

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	1
ILLINOIS CONTRACT NO. 62F62				

**PROPOSED  
HIGHWAY PLANS**

F.A.U. 1548 /79th STREET  
IL 50 (CICERO AVE.) TO LAWDALE AVE.  
SECTION: 3290RS-1  
PROJECT: NHPP-GQPR(171)  
RESURFACING (3P) AND PEDESTRIAN RAMPS  
COOK COUNTY

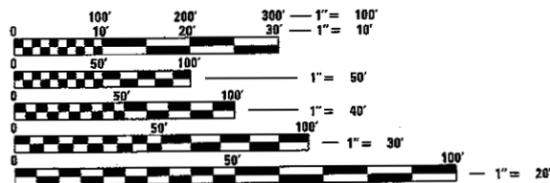
C-91-032-18

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE CITY  
OF CHICAGO

TRAFFIC DATA:

79TH STREET  
2016 - ADT = 31,400  
SPEED LIMIT = 35 MPH

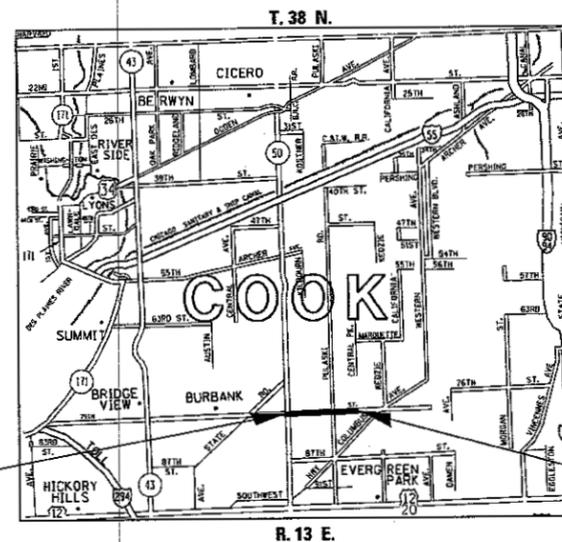


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

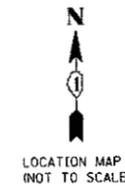
CONTRACT NO. 62F62



PROJECT BEGINS  
STA. 15 + 49

PROJECT ENDS  
STA. 89 + 61

GROSS LENGTH = NET LENGTH = 7412 FT. = 1.40 MILES



LOCATION MAP  
(NOT TO SCALE)

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 24, 2018

*Anthony J. Quigley* REGIONAL ENGINEER  
Mar 23, 2018

*Paul P. Chy* ENGINEER OF DESIGN AND ENVIRONMENT  
Mar 23, 2018

DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS



URBAN

URBAN

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
20200100	EARTH EXCAVATION	CU YD	30	30			
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	615	615			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	8	8			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	8	8			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	8	8			
25200110	SODDING, SALT TOLERANT	SO YD	615	615			
25200200	SUPPLEMENTAL WATERING	UNIT	6.2	6.2			
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	97	97			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	42302	42302			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	94	94			
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	2586	2586			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	601	601			
40603565	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70	TON	6142	6142			
42001300	PROTECTIVE COAT	SO YD	963	963			
	* SPECIALTY ITEMS						

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005			
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	39	39			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	1932	1932			
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO FT	62670	62670			
44000100	PAVEMENT REMOVAL	SO YD	61	61			
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	39	39			
44000600	SIDEWALK REMOVAL	SO FT	1942	1942			
44003100	MEDIAN REMOVAL	SO FT	400	400			
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	151	151			
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	88	88			
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	633	633			
60255500	MANHOLES TO BE ADJUSTED	EACH	2	2			
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1			
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	15	15			
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	1	1			
60619200	CONCRETE MEDIAN, TYPE SB-6.06	SO FT	874	874			
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	2	2			
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	2	2			

13

14

FILE NAME =	USER NAME = logny	DESIGNED -	REVISED -
OFFICE = District 1 Projects	PROJECT = 0102220-0-0000-Design	CHECKED -	REVISED -
PLOT SCALE = 1/8" = 1' / 16"	DATE = 2/1/2018	CHECKED -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES  
79TH STREET - IL 50 (CICERO AVE.) TO LAWDALE AVE.  
SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.L. RTE. 1548	SECTION 3290RS-1	COUNTY COOK	TOTAL SHEETS 46	SHEET NO. 3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
CONTRACT NO. 62F62				

URBAN

URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FED 20% STATE 0005					
63200310	GUARDRAIL REMOVAL	FOOT	77	77					70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	88	88					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	30	30					70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	4111	4111					
* 66900450	SPECIAL WASTE PLANS AND REPORTS	LSUM	1	1					70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	4770	4770					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	813	813					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17354	17354					
67100100	MOBILIZATION	LSUM	1	1					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2842	2842					
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1					* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	88	88					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1					* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	4111	4111					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1					* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	465	465					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	19080	19080					X0320050	CONSTRUCTION LAYOUT (SPECIAL)	LSUM	1	1					
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	6365	6365					* X4240800	DETECTABLE WARNINGS (SPECIAL)	SO FT	280	280					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	813	813					X5537800	STORM SEWERS TO BE CLEANED, 12"	FOOT	1700	1700					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	17354	17354					X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	75	75					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2496	2496					* 13	* SPECIALTY ITEMS								

M

FILE NAME =	USER NAME =	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>				<b>SUMMARY OF QUANTITIES</b> <b>79TH STREET - IL 50 (CICERO AVE.) TO LAWDALE AVE.</b>				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		CHECKED -	REVISED -									1548	3290RS-1	COOK	46	4	
		DATE -	REVISED -									SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
												CONTRACT NO. 62F62					

Revision



**NOTES:**

1. THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING BETWEEN STA. 15+49 AND STA. 89+61
2. THE CONTRACTOR SHALL MILL AROUND CONCRETE BUS PADS BETWEEN STA. 15+49 AND STA. 89+61

HOT-MIX ASPHALT MIXTURE REQUIREMENTS			
MIXTURE USES	MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING	POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, (IL 9.5 mm); 1 1/2"	4% @ 70 GYR.	QCP
	POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50; 3/4"	3.5% @ 50 GYR.	QCP
PATCHING	CLASS D PATCHES (HMA BINDER IL-19.0 mm)	4% @ 70 GYR	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)			

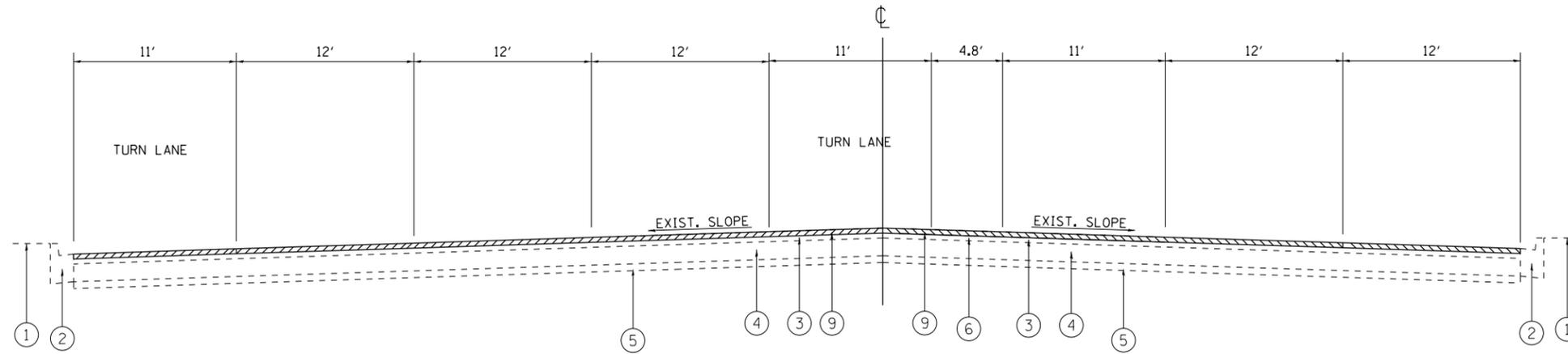
NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

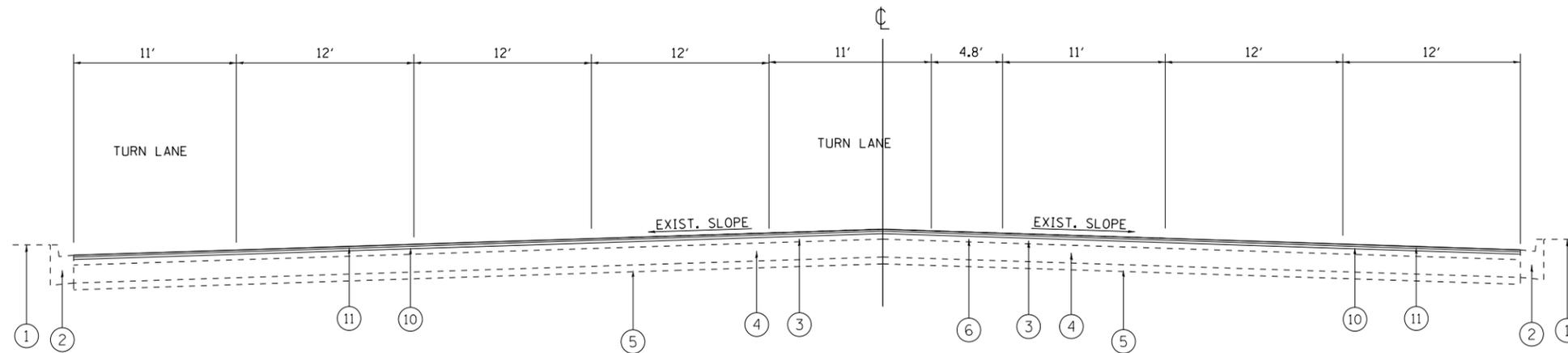
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p:\IL\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI02218\Drawings\Design\DI02218-shr-typical.dgn		CHECKED -	REVISED -			1548	329ORS-1	COOK	46	6	
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -			CONTRACT NO. 62F62			ILLINOIS FED. AID PROJECT		
	PLOT DATE = 1/26/2018	DATE -	REVISED -			SCALE:	SHEET	OF	SHEETS	STA.	TO



**79TH STREET**  
**EXISTING TYPICAL SECTION**  
 STA. 15+49 TO STA. 17+42

**LEGEND:**

- ① EXISTING SIDEWALK
- ② EXISTING COMB. CONC. CURB AND GUTTER, B6.12
- ③ EXISTING HMA SURFACE COURSE, 5" (±)
- ④ EXISTING PCC BASE COURSE, 9" (±)
- ⑤ EXISTING SUB-BASE
- ⑥ EXISTING HMA SURFACE MEDIAN 4"
- ⑦ EXISTING LANDSCAPED MEDIAN
- ⑧ EXISTING CONCRETE MEDIAN
- ⑨ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1 3/4"

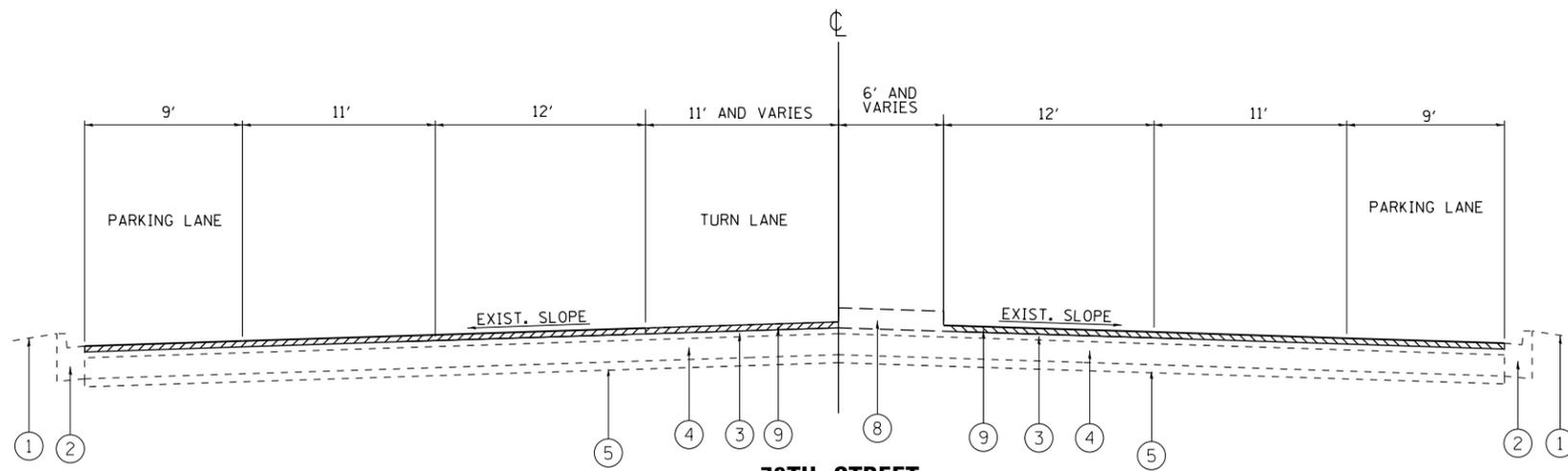


**79TH STREET**  
**PROPOSED TYPICAL SECTION**  
 STA. 15+49 TO STA. 17+42

**NOTES:**

1. THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING BETWEEN STA. 15+49 AND STA. 17+42
2. THE CONTRACTOR SHALL MILL AROUND CONCRETE BUS PADS BETWEEN STA. 15+49 AND STA. 17+42

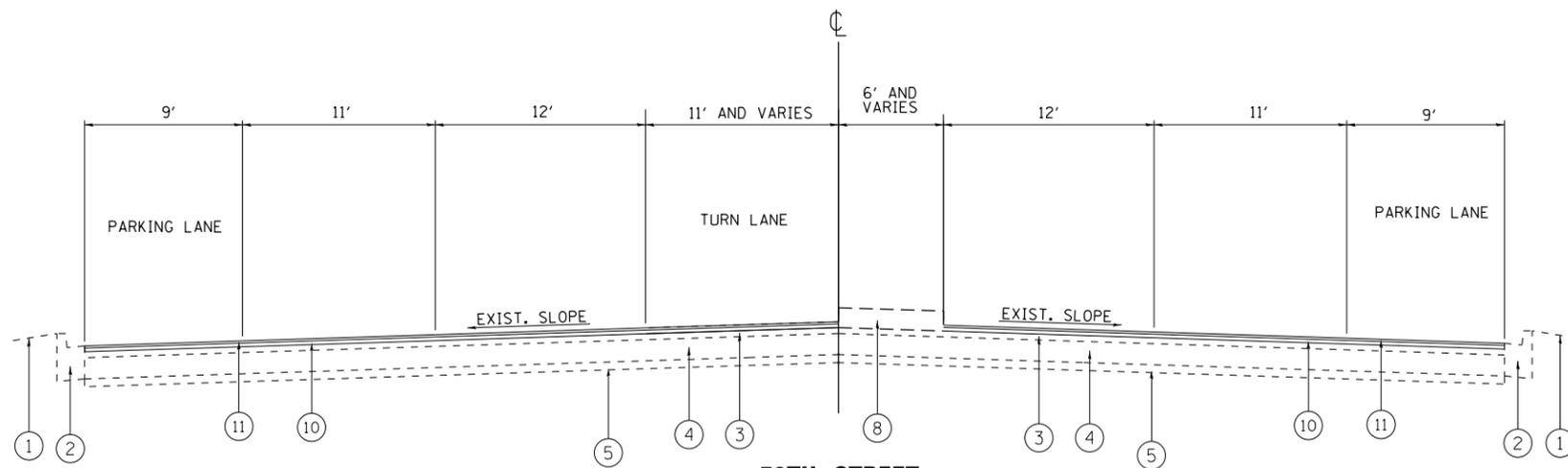
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	PLOT DATE = 1/26/2018	DATE -	REVISIONS			SCALE:	SHEET	OF	SHEETS	STA.	TO



**79TH STREET**

**EXISTING TYPICAL SECTION**

STA. 17+42 TO STA. 20+37  
 STA. 21+44 TO STA. 28+42  
 STA. 41+52 TO STA. 44+47  
 STA. 52+08 TO STA. 55+08  
 STA. 61+28 TO STA. 62+96  
 STA. 67+00 TO STA. 72+05  
 STA. 80+79 TO STA. 82+69  
 STA. 87+48 TO STA. 88+66



**79TH STREET**

**PROPOSED TYPICAL SECTION**

STA. 17+42 TO STA. 20+37  
 STA. 21+44 TO STA. 28+42  
 STA. 41+52 TO STA. 44+47  
 STA. 52+08 TO STA. 55+08  
 STA. 61+28 TO STA. 62+96  
 STA. 67+00 TO STA. 72+05  
 STA. 80+79 TO STA. 82+69  
 STA. 87+48 TO STA. 88+66

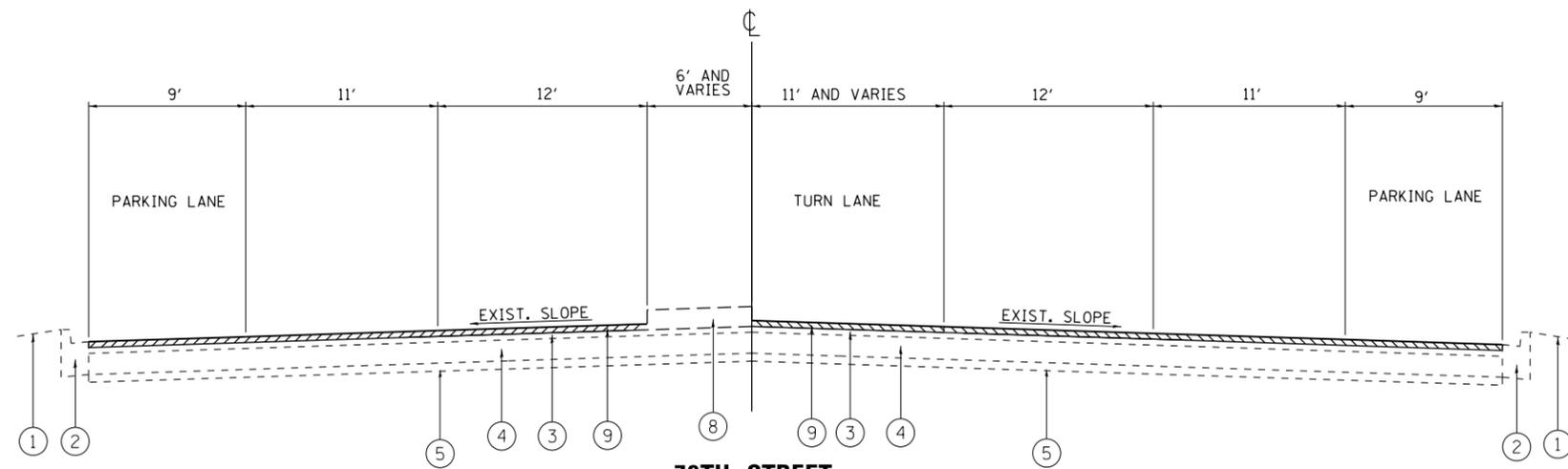
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- ⑨ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1 3/4"

**NOTES:**

1. THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING BETWEEN STA. 17+42 AND STA. 88+66
2. THE CONTRACTOR SHALL MILL AROUND CONCRETE BUS PADS BETWEEN STA. 17+42 AND STA. 88+66

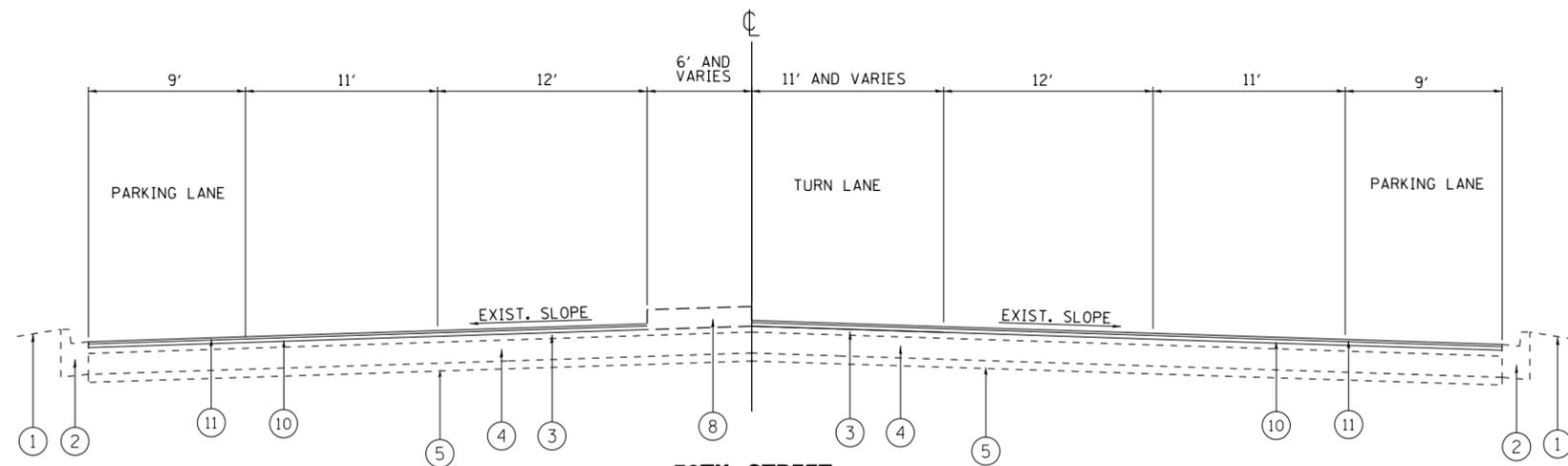
FILE NAME =	USER NAME = lacymr	DESIGNED -	REVISED 3/16/2018 ML	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS 79TH STREET — IL 50 (CICERO AVENUE) TO LAWNSDALE AVENUE</b>	F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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Default	PLLOT DATE = 3/16/2018	DATE -	REVISED -			CONTRACT NO. 62F62					
						SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT		



**79TH STREET**

**EXISTING TYPICAL SECTION**

STA. 20+37 TO STA. 21+44  
 STA. 38+92 TO STA. 41+52  
 STA. 63+93 TO STA. 67+00  
 STA. 75+12 TO STA. 78+61  
 STA. 82+69 TO STA. 84+23  
 STA. 88+66 TO STA. 89+61



**79TH STREET**

**PROPOSED TYPICAL SECTION**

STA. 20+37 TO STA. 21+44  
 STA. 38+92 TO STA. 41+52  
 STA. 63+93 TO STA. 67+00  
 STA. 75+12 TO STA. 78+61  
 STA. 82+69 TO STA. 84+23  
 STA. 88+66 TO STA. 89+61

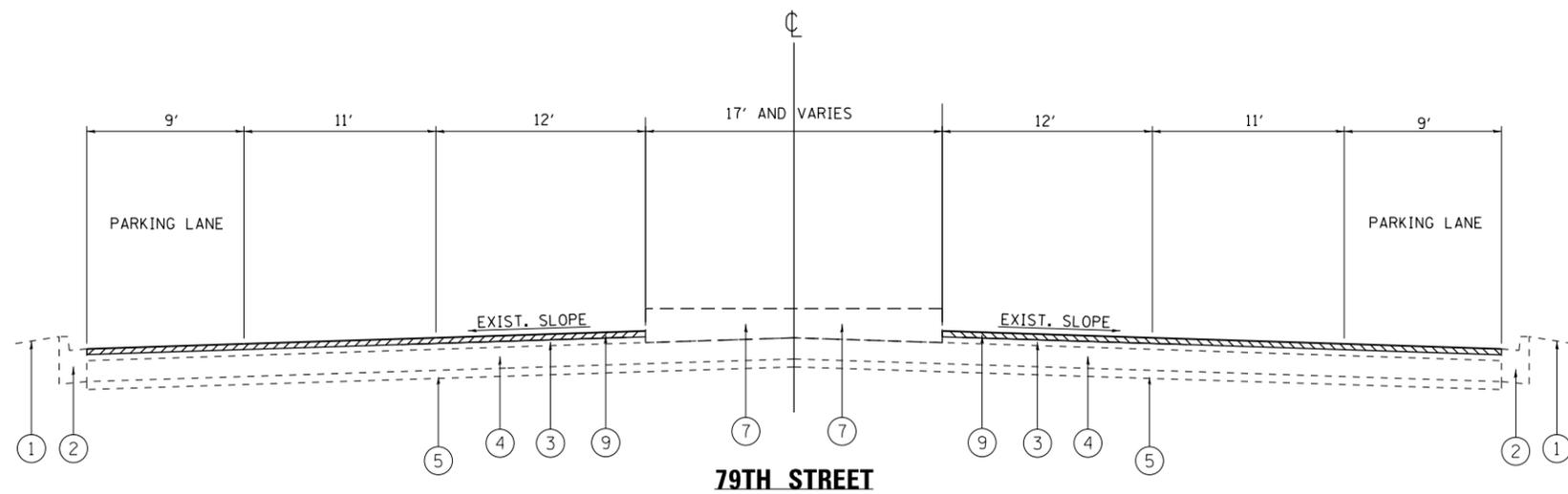
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- ① EXISTING SIDEWALK
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- ③ EXISTING HMA SURFACE COURSE, 5" (±)
- ④ EXISTING PCC BASE COURSE, 9" (±)
- ⑤ EXISTING SUB-BASE
- ⑥ EXISTING HMA SURFACE MEDIAN 4"
- ⑦ EXISTING LANDSCAPED MEDIAN
- ⑧ EXISTING CONCRETE MEDIAN
- ⑨ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1 3/4"

**NOTES:**

1. THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING BETWEEN STA. 20+37 AND STA. 89+61
2. THE CONTRACTOR SHALL MILL AROUND CONCRETE BUS PADS BETWEEN STA. 20+37 AND STA. 89+61

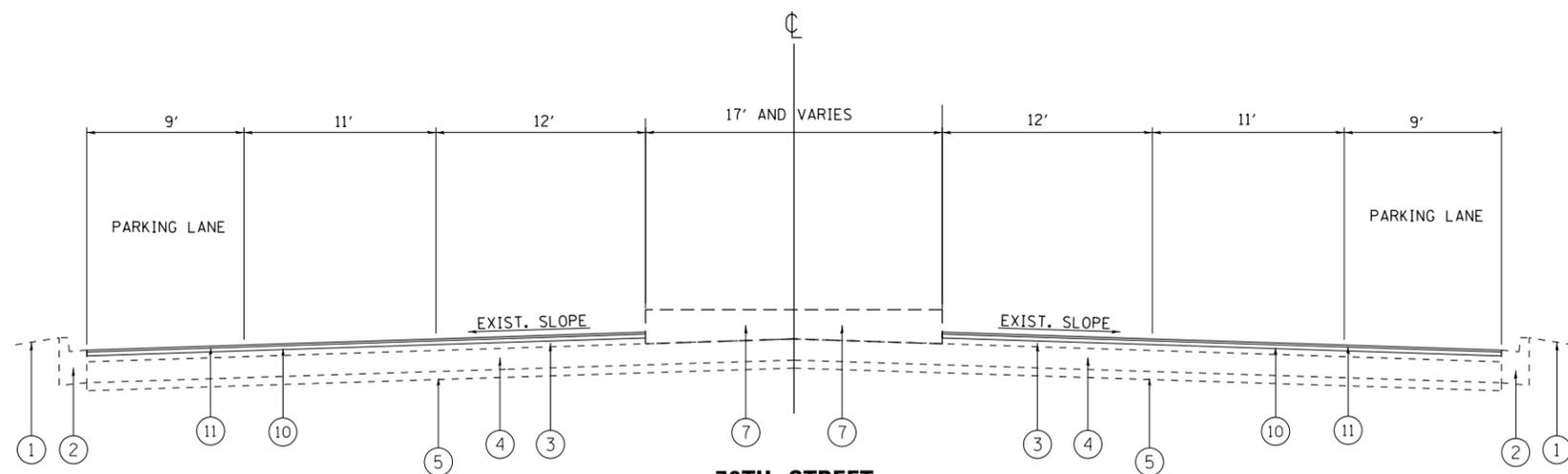
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pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI02218\Drawings\Design\DI02218-shr-typical.dgn	DRWN	REVISION	REVISION			1548	329ORS-1	COOK	46	9	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISION			CONTRACT NO. 62F62					
	PLOT DATE = 3/16/2018	DATE -	REVISION			ILLINOIS FED. AID PROJECT					



**79TH STREET**

**EXISTING TYPICAL SECTION**

STA. 28+42 TO STA. 38+92  
 STA. 44+47 TO STA. 52+08  
 STA. 55+08 TO STA. 61+28  
 STA. 62+96 TO STA. 63+93  
 STA. 72+05 TO STA. 75+12  
 STA. 78+61 TO STA. 80+79  
 STA. 84+23 TO STA. 87+48



**79TH STREET**

**PROPOSED TYPICAL SECTION**

STA. 28+42 TO STA. 38+92  
 STA. 44+47 TO STA. 52+08  
 STA. 55+08 TO STA. 61+28  
 STA. 62+96 TO STA. 63+93  
 STA. 72+05 TO STA. 75+12  
 STA. 78+61 TO STA. 80+79  
 STA. 84+23 TO STA. 87+48

**LEGEND:**

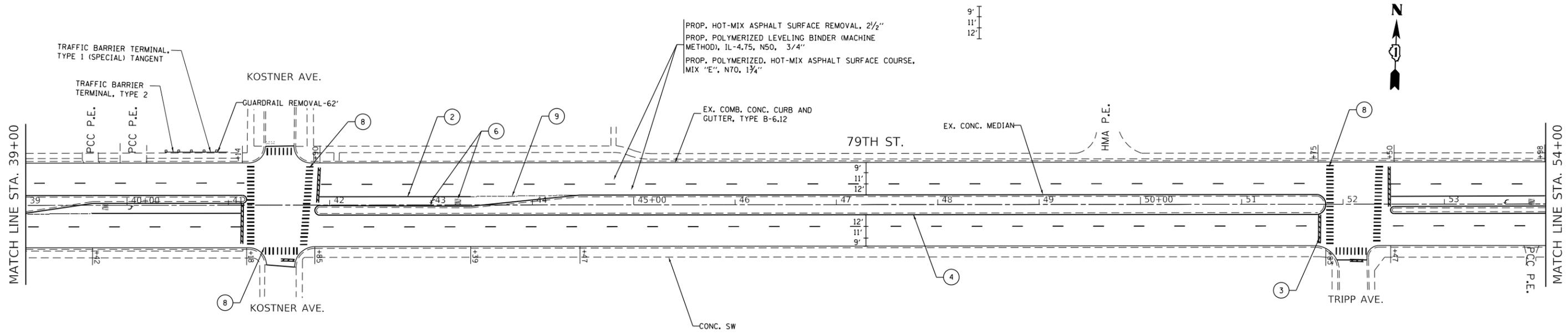
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- ② EXISTING COMB. CONC. CURB AND GUTTER, B6.12
- ③ EXISTING HMA SURFACE COURSE, 5" (±)
- ④ EXISTING PCC BASE COURSE, 9" (±)
- ⑤ EXISTING SUB-BASE
- ⑥ EXISTING HMA SURFACE MEDIAN 4"
- ⑦ EXISTING LANDSCAPED MEDIAN
- ⑧ EXISTING CONCRETE MEDIAN
- ⑨ PROPOSED HMA SURFACE REMOVAL, 2 1/2"
- ⑩ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑪ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "E", N70, 1 3/4"

**NOTES:**

1. THE CONTRACTOR SHALL MILL FIRST BEFORE PATCHING BETWEEN STA. 28+42 AND STA. 87+48
2. THE CONTRACTOR SHALL MILL AROUND CONCRETE BUS PADS BETWEEN STA. 28+42 AND STA. 87+48

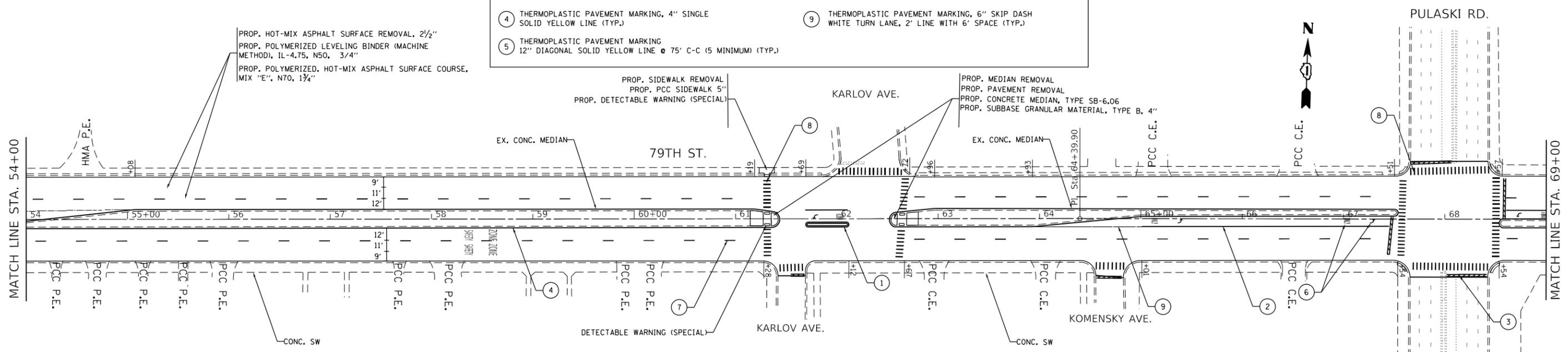
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pw:\IL\084EBIDINTEG.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\ID02218\Design\ID02218-sh-typical.dgn	DRW	REVISIONS	REVISIONS			1548	3290RS-1	COOK	46	10	
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISIONS			CONTRACT NO. 62F62					
	PLOT DATE = 3/16/2018	DATE -	REVISIONS			SCALE:	SHEET	OF	SHEETS	STA.	TO





**PAVEMENT MARKING LEGEND**

- |  |   |
|--|---|
| ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 11" C-C (TYP.)      | ⑥ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS, SOLID WHITE (TYP.)                       |
| ② THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE TURN LANE (TYP.)                            | ⑦ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 10' DASH - 30' SKIP (TYP.)      |
| ③ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)                            | ⑧ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE LONGITUDINAL BAR @ 4' C-C (6' MINIMUM) (TYP.) |
| ④ THERMOPLASTIC PAVEMENT MARKING, 4" SINGLE SOLID YELLOW LINE (TYP.)                         | ⑨ THERMOPLASTIC PAVEMENT MARKING, 6" SKIP DASH WHITE TURN LANE, 2' LINE WITH 6' SPACE (TYP.)    |
| ⑤ THERMOPLASTIC PAVEMENT MARKING 12" DIAGONAL SOLID YELLOW LINE @ 75' C-C (5 MINIMUM) (TYP.) |   |



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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

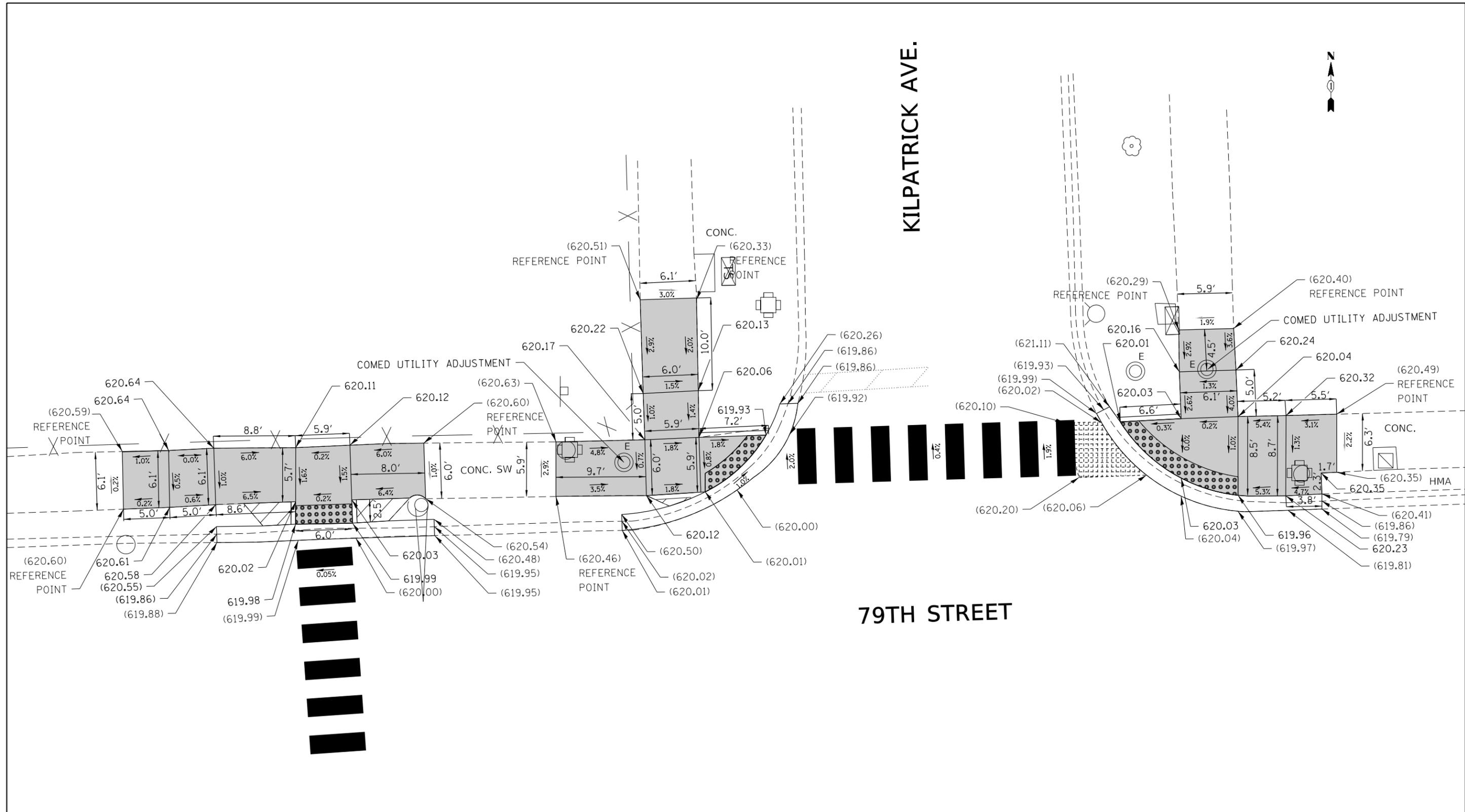
**EXISTING AND PROPOSED ROADWAY PLAN  
79TH ST. -- IL 50 (CICERO AVENUE) TO LAWDALE AVENUE**

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	DUPAGE	46	12
CONTRACT NO. 62F62				
ILLINOIS FED. AID PROJECT				







KILPATRICK AVE.

79TH STREET

**LEGEND**

xx.xx' EXISTING LENGTH  
 ( ) EXISTING ELEVATION/SLOPE

PROPOSED SIDEWALK  
 PROPOSED SIDE CURB  
 TOP OF PROPOSED SIDE CURB

CLASS D PATCH  
 DETECTABLE WARNINGS  
 SIDEWALK REMOVAL  
 REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 621.32  
 BENCHMARK: "□" CUT ON N. CONCRETE BASE OF T.S.  
 LOCATION: SW CORNER OF 79TH. ST. AND KILPATRICK AVE.

FILE NAME =	USER NAME = Bilgramis	DESIGNED -	REVISED -
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

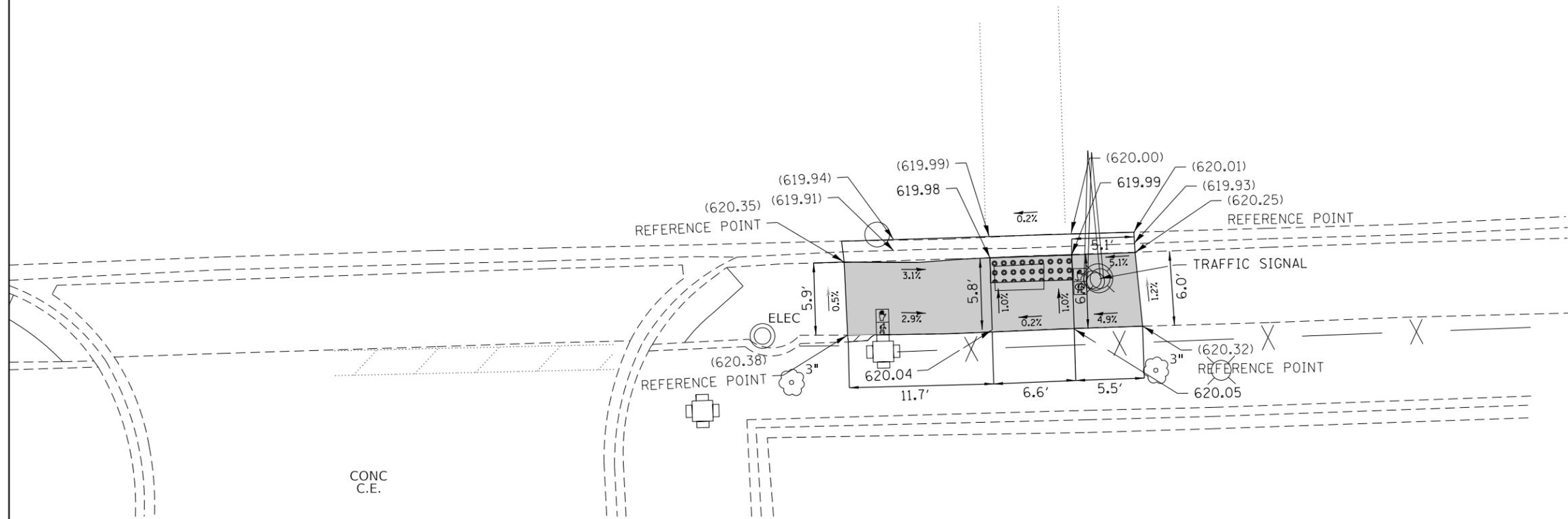
**SIDEWALK DETAIL PLAN  
 79TH STREET AT KILPATRICK AVE. (WEST)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	15
CONTRACT NO. 62F62				
ILLINOIS FED. AID PROJECT				



# 79TH STREET



## LEGEND

xx.xx' EXISTING LENGTH  
( ) EXISTING ELEVATION/SLOPE

 PROPOSED SIDEWALK  
 PROPOSED SIDE CURB  
SC TOP OF PROPOSED SIDE CURB

 CLASS D PATCH  
 DETECTABLE WARNINGS  
 SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 621.32  
BENCHMARK: "□" CUT ON N. CONCRETE BASE OF T.S.  
LOCATION: SW CORNER OF 79TH. ST. AND KILPATRICK AVE.

FILE NAME =	USER NAME = Bilgramisa	DESIGNED -	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

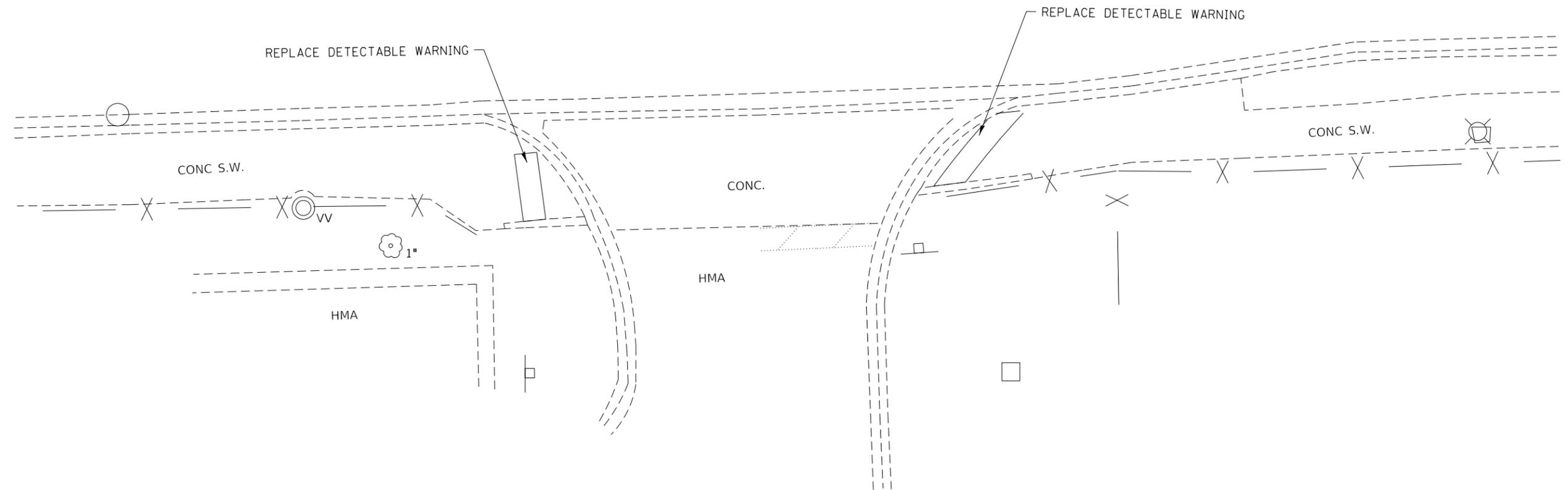
**SIDEWALK DETAIL PLAN  
79TH STREET SOUTH OF KILPATRICK AVE. (WEST)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	16
CONTRACT NO. 62F62				
ILLINOIS FED. AID PROJECT				



# 79TH STREET



## LEGEND

xx.xx' EXISTING LENGTH

( ) EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 620.83

BENCHMARK: "X" CUT ON NW BOLT OF LIGHT POLE

LOCATION: SOUTH SIDE OF 79TH STREET +/- 100' EAST OF KNOX AVE.

FILE NAME =	USER NAME = Bilgramis	DESIGNED -	REVISED -
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

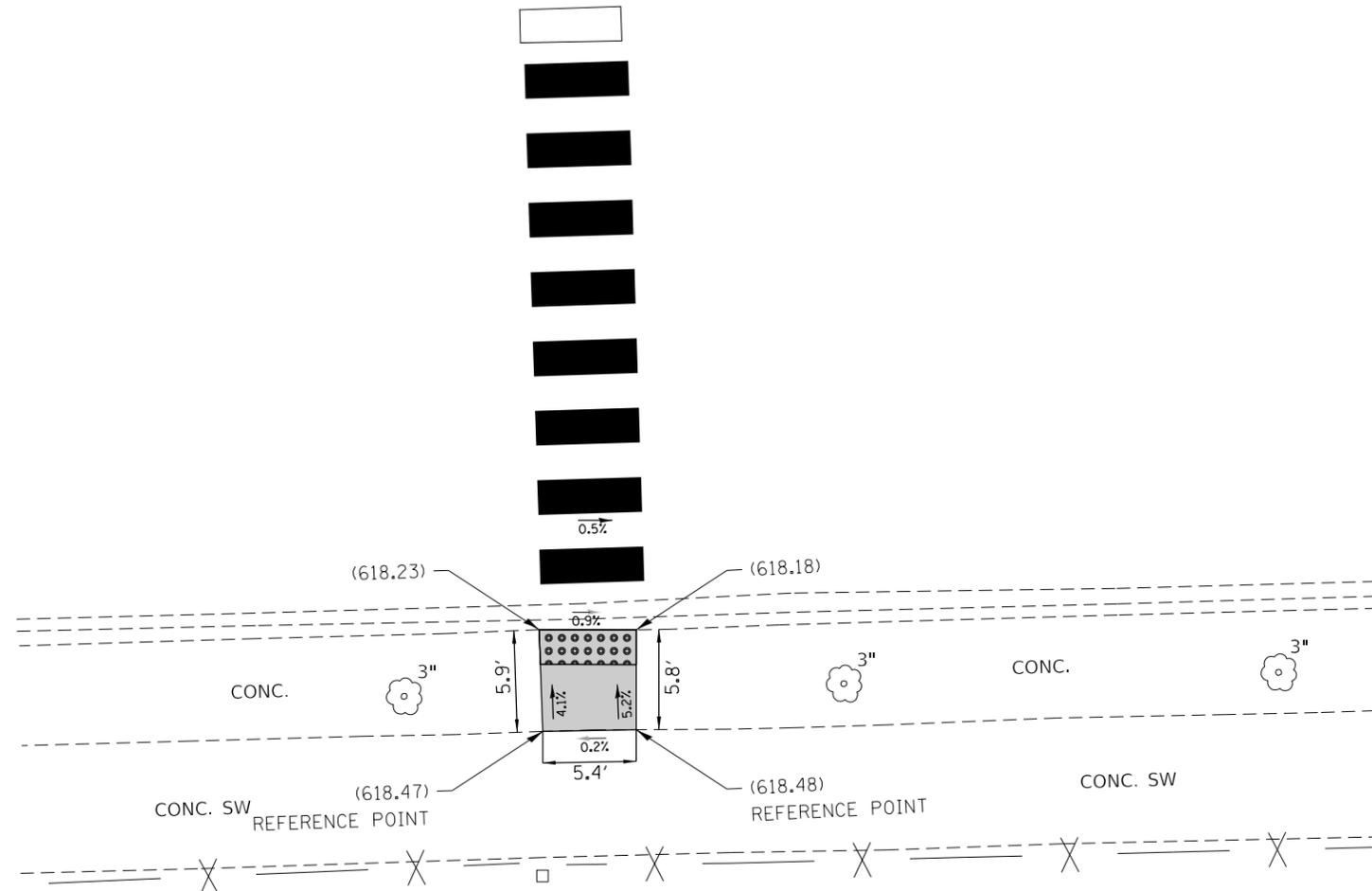
**SIDEWALK DETAIL PLAN  
79TH STREET SOUTH OF KNOX AVE.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	17
CONTRACT NO. 62F62				
ILLINOIS FED. AID PROJECT				



# 79TH STREET



## LEGEND

xx.xx' EXISTING LENGTH

( ) EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 620.18

BENCHMARK: "□" ON N. CONCRETE BASE OF LIGHTING CONTROL BOX

LOCATION: SE CORNER OF 79TH. ST. AND SPRINGFIELD AVE. (EAST)

FILE NAME =	USER NAME = Bilgramisa	DESIGNED -	REVISED -
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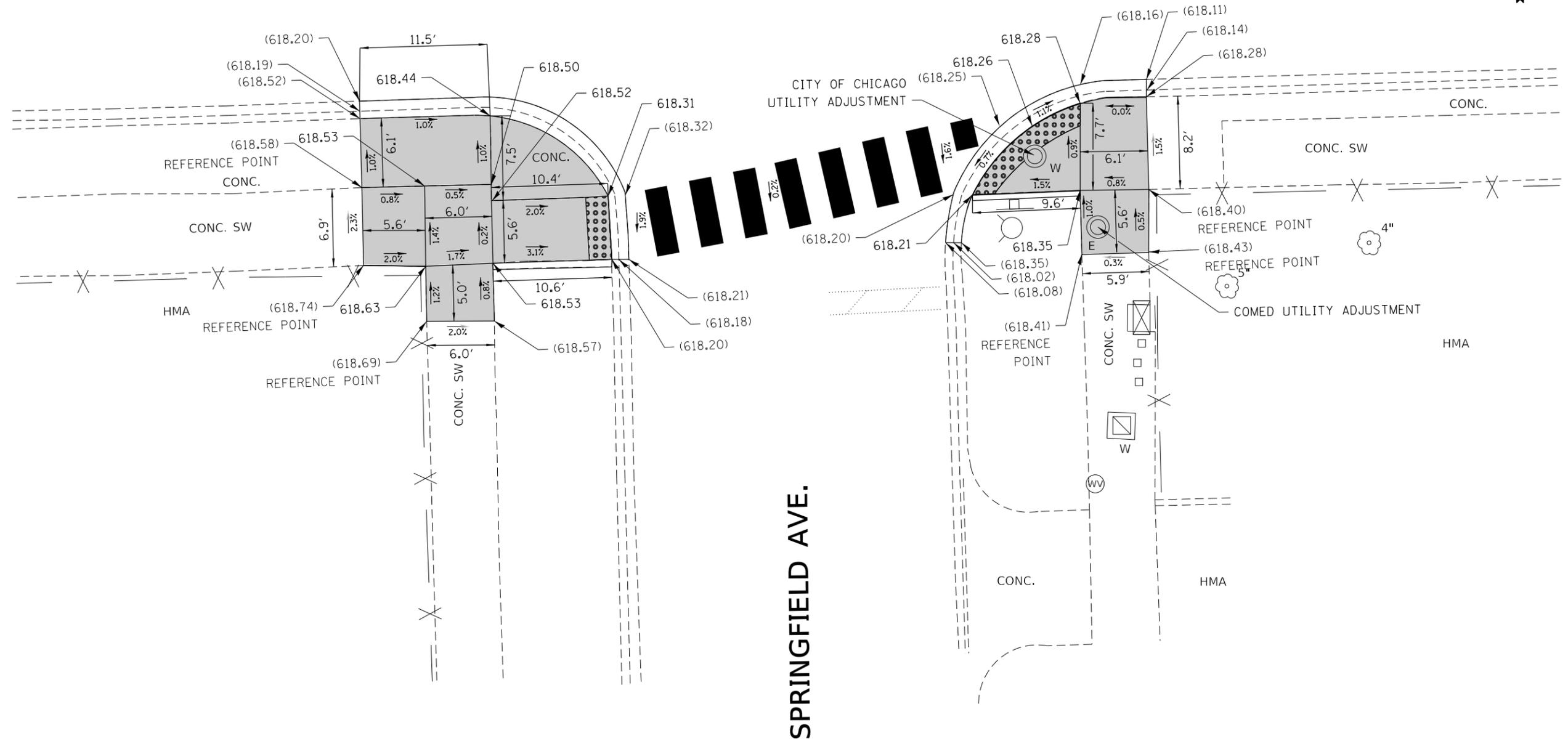
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN  
79TH STREET AT SPRINGFIELD AVE. (WEST)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	19
CONTRACT NO. 62F62				
ILLINOIS FED. AID PROJECT				

# 79TH STREET



SPRINGFIELD AVE.

## LEGEND

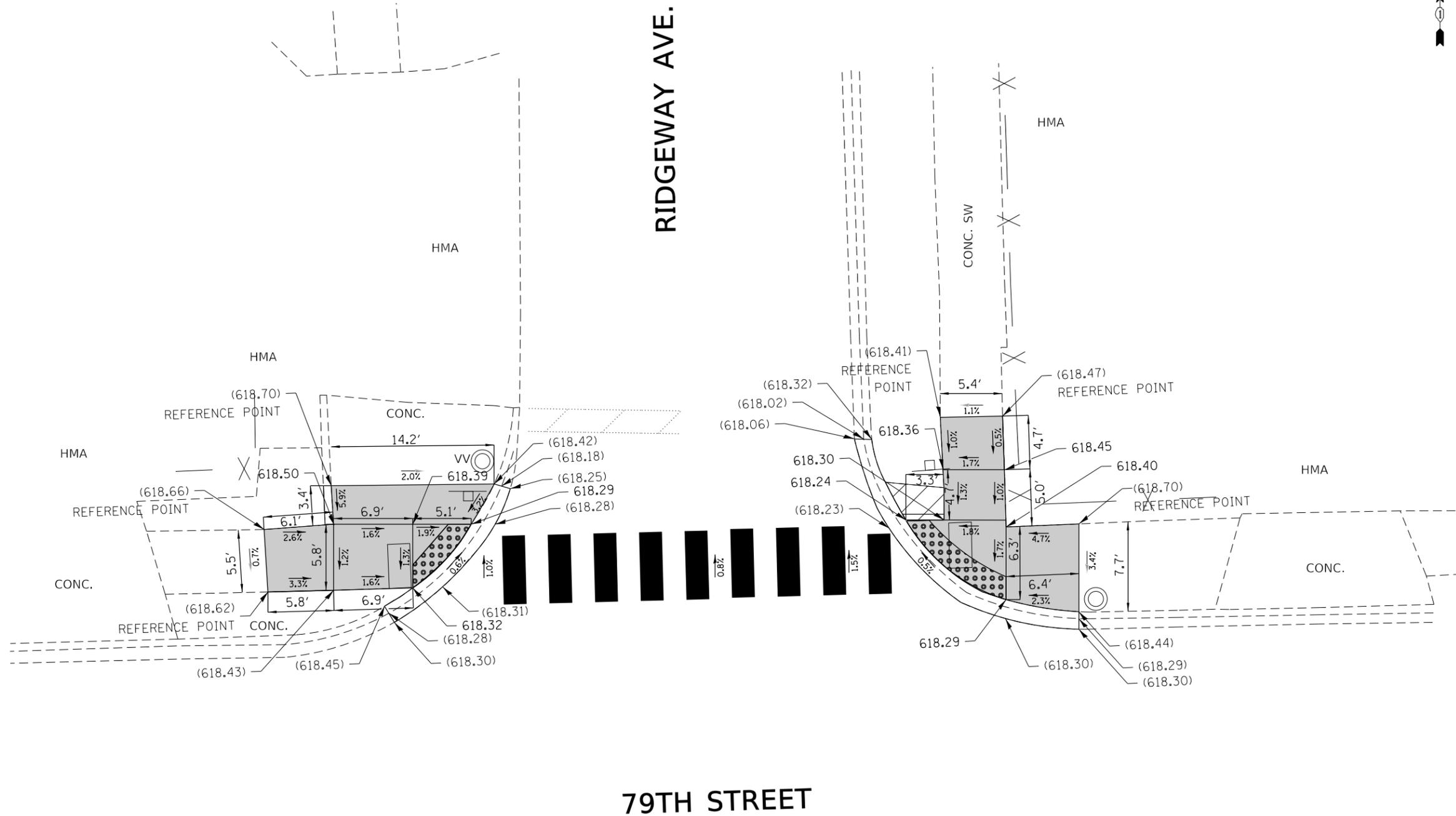
xx.xx' EXISTING LENGTH  
( ) EXISTING ELEVATION/SLOPE

 PROPOSED SIDEWALK  
 PROPOSED SIDE CURB  
 SC TOP OF PROPOSED SIDE CURB

 CLASS D PATCH  
 DETECTABLE WARNINGS  
 SIDEWALK REMOVAL  
 REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 620.18  
 BENCHMARK: "□" ON N. CONCRETE BASE OF LIGHTING CONTROL BOX  
 LOCATION: SE CORNER OF 79TH. ST. AND SPRINGFIELD AVE. (EAST)

FILE NAME =	USER NAME = Bilgomsa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SIDEWALK DETAIL PLAN 79TH STREET AT SPRINGFIELD AVE. (EAST)</b>	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLotted	DATE	DATE			CONTRACT NO. 62F62				
	PLotted	DATE	DATE			ILLINOIS FED. AID PROJECT				



**LEGEND**

xx.xx' EXISTING LENGTH  
 ( ) EXISTING ELEVATION/SLOPE

PROPOSED SIDEWALK  
 PROPOSED SIDE CURB  
 SC TOP OF PROPOSED SIDE CURB

CLASS D PATCH  
 DETECTABLE WARNINGS  
 SIDEWALK REMOVAL  
 REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 618.98  
 BENCHMARK: "□" ON N. CONCRETE BASE OF T.S. W/ LIGHT & ARM  
 LOCATION: NW CORNER OF 79TH. ST. AND LAWDALE AVE. (SOUTH)

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	PLOT DATE = 1/26/2018	DATE -	REVISED -

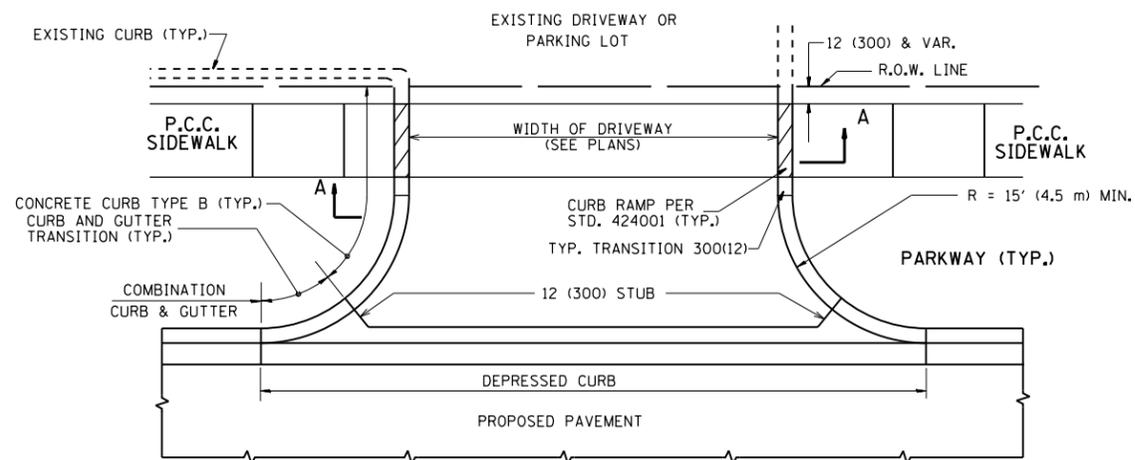
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
 79TH STREET AT RIDGEWAY AVE.**

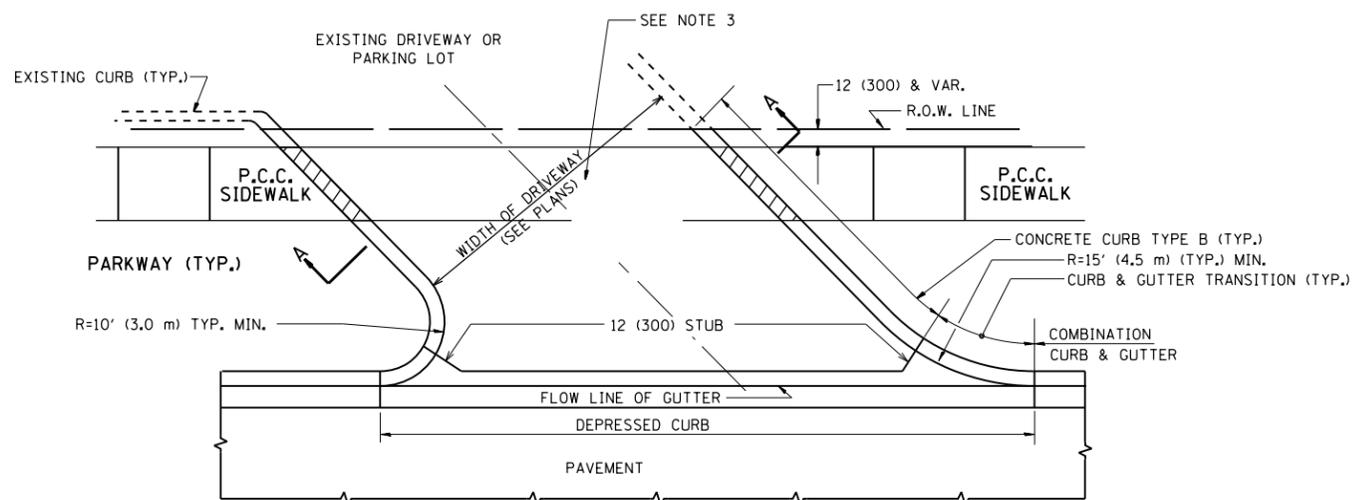
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62F62				
ILLINOIS FED. AID PROJECT				

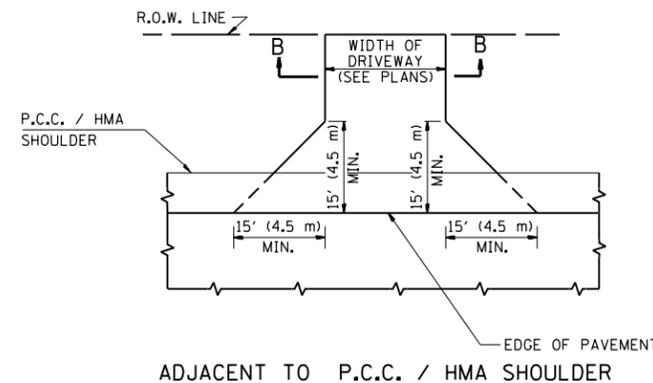
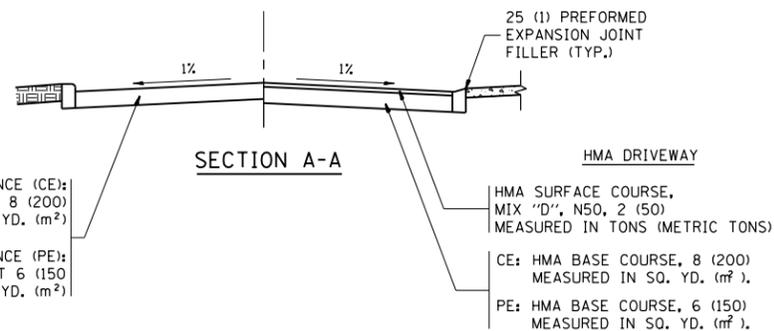




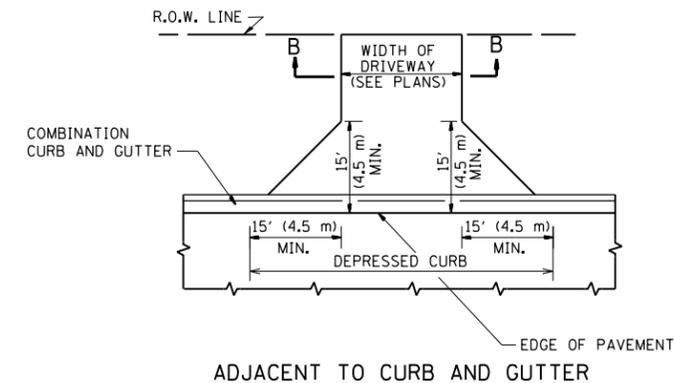
WITH CONCRETE CURB, TYPE B



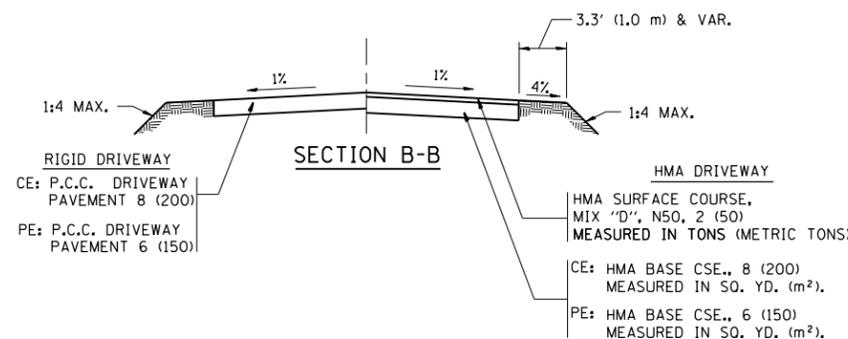
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX 'D', N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

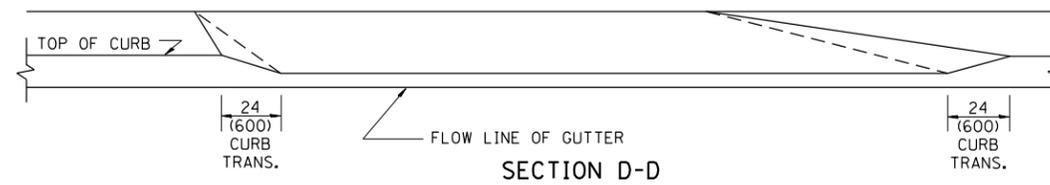
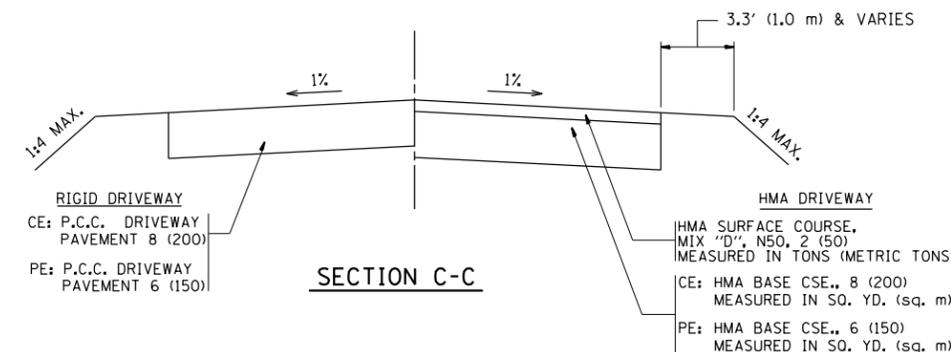
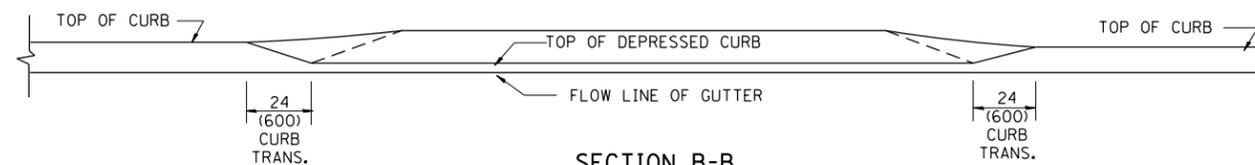
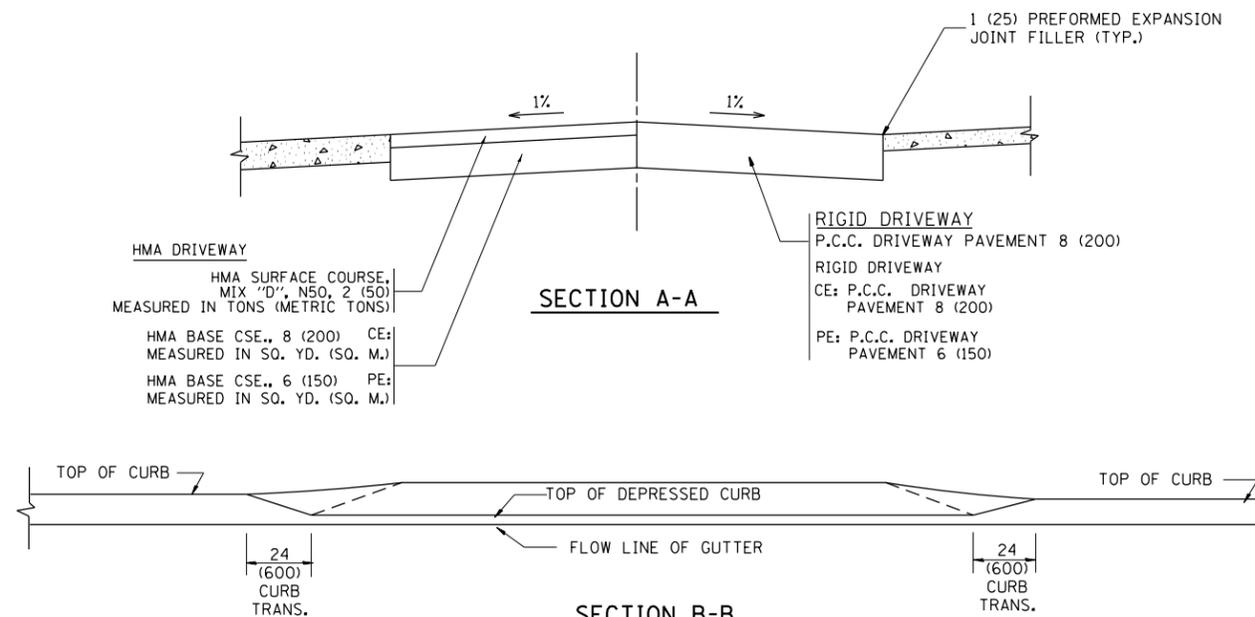
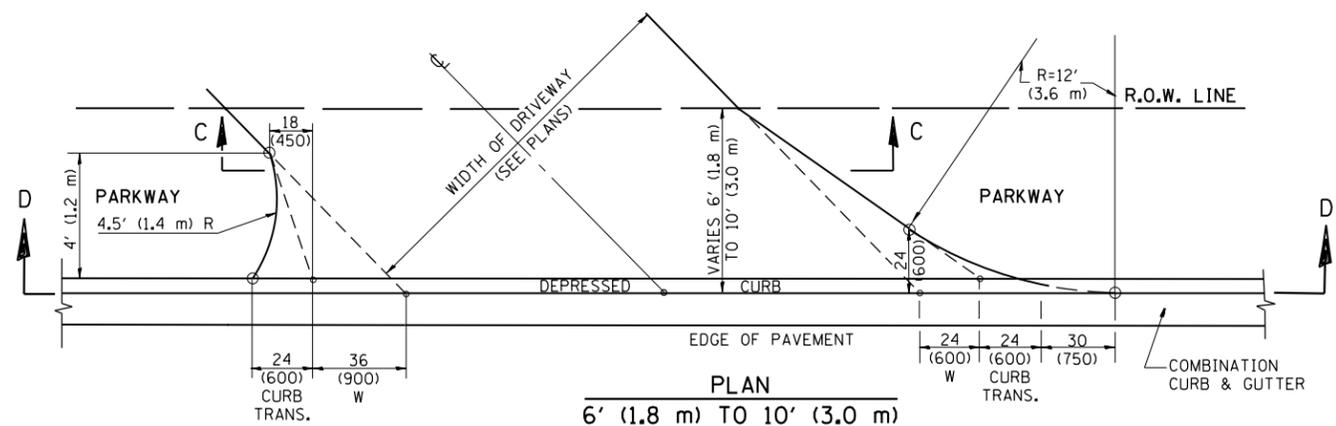
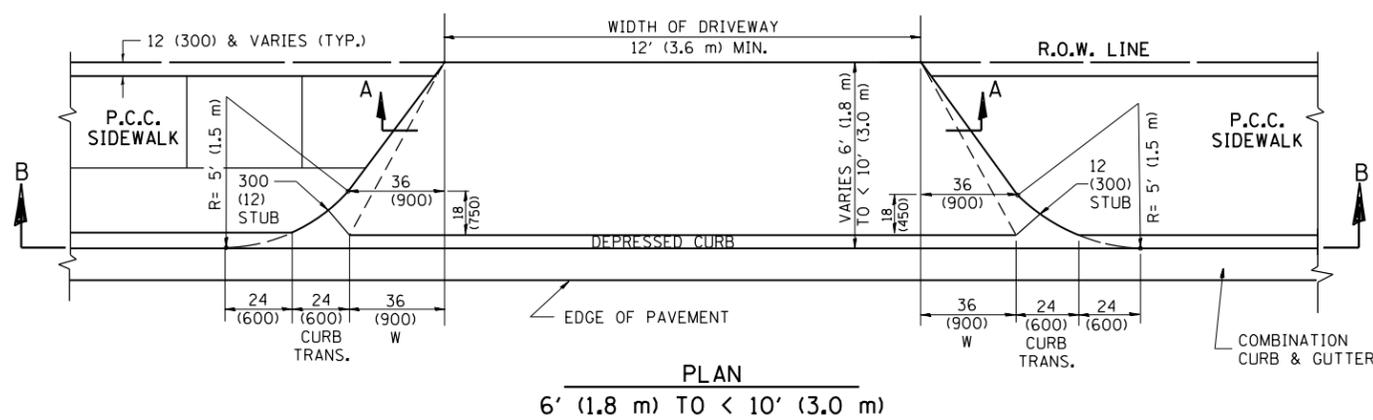
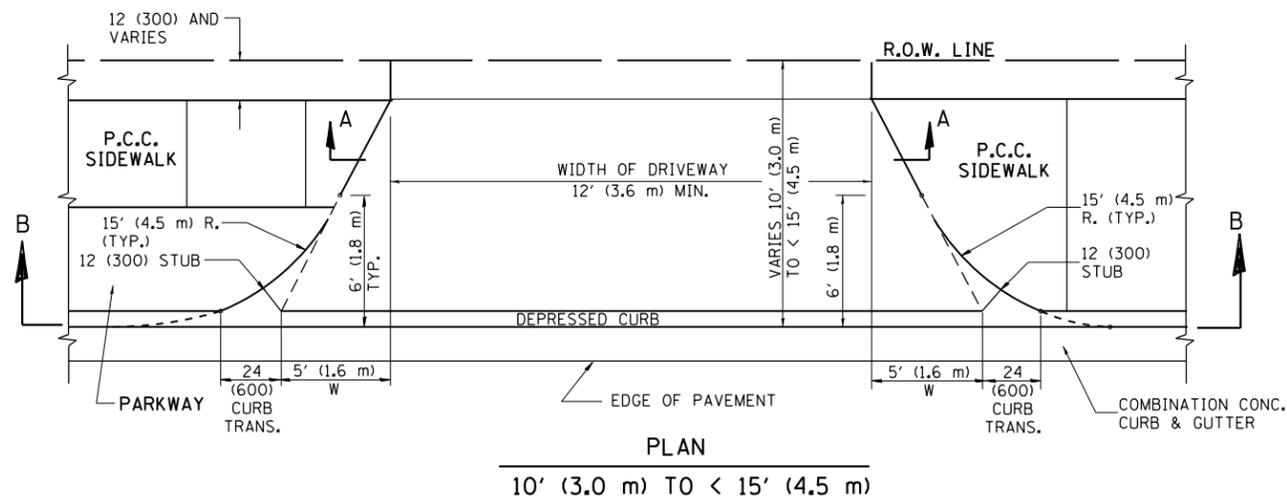
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PLOT DATE = 1/26/2018			REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.  
AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	23
BD0156-07 (BD-01)			CONTRACT NO. 62F62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

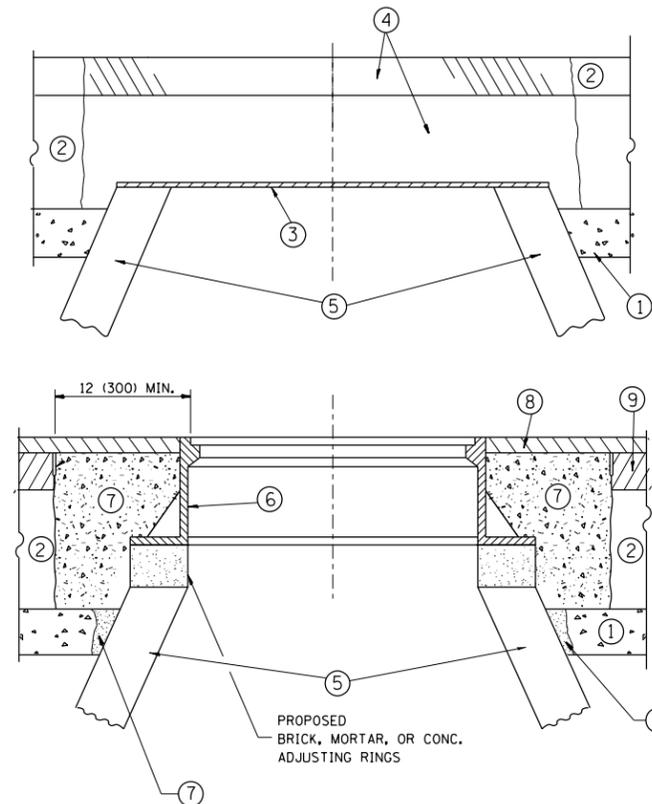
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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		DATE - 11-06-95	REVISED - R. BORO 01-01-07
			REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	24
BD400-02 (BD-02)			CONTRACT NO. 62F62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

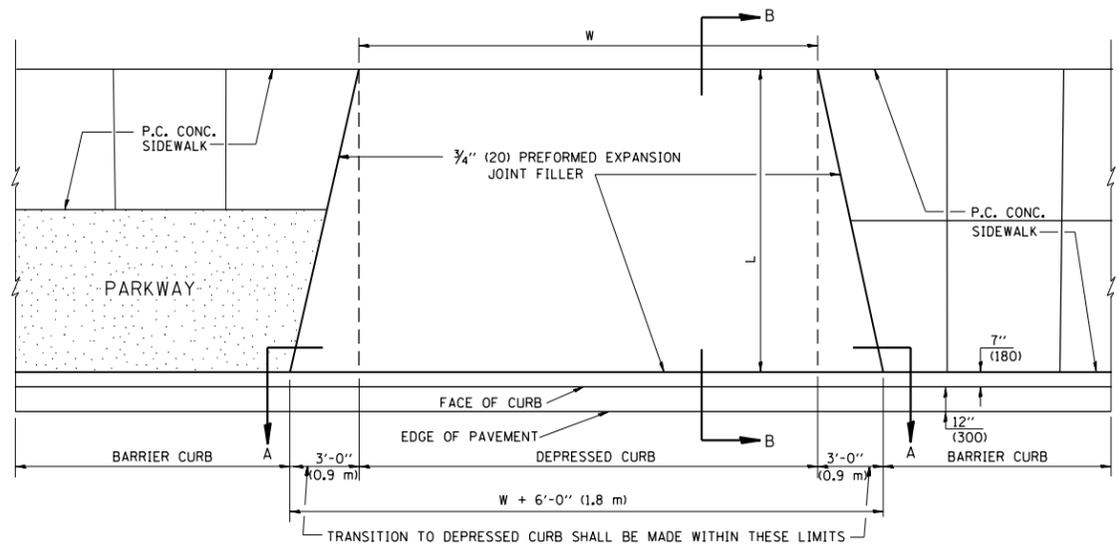
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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	PLOT DATE = 1/26/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

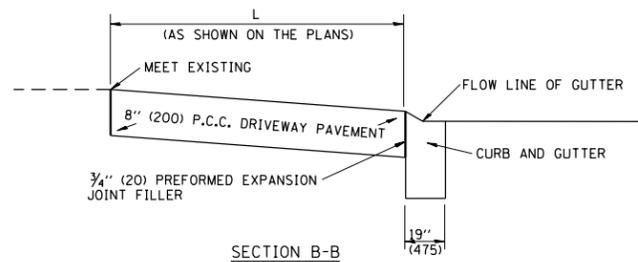
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	25
<b>BD600-03 (BD-8)</b>		<b>CONTRACT NO. 62F62</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



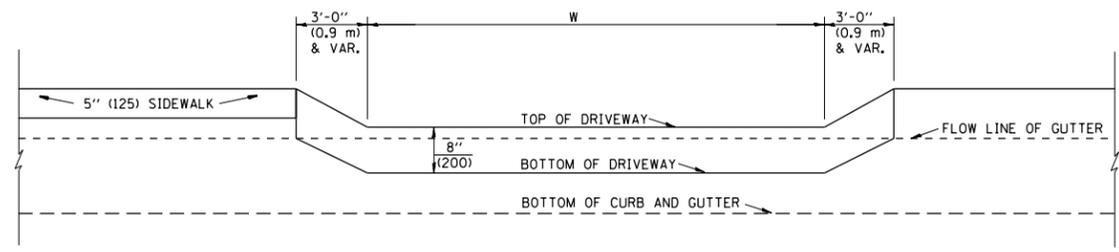
PLAN VIEW

NOTES:

1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS)
3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT 8" (200).
5. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

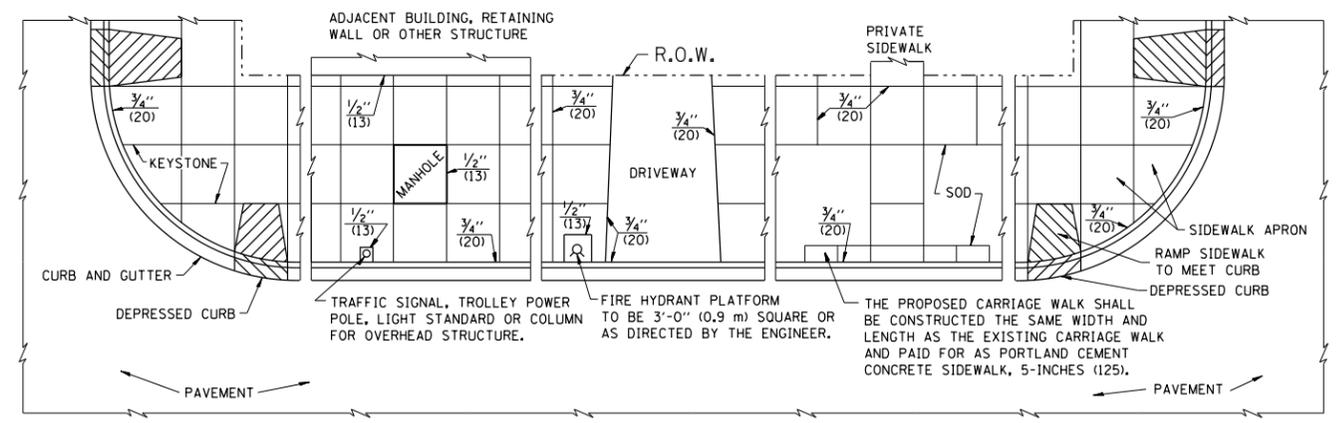


SECTION B-B



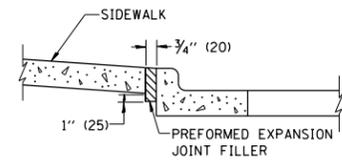
SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL



NOTES:

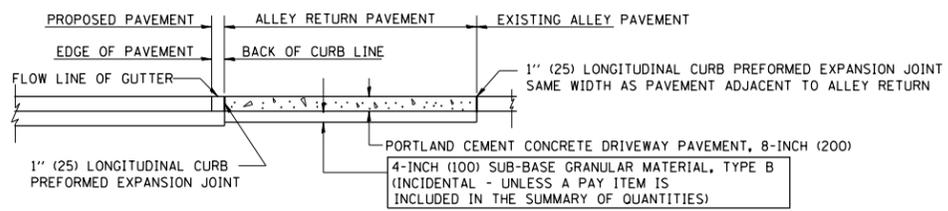
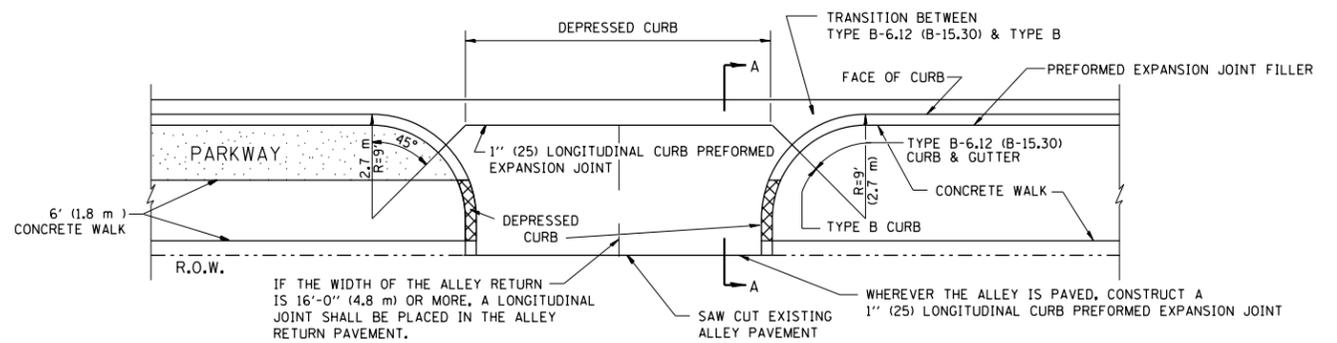
1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK, WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS A CURB.



SLOPE FOR SIDEWALK  
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

PORTLAND CEMENT CONCRETE SIDEWALK DETAILS

NOTES: NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE



SECTION A-A

ALLEY RETURN DETAIL

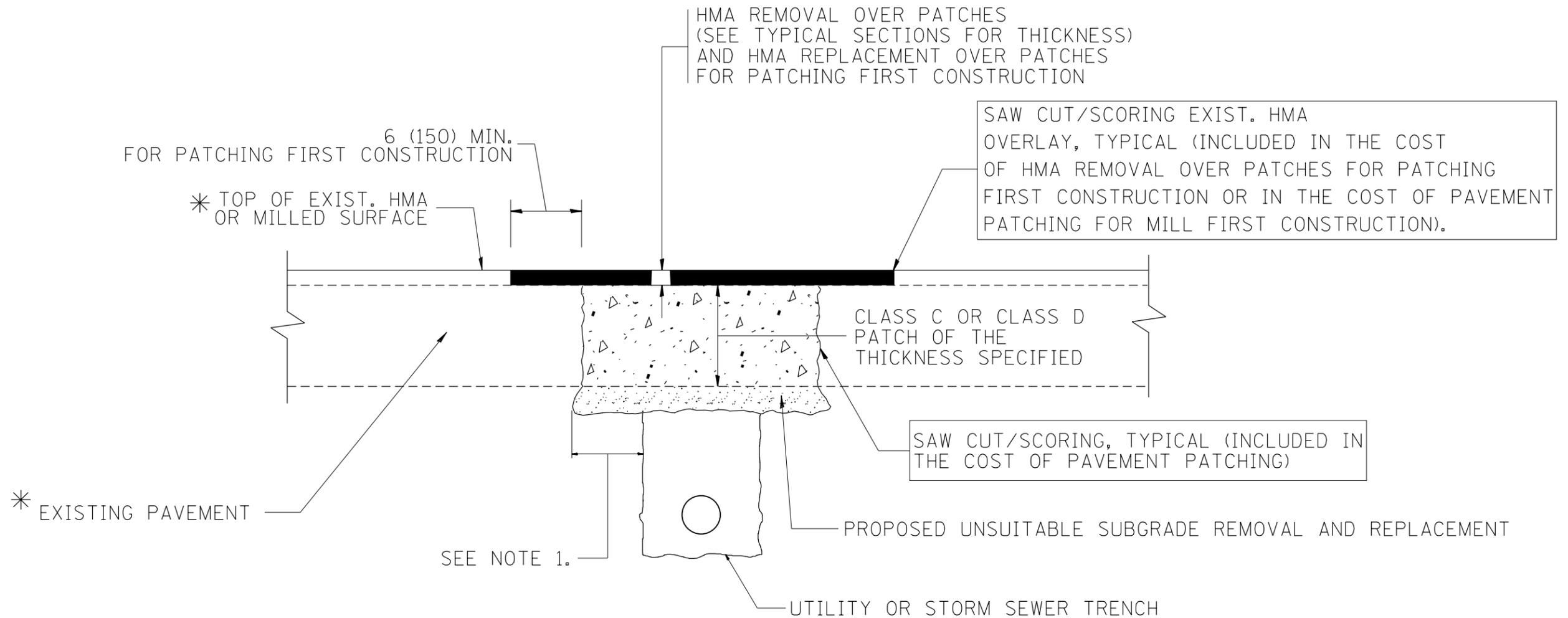
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = Bilgrimo	DESIGNED - M. DE YONG	REVISED -
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		DATE - 06-13-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO  
DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK  
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	26
BD400-03 (BD-17)		CONTRACT NO. 62F62		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Bilgrami	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\11022\Drawings\Design\DistStd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					1548	3290RS-1	COOK	46	27
	PLOT DATE = 1/26/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62F62				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

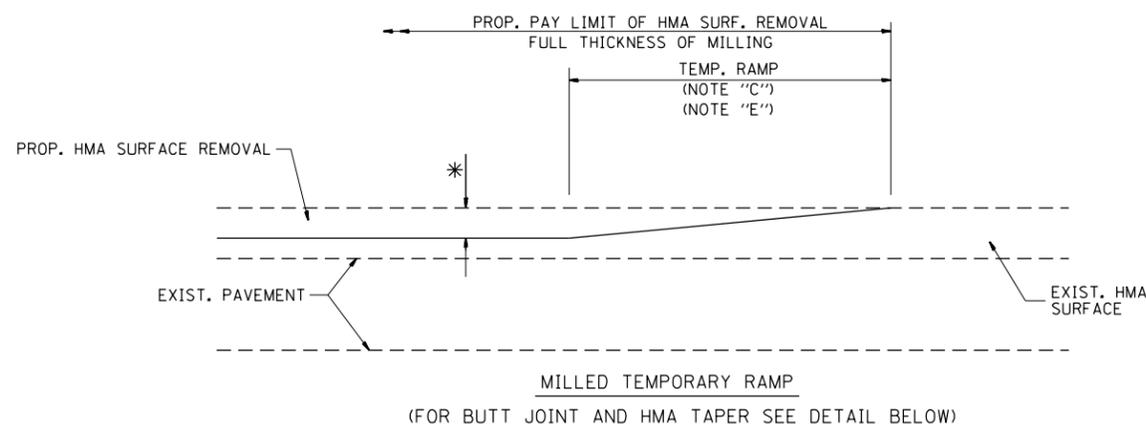
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

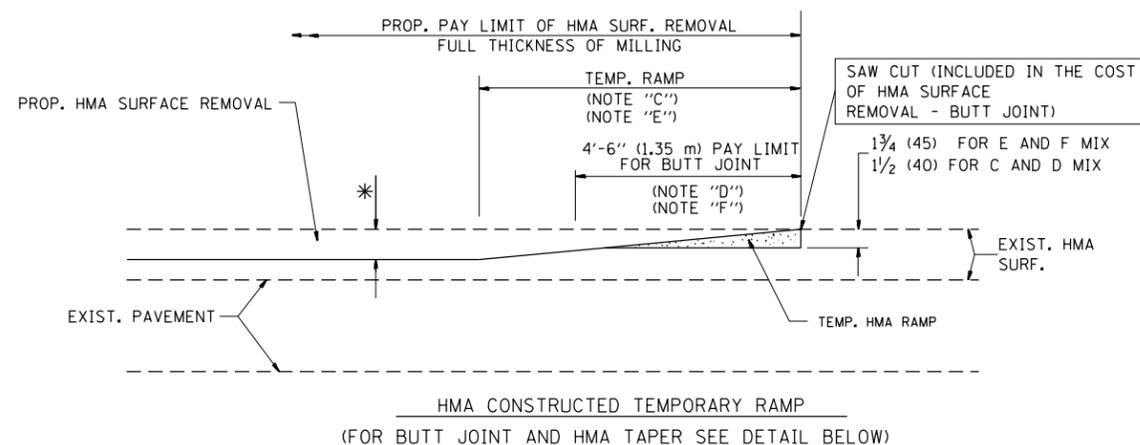
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = Bilgramisa	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\1022\Drawings\Design\DistStd.dgn	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED - A. ABBAS 03-21-97			1548	3290RS-1	COOK	46	28
PLOT DATE = 1/26/2018	DATE - 03-11-94	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 12-15-09			BD600-06 (BD-24)		CONTRACT NO. 62F62		
						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

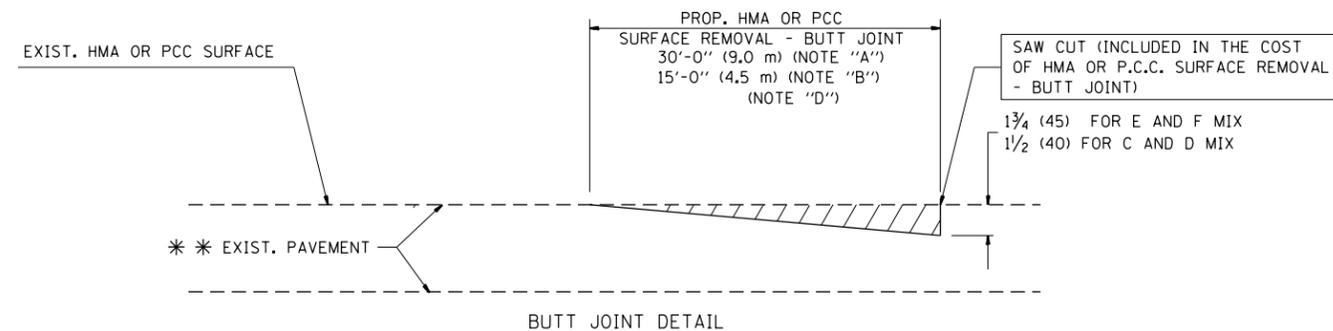


**OPTION 1**

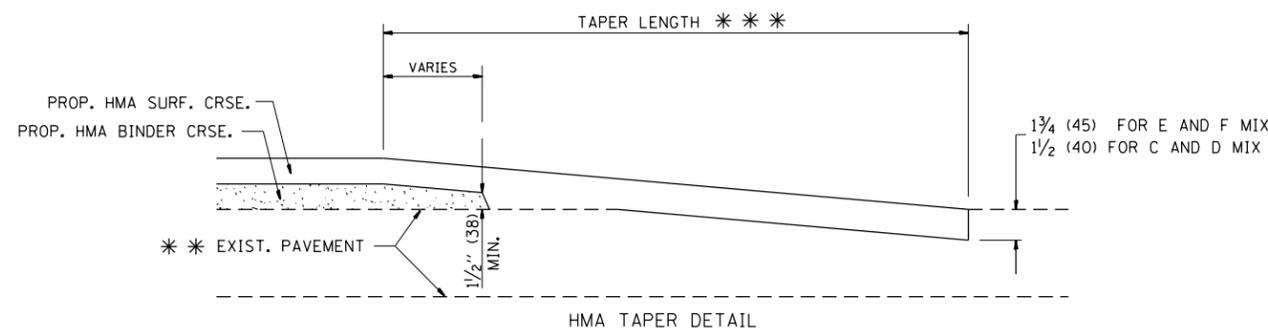


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

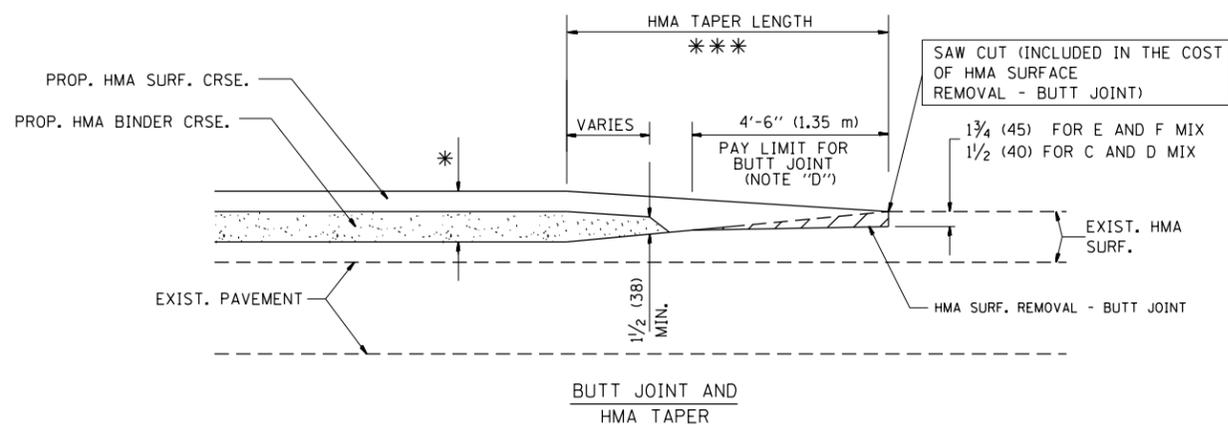
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = Bilgiris	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\11022\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 1/26/2018	CHECKED -	REVISED - M. GOMEZ 04-06-01
		DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

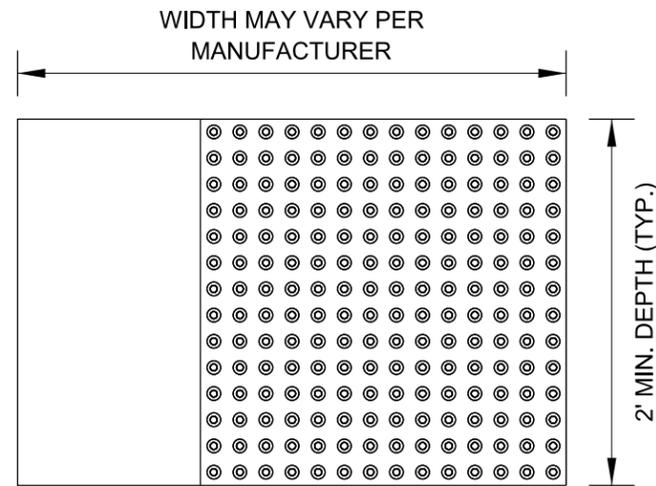
**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

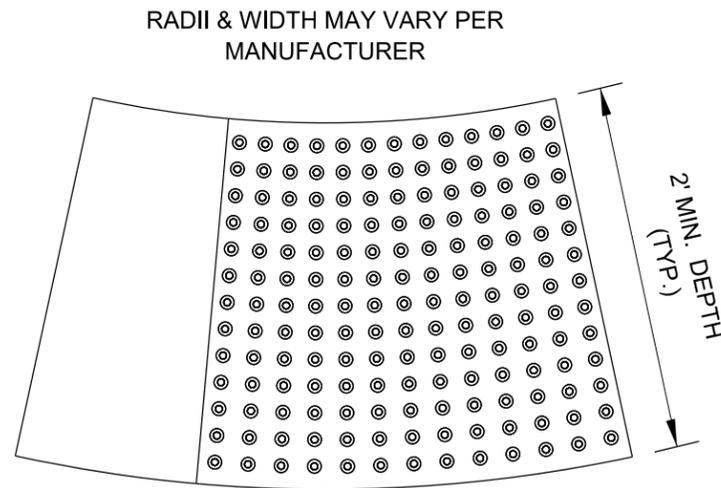
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	29
<b>BD400-05 BD32</b>		<b>CONTRACT NO. 62F62</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



### STRAIGHT DETECTABLE WARNING UNITS



### RADIAL DETECTABLE WARNING UNITS

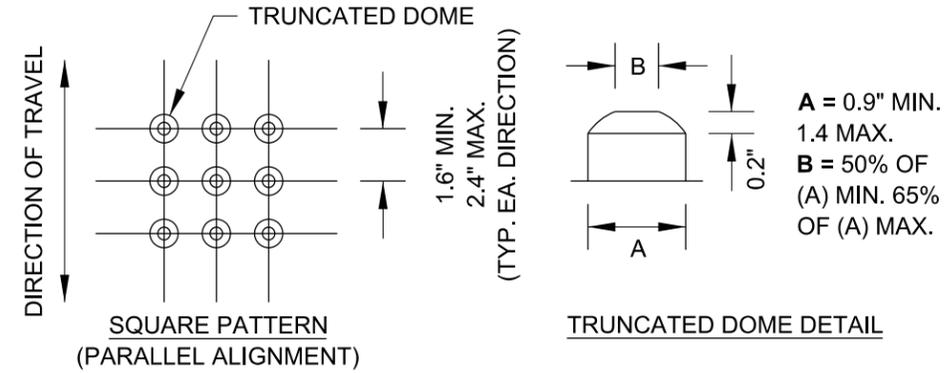


### DETECTABLE WARNING UNIT SIZES

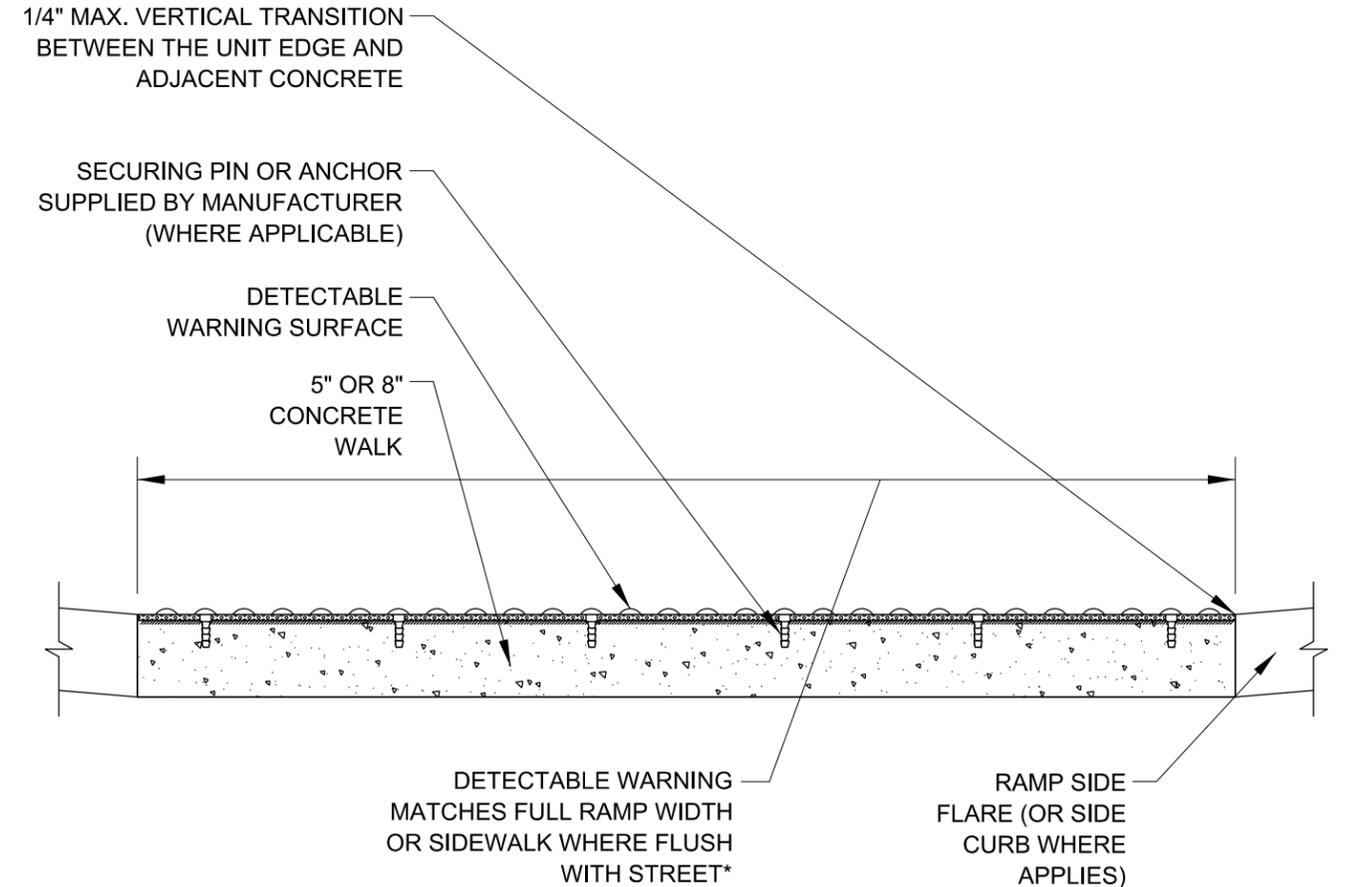
- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

### GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



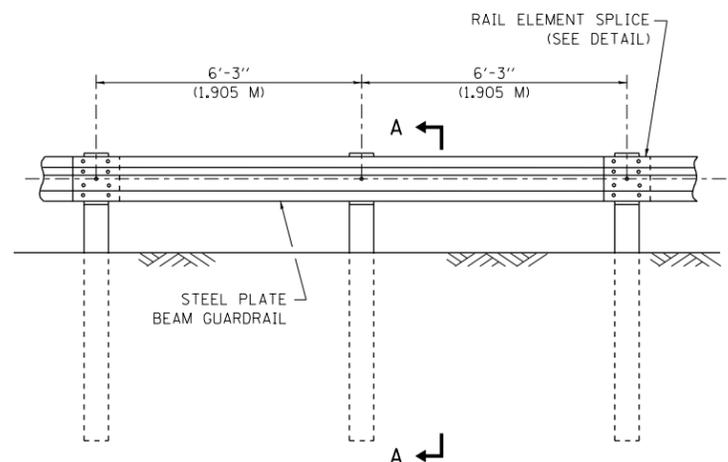
### UNIT PATTERN & DOME DETAIL



\*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

### DETECTABLE WARNING UNIT SECTION

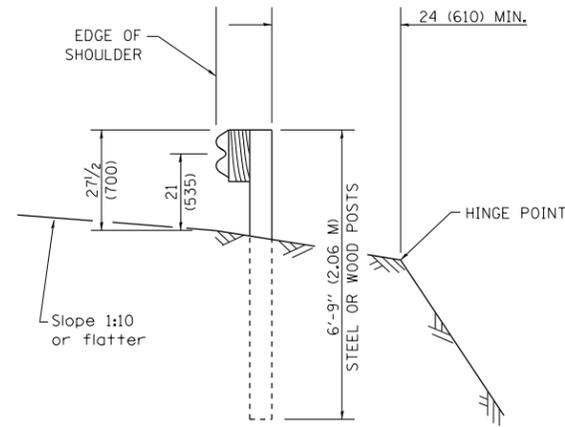
FILE NAME =	USER NAME = Bilgomsa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CITY OF CHICAGO DETECTABLE WARNINGS</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED -			1548	3290RS-1	COOK	46	31
Default	PLOT SCALE = 100.0000' / in.	DATE - 06-20-2017	REVISED -			<b>BD 58</b>		CONTRACT NO. 62F62		ILLINOIS FED. AID PROJECT
	PLOT DATE = 1/26/2018					SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	



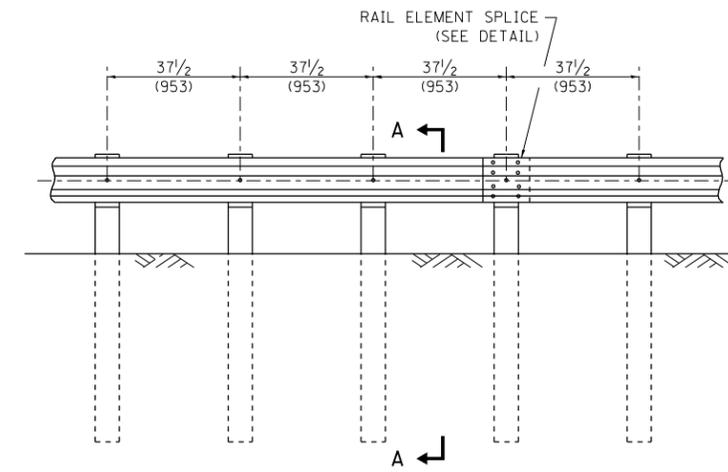
ELEVATION

**TYPE A**

6'-3" (1.905 M) TYPICAL POST SPACING



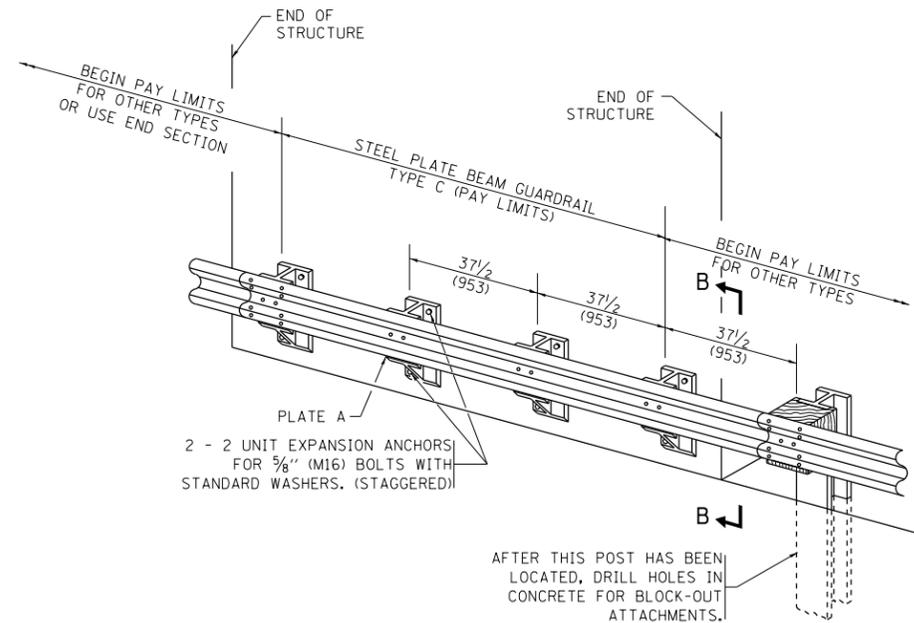
SECTION A-A



ELEVATION

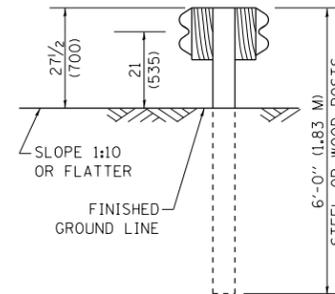
**TYPE A**

37 1/2 (953) CLOSED POST SPACING

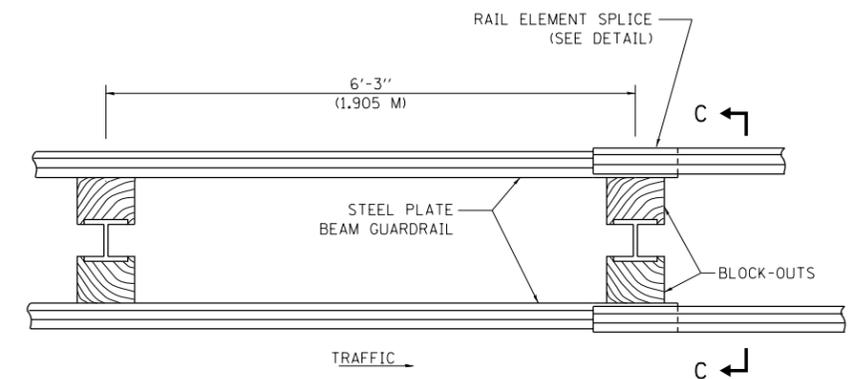


**TYPE C**

37 1/2 (953) BLOCK-OUT SPACING



SECTION C-C

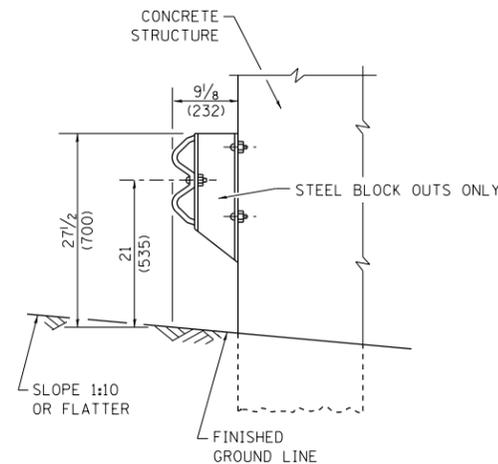


TRAFFIC

PLAN

**TYPE D**

DOUBLE STEEL PLATE BEAM GUARDRAIL  
6'-3" (1.905 M) TYPICAL POST SPACING



SECTION B-B

**GENERAL NOTES**

ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

THE EXISTING STEEL POSTS MAY BE DRILLED TO MATCH THE BOLT PATTERN SHOWN HEREIN FOR THE WOOD BLOCK-OUT, OR A NEW STEEL POST SHALL BE PROVIDED.

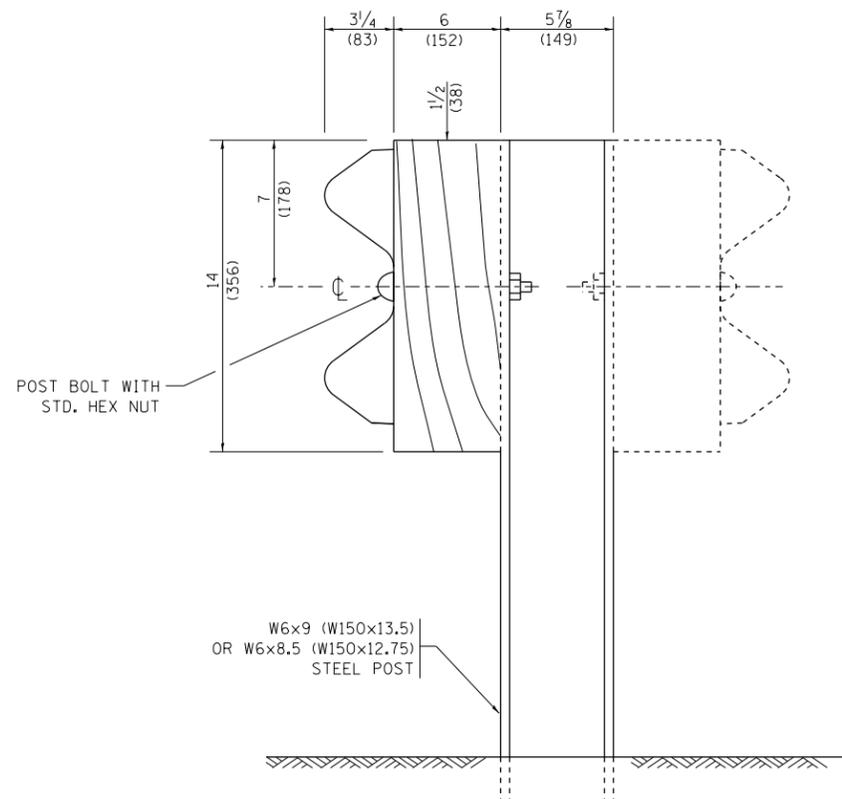
THIS DETAIL IS APPLICABLE TO THE GUARDRAIL SYSTEM USED PRIOR TO JANUARY 1, 2007. FOR DETAILS ON THE MIDWEST GUARDRAIL SYSTEM, SEE STANDARD 630001.

FILE NAME =	USER NAME = Bilgromiso	DESIGNED -	REVISED - 10-31-06
p:\11\084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED -
		DATE -	REVISED -

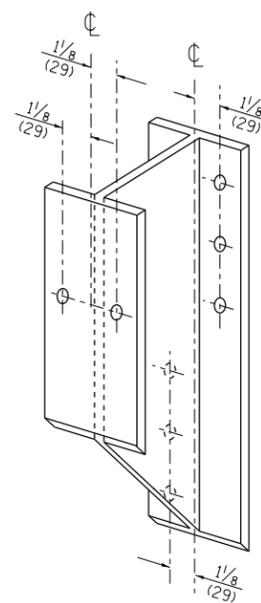
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL</b>			
SCALE: NONE	SHEET NO. 1 OF 4 SHEETS	STA.	TO STA.

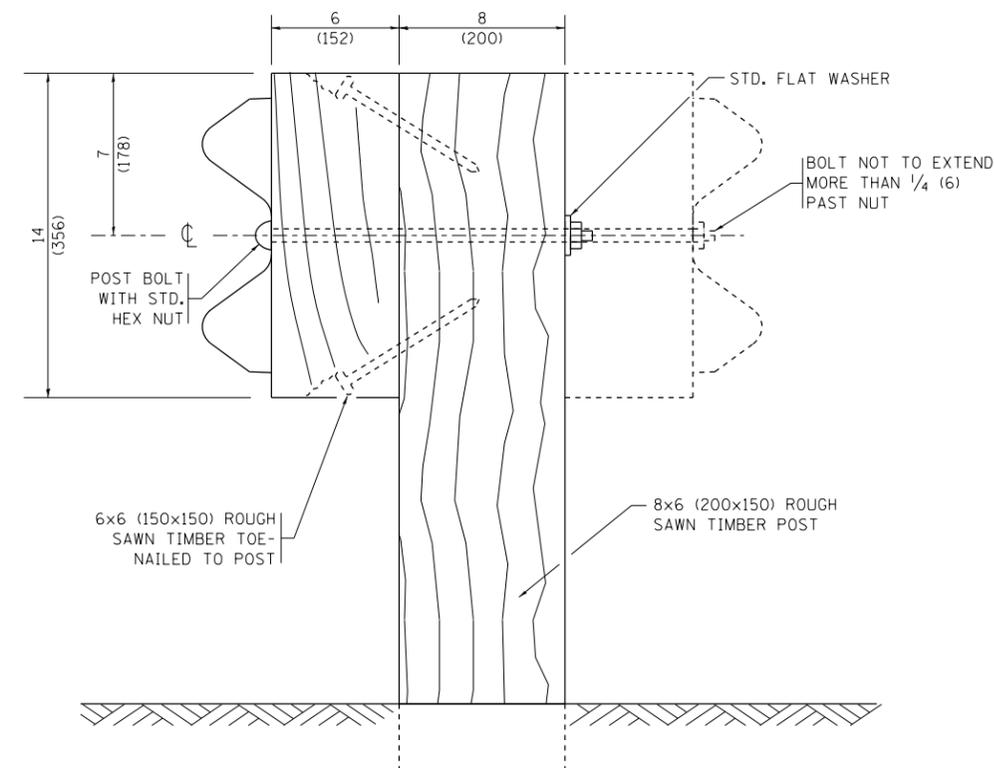
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	32
<b>BM-21</b>		<b>CONTRACT NO. 62F62</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



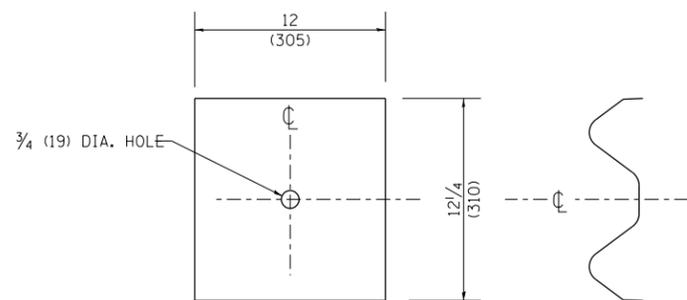
STEEL POST CONSTRUCTION



STEEL BLOCK-OUT DETAIL



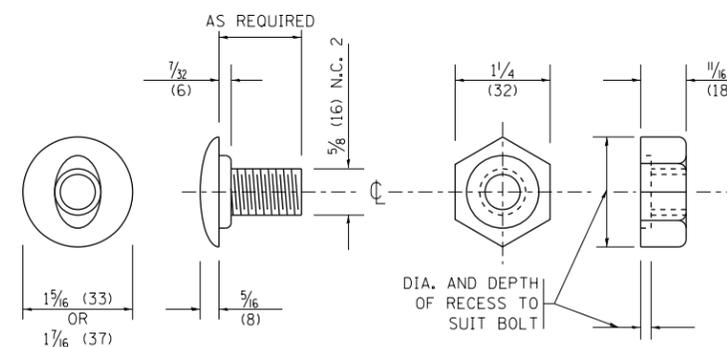
WOOD POST CONSTRUCTION



NOTE:

PLATE A SHALL BE PLACED BETWEEN RAIL ELEMENT AND BLOCK-OUT AT NON-SPLICE MOUNTING POINTS ONLY WHEN STEEL BLOCK-OUTS ARE USED.

PLATE A



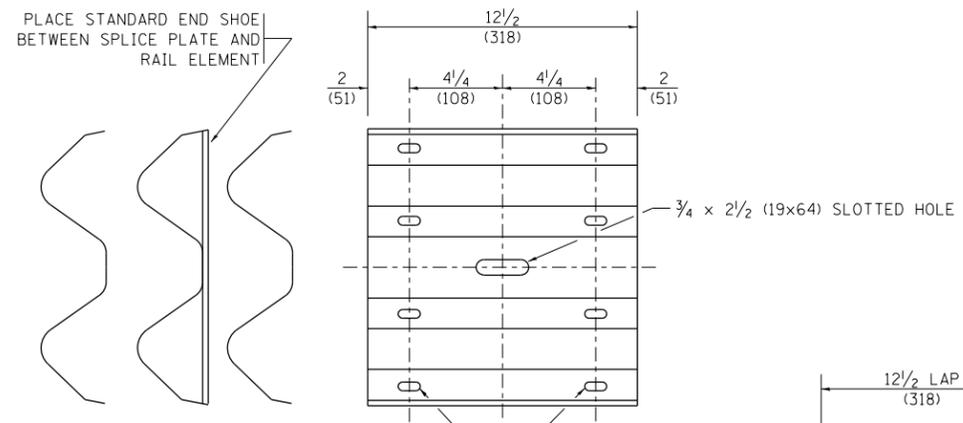
POST OR SPLICE BOLT & NUT

FILE NAME =	USER NAME = Bilgramis	DESIGNED -	REVISED - 10-31-06
p:\11\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\1022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED -
		DATE -	REVISED -

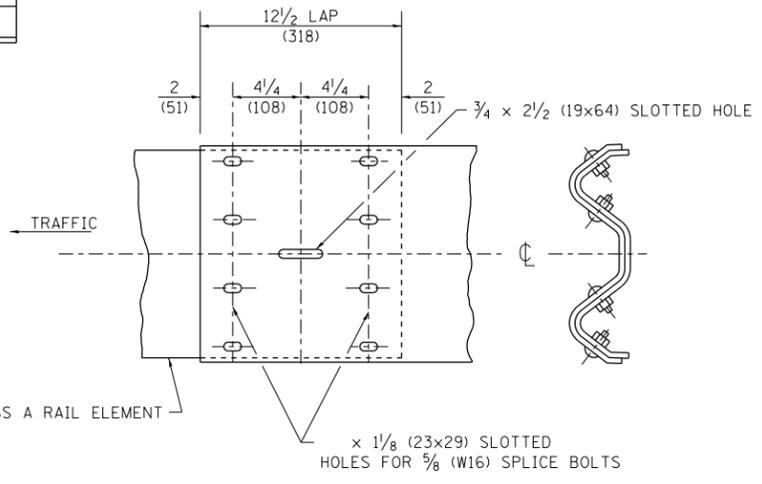
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL			
SCALE: NONE	SHEET NO. 2 OF 4 SHEETS	STA.	TO STA.

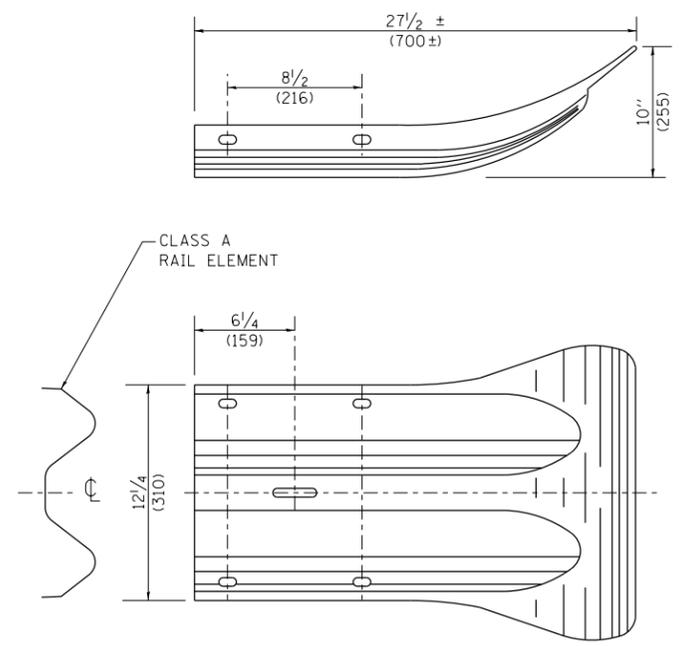
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BM-21		CONTRACT NO. 62F62		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



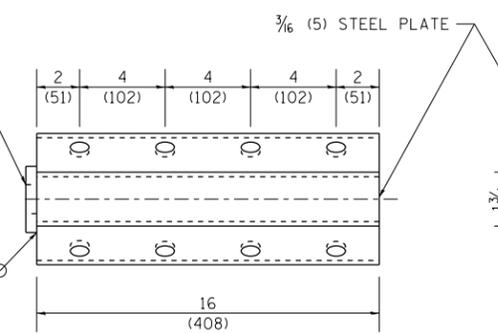
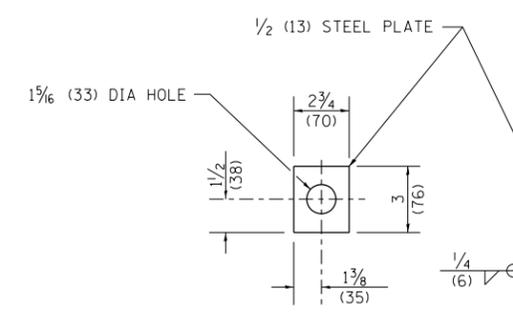
**SPLICE PLATE**



**RAIL ELEMENT SPLICE**



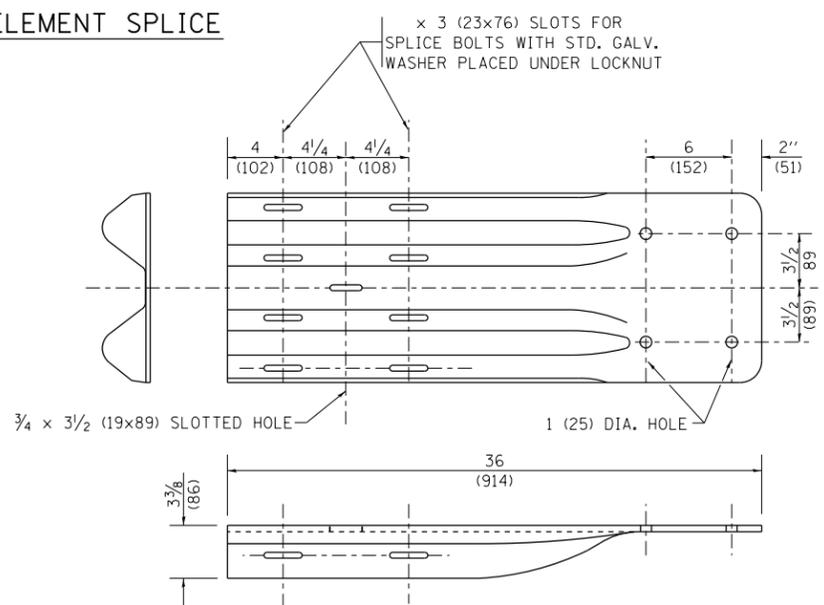
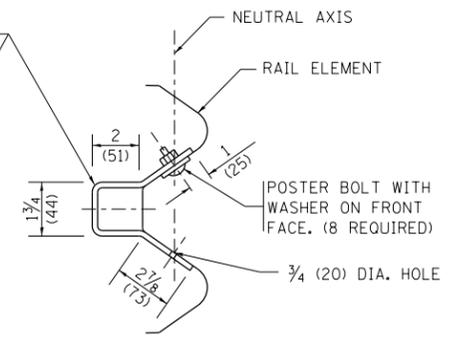
**END SECTION**



**NOTE:**

ANCHOR PLATE T SHALL BE USED TO ATTACH CABLE ASSEMBLY TO GUARDRAIL WHEN REQUIRED ON TRAFFIC BARRIER TERMINALS.

**ANCHORE PLATE T DETAILS**



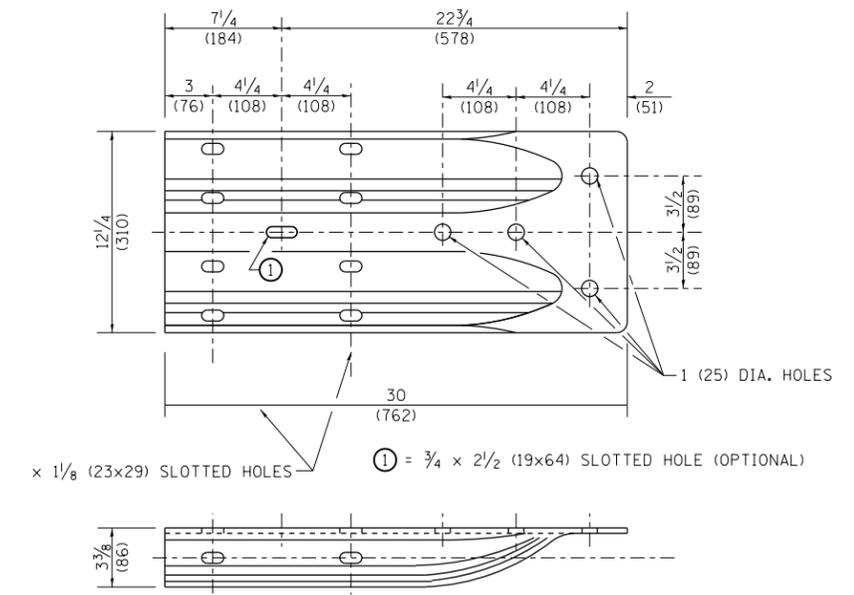
**END SHOE**

**NOTE:**

WHEN END SHOE IS ATTACHED TO A BRIDGE PARAPET WHICH HAS AN EXPANSION JOINT, THE BOLTS SHALL BE PROVIDED WITH A LOCKNUT OR DOUBLE NUT AND SHALL BE TIGHTENED ONLY TO A POINT THAT WILL ALLOW GUARDRAIL MOVEMENT.

THE STANDARD END SHOE SHALL BE ATTACHED TO THE CONCRETE WITH PRE-DRILLED OR SELF-DRILLING ANCHOR BOLTS. THE ANCHOR CONE SHALL BE SET FLUSH WITH THE SURFACE OF THE CONCRETE.

EXTERNALLY THREADED STUDS PROTRUDING FROM THE SURFACE OF THE CONCRETE WILL NOT BE PERMITTED.



**ALTERNATE END SHOE**

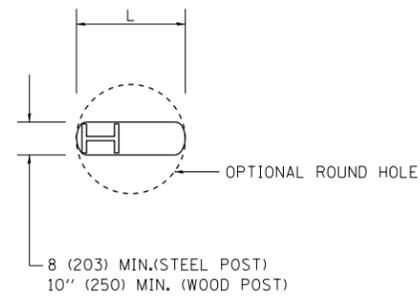
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		DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

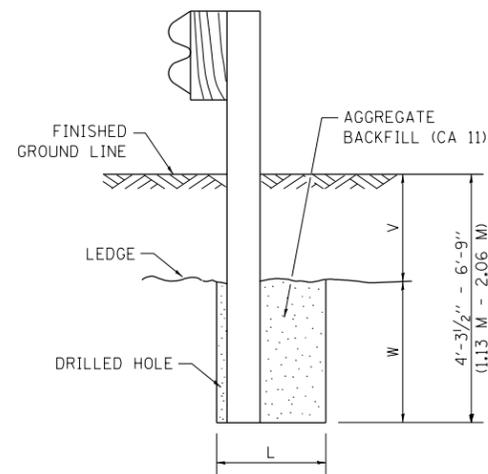
**REMOVE AND REERECT  
STEEL PLATE BEAM GUARDRAIL**

SCALE: NONE SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.U. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	34
<b>BM-21</b>		<b>CONTRACT NO. 62F62</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



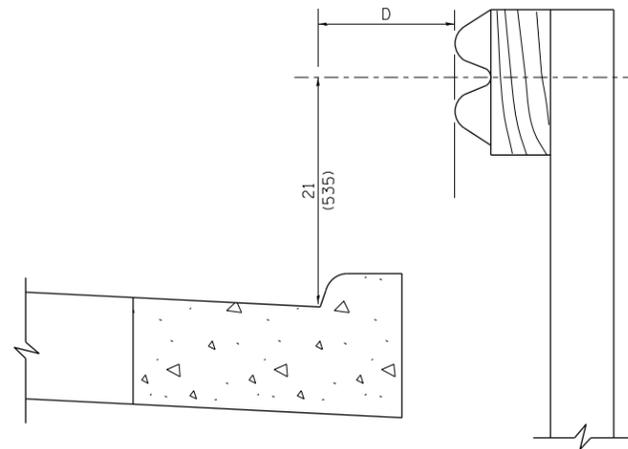
PLAN



NOTE:  
LEDGE LINE IS TOP OF ROCK LEDGE OR HARD SLAG FILL.

ELEVATION

**FOOTING FOR POST WHEN IMPERVIOUS MATERIAL IS ENCOUNTERED**



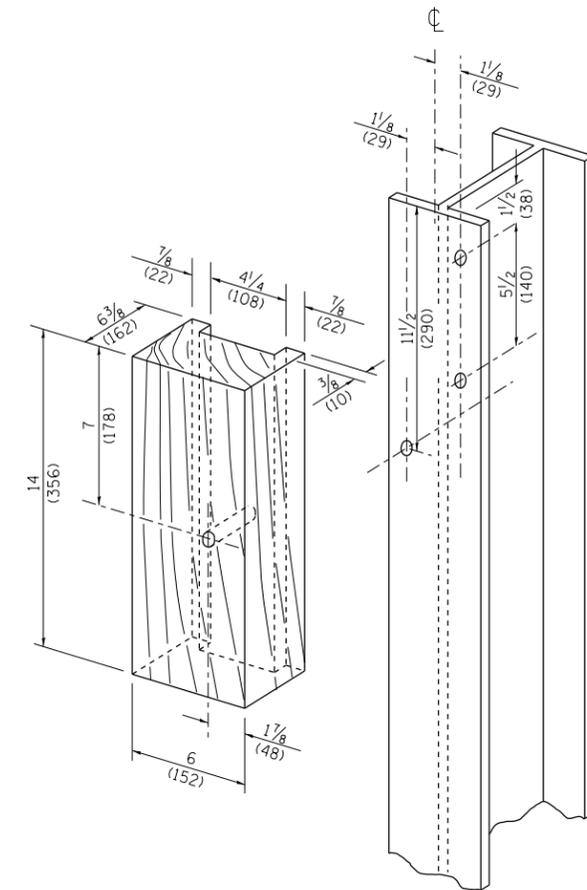
**NOTE:**

IF IT IS NECESSARY FOR D TO BE MORE THAN 12 (300) AND LESS THAN 10'-0" (3.0 M) TYPE M-2 (M-5) CURB AND GUTTER (STD. 606001) SHALL BE USED IN FRONT OF AND IN ADVANCE OF THE GUARDRAIL.

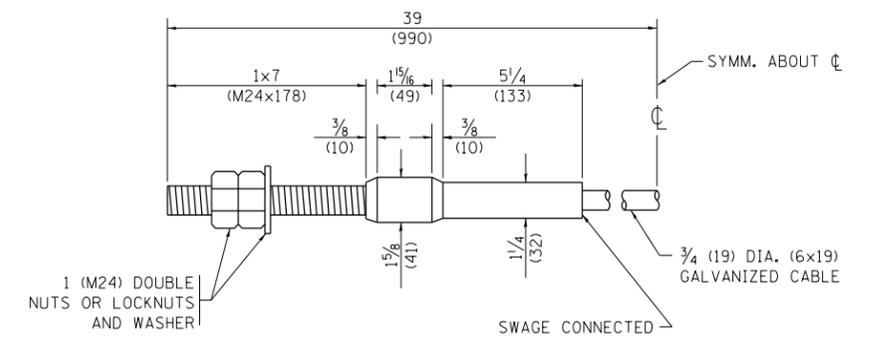
**GUARDRAIL PLACED BEHIND CURB**

(D = 0 DESIRABLE TO 12 (300) MAXIMUM)

V	W	L	
		STEEL POST	WOOD POST
0 - 18 (0 - 460)	24 (610)	21 (530)	23 (580)
>18 - 41.5 (> 460 - 825)	12 (305)	8 (203)	10 (250)
>41.5 - 53.5 (> 825 - 1.13 M)	12 - 0 (350 - 0)	8 (203)	10 (250)



**WOOD BLOCK-OUT AND STEEL POST DETAILS**



**CABLE ASSEMBLY**

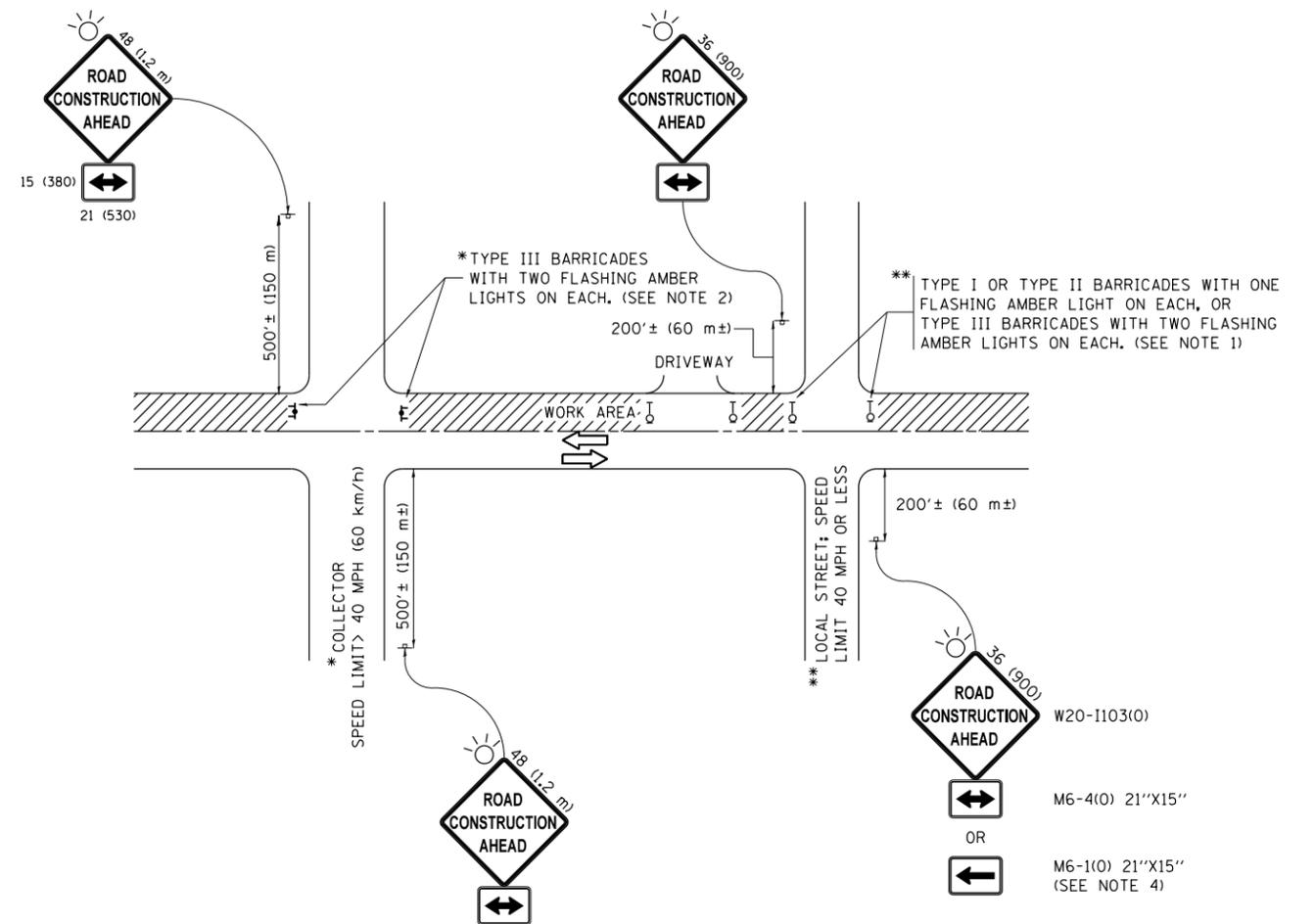
(40,000 LBS (18,100 KG) MIN. BREAKING STRENGTH)  
TIGHTEN TO TAUT TENSION

FILE NAME =	USER NAME = Bilgiris	DESIGNED -	REVISED - 10-31-06
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED -
		DATE -	REVISED -
PLOT SCALE = 100.0000' / in.			
PLOT DATE = 1/26/2018			

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL</b>			
SCALE: NONE	SHEET NO. 4 OF 4 SHEETS	STA.	TO STA.

F.A.U. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	35
<b>BM-21</b>		<b>CONTRACT NO. 62F62</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

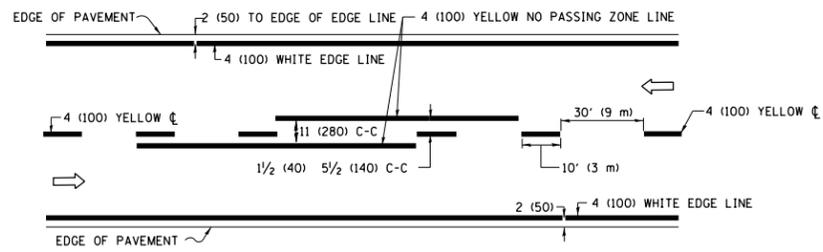
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	PLOT DATE = 1/26/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

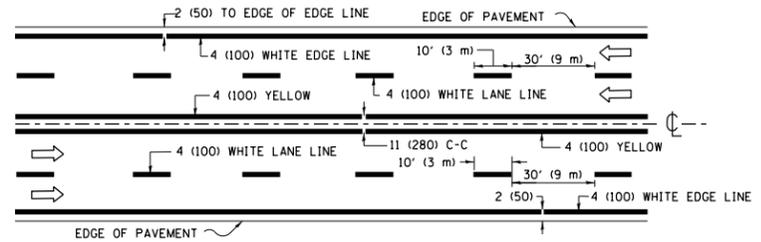
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

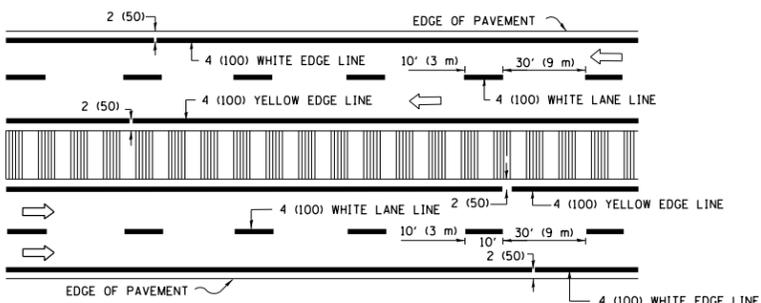
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	36
<b>TC-10</b>			<b>CONTRACT NO. 62F62</b>	
ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

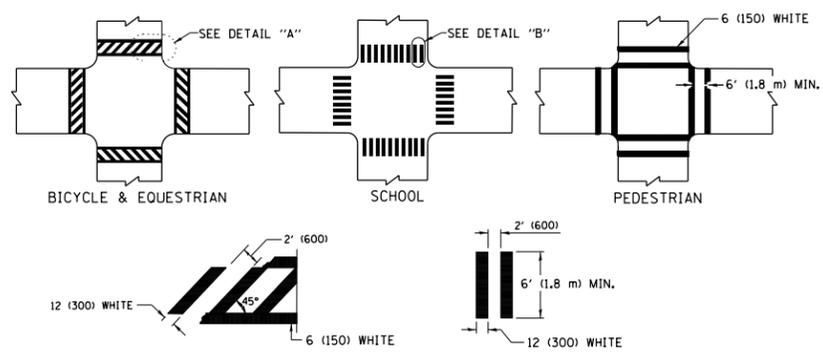


**MULTI-LANE UNDIVIDED**



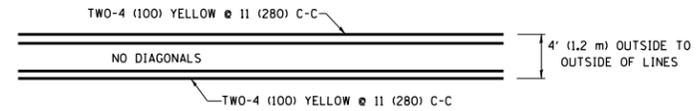
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

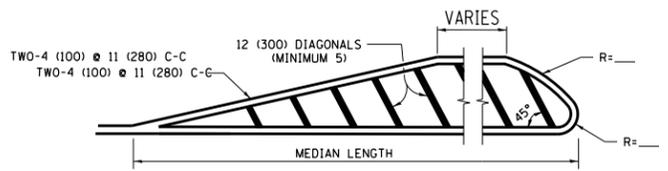


**TYPICAL CROSSWALK MARKING**

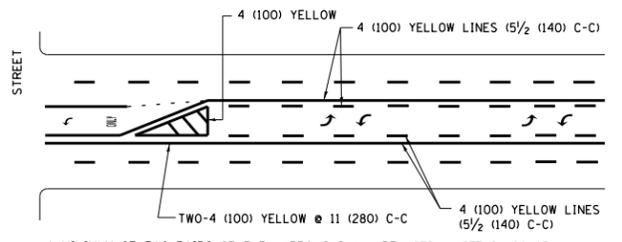
\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



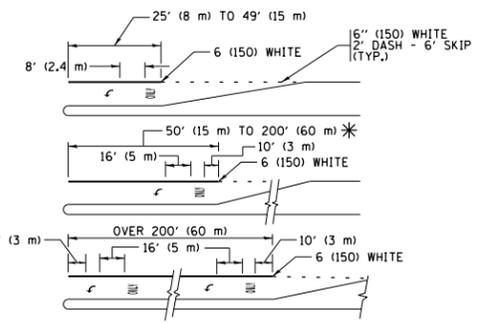
**4' (1.2 m) WIDE MEDIANS ONLY**



**MEDIANS OVER 4' (1.2 m) WIDE**



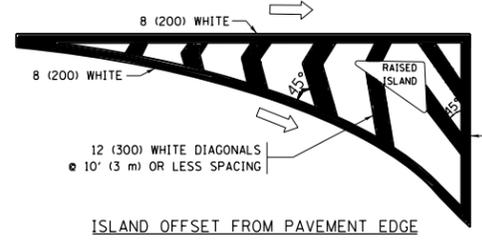
**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**



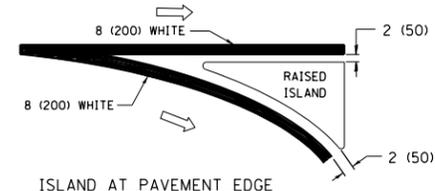
**TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING**

FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

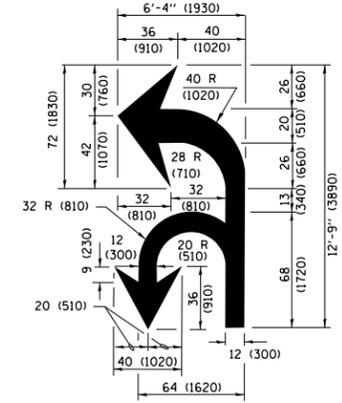
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



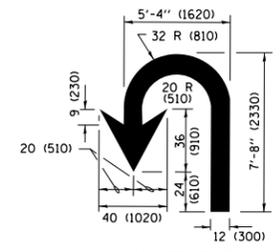
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8" (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (22.5 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

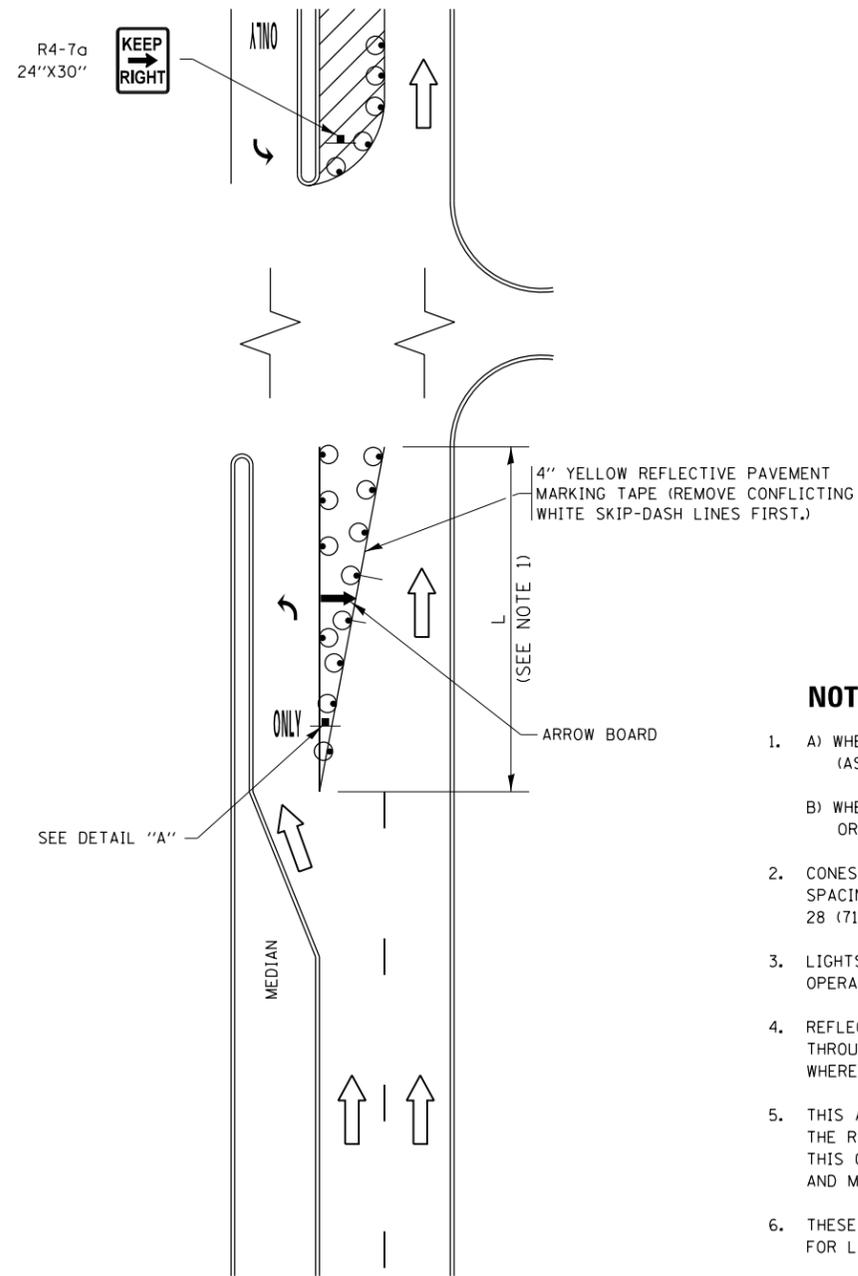
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**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

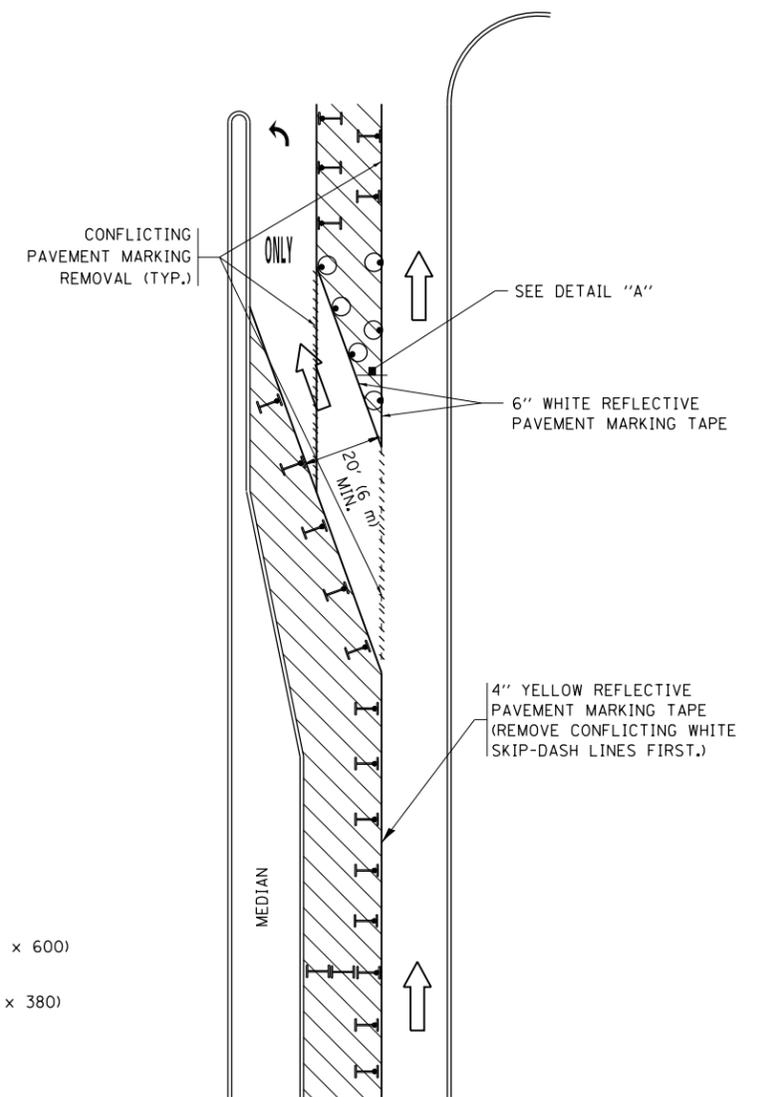
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	37
<b>TC-13</b>		<b>CONTRACT NO. 62F62</b>	ILLINOIS FED. AID PROJECT	

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

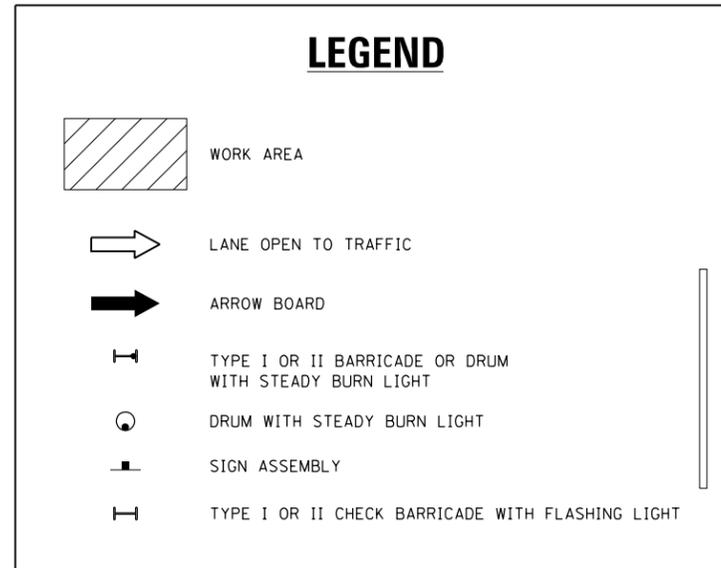


**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE

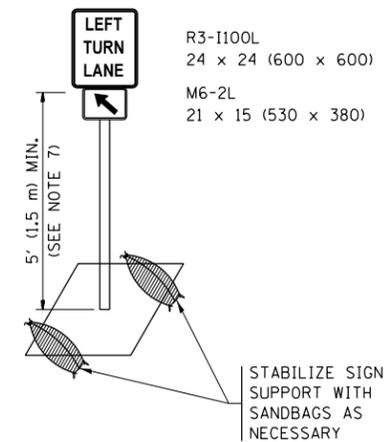


**FIGURE 2**



### NOTES:

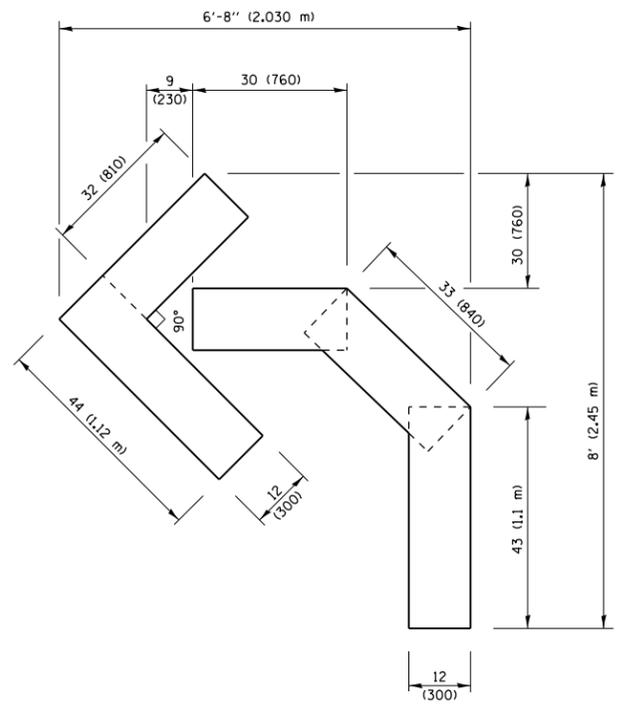
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



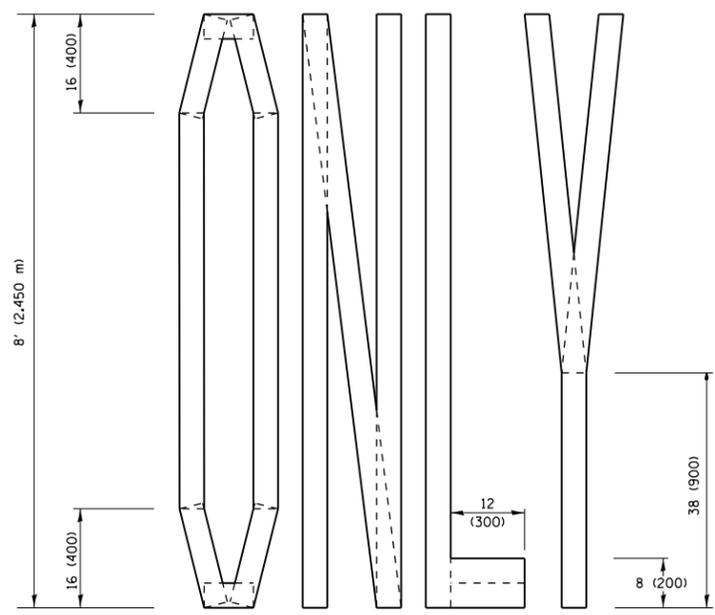
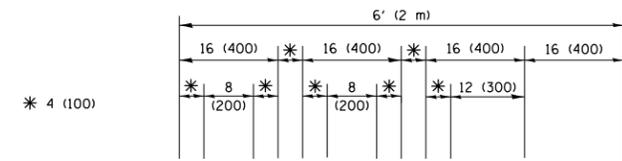
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

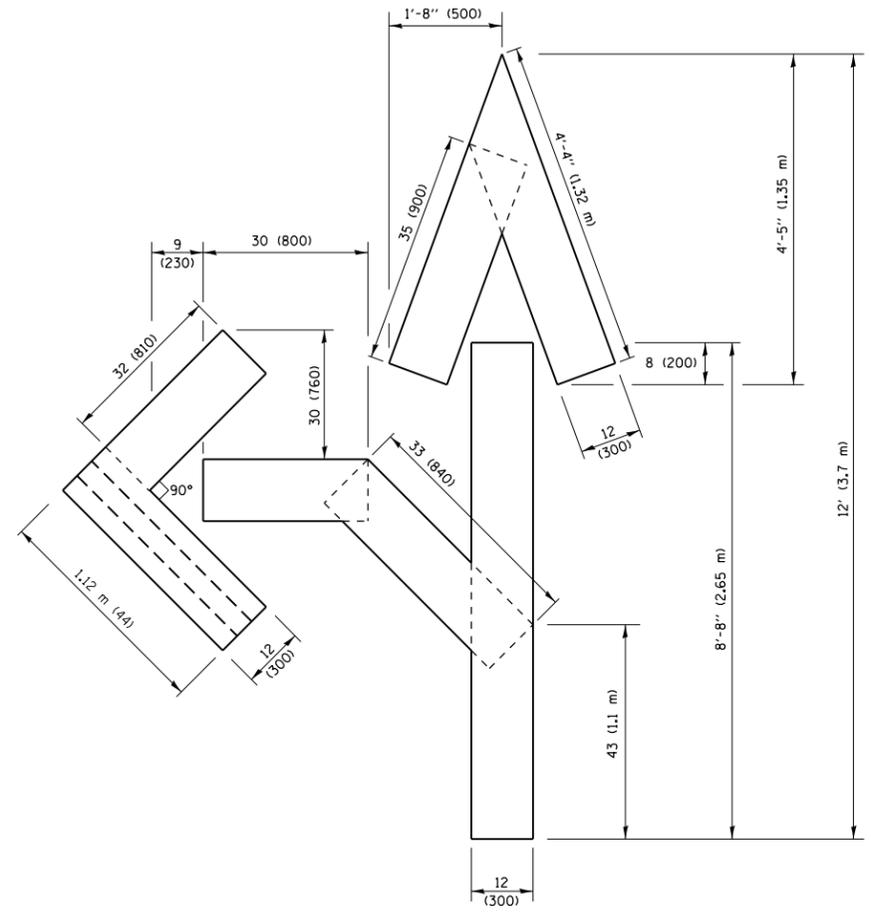
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pw:\IL\084EBIDINTEG\illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\1022\REVISED Design\HOUSEH 11-07-95	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	REVISED - A. SCHUETZE 07-01-13			1548	3290RS-1	COOK	46	38
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	PLOT DATE = 1/26/2018	REVISED - T. RAMMACHER 01-06-00	REVISED -			SCALE: NONE		SHEET 1 OF 1 SHEETS		STA. TO STA.



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

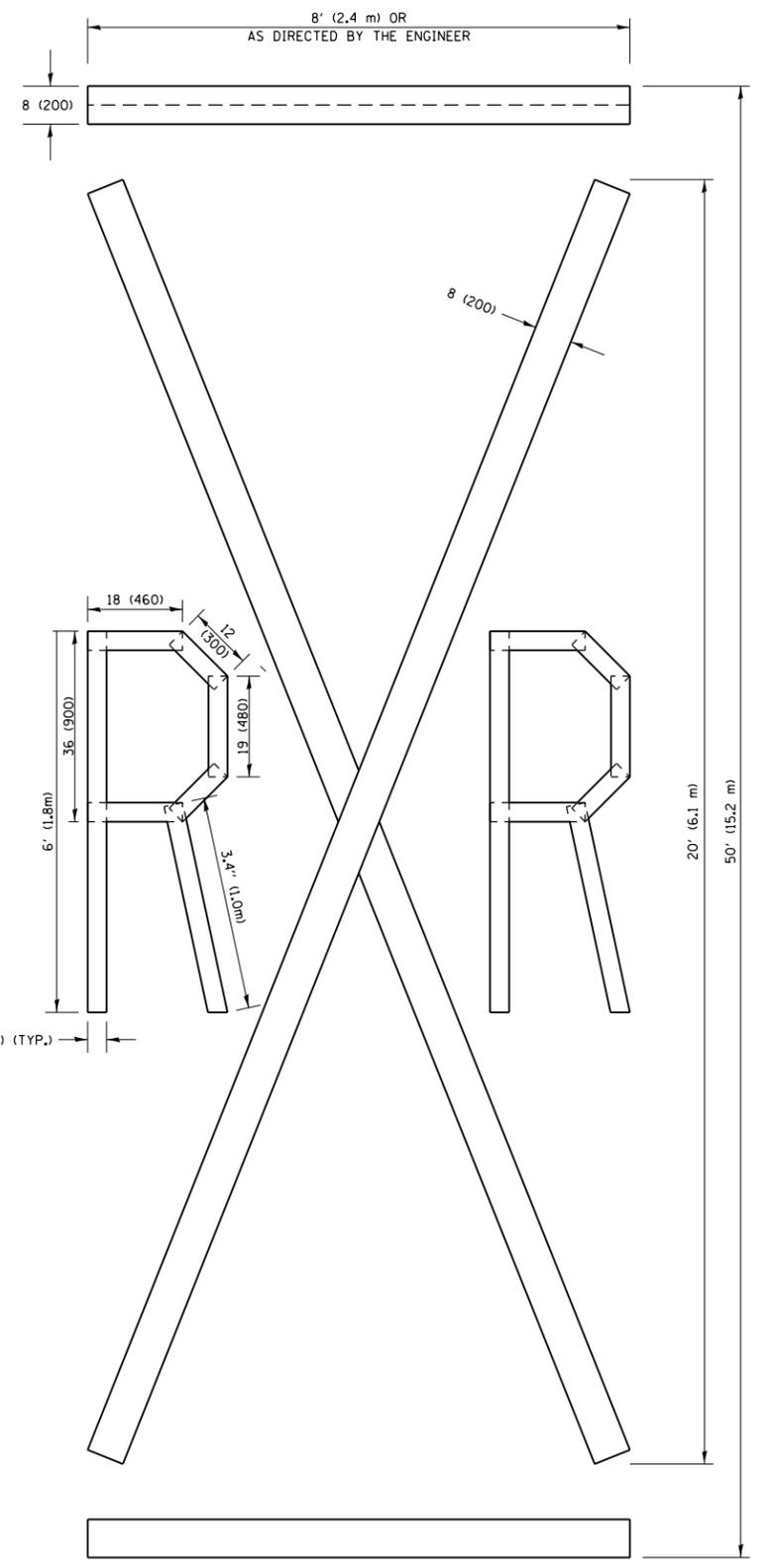


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED  
 IN LINEAR FEET OF 4" LINES TO MATCH THE  
 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS  
 THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

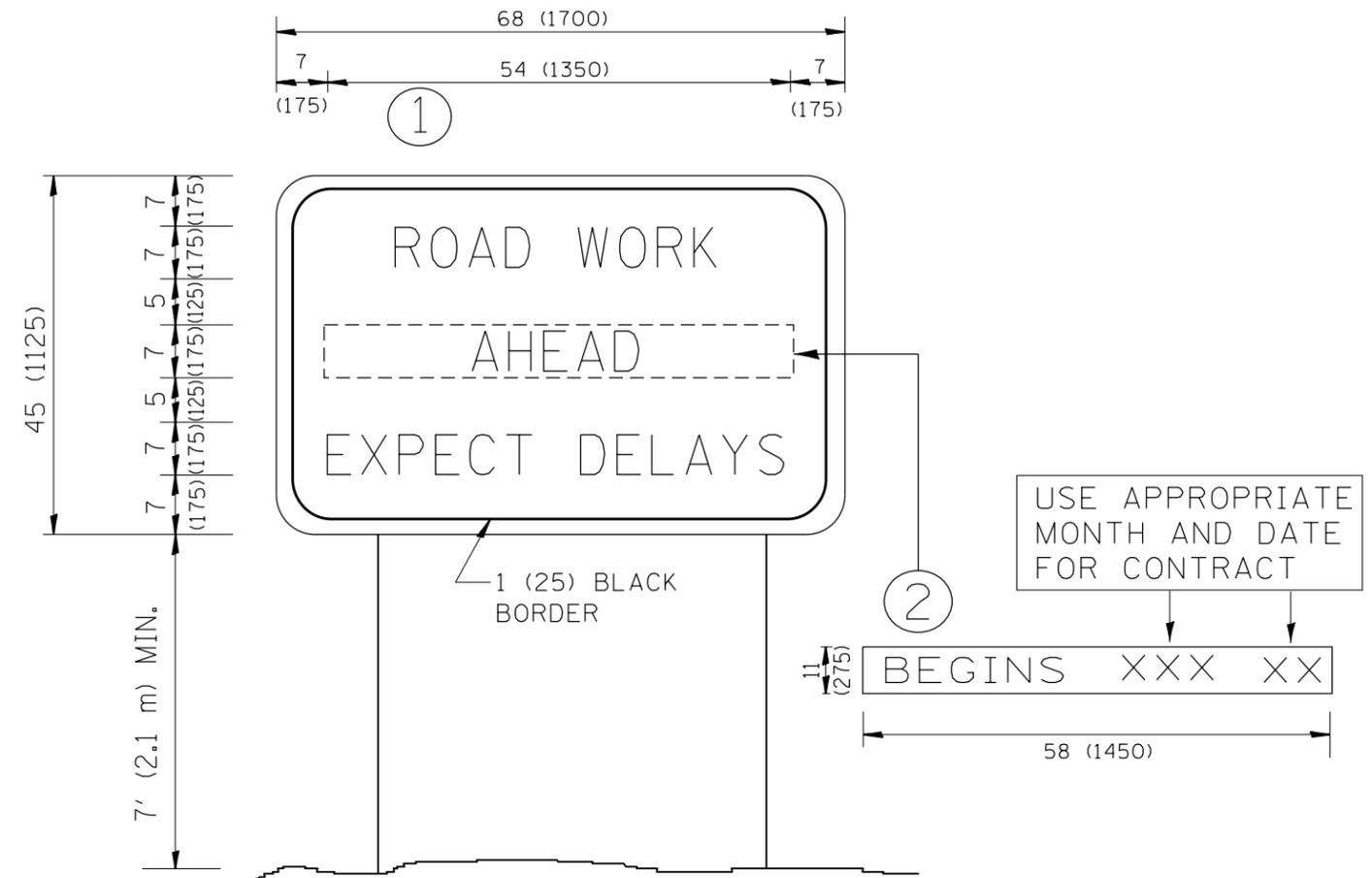
All dimensions are in inches (millimeters)  
 unless otherwise shown.

FILE NAME =	USER NAME = Bilgrimes	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\1022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	39
<b>TC-16</b>		<b>CONTRACT NO. 62F62</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

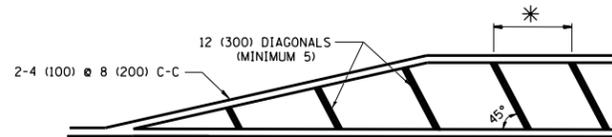
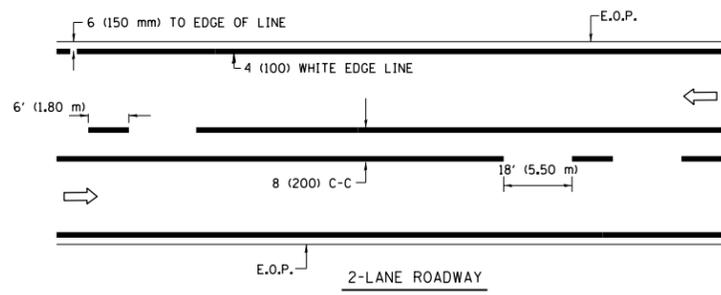


**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

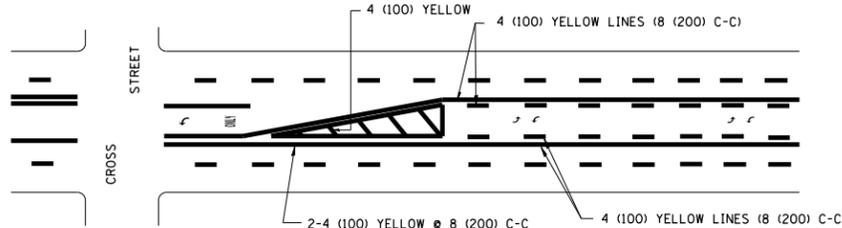
FILE NAME =	USER NAME = Bilgramisa	DESIGNED -	REVISED - R. MIRS 09-15-97	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARTERIAL ROAD INFORMATION SIGN</b>	F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\l\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI022018\Design\DistStd.dgn	DRAWN	REVISED - R. MIRS 12-11-97	1548			3290RS-1	COOK	46	40	
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99	<b>TC-22</b>			<b>CONTRACT NO. 62F62</b>				
PLOT DATE = 1/26/2018	DATE -	REVISED - C. JUCIUS 01-31-07	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT



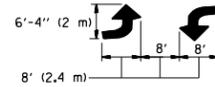
\* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

\* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

**PAINTED MEDIANS**

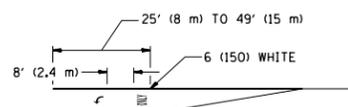
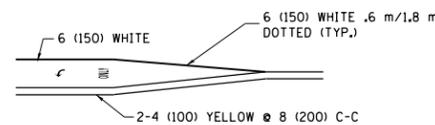


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

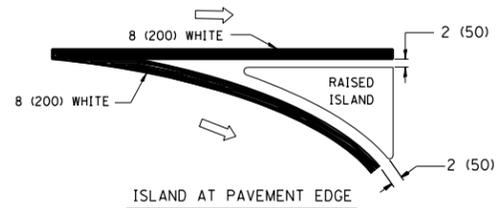
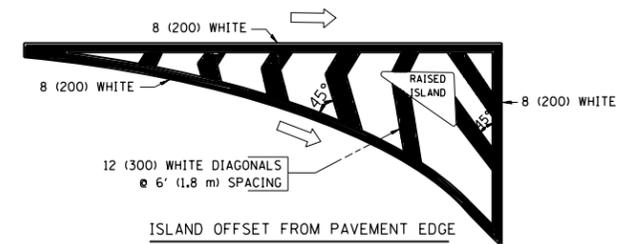
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.8 SQ. FT. (1.47 m<sup>2</sup>) ONLY AREA = 22.9 SQ. FT. (2.13 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

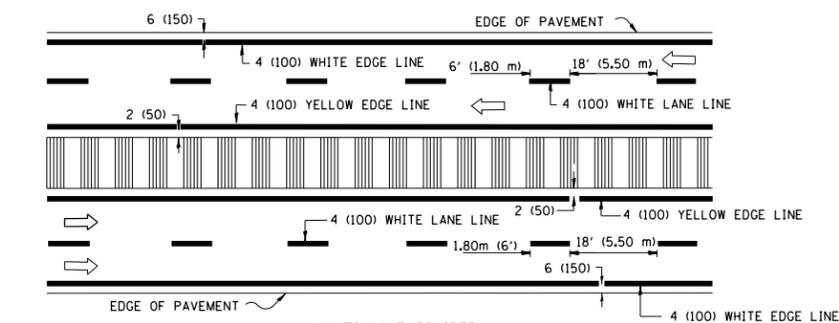
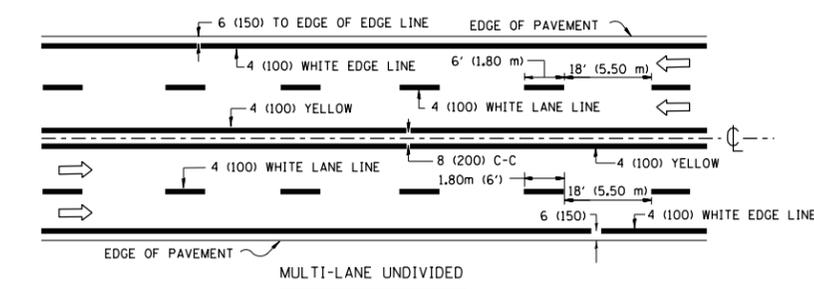


**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )

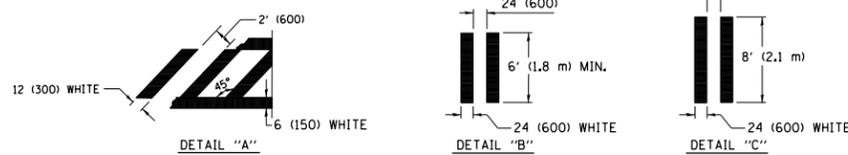
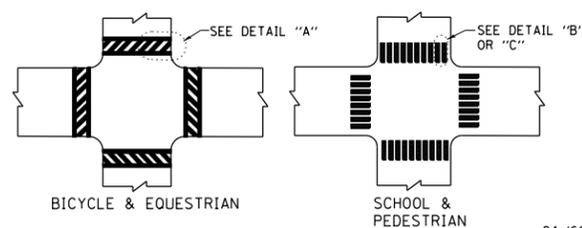
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**



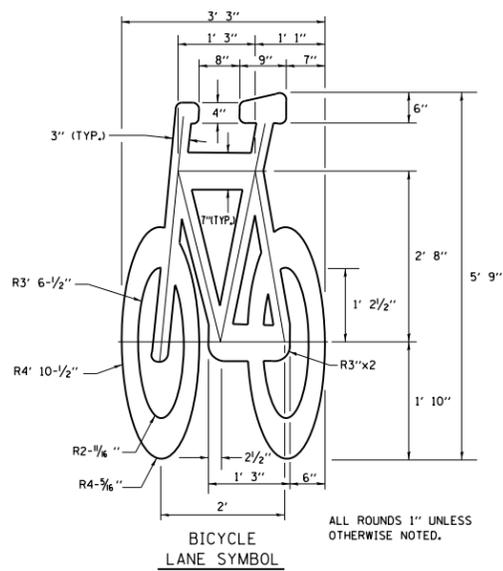
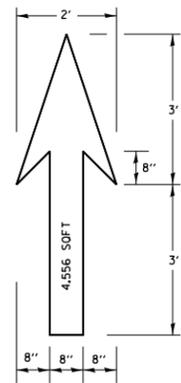
**TYPICAL CROSSWALK MARKING**

FILE NAME =	USER NAME = Bilgrimo	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
pw\11084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - K. ENG 02-28-12
		DATE -	REVISED -
			REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 3 SHEETS	STA.	TO STA.

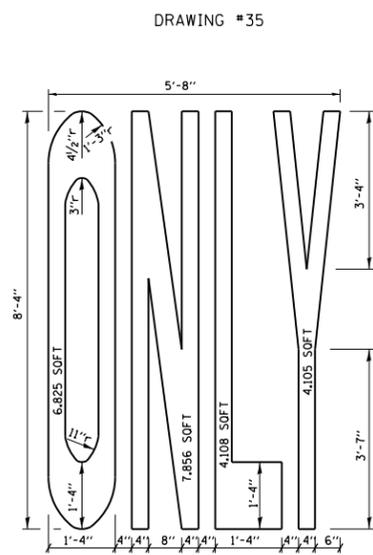
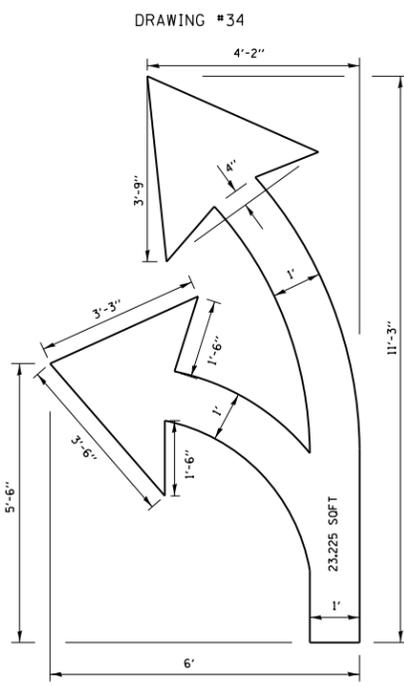
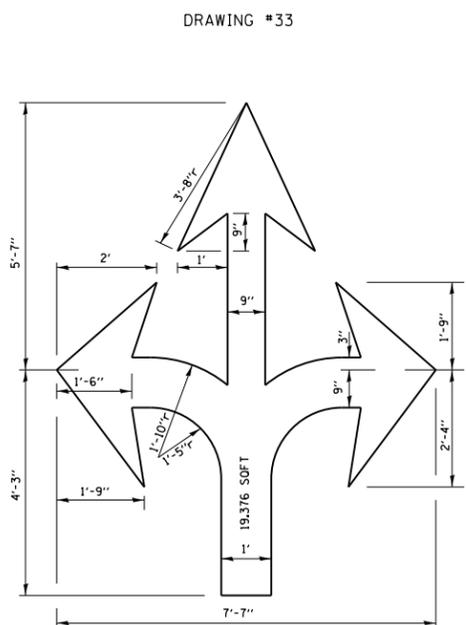
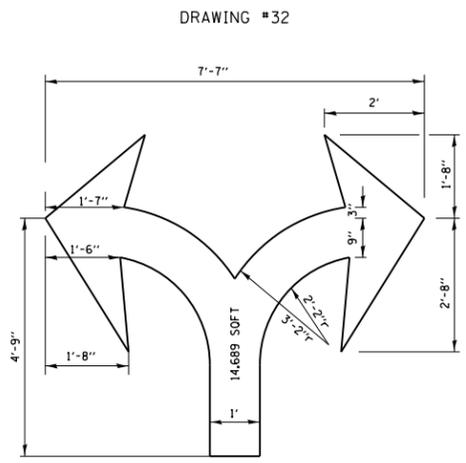
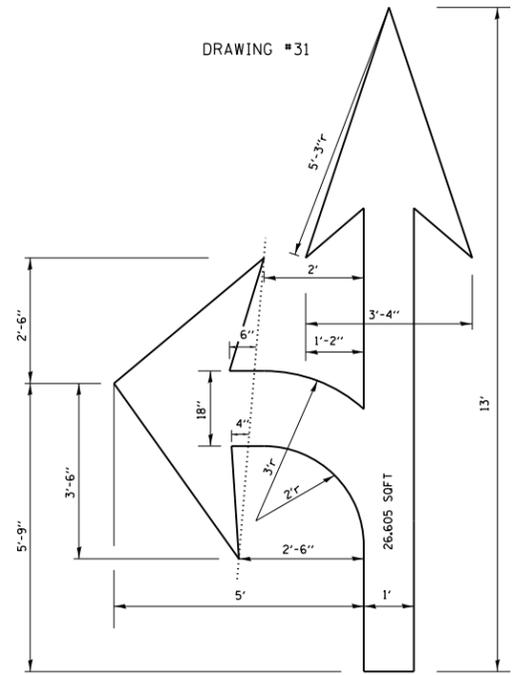
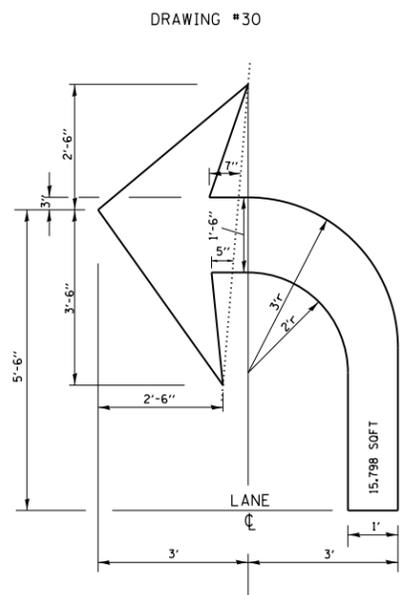
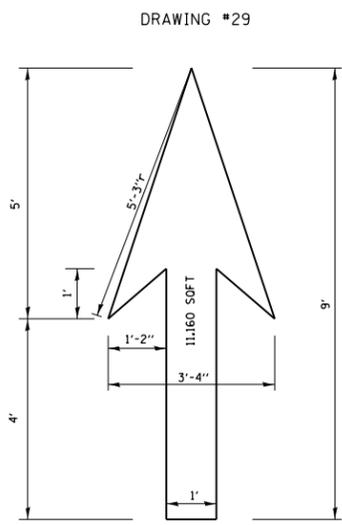
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	41
TC-24		CONTRACT NO. 62F62		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTE:**

- 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
- 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS**  
DRAWING #28



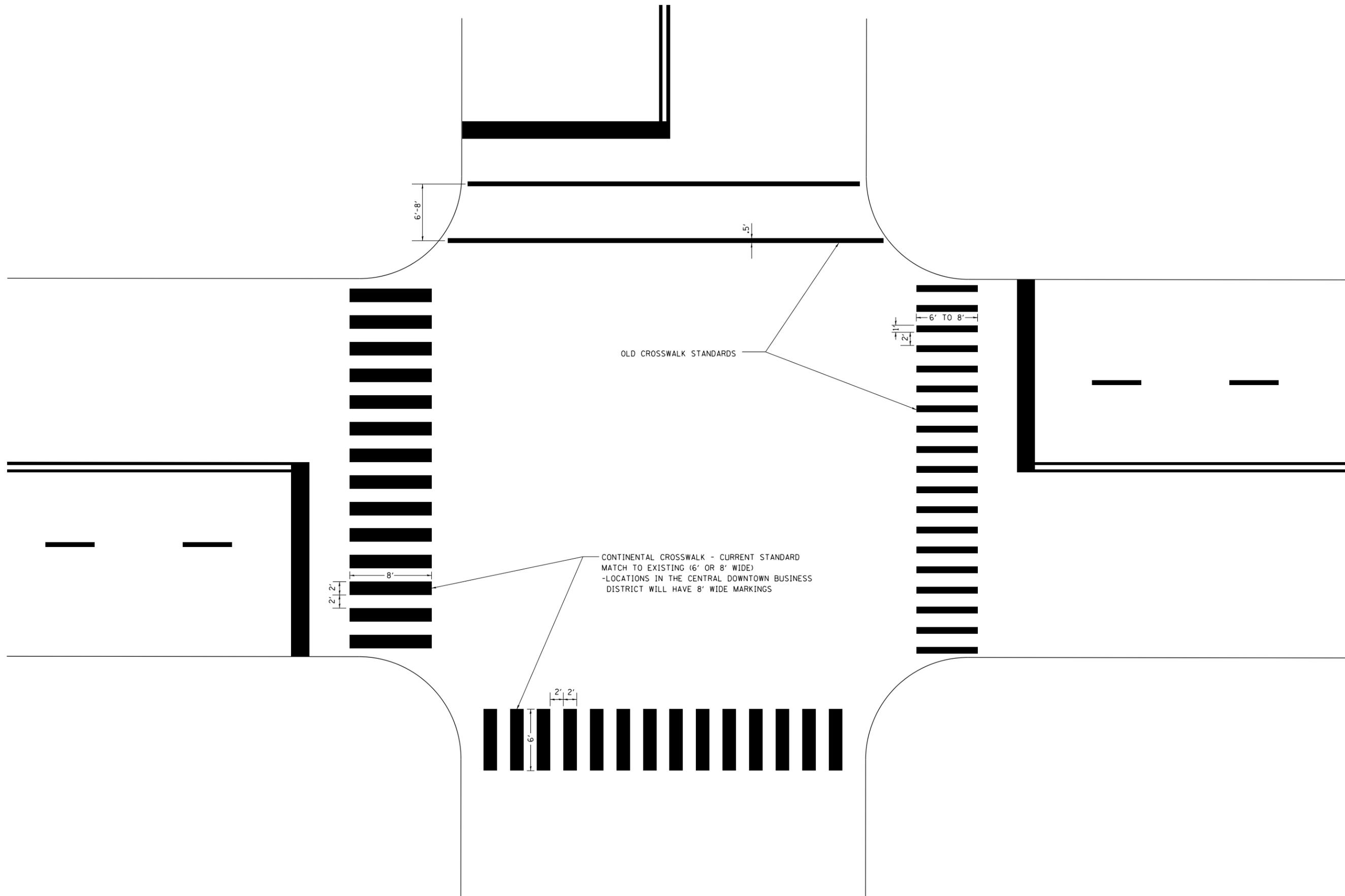
**NOTE:** ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

FILE NAME =	USER NAME = Bilgramis	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
p:\1\1084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - K. ENG 01-12-12
		DATE -	REVISED -
			REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 2 OF 3 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	42
<b>TC-24</b>		<b>CONTRACT NO. 62F62</b>		
<small>FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT</small>				

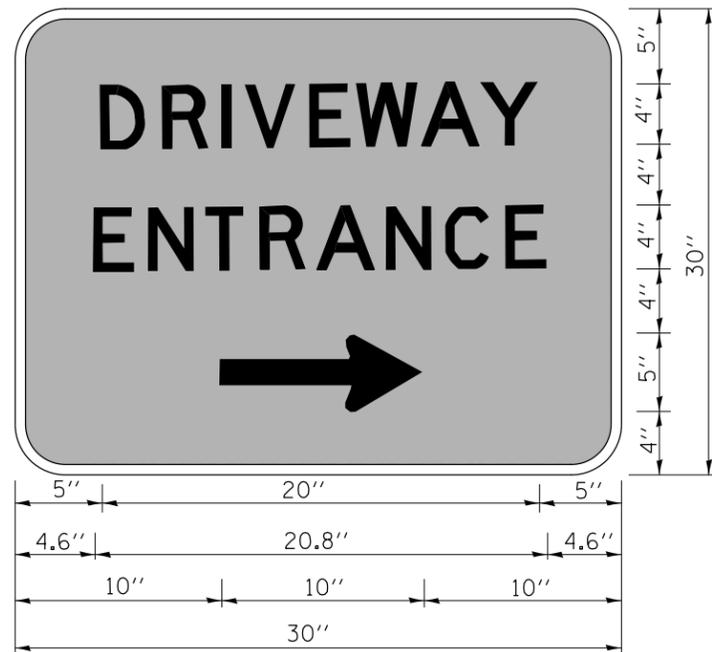


FILE NAME =	USER NAME = Bilgramisa	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
p:\11\084EBIDINTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\DI022018\Drawings\Design\DistStd.dgn			REVISED - K. ENG 01-12-12
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 1/26/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	43
<b>TC-24</b>			<b>CONTRACT NO. 62F62</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)  
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = Bilgramisa	DESIGNED -	REVISED - C. JUCIUS 02-15-07
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI022018\DWG\DistStd.dgn		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

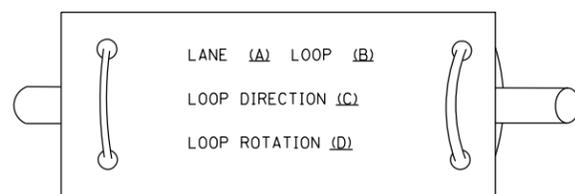
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1548	3290RS-1	COOK	46	44
TC-26			CONTRACT NO. 62F62	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

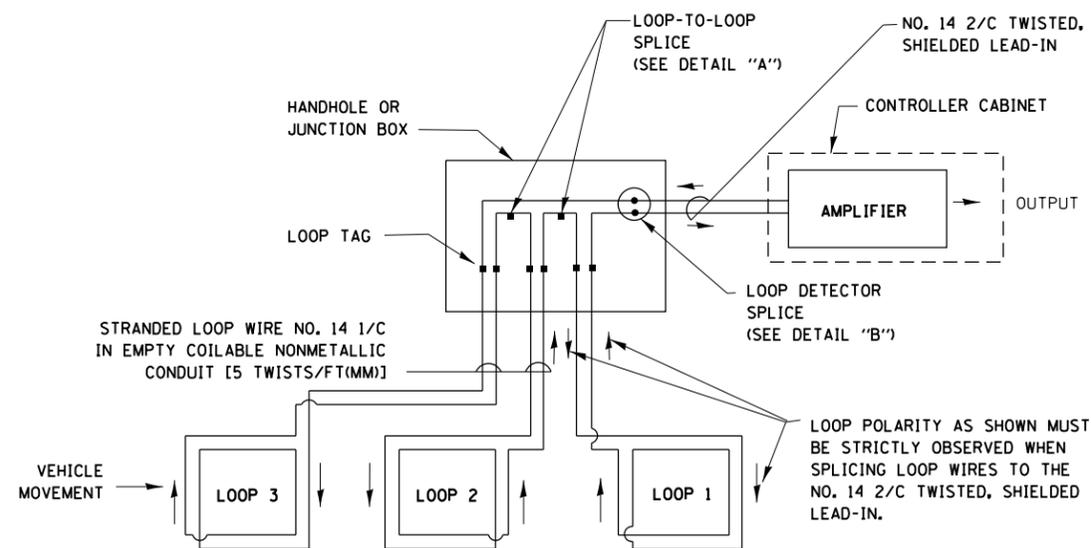
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

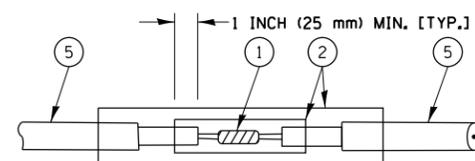


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

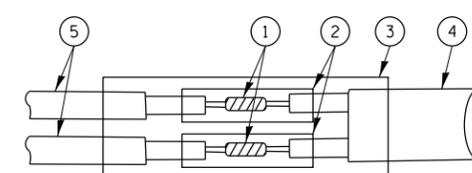


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

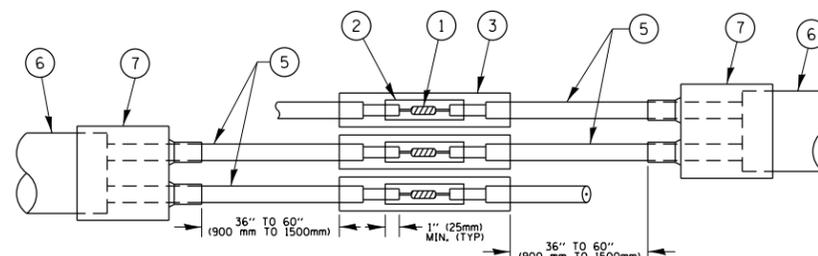


DETAIL "A"  
LOOP-TO-LOOP SPLICE

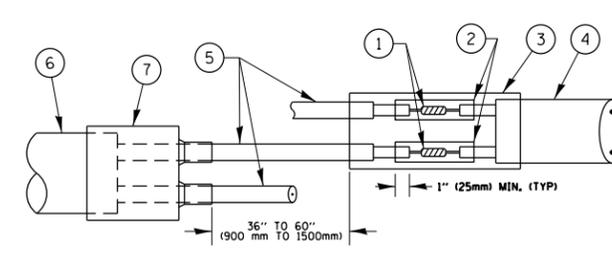


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

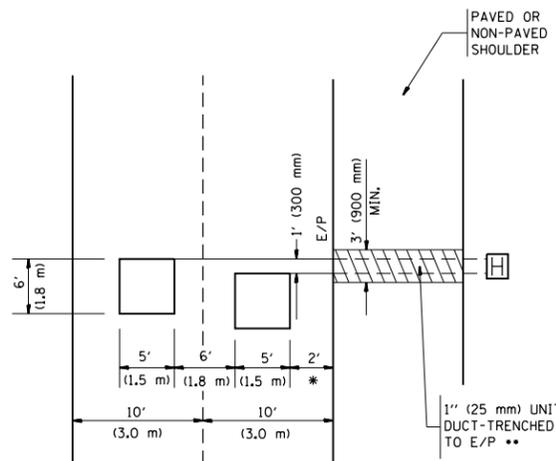
**PRE-FORMED LOOP**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



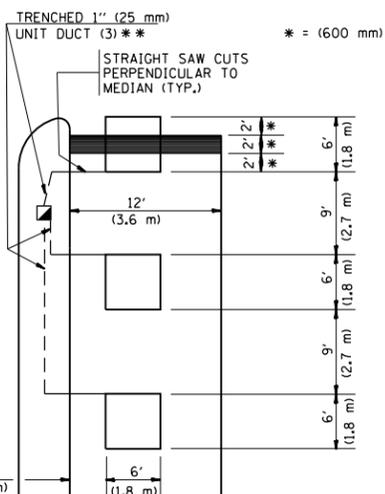
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



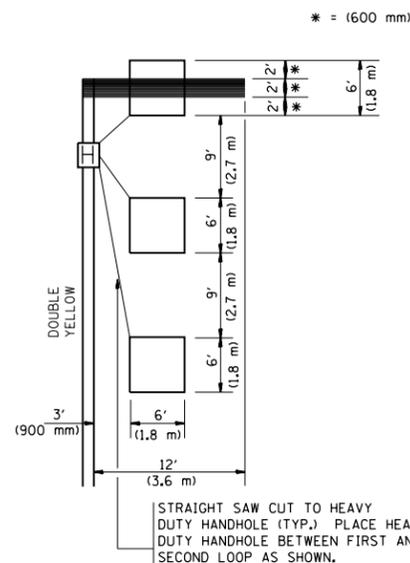
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

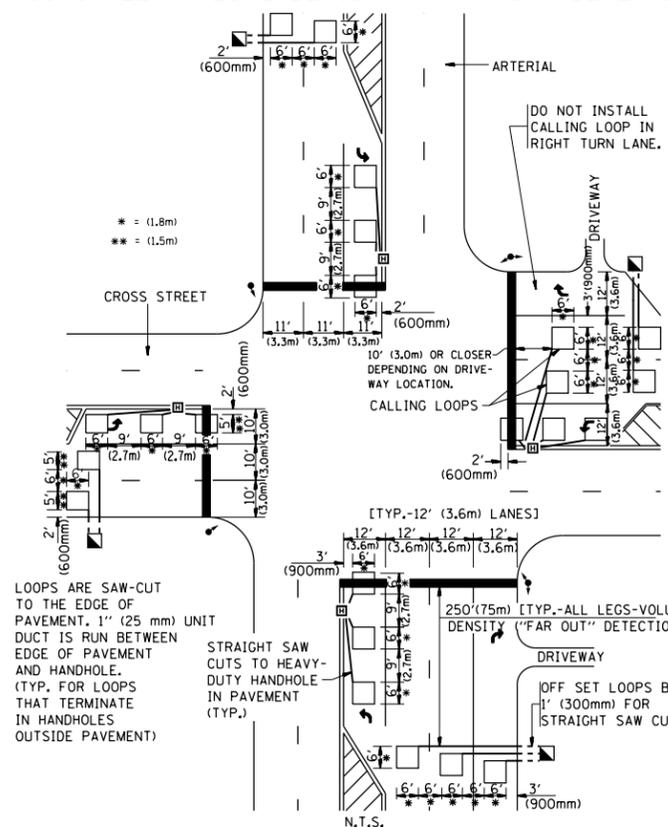
(PROTECTED / PERMITTED LEFT TURN PHASING)



\* = (600 mm)

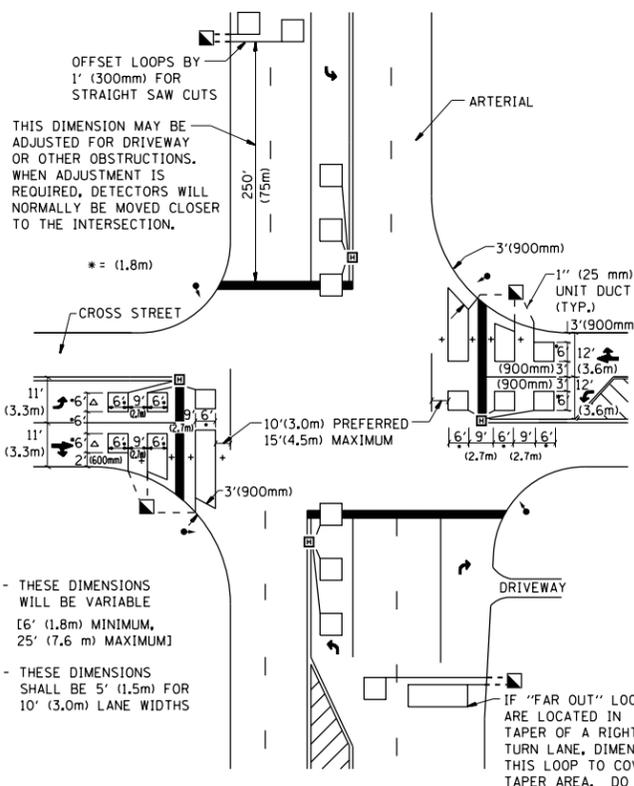
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = Bilgrimesa	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\illinois.gov\PIW001\Documents\IDOT Offices\District 1\Projects\DI022\Drawings\Design\DistStd.dgn		DRAWN -	REVISED -		1548	3290RS-1	COOK	46	46			
PLOT SCALE = 100.0000' / in.		CHECKED - R.K.F.	REVISED -		<b>TS-07</b>			<b>CONTRACT NO. 62F62</b>				
PLOT DATE = 1/26/2018		DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			