

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11) RS-9	KANE	18	1
		ILLINOIS	CONTRACT NO. 62F27	

D-91-271-17

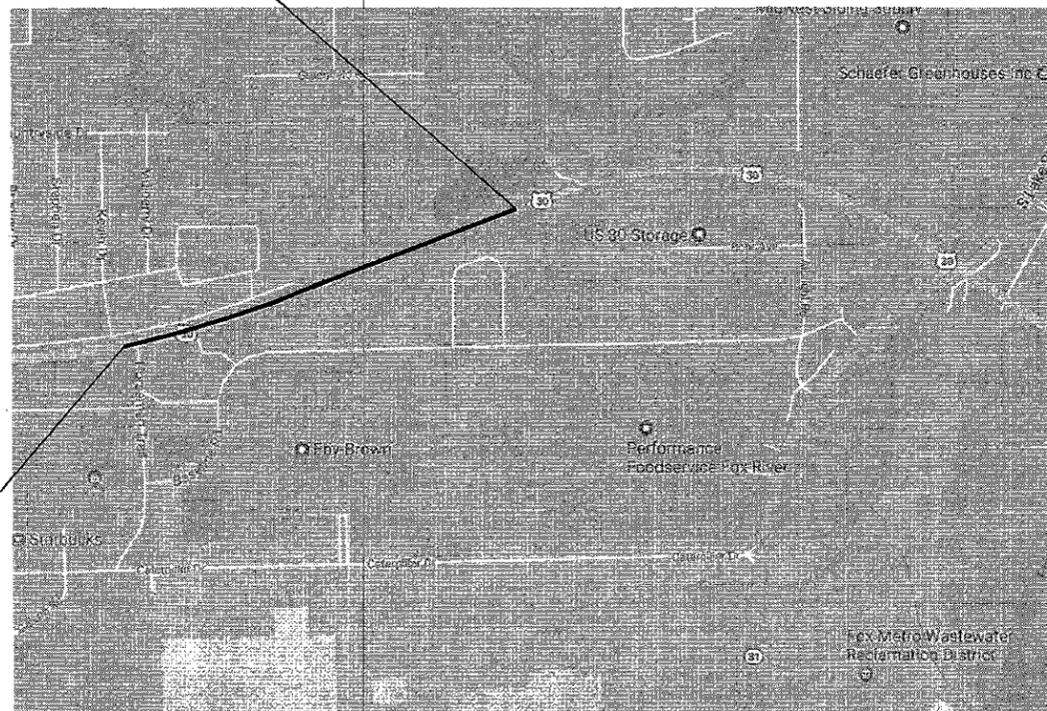
FOR INDEX OF SHEETS, SEE SHEET NO.

THIS IMPROVEMENT IS LOCATED IN  
VILLAGE OF MONTGOMERY

**TRAFFIC DATA:**  
OTHER ARTERIAL  
2016 ADT= 27,800  
SPEED LIMIT: 45 - 50 MPH

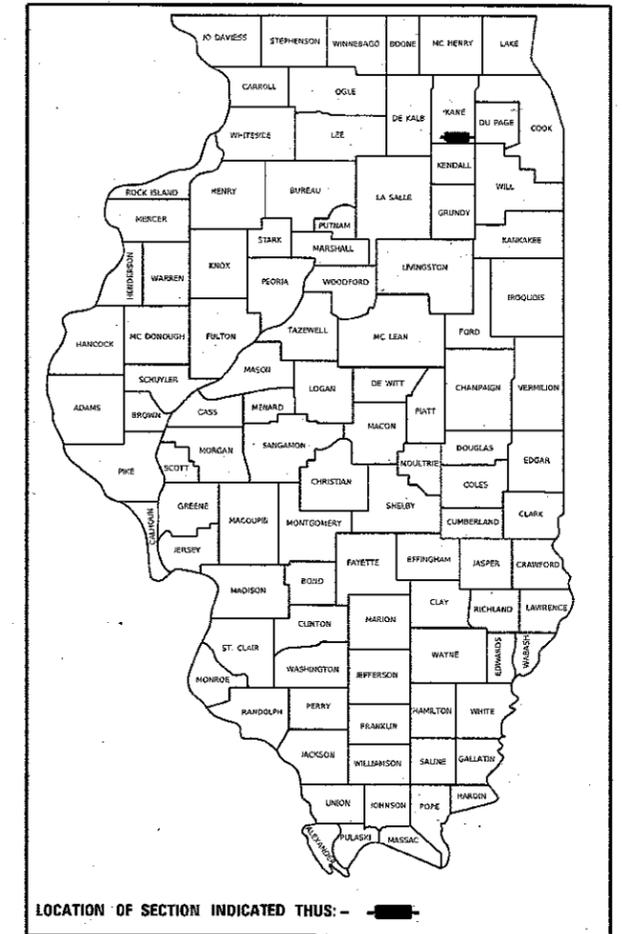
IMPROVEMENT BEGINS  
STA. 56 + 85.1

IMPROVEMENT BEGINS  
STA. 26 + 59.6

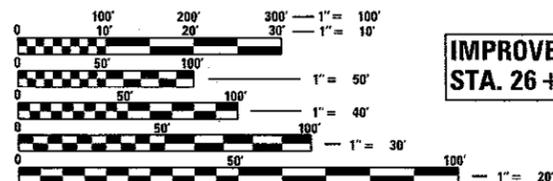


AURORA & BRISTOL TOWNSHIP

GROSS LENGTH = 3025.5 FT. = .573 MILE  
NET LENGTH = 3025.5 FT. = .573 MILE



LOCATION OF SECTION INDICATED THUS: - [thick black line]



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: J. ALAIN MIDY (847) 221-3056  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62F27

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 21, 2018  
Anthony J. Quigley / 1/18 REGIONAL ENGINEER  
Mar 23, 2018  
Paul J. Chalk ENGINEER OF DESIGN AND ENVIRONMENT  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, AND GENERAL NOTES
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5 - 6	TYPICAL SECTIONS
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9	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-08)
10	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
11	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
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15	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
16	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
17	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
18	ARTERIAL ROAD INFORMATION SIGN (TC-22)

**STATE HIGHWAY STANDARDS**

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-04	FRAMES AND LIDS TYPE 1
604091-03	FRAME AND GRATE TYPE 24
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701502-08	URBAN LANE CLOSURE, 2L, 2W, WITH BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-07	TRAFFIC CONTROL DEVICES

**GENERAL NOTES**

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGE OF MONTGOMERY.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.

THE CONTRACTOR SHALL CONTACT DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, IDOT'S TRAFFIC FIELD ENGINEER VIA E-MAIL AT DON\_CHIARUGI@ILLINOIS.GOV, MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER/TECHNICIAN.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC FIELD ENGINEER VIA E-MAIL AT CORY.JUCIUS@ILLINOIS.GOV, A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.

THE CONTRACTOR SHALL CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING FORESTRY WORK FOR LAYOUT.

USER NAME	ababawa	DESIGNED	-	REVISED	-	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>US RTE. 30 (HORSEMEN TRAIL TO IL. RTE 31 (LAKE ST.) INDEX OF SHEETS, STATE STANDARDS AND NOTES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLT SCALE	100.0000 / 1" = 100'	DRAWN	-	REVISED	-			349	(10 & 11) RS-9	KANE	18	2	
PLT DATE	2/1/2018	CHECKED	-	REVISED	-			CONTRACT NO. 62F27					
		DATE	-	REVISED	-			SCALE: NONE	SHEET	OF	SHEETS	STA.	TO

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	60	60			
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	150	150			
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	5	5			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	8077	8077			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	18	18			
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	493	493			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	106	106			
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	19	19			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	160	160			
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	9441	9441			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	2526	2526			
44002209	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 2 1/4"	SQ YD	236	236			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
44201761	CLASS D PATCHES, TYPE I, 10 INCH	SQ YD	30	30			
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	114	114			
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	80	80			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	355	355			
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	2	2			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3			
67100100	MOBILIZATION	LSUM	1	1			
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1	1			
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3042	3042			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1004	1004			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	109.8	109.8			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	11094	11094			

FILE NAME =	USER NAME = abrown	DESIGNED -	REVISED -
Office: District 1 Projects\02510\CAOData\Design\02510-02510-01		CHECKED -	REVISED -
PLOT SCALE = 100,0000' / In.		DATE -	REVISED -
PLOT DATE = 2/1/2018			

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

US RTE. 30 (HORSEMEN TRAIL TO IL. RTE 31 (LAKE ST.) SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11) RS-9	KANE	18	3
CONTRACT NO. 62F27			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

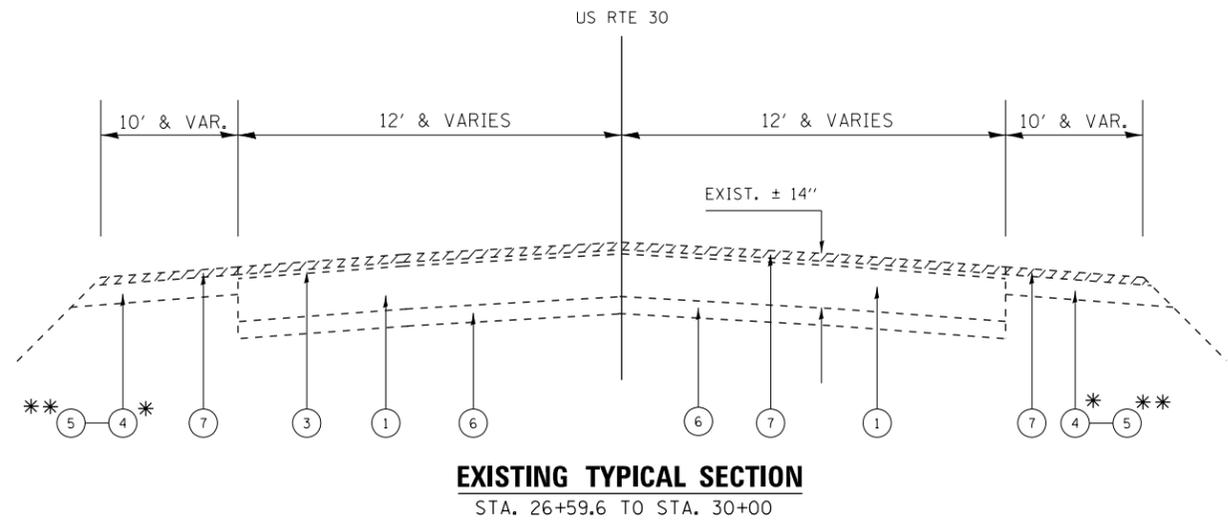
SCALE: SHEET NO. OF SHEETS STA. TO STA.

