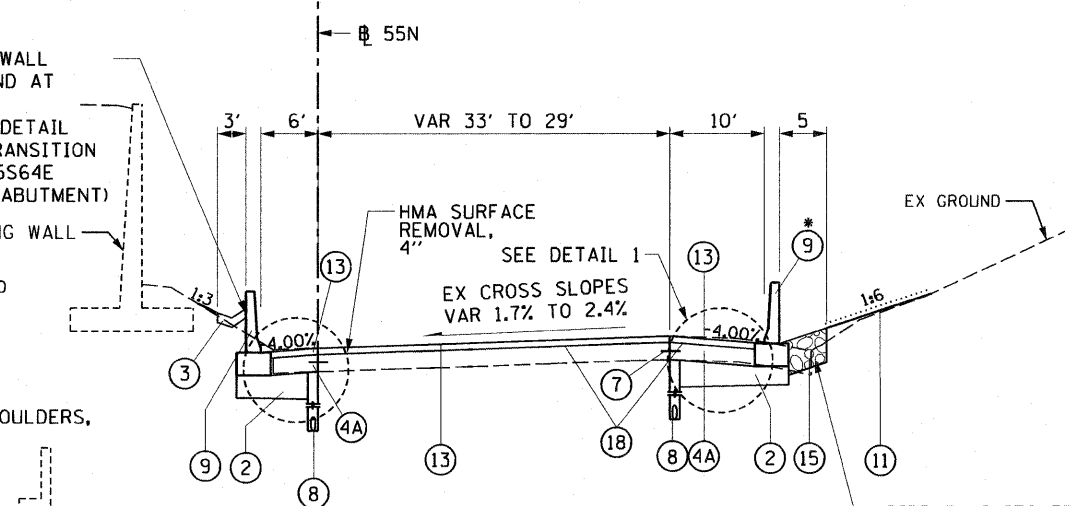


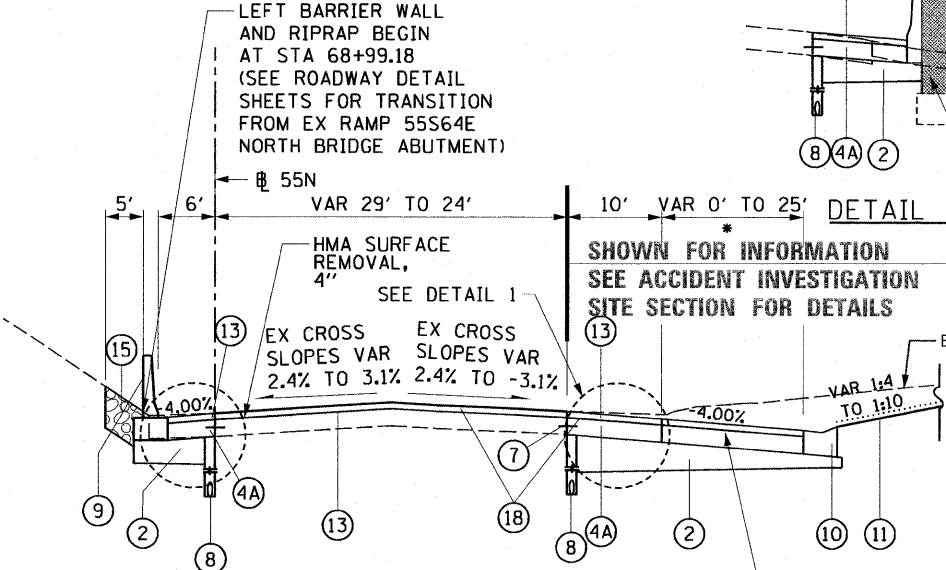
1A - PROPOSED NB I-55
STA 64+50.00 TO STA 67+12.88

*CONC BAR 1F 42HT SPL BEGINS AT STA 63+50.16



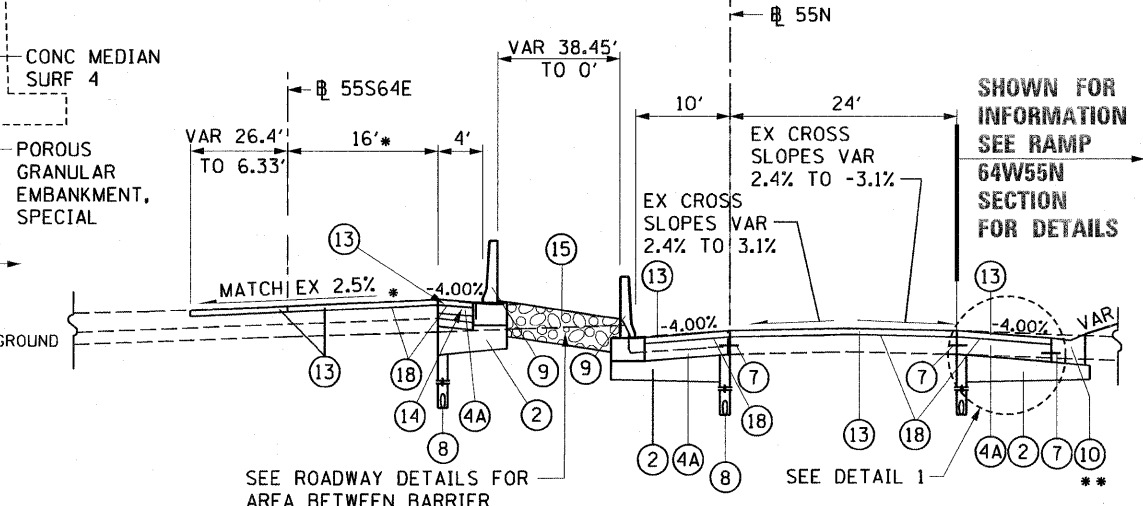
1A - PROPOSED NB I-55
STA 67+12.88 TO STA 69+57.15

*SEE DETAIL C FOR SECTION THROUGH EXISTING RAMP 55S64E BRIDGE ABUTMENT AND ALSO SEE ROADWAY DETAILS



1B - PROPOSED NB I-55
STA 69+57.15 TO STA 73+25.90

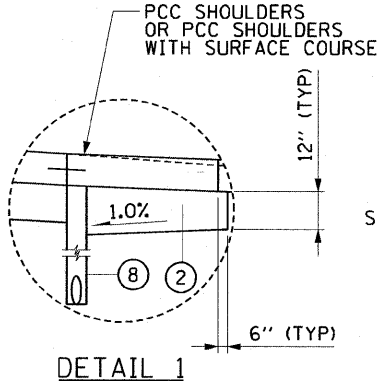
*ACCIDENT INVESTIGATION SITE FROM STA 70+75.93 TO STA 73+25.90



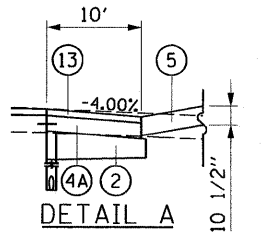
2 - PROPOSED NB I-55
STA 73+25.90 TO STA 76+04.66

* RAMP 55S64E RESURFACING BEGINS AT STA 12+93.86 AND ENDS AT STA 14+07.89 (55S64E @)
55S64E SHOULDER RECONSTRUCTION EXTENDS TO 16+00.00 (55S64E @)
** COMB CC&G TB6.24 (DEPRESSED) ENDS AT STA 73+75.20, ALSO SEE DETAILS A & B AND SEE TYPICAL *10c FOR ACCIDENT INVESTIGATION SITE DETAIL

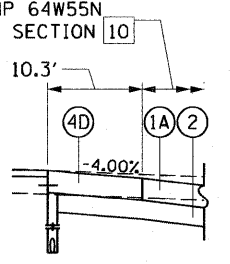
SHOWN FOR INFORMATION SEE RAMP 64W55N SECTION FOR DETAILS



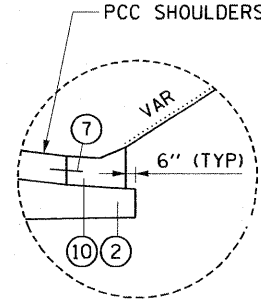
DETAIL 1



DETAIL A



DETAIL B



DETAIL 2

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	AIR VOIDS
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, SMA, N80	SBS 76-22	4% @ 80 Gyr
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, SMA, N80	SBS 76-22	4% @ 80 Gyr

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN

EXISTING LEGEND:

- (A) PCC PAVEMENT (REINFORCED)
 - (A1) - 9 1/2" AND VARIES
 - (A2) - 10" AND VARIES
 - (A3) - 10 1/4" AND VARIES
 - (A4) - 10 1/2" AND VARIES
 - (A5) - 10 3/4" AND VARIES
 - (A6) - 12" AND VARIES
- (B) PCC SHOULDERS
 - (B1) - 10 1/2" AND VARIES
 - (B2) - 12" AND VARIES
 - (B3) - 18" AND VARIES
- (C) HMA OVERLAY
 - (C1) - 2 1/4" AND VARIES
 - (C2) - 3 1/2" AND VARIES
 - (C3) - 3 3/4" AND VARIES
 - (C4) - 5 1/2" AND VARIES
 - (C5) - 6 3/4" AND VARIES
 - (C6) - 10" AND VARIES
- (D) HMA SURFACE COURSE - 6"
- (E) TEMP HMA PAVEMENT - 11 1/4"
- (F) AGGREGATE BASE COURSE, TYPE A
 - (F1) - 4" AND VARIES
 - (F2) - 4 1/2" AND VARIES
 - (F3) - 11 1/4" AND VARIES
 - (F4) - 12" AND VARIES
- (G) AGGREGATE SHOULDER, TYPE B 10 1/2"
- (H) AGGREGATE SHOULDER, TYPE B 18"
- (I) PIPE UNDERDRAIN
- (J) CONCRETE BARRIER
- (K) GUARDRAIL

I-55 TYPICAL SECTION NOTES:

1. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT IS BETWEEN 0% AND 4% THE SHOULDER SHALL BE SLOPED AT 4%. WHEN THE SUPERELEVATION RATE OF THE PAVEMENT EXCEEDS 4% THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER SLOPES WILL NOT BE GREATER THAN 8%.

I-55 PROPOSED LEGEND:

- (1) PORTLAND CEMENT CONCRETE PAVEMENT
 - (1A) - 10 1/2" (JOINTED) (RAMPS)
 - (1B) - 12 1/2" (JOINTED) (NB I-55)
 - (1C) - 14" (JOINTED) (NB I-55)
- (2) AGGREGATE BASE COURSE, TYPE A - 12"
- (3) CONCRETE GUTTER, TYPE A
- (4) PORTLAND CEMENT CONCRETE SHOULDERS
 - (4A) - 10"
 - (4B) - 10 1/2"
 - (4C) - 12 1/2"
 - (4D) - 14"
- (5) AGGREGATE SHLDS, TYPE B - THICKNESS SPECIFIED IN SECTION
- (6) *6 TIE BARS, 30" LONG AT 30" C-C (IF LONGITUDINAL SAWED JOINT) / *6 TIE BARS, 24" LONG AT 24" C-C (IF LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (7) *6 TIE BARS, 24" LONG AT 24" C-C (INCLUDED IN PRICE FOR BID FOR VARIOUS PCC ITEMS)
- (8) PIPE UNDERDRAINS - 6"
- (9) CONCRETE BARRIER SINGLE FACE, 42 INCH HEIGHT (SPECIAL)
- (10) COMB CONCRETE CURB AND GUTTER, TYPE B-6.24 (MODIFIED)
- (11) SEEDING AND MULCHING (BY OTHERS)
- (12) NOT USED
- (13) POLYMERIZED HMA SC, SMA, N80 4"
- (14) POLYMERIZED HMA BC, SMA, N80 6"
- (15) STONE RIPRAP, CLASS A4 - 16" (ON 6" BEDDING) WITH FILTER FABRIC
- (16) CONCRETE BARRIER DOUBLE FACE, 42 INCH HEIGHT
- (17) STEEL PLATE BEAM GUARDRAIL TYPE A, 6 FOOT POSTS
- (18) BITUMINOUS MATERIALS (PRIME COAT)
- (19) CONTINUOUSLY REINFORCED PCC PAVEMENT - 12 1/2"
- (20) CONTINUOUSLY REINFORCED PCC PAVEMENT - 14"
- (21) STABILIZED SUBBASE - HOT-MIX ASPHALT, 4"

I-55 TYPICAL SECTION NOTES CONTINUED:

2. WHERE THE EXISTING OVERLAY IS LESS THAN 4", THE CONTRACTOR SHALL ONLY MILL TO THE TOP OF EXISTING CONCRETE BELOW THE OVERLAY. THIS CONDITION MAY BE ENCOUNTERED FROM STATION 68+14 TO 73+00 (SB I-55 @), BUT SHOULD BE FIELD VERIFIED.

3. WHERE THE EXISTING OVERLAY IS LESS THAN 4", THE CONTRACTOR SHALL OVERLAY TO THE PROPOSED ELEVATIONS SHOWN IN THE PLANS. IN ALL OTHER LOCATIONS, THE PROPOSED OVERLAY IS ESTIMATED AT 4" BUT SHALL BE BASED ON ELEVATIONS IN THE PLANS. SECTIONS WITH EXISTING NORMAL CROWNS SHOULD BE REESTABLISHED.

FILE NAME = D8T1-76C52-sht-Typical_110.dgn

USER NAME = searsb
DESIGNED JWM
DRAWN JWM
CHECKED DBM
DATE 03-01-12

REVISIONS:
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS - I-55

SCALE: NONE SHEET NO. 11 OF 19 SHEETS STA. TO STA.

F.A.I. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
• 82-1-R(A), 82-1-R(B) ST. CLAIR 629 25
• 64/998/70 CONTRACT NO. 76C52
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT