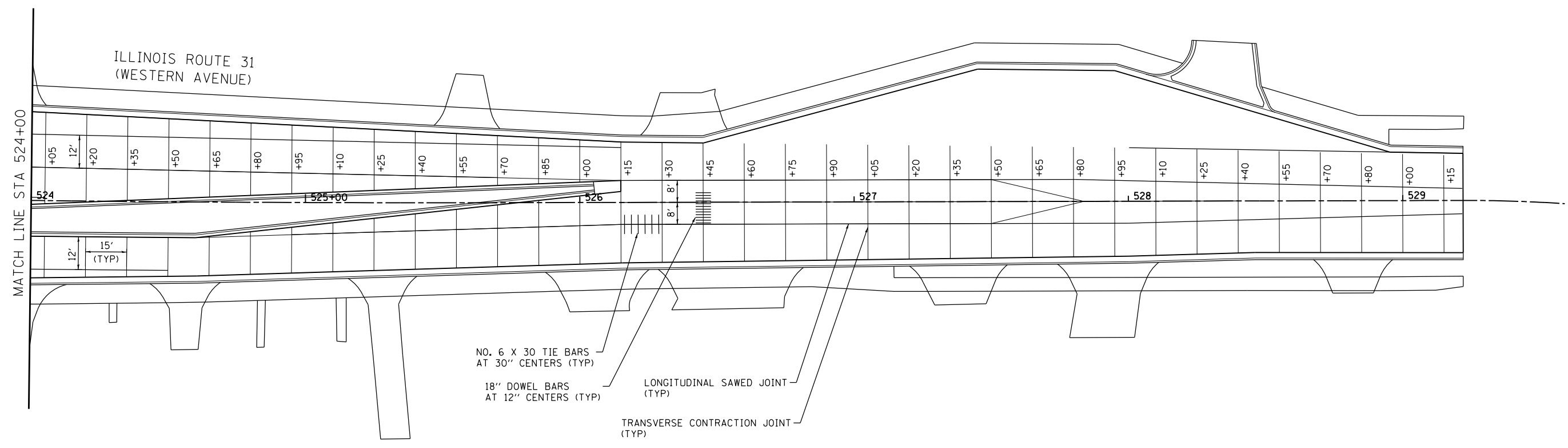
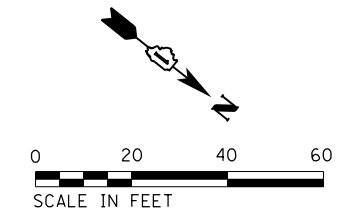
USER NAME = jmelhus	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT JOINTING PLAN
ILLINOIS ROUTE 31**

SCALE: 1"=20' SHEET NO. 5 OF 6 SHEETS STA. 518+00.00 TO STA. 524+00.00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	107
CONTRACT NO. 61E73			61E73	



NOTES:

1. JOINT LAYOUT MAY BE MODIFIED UPON APPROVAL BY ENGINEER TO INCORPORATE CHANGES DURING CONSTRUCTION.
2. REINFORCING STEEL IS NOT SHOWN ALONG EVERY JOINT BUT IS INTENDED TO BE PLACED ALONG EACH JOINT AS NEEDED.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:59:30 AM
 FILE NAME: 86140100-sh1-pv1joint-06.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOTTABLE.tbl

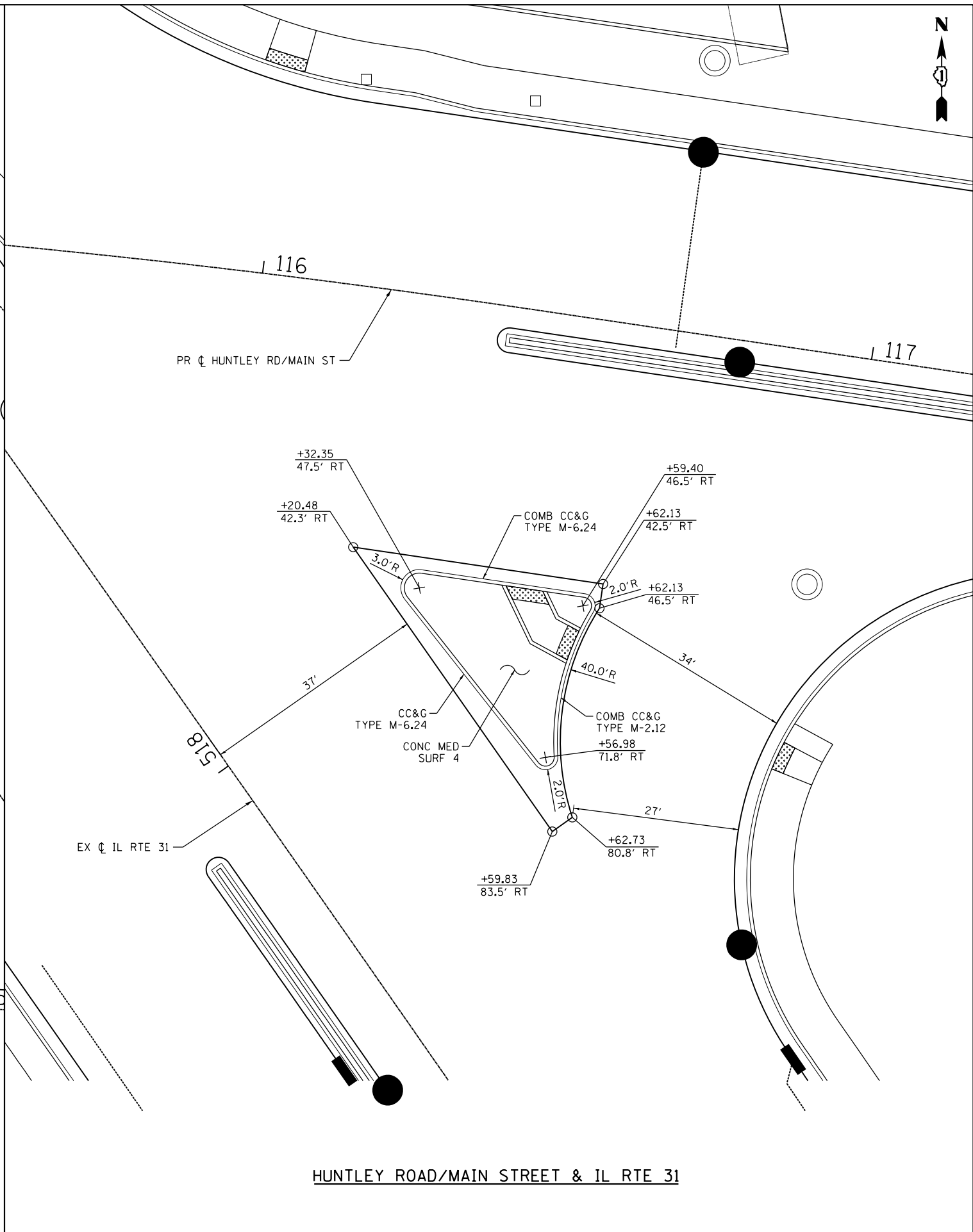
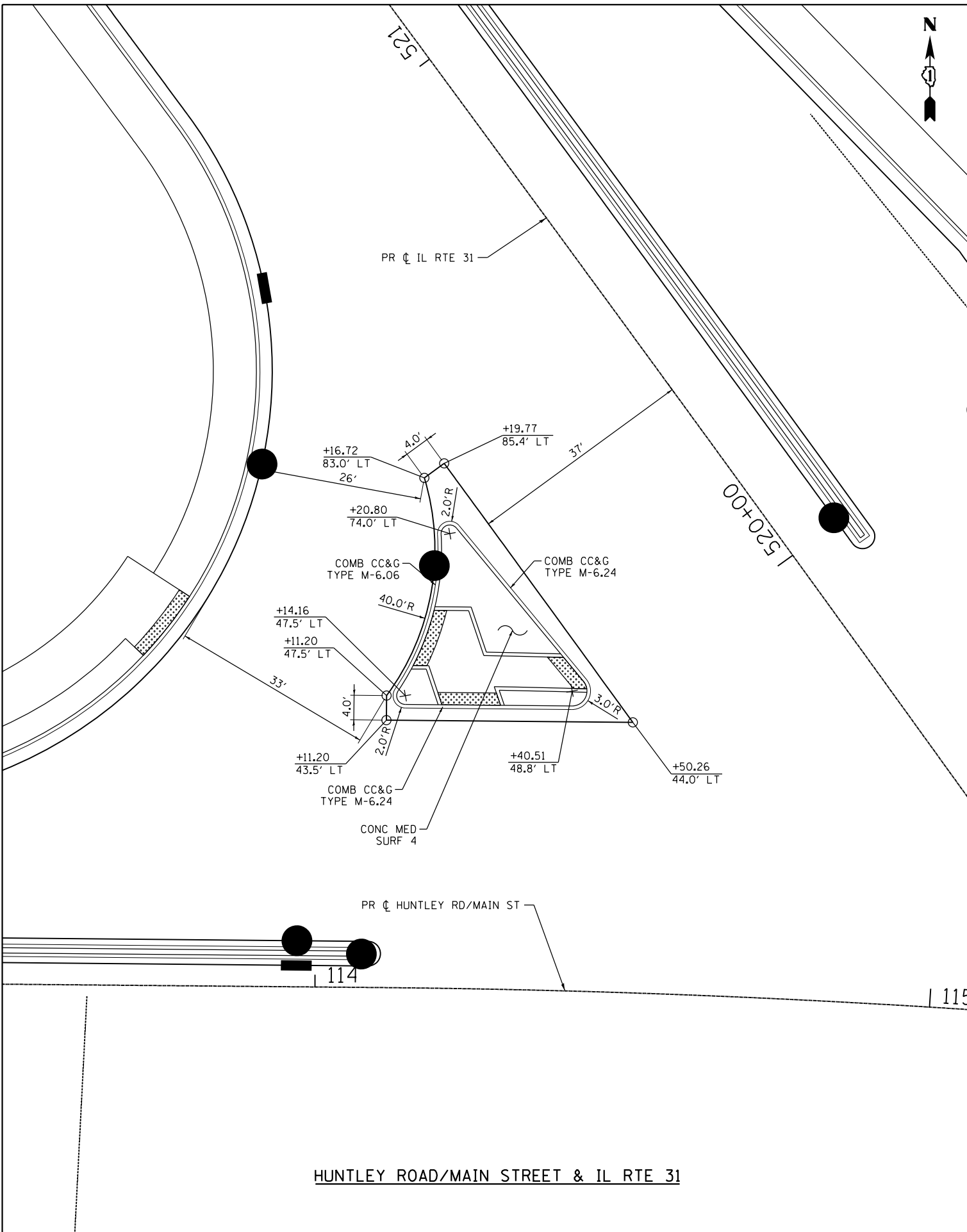


USER NAME = jmelhus	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISIONS -	
CHECKED - JRM	REVISIONS -	
DATE - 3/23/2018	REVISIONS -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT JOINTING PLAN ILLINOIS ROUTE 31		
SCALE: 1"=20'	SHEET NO. 6 OF 6 SHEETS	STA. 524+00.00 TO STA. 529+22.01

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	108
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				CONTRACT NO. 61E73



HUNTLEY ROAD/MAIN STREET & IL RTE 31

HUNTLEY ROAD/MAIN STREET & IL RTE 31

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:59:33 AM
 FILE NAME: 86140100-sh1-IslandDetails.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOT LABEL.tbl



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=10'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

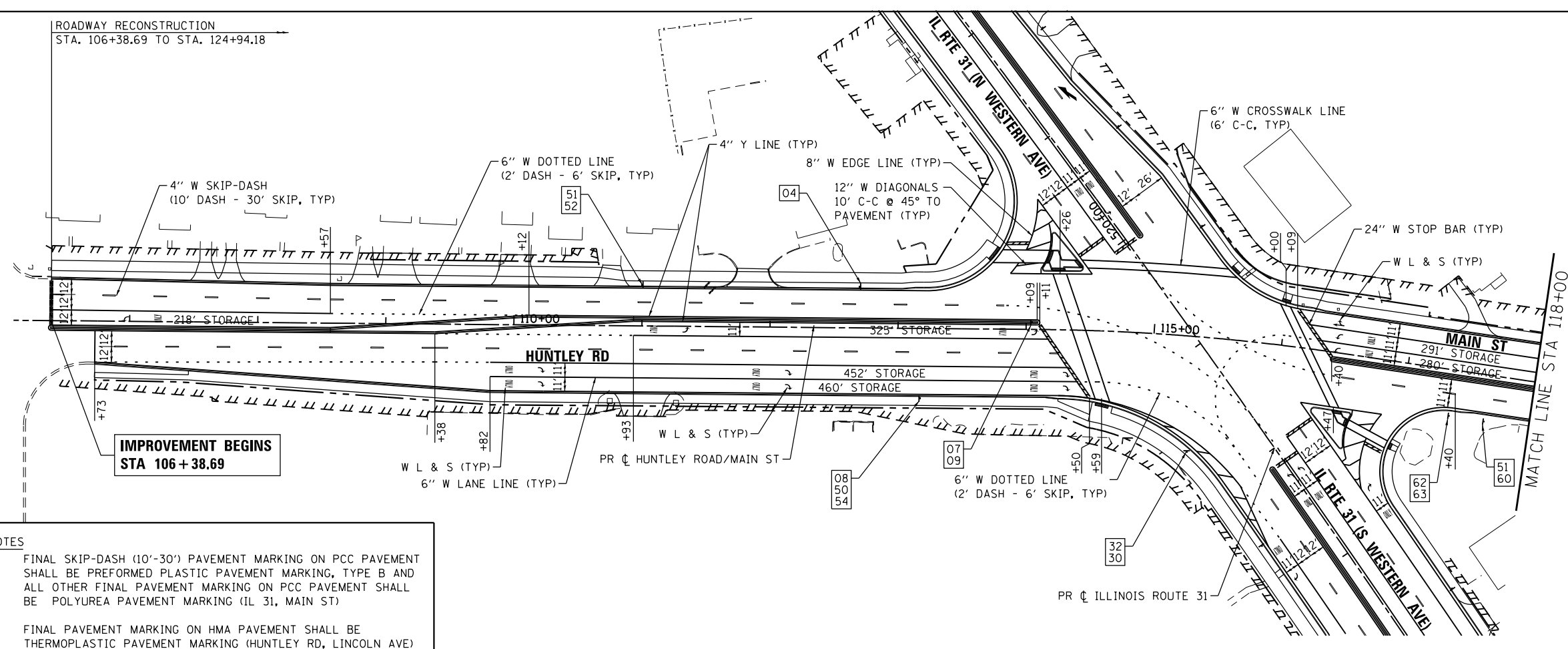
ISLAND DETAILS
 IL RTE 31 AND HUNTLEY ROAD/MAIN STREET

SCALE: 1"=10' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

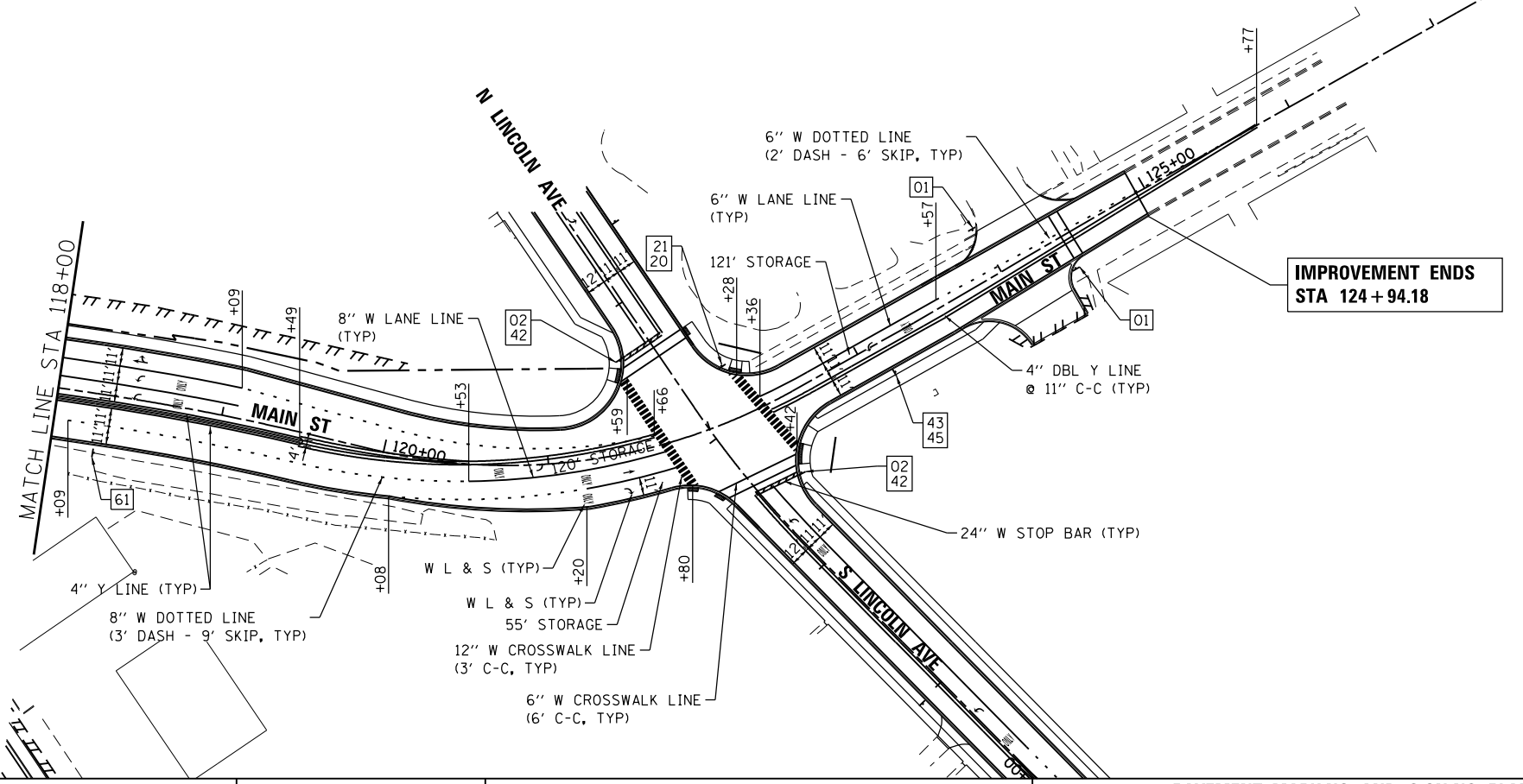
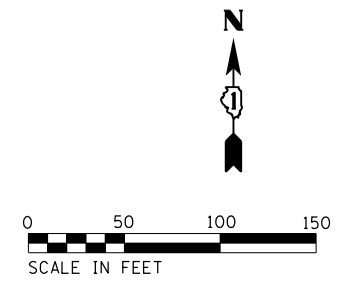
F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 109
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				61E73

86140100 File
 86140100-sh1-IslandDetails.dgn

ROADWAY RECONSTRUCTION
STA. 106+38.69 TO STA. 124+94.18



- NOTES**
- FINAL SKIP-DASH (10'-30') PAVEMENT MARKING ON PCC PAVEMENT SHALL BE PREFORMED PLASTIC PAVEMENT MARKING, TYPE B AND ALL OTHER FINAL PAVEMENT MARKING ON PCC PAVEMENT SHALL BE POLYUREA PAVEMENT MARKING (IL 31, MAIN ST)
 - FINAL PAVEMENT MARKING ON HMA PAVEMENT SHALL BE THERMOPLASTIC PAVEMENT MARKING (HUNTLEY RD, LINCOLN AVE)



COMPANY NAME: #COMPANY_NAME#
PROJECT CONTACT: #PROJECT_CONTACT#
CLIENT: #CLIENT#
DATE PLOTTED: 3/23/2018 11:39:36 AM
FILE NAME: 86140100-sht-pmk-01.dgn
PLOT NUMBER: 110115641179
PEN TABLE:



USER NAME = jmelhus	DESIGNED - JRM	REVISED -
FILE NAME = 86140100-sht-pmk-01.dgn	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=50'	CHECKED - TH	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

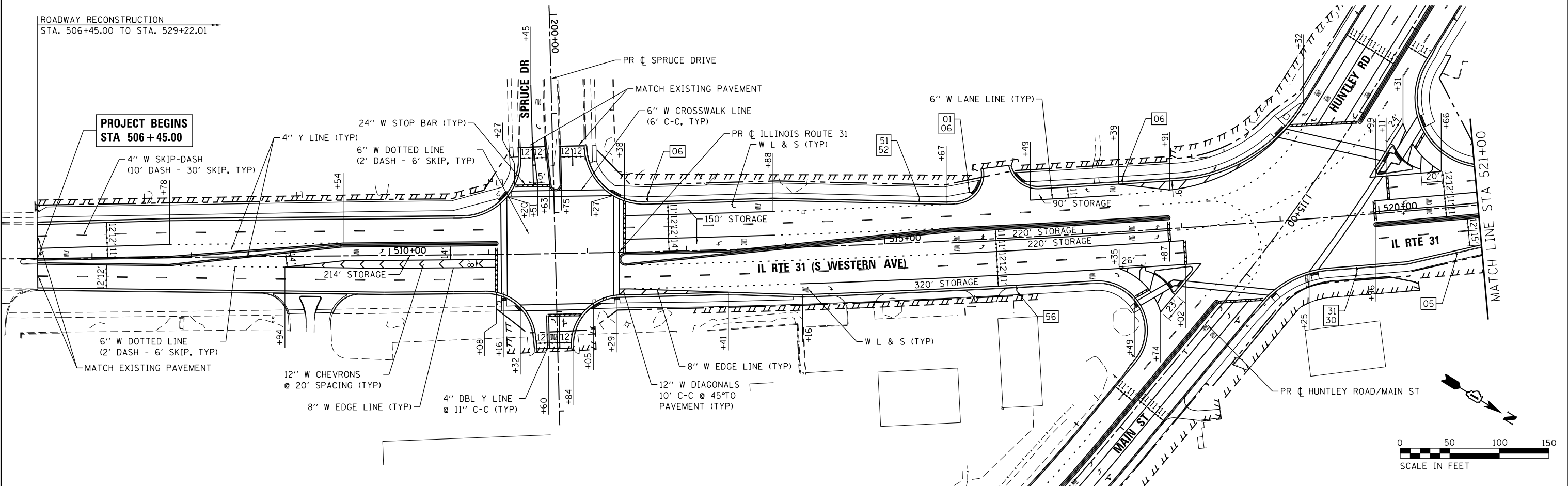
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND SIGNING PLAN
HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=50' SHEET NO. 1 OF 4 SHEETS STA. 106+38.69 TO STA. 124+94.18

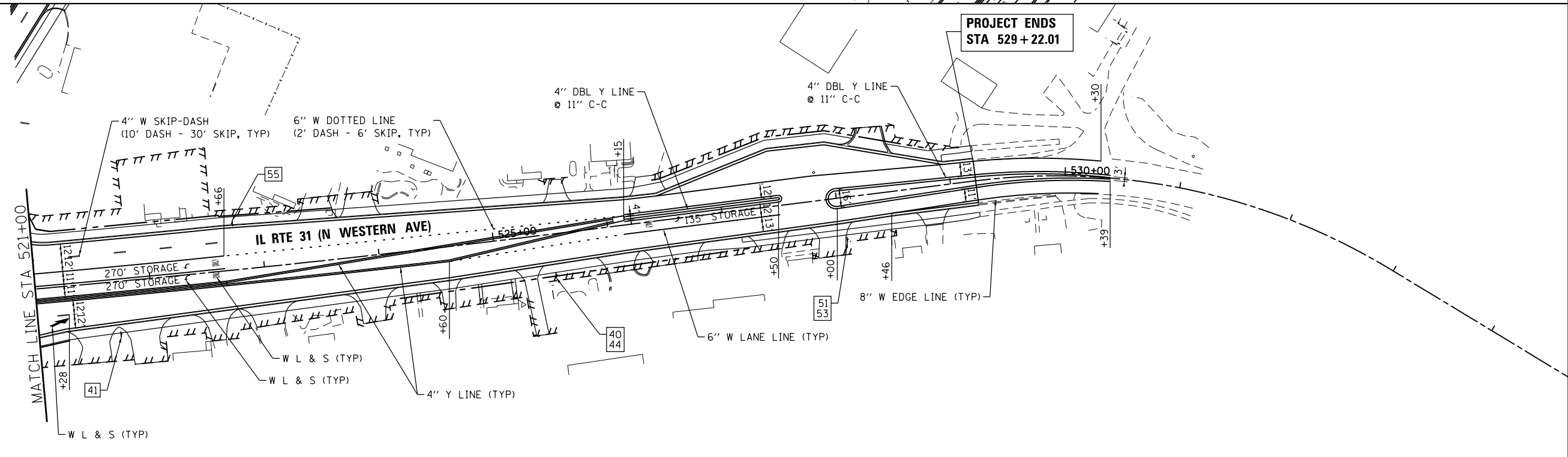
F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	110
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	CONTRACT NO. 61E73

ROADWAY RECONSTRUCTION
STA. 506+45.00 TO STA. 529+22.01



**PROJECT BEGINS
STA 506 + 45.00**

**PROJECT ENDS
STA 529 + 22.01**



- NOTES**
- FINAL SKIP-DASH (10'-30') PAVEMENT MARKING ON PCC PAVEMENT SHALL BE PREFORMED PLASTIC PAVEMENT MARKING, TYPE B AND ALL OTHER FINAL PAVEMENT MARKING ON PCC PAVEMENT SHALL BE POLYUREA PAVEMENT MARKING (IL 31, MAIN ST)
 - FINAL PAVEMENT MARKING ON HMA PAVEMENT SHALL BE THERMOPLASTIC PAVEMENT MARKING (HUNTLEY RD, LINCOLN AVE)

COMPANY NAME: HRGreen
PROJECT CONTACT: HRGreen
CLIENT: HRGreen
DATE PLOTTED: 3/23/2018 11:39:39 AM
FILE NAME: 86140100-sht-pmk-02.dgn
PLOT DATE: 3/23/2018
PEN TABLE: HRGreen



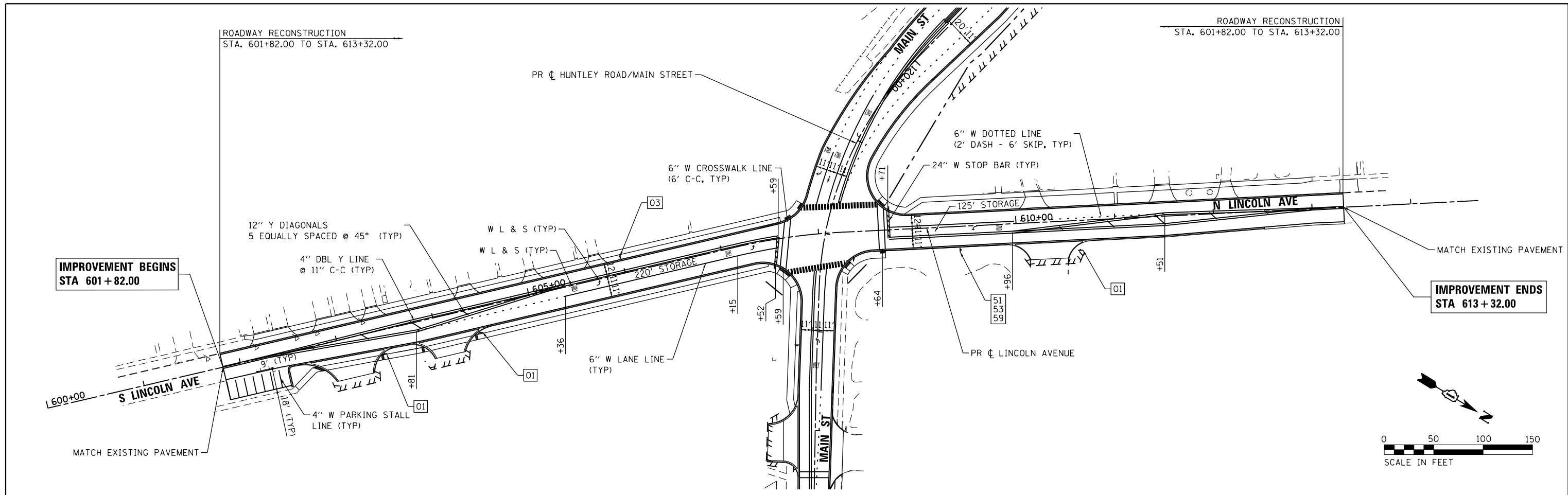
USER NAME = jmelhus	DESIGNED - JRM	REVISED -
FILE NAME = 86140100-sht-pmk-02.dgn	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=50'	CHECKED - TH	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND SIGNING PLAN
ILLINOIS ROUTE 31**

SCALE: 1"=50' SHEET NO. 2 OF 4 SHEETS STA. 506+45.00 TO STA. 529+22.01

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	111
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61E73	



- NOTES**
1. FINAL SKIP-DASH (10'-30') PAVEMENT MARKING ON PCC PAVEMENT SHALL BE PREFORMED PLASTIC PAVEMENT MARKING, TYPE B AND ALL OTHER FINAL PAVEMENT MARKING ON PCC PAVEMENT SHALL BE POLYUREA PAVEMENT MARKING (IL 31, MAIN ST)
 2. FINAL PAVEMENT MARKING ON HMA PAVEMENT SHALL BE THERMOPLASTIC PAVEMENT MARKING (HUNTLEY RD, LINCOLN AVE)

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 3/23/2018 11:39:43 AM
 FILE NAME: 86140100-sht-pmk-03.dgn
 PLOT NUMBER: IL0145611719
 PEN TABLE: PLOTTABLE.DWG



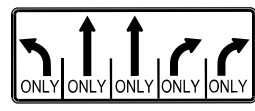
USER NAME = jmelhus	DESIGNED - JRM	REVISED -
FILE NAME = 86140100-sht-pmk-03.dgn	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=50'	CHECKED - TH	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND SIGNING PLAN
 LINCOLN AVENUE**

SCALE: 1"=50' SHEET NO. 3 OF 4 SHEETS STA. 602+10.00 TO STA. 613+00.98

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	112
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61E73	



01
R1-1
30" X 30"

02
R1-1
36" X 36"

03
R2-1
30" X 36"

04
R2-1
30" X 36"

05
R2-1
30" X 36"

06
R3-5R
30" X 36"

07
R3-5L
24" X 30"

08
R3-8e
54" X 30"

09
R4-7
24" X 30"



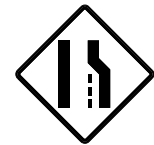
20
D3-1
24" X 9"

21
D3-1
24" X 9"

30
M1-1100
24" X 24"

31
M3-1
24" X 12"

32
M3-3
24" X 12"



40
W1-1R
36" X 36"

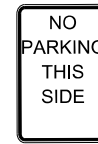
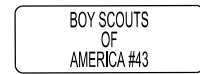
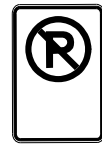
41
W4-2R
36" X 36"

42
W4-4p
24" X 12"

43
W11-2
36" X 36"

44
W13-1
24" X 24"

45
W16-9p
24" X 12"



50
CUSTOM
12" X 18"

51
CUSTOM
30" X 24"

52
CUSTOM
30" X 6"

53
CUSTOM
30" X 6"

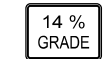
54
CUSTOM
12" X 18"

55
CUSTOM
24" X 30"

56
CUSTOM
24" X 30"

59
CUSTOM
12" X 18"

60
CUSTOM
30" X 6"



61
R3-7L
36" X 36"

62
W7-1
36" X 36"

63
W7-1
36" X 36"

NOTE: CUSTOM SIGNS ARE TO BE RELOCATED.

COMPANY NAME: HRGreen.com
PROJECT CONTACT: Illinois Professional Design Firm
DATE PLOTTED: 3/23/2018 11:59:46 AM
FILE NAME: 86140100-sh1-pmk-signs.dgn
PLOT DRIVER: ILpdf.plt
PEN TABLE: PLOTLABEL.tbl



USER NAME = jmelhus	DESIGNED - JRM	REVISED -
PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - TH	REVISED -
	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION


PAVEMENT MARKING AND SIGNING PLAN
SIGN LEGEND

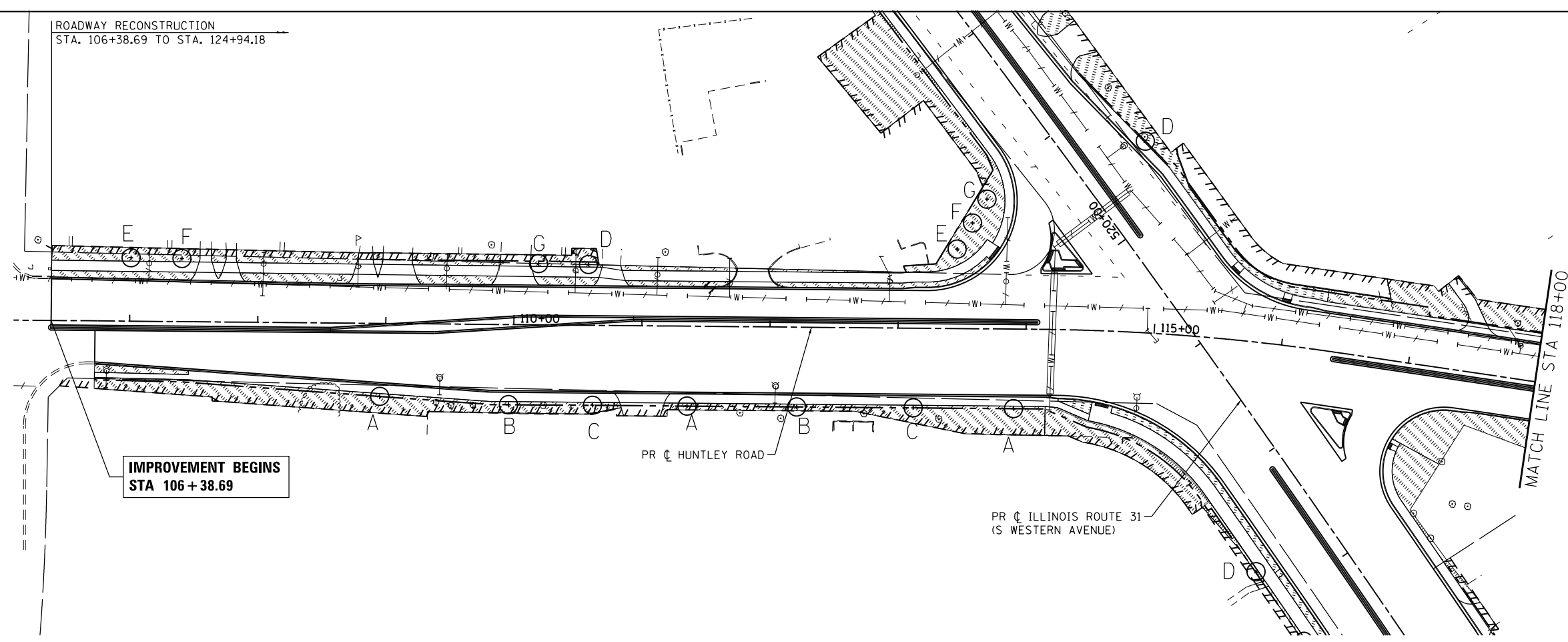
SCALE: N.T.S. SHEET NO. 4 OF 4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	113
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			61E73	

ROADWAY RECONSTRUCTION
STA. 106+38.69 TO STA. 124+94.18

LEGEND:

 SODDING, SALT TOLERANT TOPSOIL, FURNISH AND PLACE, 4"



TREE TYPE

A. TREE, CELTIS OCCIDENTALIS CHICAGOLAND, (CHICAGOLAND COMMON HACKBERRY), 2" CALIPER, BALLED AND BURLAPPED.

B. TREE, ACER X FREEMAN AUTUMN BLAZE (AUTUMN BLAZE FREEMAN MAPLE), 2" CALIPER, BALLED AND BURLAPPED.

C. TREE, GLEDITSIA TRIACANTHOS INERMIS (THORNLESS COMMON HONEYLOCUST), 2" CALIPER, BALLED AND BURLAPPED.

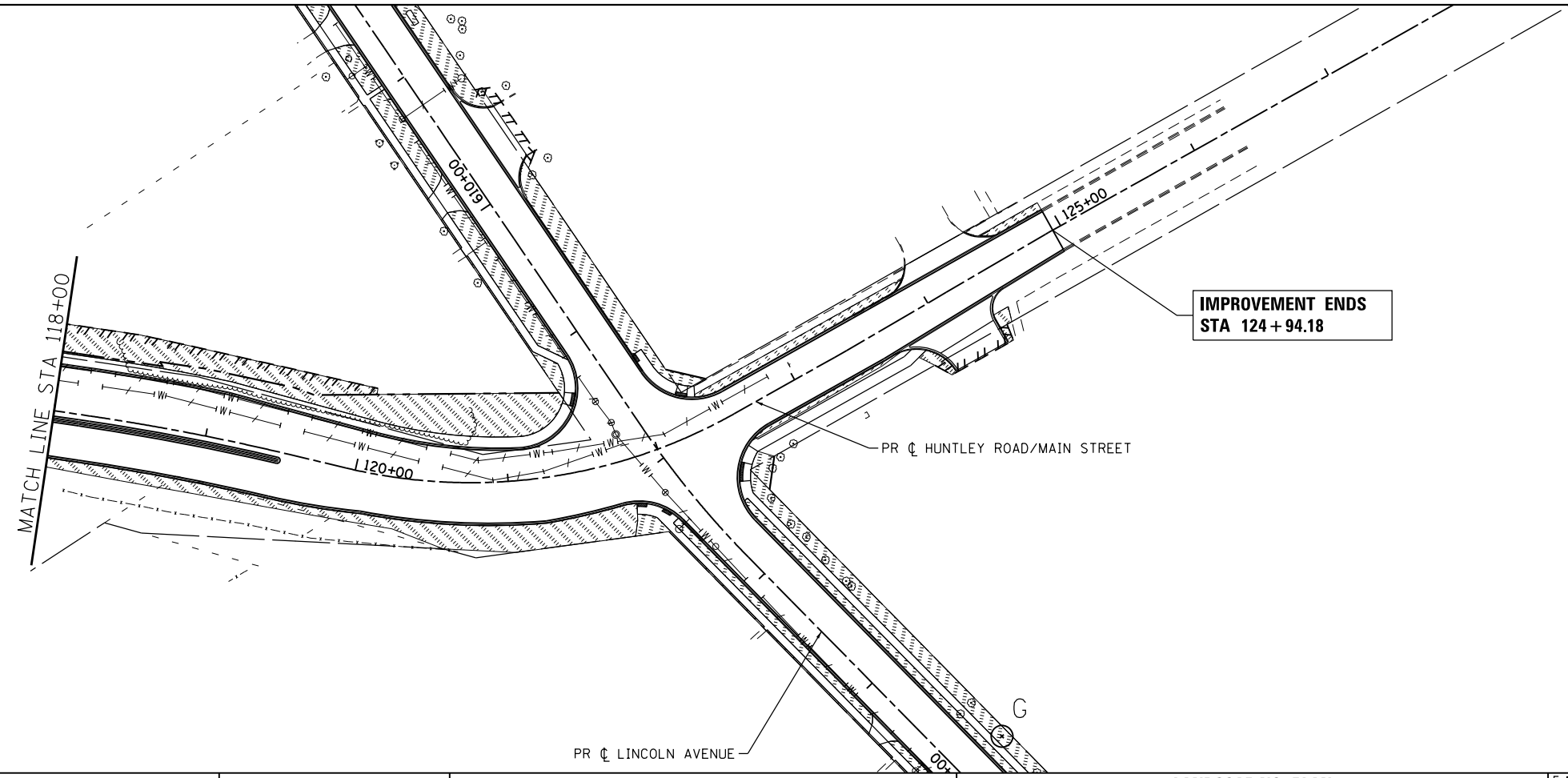
D. TREE, CERCIS CANADENSIS (EASTERN REDBUD), 2" CALIPER, TREE FORM, BALLED AND BURLAPPED.

E. TREE, MALUS PRAIRIFIRE (PRAIRIFIRE CRABAPPLE), 6' HEIGHT, CLUMP FORM, BALLED AND BURLAPPED.

F. TREE, ACER PALMATUM (JAPANESE RED MAPLE), 5' HEIGHT, BALLED AND BURLAPPED.

G. TREE, SYRINGA PEKINENSIS (PEKING LILAC), 2-1/2" CALIPER, TREE FORM, BALLED AND BURLAPPED.

Tree Type	STA	OFFSET
A	108+95.78	56.9
A	111+35.75	62.4
A	113+90.7	62.1
B	109+96.37	62.5
B	112+21.37	62.3
B	515+47.2	-57.4
C	110+61.91	62.4
C	113+12.39	64.2
C	514+91.68	-60.6
D	517+28.75	-67.3
D	110+57.96	-47.5
D	520+52.71	57.5
E	107+00.66	-49.5
E	113+45.59	-62.1
E	516+62.48	-67.4
F	107+40.38	-49.3
F	113+57.41	-82.5
F	527+49.98	-59.7
G	110+18.91	-47.8
G	113+68.56	-101.2
G	605+16.04	35.9



COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
CLIENT: HRGreen.com
DATE PLOTTED: 3/23/2018 11:39:49 AM
FILE NAME: 86140100-sht-1sp-01.dgn
PLOT SCALE: 1"=50'
PEN TABLE: HRGreen.com

HRGreen
HRGreen.com
Micro Professional Design Firm
#184-001322

USER NAME = jmelhu	DESIGNED - JRM	REVISED -
FILE NAME = 86140100-sht-1sp-01.dgn	DRAWN - JCH	REVISED -
PLOT SCALE = 1"=50'	CHECKED - TH	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

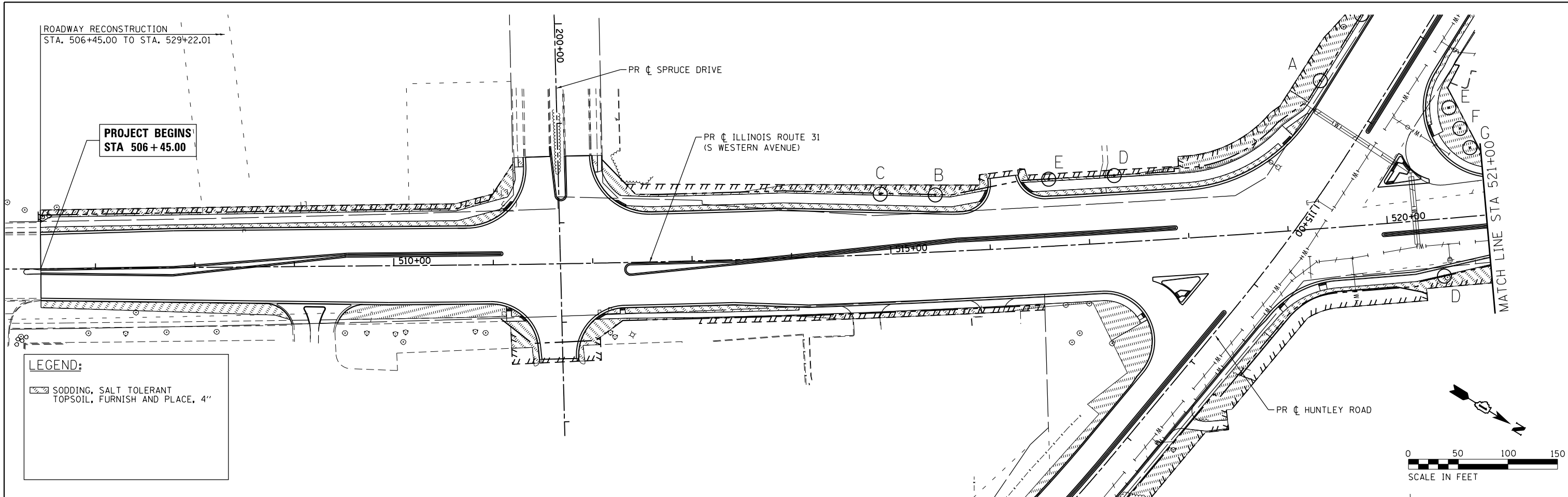
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING PLAN
HUNTLEY ROAD/MAIN STREET**

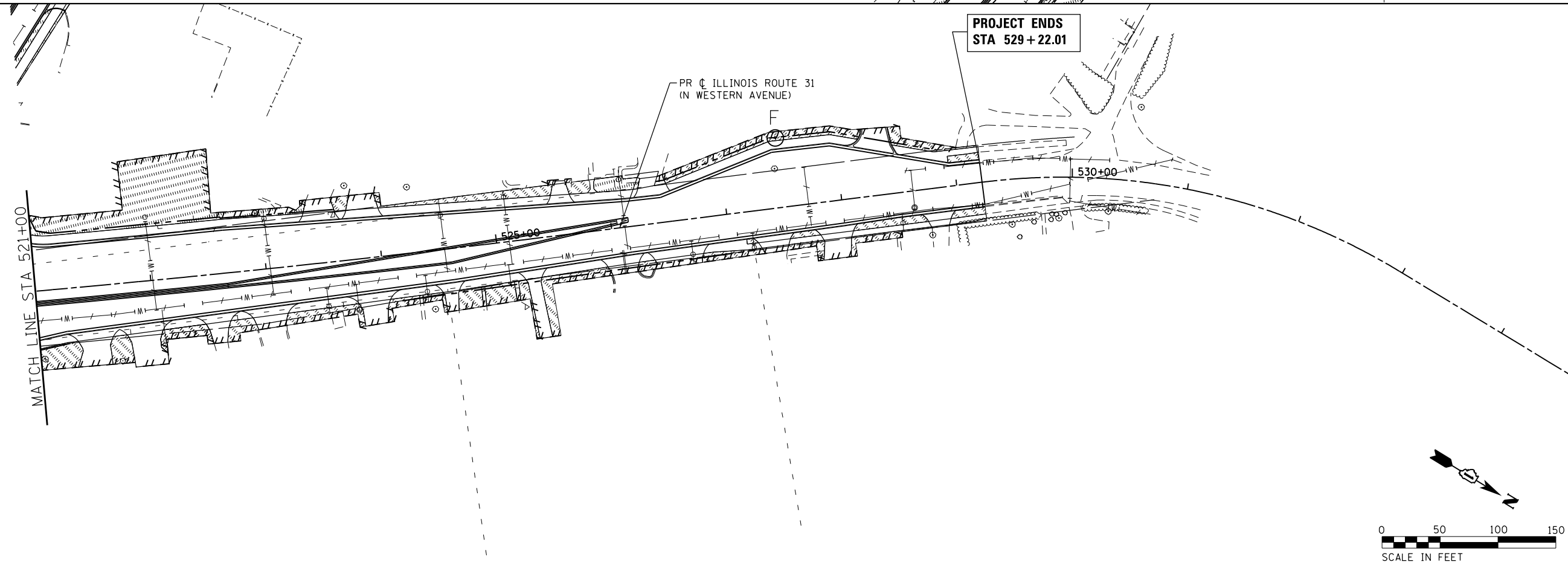
SCALE: 1"=50' SHEET NO. 1 OF 3 SHEETS STA. 106+38.69 TO STA. 124+94.18

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	114
CONTRACT NO.			61E73	

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



- TREE TYPE
- A. TREE, CECTIS OCCIDENTALIS CHICAGOLAND, (CHICAGOLAND COMMON HACKBERRY), 2" CALIPER, BALLED AND BURLAPPED.
 - B. TREE, ACER X FREEMAN AUTUMN BLAZE (AUTUMN BLAZE FREEMAN MAPLE), 2" CALIPER, BALLED AND BURLAPPED.
 - C. TREE, GLEDITSIA TRIACANTHOS INERMIS (THORNLESS COMMON HONEYLOCUST), 2" CALIPER, BALLED AND BURLAPPED.
 - D. TREE, CERCIS CANADENSIS (EASTERN REDBUD), 2" CALIPER, TREE FORM, BALLED AND BURLAPPED
 - E. TREE, MALUS PRAIRIFIRE (PRAIRIFIRE CRABAPPLE), 6' HEIGHT, CLUMP FORM, BALLED AND BURLAPPED
 - F. TREE, ACER PALMATUM (JAPANESE RED MAPLE), 5' HEIGHT, BALLED AND BURLAPPED
 - G. TREE, SYRINGA PEKINENSIS (PEKING LILAC), 2-1/2" CALIPER, TREE FORM, BALLED AND BURLAPPED



COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 3/23/2018 11:39:55 AM
 FILE NAME: 86140100-sht-1sp-02.dgn
 PLOT NUMBER: 1100145W01C19
 PEN TABLE: PLOTTABLE.ctb



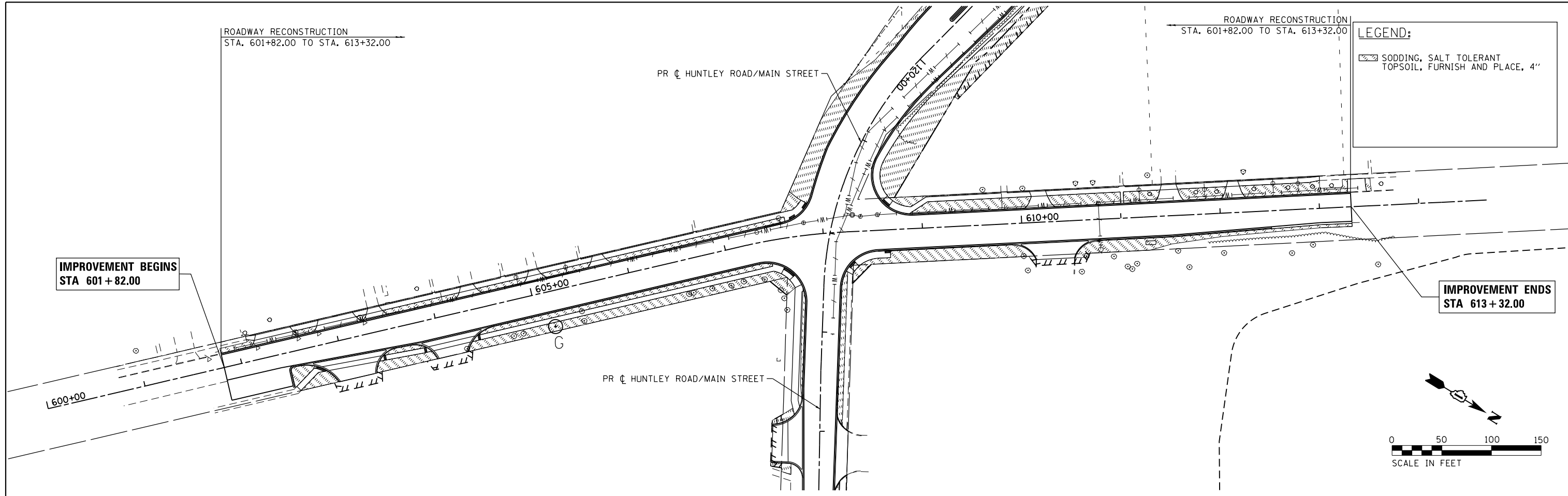
USER NAME = jmelhui	DESIGNED - JRM	REVISED -
FILE NAME = 86140100-sht-1sp-02.dgn	DRAWN - JCH	REVISED -
PLOT SCALE = 1"=50'	CHECKED - TH	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING PLAN
 ILLINOIS ROUTE 31**

SCALE: 1"=50' SHEET NO. 2 OF 3 SHEETS STA. 506+45.00 TO STA. 529+22.01

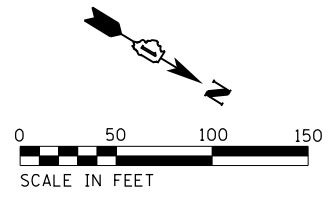
F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	115
CONTRACT NO.			61E73	
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



IMPROVEMENT BEGINS
STA 601 + 82.00

IMPROVEMENT ENDS
STA 613 + 32.00

LEGEND:
 SODDING, SALT TOLERANT TOPSOIL, FURNISH AND PLACE, 4"



TREE TYPE

- A. TREE, CELTIS OCCIDENTALIS CHICAGOLAND, (CHICAGOLAND COMMON HACKBERRY), 2" CALIPER, BALLED AND BURLAPPED.
- B. TREE, ACER X FREEMAN AUTUMN BLAZE (AUTUMN BLAZE FREEMAN MAPLE), 2" CALIPER, BALLED AND BURLAPPED.
- C. TREE, GLEDTSIA TRIACANTHOS INERMIS (THORNLESS COMMON HONEYLOCUST), 2" CALIPER, BALLED AND BURLAPPED.
- D. TREE, CERCIS CANADENSIS (EASTERN REDBUD), 2" CALIPER, TREE FORM, BALLED AND BURLAPPED.
- E. TREE, MALUS PRAIRIFIRE (PRAIRIFIRE CRABAPPLE), 6' HEIGHT, CLUMP FORM, BALLED AND BURLAPPED.
- F. TREE, ACER PALMATUM (JAPANESE RED MAPLE), 5' HEIGHT, BALLED AND BURLAPPED.
- G. TREE, SYRINGA PEKINENSIS (PEKING LILAC), 2-1/2" CALIPER, TREE FORM, BALLED AND BURLAPPED.

Tree Type	STA	OFFSET
A	108+95.78	55.9
A	111+35.75	61.5
A	113+90.7	61.2
B	109+96.37	61.3
B	112+21.37	61.8
B	114+50.28	73.7
C	110+61.91	61.3
C	113+12.39	61.3
C	518+01.69	68.8
D	517+37.5	-52.9
D	110+57.96	-47.5
D	115+34.27	-93.9
E	107+00.66	-49.5
E	113+45.59	-62.1
E	516+78.63	54.4
F	107+40.38	-49.3
F	113+57.41	-82.5
F	527+49.98	-59.7
G	110+18.91	-47.8
G	113+68.56	-101.2
G	605+16.04	35.9

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 3/23/2018 11:39:59 AM
 FILE NAME: 86140100-sht-1sp-03.dgn
 PLOT NUMBER: 11071461119
 PLOT TABLE:

HRGreen.com
 Illinois Professional Design Firm
 #184-001322

USER NAME = jmelhui	DESIGNED - JRM	REVISED -
FILE NAME = 86140100-sht-1sp-03.dgn	DRAWN - JCH	REVISED -
PLOT SCALE = 1"=50'	CHECKED - TH	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING PLAN
 LINCOLN AVENUE**

SCALE: 1"=50' SHEET NO. 3 OF 3 SHEETS STA. 602+10.00 TO STA. 613+00.98

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	116
CONTRACT NO.			61E73	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND			SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND			SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED			RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM			FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
WOOD POLE			INTERSECTION ITEM			GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE		
GUY WIRE			REMOVE ITEM					
SIGNAL HEAD			RELOCATE ITEM					
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM					
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED					
FLASHER INSTALLATION -(FS) SOLAR POWERED			MAST ARM POLE AND FOUNDATION TO BE REMOVED					
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED					
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			DETECTOR LOOP, TYPE I					
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP					
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIRMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-1505a.dgn
 FILE NAME: 86140100-sh1-1505a.dgn
 PLOT DRIVER: IL_PCH_Plotter.ctb
 PEN TABLE: PLOTTABLE.tbl
TS SHT NO. 1

FILE NAME = ts05.dgn	USER NAME = lryss	DESIGNED - IP	REVISED -
Default		DRAWN - IP	REVISED -
		CHECKED - LP	REVISED -
		DATE - 9/29/2016	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

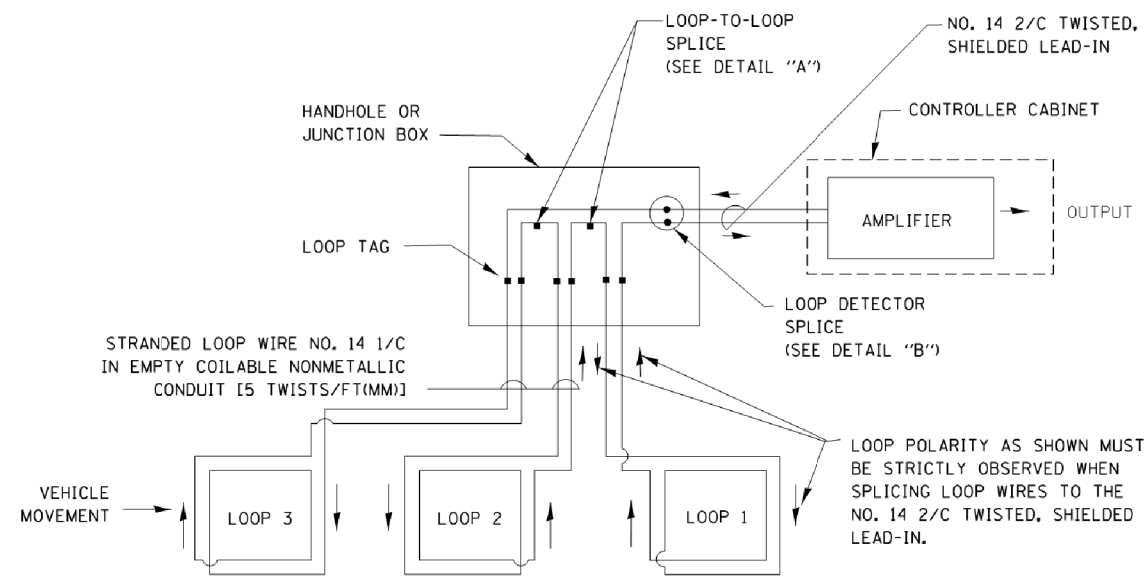
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 1 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	117
TS-05		CONTRACT NO. 61E73		
ILLINOIS FED. AID PROJECT				

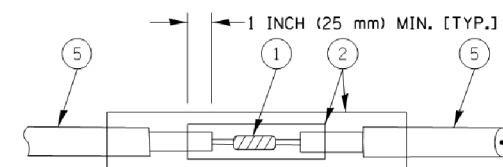
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

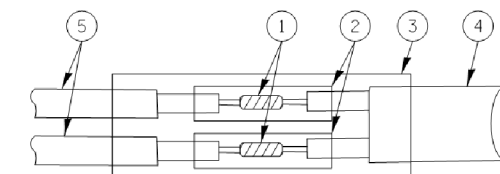


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

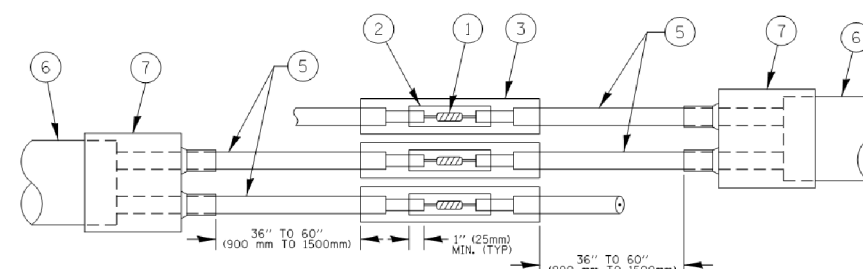


DETAIL "A"
LOOP-TO-LOOP SPLICE

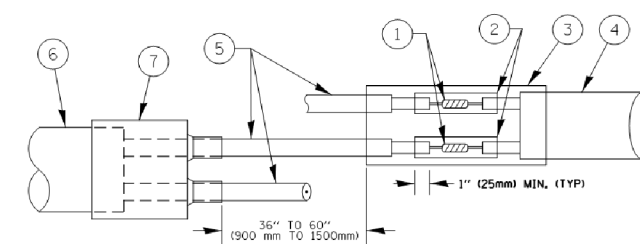


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

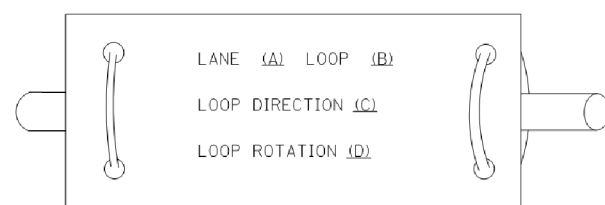


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PREFORMED LOOP

LOOP DETECTOR SPLICE

LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.

- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-d01108315\ts05.dgn
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
TS SHT NO. 2

FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
cs:\pwork\k\pwi\dot\footemj\d01108315\ts05.dgn		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

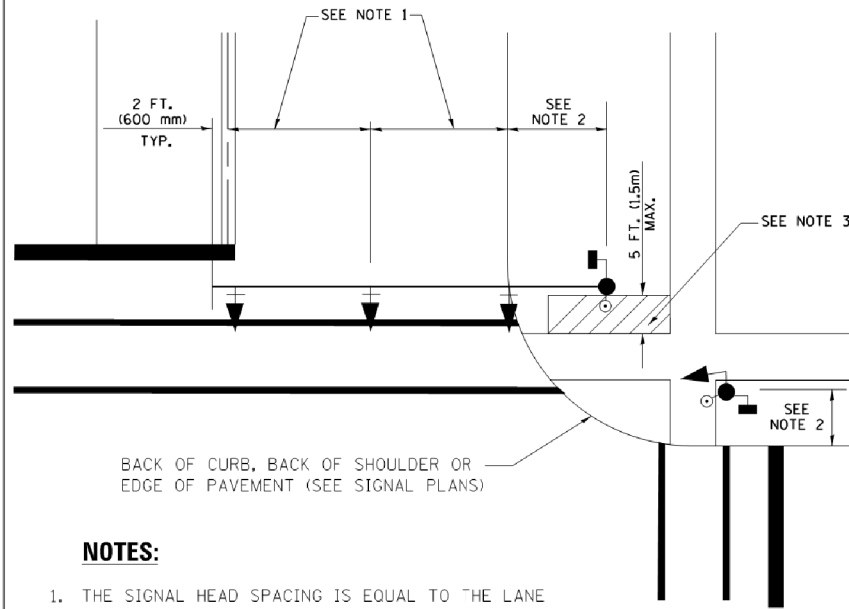
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	118
TS-05		CONTRACT NO. 61E73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

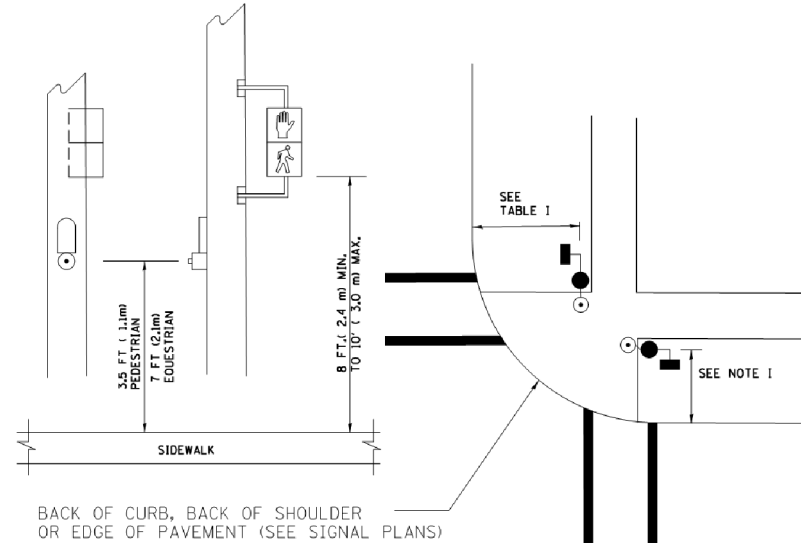
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

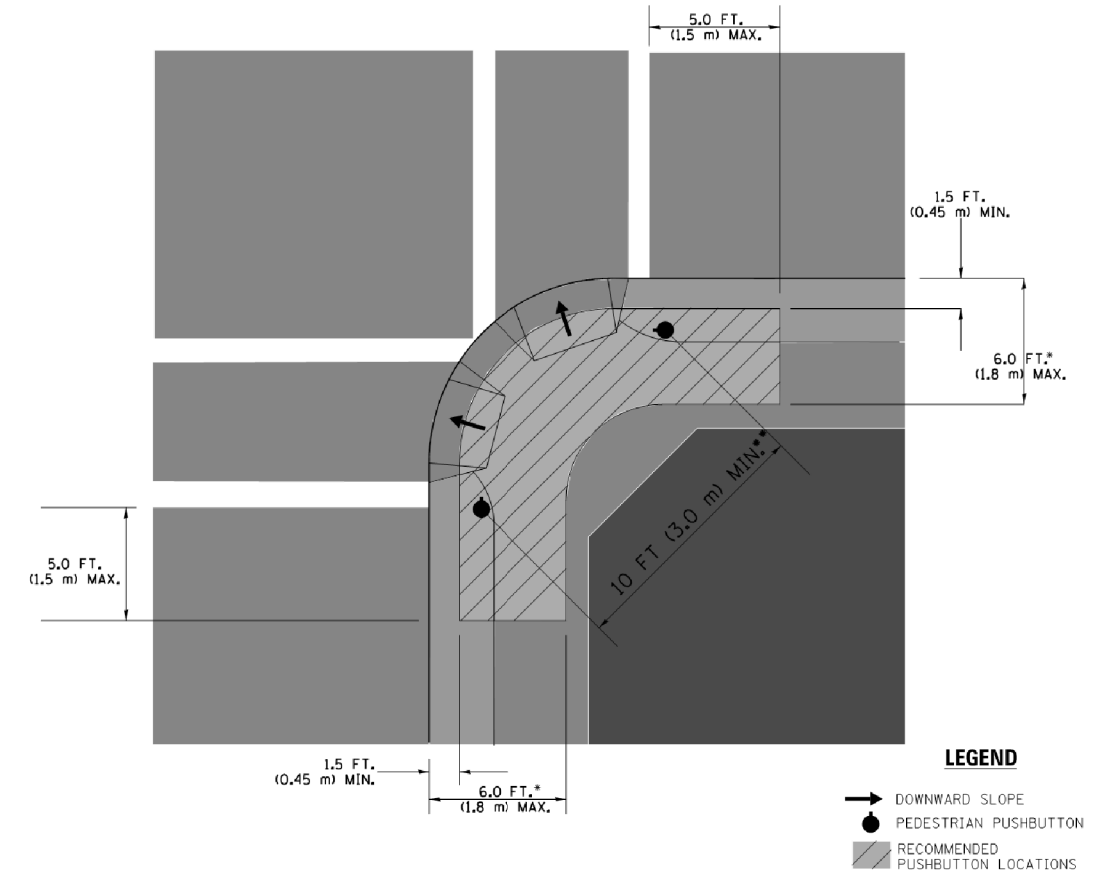
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- * WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- ** WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

COMPANY NAME: #PROJECT CONTACT #
PROJECT CONTACT: #CLIENT #
CLIENT: #FILE NAME #
FILE NAME: #PLOT DRIVER: #PLOT TABLE: #
PLOT DATE: #
TS SHT NO. 3

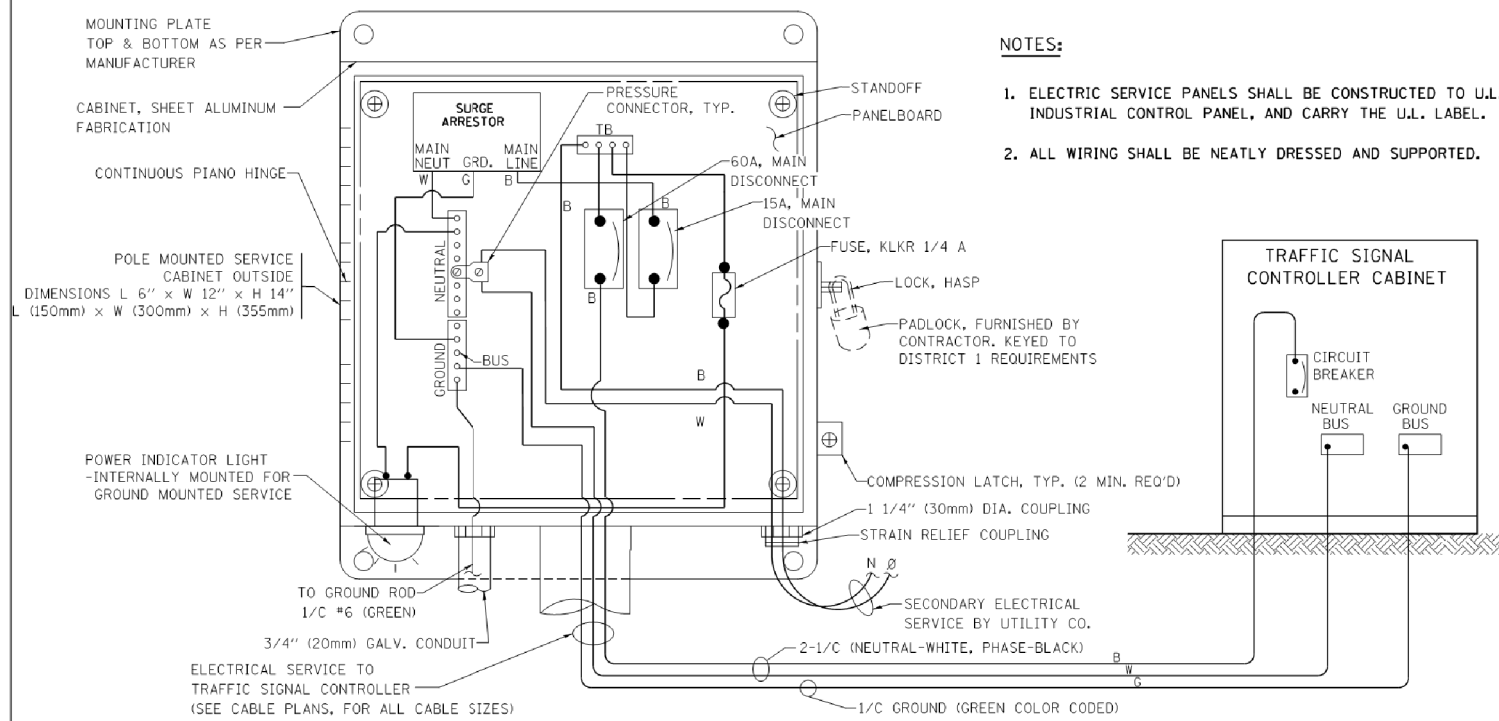
FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
es:\pwwork\p\dot\footemj\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

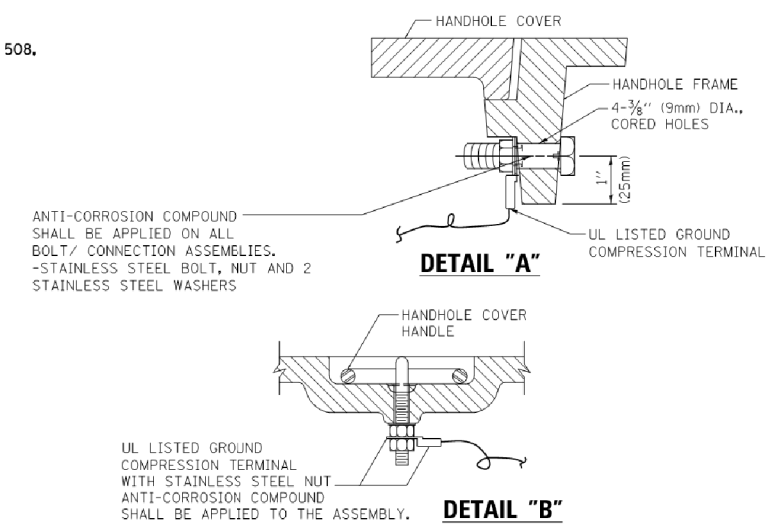
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	119
TS-05		CONTRACT NO.	61E73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 3 OF 7 SHEETS STA. TO STA.

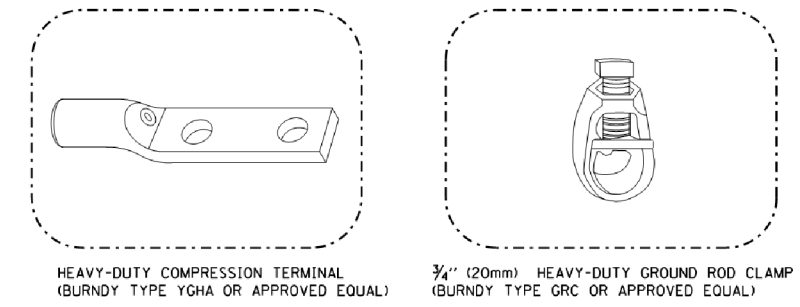
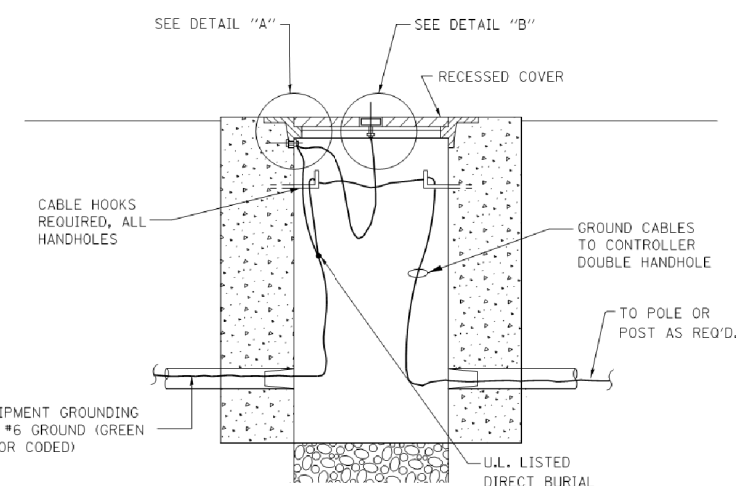


**ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)
SERVICE INSTALLATION POLE MOUNT (SHOWN)
(NOT TO SCALE)**

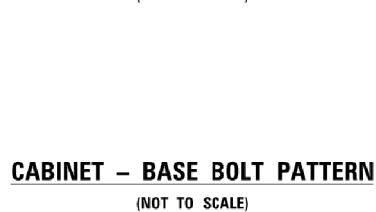
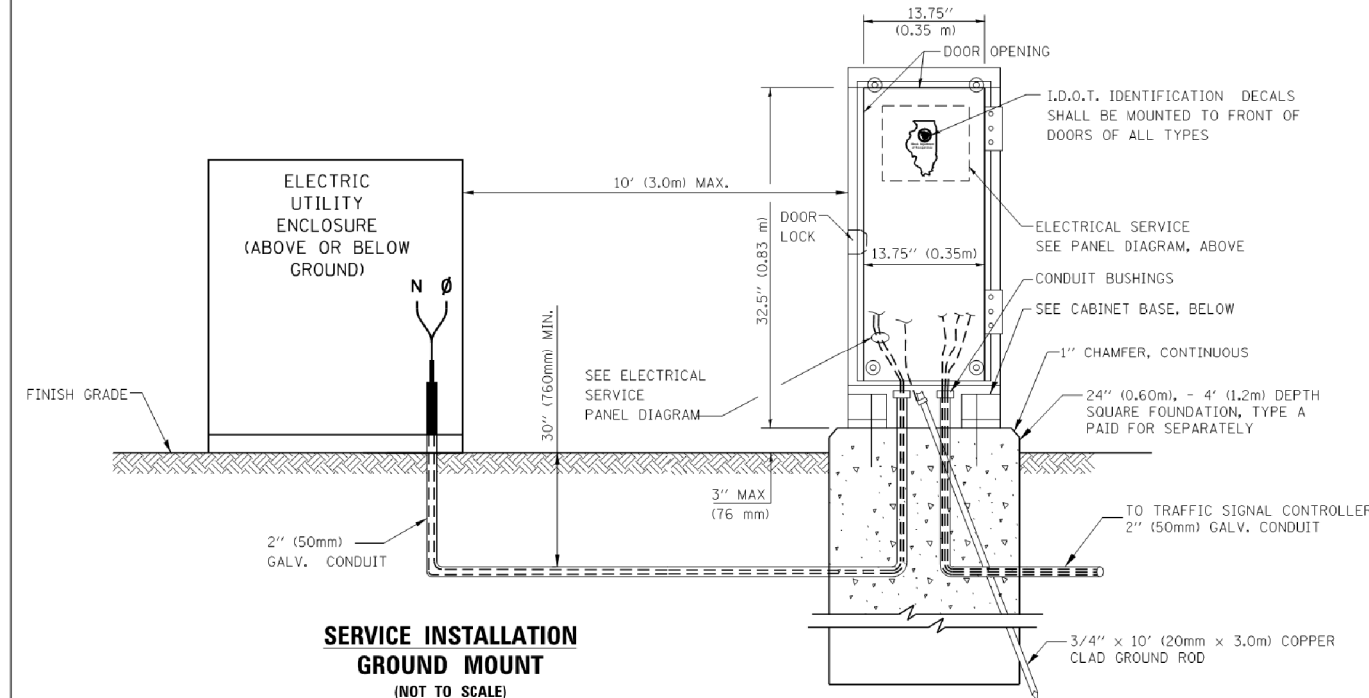
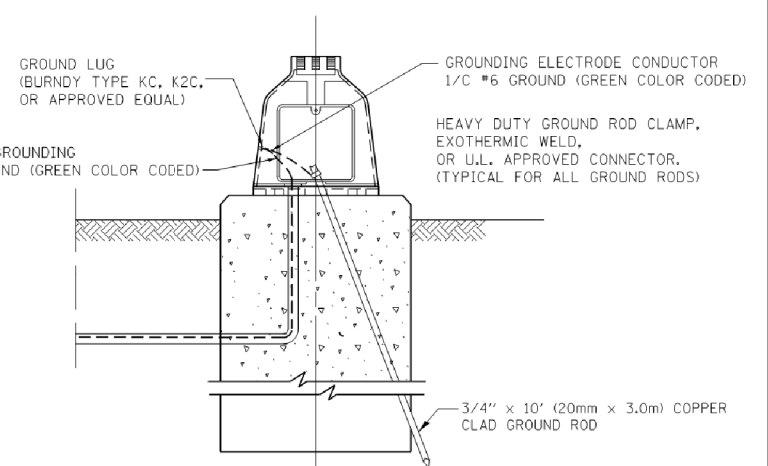
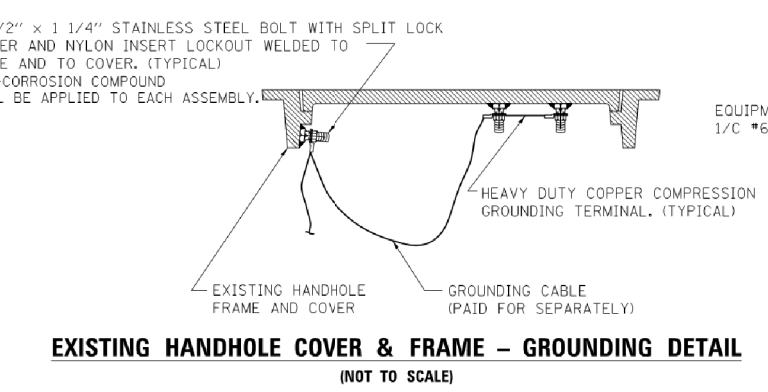


NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



COMPANY NAME: #COMPANY_NAME#
PROJECT CONTACT: #PROJECT_CONTACT#
CLIENT: #CLIENT#
FILE NAME: #FILE_NAME#
PLOT DRIVER: #PLOT_DRIVER#
PEN TABLE: #PEN_TABLE#
PLOT LABEL: #PLOT_LABEL#

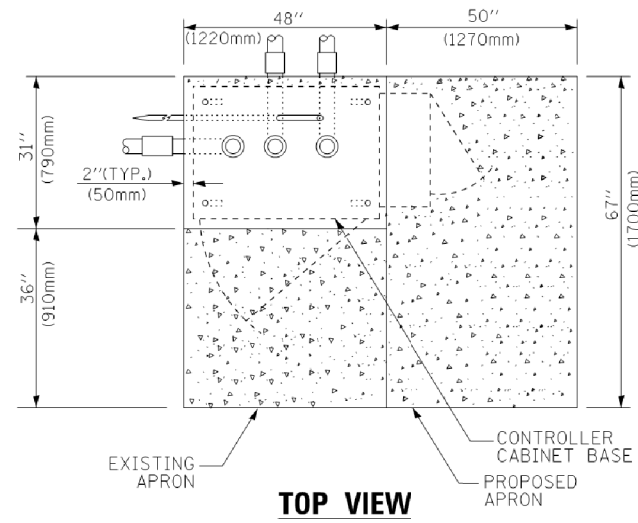
TS SHT NO. 4

FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
es:\pwork\pwork\footemj\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

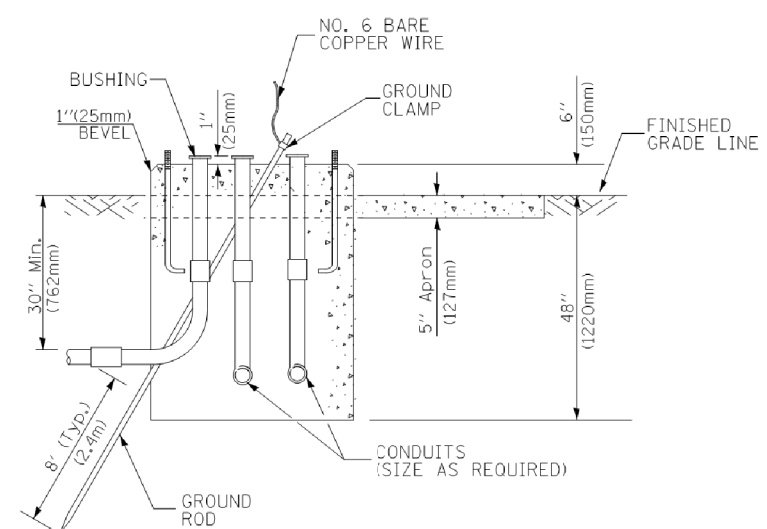
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET NO. 4 OF 7 SHEETS
STA.	TO STA.