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	730	730			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	272	272			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	48	48			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	1521	1521			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	50 FT	109.8	109.8			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	11094	11094			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	730	730			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	272	272			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	48	48			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	131	131			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	105	105			
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	42	42			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	1012	1012			
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	40	40			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2	2			
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	50 FT	4504	4504			
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	45	45			
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	3	3			
Z0030850	TEMPORARY INFORMATION SIGNING	50 FT	51.4	51.4			
Z0064800	SELECTIVE CLEARING	UNIT	6	6			

8 \* Specialty Item

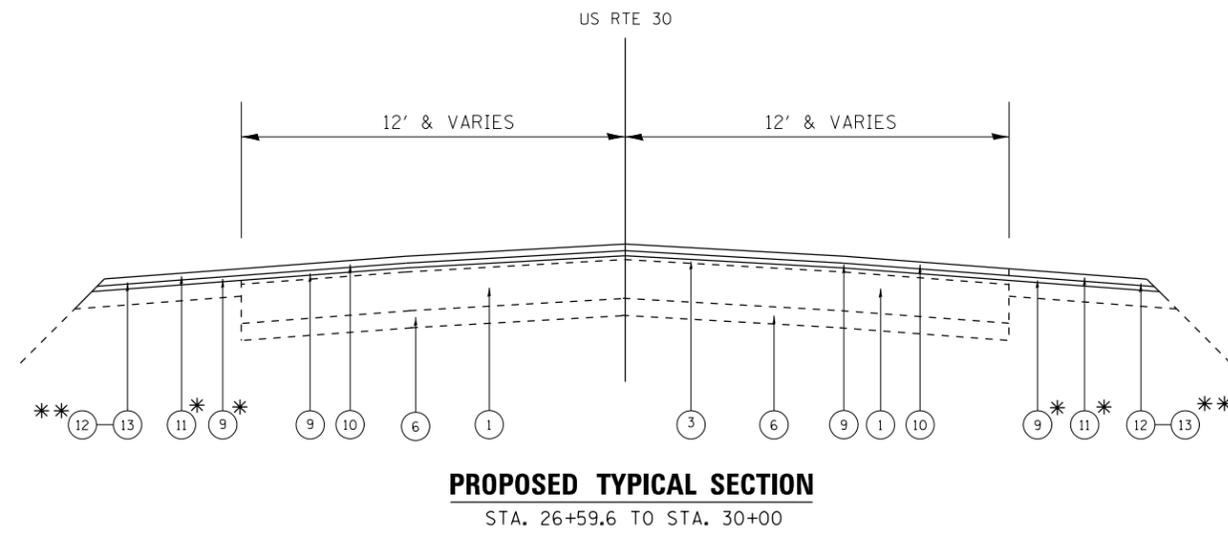
Revision



**EXISTING TYPICAL SECTION**  
STA. 26+59.6 TO STA. 30+00

**LEGEND**

- ① EXIST. P.C.C PAVEMENT ± 10"
- ② EXIST. HOT-MIX ASPHALT PAVEMENT AFTER MILLING ±1/2"
- ③ EXIST. HOT-MIX ASPHALT PAVEMENT AFTER MILLING ± 1/2"
- ④ EXIST. HOT-MIX ASPHALT SHOULDER
- ⑤ EXIST. AGGREGATE SHOULDERS
- ⑥ EXIST. SUBBASE GRANULAR MATERIAL
- ⑦ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑧ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑨ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑩ PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80, 1 3/4"
- ⑪ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- ⑫ PROP. GRADING AND SHAPING SHOULDERS
- ⑬ PROP. AGGREGATE WEDGE SHOULDER, TYPE B



**PROPOSED TYPICAL SECTION**  
STA. 26+59.6 TO STA. 30+00

\* STA. 26+59.6 TO STA. 36+31.7 - HMA SHOULDER

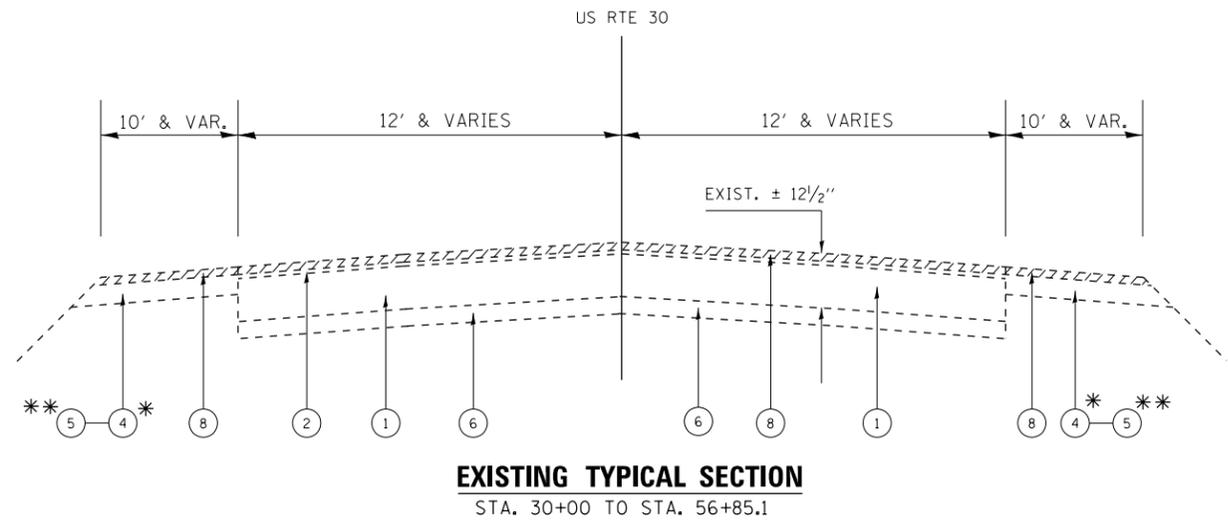
\*\* STA. 36+31.7 TO STA. 56+85.1 - AGGREGATE SHOULDER

**THE CONTRACTOR SHALL PATCH FIRST THEN MILL**

MIXTURE REQUIREMENTS		VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE USES			
<b>PAVEMENT RESURFACING</b>			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80		3.5% AT 80 GYR.	<b>QC/QA</b>
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50		3.5% AT 50 GYR.	<b>QC/QA</b>
<b>SHOULDER RESURFACING</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm)		4% AT 70 GYR.	<b>QC/QA</b>
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50		3.5% AT 50 GYR.	<b>QC/QA</b>
<b>PATCHING</b>			
CLASS D PATCHES (HMA BINDER, IL-19.0 mm)		4% AT 70 GYR.	<b>QC/QA</b>
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER, IL-19.0 mm)		4% AT 70 GYR.	<b>QC/QA</b>
QMP Designation: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP)			

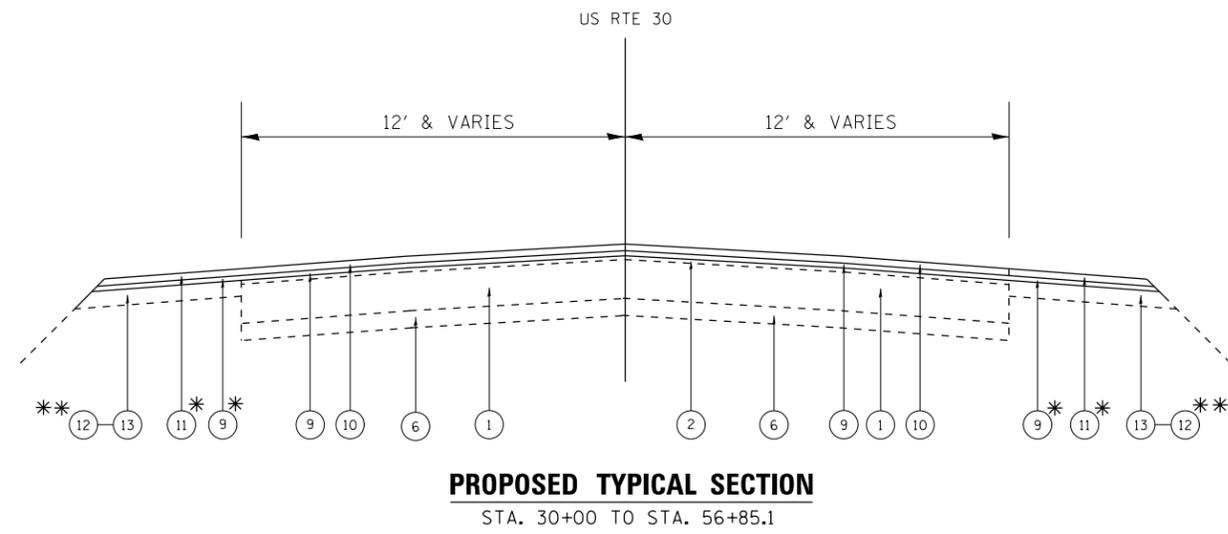
**NOTE:**

- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN
- THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE



**LEGEND**

- ① EXIST. P.C.C PAVEMENT ± 10"
- ② EXIST. HOT-MIX ASPHALT PAVEMENT AFTER MILLING ± 1/2"
- ③ EXIST. HOT-MIX ASPHALT PAVEMENT AFTER MILLING ± 1 1/2"
- ④ EXIST. HOT-MIX ASPHALT SHOULDER
- ⑤ EXIST. AGGREGATE SHOULDERS
- ⑥ EXIST. SUBBASE GRANULAR MATERIAL
- ⑦ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- ⑧ PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
- ⑨ PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ⑩ PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80, 1 3/4"
- ⑪ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- ⑫ PROP. GRADING AND SHAPING SHOULDERS
- ⑬ PROP. AGGREGATE WEDGE SHOULDER, TYPE B

