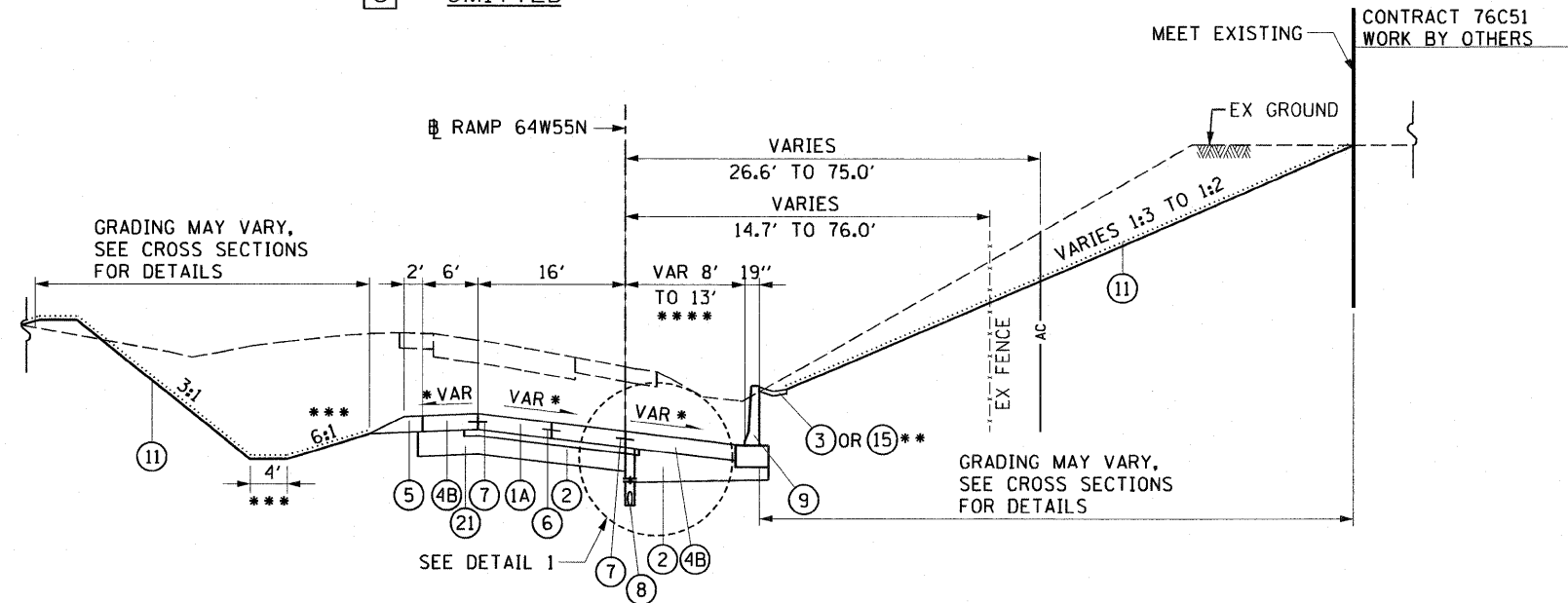
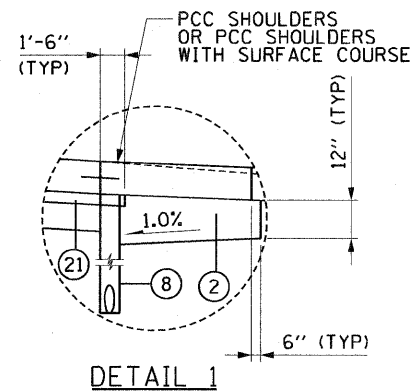


8 - OMITTED



9 - PROPOSED RAMP 64W55N
 STA 60+25.00 TO STA 64+39.83
 FOR SECTIONS OF RAMP 64W55N
 PRIOR TO STA 60+25.00,
 SEE I-64 TYPICAL SECTIONS 12 TO 18

- * SEE SE TRANSITION DATA TABLE FOR SUPERELEVATION TRANSITIONS
- ** BARRIER WALL WITH CONCRETE GUTTER TYPE A FROM STA 61+00.00 TO 61+60.00
- ** BARRIER WALL WITH 3' WIDE STONE RIPRAP, CLASS A4 FROM STA 61+60.00 TO 62+70.00
- ** BARRIER WALL WITH CONCRETE GUTTER TYPE A FROM STA 62+70.00 TO 63+90.00
- *** DITCH FROM STA 61+00.00 TO STA 61+50.00 THEN FORESLOPE VARIES FROM 1:6 MEETING FORESLOPE FROM NB I-55
- **** VARIES FROM 8' TO 13' STA 60+37.23 TO STA 61+00.00



I-55 TYPICAL SECTION NOTES:

1. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.
2. WHERE THE EXISTING OVERLAY IS LESS THAN 4", CONTRACTOR SHALL ONLY MILL TO THE TOP OF EXISTING CONCRETE BELOW THE OVERLAY. THIS CONDITION MAY BE ENCOUNTERED FROM STATION 68+14 TO 73+00 (SB I-55), BUT SHOULD BE FIELD VERIFIED.
3. WHERE THE EXISTING OVERLAY IS LESS THAN 4", THE CONTRACTOR SHALL OVERLAY TO THE PROPOSED ELEVATIONS SHOWN IN THE PLANS. IN ALL OTHER LOCATIONS, THE PROPOSED OVERLAY IS ESTIMATED AT 4" BUT SHALL BE BASED ON ELEVATIONS IN THE PLANS. SECTIONS WITH EXISTING NORMAL CROWNS SHOULD BE REESTABLISHED.

EXISTING LEGEND:

- | | |
|-------------------------------|--|
| (A) PCC PAVEMENT (REINFORCED) | (D) HMA SURFACE COURSE - 6" |
| (A1) - 9 1/2" AND VARIES | (E) TEMP HMA PAVEMENT - 11 1/4" |
| (A2) - 10" AND VARIES | (F) AGGREGATE BASE COURSE, TYPE A |
| (A3) - 10 1/4" AND VARIES | (F1) - 4" AND VARIES |
| (A4) - 10 1/2" AND VARIES | (F2) - 4 1/2" AND VARIES |
| (A5) - 10 3/4" AND VARIES | (F3) - 11 1/4" AND VARIES |
| (A6) - 12" AND VARIES | (F4) - 12" AND VARIES |
| (B) PCC SHOULDERS | (G) AGGREGATE SHOULDER, TYPE B 10 1/2" |
| (B1) - 10 1/2" AND VARIES | (H) AGGREGATE SHOULDER, TYPE B 18" |
| (B2) - 12" AND VARIES | (I) PIPE UNDERDRAIN |
| (B3) - 18" AND VARIES | (J) CONCRETE BARRIER |
| (C) HMA OVERLAY | (K) GUARDRAIL |
| (C1) - 2 1/4" AND VARIES | |
| (C2) - 3 1/2" AND VARIES | |
| (C3) - 3 3/4" AND VARIES | |
| (C4) - 5 1/2" AND VARIES | |
| (C5) - 6 3/4" AND VARIES | |
| (C6) - 10" AND VARIES | |

I-55 PROPOSED LEGEND:

- 1 PORTLAND CEMENT CONCRETE PAVEMENT
 - (1A) - 10 1/2" (JOINTED) (RAMPS)
 - (1B) - 12 1/2" (JOINTED) (NB I-55)
 - (1C) - 14" (JOINTED) (NB I-55)
- 2 AGGREGATE BASE COURSE, TYPE A - 12"
- 3 CONCRETE GUTTER, TYPE A
- 4 PORTLAND CEMENT CONCRETE SHOULDERS
 - (4A) - 10"
 - (4B) - 10 1/2"
 - (4C) - 12 1/2"
 - (4D) - 14"
- 5 AGGREGATE SHLDS, TYPE B - THICKNESS SPECIFIED IN SECTION
- 6 *6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / *6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- 7 *6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- 8 PIPE UNDERDRAINS - 6"
- 9 CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- 10 COMB CONCRETE CURB AND GUTTER, TYPE B-6.24 (MODIFIED)
- 11 SEEDING AND MULCHING (BY OTHERS)
- 12 NOT USED
- 13 POLYMERIZED HMA SC, SMA, N80 4"
- 14 POLYMERIZED HMA BC, SMA, N80 6"
- 15 STONE RIPRAP, CLASS A4 - 16" (ON 6" BEDDING) WITH FILTER FABRIC
- 16 CONCRETE BARRIER DOUBLE FACE, 42 INCH HEIGHT
- 17 STEEL PLATE BEAM GUARDRAIL TYPE A, 6 FOOT POSTS
- 18 BITUMINOUS MATERIALS (PRIME COAT)
- 19 CONTINUOUSLY REINFORCED PCC PAVEMENT - 12 1/2"
- 20 CONTINUOUSLY REINFORCED PCC PAVEMENT - 14"
- 21 STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"

RAMP 64W55N

STRUCTURAL DESIGN TRAFFIC:	YEAR	2030
PV= 2,254	SU= 141	MU= 423
ROAD/STREET CLASSIFICATION:	CLASS	1
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= 80%	S= 5%	M= 15%
TRAFFIC FACTOR:	ACTUAL TF= 6.30	AC TYPE= 20
	MINIMUM TF= 11.17	
PG GRADE:	BINDER= NA	SURFACE= NA
SUBGRADE SUPPORT RATING	SSR= POOR	

FILE NAME = DBT-i-76C52-sht-Typical.13.dgn

USER NAME = searsb
 PLOT SCALE = 20,000 / in.
 PLOT DATE = 3/1/2012

DESIGNED JWM
 DRAWN JWM
 CHECKED DBM
 DATE 03-01-10

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS - I-55

SCALE: NONE SHEET NO. 14 OF 19 SHEETS STA. 54+39.64 TO STA. 64+38.91

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
• 64/998/70	82-1-R(A), 82-1-R(B)	ST. CLAIR	629	28
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 76C52	