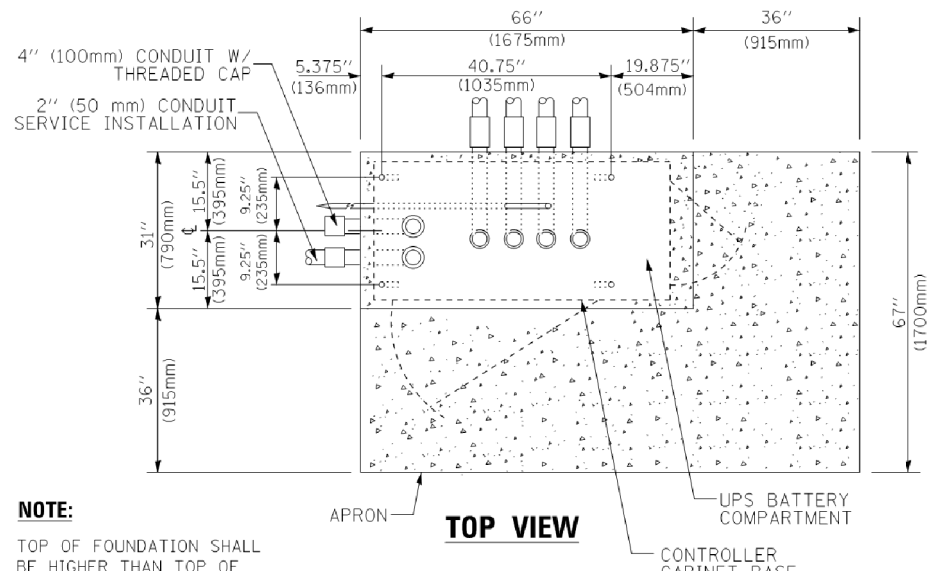
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	120
TS-05		CONTRACT NO.	61E73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TOP VIEW

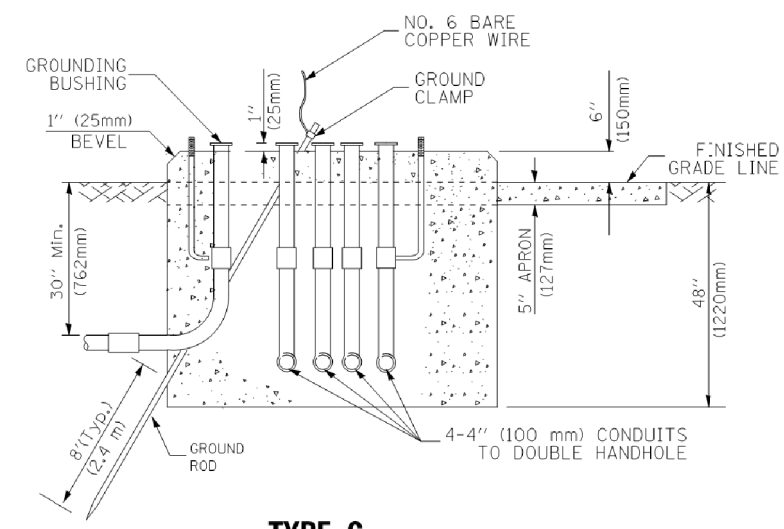


**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

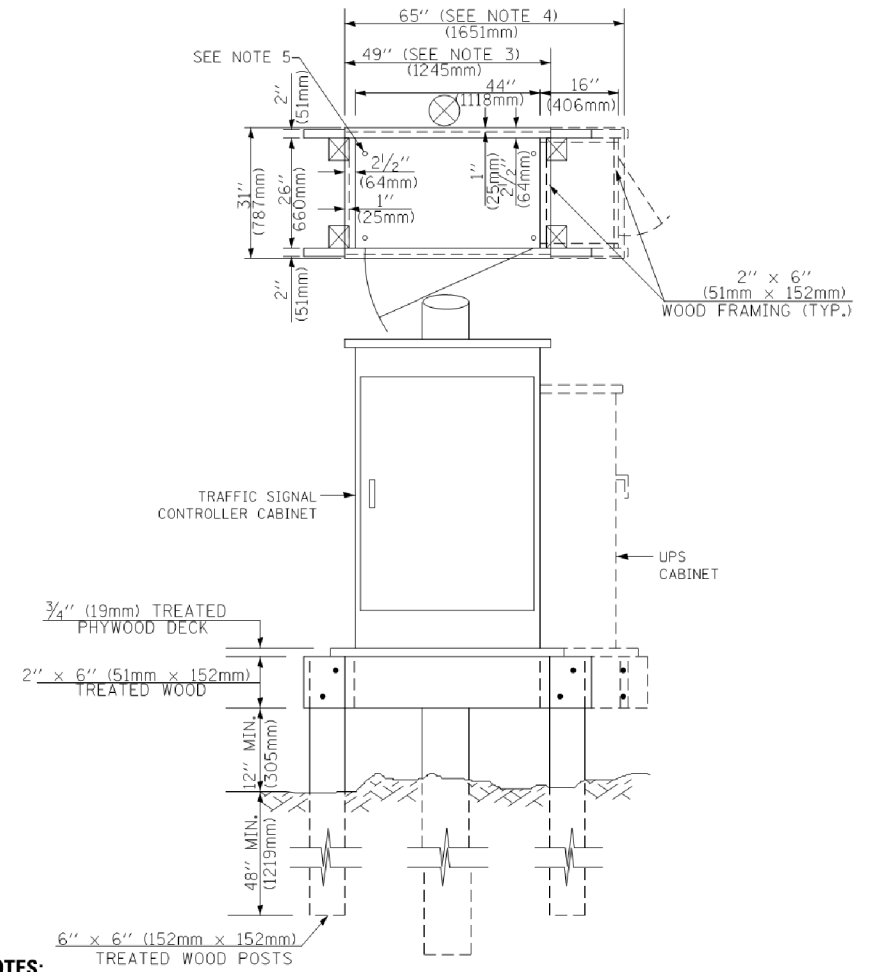


TOP VIEW

NOTE:
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

- BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
- PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
- PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
- DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
- FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

DEPTH OF FOUNDATION

MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

- These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
- Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
- Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
- For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

COMPANY NAME: #COMPANY NAME #
PROJECT CONTACT: #PROJECT CONTACT #
CLIENT: #CLIENT #
FILE NAME: 86140100-sh1-d0108315-ts05.dgn
PLOT DRIVER: ilpctch\pwr\ts05.dgn
PLOT TABLE: PLOT LABEL.tbl
FILE NAME: 86140100-sh1-d0108315-ts05.dgn
PLOT DRIVER: ilpctch\pwr\ts05.dgn
PLOT TABLE: PLOT LABEL.tbl

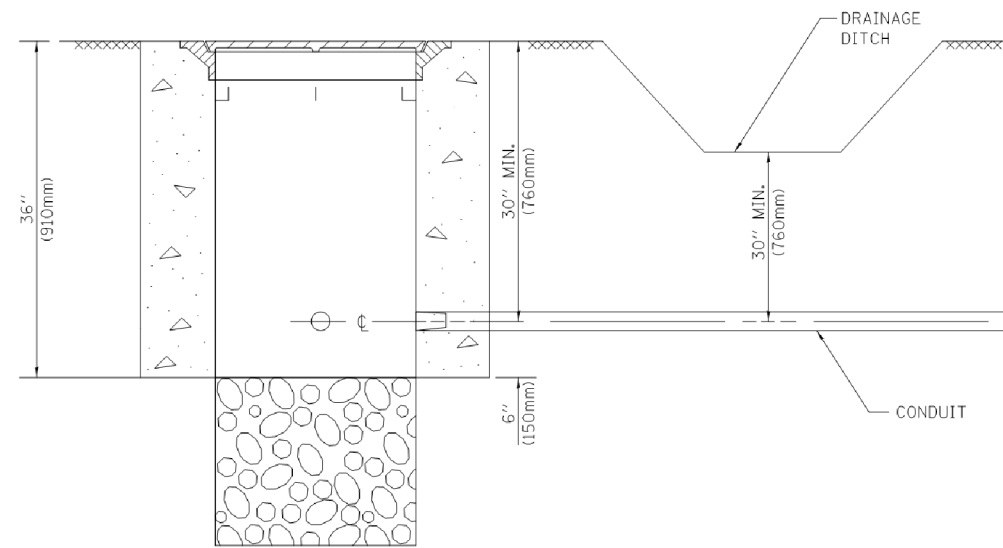
TS SHT NO. 5

FILE NAME =	USER NAME = footemj	DESIGNED - DAG	REVISED - DAG 1-1-14
cs:\pwr\work\pwr\dot\footemj\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
STANDARD TRAFFIC SIGNAL DESIGN DETAILS			
SCALE: NONE	SHEET NO. 5 OF 7 SHEETS	STA.	TO STA.

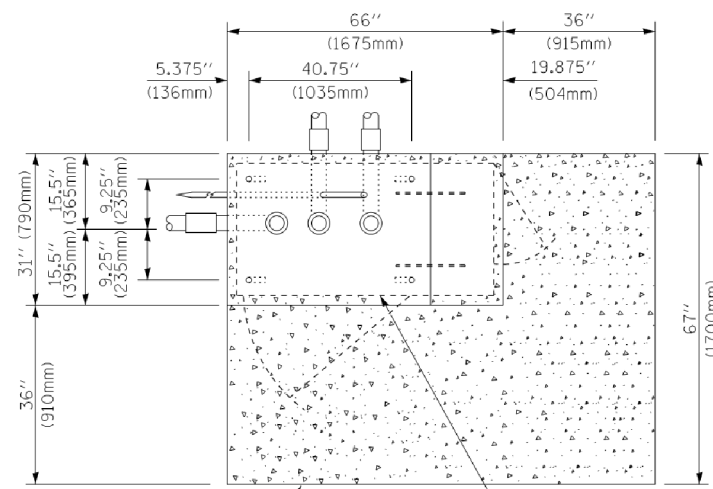
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	121
TS-05		CONTRACT NO. 61E73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



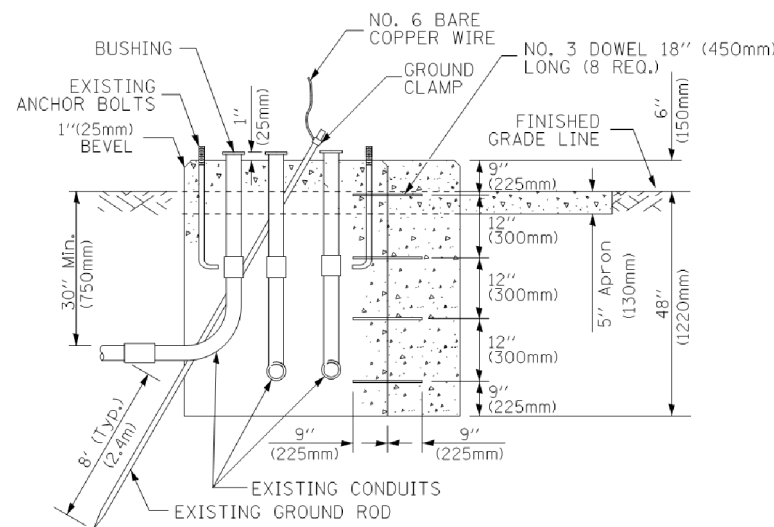
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



TOP VIEW
(NOT TO SCALE)

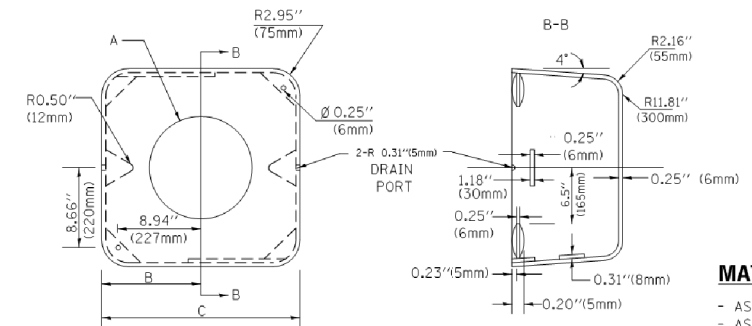


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0,000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-0-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIABLES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIABLES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIABLES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

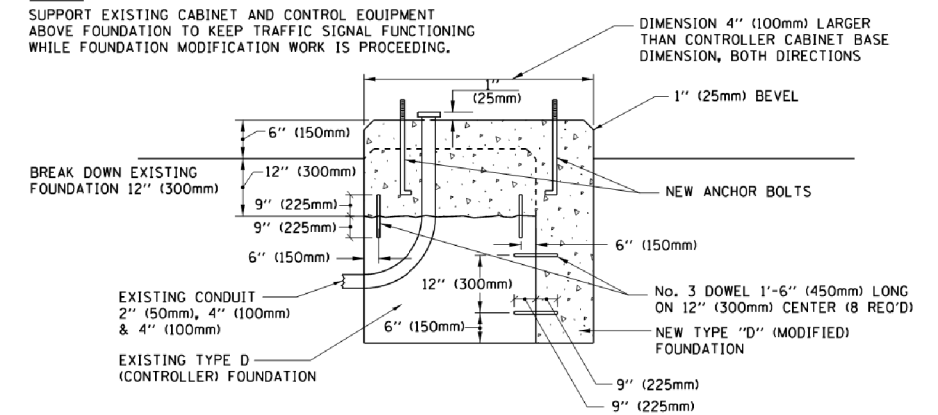
SHROUD

NOTES:

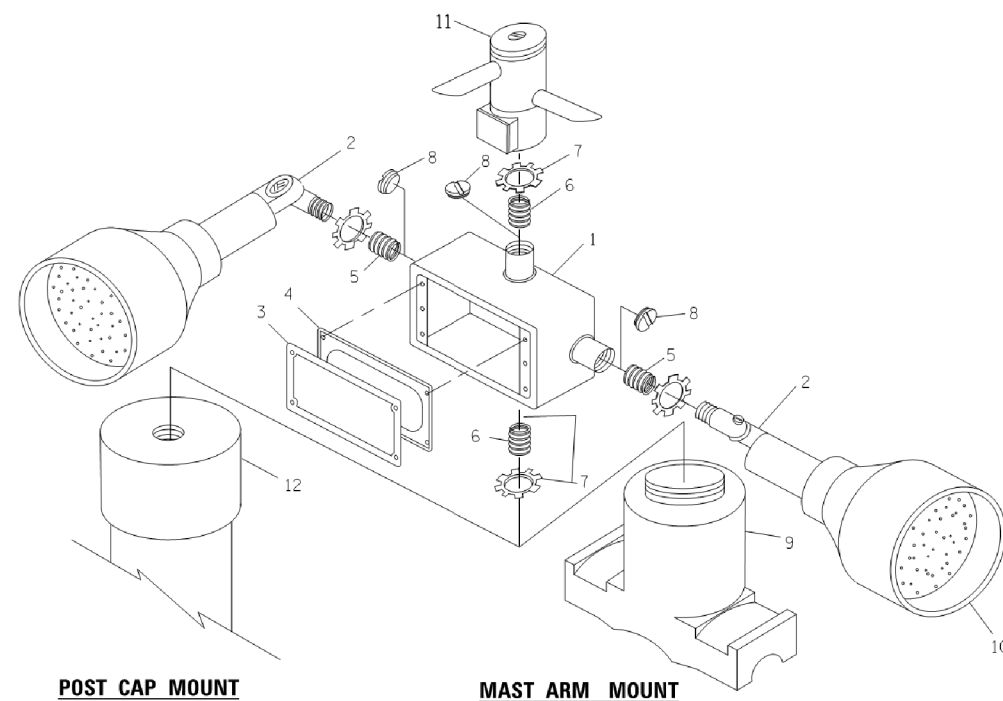
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



POST CAP MOUNT

MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

FILE NAME =	USER NAME = footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
cs:\pwwork\piv\dot\footemj\d0108315\ts05.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

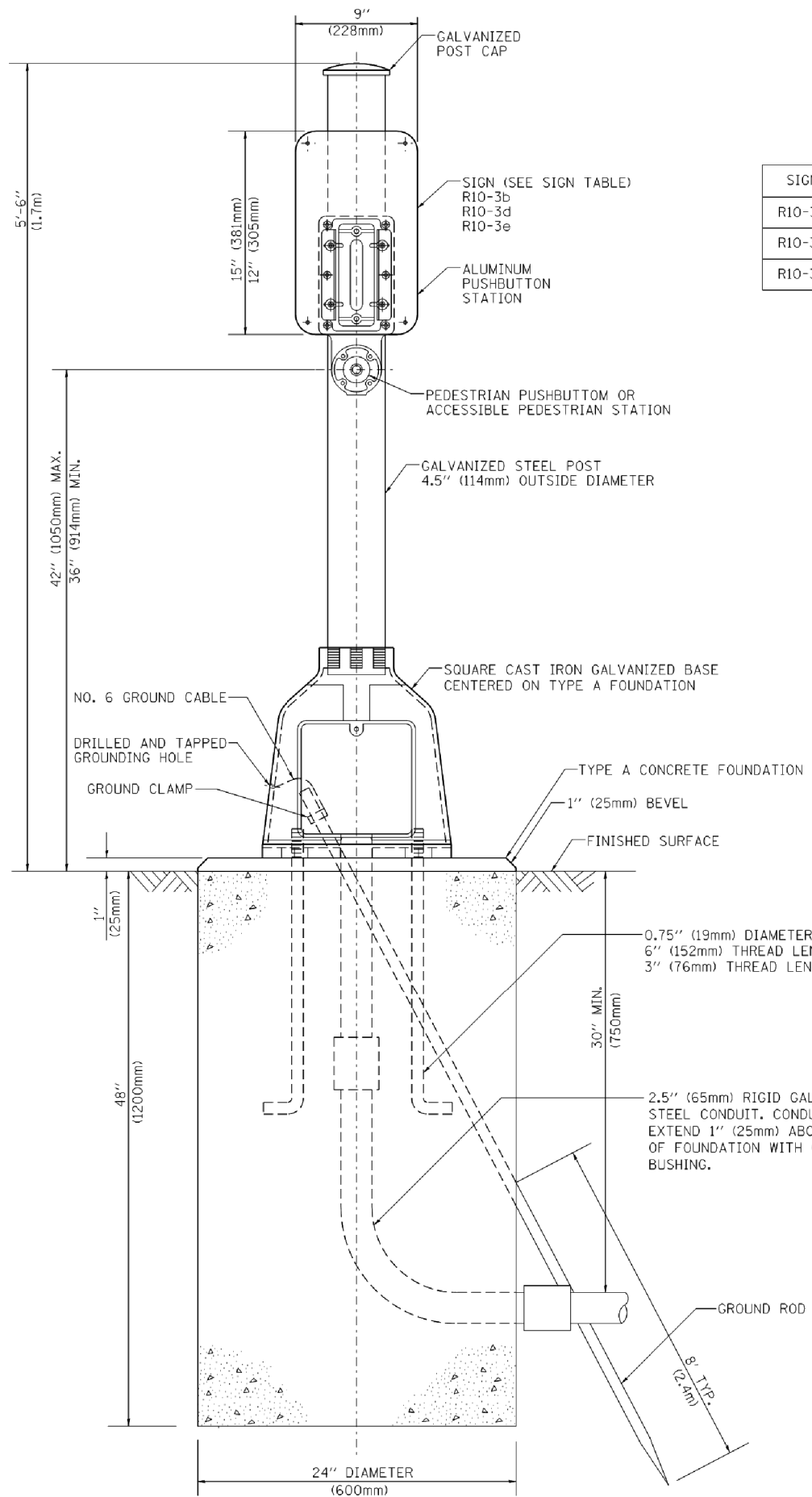
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	122
TS-05		CONTRACT NO. 61E73		

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

COMPANY NAME: #PROJECT CONTACTS
PROJECT CONTACT: #CLIENTS
CLIENT: #CLIENTS
FILE NAME: 86140100-shi-11-11-06 AM
FILE NAME: 86140100-shi-11-11-06 AM
PLOT DRIVER: ILPCH.DWG
PEN TABLE: PLOT LABEL.TB
PLOT LABEL.TB

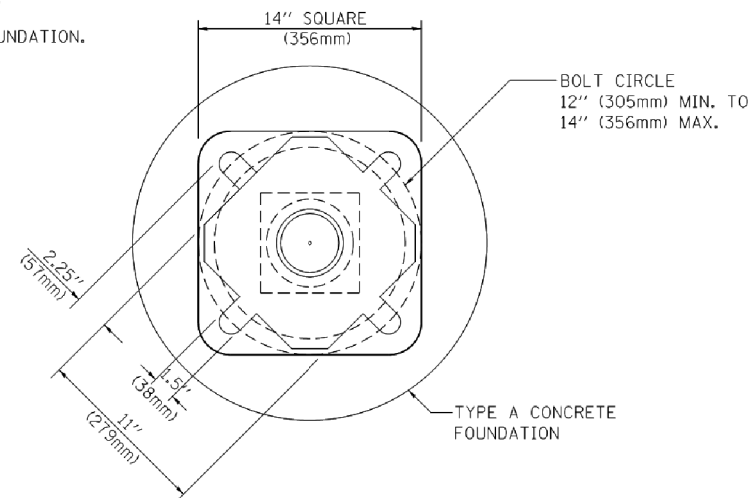
TS SHT NO. 6

86140100-shi-11-11-06 AM
86140100-shi-11-11-06 AM



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



BOLT PATTERN

PEDESTRIAN PUSH BUTTON POST, TYPE A

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
 TS SHT NO. 7

FILE NAME =	USER NAME = footemj	DESIGNED - DAG	REVISED - DAG 1-1-14
cs:\pwork\pwork\footemj\d0108315\ts05.dgn		DRAWN - GND	REVISED -
	PLOT SCALE = 50.0000' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10/1/2012	REVISED -

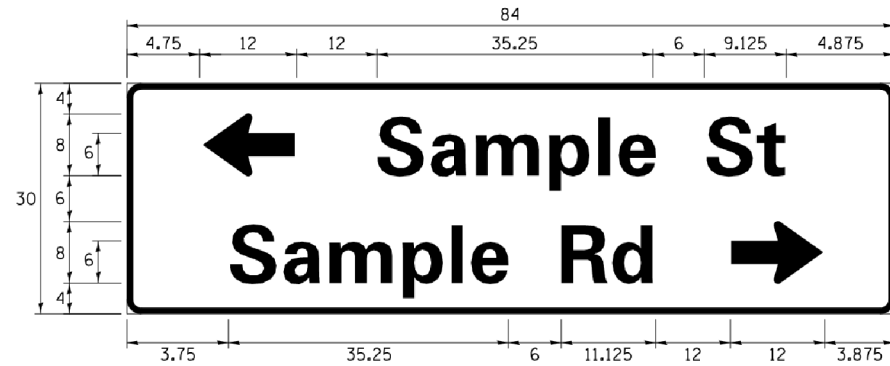
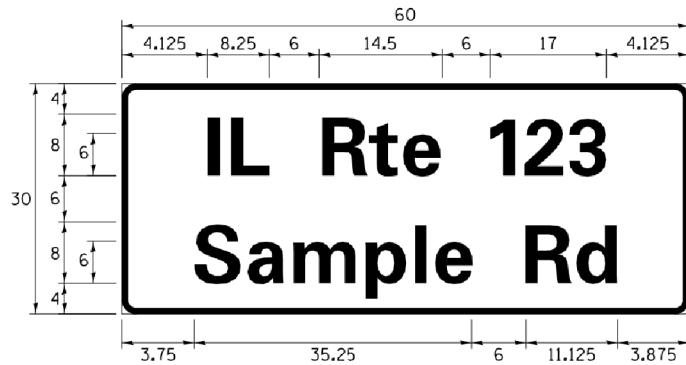
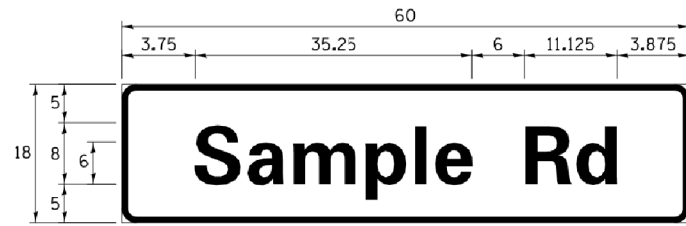
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
 STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	123
TS-05		CONTRACT NO. 61E73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

ALL DIMENSIONS ARE IN INCHES EXCEPT NOTED OTHERWISE

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS SHALL BE 3/4" WIDE. CORNER RADIUS SHALL BE 1-7/8". THE SPACING BETWEEN THE WORDS SHOULD BE 6", IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA
- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
- SIGN SCREWS PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING
- BRACKETS

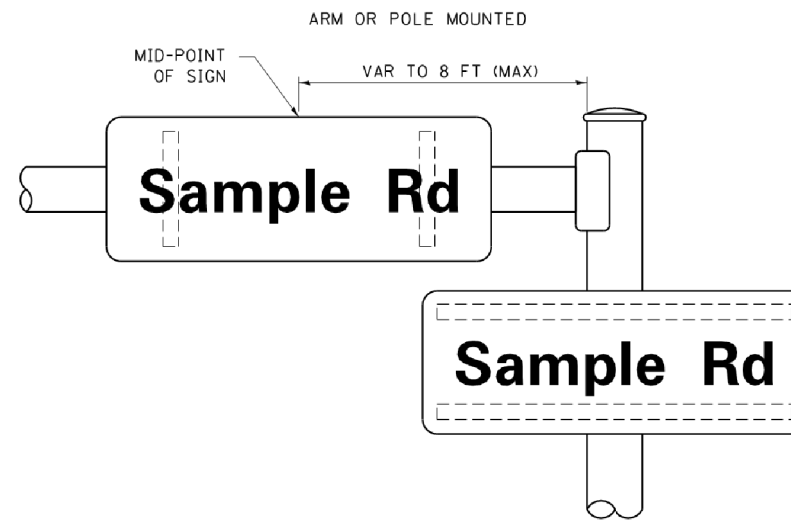
OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

STANDARD ALPHABETS SPACING CHART

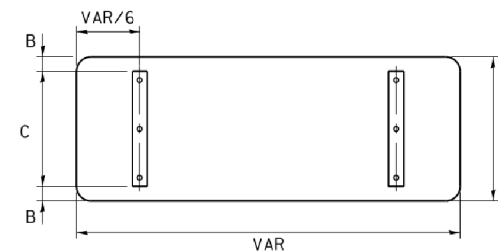
(8") UPPER CASE AND (6") LOWER CASE

CHARACTER	FHWA SERIES "C"			FHWA SERIES "D"			
	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

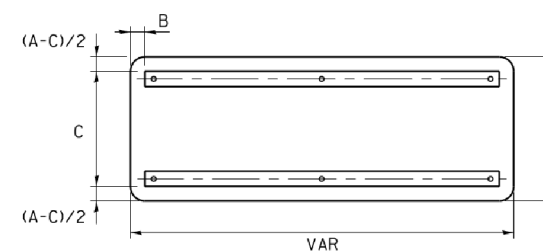
MOUNTING LOCATION



SUPPORTING CHANNELS



A	B	C
18"	2"	14"
30"	2"	24"



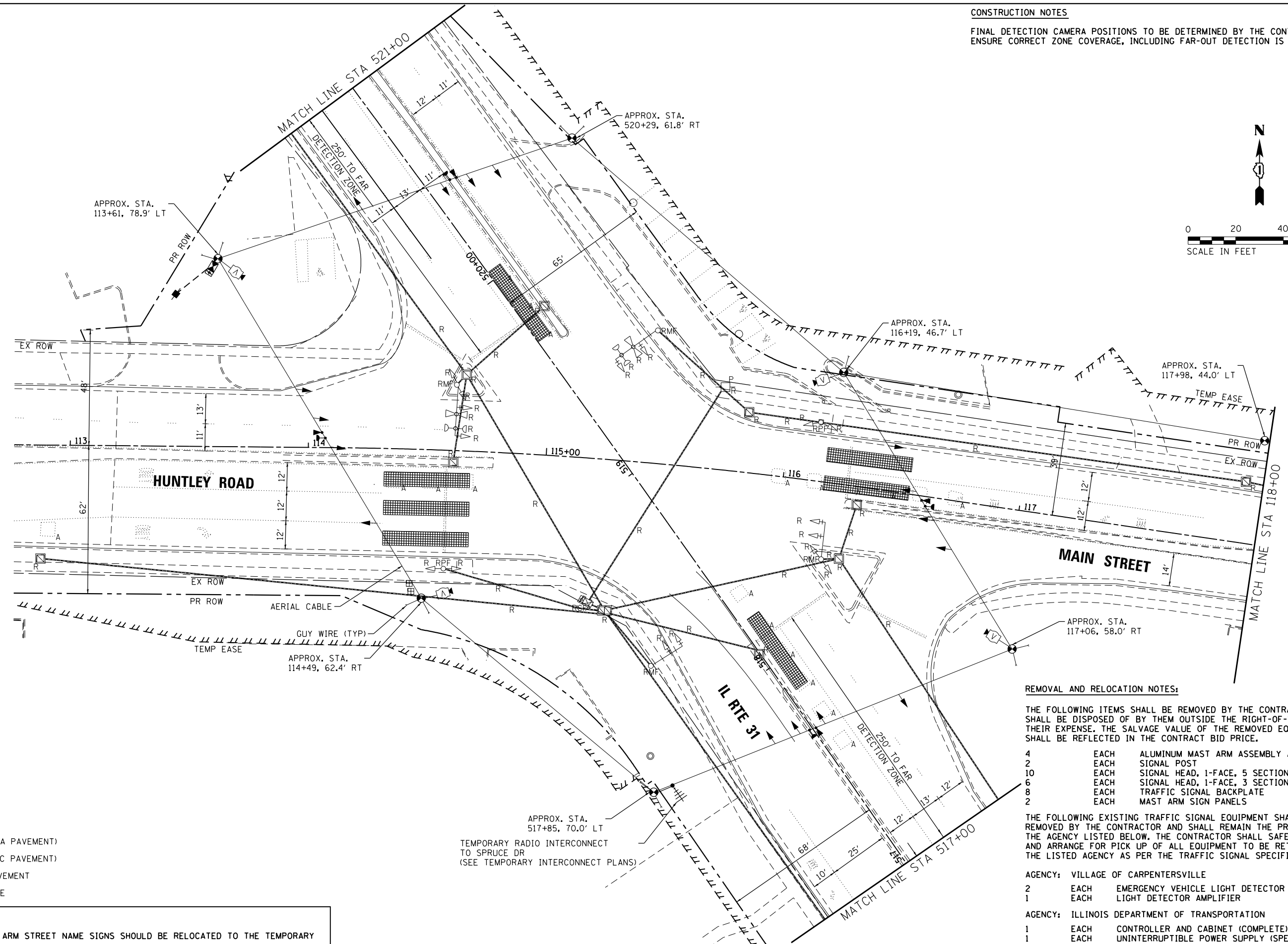
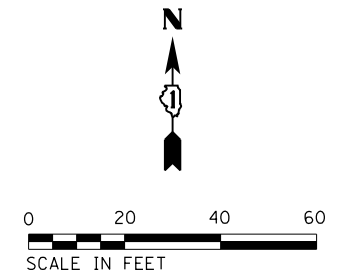
A	B	C
18"	2"	12"
30"	2"	22"

COMPANY NAME: #PROJECT CONTACT#
 PROJECT CONTACT: #CLIENT#
 CLIENT: #CLIENT#
 FILE NAME: #PROJECT#
 PLOT DRIVER: #PROJECT#
 PEN TABLE: #PROJECT#
 PLOT LABEL: #PROJECT#
TS SHT NO. 8

FILE NAME =	USER NAME = drvakosgn	DESIGNED - LP/IP	REVISED - LP 07/01/2015	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DISTRICT ONE		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default		CHECKED - IP	REVISED -			MAST ARM MOUNTED STREET NAME SIGNS		3887	08-00083-00-CH	KANE	259	124
	PLOT SCALE = 50.0000' / in.	DATE - 10/01/2014	REVISED -	SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	CONTRACT NO. 61E73	
	PLOT DATE = 7/31/2015			ILLINOIS FED. AID PROJECT								

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

NOTES:

- THE EXISTING MAST ARM STREET NAME SIGNS SHOULD BE RELOCATED TO THE TEMPORARY TRAFFIC SIGNALS.

REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- | | | |
|----|------|-------------------------------------|
| 4 | EACH | ALUMINUM MAST ARM ASSEMBLY AND POLE |
| 2 | EACH | SIGNAL POST |
| 10 | EACH | SIGNAL HEAD, 1-FACE, 5 SECTION |
| 6 | EACH | SIGNAL HEAD, 1-FACE, 3 SECTION |
| 8 | EACH | TRAFFIC SIGNAL BACKPLATE |
| 2 | EACH | MAST ARM SIGN PANELS |

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

- AGENCY: VILLAGE OF CARPENTERSVILLE
- | | | |
|---|------|----------------------------------|
| 2 | EACH | EMERGENCY VEHICLE LIGHT DETECTOR |
| 1 | EACH | LIGHT DETECTOR AMPLIFIER |
- AGENCY: ILLINOIS DEPARTMENT OF TRANSPORTATION
- | | | |
|---|------|--|
| 1 | EACH | CONTROLLER AND CABINET (COMPLETE) |
| 1 | EACH | UNINTERRUPTIBLE POWER SUPPLY (SPECIAL) |

**TS A.205
EAGLE 2A**

COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
 PLOT LABEL: #PLOT_LABEL#
 TS_SHT NO. 9



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=28'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-PRESTAGE (SHEET 1 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

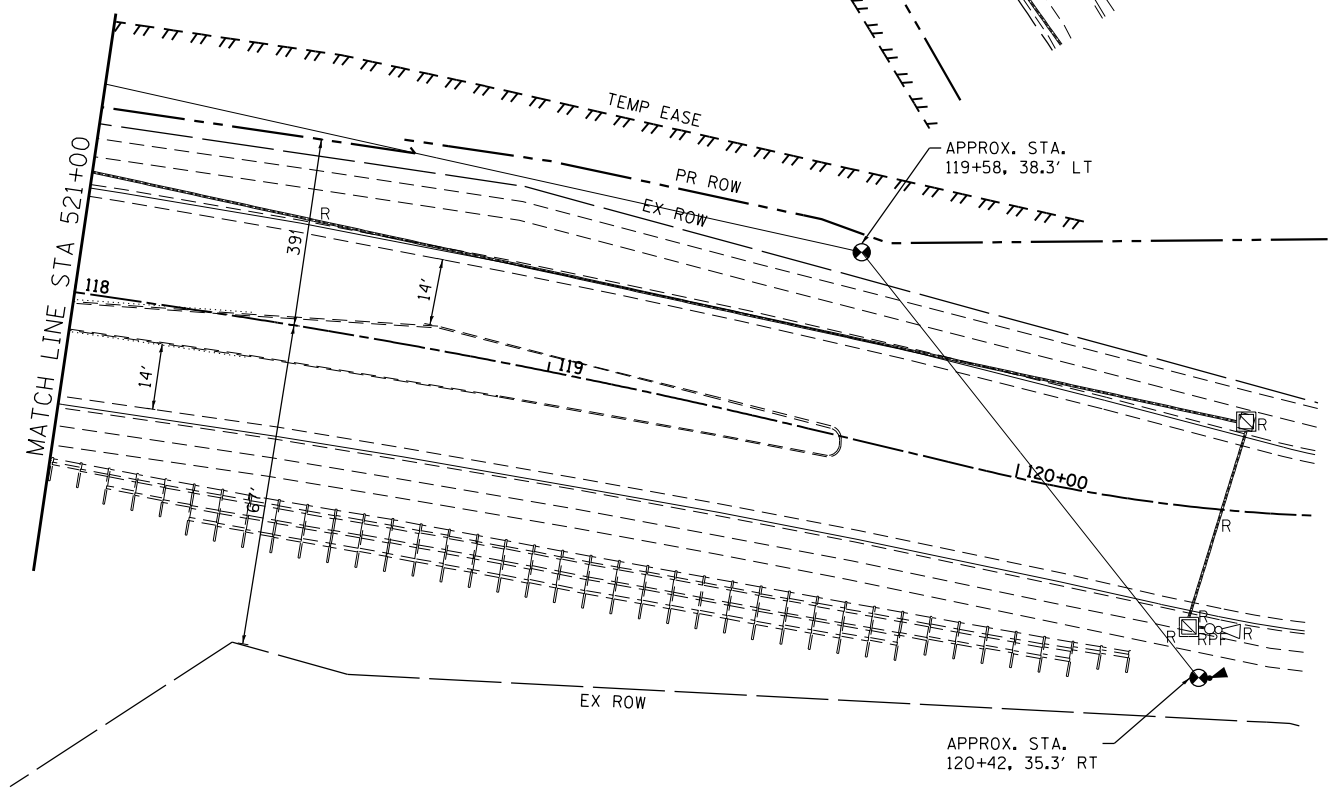
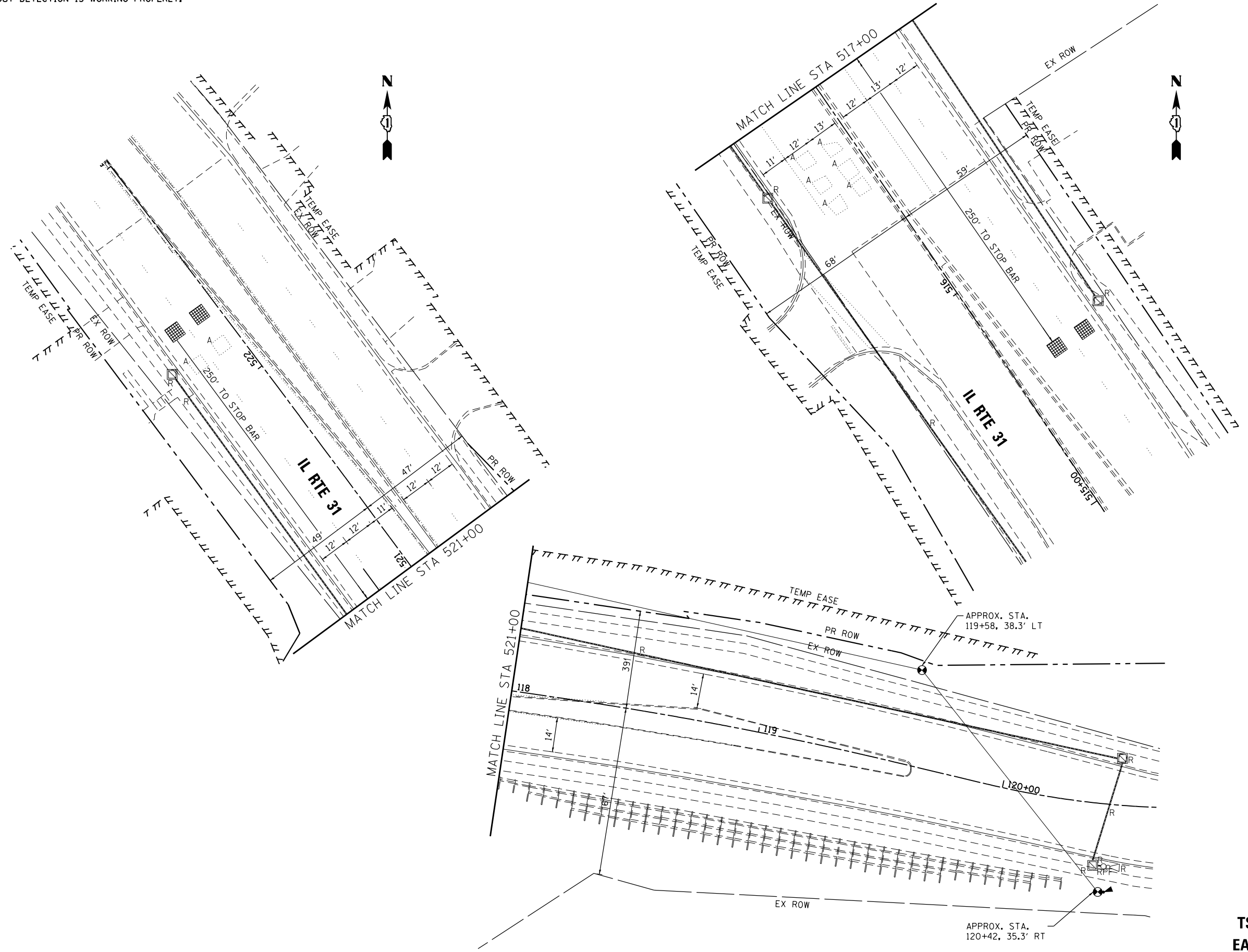
SCALE: 1"=20' SHEET NO. 9 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	125
CONTRACT NO. 61E73				

FED. ROAD DIST. NO. [] ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



LEGEND:

- WORK ZONE (HMA PAVEMENT)
- WORK ZONE (PCC PAVEMENT)
- TEMPORARY PAVEMENT
- DETECTION ZONE

0 20 40 60
SCALE IN FEET

COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-18gpr-02.dgn
 PLOT DRIVER: ILpch.plt
 PEN TABLE: PLOT LABEL.tbl
TS SHT NO. 10



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN—PRESTAGE (SHEET 2 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 10 OF 53 SHEETS STA. _____ TO STA. _____

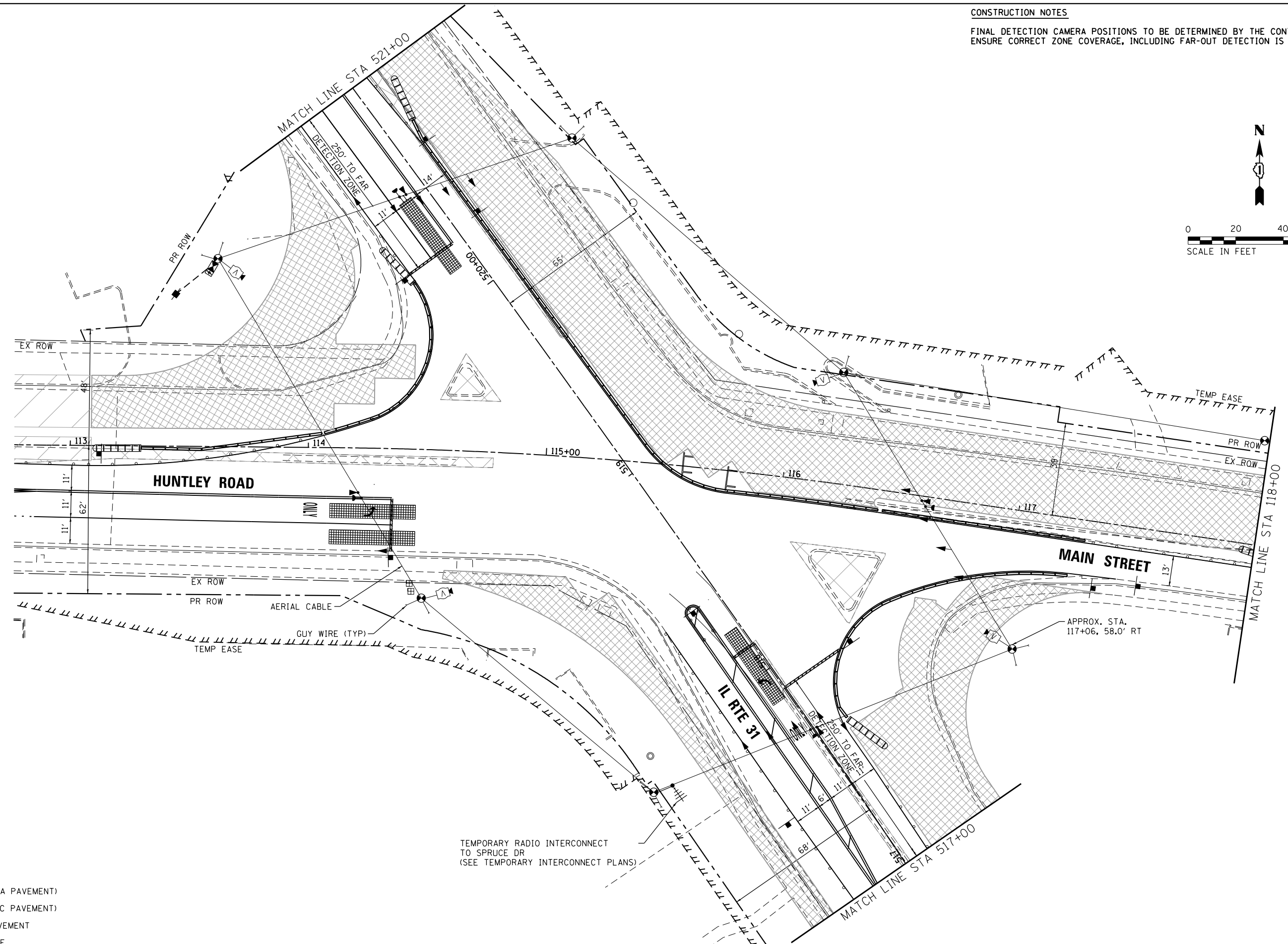
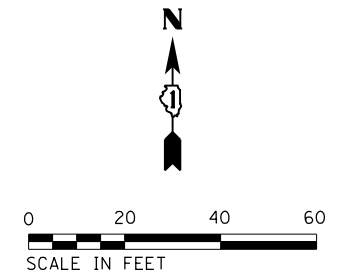
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	126
CONTRACT NO.			61E73	

**TS A.205
EAGLE 2A**

FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-1st-01.dgn
 PLOT DRIVER: #PLOT DRIVER#
 PEN TABLE: #PEN TABLE#
TS SHT NO. 11



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 1 (SHEET 1 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 11 OF 53 SHEETS STA. _____ TO STA. _____

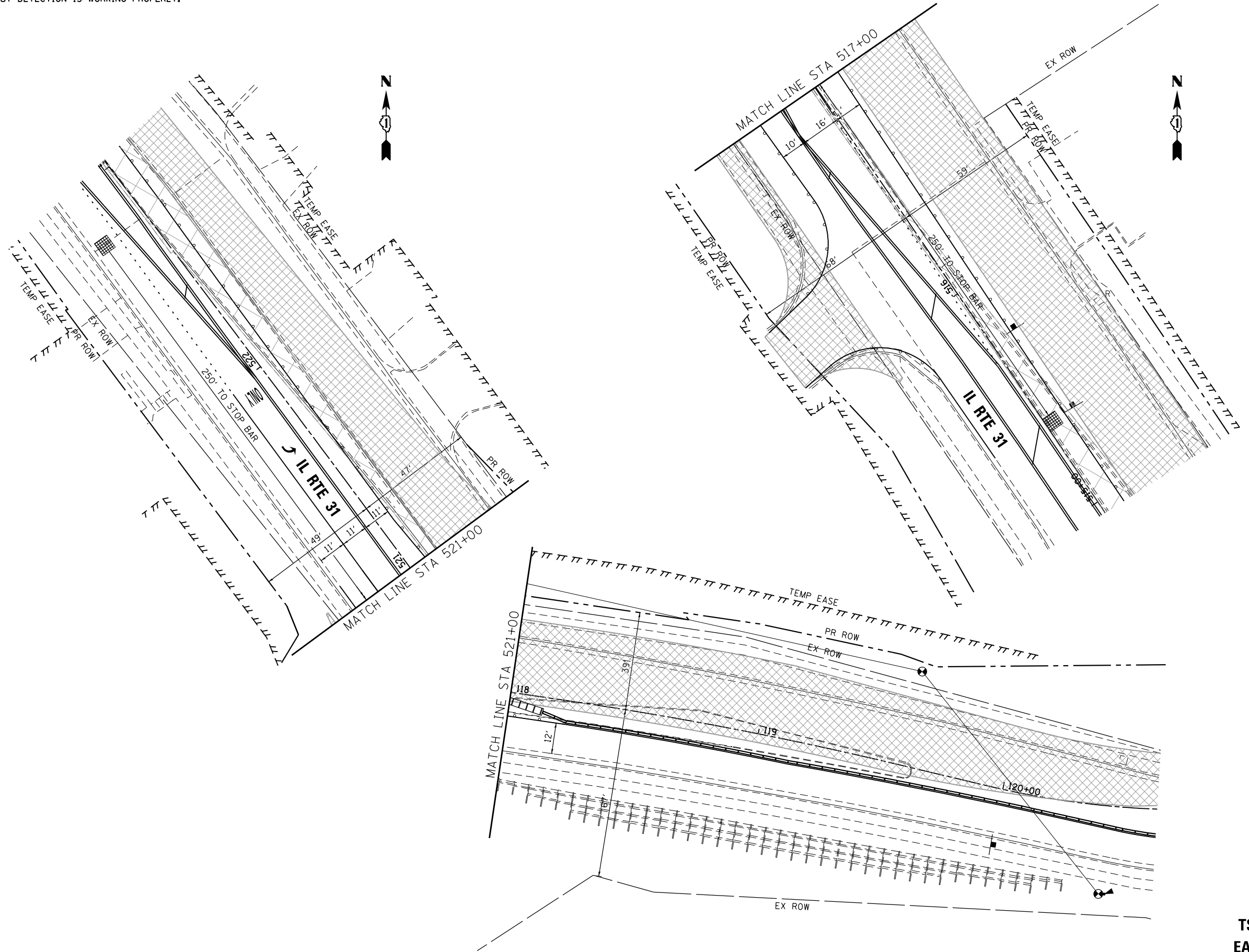
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	127
CONTRACT NO. 61E73				

**TS A.205
EAGLE 2A**

FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



LEGEND:

- WORK ZONE (HMA PAVEMENT)
- WORK ZONE (PCC PAVEMENT)
- TEMPORARY PAVEMENT
- DETECTION ZONE

0 20 40 60
SCALE IN FEET

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-tsg1-02.dgn
 PLOT DRIVER: ILpch.plt
 PEN TABLE: PLOTLABEL.tbl

TS SHT NO. 12



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 1 (SHEET 2 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

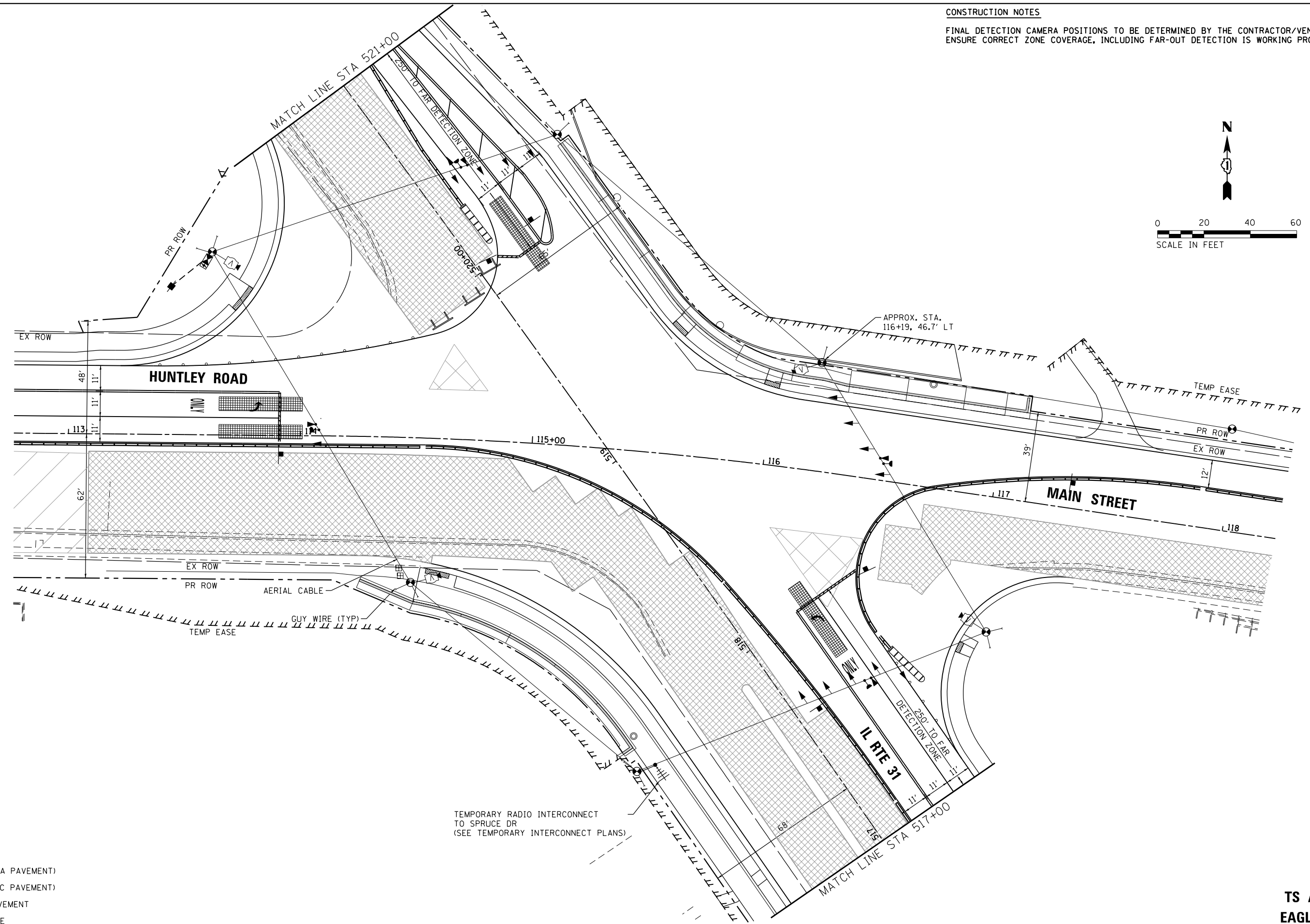
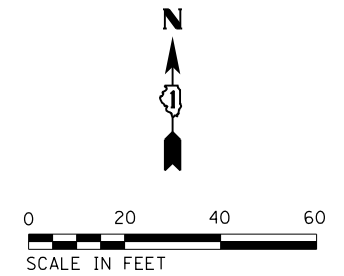
SCALE: 1"=20' SHEET NO. 12 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	128
FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

**TS A.205
EAGLE 2A**

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



TEMPORARY RADIO INTERCONNECT TO SPRUCE DR (SEE TEMPORARY INTERCONNECT PLANS)

- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
 PLOT LABEL: #PLOT_LABEL#

TS_SHT NO. 13



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISOR -	REVISION -
CHECKED - JRM	REVISOR -	REVISION -
DATE - 3/23/18	REVISOR -	REVISION -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 2 (SHEET 1 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 13 OF 53 SHEETS STA. _____ TO STA. _____

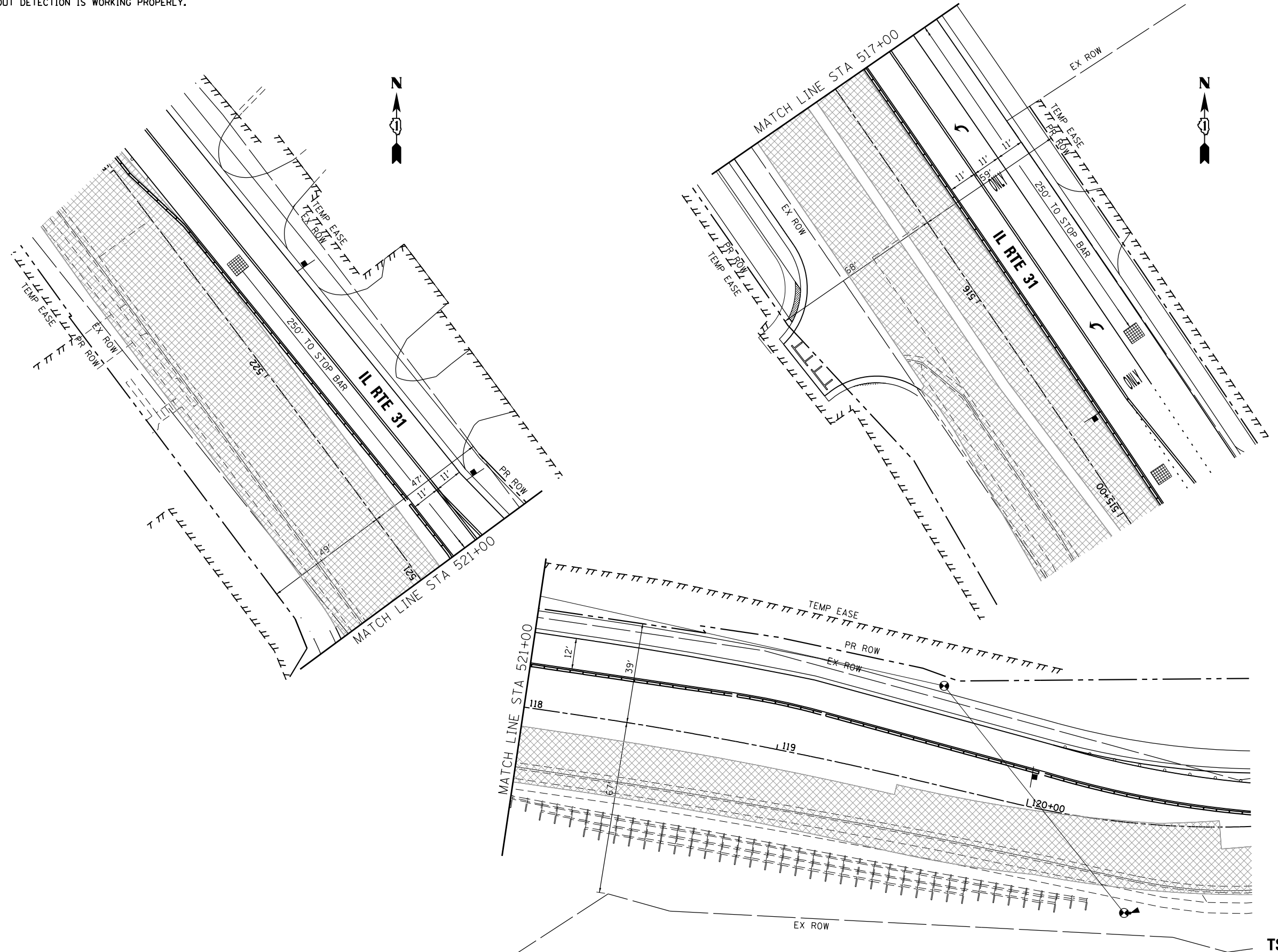
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	129
FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

**TS A.205
EAGLE 2A**

86140100.dgn
86140100-sh1-181g2-01.dgn

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-11-11-18-02.dgn
 PLOT DRIVER: IL_PCH_PLOT.DWG
 PEN TABLE: PLOT LABEL.tbl
TS SHT NO. 14



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 2 (SHEET 2 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 14 OF 53 SHEETS STA. _____ TO STA. _____

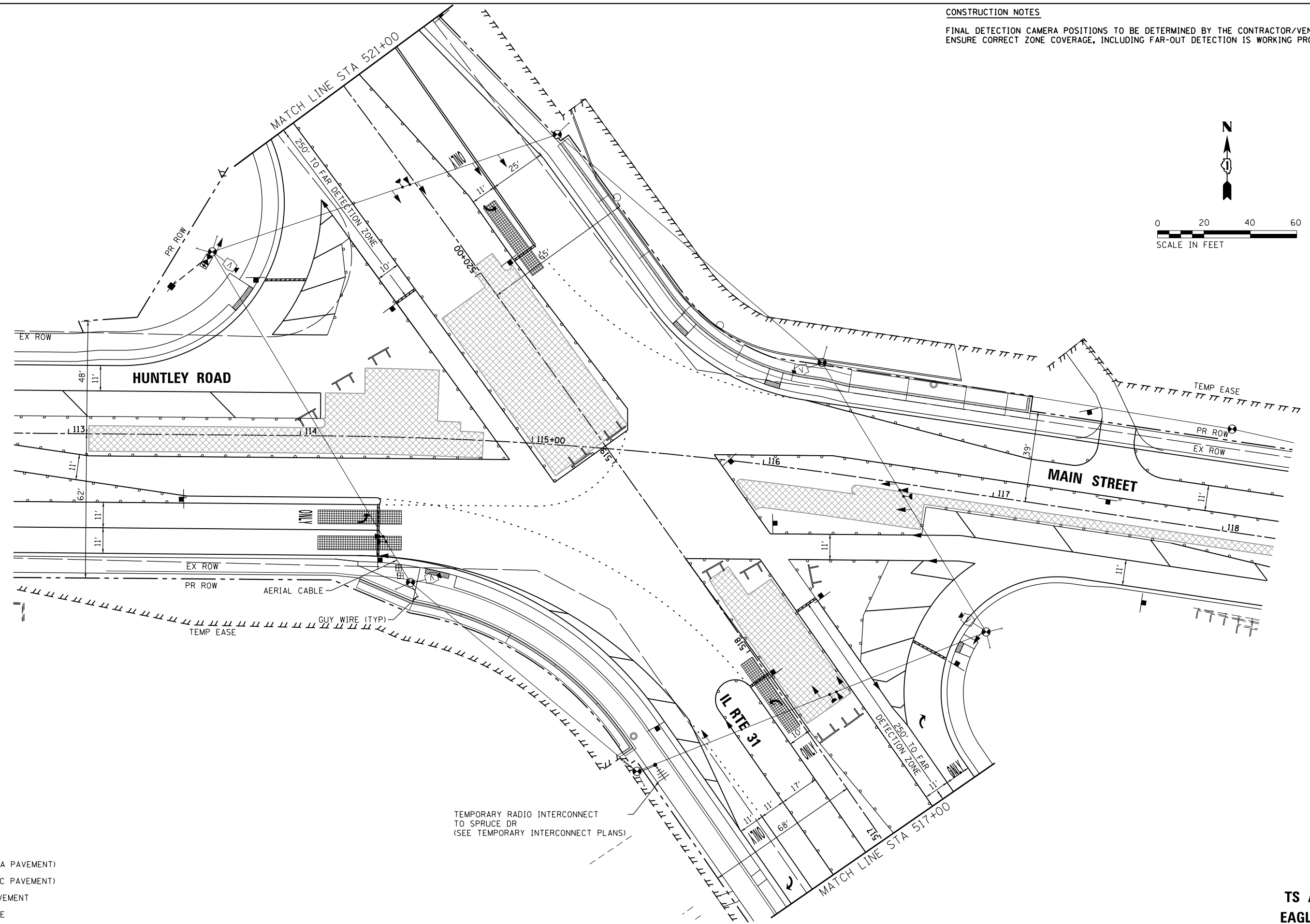
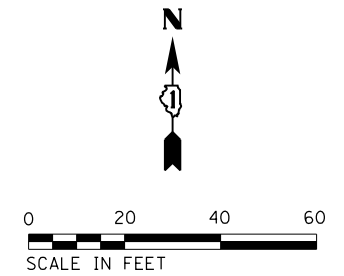
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	130
CONTRACT NO. 61E73				

**TS A.205
EAGLE 2A**

86140100
86140100-sh1-18182-02.dgn

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



TEMPORARY RADIO INTERCONNECT TO SPRUCE DR (SEE TEMPORARY INTERCONNECT PLANS)

- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 CLIENT: HRGreen.com
 FILE NAME: 86140100-sh1-181g3-01.dgn
 PLOT DRIVER: ILpch.plt
 PEN TABLE: PLOTLABEL.tbl

TS SHT NO. 15



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
 EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3 (SHEET 1 OF 2)
 IL RTE 31 AND HUNTLEY ROAD/MAIN STREET

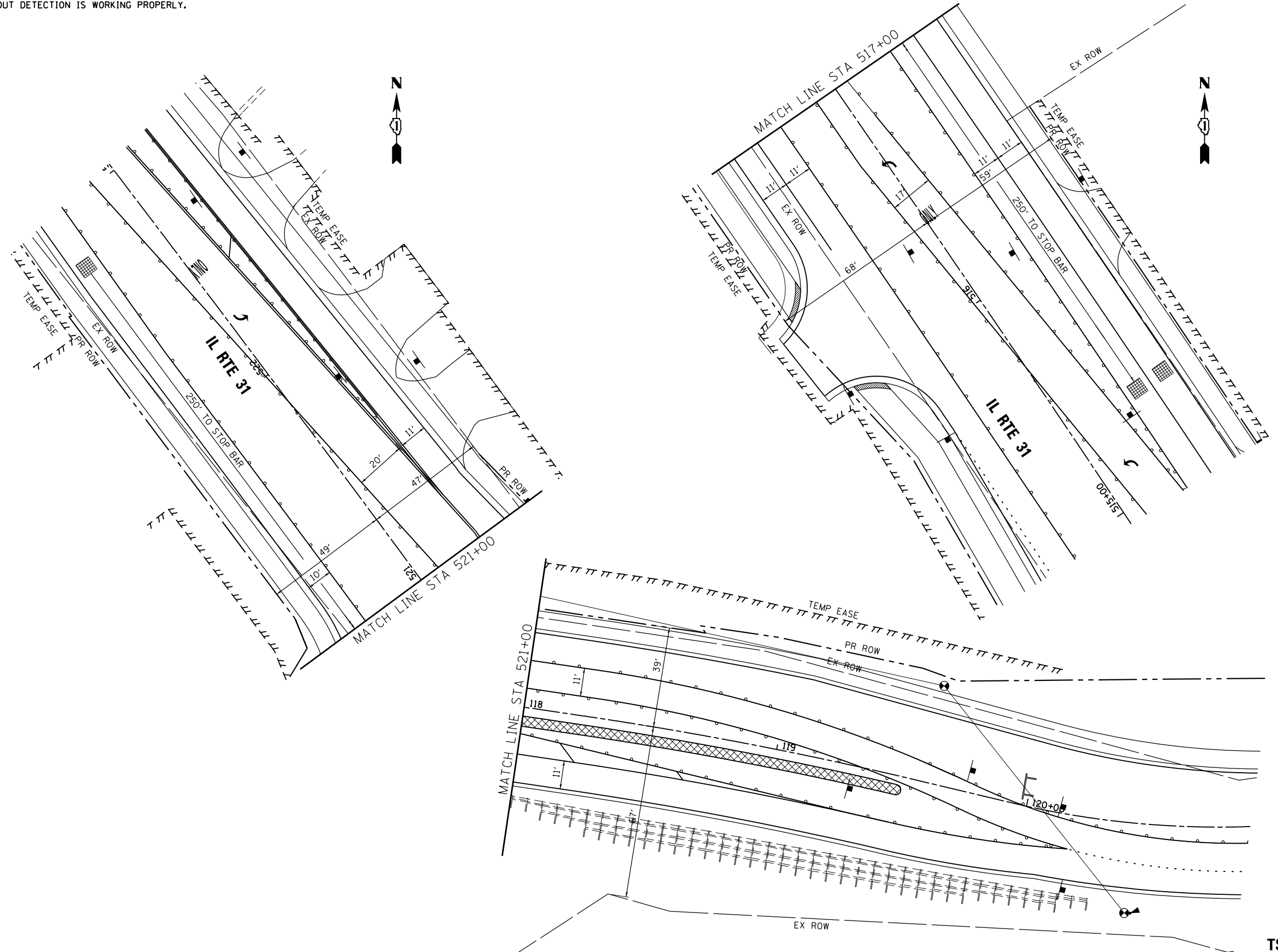
SCALE: 1"=20' SHEET NO. 15 OF 53 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	131
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

TS A.205
 EAGLE 2A

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#

TS SHT NO. 16



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
 EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3 (SHEET 2 OF 2)
 IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 16 OF 53 SHEETS STA. _____ TO STA. _____

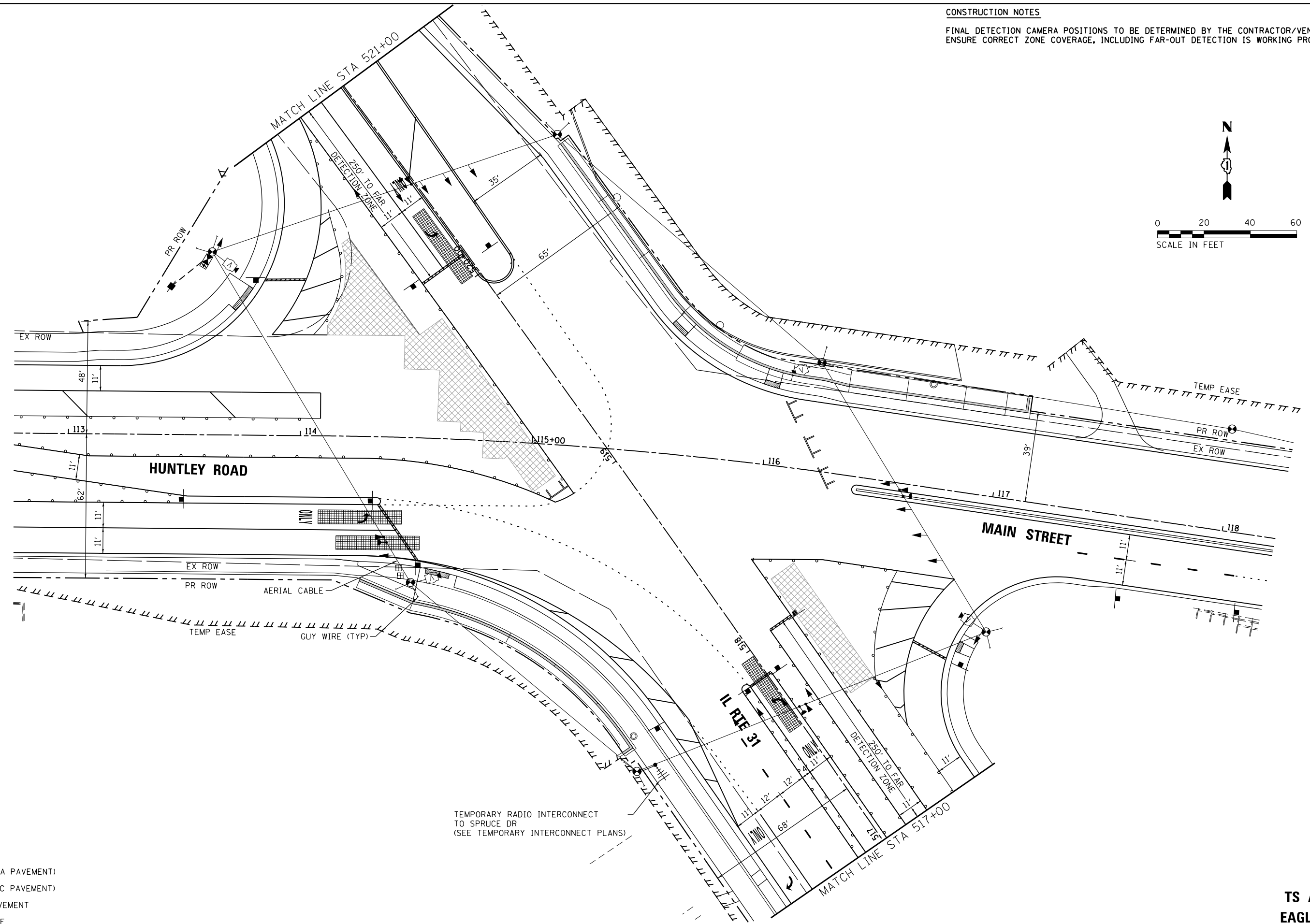
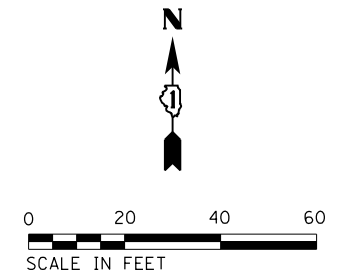
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	132
CONTRACT NO.			61E73	

**TS A.205
 EAGLE 2A**

FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



TEMPORARY RADIO INTERCONNECT TO SPRUCE DR (SEE TEMPORARY INTERCONNECT PLANS)

- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhui
 CLIENT: HRGreen.com
 FILE NAME: 86140100-sh1-18193a-01.dgn
 PLOT DRIVER: IL_PCH_PLOTTER.CFG
 PEN TABLE: PLOT_LABEL.tbl
TS_SHT NO. 17



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3A (SHEET 1 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

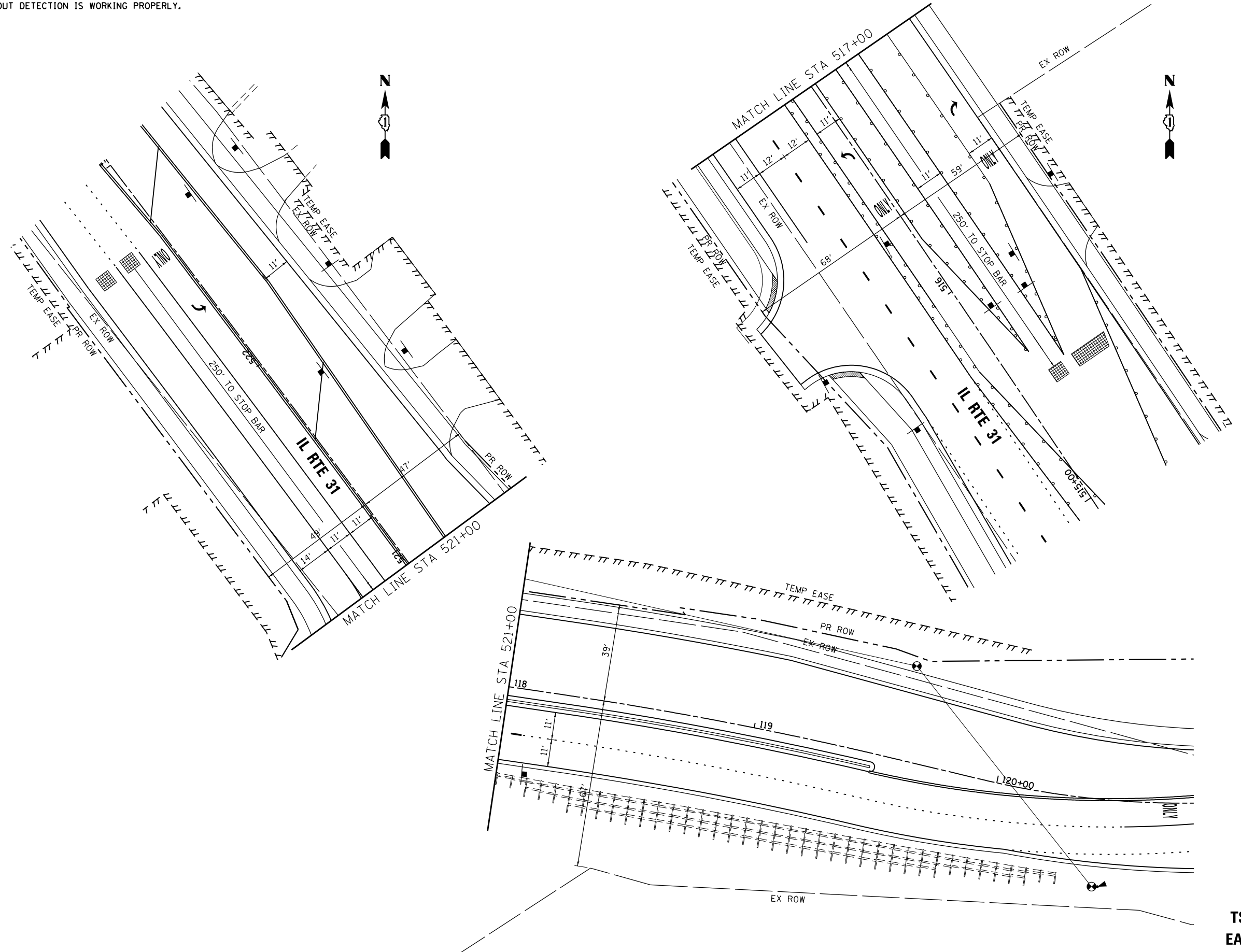
SCALE: 1"=20' SHEET NO. 17 OF 53 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	133
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

**TS A.205
EAGLE 2A**

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



LEGEND:

- WORK ZONE (HMA PAVEMENT)
- WORK ZONE (PCC PAVEMENT)
- TEMPORARY PAVEMENT
- DETECTION ZONE

0 20 40 60
SCALE IN FEET

COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-181g3a-02.dgn
 PLOT DRIVER: ILpch.plw
 PEN TABLE: PLOT LABEL.tbl
TS SHT NO. 18



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISOR -	REVISION -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISOR -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISION -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3A (SHEET 2 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 18 OF 53 SHEETS STA. _____ TO STA. _____

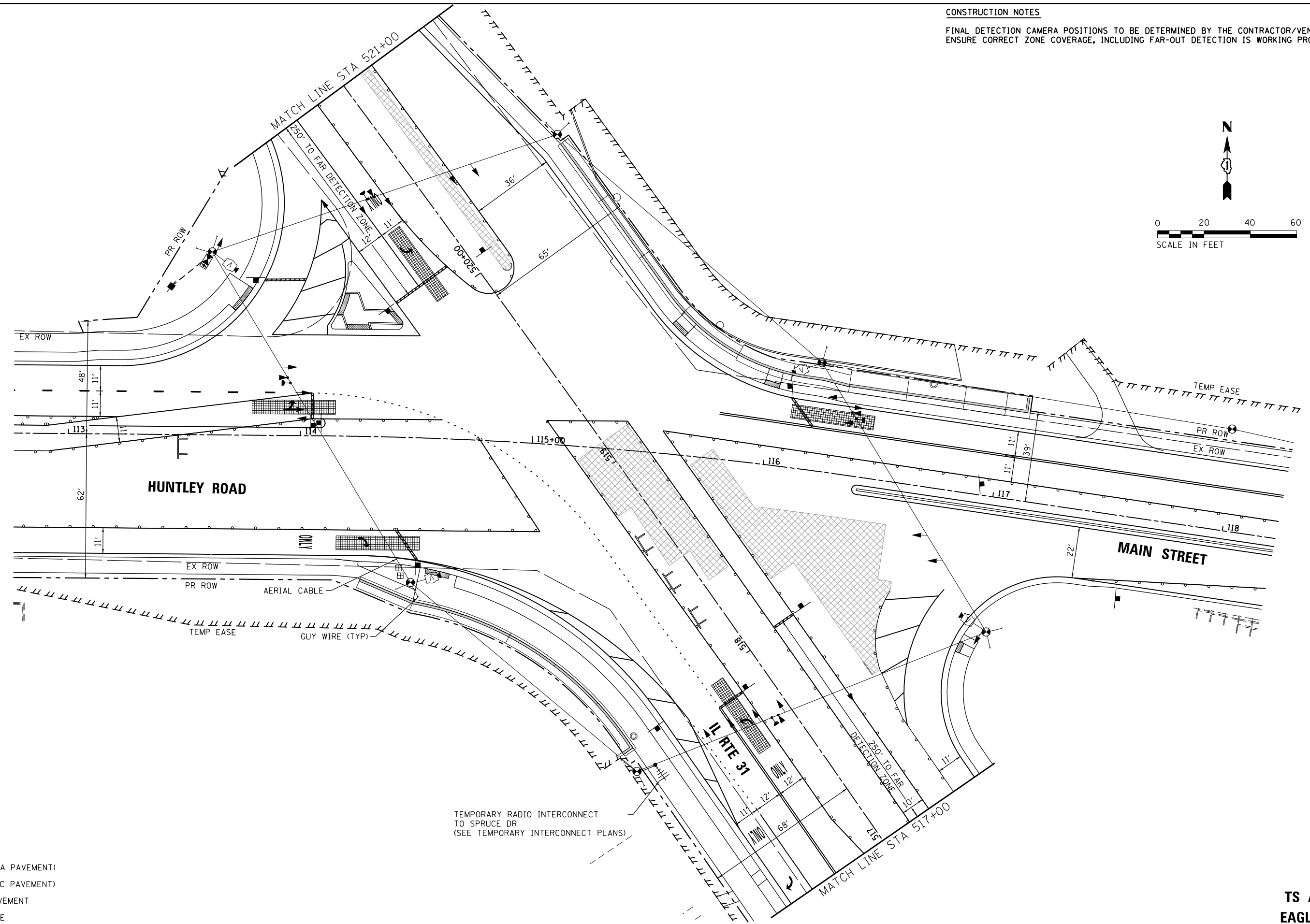
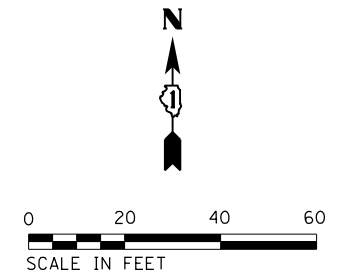
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	134
CONTRACT NO.			61E73	

**TS A.205
EAGLE 2A**

FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

TEMPORARY RADIO INTERCONNECT TO SPRUCE DR (SEE TEMPORARY INTERCONNECT PLANS)

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
 86140100-sh1-1813b-01.dgn
 86140100-sh1-1813b-01.dgn

TS SHT NO. 19



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3B (SHEET 1 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 19 OF 53 SHEETS STA. _____ TO STA. _____

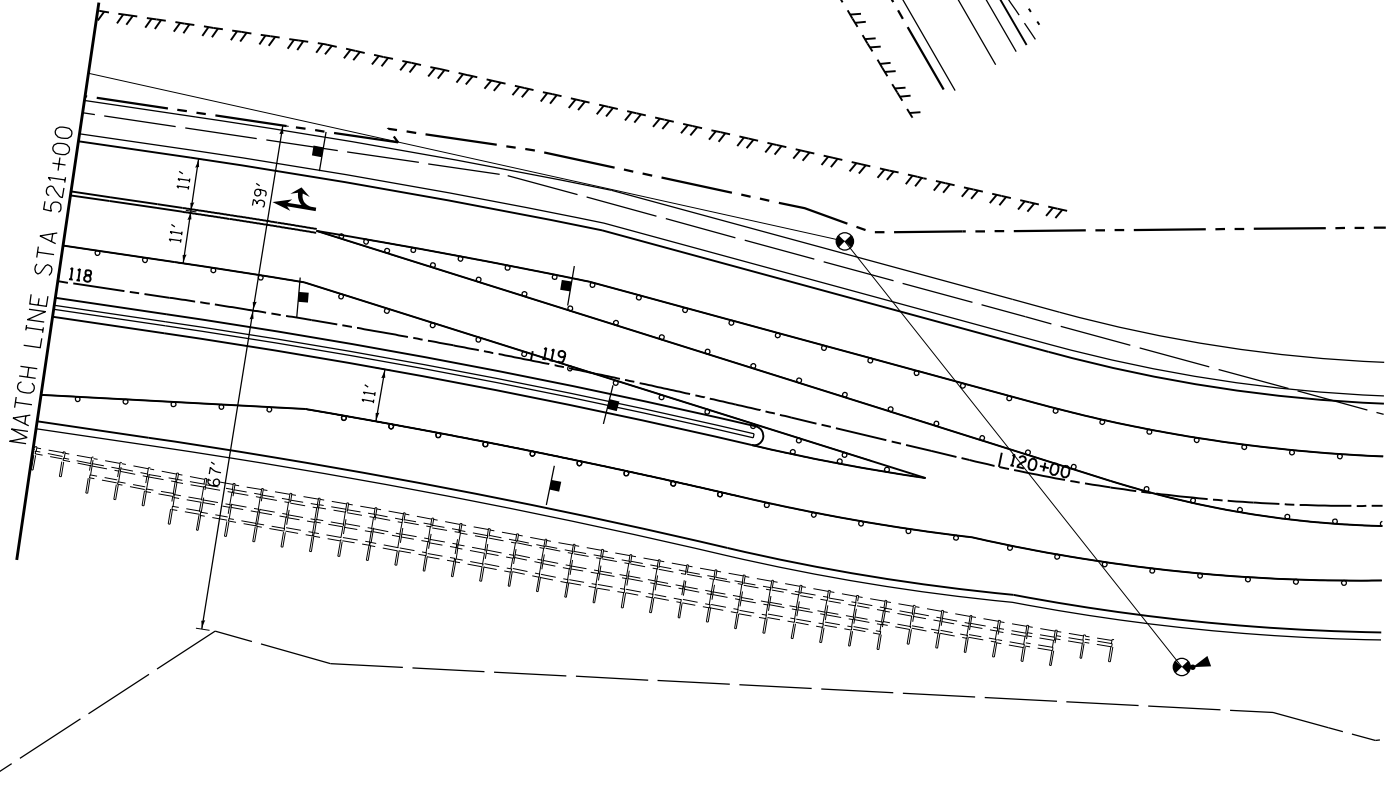
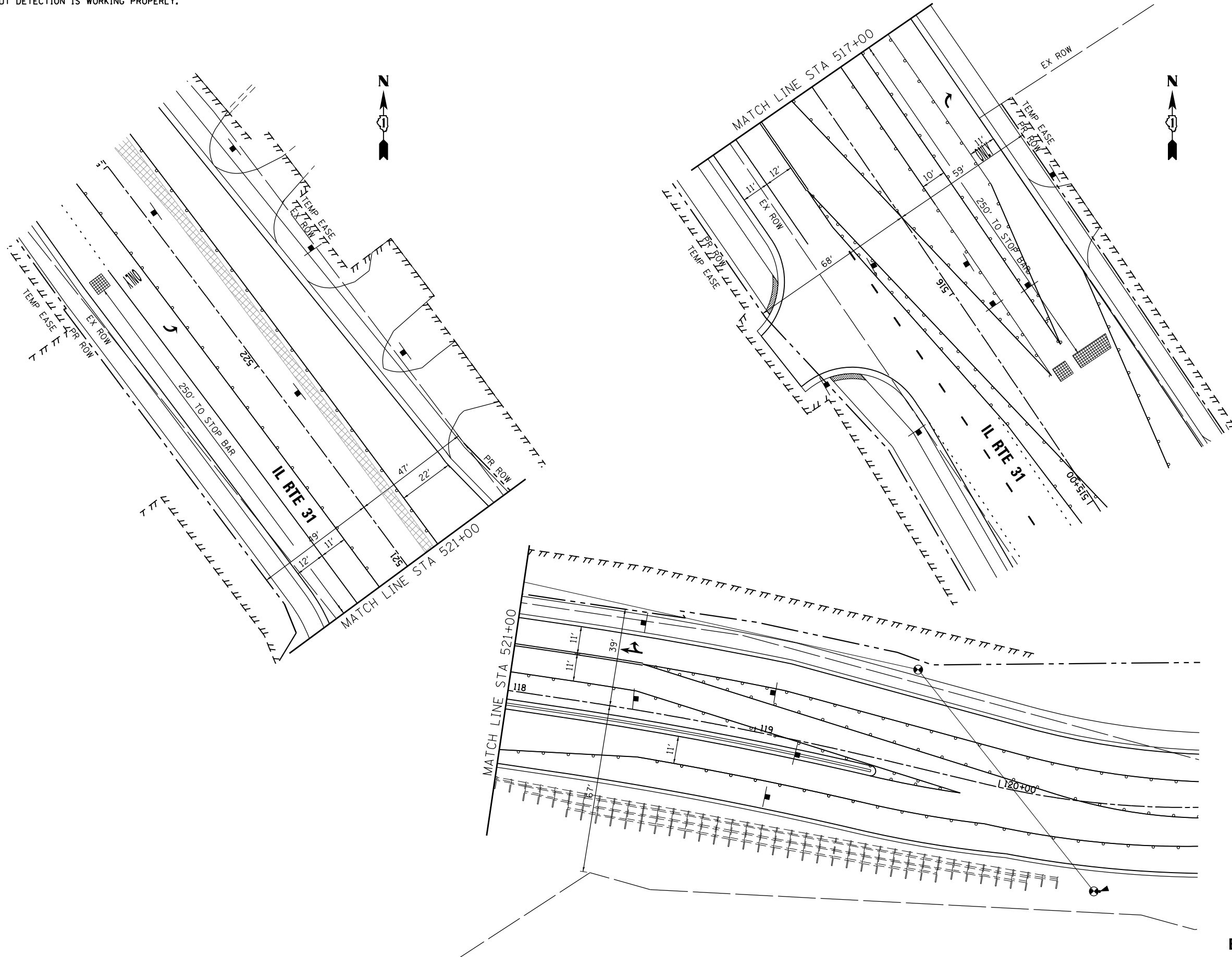
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	135
CONTRACT NO. 61E73				

**TS A.205
EAGLE 2A**

FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-11-11-18-02.dgn
 PLOT DRIVER: IL_rte31_01.ctb
 PEN TABLE: PLOT_LABEL.tbl
TS_SHT NO. 20



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3B (SHEET 2 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

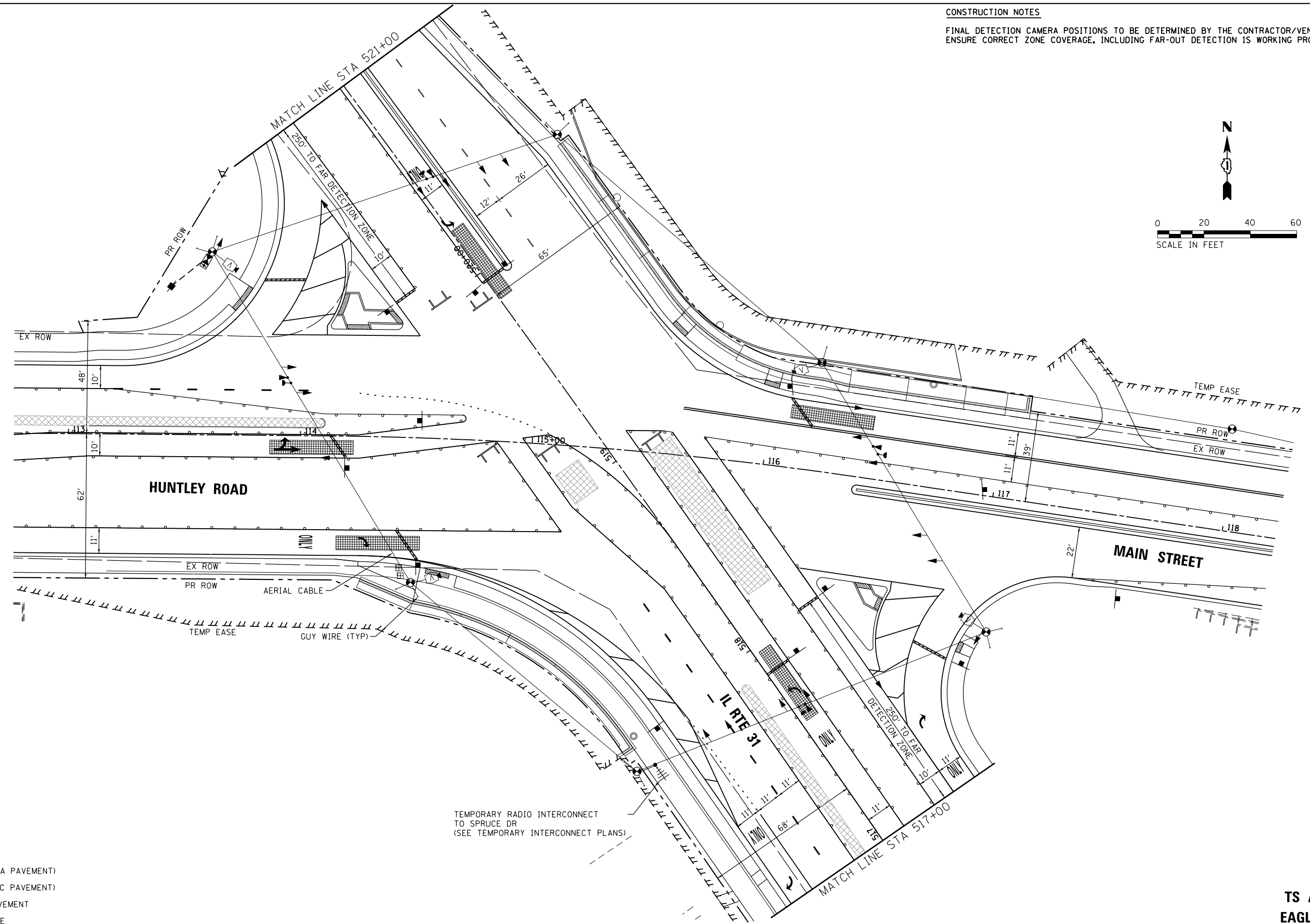
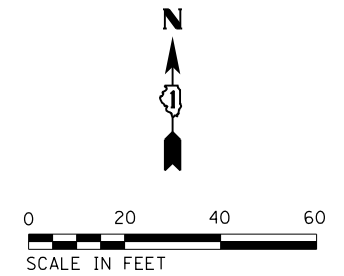
SCALE: 1"=20' SHEET NO. 20 OF 53 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	136
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

**TS A.205
EAGLE 2A**

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



TEMPORARY RADIO INTERCONNECT TO SPRUCE DR (SEE TEMPORARY INTERCONNECT PLANS)

- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3C (SHEET 1 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	137
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

USER NAME = jmelhui	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

SCALE: 1"=20' SHEET NO. 21 OF 53 SHEETS STA. TO STA.

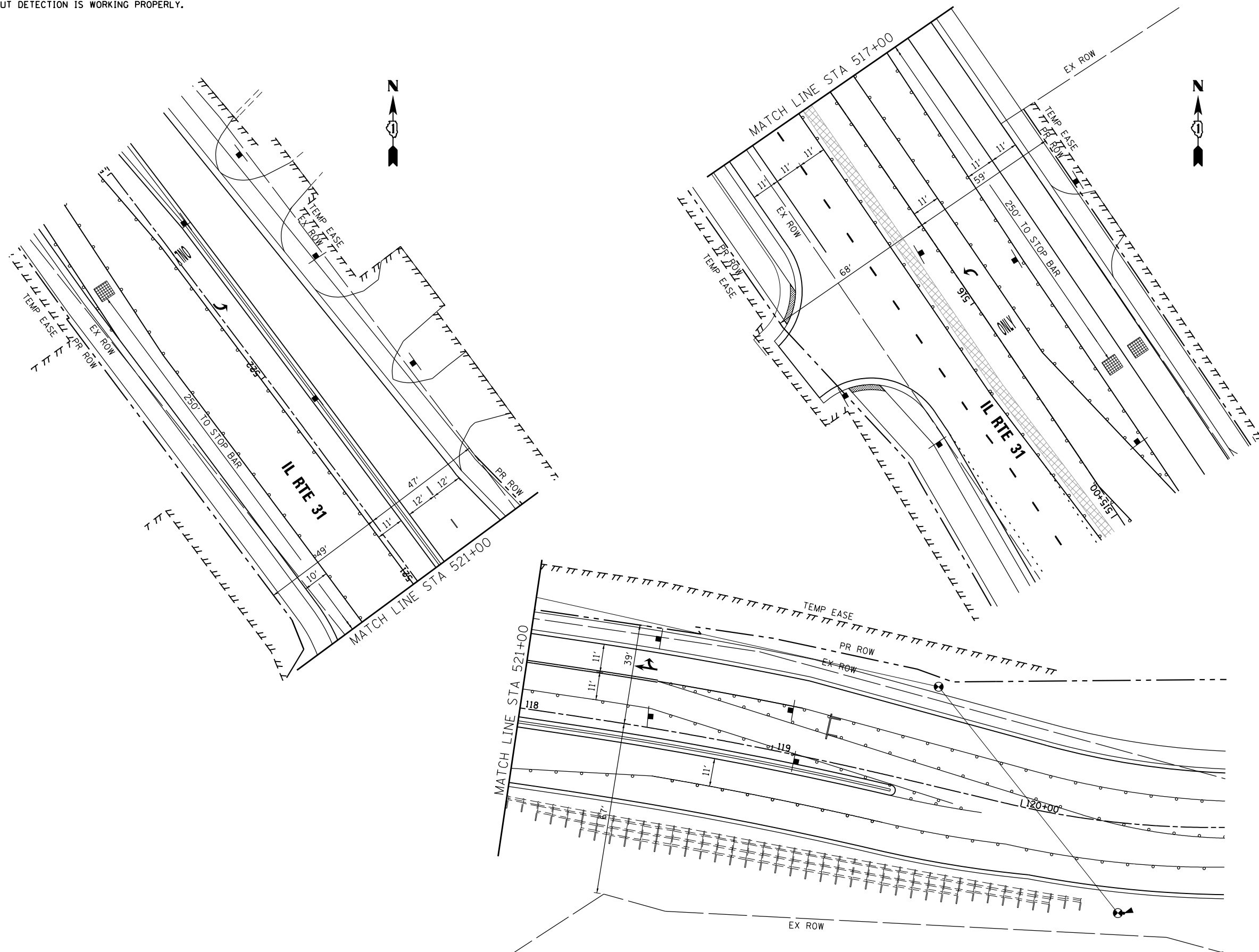
COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
CLIENT: HRGreen.com
FILE NAME: 86140100-sh1-181g3c-01.dgn
PLOT DRIVER: IL_pch_low01.ctb
PEN TABLE: PLOT LABEL.tbl
TS_SHT NO. 21

TS A.205
EAGLE 2A

86140100
86140100-sh1-181g3c-01.dgn

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-1813c-02.dgn
 PLOT DRIVER: ILpch.plw
 PEN TABLE: PLOT LABEL.tbl
TS SHT NO. 22



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

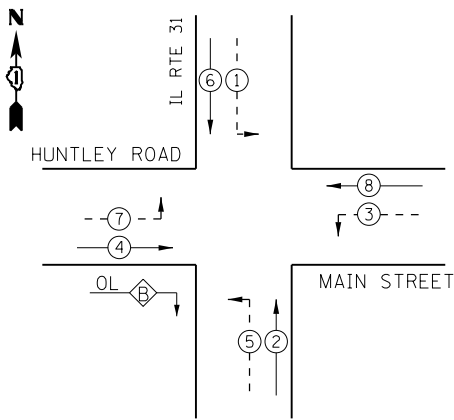
**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3C (SHEET 2 OF 2)
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: 1"=20' SHEET NO. 22 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	138
FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

**TS A.205
EAGLE 2A**

PROPOSED CONTROLLER SEQUENCE



LEGEND:

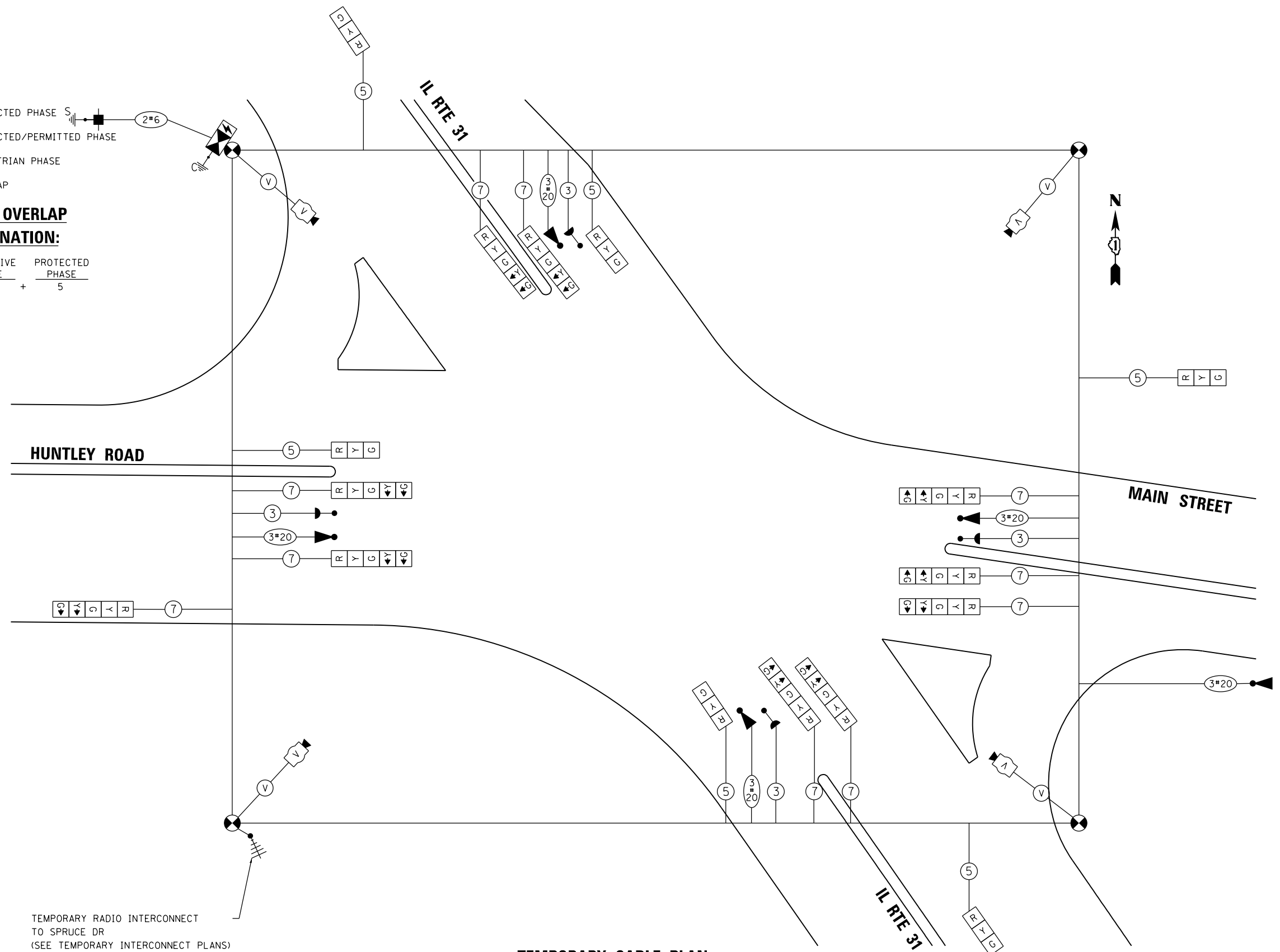
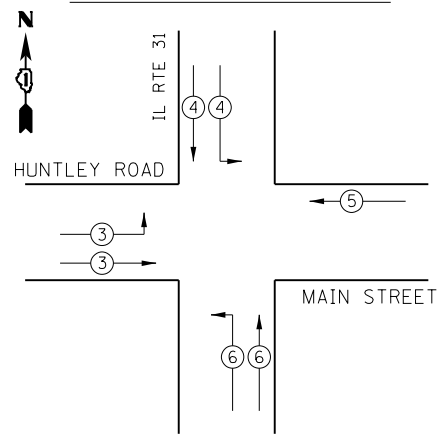
- ← ⊙ ⊛ → PROTECTED PHASE
- ← ⊙ ⊛ → PROTECTED/PERMITTED PHASE
- ← ⊙ ⊛ → PEDESTRIAN PHASE
- ← ⊙ ⊛ → OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER = PERMISSIVE PHASE + PROTECTED PHASE

B = 4 + 5

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



TEMPORARY RADIO INTERCONNECT TO SPRUCE DR (SEE TEMPORARY INTERCONNECT PLANS)

TEMPORARY CABLE PLAN PRESTAGE

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	16	11	50	88.0
(YELLOW)	16	20	5	16.0
(GREEN)	16	12	45	86.4
PERMISSIVE ARROW	10	10	10	10.0
PED. SIGNAL	-	20	100	-
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				325.4

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W. CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: TOM MAHAR
PHONE: 815-724-5010
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION
 FILE NAME: 86140100-sh1-11gcbahunt-01.dgn
 PLOT DRIVER: ILpch.fwsoftcf9
 PEN TABLE: PLOT LABEL.tbl
 TS_SHT NO. 23



DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISED -
CHECKED - JRM	REVISED -
DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

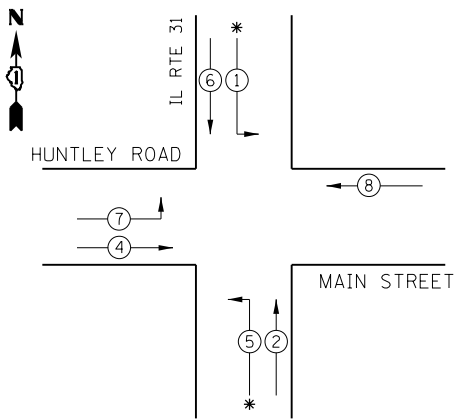
SCALE: N.T.S. SHEET NO. 23 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	139
CONTRACT NO.			61E73	

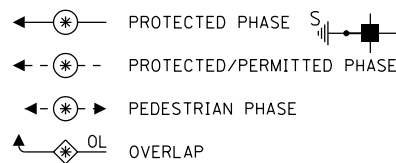
**TS A.205
EAGLE 2A**

FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

PROPOSED CONTROLLER SEQUENCE

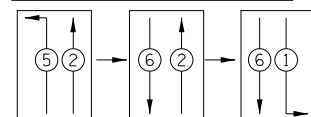


LEGEND:

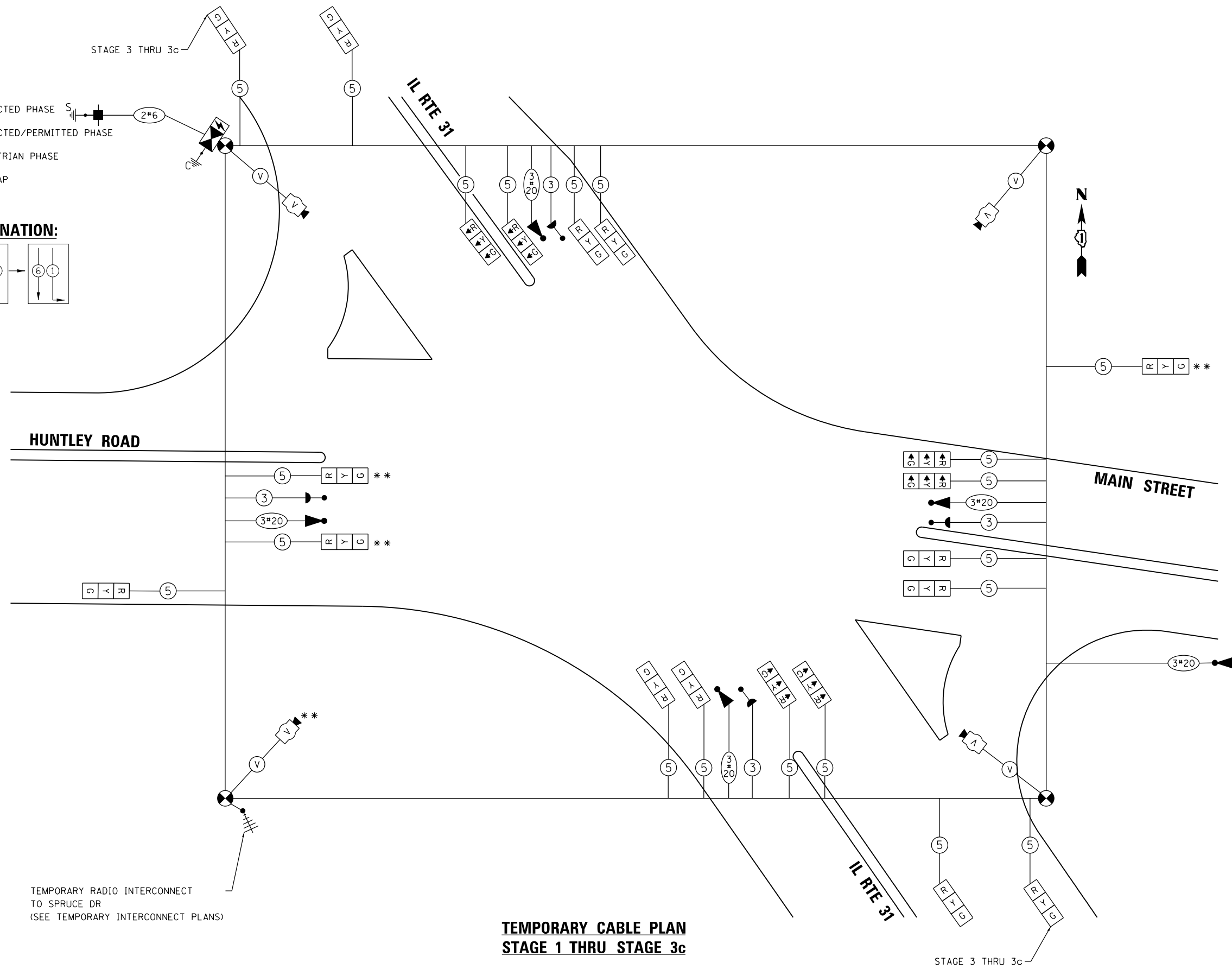
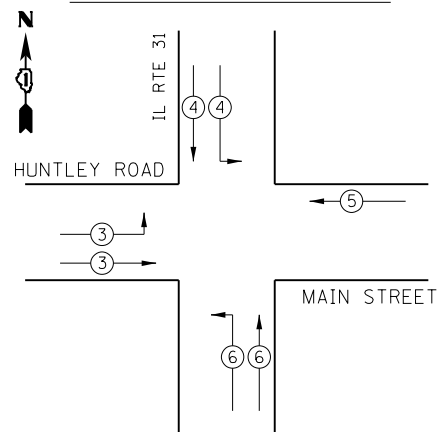


LEAD-LAG

PHASE DESIGNATION:



PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	18	11	50	99.0
(YELLOW)	18	20	5	18.0
(GREEN)	18	12	45	97.2
PERMISSIVE ARROW	-	10	10	-
PED. SIGNAL	-	20	100	-
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				339.2

ENERGY COSTS TO:
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 201 W. CENTER COURT
 SCHAUMBURG, IL 60196
 ENERGY SUPPLY: CONTACT: TOM MAHAR
 PHONE: 815-724-5010
 COMPANY: COMMONWEALTH EDISON
 ACCOUNT NUMBER: ---

TEMPORARY RADIO INTERCONNECT TO SPRUCE DR (SEE TEMPORARY INTERCONNECT PLANS)

* NORTHBOUND AND SOUTHBOUND LEFT TURNS MUST BE SEPARATED USING LEAD-LAG PHASING. SIGNAL TIMING AND PHASE DESIGNATION SHALL BE DETERMINED BY SCAT CONSULTANT PRIOR TO CONSTRUCTION.

** CAMERA SHALL BE DISCONNECTED AND SIGNAL HEADS SHALL BE BAGGED AND DISCONNECTED IN STAGES 1, 2, 3, AND 3A.

TEMPORARY CABLE PLAN STAGE 1 THRU STAGE 3c

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-181gcbthunt-02.dgn
 PLOT DRIVER: ILpch.fwsoftc19
 PEN TABLE: PLOTLABEL.tbl
 TS SHT NO. 24



USER NAME = jmeihui	DESIGNED - TAY	REVISED -
PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM IL RTE 31 AND HUNTLEY ROAD/MAIN STREET

SCALE: N.T.S. SHEET NO. 24 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	140
CONTRACT NO.			61E73	

TS A.205 EAGLE 2A

FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.

REMOVAL AND RELOCATION NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 2 EACH ALUMINUM MAST ARM ASSEMBLY AND POLE
- 4 EACH SIGNAL POST
- 2 EACH SIGNAL HEAD, 2-FACE, 1-3 SECTION, 1-5 SECTION
- 2 EACH SIGNAL HEAD, 3-FACE, 1-3 SECTION, 2-5 SECTION
- 3 EACH TRAFFIC SIGNAL BACKPLATE

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE FOR PICK UP OF ALL EQUIPMENT TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

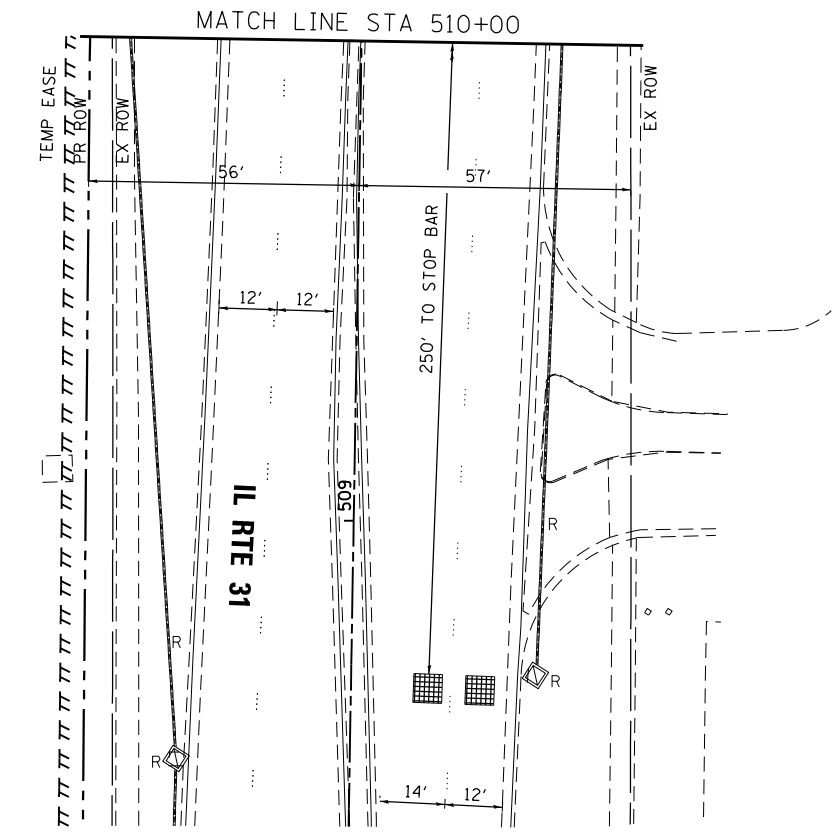
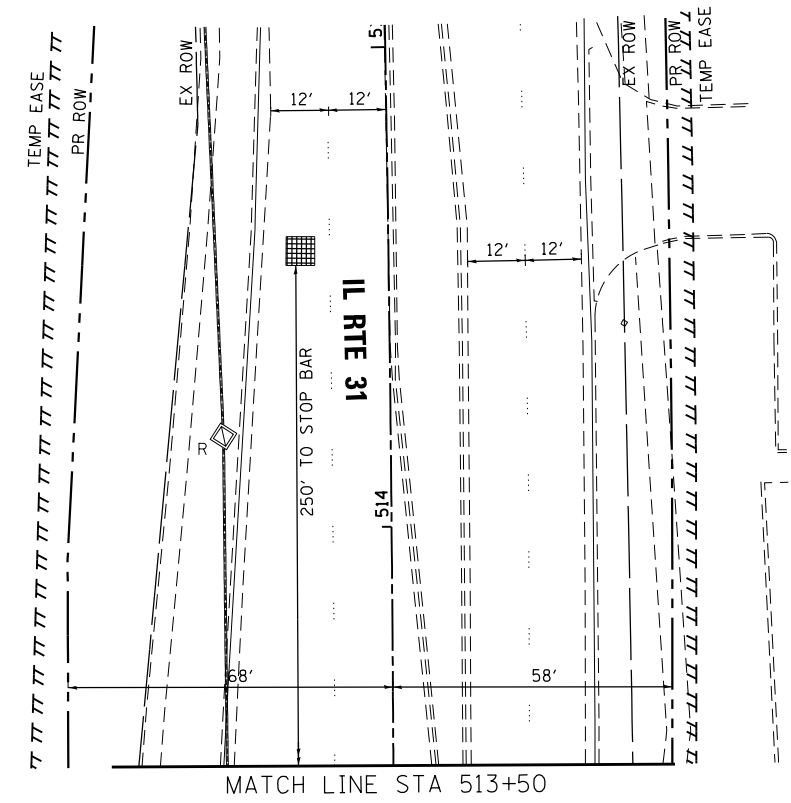
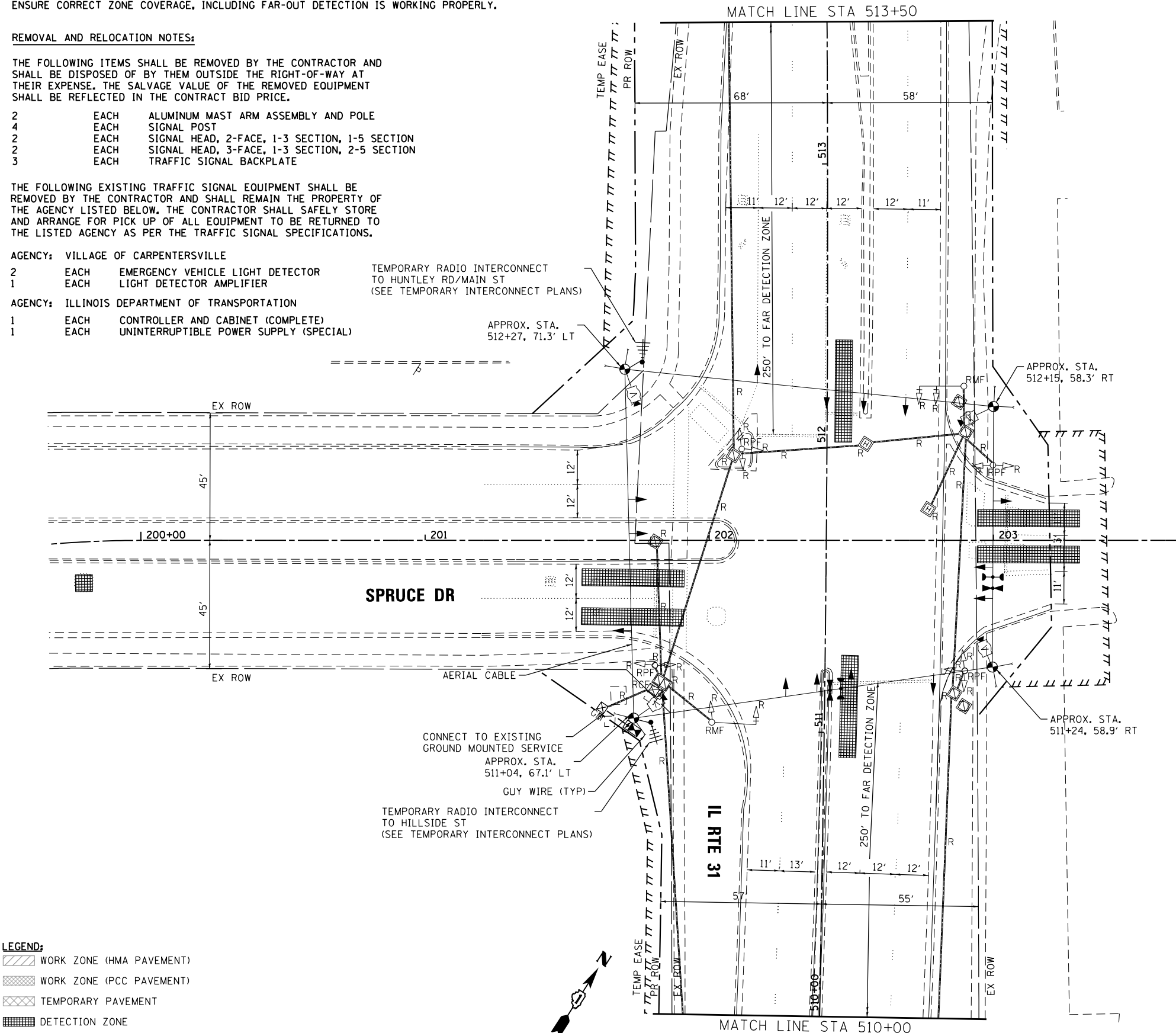
AGENCY: VILLAGE OF CARPENTERSVILLE

- 2 EACH EMERGENCY VEHICLE LIGHT DETECTOR
- 1 EACH LIGHT DETECTOR AMPLIFIER

TEMPORARY RADIO INTERCONNECT TO HUNTLEY RD/MAIN ST (SEE TEMPORARY INTERCONNECT PLANS)

AGENCY: ILLINOIS DEPARTMENT OF TRANSPORTATION

- 1 EACH CONTROLLER AND CABINET (COMPLETE)
- 1 EACH UNINTERRUPTIBLE POWER SUPPLY (SPECIAL)



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

NOTES:

- THE EXISTING MAST ARM STREET NAME SIGNS SHOULD BE RELOCATED TO THE TEMPORARY TRAFFIC SIGNALS.

COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
 PLOT LABEL: #PLOT_LABEL#
 TS_SHT NO. 25

HRGreen.com
 Illinois Professional Design Firm
 #184-001322

USER NAME = jmelhus	DESIGNED - TAY	REVISED -
PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-PRESTAGE
IL RTE 31 AND SPRUCE DRIVE**

SCALE: 1"=20' SHEET NO. 25 OF 53 SHEETS STA. TO STA.

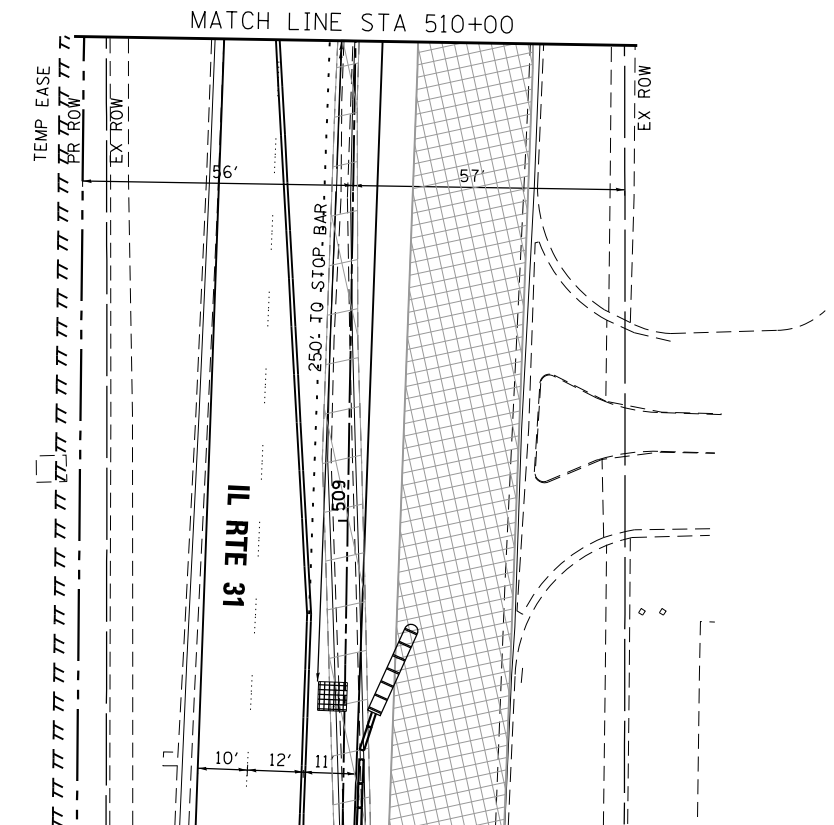
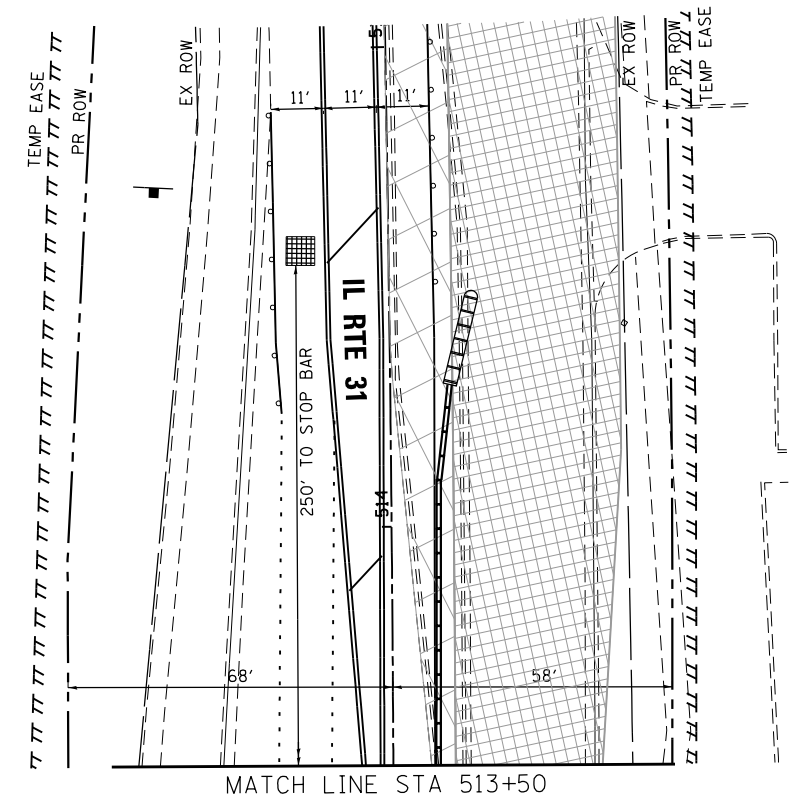
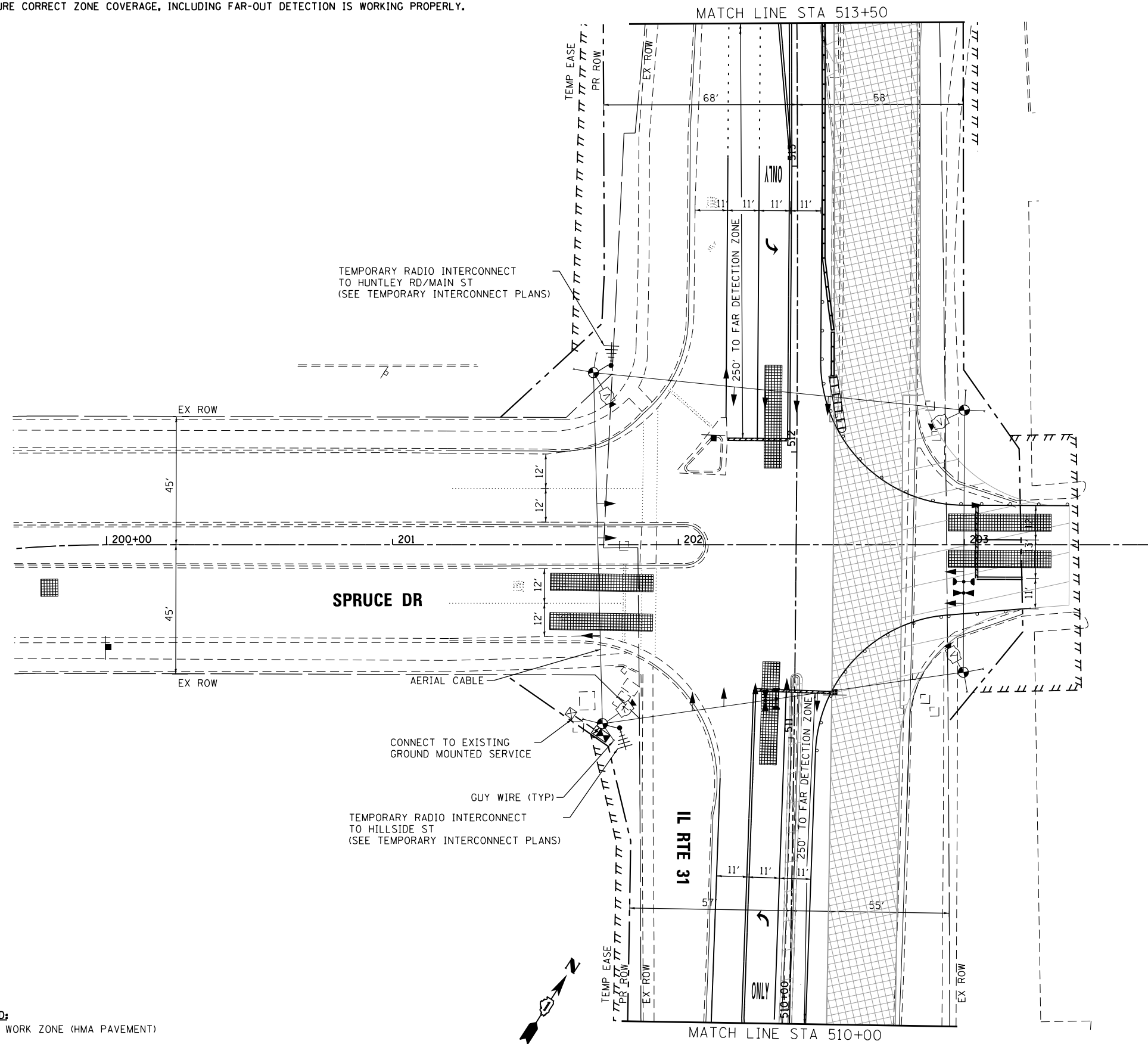
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	141
CONTRACT NO.			61E73	

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

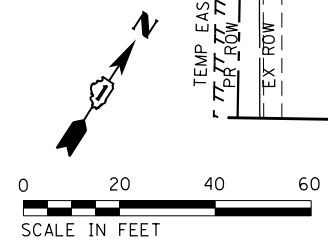
**TS A.204
EAGLE 2A**

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-1st-03.dgn
 PLOT DRIVER: ILpch.plt
 PEN TABLE: PLOTLABEL.tbl
TS SHT NO. 26



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 1
IL RTE 31 AND SPRUCE DRIVE**

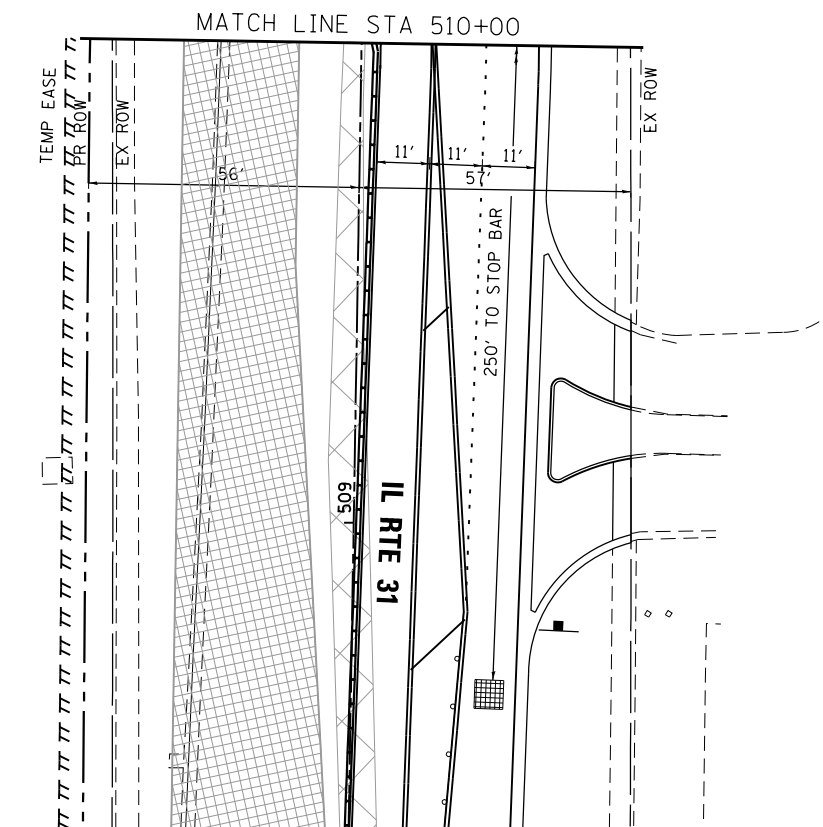
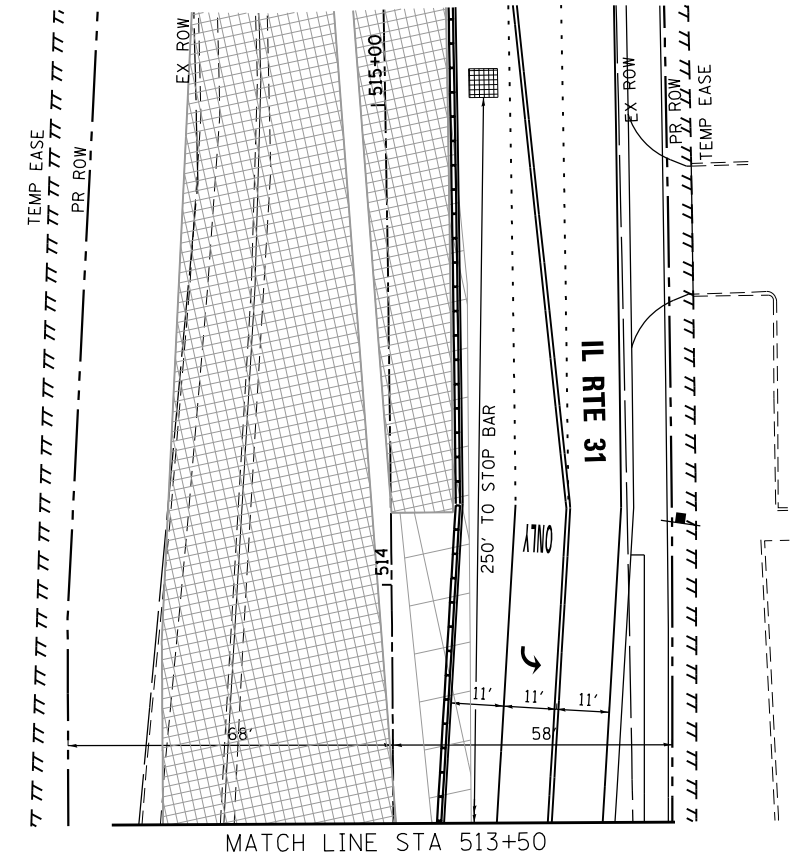
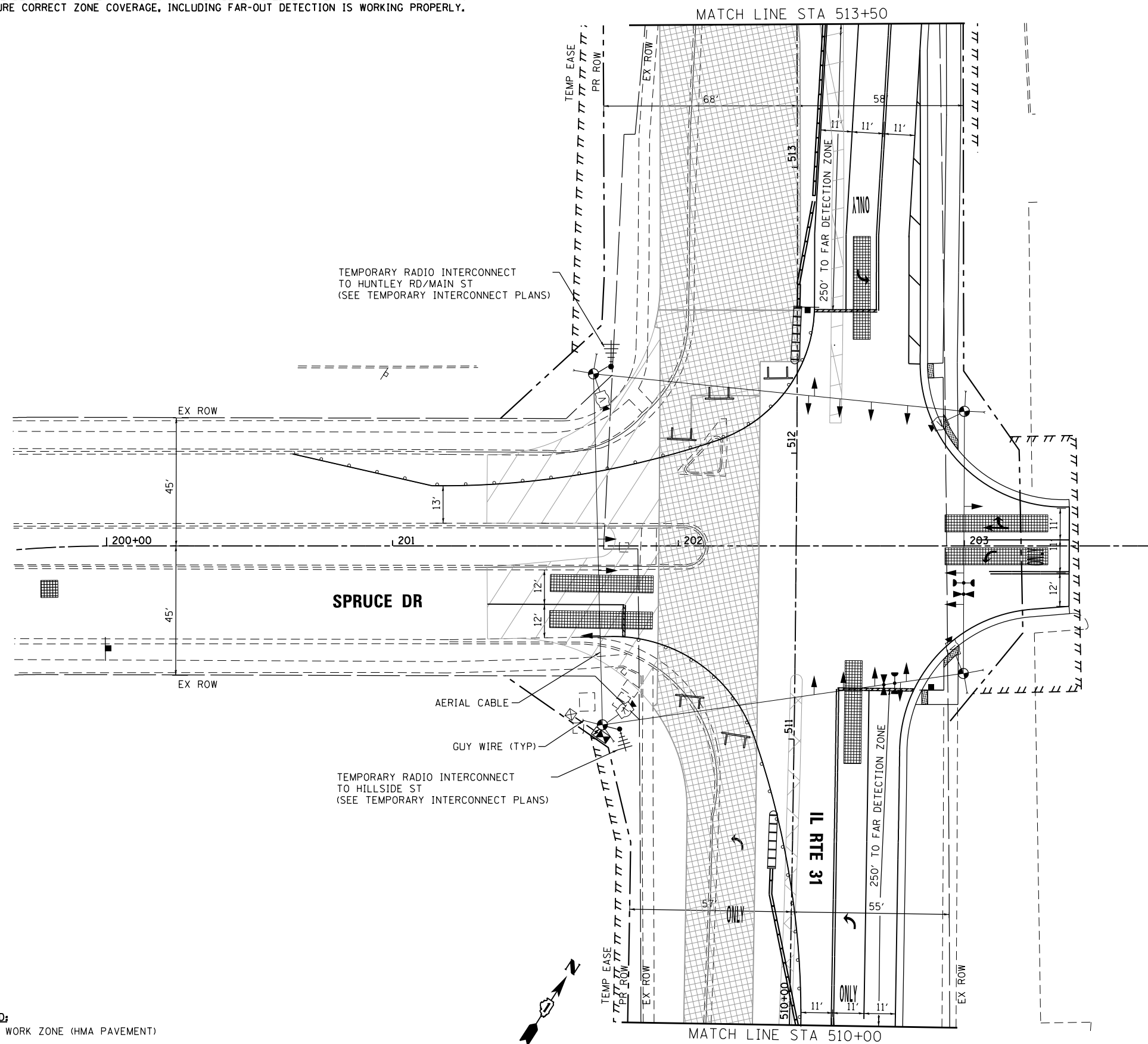
SCALE: 1"=20' SHEET NO. 26 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 142
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 61E73	

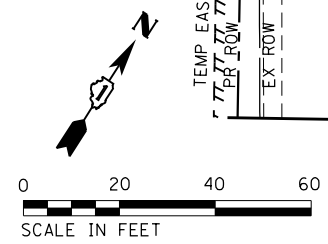
**TS A.204
EAGLE 2A**

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: HRGreen.com
 FILE NAME: 86140100-sh1-191g2-03.dgn
 PLOT DRIVER: IL_rte31_traffic_signal.dgn
 PEN TABLE: PLOT_LABEL.tbl
TS SHT NO. 27



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 2
IL RTE 31 AND SPRUCE DRIVE**

SCALE: 1"=20' SHEET NO. 27 OF 53 SHEETS STA. TO STA.

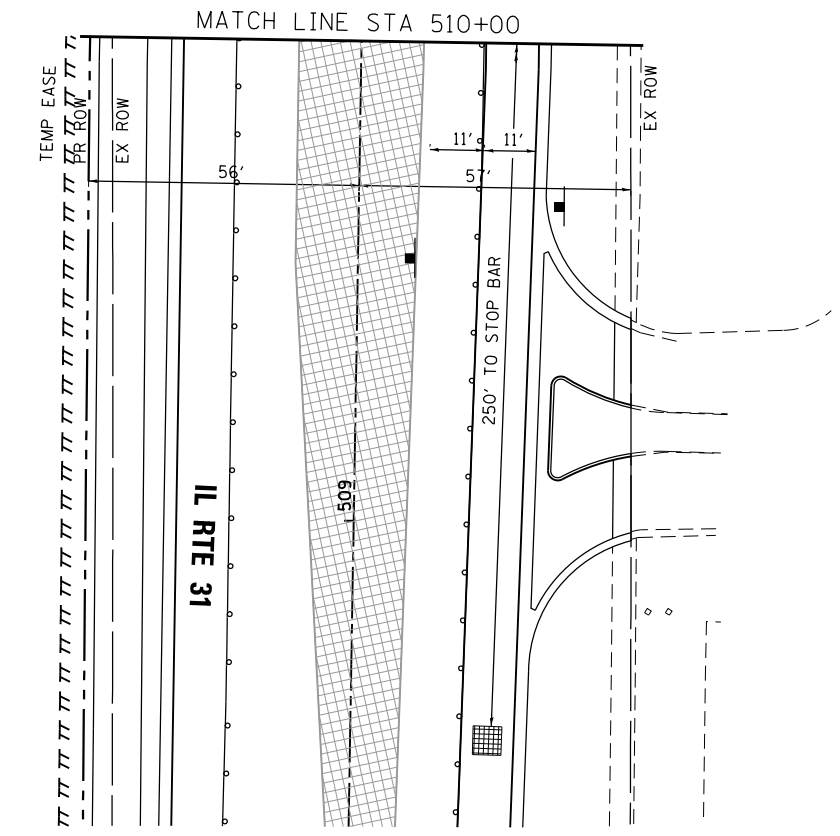
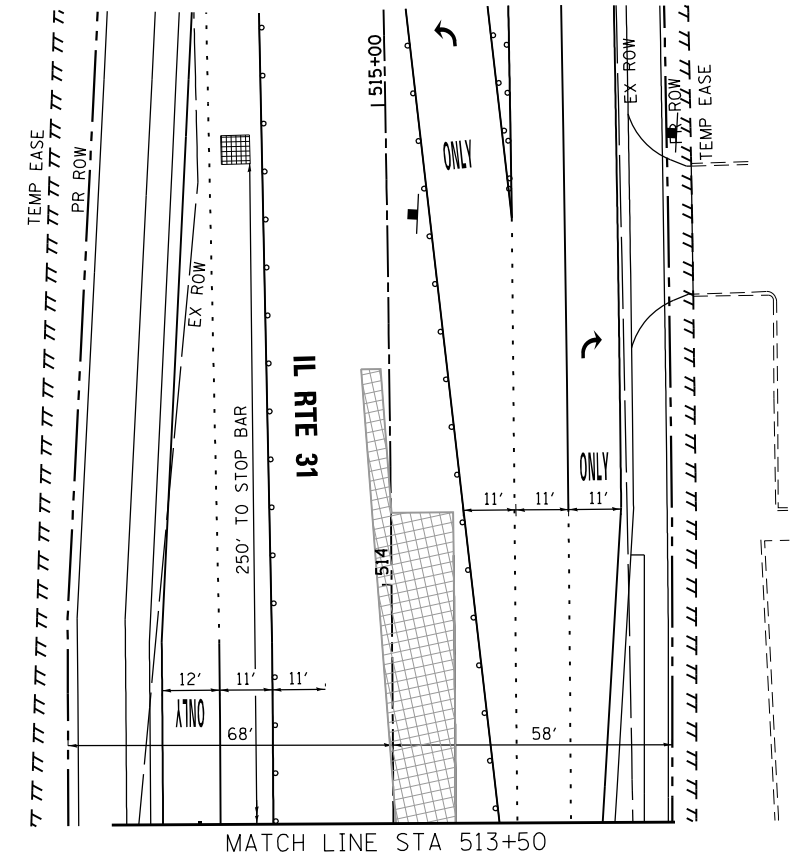
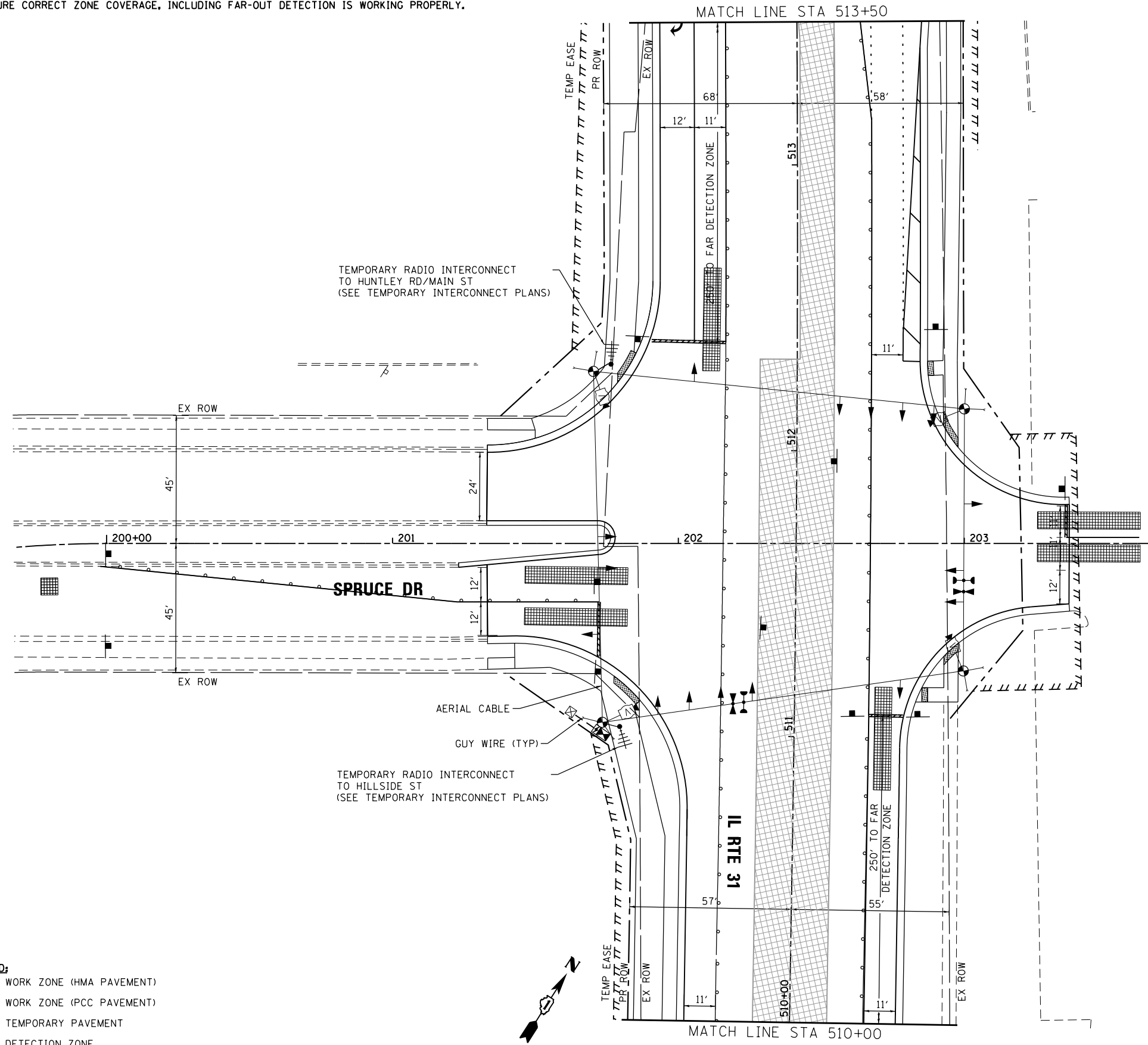
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	143
CONTRACT NO.			61E73	

**TS A.204
EAGLE 2A**

86140100
in: 86140100-sh1-191g2-03.dgn

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



LEGEND:

- WORK ZONE (HMA PAVEMENT)
- WORK ZONE (PCC PAVEMENT)
- TEMPORARY PAVEMENT
- DETECTION ZONE

0 20 40 60
SCALE IN FEET

TS SHT NO. 28

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jme1hui
 CLIENT: HRGreen.com
 FILE NAME: 86140100-sh1-1913-03.dgn
 PLOT DRIVER: IL_rte31_1913-03.dgn
 PEN TABLE: PLOT_LABEL.tbl

USER NAME = jme1hui	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISOR -	REVISION -
CHECKED - JRM	REVISOR -	REVISION -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISION -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3
IL RTE 31 AND SPRUCE DRIVE**

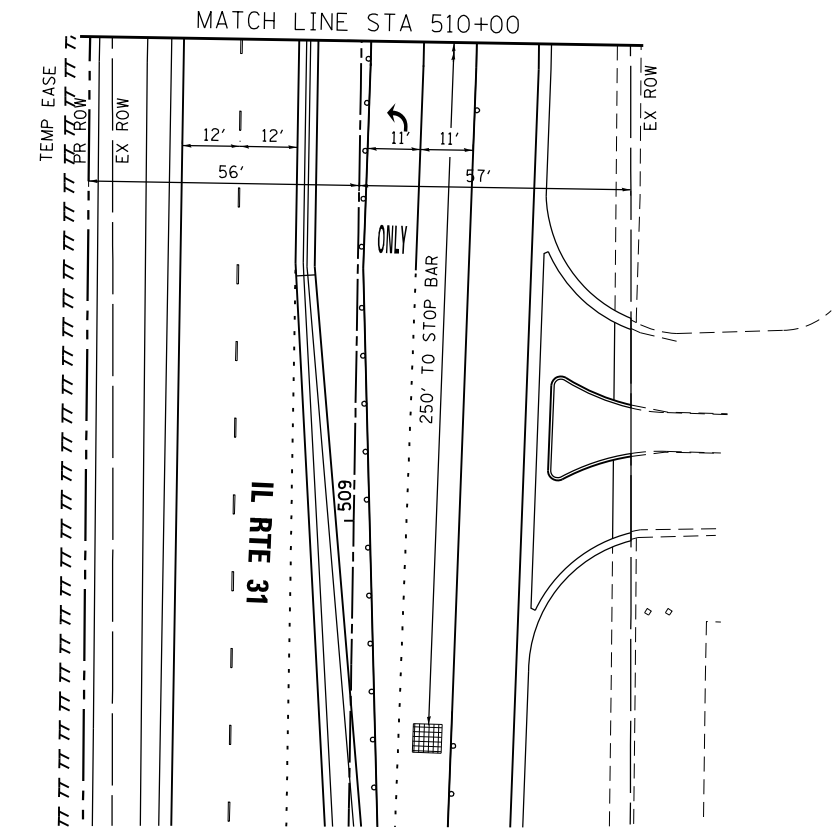
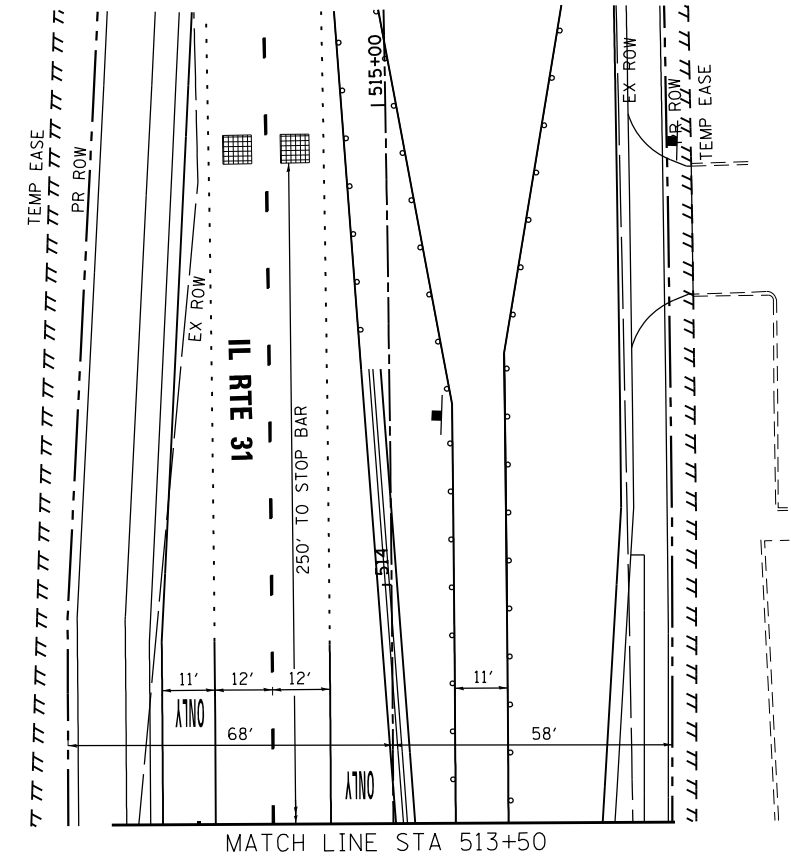
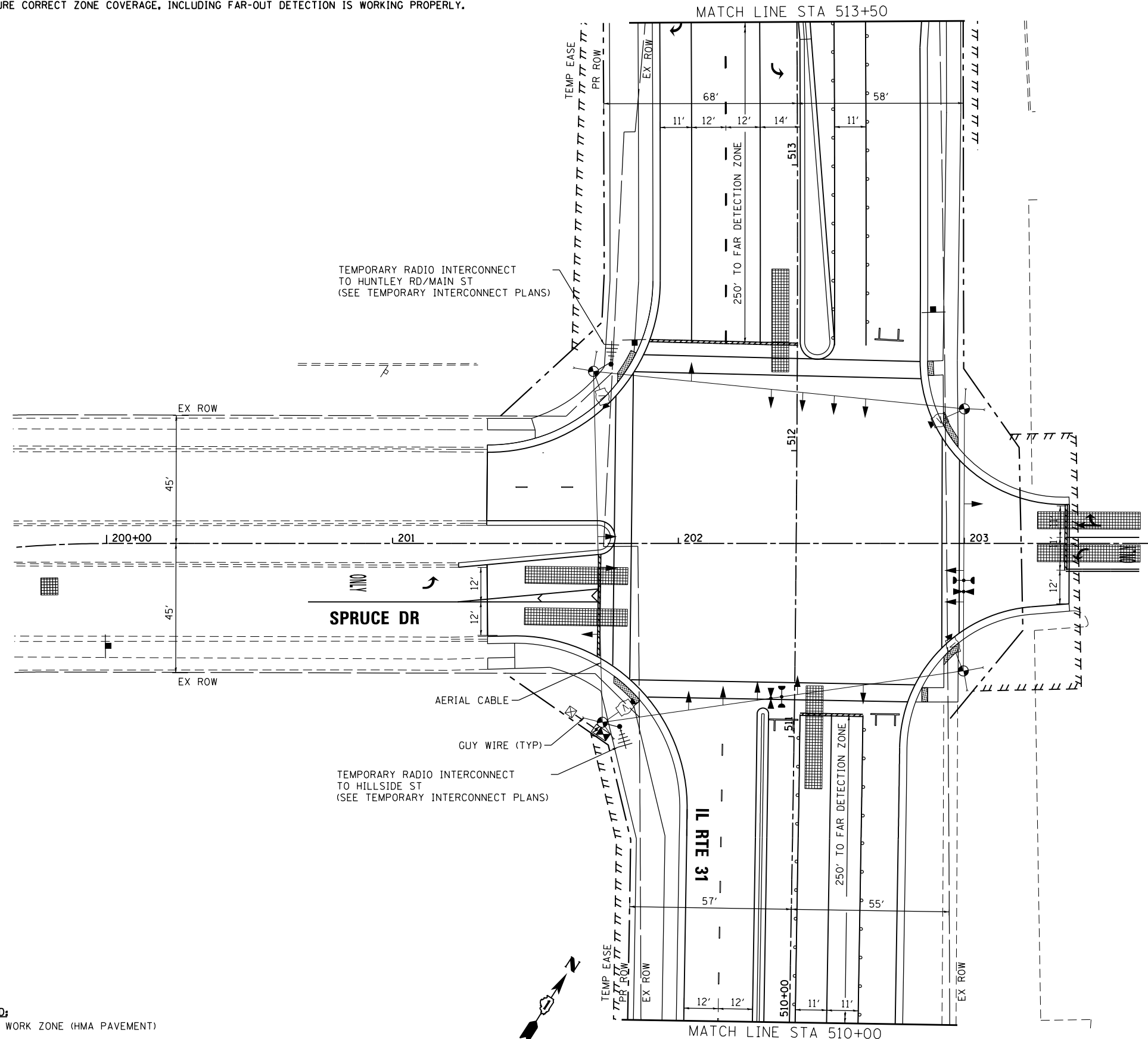
SCALE: 1"=20' SHEET NO. 28 OF 53 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	144
CONTRACT NO.			61E73	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

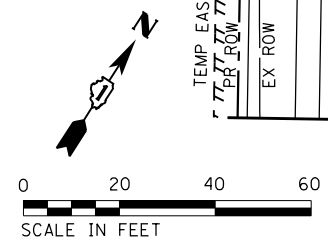
**TS A.204
EAGLE 2A**

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-1913a-03.dgn
 PLOT DRIVER: ILpch.plw
 PEN TABLE: PLOT LABEL.tbl

TS SHT NO. 29



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
 EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3A
 IL RTE 31 AND SPRUCE DRIVE**

SCALE: 1"=20' SHEET NO. 29 OF 53 SHEETS STA. TO STA.

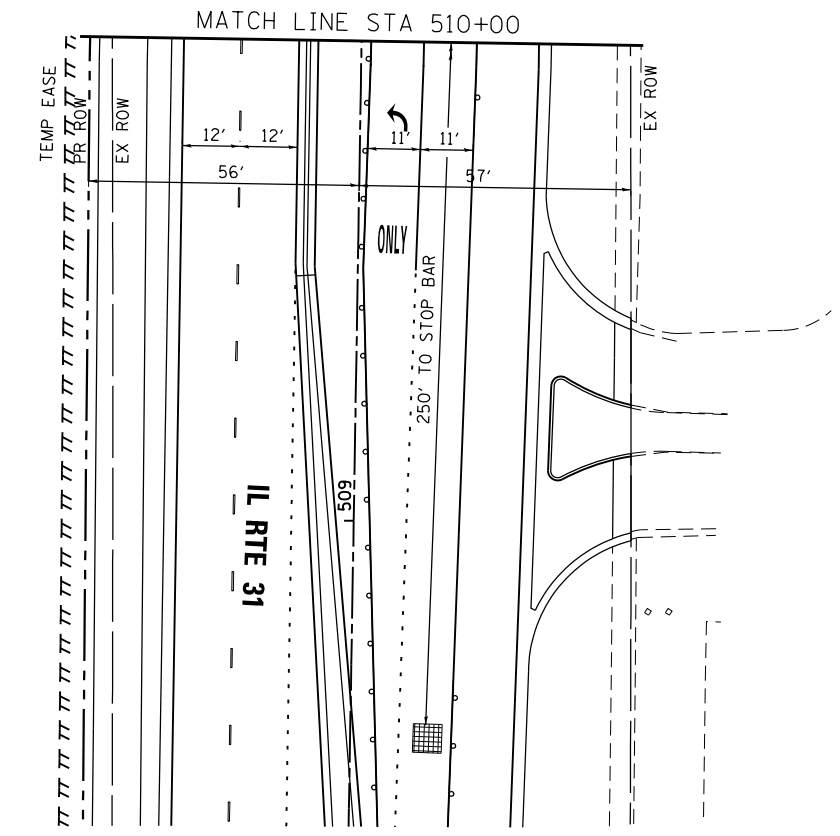
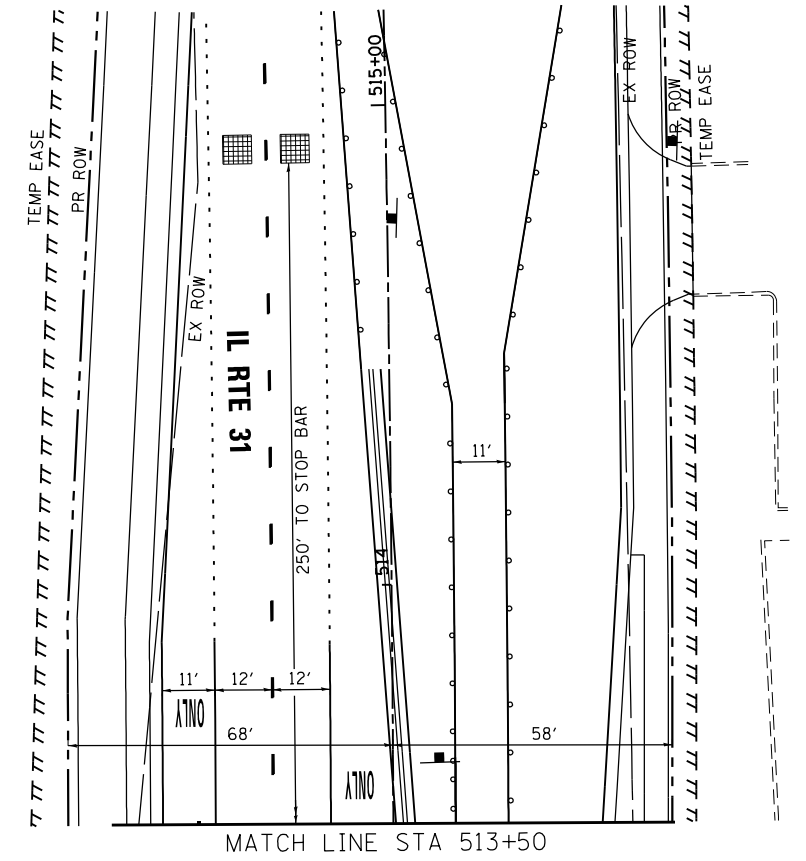
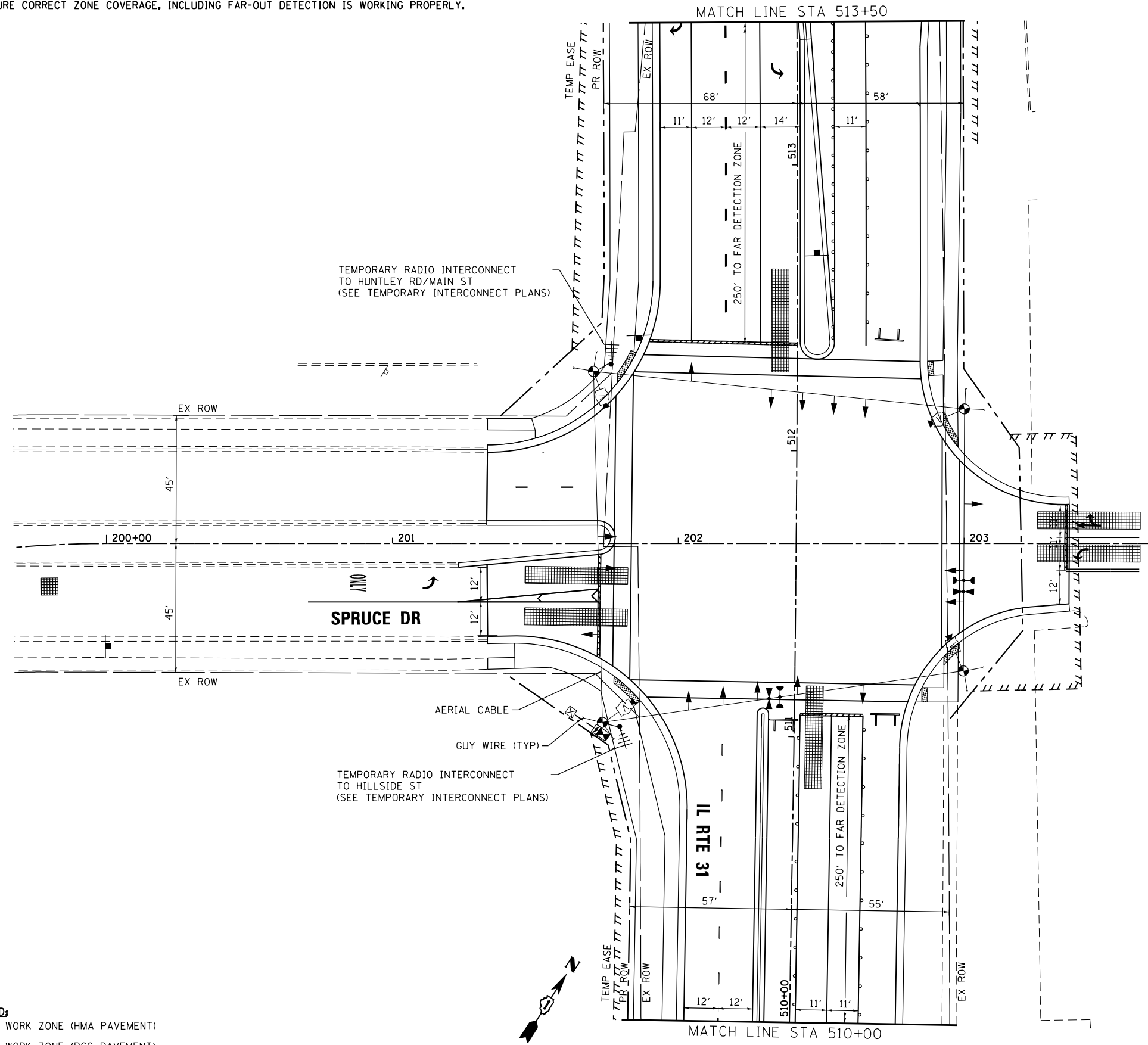
F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 145
CONTRACT NO. 61E73				

**TS A.204
 EAGLE 2A**

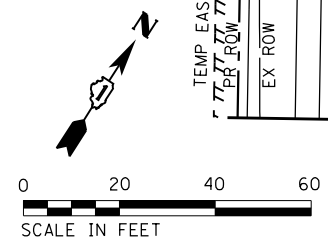
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen
 PROJECT CONTACT: jme1hu
 CLIENT: HRGreen
 FILE NAME: 86140100-sh1-1913b-03.dgn
 PLOT DRIVER: IL_rch_low_rpt.ctb
 PEN TABLE: PLOT_LABEL.tbl

TS SHT NO. 30



USER NAME = jme1hu	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
 EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3B
 IL RTE 31 AND SPRUCE DRIVE**

SCALE: 1"=20' SHEET NO. 30 OF 53 SHEETS STA. TO STA.

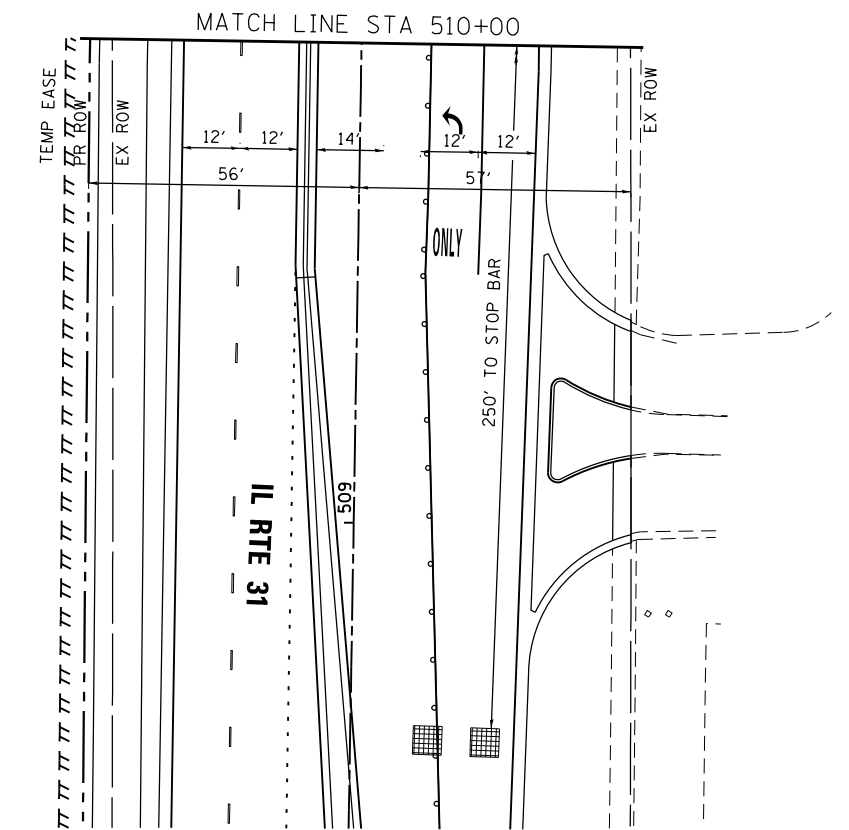
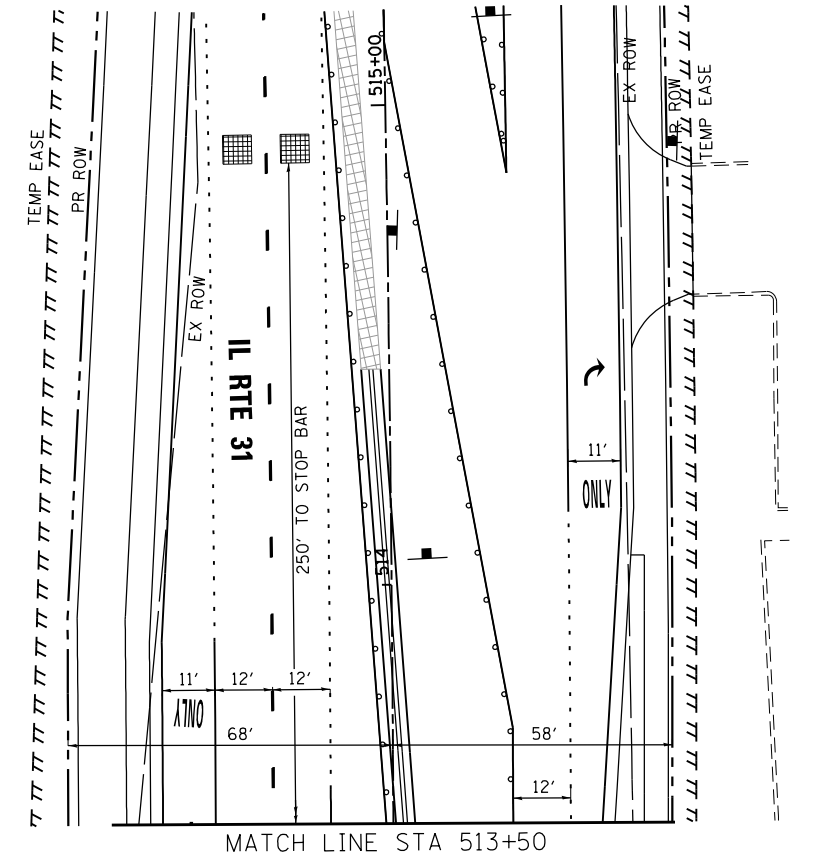
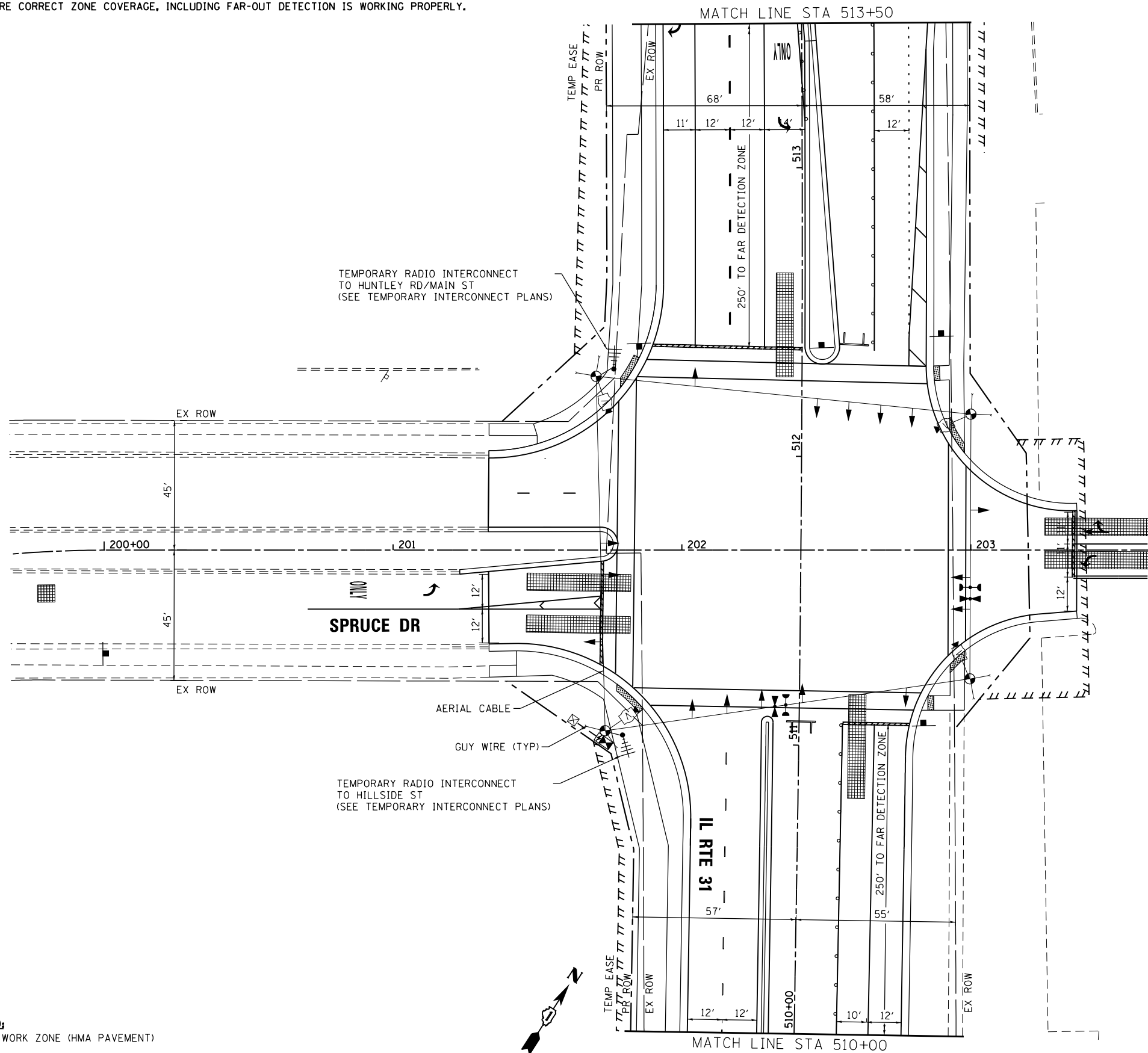
F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 146
CONTRACT NO. 61E73				

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

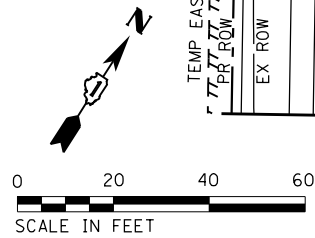
**TS A.204
 EAGLE 2A**

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE



COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-1913c-03.dgn
 PLOT DRIVER: ILpch.plt
 PEN TABLE: PLOT LABEL.tbl

TS SHT NO. 31



USER NAME = jme1hui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

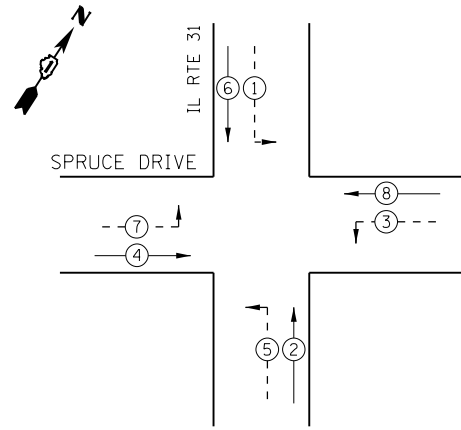
**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
 EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3C
 IL RTE 31 AND SPRUCE DRIVE**

SCALE: 1"=20'	SHEET NO. 31 OF 53 SHEETS	STA. _____ TO STA. _____
---------------	---------------------------	--------------------------

F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 147
CONTRACT NO. 61E73			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

**TS A.204
 EAGLE 2A**

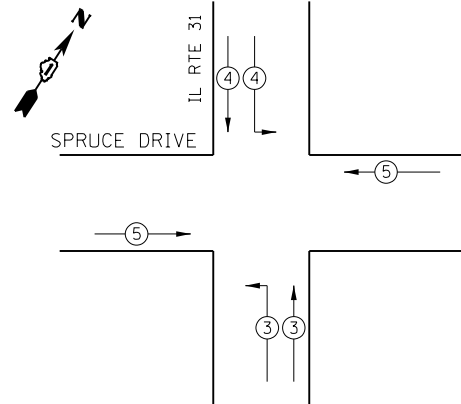
PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← (⊙) ← PROTECTED PHASE
- ← (⊙) ← PROTECTED/PERMITTED PHASE
- ← (⊙) ← PEDESTRIAN PHASE
- ← (⊙) ← OVERLAP

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	14	11	50	77.0
(YELLOW)	14	20	5	14.0
(GREEN)	14	12	45	75.6
PERMISSIVE ARROW	8	10	10	8.0
PED. SIGNAL	-	20	100	-
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				299.6

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W. CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: TOM MAHAR
PHONE: 815-724-5010
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

* NORTHBOUND AND SOUTHBOUND LEFT TURNS MUST BE SEPARATED USING LEAD-LAG PHASING. SIGNAL TIMING AND PHASE DESIGNATION SHALL BE DETERMINED BY SCAT CONSULTANT PRIOR TO CONSTRUCTION.

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
 TS_SHT NO. 32



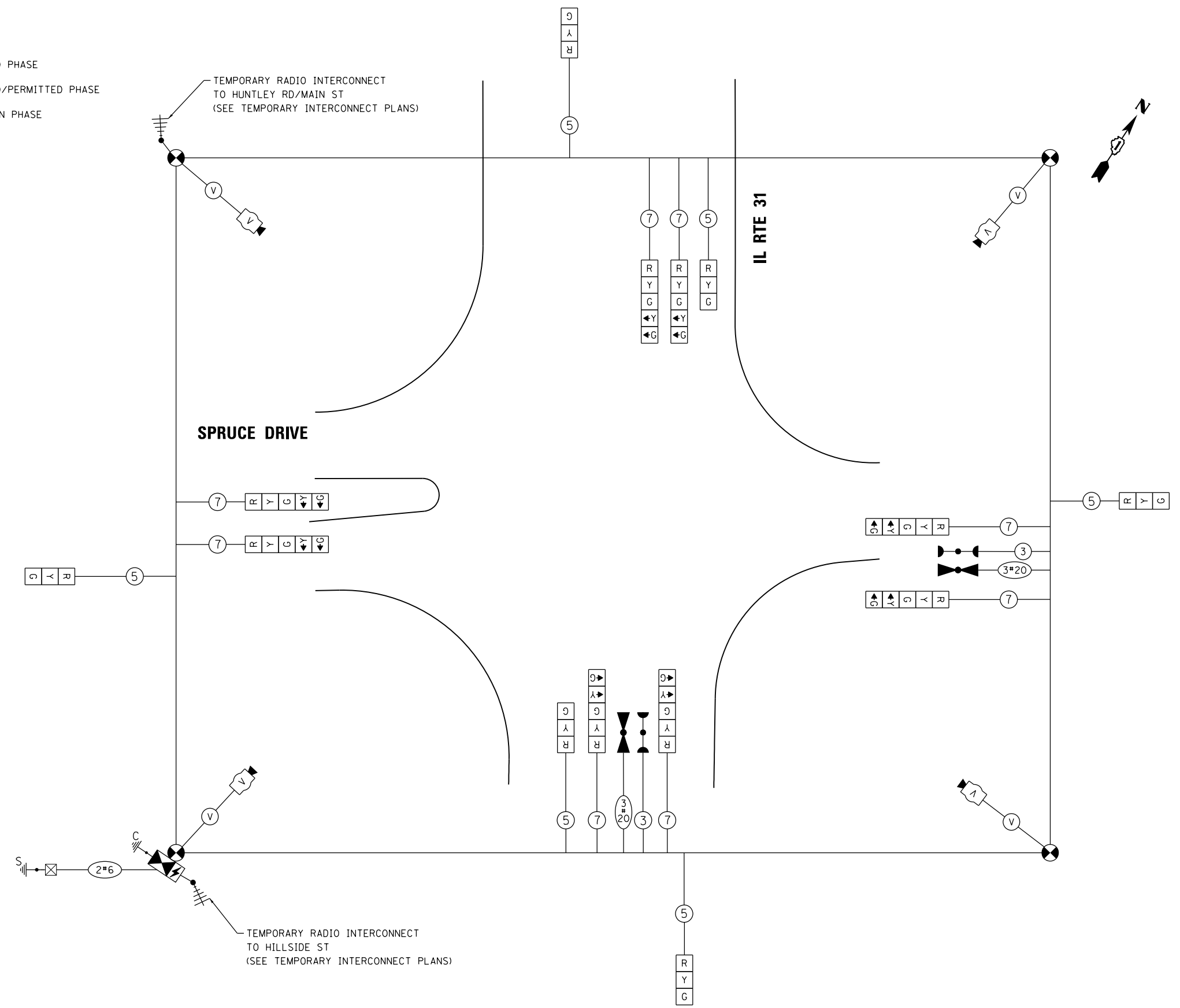
USER NAME = jme1hui	DESIGNED - TAY	REVISED - -
PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED - -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED - -
	DATE - 3/23/18	REVISED - -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM
IL RTE 31 AND SPRUCE DRIVE**

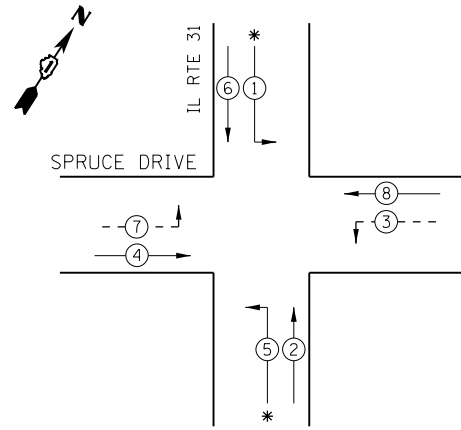
SCALE: N.T.S. SHEET NO. 32 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	148
CONTRACT NO.			61E73	



TS A.204
EAGLE 2A

PROPOSED CONTROLLER SEQUENCE

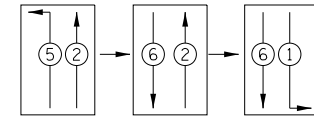


LEGEND:

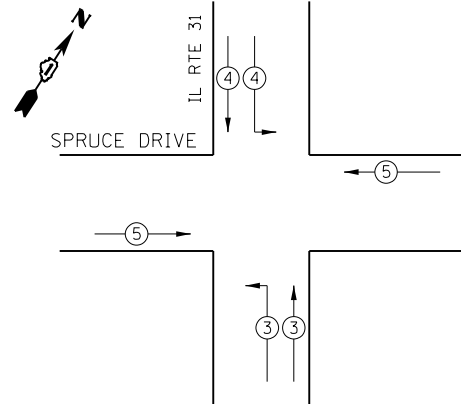
- ← (circle with asterisk) → PROTECTED PHASE
- ← (circle with asterisk and dashed line) → PROTECTED/PERMITTED PHASE
- ← (circle with asterisk and double arrow) → PEDESTRIAN PHASE
- ← (circle with asterisk and OL) → OVERLAP

LEAD-LAG

PHASE DESIGNATION:



PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

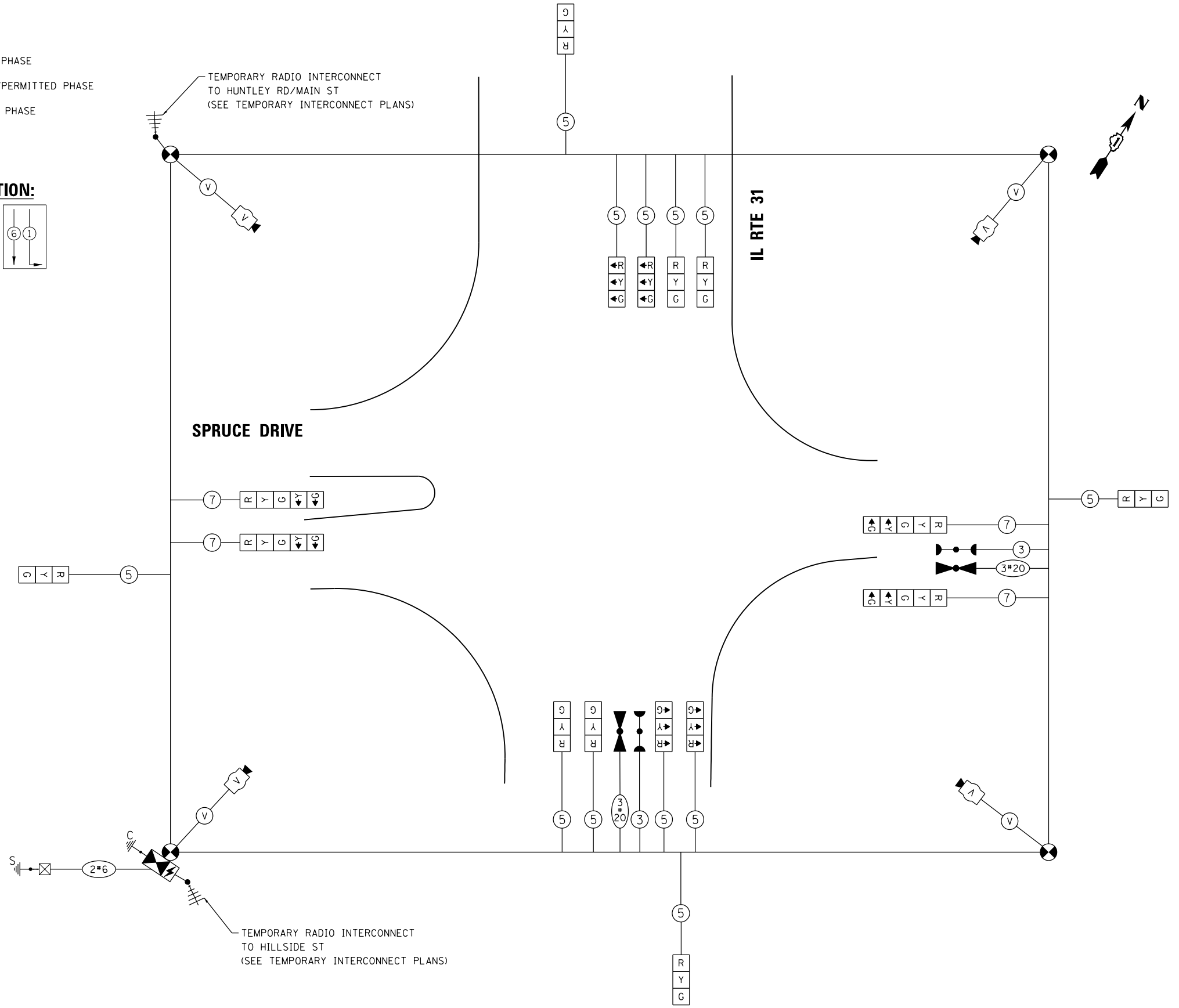
TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	16	11	50	88.0
(YELLOW)	16	20	5	16.0
(GREEN)	16	12	45	86.4
PERMISSIVE ARROW	4	10	10	4.0
PED. SIGNAL	-	20	100	-
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				319.4

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W. CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: TOM MAHAR
PHONE: 815-724-5010
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

* NORTHBOUND AND SOUTHBOUND LEFT TURNS MUST BE SEPARATED USING LEAD-LAG PHASING. SIGNAL TIMING AND PHASE DESIGNATION SHALL BE DETERMINED BY SCAT CONSULTANT PRIOR TO CONSTRUCTION.



**TEMPORARY CABLE PLAN
STAGE 1 THRU STAGE 3c**

**TS A.204
EAGLE 2A**

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION
 FILE NAME: 86140100-sh1-181gcbcspruce-02.dgn
 PLOT DRIVER: ILpchf_lowoftc19
 PEN TABLE: PLOTLABEL.tbl
TS SHT NO. 33



USER NAME = jmelhui	DESIGNED - TAY	REVISED - -
	DRAWN - DMS	REVISED - -
PLOT SCALE = N.T.S.	CHECKED - JRM	REVISED - -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED - -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

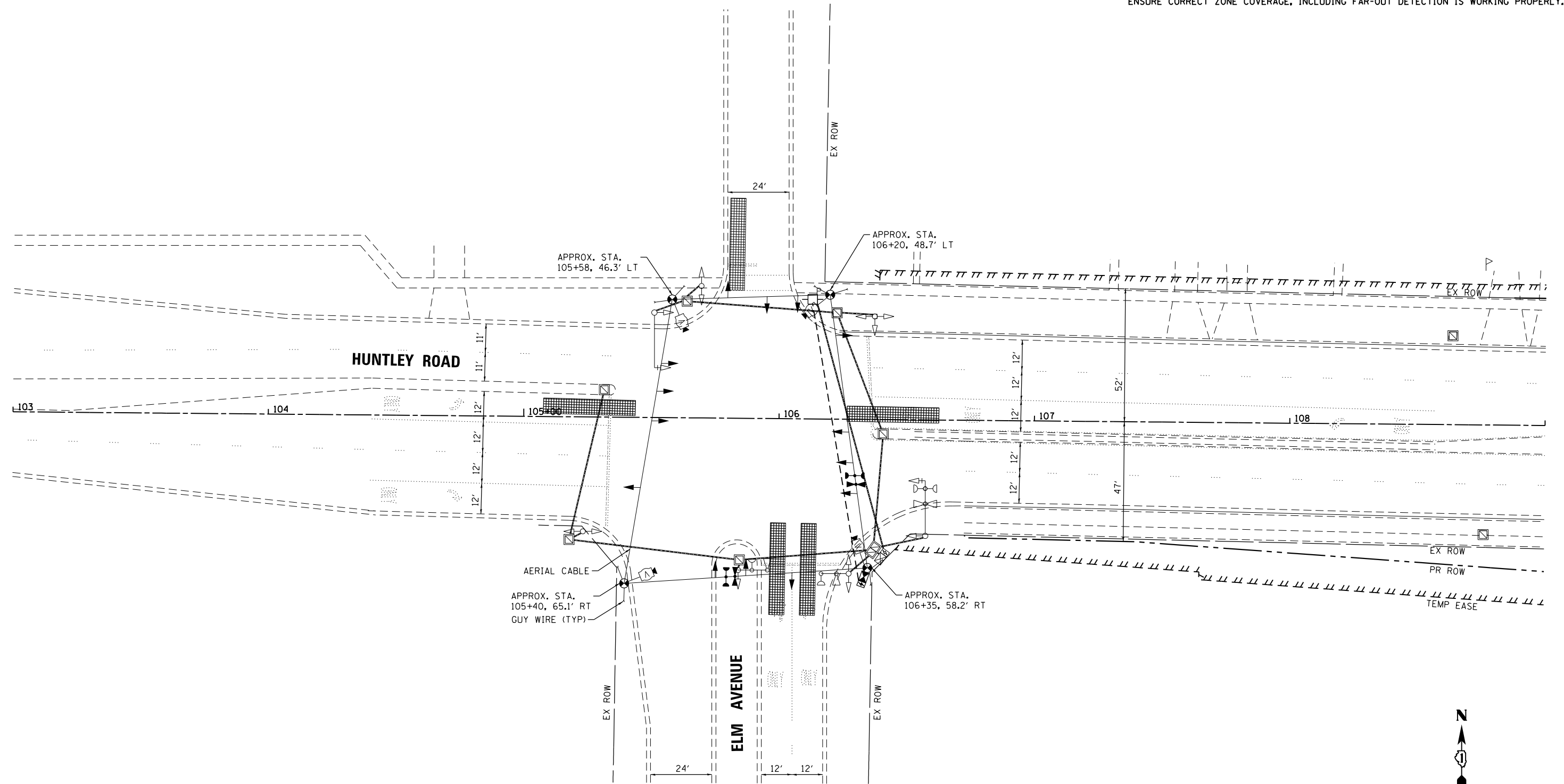
**TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM
IL RTE 31 AND SPRUCE DRIVE**

SCALE: N.T.S. SHEET NO. 33 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	149
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

CONSTRUCTION NOTES

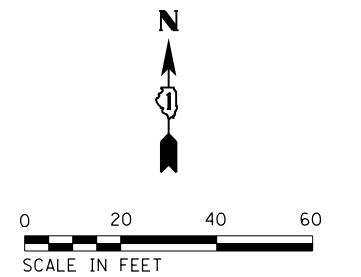
FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

NOTES:

- ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE PROTECTED AND MAINTAINED DURING CONSTRUCTION. EXISTING SIGNAL HEADS SHALL BE BAGGED. AFTER CONSTRUCTION IS COMPLETE, THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RESTORED TO ITS ORIGINAL STATE OF OPERATION.



COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-18gpr-04.dgn
 PLOT DRIVER: ILpch.plt
 PEN TABLE: PLOT LABEL.tbl
TS SHT NO. 34



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-PRESTAGE
IL RTE 31 AND ELM AVENUE**

SCALE: 1"=20' SHEET NO. 34 OF 53 SHEETS STA. _____ TO STA. _____

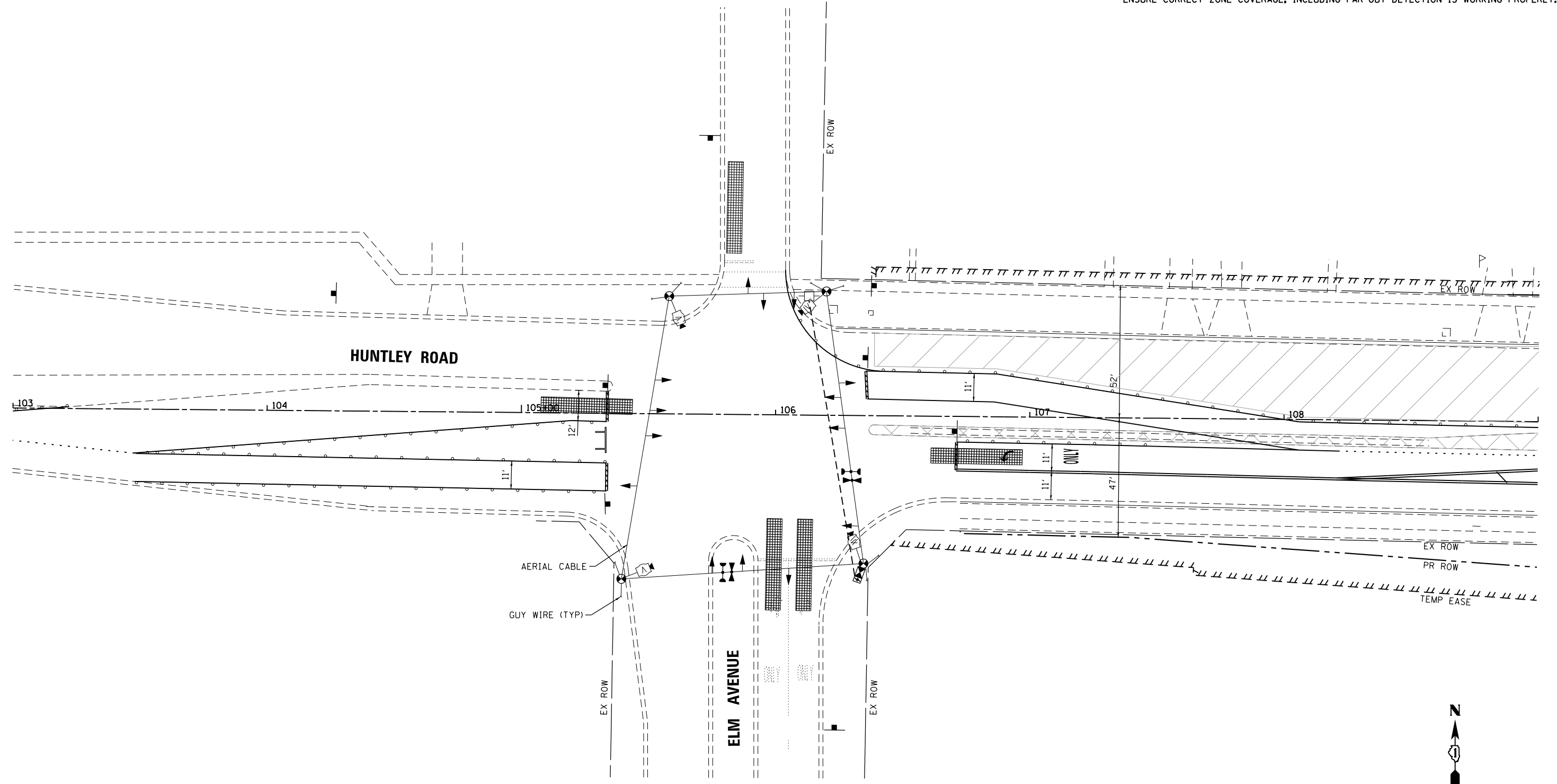
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	150
CONTRACT NO.			61E73	

**TS A.208
EAGLE 2A**

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

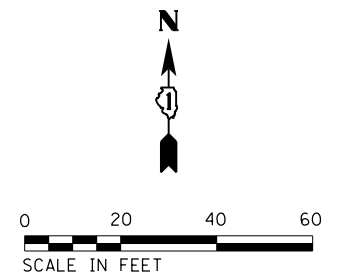
CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

- NOTES:**
- ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE PROTECTED AND MAINTAINED DURING CONSTRUCTION. EXISTING SIGNAL HEADS SHALL BE BAGGED. AFTER CONSTRUCTION IS COMPLETE, THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RESTORED TO ITS ORIGINAL STATE OF OPERATION.



COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: HRGreen.com
 FILE NAME: 86140100-sh1-t1sg1-04.dgn
 PLOT DRIVER: ILpch.plt
 PEN TABLE: PLOT LABEL.tbl
TS SHT NO. 35



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 1
IL RTE 31 AND ELM AVENUE**

SCALE: 1"=20' SHEET NO. 35 OF 53 SHEETS STA. _____ TO STA. _____

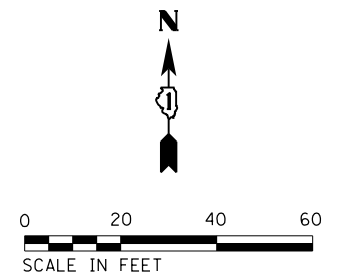
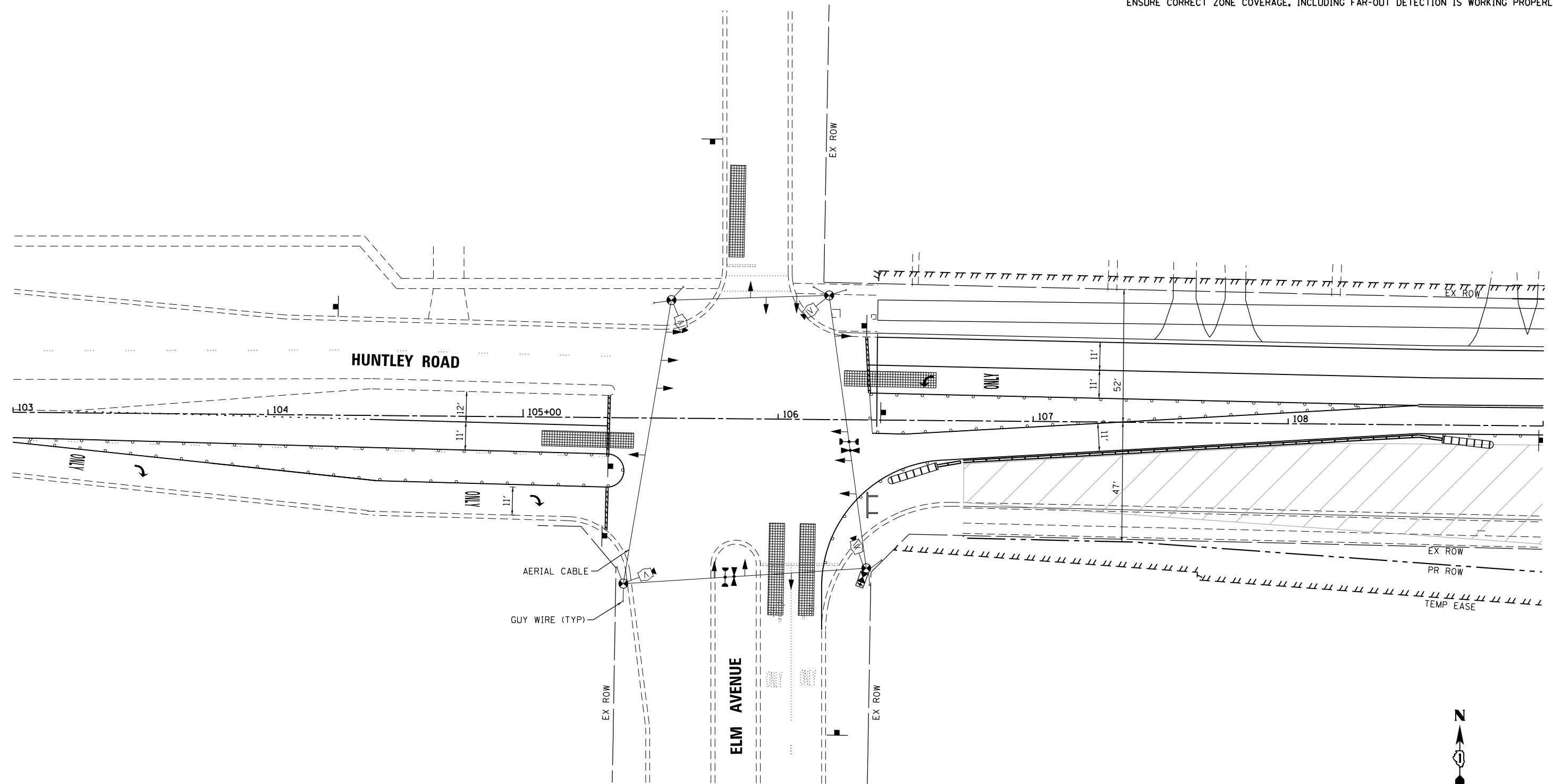
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	151
CONTRACT NO.			61E73	

**TS A.208
EAGLE 2A**

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-181g2-04.dgn
 PLOT DRIVER: ILpch.plw
 PEN TABLE: PLOTLABEL.tbl

TS SHT NO. 36



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
 EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 2
 IL RTE 31 AND ELM AVENUE**

SCALE: 1"=20' SHEET NO. 36 OF 53 SHEETS STA. _____ TO STA. _____

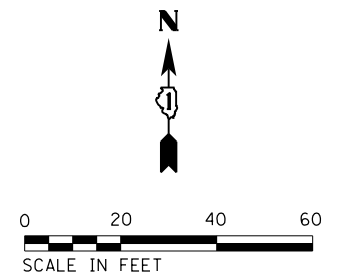
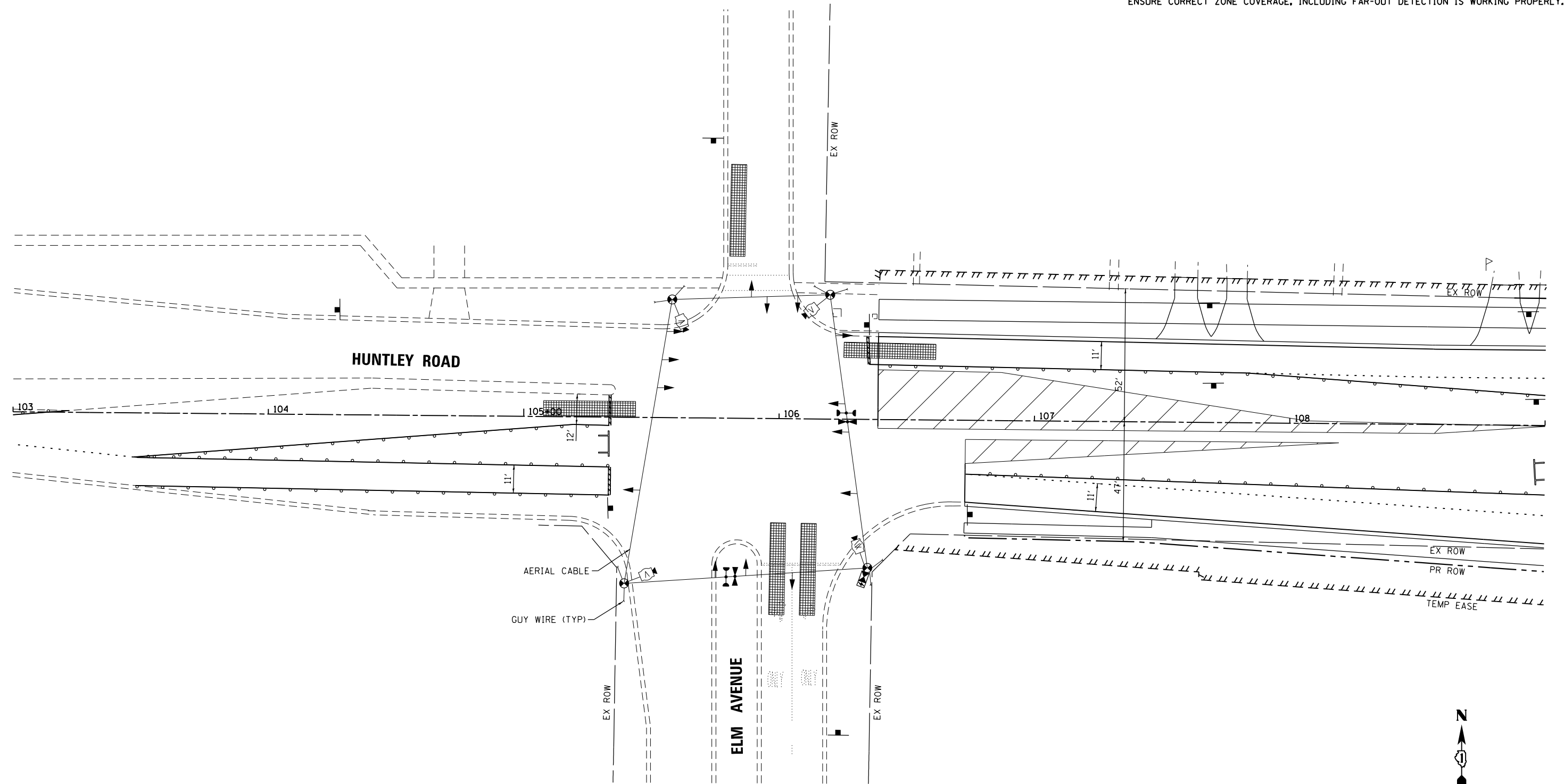
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	152
CONTRACT NO. 61E73				

**TS A.208
 EAGLE 2A**

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

NOTES:

- ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE PROTECTED AND MAINTAINED DURING CONSTRUCTION. EXISTING SIGNAL HEADS SHALL BE BAGGED. AFTER CONSTRUCTION IS COMPLETE, THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RESTORED TO ITS ORIGINAL STATE OF OPERATION.

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
 TS SHT NO. 37

HRGreen.com Illinois Professional Design Firm #184-001322	USER NAME = jmelhus	DESIGNED - TAY	REVISED - -
	PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED - -
	PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED - -
		DATE - 3/23/18	REVISED - -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3
IL RTE 31 AND ELM AVENUE

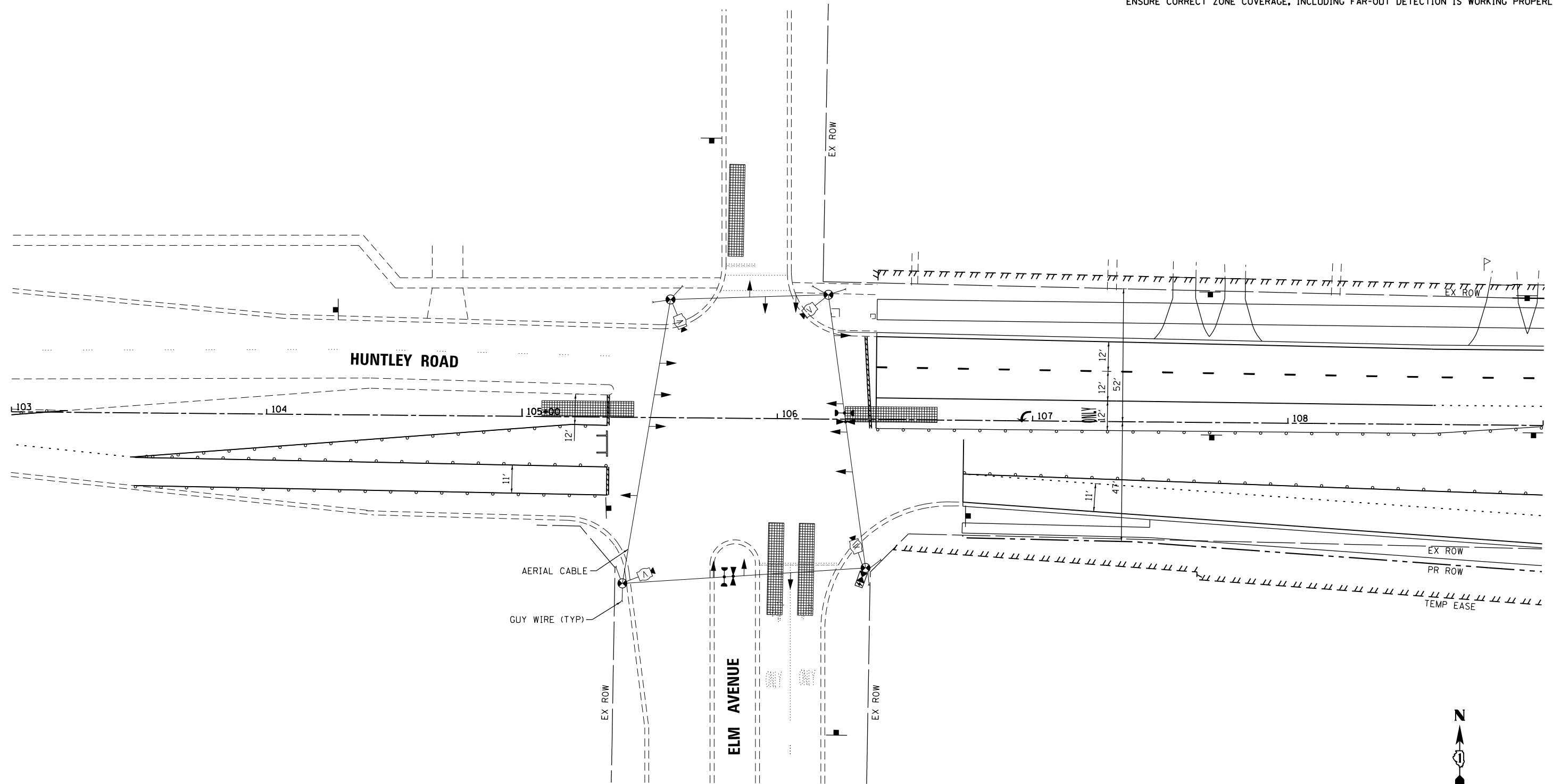
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	153
CONTRACT NO.			61E73	
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT				

TS A.208
EAGLE 2A

SCALE: 1"=20' SHEET NO. 37 OF 53 SHEETS STA. _____ TO STA. _____

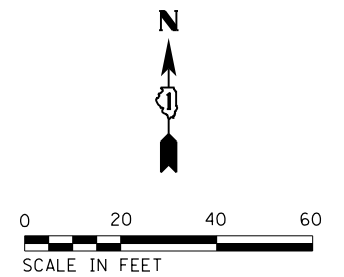
CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

- NOTES:**
- ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE PROTECTED AND MAINTAINED DURING CONSTRUCTION. EXISTING SIGNAL HEADS SHALL BE BAGGED. AFTER CONSTRUCTION IS COMPLETE, THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RESTORED TO ITS ORIGINAL STATE OF OPERATION.



COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
 TS SHT NO. 38
 86140100
 86140100-sh1-181g3a-04.dgn

HRGreen.com Illinois Professional Design Firm #184-001322	USER NAME = jmelhus	DESIGNED - TAY	REVISED - -
	PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED - -
	PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED - -
		DATE - 3/23/18	REVISED - -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3A
IL RTE 31 AND ELM AVENUE

SCALE: 1"=20' SHEET NO. 38 OF 53 SHEETS STA. _____ TO STA. _____

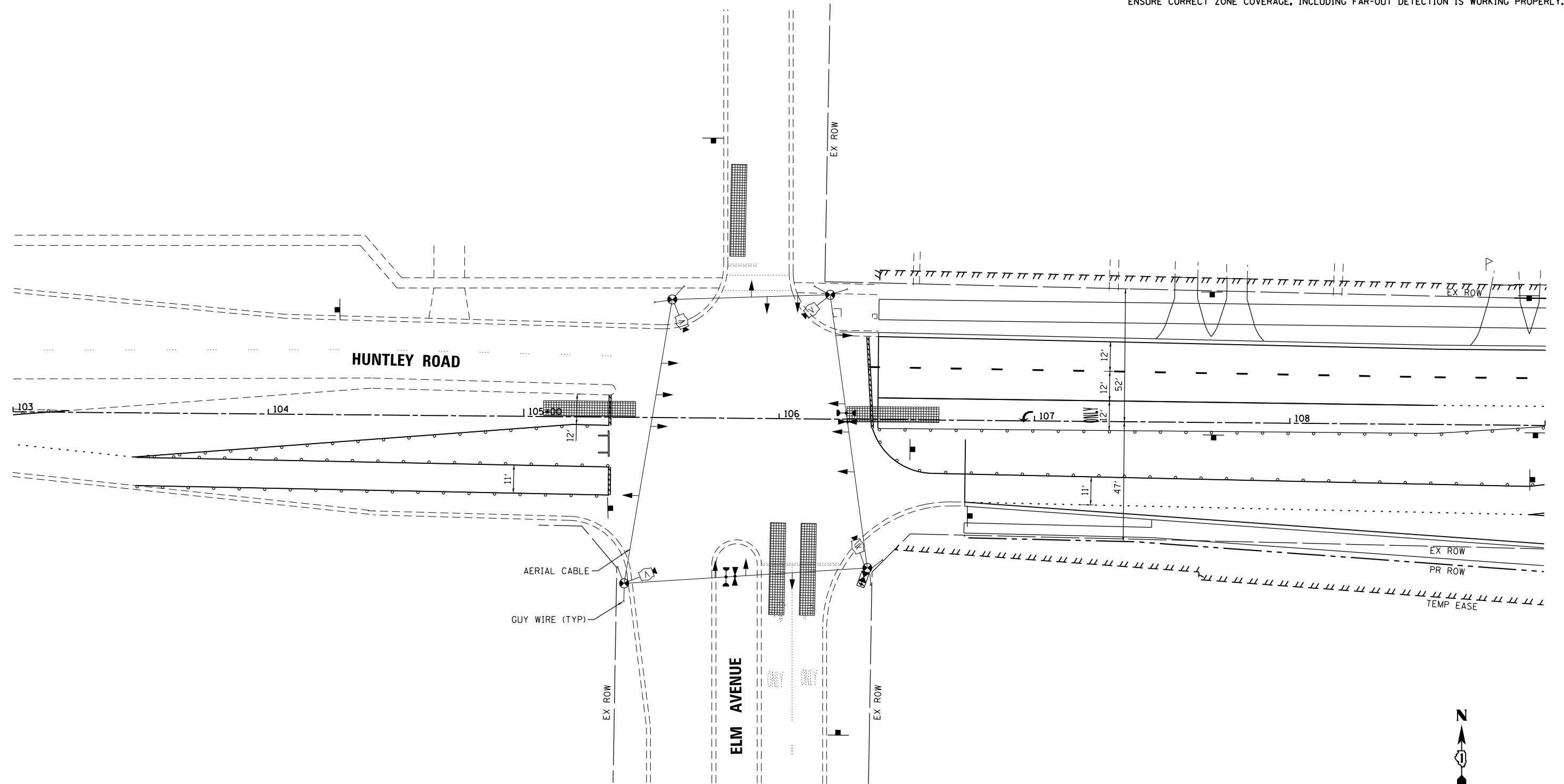
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	154
CONTRACT NO.			61E73	

TS A.208
EAGLE 2A

FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT

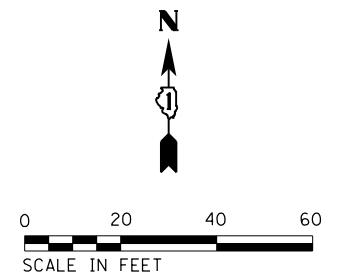
CONSTRUCTION NOTES

FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

- NOTES:**
- ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE PROTECTED AND MAINTAINED DURING CONSTRUCTION. EXISTING SIGNAL HEADS SHALL BE BAGGED. AFTER CONSTRUCTION IS COMPLETE, THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RESTORED TO ITS ORIGINAL STATE OF OPERATION.



COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-11-14-18-30-AM
 PLOT DRIVER: IL_rch_pwr_sht.ctb
 PEN TABLE: PLOT_LABEL.tbl
 TS_SHT NO. 39



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3B
IL RTE 31 AND ELM AVENUE**

SCALE: 1"=20' SHEET NO. 39 OF 53 SHEETS STA. _____ TO STA. _____

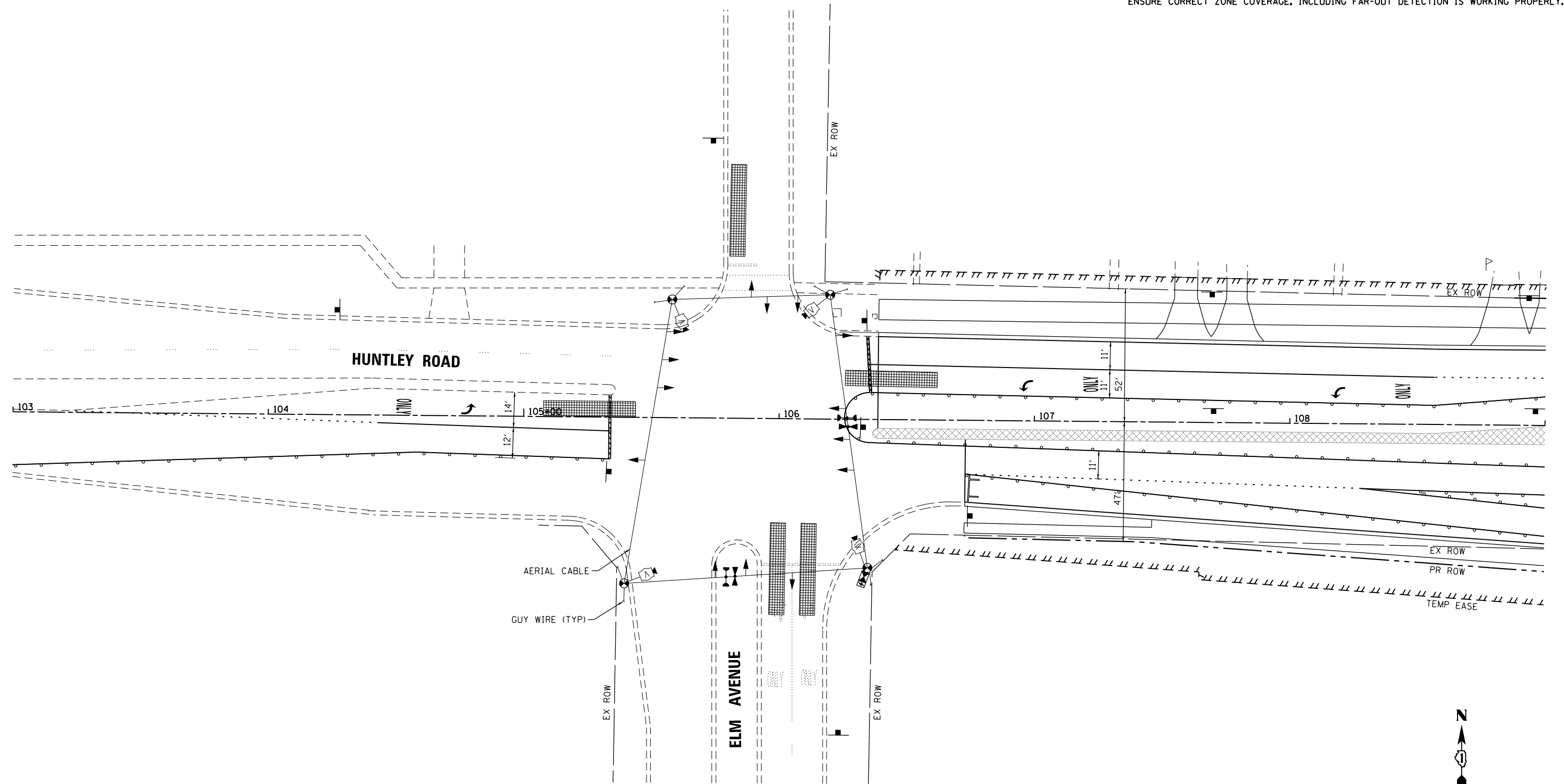
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	155
CONTRACT NO. 61E73				

**TS A.208
EAGLE 2A**

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

CONSTRUCTION NOTES

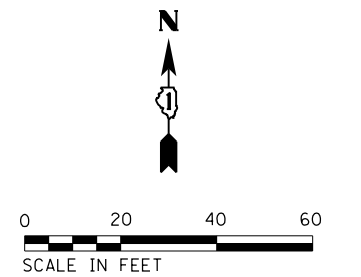
FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.



- LEGEND:**
- WORK ZONE (HMA PAVEMENT)
 - WORK ZONE (PCC PAVEMENT)
 - TEMPORARY PAVEMENT
 - DETECTION ZONE

NOTES:

- ALL EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE PROTECTED AND MAINTAINED DURING CONSTRUCTION. EXISTING SIGNAL HEADS SHALL BE BAGGED. AFTER CONSTRUCTION IS COMPLETE, THE EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE RESTORED TO ITS ORIGINAL STATE OF OPERATION.



COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-181g3c-04.dgn
 PLOT DRIVER: ILpch.plw
 PEN TABLE: PLOT LABEL.tbl
 TS SHT NO. 40



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISIONS -	
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISIONS -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISIONS -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND REMOVE
EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 3C
IL RTE 31 AND ELM AVENUE**

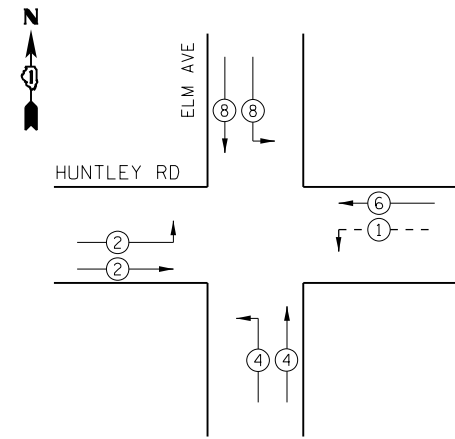
SCALE: 1"=20' SHEET NO. 40 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	156
CONTRACT NO.			61E73	

**TS A.208
EAGLE 2A**

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

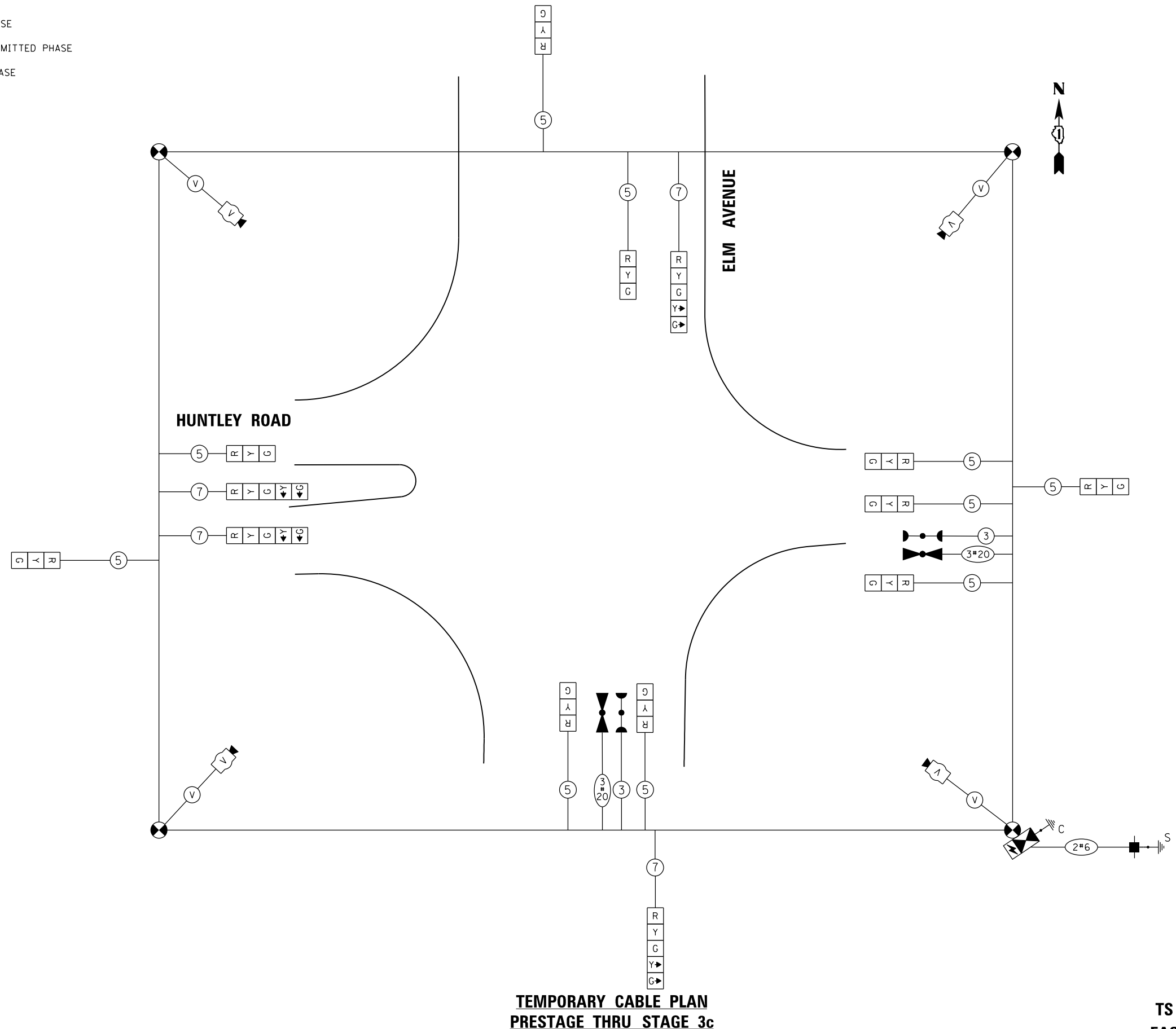
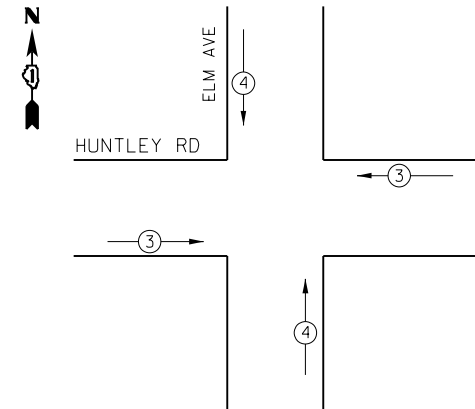
PROPOSED CONTROLLER SEQUENCE



LEGEND:

- ← ⊙ → PROTECTED PHASE
- ← ⊙ - - ⊙ → PROTECTED/PERMITTED PHASE
- ← ⊙ ⊙ → PEDESTRIAN PHASE
- ← ⊙ ⊙ OL → OVERLAP

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



**TEMPORARY CABLE PLAN
PRESTAGE THRU STAGE 3c**

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	14	11	50	77.0
(YELLOW)	14	20	5	14.0
(GREEN)	14	12	45	75.6
PERMISSIVE ARROW	4	10	10	4.0
PED. SIGNAL	-	20	100	-
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				319.4

ENERGY COSTS TO:
VILLAGE OF CARPENTERSVILLE
 1200 L.W. BESINGER DRIVE
 CARPENTERSVILLE, IL 60110
 ENERGY SUPPLY: CONTACT: TOM MAHAR
 PHONE: 815-724-5010
 COMPANY: COMMONWEALTH EDISON
 ACCOUNT NUMBER: ---

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: #FILE_NAME#
 PLOT DRIVER: #PLOT_DRIVER#
 PEN TABLE: #PEN_TABLE#
TS SHT NO. 41



USER NAME = jme1hui
 DESIGNED - TAY
 DRAWN - DMS
 CHECKED - JRM
 DATE - 3/23/18

REVISED - -
 REVISED - -
 REVISED - -
 REVISED - -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

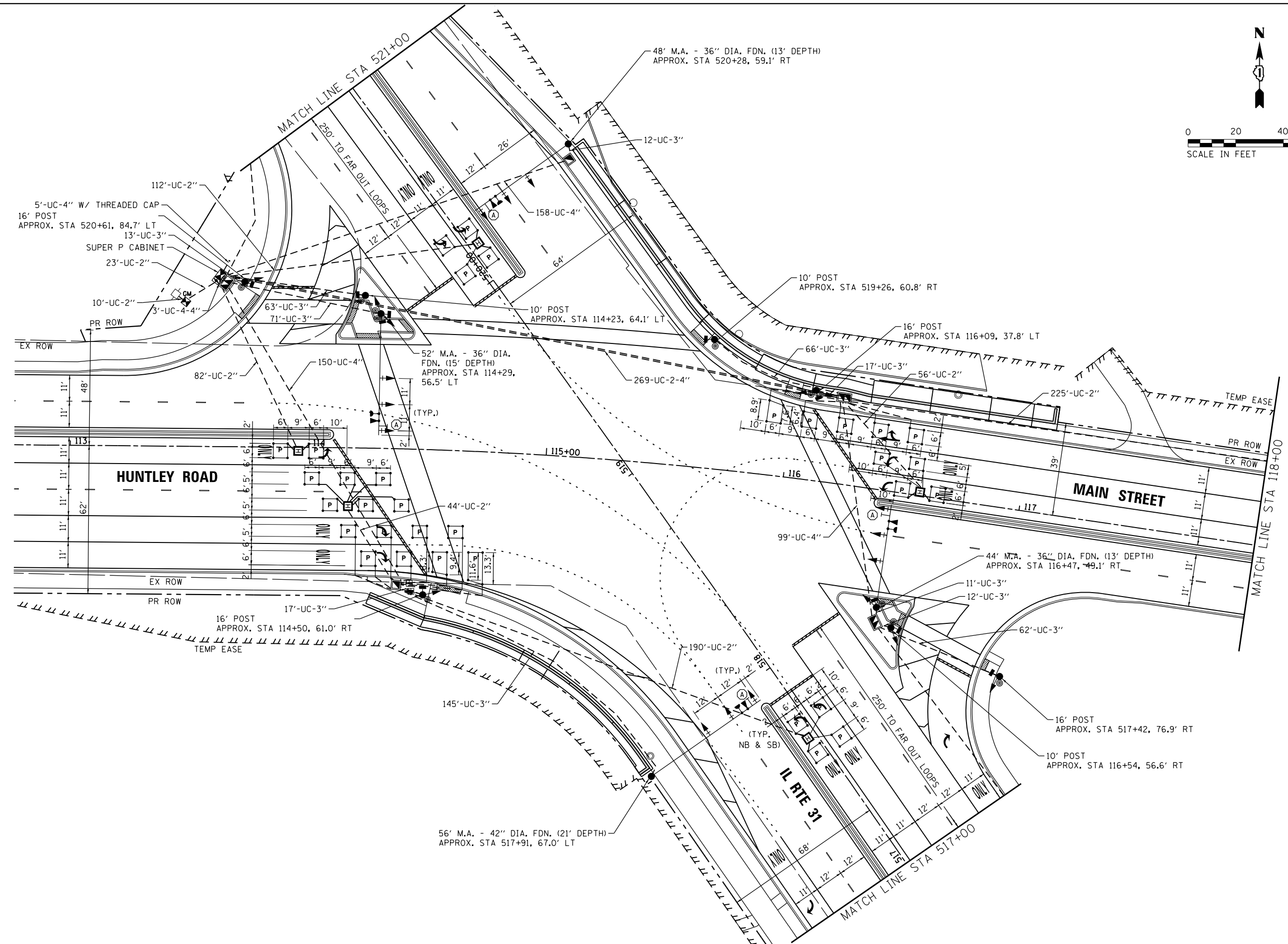
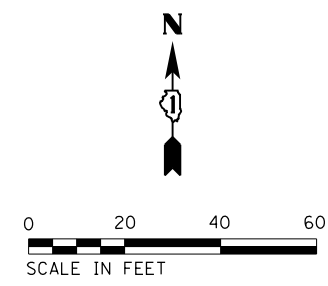
**TEMPORARY CABLE PLAN AND PHASE DESIGNATION DIAGRAM
IL RTE 31 AND ELM AVENUE**

SCALE: N.T.S. SHEET NO. 41 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	157
CONTRACT NO.			61E73	

**TS A.208
EAGLE 2A**

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 CLIENT: HRGreen.com
 FILE NAME: 86140100-sh1-signhuntley-01.dgn
 PLOT DRIVER: ILPLOT
 PEN TABLE: PLOT LABEL.tbl
 PLOT DATE: 3/23/2018

SIGN (A)
 LEFT ON GREEN
 ARROW ONLY
 R10-5
 30" X 36"
 4 - REQUIRED

TS SHT NO. 42



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISIONS -	
CHECKED - JRM	REVISIONS -	
DATE - 3/23/18	REVISIONS -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

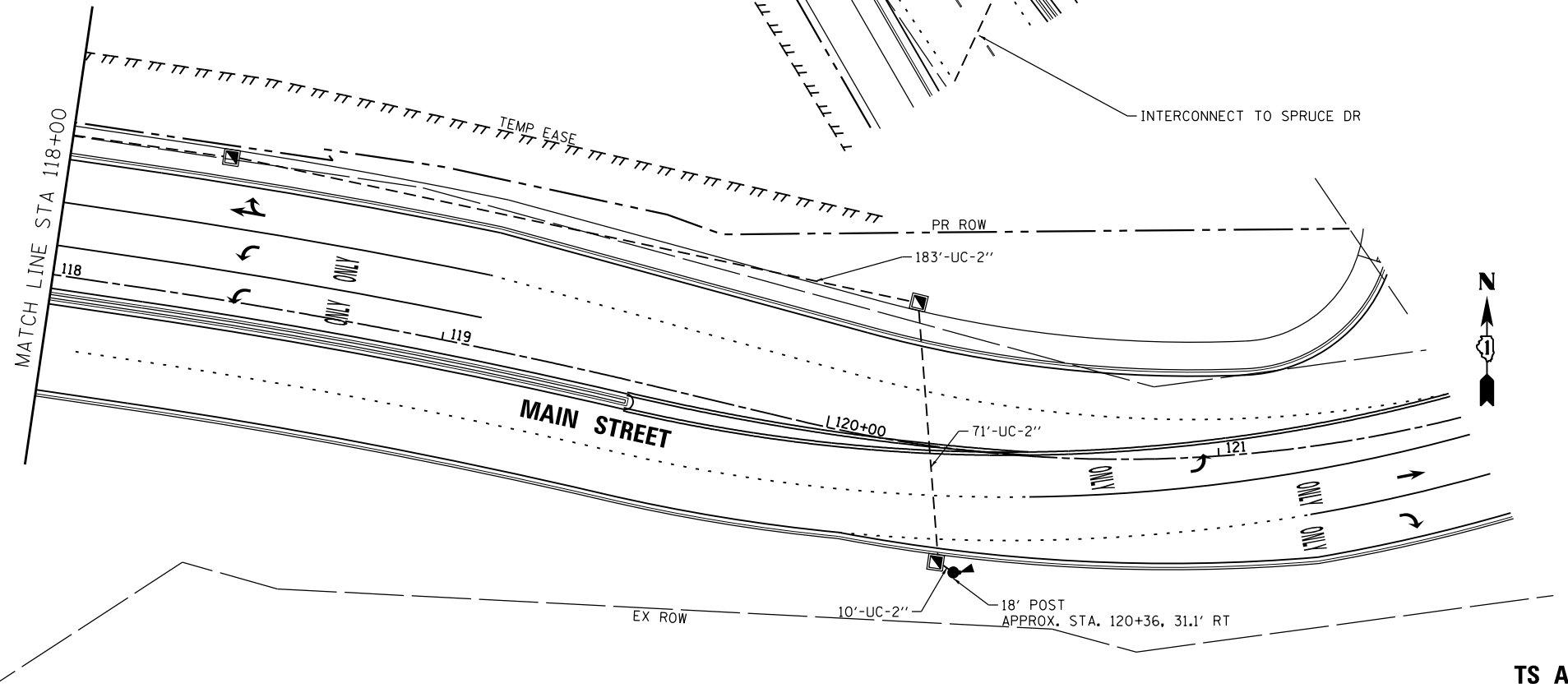
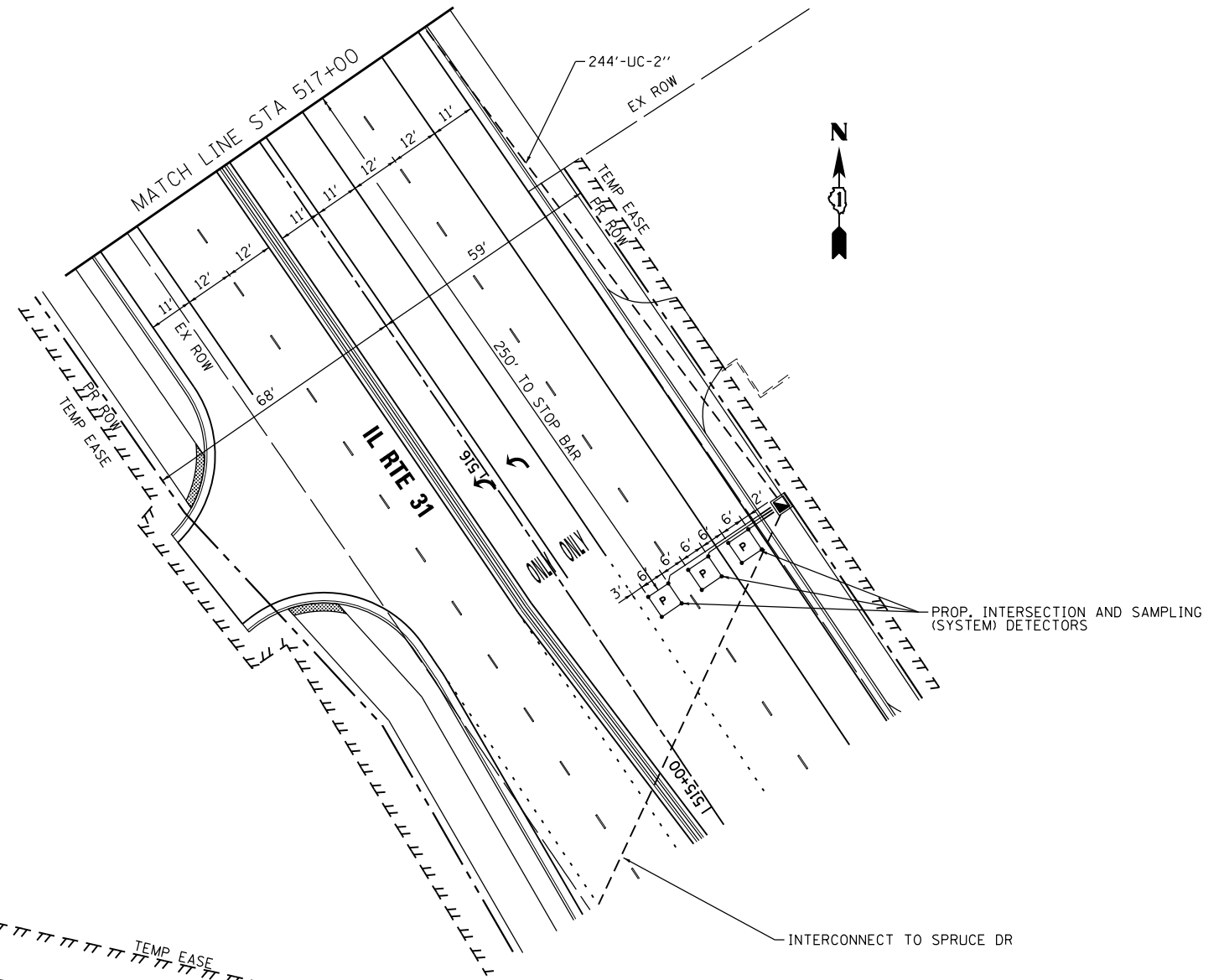
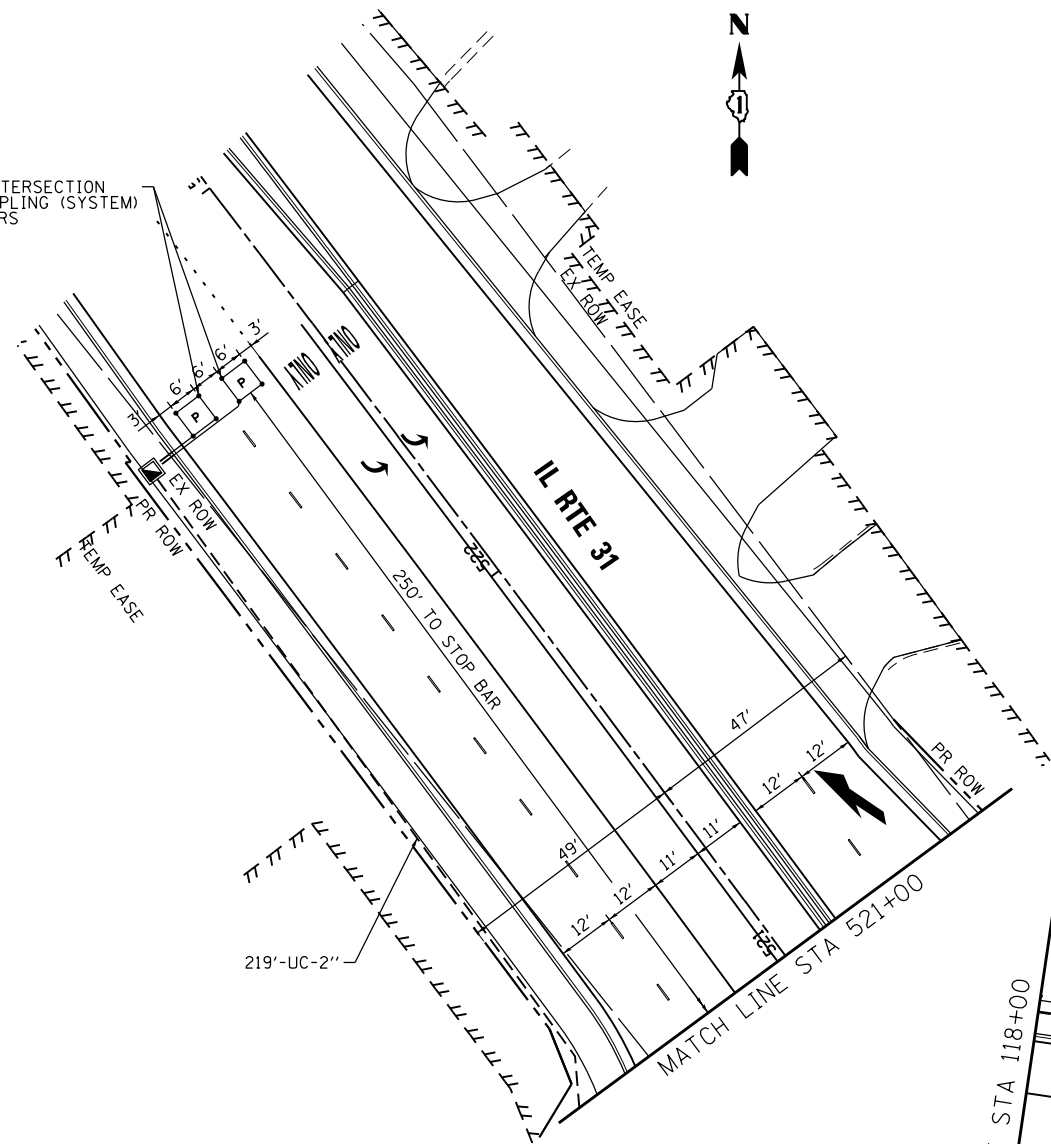
TRAFFIC SIGNAL INSTALLATION PLAN (SHEET 1 OF 2)
 IL RTE 31 AND HUNTLEY ROAD/MAIN STREET

SCALE: 1"=20' SHEET NO. 42 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 158
FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

TS A.205
 EAGLE 2A

PROP. INTERSECTION AND SAMPLING (SYSTEM) DETECTORS



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhui
 CLIENT: HRGreen.com
 FILE NAME: 86140100-sh1-sighuntley-02.dgn
 PLOT DRIVER: IL2014.plt
 PEN TABLE: PLOT LABEL.tbl
TS SHT NO. 43



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = 1"=20'	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL INSTALLATION PLAN (SHEET 2 OF 2)
 IL RTE 31 AND HUNTLEY ROAD/MAIN STREET

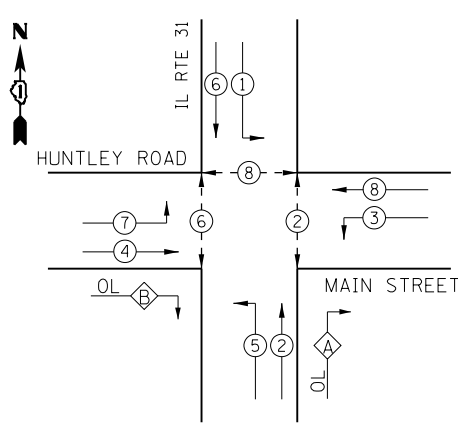
SCALE: 1"=20' SHEET NO. 43 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	159
CONTRACT NO. 61E73				

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

TS A.205
 EAGLE 2A

PROPOSED CONTROLLER SEQUENCE

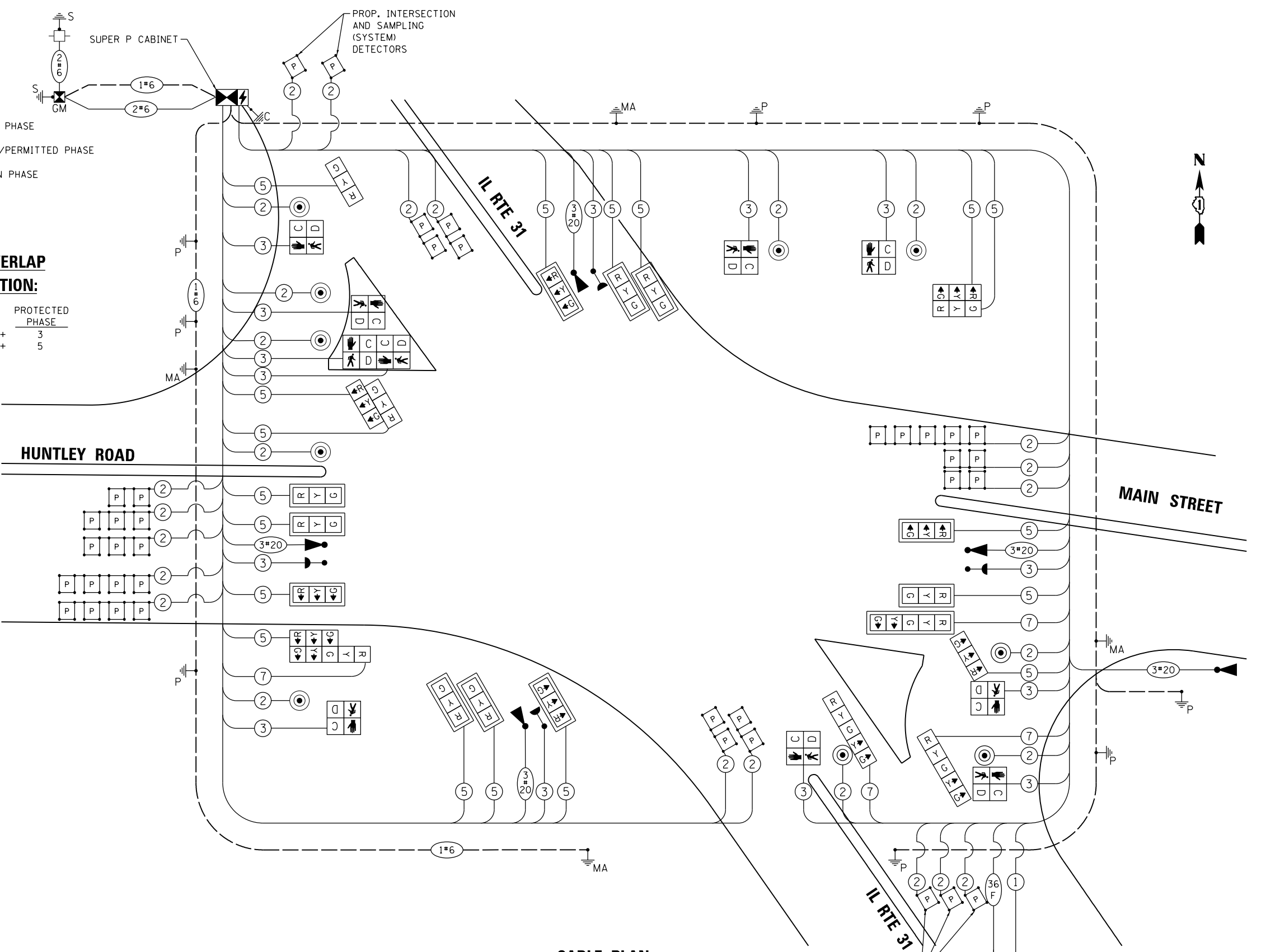
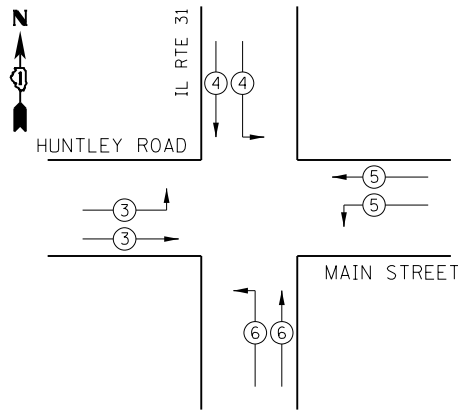


- LEGEND:**
- ⊙ — PROTECTED PHASE
 - ⊙ — PROTECTED/PERMITTED PHASE
 - ⊙ — PEDESTRIAN PHASE
 - ⊙ — OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
A	= 2	+ 3
B	= 4	+ 5

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



CABLE PLAN

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	22	11	50	121.0
(YELLOW)	22	20	5	22.0
(GREEN)	22	12	45	118.8
PERMISSIVE ARROW	8	10	10	8.0
PED. SIGNAL	10	20	100	200.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				594.8

ENERGY COSTS TO:
 ILLINOIS DEPARTMENT OF TRANSPORTATION
 201 W. CENTER COURT
 SCHAUMBURG, IL 60196
 ENERGY SUPPLY: CONTACT: TOM MAHAR
 PHONE: 815-724-5010
 COMPANY: COMMONWEALTH EDISON
 ACCOUNT NUMBER: ---

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION
 FILE NAME: 86140100-sh1-114845.dgn
 PLOT DRIVER: IL_PCH_PLOT_C19
 PEN TABLE: PLOT_LABEL.tbl
TS SHT NO. 44



DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISED -
CHECKED - JRM	REVISED -
DATE - 3/23/18	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, AND EMERGENCY VEHICLE PREEMPTION SEQUENCE IL RTE 31 AND HUNTLEY ROAD/MAIN STREET

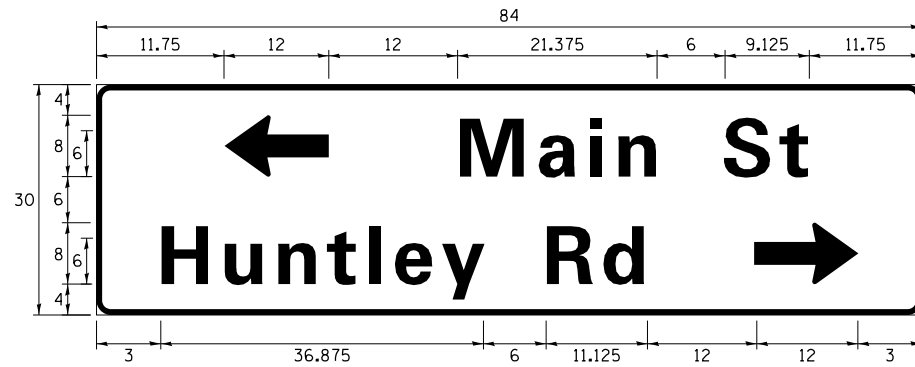
SCALE: N.T.S. SHEET NO. 44 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 160
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

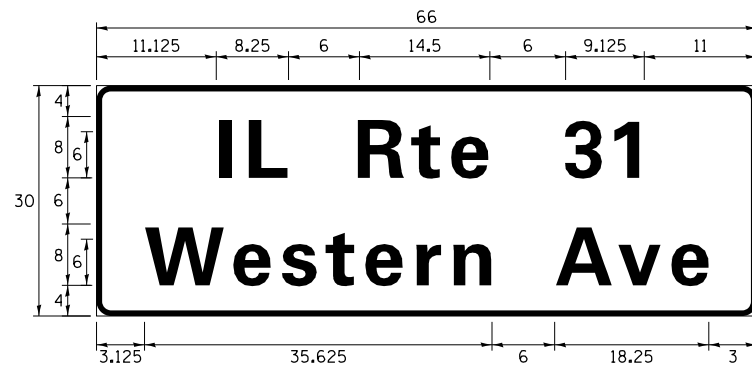
TS A.205 EAGLE 2A

SIGN PANEL – TYPE 1 OR TYPE 2

ALL DIMENSIONS ARE IN INCHES UNLESS NOTED OTHERWISE



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	17.50	2	ZZ	2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	13.75	2	ZZ	2

NOTE: FOR ADDITIONAL DESIGN AND INSTALLATION INFORMATION PLEASE SEE DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS DETAIL.

SUMMARY OF QUANTITIES

SIGN PANEL - TYPE 1	SQ FT	30.00
SIGN PANEL - TYPE 2	SQ FT	62.50
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	1,369.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	489.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	962.0
HANDHOLE	EACH	7
HEAVY-DUTY HANDHOLE	EACH	5
DOUBLE HANDHOLE	EACH	3
TRANSCEIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	2,640.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	3,950.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	4,788.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1,608.0
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	5,546.5
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	52.5
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	3,817.0
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	3
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	4
TRAFFIC SIGNAL POST, GALVANIZED STEEL 18 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 44 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 48 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 52 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 56 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	36.0
CONCRETE FOUNDATION, TYPE C	FOOT	4.0
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	41.0
CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	21.0
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	11
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	7
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	3
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	10
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	12
INDUCTIVE LOOP DETECTOR	EACH	17
PREFORMED DETECTOR LOOP	FOOT	1,242
LIGHT DETECTOR	EACH	5
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	10
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	10
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	8
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	2,040.0
FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET (SPECIAL)	EACH	1
SERVICE INSTALLATION, GROUND MOUNTED, METERED	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-sigcabrunt-02.dgn
 PLOT DRIVER: IL_rte31_western_ave.dgn
 PLOT TABLE: PLOT_LABEL.tbl
TS_SHT NO. 45



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = N.T.S.	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

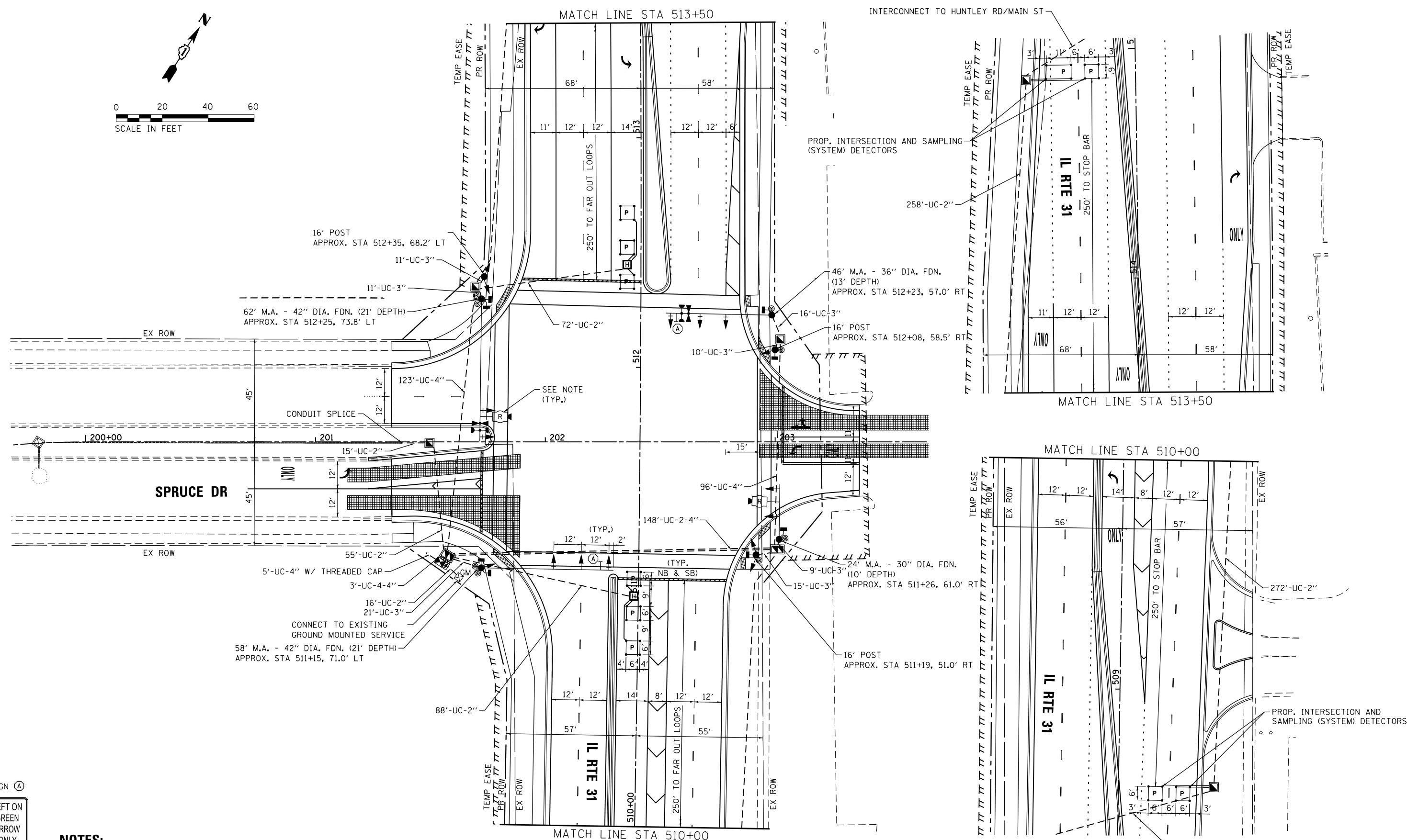
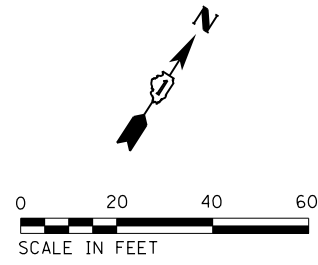
**MAST ARM MOUNTED STREET NAME SIGNS
AND SCHEDULE OF QUANTITIES
IL RTE 31 AND HUNTLEY ROAD/MAIN STREET**

SCALE: N.T.S. SHEET NO. 45 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	161
CONTRACT NO.			61E73	

**TS A.205
EAGLE 2A**

FED. ROAD DIST. NO. [] ILLINOIS FED. AID PROJECT



NOTES:

- 1. THE MOUNTING LOCATIONS OF THE RADAR DETECTORS WILL BE DETERMINED PER THE MANUFACTURER'S RECOMMENDATIONS.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jme1hu
 CLIENT: HRGreen.com
 FILE NAME: 86140100-sh1-sigspruce-01.dgn
 PLOT DRIVER: IL_rte31_sigspruce-01.dgn
 PEN TABLE: PLOT_LABEL.tbl
TS_SHT NO. 46



USER NAME = jme1hu	DESIGNED - TAY	REVISED -
PLOT SCALE = 1"=20'	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

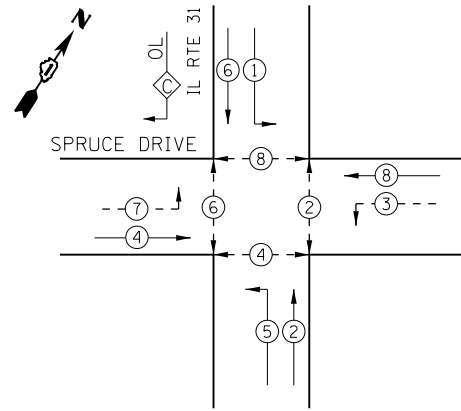
**TRAFFIC SIGNAL INSTALLATION PLAN (SHEET 1 OF 1)
 IL RTE 31 AND SPRUCE DRIVE**

SCALE: 1"=20' SHEET NO. 46 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	162
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

**TS A.204
 EAGLE 2A**

PROPOSED CONTROLLER SEQUENCE



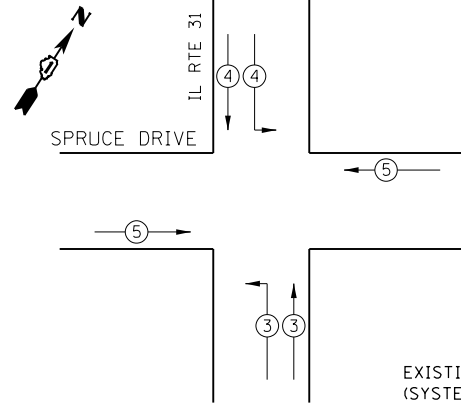
LEGEND:

- ← * → PROTECTED PHASE
- ← * - PROTECTED/PERMITTED PHASE
- ← * → PEDESTRIAN PHASE
- ← * OL → OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

OVERLAP LETTER	PERMISSIVE PHASE	PROTECTED PHASE
C	= 6	+ 7

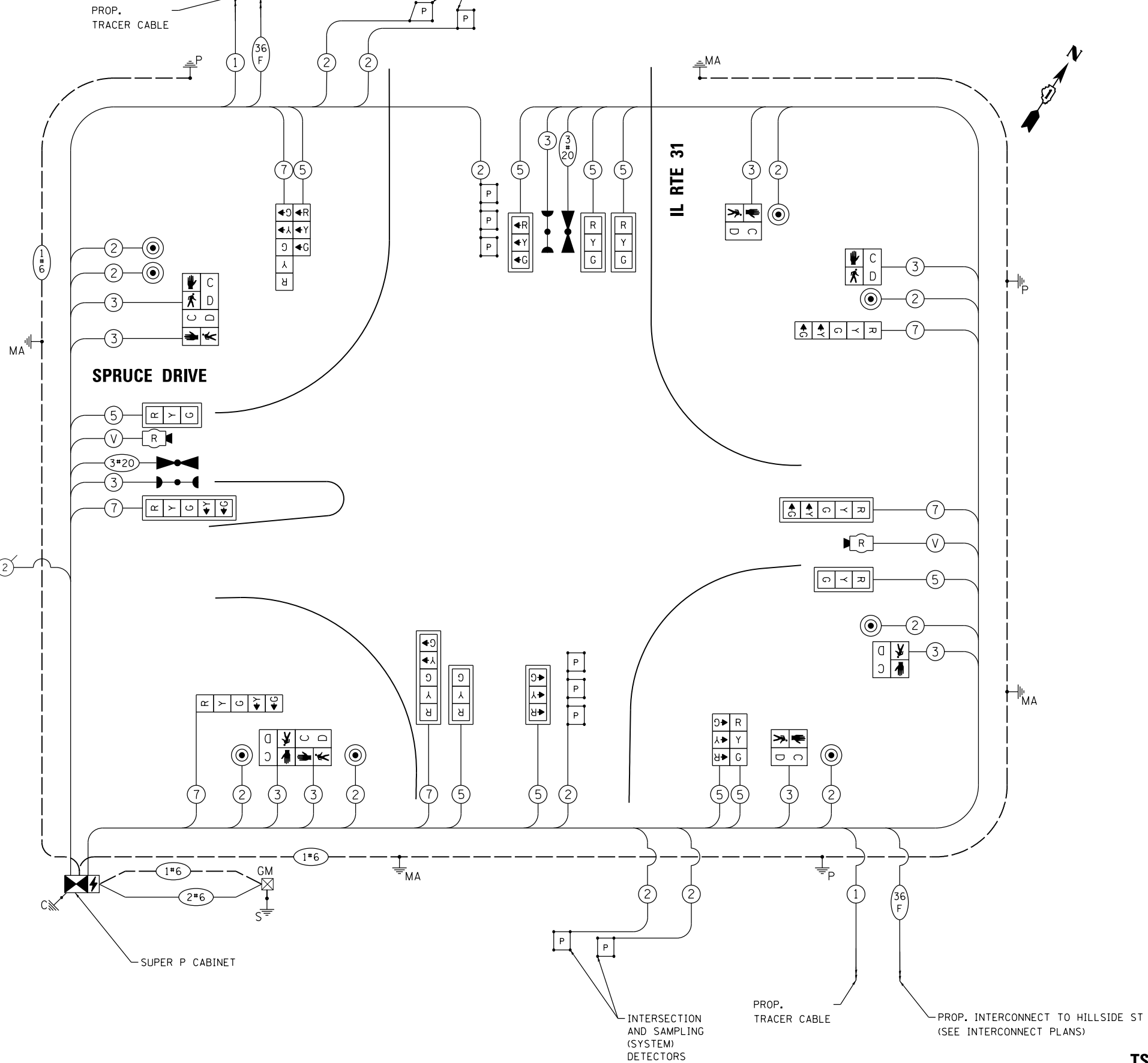
PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE



EXISTING INTERSECTION AND SAMPLING (SYSTEM) DETECTOR

PROP. INTERCONNECT TO HUNTLEY RD/MAIN ST (SEE INTERCONNECT PLANS)

INTERSECTION AND SAMPLING (SYSTEM) DETECTORS



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	16	11	50	88.0
(YELLOW)	16	20	5	16.0
(GREEN)	16	12	45	86.4
PERMISSIVE ARROW	12	10	10	12.0
PED. SIGNAL	8	20	100	160.0
CONTROLLER	1	100	100	100.0
UPS	1	25	100	25.0
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				487.4

ENERGY COSTS TO:

ILLINOIS DEPARTMENT OF TRANSPORTATION
201 W. CENTER COURT
SCHAUMBURG, IL 60196

ENERGY SUPPLY: CONTACT: TOM MAHAR
PHONE: 815-724-5010
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

COMPANY NAME: HRGreen
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION
 FILE NAME: 86140100-sh1-sigcabspurpose-01.dgn
 PLOT DRIVER: IL_pch_fw_sht_c19
 PEN TABLE: PLOT_LABEL.tbl
TS_SHT NO. 47



DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISED -
CHECKED - JRM	REVISED -
DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, AND EMERGENCY VEHICLE PREEMPTION SEQUENCE IL RTE 31 AND SPRUCE DRIVE

SCALE: N.T.S. SHEET NO. 47 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	163
CONTRACT NO.			61E73	

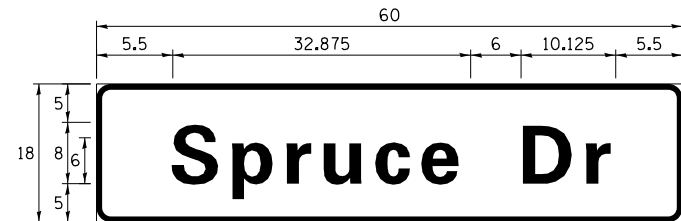
**TS A.204
EAGLE 2A**

FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT

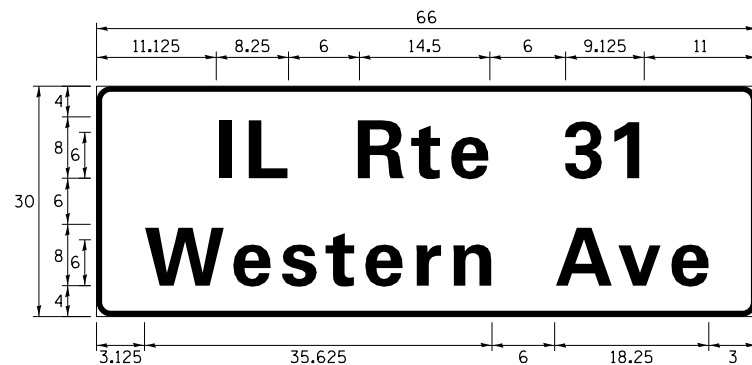
SUMMARY OF QUANTITIES

SIGN PANEL – TYPE 1 OR TYPE 2

ALL DIMENSIONS ARE IN INCHES UNLESS NOTED OTHERWISE



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	7.50	1	ZZ	2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D	13.75	2	ZZ	2

NOTE: FOR ADDITIONAL DESIGN AND INSTALLATION INFORMATION PLEASE SEE DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS DETAIL.

SIGN PANEL - TYPE 1	SO FT	30.00
SIGN PANEL - TYPE 2	SO FT	27.50
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	776.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	93.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	532.0
HANDHOLE	EACH	5
HEAVY-DUTY HANDHOLE	EACH	2
DOUBLE HANDHOLE	EACH	2
TRANSCEIVER - FIBER OPTIC	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	1,489.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2,115.5
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2,250.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	1,084.0
ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	2,388.0
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	50.5
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1,392.5
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	3
STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 46 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 58 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 62 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	16.0
CONCRETE FOUNDATION, TYPE C	FOOT	4.0
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10.0
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	13.0
CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	42.0
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	7
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	3
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	3
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	3
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	10
INDUCTIVE LOOP DETECTOR	EACH	6
PREFORMED DETECTOR LOOP	FOOT	354
LIGHT DETECTOR	EACH	4
LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	8
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	255.0
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	11
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	8
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	562.5
ROD AND CLEAN EXISTING CONDUIT	FOOT	159
FULL-ACTUATED CONTROLLER AND TYPE SUPER P CABINET (SPECIAL)	EACH	1
SERVICE INSTALLATION, GROUND MOUNTED, METERED	EACH	1
RADAR VEHICLE DETECTION SYSTEM, SINGLE APPROACH, STOP BAR	EACH	2
CONDUIT SPLICE	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1

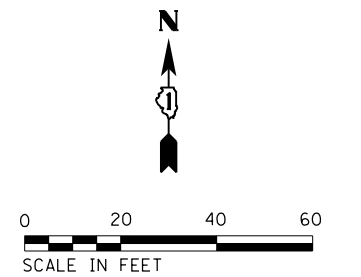
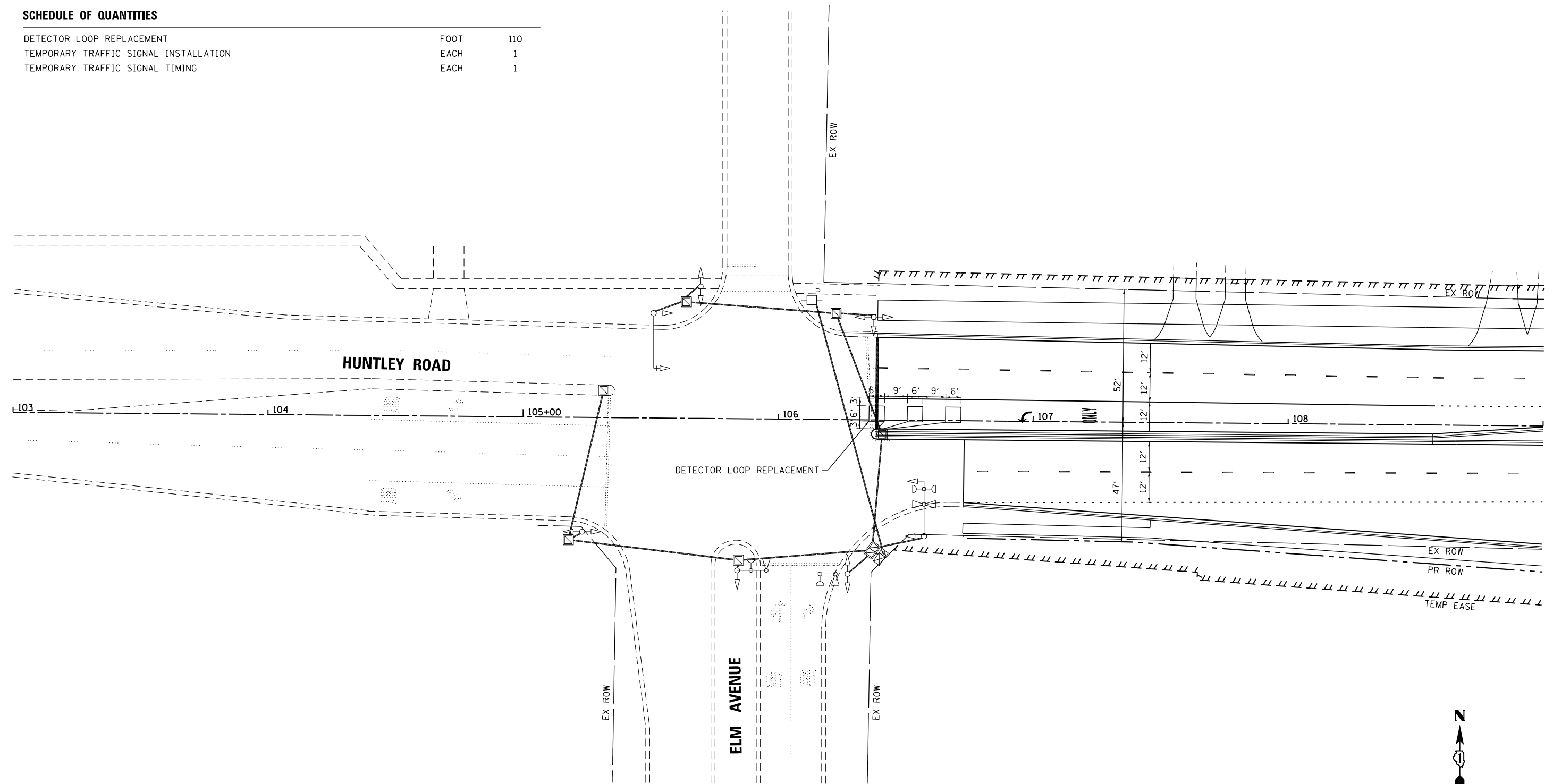
COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-slgcbspruce-02.dgn
 PLOT DRIVER: IL_rte46_02.dwg
 PLOT TABLE: PLOT_LABEL.tbl
TS_SHT NO. 48

HRGreen.com Illinois Professional Design Firm #184-001322	USER NAME = jmelhui DESIGNED - TAY DRAWN - DMS CHECKED - JRM DATE = 3/23/2018	REVISED - - REVISED - - REVISED - - REVISED - -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAST ARM MOUNTED STREET NAME SIGNS AND SCHEDULE OF QUANTITIES IL RTE 31 AND SPRUCE DRIVE	F.A.U. RTE. 3887 SECTION 08-00083-00-CH COUNTY KANE TOTAL SHEETS 259 SHEET NO. 164	CONTRACT NO. 61E73
	PLOT SCALE = N.T.S. PLOT DATE = 3/23/2018	SCALE: N.T.S. SHEET NO. 48 OF 53 SHEETS STA. _____ TO STA. _____			FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT	

**TS A.204
EAGLE 2A**

SCHEDULE OF QUANTITIES

DETECTOR LOOP REPLACEMENT	FOOT	110
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 FILE NAME: 86140100-sh1-sigem-01.dgn
 PLOT DRIVER: ILpch.plw
 PEN TABLE: PLOTLABEL.tbl
TS SHT NO. 49



USER NAME = jmelhui	DESIGNED - TAY	REVISED -
PLOT SCALE = 1"=20'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

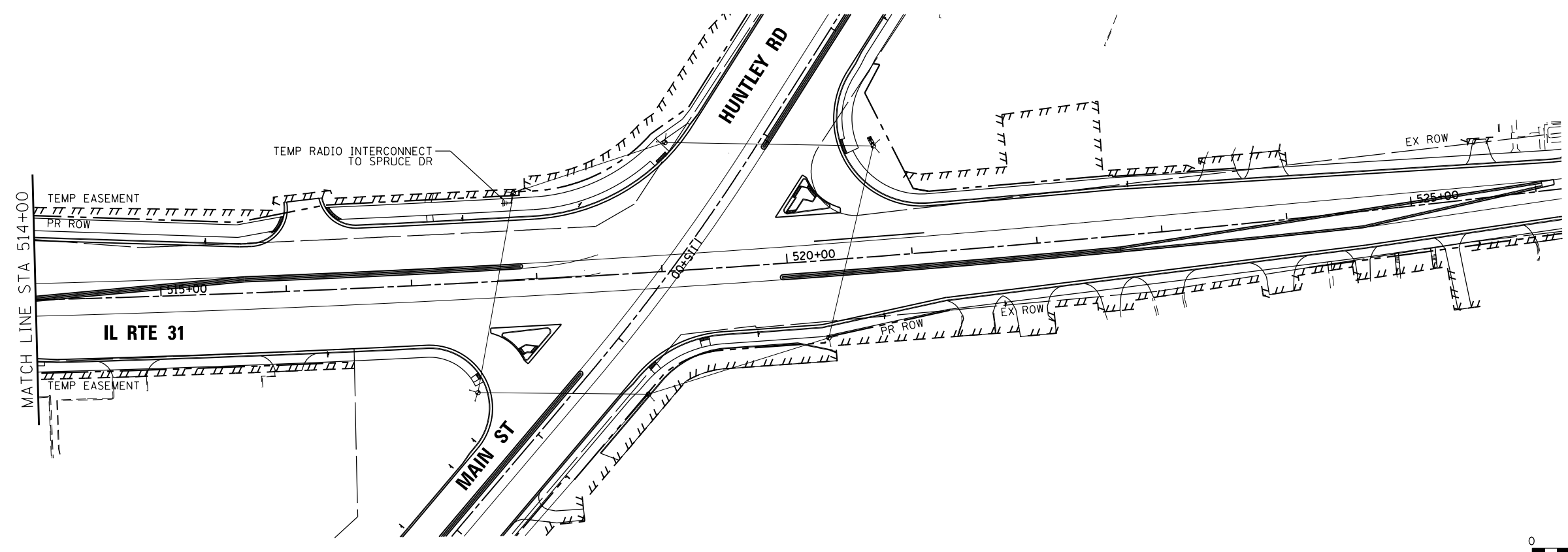
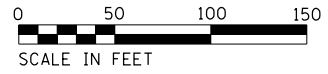
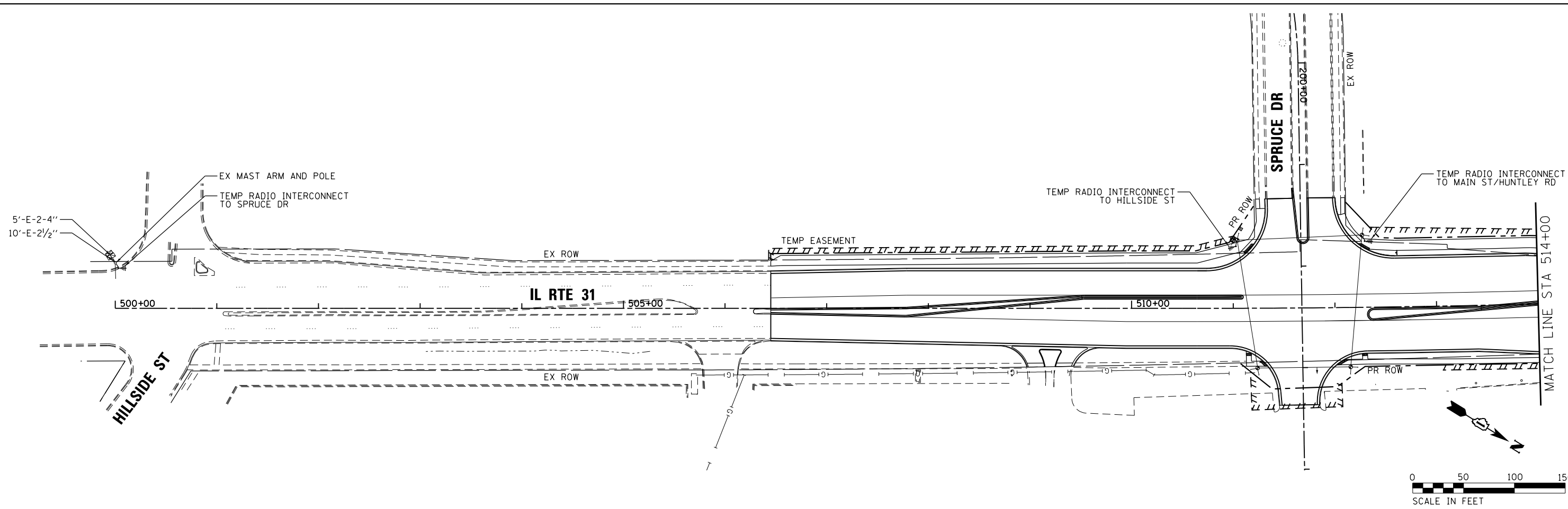
TRAFFIC SIGNAL INSTALLATION PLAN (SHEET 1 OF 1)
IL RTE 31 AND ELM AVENUE

SCALE: 1"=20' SHEET NO. 49 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	165
CONTRACT NO.			61E73	

TS A.208
EAGLE 2A

FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT



EAGLE 2A

COMPANY NAME: #COMPANY NAME#
 PROJECT CONTACT: #PROJECT CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 3/23/2018 11:49:04 AM
 FILE NAME: 86140100-shr-tsigt-01.dgn
 PLOT DRIVER: IL_defr.dwt
 PEN TABLE: PLOTLABEL.tbl
TS SHT NO. 50



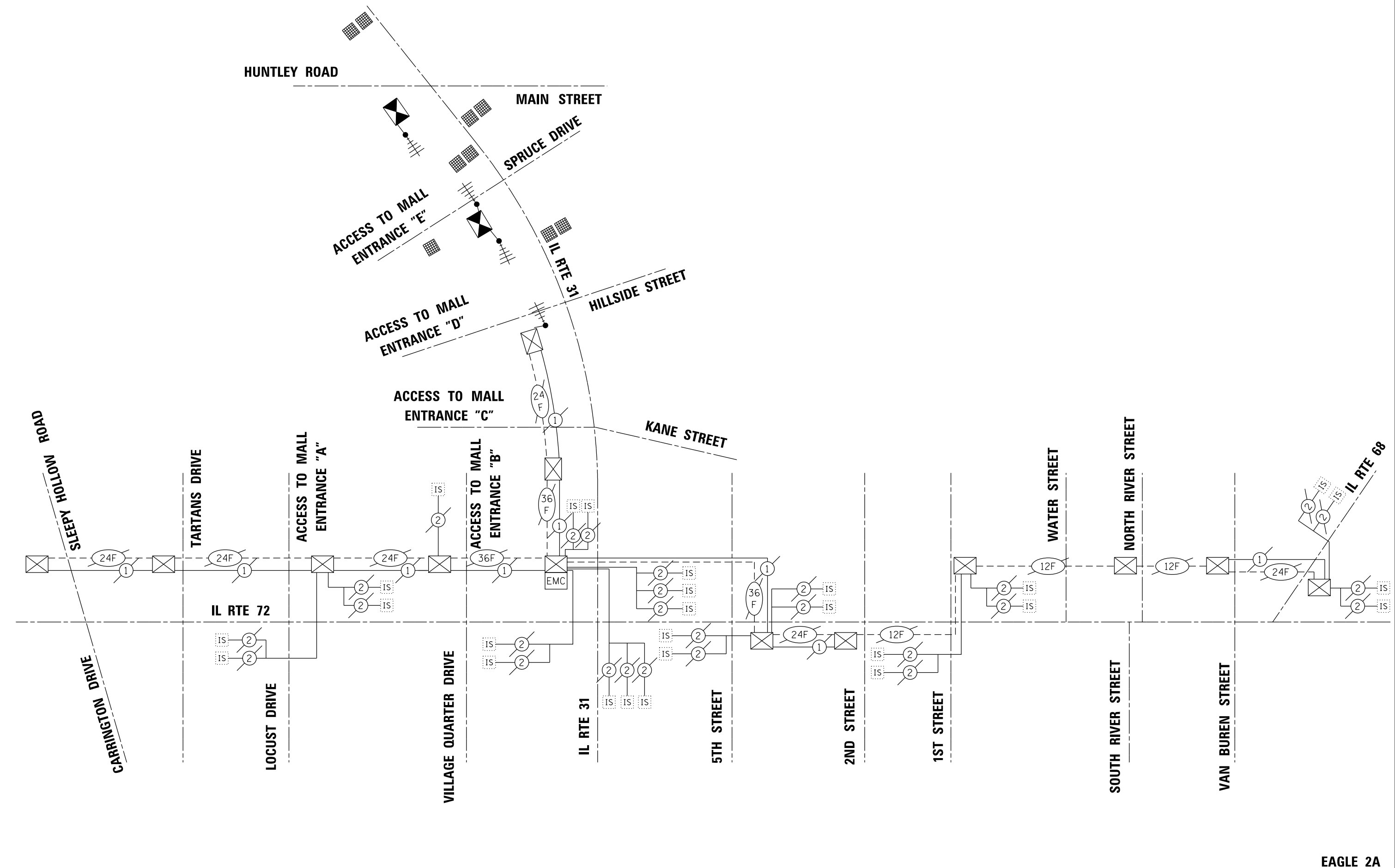
USER NAME = jmelhus	DESIGNED - TAY	REVISED -
FILE NAME = 86140100-shr-tsigt-01.dgn	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=50'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT PLAN (SHEET 1 OF 1)
ILLINOIS ROUTE 31
 SCALE: 1"=50' SHEET NO. 50 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE. 3887	SECTION NO. 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 166
FED. ROAD DIST. NO. _____ ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

COMPANY NAME: HRGreen
 PROJECT CONTACT: HRGreen.com
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION
 FILE NAME: 86140100-sh1-tsgint-schem.dgn
 PLOT DRIVER: ILpch.fwsoft.cfy
 PEN TABLE: PLOTLABEL.tbl
 TS_SHT NO. 51



EAGLE 2A



USER NAME = jme1hui	DESIGNED - TAY	REVISED -
DRAWN - DMS	CHECKED - JRM	REVISED -
PLOT SCALE = N.T.S.	DATE - 3/23/18	REVISED -
PLOT DATE = 3/23/2018		

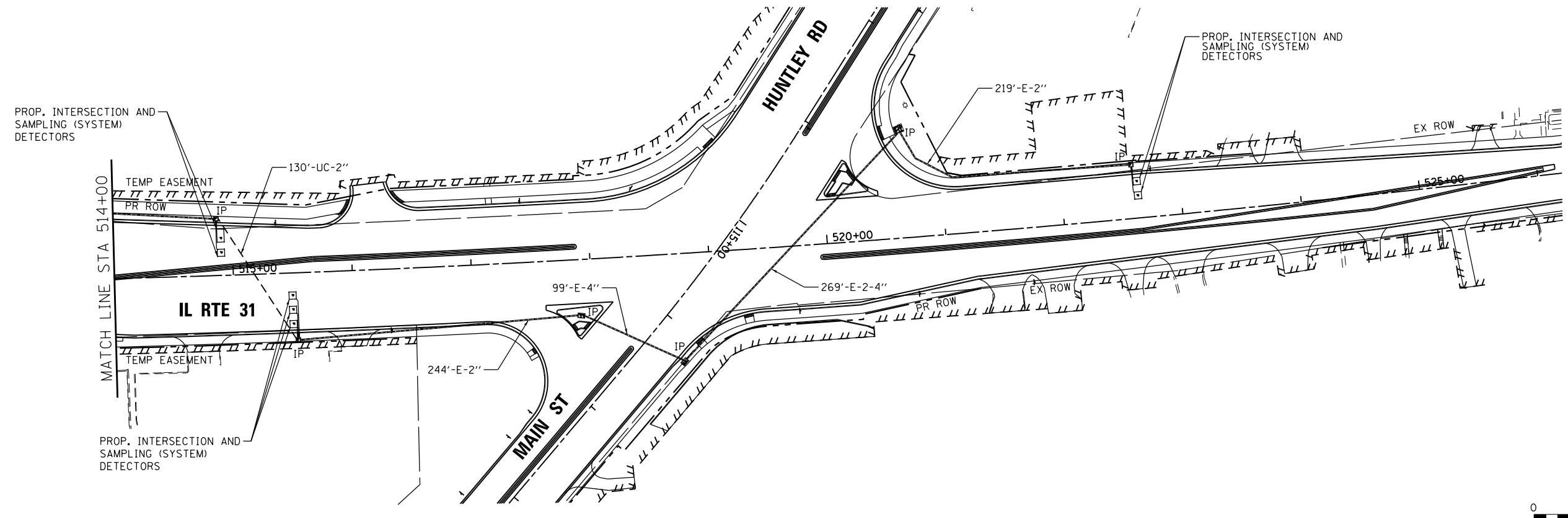
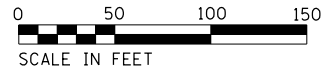
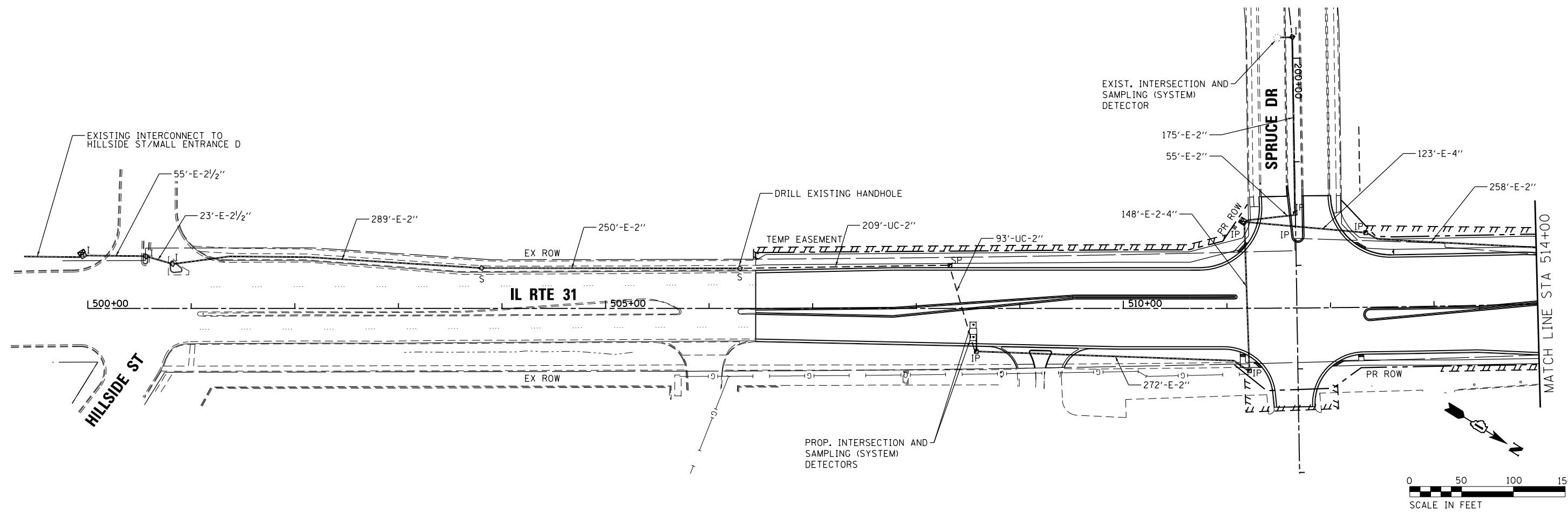
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT SCHEMATIC
IL RTE 31 AND IL RTE 72

SCALE: N.T.S. SHEET NO. 51 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	167
CONTRACT NO. 61E73				

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



COMPANY NAME: #COMPANY NAME#
 PROJECT CONTACT: #PROJECT CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 3/23/2018 11:49:10 AM
 FILE NAME: 86140100-sht-sigint-01.dgn
 PLOT DRIVER: il_dpr.dwg.ctb
 PEN TABLE: PLOT LABEL.tbl
TS SHT NO. 52



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
FILE NAME = 86140100-sht-sigint-01.dgn	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=50'	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

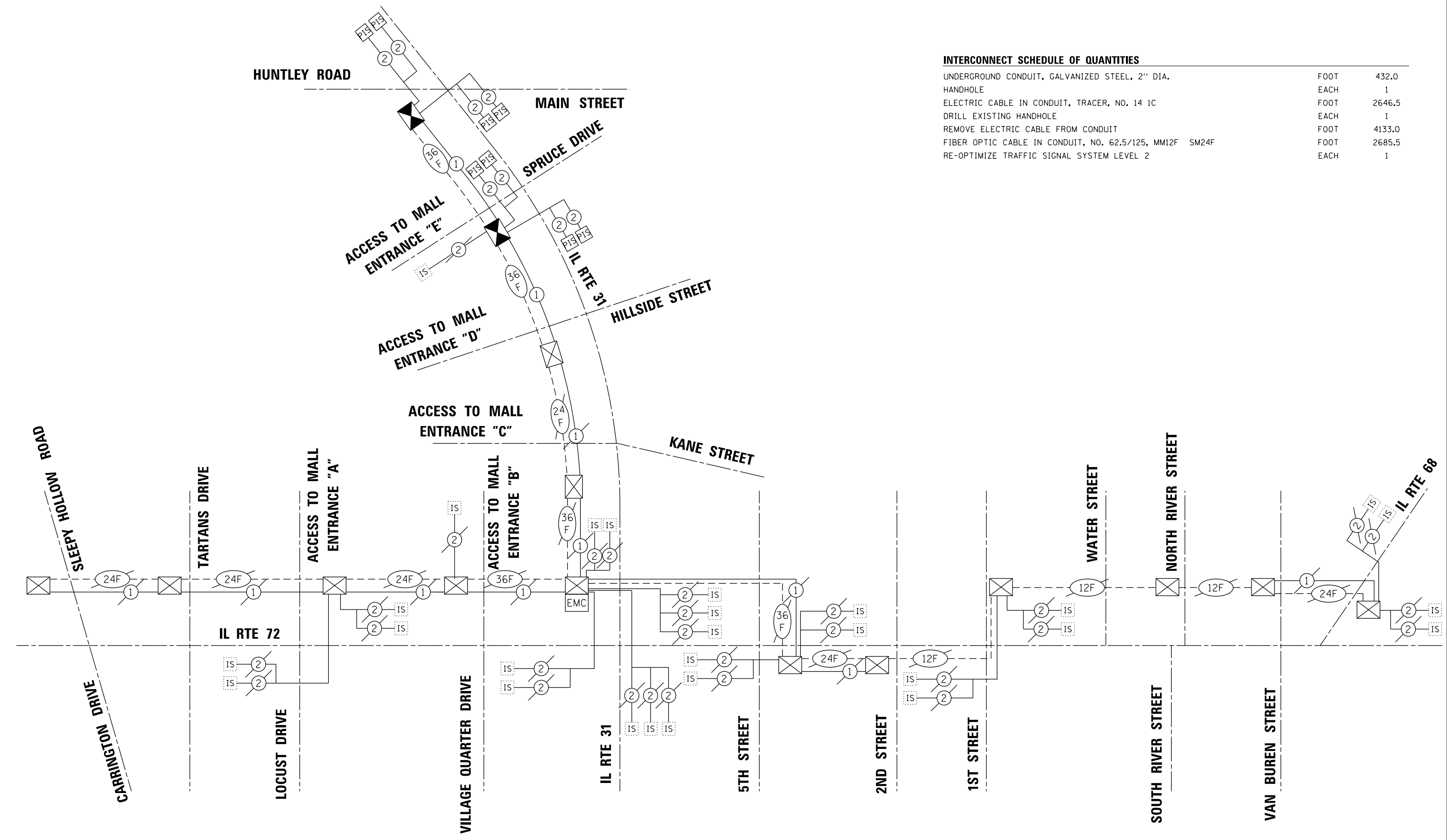
PROPOSED INTERCONNECT PLAN (SHEET 1 OF 1)
ILLINOIS ROUTE 31

SCALE: 1"=50' SHEET NO. 52 OF 53 SHEETS STA. _____ TO STA. _____

F.A.U. RTE.	SECTION NO.	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	168
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. [] ILLINOIS FED. AID PROJECT				

EAGLE 2A

COMPANY NAME: HRGreen
 PROJECT CONTACT: jme1hui
 CLIENT: HRGreen
 FILE NAME: 86140100-sh1-sight-schem.dgn
 PLOT DRIVER: ILpch.plw
 PEN TABLE: PLOT LABEL.tbl
 TS SHT NO. 53



INTERCONNECT SCHEDULE OF QUANTITIES

UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	432.0
HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	2646.5
DRILL EXISTING HANDHOLE	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	4133.0
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	2685.5
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 2	EACH	1



HRGreen.com
 Illinois Professional Design Firm
 #184-001322
 USER NAME = jme1hui
 DESIGNED - TAY
 DRAWN - DMS
 CHECKED - JRM
 DATE - 3/23/18
 PLOT SCALE = N.T.S.
 PLOT DATE = 3/23/2018

DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISED -
CHECKED - JRM	REVISED -
DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED INTERCONNECT SCHEMATIC AND SCHEDULE OF QUANTITIES
IL RTE 31 AND IL RTE 72
 SCALE: N.T.S. SHEET NO. 53 OF 53 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	169
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

EAGLE 2A

GENERAL NOTES

- WHERE SEPARATE CIRCUIT RUNS ARE TO BE INSTALLED PARALLEL WITH EACH OTHER, ONE COMMON TRENCH SHALL BE USED.
- THE CONTRACTOR SHALL CONSULT WITH RESIDENT ENGINEER IN THE FIELD, AND FINALIZE ALL EXISTING ROADWAY LIGHTING ITEMS.
- THE RESPONSIBILITY FOR COORDINATING FINISHED SIDEWALK ELEVATIONS WITH THE TOP OF THE FOUNDATIONS HEIGHTS SHALL REMAIN WITH THE CONTRACTOR.
- THE CONTRACTOR SHALL SUBMIT FOR RESIDENT ENGINEER'S REVIEW WITHIN 30 DAYS OF CONTRACT EXECUTION DATA AND DETAIL SHOP DRAWINGS :
 - TRENCH, ELECTRICAL WARNING TAPE
 - FOUNDATION: CONCRETE MIX, RACEWAYS, ANCHOR BOLTS WITH NUTS & WASHERS.
 - CONDUIT: CONDUIT AND CONDUIT FITTINGS, BONDING COMPOUND
 - GROUND ROD: GROUND ROD, COPPER WIRE, EXOTHERMIC WELD.
 - UNIT DUCT/CABLES
 - ELECTRIC CABLES
 - ELECTRIC TAPES, QUICK DISCONNECT, FUSE & LAMP.
 - LIGHT POLES/ARM: DETAILS SHOP DRAWING, WIND LOAD CALCULATIONS (SHALL BE REVIEWED BY CONSULTANT CIVIL ENGINEER)
 - LUMINAIRE: LUMINAIRE WITH BALLAST ASSEMBLY OR DRIVER
 - LIGHTING CONTROLLER: CIRCUITRY DETAIL, CATALOG ON MATERIALS.
- ALL WORK SHALL BE NEW, UNLESS OTHERWISE SPECIFIED.
- FINISH PAINT: THE PAINT COLOR AND FINISH SHALL BE POWDER COATED. FOR THE LIGHT POLE AND LUMINAIRE THE VILLAGE OF CARPENTERSVILLE SHALL APPROVE THE PAINT COLOR AND FINISH.
- LIGHT POLE FOUNDATION HOLE, EACH HOLE FOR THE FOUNDATIONS SHALL BE INSPECTED BY THE RESIDENT ENGINEER PRIOR TO POURING CONCRETE FOUNDATIONS.
- THE CONTRACTOR SHALL SUBMIT FOUR (4) SETS OF FULL SIZED COMPLETE AND ACCURATE "RECORD DRAWINGS" TO THE ENGINEER FOR REVIEW AND COMMENT. THE RECORD DRAWINGS SHALL BE UPDATED ON A REGULAR BASIS AND DEPICT ALL ROADWAY LIGHTING INSTALLATIONS WITH ANY CHANGES INDICATED IN RED. "RESPONSIBLE RECORD DRAWINGS" SHALL BE SUBMITTED AT LEAST SEVEN (7) DAYS BEFORE SCHEDULING A FINAL INSPECTION.
- THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. AT (1-800-892-0123) TO LOCATE AND MARK/STAKE ALL UNDERGROUND UTILITIES.
- THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF CARPENTERSVILLE TO LOCATE AND MARK/STAKE ALL VILLAGE OWNED UNDERGROUND UTILITIES.
- THE CONTRACTOR SHALL GIVE IN WRITING TO THE ENGINEER FOR REVIEW, CONSTRUCTION STAGING FOR THE PROPOSED ROADWAY LIGHTING WORK, AND THE CONTRACTOR SHALL OBTAIN PRIOR WRITTEN APPROVAL FROM THE RESIDENT ENGINEER.
- THE LIGHT POLE SETBACK: VARIES: MEASURED FROM BACK OF CURB TO THE CENTER OF POLE.
- THIS PROJECT INCLUDES THE RELOCATION OF ORNAMENTAL LIGHTING SYSTEM ALONG MAIN STREET FROM LINCOLN AVENUE TO IL 31 AND AT LINCOLN AVENUE @ MAIN STREET INTERSECTION. THE PROPOSED LIGHTING SHALL BE OWNED AND MAINTAINED BY THE VILLAGE OF CARPENTERSVILLE.
- THE CONTRACTOR SHALL CONTACT THE ELECTRIC UTILITY COMPANY TO COORDINATE THE ELECTRIC SERVICE WORK. THE FIELD CONTACT PERSON IS HUGO SILVA @ 630-437-3182.
- GROUND RODS SHALL BE INSTALLED AT EACH LIGHTING UNIT PER LT-7.
- RESTORATION OF PARKWAY AND PROJECT SITE SHALL BE IN ACCORDANCE WITH ALL APPLICABLE CODES, REGULATIONS AND CONTRACT DOCUMENTS.
- THE CONTRACTOR SHALL CHISEL ARROWS LOCATING ALL CONDUIT CROSSINGS ACROSS ROADWAYS. THE CHISEL MARKS SHALL BE ON THE CURB.
- ALL STREET CROSSINGS SHALL HAVE GALVANIZED STEEL CONDUIT OF THE SIZE INDICATED PUSHED BENEATH THEM. PAYMENT SHALL BE LIMITED TO THE ACTUAL WIDTH PLUS TWO FEET. LENGTH OF CONDUIT SHALL BE THE WIDTH OF ROAD PLUS 4- FEET (EXTERND 2- FEET BEYOND ROAD, EACH SIDE)
- ALL ELECTRICAL EQUIPMENT AND PRODUCTS SHALL BE UL LISTED AND LABELED.
- ALL POLE HANDHOLES SHALL FACE AWAY FROM TRAFFIC.
- LUMINAIRES SHALL BE LEVEL AND HAVE A TIGHT FIT ON MAST ARMS TO THE OWNER'S SATISFACTION. THIS WORK SHALL INCLUDE FIELD ADJUSTING OF THE LUMINAIRE WHICH WILL BE INCLUDED IN THE COST OF THE LUMINAIRE PAY ITEM.
- ALL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE FOLLOWING SPECIFICATIONS, WHICH ARE HEREBY MADE A PART HEREOF:
 - "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION IN ILLINOIS," AS PREPARED BY IDOT.
 - "THE NATIONAL ELECTRICAL CODE"
 - MUNICIPAL CODES & STANDARDS
- TO MAINTAIN THE STRUCTURAL INTEGRITY OF LIGHT POLES WITH MAST ARMS, THEY SHALL NOT BE ERECTED AND LEFT TO STAND WITHOUT LUMINAIRES. NOTE THAT THE CONTRACTOR SHALL NOT BE PAID FOR POLES UNTIL LUMINAIRES ARE INSTALLED.
- BEFORE INSTALLING LIGHT STANDARDS NEAR OVERHEAD UTILITIES, CALL ComEd FOR LOCATION APPROVAL.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO MARK THE PROPOSED LOCATIONS OF ALL LIGHT POLES AND LIGHTING CONTROLLERS FOR EXAMINATION AND CONFIRMATION WITH THE RESIDENT ENGINEER. THE CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS PRIOR TO AUGERING FOR LIGHT POLE FOUNDATIONS. THE EXACT LOCATIONS OF ALL PROPOSED ITEMS SHALL BE CONFIRMED WITH THE RESIDENT ENGINEER PRIOR TO STARTING WORK.
- THE CONTRACTOR SHALL PERFORM ELECTRICAL TESTING AND VERIFY THAT THE INSTALLATION COMPLIES WITH THE LATEST EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- NO UNDERGROUND SPlicing IS ALLOWED.
- ROADWAY LUMINAIRES SHALL BE 58W LED UNLESS OTHERWISE SPECIFIED IN THE PLANS.
- UNIT DUCT SHALL BE 1 1/2" DIA. DUCT WITH 5-1/c NO.6 (BLACK-RED, BLUE-ORANGE, WHITE) 1/c NO.6 (GREEN) GROUND SOLID COLOR CODED INSULATED CONDUCTOR.
- THE PROPOSED TRENCH SHALL BE 36 INCH BELOW FINISHED GRADE.
- THE CONTRACTOR SHALL INSTALL 3/4" DIA. EMPTY COILABLE NONMETALLIC CONDUIT BELOW UNIT DUCT MEANT FOR ORNAMENTAL LIGHTING UNITS. THE CONTRACTOR SHALL ALSO INSTALL 3/8" NYLON ROPE IN IT.

LEGEND:

- EXISTING ORNAMENTAL LIGHTING UNIT TO BE REMOVED AND RELOCATED
- EXISTING ORNAMENTAL LIGHTING UNIT TO REMAIN
- EXISTING UTILITY SERVICE POLE, WITH LIGHT
- EXISTING UTILITY SERVICE POLE
- EXISTING HEAVY DUTY HANDHOLE
- EXISTING LIGHTING CONTROLLER, 200 AMPERE, 120/240 VOLT, 1 PHASE, 3 WIRE
- RELOCATED ORNAMENTAL LIGHTING UNIT (ACORN)
- LIGHT POLE, ALUMINUM, 25 FOOT MOUNTING HEIGHT, 6 FT MAST ARM, 11 1/2" DIA BOLT CIRCLE LUMINAIRE, LED, HORIZONTAL MOUNT, 58 WATT CLEAR LAMP, 120 VOLT
- STA. 156+67

Z W 2

POLE SEQUENCE NUMBER
CIRCUIT IDENTIFIER
CONTROLLER IDENTIFIER
- UNIT DUCT, 600V, 6-1C NO. 4, 1/C NO. 6 GROUND, (XLP-TYPE-USE) 1 1/2" DIA POLYETHYLENE (BETWEEN DECORATIVE POLES)
- GROUND ROD, 5/8" DIA. x 8 FT
- CABLE IN CONDUIT, SIZE AS NOTED
- REL
RELOCATED PROPOSED HANDHOLE

SUMMARY OF QUANTITIES

PAY ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	105
81028710	UNDERGROUND CONDUIT, COILABLE NONMETALLIC CONDUIT, 3/4" DIA.	FOOT	1,174
81400100	HANDHOLE	EACH	4
81603136	UNIT DUCT, 600V, 5-1C NO.4, 1/C NO.6 GROUND, (XLP-TYPE USE), 1 1/2" DIA, POLYETHYLENE	FOOT	1,300
83600200	LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	171
84200500	REMOVAL OF LIGHTING UNIT, SALVAGE	EACH	17
84200600	REMOVAL OF LIGHTING UNIT, NO SALVAGE	EACH	6
84200804	REMOVAL OF POLE FOUNDATION	EACH	23
84400105	RELOCATE EXISTING LIGHTING UNIT	EACH	16
X0326654	ORNAMENTAL LIGHT UNIT, COMPLETE	EACH	3
X8211125	LUMINAIRE, LED, HORIZONTAL MOUNT, SPECIAL	EACH	8

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:49:17 AM
 FILE NAME: 86140100-sh1-light-01-gen.dgn
 PLOT DRIVER: ILpdf.dwt
 PEN TABLE: PLOT LABEL.tbl

HRGreen.com
Illinois Professional Design Firm
#184-001322

USER NAME = jmelhus	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = N.T.S.	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

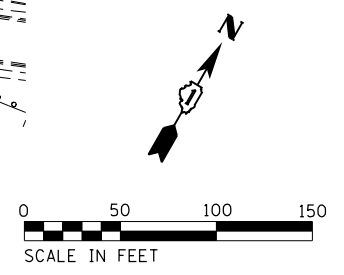
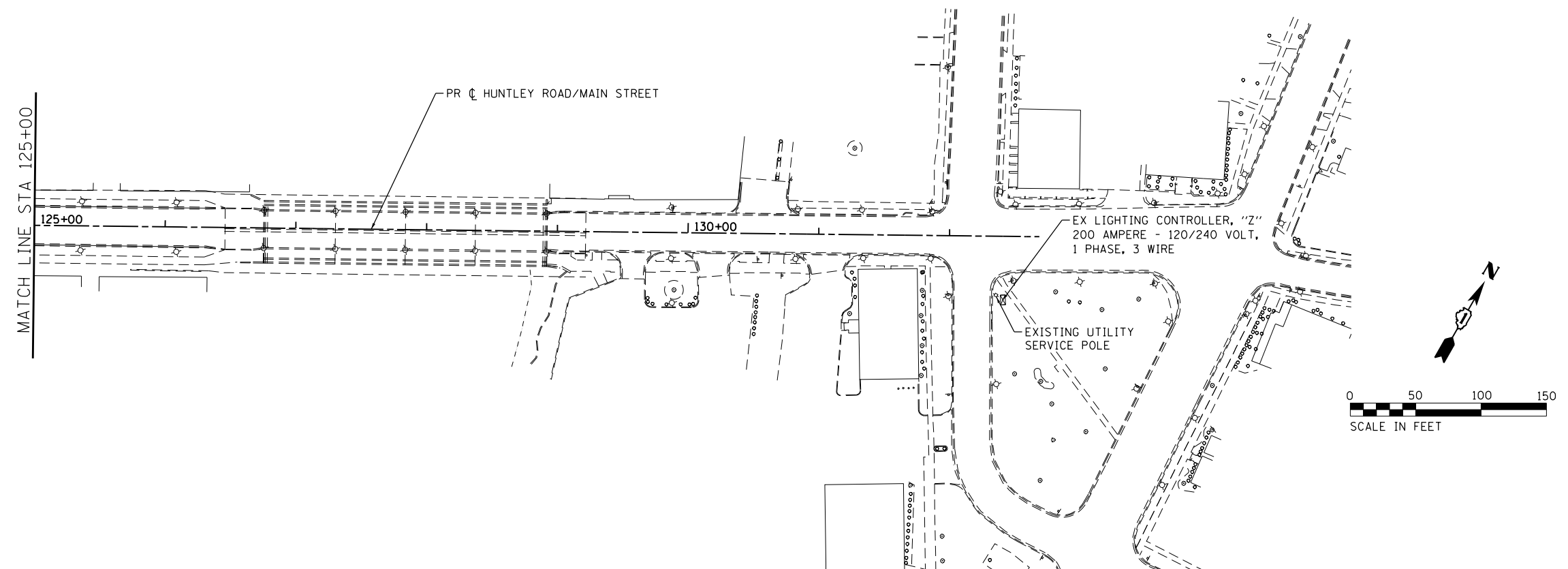
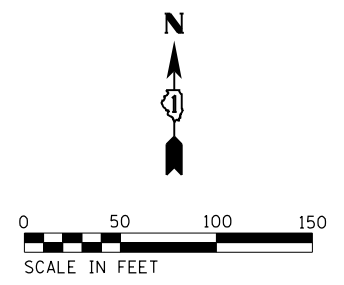
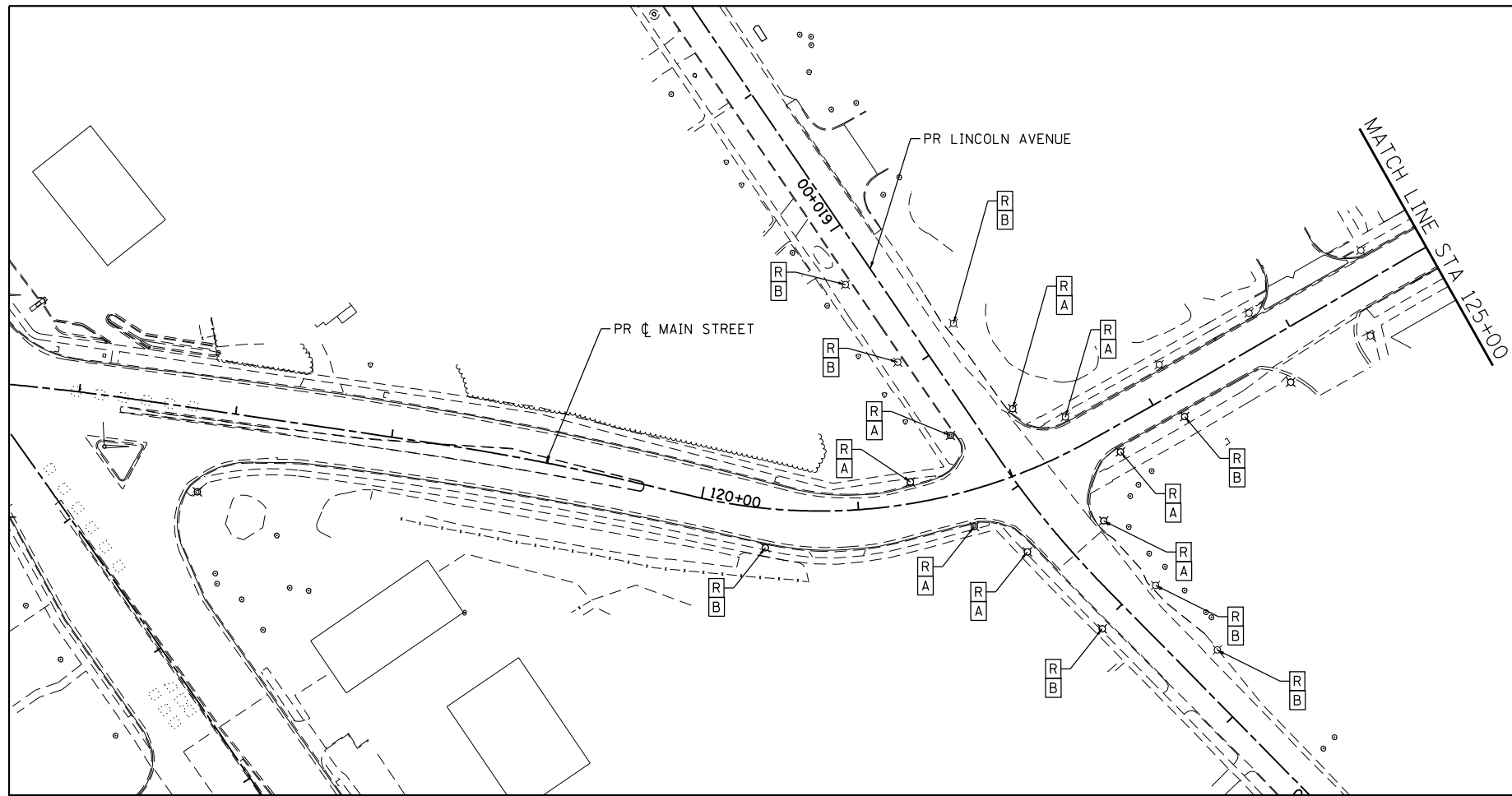
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BILL OF MATERIALS, SYMBOLS & GENERAL NOTES
MAIN ST/LINCOLN AVENUE**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	170
CONTRACT NO.			61E73	
SCALE: N.T.S.		SHEET NO. 1 OF 9 SHEETS	STA.	TO STA.
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	61E73

LEGEND:

A	COBRA LIGHT HEAD
B	ACORN LIGHT HEAD
R	REMOVE & RELOCATE



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 3/23/2018 11:49:21 AM
 FILE NAME: 86140100-sht-light-02-rem.dgn
 PLOT NUMBER: 120148E171
 PEN TABLE:



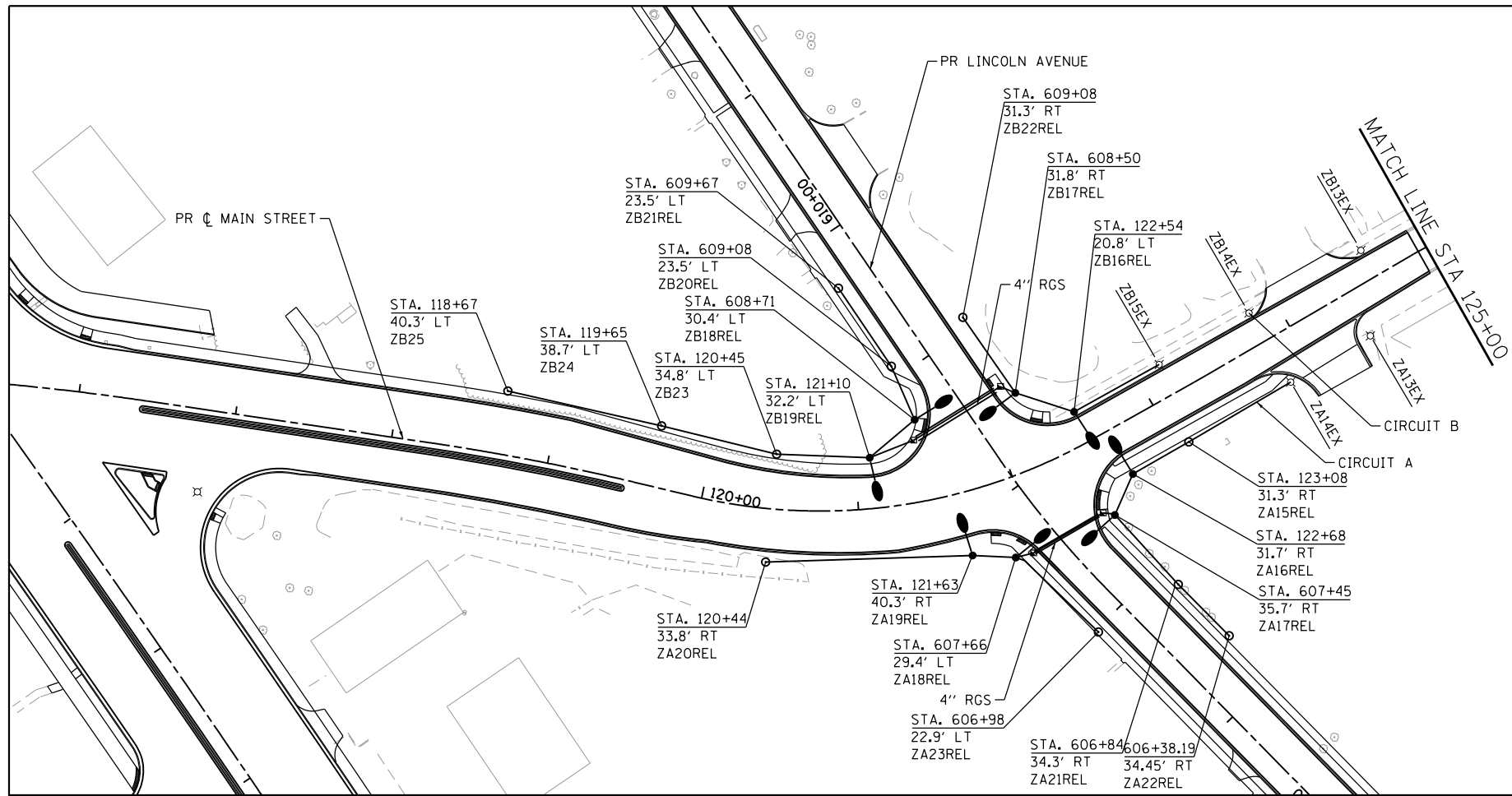
USER NAME = jmelhui	DESIGNED - TAY	REVISED -
FILE NAME = 86140100-sht-light-02-rem.dgn	DRAWN - DMS	REVISED -
PLOT SCALE = N.T.S.	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING LIGHTING REMOVAL PLAN
 MAIN ST/LINCOLN AVENUE**

SCALE: N.T.S. SHEET NO. 2 OF 9 SHEETS STA. TO STA.

F.A.U. RTE. 3887	SECTION NO. 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 171
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

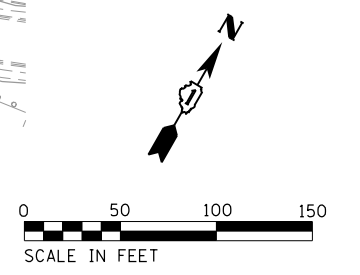
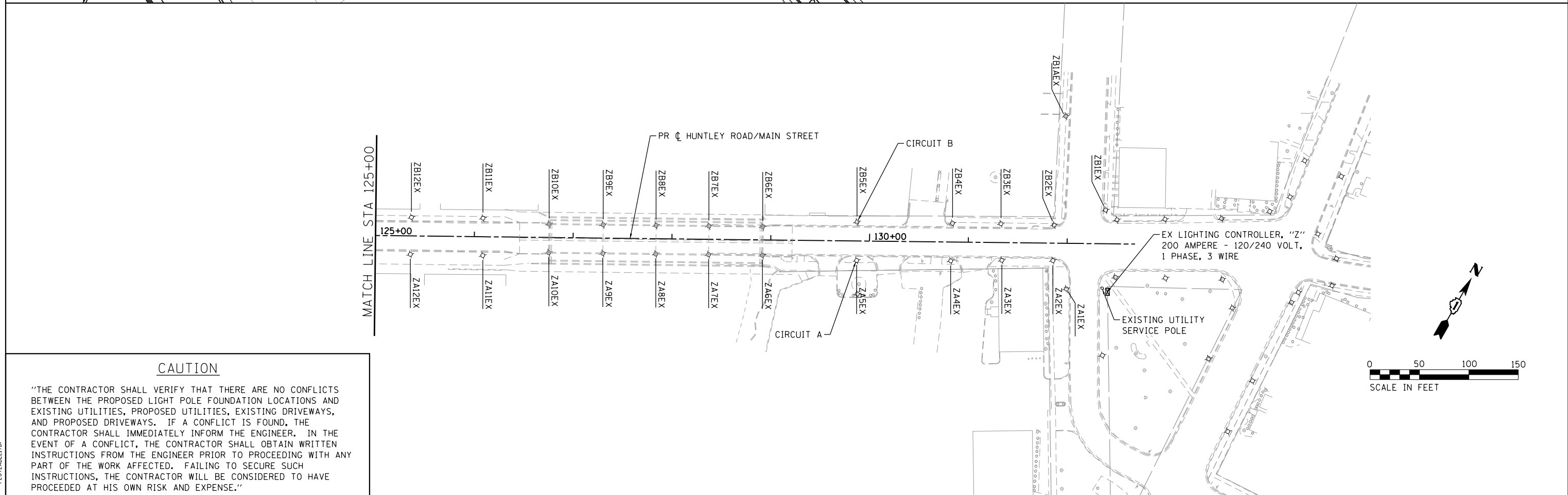
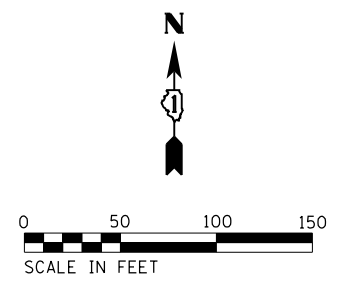


NOTES

1. FOR LEGEND AND GENERAL NOTES SEE SHEET 1 OF 9.
2. RGS STANDS FOR UNDERGROUND CONDUIT, GALVANIZED STEEL.
3. EXISTING COMED MAST ARMS AND LUMINAIRES SHALL BE REMOVED BY COMED. CONTRACTOR SHALL CONTACT COMED FOR COORDINATION.

LEGEND:

- ACORN LIGHT HEAD
- COBRA LIGHT HEAD
- ⊠ EX LIGHT POLE TO REMAIN



CAUTION

"THE CONTRACTOR SHALL VERIFY THAT THERE ARE NO CONFLICTS BETWEEN THE PROPOSED LIGHT POLE FOUNDATION LOCATIONS AND EXISTING UTILITIES, PROPOSED UTILITIES, EXISTING DRIVEWAYS, AND PROPOSED DRIVEWAYS. IF A CONFLICT IS FOUND, THE CONTRACTOR SHALL IMMEDIATELY INFORM THE ENGINEER. IN THE EVENT OF A CONFLICT, THE CONTRACTOR SHALL OBTAIN WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF THE WORK AFFECTED. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE."

COMPANY NAME: #COMPANY_NAME#
 PROJECT CONTACT: #PROJECT_CONTACT#
 CLIENT: #CLIENT#
 DATE PLOTTED: 3/23/2018 11:49:24 AM
 FILE NAME: 86140100-sht-light-03-prop.dgn
 PLOT NUMBER: 11-001-03-PROP-01
 PEN TABLE: PLOTTABLE



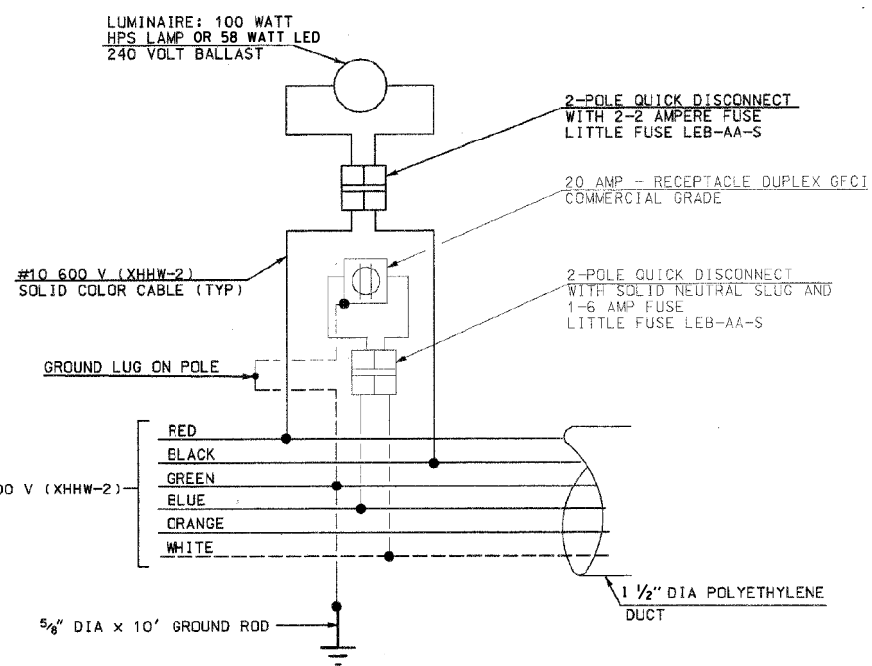
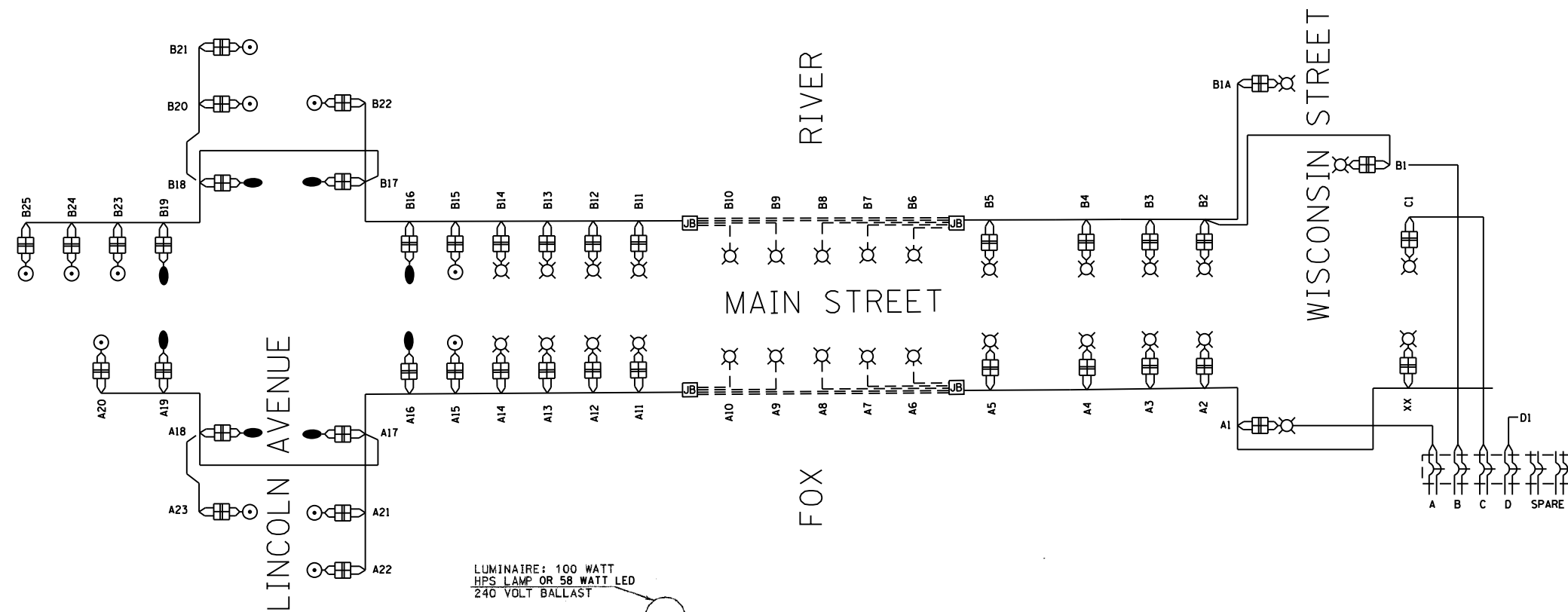
USER NAME = jmelhus	DESIGNED - TAY	REVISED -
FILE NAME = 86140100-sht-light-03-prop.dgn	DRAWN - DMS	REVISED -
PLOT SCALE = N.T.S.	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED LIGHTING IMPROVEMENT
MAIN ST/LINCOLN AVENUE**

SCALE: N.T.S. SHEET NO. 3 OF 9 SHEETS STA. TO STA.

F.A.U. RTE. 3887	SECTION NO. 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 172
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	



CIRCUIT LOAD

CIRCUIT	240 VOLT	
	AMPERE	WATTS
A	4.12.0	2,434 2,702
B	4.13.8	2,434 3,092
C	9.0	1,950
D	8.4	1,824
E	—	—
F	—	—
TOTAL	40.2	8,642

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:49:27 AM
 FILE NAME: 86140100-sh1-light-04-LumDiagram.dgn
 PLOT DRIVER: ILPlotDriver.fcf
 PLOT TABLE: PLOTLABEL.tbl

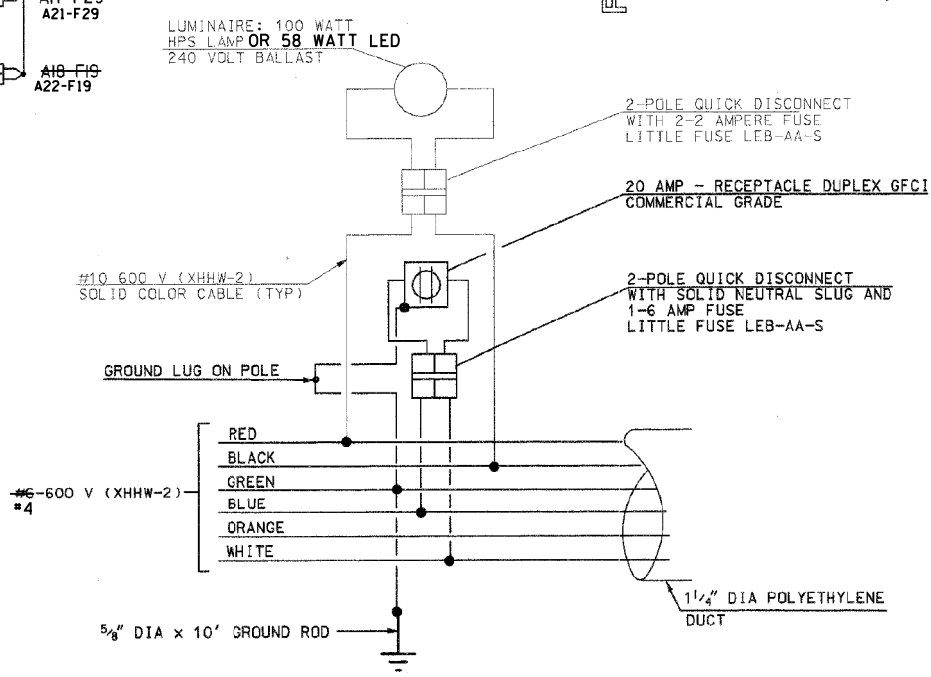
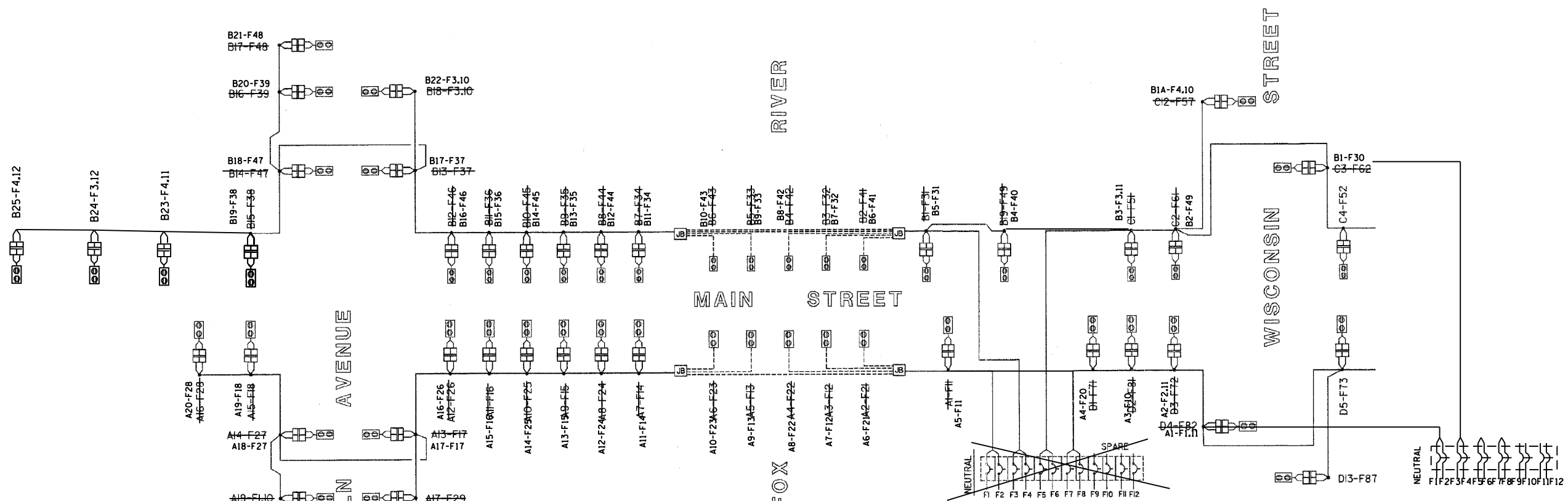


USER NAME = jmelhui	DESIGNED - TAY	REVISED -
DRAWN - DMS	REVISED -	
CHECKED - JRM	REVISED -	
DATE - 3/23/18	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

ONE LINE CIRCUIT DIAGRAM LIGHTING PANEL
 MAIN ST/LINCOLN AVENUE
 SCALE: N.T.S. SHEET NO. 4 OF 9 SHEETS STA. TO STA.

F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 173
CONTRACT NO. 61E73			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT 61E73	



CIRCUIT LOAD

120 VOLT-PHASE BLUE			120 VOLT-PHASE ORANGE		
CIRCUIT	AMPERE	WATTS	CIRCUIT	AMPERE	WATTS
F1	30.0 36.0	3,600 4,320	F2	27.0 33.0	3,240 3,960
F3	30.0 39.0	3,600 4,680	F4	27.0 39.0	3,240 4,680
F5	24.0	2,880	F6	21.0	2,520
F7	21.0	2,520	F8	21.0	2,520
F9	—	—	F10	—	—
F11	—	—	F12	—	—
TOTAL	105.0	12,600	TOTAL	96.0	11,520

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:49:40 AM
 FILE NAME: 86140100-sh1-light-05-RecDiagram.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOTLABEL.tbl



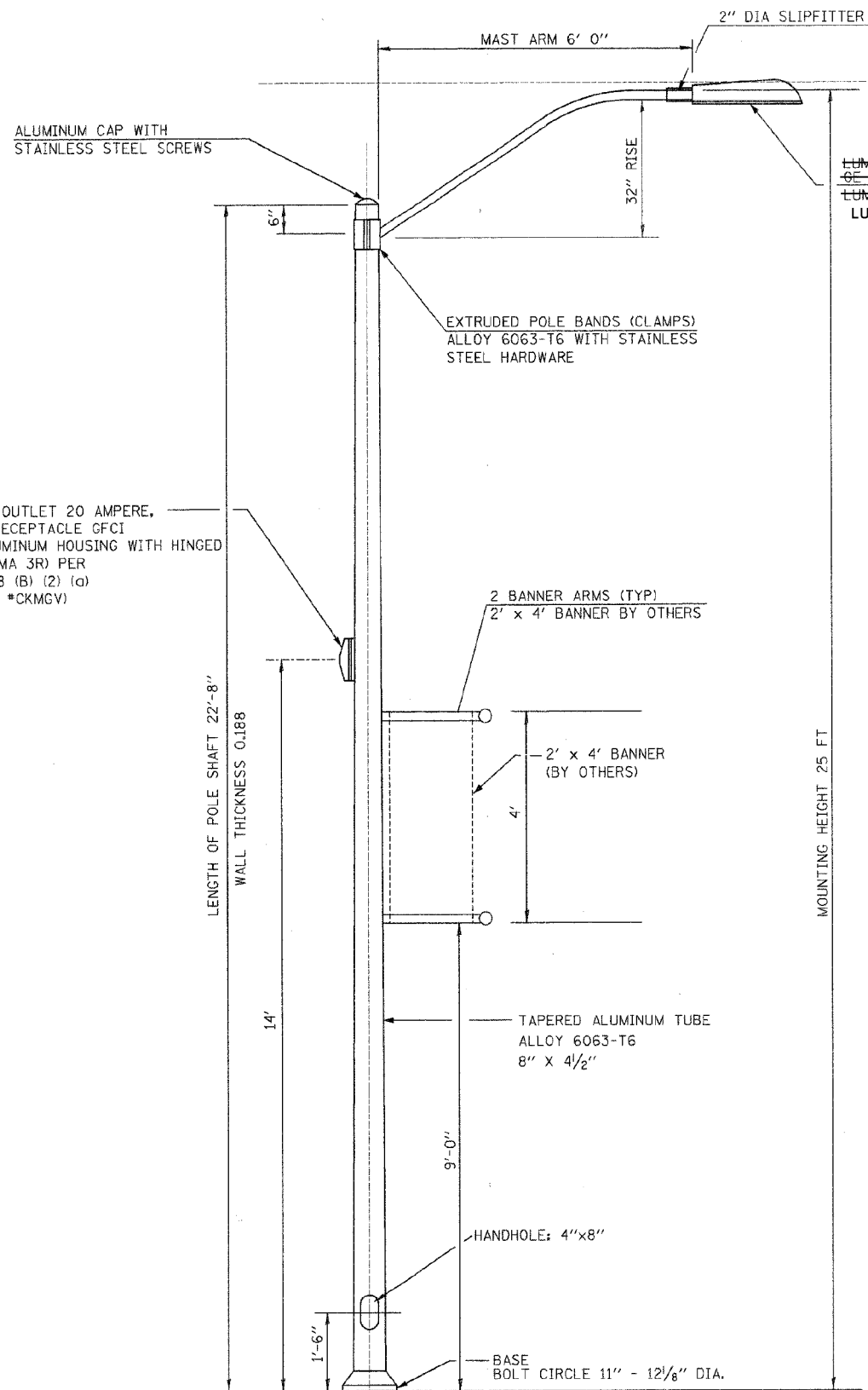
USER NAME = jmelhui	DESIGNED - TAY	REVISED -
	DRAWN - DMS	REVISED -
PLOT SCALE = N.T.S.	CHECKED - JRM	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ONE LINE CIRCUIT DIAGRAM FESTOON PANEL
MAIN ST/LINCOLN AVENUE**

SCALE: N.T.S. SHEET NO. 5 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	174
CONTRACT NO. 61E73			61E73	
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	61E73	



LUMINAIRE SODIUM VAPOR HORIZONTAL MOUNT 100 WATT
 GE M 250A2 POWER/DOOR, PHOTOMETRIC #CE7285
 LUMINAIRE LUMEC OR APPROVED EQUAL
 LUMINAIRE, LED, HORIZONTAL MOUNT, 58 WATT,
 GE EVOLVE, LED ROADWAY, ERLI

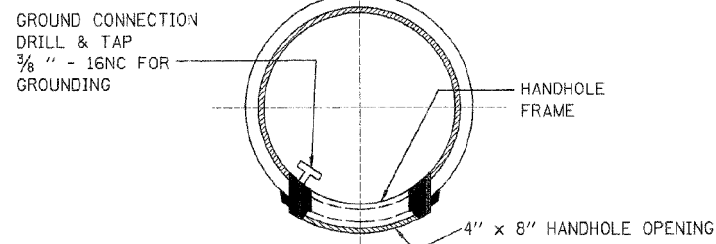
FESTOON OUTLET 20 AMPERE,
 DUPLEX RECEPTACLE GFCI
 CAST ALUMINUM HOUSING WITH HINGED
 DOOR (NEMA 3R) PER
 NEC 406.8 (B) (2) (a)
 (RED DOT *CKMGV)

LENGTH OF POLE SHAFT 22'-8"
 WALL THICKNESS 0.188

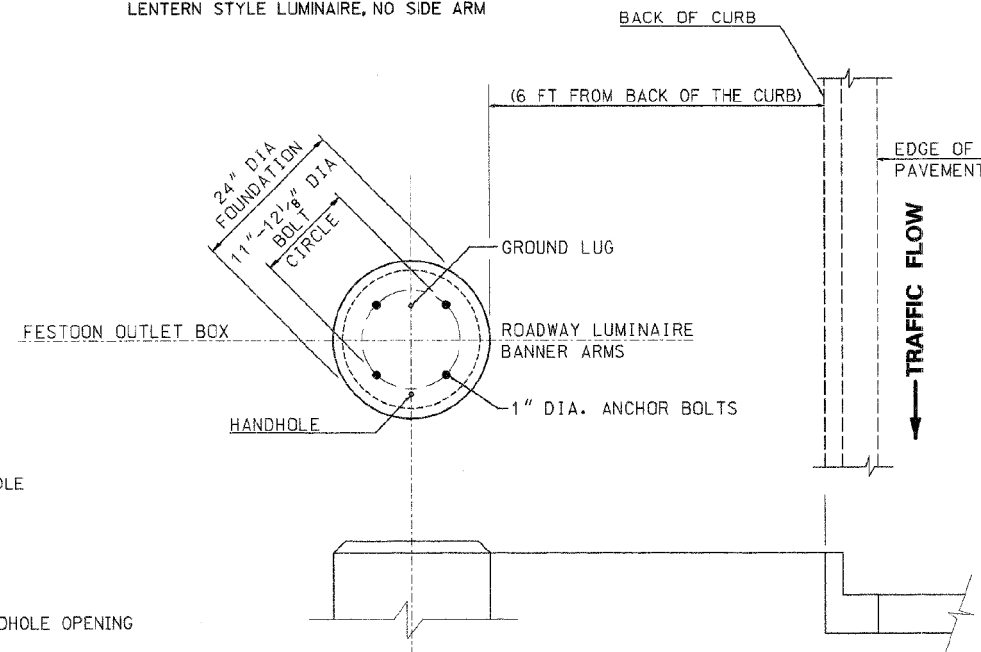
25 FT. M.H. LIGHT POLE

MOUNTING HEIGHT 25 FT

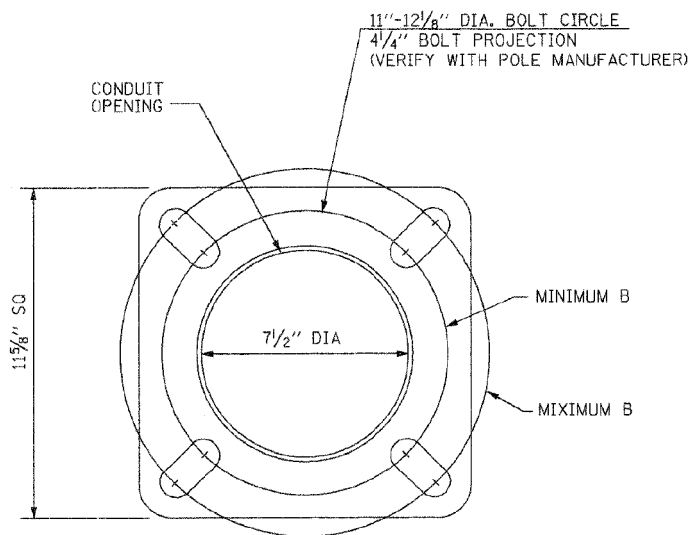
NOTE:
 16' POLE HAS A TENON MOUNT HEXAGONAL
 LANTERN STYLE LUMINAIRE, NO SIDE ARM



SECTION THROUGH HANDHOLE



POSITION OF ANCHOR BOLTS AND POLE ATTACHMENT (TYP)



NOTES

1. THE POLE RECEPTACLE HOUSING, AND LUMINAIRE SHALL BE PAINTED IN POLESTER POWDER COATED TEXTURED BLACK COLOR FINISH.
2. ANCHOR BOLT CIRCLE AND BOLT SLOTS IN POLE BASE PLATE, SHALL BE VARIFIED WITH POLE MANUFACTURED.
3. LUMINAIRES ARE TO BE INSTALLED AS SOON AS POSSIBLE AFTER POLE ERECTED PER MANUFACTURER RECOMMENDATIONS. THE POLE WILL NOT BE PAID FOR UNTIL THE LUMINAIRE IS INSTALLED.
4. THE LIGHT POLE SHALL MEET 80 MPH WIND SPEED AASHTO DESIGN CRITERIA. THE CONTRACTOR SHALL FURNISH WIND LOAD CALCULATIONS MEETING AASHTO DESIGN CRITERIA.
5. THE DUCT SHALL EXTEND INTO POLE A MINIMUM OF 304.3 mm (12") ABOVE THE BASE.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:49:53 AM
 FILE NAME: 86140100-sh1-light-06-LightPoleMntgHeight.dgn
 PLOT DRIVER: ILpdf.pc.plt
 PLOT TABLE: PLOT LABEL.tbl

HRGreen.com
 Illinois Professional Design Firm
 #184-001322

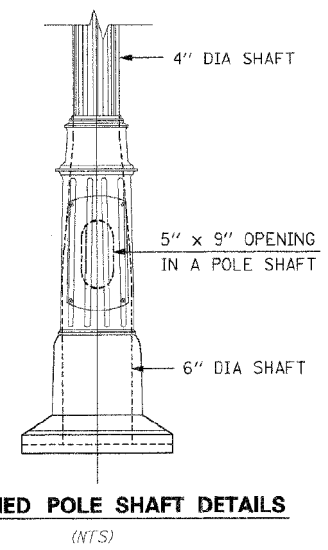
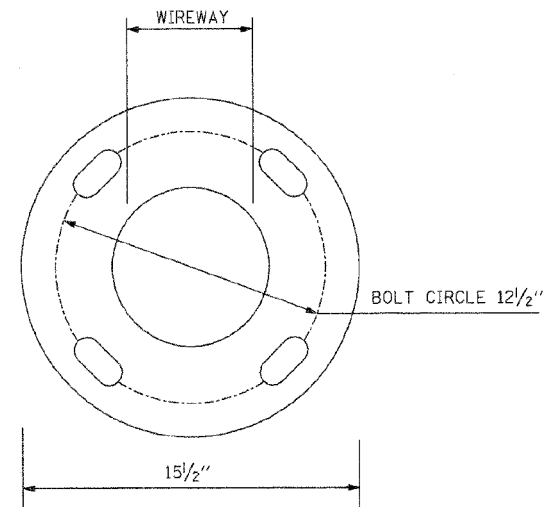
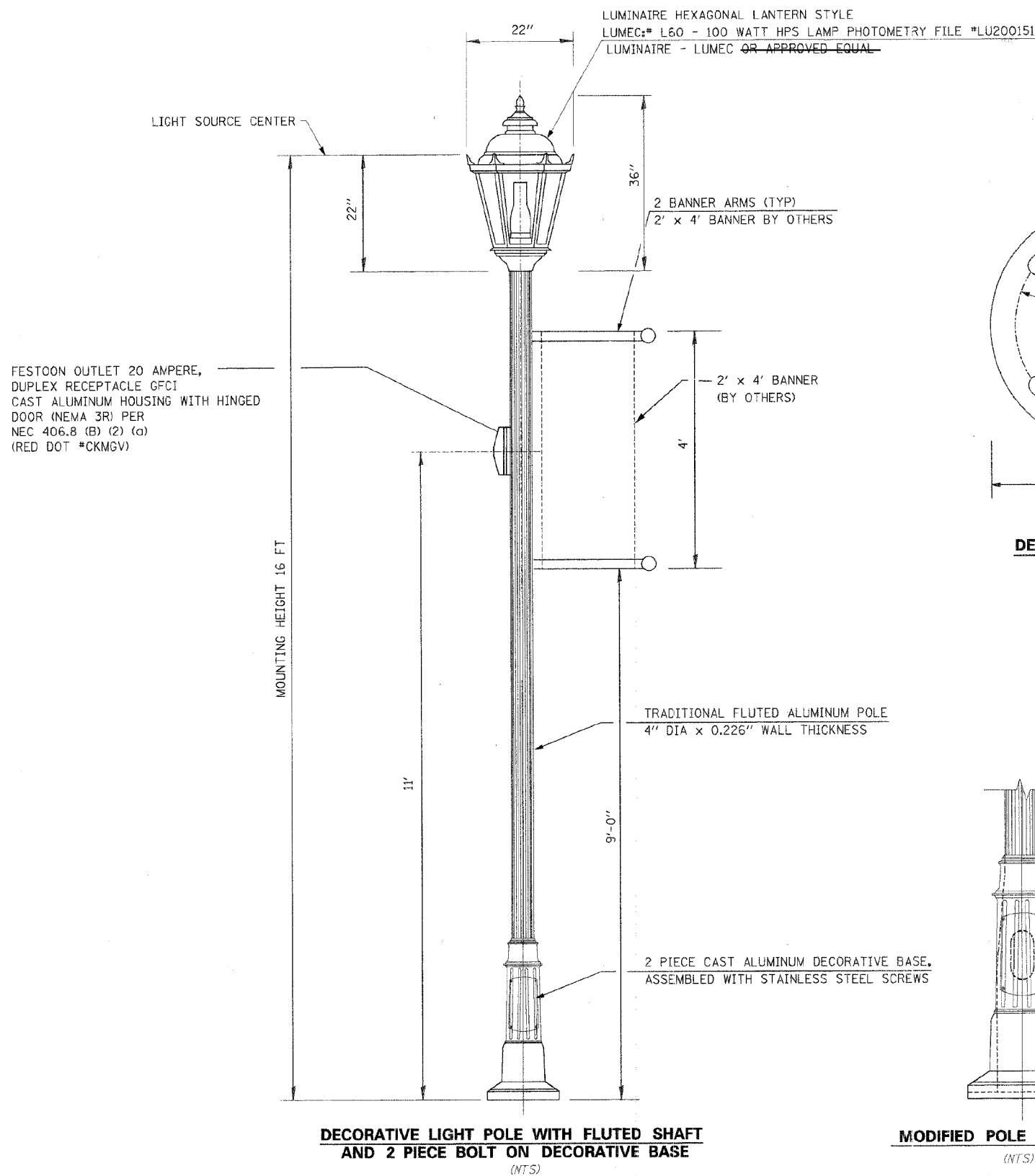
USER NAME = jmelhu	DESIGNED - TAY	REVISED -
PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

LIGHT POLE ALUMINUM 25 FT MOUNTING HEIGHT
 MAIN ST/LINCOLN AVENUE

SCALE: N.T.S. SHEET NO. 6 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	175
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73 61E73	



NOTES

1. THE POLE RECEPTACLE HOUSING, AND LUMINAIRE SHALL BE PAINTED IN POLEMASTER POWDER COATED TEXTURED BLACK COLOR FINISH.
2. ANCHOR BOLT CIRCLE AND BOLT SLOTS IN POLE BASE PLATE, SHALL BE VARIFIED WITH POLE MANUFACTURED.
3. LUMINAIRES ARE TO BE INSTALLED AS SOON AS POSSIBLE AFTER POLE ERECTED PER MANUFACTURER RECOMMENDATIONS. THE POLE WILL NOT BE PAID FOR UNTIL THE LUMINAIRE IS INSTALLED.
4. THE LIGHT POLE SHALL MEET 80 MPH WIND SPEED AASHTO DESIGN CRITERIA. THE CONTRACTOR SHALL FURNISH WIND LOAD CALCULATIONS MEETING AASHTO DESIGN CRITERIA.
5. THE DUCT SHALL EXTEND INTO POLE A MINIMUM OF 304.8 mm (12") ABOVE THE BASE.

COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
DATE PLOTTED: 3/23/2018 11:50:07 AM
FILE NAME: 86140100-sh1-light-07-DecPoleMntgLight.dgn
PLOT DRIVER: ILpdf.plt
PLOT TABLE: PLOT LABEL.tbl

HRGreen
HRGreen.com
Illinois Professional Design Firm
#184-001322

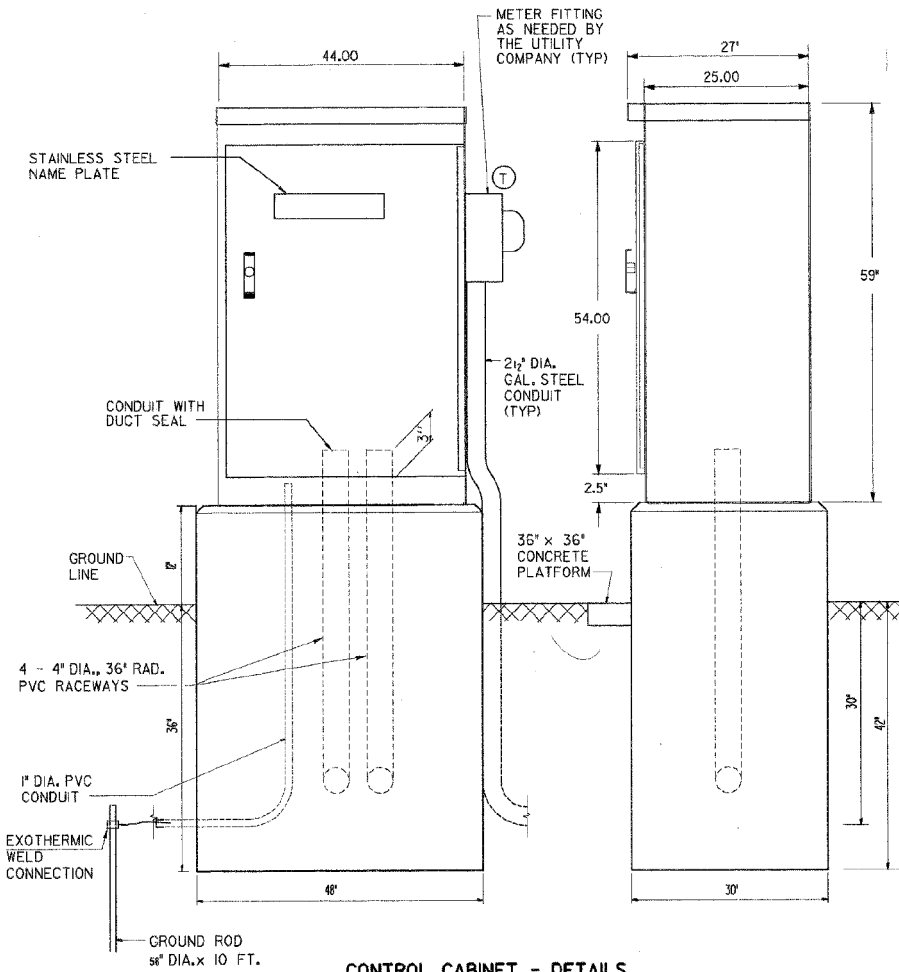
USER NAME = jmelhus	DESIGNED - TAY	REVISED -
PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DECORATIVE POLE 16 FT MOUNTING HEIGHT
MAIN ST/LINCOLN AVENUE**

SCALE: N.T.S. SHEET NO. 7 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	176
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73 61E73	



CONTROL CABINET - DETAILS

NOTES

THE CABINET SHALL BE FABRICATED FROM 0.125" THICK ALUMINUM ALLOY SHEET AND SHALL BE REINFORCED WITH ALUMINUM ANGLES. THE CABINET DOOR SHALL BE NEMA TYPE 3R CONSTRUCTION WITH NEOPRENE GASKET. THE DOOR SHALL HAVE STAINLESS STEEL HINGES AND THREE POINT LOCKING SYSTEM.

THE CONTRACTOR SHALL REMOVE VEGETATION AND TOPSOIL, LEVEL THE AREA IN FRONT OF THE CONTROL CABINET DOOR AND PLACE LENGTH WISE, PARALLEL TO CONTROL CABINET. A PRECAST PAD, 36"x36"x3" MINIMUM SIZE. THE COST OF LABOR AND MATERIALS SHALL BE INCIDENTAL TO THE CONTROL CABINET.

CONTROL WIRE SHALL BE #12 AWG, 600V, TYPE "NTW" STRANDED COPPER GRAY SWITCH BOARD WIRE. THE ENDS OF ALL CONTROL WIRES SHALL BE IDENTIFIED.

THE ELECTRIC METER BOX SHALL BE MOUNTED ON THE SIDE OF THE CONTROL CABINET, NEAR TO THE SERVICE POLE AND/OR AS DIRECTED BY THE ENGINEER.

ALL CONTROL CABINET ITEMS SHALL HAVE SUITABLE IDENTIFICATION. OPEN CIRCUIT BREAKERS, CONTACTORS AND OTHER OPEN DEVICES SHALL HAVE PERMANENT SELF STICKING TAGS. DEVICES IN ENCLOSURES SHALL HAVE ENGRAVED 2-COLOR LAMINATED PLASTIC NAMEPLATES ATTACHED TO ENCLOSURES WITH SCREWS. NAMEPLATES SHALL BE ENGRAVED TO CORRESPOND TO DESIGNATIONS ON THE DRAWINGS. INTERNAL CABINET WIRING SHALL BE IDENTIFIED AS INDICATED OR AS DIRECTED BY THE ENGINEER BY MEANS OF SELF-STICKING TAGS APPLIED AT EACH CONNECTED END. IDENTIFICATION SHALL BE MADE BY THE CABINET MANUFACTURER.

ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.
R = RED BL = BLUE W = WHITE
B = BLACK Y = YELLOW G = GREEN

PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDED FROM DEVICES IN BOXES OR CABINETS WITHIN THE CONTROL CABINET.

THE CABINET SHALL BE PRIMED AND PAINTED AS SPECIFIED. THE OUTSIDE SURFACE OF THE CONTROL CABINET SHALL HAVE A BLACK COAT POWDER FINISH.

THE HEADS OF CONNECTOR SCREWS SHALL BE PAINTED WHITE FOR NEUTRAL BUSS CONNECTION AND GREEN FOR GROUND BUSS CONNECTORS.

ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.

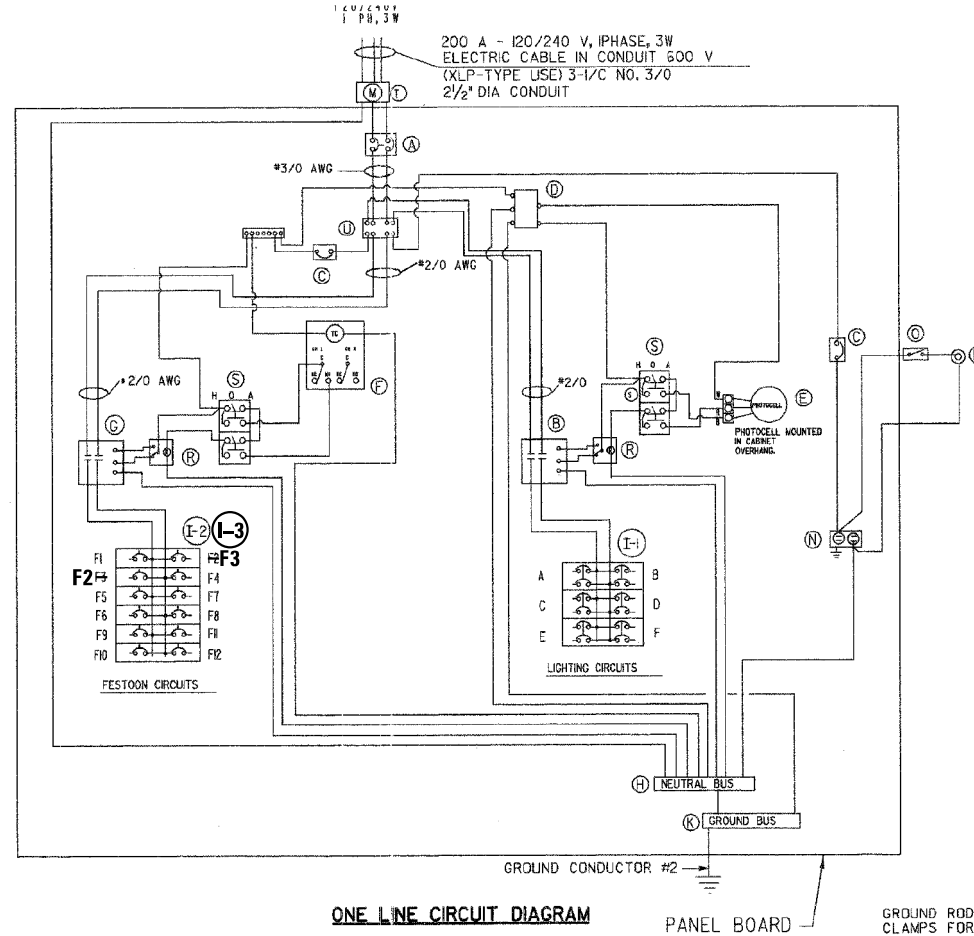
THE CONTROLLER SHALL BE CONSTRUCTED TO U.L. STD. 508 AND BEAR THE U.L. LABEL "ENCLOSED INDUSTRIAL CONTROL PANEL".

PROVIDE A HOLDER AND WATERPROOF POUCH ON THE INNER SIDE OF THE CONTROLLER DOOR. THE HOLDER AND POUCH SHALL BE MOUNTED SO THAT RAIN WATER OR CONDENSED WATER CANNOT ENTER THE POUCH WITH THE CABINET DOOR OPEN. FURNISH A LAMINATED THE APPROVED COPY OF THE "CONTROL CABINET WIRING DIAGRAM".

THE CONTRACTOR SHALL OBTAIN ANCHOR BOLTS LOCATION TEMPLATE FROM THE FABRICATOR OF LIGHTING CONTROLLER

THE CONTROLLER FABRICATION SHALL COMPLY TO ARTICLE 1068.01 OF THE STANDARD SPECIFICATIONS

THE CONTROLLER CABINET DOOR SHOULD OPEN AWAY FROM THE STREET

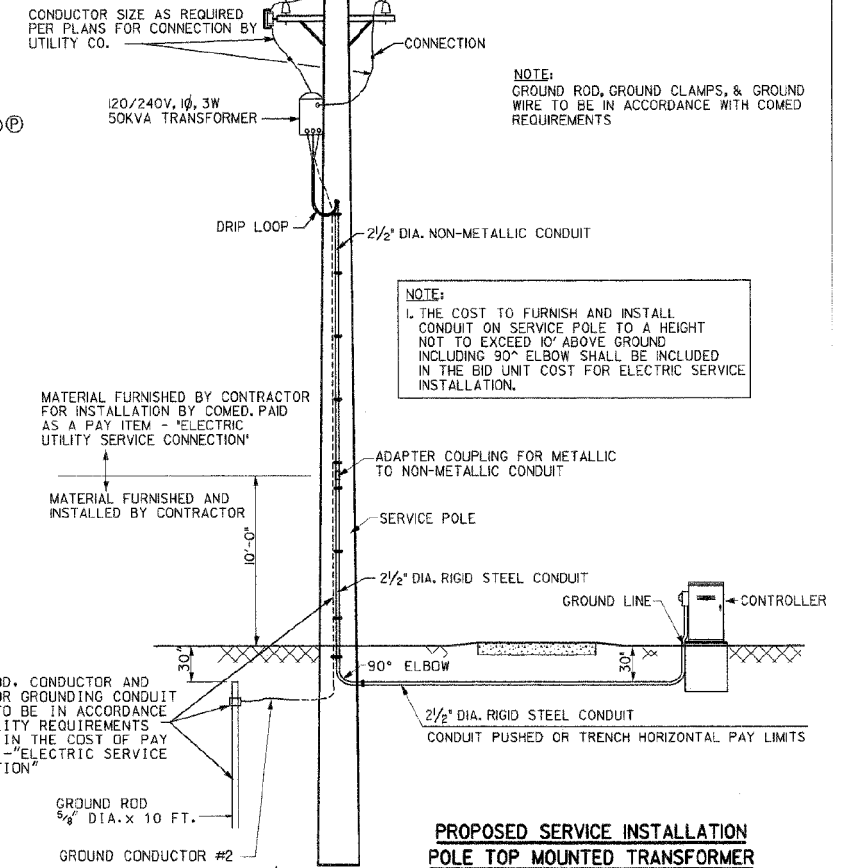


ONE LINE CIRCUIT DIAGRAM

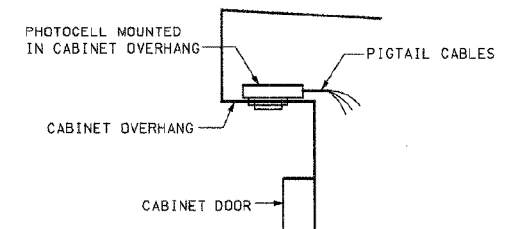
BILL OF MATERIALS

ITEM	QTY	DESCRIPTION
A	1	MAIN BREAKER 2P/225A C/H ED2225L
B	2	CONTACTOR 2P/100A SQ.D8903PB010V38
C	2	BREAKER 1P/15A C/H EHD1015
D	1	SURGE ARRESTOR ED00 SHA2403
E	1	PHOTOCELL FISHER PIERCE FA105M
F	1	ASTRONOMICAL TIME CLK.TORX DZS200A
G	1	CONTACTOR 2P/150A SQ.D8903PB10V38
H	1	NEUTRAL BUS
I1	6	BREAKER 2P/30A C/H EHD2030L
I2	8	BREAKER 1P/40A C/H EHD1040L
J		RESERVED
K	1	GROUND BUS
N	1	GFCI RECEPTACLE 20A LEVITON 8899
O	1	DOOR SWITCH MICRO BA2R01-A2
P	1	LIGHT FIXTURE LEVITON 29816C
R	2	RELAY MAGNECRAFT W389ACX-9
S	2	H-O-A SWITCH SQ.D 9001 KYK-111
T	1	MILBANK U-4363 METER FITTING
U	1	MARATHON 1452129 SPLICE BLOCK

I3 8 BREAKER 1P/50A C/H EHD1050L



PROPOSED SERVICE INSTALLATION POLE TOP MOUNTED TRANSFORMER



PHOTOCELL IN CABINET OVERHANG DETAIL
NOT TO SCALE

NOTE:
CHANGE EXISTING CIRCUIT BREAKERS FOR F1 THRU F4 FROM 40A TO 50A.

COMPANY NAME: HRGreen.com
PROJECT CONTACT: Illinois Professional Design Firm #184-001322
DATE PLOTTED: 3/23/2018 11:50:21 AM
FILE NAME: 86140100-sht-light-08-LightingController.dgn
PLOT DRIVER: ILpdf.lwp
PLOT TABLE: PLOT LABEL.tbl



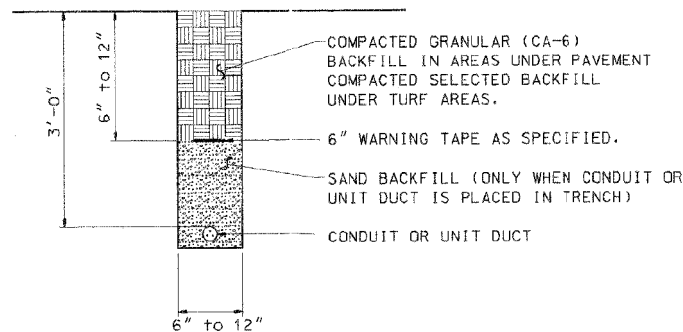
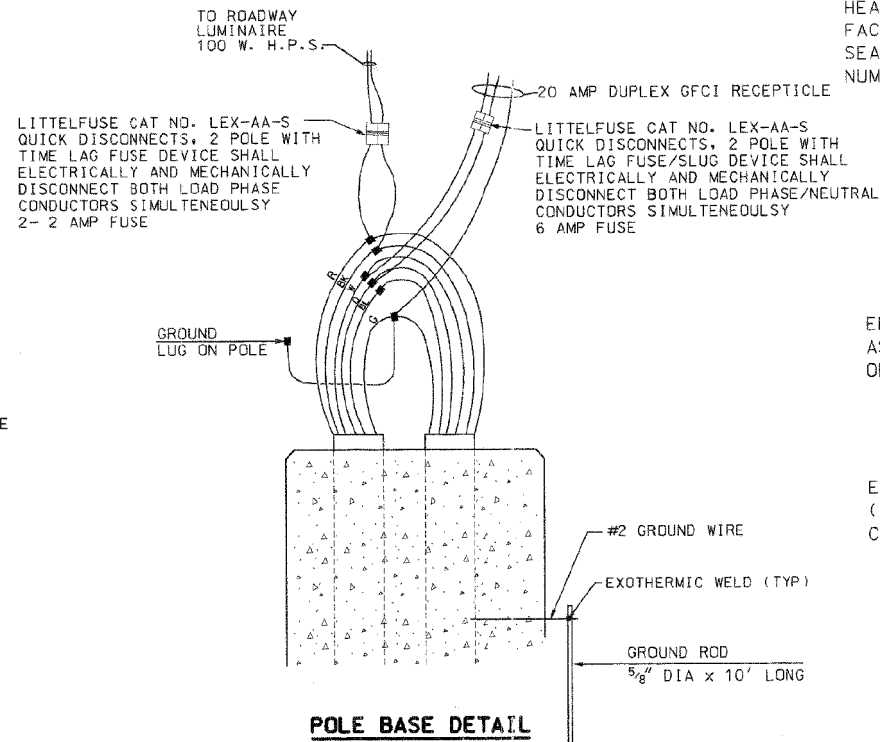
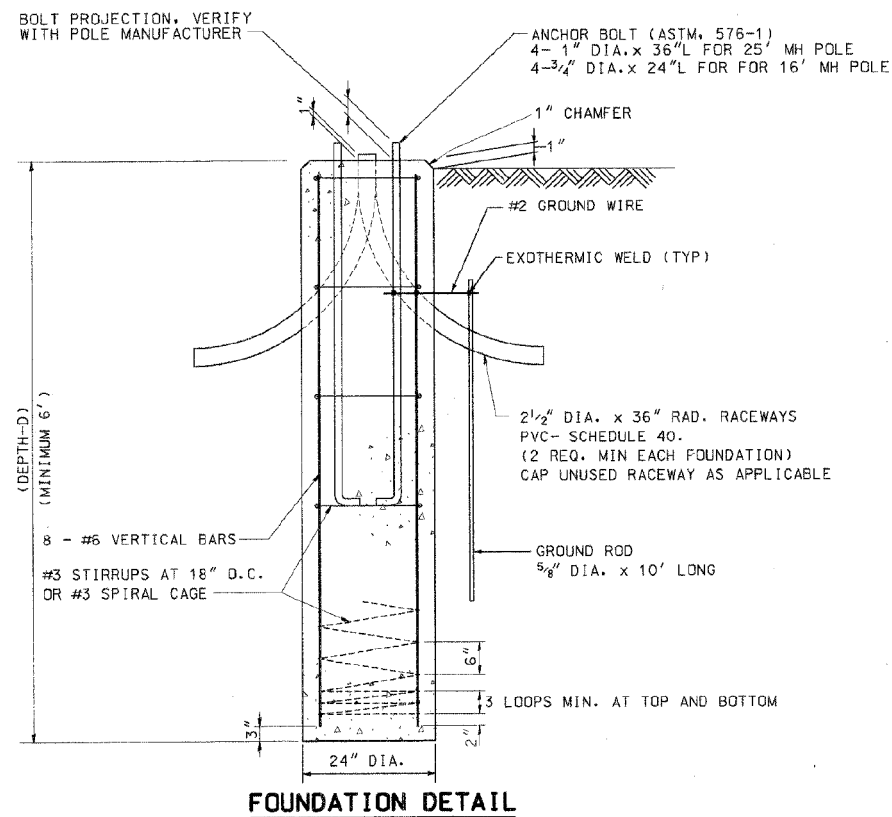
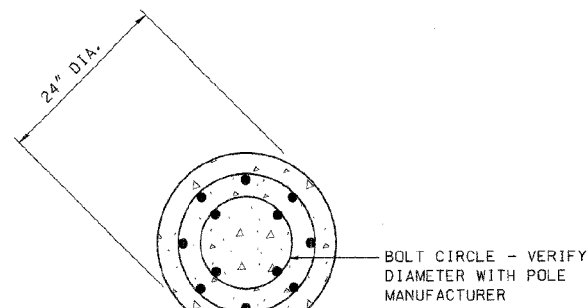
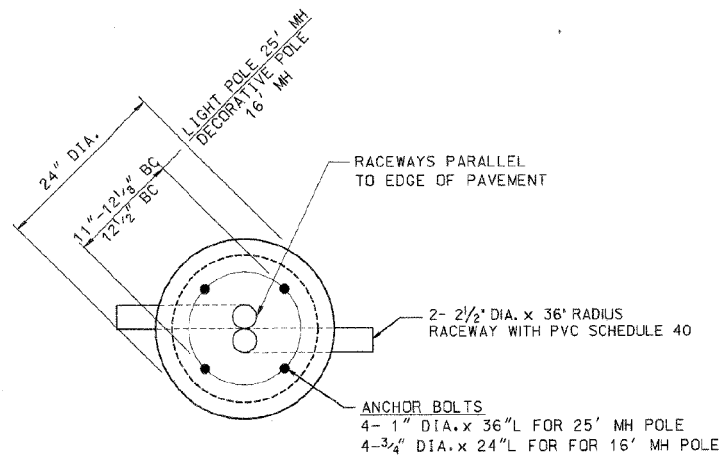
USER NAME = jmelhus	DESIGNED - TAY	REVISED -
PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

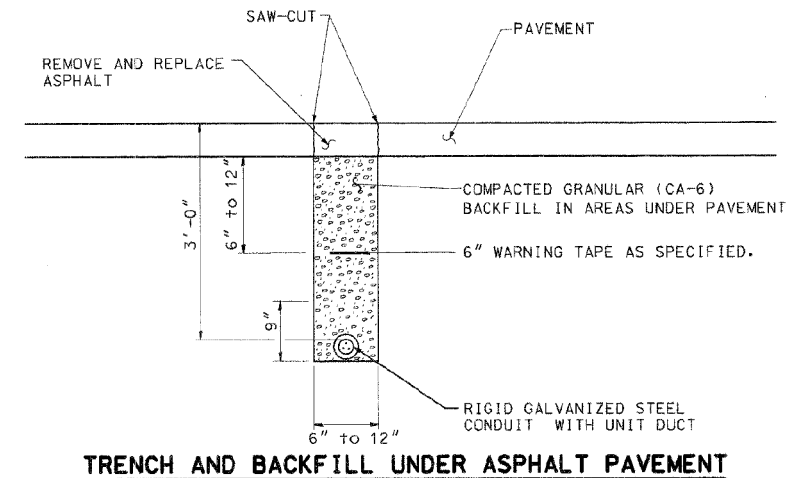
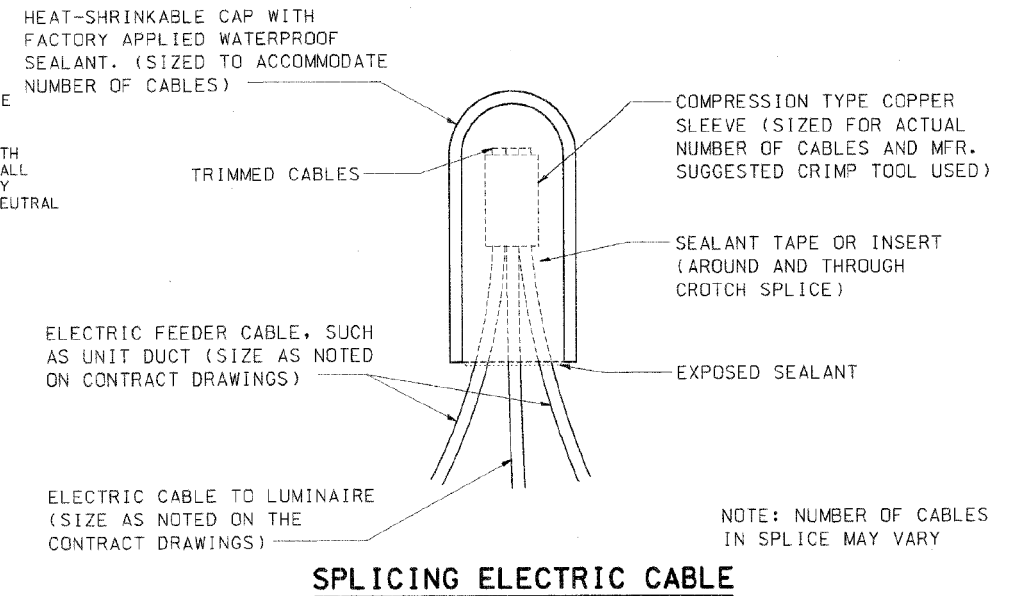
**LIGHTING CONTROLLER, SPECIAL
MAIN ST/LINCOLN AVENUE**

SCALE: N.T.S. SHEET NO. 8 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	177
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61E73	
			61E73	



- NOTES: FOUNDATION**
1. THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
 2. THE DESIGN DEPTH "D" OF FOUNDATION SHALL BE ACCORDING TO FIELD CONDITIONS.
 3. EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER 24" IN DIAMETER.
 4. THE CONTRACTOR SHALL USE #3 SPIRAL AT 6" PITCH OR AT HIS OPTION MAY SUBSTITUTE #3 TIES AT 12" O.C.
 5. THE ANCHOR BOLTS TIED WITH REINFORCEMENT AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
 6. CONCRETE SHALL BE CLASS "SI". CONCRETE FOUNDATION MUST BE CURED FOR (10) TEN DAYS BEFORE THE LIGHT STANDARD IS ERECTED.
 7. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT POLE IS ERECTED.
 8. ANCHOR BOLTS PROJECTION ABOVE THE TOP OF THE FOUNDATION SHALL BE ACCORDING TO THE REQUIREMENTS OF POLE MANUFACTURER.
 9. RACEWAYS SHALL PROJECT 1" ABOVE THE TOP OF THE FOUNDATION.
 10. REINFORCEMENT BARS SHALL COMPLY WITH ARTICLE 1006.10 OF THE STANDARD SPECIFICATION.
 11. THE GROUND ROD, REINFORCING BAR AND ANCHOR BOLT SHALL BE TIED WITH #2 COPPER WIRE AND CONNECTION SHALL BE EXOTHERMIC WELD.



FOUNDATION DESIGN TABLE

TYPE OF SOIL	DESIGN DEPTH OF FOUNDN		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	13'-0"	15'-0"	8-#6 X 12'-6"	*3 X 122'	8-#6 X 12'-6"	*3 X 141'
MEDIUM CLAY	9'-6"	10'-9"	8-#6 X 9'-0"	*3 X 90'	8-#6 X 10'-0"	*3 X 100'
STIFF CLAY	7'-0"	8'-0"	8-#6 X 6'-6"	*3 X 66'	8-#6 X 7'-6"	*3 X 75'
LOOSE SAND	9'-0"	10'-0"	8-#6 X 8'-6"	*3 X 85'	8-#6 X 9'-6"	*3 X 94'
MEDIUM SAND	8'-3"	9'-0"	8-#6 X 8'-0"	*3 X 78'	8-#6 X 8'-6"	*3 X 85'
DENSE SAND	7'-9"	9'-0"	8-#6 X 7'-6"	*3 X 73'	8-#6 X 8'-0"	*3 X 85'
ROCK OR SOLIDIFIED SLAG	5'-0"	5'-0"	NONE	NONE	NONE	NONE

(FROM IDOT STANDARD FOUNDATION DRAWING)

COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
DATE PLOTTED: 3/23/2018 11:50:33 AM
FILE NAME: 86140100-sh1-light-09-ElectricalDetails.dgn
PLOT DRIVER: ILpdf.plt
PLOT TABLE: PLOT LABEL.tbl



USER NAME = jmelhus	DESIGNED - TAY	REVISED -
PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED -
PLOT DATE = 3/23/2018	CHECKED - JRM	REVISED -
	DATE - 3/23/18	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ELECTRICAL DETAILS
MAIN ST/LINCOLN AVENUE

SCALE: N.T.S. SHEET NO. 9 OF 9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	178
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73 61E73	

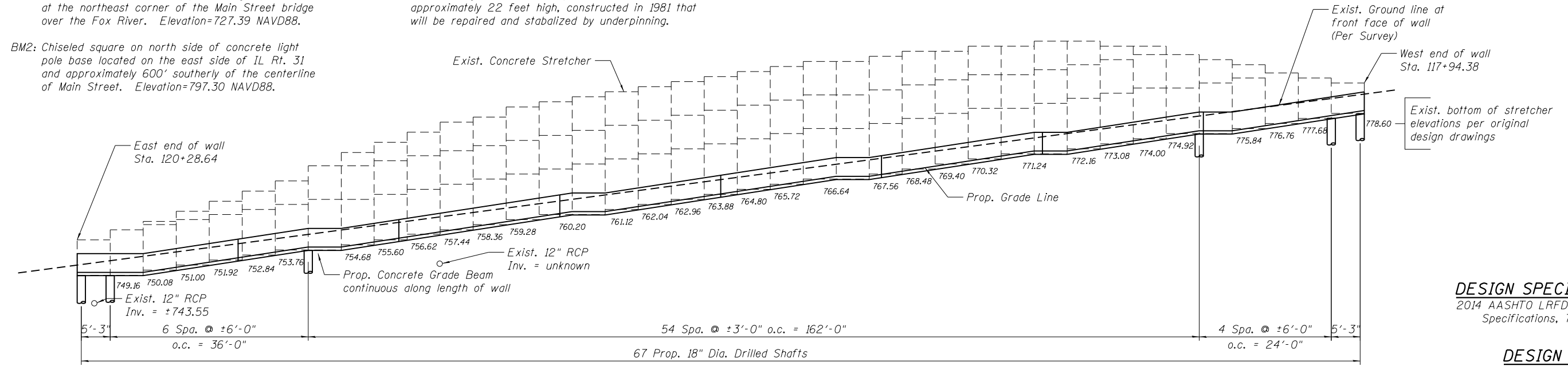
Benchmarks:

BM1: Aluminum disk located in top of bridge wall located at the northeast corner of the Main Street bridge over the Fox River. Elevation=727.39 NAVD88.

BM2: Chiseled square on north side of concrete light pole base located on the east side of IL Rt. 31 and approximately 600' southerly of the centerline of Main Street. Elevation=797.30 NAVD88.

Existing Structure:

The existing structure is a precast concrete crib wall approximately 22 feet high, constructed in 1981 that will be repaired and stabilized by underpinning.

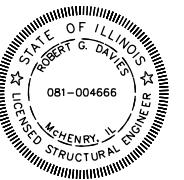
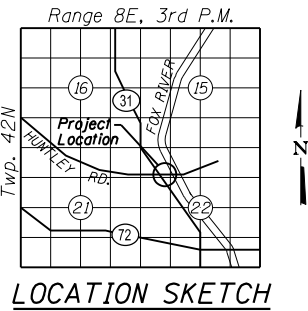
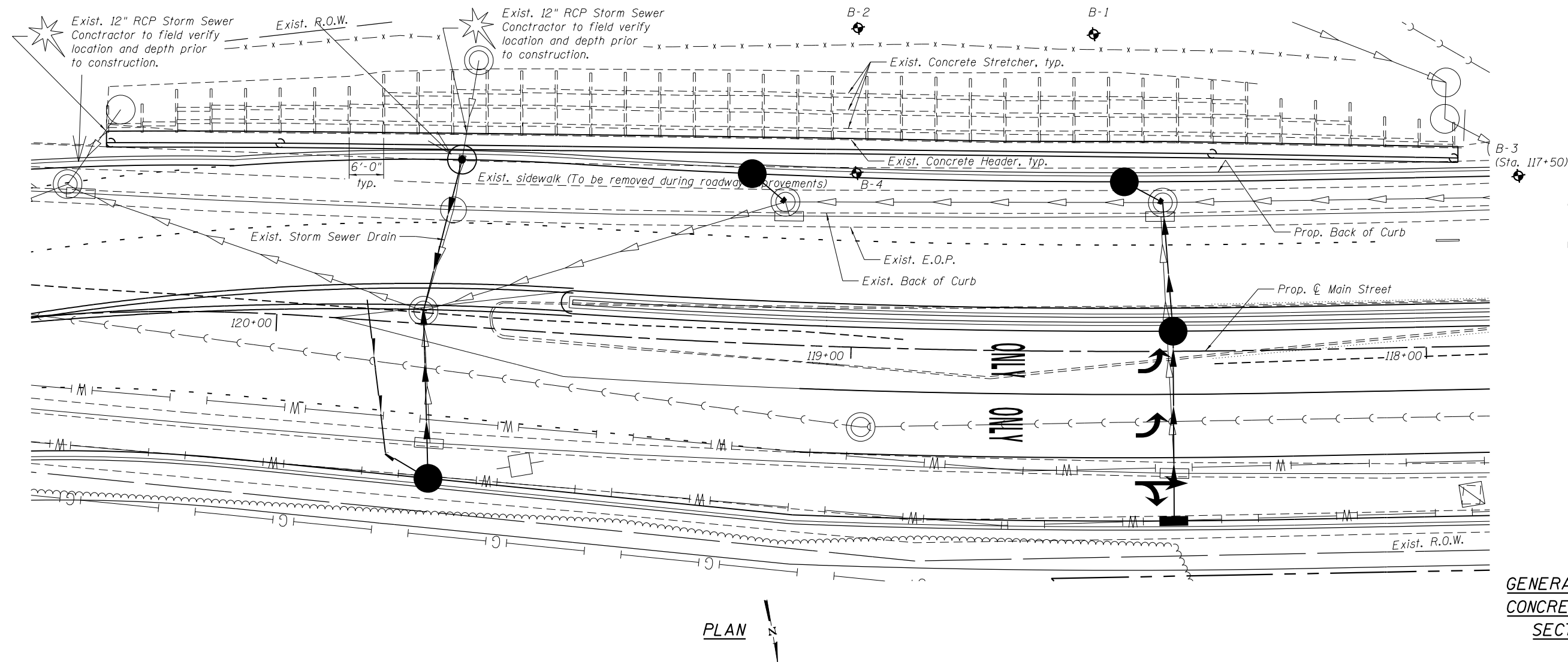


ELEVATION
(Looking South)

DESIGN SPECIFICATIONS
2014 AASHTO LRFD Bridge Design Specifications, 7th Edition

DESIGN STRESSES
FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)



GENERAL PLAN AND ELEVATION
CONCRETE CRIB WALL REPAIRS
SECTION 08-00083-00-CH
KANE COUNTY

COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
DATE PLOTTED: 3/23/2018 11:50:47 AM
FILE NAME: 86140100-str-08E01.dgn
PLOT DRIVER: ILpdf.plt
PLOT TABLE: PLOT LABEL.tbl



USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

SCALE: SHEET NO. SCW-1 OF SCW-4 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	179
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

GENERAL NOTES

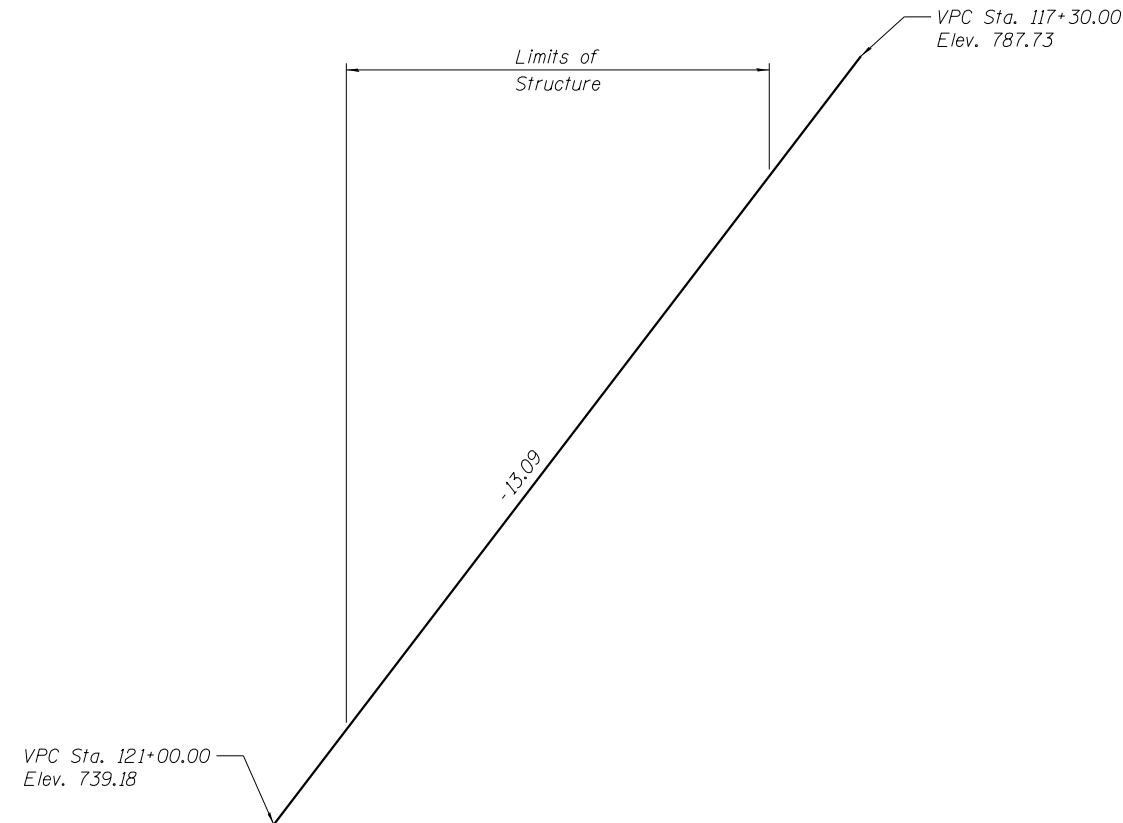
1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3. The crib wall reinforcement work must be completed before any excavation for roadway improvements occur within 10 feet of the front of the existing wall.
4. Construction Sequence:
 - a. Prior to excavation in front of wall, drill and install 18" ϕ drilled shafts. Alternate drilled shaft installation such that open holes are not less than 6'-0" on center at any time.
 - b. Excavate and Construct alternating segments of grade beam (See suggested sequence on sheet SCW-3). It will be necessary to field bend #4 p(E) and p (E) bars. It may be necessary to support bottom stretch with concrete blocks (cost included).
 - c. After the minimum 72 hour cure time, excavate remaining segments and construct remainder of grade beam.
5. Stud Shear Connectors shall be installed in accordance with Article 505.08 (m) of the Standard Specifications, but paid for under Furnishing and Erecting Structural Steel.
6. Protective Coat shall be applied to the front and top faces of the grade beam per Article 503.19 of the Standard Specifications.
7. Excavation of entire length of grade beam at one time will not be permitted unless Contractor can devise another acceptable method to stabilize the existing crib wall.

TOTAL BILL OF MATERIALS

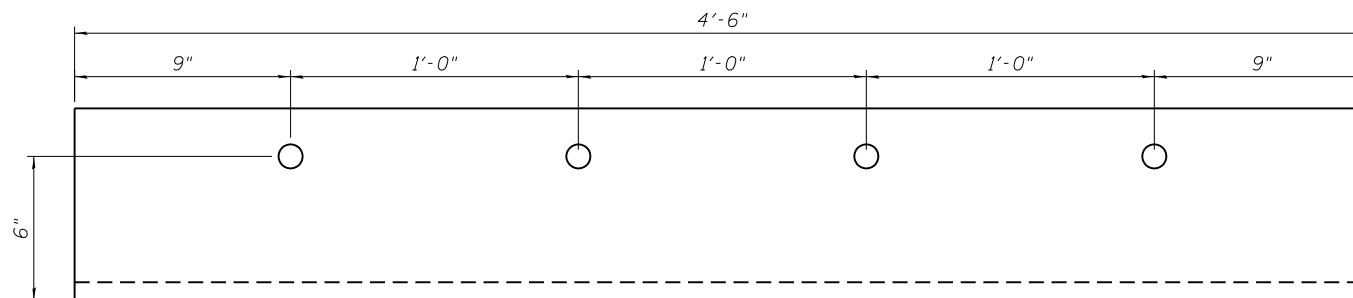
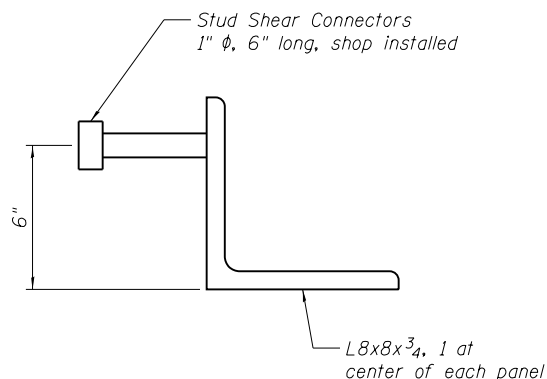
Item	Unit	Total
Structure Excavation	Cu Yd	135
Concrete Structures	Cu Yd	70.4
Protective Coat	Sq Yd	158
Furnishing and Erecting Structural Steel	Pound	7,070
Reinforcement Bars, Epoxy Coated	Pound	16,030
Drilled Shaft in Soil	Cu Yd	87.7

INDEX OF SHEETS

- SCW-1. General Plan and Elevation Concrete Crib Wall
- SCW-2. Concrete Crib Wall General Details
- SCW-3. Concrete Crib Wall Typical Sections and Details
- SCW-4. Concrete Crib Wall Soil Boring Logs



PROFILE GRADE HUNTLEY RD/ MAIN ST.



STEEL ANGLE DETAILS

Assembly to be Hot Dip Galvanized in accordance with AASHTO M111. Cost included under Furnishing and Erecting Structural Steel.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 DATE PLOTTED: 3/23/2018 11:56:49 AM
 FILE NAME: 86140100-str-con01.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOT LABEL.tbl



USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

**CONCRETE CRIB WALL GENERAL DETAILS
 ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SCW-2 OF SCW-4 SHEETS STA. TO STA.

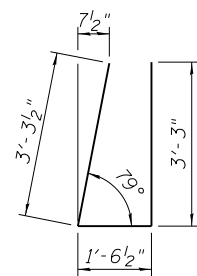
F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 180
CONTRACT NO. 61E73			ILLINOIS FED. AID PROJECT	

TOP OF PILE ELEVATIONS

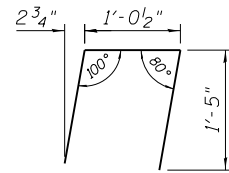
Pile No.	T/Pile Elevation
1	749.08
2	749.08
3	749.20
4	750.12
5	751.04
6	751.96
7	752.88
8	753.68
9	753.68
10	753.80
11	754.26
12	754.72
13	755.18
14	755.64
15	756.10
16	756.56
17	757.02
18	757.48
19	757.94
20	758.40
21	758.86
22	759.32
23	759.78

Pile No.	T/Pile Elevation
24	760.12
25	760.12
26	760.24
27	760.70
28	761.16
29	761.62
30	762.08
31	762.54
32	763.00
33	763.46
34	763.92
35	764.38
36	764.84
37	765.30
38	765.76
39	766.22
40	766.56
41	766.56
42	766.68
43	767.14
44	767.60
45	768.06

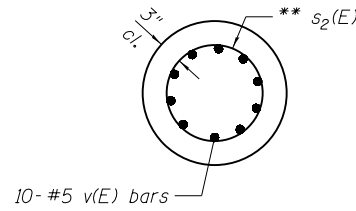
Pile No.	T/Pile Elevation
46	768.52
47	768.98
48	769.44
49	769.90
50	770.36
51	770.82
52	771.16
53	771.16
54	771.28
55	771.74
56	772.20
57	772.66
58	773.12
59	773.58
60	774.04
61	774.50
62	774.84
63	774.96
64	775.88
65	776.80
66	777.72
67	778.52



BAR s(E)

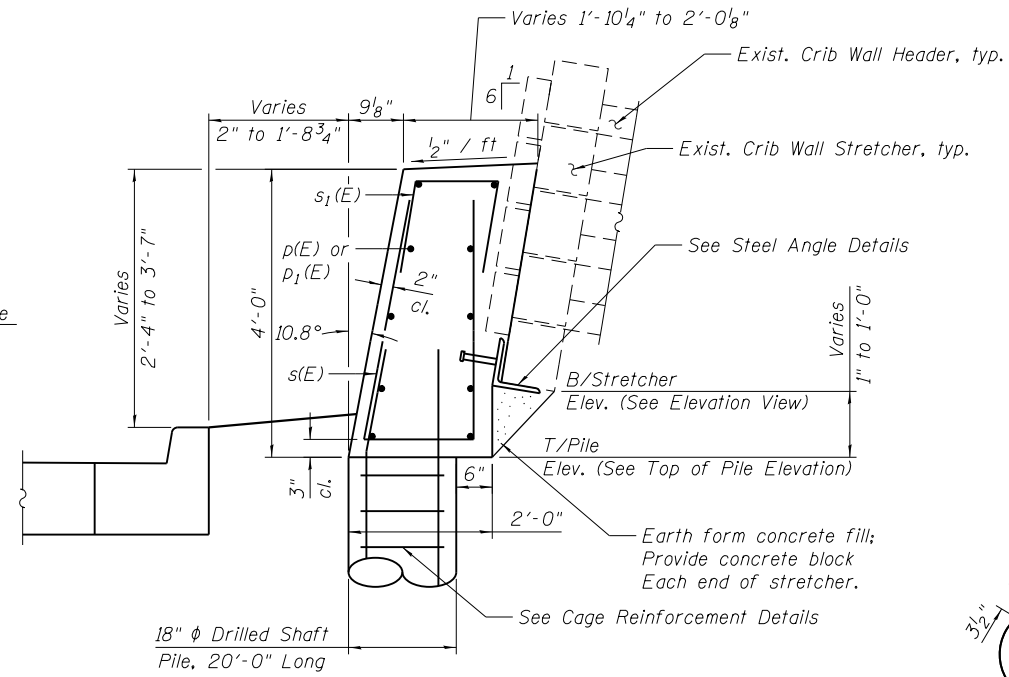
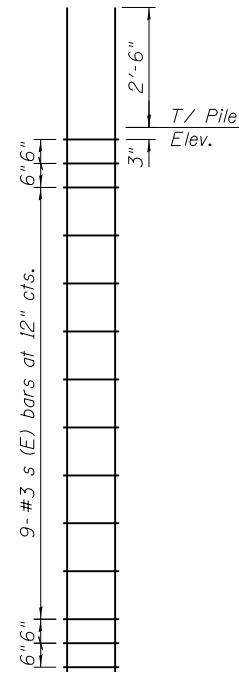


BAR s1(E)



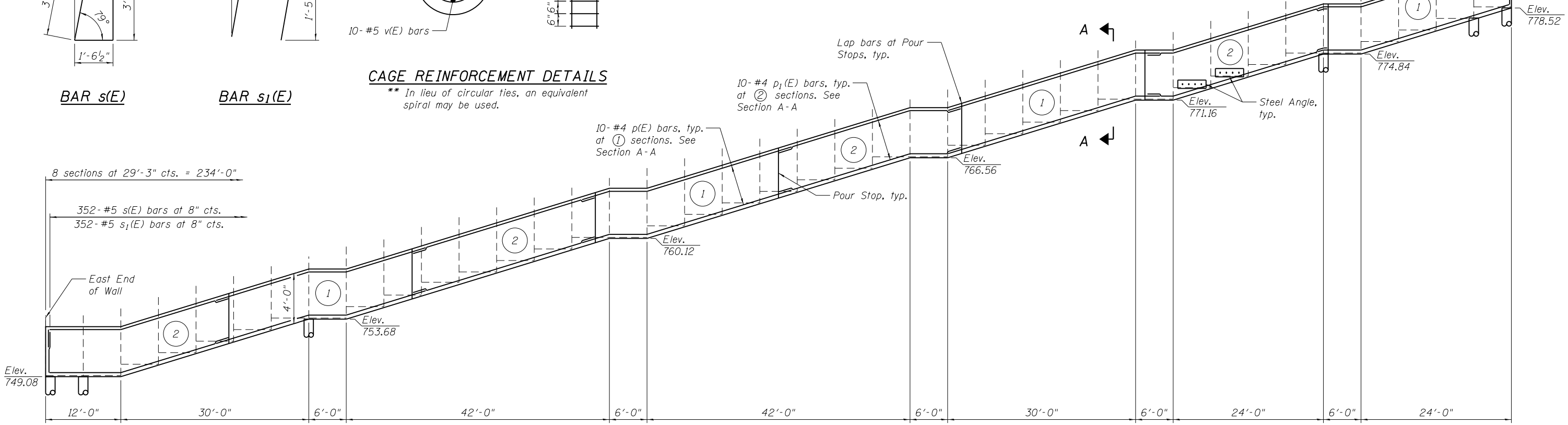
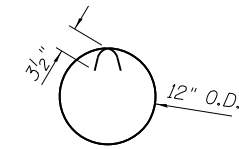
CAGE REINFORCEMENT DETAILS

** In lieu of circular ties, an equivalent spiral may be used.



SECTION A-A

BAR s1(E)



CONCRETE GRADE BEAM ELEVATION VIEW

Note: Construct ① sections first. Do not excavate any soil in ② sections until construction of ① sections are complete.

CONCRETE GRADE BEAM BILL OF MATERIALS

Bar	No.	Size	Length	Shape
p (E)	40	#4	34'- 0"	—
p1(E)	40	#4	29'- 3"	—
s(E)	352	#5	8'- 1"	—
s1(E)	352	#5	3'- 11"	—
s2(E)	871	#3	3'- 8"	—
v(E)	670	#5	12'- 6"	—
Structure Excavation			Cu Yd	135
Concrete Structures			Cu Yd	70.4
Reinforcement Bars, Epoxy Coated			Pound	16,030

MIN. BAR LAP
#4 bar = 2'-0"

COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
DATE PLOTTED: 3/23/2018 11:50:51 AM
FILE NAME: 86140100-str-04101-1.dgn
PLOT DRIVER: ILpdf.plt
PLOT TABLE: PLOT LABEL.tbl



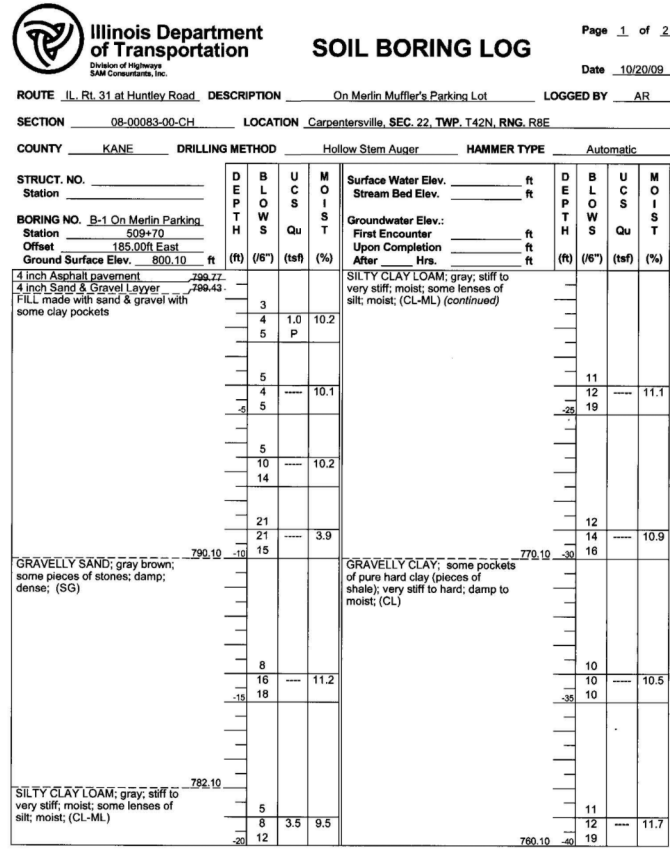
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

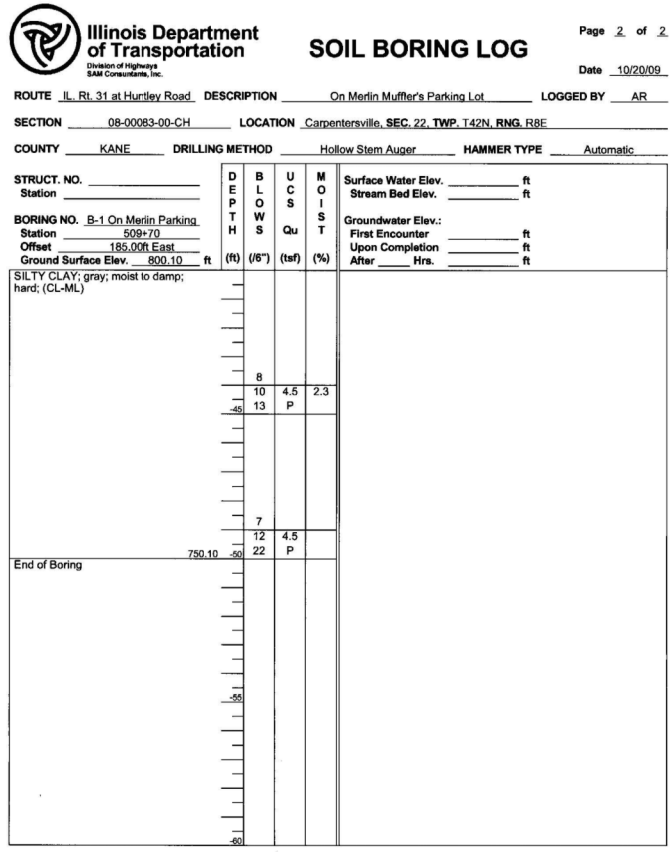
**CONCRETE CRIB WALL TYPICAL SECTIONS AND DETAILS
ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SCW-3 OF SCW-4 SHEETS STA. TO STA.

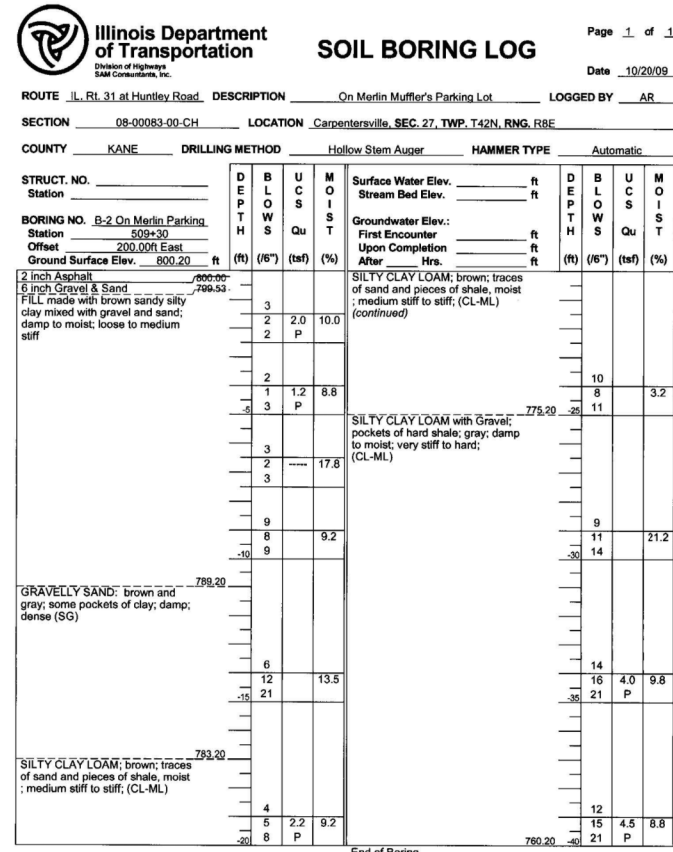
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	181
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	



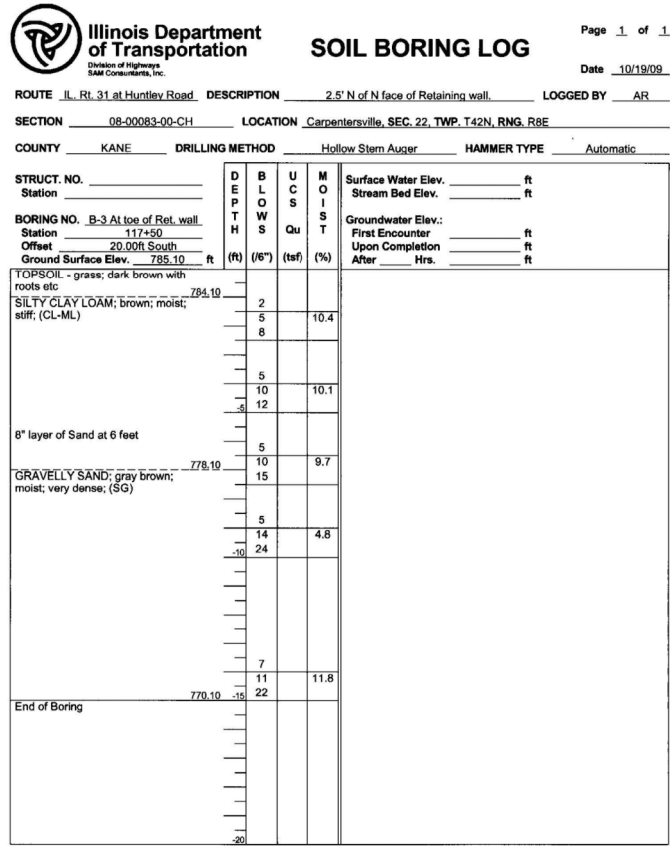
The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, from 137 (Rev. 8-99)



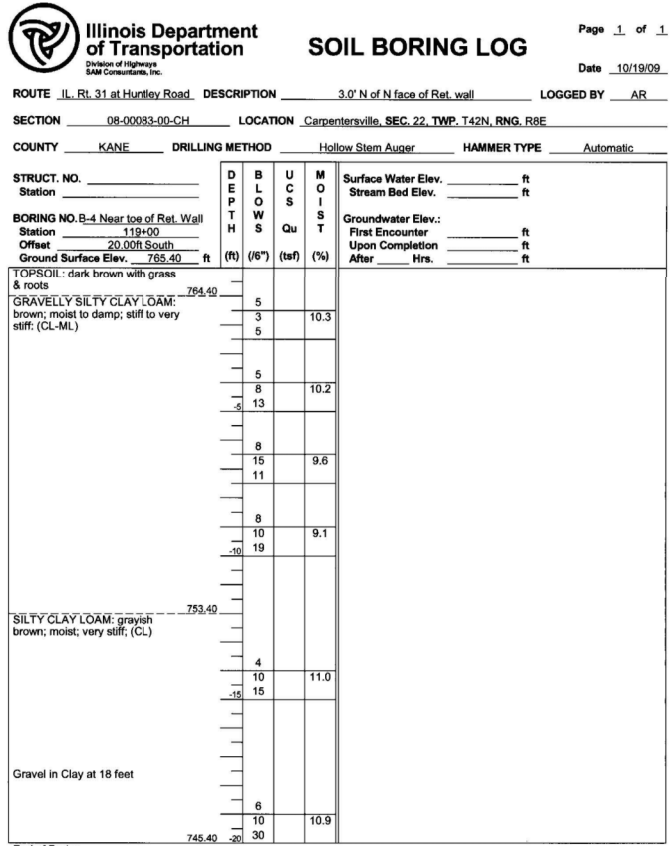
The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, from 137 (Rev. 8-99)



The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, from 137 (Rev. 8-99)



The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, from 137 (Rev. 8-99)



The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
 BBS, from 137 (Rev. 8-99)

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:50:54 AM
 FILE NAME: 8614000-str-5801-1.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOT LABEL.tbl

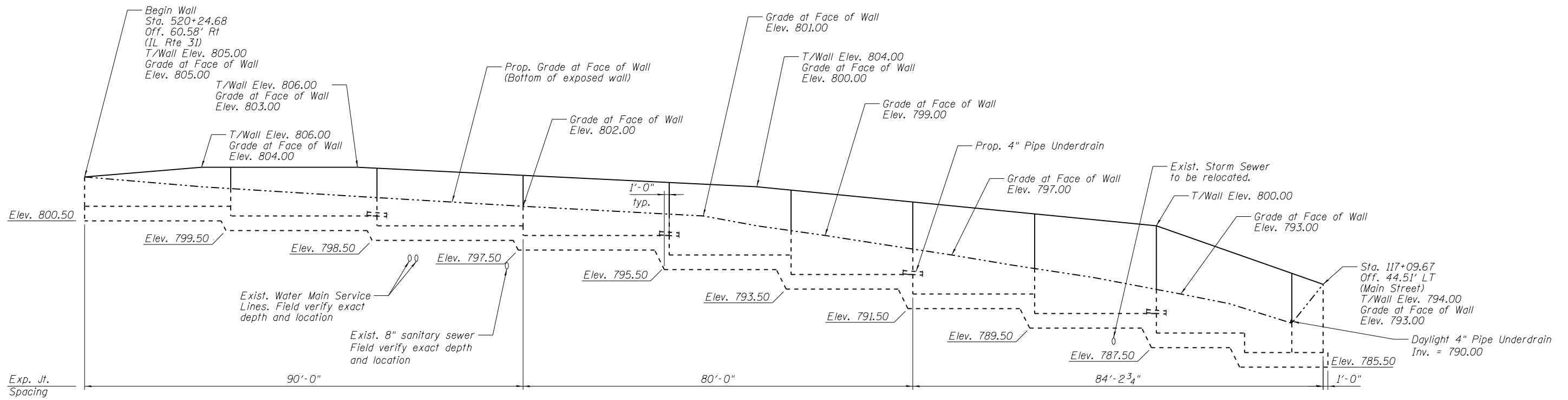


USER NAME = jmelhu	DESIGNED - TOR	REVISED -
PLOT SCALE =	DRAWN - WJH	REVISED -
PLOT DATE = 3/23/2018	CHECKED - RGD	REVISED -
	DATE - 3/23/18	REVISED -

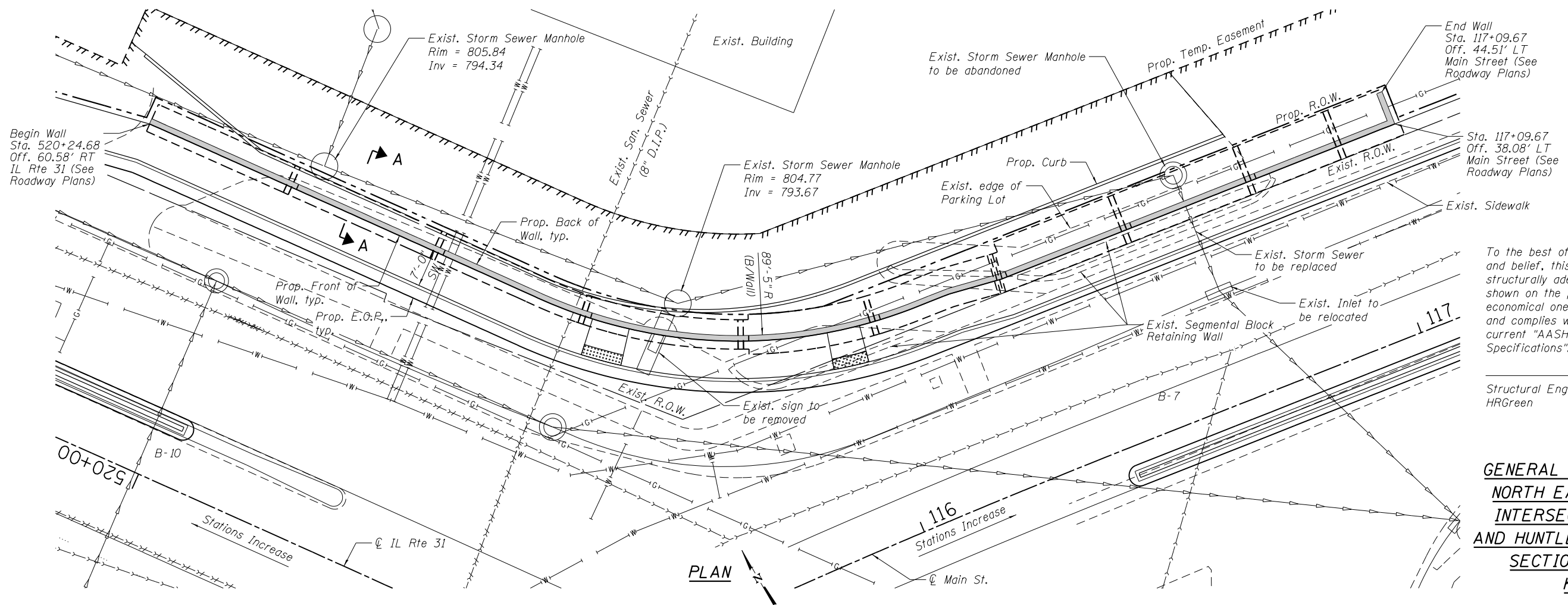
VILLAGE OF CARPENTERSVILLE

SCALE:		SHEET NO. SCW-4 OF SCW-4 SHEETS	STA. TO STA.
--------	--	---------------------------------	--------------

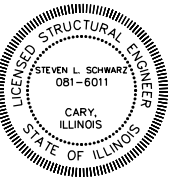
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	182
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



ELEVATION



PLAN



To the best of my knowledge, information and belief, this retaining wall design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current "AASHTO LFRD Bridge Design Specifications".

Structural Engineer Expires: 11/30/2018
HRGreen

**GENERAL PLAN AND ELEVATION
NORTH EAST RETAINING WALL
INTERSECTION OF IL. RTE 31
AND HUNTLEY ROAD/MAIN STREET
SECTION 08-00083-00-CH
KANE COUNTY**

COMPANY NAME: HRGreen.com
PROJECT CONTACT: #9011111111
DATE PLOTTED: 3/23/2018 11:59:03 AM
FILE NAME: 86140100-str-08083.dgn
PLOT DRIVER: ILpdf.plt
PLOT TABLE: PLOT LABEL.tbl



USER NAME = jmelhu	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

SCALE: SHEET NO. SNE-1 OF SNE-9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	183
CONTRACT NO. 61E73			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	

BENCHMARK

- BM1: Cut X on west bolt of fire hydrant located on south side of Huntley Road and approximately 875' west of the centerline of IL Rt. 31. Elevation=815.43 NAVD88.
- BM2: Chiseled square on north side of concrete light pole base located on the east side of IL Rt. 31 and approximately 600' southerly of the centerline of Main Street. Elevation=797.30 NAVD88.

GENERAL NOTES

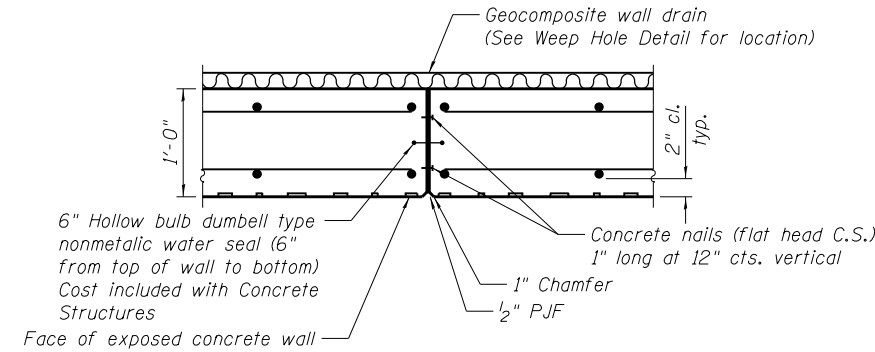
- Removal and disposal of existing segmental block retaining walls are included in the unit price for Structure Excavation.
- Plan dimensions of retaining wall measured at front face of wall.
- Reinforcement bars designated (E) shall be epoxy coated. All longitudinal bars are to be field bent.
- Protective Coat shall be applied to the top face and the front face (full height) of the wall over its entire length. See Article 503.19 of the Standard Specifications.
- Backfill shall be placed behind the wall after concrete has reached it's specified compressive strength, or 14 days, whichever is longer. See Article 502.10 of the Standard Specifications.
- FORM LINER TEXTURED SURFACE**
A form liner pattern (below the 4" coping) shall be applied to the walls, which shall be Ledgestone by Karlson Forming Specialties, Inc.
The form liner pattern for the edges of the 4" coping shall be Stone Texture also by Karlson Forming Specialties, Inc.
The top surface of the 4" coping shall be finished with a roughened texture, to simulate natural stone.
- COLORATION**
All areas receiving Form Liner Textured Surface shall receive concrete stain, including coping on top of the retaining walls. Final concrete surface shall be provided and applied to duplicate the effect of a random stone wall with individual stones of various colors ranging from four shades of gray and tan.

TOTAL BILL OF MATERIAL

Item	Unit	Total
Porous Granular Backfill	Cu Yd	210
Structure Excavation	Cu Yd	1,700
Concrete Structures	Cu Yd	175.5
Form Liner Textured Surface	Sq Ft	1,125
Protective Coat	Sq Yd	235
Reinforcement Bars, Epoxy Coated	Pound	22,410
Geocomposite Wall Drain	Sq Yd	140
Pipe Underdrains for Structures 4"	Foot	275

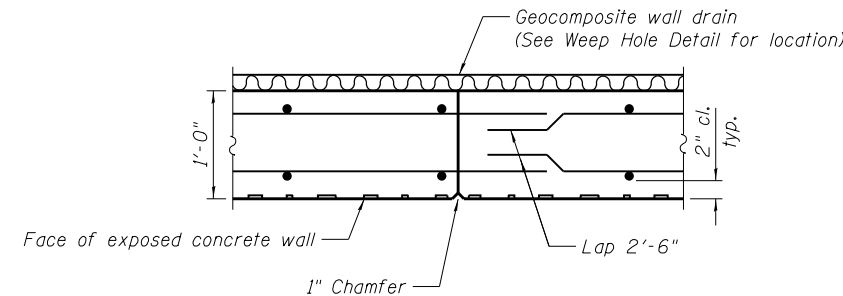
INDEX OF SHEETS

- SNE-1. General Plan and Elevation North East Retaining Wall
 SNE-2. NE Corner Retaining Wall General Details
 SNE-3. NE Corner Retaining Wall Elevation and Footing Plan
 SNE-4. NE Corner Retaining Wall Elevation and Footing Plan
 SNE-5. NE Corner Retaining Wall Elevation and Footing Plan
 SNE-6. NE Corner Retaining Wall Elevation and Footing Plan
 SNE-7. NE Corner Retaining Wall Typical Sections and Details
 SNE-8. NE Corner Retaining Wall Typical Sections and Details
 SNE-9. NE Corner Retaining Wall Soil Boring Logs

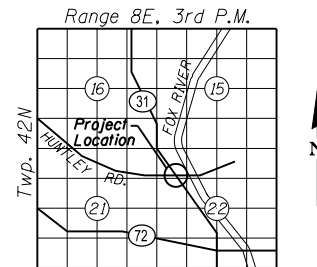


EXPANSION JOINT

Cost included with Concrete Structures



CONSTRUCTION JOINT DETAIL



LOCATION SKETCH

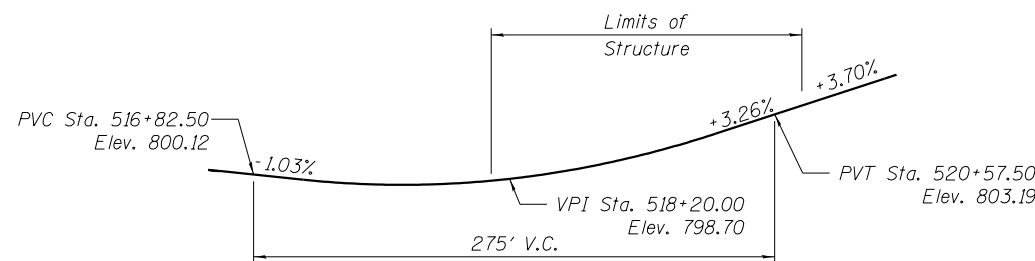
DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition

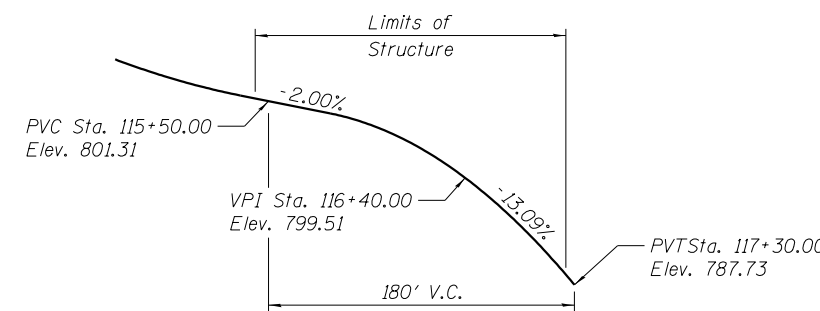
DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
 fy = 60,000 psi (Reinforcement)



PROFILE GRADE RTE 31



PROFILE GRADE HUNTLEY RD/ MAIN ST.

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 DATE PLOTTED: 3/23/2018 11:56:06 AM
 FILE NAME: 86140100-str-0a002.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOTLABEL.tbl



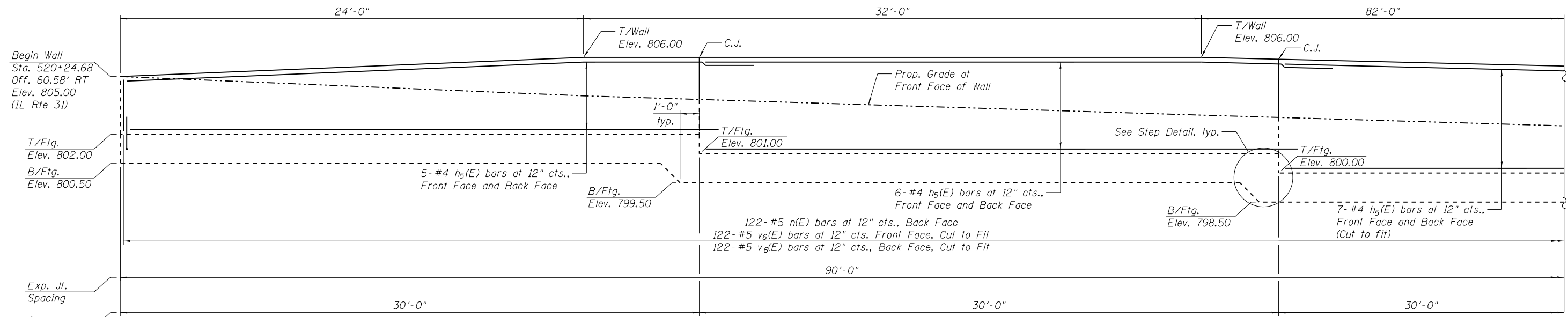
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

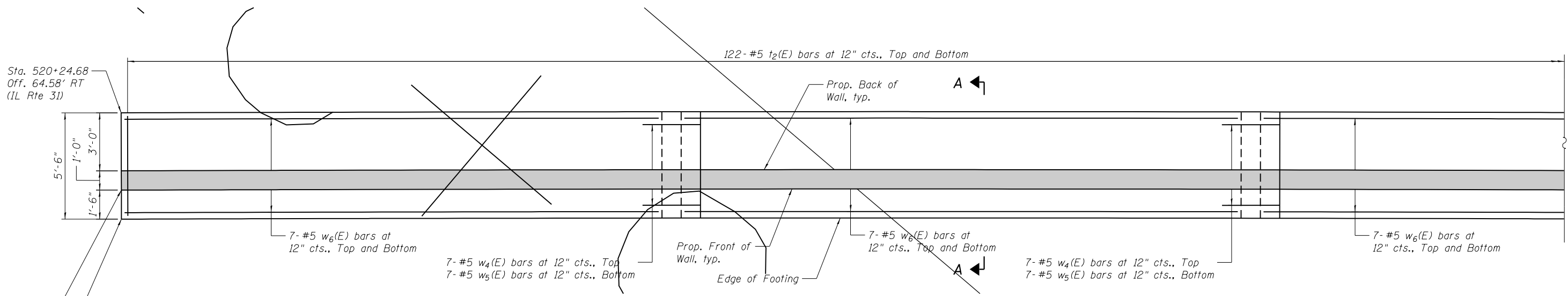
NE CORNER RETAINING WALL GENERAL DETAILS
ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET

SCALE: SHEET NO. SNE-2 OF SNE-9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	184
CONTRACT NO.			61E73	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



ELEVATION



FOOTING PLAN

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 DATE PLOTTED: 3/23/2018 10:50:09 AM
 FILE NAME: 86140100-str-Ft02-1.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOT LABEL.tbl



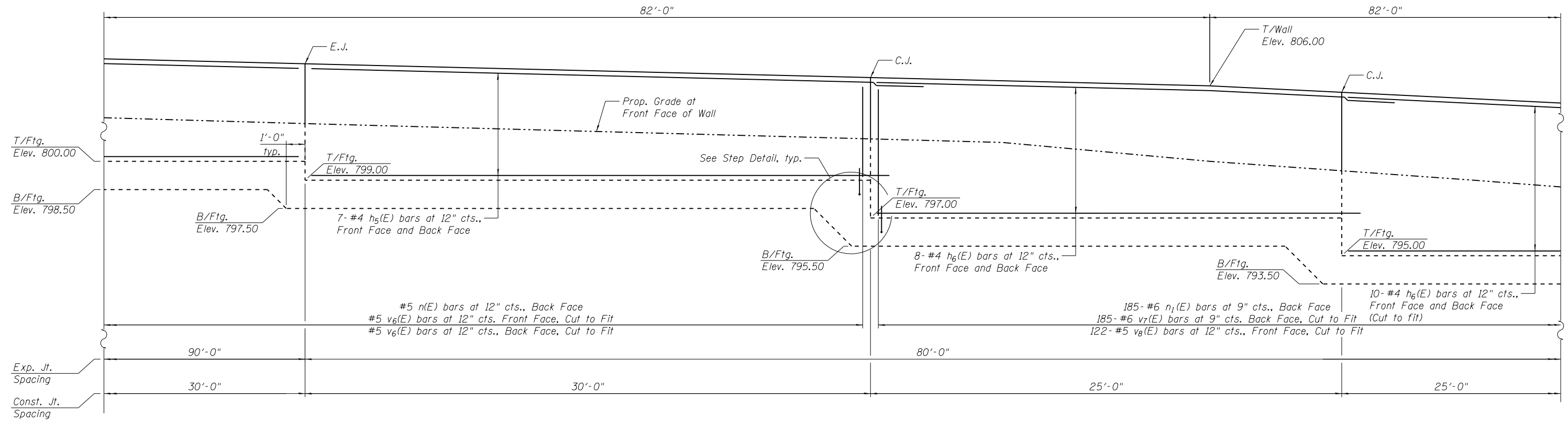
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

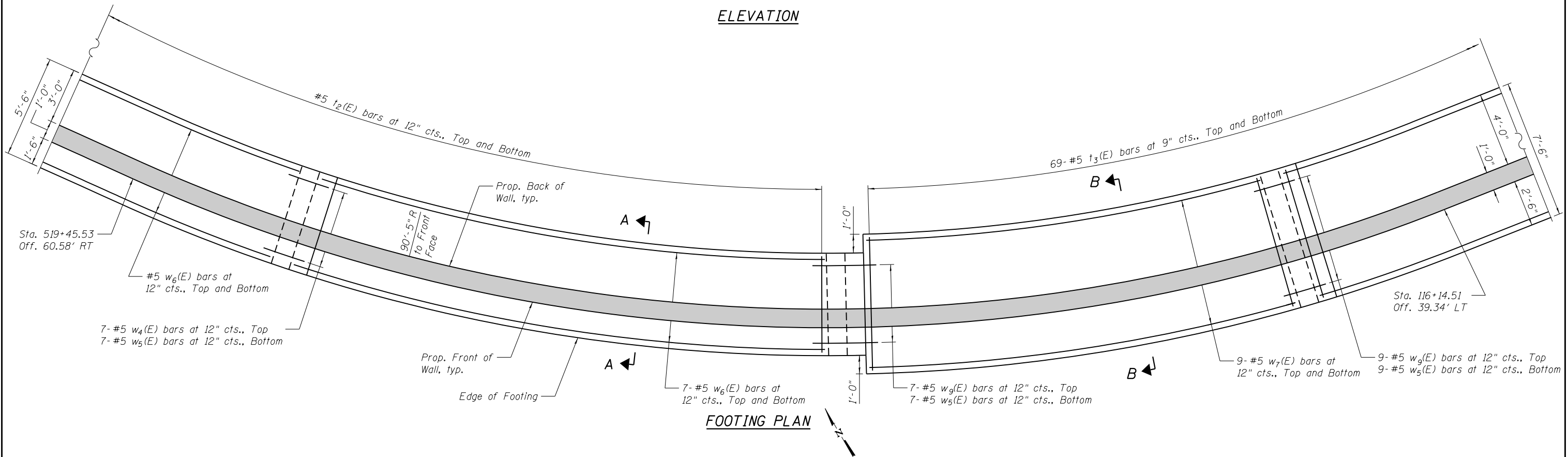
**NE CORNER RETAINING WALL ELEVATION AND FOOTING PLAN
 ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SNE-3 OF SNE-9 SHEETS STA. TO STA.

F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 185
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	



ELEVATION



FOOTING PLAN

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 10:51:12 AM
 FILE NAME: 86140100-str-FR02-2.dgn
 PLOT DRIVER: ILpdf.dwt
 PLOT TABLE: PLOT LABEL.tbl



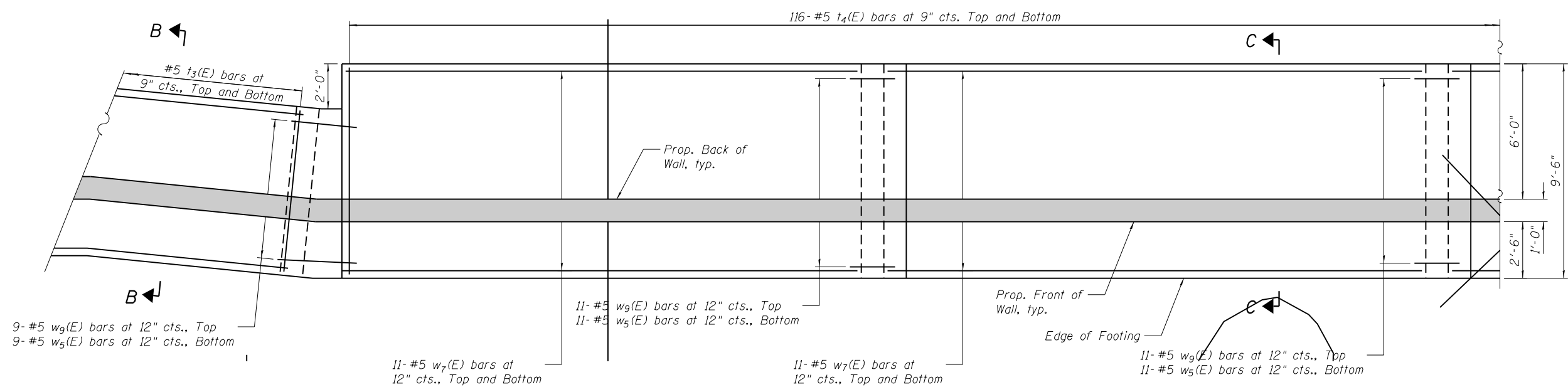
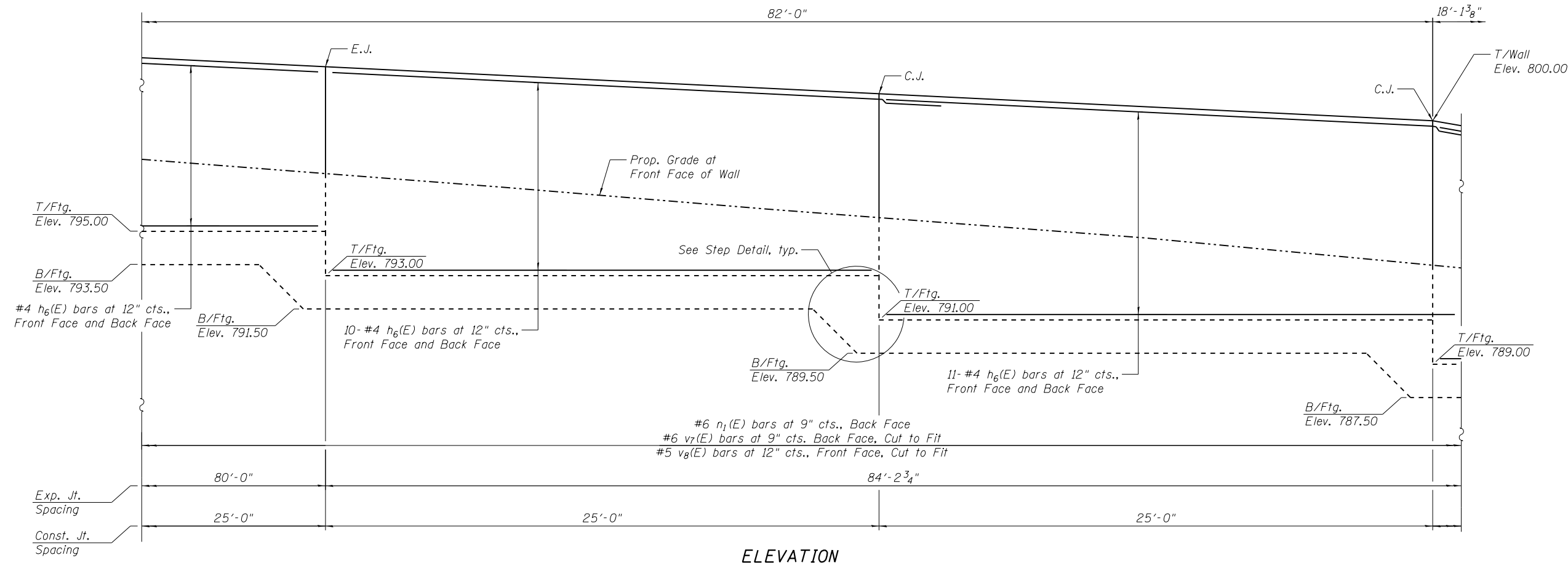
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

**NE CORNER RETAINING WALL ELEVATION AND FOOTING PLAN
 ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SNE-4 OF SNE-9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	186
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:51:14 AM
 FILE NAME: 86140100-str-Ft02-1.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOT LABEL.tbl



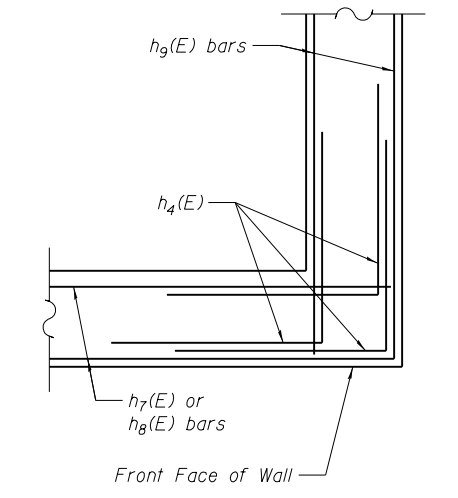
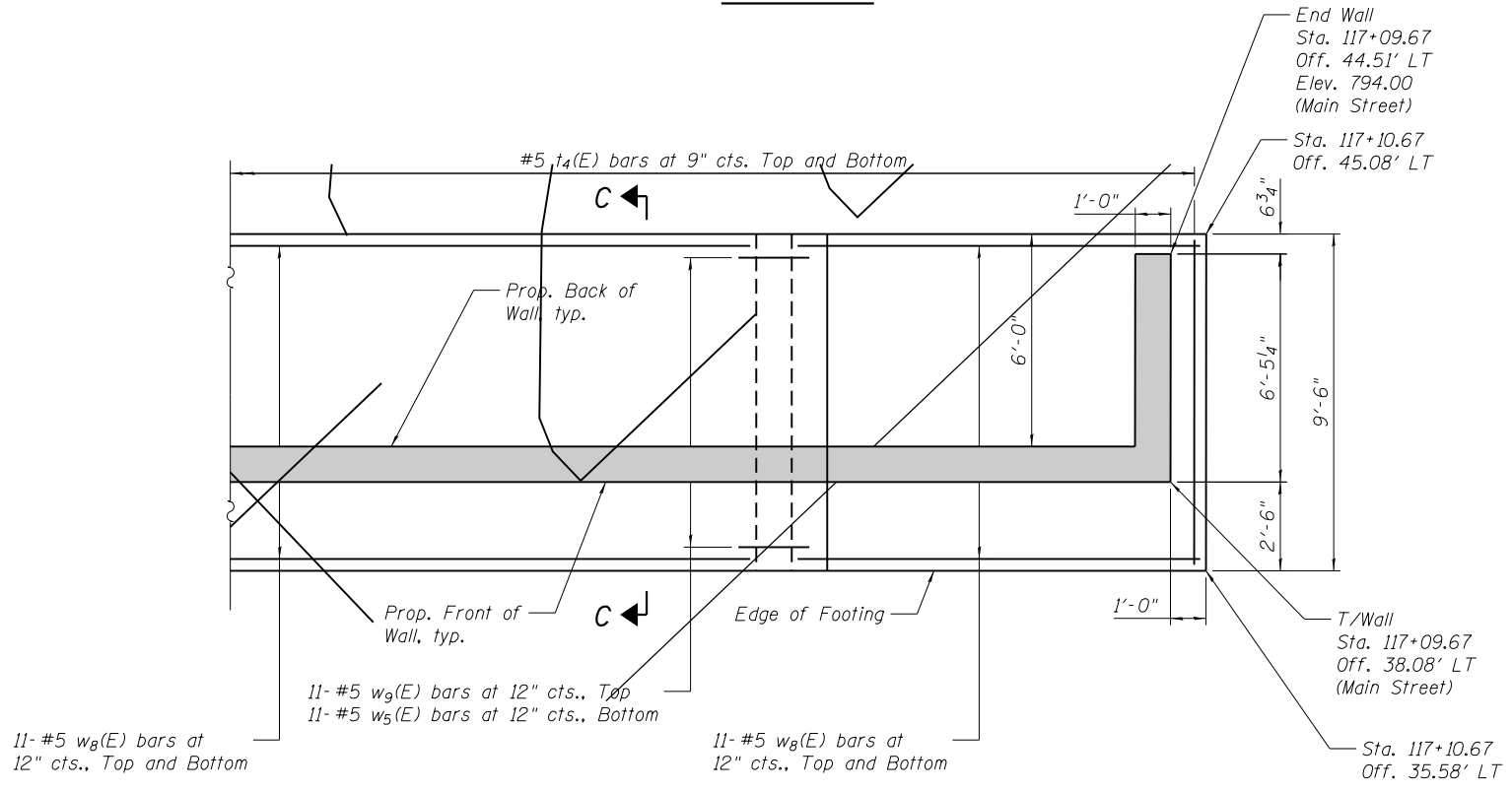
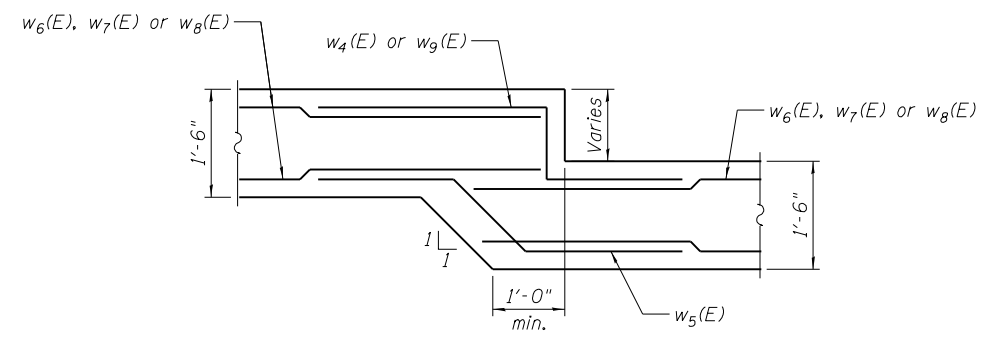
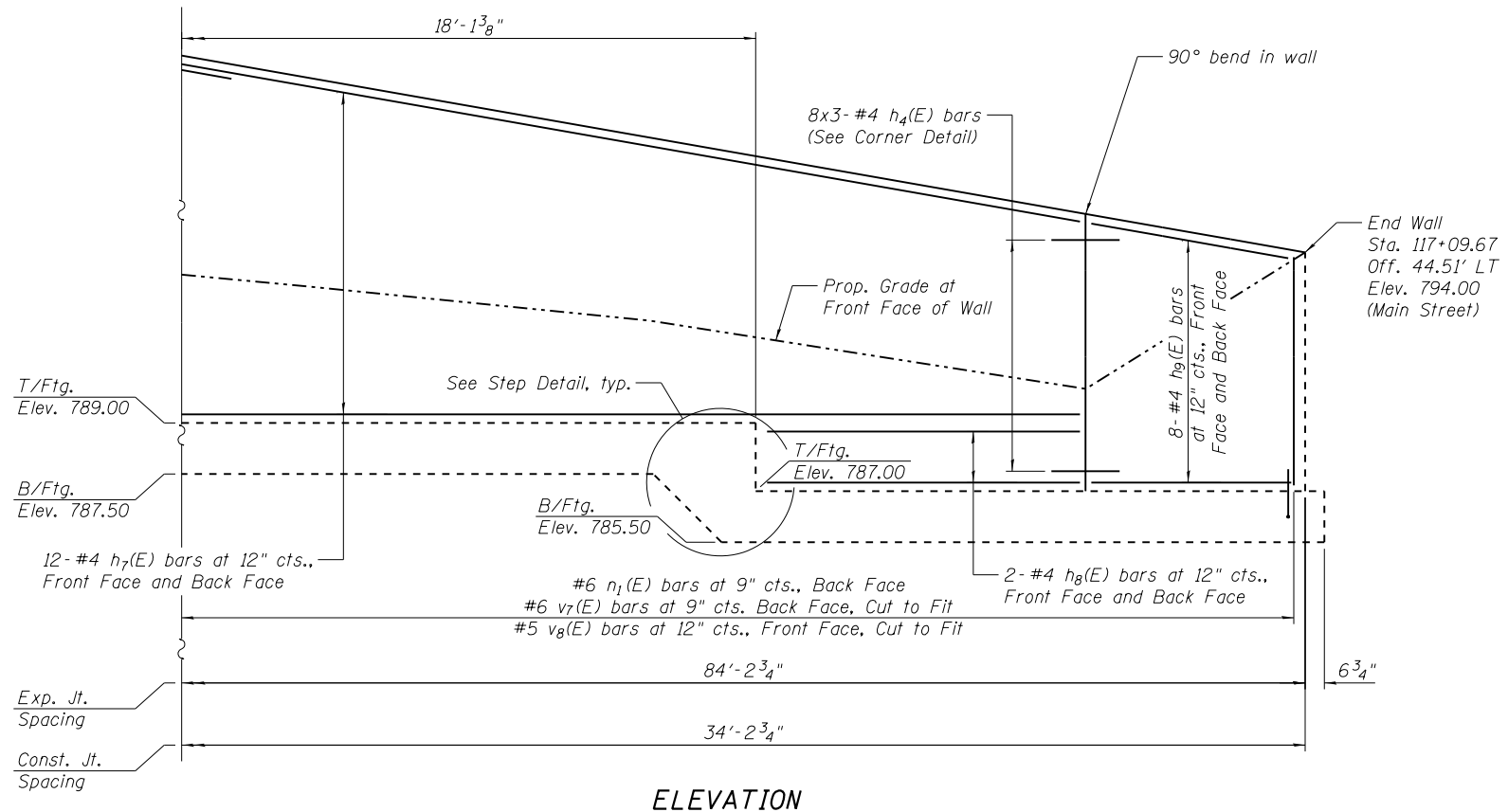
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

NE CORNER RETAINING WALL ELEVATION AND FOOTING PLAN
ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET

F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 187
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

SCALE: SHEET NO. SNE-5 OF SNE-9 SHEETS STA. TO STA.



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:51:17 AM
 FILE NAME: 86140100-str-Ft02-4.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOT LABEL.tbl



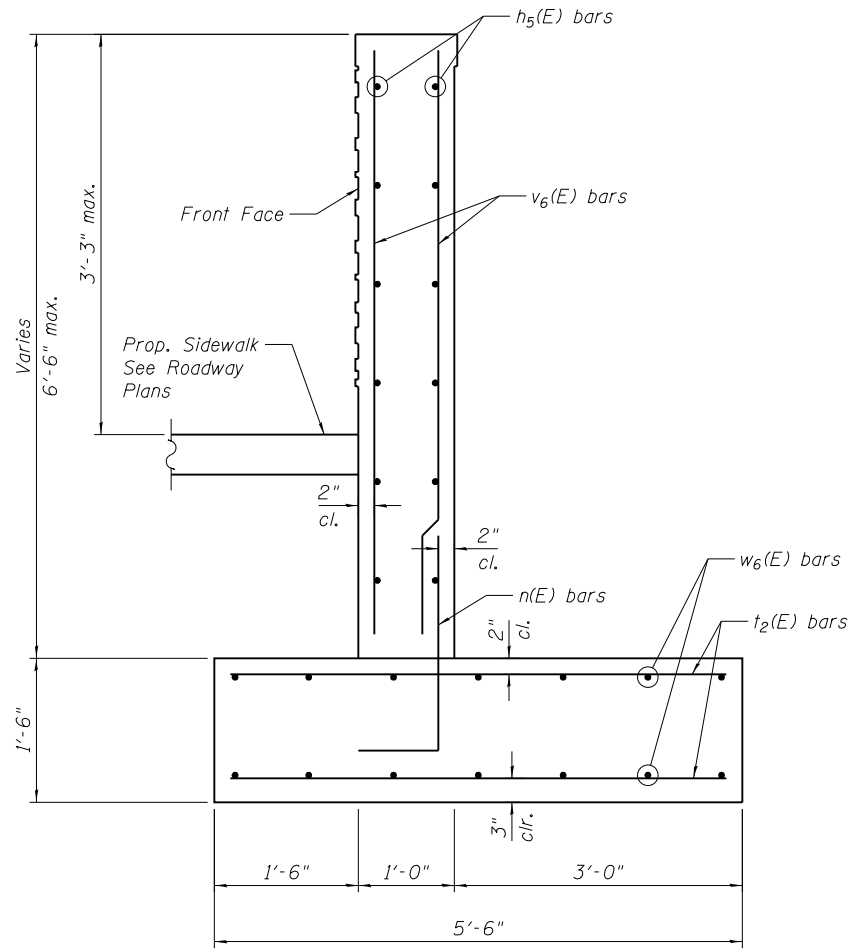
USER NAME = jmelhu	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

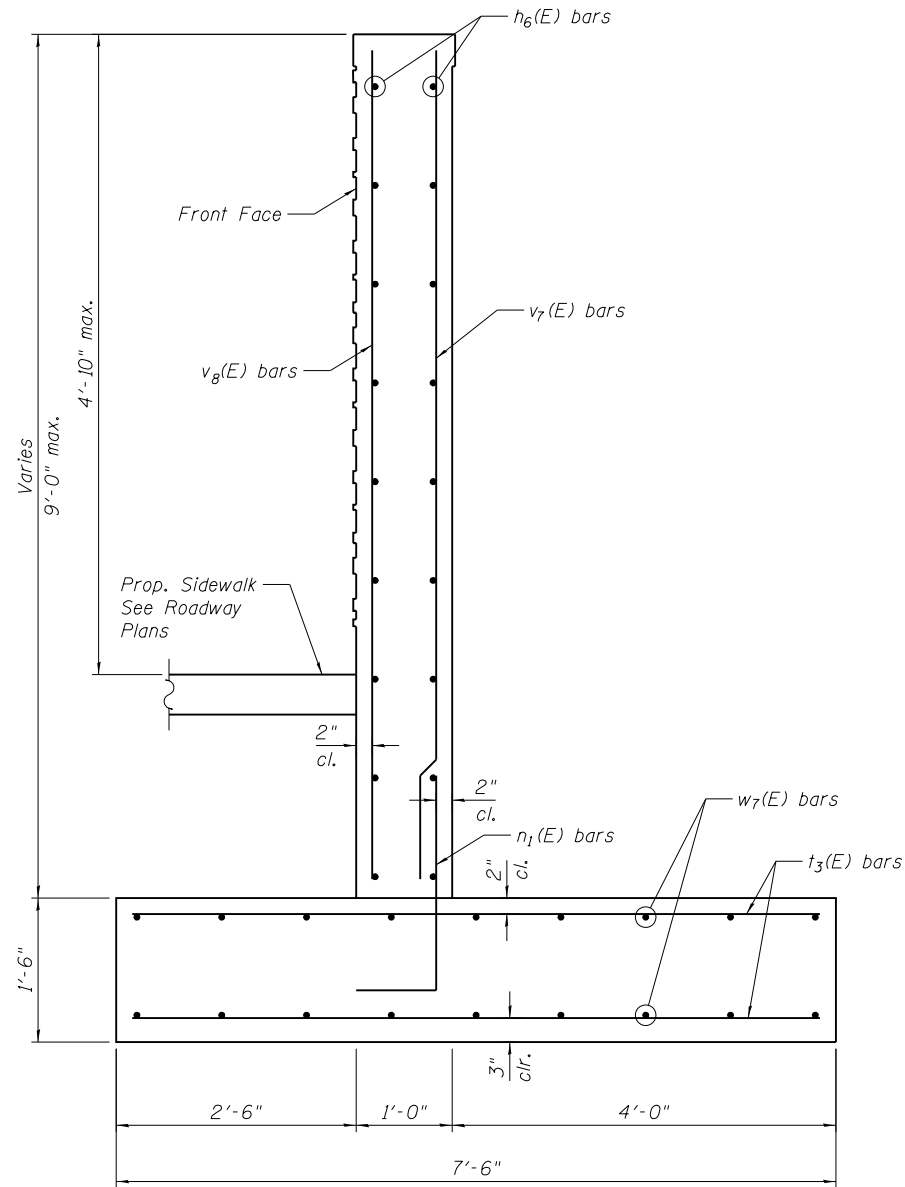
**NE CORNER RETAINING WALL ELEVATION AND FOOTING PLAN
 ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SNE-6 OF SNE-9 SHEETS STA. TO STA.

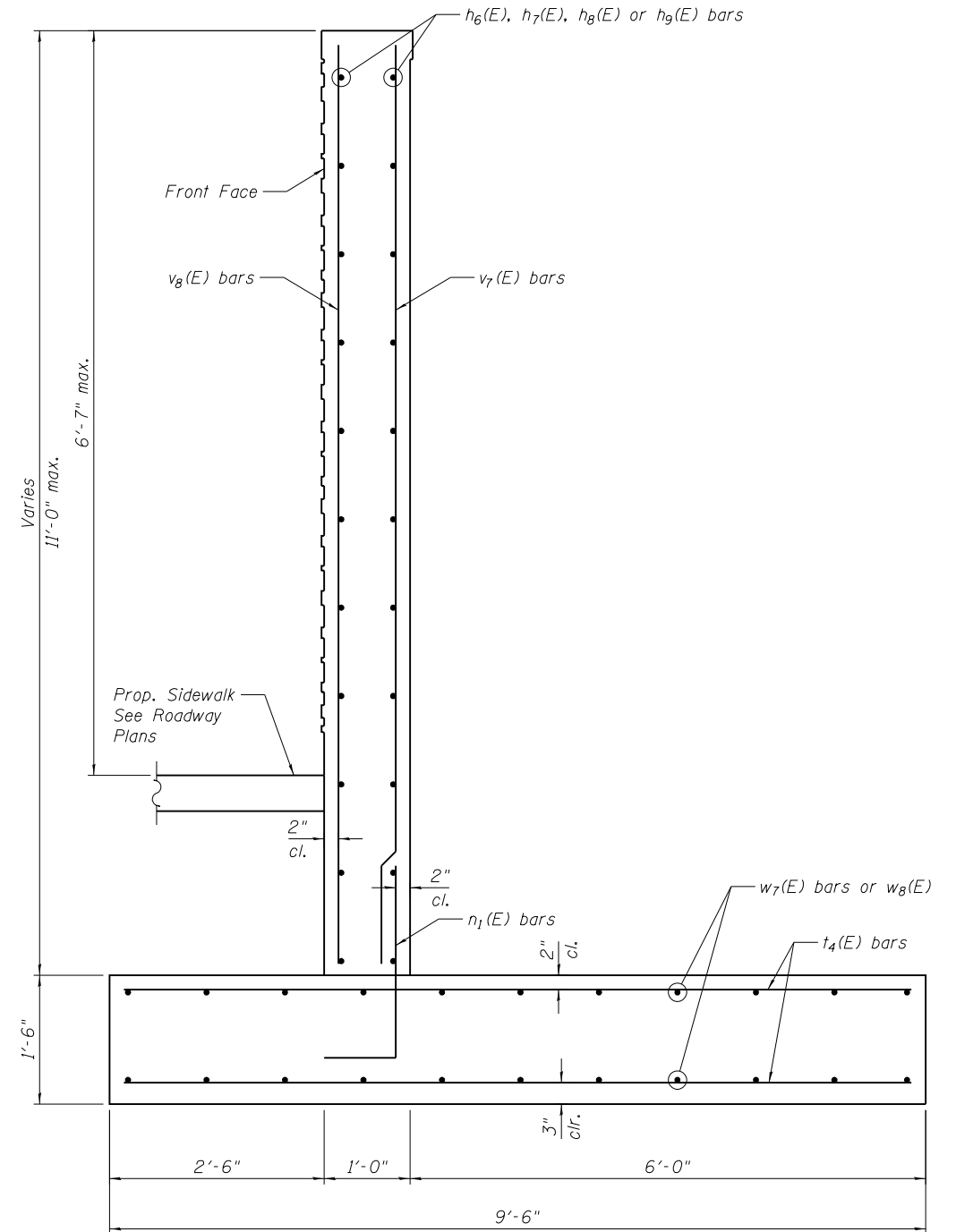
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	188
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	



SECTION A-A



SECTION B-B



SECTION C-C

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:51:19 AM
 FILE NAME: 86140100-str-0a102-1.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOT LABEL.tbl



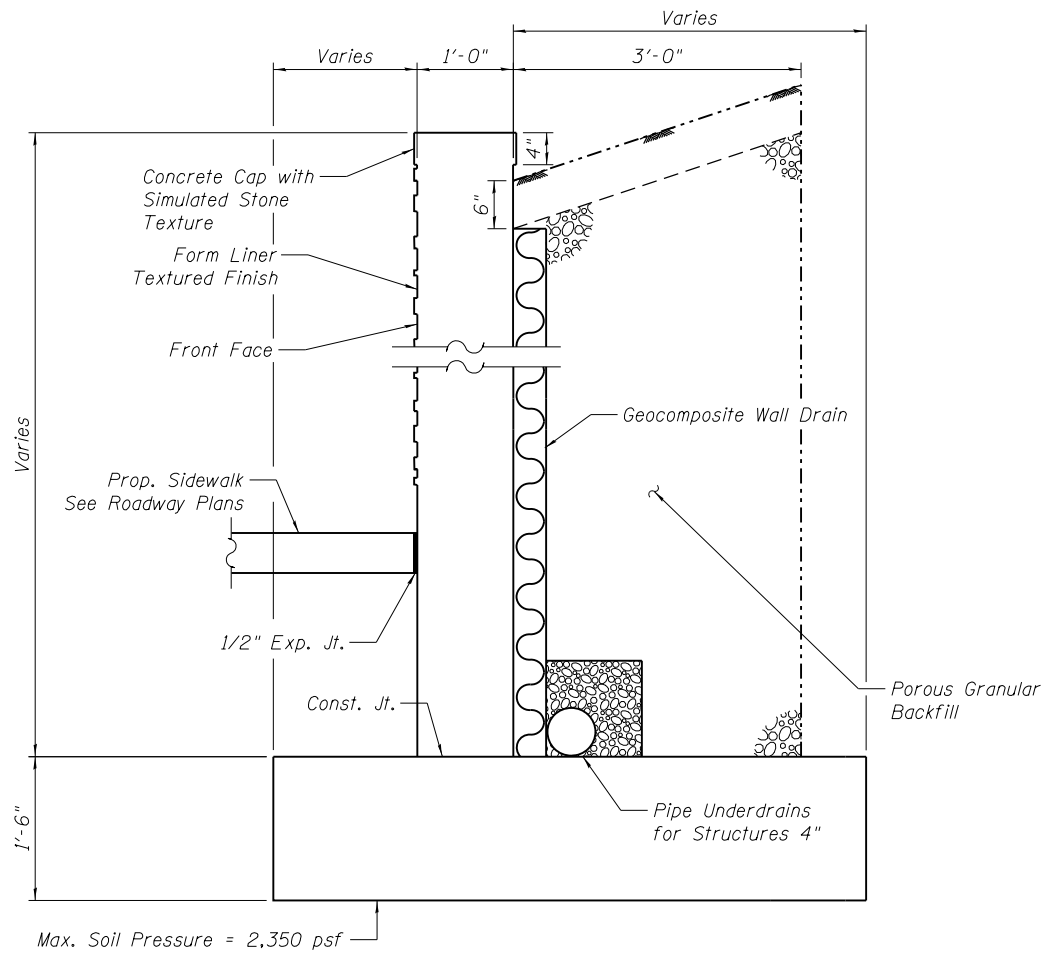
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

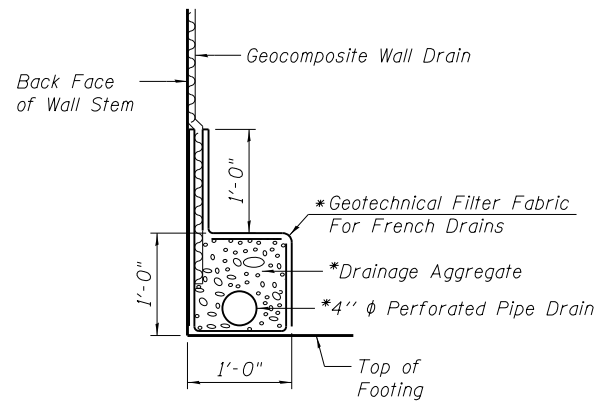
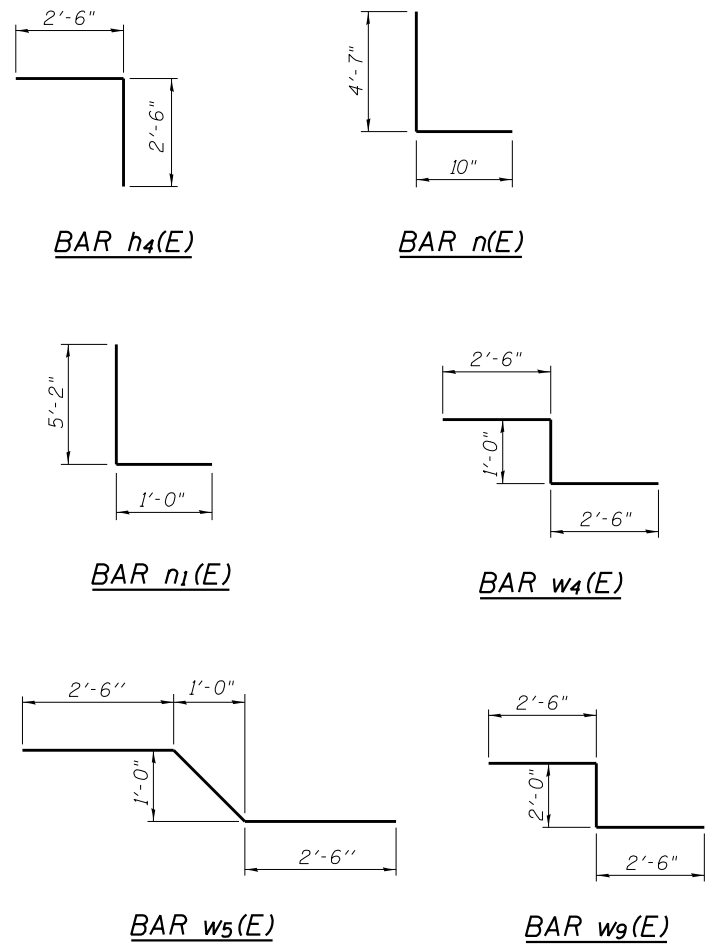
**NE CORNER RETAINING WALL TYPICAL SECTIONS AND DETAILS
 ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SNE-7 OF SNE-9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	189
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



TYPICAL WALL SECTION



PIPE UNDERDRAIN DETAIL

- * Included in the cost of Pipe Underdrains for Structures.
- ** Backfill remainder of structure excavation and over excavation with same material specified for roadway embankment.

BILL OF MATERIALS

Bar	No.	Size	Length	Shape
h ₄ (E)	24	#4	5'- 0"	└
h ₅ (E)	50	#4	32'- 8"	—
h ₆ (E)	78	#4	27'- 7"	—
h ₇ (E)	24	#4	27'- 5"	—
h ₈ (E)	4	#4	9'- 4"	—
h ₉ (E)	16	#4	6'- 1"	—
n(E)	122	#5	5'- 5"	└
n ₁ (E)	185	#6	6'- 2"	└
t ₂ (E)	244	#5	6'- 0"	—
t ₃ (E)	138	#5	8'- 0"	—
t ₄ (E)	232	#5	10'- 0"	—
v ₆ (E)	244	#5	6'- 2"	—
v ₇ (E)	185	#6	10'- 8"	—
v ₈ (E)	122	#5	10'- 8"	—
w ₄ (E)	21	#5	6'- 0"	└
w ₅ (E)	79	#5	6'- 5"	└
w ₆ (E)	56	#5	30'- 8"	—
w ₇ (E)	80	#5	25'- 8"	—
w ₈ (E)	44	#5	18'- 9"	—
w ₉ (E)	58	#5	7'- 0"	└
Porous Granular Backfill			Cu Yd	210
Structure Excavation			Cu Yd	1,700
Concrete Structures			Cu Yd	175.5
Form Liner Textured Surface			Sq Ft	1,125
Reinforcement Bars, Epoxy Coated			Pound	22,410
Geocomposite Wall Drain			Sq Yd	140

Bars indicated thus 1 x 2-#8 etc. indicates 1 line of bars with 2 lengths per line.

MINIMUM BAR LAP

- #5 bar = 3'-3"
- #6 bar = 3'-10"

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:51:23 AM
 FILE NAME: 86140100-str-0a-102-2.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOT LABEL.tbl



USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

**NE CORNER RETAINING WALL TYPICAL SECTIONS AND DETAILS
ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SNE-8 OF SNE-9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	190
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 61E73	

ROUTE IL Rt. 31 at Huntley Road DESCRIPTION In roadway in N half LOGGED BY AR
 SECTION 08-00083-00-CH LOCATION Carpentersville, SEC. 22, TWP. T42N, RNG. R8E
 COUNTY KANE DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO. Station	D E P T H	B L O W S	U C S Q _u	M O I S T	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.: First Encounter Upon Completion After
					ft	ft	
BORING NO. B-7 In Pavement Station 116+50 Offset 12.00ft North Ground Surface Elev. 796.50							
8" Concrete Pavement					795.83		
FILL - made with gray and brown Sandy Silty Clay and Gravel	9						
	11			4.7			
	12						
	3						
	3			13.4			
	4						
791.50							
SANDY GRAVEL; brown; some 1/4" size stones present; damp; dense to very dense, (GP)	9						
	10			2.1			
	11						
	24						
	30			2.6			
	33						
786.50							
End of Boring							

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)

ROUTE IL Rt. 31 at Huntley Road DESCRIPTION In IL 31 in the East half LOGGED BY AR
 SECTION 08-00083-00-CH LOCATION Carpentersville, SEC. 22, TWP. T42N, RNG. R8E
 COUNTY KANE DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO. Station	D E P T H	B L O W S	U C S Q _u	M O I S T	Surface Water Elev.	Stream Bed Elev.	Groundwater Elev.: First Encounter Upon Completion After
					ft	ft	
BORING NO. B-10 In pavement Station 520+00 Offset 10.00ft East Ground Surface Elev. 805.40							
14" Asphalt Pavement in Layers (3"x4"x3"x3")					805.20		
FILL - made with gravel and clay; damp; dense	6			4.1			
	11						
	7						
803.40							
GRAVELLY COARSE SAND; red brown; damp; loose to medium dense; (SPG)	9			4.9			
	9						
	10						
	3			5.0			
	2						
	2						
	4			5.8			
	4						
	6						
796.40							
End of Boring							

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
 The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhu
 DATE PLOTTED: 3/23/2018 1:56:25 AM
 FILE NAME: 86140100-str-5802-1.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOTLABEL.tbl



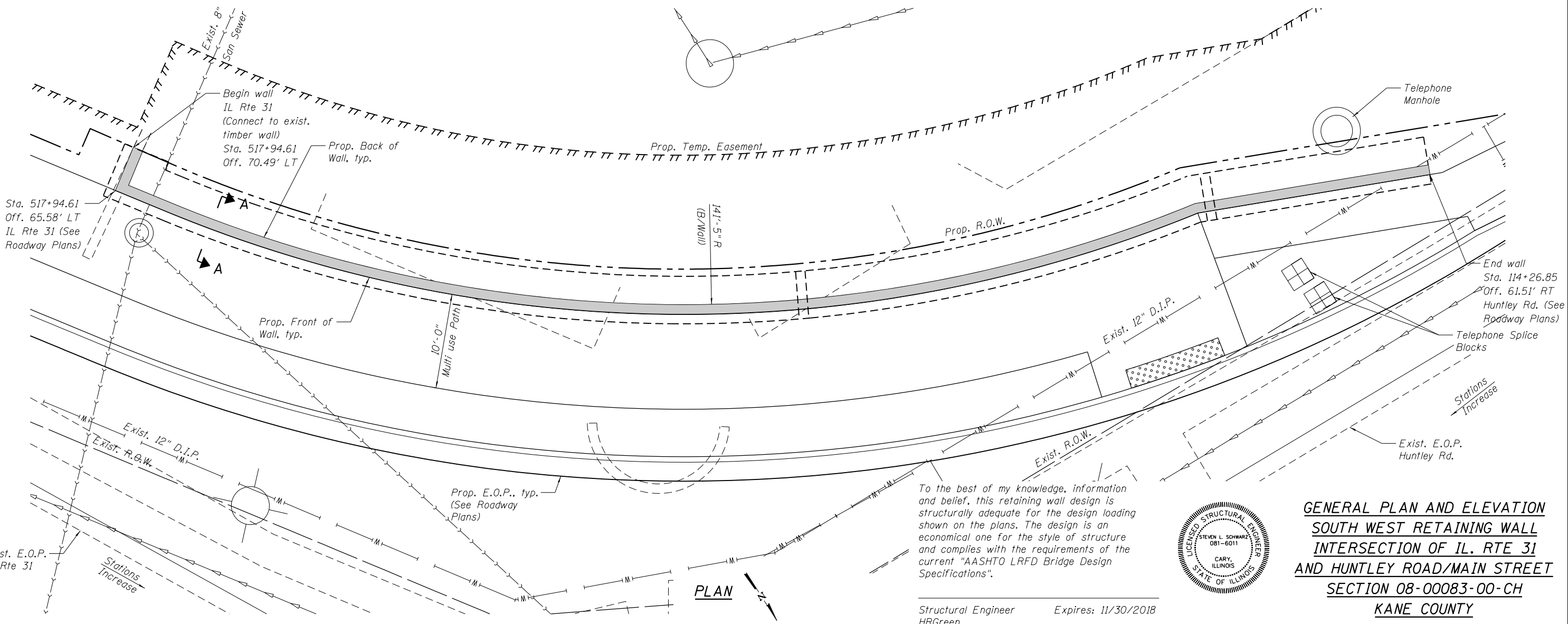
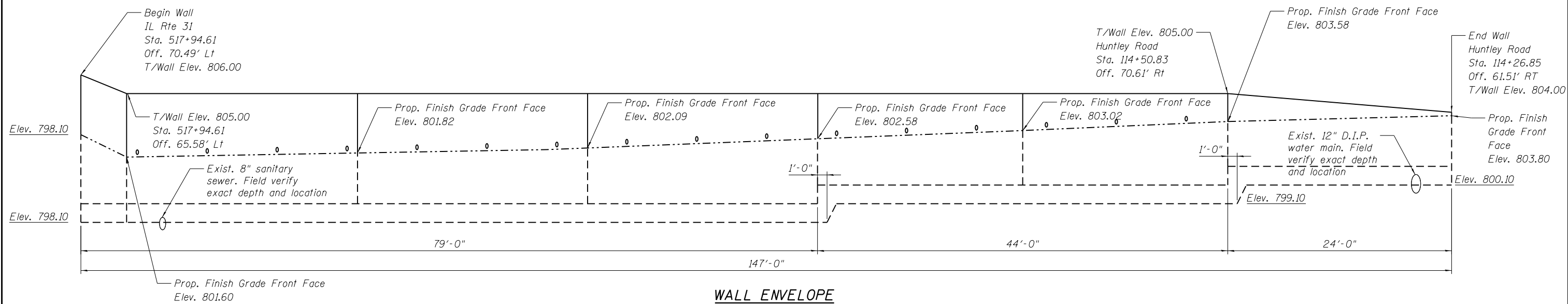
USER NAME = jmelhu	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

SOIL BORING LOGS
 ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET

SCALE: SHEET NO. SNE-9 OF SNE-9 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	191
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



**GENERAL PLAN AND ELEVATION
SOUTH WEST RETAINING WALL
INTERSECTION OF IL. RTE 31
AND HUNTLEY ROAD/MAIN STREET
SECTION 08-00083-00-CH
KANE COUNTY**

Structural Engineer Expires: 11/30/2018
HRGreen

COMPANY NAME: HRGreen.com
PROJECT CONTACT: HRGreen.com
DATE PLOTTED: 3/23/2018 10:56:31 AM
FILE NAME: 0814000-ssw-1-0814000.dwg
PLOT DRIVER: ILDef.dwg
PLOT TABLE: PLOT LABEL.tbl



USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

SCALE: SHEET NO. SSW-1 OF SSW-5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	192
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	

BENCHMARK

- BM1: Cut X on west bolt of fire hydrant located on south side of Huntley Road and approximately 875' west of the centerline of IL Rt. 31. Elevation=815.43 NAVD88.
- BM2: Chiseled square on north side of concrete light pole base located on the east side of IL Rt. 31 and approximately 600' southerly of the centerline of Main Street. Elevation=797.30. NAVD88

GENERAL NOTES

- Plan dimensions of retaining wall measured at front face of wall.
- Reinforcement bars designated (E) shall be epoxy coated.
- Protective Coat shall be applied to the top face and the front face (full height) of the wall over its entire length. See Article 503.19 of the Standard Specifications.
- Backfill shall be placed behind the wall after concrete has reached it's specified compressive strength, or 14 days, whichever is longer. See Article 502.10 of the Standard Specifications.
- FORM LINER TEXTURED SURFACE

A form liner pattern (below the 4" coping) shall be applied to the walls, which shall be Ledgestone by Karlson Forming Specialties, Inc.

The form liner pattern for the edges of the 4" coping shall be Stone Texture also by Karlson Forming Specialties, Inc.

The top surface of the 4" coping shall be finished with a roughened texture, to simulate natural stone.

- COLORATION

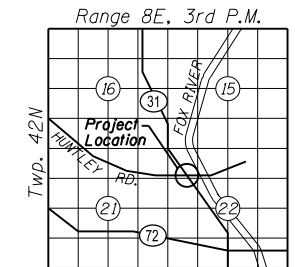
All areas receiving Form Liner Textured Surface shall receive concrete stain, including coping on top of the retaining walls. Final concrete surface shall be provided and applied to duplicate the effect of a random stone wall with individual stones of various colors ranging from four shades of gray and tan.

TOTAL BILL OF MATERIAL

Item	Unit	Total
Porous Granular Backfill	Cu Yd	80
Structure Excavation	Cu Yd	605
Concrete Structures	Cu Yd	67.3
Form Liner Textured Surface	Sq Ft	275
Protective Coat	Sq Yd	95
Reinforcement Bars, Epoxy Coated	Pound	6,730
Geocomposite Wall Drain	Sq Yd	40

INDEX OF SHEETS

- SSW-1. General Plan and Elevation South West Retaining Wall
- SSW-2. SW Corner Retaining Wall General Details
- SSW-3. SW Corner Retaining Wall Elevation and Footing Plan
- SSW-4. SW Corner Retaining Wall Elevation and Footing Plan
- SSW-5. SW Corner Retaining Wall Typical Sections and Details



LOCATION SKETCH

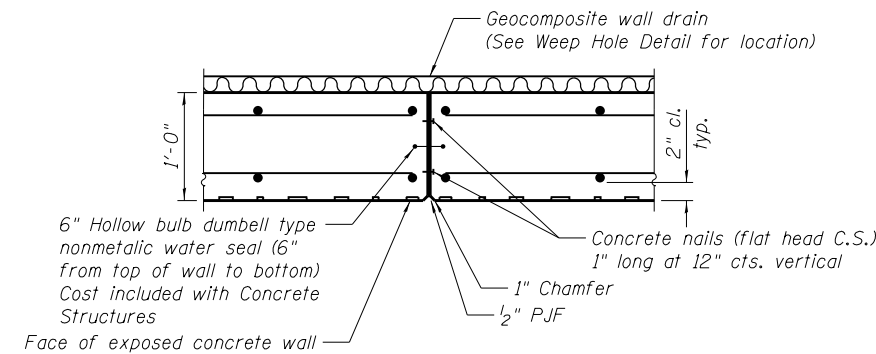
DESIGN SPECIFICATIONS

2014 AASHTO LRFD Bridge Design Specifications, 7th Edition

DESIGN STRESSES

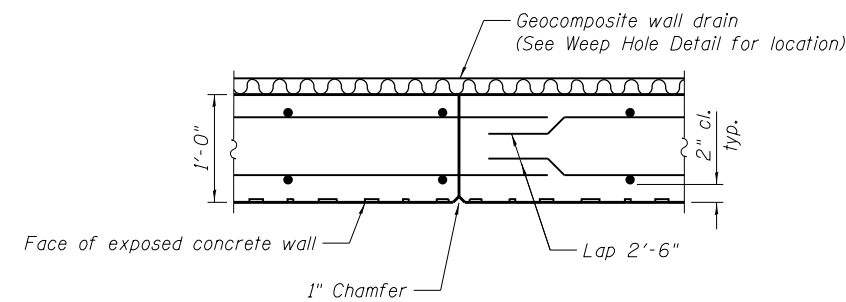
FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)

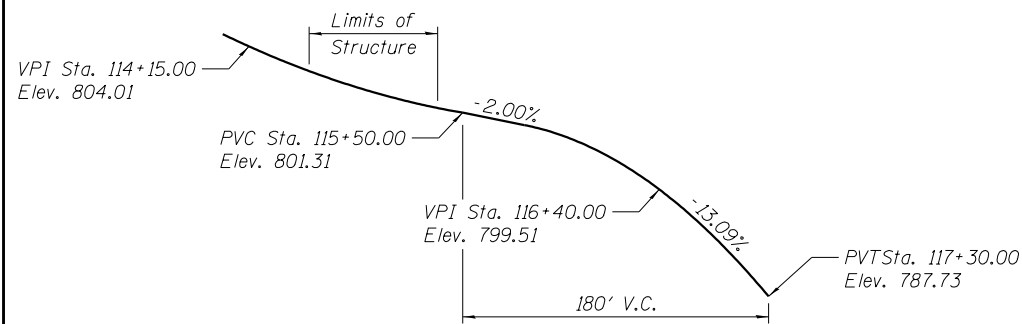


EXPANSION JOINT

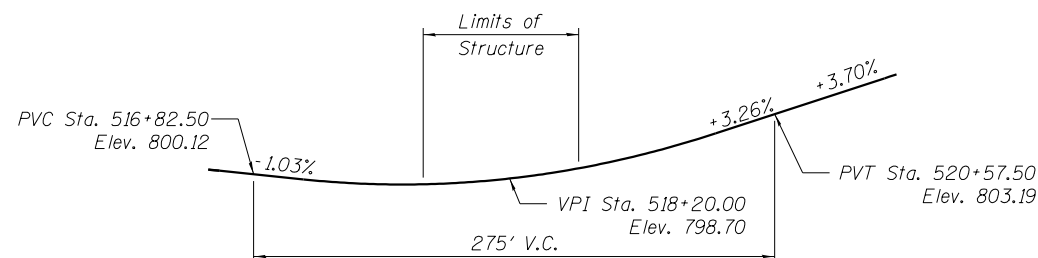
Cost included with Concrete Structures



CONSTRUCTION JOINT DETAIL



PROFILE GRADE HUNTLEY RD/ MAIN ST.



PROFILE GRADE RTE 31

COMPANY NAME: HRGreen.com
PROJECT CONTACT: #184-001322
DATE PLOTTED: 3/23/2018 11:51:33 AM
FILE NAME: 86140100-str-08003.dgn
PLOT DRIVER: ILpdf.plt
PLOT TABLE: PLOT LABEL.tbl



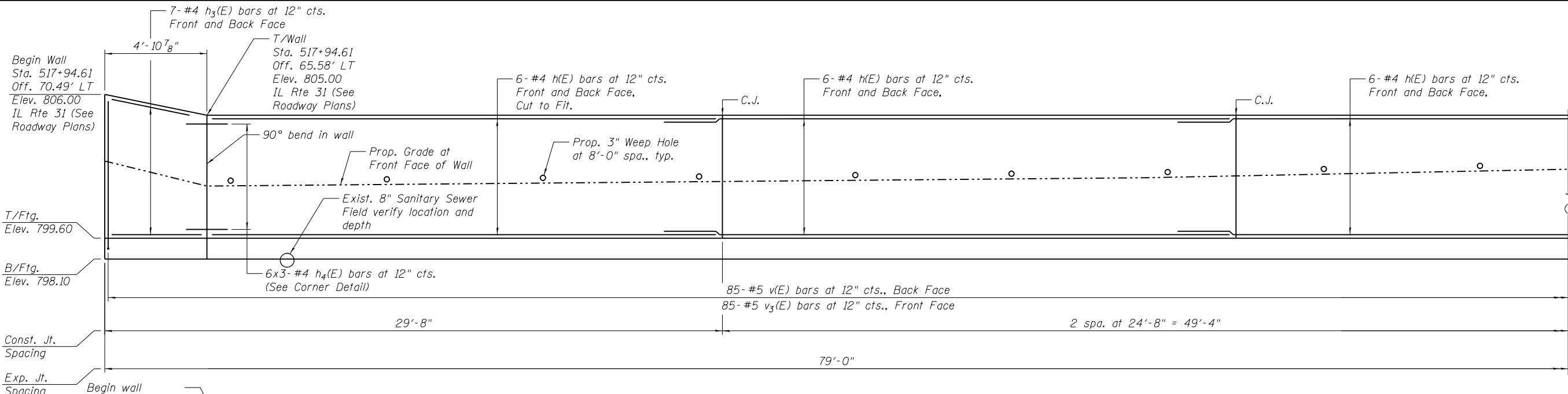
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

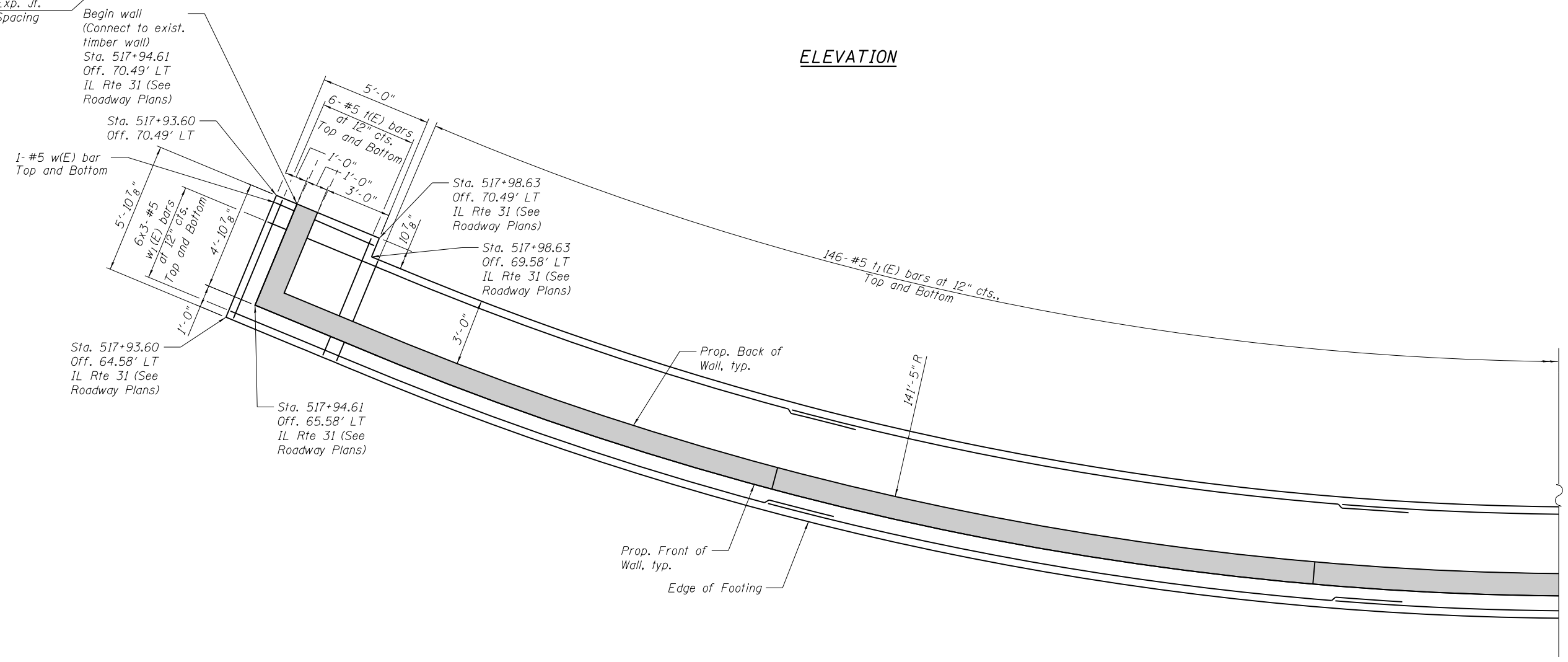
**SW CORNER RETAINING WALL GENERAL DETAILS
ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SSW-2 OF SSW-5 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	193
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	



ELEVATION



FOOTING PLAN

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 DATE PLOTTED: 3/23/2018 10:56:36 AM
 FILE NAME: 86140100-str-Fig03-1.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOT LABEL.tbl



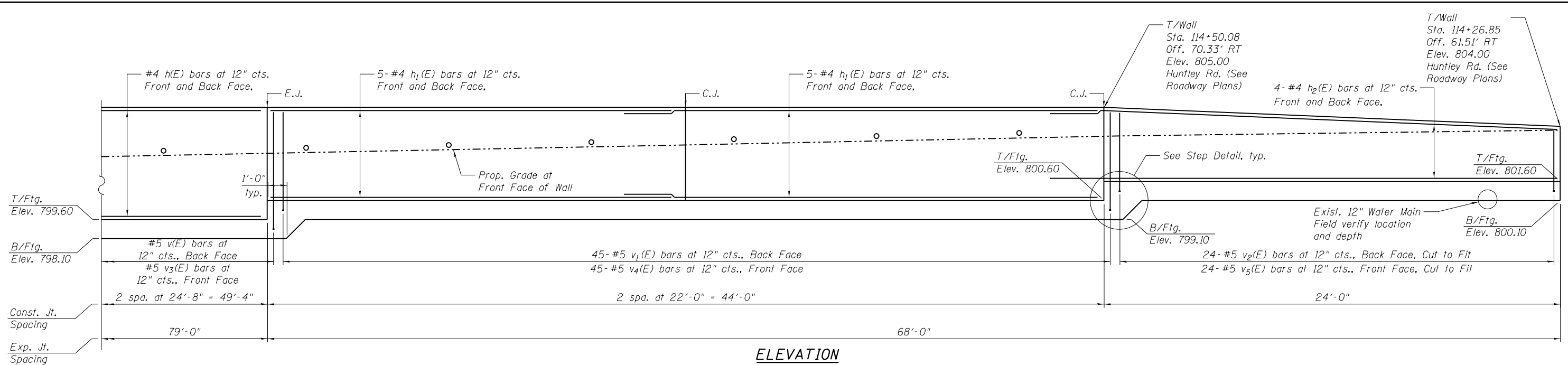
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

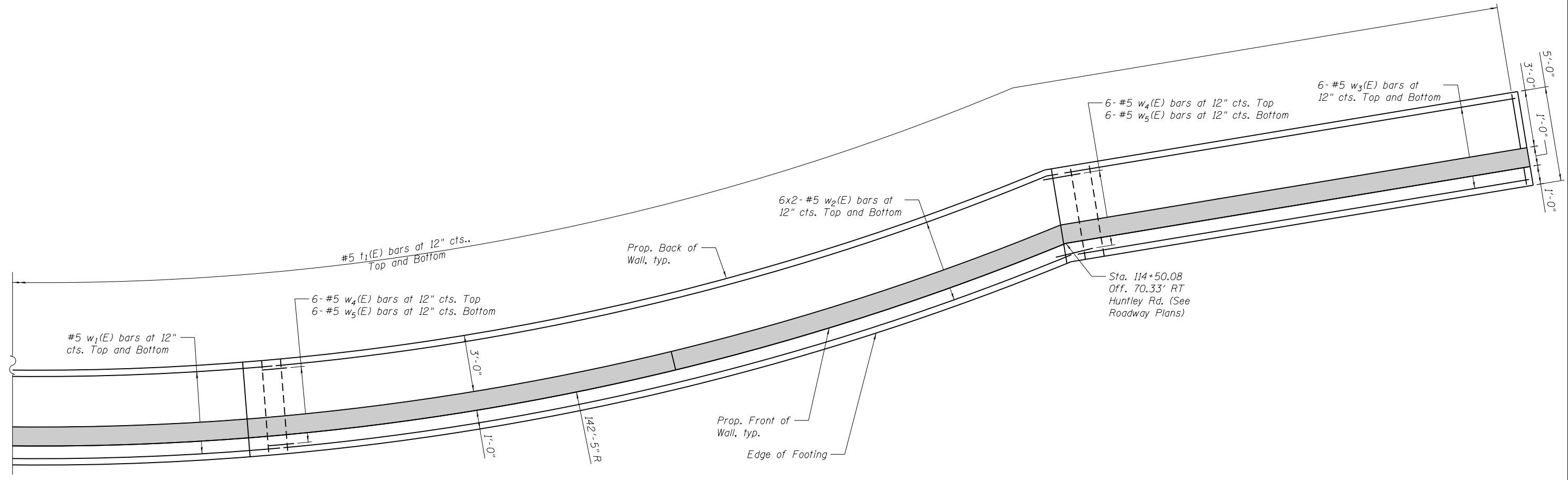
**SW CORNER RETAINING WALL ELEVATION AND FOOTING PLAN
 ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SSW-3 OF SSW-5 SHEETS STA. TO STA.

F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 194
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	



ELEVATION



FOOTING PLAN

COMPANY NAME: HRGreen.com
 PROJECT CONTACT: jmelhus
 DATE PLOTTED: 3/23/2018 1:56:39 AM
 FILE NAME: 86140100-str-F1003-2.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOT LABEL.tbl



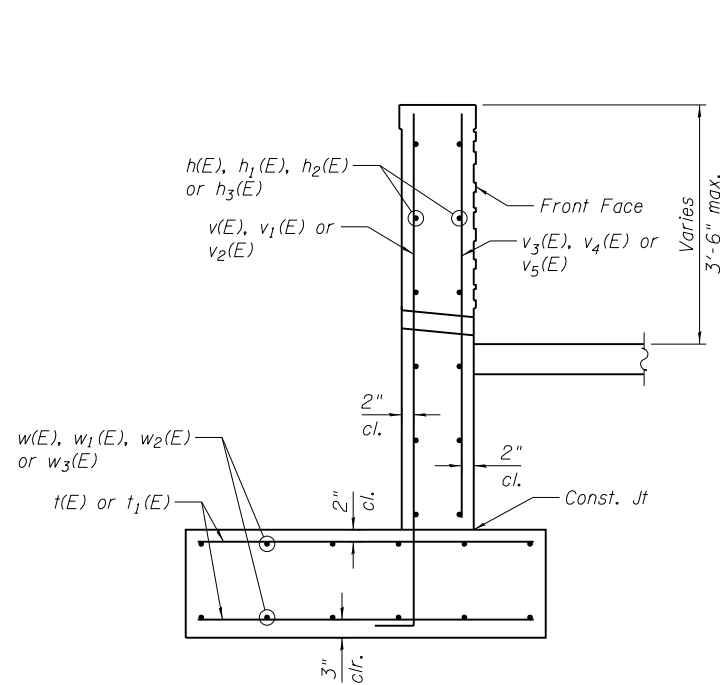
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

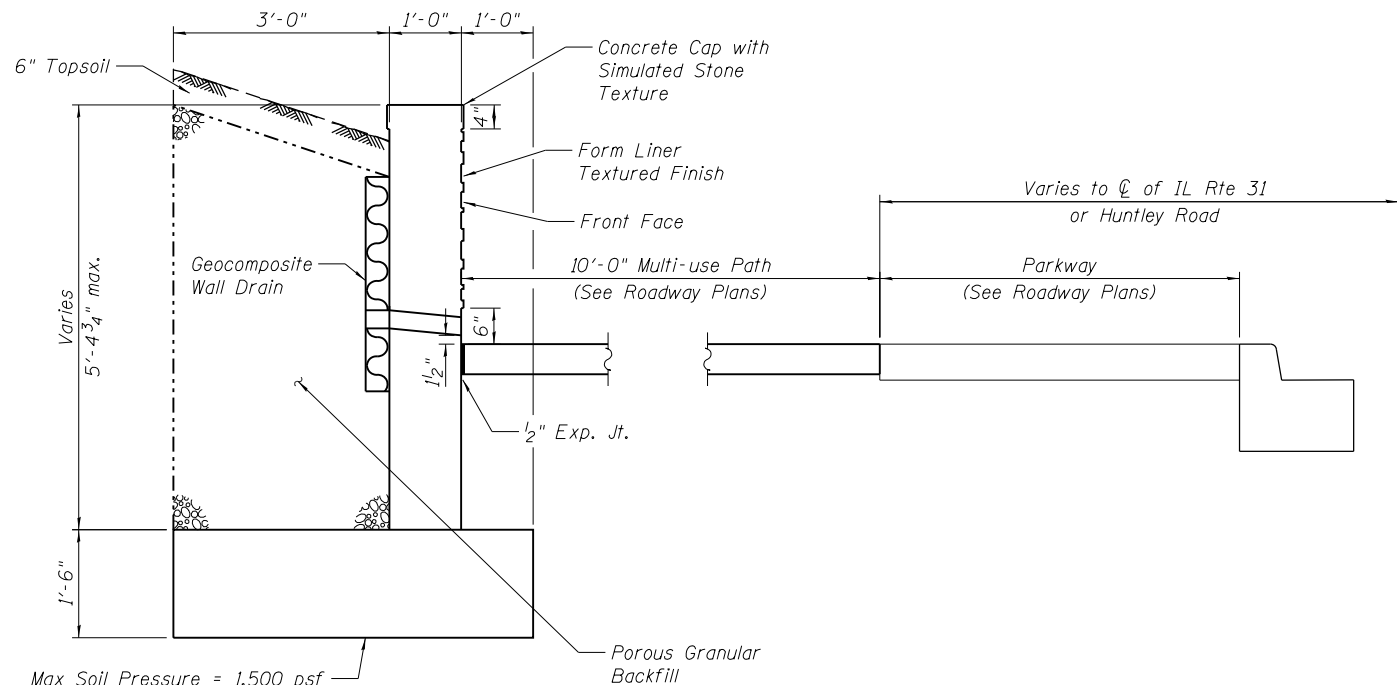
**SW CORNER RETAINING WALL ELEVATION AND FOOTING PLAN
 ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET**

SCALE: SHEET NO. SSW-4 OF SSW-5 SHEETS STA. TO STA.

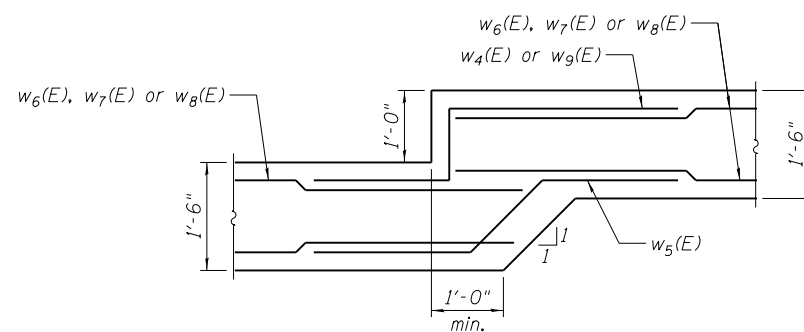
F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 195
CONTRACT NO. 61E73				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				



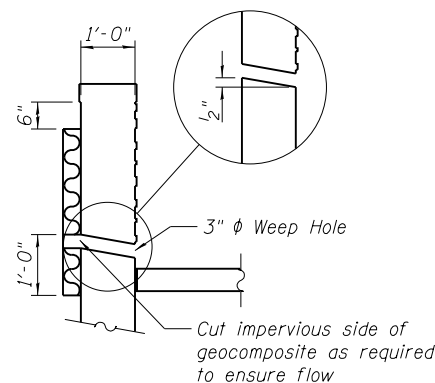
SECTION A-A
Showing rebar placement



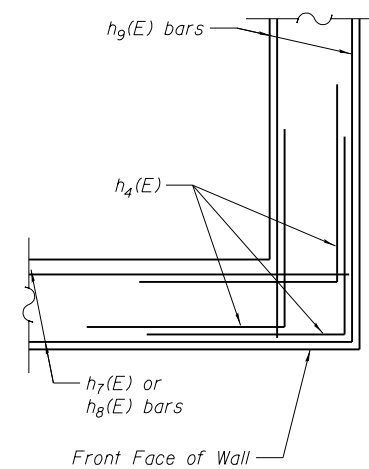
SECTION A-A
Showing dimensions



STEP DETAIL



WEEP HOLE DRAIN DETAIL



CORNER DETAIL - PLAN VIEW

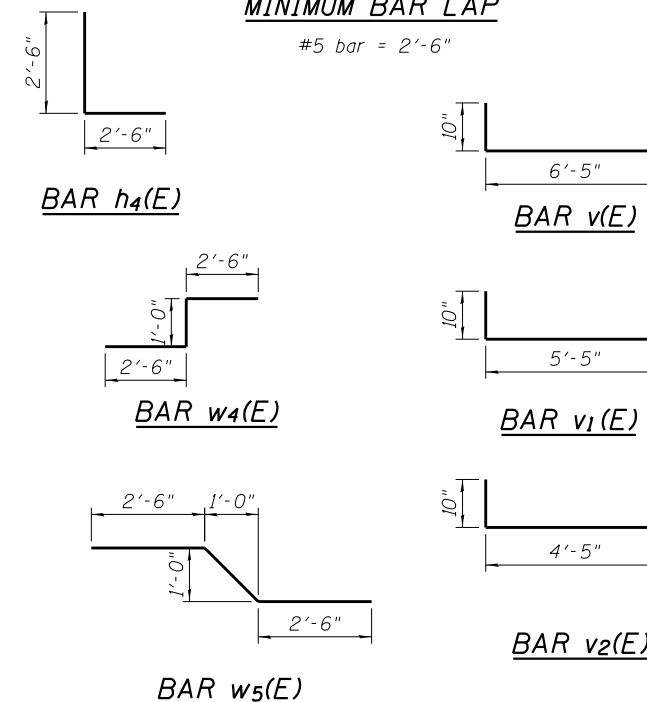
BILL OF MATERIALS

Bar	No.	Size	Length	Shape
h (E)	36	#4	27'- 11"	—
h1 (E)	20	#4	25'- 3"	—
h2 (E)	8	#4	27'- 3"	—
h3 (E)	14	#4	4'- 6"	—
h4 (E)	18	#4	5'- 0"	L
t (E)	12	#5	5'- 6"	—
t1 (E)	292	#5	4'- 8"	—
v (E)	85	#5	7'- 3"	L
v1 (E)	45	#5	6'- 3"	L
v2 (E)	24	#5	5'- 3"	L
v3 (E)	85	#5	5'- 0"	—
v4 (E)	45	#5	4'- 0"	—
v5 (E)	24	#5	3'- 0"	—
w (E)	2	#5	4'- 8"	—
w1 (E)	36	#5	30'- 1"	—
w2 (E)	24	#5	24'- 7"	—
w3 (E)	12	#5	23'- 8"	—
w4 (E)	12	#5	6'- 0"	—
w5 (E)	12	#5	6'- 5"	—
Porous Granular Backfill		Cu Yd	80	
Structure Excavation		Cu Yd	605	
Concrete Structures		Cu Yd	67.3	
Form Liner Textured Surface		Sq Ft	275	
Reinforcement Bars, Epoxy Coated		Pound	6,730	
Geocomposite Wall Drain		Sq Yd	40	

Bars indicated thus 1 x 2-#8 etc. indicates 1 line of bars with 2 lengths per line.

MINIMUM BAR LAP

#5 bar = 2'-6"



COMPANY NAME: HRGreen.com
 PROJECT CONTACT: HRGreen.com
 DATE PLOTTED: 3/23/2018 11:51:41 AM
 FILE NAME: 86140100-str-0a1031.dgn
 PLOT DRIVER: ILpdf.plt
 PEN TABLE: PLOT LABEL.tbl



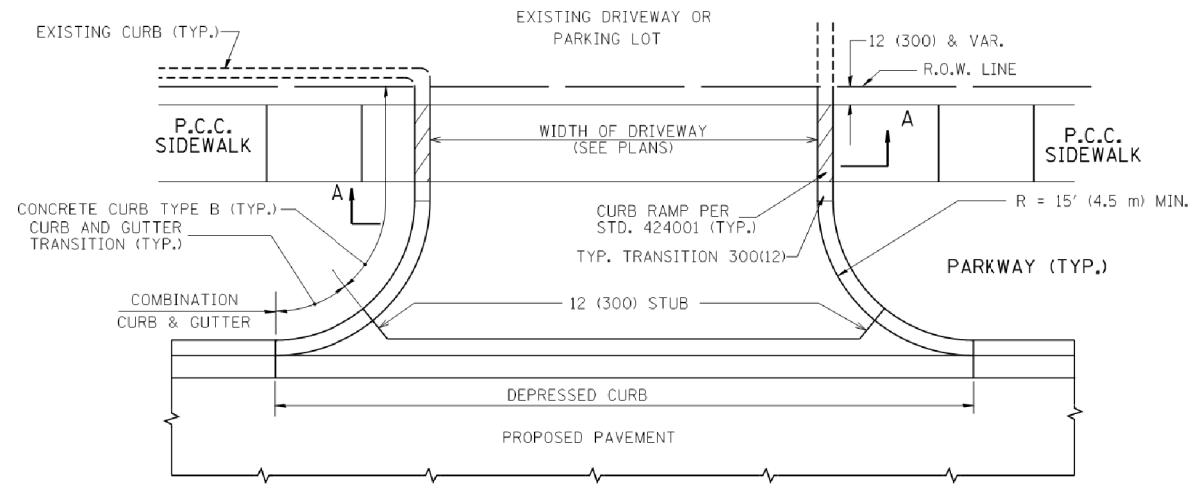
USER NAME = jmelhus	DESIGNED - TOR	REVISED -
	DRAWN - WJH	REVISED -
PLOT SCALE =	CHECKED - RGD	REVISED -
PLOT DATE = 3/23/2018	DATE - 3/23/18	REVISED -

VILLAGE OF CARPENTERSVILLE

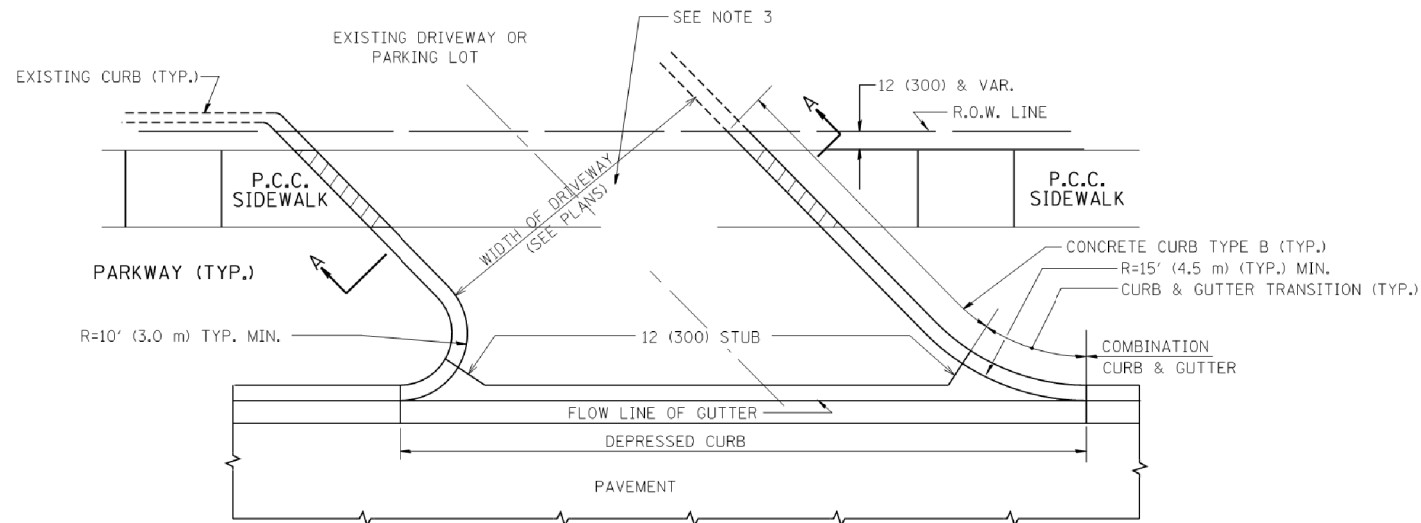
SW CORNER RETAINING WALL TYPICAL SECTIONS AND DETAILS
ILLINOIS ROUTE 31 AND HUNTLEY ROAD / MAIN STREET

SCALE: SHEET NO. SSW-5 OF SSW-5 SHEETS STA. TO STA.

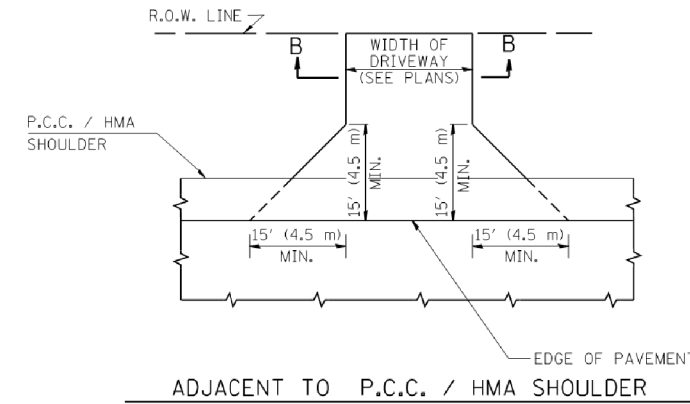
F.A.U. RTE. 3887	SECTION 08-00083-00-CH	COUNTY KANE	TOTAL SHEETS 259	SHEET NO. 196
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E73	



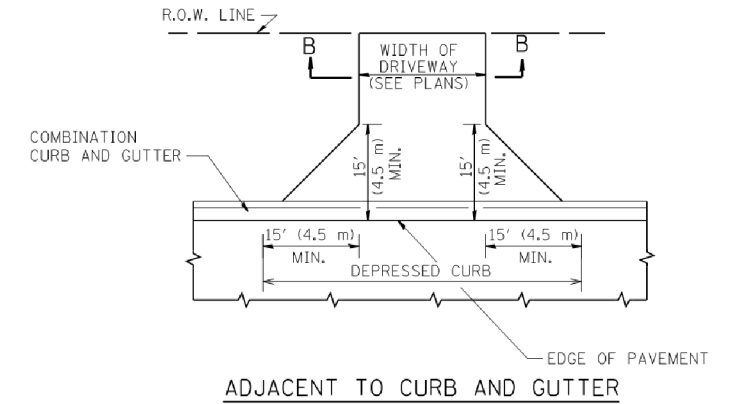
WITH CONCRETE CURB, TYPE B



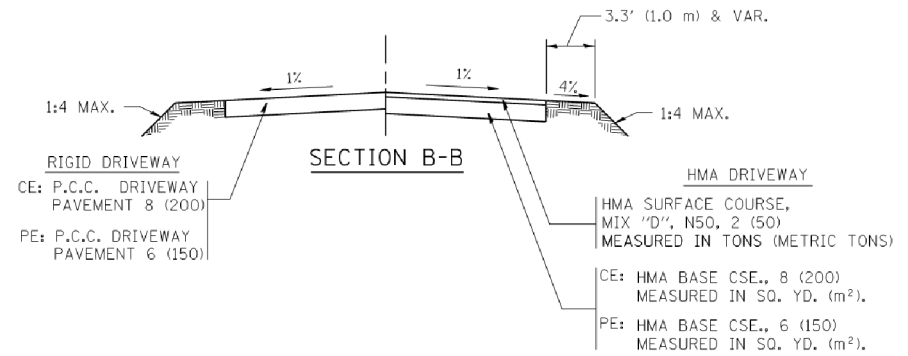
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX 'D', N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

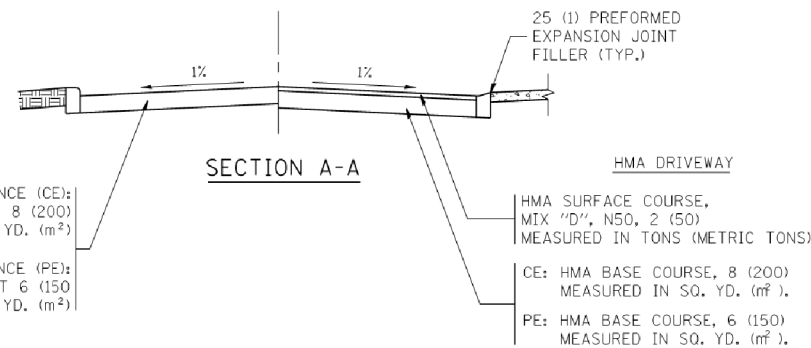
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



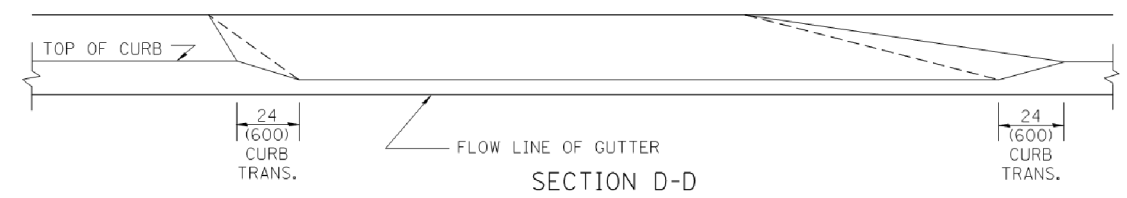
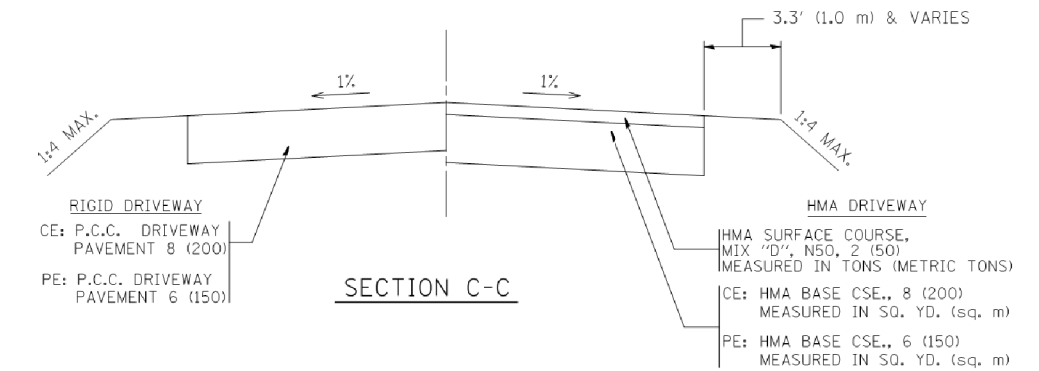
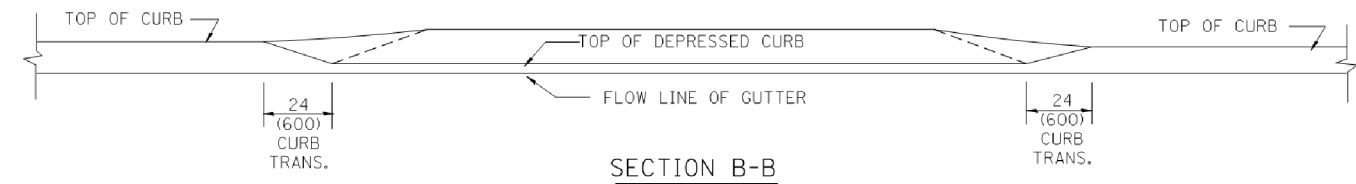
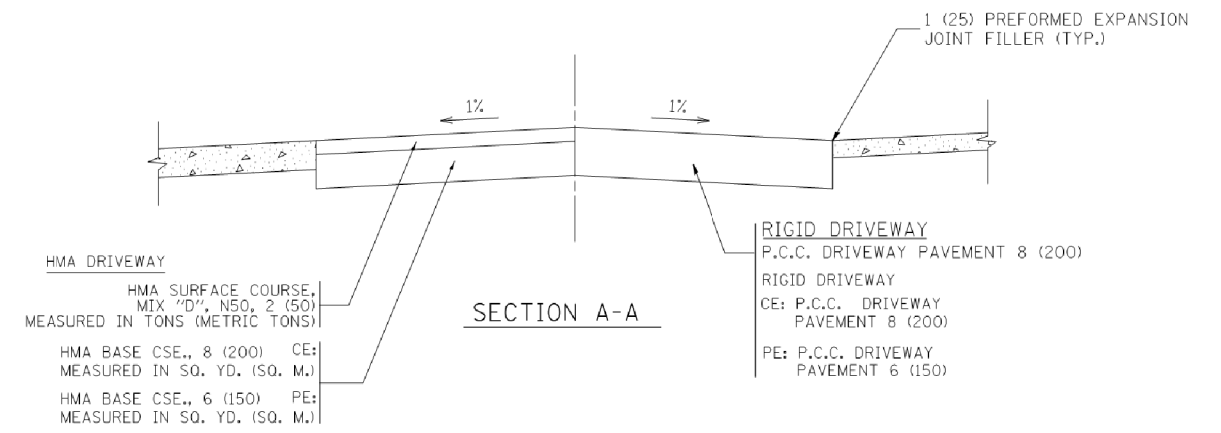
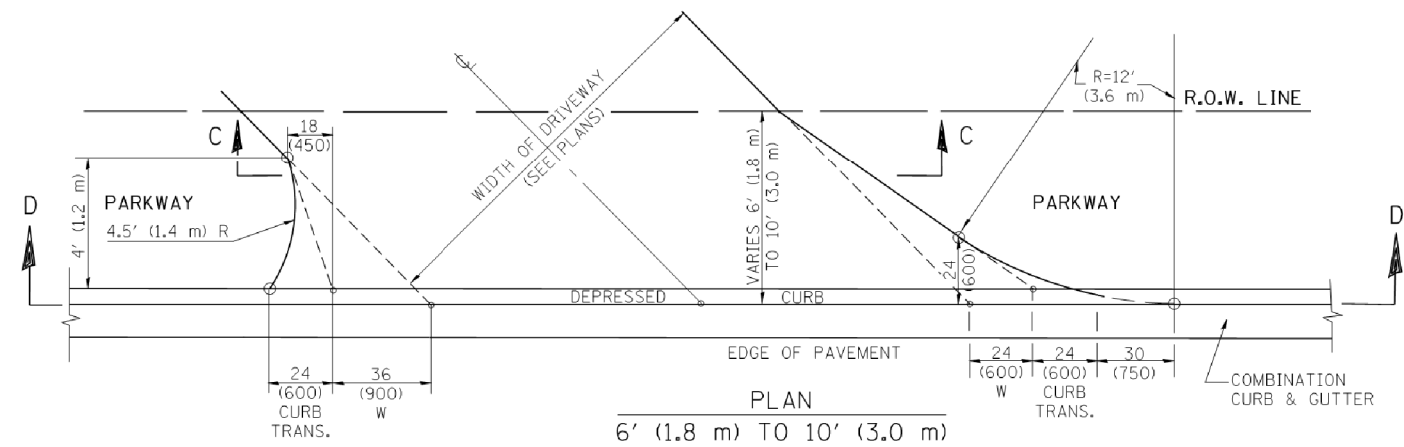
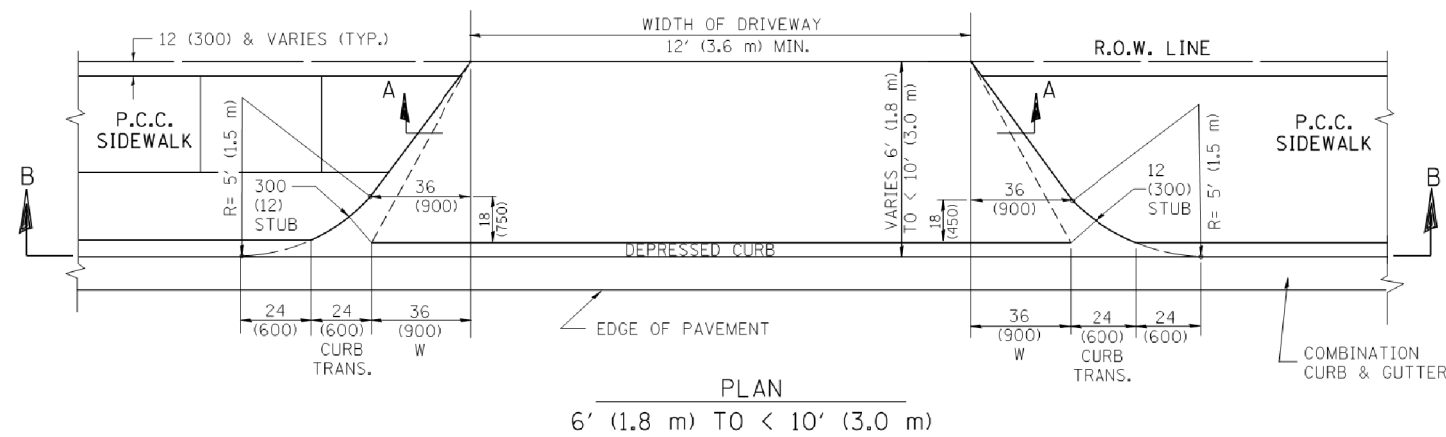
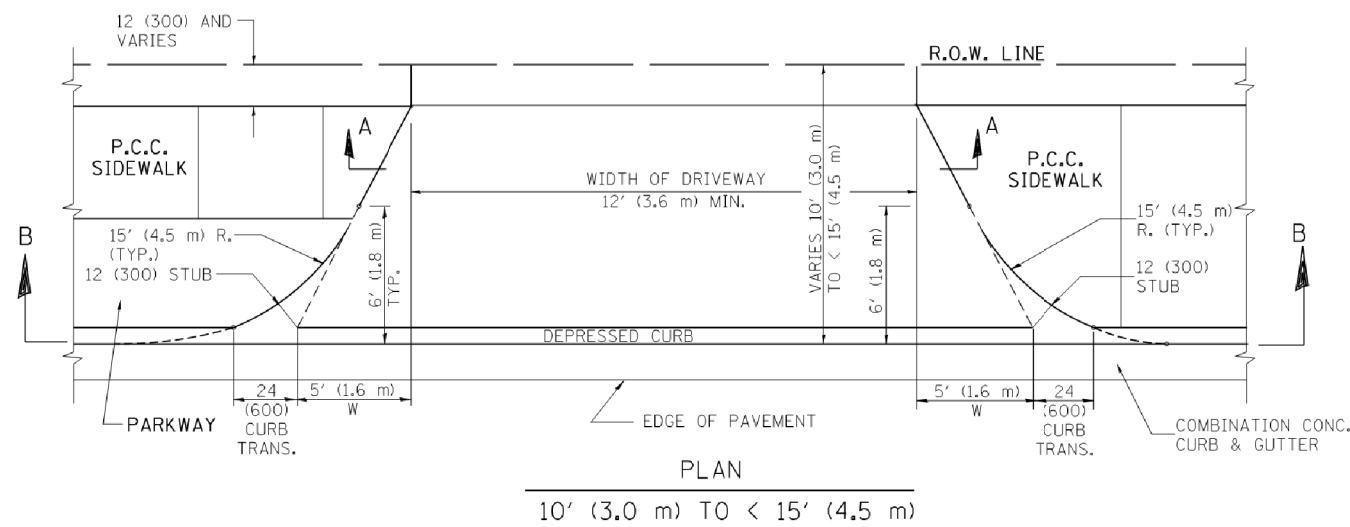
COMPANY NAME: #COMPANY NAME#
 PROJECT CONTACT: #PROJECT CONTACT#
 DATE PLOTTED: 3/23/2018 11:51:44 AM
 FILE NAME: 86140100-sh1-details-8001.dgn
 PLOT DRIVER: ILpdf.plt
 PLOT TABLE: PLOT LABEL.tbl

FILE NAME =	USER NAME = lsgso	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
ca:\p\work\p\dot\lsgso\0108315\bd01.dgn		DRAWN -	REVISED - R. BORO 01-01-07
		PLOT SCALE = 50.0000' / 1"	REVISED - R. BORO 06-11-08
		CHECKED -	REVISED - R. BORO 09-06-11
		DATE - 11-04-95	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	197
BD0156-07 (BD-01)			CONTRACT NO. 61E73	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

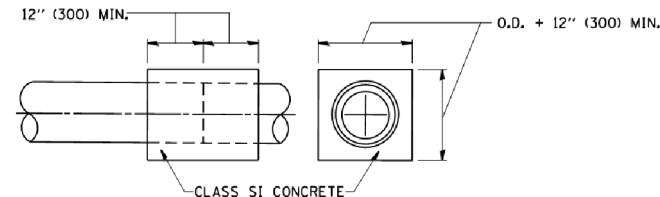
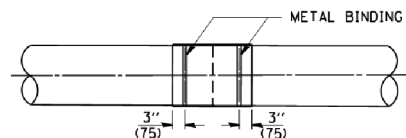
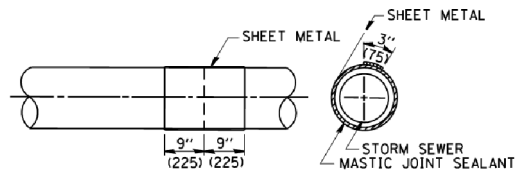
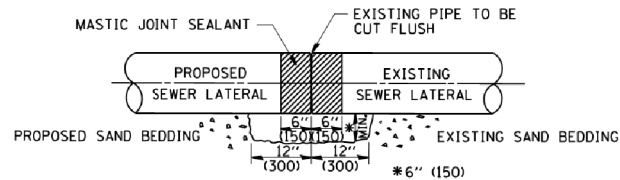
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

COMPANY NAME: #COMPANY CONTACTS
PROJECT CONTACT: #PROJECT CONTACTS
DATE PLOTTED: 3/23/2018 11:56:56 AM
FILE NAME: 86140100-sh1-details-8002.dgn
PLOT DRIVER: ILpdf.plt
PLOT TABLE: PLOT LABEL.tbl

FILE NAME =	USER NAME = lgyso	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-05-01
ca:\prj\work\p\dot\lgyso\d0108315\bd02.dgn		DRAWN -	REVISED - P. LofLEUR 04-15-03
		CHECKED -	REVISED - R. BORO 01-01-07
		DATE - 11-06-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

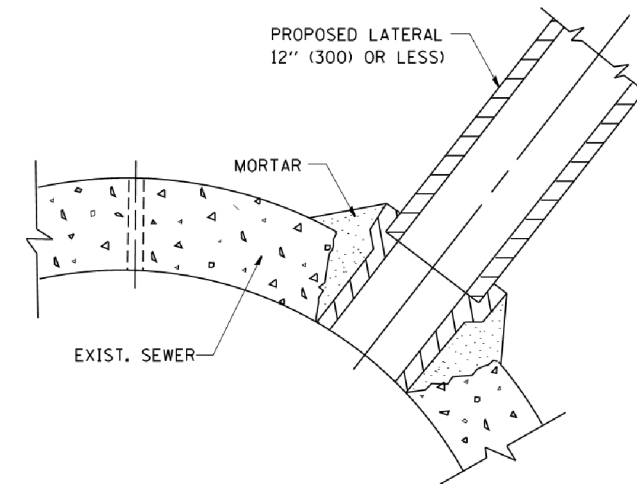
DRIVEWAY DETAILS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)		3887	08-00083-00-CH	KANE	259	198
SCALE: NONE		BD400-02 (BD-02)		CONTRACT NO. 61E73		
SHEET NO. 1 OF 1 SHEETS		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



DETAIL "B"
CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OOZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"
PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

COMPANY NAME: #COMPANY NAME#
PROJECT CONTACT: #PROJECT CONTACT#
DATE PLOTTED: 3/23/2008 11:52:09 AM
FILE NAME: 86140100-sh1-details-8007.dgn
PLOT DRIVER: ILpdf.plt
PEN TABLE: PLOT LABEL.tbl

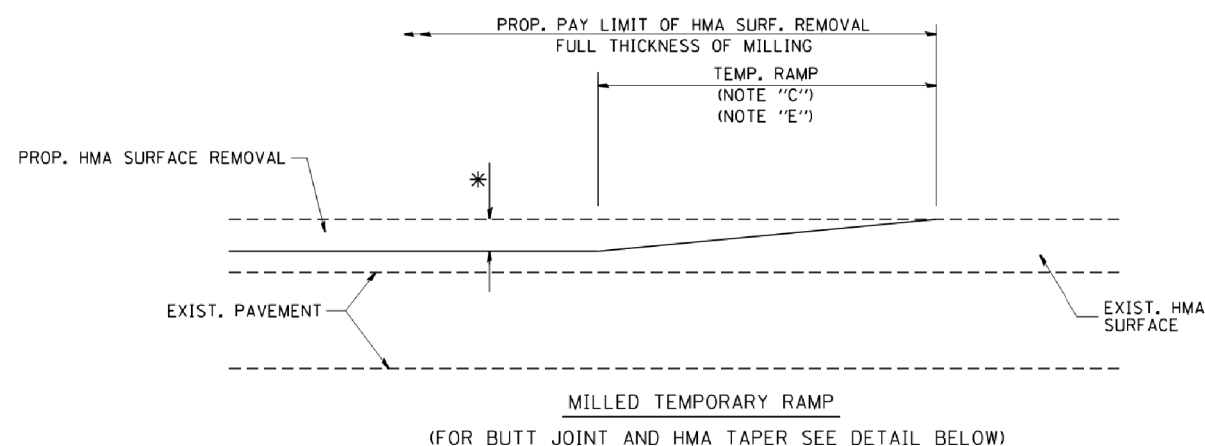
FILE NAME =	USER NAME = gajlanobt	DESIGNED - M. DE YONG	REVISED - M. DE YONG 05-08-92
W:\data\22x34\bd07.dgn		DRAWN -	REVISED - R. SHAH 09-09-94
		CHECKED -	REVISED - R. SHAH 10-25-94
		DATE - 07-25-90	REVISED - R. SHAH 06-12-96

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

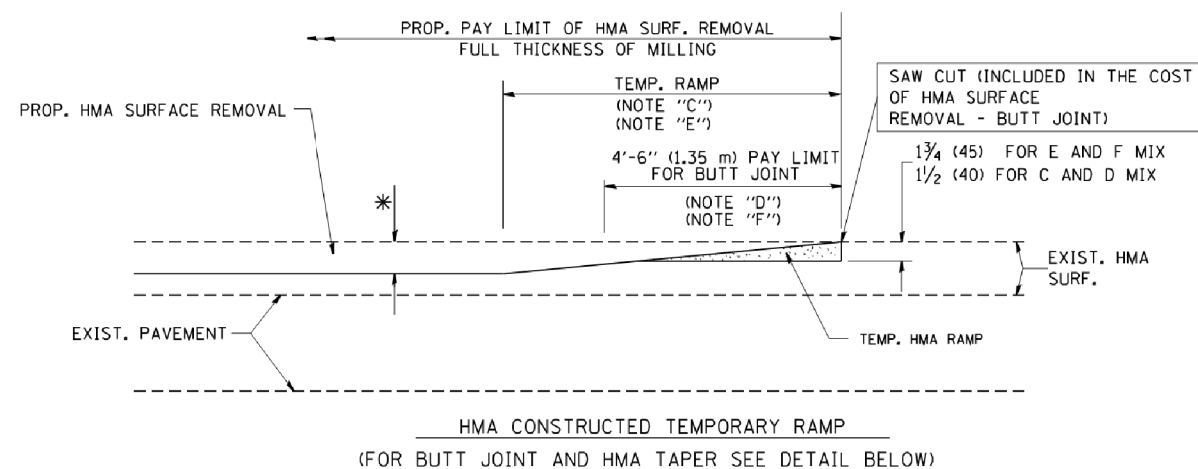
DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	199
BD500-01 (BD-7)		CONTRACT NO. 61E73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

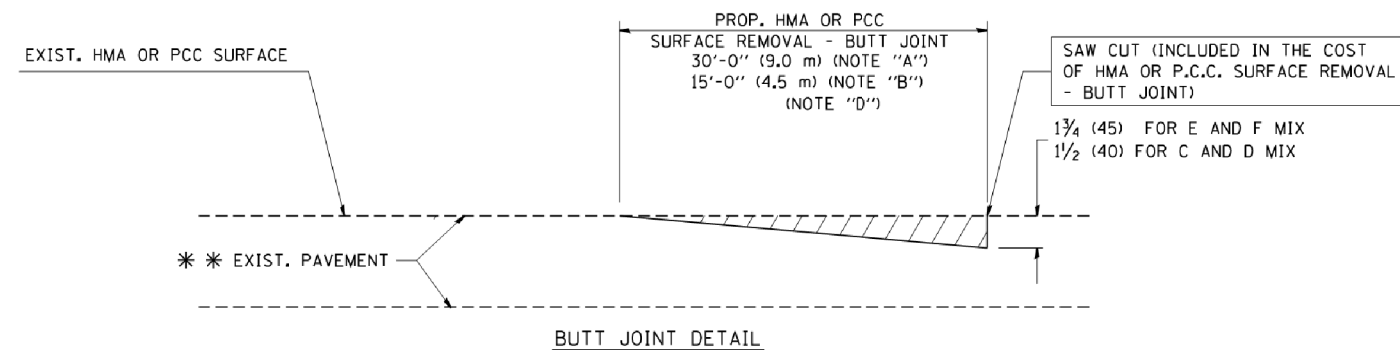


OPTION 1

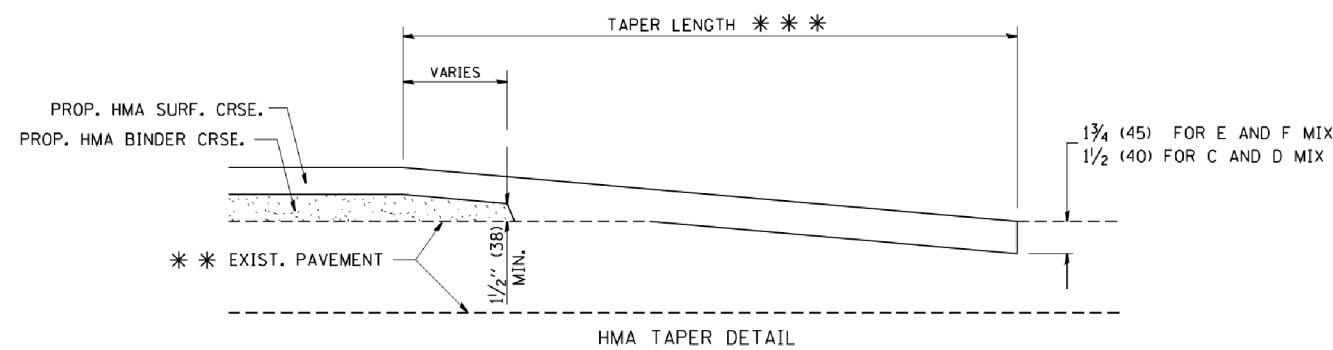


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

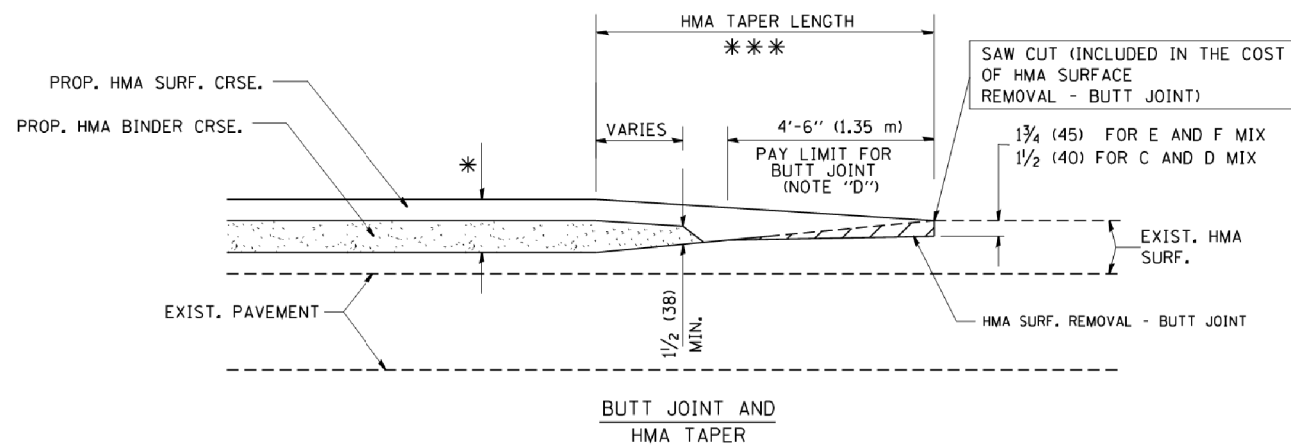
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

COMPANY NAME: #COMPANY NAME#
PROJECT CONTACT: #PROJECT CONTACT#
DATE PLOTTED: 3/23/2018 11:52:20 AM
FILE NAME: 86140100-sh1-details-8032.dgn
PLOT DRIVER: ILpdf.plt
PLOT TABLE: PLOT LABEL.tbl

FILE NAME =	USER NAME = gaglianob	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
W:\d1ststd\22x34\bd32.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 "/> <td>CHECKED -</td> <td>REVISED - M. GOMEZ 04-06-01</td>	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3887	08-00083-00-CH	KANE	259	200
BD400-05 BD32		CONTRACT NO. 61E73		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				