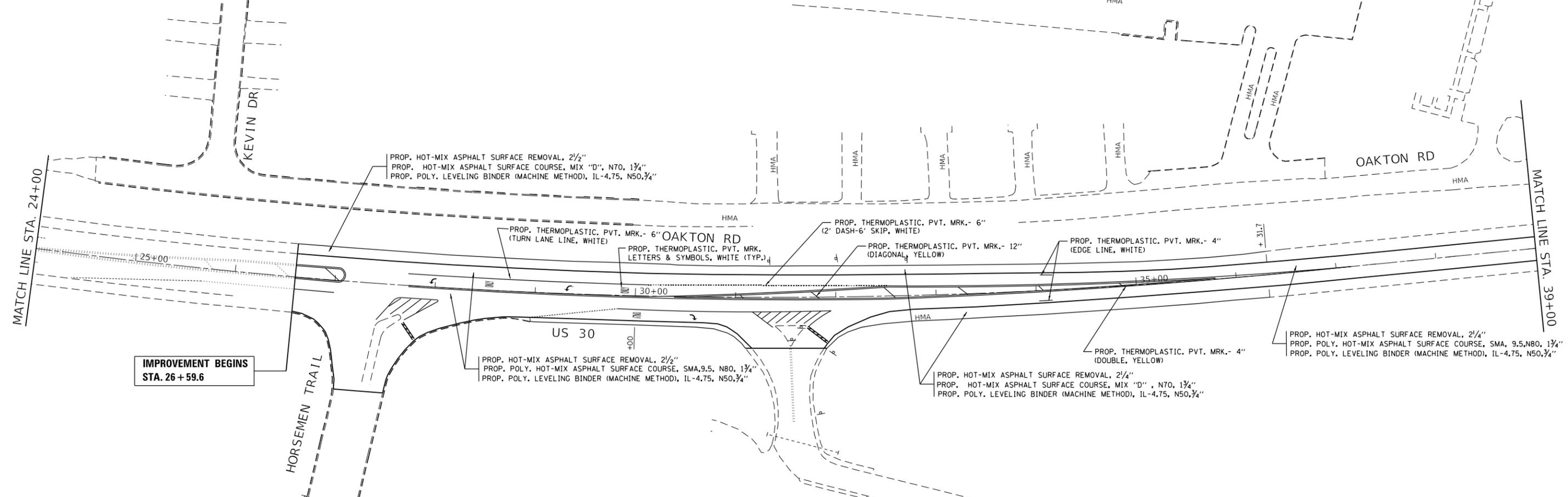
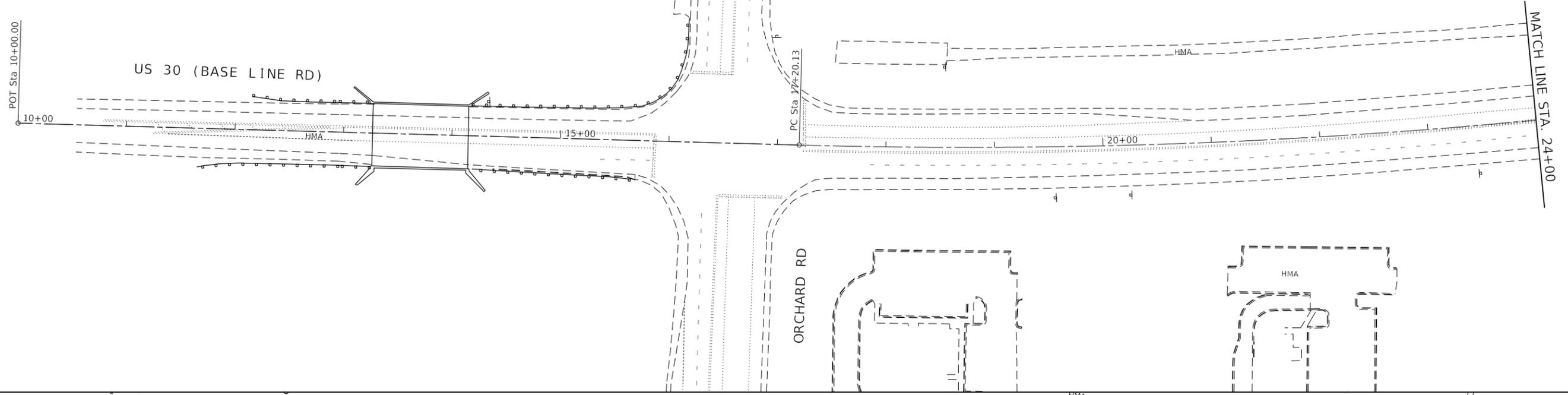


\* STA. 26+59.6 TO STA. 36+31.7 - HMA SHOULDER

\*\* STA. 36+31.7 TO STA. 56+85.1 - AGGREGATE SHOULDER

**THE CONTRACTOR SHALL PATCH FIRST THEN MILL**

FILE NAME =	USER NAME = abebawa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING &amp; PROPOSED TYPICAL SECTIONS U.S. RTE. 30 - HORSEMEN TO IL RTE 31 (LAKE ST.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
						349	(10 & 11) RS-9	KANE	18	6	
						CONTRACT NO. 62F27					
Default	PLOT DATE = 1/26/2018	DATE -	REVISED -			SCALE:	SHEET 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	



**IMPROVEMENT BEGINS  
STA. 26 + 59.6**

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"  
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

PROP. THERMOPLASTIC. PVT. MKR.- 6" OAKTON RD  
(TURN LANE LINE, WHITE)  
PROP. THERMOPLASTIC. PVT. MKR. LETTERS & SYMBOLS, WHITE (TYP.)

PROP. THERMOPLASTIC. PVT. MKR.- 6" (2' DASH-6' SKIP, WHITE)  
PROP. THERMOPLASTIC. PVT. MKR.- 12" (DIAGONAL, YELLOW)

PROP. THERMOPLASTIC. PVT. MKR.- 4" (EDGE LINE, WHITE)  
PROP. THERMOPLASTIC. PVT. MKR.- 4" (DOUBLE, YELLOW)

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"  
PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80, 1 3/4"  
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"  
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  
PROP. POLY. HOT-MIX ASPHALT SURFACE COURSE, SMA, 9.5, N80, 1 3/4"  
PROP. POLY. LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"

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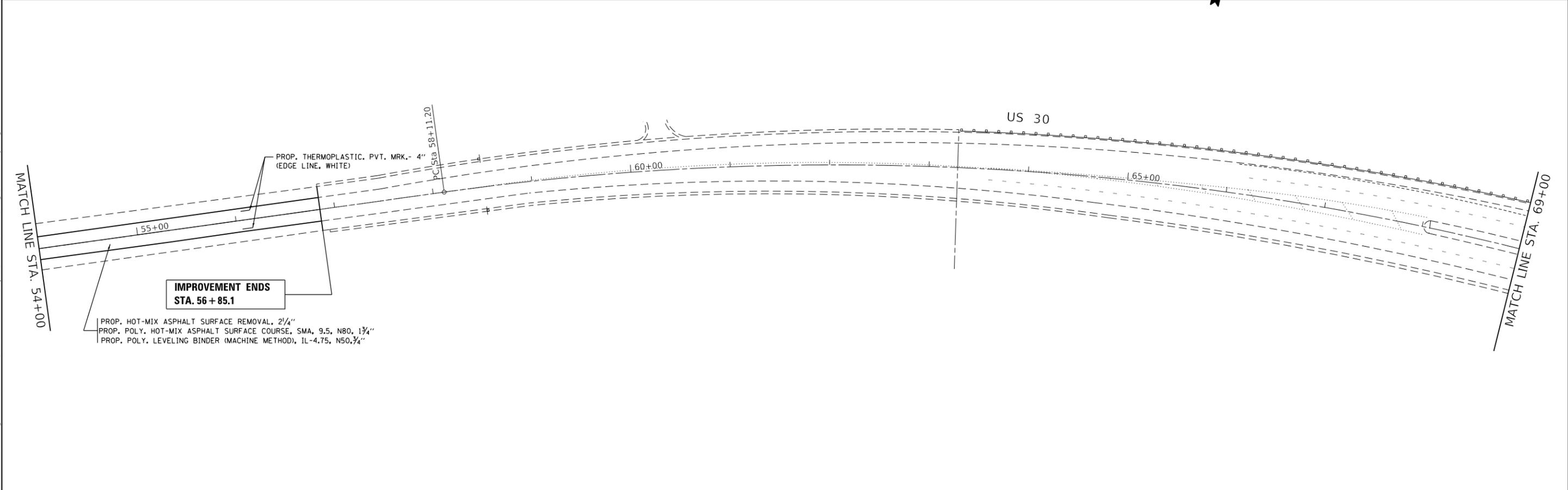
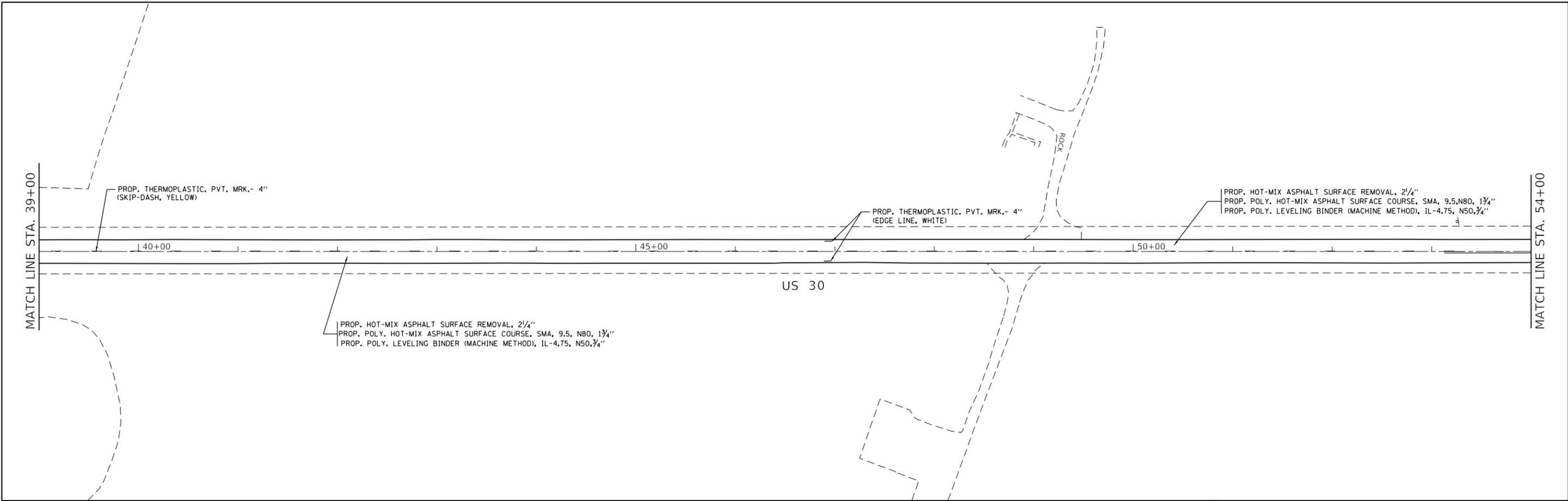
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/26/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
US RTE. 30 (HORSEMEN TRAIL TO IL. RTE 31 (LAKE ST.))**

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(106.11)RS-9	KANE	18	7
CONTRACT NO. 62F27				
ILLINOIS FED. AID PROJECT				



MODEL: Default  
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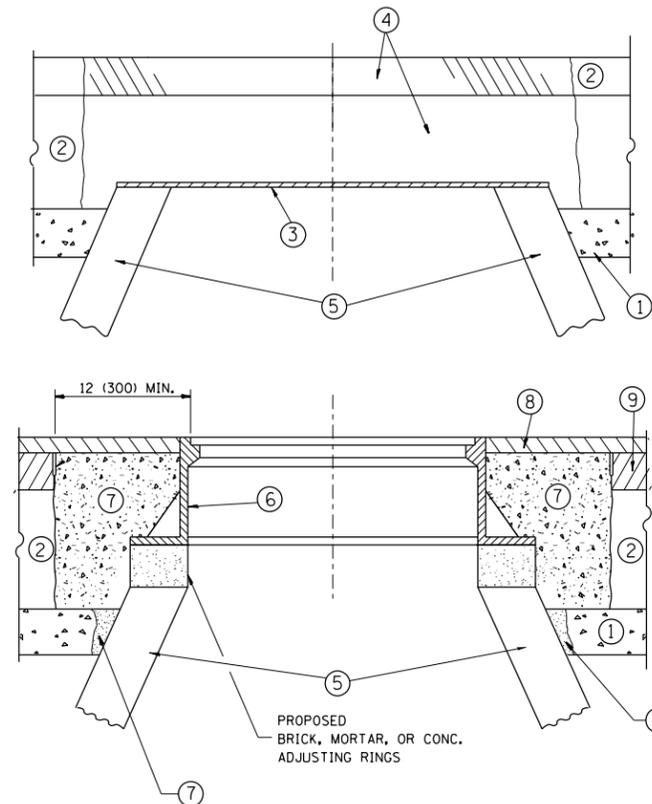
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/26/2018	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**US RTE. 30 (HORSEMEN TRAIL TO IL. RTE 31 (LAKE ST.))**

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(106.11)RS-9	KANE	18	8
CONTRACT NO. 62F27				
ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

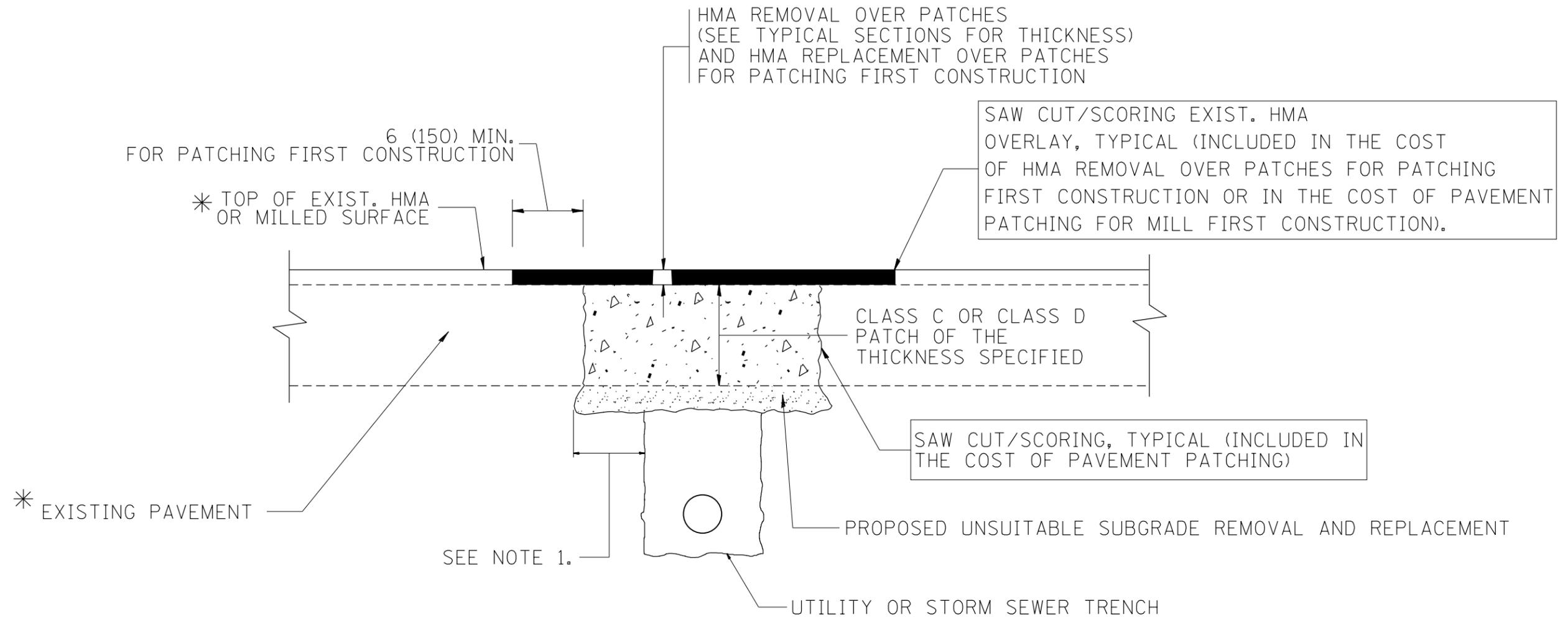
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = abebawa	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
p:\11\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI2511\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
		DATE - 10-25-94	REVISED - R. BORO 03-09-11
			REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11) RS-9	KANE	18	9
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 62F27</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

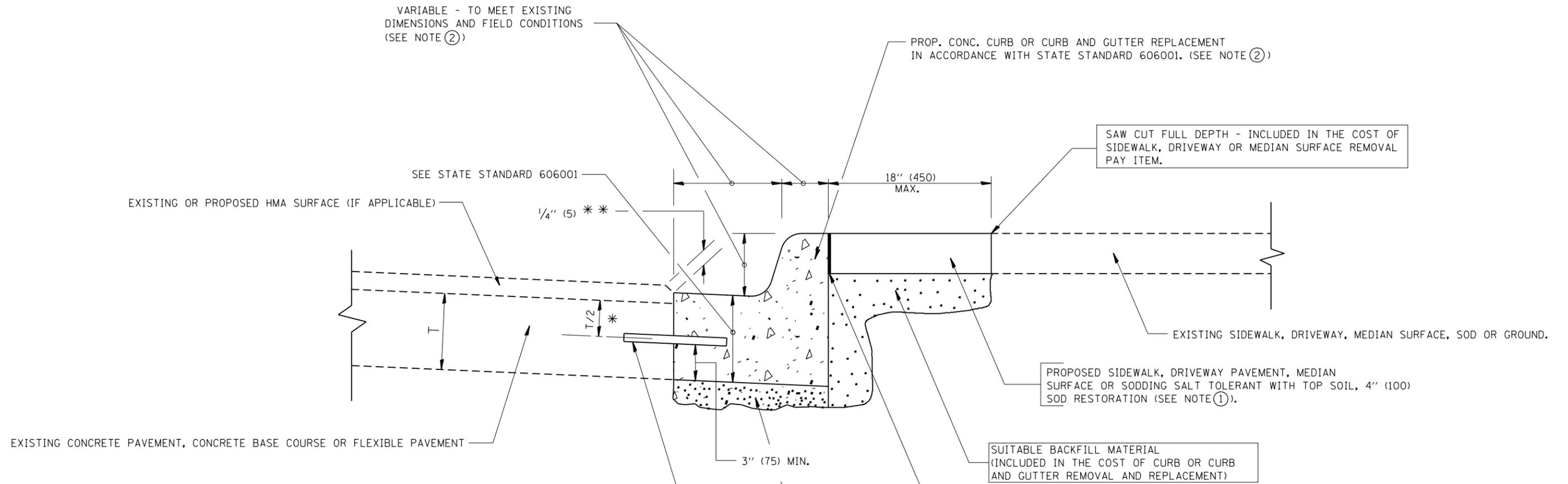
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abebawa	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI2511\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07					349	(10 & 11) RS-9	KANE	18	10
PLOT SCALE = 100.0000' / 1" =	DATE - 10-25-94	REVISED - R. BORO 09-04-07	REVISED - K. ENG 10-27-08		BD400-04 (BD-22)			CONTRACT NO. 62F27				
PLOT DATE = 1/26/2018					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
  - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
  - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
  - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
  - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

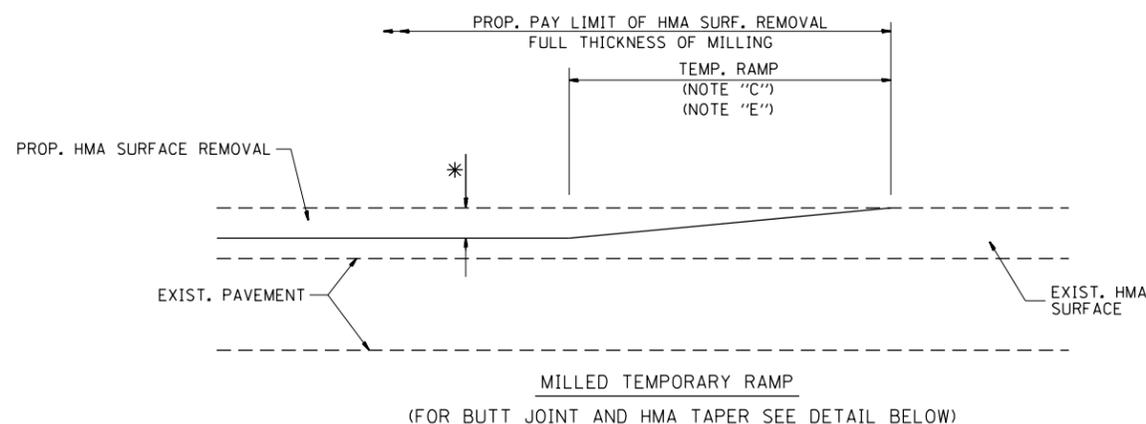
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

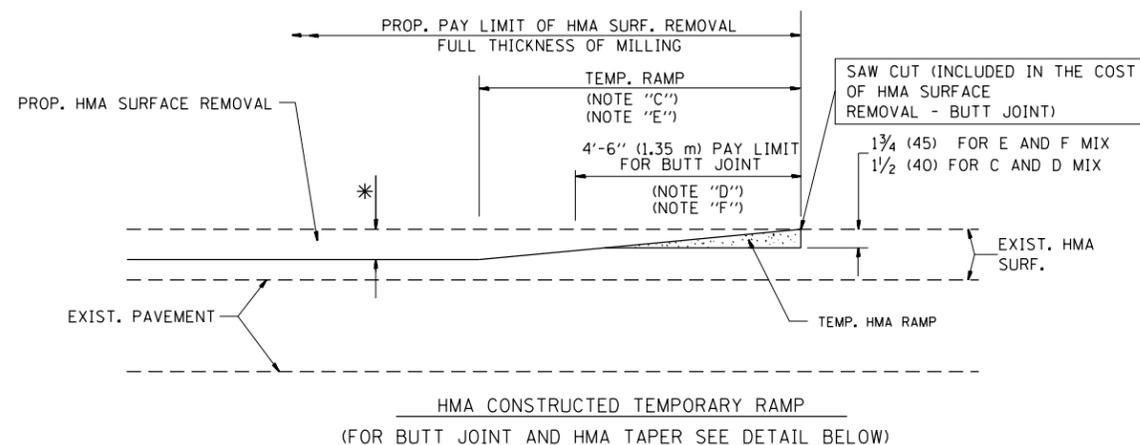
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abebawa	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - A. ABBAS 03-21-97			349	(10 & 11) RS-9	KANE	18	11	
			REVISED - M. GOMEZ 01-22-01			<b>BD600-06 (BD-24)</b>		CONTRACT NO. 62F27			
			REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
PLOT SCALE = 1/26/2018		DATE - 03-11-94		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.				

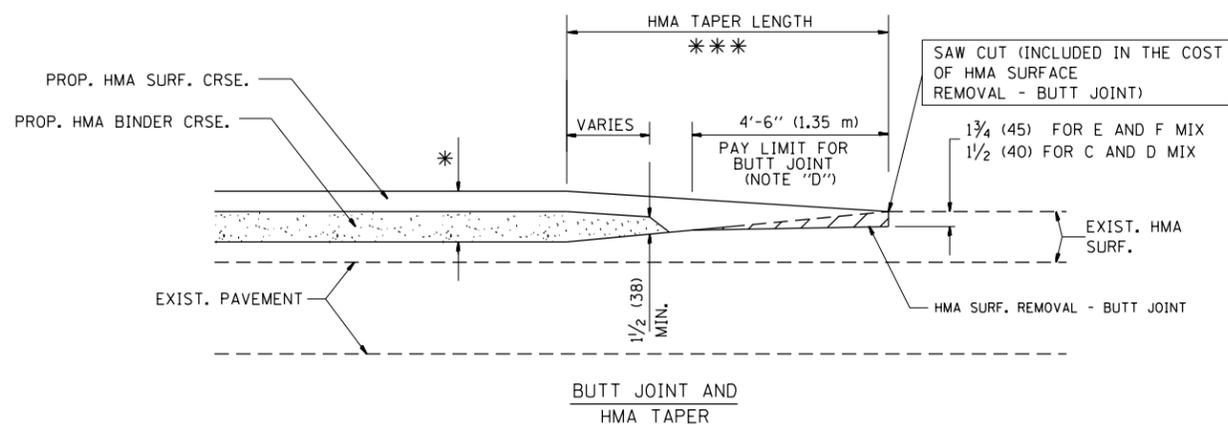


**OPTION 1**

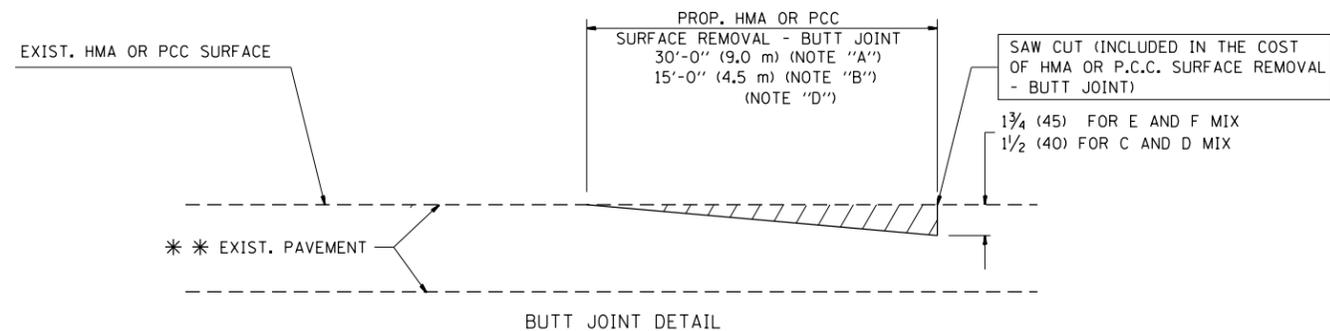


**OPTION 2**

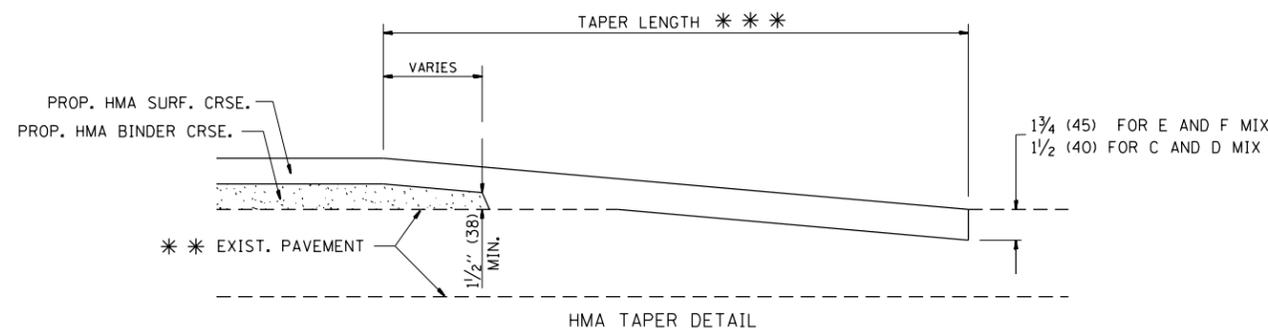
**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

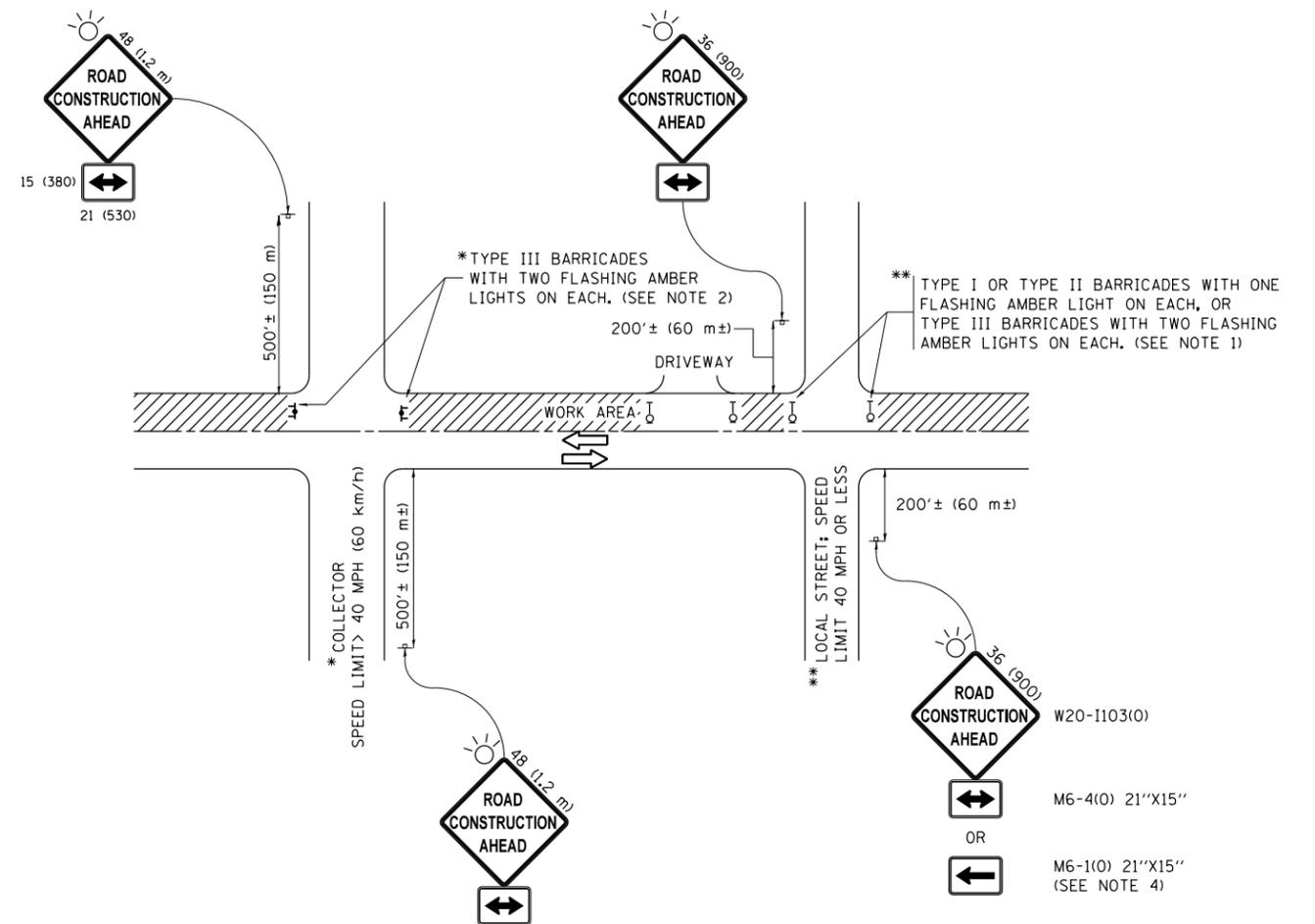
FILE NAME =	USER NAME = abebawa	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI2511\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 1/26/2018	CHECKED -	REVISED - M. GOMEZ 04-06-01
		DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11) RS-9	KANE	18	12
BD400-05 BD32		CONTRACT NO. 62F27		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

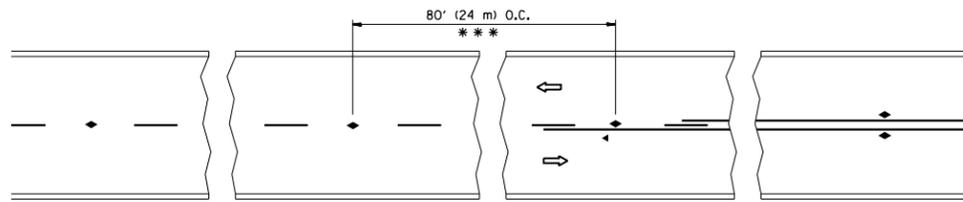
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p:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI2511\Drawings\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 1/26/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

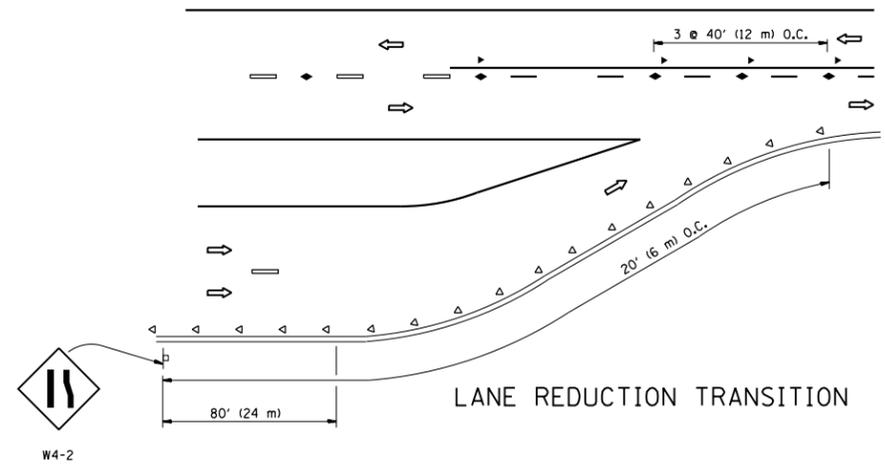
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11) RS-9	KANE	18	13
<b>TC-10</b>			<b>CONTRACT NO. 62F27</b>	
ILLINOIS FED. AID PROJECT				

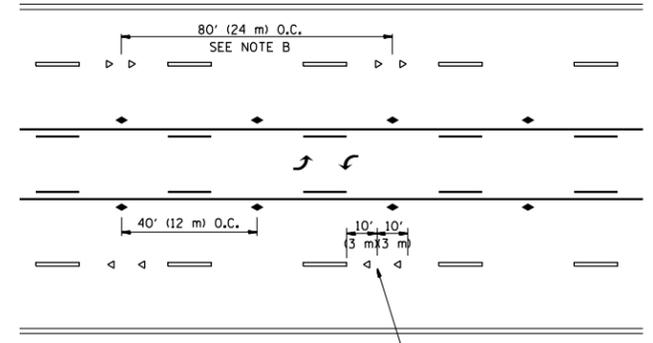


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

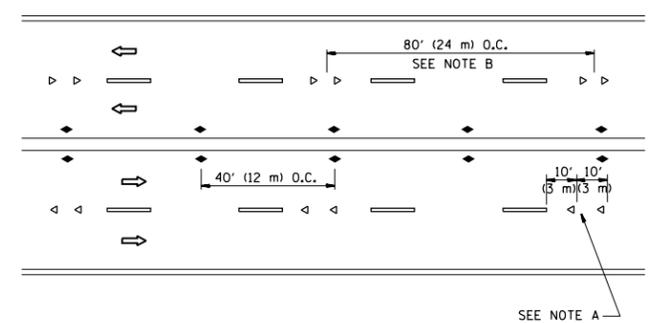
TWO-LANE/TWO-WAY



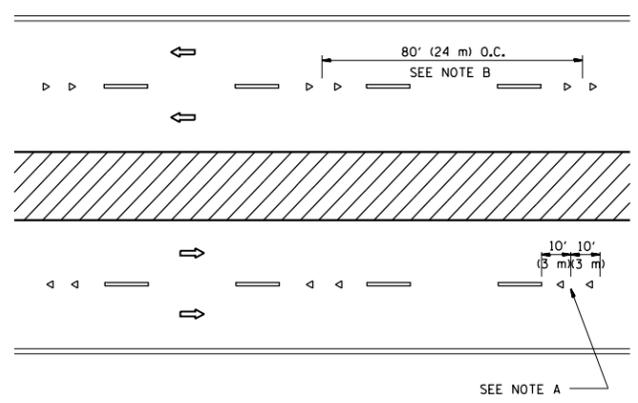
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

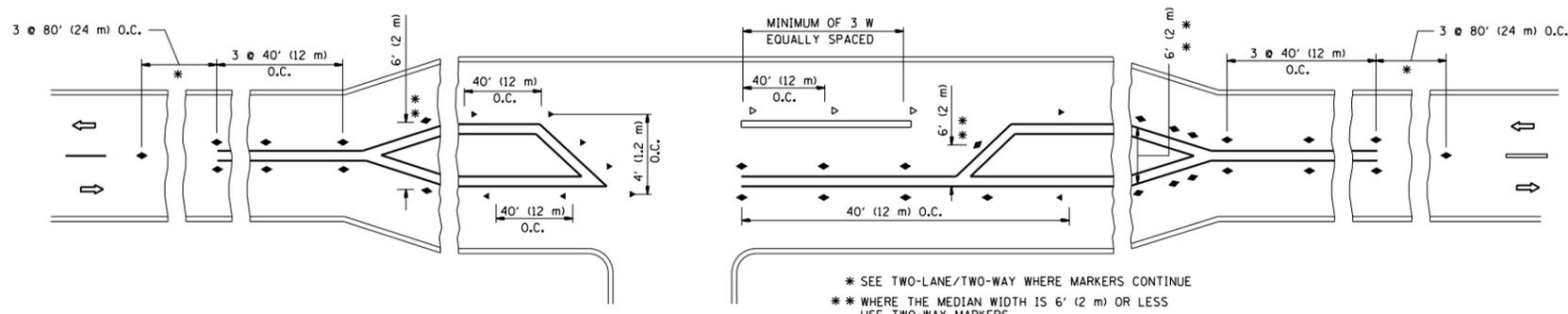
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

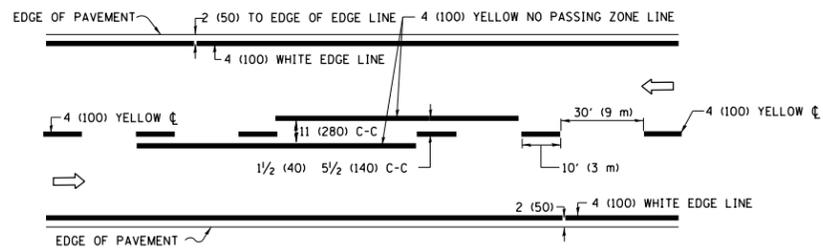


LEFT TURN

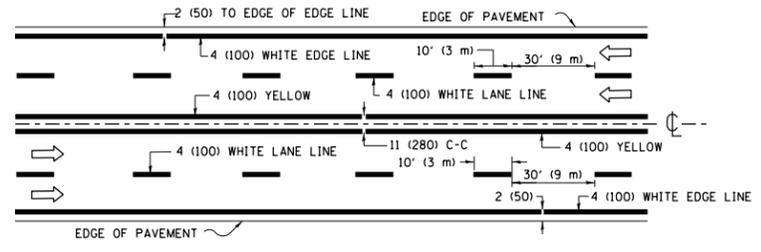
\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

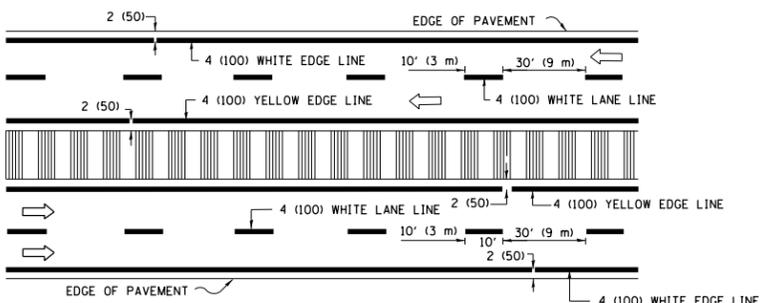
FILE NAME = p:\11\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI2511\Drawings\Design\DistStd.dgn	USER NAME = abebawa	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>			F.A.P. RTE. 349	SECTION (10 & 11) RS-9	COUNTY KANE	TOTAL SHEETS 18	SHEET NO. 14
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED - T. RAMMACHER 01-06-00					<b>TC-11</b>		<b>CONTRACT NO. 62F27</b>		
PLOT DATE = 1/26/2018	DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					



**2-LANE ROADWAY**

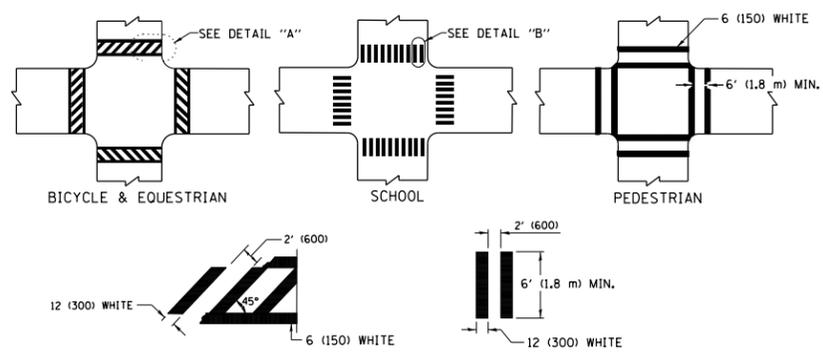


**MULTI-LANE UNDIVIDED**



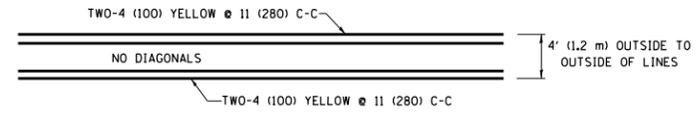
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

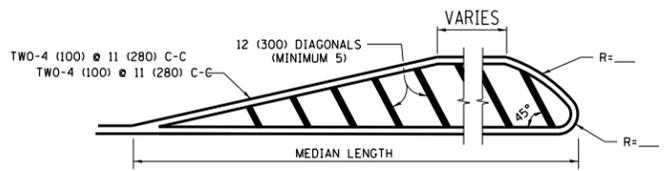


**TYPICAL CROSSWALK MARKING**

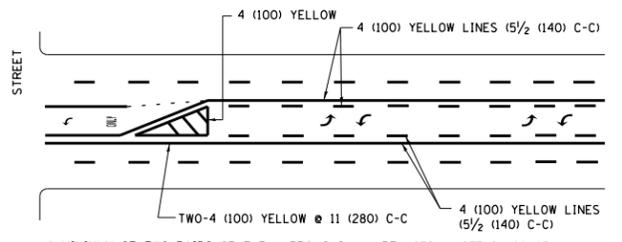
\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



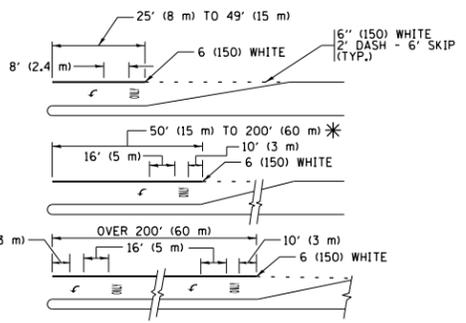
**4' (1.2 m) WIDE MEDIANS ONLY**



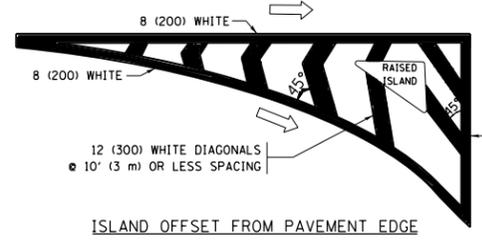
**MEDIANS OVER 4' (1.2 m) WIDE**



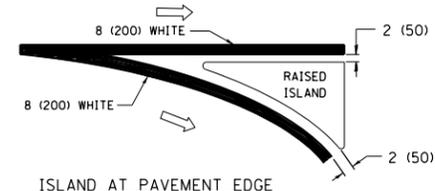
**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**



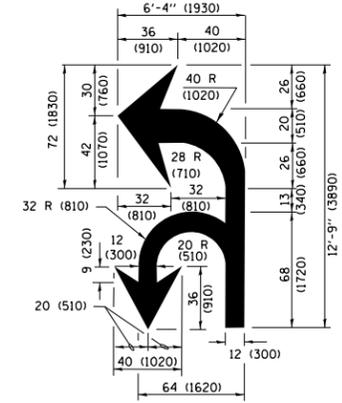
**TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING**



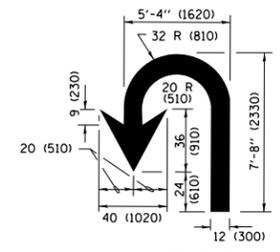
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

**LANE REDUCTION TRANSITION**  
\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (22.5 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

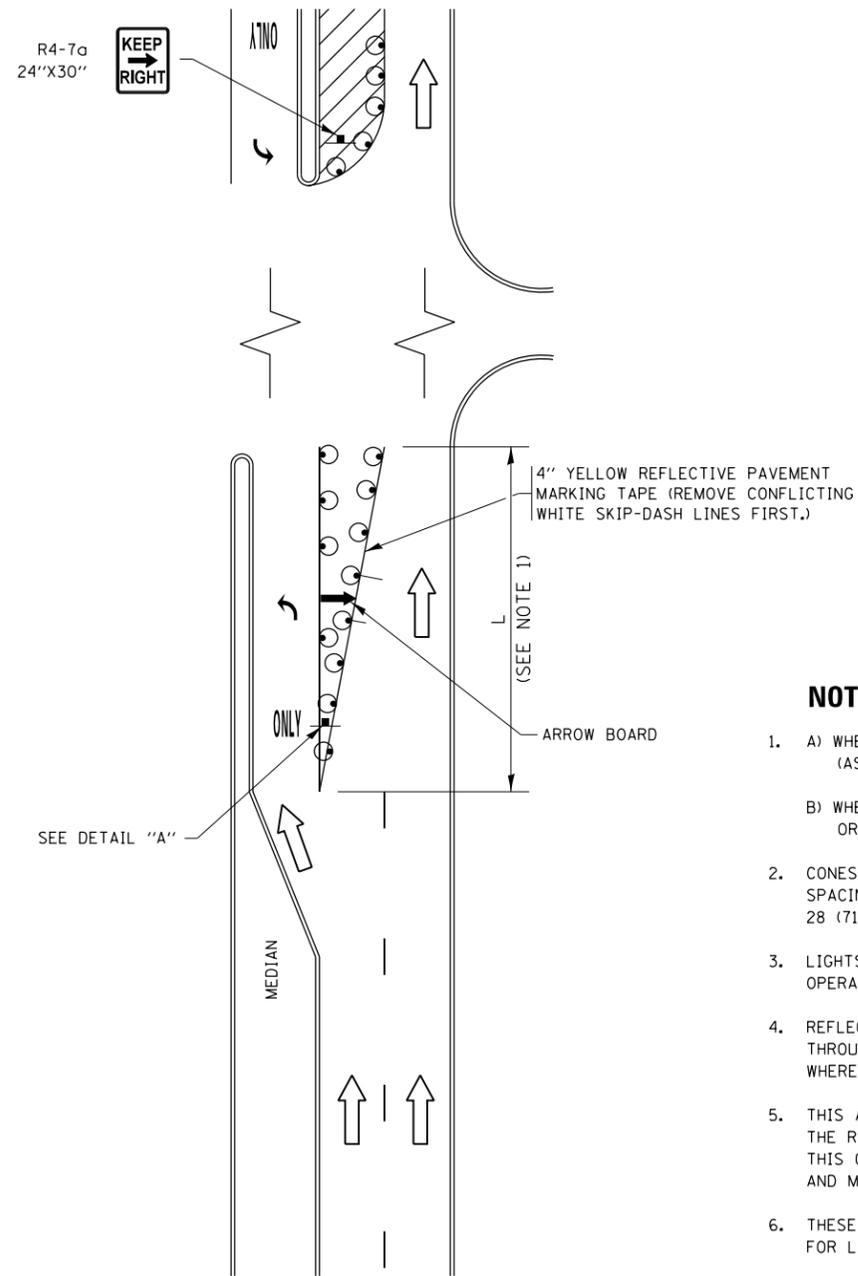
FILE NAME =	USER NAME = abebawa	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI2511\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 1/26/2018		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	

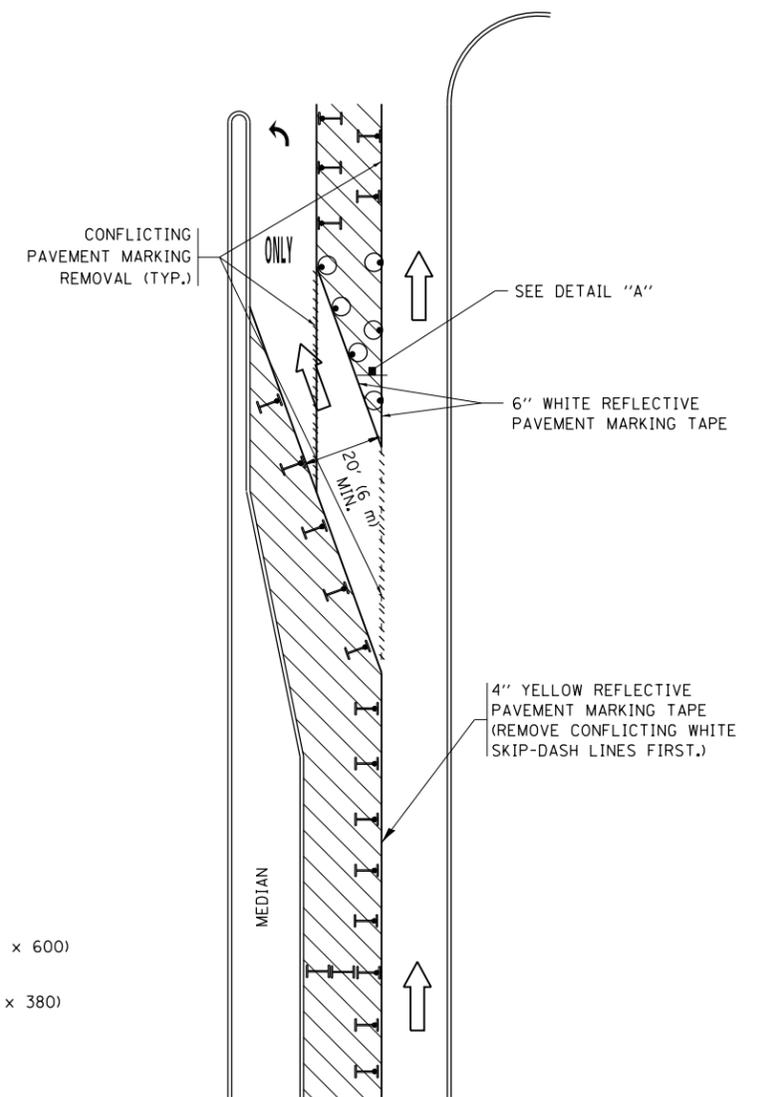
F.A.P. RTE. 349	SECTION (10 & 11) RS-9	COUNTY KANE	TOTAL SHEETS 18	SHEET NO. 15
<b>TC-13</b>		CONTRACT NO. 62F27	ILLINOIS FED. AID PROJECT	

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

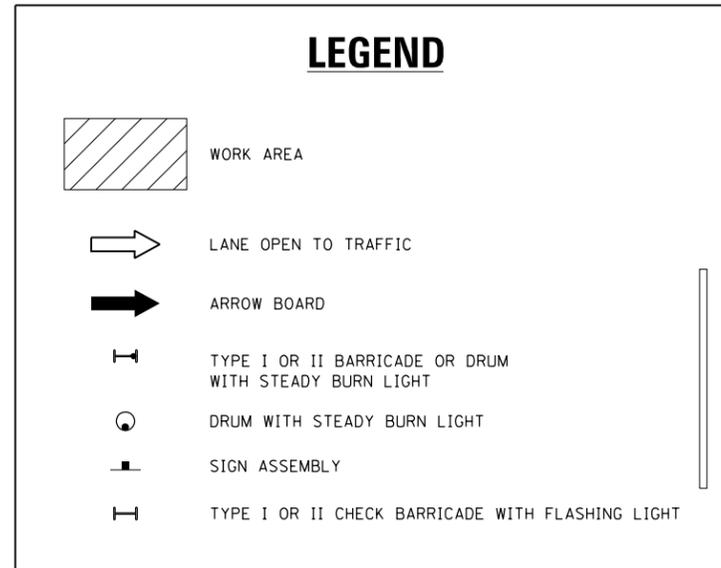


**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE

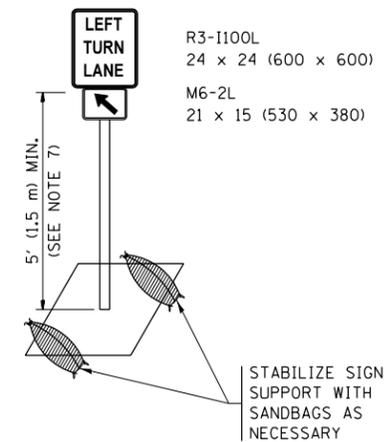


**FIGURE 2**



### NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

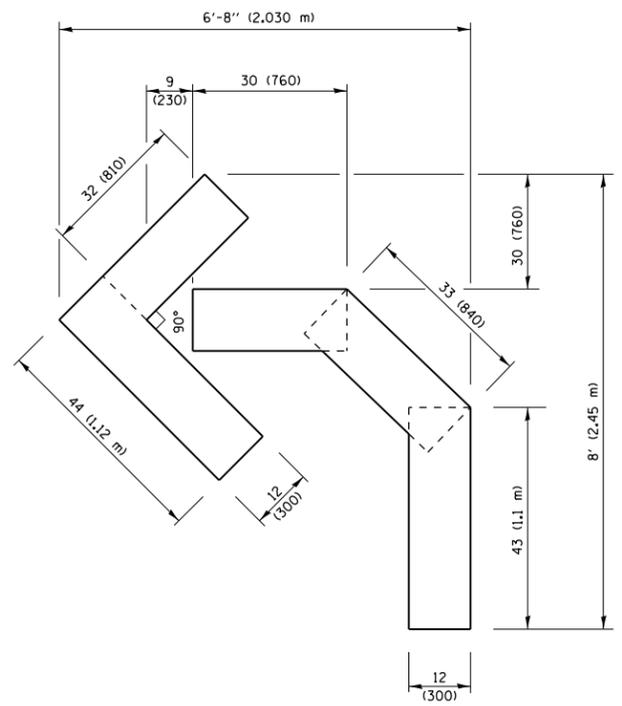


**DETAIL A**

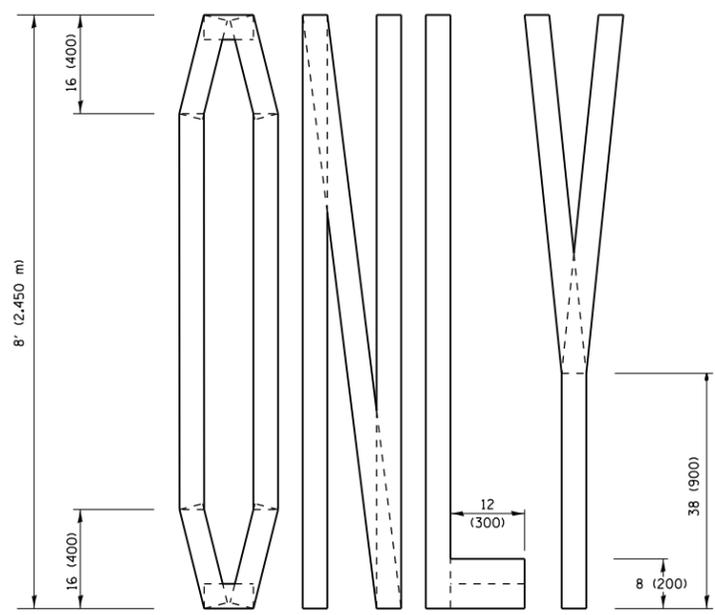
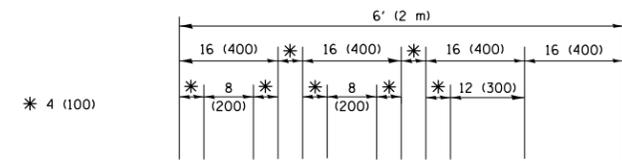
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = abebawa	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\11251111\REVISED Design\11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			349	(10 & 11) RS-9	KANE	18	16
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			<b>TC-14</b>		<b>CONTRACT NO. 62F27</b>		
	PLOT DATE = 1/26/2018	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.

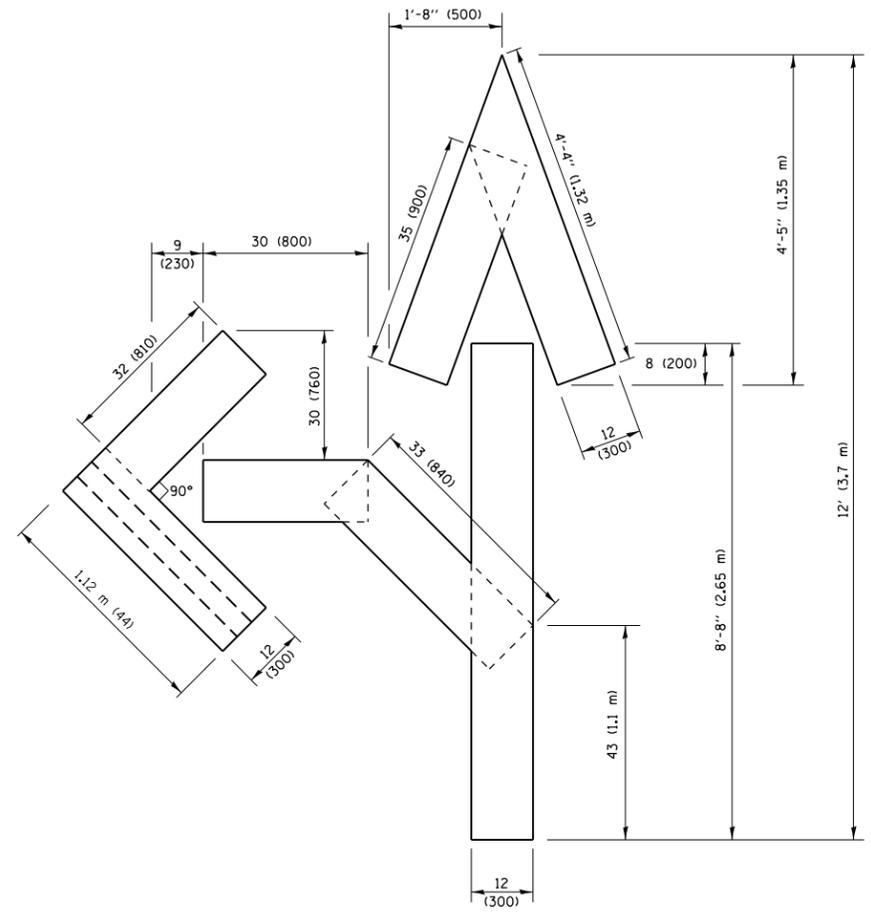
ILLINOIS FED. AID PROJECT



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

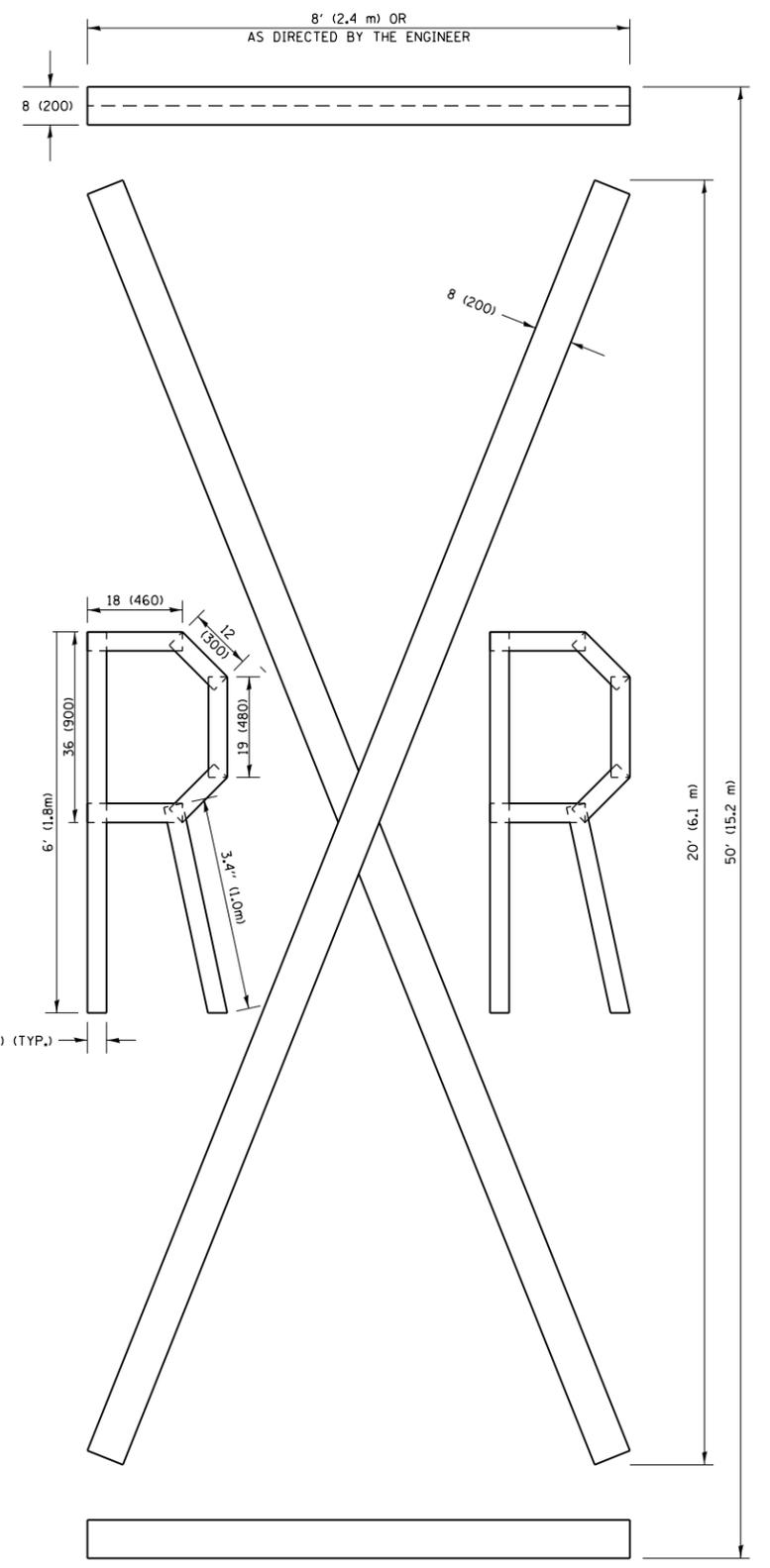


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

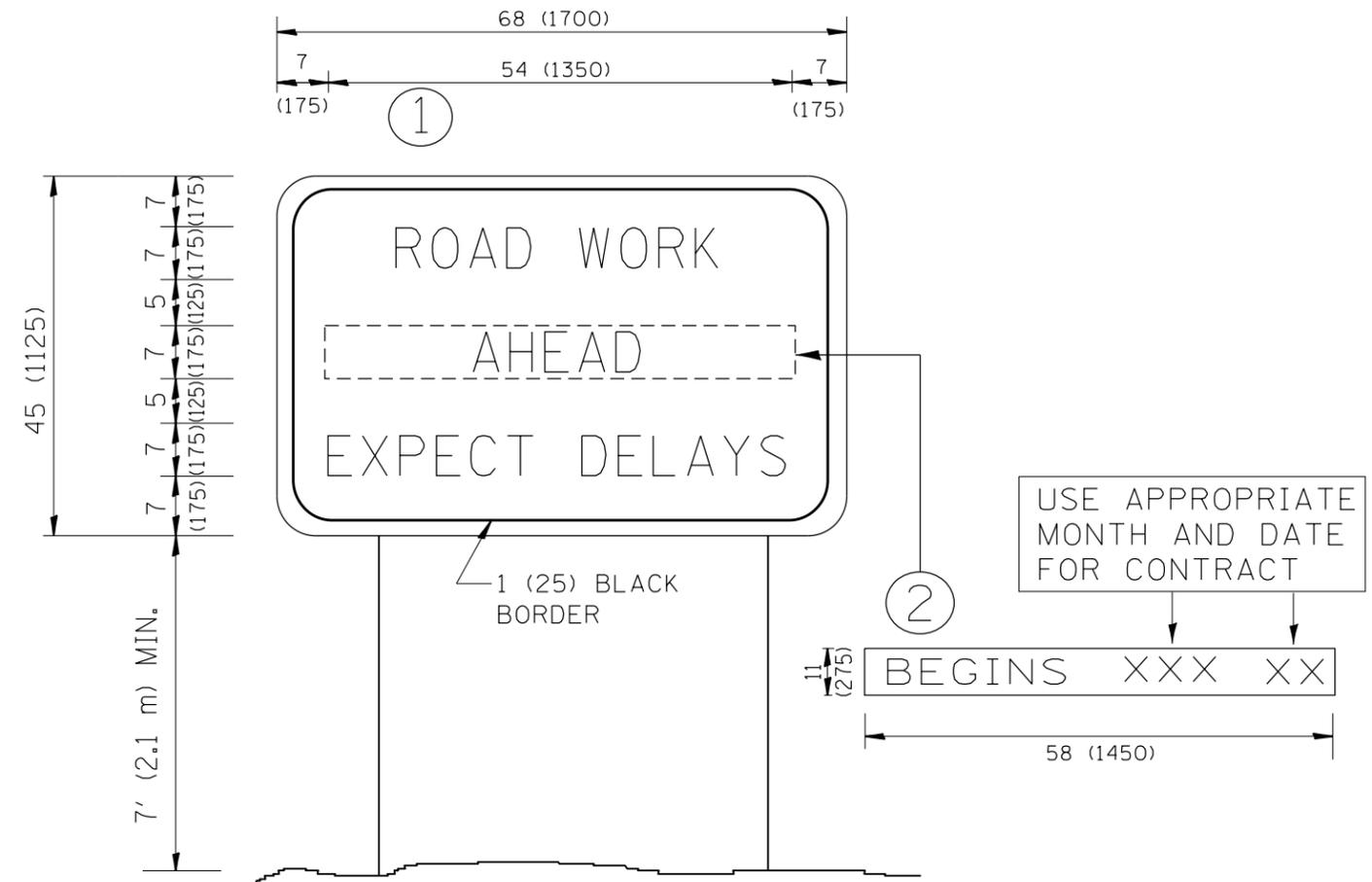
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = abebawa	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
p:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\0125\1111\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11) RS-9	KANE	18	17
<b>TC-16</b>		<b>CONTRACT NO. 62F27</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abebawa	DESIGNED -	REVISED - R. MIRS 09-15-97
pw:\IL\084EBIDINTEG.illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI2511\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - R. MIRS 12-11-97
		DATE -	REVISED - T. RAMMACHER 02-02-99
			REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
349	(10 & 11) RS-9	KANE	18	18
TC-22		CONTRACT NO. 62F27		
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				