

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	1
ILLINOIS CONTRACT NO. 62F65				

PROPOSED HIGHWAY PLANS

F.A.P. 341 /IL 72 (TOUHY AVE.)
E/O MT. PROSPECT RD. TO LEE ST.
SECTION: 3035RS-1
PROJECT:NHPP-HTVM(772)
RESURFACING (3P) AND PEDESTRIAN RAMPS
COOK COUNTY

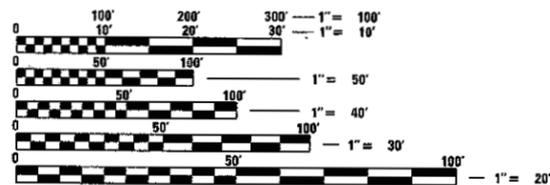
C-91-036-18

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN
CITY OF DES PLAINES
VILLAGE OF ROSEMONT

TRAFFIC DATA:

IL 72
2016 - ADT = 36,500
SPEED LIMIT = 45 MPH

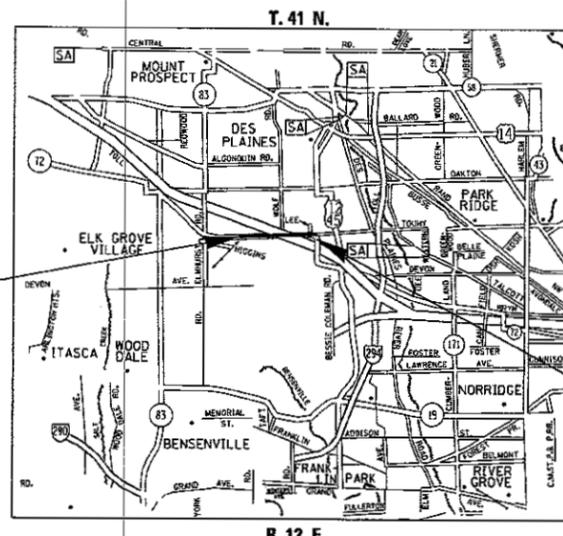


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

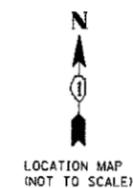
J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT BEGINS
STA. 35 + 72
IL 72 (TOUHY AVE.)

RESURFACING OMISSION :
STA. 58 + 67 TO
STA. 81 + 74



GROSS LENGTH = 7369 FT. = 1.4 MILES
NET LENGTH = 5062.15 FT. = 0.96 MILES



LOCATION MAP (NOT TO SCALE)

PROJECT ENDS
STA. 104 + 00 (TOUHY AVE.)
STA. 15 + 41 (HIGGINS RD.)

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62F65

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED ~~February 1~~ 20 18

Anthony J. Quinley REGIONAL ENGINEER

Mar 23 20 18

Adam ENGINEER OF DESIGN AND ENVIRONMENT

Mar 23 20 18

David P. Clark DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-10	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-03	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-03	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424016-04	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-04	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS, TYPE 1
604091-03	FRAME AND GRATE, TYPE 24
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24' (600 mm) FROM PAVEMENT EDGE
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS > 45 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE 1W OR 2W WITH NONTRANSVERSABLE MEDIAN
701602-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
814001-03	HANDHOLES
886001-01	DETECTOR LOOP INSTALLATIONS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC.
- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER TO EXISTING CURBS AND GUTTERS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITIES OF CHICAGO, DES PLAINES, AND ROSEMONT.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION OF THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- CONTACT THE IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST TWO WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT.
- ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL, P.C.C. SIDEWALK 5", AND DRAINAGE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING OF WORK.
- THE RESIDENT ENGINEER SHALL CONTACT DON CHIARUGI, AREA TRAFFIC FIELD ENGINEER, AT DON.CHIARUGI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS. WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS
- PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

FILE NAME p:\il\094EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\094EBID\Design\0182618-shr-gennote.dgn	USER NAME = almezcata	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES F.A.P. 341 IL 72 (E/O MT, PROSPECT RD. - LEE ST.)	F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 100.0000 "/ in.	CHECKED -	REVISED -	341			3035RS-1	COOK	36	2		
PLOT DATE = 2/2/2018	DATE -	REVISED -	SHEET NO. 2 OF SHEETS			STA.	TO STA.		CONTRACT NO. 62F65		
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005				
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH	5	5				
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	2	2				
20200100	EARTH EXCAVATION	CU YD	10	10				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	266	266				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	4	4				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	4	4				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	4	4				
25200110	SODDING, SALT TOLERANT	SO YD	266	266				
25200200	SUPPLEMENTAL WATERING	UNIT	2.7	2.7				
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	45	45				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	26956	26956				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	59.9	59.9				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1648	1648				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	335	335				
	*SPECIALTY ITEMS							

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005				
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	67	67				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	5	5				
42001300	PROTECTIVE COAT	SO YD	79	79				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	65	65				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	541	541				
42400800	DETECTABLE WARNINGS	SO FT	58	58				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	39935	39935				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	110	110				
44000600	SIDEWALK REMOVAL	SO FT	481	481				
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SO YD	299	299				
44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	204	204				
44201757	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	55	55				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	4	4				

14

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FILE NAME =	USER NAME = dlnwrcf	DESIGNED -	REVISED -
OFFICE = District 1 Project 1028/PCAD/1028/1028/1028	PROJECT =	CHECKED -	REVISED -
PLOT SCALE = 100,000 / 1"	DATE =		
PLOT DATE = 2/7/2018			

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
IL 72 - E0 MOUNT PROSPECT ROAD TO LEE STREET			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 62F65				

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2			
60266600	VALVE BOXES TO BE ADJUSTED	EACH	2	2			
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	25	25			
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	16	16			
60404940	FRAMES AND GRATES, TYPE 23	EACH	9	9			
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	16	16			
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	10	10			
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	10	10			
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1			
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6			
67100100	MOBILIZATION	LSUM	1	1			
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1			
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1			
M	* SPECIALTY ITEMS						

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1			
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	20884	20884			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	6967	6967			
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	911	911			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	15114	15114			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2187	2187			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	475	475			
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	135	135			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	247	247			
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	5221	5221			
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	911	911			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	15114	15114			
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2187	2187			

FILE NAME =	USER NAME = amnkatta	DESIGNED -	REVISED -
PROJECT =	PROJECT =	CHECKED -	REVISED -
PLOT SCALE = 100,000 / 1 in.	DATE = 2/2/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL 72 - E/O MOUNT PROSPECT ROAD TO LEE STREET

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	4
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62F65	

SCALE: SHEET NO. OF SHEETS STA. TO STA.

URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	475	475			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	135	135			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	247	247			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	672	672			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	672	672			
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	786	786			
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1			
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	3914	3914			
X5537800	STORM SEWERS TO BE CLEANED. 12"	FOOT	75	75			
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	32	32			
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	7884	7884			
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1669	1669			
12	* SPECIALTY ITEMS						

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	32	32			
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4			
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1			
3							

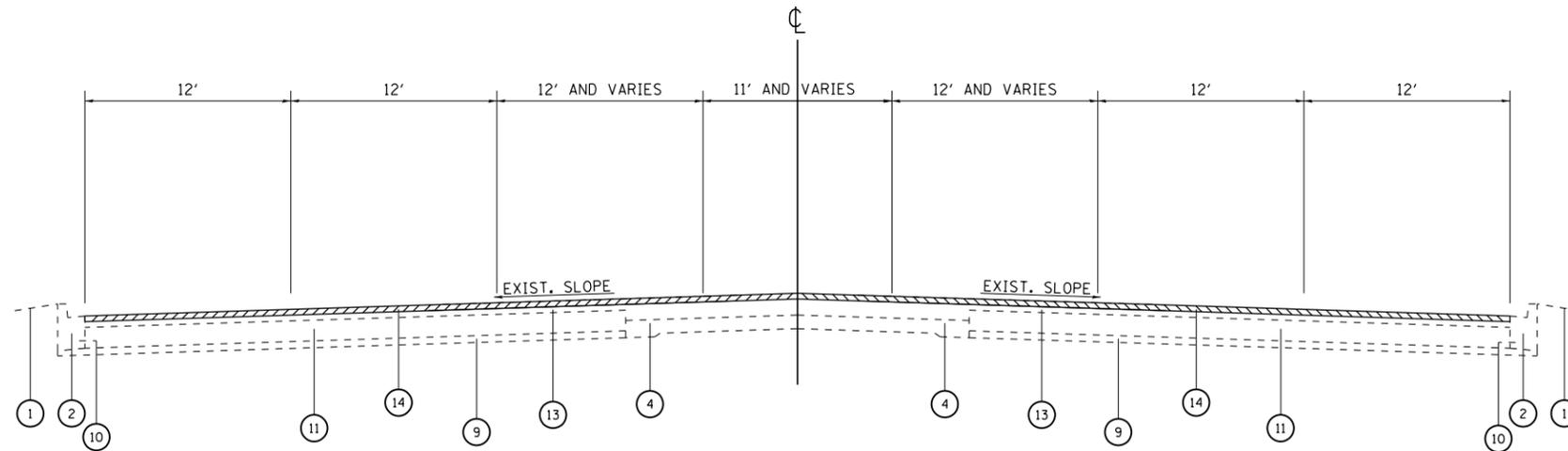
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 PLOT DATE = 2/2/2008 DATE -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

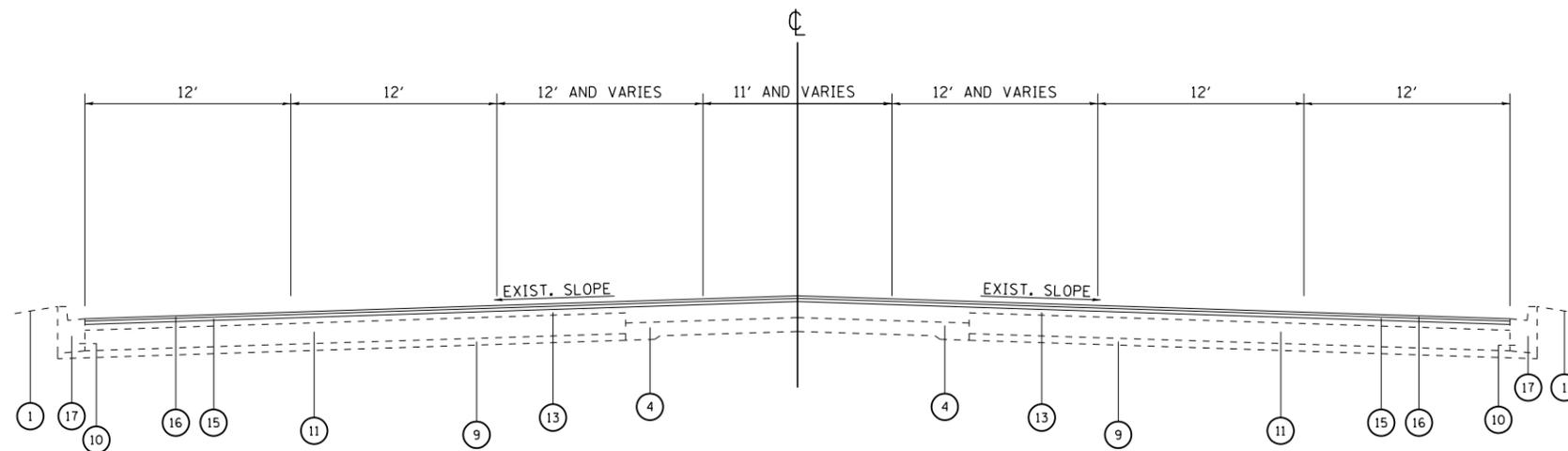
SUMMARY OF QUANTITIES
 IL 72 -- E0 MOUNT PROSPECT ROAD TO LEE STREET

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	5
CONTRACT NO. 62F65				
SCALE: SHEET NO. OF SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

REVISION



IL 72 (TOUHY AVE.)
EXISTING TYPICAL SECTION
 STA. 35+72 TO STA. 58+67



IL 72 (TOUHY AVE.)
PROPOSED TYPICAL SECTION
 STA. 35+72 TO STA. 58+67

EXISTING CONDITIONS:

- ① EXISTING SOD
- ② EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.18
- ③ EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.24
- ④ EXISTING CONCRETE CURB, TYPE B-6
- ⑤ EXISTING CONCRETE MEDIAN, BARRIER OR MOUNTABLE
- ⑥ EXISTING CONCRETE MEDIAN, CORRUGATED OR BARRIER (DOWELED)
- ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4" & VARIES
- ⑧ EXISTING GRASS MEDIAN
- ⑨ EXISTING SUB-BASE, GRANULAR, 4" OR 6"
- ⑩ EXISTING TIE BARS
- ⑪ EXISTING PCC BASE COURSE, 9"(±)
- ⑫ EXISTING PCC PAVEMENT, (9"-7"-9")
- ⑬ EXISTING HMA SURFACE COURSE, 4"(±)

PROPOSED CONDITIONS:

- ⑭ PROPOSED HMA SURFACE REMOVAL, 2½"
- ⑮ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
- ⑯ PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"
- ⑰ COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

NOTE 3: CONTRACTOR SHALL PATCH FIRST BEFORE MILLING OPERATIONS

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING	POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"	3.5% @ 80 GYR.	OCP
	POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50; ¾"	3.5% @ 50 GYR.	OCP
PATCHING	CLASS D PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	OC/OA
	HMA REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% @ 70 GYR	OC/OA
DRIVEWAYS	HMA SURFACE COURSE, MIX "D", N50, (IL-9.5 mm); 2"	4% @ 50 GYR.	OC/OA
	HMA BASE COURSE, (HMA BINDER IL-19 mm); CE - 8"	4% @ 50 GYR.	OC/OA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (OC/OA); QUALITY CONTROL FOR PERFORMANCE (OCP)

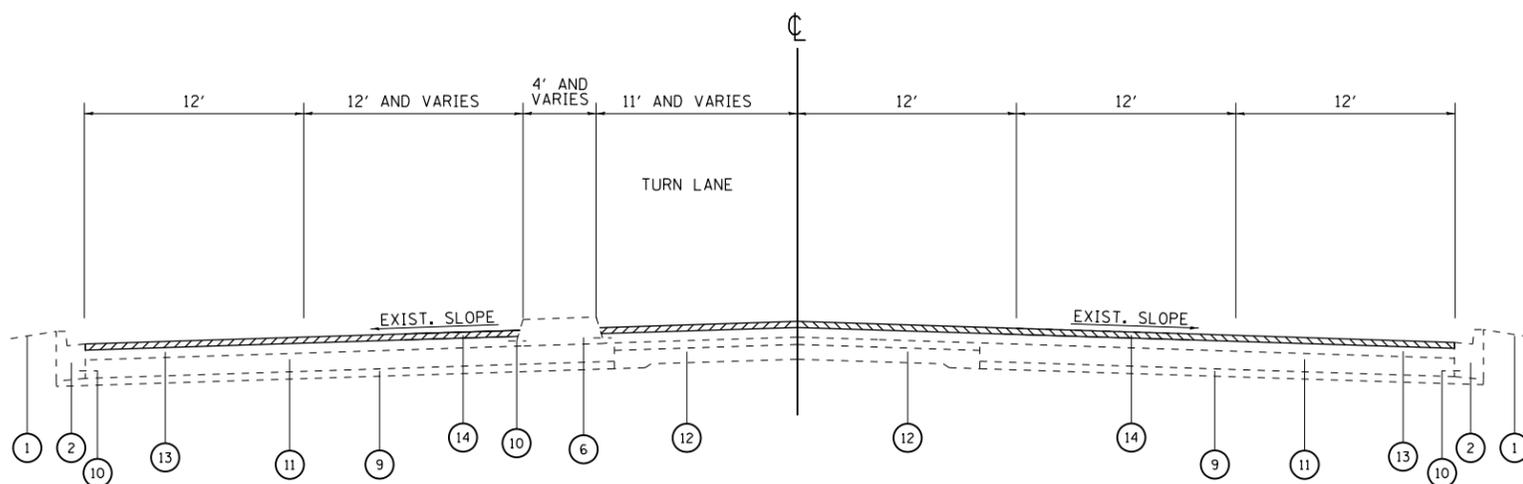
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Default	PLOT DATE = 2/2/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED TYPICAL SECTIONS
IL 72 (TOUHY AVE.) - (EO MT. PROSPECT RD. TO LEE ST.)

SCALE: NONE SHEET OF SHEETS STA. TO STA.

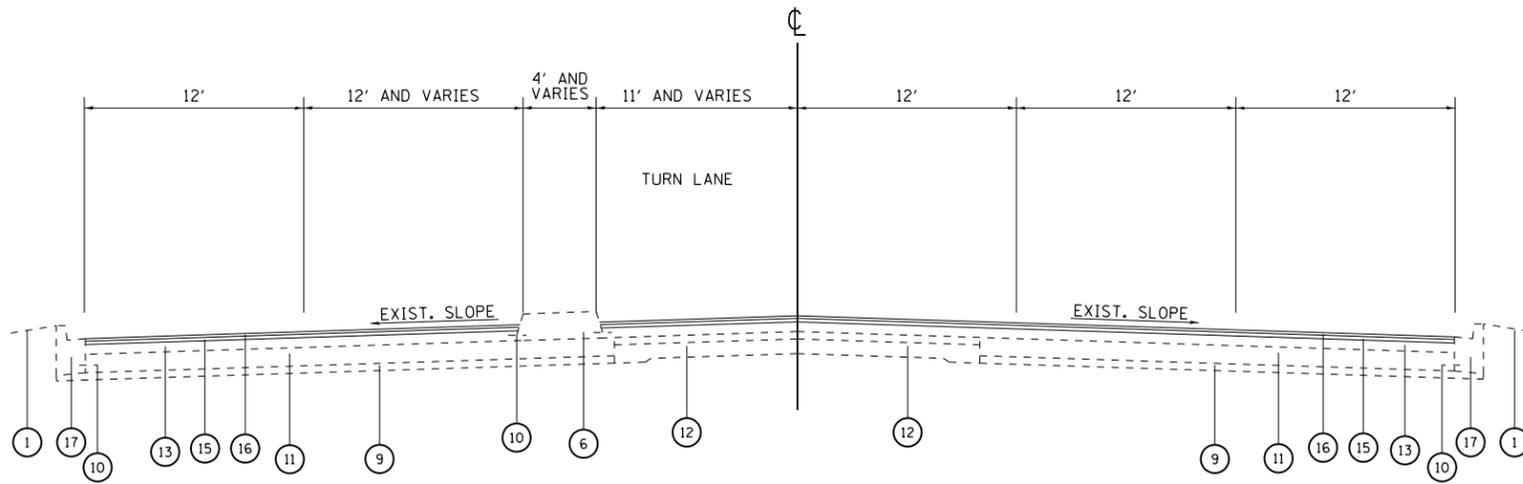
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	6
CONTRACT NO. 62F65				
ILLINOIS FED. AID PROJECT				



IL 72 (TOUHY AVE.)
EXISTING TYPICAL SECTION
 STA. 81+74 TO STA. 86+77

EXISTING CONDITIONS:

- ① EXISTING SOD
- ② EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.18
- ③ EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.24
- ④ EXISTING CONCRETE CURB, TYPE B-6
- ⑤ EXISTING CONCRETE MEDIAN, BARRIER OR MOUNTABLE
- ⑥ EXISTING CONCRETE MEDIAN, CORRUGATED OR BARRIER (DOWELED)
- ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4" & VARIES
- ⑧ EXISTING GRASS MEDIAN
- ⑨ EXISTING SUB-BASE, GRANULAR, 4" OR 6"
- ⑩ EXISTING TIE BARS
- ⑪ EXISTING PCC BASE COURSE, 9" (±)
- ⑫ EXISTING PCC PAVEMENT, (9"-7"-9")
- ⑬ EXISTING HMA SURFACE COURSE, 4" (±)



IL 72 (TOUHY AVE.)
PROPOSED TYPICAL SECTION
 STA. 81+74 TO STA. 86+77

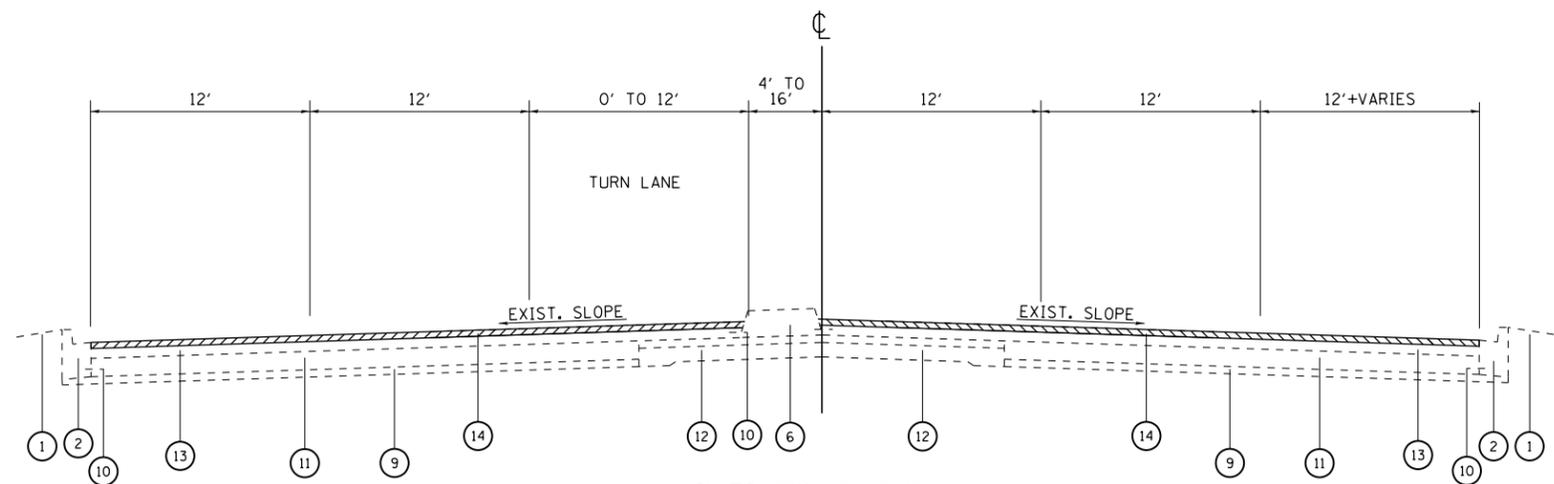
PROPOSED CONDITIONS:

- ⑭ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑮ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑯ PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1 3/4"
- ⑰ COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)

NOTES:

- 1. NO MILLING AND RESURFACING IS TO BE DONE BETWEEN STA. 81+74 AND STA. 84+36 ON THE RIGHT SIDE OF IL 72

FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS IL 72 (TOUHY AVE.) - (E/O MT. PROSPECT RD. TO LEE ST.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
p:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI026\BROWNS\Design\DI02618-sh-typical.dgn	PLotted	REVISIONS	REVISIONS			341	3035RS-1	COOK	36	7	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 62F65					
	PLOT DATE = 2/2/2018	DATE -	REVISED -			SCALE: NONE	SHEET	OF	SHEETS	STA.	TO STA.



IL 72 (TOUHY AVE.)
EXISTING TYPICAL SECTION
 STA. 86+77 TO STA. 99+29

EXISTING CONDITIONS:

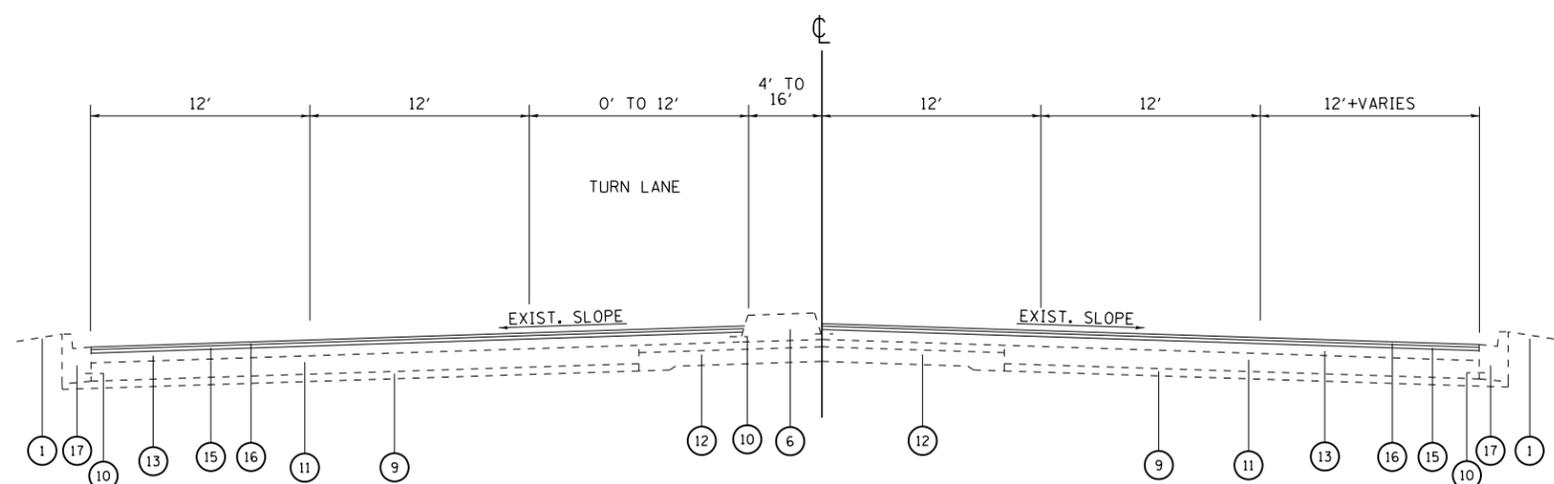
- ① EXISTING SOD
- ② EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.18
- ③ EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.24
- ④ EXISTING CONCRETE CURB, TYPE B-6
- ⑤ EXISTING CONCRETE MEDIAN, BARRIER OR MOUNTABLE
- ⑥ EXISTING CONCRETE MEDIAN, CORRUGATED OR BARRIER (DOWELED)
- ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4" & VARIES
- ⑧ EXISTING GRASS MEDIAN
- ⑨ EXISTING SUB-BASE, GRANULAR, 4" OR 6"
- ⑩ EXISTING TIE BARS
- ⑪ EXISTING PCC BASE COURSE, 9"(±)
- ⑫ EXISTING PCC PAVEMENT, (9"-7"-9")
- ⑬ EXISTING HMA SURFACE COURSE, 4"(±)

PROPOSED CONDITIONS:

- ⑭ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑮ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑯ PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1 3/4"
- ⑰ COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)

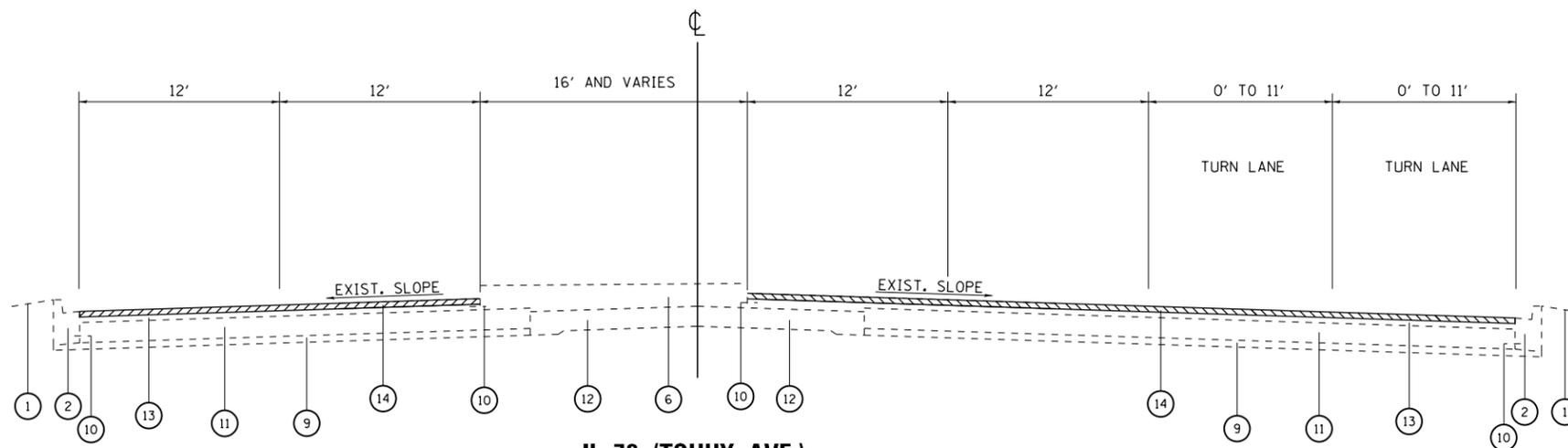
NOTES:

- 1. LEFT TURN LANE EXISTS BETWEEN STA. 87+74 AND STA. 90+33 ON THE RIGHT SIDE OF THE MEDIAN



IL 72 (TOUHY AVE.)
PROPOSED TYPICAL SECTION
 STA. 86+77 TO STA. 99+29

FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS IL 72 (TOUHY AVE.) - (E/O MT. PROSPECT RD. TO LEE ST.)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default	PLOT DATE = 2/2/2018	DATE -	REVISED -					341	3035RS-1	COOK	36	8
					CONTRACT NO. 62F65			ILLINOIS FED. AID PROJECT				
					SCALE: NONE	SHEET	OF	SHEETS	STA.	TO	STA.	



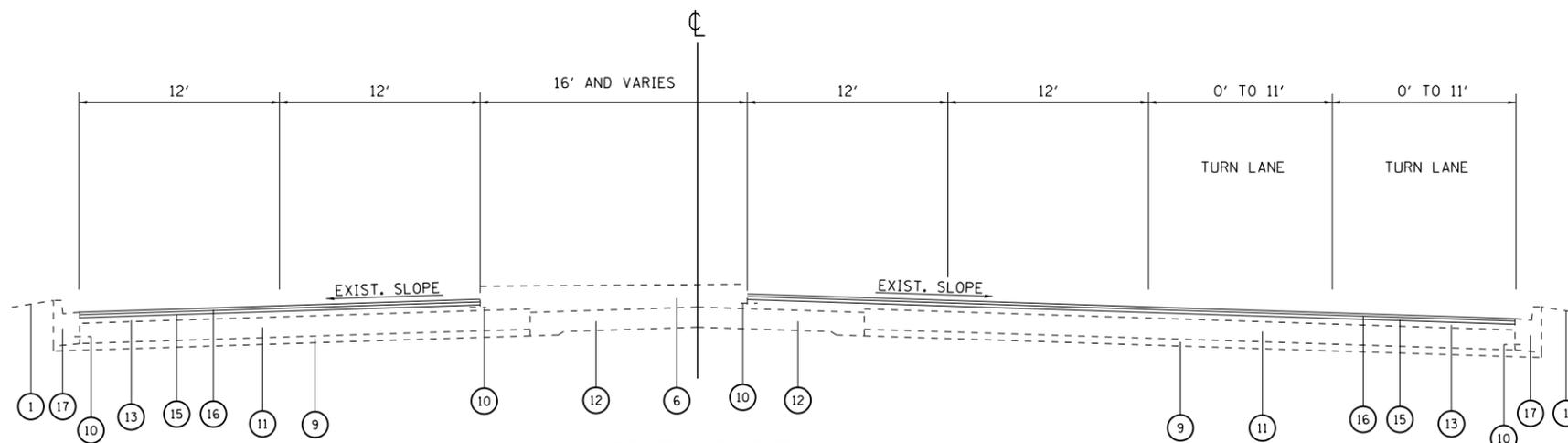
IL 72 (TOUHY AVE.)
EXISTING TYPICAL SECTION
 STA. 99+29 TO STA. 104+00

EXISTING CONDITIONS:

- ① EXISTING SOD
- ② EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.18
- ③ EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.24
- ④ EXISTING CONCRETE CURB, TYPE B-6
- ⑤ EXISTING CONCRETE MEDIAN, BARRIER OR MOUNTABLE
- ⑥ EXISTING CONCRETE MEDIAN, CORRUGATED OR BARRIER (DOWELED)
- ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4" & VARIES
- ⑧ EXISTING GRASS MEDIAN
- ⑨ EXISTING SUB-BASE, GRANULAR, 4" OR 6"
- ⑩ EXISTING TIE BARS
- ⑪ EXISTING PCC BASE COURSE, 9" (±)
- ⑫ EXISTING PCC PAVEMENT, (9"-7"-9")
- ⑬ EXISTING HMA SURFACE COURSE, 4" (±)

PROPOSED CONDITIONS:

- ⑭ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑮ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑯ PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1 3/4"
- ⑰ COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)



IL 72 (TOUHY AVE.)
PROPOSED TYPICAL SECTION
 STA. 99+29 TO STA. 104+00

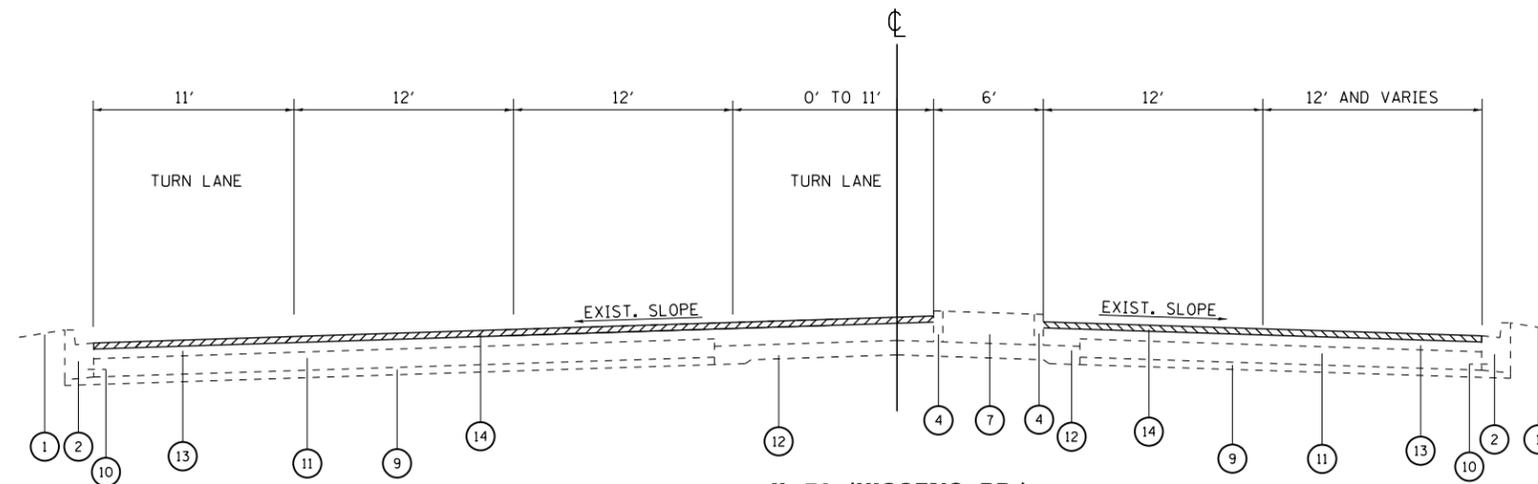
FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS IL 72 (TOUHY AVE.) - (E/O MT. PROSPECT RD. TO LEE ST.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI0268\Drawings\Design\DI02618-sh-typical.dgn	PLotted	CHECKED -	REVISED -			341	3035RS-1	COOK	36	9
Default	PLotted	DATE -	REVISED -			CONTRACT NO. 62F65				
	PLotted					SCALE:	SHEET	OF	SHEETS	STA.
						ILLINOIS FED. AID PROJECT				

EXISTING CONDITIONS:

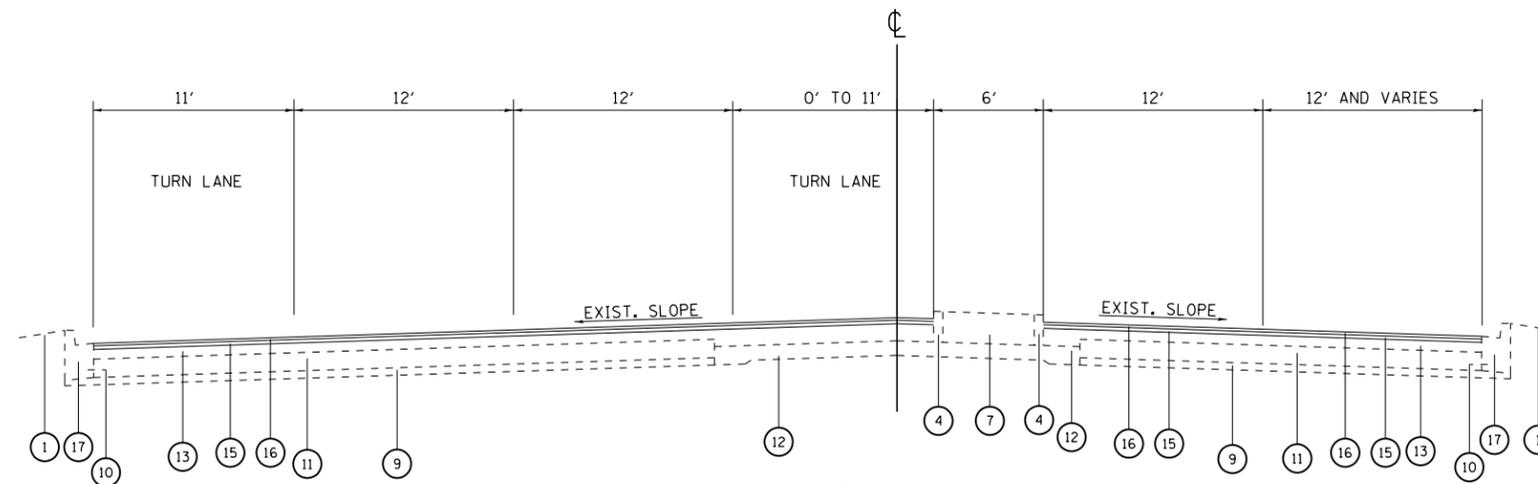
- ① EXISTING SOD
- ② EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.18
- ③ EXISTING COMB. CONC. CURB AND GUTTER, TYPE B-6.24
- ④ EXISTING CONCRETE CURB, TYPE B-6
- ⑤ EXISTING CONCRETE MEDIAN, BARRIER OR MOUNTABLE
- ⑥ EXISTING CONCRETE MEDIAN, CORRUGATED OR BARRIER (DOWELED)
- ⑦ EXISTING CONCRETE MEDIAN SURFACE, 4" & VARIES
- ⑧ EXISTING GRASS MEDIAN
- ⑨ EXISTING SUB-BASE, GRANULAR, 4" OR 6"
- ⑩ EXISTING TIE BARS
- ⑪ EXISTING PCC BASE COURSE, 9"(±)
- ⑫ EXISTING PCC PAVEMENT, (9"-7"-9")
- ⑬ EXISTING HMA SURFACE COURSE, 4"(±)

PROPOSED CONDITIONS:

- ⑭ PROPOSED HMA SURFACE REMOVAL, 2½"
- ⑮ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾"
- ⑯ PROPOSED POLYMERIZED HMA SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1¾"
- ⑰ COMB. CONC. CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DETERMINED BY THE ENGINEER)



IL 72 (HIGGINS RD.)
EXISTING TYPICAL SECTION
 HIGGINS RD. AND LEE ST. (SOUTH QUADRANT)



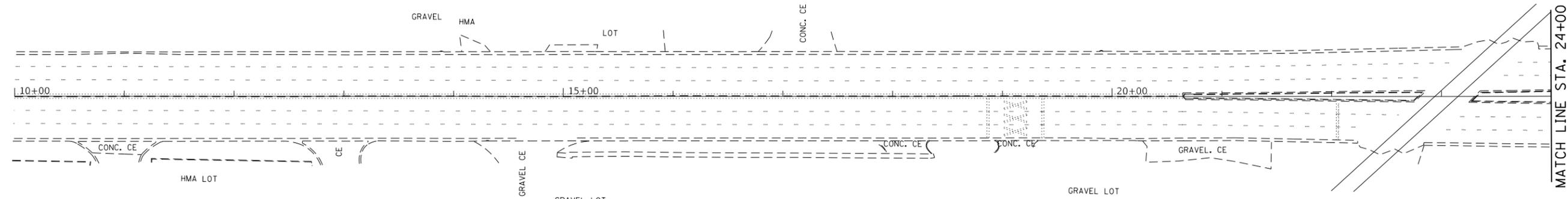
IL 72 (HIGGINS RD.)
PROPOSED TYPICAL SECTION
 HIGGINS RD. AND LEE ST. (SOUTH QUADRANT)

NOTES:

- 1. THE CONTRACTOR SHALL PATCH BEFORE MILLING BETWEEN HIGGINS RD. AND LEE ST.

FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS IL 72 (TOUHY AVE.) - (E/O MT. PROSPECT RD. TO LEE ST.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI026\BROWNS\Design\DI02618-sh-typical.dgn	PLotted	CHECKED -	REVISED -			341	3035RS-1	COOK	36	10
Default	PLot SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62F65				
	PLot DATE = 2/2/2018					SCALE:	SHEET	OF	SHEETS	STA.

IL. ROUTE 72 (TOUHY AVE.)



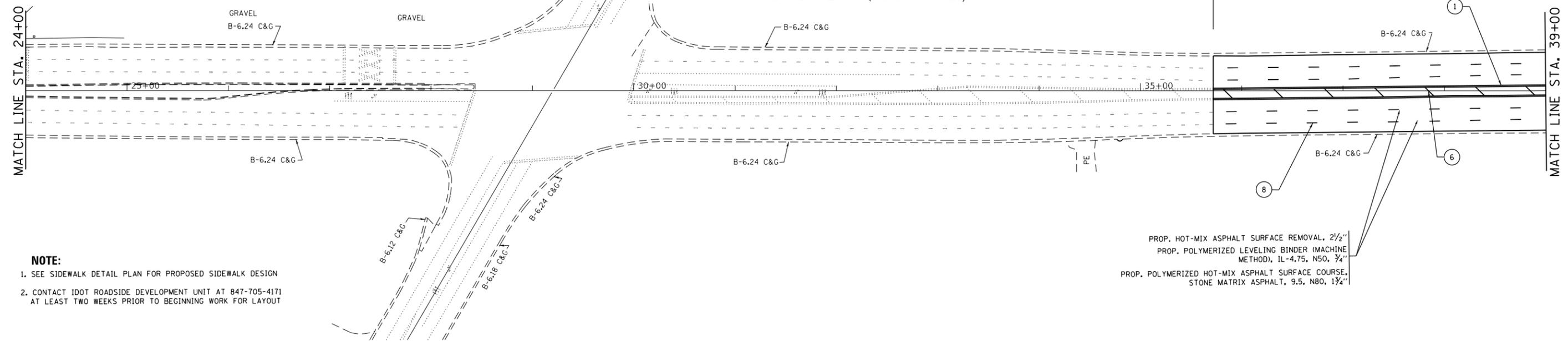
PAVEMENT MARKING LEGEND

- | | |
|---|--|
| ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.) | ⑧ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP (TYP.) |
| ② THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.) | ⑨ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL SOLID WHITE LINE @ 10' C-C (TYP.) |
| ③ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.) | ⑩ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' LINE WITH 6' SPACE (TYP.) |
| ④ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP. UNLESS OTHERWISE NOTED) | ⑪ THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE ISLAND MARKING (TYP.) |
| ⑤ THERMOPLASTIC PAVEMENT MARKING, 4" SINGLE SOLID YELLOW LINE (TYP.) | ⑫ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.) |
| ⑥ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL SOLID YELLOW LINE AT 75' C-C (TYP.) | ⑬ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 3' LINE WITH 9' SPACE (TYP.) |
| ⑦ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, SOLID WHITE (TYP.) | |

IL. ROUTE 72 (TOUHY AVE.)



**PROJECT BEGINS
STA. 35 + 72**



PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
 PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
 PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80, 1 3/4"

NOTE:
 1. SEE SIDEWALK DETAIL PLAN FOR PROPOSED SIDEWALK DESIGN
 2. CONTACT IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST TWO WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT

MODEL: Default
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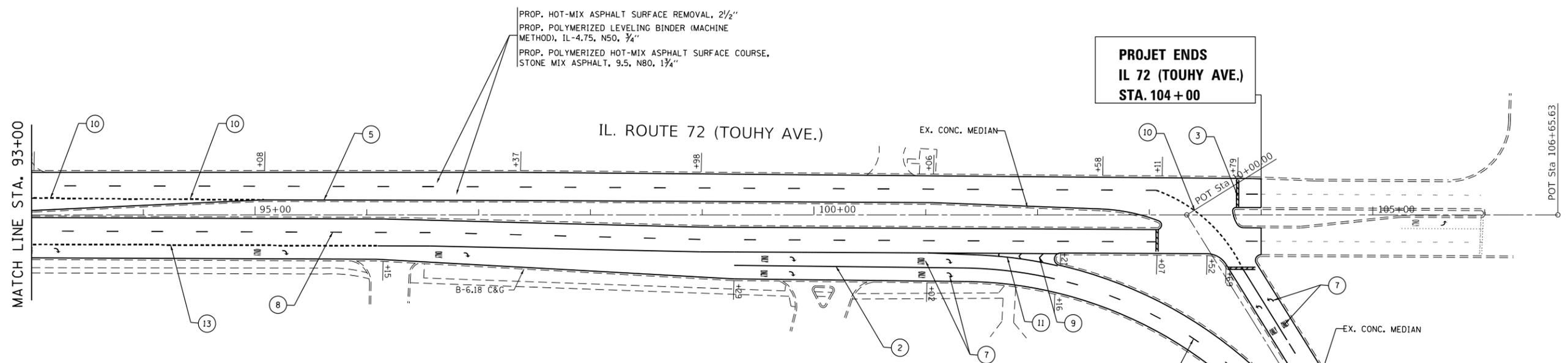
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	DRAWN - RWB	REVISED -
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PLOT DATE = 2/2/2018	DATE - 8/10/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 72 FROM E/O MT. PROSPECT ROAD TO LEE STREET
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1"= 50' SHEET 2 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	11
CONTRACT NO. 62F65				
ILLINOIS FED. AID PROJECT				



**PROJET ENDS
IL 72 (TOUHY AVE.)
STA. 104 + 00**

PAVEMENT MARKING LEGEND	
① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)	⑧ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP (TYP.)
② THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.)	⑨ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL SOLID WHITE LINE @ 10' C-C (TYP.)
③ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)	⑩ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' LINE WITH 6' SPACE (TYP.)
④ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP. UNLESS OTHERWISE NOTED)	⑪ THERMOPLASTIC PAVEMENT MARKING, 8" SOLID WHITE ISLAND MARKING (TYP.)
⑤ THERMOPLASTIC PAVEMENT MARKING, 4" SINGLE SOLID YELLOW LINE (TYP.)	⑫ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW LANE LINE, 10' DASH - 30' SKIP (TYP.)
⑥ THERMOPLASTIC PAVEMENT MARKING, 12" DIAGONAL SOLID YELLOW LINE AT 75° C-C (TYP.)	⑬ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 3' LINE WITH 9' SPACE (TYP.)
⑦ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, SOLID WHITE (TYP.)	

EXIST. CURVE IL 72
 P.I. STA. = 15+56.51
 $\Delta = 30^\circ 54' 41"$
 $D = 730' 26"$
 $R = 784.11'$
 $T = 216.80'$
 $L = 423.03'$
 $E = 29.42'$
 $TX =$
 $SE RUN =$
 P.C. STA. = 13+39.71
 P.T. STA. = 17+62.74

**PROJET ENDS:
IL 72 (HIGGINS RD.)
STA. 15 + 41**

IL. RTE. 72 (LEE ST.)



NOTE:
 1. SEE SIDEWALK DETAIL PLAN FOR PROPOSED SIDEWALK DESIGN
 2. CONTACT IDOT ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 AT LEAST TWO WEEKS PRIOR TO BEGINNING WORK FOR LAYOUT

MODEL: Default
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	DRAWN - RWB	REVISED -
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PLOT DATE = 2/2/2018	DATE - 8/10/2017	REVISED -

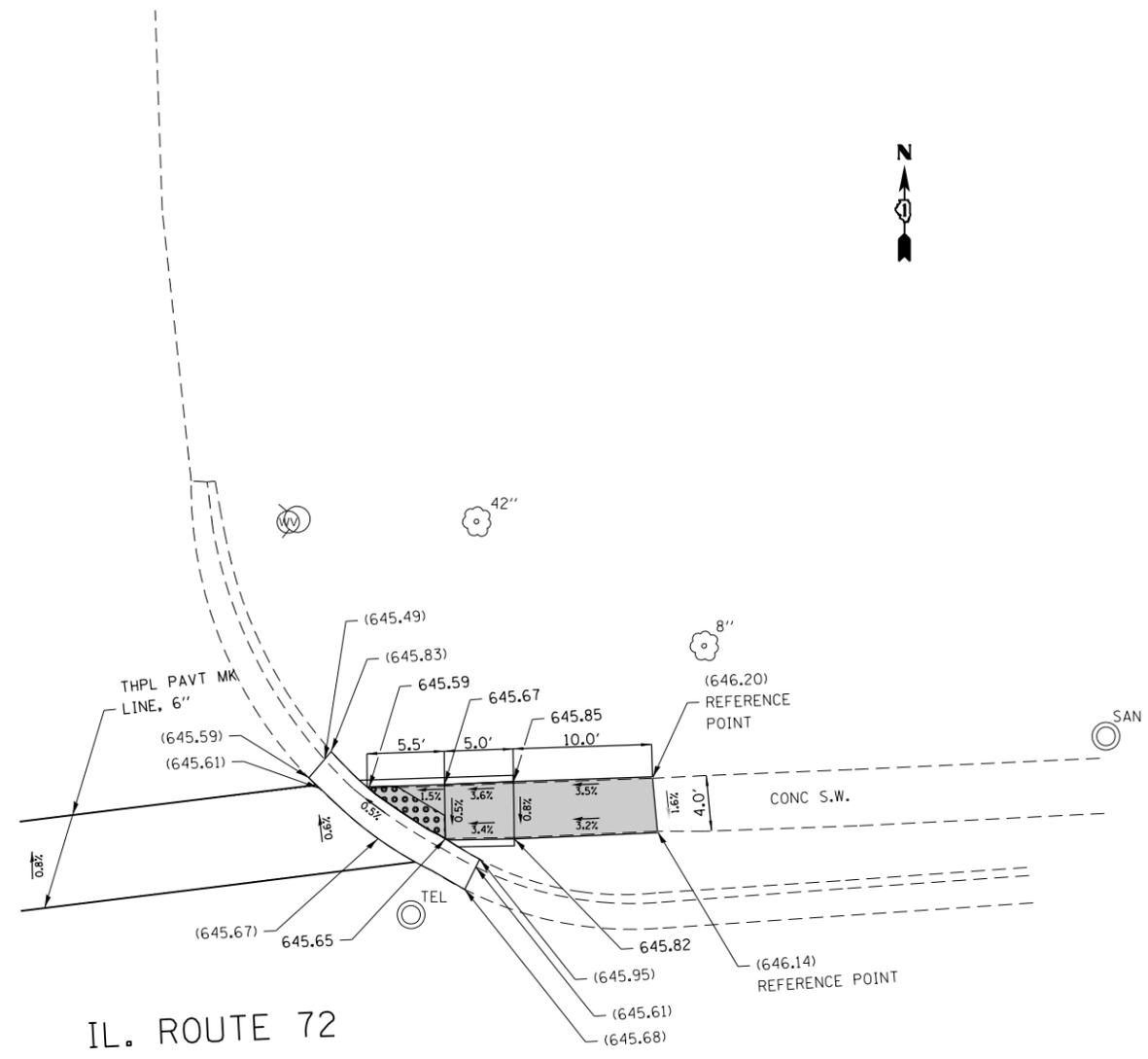
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**IL 72 FROM E/O MT. PROSPECT ROAD TO LEE STREET
ROADWAY AND PAVEMENT MARKING PLAN**

SCALE: 1" = 50' SHEET 4 OF 4 SHEETS STA. TO STA.

F.A.P. RTE. 341	SECTION 3035RS-1	COUNTY COOK	TOTAL SHEETS 36	SHEET NO. 14
CONTRACT NO. 62F65				
ILLINOIS FED. AID PROJECT				

WESTVIEW DR.



LEGEND

xx.xx' EXISTING LENGTH
 () EXISTING ELEVATION/SLOPE

PROPOSED SIDEWALK
 PROPOSED SIDE CURB
 SC TOP OF PROPOSED SIDE CURB

CLASS D PATCH
 DETECTABLE WARNINGS
 SIDEWALK REMOVAL
 REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 648.22
 BENCHMARK: TOP SPINDLE OF FIRE HYDRANT
 LOCATION: NE CORNER OF TOUHY & WESTVIEW DR.

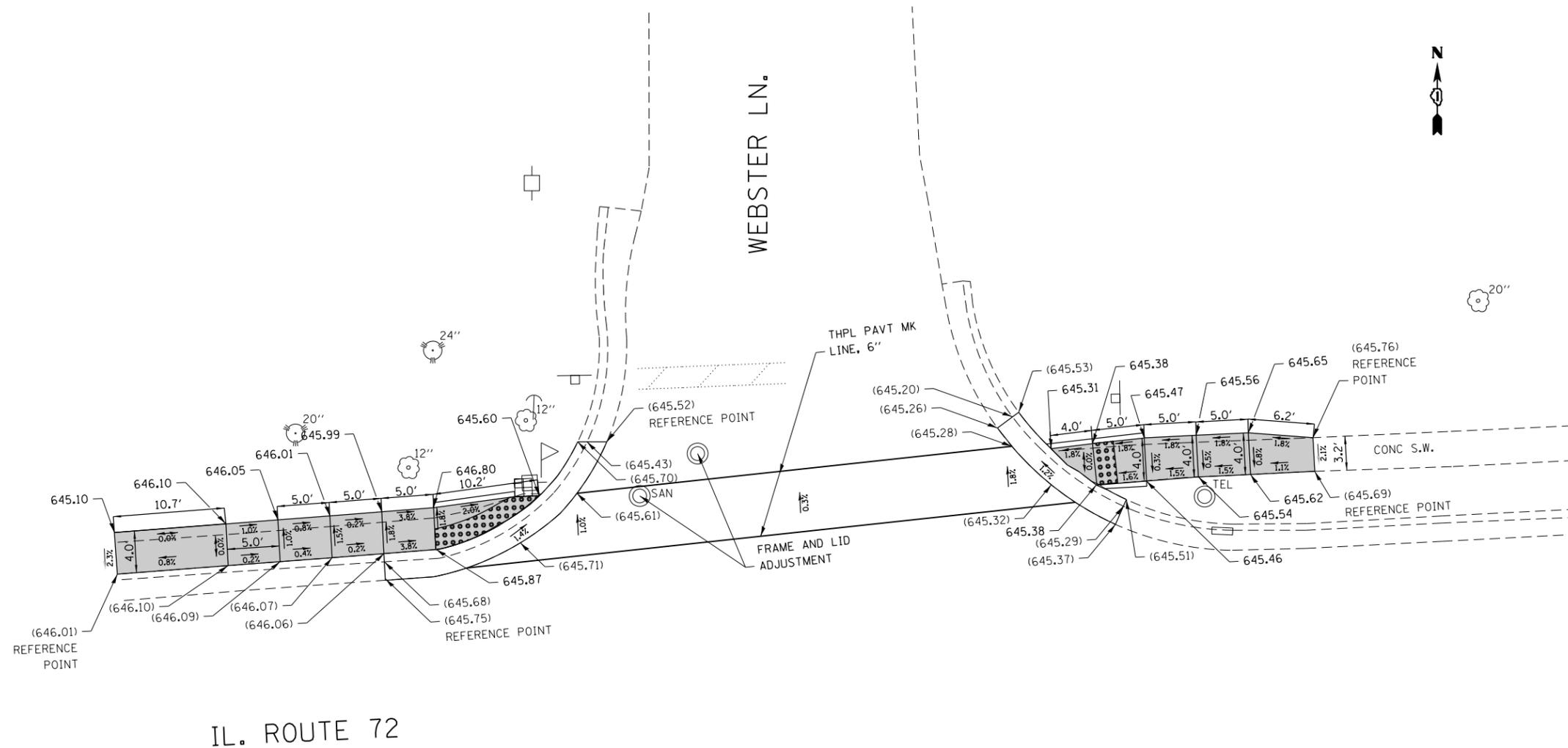
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Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 2/2/2018	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
 IL 72 AT WEBSTER LANE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	15
CONTRACT NO. 62F65				
ILLINOIS FED. AID PROJECT				



LEGEND

xx.xx' EXISTING LENGTH
 () EXISTING ELEVATION/SLOPE

PROPOSED SIDEWALK
 PROPOSED SIDE CURB
 SC TOP OF PROPOSED SIDE CURB

CLASS D PATCH
 DETECTABLE WARNINGS
 SIDEWALK REMOVAL
 REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 648.33
 BENCHMARK: TOP SPINDLE OF FIRE HYDRANT
 LOCATION: WEST SIDE OF WEBSTER LANE, ± 300' NORTH OF IL 72

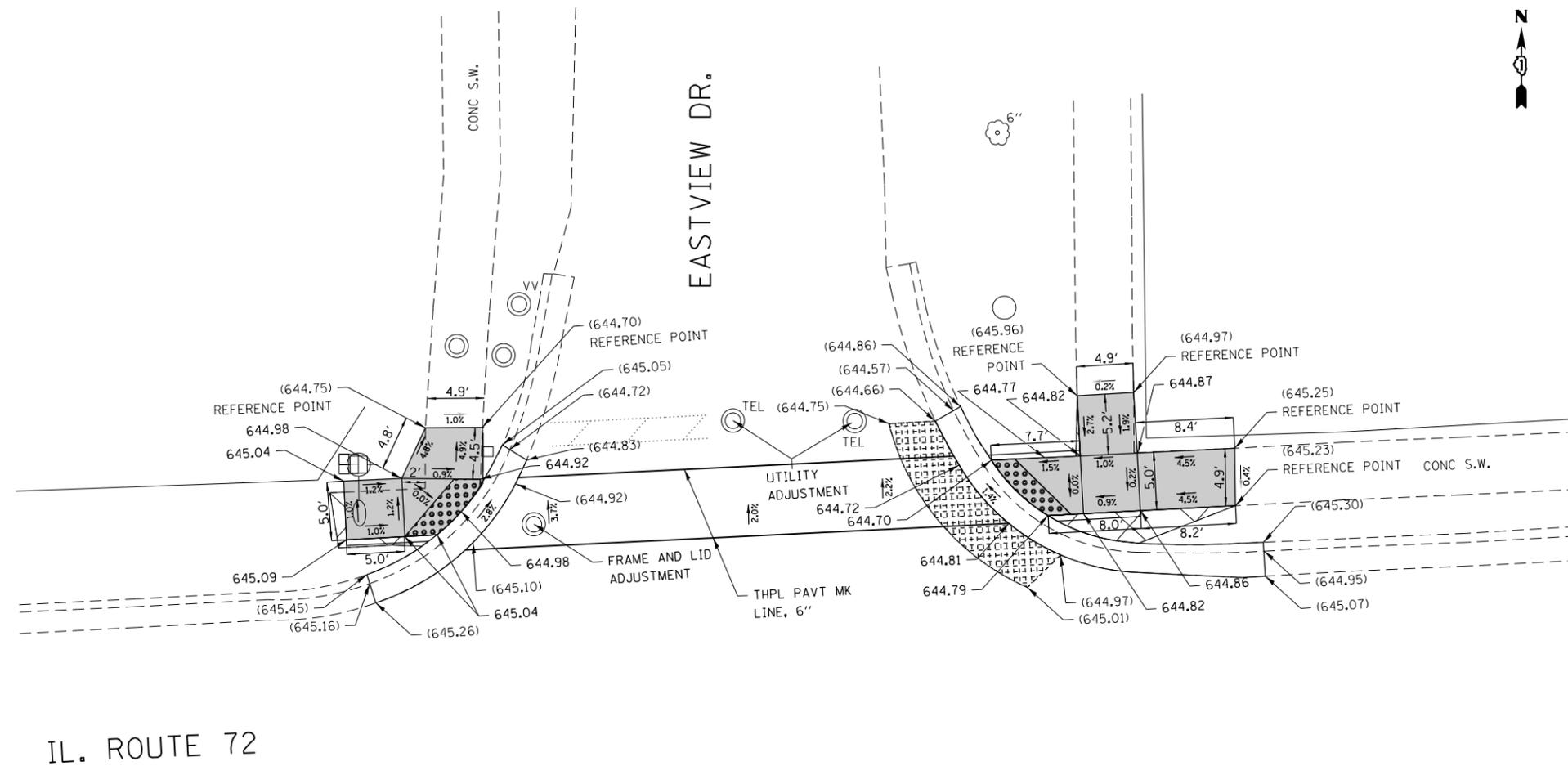
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 2/2/2018	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
 IL 72 AT WEBSTER LANE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	16
CONTRACT NO. 62F65				
ILLINOIS FED. AID PROJECT				



LEGEND

xx.xx' EXISTING LENGTH
 () EXISTING ELEVATION/SLOPE

 PROPOSED SIDEWALK
 PROPOSED SIDE CURB
 SC TOP OF PROPOSED SIDE CURB

 CLASS D PATCH
 DETECTABLE WARNINGS
 SIDEWALK REMOVAL
 REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 648.15
 BENCHMARK: TOP SPINDLE OF FIRE HYDRANT
 LOCATION: NORTH SIDE OF IL 72, ± 250' EAST OF EASTVIEW DRIVE

FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED -
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	PLOT DATE = 2/2/2018	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

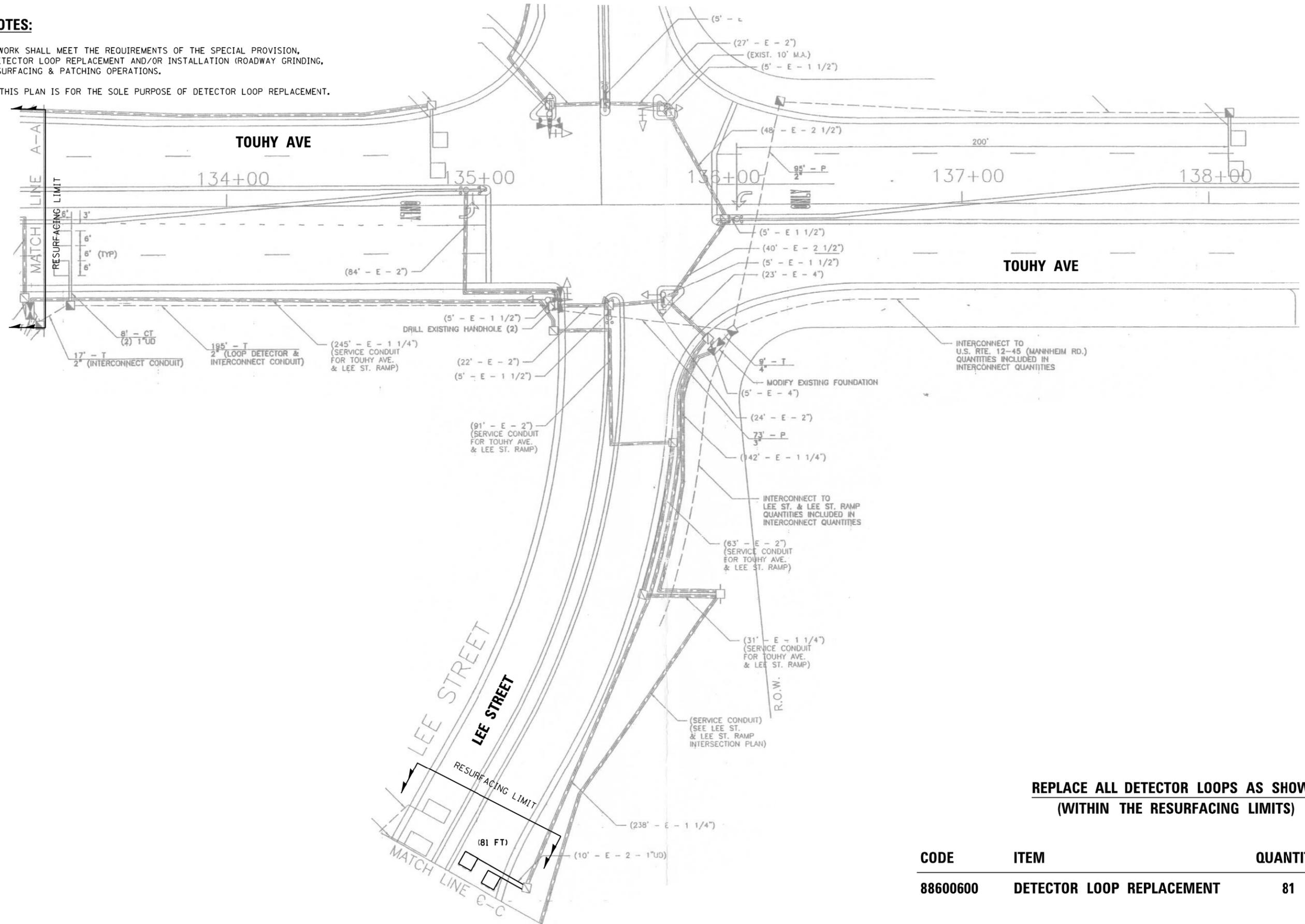
**SIDEWALK DETAIL PLAN
 IL 72 AT EASTVIEW DR.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	17
CONTRACT NO. 62F65				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	81	FOOT

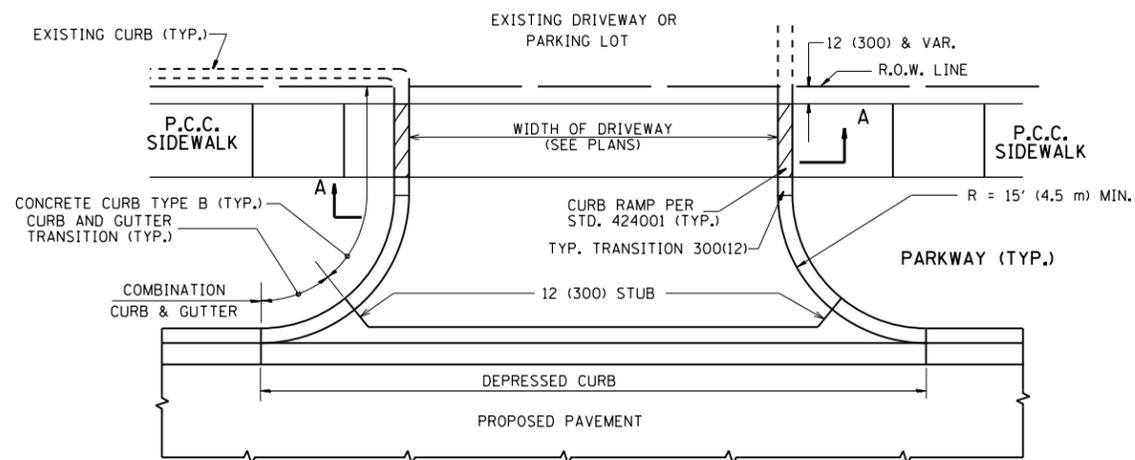
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TS5445 - TOUHY AVE AT LEE ST.08072017.dgn		DRAWN - BAS	REVISED -
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	PLOT DATE = 8/7/2017	DATE - 08/07/2017	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

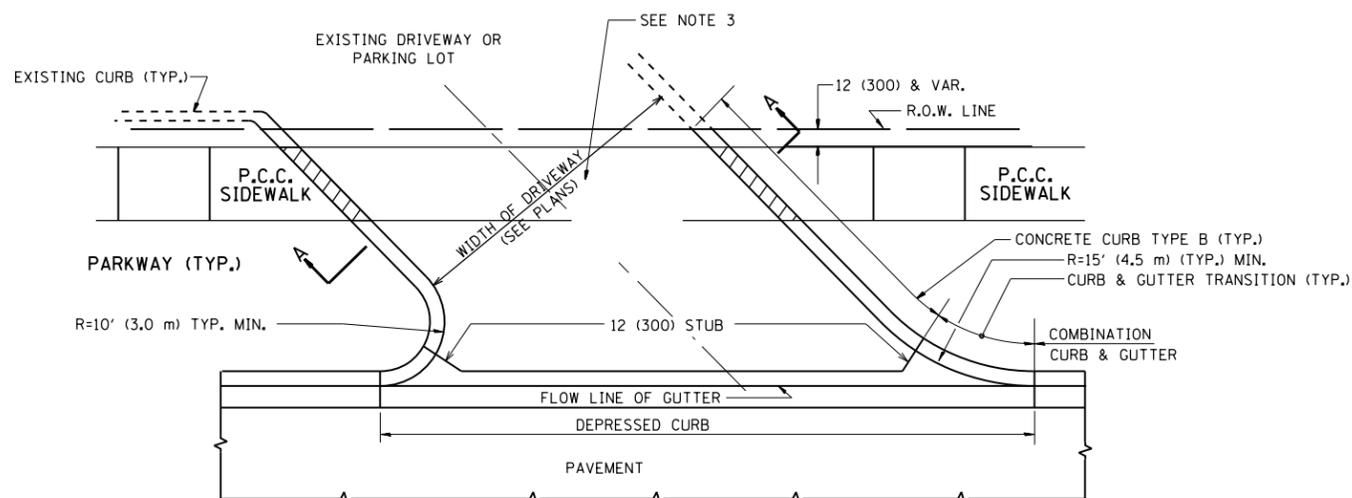
**DETECTOR LOOP REPLACEMENT PLAN
TOUHY AVE AT LEE ST**

SCALE: SHEET OF SHEETS STA. TO STA.

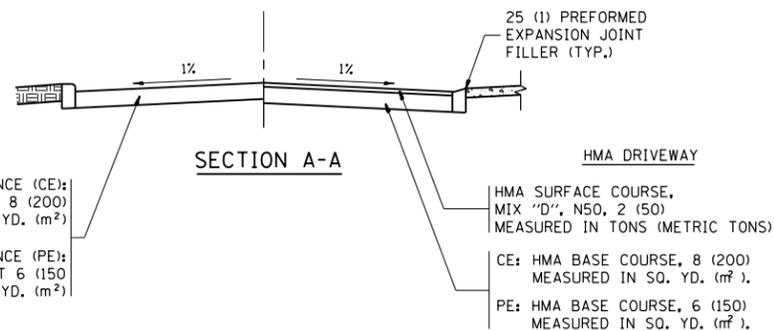
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	19
CONTRACT NO. 62F65				
ILLINOIS FED. AID PROJECT				



WITH CONCRETE CURB, TYPE B

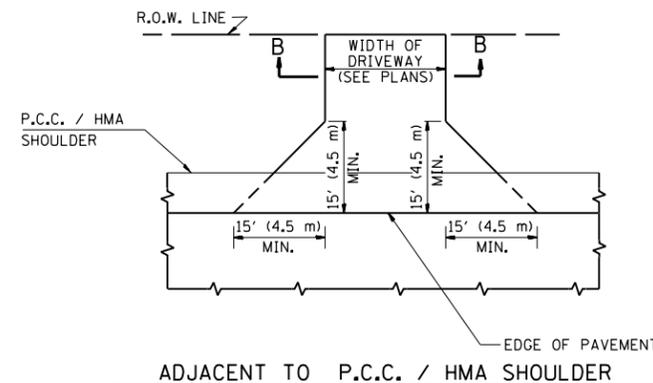


WITH CONCRETE CURB, TYPE B

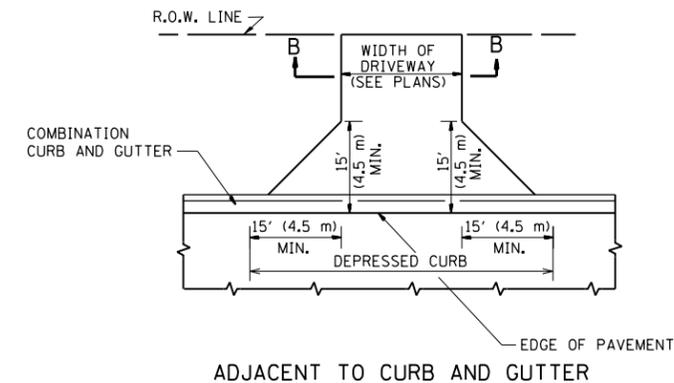


RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)

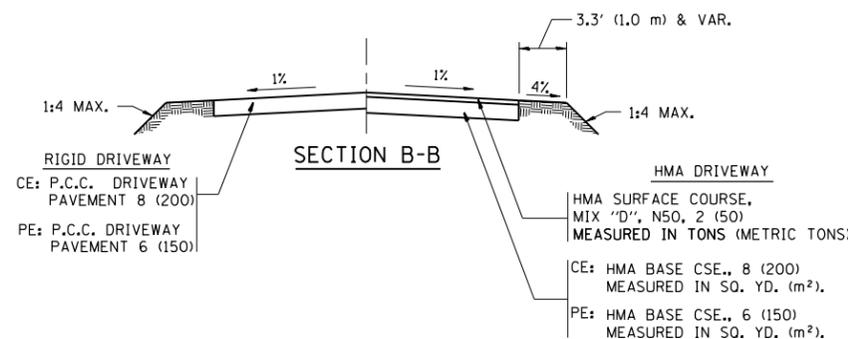
HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²),
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

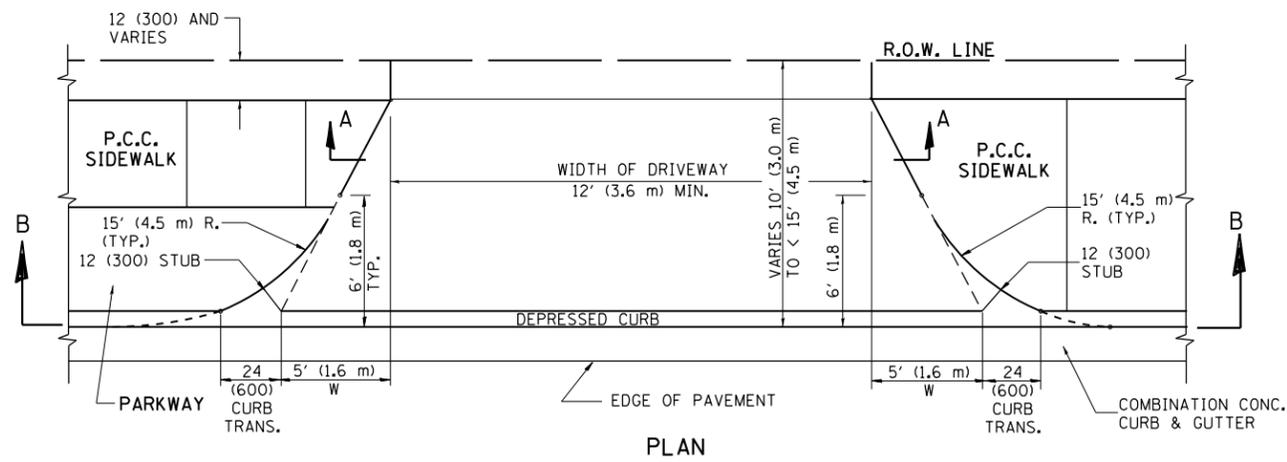
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = alnemratia	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI026\BROWNS\State\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0002' / in.		DATE - 11-04-95	REVISED - R. BORO 06-11-08
PLOT DATE = 2/2/2018			REVISED - R. BORO 09-06-11

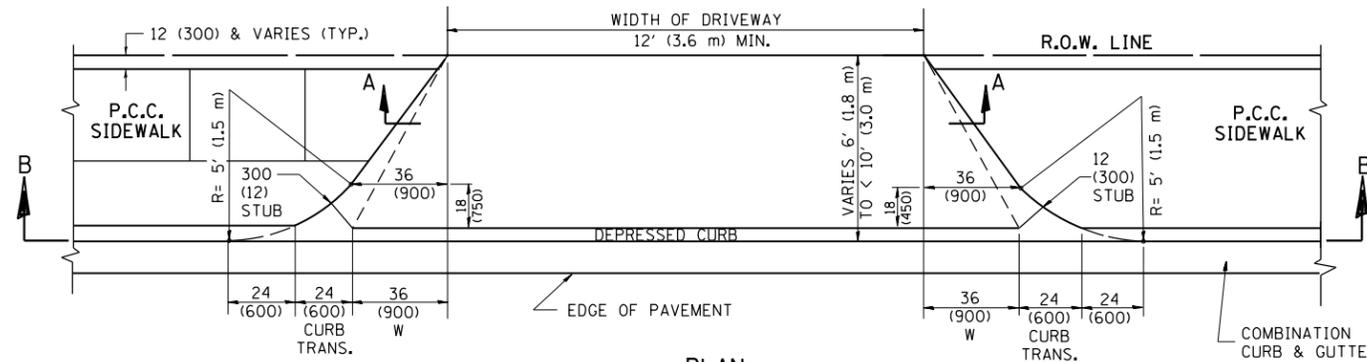
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

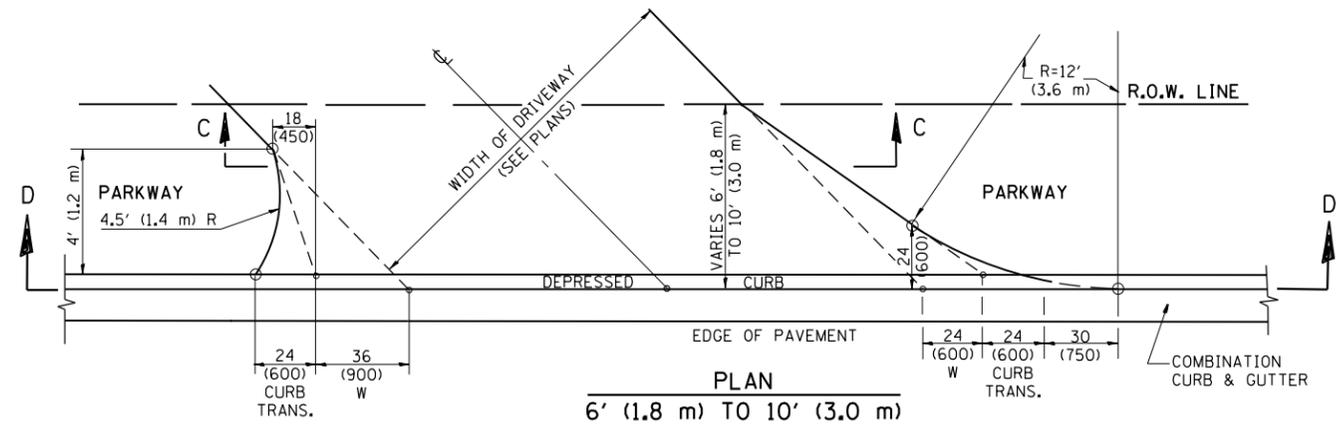
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	21
BD0156-07 (BD-01)		CONTRACT NO. 62F65		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



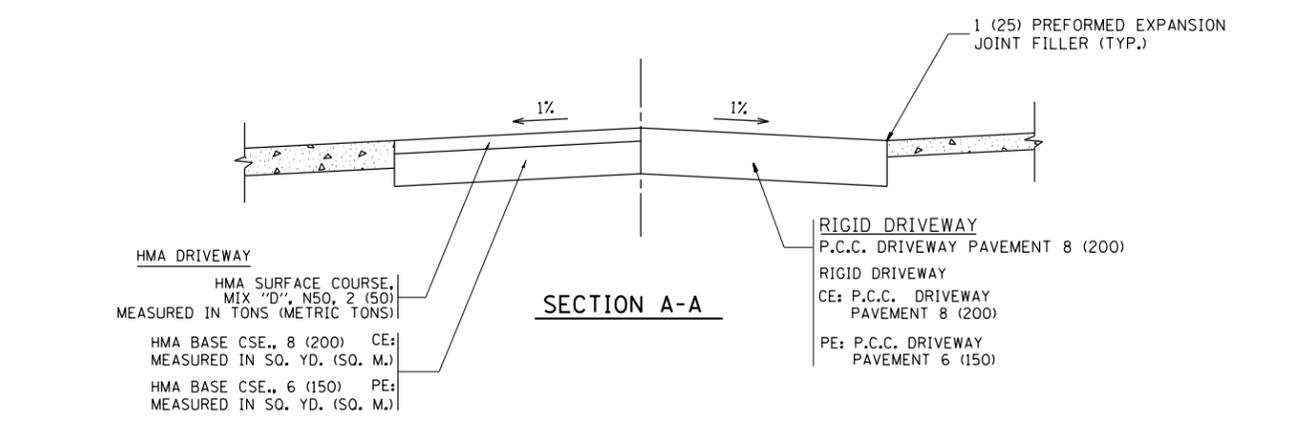
PLAN
10' (3.0 m) TO < 15' (4.5 m)



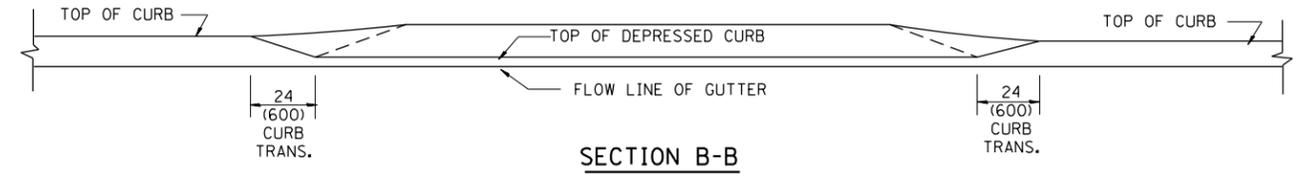
PLAN
6' (1.8 m) TO < 10' (3.0 m)



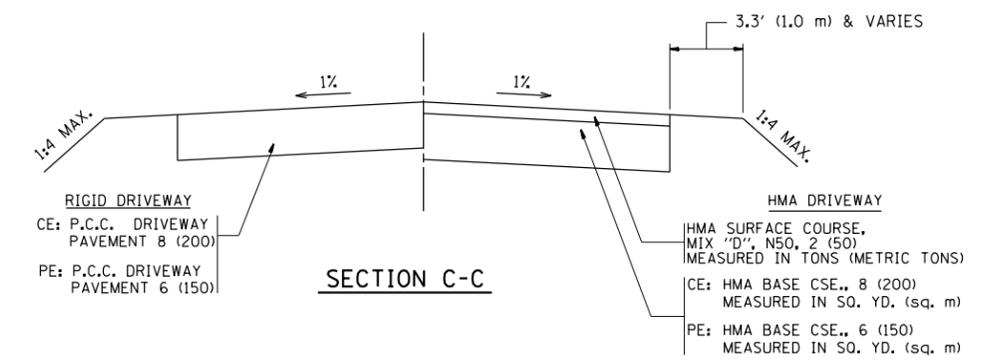
PLAN
6' (1.8 m) TO 10' (3.0 m)



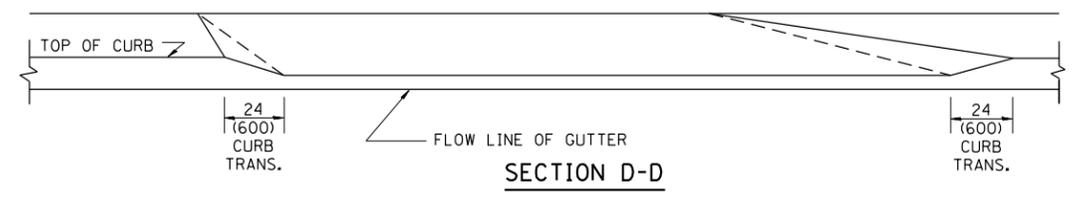
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

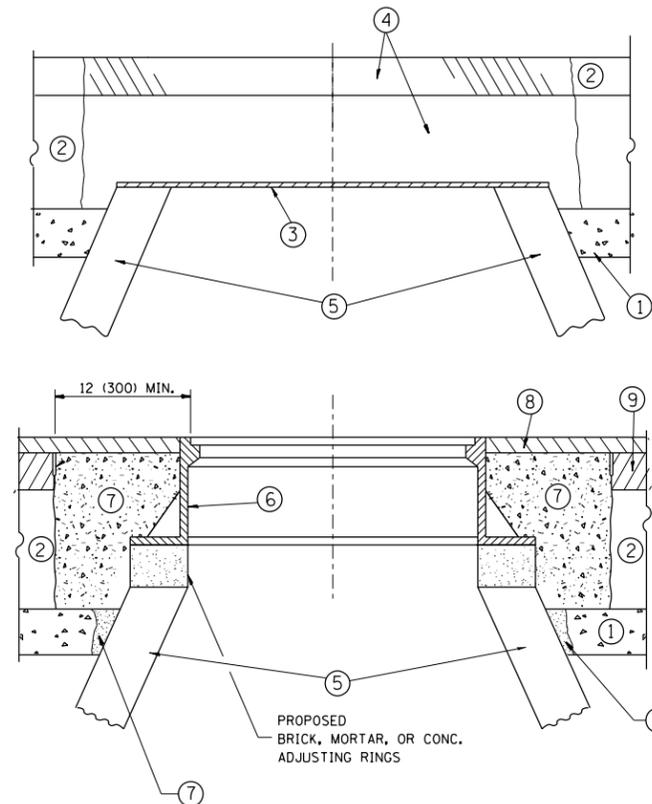
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = alnemratia	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\1026\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - P. LaFLEUR 04-15-03
		DATE - 11-06-95	REVISED - R. BORO 01-01-07
			REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	22
BD400-02 (BD-02)			CONTRACT NO. 62F65	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

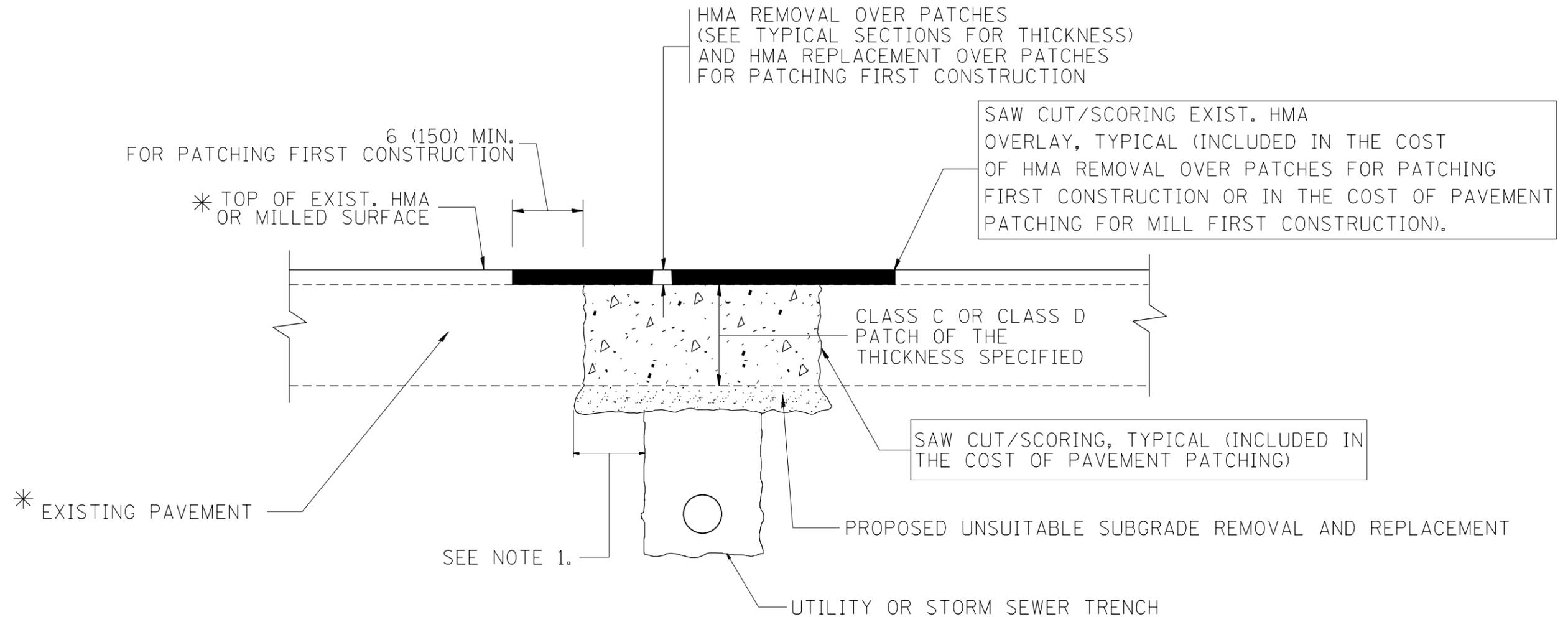
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = alnemratia	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
pw\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI026\Drawings\Design\DistStd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 2/2/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	23
BD600-03 (BD-8)		CONTRACT NO. 62F65		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = alnemratia	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\11026\Drawings\Design\DistStd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					341	3035RS-1	COOK	36	24
	PLOT DATE = 2/2/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62F65				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

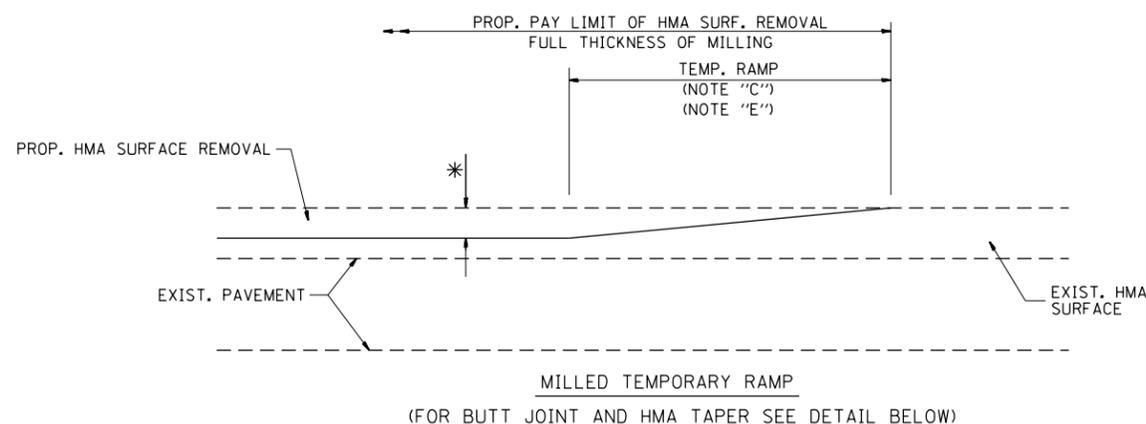
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

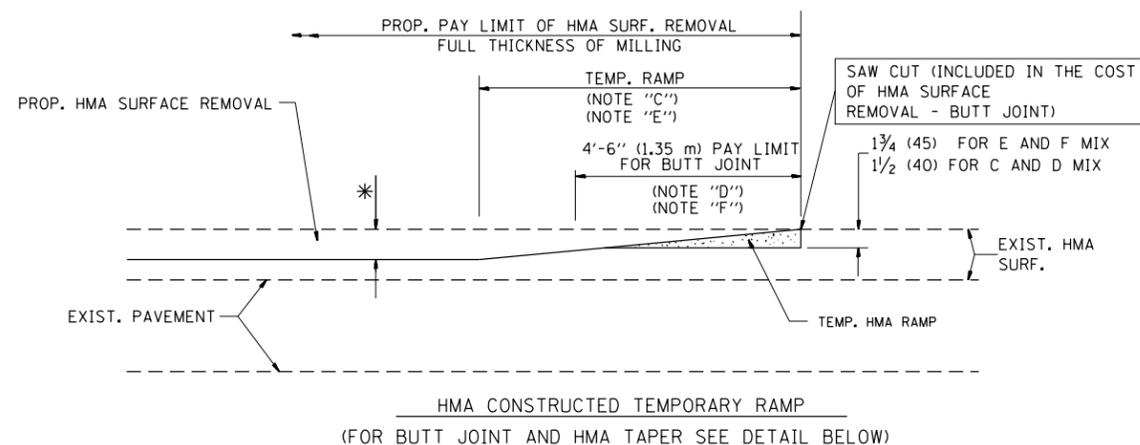
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = alnemratia	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI026\BROWNS\Browns\Design\DistStd.dgn		REVISED - A. ABBAS 03-21-97	REVISED - M. GOMEZ 01-22-01			341	3035RS-1	COOK	36	25
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 62F65		
PLOT DATE = 2/2/2018	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

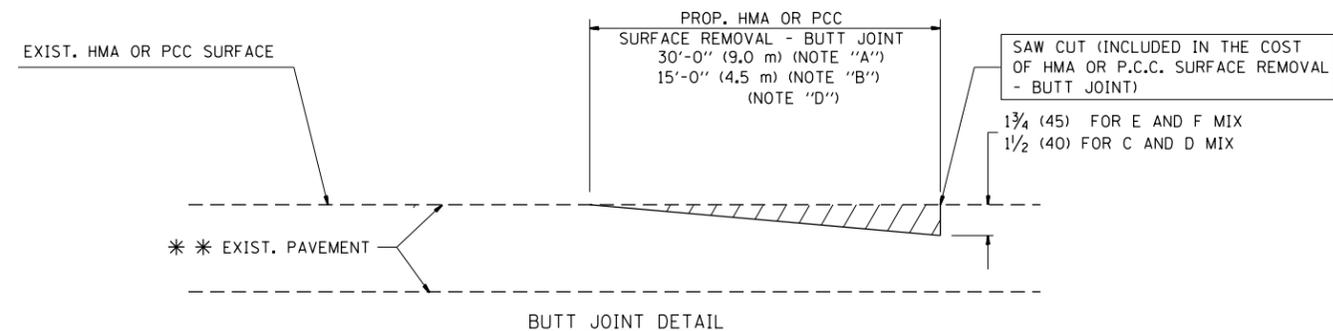


OPTION 1

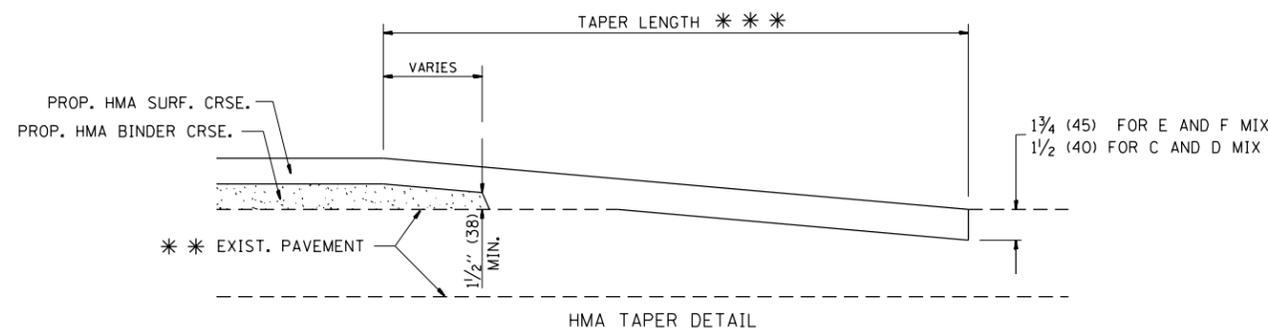


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

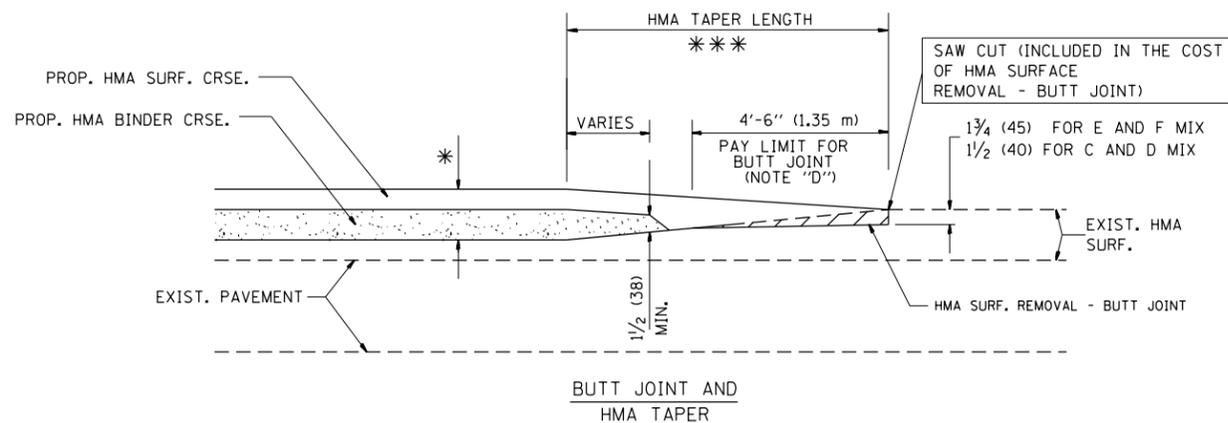
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

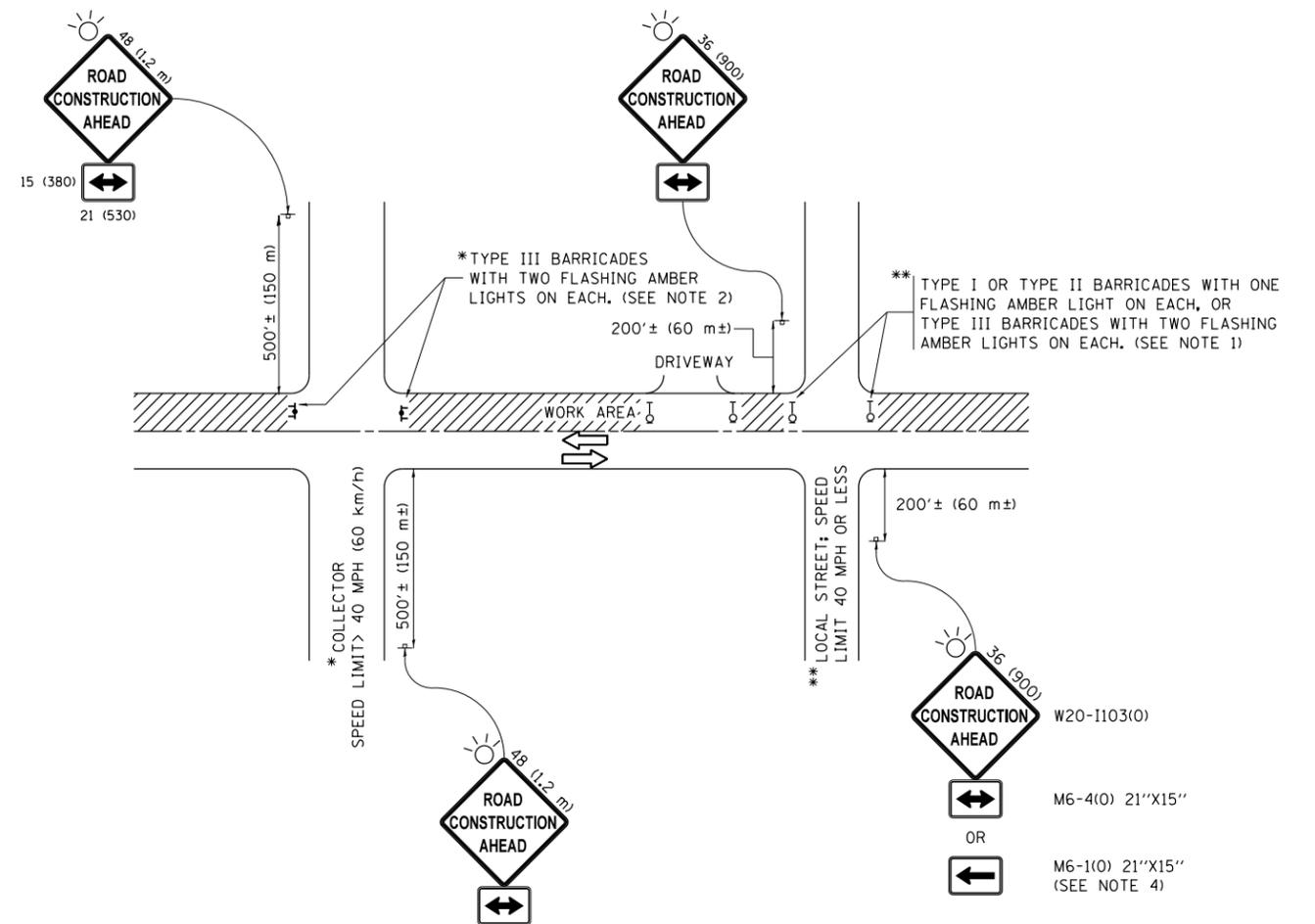
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p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\11026\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 2/2/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	26
BD400-05 BD32		CONTRACT NO. 62F65		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

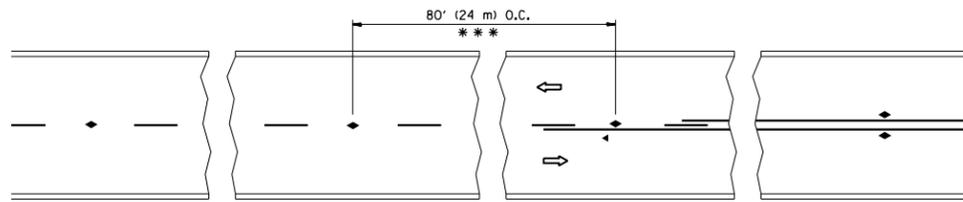
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 2/2/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

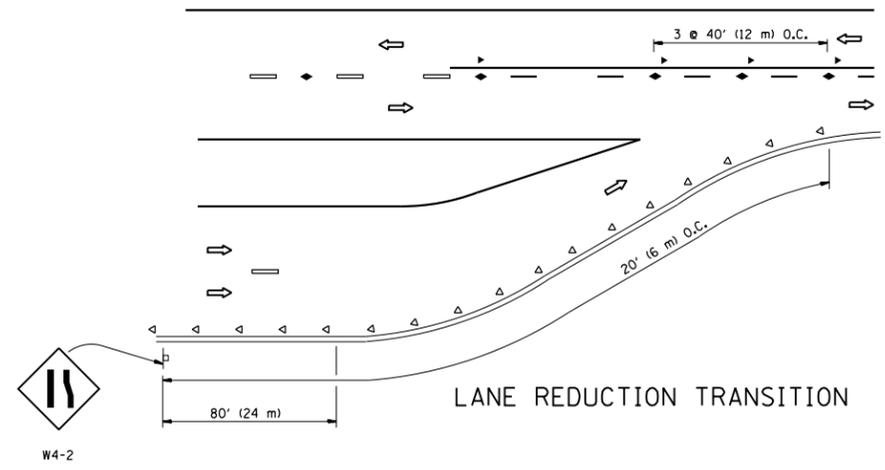
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 62F65	
ILLINOIS FED. AID PROJECT				

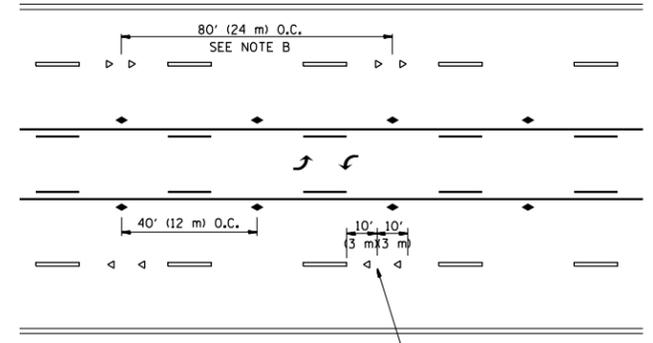


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

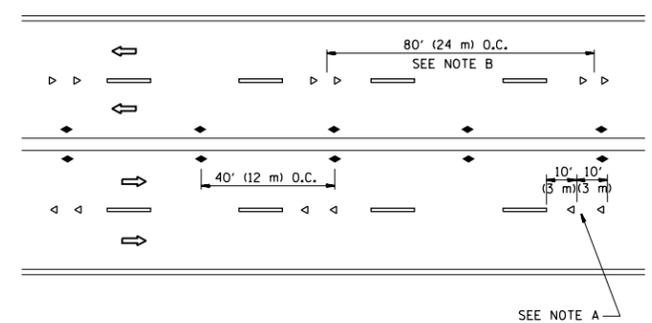
TWO-LANE/TWO-WAY



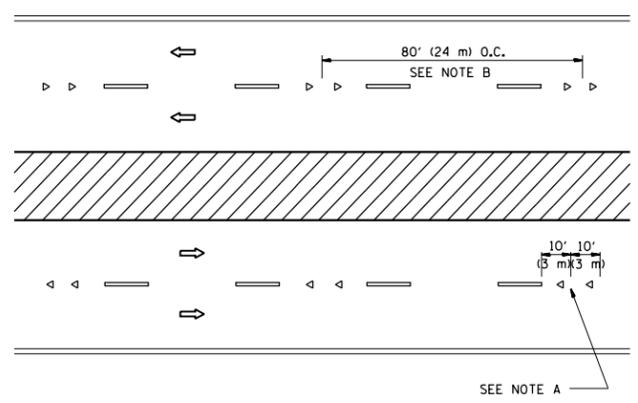
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

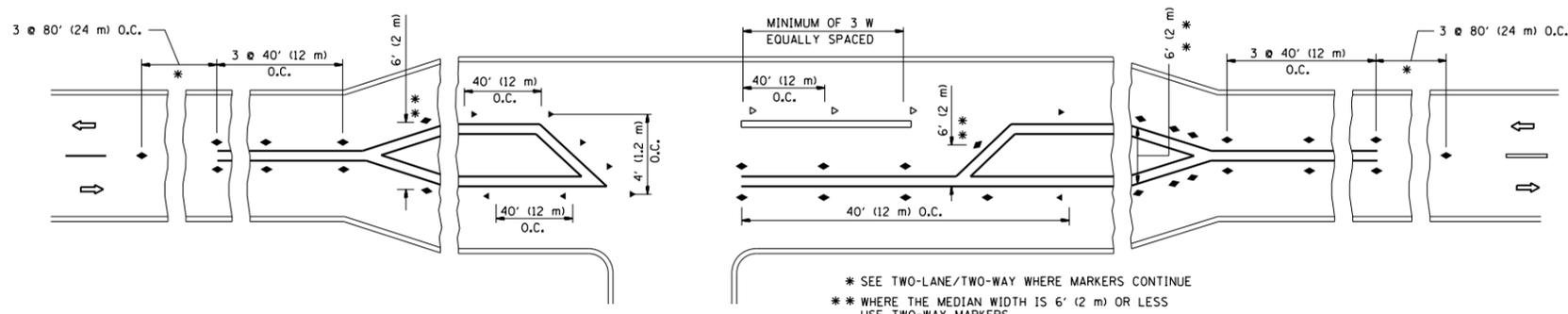
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

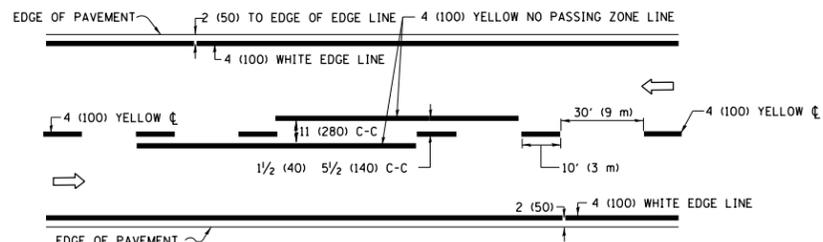


LEFT TURN

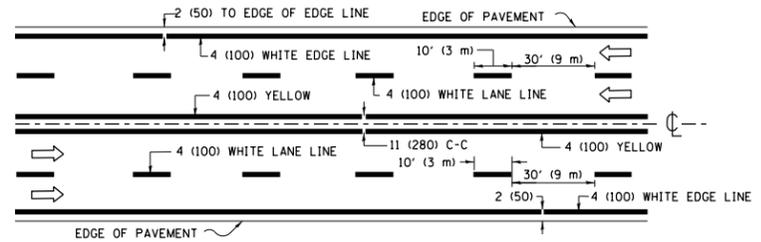
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

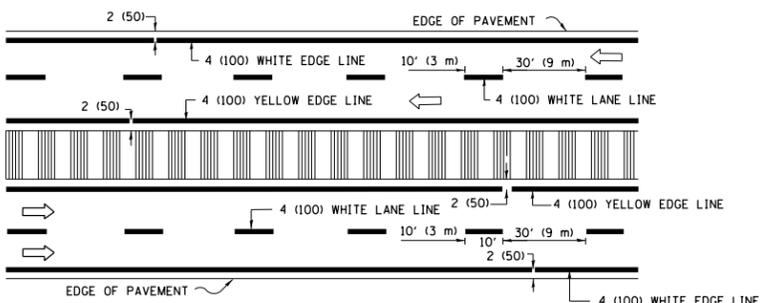
FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI026\BROWN\Design\DistStd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99					341	3035RS-1	COOK	36	28
PLOT DATE = 2/2/2018	DATE -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCIUS 09-09-09		TC-11			CONTRACT NO. 62F65				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

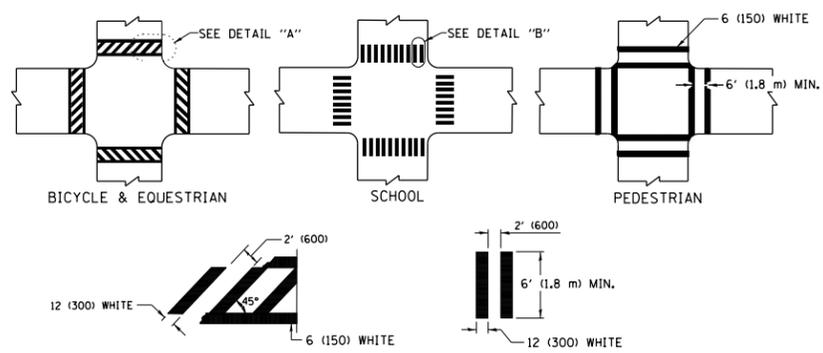


MULTI-LANE UNDIVIDED



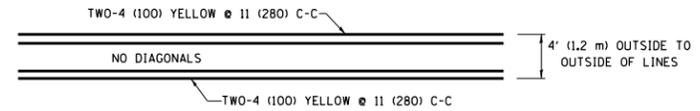
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

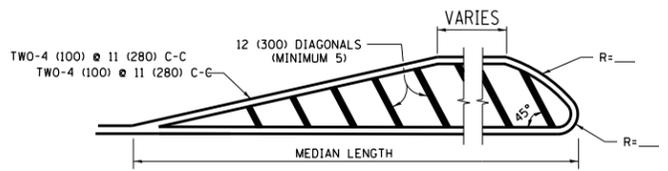


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

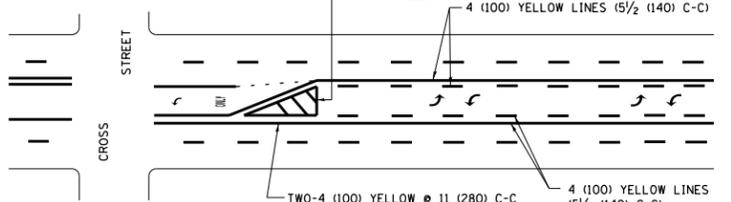


4' (1.2 m) WIDE MEDIANS ONLY



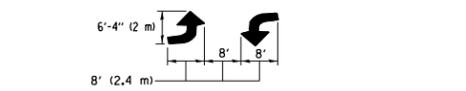
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING

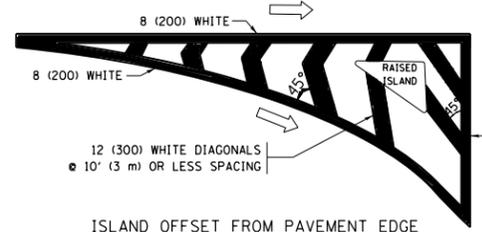
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



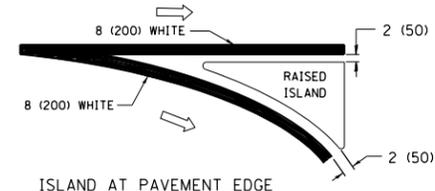
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

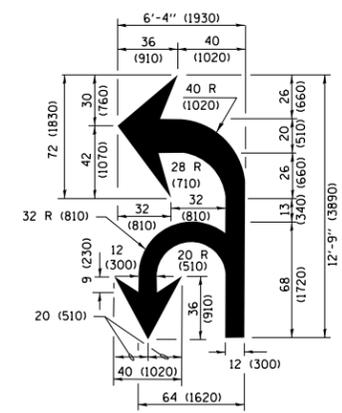
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



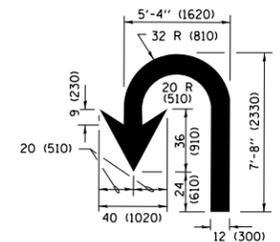
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = alnemratia	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01026\01026\Design\DistStd.dgn			REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 2/2/2018	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	29
TC-13		CONTRACT NO. 62F65		
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

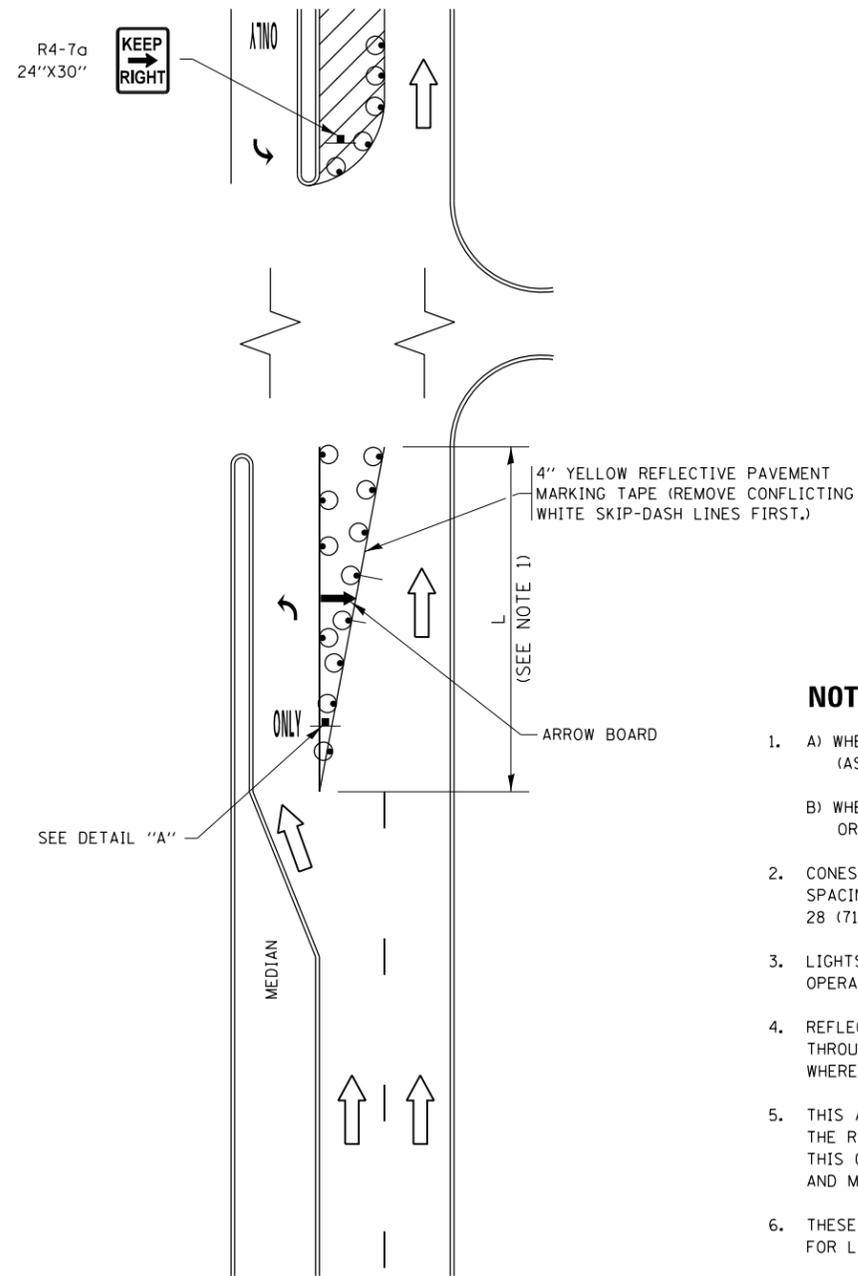


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

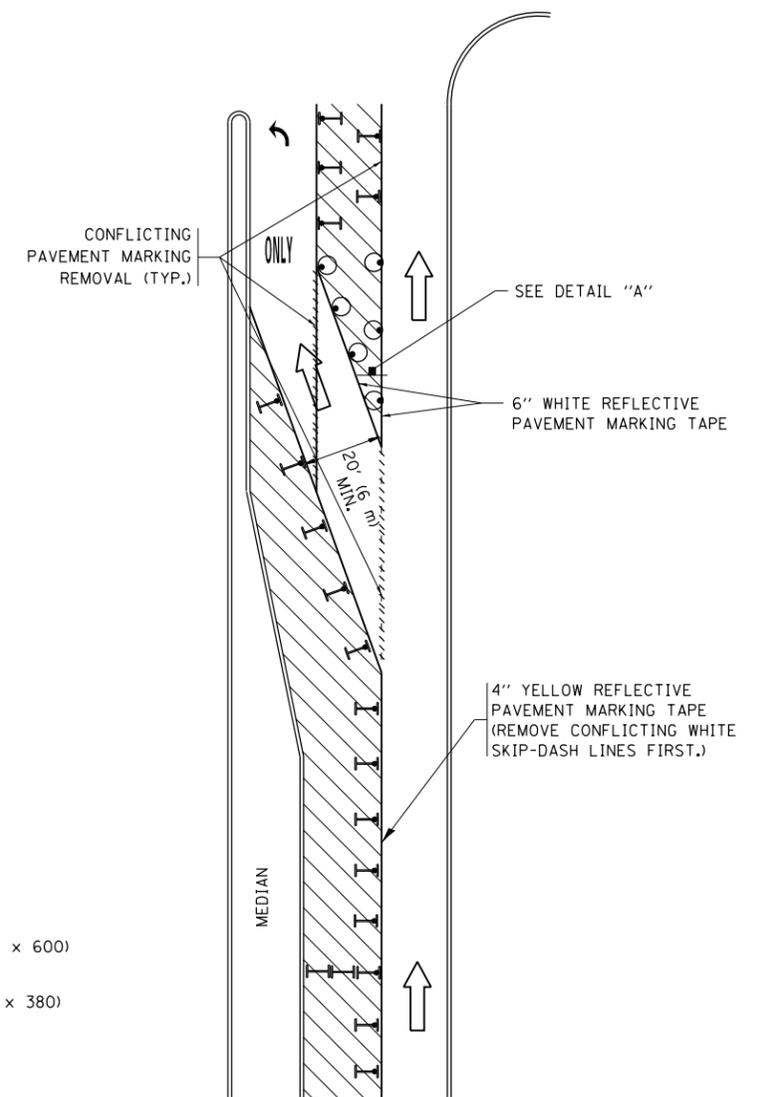


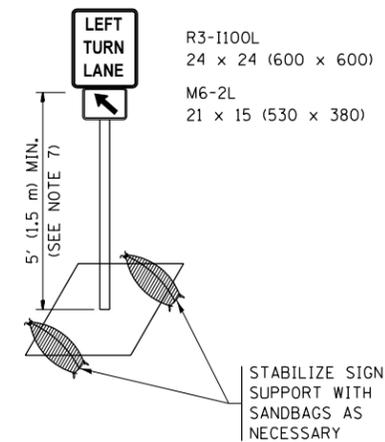
FIGURE 2

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

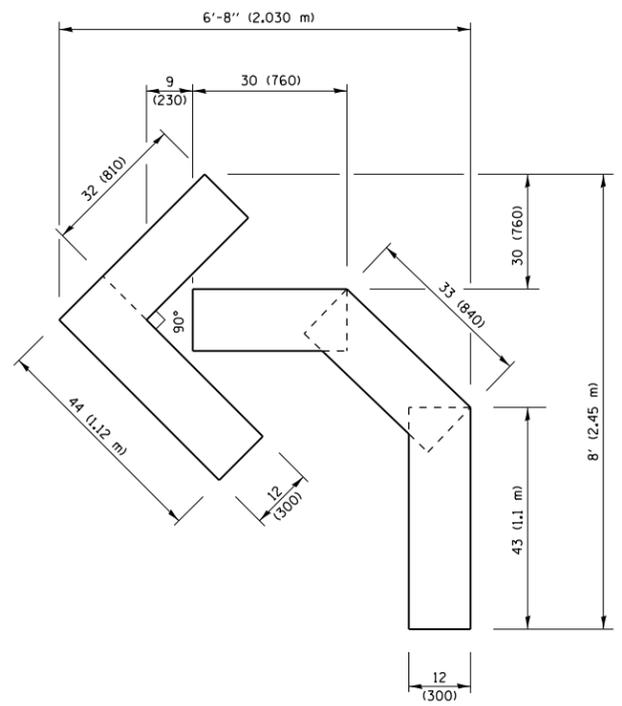
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



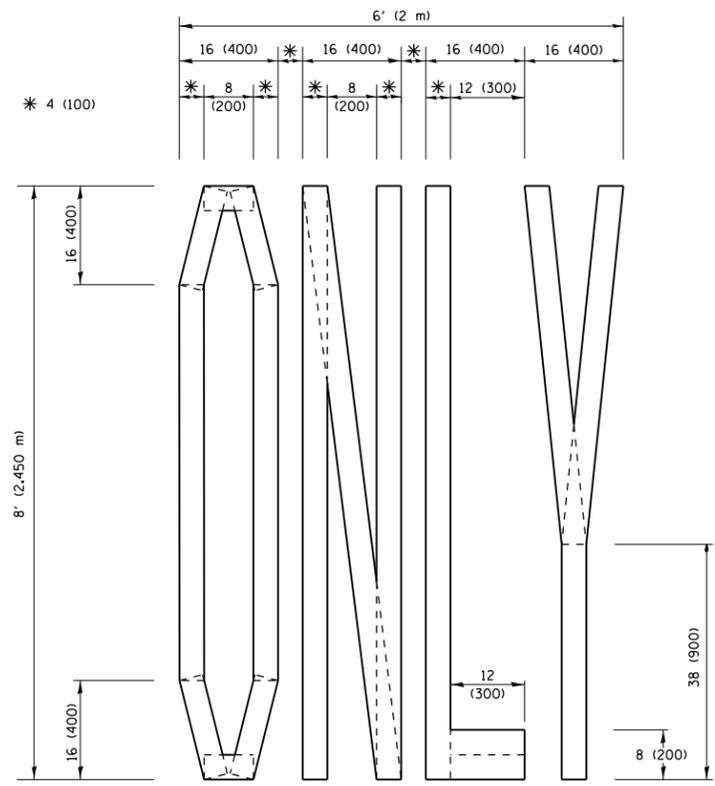
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

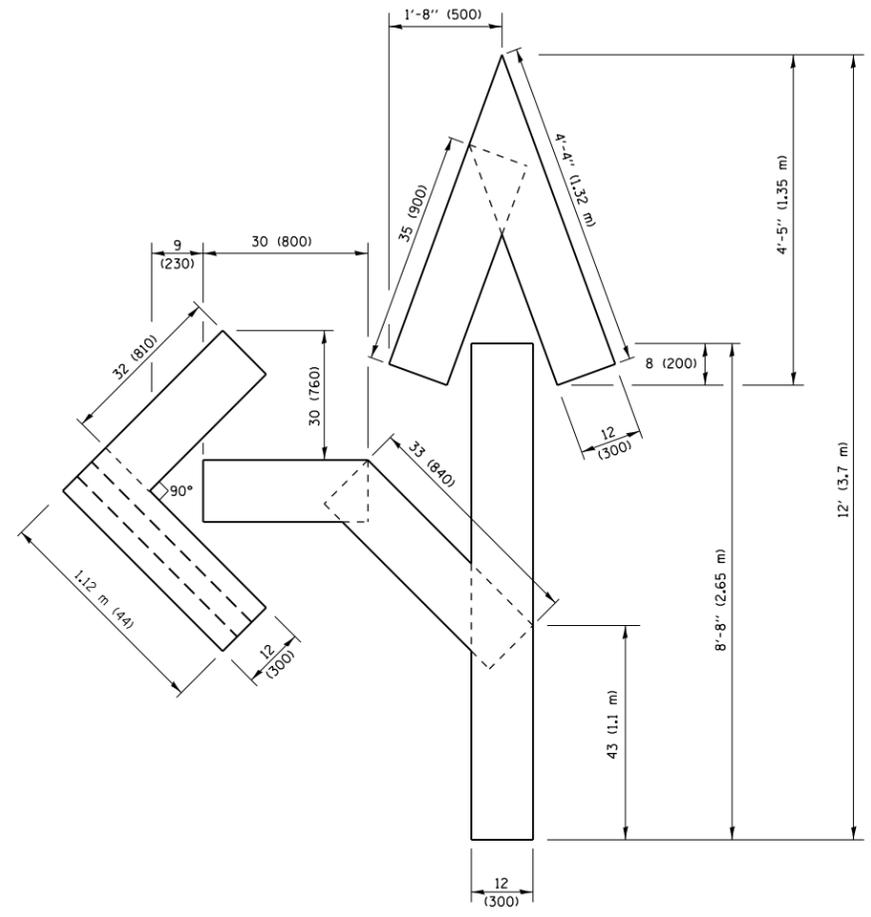
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Default		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13					341	3035RS-1	COOK	36	30
		REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		TC-14			CONTRACT NO. 62F65				
		REVISED - T. RAMMACHER 01-06-00	REVISED -		ILLINOIS FED. AID PROJECT							



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

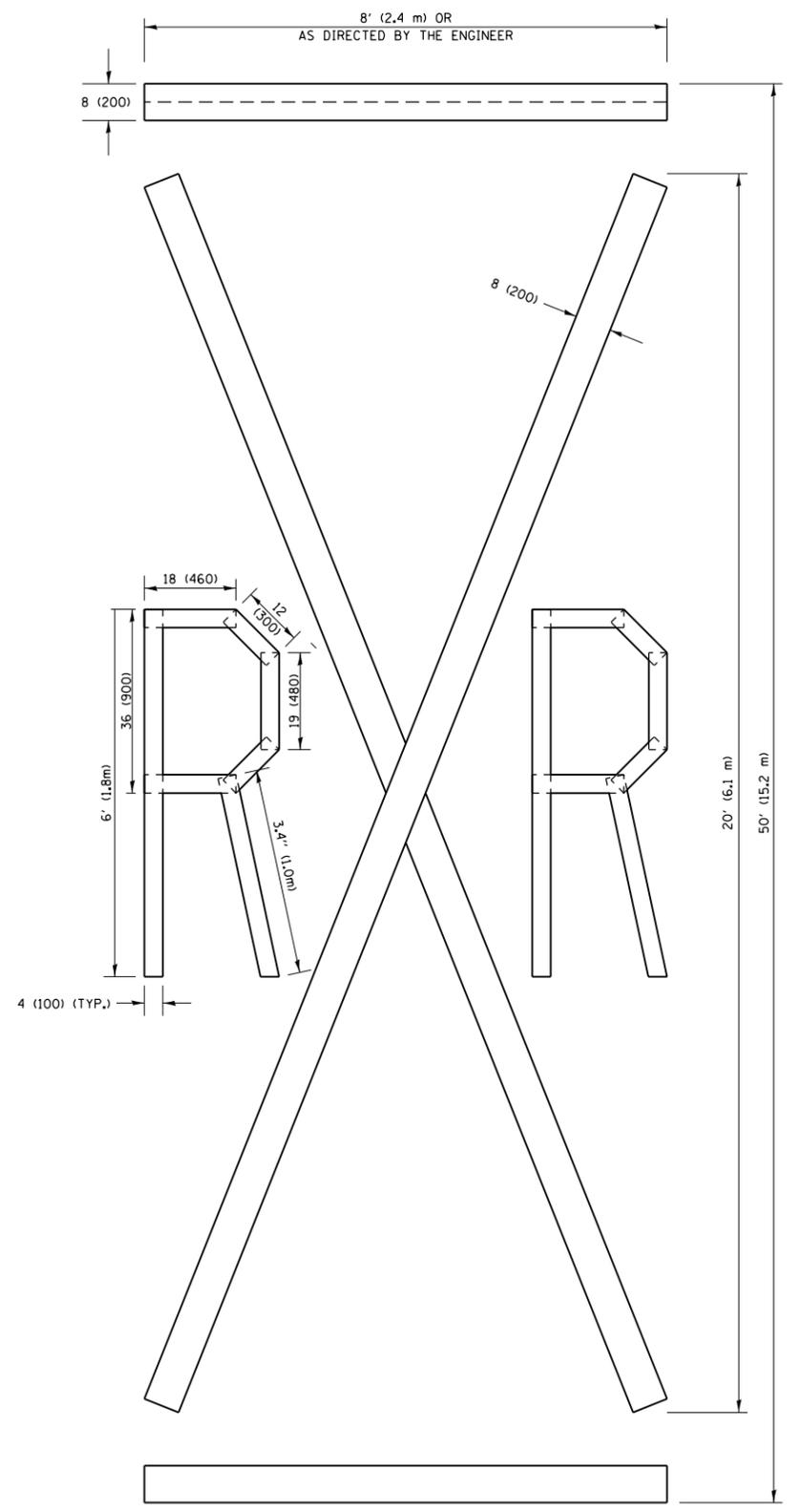


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

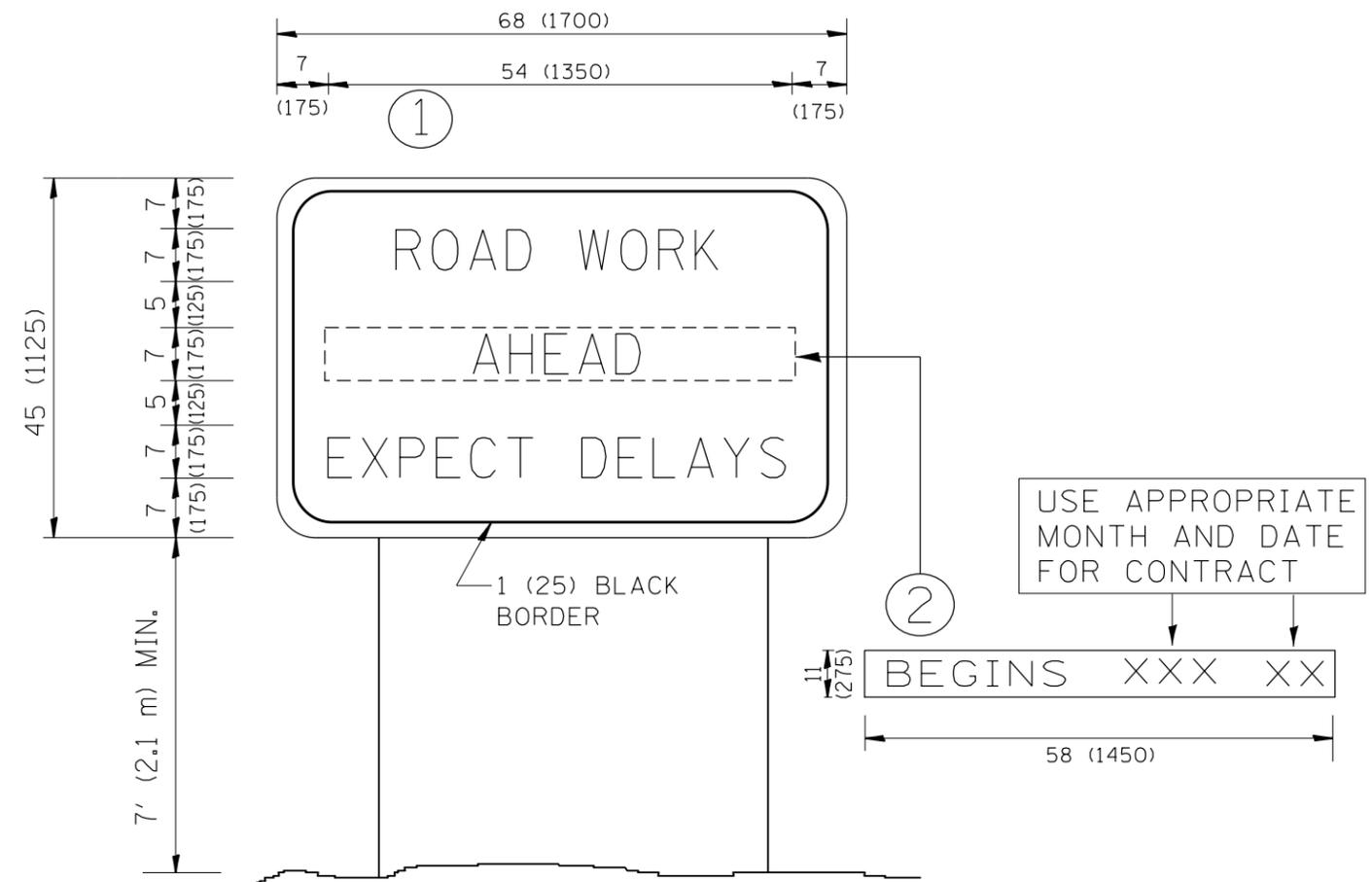
All dimensions are in inches (millimeters) unless otherwise shown.

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		CHECKED -	REVISED - E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	31
TC-16		CONTRACT NO. 62F65		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

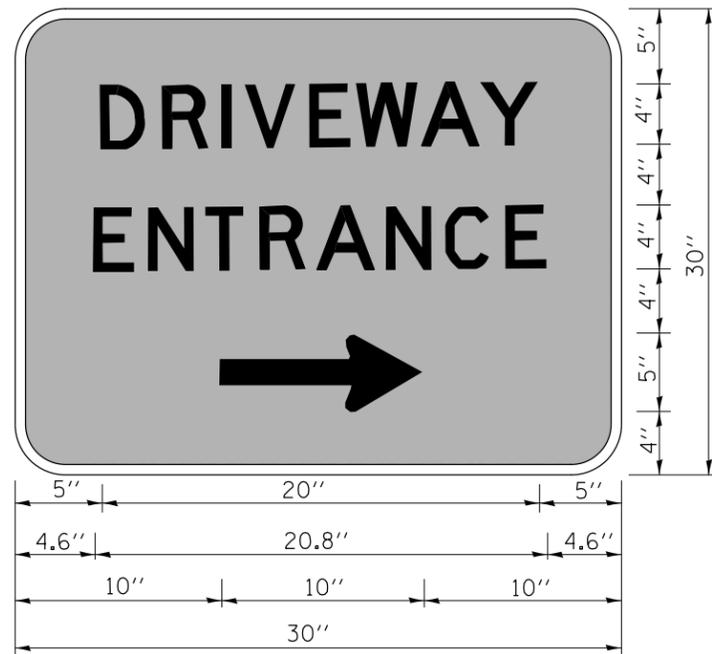
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		PLOT SCALE = 100.0000' / 1in.	REVISED - T. RAMMACHER 02-02-99
		PLOT DATE = 2/2/2018	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	32
TC-22		CONTRACT NO. 62F65		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED - C. JUCIUS 02-15-07
p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI026\Drawings\Design\DistStd.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED -
	PLOT DATE = 2/2/2018	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

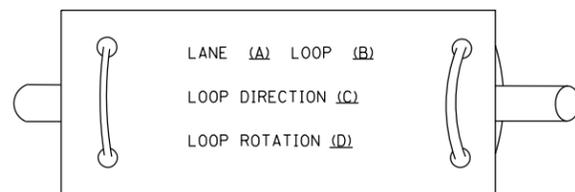
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	33
TC-26			CONTRACT NO. 62F65	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

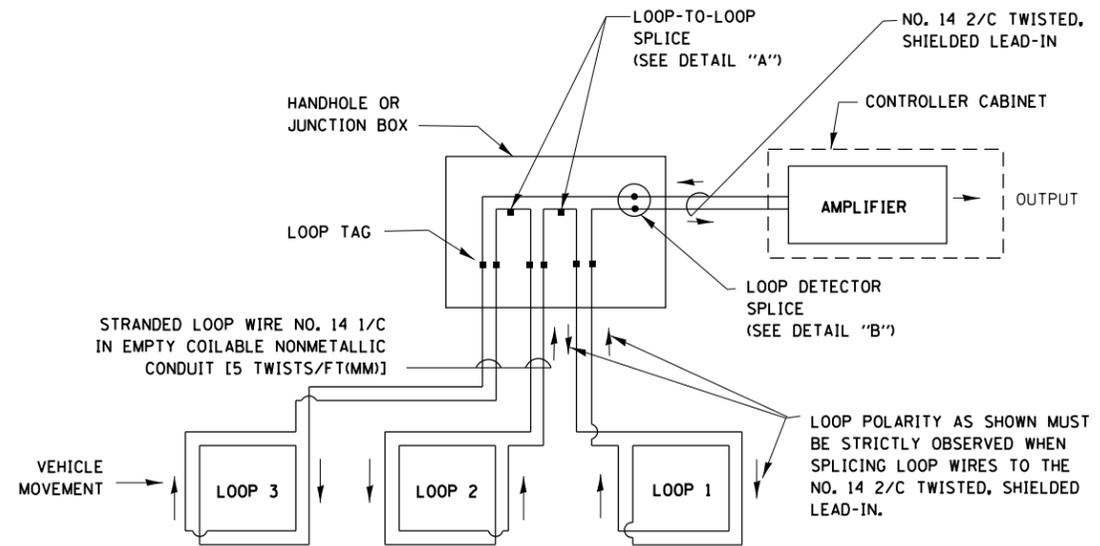
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

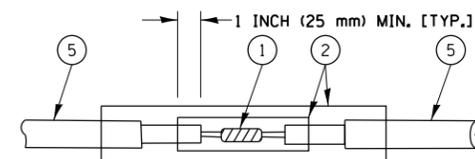


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

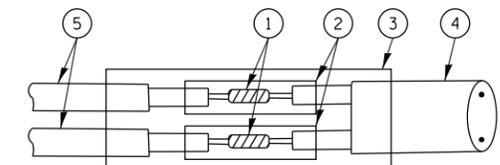


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

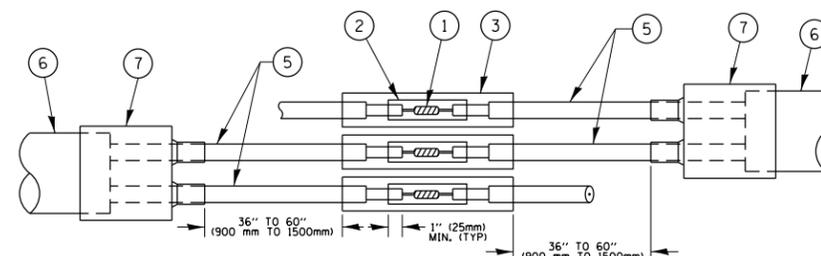


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

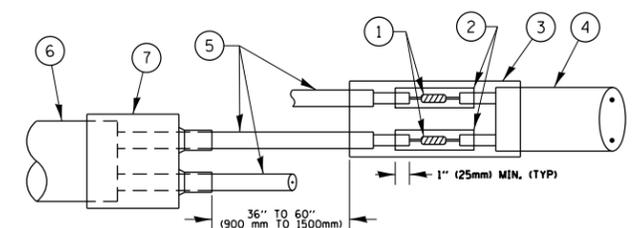


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED -
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Default	PLOT DATE = 2/2/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

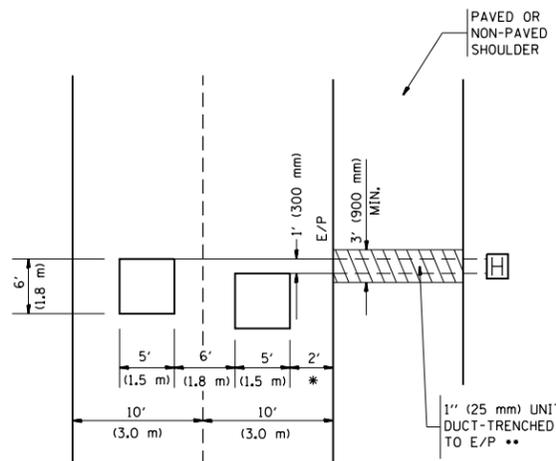
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
341	3035RS-1	COOK	36	34
TS-05		CONTRACT NO. 62F65		
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



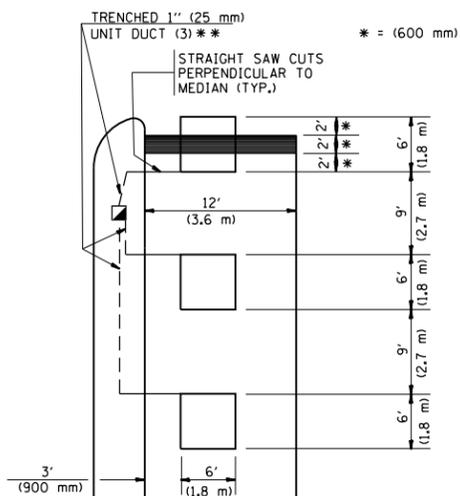
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



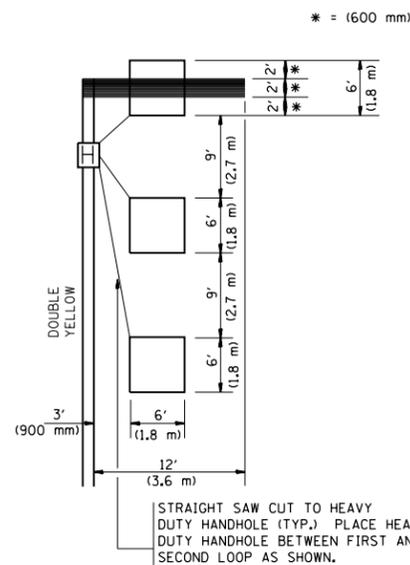
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

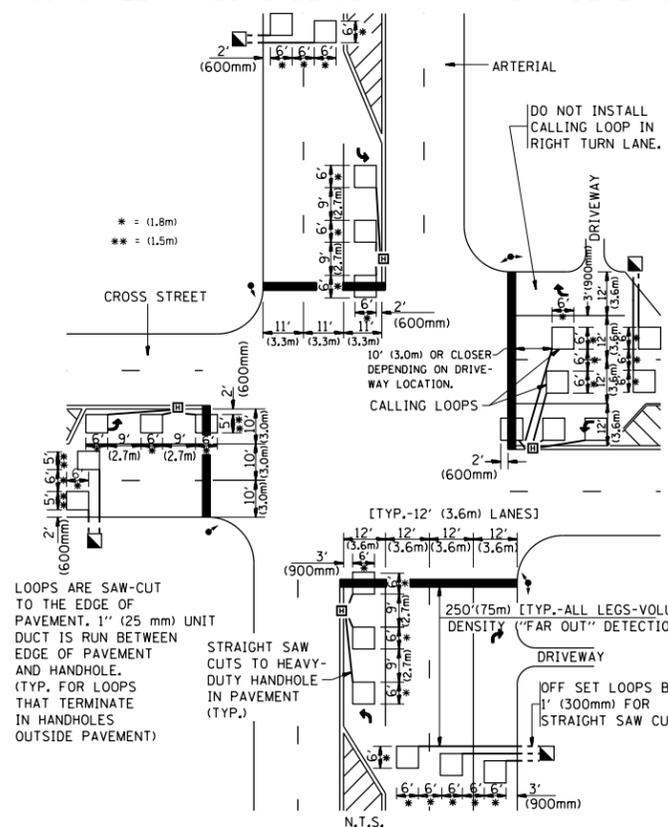
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

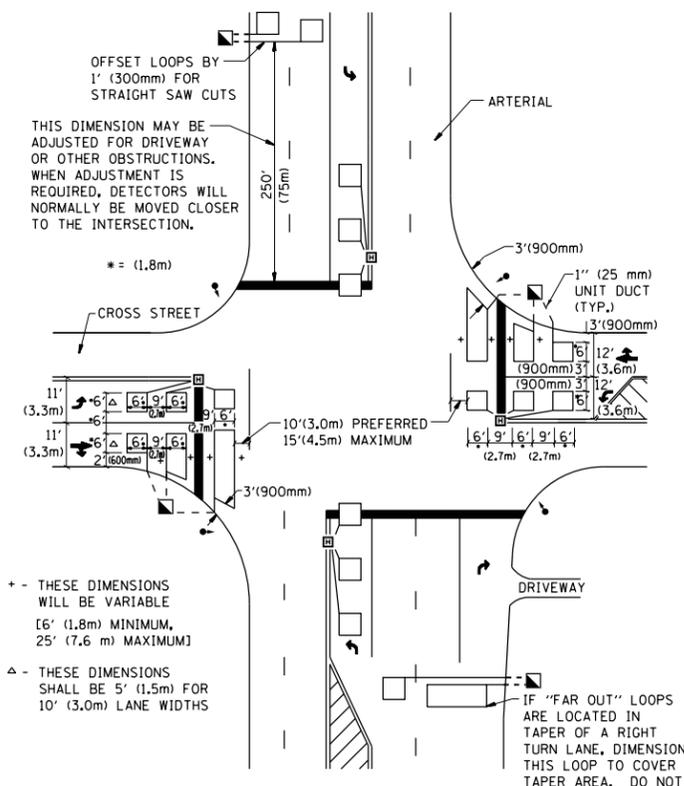
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

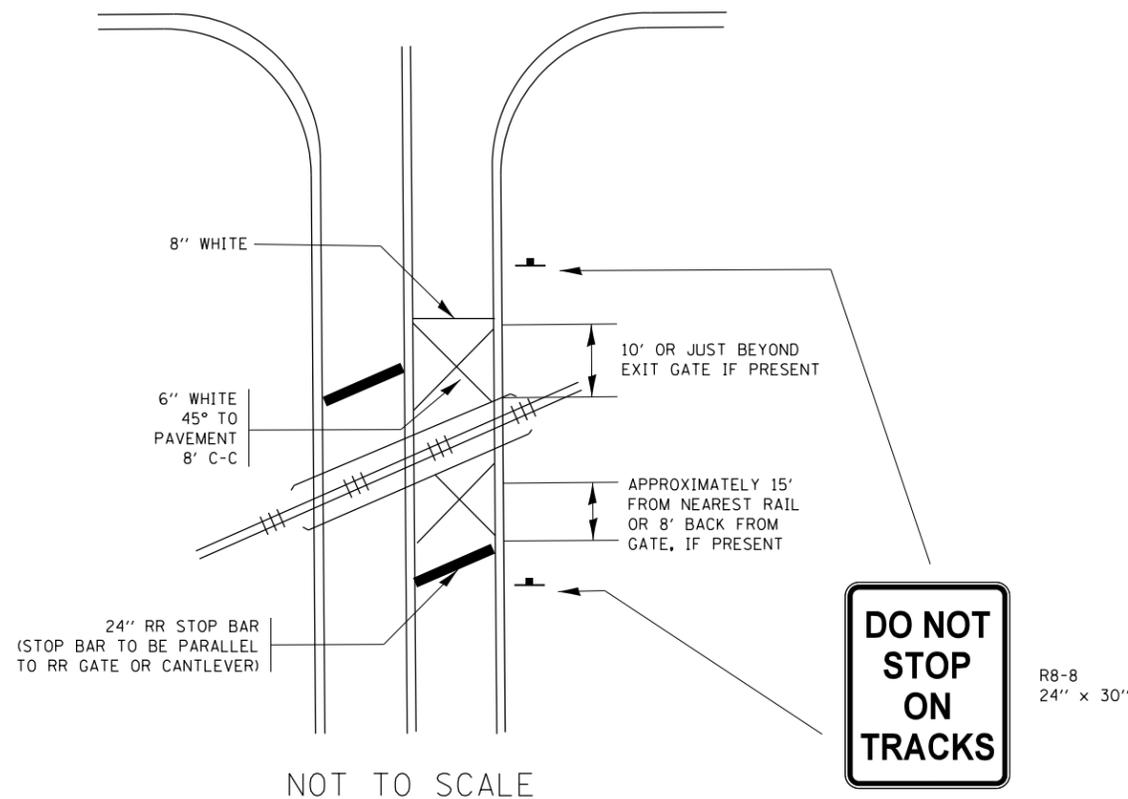
ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = alnematia	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING			F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\11inois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI026\Drawings\Design\DistStd.dgn	DRWN -	REVISED -	REVISED -					341	3035RS-1	COOK	36	35
PLOT SCALE = 100.0000' / 1"	CHECKED - R.K.F.	REVISED -	REVISED -					TS-07		CONTRACT NO. 62F65		
PLOT DATE = 2/2/2018	DATE -	REVISED -	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.					

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

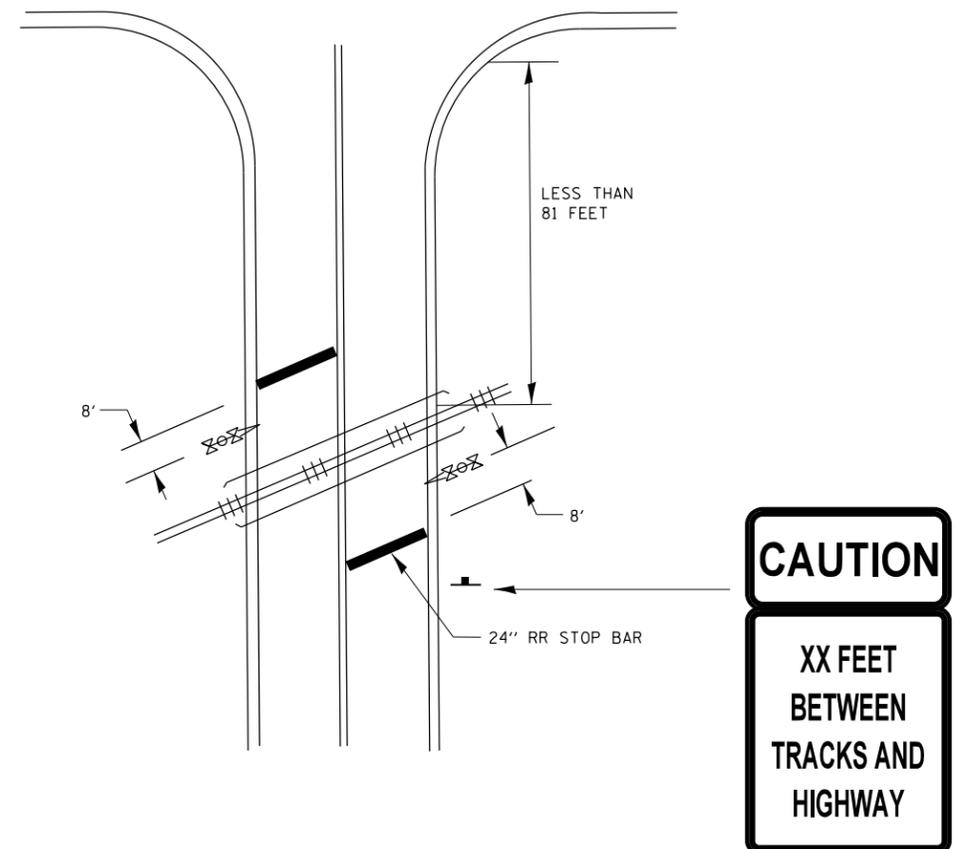
WITH SIGNALIZED INTERSECTION



NOTE:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON-SIGNALIZED INTERSECTION 81' OR LESS TO CLOSEST RAIL



NOTE:

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = alnemratia	DESIGNED -	REVISED - 02-25-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
			REVISED - 04-26-12			341	3035RS-1	COOK	36	36	
			REVISED - A.R. 07-11-16			TC-23		CONTRACT NO. 62F65			
Default	PLOT DATE = 2/2/2018	DATE -	REVISED -			SCALE: NONE	SHEET 2	OF 2 SHEETS	STA.	TO STA.	