

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-RS	COOK	63	1
ILLINOIS			CONTRACT NO. 62D51	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED HIGHWAY PLANS

FOR INDEX OF SHEETS SEE SHEET NO.2

THE PROJECT IS LOCATED IN THE CITIES OF DES PLAINES, PARK RIDGE, CHICAGO, AND THE VILLAGE OF ROSEMONT

**F.A.U. 1350: IL ROUTE 72 (HIGGINS ROAD)
TOUHY AVENUE TO CUMBERLAND AVENUE;
F.A.I. 190: I-190 RAMPS AT US 12/45
SECTION: 2016-058-RS
RESURFACING (3P); ADA RAMPS
PROJECT: STP-NHPP - TOIK (586)
COOK COUNTY
C-91-045-17**

**TRAFFIC DATA:
IL 72 (HIGGINS):
2017 ADT = 26,000
POSTED SPEED LIMIT = 35 - 45 MPH**

**I-190 AND US 12/45 RAMPS:
2017 ADT = 13,000
POSTED SPEED LIMIT = 25 - 35 MPH**

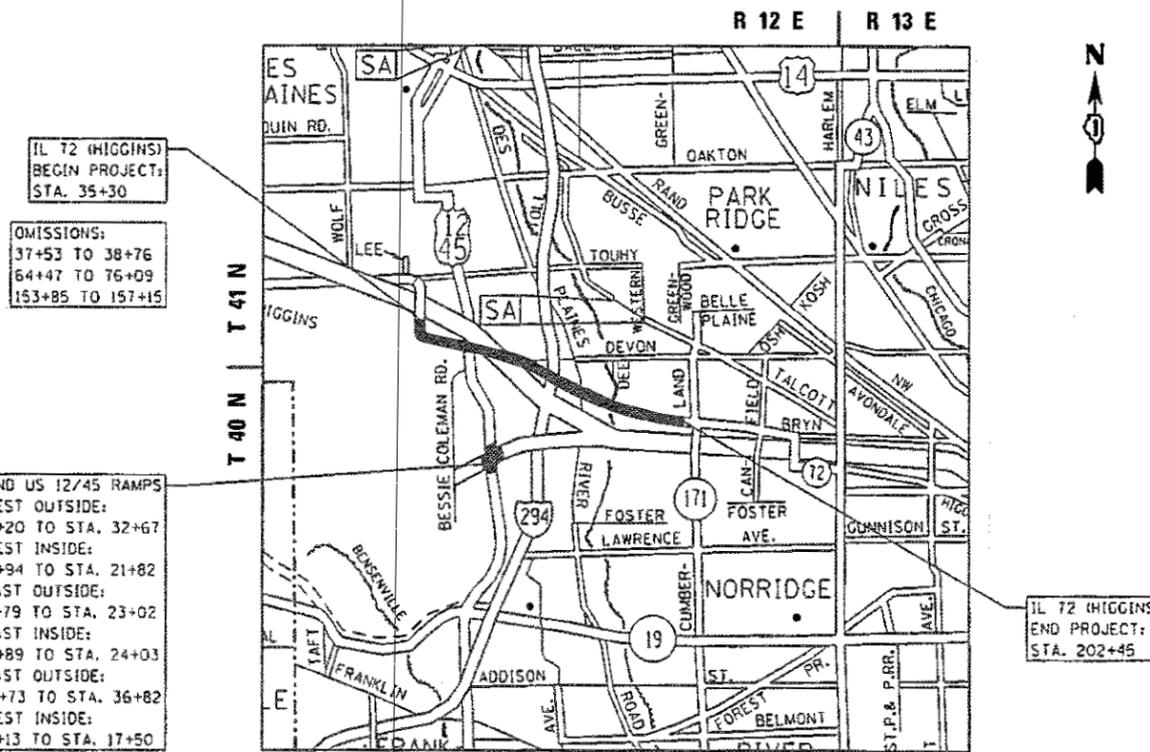


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

**J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811**

**PROJECT ENGINEER: JENPAI CHANG (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247**

CONTRACT NO. 62D51



IL 72 (HIGGINS)
BEGIN PROJECT:
STA. 35+30

OMISSIONS:
37+53 TO 38+76
64+47 TO 76+09
153+85 TO 157+15

I-190 AND US 12/45 RAMPS
NORTHWEST OUTSIDE:
STA. 15+20 TO STA. 32+67
NORTHWEST INSIDE:
STA. 12+94 TO STA. 21+82
NORTHEAST OUTSIDE:
STA. 11+79 TO STA. 23+02
NORTHEAST INSIDE:
STA. 17+89 TO STA. 24+03
SOUTHEAST OUTSIDE:
STA. 27+73 TO STA. 36+82
SOUTHWEST INSIDE:
STA. 12+13 TO STA. 17+50

IL 72 (HIGGINS)
END PROJECT:
STA. 202+45

MAINE, JEFFERSON, AND LEYDEN TOWNSHIPS

GROSS LENGTH = 22533.00 FT = 4.268 MI
NET LENGTH = 20918.00 FT = 3.962 MI

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED February 1, 2018

Anthony J. Buzgala / 1/18
REGIONAL ENGINEER

Mar 23, 2018

Chris [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

Mar 23, 2018

Paul [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

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LIST OF STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-10	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-03	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-04	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-04	DEPRESSED CORNER FOR SIDEWALKS
424026-02	ENTRANCE/ALLEY PEDESTRIAN CROSSINGS
424031-01	MEDIAN PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
602001-02	CATCH BASIN, TYPE A
602401-04	MANHOLE, TYPE A
604001-04	FRAME AND LIDS, TYPE 1
604051-04	FRAME AND GRATE, TYPE 11
604091-03	FRAME AND GRATE, TYPE 24
606001-07	CONCRETE CURB TYPE B COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
630001-12	STEEL PLATE BEAM GUARDRAIL
631046-04	TRAFFIC BARRIER TERMINAL, TYPE 10
635001-02	DELINEATORS
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≥ 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤ 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
814001-03	HANDHOLES
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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	PLOT SCALE = 100.0000" / in.	CHECKED -	REVISED -
Default	PLOT DATE = 2/22/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS AND STANDARDS
IL 72 (TOUHY AVENUE TO CUMBERLAND AVE)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-RS	COOK	63	2
			CONTRACT NO. 62D51	
		ILLINOIS	FED. AID PROJECT	

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
2. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITIES OF DES PLAINES, PARK RIDGE, CHICAGO, AND VILLAGE OF ROSEMONT.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
6. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
7. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
8. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
9. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
10. DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
11. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
12. FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
13. SIDEWALK REMOVAL AND P.C.C. SIDEWALK 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
14. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
15. THE ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC FIELD ENGINEER AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
16. THE ENGINEER SHALL CONTACT MICHAEL J. RAIMONDI, DIRECTOR OF PUBLIC WORKS OF VILLAGE OF ROSEMONT, A MINIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING OF CONSTRUCTION.
17. WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED THEIR LOCATION.
18. THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF CONSTRUCTION.
19. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
20. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
21. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPERATE PAY ITEM HAS BEEN PROVIDED.
22. FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
23. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
24. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
25. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
26. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (45 KM/H) OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (45 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).
27. OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.
28. THE RESIDENT ENGINEER SHALL VERIFY ALL EXISTING PAVEMENT MARKINGS BEFORE MILLING.
29. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.
30. TREE REMOVAL (6 TO 15 UNITS DIAMETER) IS TO BE USED AS DIRECTED BY THE ENGINEER IN AREAS WHERE NO OTHER FORESTRY WORK IS SPECIFIED.
31. CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING FORESTRY WORK LAYOUT.
32. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
33. THE REMOVAL OF GUARDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT OF "GUARDRAIL REMOVAL".

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS AND STANDARDS		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\11084EB\INTEG.illinois.gov\PWD\DOT\Documents\DOT Offices\District 1\Projects\104517\CADD\Drawings\7-shr-genr00e.dgn		CHECKED -	REVISED -		IL 72 (TOUHY AVENUE TO CUMBERLAND AVE)		1350	2106-058-RS	COOK	63	3
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.		CONTRACT NO. 62D51				
PLOT DATE = 2/22/2018					ILLINOIS FED. AID PROJECT						

URBAN NHPP STP

URBAN NHPP STP

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 72 0005	I-190 RAMPS 0005		
				ROADWAY	ROADWAY		
				80% FEDERAL 20% STATE	80% FEDERAL 20% STATE		
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	100	100			
20200100	EARTH EXCAVATION	CU YD	52	52			
21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	43	43			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	5	5			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	5	5			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	5	5			
25100115	MULCH, METHOD 2	ACRE	1.4	1.4			
25200110	SODDING, SALT TOLERANT	SQ YD	383	383			
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	139	139			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	84570	69567	15003		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	155	155			
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	5967	4439	1528		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	685	685			
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	4	4			

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE			
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 72 0005	I-190 RAMPS 0005		
				ROADWAY	ROADWAY		
				80% FEDERAL 20% STATE	80% FEDERAL 20% STATE		
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	735		735		
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	11854	10100	1754		
42001300	PROTECTIVE COAT	SO YD	1101	1101			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2821	2821			
42400800	DETECTABLE WARNINGS	SO FT	237	237			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	96223	96223			
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SO YD	6840	6840			
44000162	HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"	SO YD	22227		22227		
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	138	138			
44000300	CURB REMOVAL	FOOT	23	23			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	121	121			
44000600	SIDEWALK REMOVAL	SO FT	2431	2431			
44003100	MEDIAN REMOVAL	SO FT	630	630			

* SPECIALTY ITEM
NON-PARTICIPATING ITEM (100% STATE)

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URBAN NHPP STP

URBAN NHPP STP

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 72	I-190 RAMP			
				0005	0005			
				ROADWAY	ROADWAY			
				80% FEDERAL	80% FEDERAL			
				20% STATE	20% STATE			
44004250	PAVED SHOULDER REMOVAL	SO YD	4.9	4.9				
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	3106	3074	32			
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	160	80	80			
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	1040	120	920			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	179	51	128			
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	8	6	2			
60255500	MANHOLES TO BE ADJUSTED	EACH	1	1				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	21	18	3			
60404800	FRAMES AND GRATES, TYPE 11	EACH	4	4				
60404940	FRAMES AND GRATES, TYPE 23	EACH	1		1			
60404950	FRAMES AND GRATES, TYPE 24	EACH	14	14				
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	2	2				
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	3	2	1			
60600605	CONCRETE CURB, TYPE B	FOOT	23	23				
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	56	56				

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 72	I-190 RAMP			
				0005	0005			
				ROADWAY	ROADWAY			
				80% FEDERAL	80% FEDERAL			
				20% STATE	20% STATE			
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	65	65				
60618320	CONCRETE MEDIAN SURFACE, 6 INCH	SO FT	141	141				
60619600	CONCRETE MEDIAN, TYPE SB-6.12	SO FT	120	120				
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	37.5	37.5				
* 63100105	TRAFFIC BARRIER TERMINAL, TYPE 10	EACH	1	1				
63200310	GUARDRAIL REMOVAL	FOOT	37.5	37.5				
63500105	DELINEATORS	EACH	127		127			
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	52	52				
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2				
67100100	MOBILIZATION	LSUM	1	1				
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	LSUM	1	1				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	LSUM	1	1				

* SPECIALTY ITEM
* NON-PARTICIPATING ITEM (100% STATE)

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL 72 (TOUHY AVENUE TO CUMBERLAND AVE)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-RS	COOK	63	5
SCALE:			SHEET NO. OF SHEETS	STA. TO STA.
FED. ROAD DIST. NO. 1			ILLINOIS	FED. AID PROJECT

CONTRACT NO. 62D51

URBAN NHPP STP

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SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 72	I-190 RAMP		
				0005	0005		
				ROADWAY	ROADWAY		
				80% FEDERAL 20% STATE	80% FEDERAL 20% STATE		
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1			
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	LSUM	1	1			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1			
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	51524	51524			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	17175	17175			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	1898	1898			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	43165	43165			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	8562	8562			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	783	783			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1639	1639			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1122	1122			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	25762	25762			

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 72	I-190 RAMP		
				0005	0005		
				ROADWAY	ROADWAY		
				80% FEDERAL 20% STATE	80% FEDERAL 20% STATE		
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	1898	1898			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	54987	43165	11822		
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	8562	8562			
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	783	783			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1639	1639			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1122	1122			
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	108	108			
* 78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	89	89			
* 78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	21	21			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1590	1590			
* 78200006	GUARDRAIL REFLECTORS, TYPE B	EACH	2	2			

* SPECIALTY ITEM
NON-PARTICIPATING ITEM (100% STATE)

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URBAN NHPP STP

URBAN NHPP STP

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 72	I-190 RAMP			
				0005	0005	ROADWAY	ROADWAY	
				80% FEDERAL	80% FEDERAL			
				20% STATE	20% STATE			
78300200	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1272	1272				
	REMOVAL							
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL	EACH	5	5				
	INSTALLATION							
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	3655	3655				
* 89500400	RELOCATE EXISTING PEDESTRIAN	EACH	6	6				
	PUSH-BUTTON							
* 89502376	REBUILD EXISTING HANDHOLE	EACH	3	3				
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1				
X0327979	PAVEMENT MARKING REMOVAL - GRINDING	SO FT	57	57				
X0327980	PAVEMENT MARKING REMOVAL - WATER	SO FT	123	123				
	BLASTING							
X2010350	TREE REMOVAL, ACRES (SPECIAL)	ACRE	1.4	1.4				
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	90	26	64			
X2501800	SEEDING, CLASS 4 (MODIFIED)	ACRE	1.4	1.4				
# X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	70		70			
X6030310	FRAMES AND LIDS TO BE ADJUSTED	EACH	100	87	13			
	(SPECIAL)							

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	IL 72	I-190 RAMP			
				0005	0005	ROADWAY	ROADWAY	
				80% FEDERAL	80% FEDERAL			
				20% STATE	20% STATE			
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	90		90			
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	33559	33559				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER	FOOT	2756	996	1760			
	REMOVAL AND REPLACEMENT							
# Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	181	165	16			
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	154.2	154.2				
Ø Z0076600	TRAINEES	HOOR	500	500				
Ø Z0076604	TRAINEES- TRAINING PROGRAM GRADUATE	HOOR	500	500				

* SPECIALTY ITEM
NON-PARTICIPATING ITEM (100% STATE)
Ø 0042

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
pw:\IL084EBID\INTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\ID104517\CADData\Design\DRANN7-shl-SOQ.dgn		CHECKED -	REVISED -
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -
PLOT DATE = 2/2/2018			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

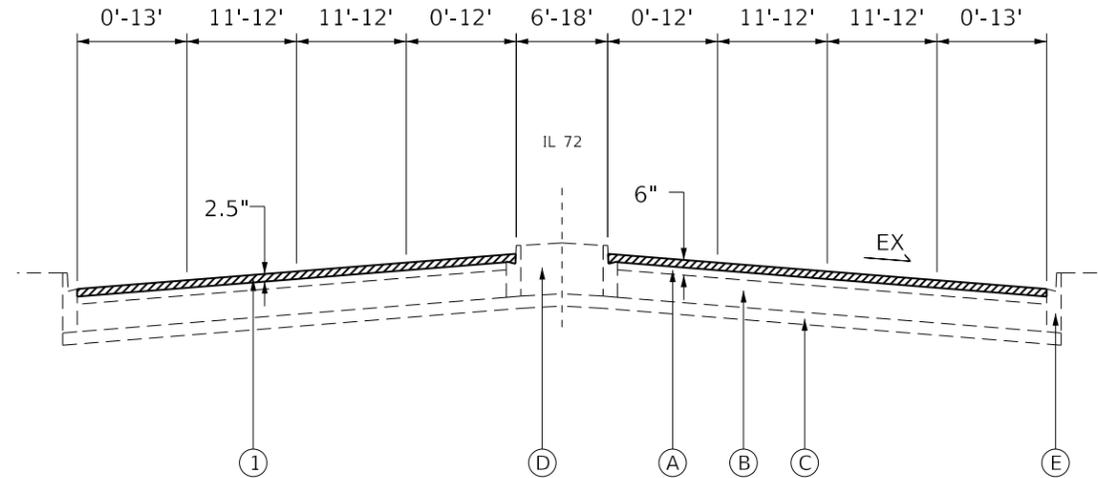
SUMMARY OF QUANTITIES
IL 72 (TOUHY AVENUE TO CUMBERLAND AVE)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-RS	COOK	63	7
CONTRACT NO. 62D51				
SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

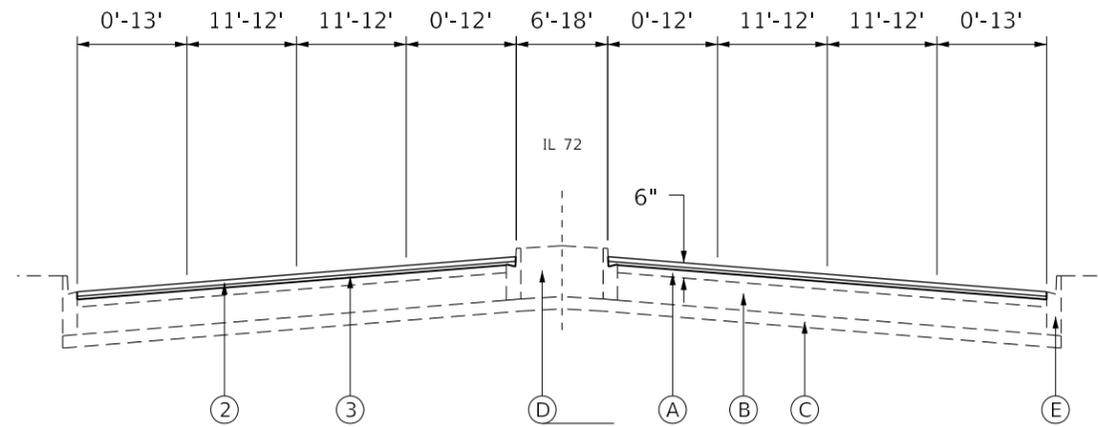
LEGEND

- (A) EXISTING HMA SURFACE COURSE
- (B) EXISTING P.C.C PAVEMENT, 9"
- (C) EXISTING SUB-BASE GRANULAR MATERIAL
- (D) EXISTING MEDIAN
- (E) EXISTING CURB AND GUTTER
- (F) EXISTING AGGREGATE SHOULDER
- (G) EXISTING SIDEWALK, 5"
- (H) EXISTING GUARDRAIL
- (I) EXISTING P.C.C. PAVEMENT, 10"
- (J) EXISTING PIPE UNDERDRAINS, 4"
- (K) EXISTING BITUMINOUS SHOULDER AT EXISTING GUARDRAIL, 6"

- (L) EXISTING FA-2, 3'
- (M) EXISTING BITUMINOUS BASE COURSE, 10"
- (N) EXISTING BITUMINOUS SHOULDER, 10"
- (O) EXISTING TOPSOIL
- (1) PROPOSED HMA SURFACE REMOVAL
- (2) PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
- (3) PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (4) PROPOSED POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1 1/4"
- (5) AGGREGATE WEDGE SHOULDER, TYPE B
- (6) PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 2"
- (7) PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"



EXISTING TYPICAL SECTION
IL 72 (TORRENCE AVENUE)
STA. 13+62 TO 15+40
STA. 38+76 TO 64+47



PROPOSED TYPICAL SECTION
IL 72 (TORRENCE AVENUE)
STA. 13+62 TO 15+40
STA. 38+76 TO 64+47

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE TYPE	AIR VOIDS (%) @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)	IC
PAVEMENT RESURFACING: IL 72			
POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1 3/4" (IL-9.5mm)	4% @ 70 GYR.	PFP	
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1 1/4", 3/4"	3.5% @ 50 GYR.	QCP	9 PASSES
PAVEMENT RESURFACING: I-190 RAMPS AT US 12/ US 45			
HMA SURFACE COURSE, MIX "D", N70, 2" (IL-9.5mm)	4% @ 70GYR.	QC/QA	
POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 2" (IL-9.5mm)	4% @ 70 GYR.	PFP	
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1 1/4"	3.5% @ 50 GYR.	QCP	9 PASSES
PATCHING			
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/QA	
DRIVEWAYS			
HMA SURFACE COURSE, MIX "D", N50, 2" (IL-9.5mm)	4% @ 50GYR.	QC/QA	
HMA BASE COURSE, 8" (HMA BINDER IL-19.0mm)	4% @ 50GYR.	QC/QA	
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)			

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL SURFACE HOT-MIX ASPHALT MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE SBS/SBR "PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
- NOTE 3: FOR USE OF RECYCLED MATERIALS, SEE DISTRICT ONE SPECIAL PROVISION.
- NOTE 4: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.
- NOTE 5: THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING.
- NOTE 6: WHEN A NUMBER OF ROLLER PASSES IS SPECIFIED, THE CONTRACTOR MAY OPT TO USE INTELLIGENT COMPACTION (IC) IN LIEU OF DENSITY TESTING UNDER THE QUALITY CONTROL FOR PERFORMANCE (QCP) PROGRAM.

STA. 13+62 TO 15+40
STA. 38+76 TO 64+47

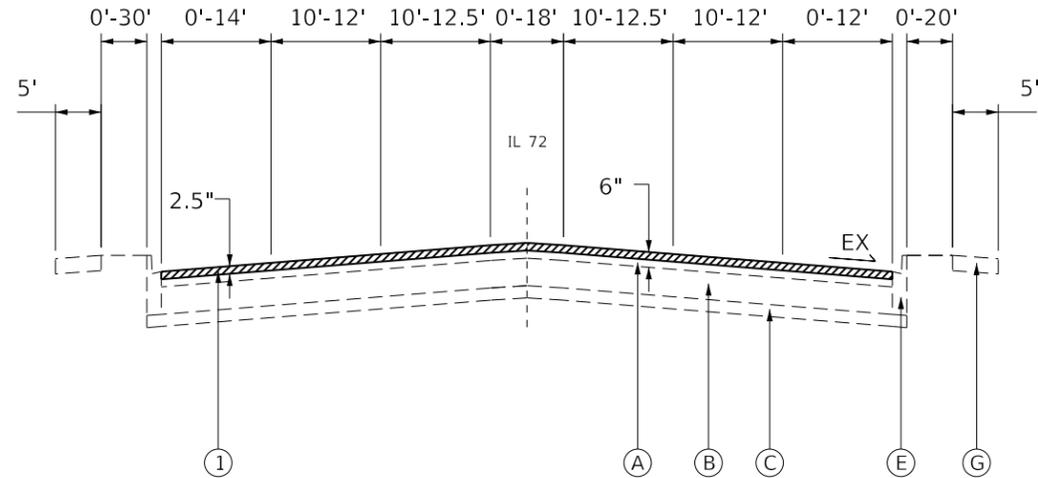
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pwz\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\104517\CADD\Drawings\17-shr-typical.dgn		CHECKED -	REVISED -			1350	2106-058-RS	COOK	63	8	
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 62D51					
Default						SCALE:	SHEET	OF	SHEETS	STA.	TO

ILLINOIS FED. AID PROJECT

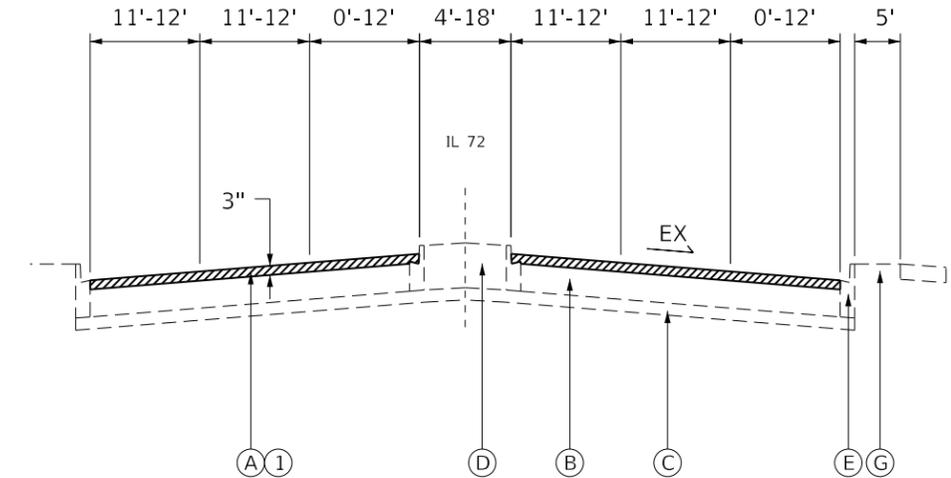
LEGEND

- (A) EXISTING HMA SURFACE COURSE
- (B) EXISTING P.C.C PAVEMENT, 9"
- (C) EXISTING SUB-BASE GRANULAR MATERIAL
- (D) EXISTING MEDIAN
- (E) EXISTING CURB AND GUTTER
- (F) EXISTING AGGREGATE SHOULDER
- (G) EXISTING SIDEWALK, 5"
- (H) EXISTING GUARDRAIL
- (I) EXISTING P.C.C. PAVEMENT, 10"
- (J) EXISTING PIPE UNDERDRAINS, 4"
- (K) EXISTING BITUMINOUS SHOULDER AT EXISTING GUARDRAIL, 6"

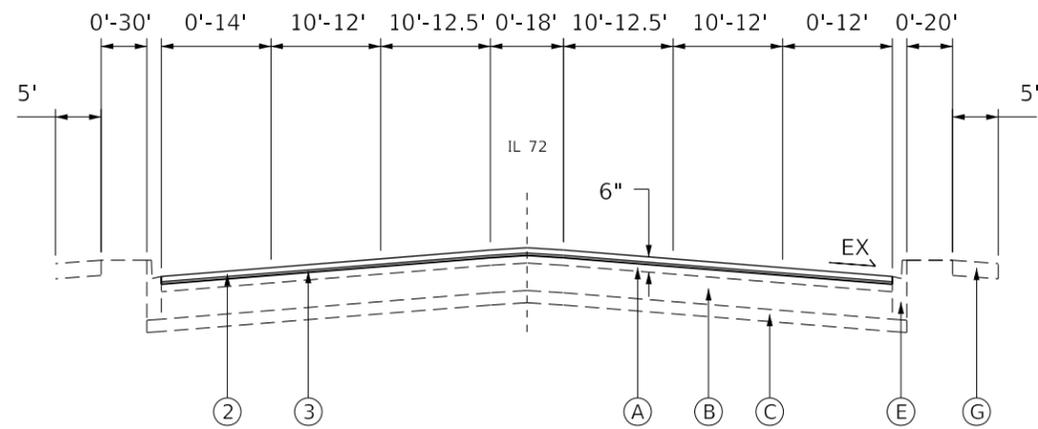
- (L) EXISTING FA-2, 3'
- (M) EXISTING BITUMINOUS BASE COURSE, 10"
- (N) EXISTING BITUMINOUS SHOULDER, 10"
- (O) EXISTING TOPSOIL
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- (7) PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"



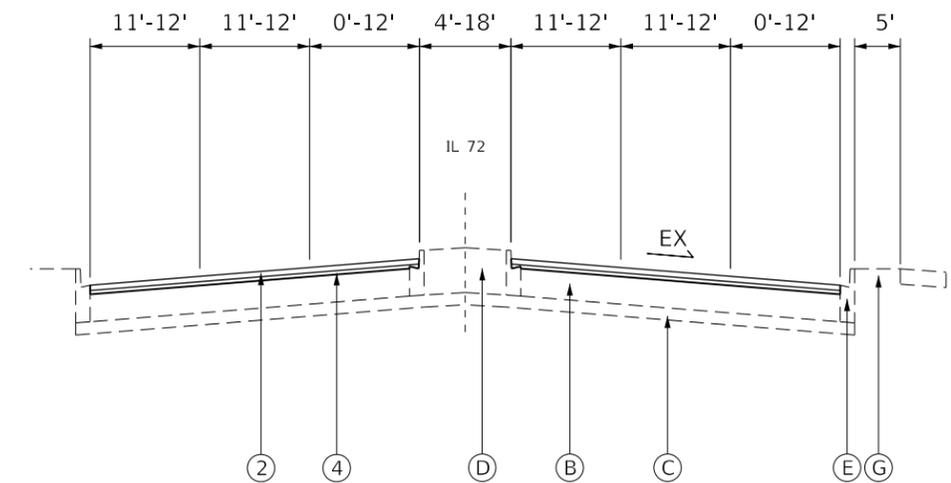
EXISTING TYPICAL SECTION
 IL 72 (TORRENCE AVENUE)
 STA. 35+30 TO 37+53
 STA. 76+09 TO 145+00
 STA. 157+13 TO 162+00
 STA. 176+50 TO 202+45



EXISTING TYPICAL SECTION
 IL 72 (TORRENCE AVENUE)
 STA. 145+00 TO 153+85



PROPOSED TYPICAL SECTION
 IL 72 (TORRENCE AVENUE)
 STA. 35+30 TO 37+53
 STA. 76+09 TO 145+00
 STA. 157+13 TO 162+00
 STA. 176+50 TO 202+45



PROPOSED TYPICAL SECTION
 IL 72 (TORRENCE AVENUE)
 STA. 145+00 TO 153+85

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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Default	DATE -	REVISED -	
	PLOT DATE = 2/2/2018		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED TYPICAL SECTIONS
 IL 72 (TOUHY AVENUE TO CUMBERLAND AVE)**

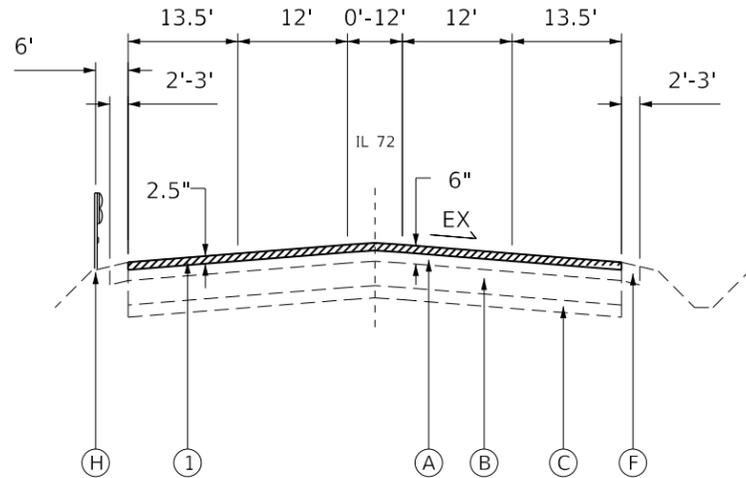
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-RS	COOK	63	9
			CONTRACT NO. 62D51	
		ILLINOIS	FED. AID PROJECT	

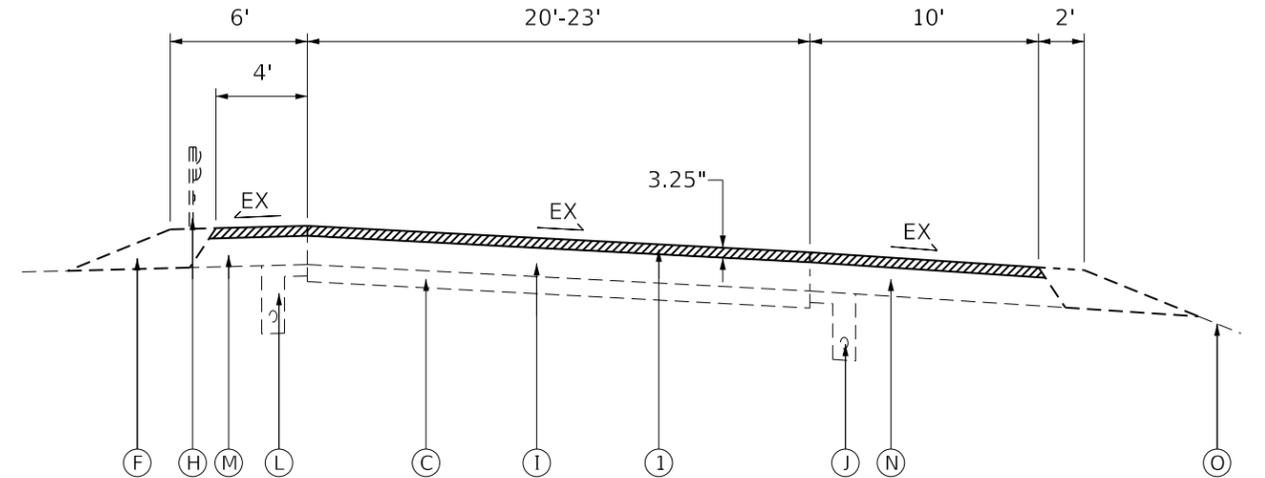
LEGEND

- (A) EXISTING HMA SURFACE COURSE
- (B) EXISTING P.C.C PAVEMENT, 9"
- (C) EXISTING SUB-BASE GRANULAR MATERIAL
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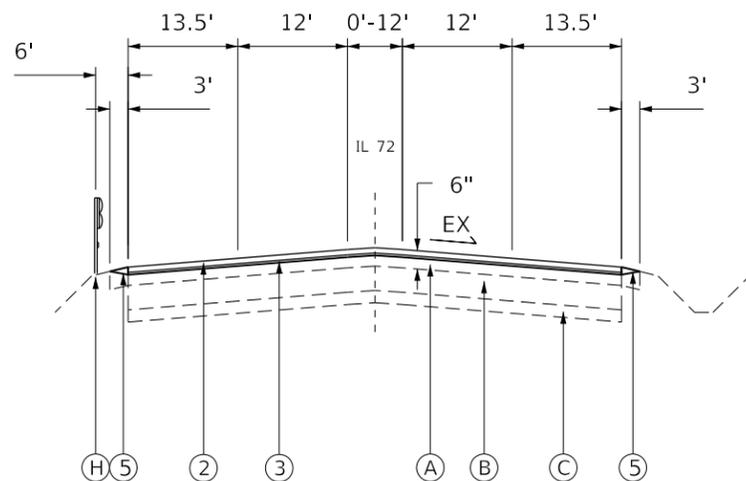
- (L) EXISTING FA-2, 3'
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- (N) EXISTING BITUMINOUS SHOULDER, 10"
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- (7) PROPOSED HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"



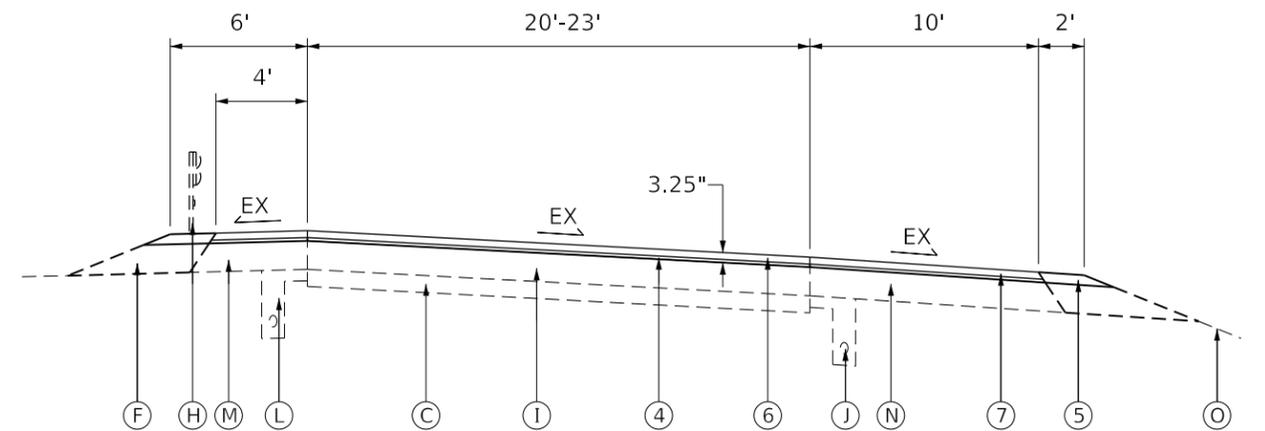
EXISTING TYPICAL SECTION
IL 72 (TORRENCE AVENUE)
162+00 TO 176+50



EXISTING TYPICAL SECTION
I-190 RAMPS AT US 12/ US 45 (MANHEIM ROAD)
NW OUTSIDE: STA. 15+20 TO 32+67 NE OUTSIDE: STA. 11+79 TO 23+02
NW INSIDE: STA. 12+94 TO 21+82 NE INSIDE: STA. 17+89 TO 24+03
SW INSIDE: STA. 12+13 TO 17+50 SE OUTSIDE: STA. 23+83 TO 36+82



PROPOSED TYPICAL SECTION
IL 72 (TORRENCE AVENUE)
162+00 TO 176+50



PROPOSED TYPICAL SECTION
I-190 RAMPS AT US 12/ US 45 (MANHEIM ROAD)
NW OUTSIDE: STA. 15+20 TO 32+67 NE OUTSIDE: STA. 11+79 TO 23+02
NW INSIDE: STA. 12+94 TO 21+82 NE INSIDE: STA. 17+89 TO 24+03
SW INSIDE: STA. 12+13 TO 17+50 SE OUTSIDE: STA. 23+83 TO 36+82

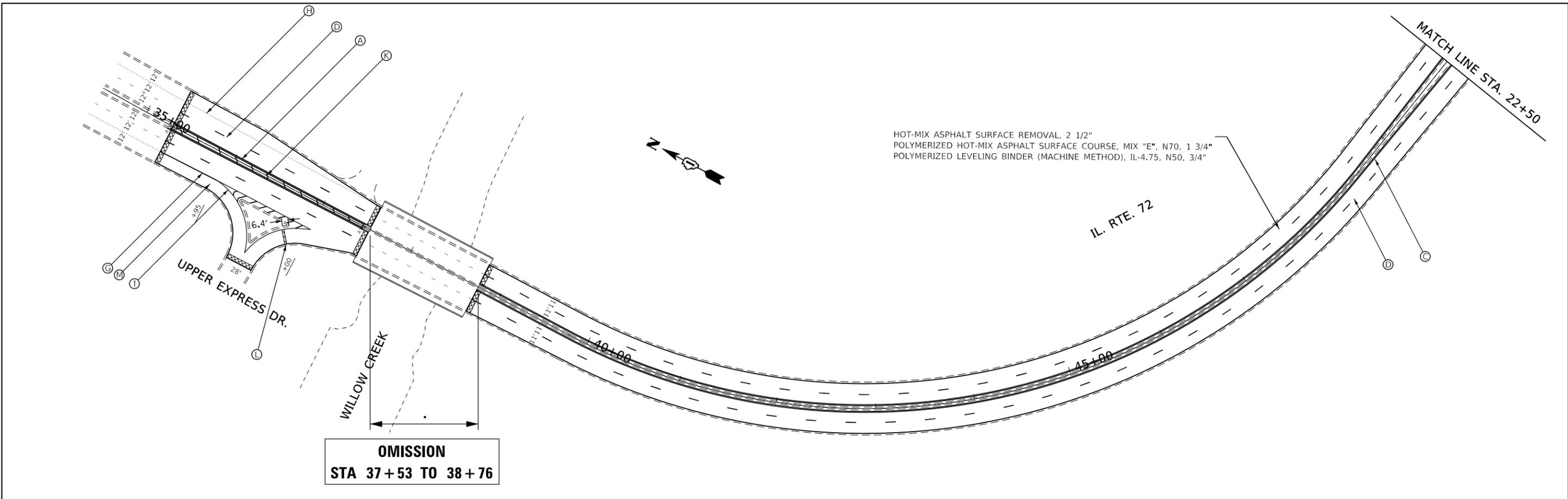
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	PLOT DATE = 2/2/2018		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

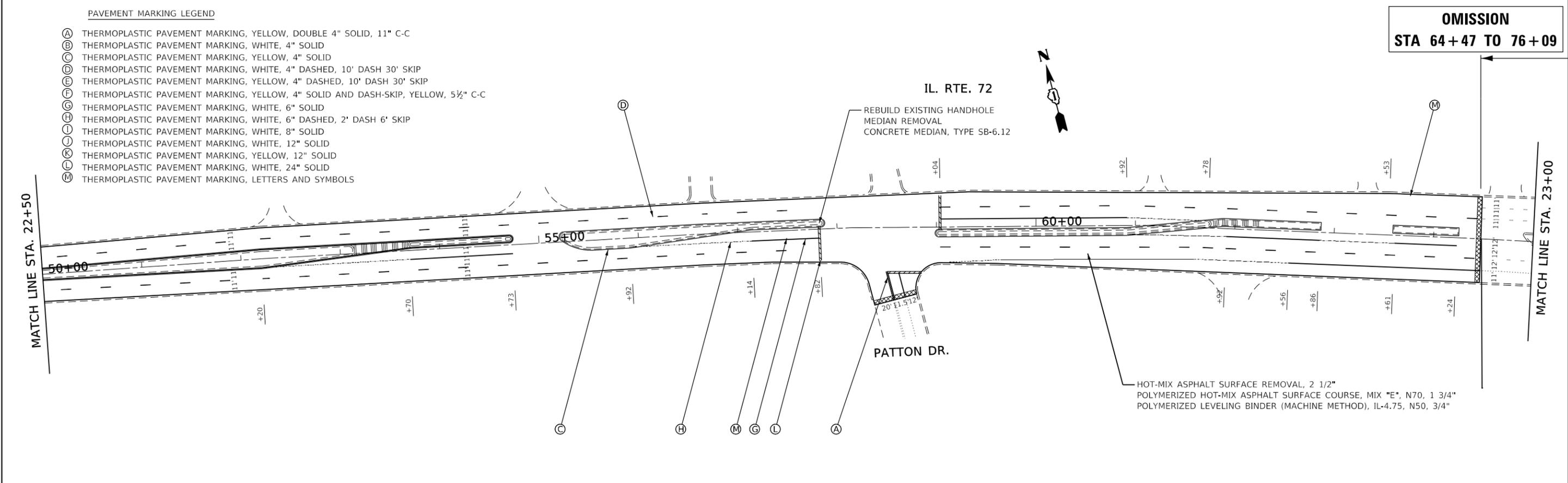
**EXISTING AND PROPOSED TYPICAL SECTIONS
IL 72 (TOUHY AVENUE TO CUMBERLAND AVE)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-RS	COOK	63	10
CONTRACT NO. 62D51				
ILLINOIS FED. AID PROJECT				

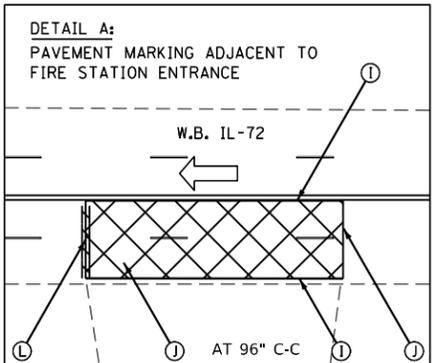
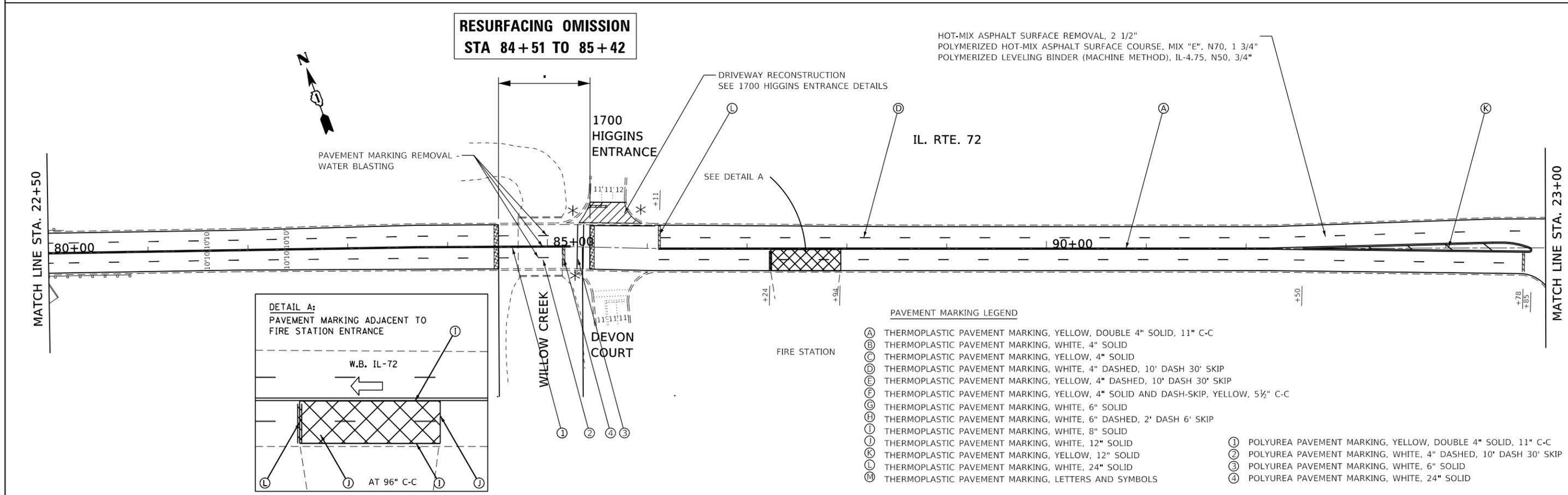
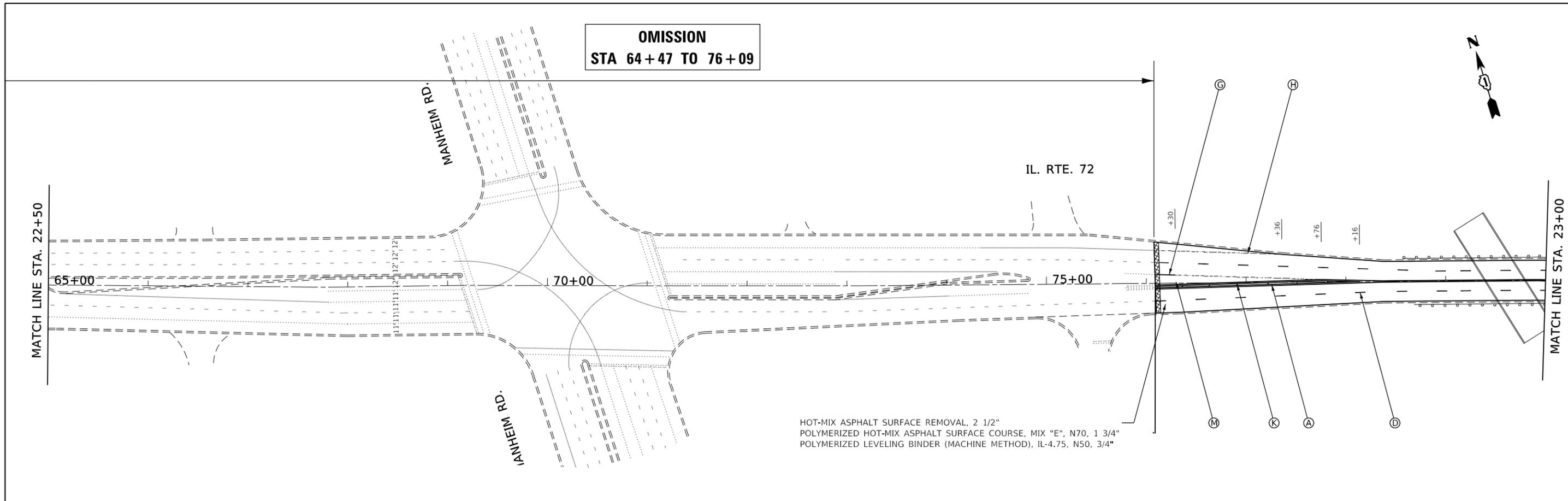


- PAVEMENT MARKING LEGEND**
- (A) THERMOPLASTIC PAVEMENT MARKING, YELLOW, DOUBLE 4" SOLID, 11" C-C
 - (B) THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" SOLID
 - (C) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID
 - (D) THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" DASHED, 10' DASH 30' SKIP
 - (E) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" DASHED, 10' DASH 30' SKIP
 - (F) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID AND DASH-SKIP, YELLOW, 5 1/2" C-C
 - (G) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" SOLID
 - (H) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" DASHED, 2' DASH 6' SKIP
 - (I) THERMOPLASTIC PAVEMENT MARKING, WHITE, 8" SOLID
 - (J) THERMOPLASTIC PAVEMENT MARKING, WHITE, 12" SOLID
 - (K) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 12" SOLID
 - (L) THERMOPLASTIC PAVEMENT MARKING, WHITE, 24" SOLID
 - (M) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS



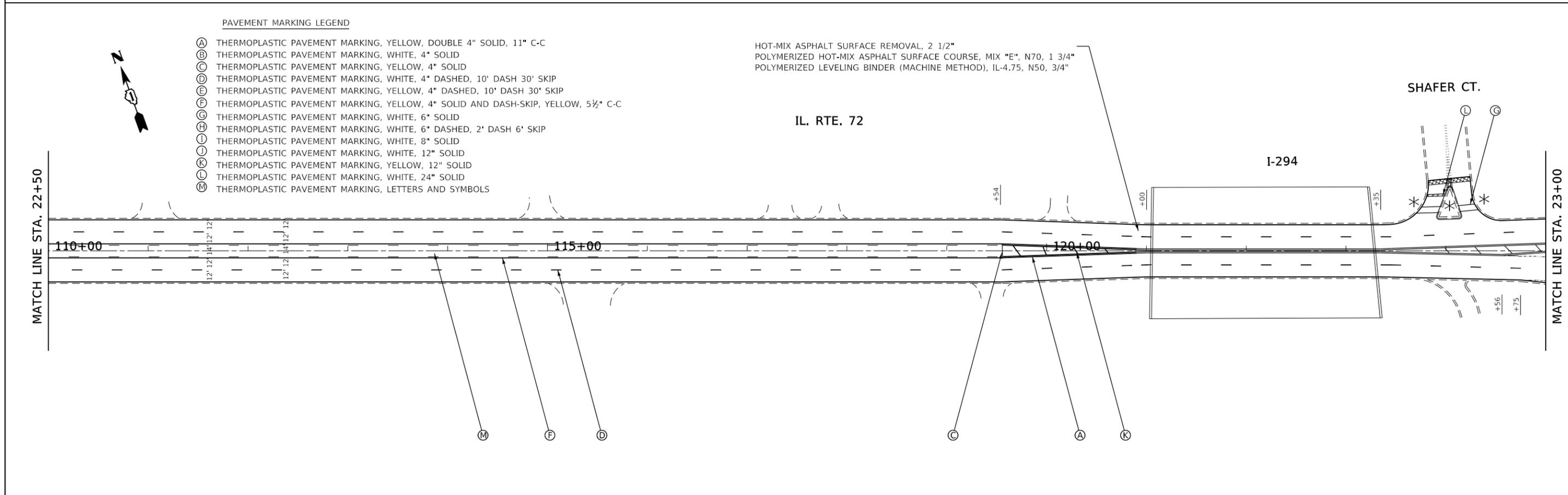
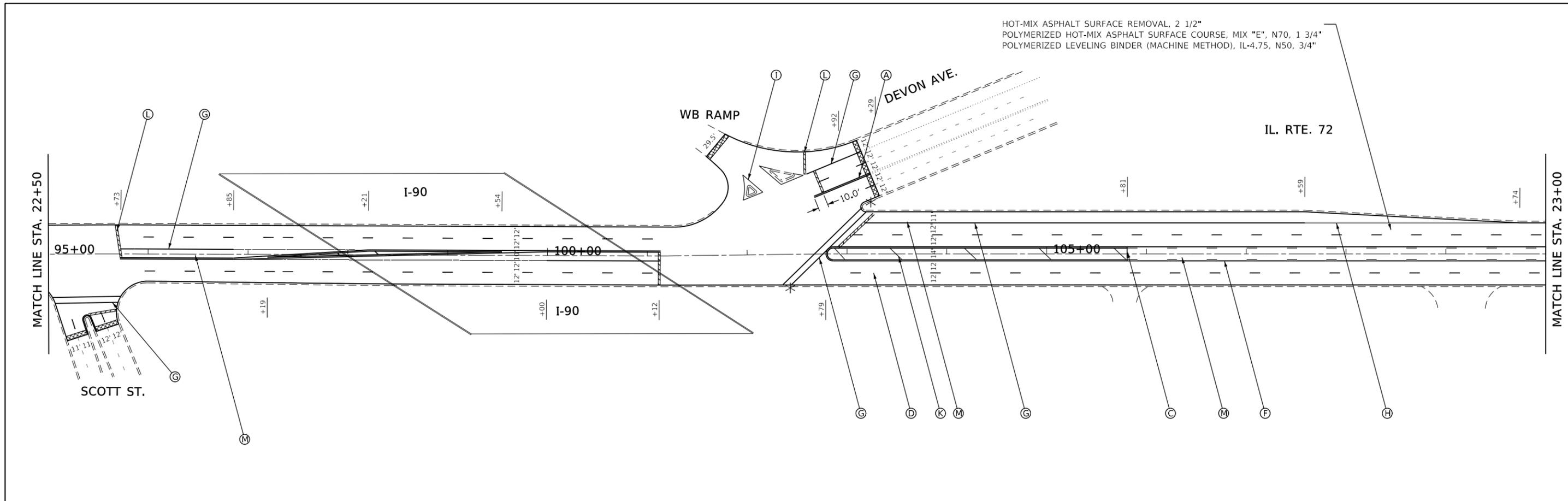
FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLANS IL 72 (HIGGINS ROAD) TOUHY AVENUE TO CUMBERLAND AVENUE	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pwz\VL084EBID\INTEG.illinois.gov\PWIDOT\Documents\IDOT_Offices\District 1\Projects\104517\CADDData\Design\DRAWN17-shft-plan.dgn		CHECKED -	REVISED -			1350	2016-058-RS	COOK	63	11	
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 62D51					
Default						ILLINOIS FED. AID PROJECT					

SCALE: 1"=50' SHEET OF SHEETS STA. 9+85 TO STA. 37+00



- PAVEMENT MARKING LEGEND**
- (A) THERMOPLASTIC PAVEMENT MARKING, YELLOW, DOUBLE 4" SOLID, 11" C-C
 - (B) THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" SOLID
 - (C) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID
 - (D) THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" DASHED, 10' DASH 30' SKIP
 - (E) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" DASHED, 10' DASH 30' SKIP
 - (F) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID AND DASH-SKIP, YELLOW, 5 1/2" C-C
 - (G) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" SOLID
 - (H) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" DASHED, 2' DASH 6' SKIP
 - (I) THERMOPLASTIC PAVEMENT MARKING, WHITE, 8" SOLID
 - (J) THERMOPLASTIC PAVEMENT MARKING, WHITE, 12" SOLID
 - (K) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 12" SOLID
 - (L) THERMOPLASTIC PAVEMENT MARKING, WHITE, 24" SOLID
 - (M) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
 - (1) POLYUREA PAVEMENT MARKING, YELLOW, DOUBLE 4" SOLID, 11" C-C
 - (2) POLYUREA PAVEMENT MARKING, WHITE, 4" DASHED, 10' DASH 30' SKIP
 - (3) POLYUREA PAVEMENT MARKING, WHITE, 6" SOLID
 - (4) POLYUREA PAVEMENT MARKING, WHITE, 24" SOLID

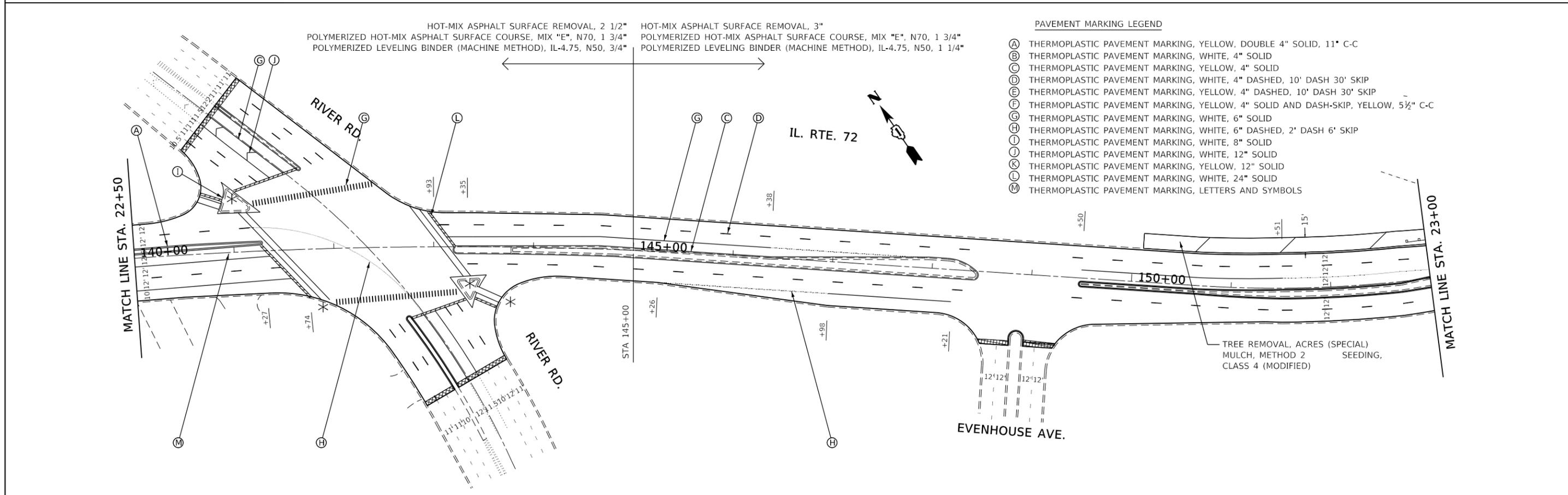
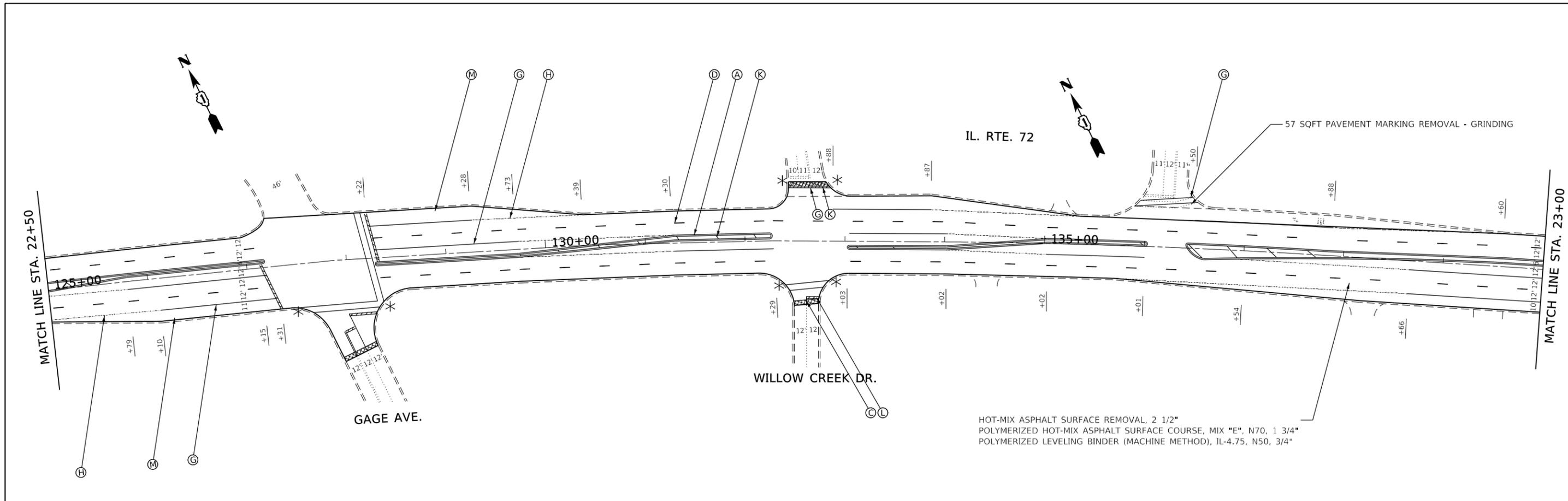
FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLANS IL 72 (HIGGINS ROAD) TOUHY AVENUE TO CUMBERLAND AVENUE	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 62D51					
Default						SCALE: 1"=50'	SHEET	OF	SHEETS	STA. 9+85	TO STA. 37+00



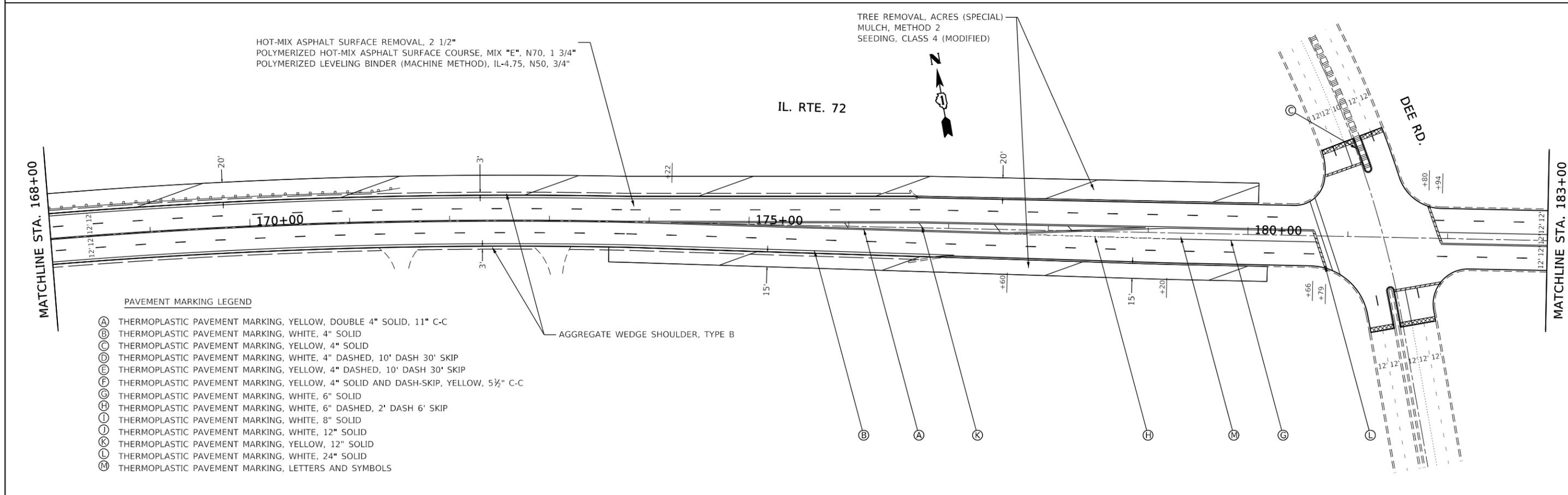
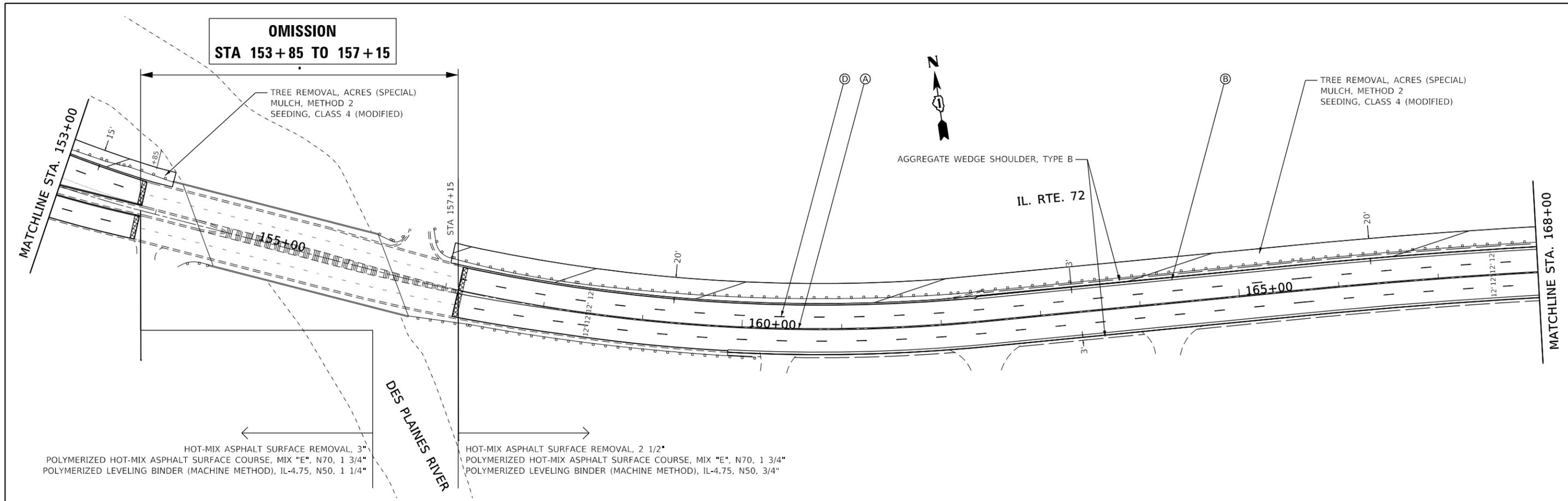
PAVEMENT MARKING LEGEND

- Ⓐ THERMOPLASTIC PAVEMENT MARKING, YELLOW, DOUBLE 4" SOLID, 11" C-C
- Ⓑ THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" SOLID
- Ⓒ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID
- Ⓓ THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" DASHED, 10' DASH 30' SKIP
- Ⓔ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" DASHED, 10' DASH 30' SKIP
- Ⓕ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID AND DASH-SKIP, YELLOW, 5 1/2" C-C
- Ⓖ THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" SOLID
- Ⓗ THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" DASHED, 2' DASH 6' SKIP
- Ⓘ THERMOPLASTIC PAVEMENT MARKING, WHITE, 8" SOLID
- Ⓚ THERMOPLASTIC PAVEMENT MARKING, WHITE, 12" SOLID
- Ⓛ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 12" SOLID
- Ⓜ THERMOPLASTIC PAVEMENT MARKING, WHITE, 24" SOLID
- Ⓝ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS

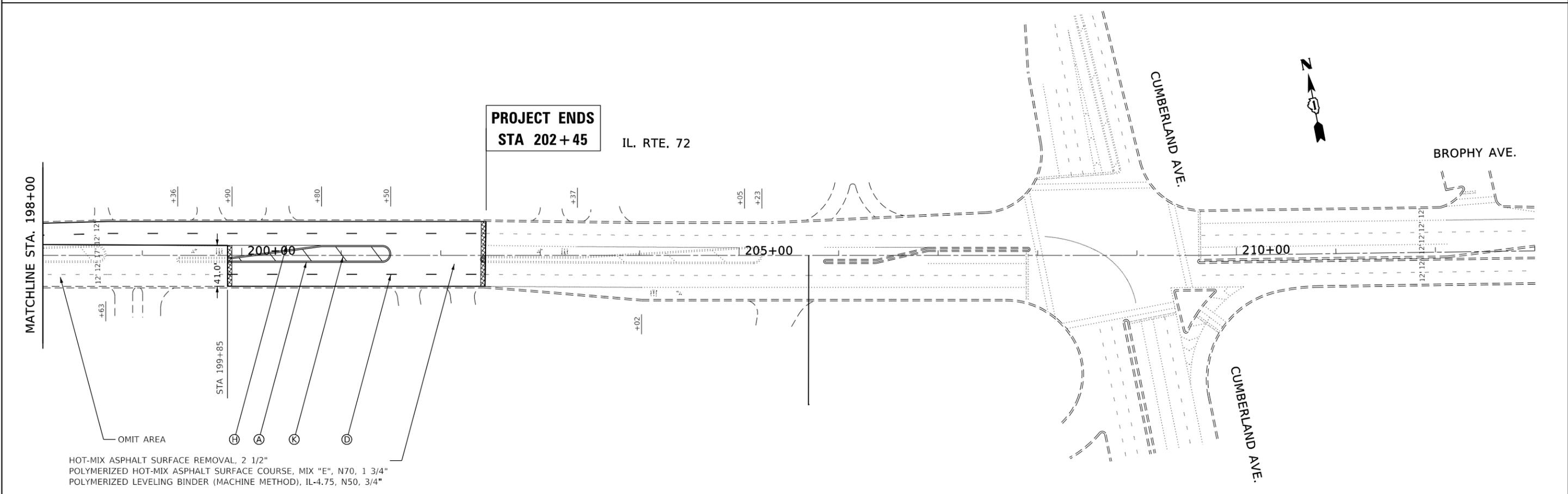
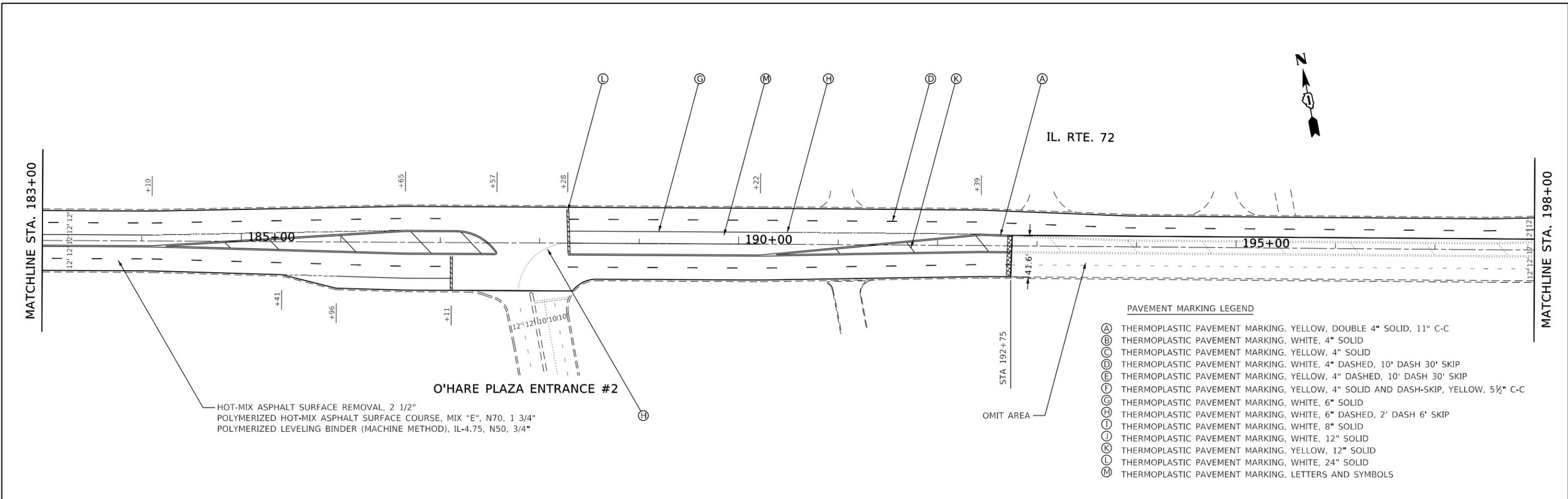
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Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62D51			ILLINOIS FED. AID PROJECT	
	PLOT DATE = 2/2/2018					SCALE: 1"=50'		SHEET OF SHEETS		STA. 9+85 TO STA. 37+00



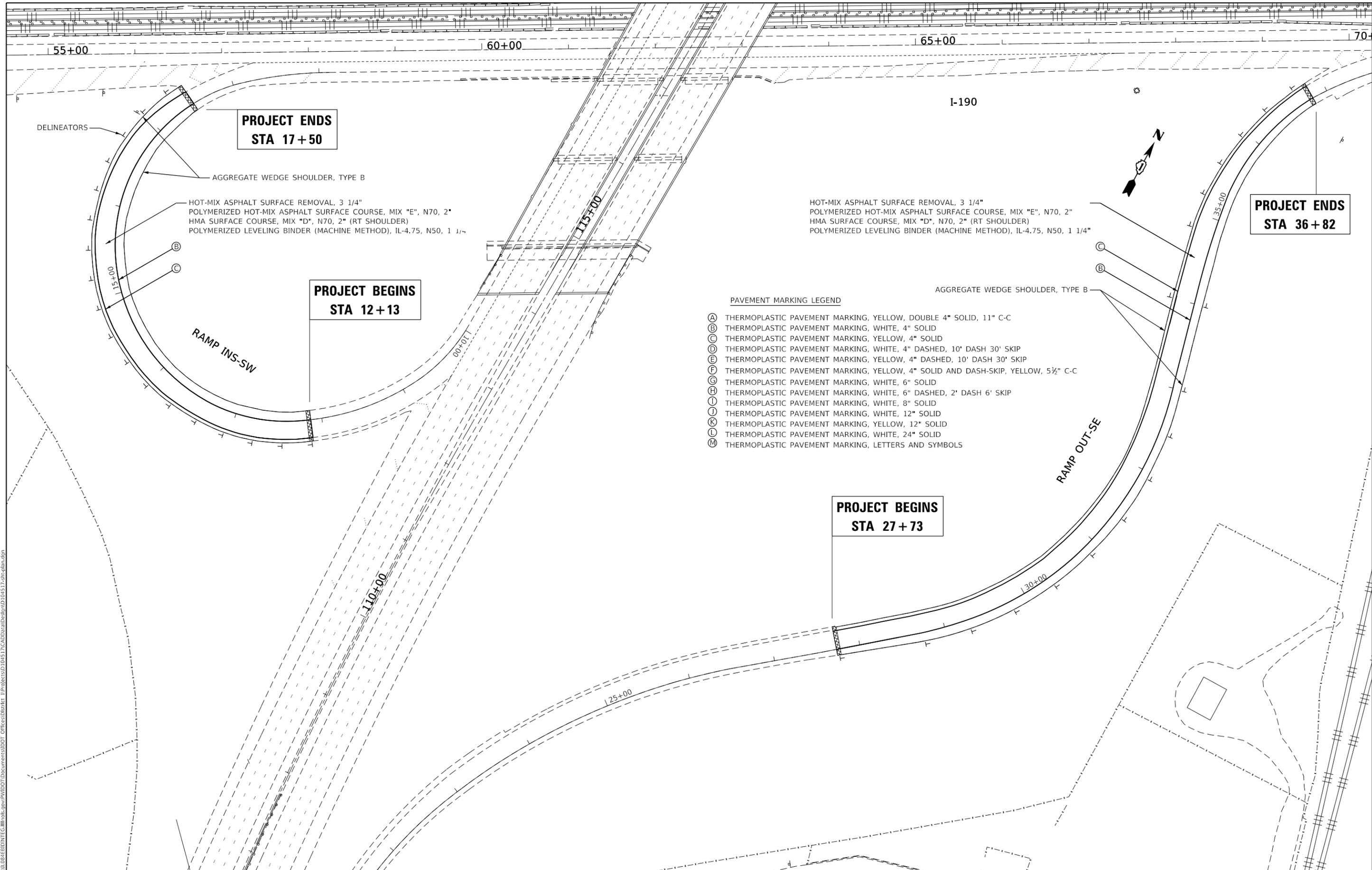
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pwz\IL084EBID\INTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\D104517\CADDData\Design\DRAWN 7-shft-plan.dgn		CHECKED -	REVISED -			1350	2016-058-RS	COOK	63	14	
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 62D51					
Default						ILLINOIS FED. AID PROJECT					



FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLANS IL 72 (HIGGINS ROAD) TOUHY AVENUE TO CUMBERLAND AVENUE	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pwz\VL084EBID\INTEG.illinois.gov\PWIDOT\Documents\IDOT_Offices\District 1\Projects\104517\CADDData\Design\DRAWN17-shit-plan.dgn	PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -			1350	2016-058-RS	COOK	\$TOPG\$	15
Default	PLOT DATE = 2/2/2018	DATE -	REVISED -			SCALE: 1"=50' SHEET OF SHEETS STA. 9+85 TO STA. 37+00			CONTRACT NO. 62D51	
						ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY PLANS IL 72 (HIGGINS ROAD) TOUHY AVENUE TO CUMBERLAND AVENUE	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pwz\VL084EBID\INTEG.illinois.gov\PWIDOT\Documents\IDOT_Offices\District 1\Projects\ID104517\CADDData\Design\DRAWN17-shft-plan.dgn		REVISOR -	REVISOR -			1350	2016-058-RS	COOK	63	16	
PLOT SCALE = 100,0000 ' / in.		CHECKED -	REVISOR -			CONTRACT NO. 62D51					
PLOT DATE = 2/2/2018		DATE -	REVISOR -			ILLINOIS FED. AID PROJECT					
					SCALE: 1"=50'	SHEET OF SHEETS		STA. 9+85 TO STA. 37+00			



HOT-MIX ASPHALT SURFACE REMOVAL, 3 1/4"
 POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 2"
 HMA SURFACE COURSE, MIX "D", N70, 2" (RT SHOULDER)
 POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1 1/4"

- PAVEMENT MARKING LEGEND**
- (A) THERMOPLASTIC PAVEMENT MARKING, YELLOW, DOUBLE 4" SOLID, 11" C-C
 - (B) THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" SOLID
 - (C) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID
 - (D) THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" DASHED, 10' DASH 30' SKIP
 - (E) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" DASHED, 10' DASH 30' SKIP
 - (F) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID AND DASH-SKIP, YELLOW, 5 1/2" C-C
 - (G) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" SOLID
 - (H) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" DASHED, 2' DASH 6' SKIP
 - (I) THERMOPLASTIC PAVEMENT MARKING, WHITE, 8" SOLID
 - (L) THERMOPLASTIC PAVEMENT MARKING, WHITE, 12" SOLID
 - (K) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 12" SOLID
 - (M) THERMOPLASTIC PAVEMENT MARKING, WHITE, 24" SOLID
 - (N) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS

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USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/2/2018	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLANS I-190 RAMPS
 AT US 12/45 (MANHEIM ROAD)**

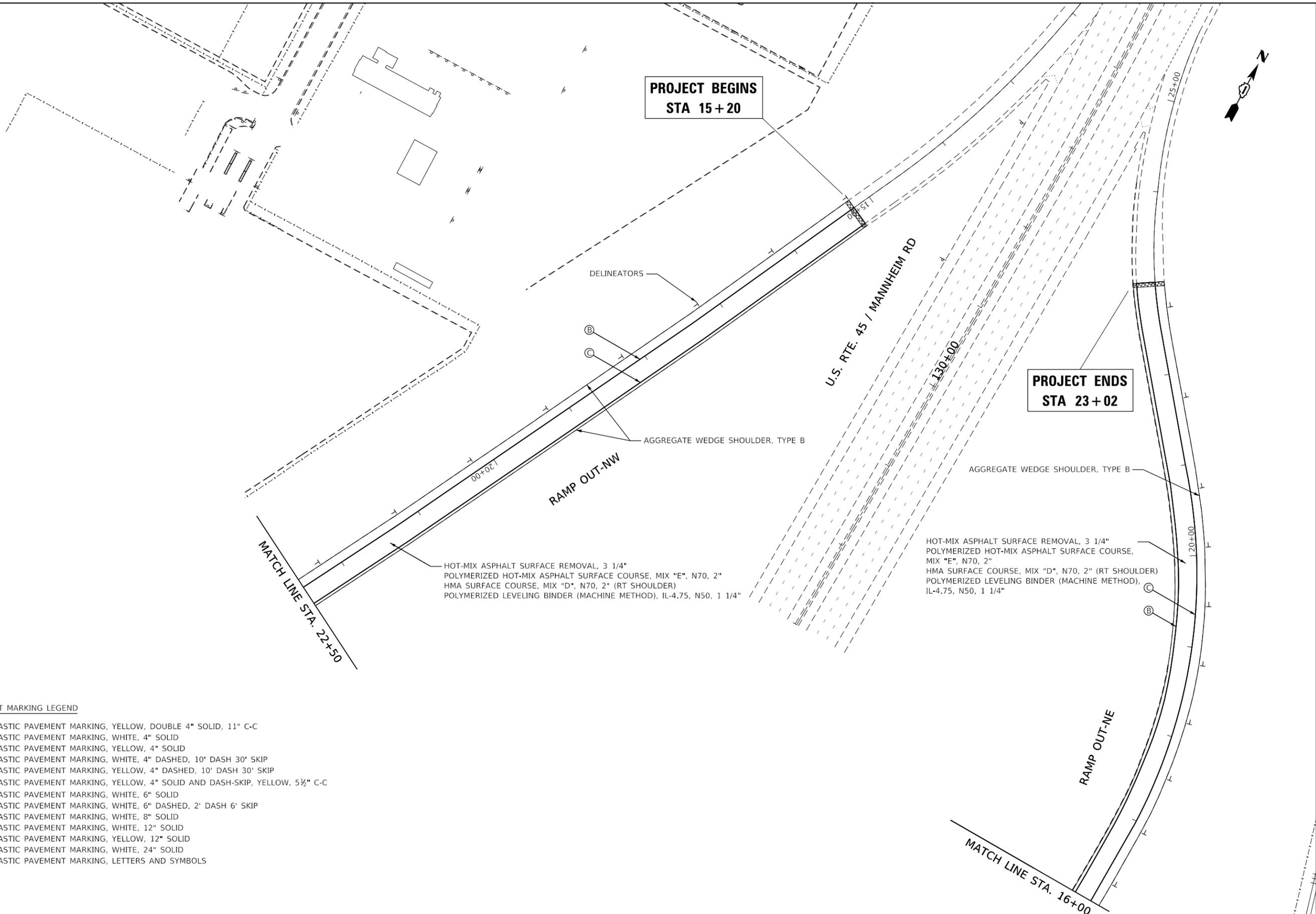
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-R5	COOK	63	17
CONTRACT NO. 62D51				
ILLINOIS FED. AID PROJECT				

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PAVEMENT MARKING LEGEND

- Ⓐ THERMOPLASTIC PAVEMENT MARKING, YELLOW, DOUBLE 4" SOLID, 11" C-C
- Ⓑ THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" SOLID
- Ⓒ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID
- Ⓓ THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" DASHED, 10' DASH 30' SKIP
- Ⓔ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" DASHED, 10' DASH 30' SKIP
- Ⓕ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID AND DASH-SKIP, YELLOW, 5½" C-C
- Ⓖ THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" SOLID
- Ⓗ THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" DASHED, 2' DASH 6' SKIP
- Ⓘ THERMOPLASTIC PAVEMENT MARKING, WHITE, 8" SOLID
- Ⓚ THERMOPLASTIC PAVEMENT MARKING, WHITE, 12" SOLID
- Ⓛ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 12" SOLID
- Ⓜ THERMOPLASTIC PAVEMENT MARKING, WHITE, 24" SOLID
- Ⓝ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS



USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 2/2/2018	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLANS I-190 RAMPS
AT US 12/45 (MANHEIM ROAD)

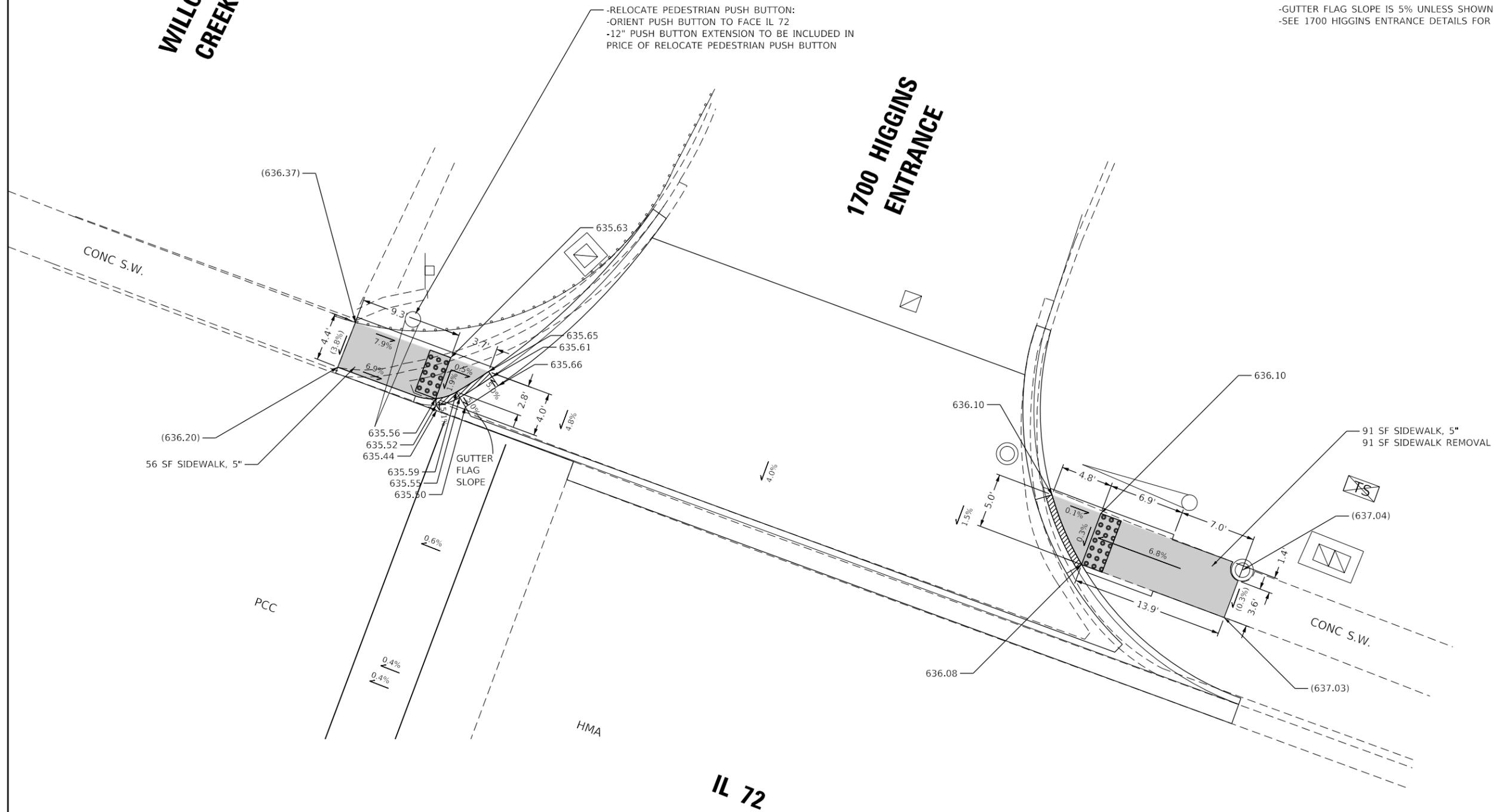
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-R5	COOK	63	19
CONTRACT NO. 62D51				
		ILLINOIS	FED. AID PROJECT	

SCALE: SHEET OF SHEETS STA. TO STA.

WILLOW CREEK

1700 HIGGINS ENTRANCE

NOTE:
 -GUTTER FLAG SLOPE IS 5% UNLESS SHOWN OTHERWISE
 -SEE 1700 HIGGINS ENTRANCE DETAILS FOR DRIVEWAY ELEVATIONS AND DIMENSIONS



-RELOCATE PEDESTRIAN PUSH BUTTON:
 -ORIENT PUSH BUTTON TO FACE IL 72
 -12" PUSH BUTTON EXTENSION TO BE INCLUDED IN PRICE OF RELOCATE PEDESTRIAN PUSH BUTTON

REFERENCE BENCH MARK ELEV 638.58

BENCHMARK: CUT SQUARE ON SE CORNER OF CONCRETE BASE OF TRAFFIC CONTROL BOX
 LOCATION: TRAFFIC CONTROL BOX ON NE CORNER OF HIGGINS AND 1700 HIGGINS ENTRANCE

LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 638.58

BENCHMARK: CUT SQUARE ON SE CORNER OF CONCRETE BASE OF TRAFFIC CONTROL BOX
 LOCATION: TRAFFIC CONTROL BOX ON NE CORNER OF HIGGINS AND 1700 HIGGINS ENTRANCE

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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	PLOT DATE = 2/2/2018	DATE -	REVISION -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS (IL 72 AND 1700 HIGGINS ENTRANCE)
 IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)**

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

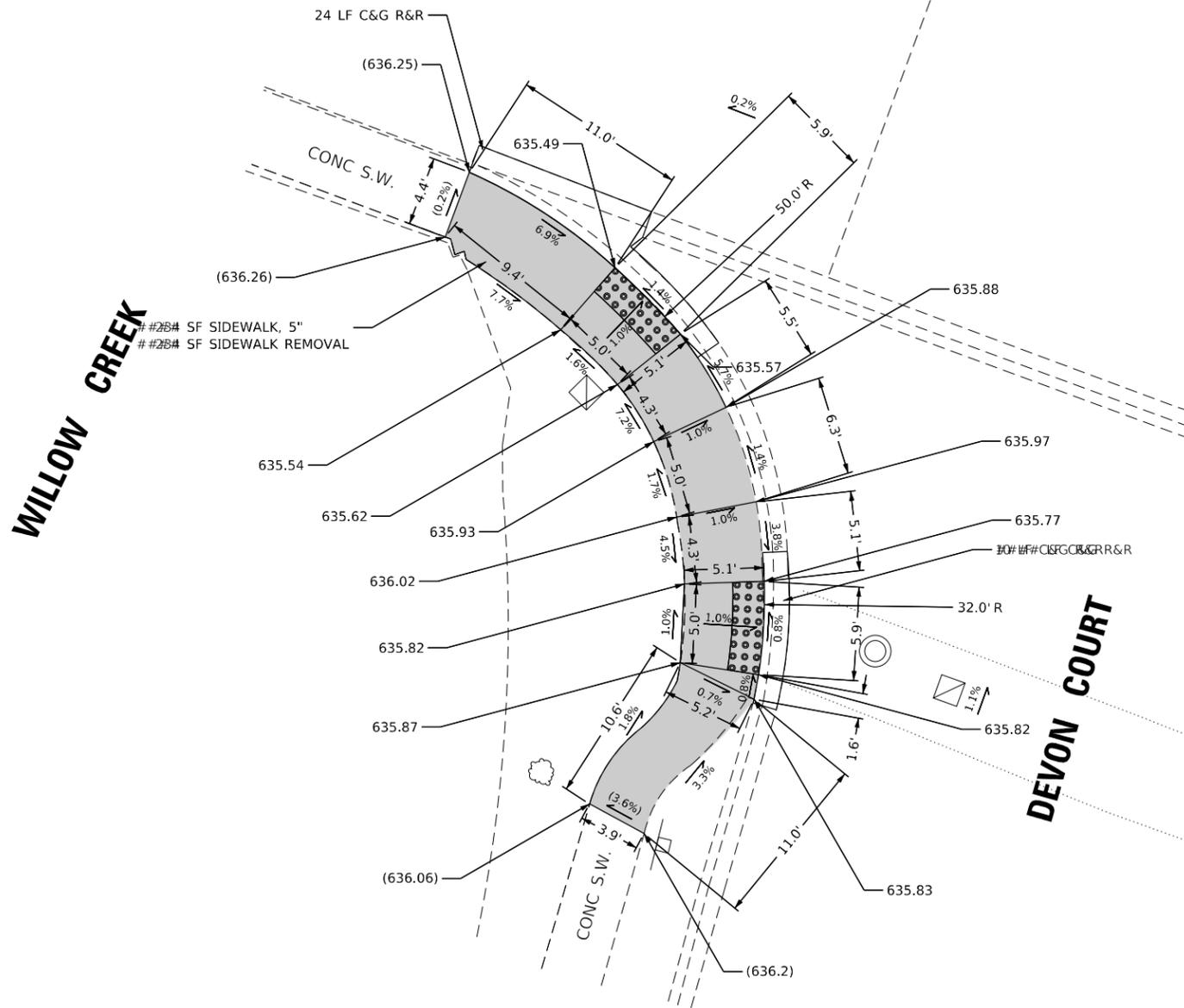
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-R5	COOK	63	20
CONTRACT NO62D51				
ILLINOIS FED. AID PROJECT				

NOTE:
GUTTER FLAG SLOPE IS 5% UNLESS SHOWN OTHERWISE

IL 72 PCC



HMA



REFERENCE BENCH MARK ELEV 638.58

BENCHMARK: CUT SQUARE ON SE CORNER OF CONCRETE BASE OF TRAFFIC CONTROL BOX
LOCATION: TRAFFIC CONTROL BOX ON NE CORNER OF HIGGINS AND 1700 HIGGINS ENTRANCE

LEGEND

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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	PLOT DATE = 2/2/2018	DATE -	REVISION -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS (IL 72 AND DEVON COURT)
IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)

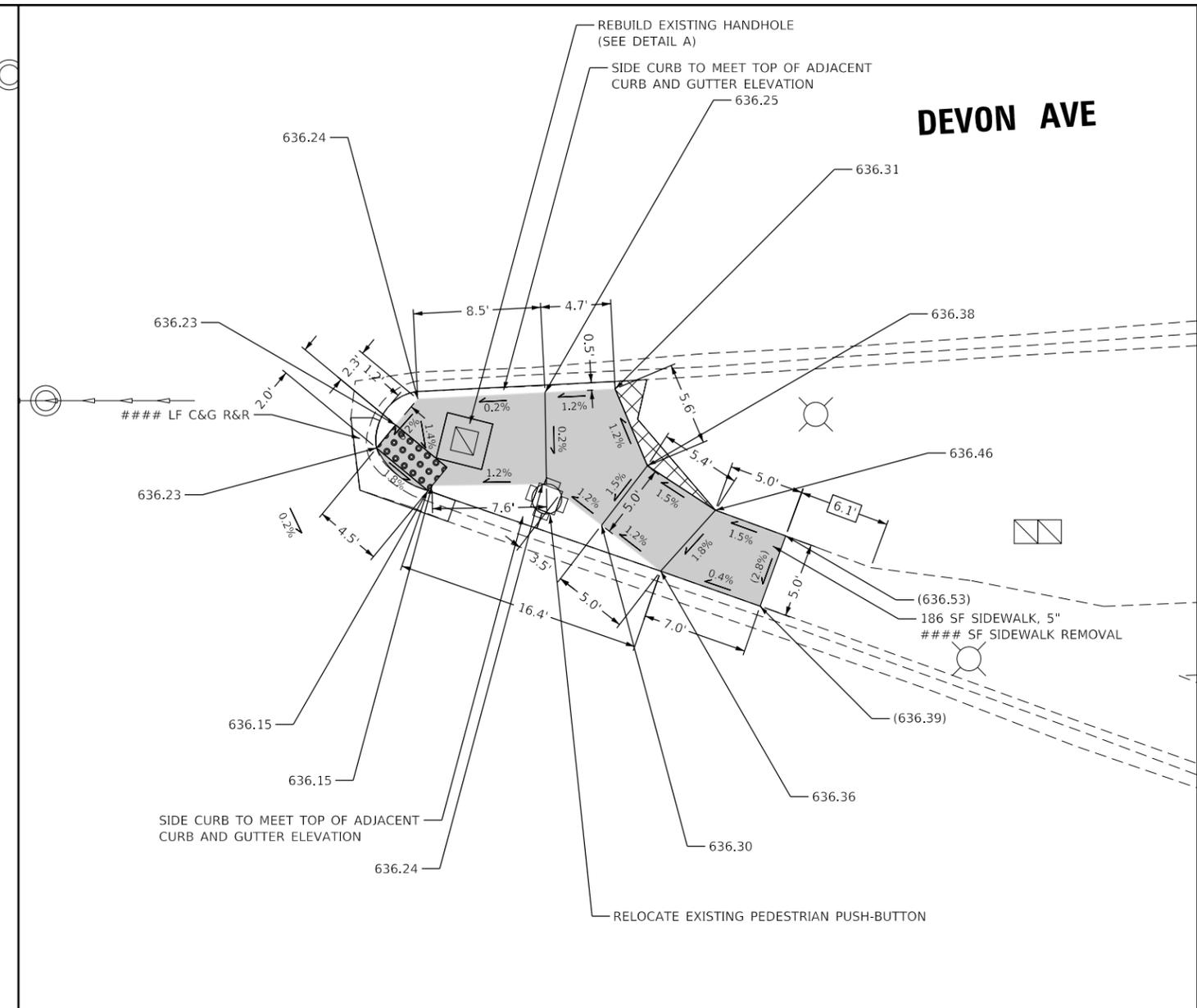
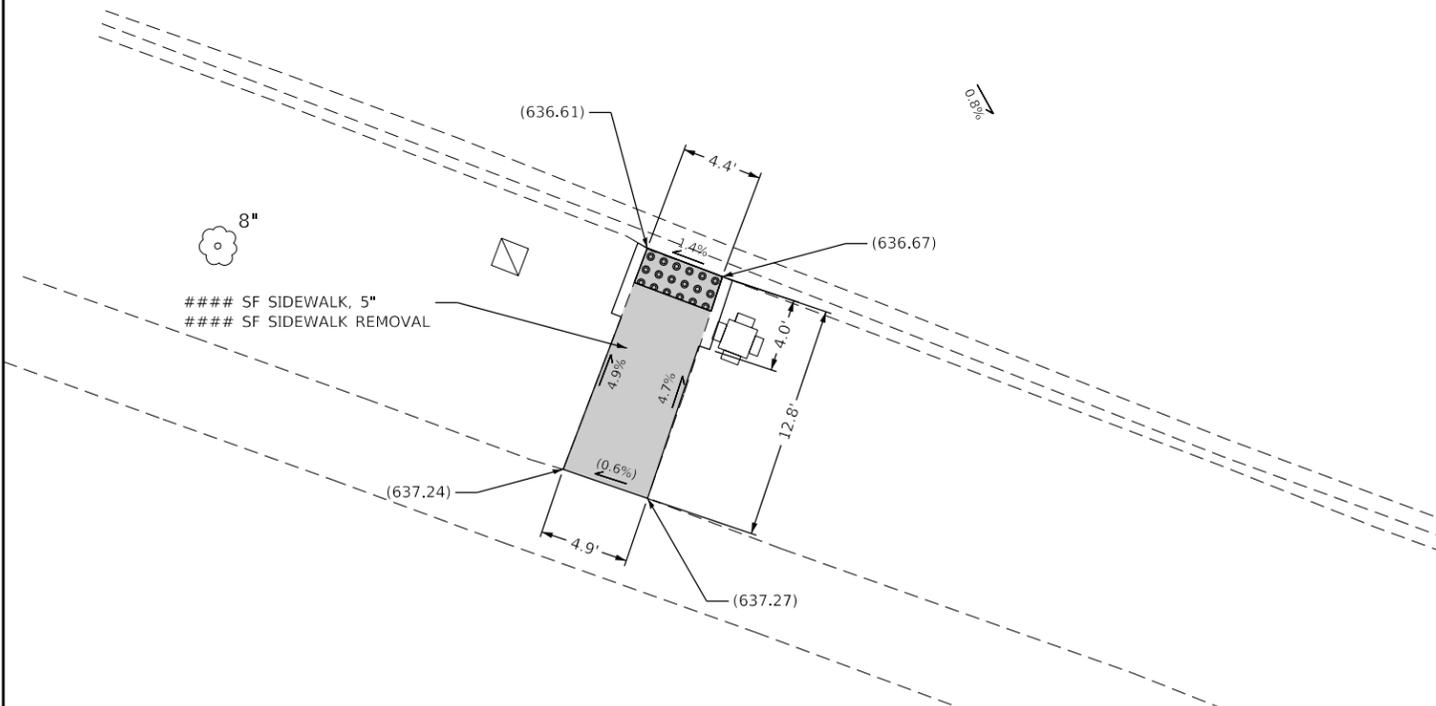
SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-R5	COOK	63	21
CONTRACT NO 62D51				
ILLINOIS FED. AID PROJECT				

NOTE:
GUTTER FLAG SLOPE IS 5% UNLESS SHOWN OTHERWISE

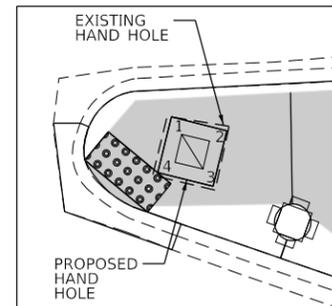


IL 72



DEVON AVE

DETAIL A:



REBUILD EXISTING HANDHOLE
PROPOSED CORNER ELEVATIONS:

POINT	ELEVATION
1	636.23
2	636.23
3	636.20
4	636.18



IL 72

REFERENCE BENCH MARK ELEV 637.06
BENCHMARK: CUT SQUARE ON N SIDE OF TRAFFIC POST CONCRETE FOUNDATION
LOCATION: TRAFFIC SIGNAL CONCRETE FOUNDATION 5 FT SOUTH OF IL 72 EOP, 2 FT EAST OF SOUTH SIDEWALK

LEGEND

- xx.xx' EXISTING LENGTH
- ===== PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- ▣ SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 637.06
BENCHMARK: CUT SQUARE ON N SIDE OF TRAFFIC POST CONCRETE FOUNDATION
LOCATION: TRAFFIC SIGNAL CONCRETE FOUNDATION 5 FT SOUTH OF IL 72 EOP, 2 FT EAST OF SOUTH SIDEWALK

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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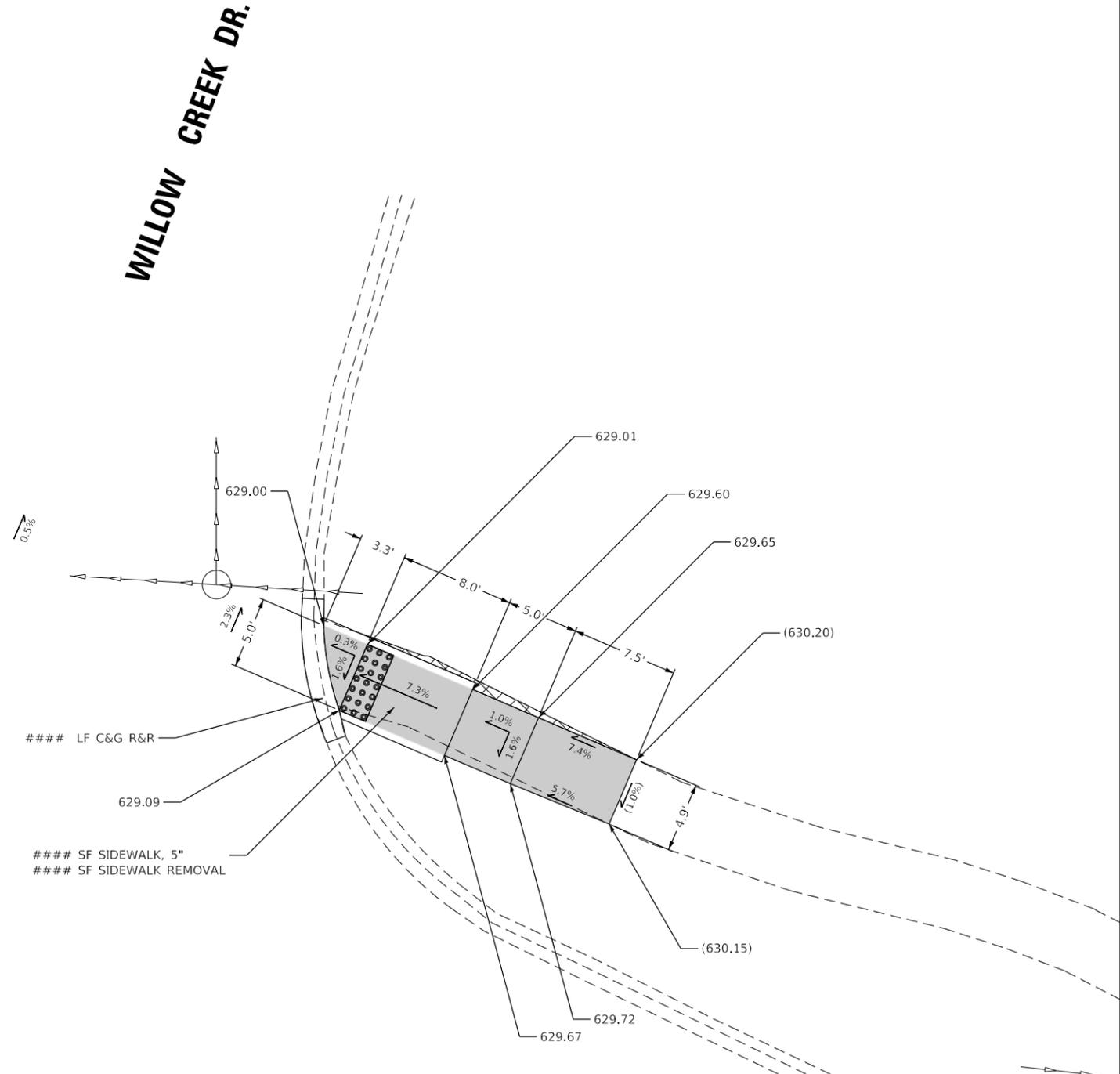
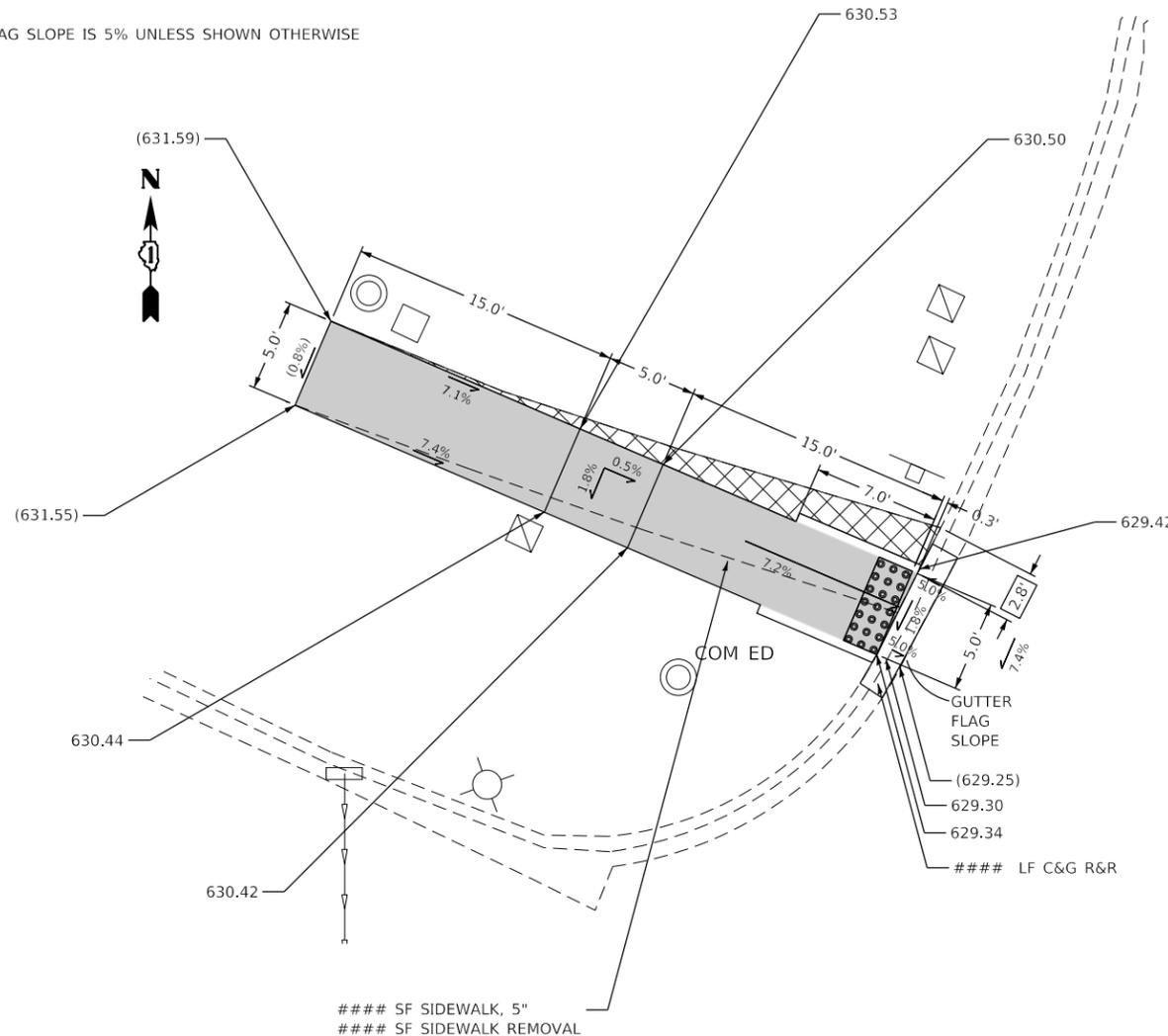
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS (IL 72 AND DEVON AVE)
IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)**

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-R5	COOK	63	22
CONTRACT NO 62D51				
ILLINOIS FED. AID PROJECT				

NOTE:
GUTTER FLAG SLOPE IS 5% UNLESS SHOWN OTHERWISE



IL 72

WILLOW CREEK DR.

REFERENCE BENCH MARK ELEV 631.65
BENCHMARK: CUT CROSS ON NE BOLT OF 7-ELEVEN SIGN
LOCATION: 7-ELEVEN SIGN ON SW CORNER OF WILLOW CREEK DR 15 FT W OF WILLOW CREEK DR EOP

LEGEND

xx.xx'	EXISTING LENGTH		PROPOSED SIDEWALK
	PROPOSED SIDE CURB		DETECTABLE WARNINGS
()	EXISTING ELEVATION/SLOPE		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 631.65
BENCHMARK: CUT CROSS ON NE BOLT OF 7-ELEVEN SIGN
LOCATION: 7-ELEVEN SIGN ON SW CORNER OF WILLOW CREEK DR 15 FT W OF WILLOW CREEK DR EOP

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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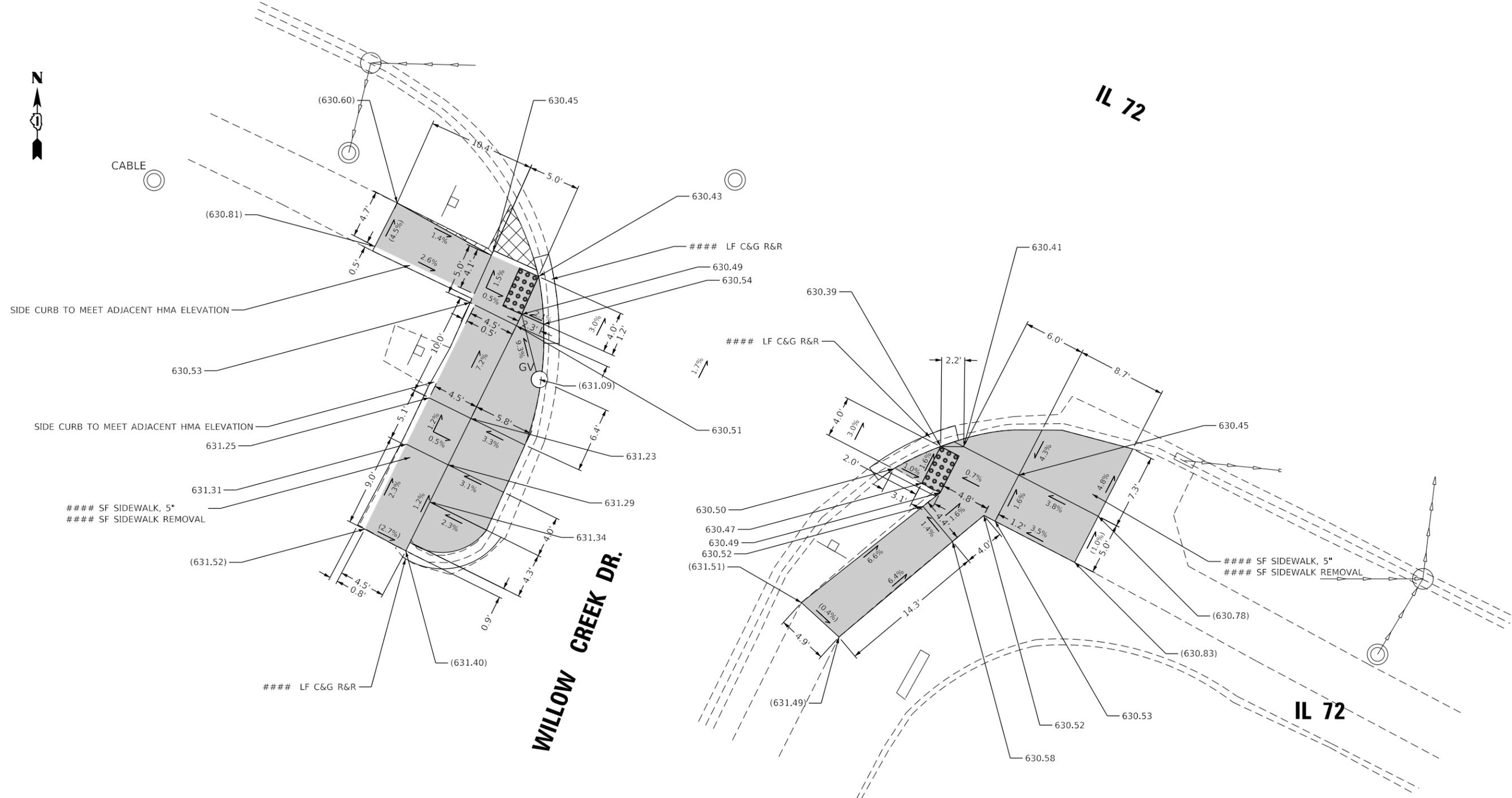
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS (IL 72 AND WILLOW CREEK DR.)
IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-R5	COOK	63	25
CONTRACT NO 62D51				
ILLINOIS FED. AID PROJECT				

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

NOTE:
GUTTER FLAG SLOPE IS 5% UNLESS SHOWN OTHERWISE



REFERENCE BENCH MARK ELEV 631.65

BENCHMARK: CUT CROSS ON NE BOLT OF 7-ELEVEN SIGN

LOCATION: 7-ELEVEN SIGN ON SW CORNER OF WILLOW CREEK DR 15 FT W OF WILLOW CREEK DR EOP

LEGEND

xx.xx'

EXISTING LENGTH

=====

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 631.65

BENCHMARK: CUT CROSS ON NE BOLT OF 7-ELEVEN SIGN

LOCATION: 7-ELEVEN SIGN ON SW CORNER OF WILLOW CREEK DR 15 FT W OF WILLOW CREEK DR EOP

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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	PLOT DATE = 2/2/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

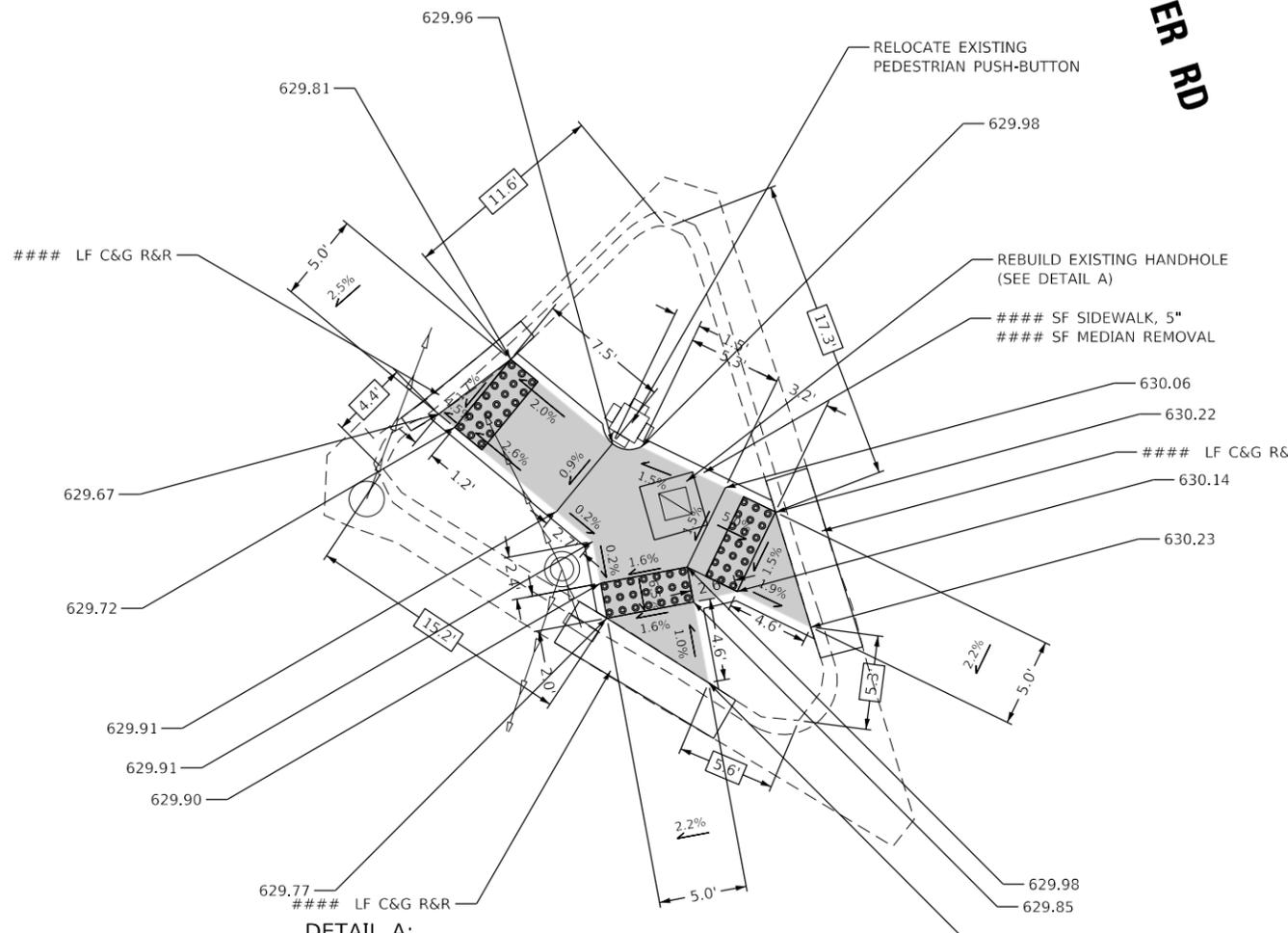
ADA RAMP DETAILS (IL 72 AND WILLOW CREEK DR.)
IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)

SCALE: 1"=5'

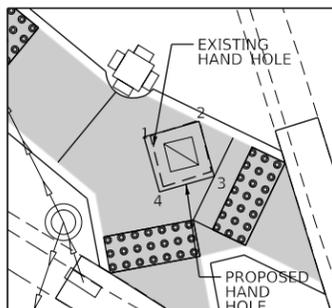
SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-R5	COOK	63	26
CONTRACT NO 62D51				
ILLINOIS FED. AID PROJECT				

NOTE:
GUTTER FLAG SLOPE IS 5% UNLESS SHOWN OTHERWISE



DETAIL A:

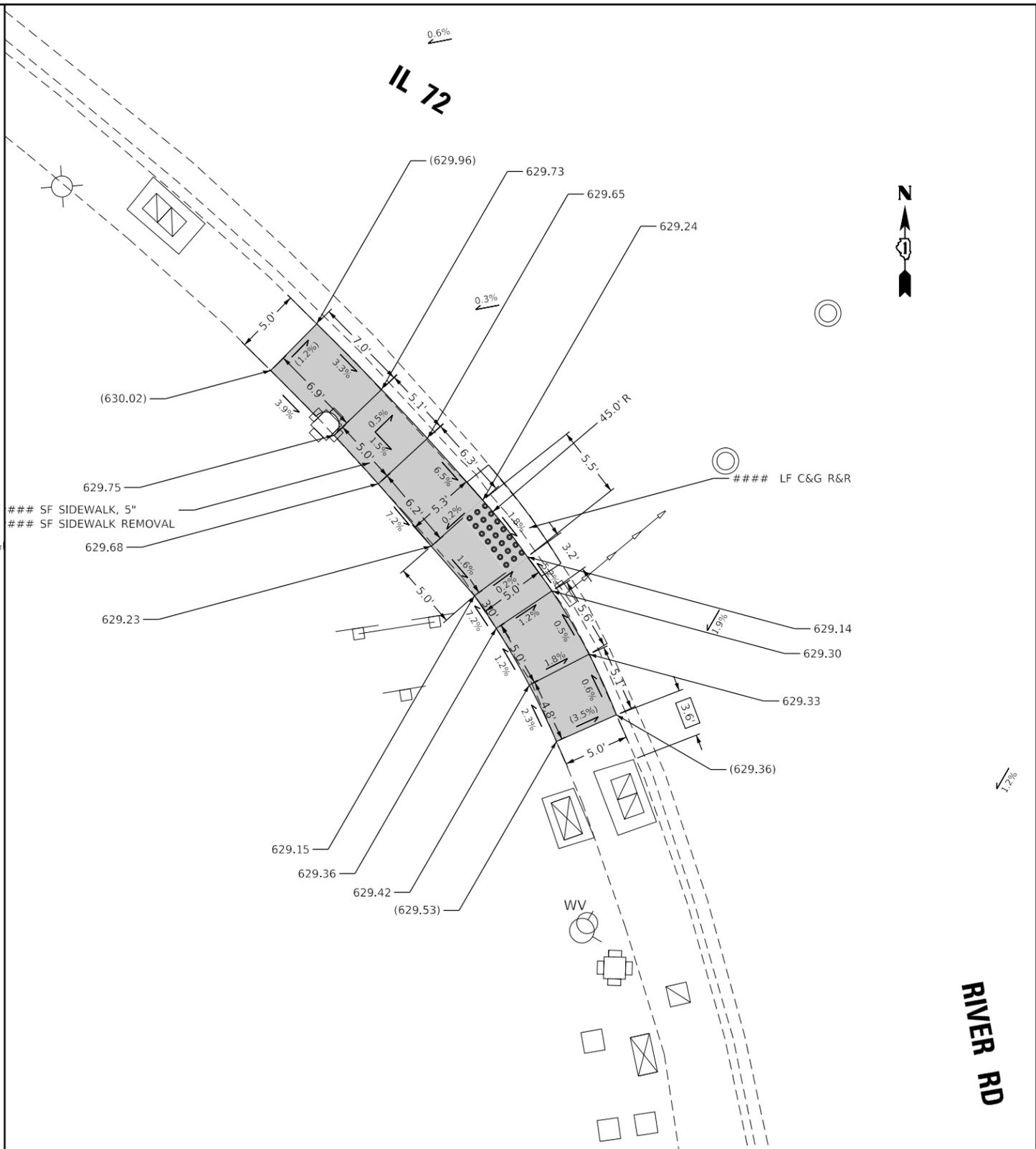


REBUILD EXISTING HANDHOLE
PROPOSED CORNER ELEVATIONS:

POINT	ELEVATION
1	629.97
2	630.03
3	630.02
4	629.97

IL 72

RIVER RD



RIVER RD

REFERENCE BENCH MARK ELEV 630.50
BENCHMARK: CUT SQUARE ON E SIDE OF EAST CONCRETE BASE OF MOBIL SIGN
LOCATION: EAST CONCRETE BASE OF MOBIL SIGN ON SW CORNER OF RIVER RD, 11 FT W OF RIVER RD EOP

LEGEND

- xx.xx' EXISTING LENGTH
- ===== PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 630.50
BENCHMARK: CUT SQUARE ON E SIDE OF EAST CONCRETE BASE OF MOBIL SIGN
LOCATION: EAST CONCRETE BASE OF MOBIL SIGN ON SW CORNER OF RIVER RD, 11 FT W OF RIVER RD EOP

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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	PLOT DATE = 2/2/2018	DATE -	REVISOR -

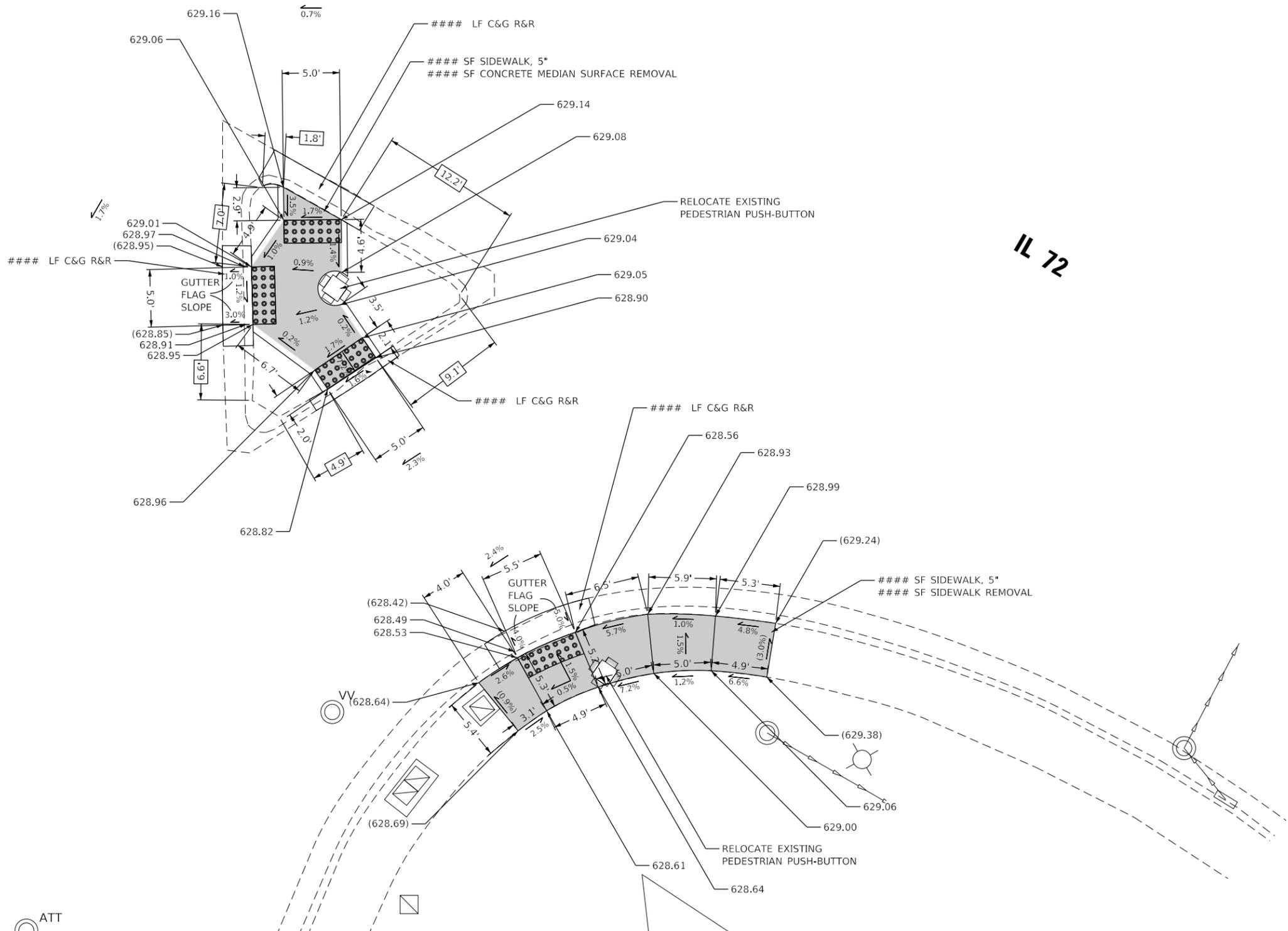
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ADA RAMP DETAILS (IL 72 AND RIVER RD)
IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)

SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-R5	COOK	63	27
CONTRACT NO 62D51				
ILLINOIS FED. AID PROJECT				

1.2%



IL 72

RIVER RD

ATT

REFERENCE BENCH MARK ELEV 630.50
 BENCHMARK: CUT SQUARE ON E SIDE OF EAST CONCRETE BASE OF MOBIL SIGN
 LOCATION: EAST CONCRETE BASE OF MOBIL SIGN ON SW CORNER OF RIVER RD, 11 FT W OF RIVER RD EOP

LEGEND

xx.xx'	EXISTING LENGTH		PROPOSED SIDEWALK
	PROPOSED SIDE CURB		DETECTABLE WARNINGS
()	EXISTING ELEVATION/SLOPE		SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

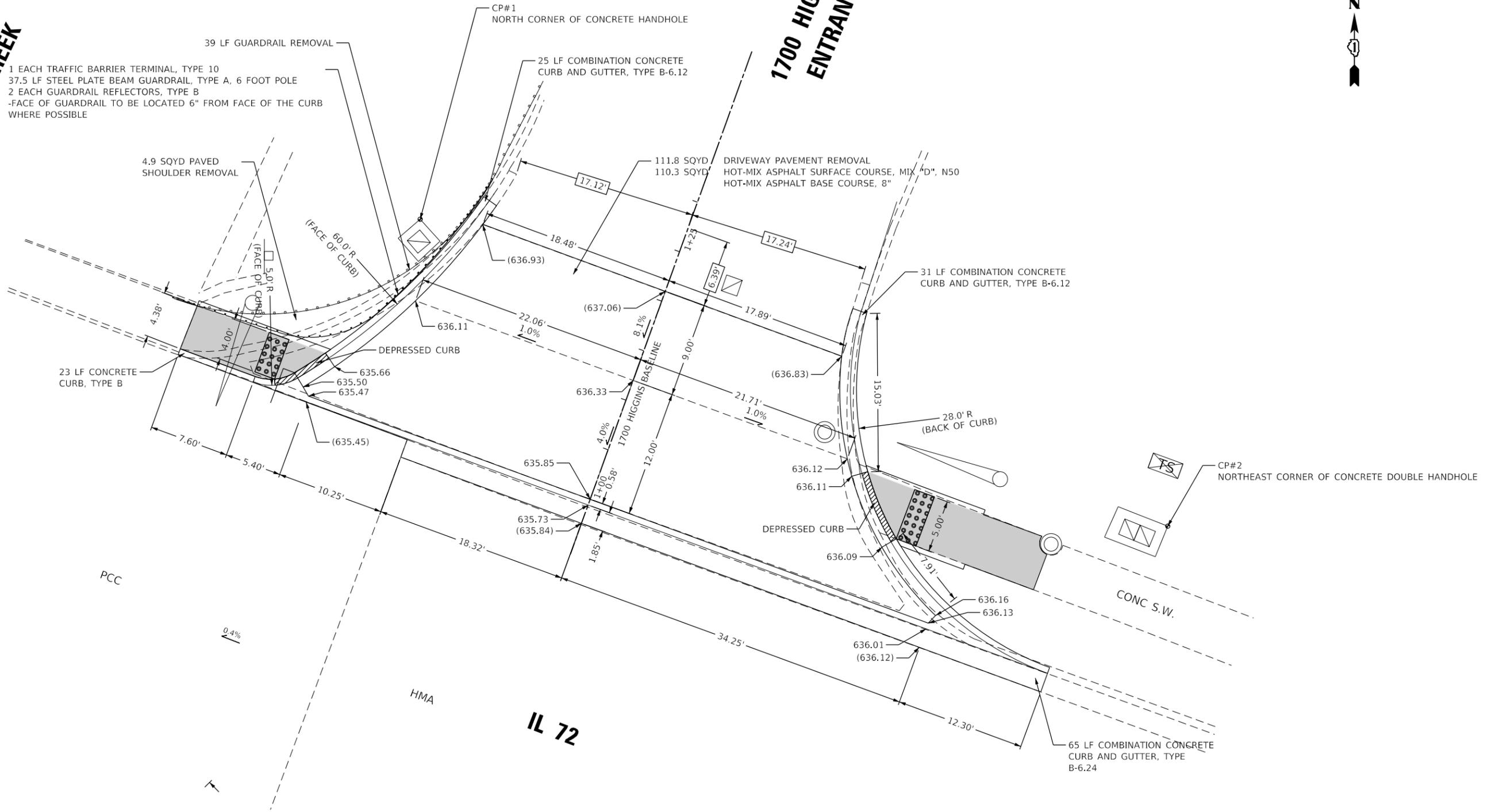
REFERENCE BENCH MARK ELEV 630.50
 BENCHMARK: CUT SQUARE ON E SIDE OF EAST CONCRETE BASE OF MOBIL SIGN
 LOCATION: EAST CONCRETE BASE OF MOBIL SIGN ON SW CORNER OF RIVER RD, 11 FT W OF RIVER RD EOP

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ADA RAMP DETAILS (IL 72 AND RIVER RD) IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pwz\VL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT_Offices\District 1\Projects\104517\CADDData\Design\17-shd-details.dgn		CHECKED -	REVISED -			1350	2106-058-R5	COOK	63	28
Default	PLOT SCALE = 10.0000 ' / in.	DATE -	REVISED -			CONTRACT NO62D51				
	PLOT DATE = 2/2/2018			SCALE: 1"=5'	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			

NOTE:
SEE ADA RAMP DETAILS FOR SIDEWALK ELEVATIONS, SLOPES, AND DIMENSIONS

WILLOW CREEK

- 1 EACH TRAFFIC BARRIER TERMINAL, TYPE 10
- 37.5 LF STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POLE
- 2 EACH GUARDRAIL REFLECTORS, TYPE B
- FACE OF GUARDRAIL TO BE LOCATED 6" FROM FACE OF THE CURB WHERE POSSIBLE



LEGEND

xx.xx'

EXISTING LENGTH

=====

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 638.58

BENCHMARK: CUT SQUARE ON SE CORNER OF CONCRETE BASE OF TRAFFIC CONTROL BOX

LOCATION: TRAFFIC CONTROL BOX ON NE CORNER OF HIGGINS AND 1700 HIGGINS CENTER

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
pww\1084EBID\INTEG\Illinois.gov\PWIDOT\Documents\1\DOT Offices\District 1\Projects\104517\CADDData\Design\17-shft-details.dgn		REVISOR -	REVISION -
Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISOR -
	PLOT DATE = 2/2/2018	DATE -	REVISION -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**1700 HIGGINS ENTRANCE DETAILS (SHEET 1)
IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)**

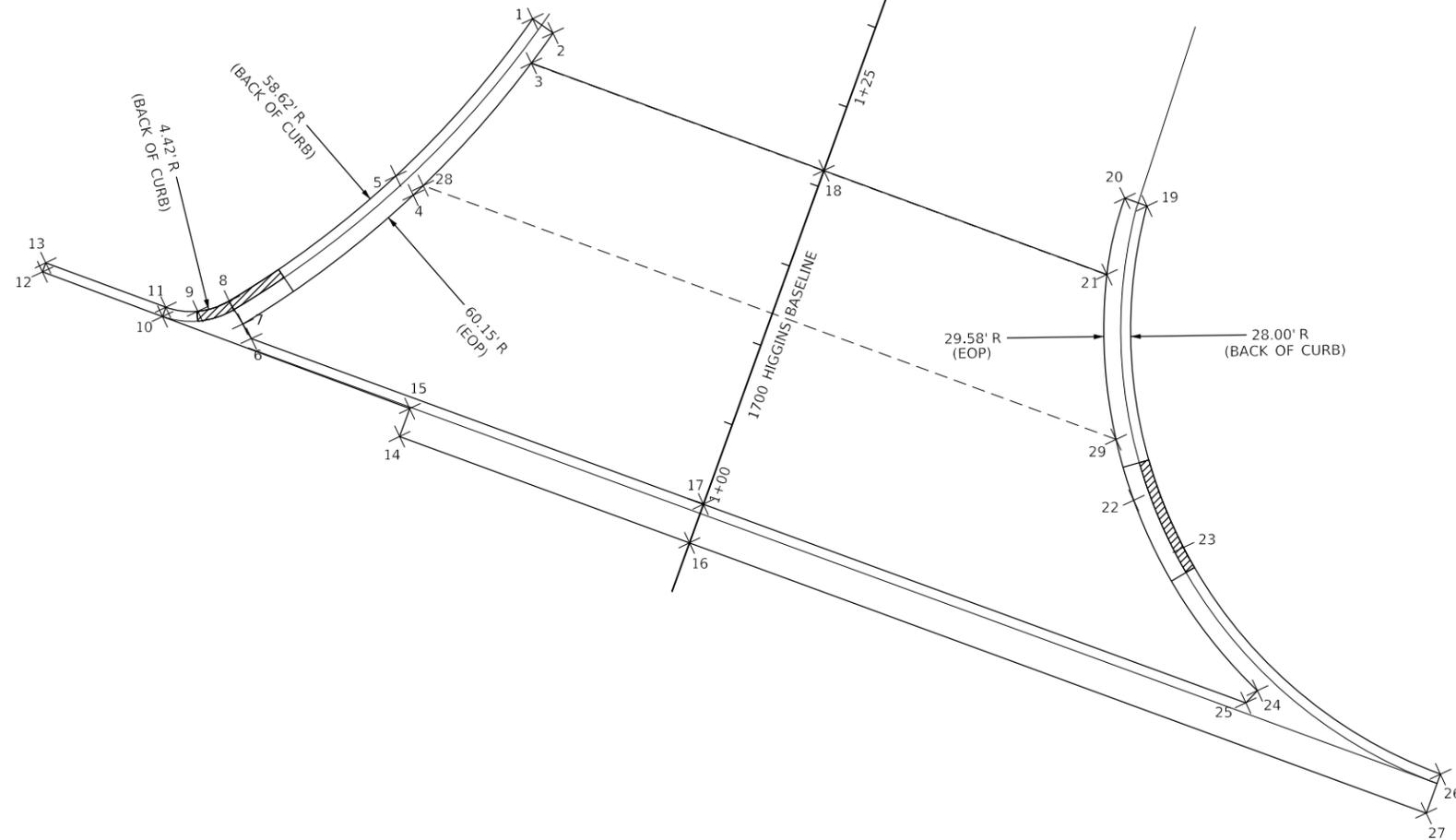
SCALE: 1"=5' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-R5	COOK	63	29
CONTRACT NO62D51				
ILLINOIS FED. AID PROJECT				

NOTE:
SEE ADA RAMP DETAILS FOR SIDEWALK ELEVATIONS, SLOPES, AND DIMENSIONS

WILLOW CREEK

1700 HIGGINS
ENTRANCE



IL 72

POINT	ELEVATION	OFFSET	STATION	EASTING	NORTHING
1	-	-19.31	1+23.57	1107706.8426	1941844.45
2	-	-17.88	1+23.19	1107708.0511	1941843.61
3	636.93	-18.49	1+21.07	1107706.7649	1941841.83
4	-	-22.41	1+11.35	1107699.7602	1941834.02
5	-	-23.77	1+12.05	1107698.7219	1941835.14
6	635.47	-28.55	1+00.09	1107690.1558	1941825.52
7	635.50	-29.33	1+00.72	1107689.6442	1941826.38
8	635.59	-30.55	1+01.73	1107688.8342	1941827.74
9	635.56	-32.10	1+00.52	1107686.9706	1941827.14
10	-	-33.96	0+99.52	1107684.8793	1941826.83
11	-	-33.96	1+00.11	1107685.0766	1941827.38
12	-	-41.56	0+99.59	1107677.7551	1941829.47
13	636.20	-41.55	1+00.15	1107677.9514	1941830.00
14	-	-18.32	0+97.63	1107698.9453	1941819.72
15	-	-18.31	0+99.39	1107699.5530	1941821.38
16	635.84	0.00	0+97.56	1107716.1457	1941813.43
17	635.85	0.00	1+00.00	1107716.9750	1941815.72
18	637.06	0.00	1+21.00	1107724.1204	1941835.47
19	-	18.75	1+25.55	1107743.2962	1941833.36
20	-	17.35	1+25.54	1107741.9844	1941833.84
21	636.83	17.89	1+20.93	1107740.9192	1941829.31
22	-	23.93	1+08.90	1107742.5109	1941815.95
23	-	27.65	1+07.28	1107745.4571	1941813.16
24	636.16	34.67	1+00.81	1107749.8475	1941804.68
25	636.13	34.26	0+99.89	1107749.1555	1941803.96
26	-	46.56	0+99.85	1107760.7102	1941799.74
27	-	46.55	0+97.39	1107759.8654	1941797.43
28	636.11	-22.05	1+12.07	1107700.3456	1941834.57
29	636.12	21.72	1+11.93	1107741.4574	1941819.55
CP#1	637.10	-24.27	1+19.50	1107700.7921	1941842.32
CP#2	637.49	52.57	1+16.30	1107771.9524	1941813.17

LEGEND

xx.xx'

EXISTING LENGTH

==

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 638.58

BENCHMARK: CUT SQUARE ON SE CORNER OF CONCRETE BASE OF TRAFFIC CONTROL BOX

LOCATION: TRAFFIC CONTROL BOX ON NE CORNER OF HIGGINS AND 1700 HIGGINS CENTER

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
pwz\VL084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT_Offices\District 1\Projects\104517\CADDData\Design\DRAWN7-shd-details.dgn			REVISED -
Default	PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/2/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

1700 HIGGINS ENTRANCE DETAILS (SHEET 2)
IL 72 (TOUHY AVENUE TO CUMBERLAND AVENUE)

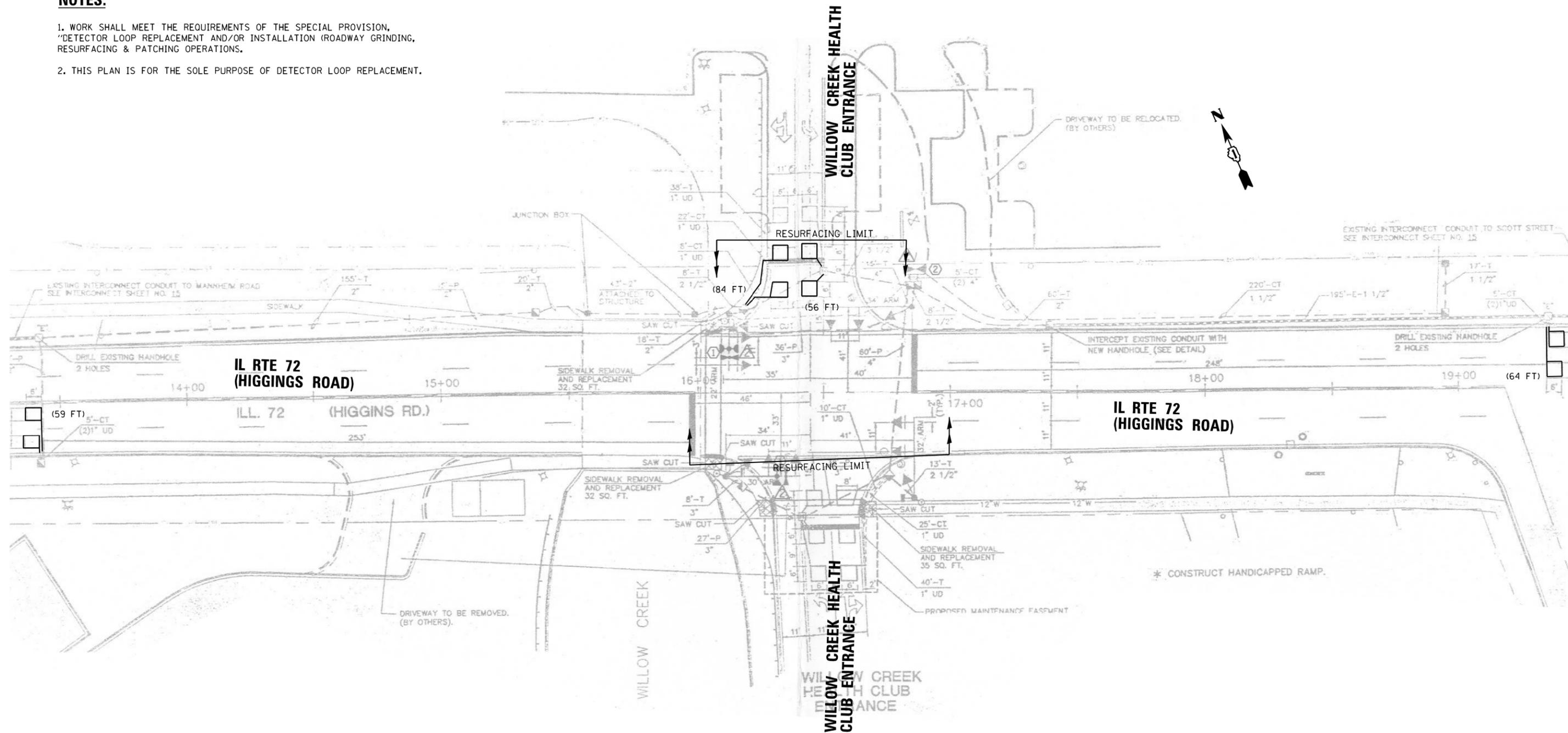
SCALE: 1"=5'

SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2106-058-R5	COOK	63	30
CONTRACT NO 62D51				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



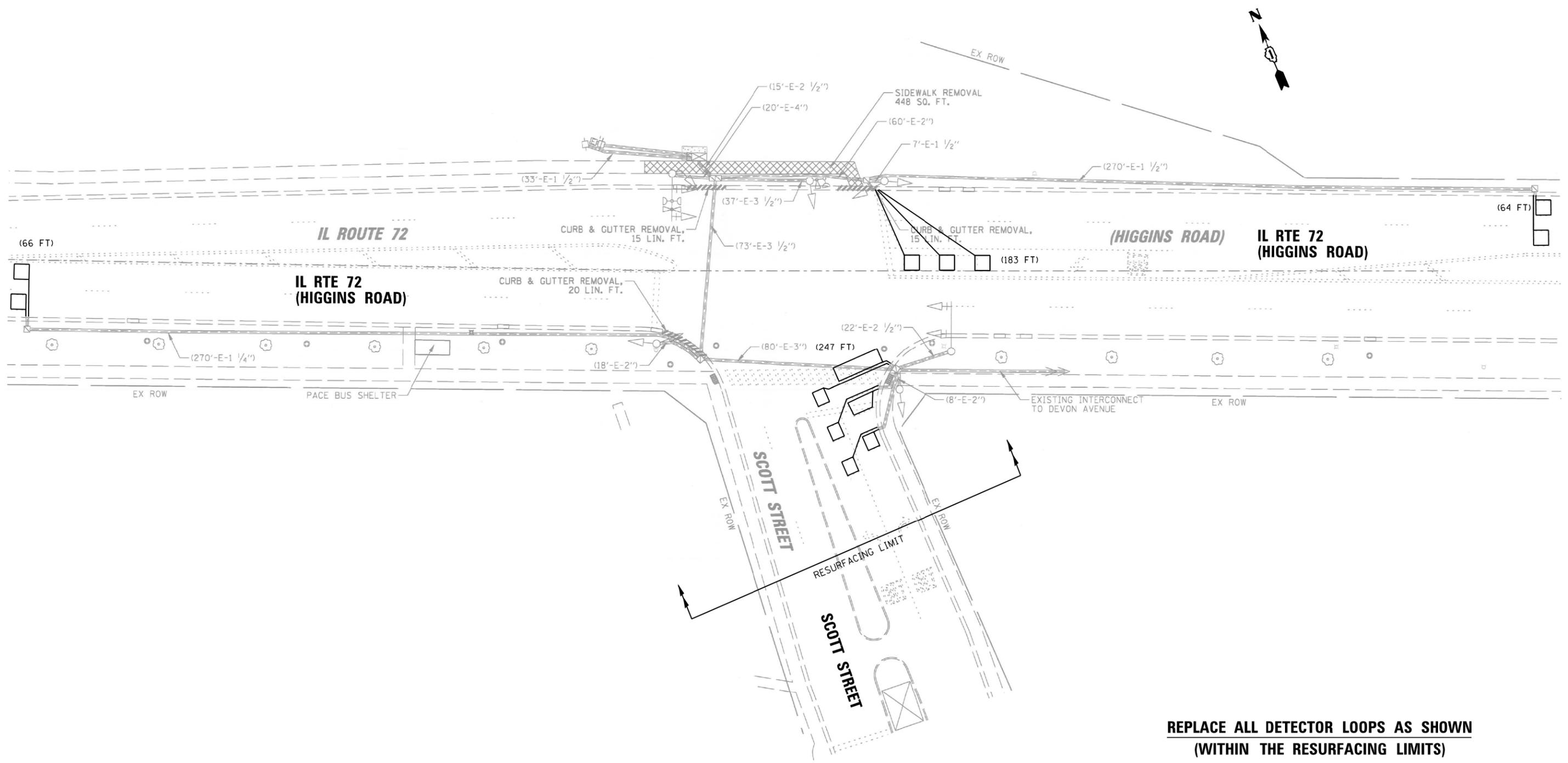
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	263	FOOT

FILE NAME = TS 1102 IL RTE 72 & WILLOW CREEK.03092017.dgn	USER NAME = SNOWBA	DESIGNED - ZGJ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL RTE 72 (HIGGINS ROAD) AT WILLOW CREEK HEALTH CLUB ENTRANCE	F.A.U. = 1350	SECTION = 2016-058-RS	COUNTY = COOK	TOTAL SHEETS = 63	SHEET NO. = 31
PLOT SCALE = 40,0000' / in.	CHECKED - LP	REVISIED -	SCALE: SHEET OF SHEETS STA. TO STA.			CONTRACT NO. 62D51				
DATE = 03/09/2017	REVISIED -					ILLINOIS FED. AID PROJECT				
Default	DATE = 8/1/2017									

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



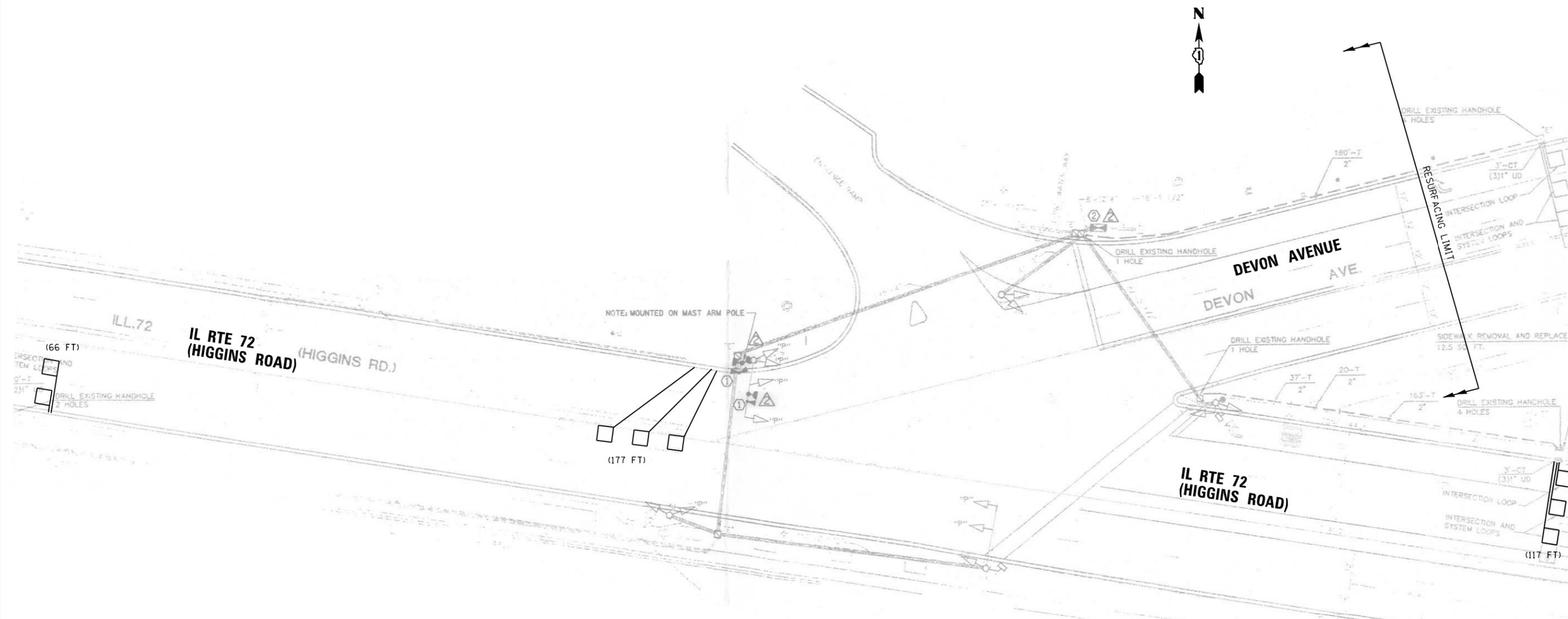
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	560	FOOT

FILE NAME =	USER NAME = jovanmardzgj	DESIGNED - ZGJ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL RTE 72 AT SCOTT STREET	F.A.U.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
TS 9090 IL RTE 72 & SCOTT STREET.03072017.dgn	DRAWN - ZGJ	REVISED -	1350			2016-058-RS	COOK	63	32			
Default	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISED -			CONTRACT NO. 62051						
	PLOT DATE = 3/7/2017	DATE - 03/07/2017	REVISED -			ILLINOIS FED. AID PROJECT						
						SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.

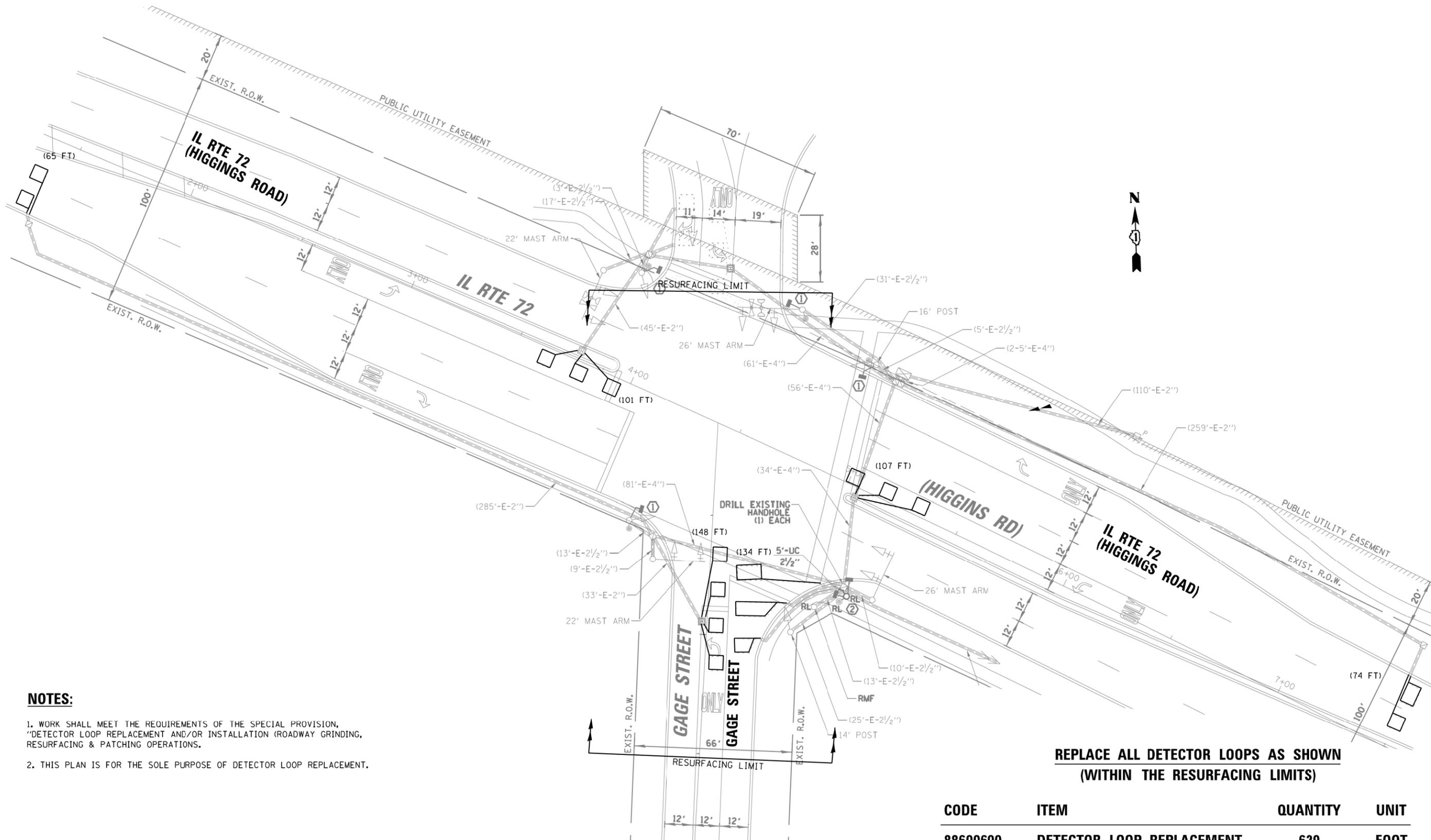
NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

ITEM	QUANTITY	UNIT
88600600 DETECTOR LOOP REPLACEMENT	360	FOOT



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	629	FOOT

FILE NAME =	USER NAME = jovanmardzgj	DESIGNED - ZGJ	REVISED -
TS 4982 IL RTE 72 & GAGE AVENUE.03072017.dgn		DRAWN - ZGJ	REVISED -
Default	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 3/9/2017	DATE - 03/09/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

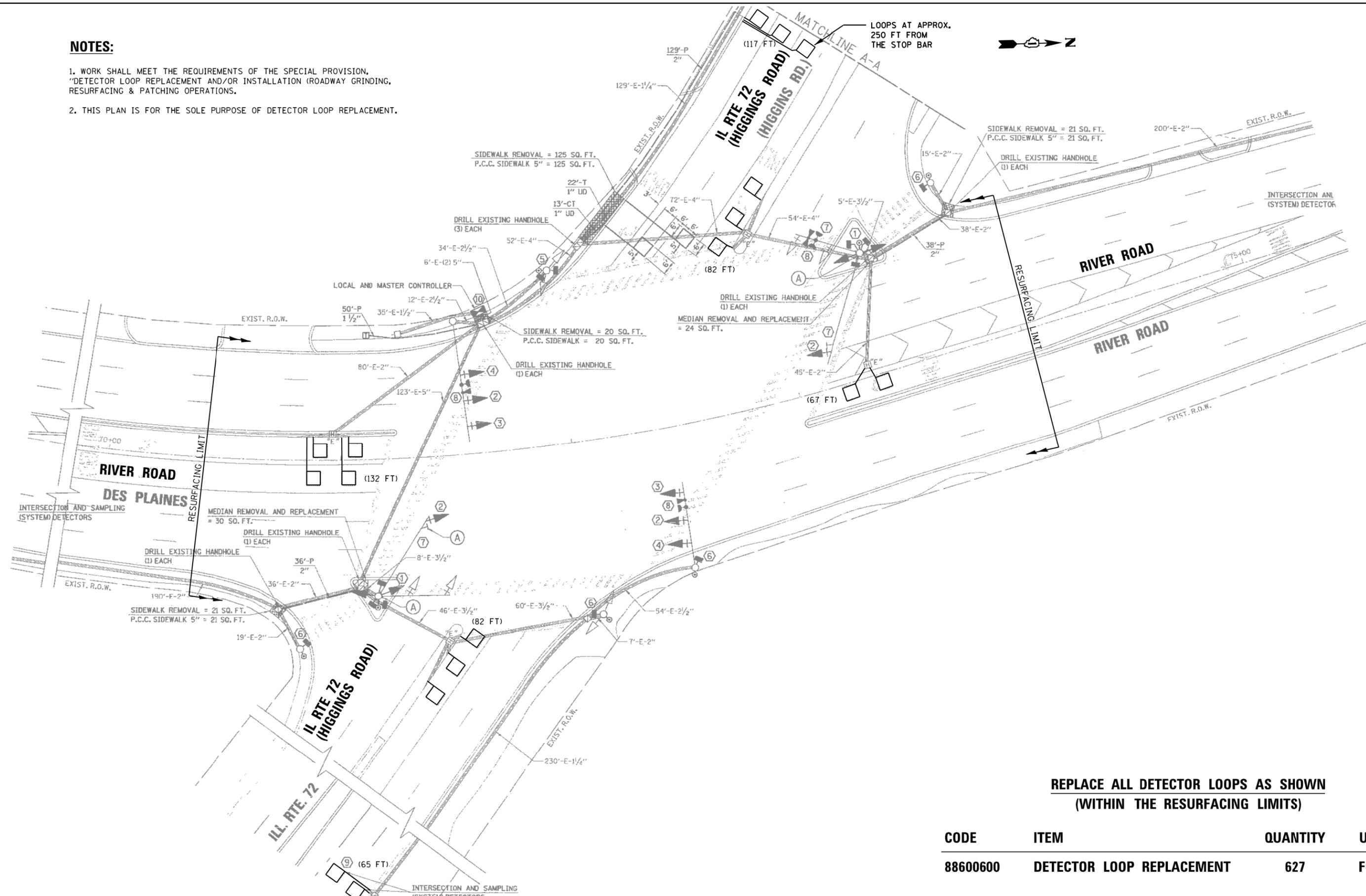
**DETECTOR LOOP REPLACEMENT PLAN
IL RTE 72 (HIGGINS ROAD) AT GAGE AVENUE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	34
CONTRACT NO. 62051				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



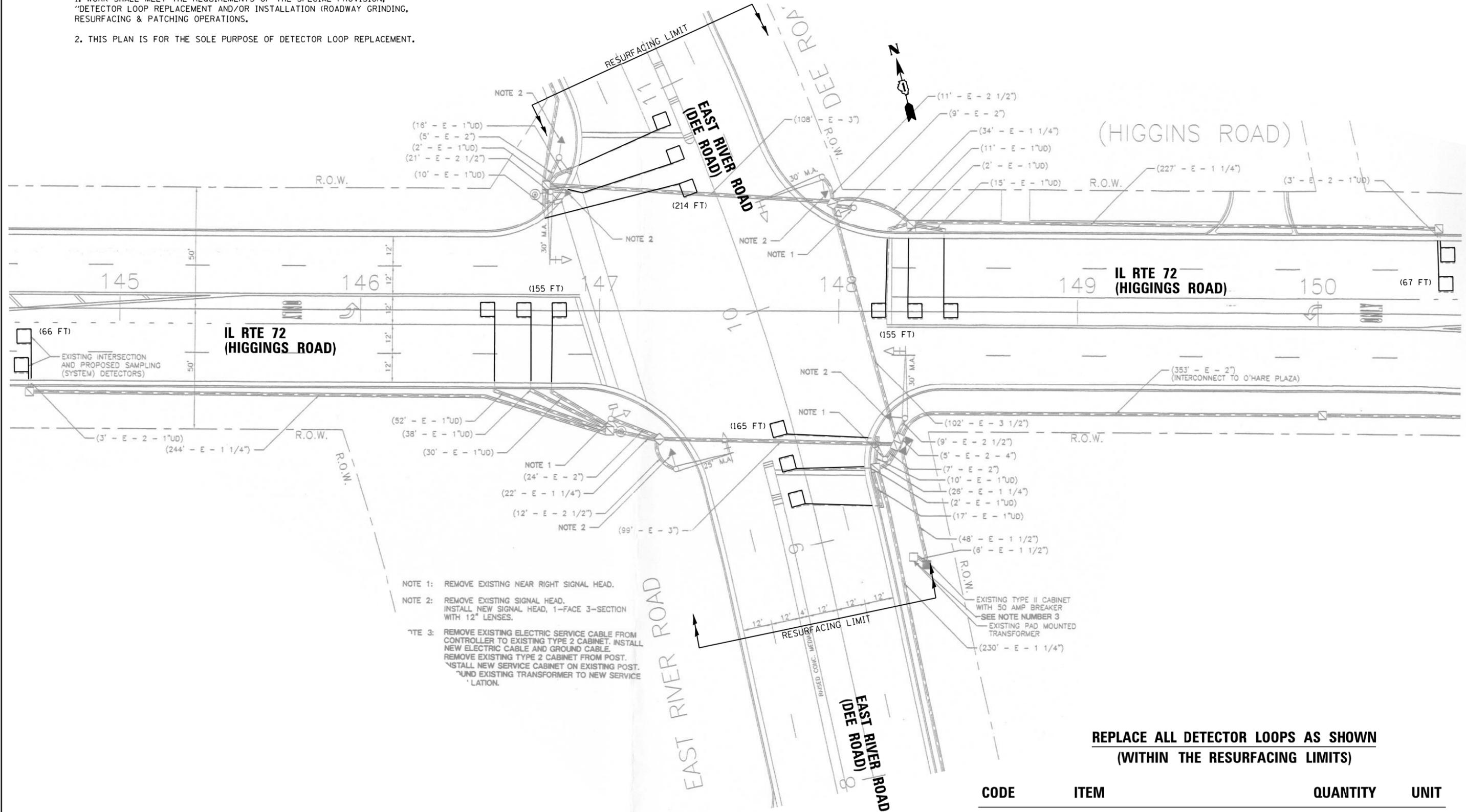
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	627	FOOT

FILE NAME = TS 9075 IL RTE 72 & RIVER ROAD_03072017.dgn	USER NAME = jovanmardzic	DESIGNED - ZGJ	REVISD -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL RTE 72 (HIGGINS ROAD) AT RIVER ROAD	F.A.U. = 1350	SECTION = 2016-058-RS	COUNTY = COOK	TOTAL SHEETS = 63	SHEET NO. = 35	
Default	PLOT SCALE = 40.0000' / in.	CHECKED - LP	REVISD -			CONTRACT NO. 62051					
	PLOT DATE = 3/7/2017	DATE - 03/07/2017	REVISD -			ILLINOIS FED. AID PROJECT					
						SCALE:	SHEET	OF	SHEETS	STA.	TO

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



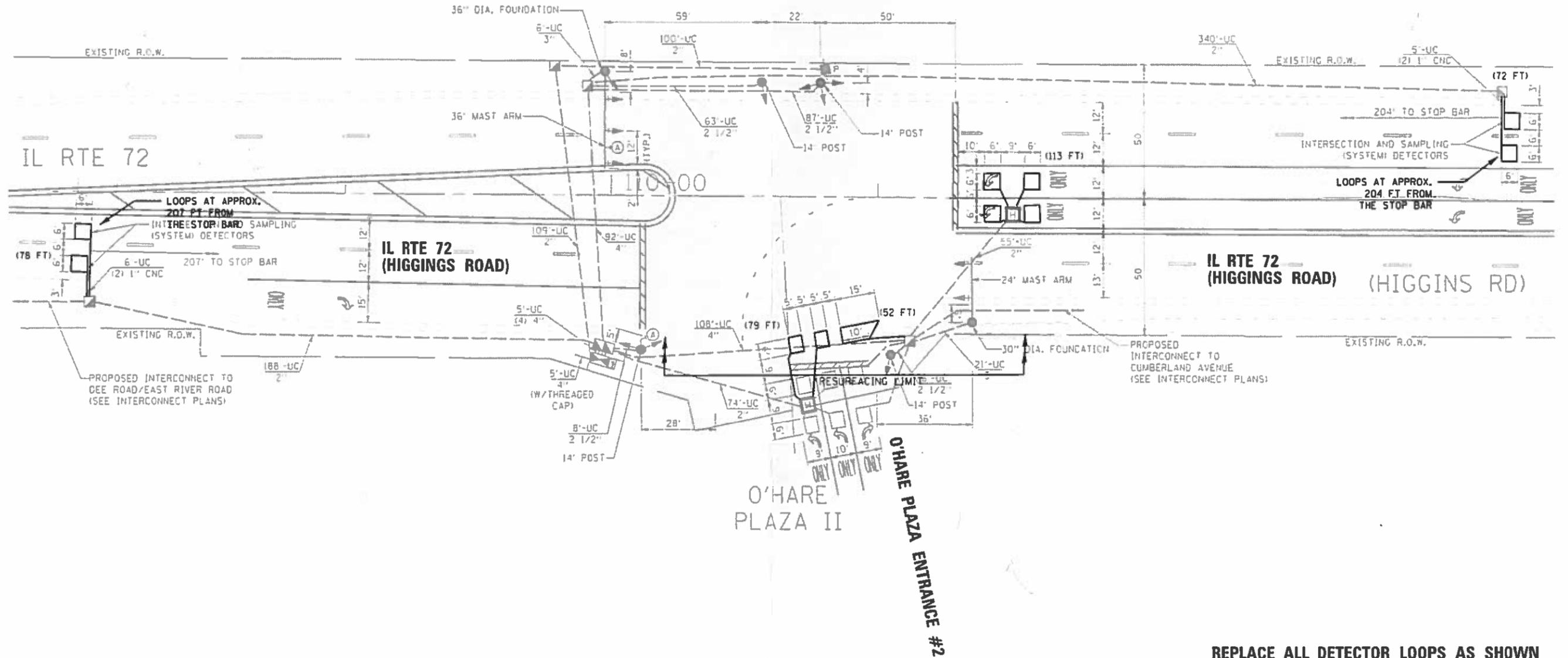
- NOTE 1: REMOVE EXISTING NEAR RIGHT SIGNAL HEAD.
- NOTE 2: REMOVE EXISTING SIGNAL HEAD. INSTALL NEW SIGNAL HEAD, 1-FACE 3-SECTION WITH 12" LENSES.
- NOTE 3: REMOVE EXISTING ELECTRIC SERVICE CABLE FROM CONTROLLER TO EXISTING TYPE 2 CABINET. INSTALL NEW ELECTRIC CABLE AND GROUND CABLE. REMOVE EXISTING TYPE 2 CABINET FROM POST. INSTALL NEW SERVICE CABINET ON EXISTING POST. GROUND EXISTING TRANSFORMER TO NEW SERVICE CABINET.

**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	822	FOOT

NOTES:

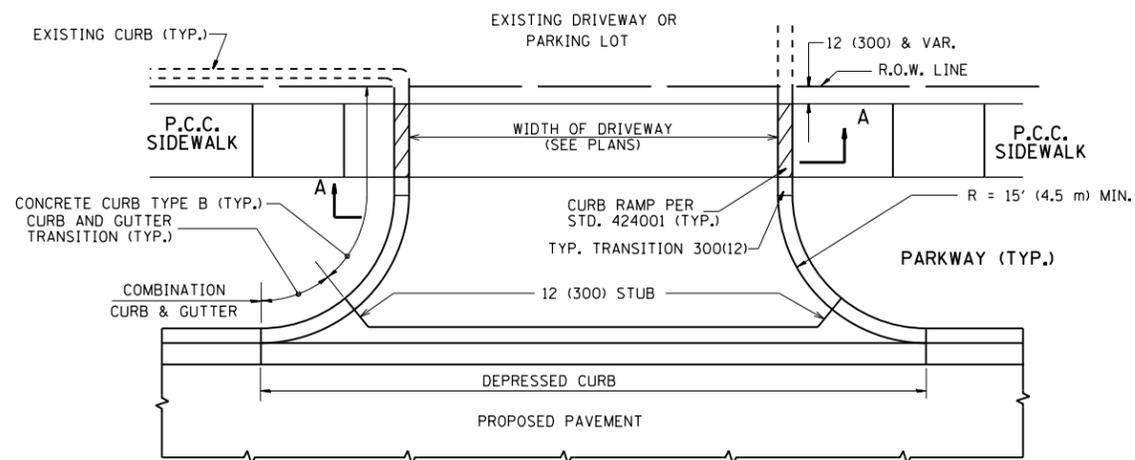
1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



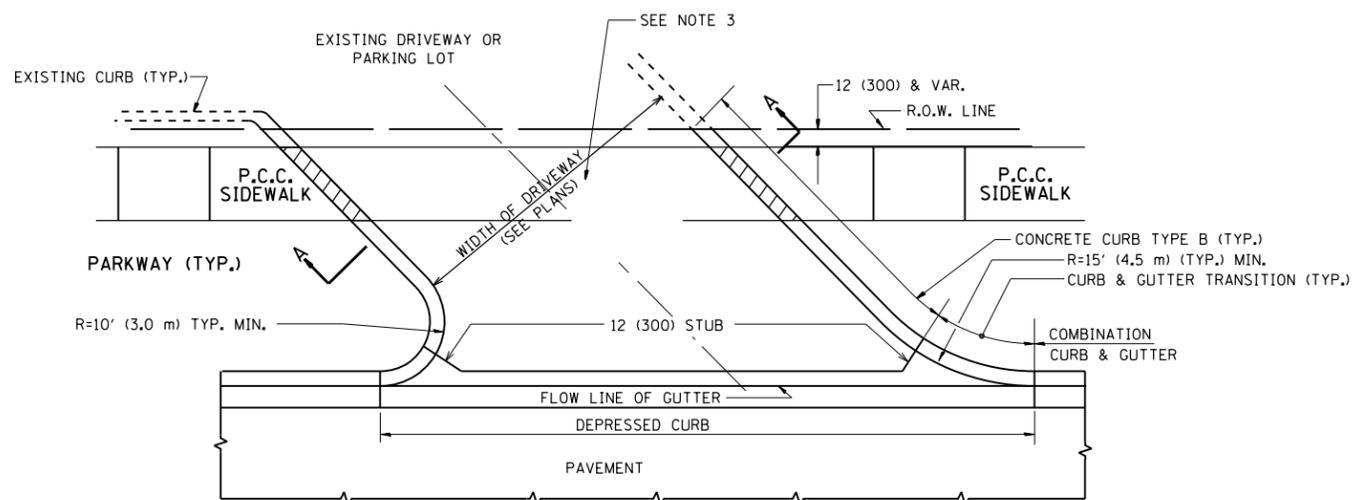
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	394	FOOT

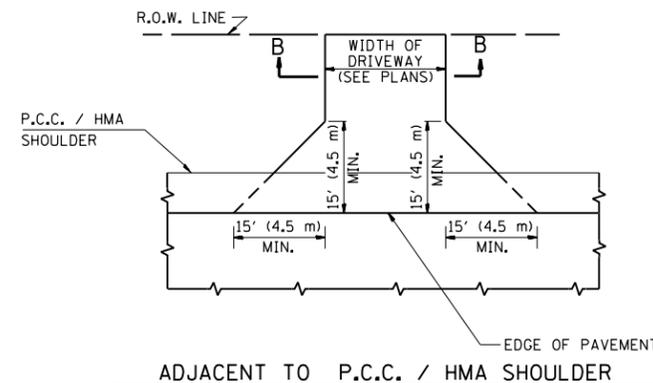
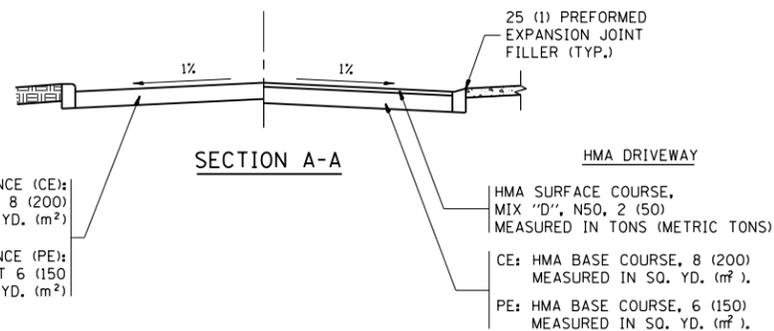
FILE NAME = 15 3265 IL RTE 72 & O'HARE PLAZA ENTRANCE 2.05222817.dgn	USER NAME = SNOUBA	DESIGNED - BAS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN IL RTE 72 (HIGGINS ROAD) AT O'HARE PLAZA ENTRANCE #2	F.A.U. = 1350	SECTION = 2016-058-RS	COUNTY = COOK	TOTAL SHEETS = 63	SHEET NO. = 37	
PLOT SCALE = 48.8291 / 1" = 100'	CHECKED - LP	DATE = 06/22/2017	REVISED -			SCALE: SHEET OF SHEETS STA. TO STA.	CONTRACT NO. 62D51				
PLOT DATE = 6/22/2017	DATE = 06/22/2017	REVISED -				ILLINOIS FED. AID PROJECT					
Default											



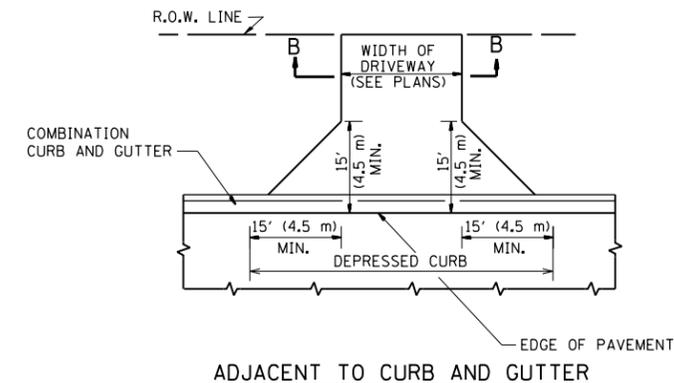
WITH CONCRETE CURB, TYPE B



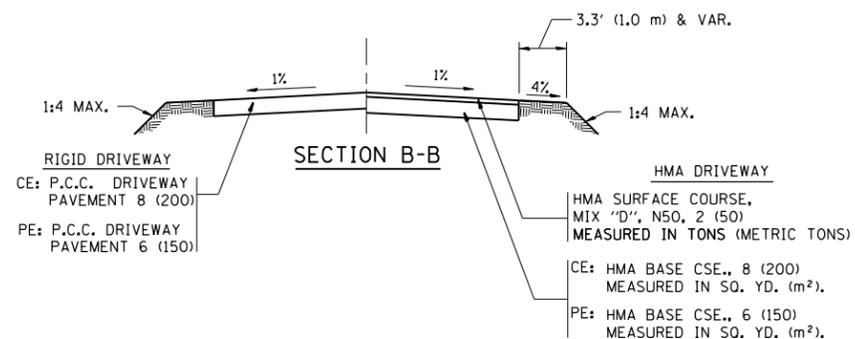
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

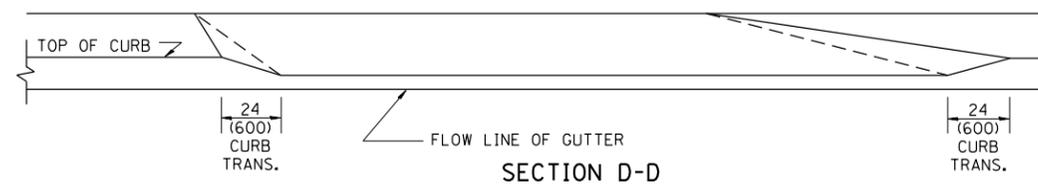
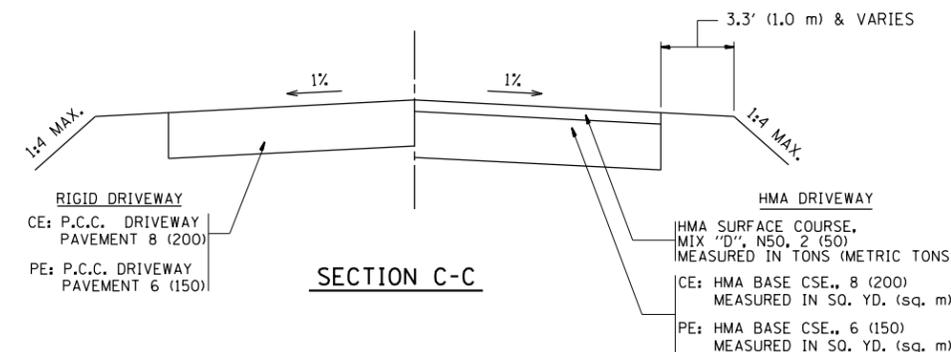
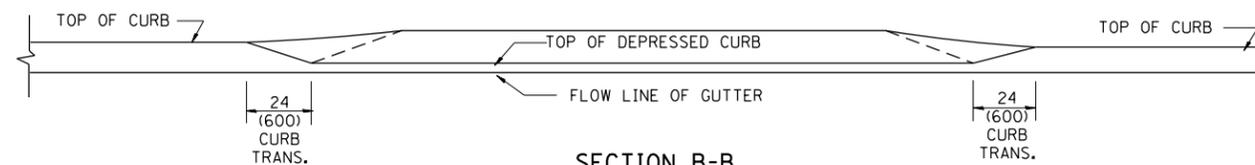
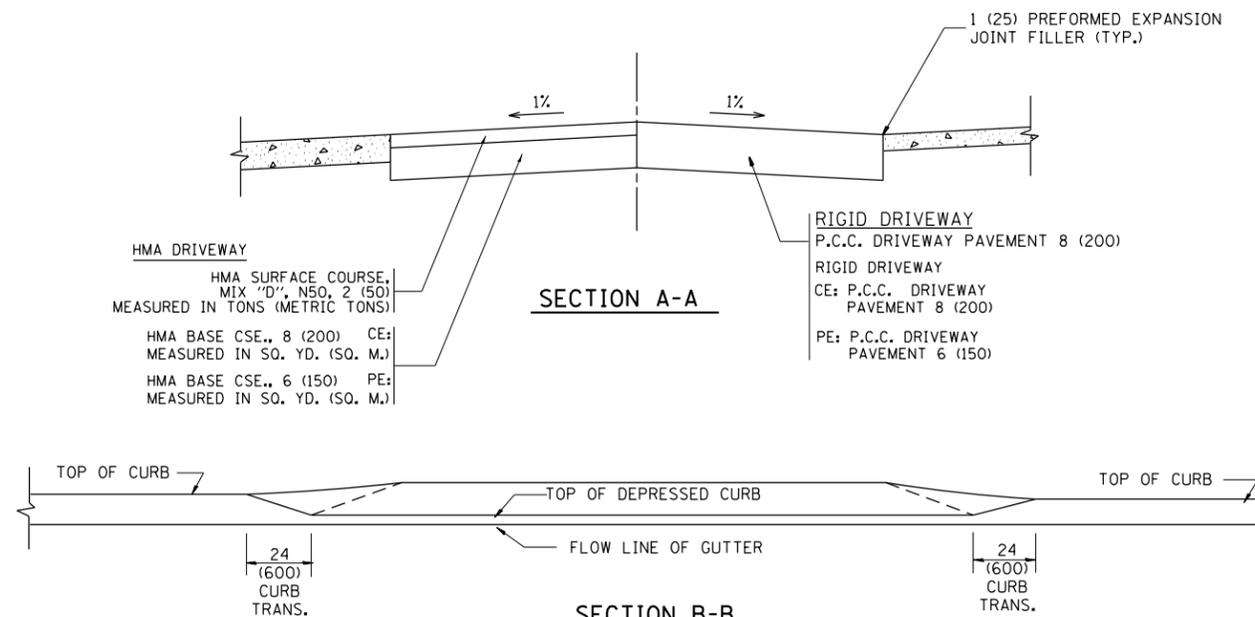
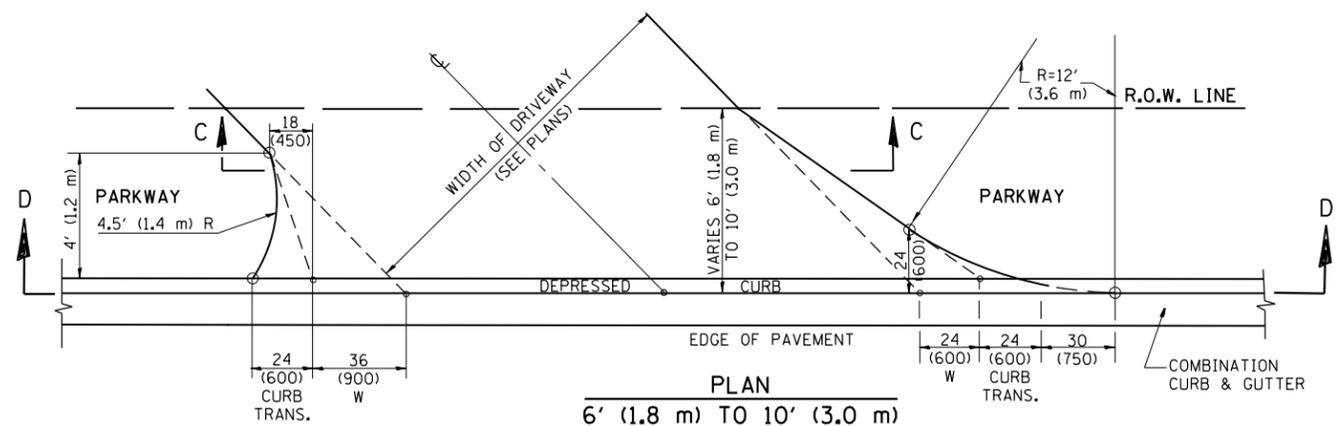
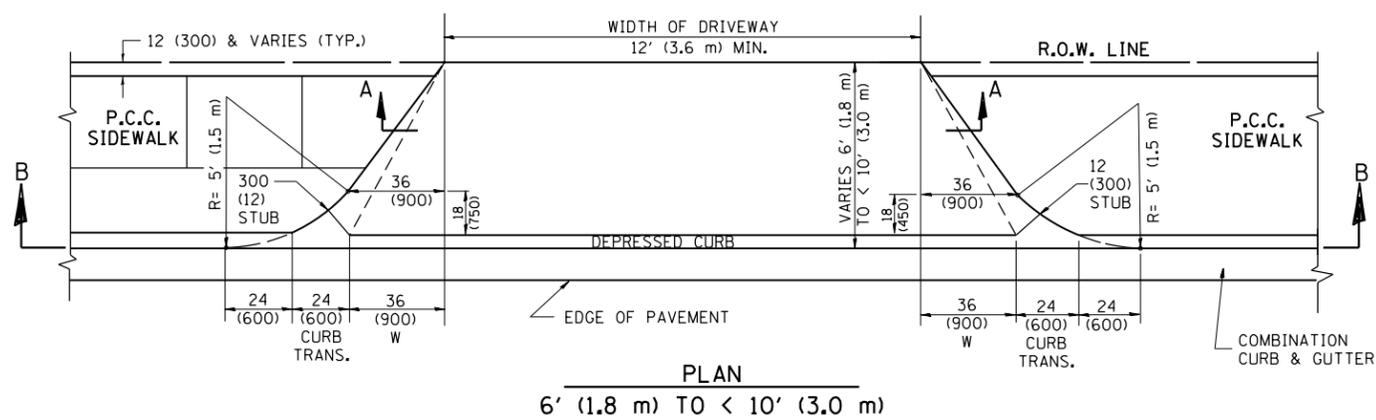
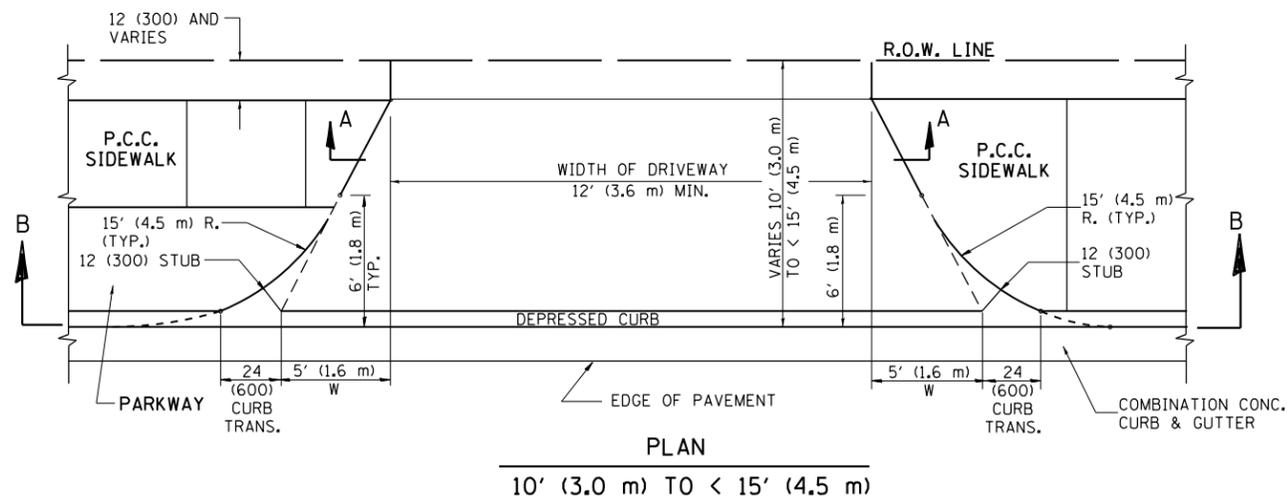
FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
pw:\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI045\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
		DATE - 11-04-95	REVISED - R. BORO 06-11-08
			REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	38
BD0156-07 (BD-01)			CONTRACT NO. 62D51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

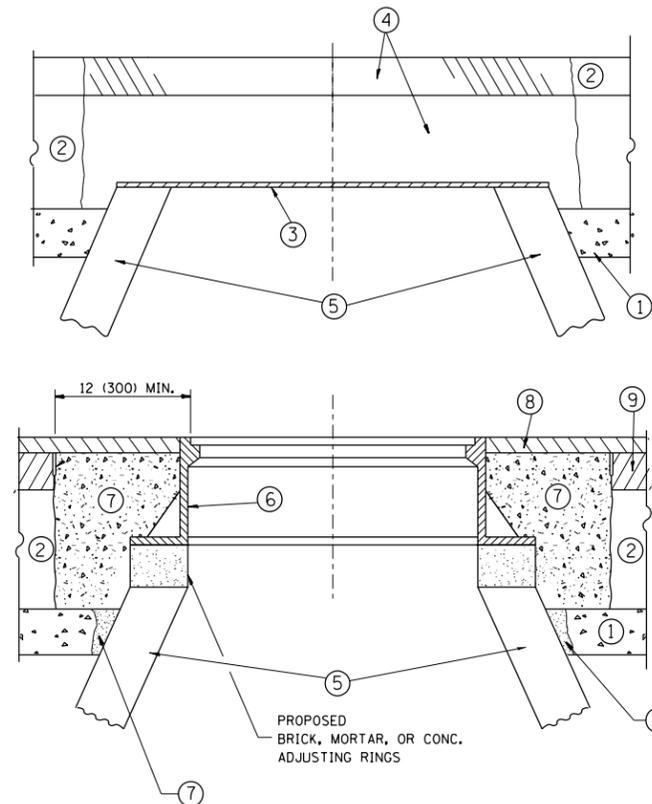
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
pw\1\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI045\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - P. LaFLEUR 04-15-03
		DATE - 11-06-95	REVISED - R. BORO 01-01-07
			REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS			
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	39
BD400-02 (BD-02)			CONTRACT NO. 62D51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

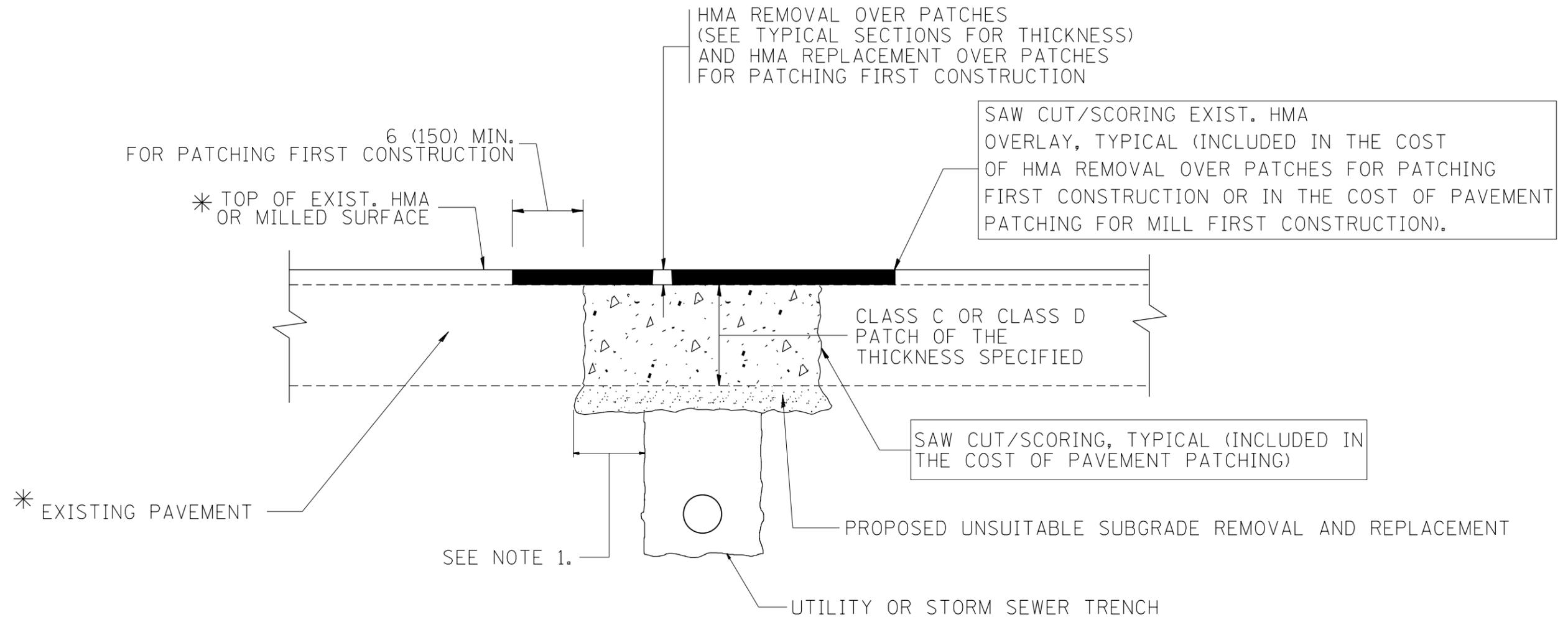
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI045\Drawings\Design\Diststd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 2/2/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	40
BD600-03 (BD-8)		CONTRACT NO. 62D51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI045\Drawings\Design\Diststd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					1350	2016-058-RS	COOK	63	41
PLOT DATE = 2/2/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07	REVISED - K. ENG 10-27-08		BD400-04 (BD-22)			CONTRACT NO. 62D51				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

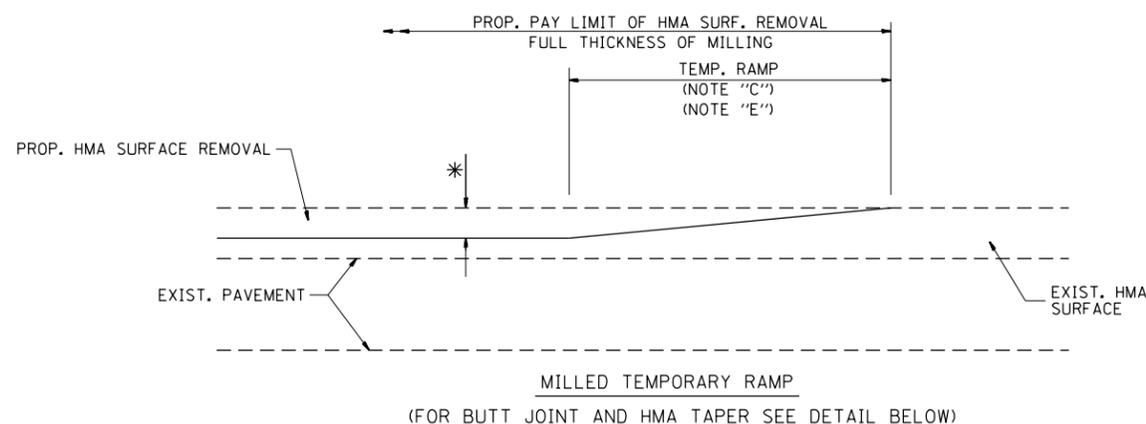
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

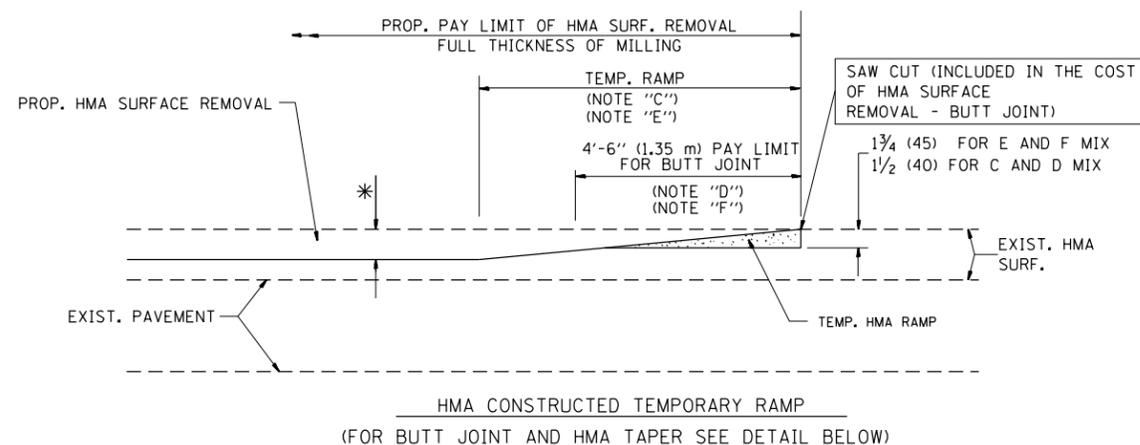
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI045\Drawings\Design\Diststd.dgn		REVISION	REVISED - A. ABBAS 03-21-97			1350	2016-058-RS	COOK	63	42
		CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 62D51		
		DATE - 03-11-94	REVISED - R. BORO 12-15-09			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.

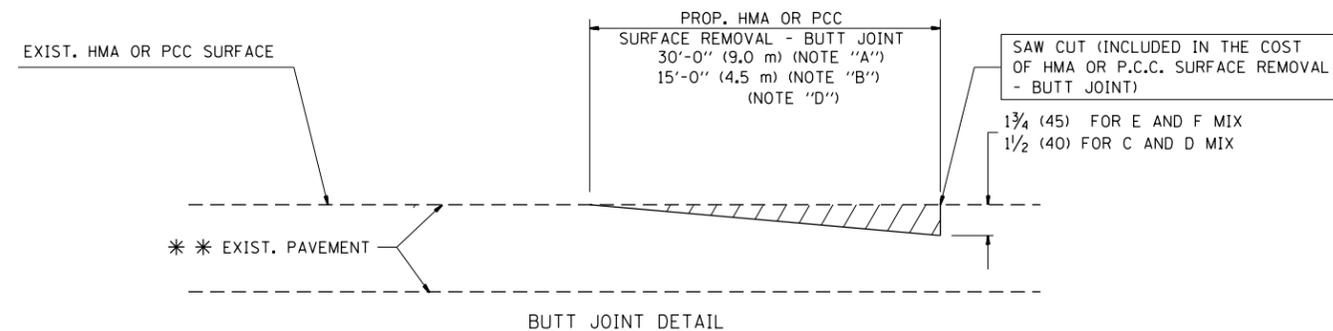


OPTION 1

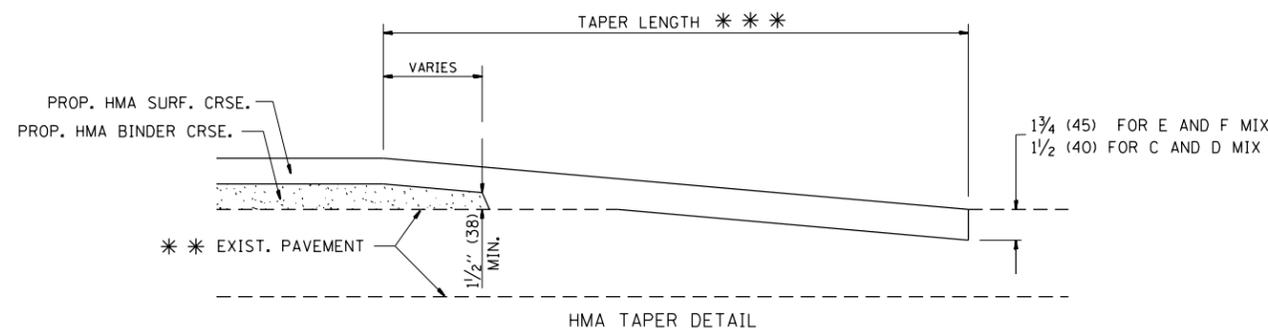


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

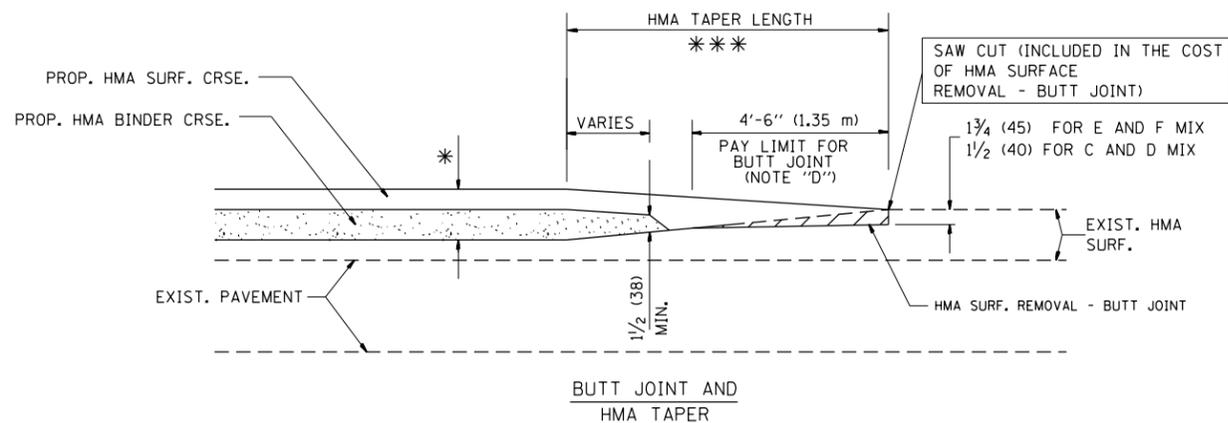
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

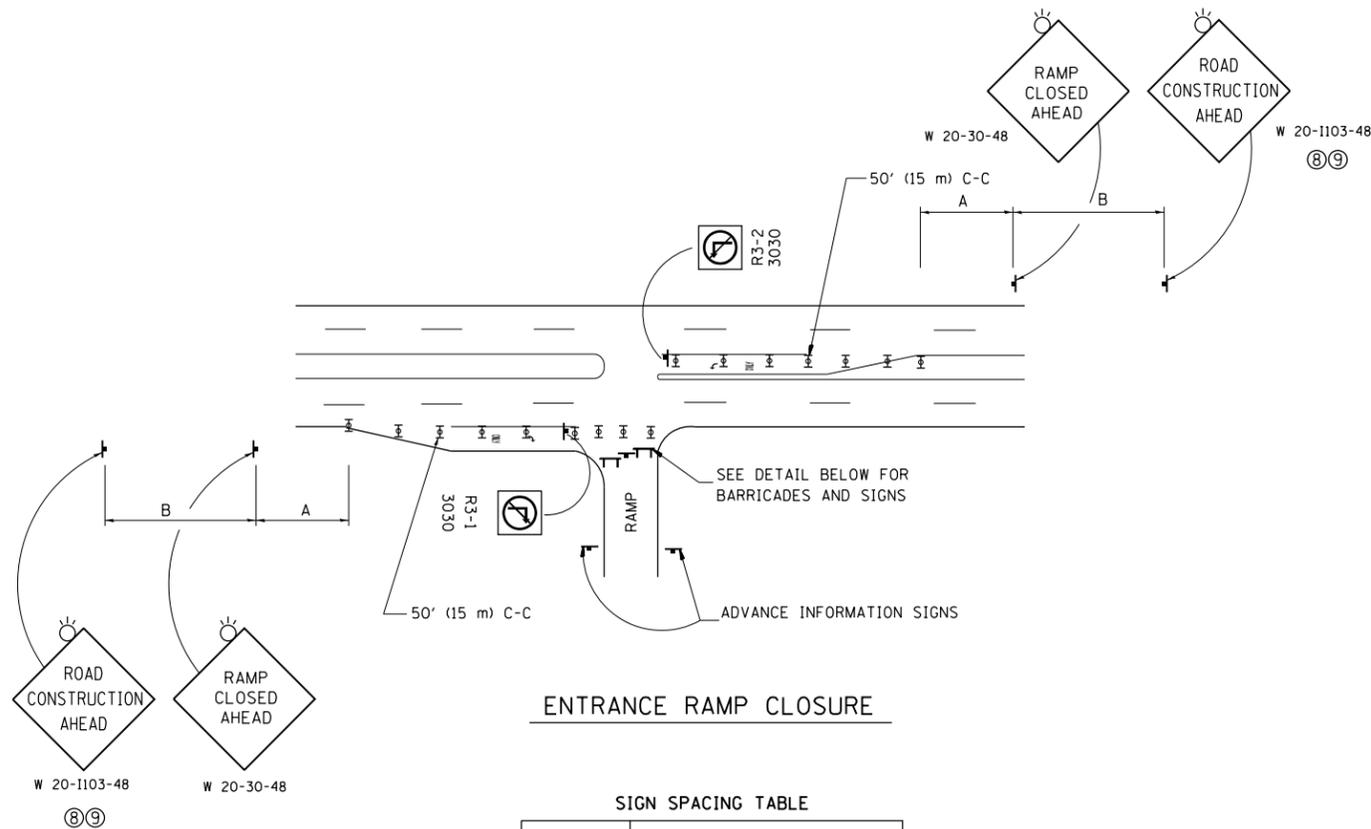
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	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 2/2/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	43
BD400-05 BD32		CONTRACT NO. 62D51		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

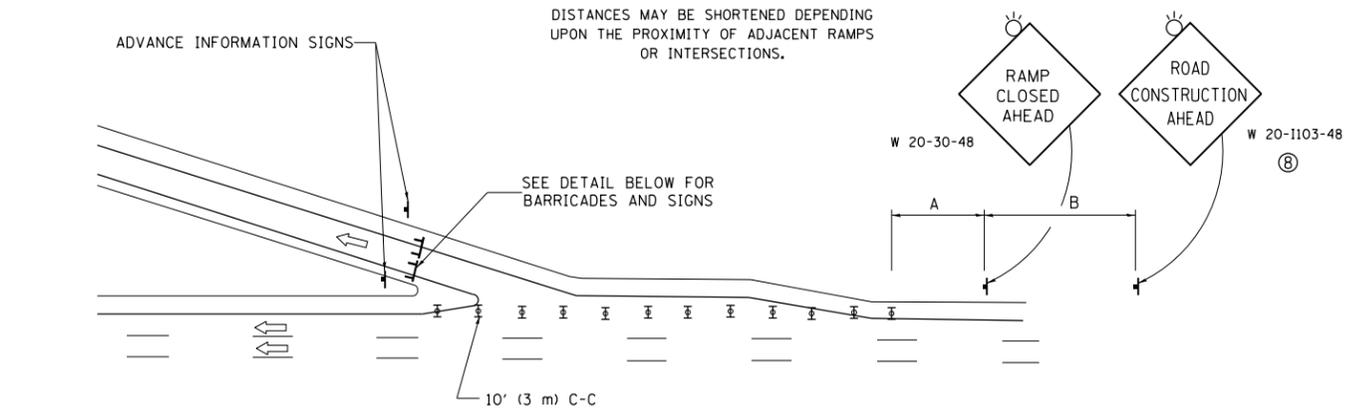


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

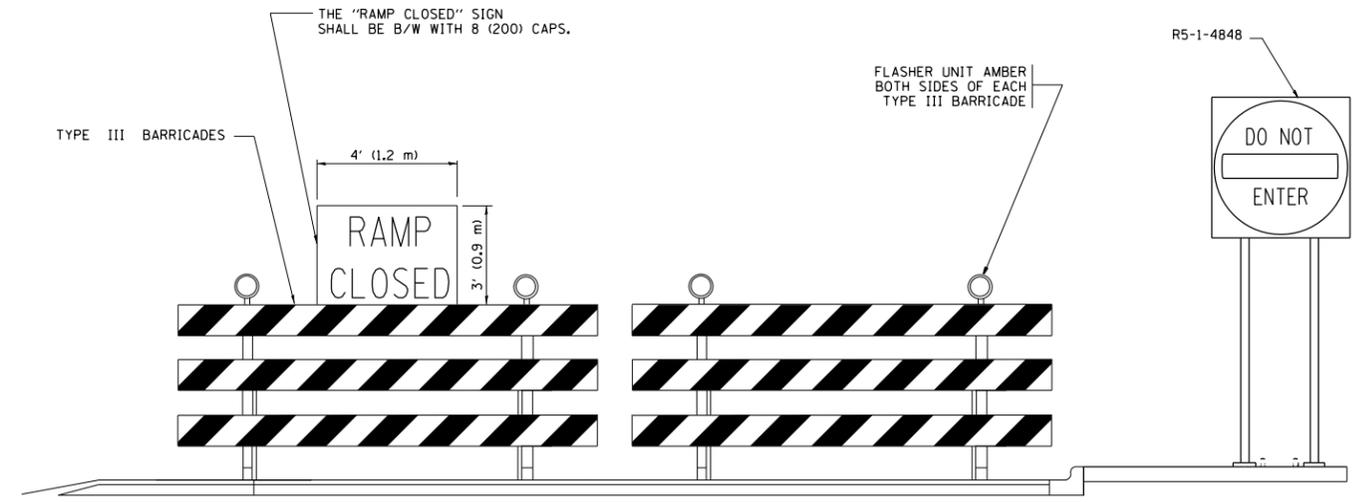
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

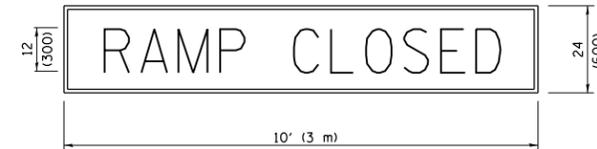
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM
- ⊓ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

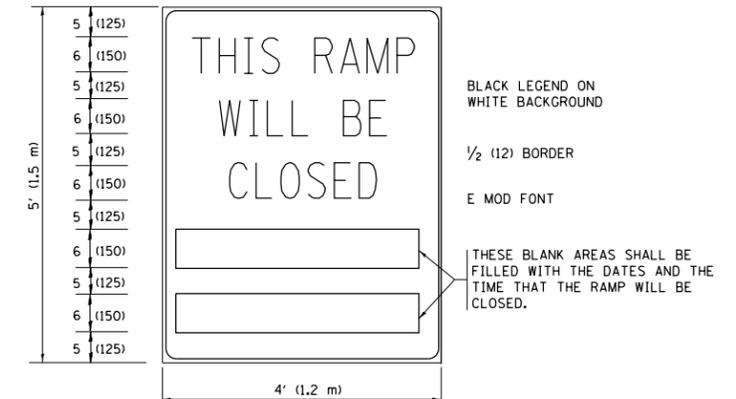
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

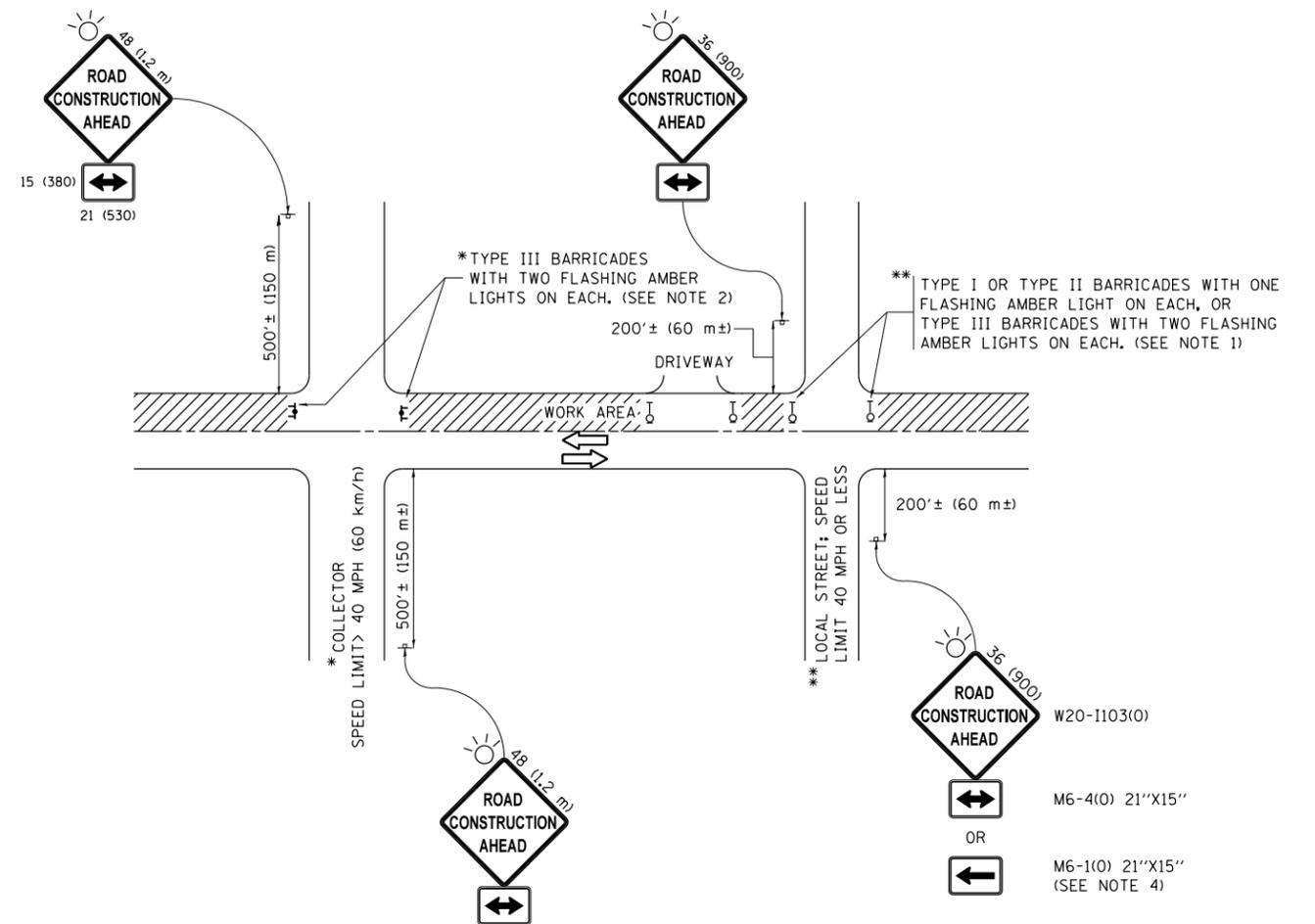
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Default	PLOT SCALE = 100.0000' / 1in.	DATE - 02-83	REVISED - M.D. 06-13
	PLOT DATE = 2/2/2018		REVISED - M.D. 01-18

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
190	2016-058-RS	COOK	63	45
TC-08		CONTRACT NO. 62D51		
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

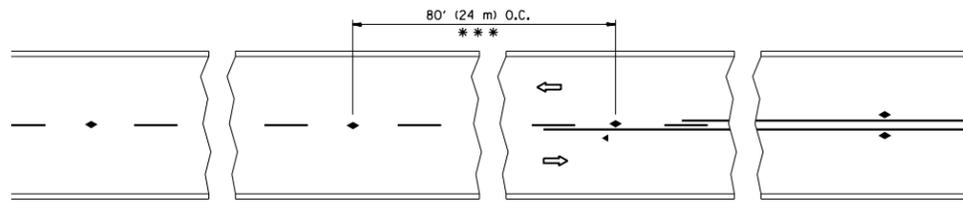
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	PLOT DATE = 2/2/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

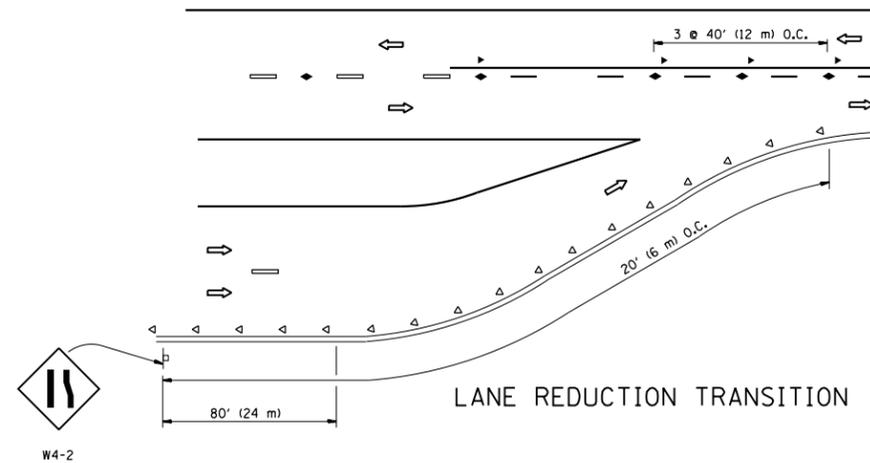
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	46
TC-10			CONTRACT NO. 62D51	
ILLINOIS FED. AID PROJECT				

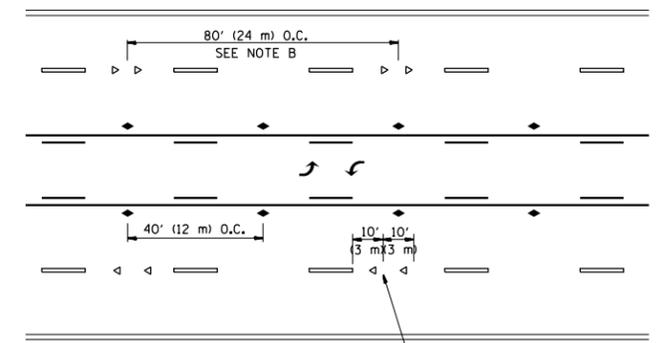


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

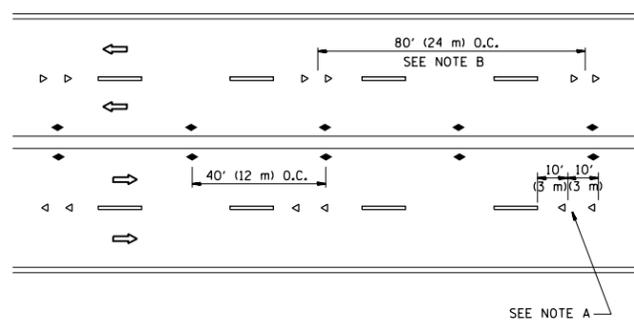
TWO-LANE/TWO-WAY



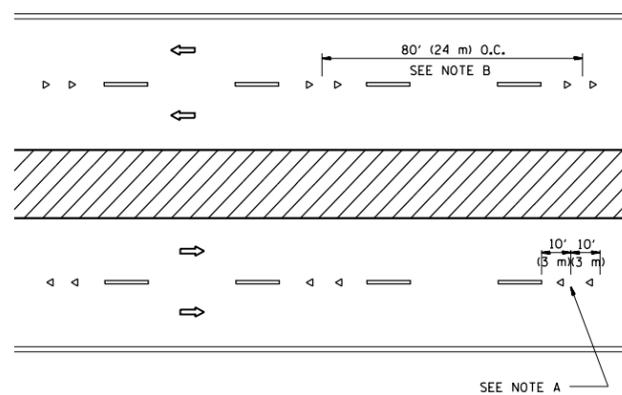
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

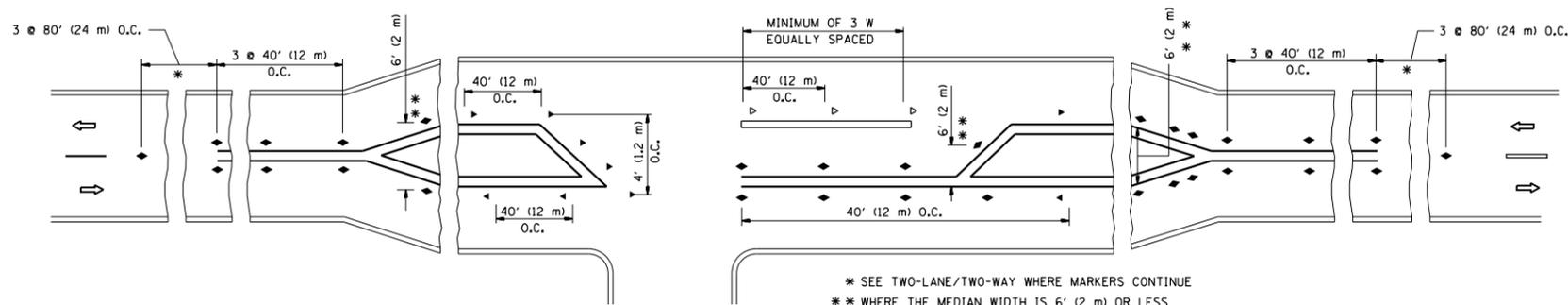
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

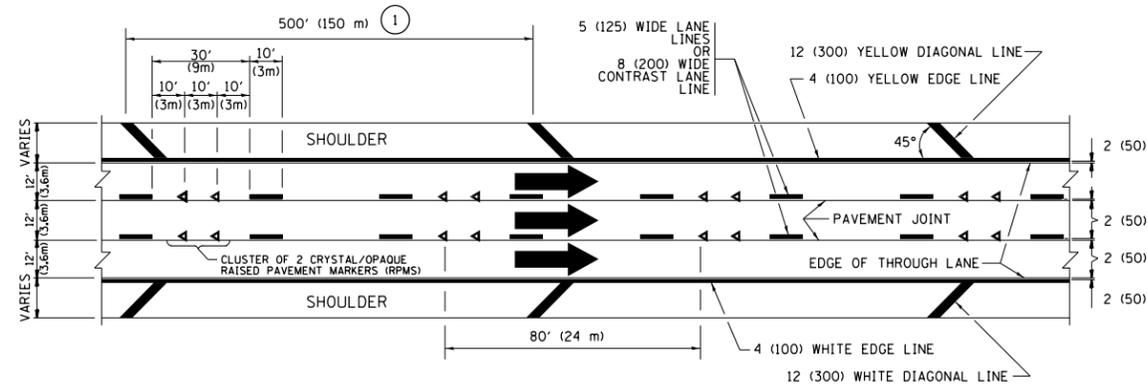
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw:\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI045\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - T. RAMMACHER 03-12-99
		DATE -	REVISED - T. RAMMACHER 01-06-00
			REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

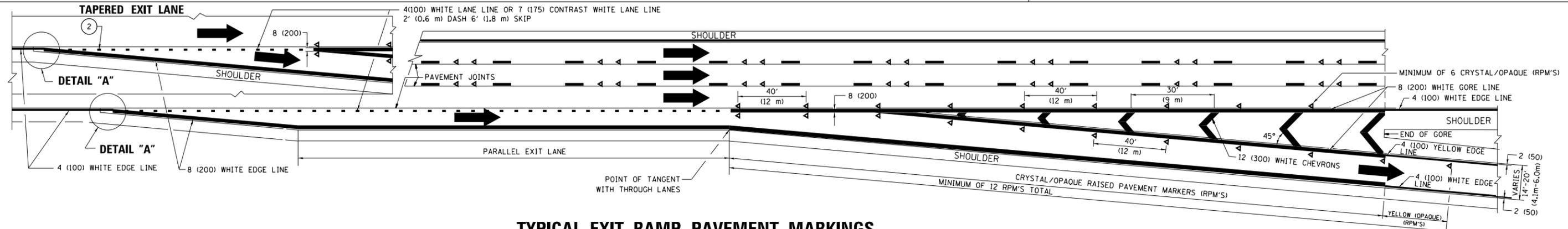
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	47
TC-11			CONTRACT NO. 62D51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



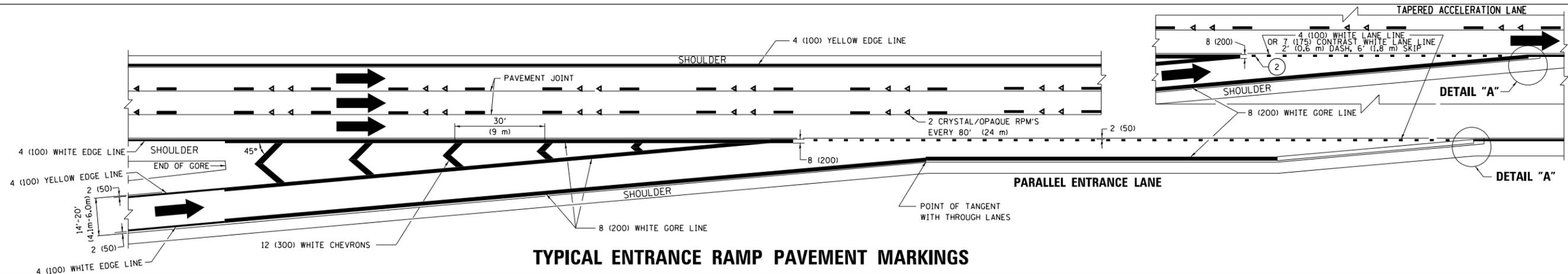
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

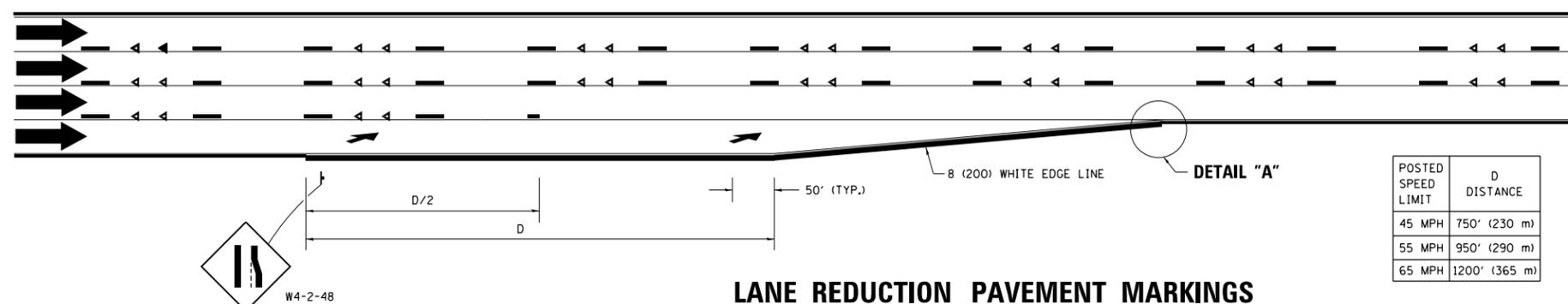
1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



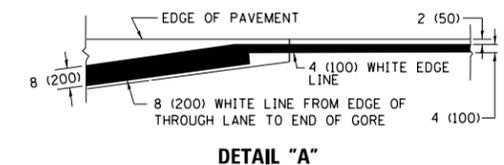
TYPICAL EXIT RAMP PAVEMENT MARKINGS



TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



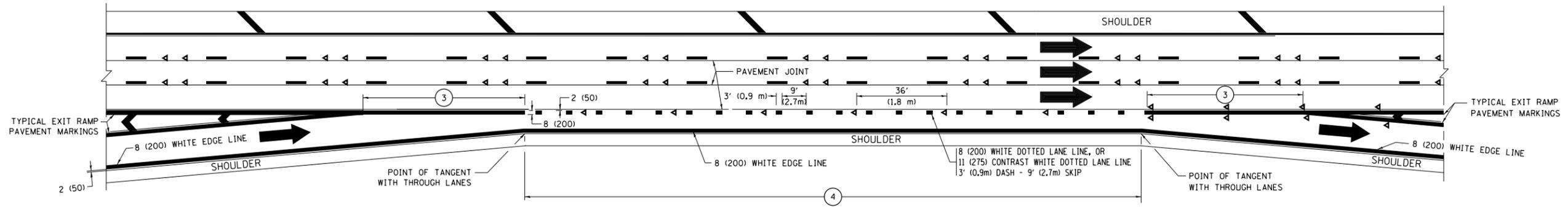
LANE REDUCTION PAVEMENT MARKINGS



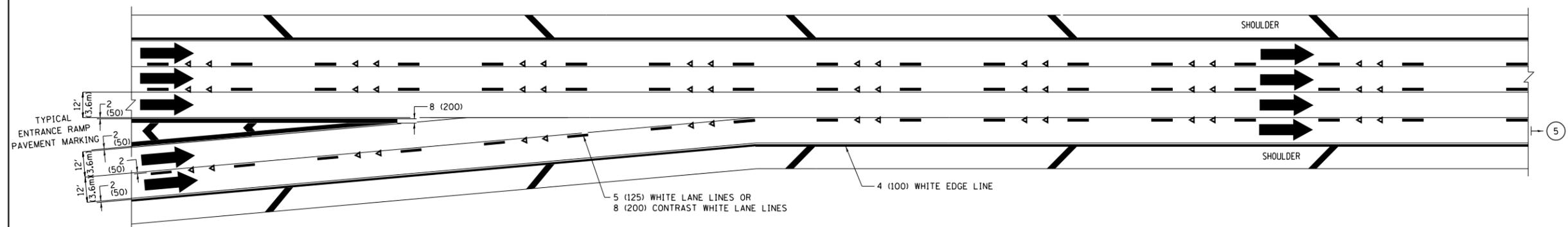
NOTES:

1. THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
2. 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

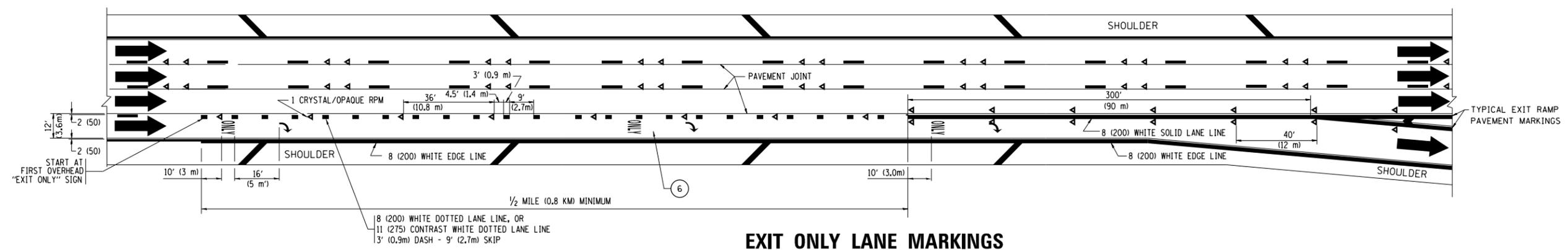
POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



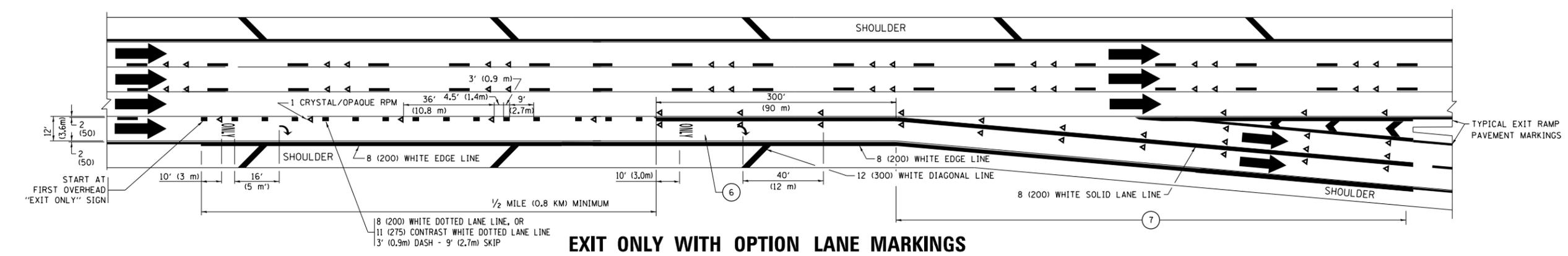
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS

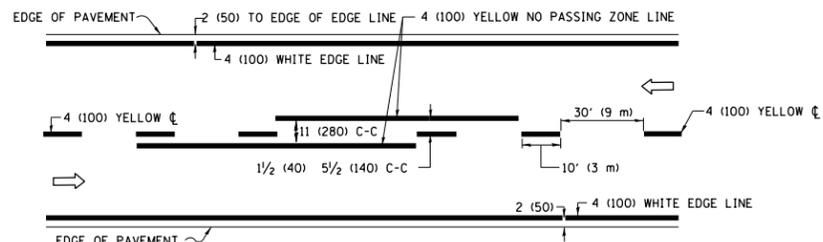


EXIT ONLY LANE MARKINGS

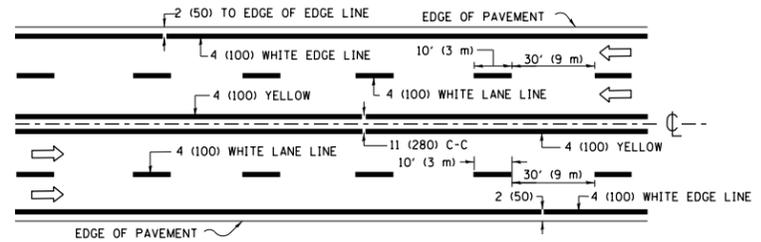


EXIT ONLY WITH OPTION LANE MARKINGS

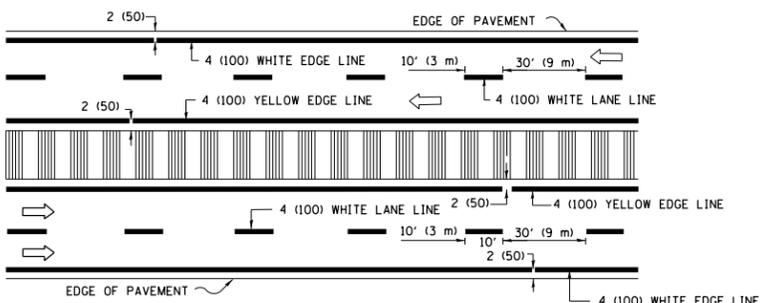
- NOTES:**
- 3 OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - 4 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - 5 FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - 6 ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - 7 CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.



2-LANE ROADWAY

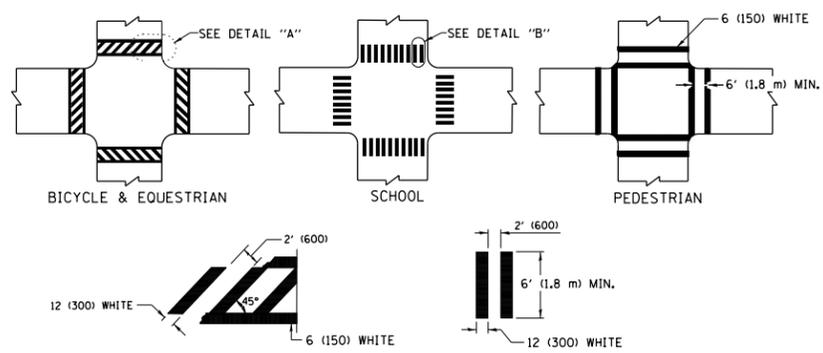


MULTI-LANE UNDIVIDED



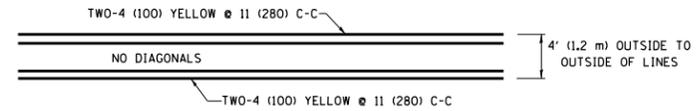
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

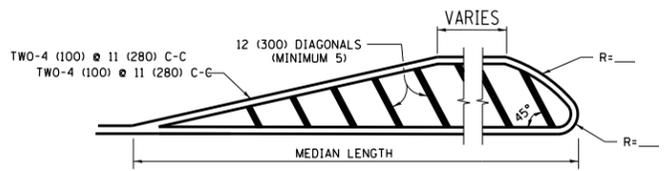


TYPICAL CROSSWALK MARKING

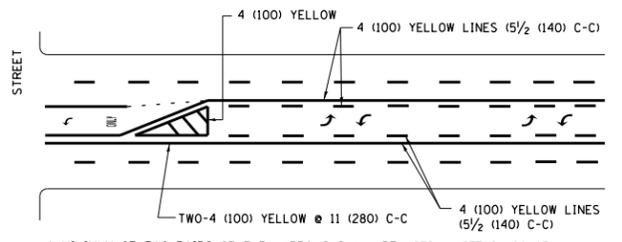
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



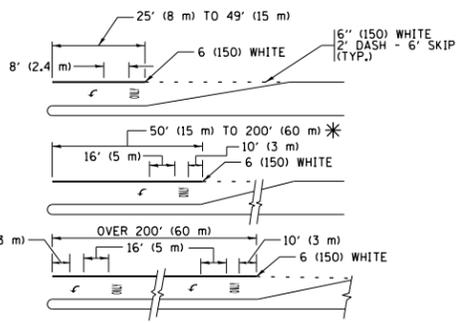
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE



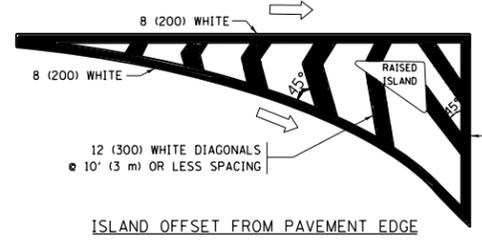
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



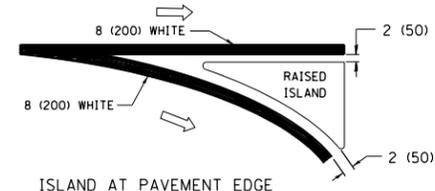
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

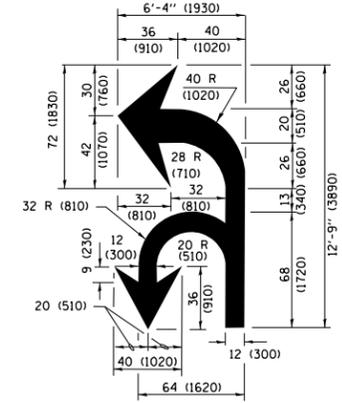
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



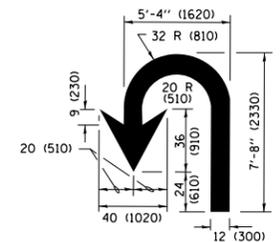
ISLAND OFFSET FROM PAVEMENT EDGE



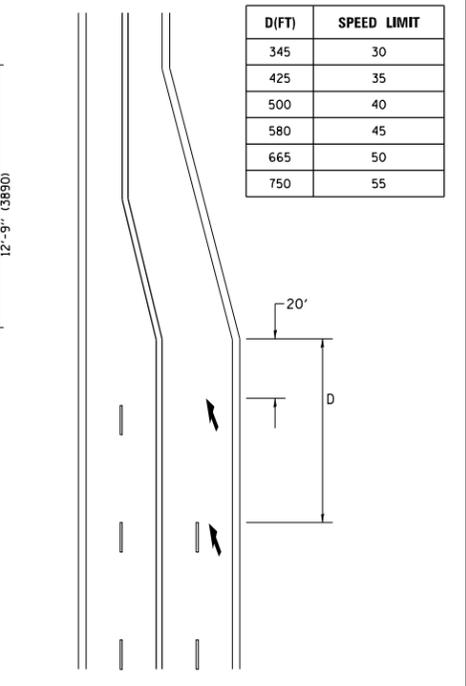
ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN



LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01045\Drawings\Design\Diststd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 2/2/2018		REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	50
TC-13		CONTRACT NO. 62D51		
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

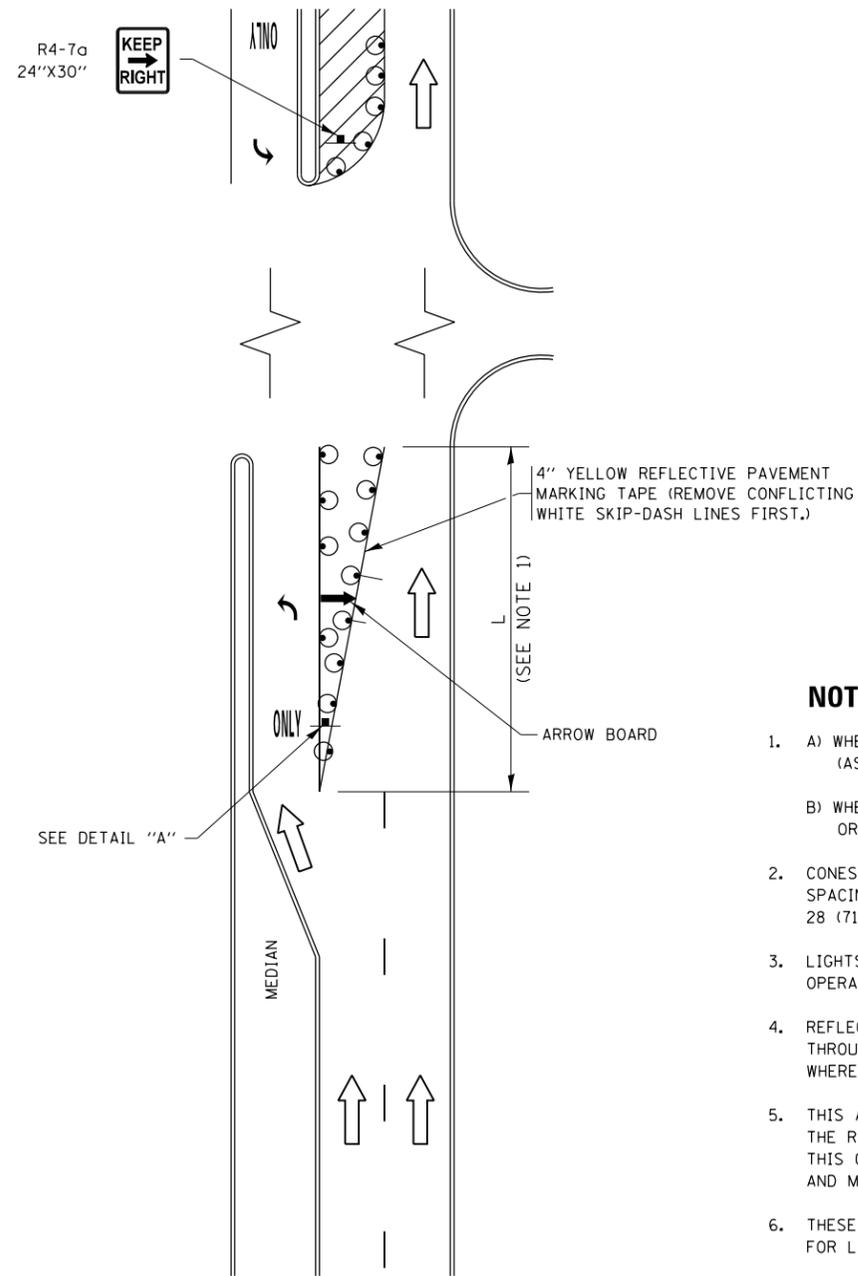


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

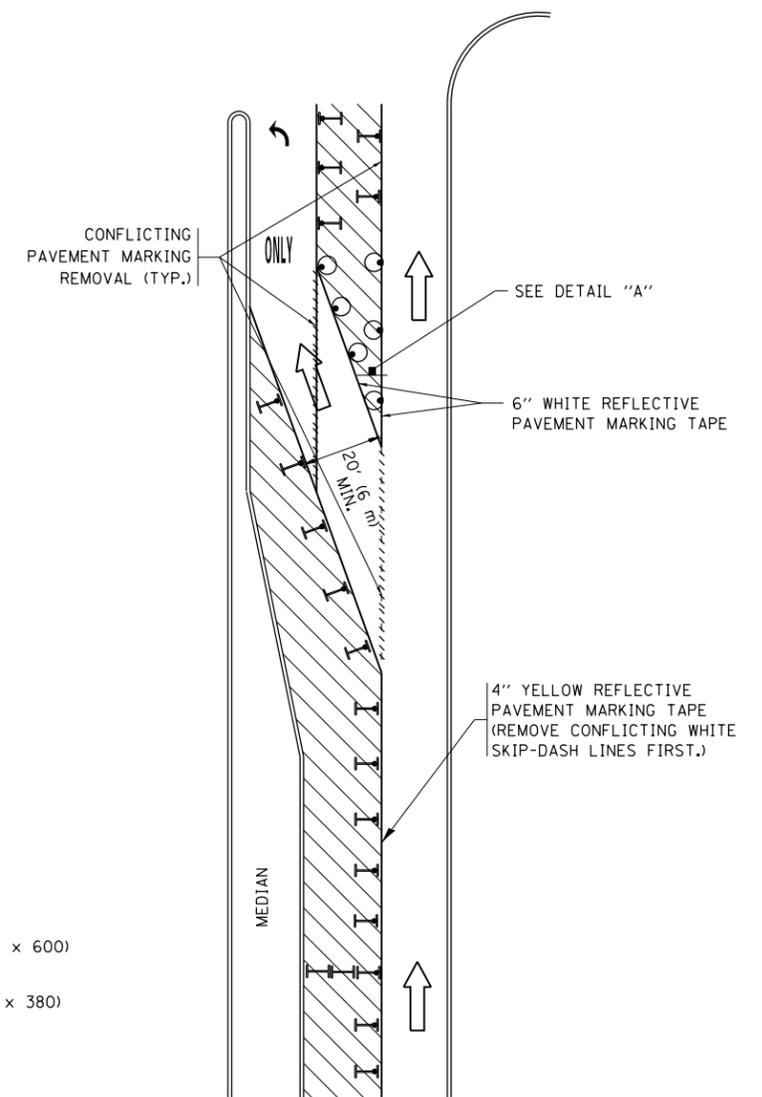
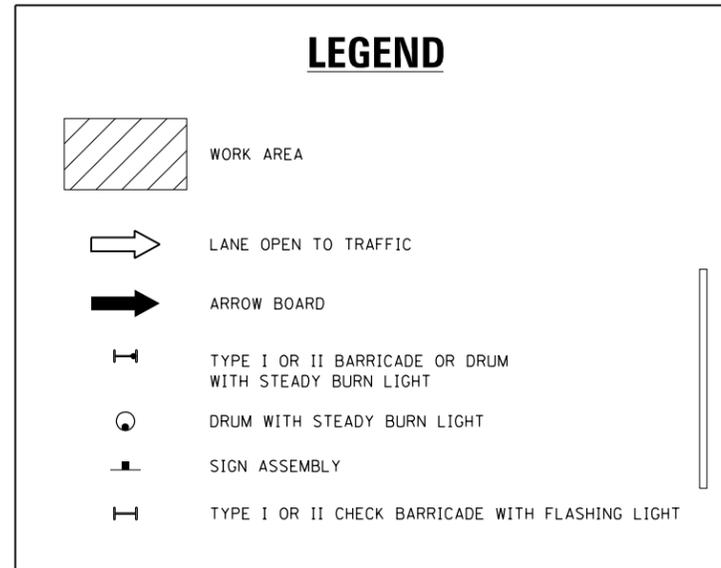
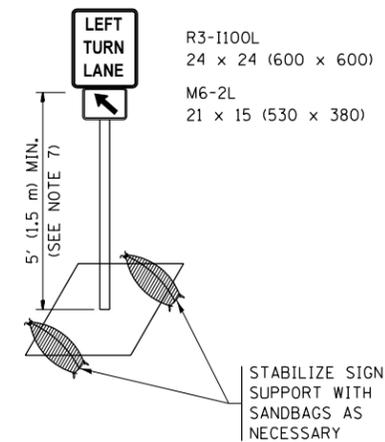


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

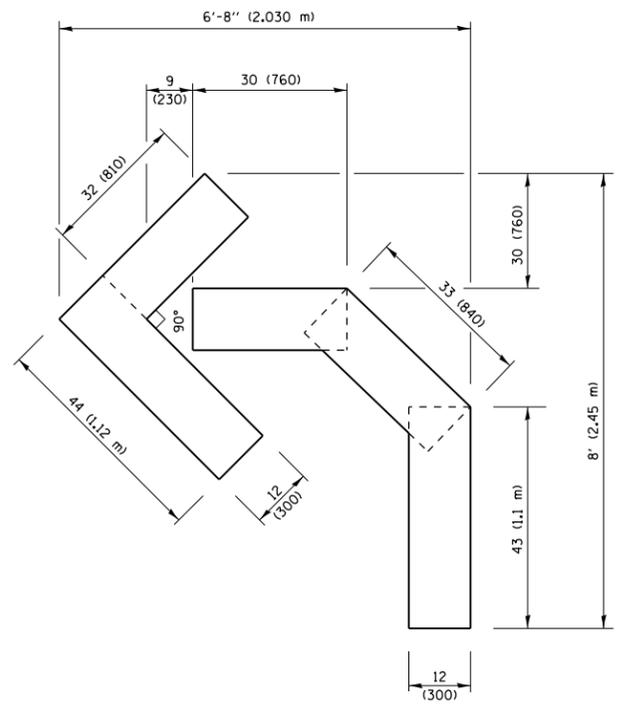


DETAIL A

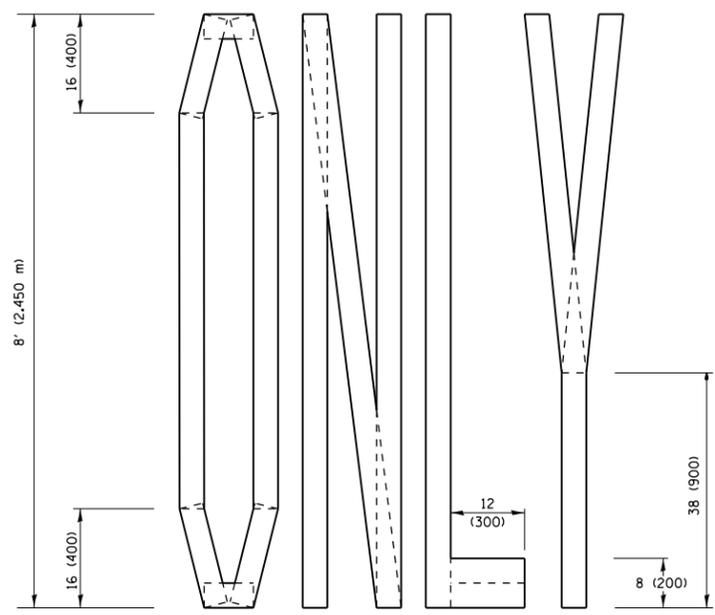
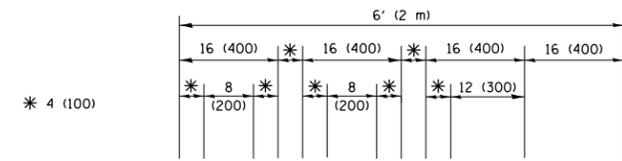
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = JOHNSTONKD	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI045\REVISED Design\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			1350	2016-058-RS	COOK	63	51
Default	PLOT SCALE = 100.0000' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			TC-14		CONTRACT NO. 62D51		
	PLOT DATE = 2/2/2018	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.

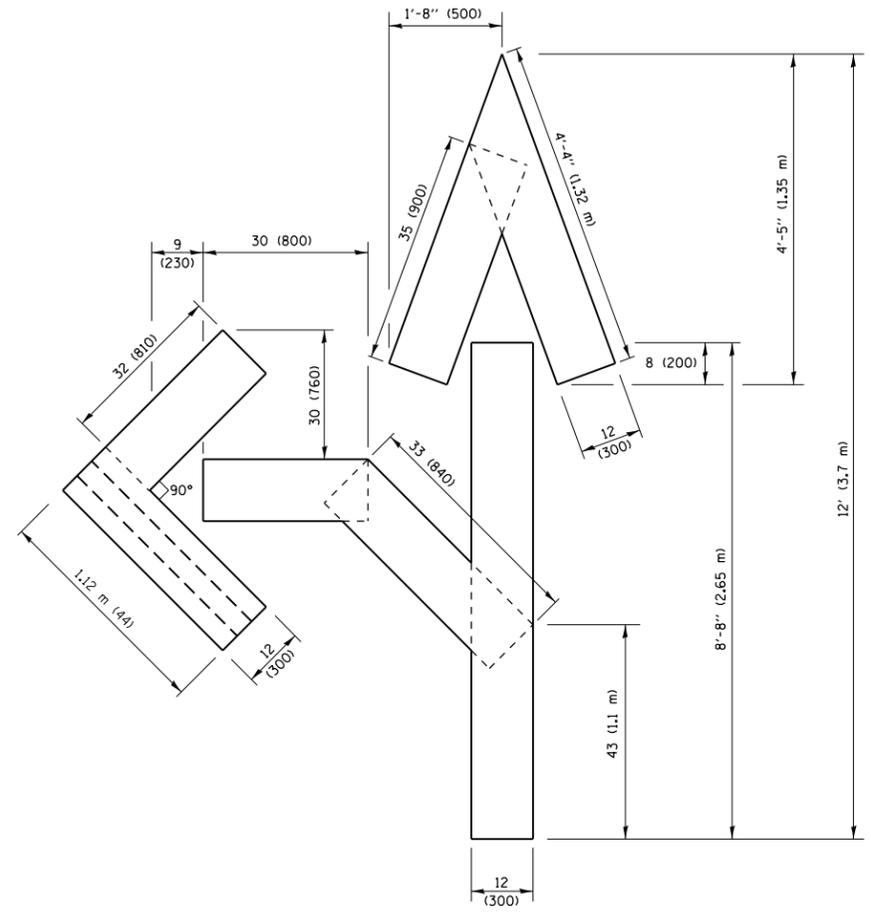
ILLINOIS FED. AID PROJECT



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

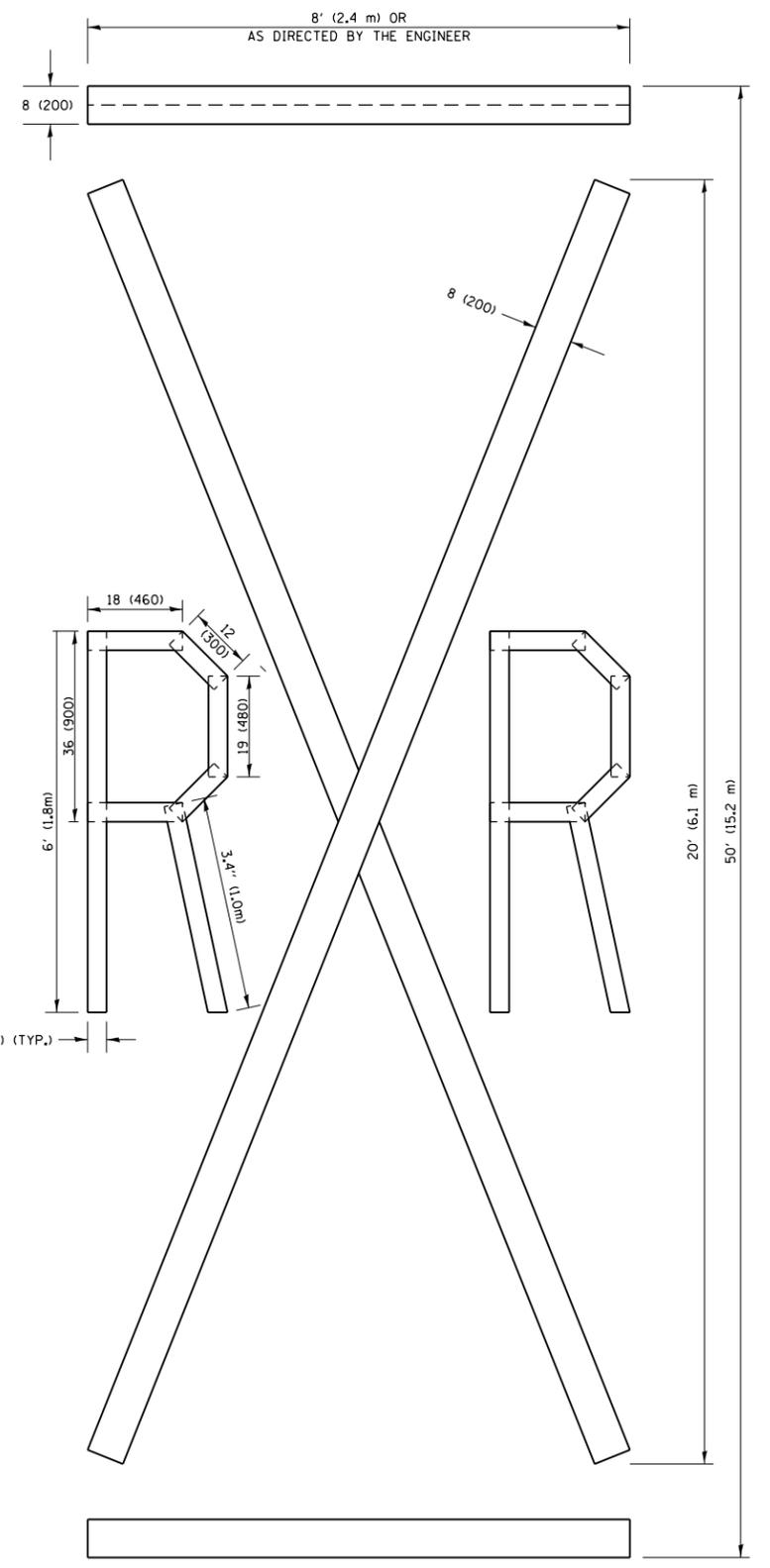


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

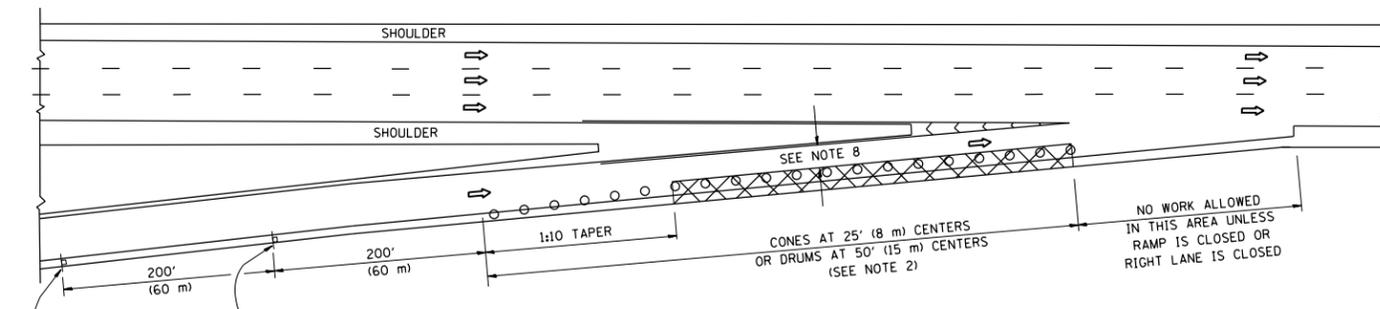
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		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

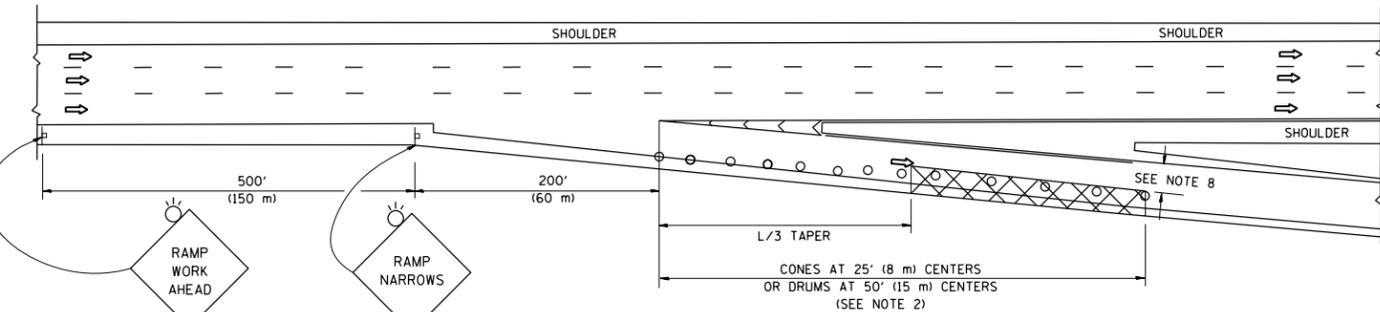
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-16			CONTRACT NO. 62D51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

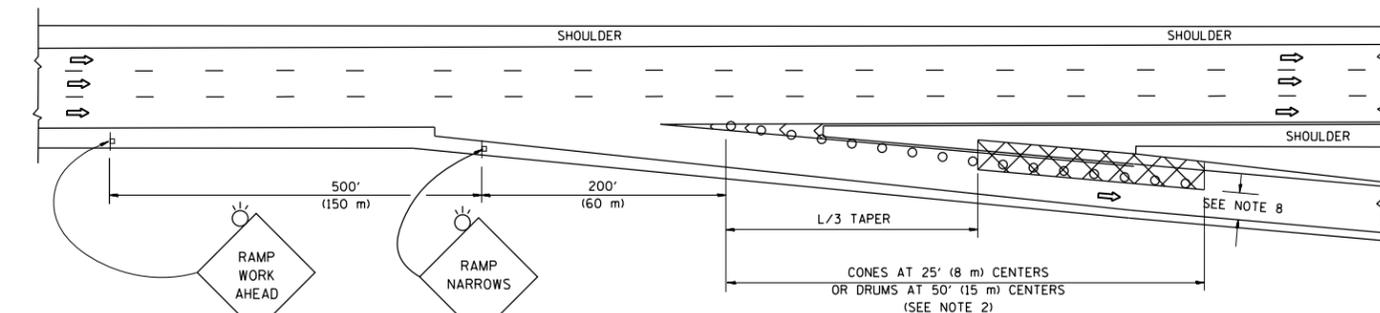
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

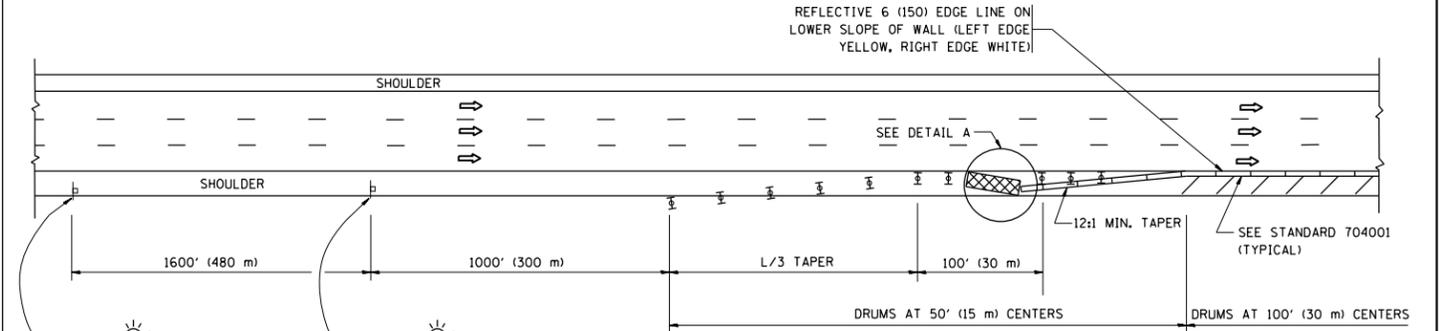
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

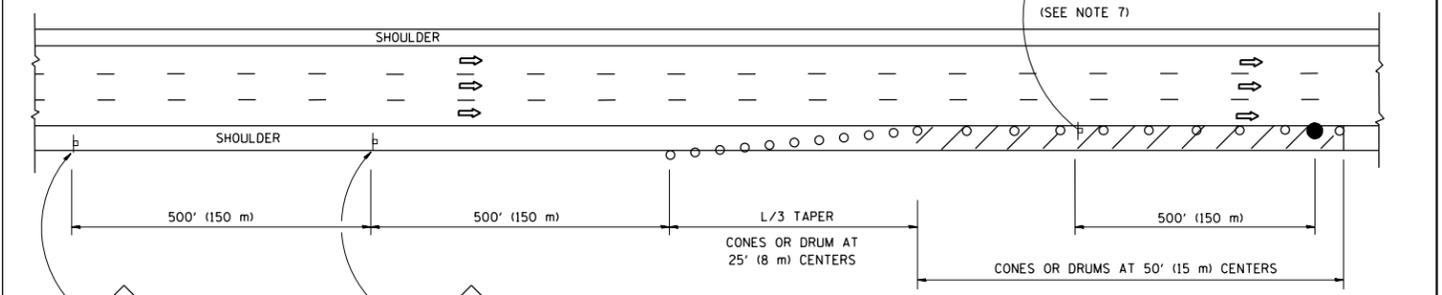
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

SHOULDER CLOSURE DETAILS

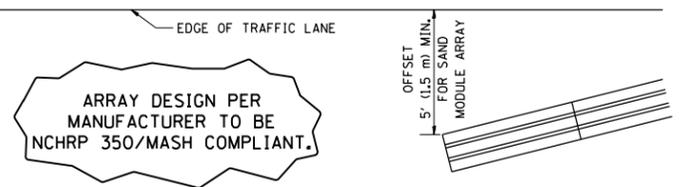


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

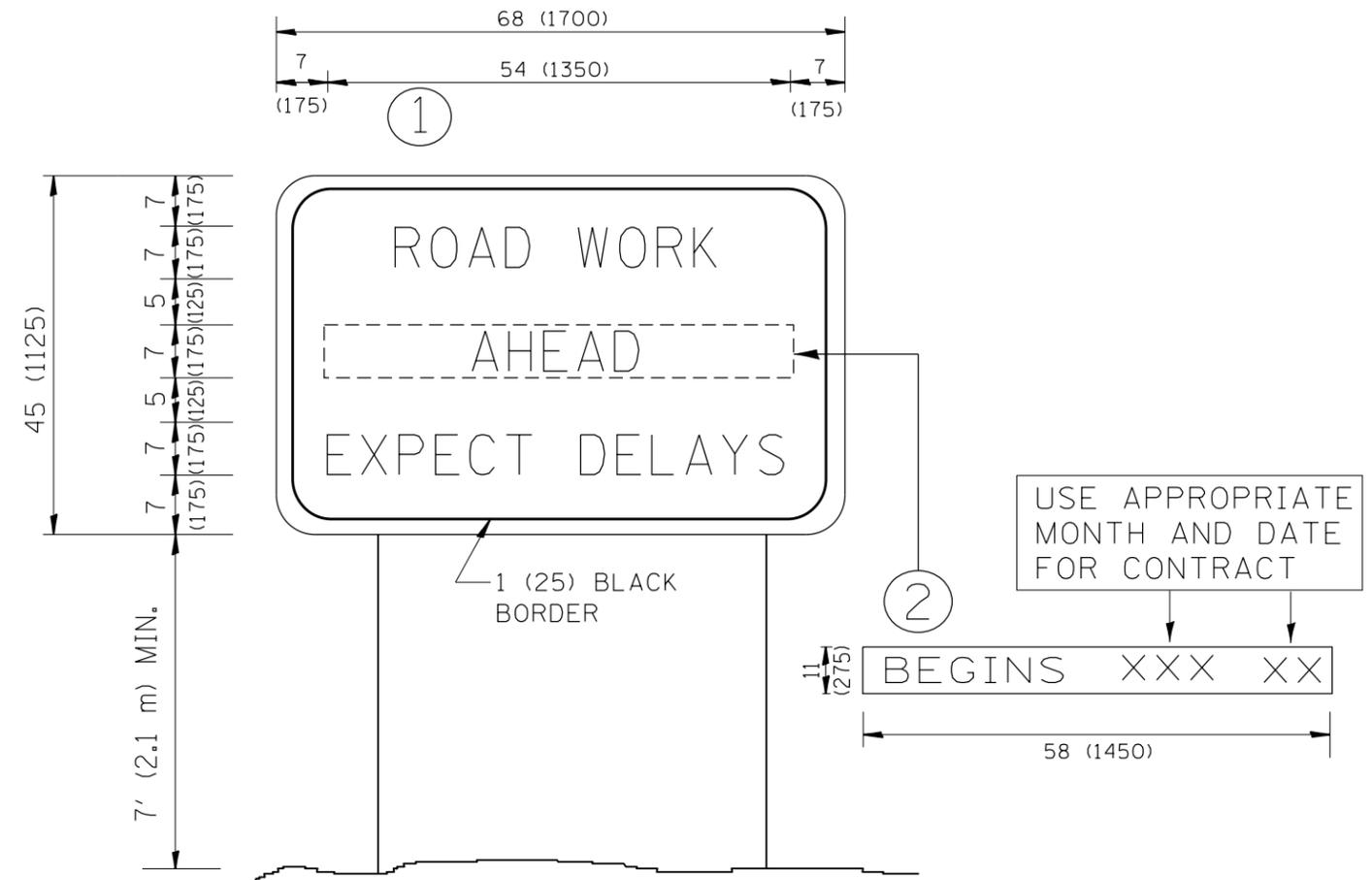
FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED - S.P.B. 01-07
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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M.D. 06-13
	PLOT DATE = 2/2/2018	DATE - 11-96	REVISED - M.D. 01-18

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
190	2016-058-RS	COOK	63	53
TC-17		CONTRACT NO. 62D51		
ILLINOIS FED. AID PROJECT				

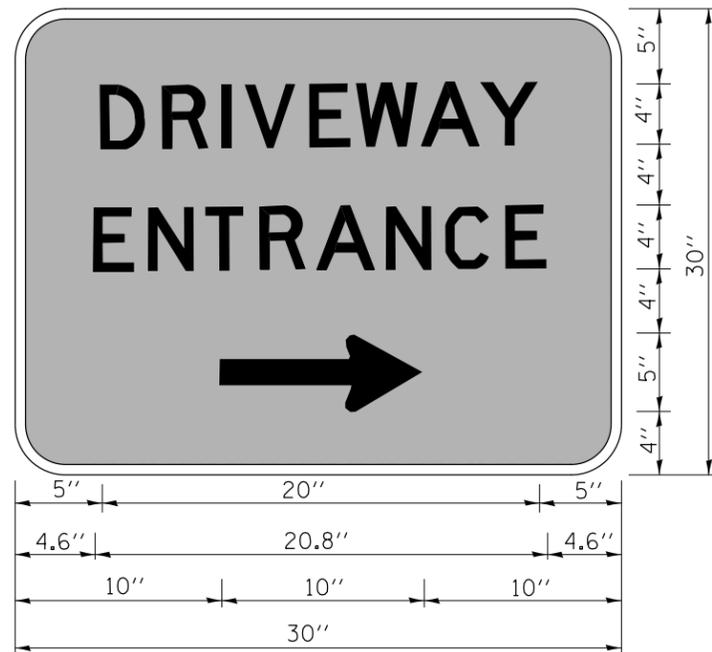


NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI045\Drawings\Design\Diststd.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	1350			2016-058-RS	COOK	63	54	
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 62D51				
PLOT DATE = 2/2/2018	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED - C. JUCIUS 02-15-07
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	PLOT SCALE = 100.0000' / 1"	DATE -	REVISED -
	PLOT DATE = 2/2/2018		

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	55
TC-26			CONTRACT NO. 62D51	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TRAFFIC SIGNAL LEGEND

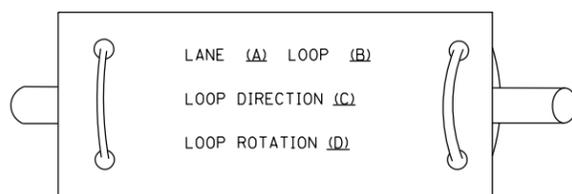
(NOT TO SCALE)

ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND	 	 	SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND	 	 	SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED	 	 	RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM	S	SP	FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
WOOD POLE			INTERSECTION ITEM	I	IP	GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE		
GUY WIRE			REMOVE ITEM		R			
SIGNAL HEAD			RELOCATE ITEM		RL			
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM		A			
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF			
FLASHER INSTALLATION -(FS) SOLAR POWERED	 	 	MAST ARM POLE AND FOUNDATION TO BE REMOVED		RMF			
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF			
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			DETECTOR LOOP, TYPE I	 	 			
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP	 	 			
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR	 	 			
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR	 	 			
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR	 	 			
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIRMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

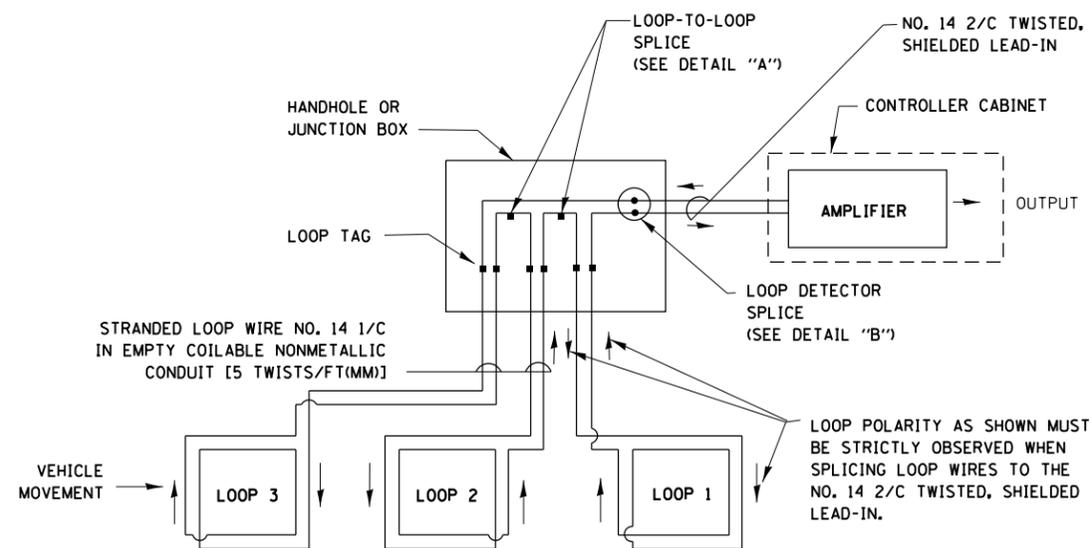
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

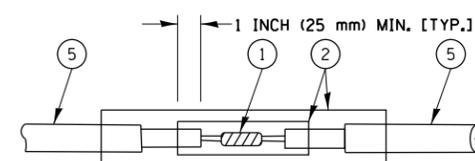


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

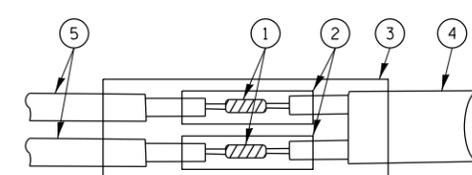


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

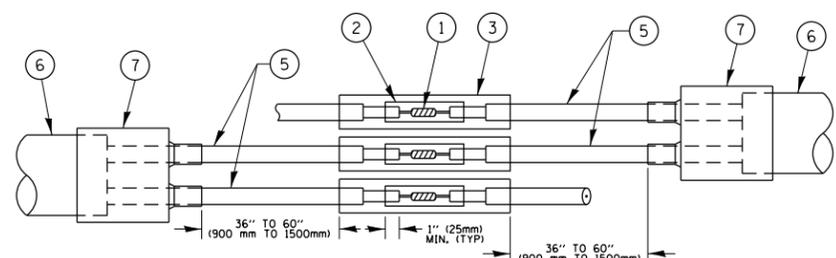


DETAIL "A"
LOOP-TO-LOOP SPLICE

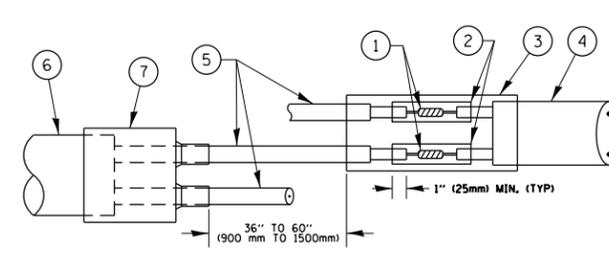


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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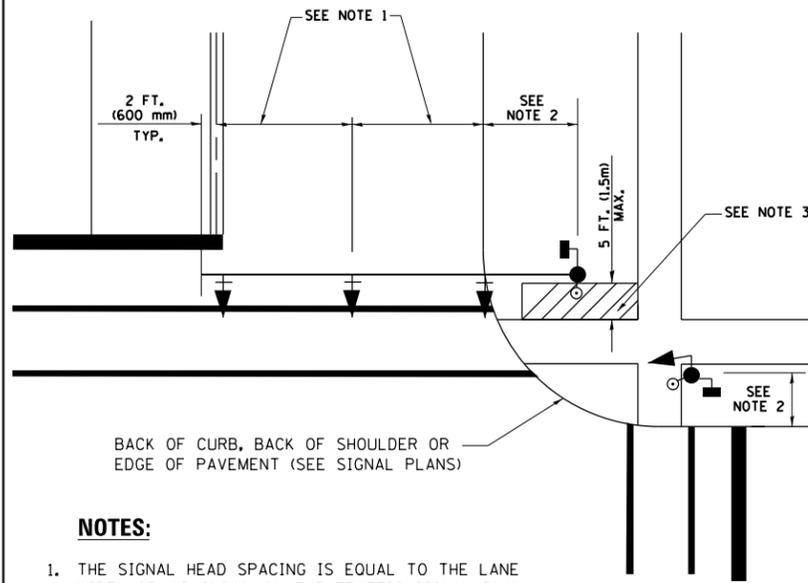
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	57
TS-05		CONTRACT NO. 62D51		
ILLINOIS FED. AID PROJECT				

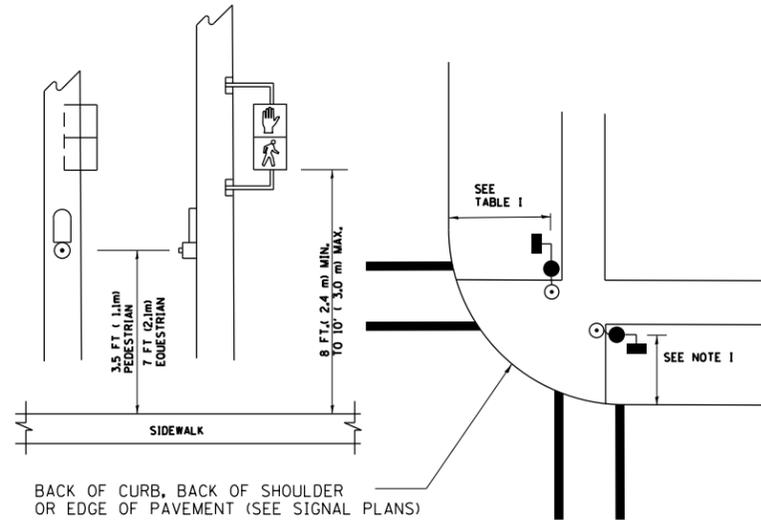
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

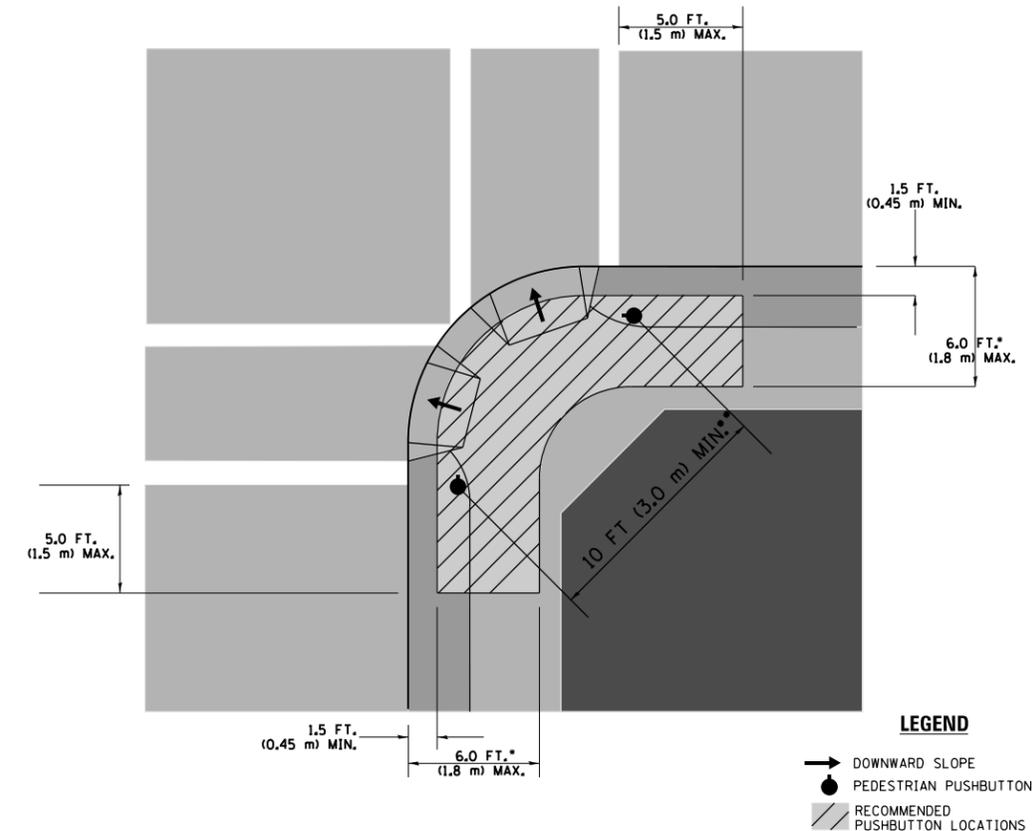
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

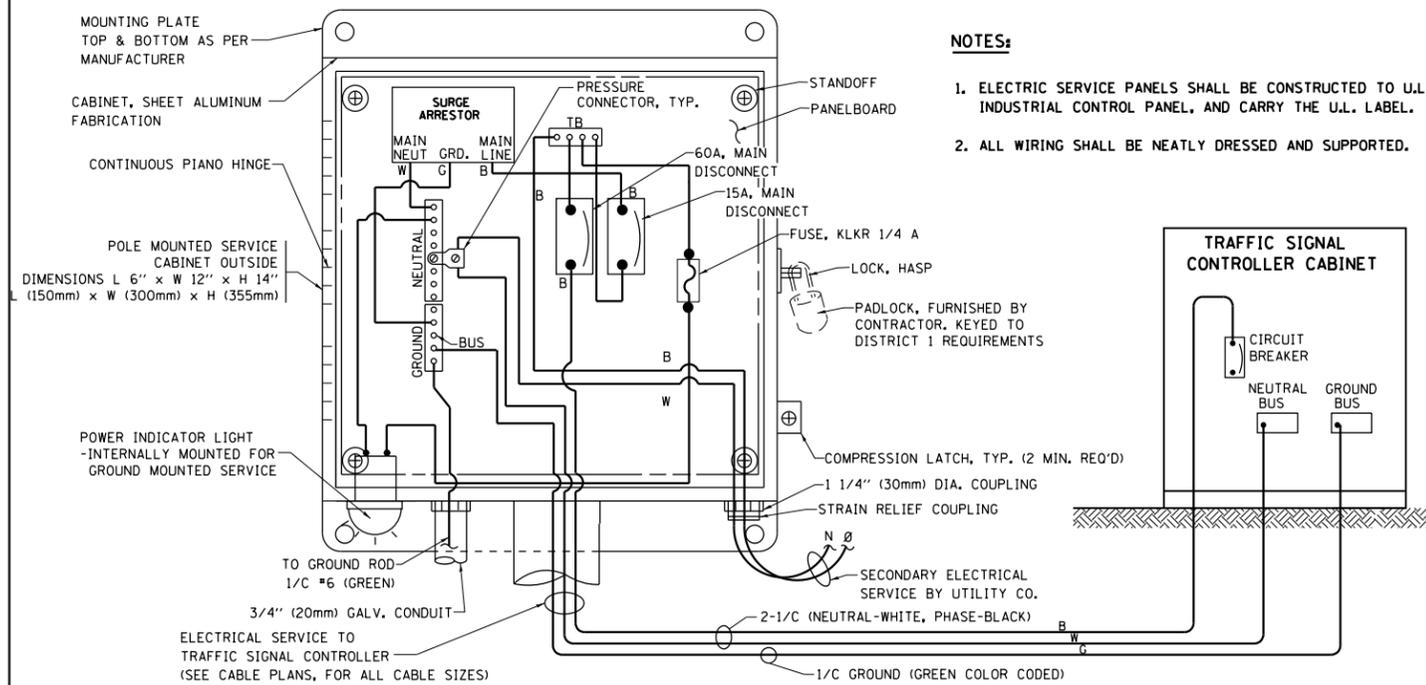
1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

TRAFFIC SIGNAL EQUIPMENT OFFSET

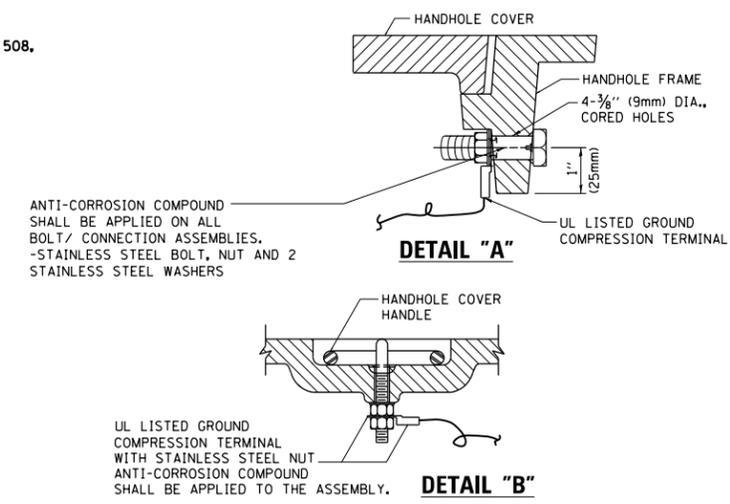
TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

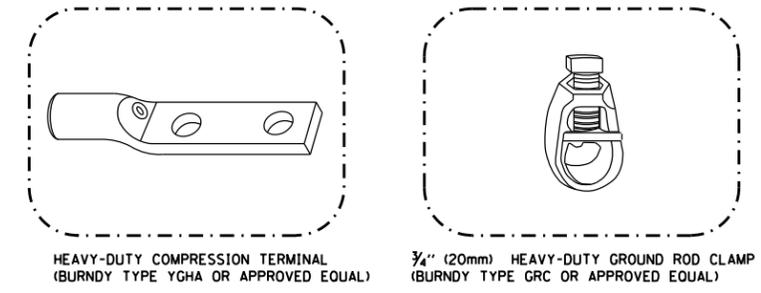
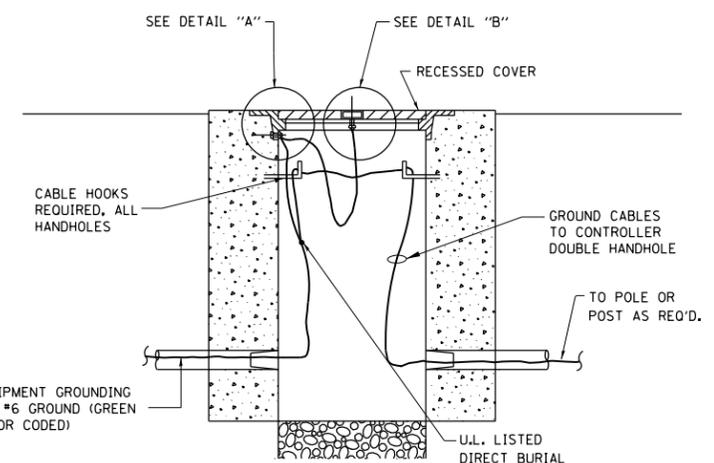


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE) SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)

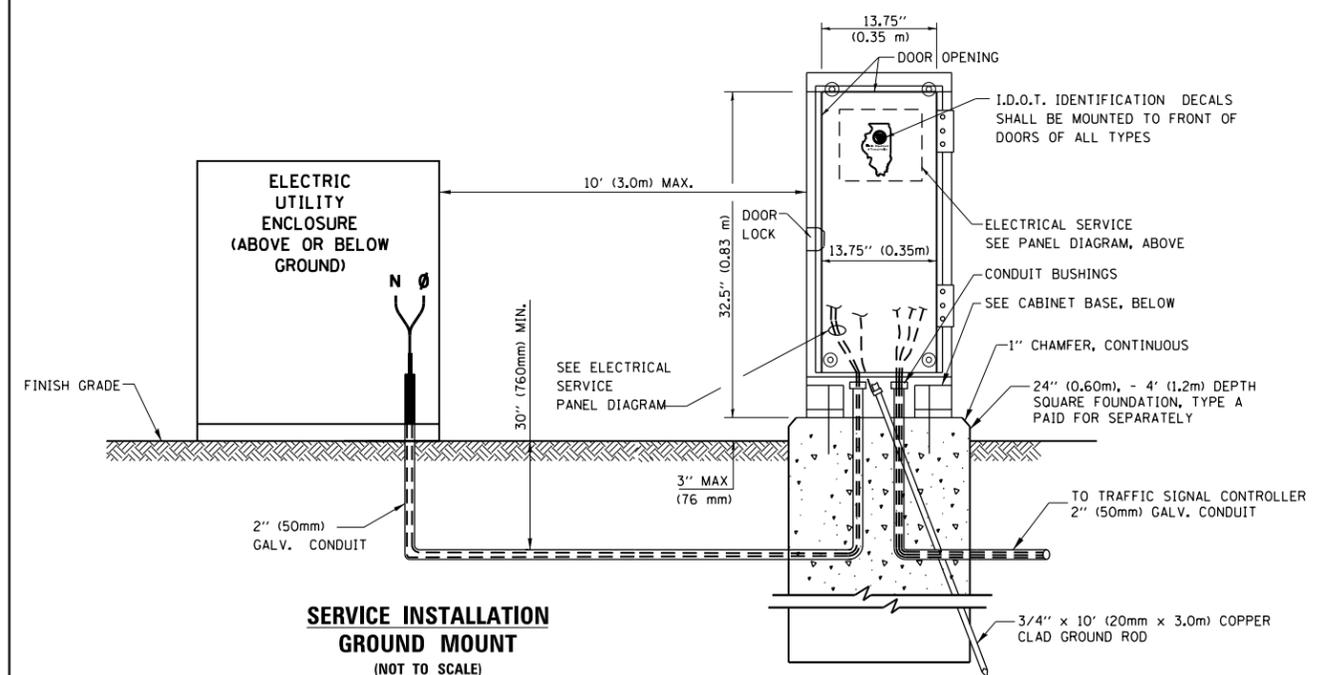
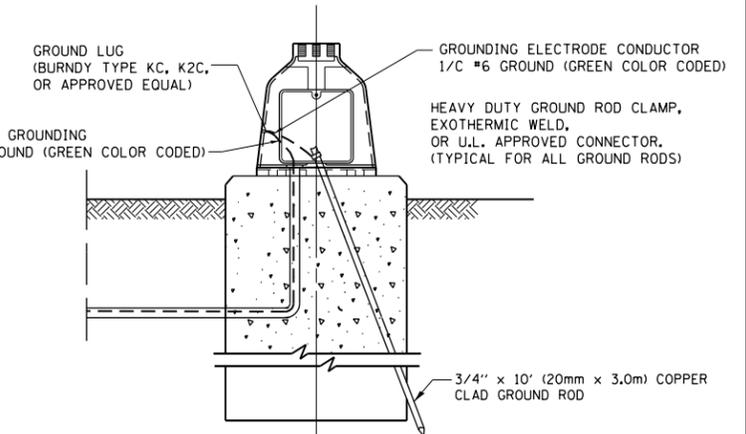
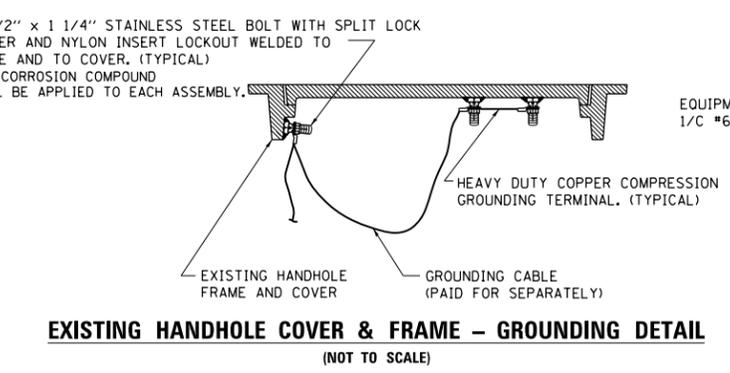


NOTES:
GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN ENCLOSED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

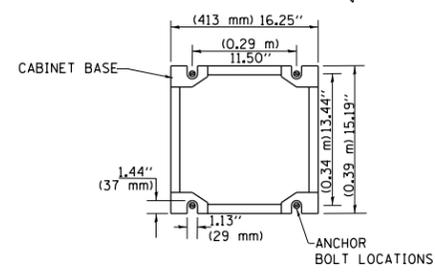


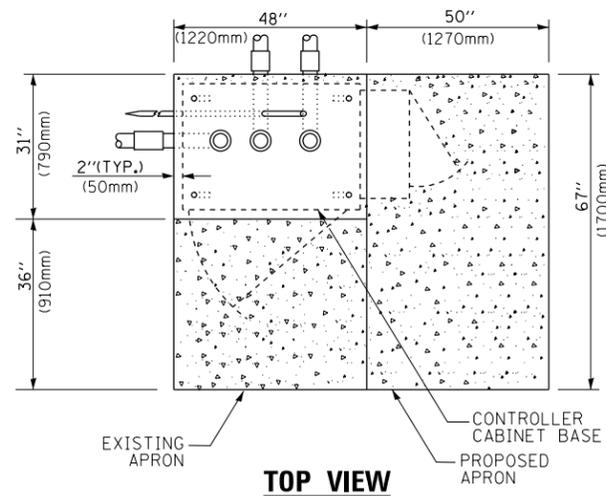
- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



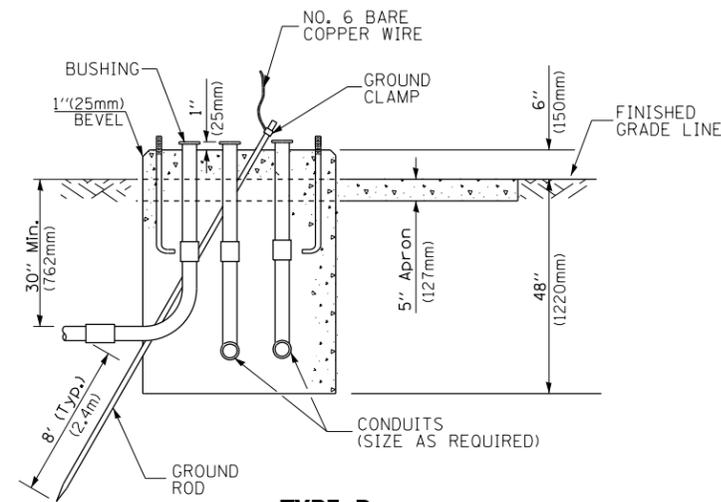
SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)

CABINET - BASE BOLT PATTERN (NOT TO SCALE)

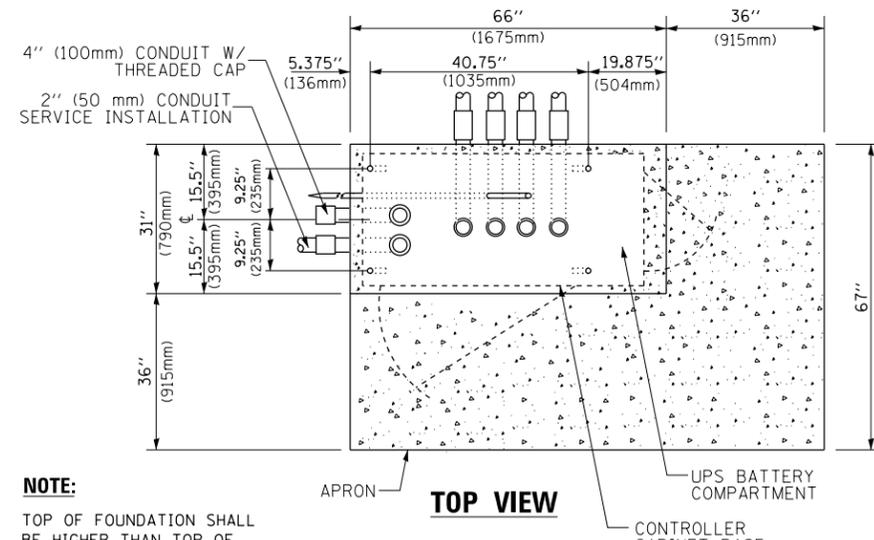




TOP VIEW

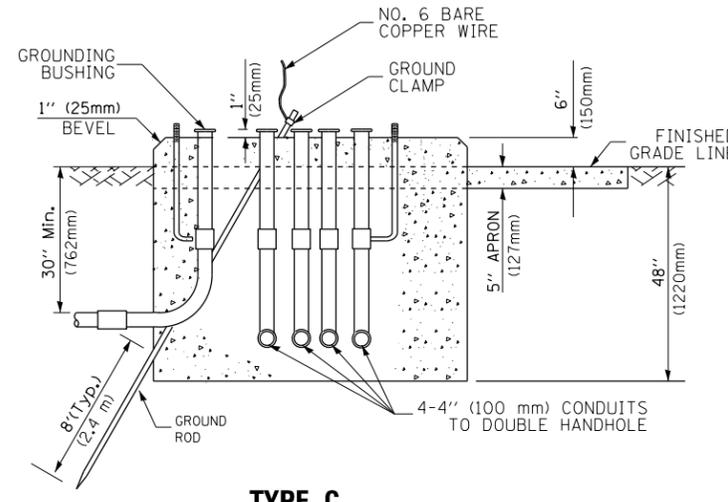


**TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET**

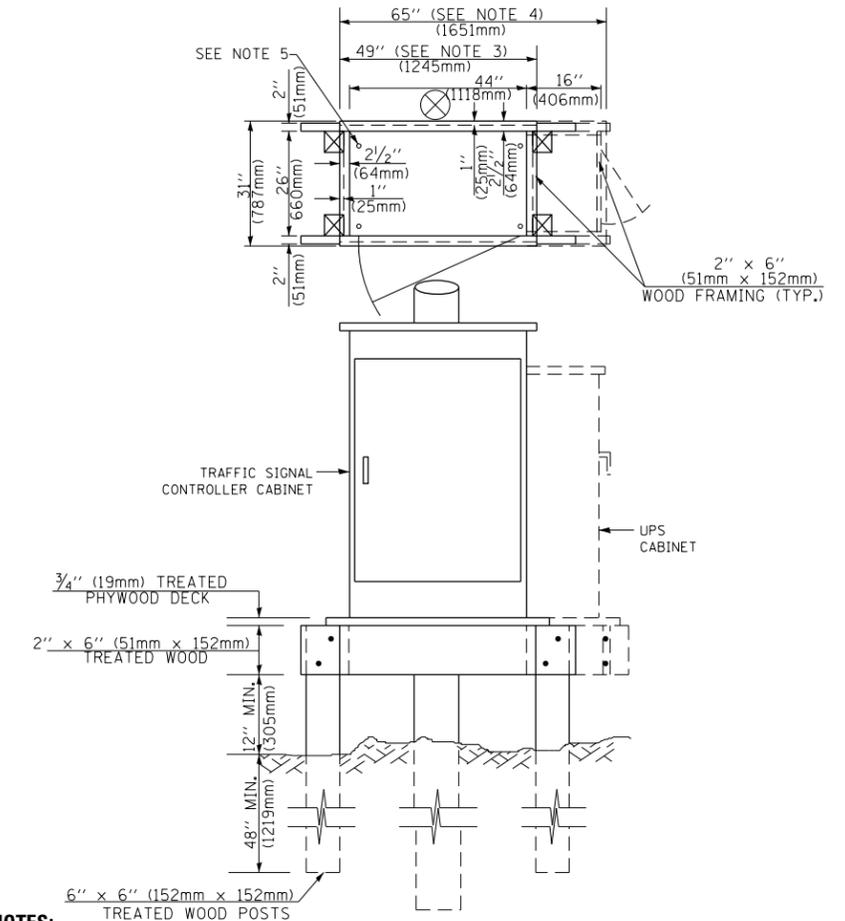


TOP VIEW

NOTE:
TOP OF FOUNDATION SHALL BE HIGHER THAN TOP OF DOUBLE HANDHOLE



**TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS**



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

**TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

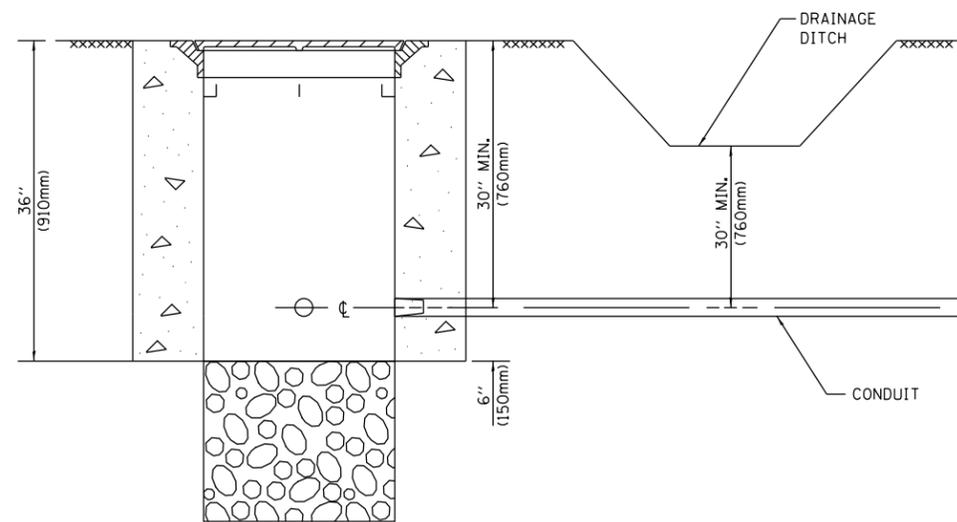
DEPTH OF FOUNDATION

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and up to 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m) and up to 85' (25.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average unconfined compressive strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

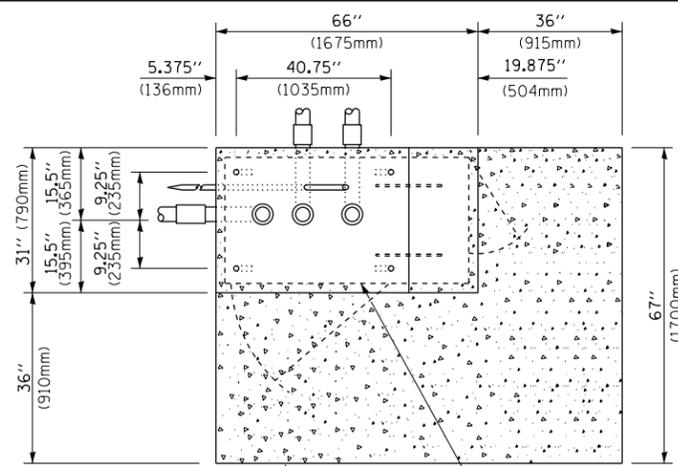
DEPTH OF MAST ARM FOUNDATIONS, TYPE E



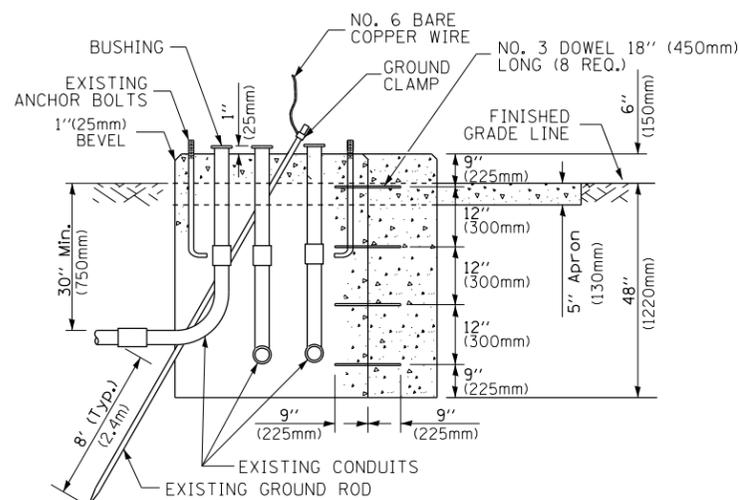
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



TOP VIEW
(NOT TO SCALE)

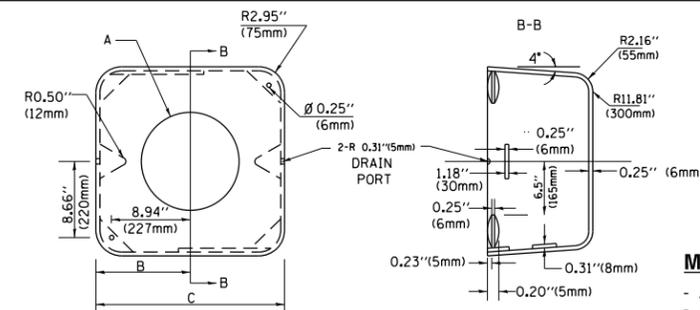


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIABLES	9.5" (241mm)	19" (483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIABLES	10.75" (273mm)	21.5" (546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIABLES	13.0" (330mm)	26" (660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIABLES	18.5" (470mm)	37" (940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

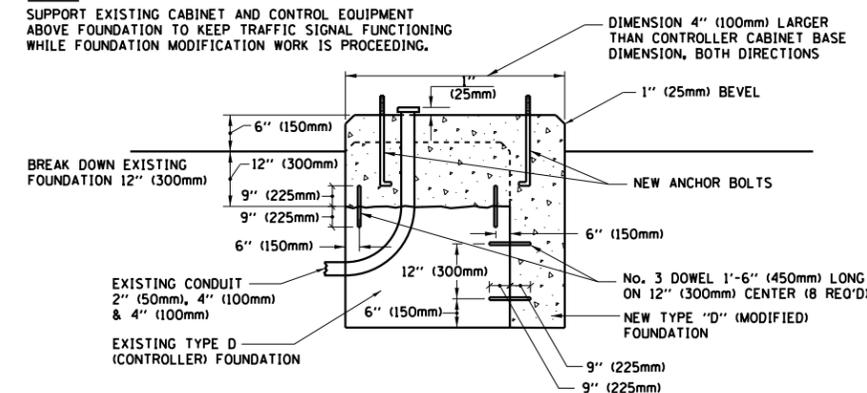
SHROUD

NOTES:

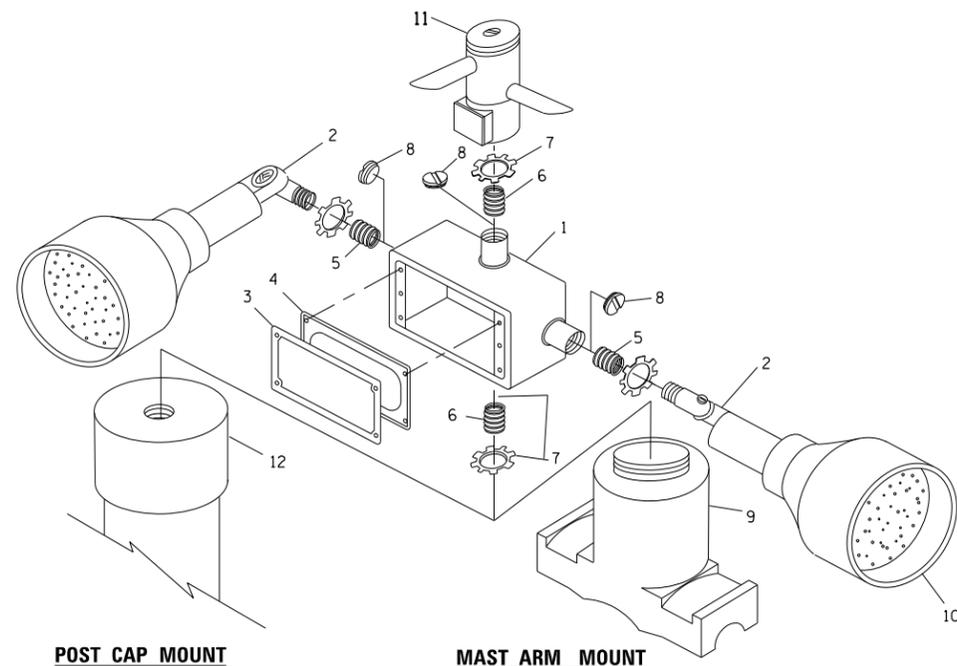
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

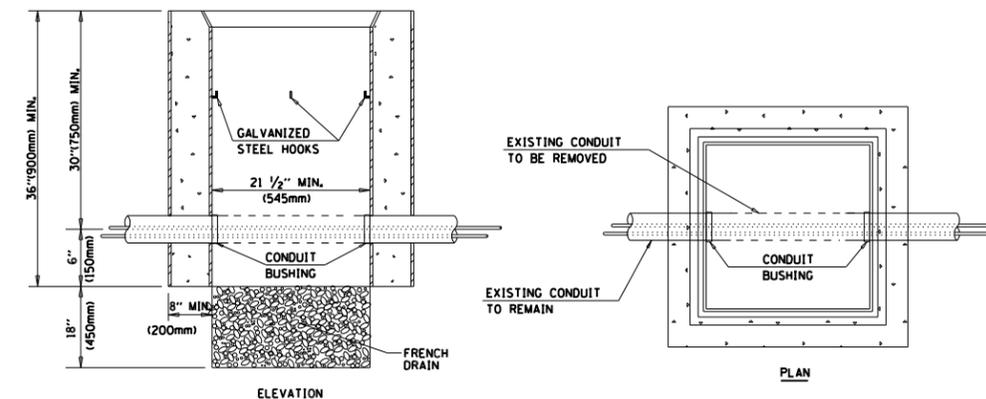
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

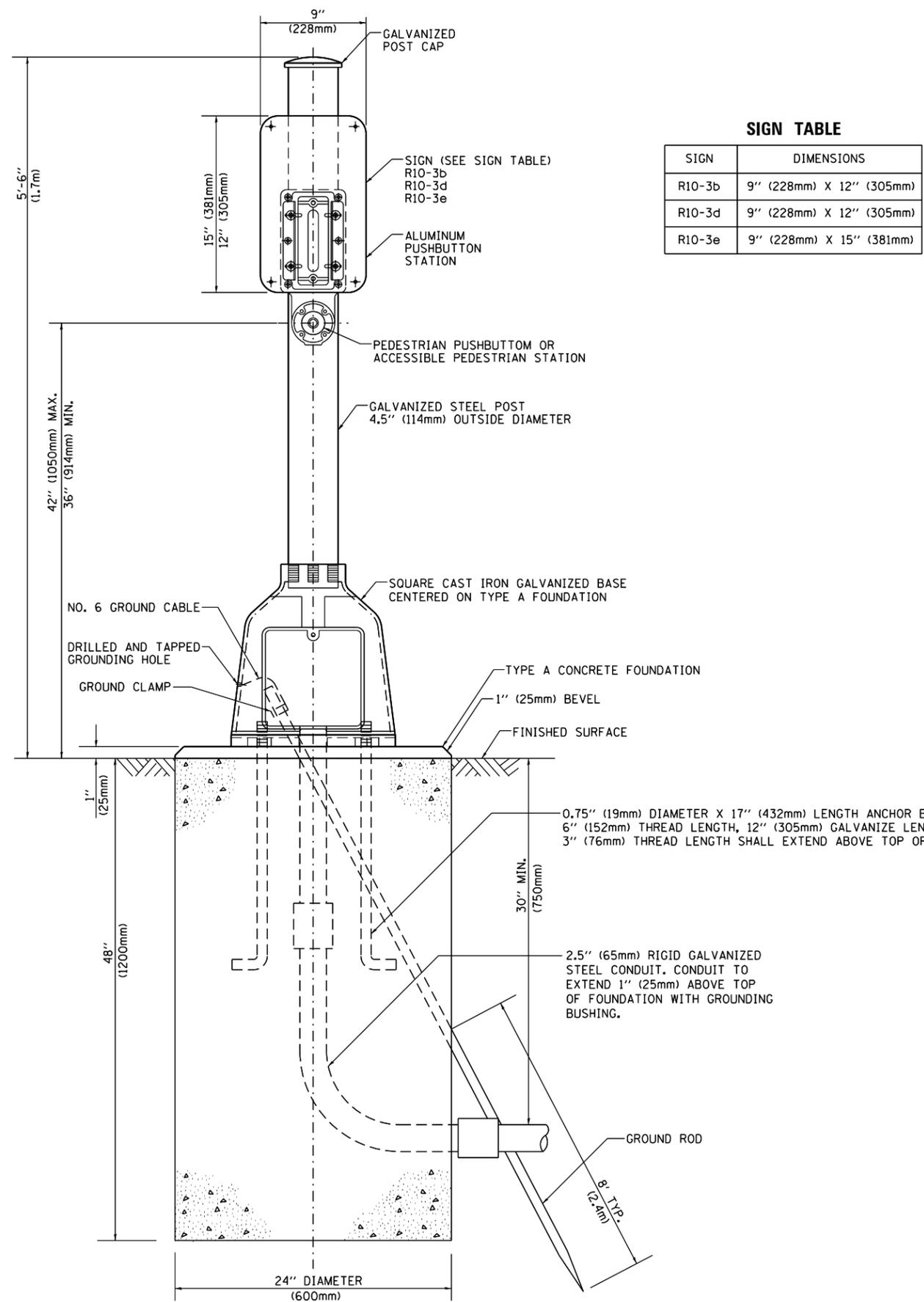
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

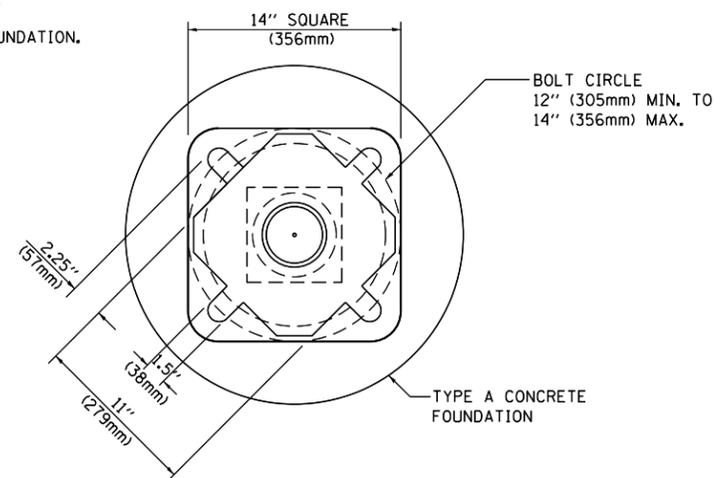
SCALE: NONE SHEET 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	61
	TS-05			CONTRACT NO. 62D51
ILLINOIS FED. AID PROJECT				



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



BOLT PATTERN
PEDESTRIAN PUSH BUTTON POST, TYPE A

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

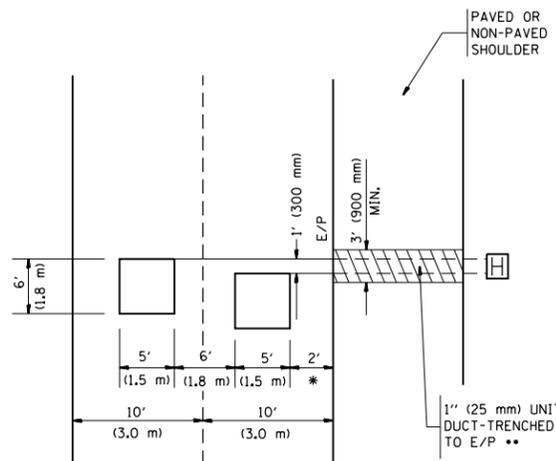
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1350	2016-058-RS	COOK	63	62
TS-05			CONTRACT NO. 62D51	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



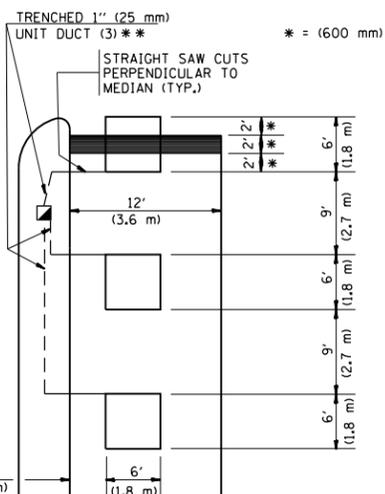
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



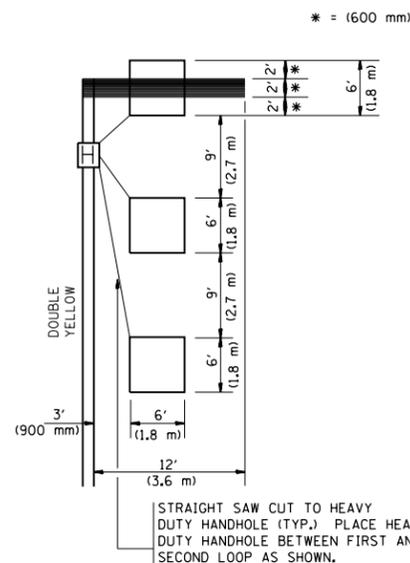
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

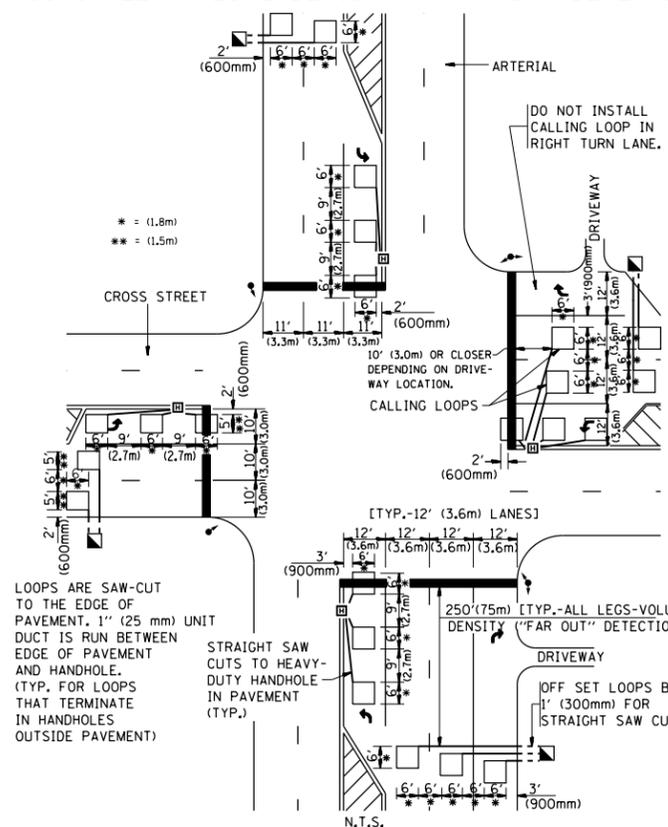
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

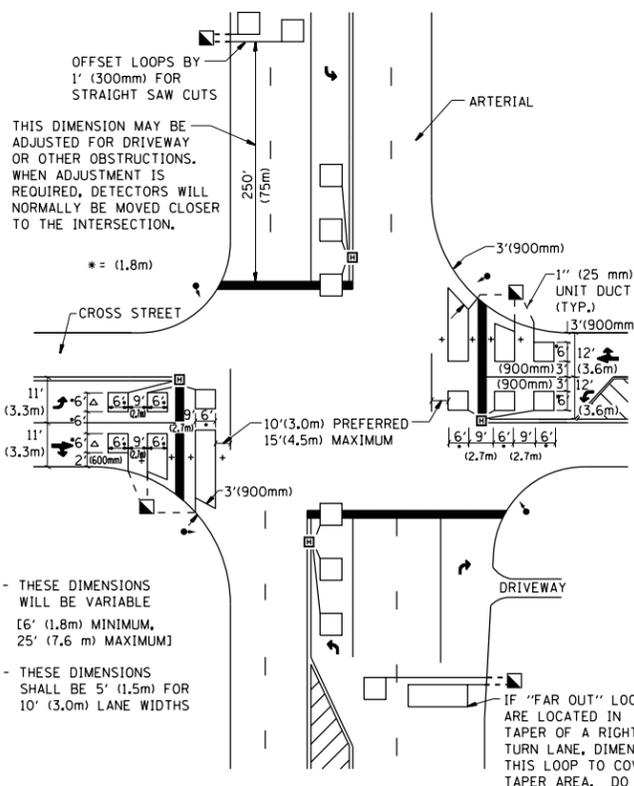
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = JOHNSTONKD	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 100.0000' / 1"	CHECKED - R.K.F.	REVISIONS	REVISIONS			TS-07		CONTRACT NO. 62D51		
PLOT DATE = 2/2/2018	DATE -	REVISIONS	REVISIONS			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT