

WAUKEGAN PORT DISTRICT WAUKEGAN, LAKE COUNTY, ILLINOIS

CONSTRUCTION PLANS FOR WAUKEGAN NATIONAL AIRPORT

REHABILITATE MAIN PARKING LOT

ILLINOIS PROJECT: UGN-5119
 S.B.G. PROJECT: 3-17-SBGP-TBD

MARCH 1, 2024



811 Know what's below.
 Call before you dig.
 J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

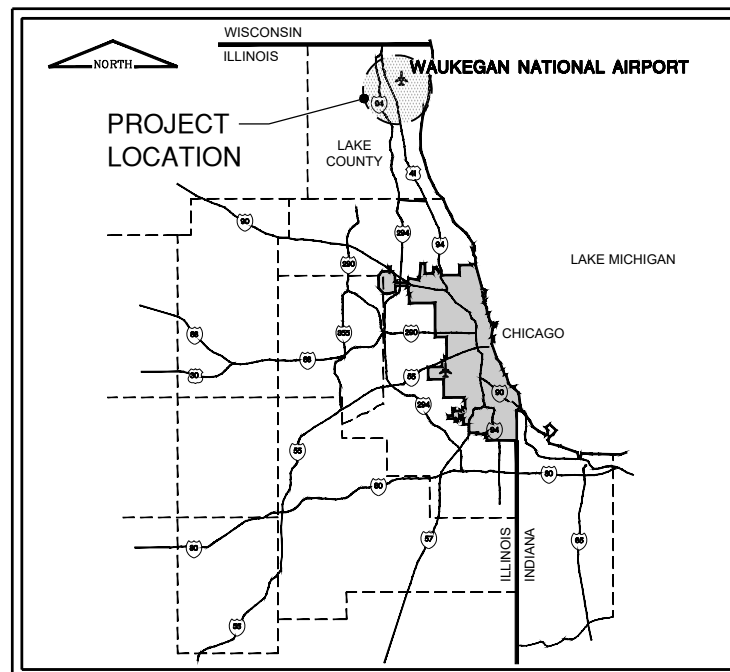
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS. OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

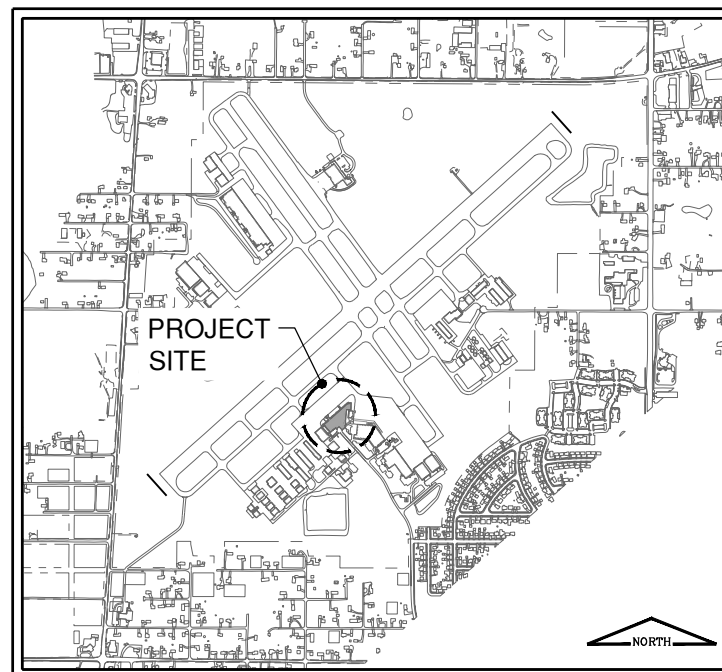


WAUKEGAN NATIONAL AIRPORT
 WAUKEGAN PORT DISTRICT
 2601 Plane Rest Drive
 Waukegan, Illinois 60087
 Telephone: 847.244.0055
 Fax: 847.244.3813

APPROVED BY
 SKIP GOSS, GENERAL MANAGER
 DATE MARCH 1 2024



LOCATION MAP



SITE PLAN

CALL J.U.L.I.E. BEFORE
 EXCAVATING AT 811
 WAUKEGAN NATIONAL AIRPORT

TOWNSHIP: 46 NORTH
 RANGE: 12 EAST
 SECTION: 31 AND 32
 COUNTY: LAKE
 TOWNSHIP: BENTON



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22004938.00

SUBMITTED BY
 CRAIG LOUDEN, P.E.
 DATE MARCH 1 2024



CONSULTANTS

IL PROJECT: UGN-5119
IL LETTING ITEM: 07A
IL CONTRACT: WA080

REHABILITATE MAIN
PARKING LOT

FINAL
MARCH 1, 2024

OWNER



MARK | DATE | DESCRIPTION

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**INDEX TO SHEETS/
SUMMARY OF QUANTITIES**

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SUMMARY OF QUANTITIES

SUMMARY OF QUANTITIES				
BASE BID				
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1	
AR150520	MOBILIZATION	L SUM	1	
AR152511	SUBGRADE REPAIR	SQ YD	270	
AR156520	INLET PROTECTION	EACH	1	
AR201663	SAND MIX CRACK REPAIR	FOOT	575	
AR401610	BITUMINOUS SURFACE COURSE	TON	615	
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	5,260	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	520	
AR501600	PCC SIDEWALK	SQ FT	190	
AR501690	PCC SIDEWALK REMOVAL	SQ FT	190	
AR603510	BITUMINOUS TACK COAT	GALLON	635	
AR620520	PAVEMENT MARKING-WATERBORNE	SQ FT	1,655	
AR620900	PAVEMENT MARKING REMOVAL	SQ FT	40	
AR754900	REMOVE CONCRETE CURB	FOOT	145	
AR754920	REPLACE CONCRETE CURB	FOOT	145	
ADDITIVE ALTERNATE #1				
ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AS110202	2" PVC DUCT - DIRECT BURY	FOOT	20	
AS110900	REMOVE DUCT	FOOT	20	
AS401916	REM & REP BIT PAVEMENT - TYPE B	SQ YD	24	
AS800082	2-1/C #10 XLP-USE, 1/C #10 GND	FOOT	575	
AS800083	2-1/C #8 XLP-USE, 1/C #10 GND	FOOT	875	
AS910100	ROADWAY LIGHT POLE	EACH	6	
AS910900	REMOVE ROADWAY LIGHT POLE	EACH	6	



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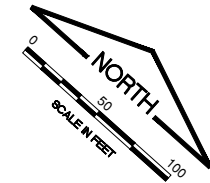


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SITE PLAN
AND PROJECT
CONTROL PLAN



UGN-E

APRON
(PCC)

STA. 19+08.87
N: 2095200.958
E: 1109691.903

19+00

CP-2

CP-3

TURF

18+00

17+00

HANGAR

ELEC
VAULT

HANGAR

TURF

EXISTING AUTO
PARKING LOT

TURF

ACCESS
DRIVE
(PCC)

HANGAR

HANGAR

16+00

15+00

14+00

13+00

12+00

McAREE ROAD

STA. 12+00.00
N: 2094673.133
E: 1110165.079

AIR TRAFFIC CONTROL TOWER

UGN
OFFICES

LEGEND

- PROPOSED WORK LIMITS
- EXISTING BUILDING
- CONTROL POINT
- EXISTING FENCE

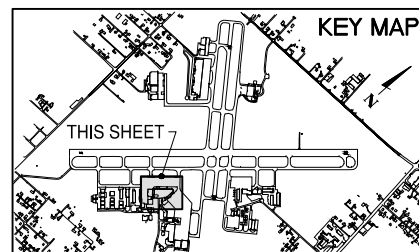
NOTES

1. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS/HER HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
3. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO THE CONTRACT.
4. THE CONTRACTOR WILL BE REQUIRED TO FURNISH AND PLACE CONSTRUCTION LAYOUT STAKES FOR THIS PROJECT. CONSTRUCTION LAYOUT SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM FOR WHICH THE LAYOUT IS REQUIRED.

CONTROL POINT TABLE
(ILLINOIS STATE PLANE, NAD 83 / NAVD 88)

POINT	DESCRIPTION	NORTHING	EASTING	ELEVATION
CP-1	IRON ROD IN TURF	2095116.747	1109696.413	-
CP-3	CROSS CUT ON CURB	2095293.275	1109912.215	-
UGN-E	PACS	2095587.450	1109729.570	705.070

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ARCHITECT/ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL, RELOCATION OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ARCHITECT/ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.



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IL CONTRACT: WA080

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PARKING LOT

FINAL
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**CONSTRUCTION
ACTIVITY PLAN**

LEGEND

- WORK AREA LIMITS
- CONTRACTOR'S STAGING AND STORAGE AREA
- IDOT TYPE II BARRICADES
- IDOT TYPE III BARRICADES
- LOW PROFILE BARRICADES
- CRITICAL PROJECT/AIRSPACE POINTS
- CONTRACTOR'S HAUL ROUTE
- AIRCRAFT MOVEMENT AREA
- "NO CONSTRUCTION TRAFFIC" SIGN
- STOP SIGN

CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE

SUGGESTED SEQUENCE OF CONSTRUCTION (EACH WORK AREA)

- NOTIFY RESIDENT ENGINEER AND WAUKEGAN NATIONAL AIRPORT AT LEAST 7 BUSINESS DAYS PRIOR TO INITIAL START OF CONSTRUCTION.
- PLACE TRAFFIC CONTROL.
- LOCATE ALL UTILITIES.
- COMPLETE REMOVAL ITEMS, ADJUSTMENT ITEMS AND MILLING.
- COMPLETE PAVEMENT IMPROVEMENTS AND PAVEMENT MARKINGS (FIRST COAT).
- NOTIFY RESIDENT ENGINEER AND WAUKEGAN NATIONAL AIRPORT AT LEAST 3 BUSINESS DAYS PRIOR TO CHANGING TO NEXT PHASE/WORK AREA.
- CLEAN PAVEMENT.
- REMOVE BARRICADES AND OPEN PAVEMENT TO VEHICLES.
- COMPLETE PAVEMENT IMPROVEMENTS AND PAVEMENT MARKINGS (SECOND COAT).

NOTES

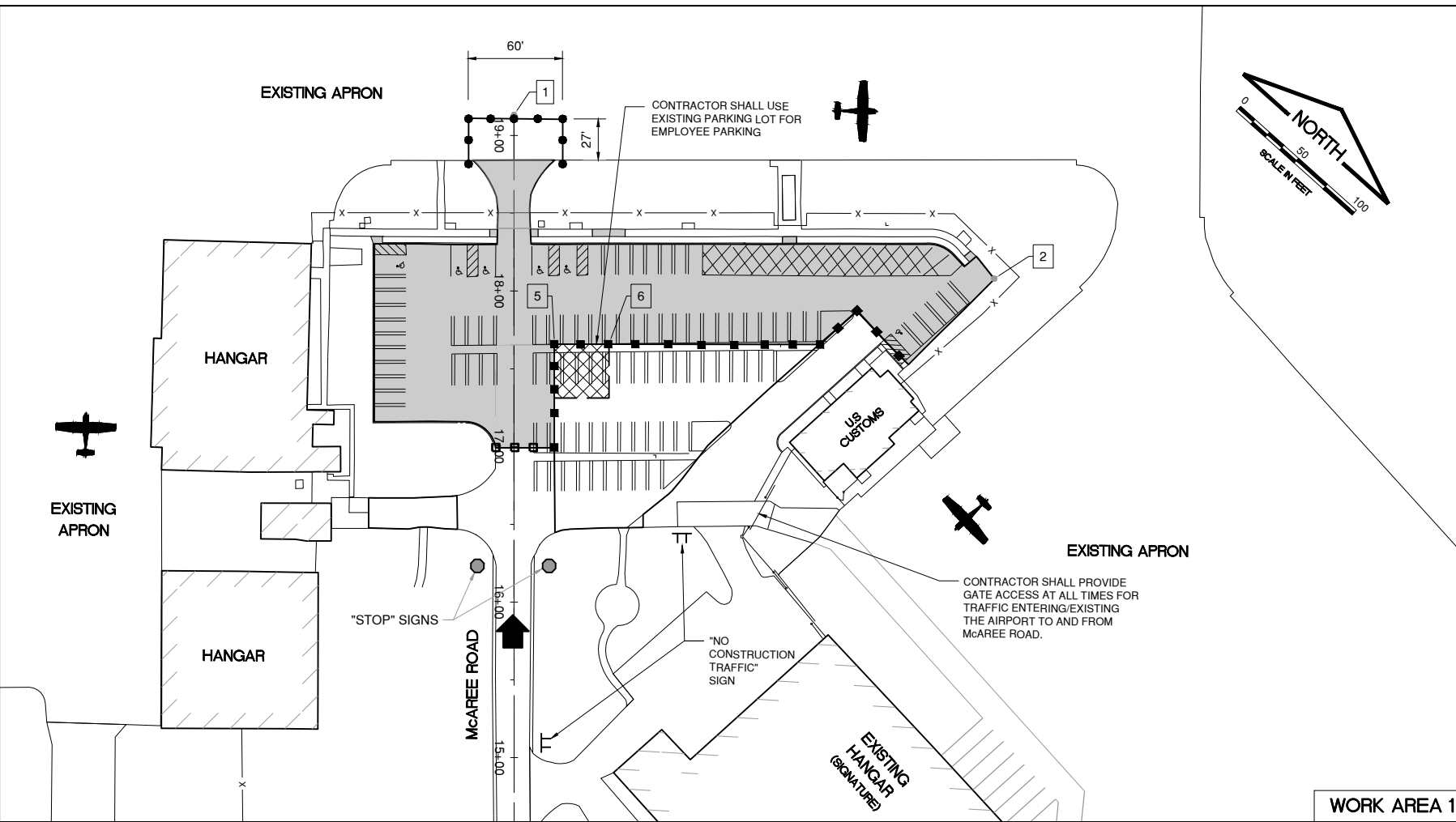
1. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING VEHICULAR ACCESS TO ALL AREAS AT ALL TIMES UNLESS OTHERWISE SHOWN. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED, HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN VEHICULAR ACCESS AND MUST BE APPROVED BY THE AIRPORT EXECUTIVE DIRECTOR AND RESIDENT ENGINEER.
2. CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL (BARRICADES, CONES, FLAGGERS, ETC.) AND PERSONNEL TO SAFELY DIRECT TRAFFIC FOR THE DURATION OF THIS WORK. COST OF PROVIDING TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.
3. ACCESS TO AND FROM McAREE ROAD SHALL BE AVAILABLE AT ALL TIMES. FLAGGERS SHALL DIRECT TRAFFIC DURING CONSTRUCTION OPERATION.
4. PHASE 1 AND 2 SHALL NOT BE COMPLETED AT THE SAME TIME.
5. FIRST COAT OF PAVEMENT MARKING SHALL BE APPLIED AT THE END OF EACH PHASE, IN ORDER TO OPEN PAVEMENT TO TRAFFIC. SECOND COAT OF FIRST PAVEMENT MARKING SHALL BE APPLIED AFTER A MINIMUM OF 30 DAYS FROM FINAL PAVING DATE.
6. CONTRACTOR TO MAINTAIN PEDESTRIAN ACCESS TO ALL BUILDINGS AT ALL TIMES.
7. WORK AREAS 1 AND 2 CANNOT BE COMBINED WITHOUT AUTHORIZATION FROM AIRPORT MANAGEMENT.
8. CONTRACTOR SHALL PROTECT OPEN EXCAVATIONS FROM PEDESTRIANS AT ALL TIMES.

CRITICAL PROJECT/AIRSPACE POINTS

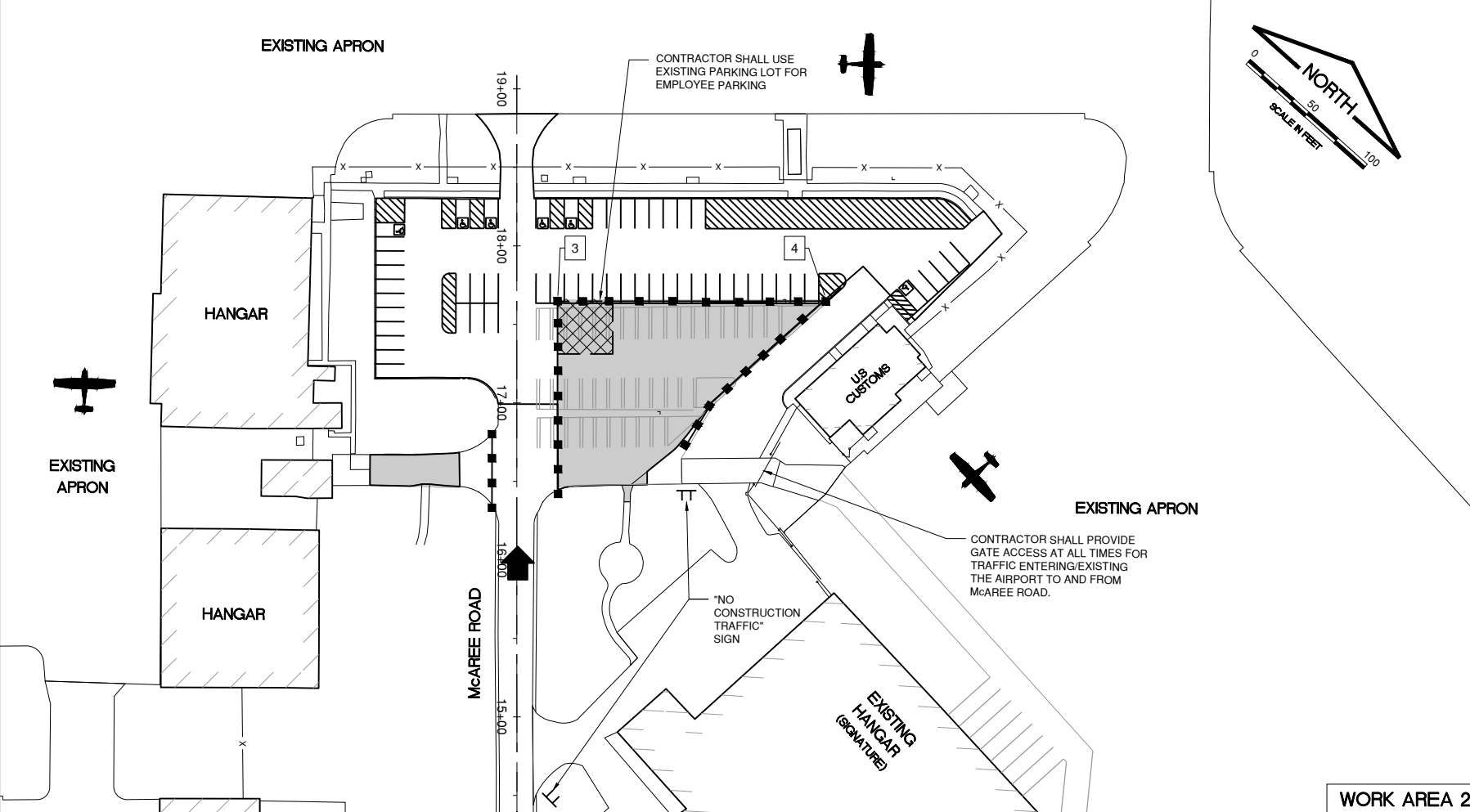
POINT	APPROXIMATE ELEVATION OF GROUND (FT)	ANTICIPATED EQUIPMENT AND HEIGHT	APPROXIMATE ELEVATION OF EQUIPMENT (FT)	LATITUDE	LONGITUDE
1	708	SEMI/DUMP TRUCK - 25'	733	42°25'04.22"	87°52'07.61"
2	708	SEMI/DUMP TRUCK - 25'	733	42°25'05.52"	87°52'03.66"
3	708	SEMI/DUMP TRUCK - 25'	733	42°25'03.08"	87°52'06.45"
4	708	SEMI/DUMP TRUCK - 25'	733	42°25'04.45"	87°52'04.38"
5	708	SEMI/DUMP TRUCK - 25'	733	42°25'03.34"	87°52'06.09"
6	708	SEMI/DUMP TRUCK - 25'	733	42°25'03.57"	87°52'05.74"

IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND PROTECT ALL AIRPORT-OWNED UTILITIES WITHIN THE CONSTRUCTION LIMITS. THIS INCLUDES ALL ELECTRICAL CABLES, STORM SEWER, DRAIN TILE, GAS, COMMUNICATIONS, PHONE, SANITARY SEWER AND WATERMAIN

THE CONTRACTOR SHALL BE IN CONFORMANCE WITH THE ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, STATE OF ILLINOIS, DEPARTMENT OF TRANSPORTATION, DIVISION OF AERONAUTICS, ADOPTED MARCH 22, 2023. GENERAL PROVISIONS SECTIONS 40-05 MAINTENANCE OF TRAFFIC, 70-14 OPENING SECTIONS OF THE WORK TO TRAFFIC, 70-16 CONTRACTORS RESPONSIBILITY FOR UTILITY SERVICE AND FACILITIES OF OTHERS, 70-25 CONTRACTORS RESPONSIBILITY FOR SAFETY DURING CONSTRUCTION AND 80-13 WORK AREA, STORAGE AREA AND SEQUENCE OF OPERATIONS.



WORK AREA 1



WORK AREA 2

Path: K:\Waukegan\AP\22004938.00_RehabMainParkingLot\Draw\Sheets\UGN-20222601 - Constr Activity Plan.dwg
Date: Wednesday, March 27, 2024 3:11:40 PM

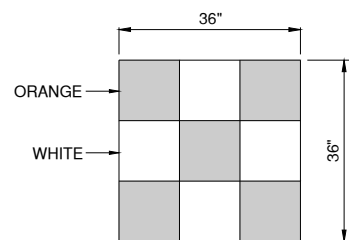
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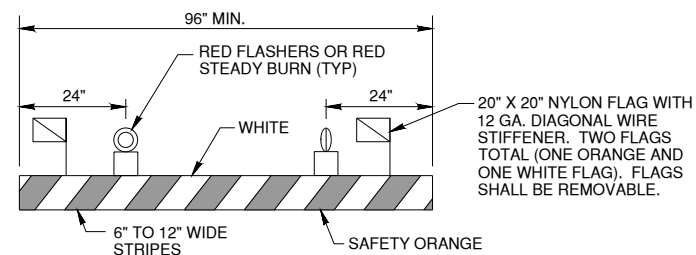
REHABILITATE MAIN PARKING LOT**FINAL
MARCH 1, 2024****OWNER**

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SHEET TITLE**CONSTRUCTION
ACTIVITY PLAN
DETAILS****CONSTRUCTION EQUIPMENT
AND TRUCK SIGNAL FLAG**

NOT TO SCALE

**PLAN VIEW****SIDE VIEW****AIRSIDE LOW PROFILE LIGHTED BARRICADE**

NOT TO SCALE

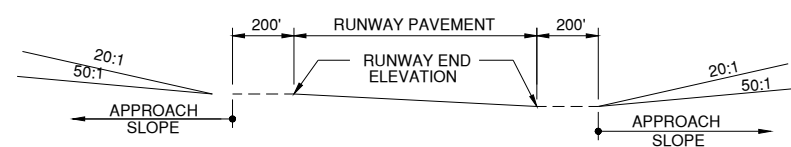
BARRICADE NOTES

- FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
- PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
- ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

GROUND CONTROL FREQUENCY: 121.65 MHz

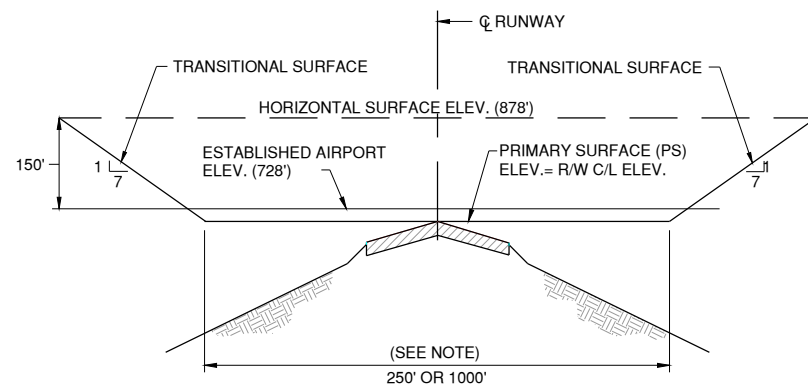
MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: DUMP TRUCK IN DUMP POSITION - 25'

IN THE EVENT THE CONTRACTOR PROPOSES TO UTILIZE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN WHAT IS LISTED, THE CONTRACTOR WILL BE RESPONSIBLE TO SUBMIT FAA FORM 7460-1 FOR AIRSPACE APPROVAL. THE RESIDENT ENGINEER WILL PROVIDE BASE AIRPORT INFORMATION FOR THE CONTRACTOR'S USE.

**TYPICAL PROFILE
F.A.R. PART 77 IMAGINARY SURFACES**

NO SCALE

RUNWAY END	ELEVATION	APPROACH SLOPE
5	724.7	34:1
23	723.1	50:1
14	727.6	20:1
32	712.1	20:1

**TYPICAL SECTION
F.A.R. PART 77 IMAGINARY SURFACES**

NO SCALE

NOTE:

IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY
 RUNWAY 14-32 250' PS (125' LT & RT OF CENTERLINE)
 RUNWAY 5-23 1000' PS (500' LT & RT OF CENTERLINE)

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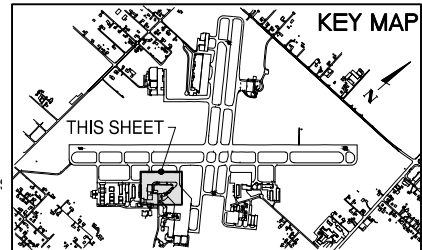
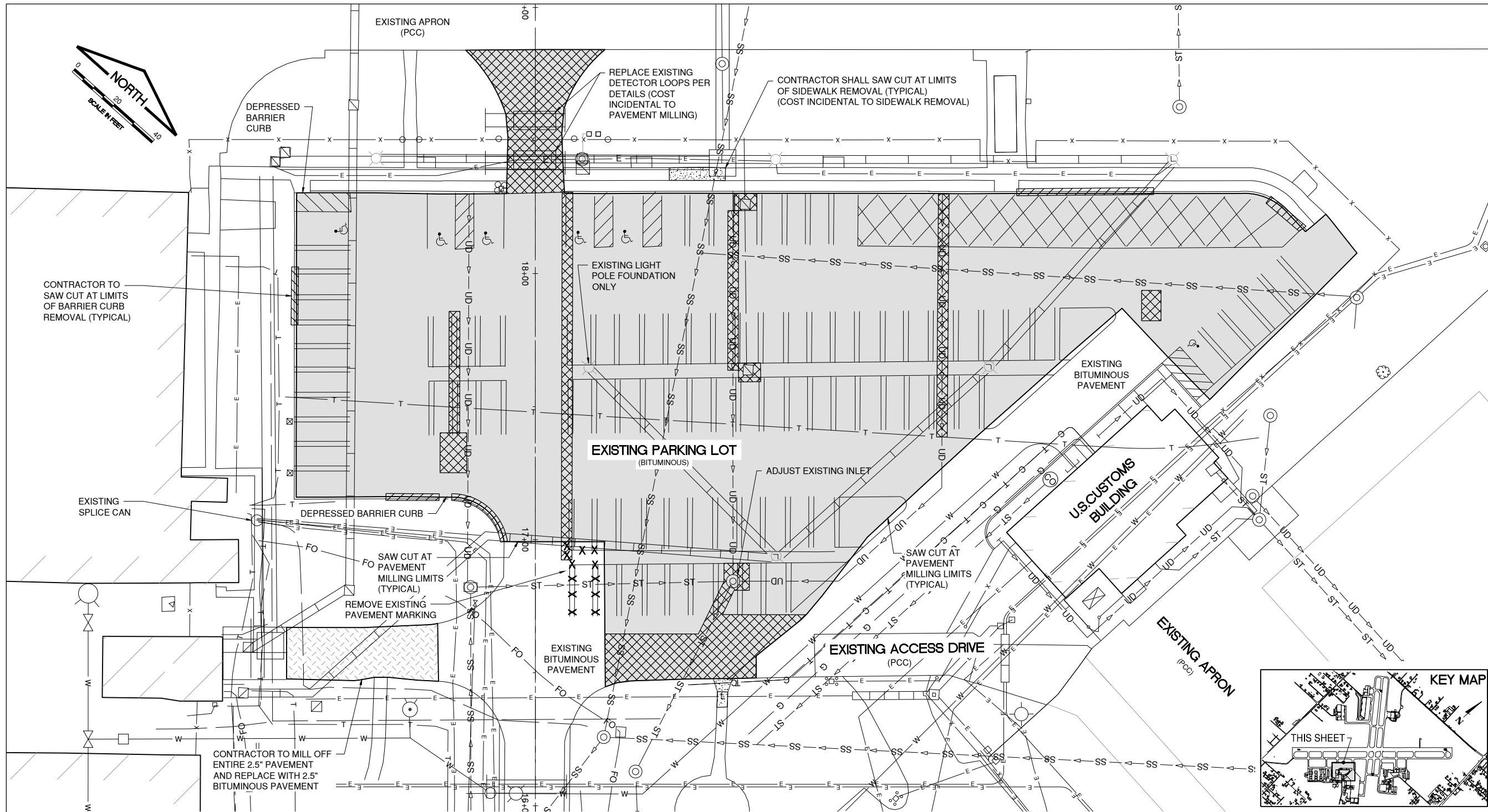
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**EXISTING CONDITIONS
 PROPOSED REMOVALS**



LEGEND

	2" BITUMINOUS PAVEMENT MILLING AND OVERLAY		EXISTING LIGHT POLE
	2.5" BITUMINOUS PAVEMENT MILLING AND OVERLAY		EXISTING DRAINAGE INLET
	REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B		EXISTING STAKE MOUNTED TAXIWAY LIGHT
	PCC SIDEWALK REMOVAL AND REPLACEMENT (AR501600)		EXISTING ELECTRICAL/STORM/SANITARY MANHOLE
	REMOVE AND REPLACE CONCRETE CURB (AR754900 AND AR754920)		EXISTING CONDUIT/DUCT BANK
	EXISTING BUILDING		EXISTING ELECTRIC HANDHOLE/STRUCTURE
	REMOVAL ITEM		EXISTING BASE MOUNTED TAXIWAY LIGHT
	EXISTING 6" CLASS E FENCE		EXISTING SANITARY CLEANOUT
	EXISTING SANITARY SEWER		EXISTING BUSH/TREE
	EXISTING UNDERDRAIN		EXISTING SIGN
	EXISTING STORM SEWER		EXISTING GATE
	EXISTING UNDERGROUND ELECTRIC		ELECTRIC VEHICLE CHARGER
	EXISTING TELEPHONE/COMMUNICATION		BARRIER CURB
	EXISTING WATERMAIN		

NOTES

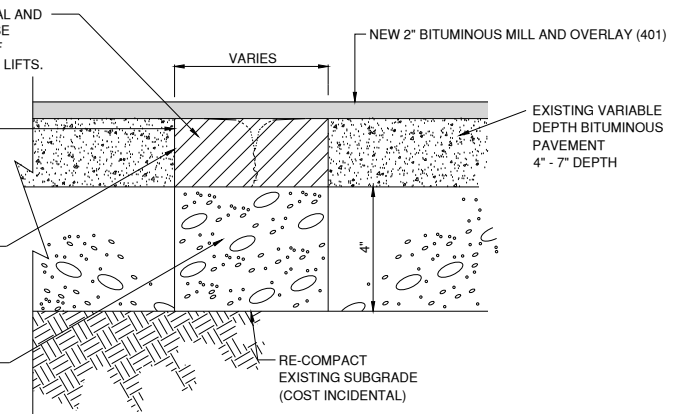
1. THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.
2. THE EXISTING BITUMINOUS PAVEMENT SHALL BE SAWCUT AT THE MILLING LIMITS TO PROVIDE A CLEAN VERTICAL FACE. COST FOR SAWCUT SHALL BE INCLUDED IN THE CONTRACT.
3. PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE SWEEPED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED. ALL CRACKS TO BE WIDER THAN 1" SHALL BE REPAIRED UNDER ITEM AR201663, SAND MIX CRACK REPAIR AS APPROVED BY THE RESIDENT ENGINEER
4. REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B LOCATIONS ARE APPROXIMATE. CONTRACTOR SHALL COORDINATE THE EXACT LAYOUT LIMITS IN THE FIELD WITH THE RESIDENT ENGINEER. MINIMUM WIDTH OF REPAIRS SHALL BE NO LESS THAN 3'.
5. REMOVE AND REPLACE CONCRETE CURB SHALL BE SAWCUT, REMOVED AND REPLACED TO MATCH THE EXISTING CONDITIONS. CONTRACTOR SHALL COORDINATE THE EXACT LAYOUT LIMITS IN THE FIELD WITH THE RESIDENT ENGINEER.
6. PCC SIDEWALK REMOVAL AND REPLACEMENT LOCATIONS ARE APPROXIMATE. CONTRACTOR SHALL COORDINATE THE EXACT LAYOUT LIMITS IN THE FIELD WITH THE RESIDENT ENGINEER.

FULL DEPTH PAVEMENT REMOVAL AND REPLACE WITH BITUMINOUS BASE COURSE (401) TO MATCH TOP OF MILLED SURFACE IN 2" MAXIMUM LIFTS. COST INCIDENTAL TO PATCH.

CLEAN AND TACK (603) PATCH EDGES AND AGGREGATE BASE (COST INCIDENTAL)

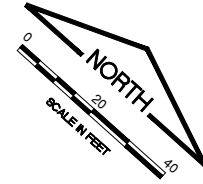
EXISTING PAVEMENT SHALL BE SAW CUT FULL DEPTH TO PRODUCE A NEAT VERTICAL FACE. COST INCIDENTAL TO PATCH.

REMOVE AND REPLACE EXISTING AGGREGATE BASE COURSE (COST INCIDENTAL TO PATCH)

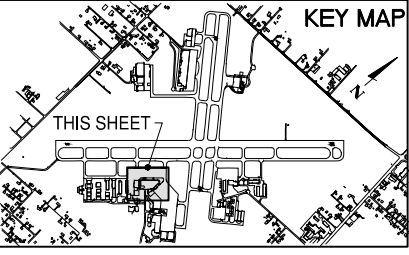
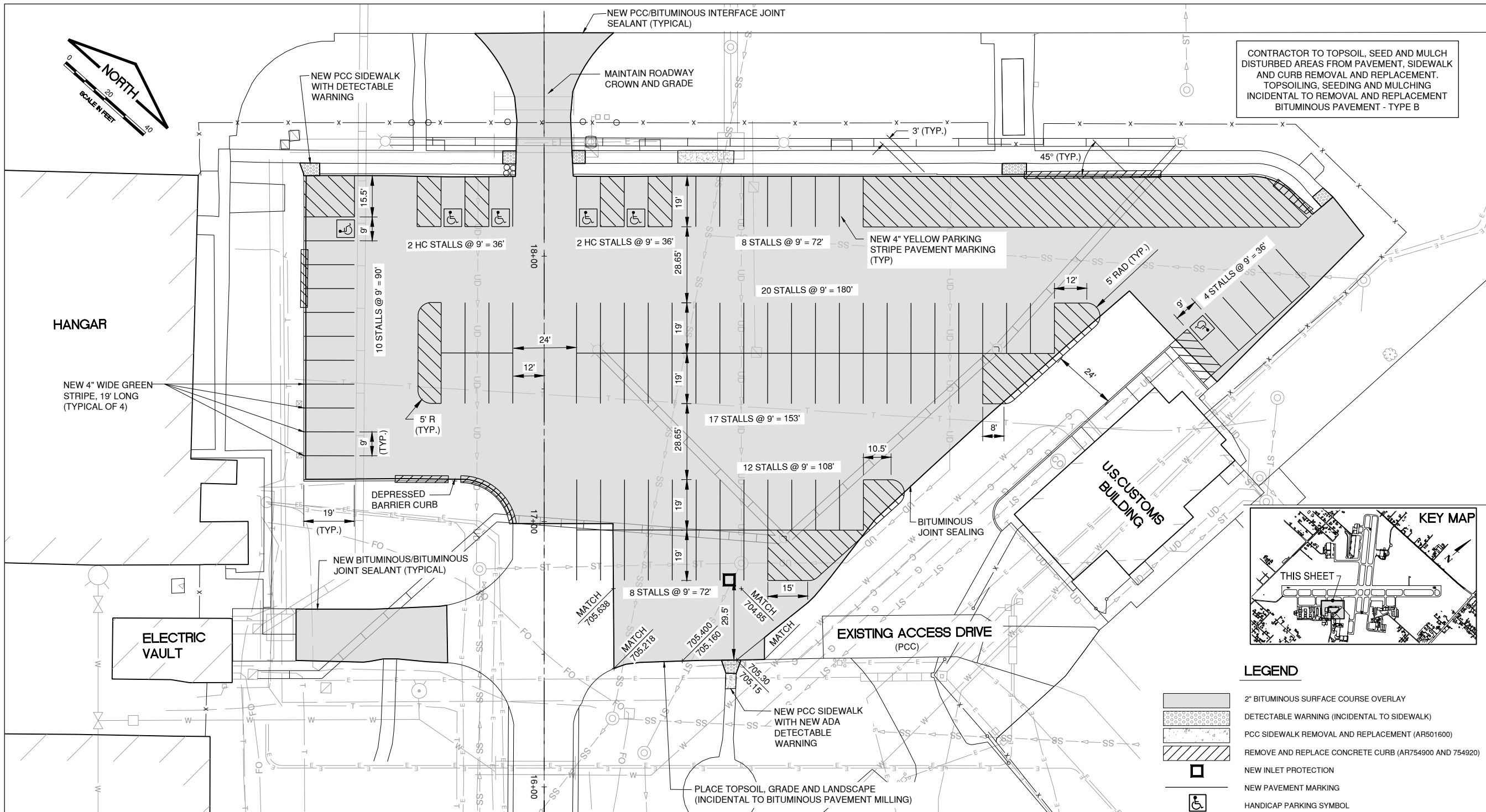


**REMOVE AND REPLACE
 BITUMINOUS PAVEMENT - TYPE B**

NOT TO SCALE

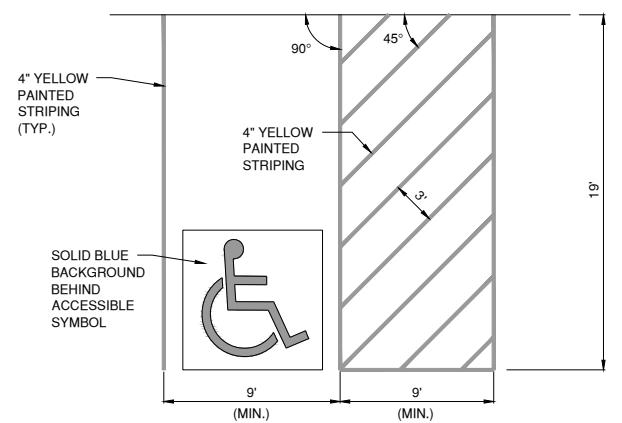


CONTRACTOR TO TOPSOIL, SEED AND MULCH DISTURBED AREAS FROM PAVEMENT, SIDEWALK AND CURB REMOVAL AND REPLACEMENT. TOPSOILING, SEEDING AND MULCHING INCIDENTAL TO REMOVAL AND REPLACEMENT BITUMINOUS PAVEMENT - TYPE B

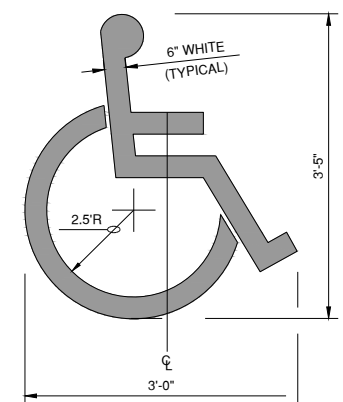


LEGEND

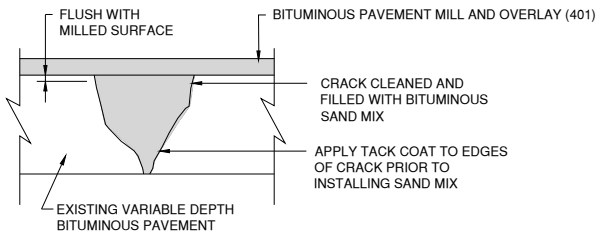
- 2" BITUMINOUS SURFACE COURSE OVERLAY
- DETECTABLE WARNING (INCIDENTAL TO SIDEWALK)
- PCC SIDEWALK REMOVAL AND REPLACEMENT (AR501600)
- REMOVE AND REPLACE CONCRETE CURB (AR754900 AND 754920)
- NEW INLET PROTECTION
- NEW PAVEMENT MARKING
- HANDICAP PARKING SYMBOL



HANDICAP PARKING STALL STRIPING DETAIL
 NOT TO SCALE

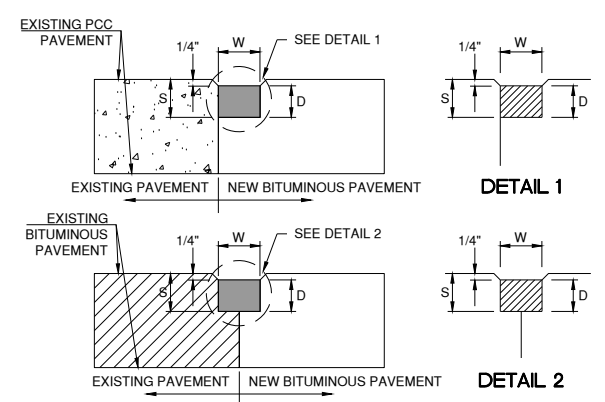


ACCESSIBLE PARKING SYMBOL
 NOT TO SCALE
 LOCATE AT EDGE OF PARKING SPACE



HMA SAND MIX CRACK REPAIR DETAIL
 NOT TO SCALE

- CRACK REPAIR NOTES:**
- CRACKS TO BE FILLED WITH SAND MIX SHALL BE DESIGNATED BY THE RESIDENT ENGINEER.
 - SAND MIX CRACK REPAIR SHALL BE USED ON ALL CRACKS GREATER THAN 1" WIDE AS DESIGNATED BY THE RESIDENT ENGINEER.
 - CONTRACTOR SHALL ROUTE, CRACK PLOW AND REMOVE LOOSE FILLER, LOOSE AGGREGATE AND LAITANCE WITH COMPRESSED AIR AS SPECIFIED PRIOR TO SEALING.



CONCRETE OR BITUMINOUS / BITUMINOUS INTERFACE JOINT SEALING
 NOT TO SCALE

NOTE: JOINT SEALING SHALL BE INCIDENTAL TO BITUMINOUS SURFACE COURSE (TYPICAL).

		DETAIL 1 & 2
		HOT/COLD POUR
W=WIDTH OF SEALANT RESERVOIR (IN.)	1/2	
D=DEPTH OF SEALANT RESERVOIR (IN.)	0	
B=BACKER ROD DIAMETER (IN.)	N/A	
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	3/4	

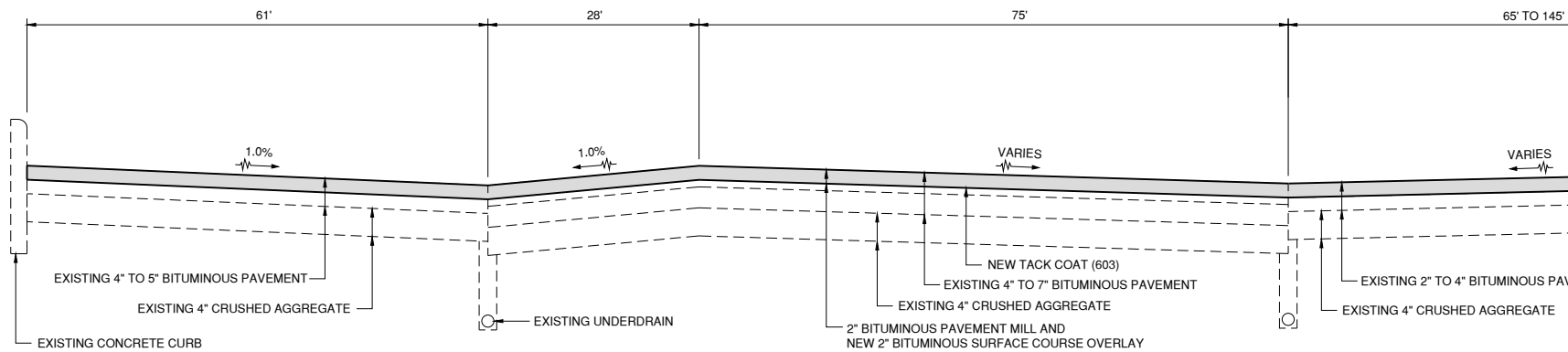
NOTE: JOINT SEALING SHALL BE PER SECTION 605 OF SPECIAL PROVISIONS.

MARK	DATE	DESCRIPTION

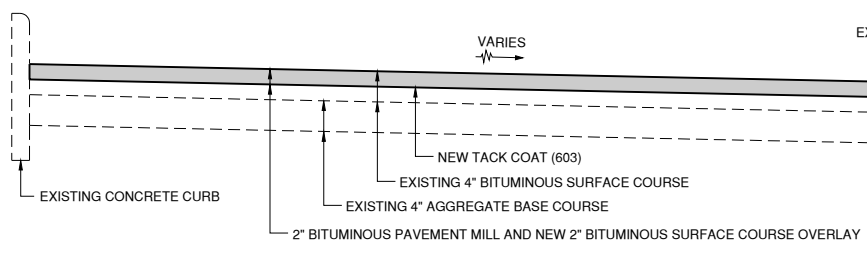
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CAD DWG FILE:	
DESIGNED BY:	LB
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
COPYRIGHT:	

PROPOSED IMPROVEMENTS

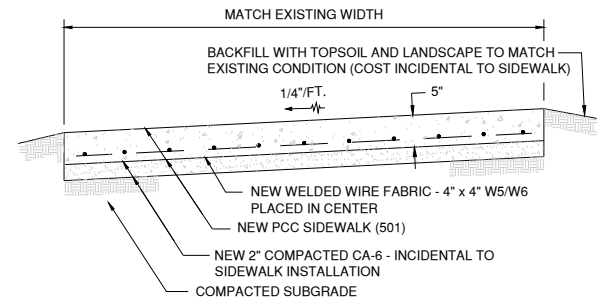
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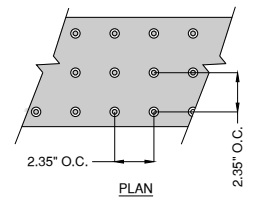
TYPICAL SECTION A-A
NOT TO SCALE



TYPICAL SECTION B-B
NOT TO SCALE



NEW SIDEWALK TYPICAL SECTION
NOT TO SCALE



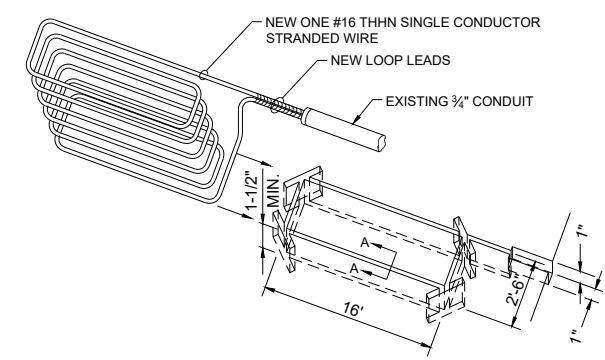
NOTE: PREFABRICATED PANELS PER A.D.A. ACCESSIBILITY GUIDELINES SECTION 4.29.2 & A4.29.2 SHALL BE USED.

DETECTABLE WARNING DETAIL

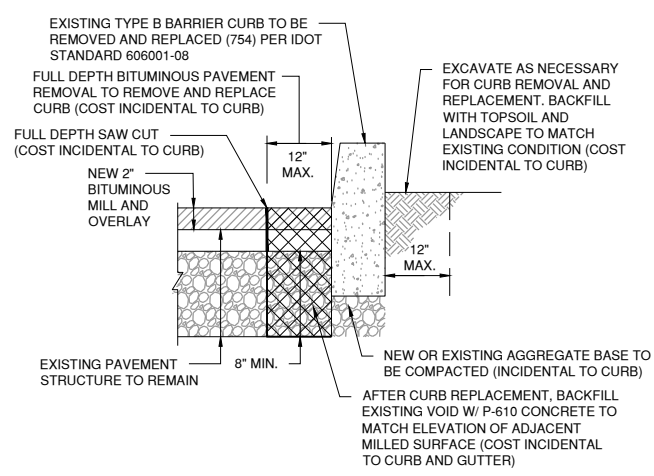
NOT TO SCALE

NOTES

- INSTALL DETECTABLE WARNING SURFACE IN ACCORDANCE WITH SECTION 424 OF THE IDOT HIGHWAY STANDARDS.



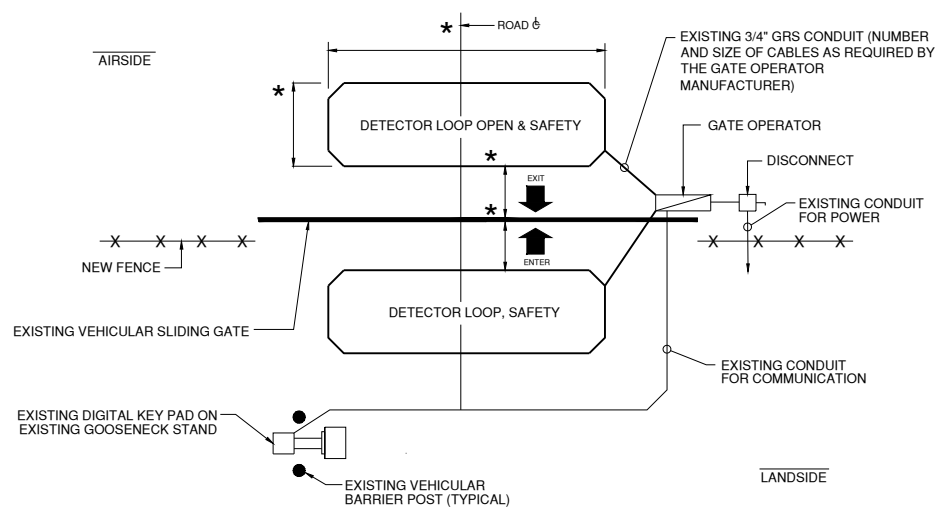
PERSPECTIVE VIEW OF DETECTOR LOOP SAW SLOT
NOT TO SCALE



BARRIER CURB DETAIL
NOT TO SCALE

NOTES

- ALL CURBS SHALL BE CURED AND PROTECTED TO THE REQUIREMENTS OF ARTICLE 606 OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION. MEMBRANE CURING WITH W.R. MEADOWS CS 309, OR APPROVED EQUAL, WILL BE ALLOWED WITH A WHITE FUGITIVE DYE, AS PER TYPE II MEMBRANE CURING.



NEW DETECTOR LOOP LAYOUT
NOT TO SCALE

* PER MANUFACTURERS RECOMMENDATION CONTRACTOR SHALL COORDINATE THIS WORK WITH ENGINEER.

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STORM WATER POLLUTION PREVENTION NOTES:

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT AND TEMPORARY EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF CONSTRUCTING A MILL AND OVERLAY ON THE MAIN PARKING LOT AT WAUKEGAN NATIONAL AIRPORT. THE PROJECT INCLUDES PAVEMENT REHABILITATION, FULL DEPTH EARTH REMOVAL AND REPLACEMENT AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES:

1. INSTALL TEMPORARY EROSION CONTROL MEASURES.
2. PAVEMENT MILLING AND REMOVAL.
3. FULL DEPTH PATCH REMOVAL AND REPLACEMENT.
4. CONSTRUCT NEW OVERLAYS
5. REMOVAL AND DISPOSAL OF TEMPORARY EROSION CONTROL MEASURES.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION IS LOCATED IN THE DES PLAINES RIVER WATERSHED. THE SITE DRAINS THROUGH A SERIES OF OVERLAND FLOW ROUTES/STORM SEWER INTO THE SUBURBAN COUNTRY CLUB TRIBUTARY THAT OUTLETS INTO ULTIMATELY THE DES PLAINES RIVER.

SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

- A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
 5. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
 6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

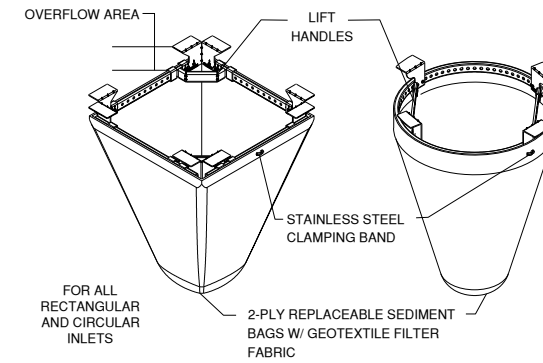
CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

SOIL EROSION AND SEDIMENT CONTROL NOTES:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, COOK COUNTY, CHICAGO EXECUTIVE AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

STAGING AND PROTECTION NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE STORM WATER POLLUTION PREVENTION DEVICES AT THE STAGING AREA. THESE DEVICES, INCLUDING INLET PROTECTIONS, SILT FENCE, BALES, DITCH CHECKS, STABILIZED CONSTRUCTION ENTRANCES, ETC., SHALL NOT BE PAID FOR, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
2. AT THE COMPLETION OF WORK, THE STAGING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS, AT NO ADDITIONAL COST TO THE CONTRACT. THIS WORK SHALL INCLUDE REMOVING ANY AND ALL STORM WATER POLLUTION PREVENTION DEVICES, RESTORING RUTTED AREAS, SEEDING AND MULCHING.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE STAGING AREA(S) IN A MANNER TO PREVENT POLLUTION AND SILTATION OF THE EXISTING STORM SEWER SYSTEM.
4. ALL EQUIPMENT FUELING AND GREASING SHALL BE COMPLETED AT THE STAGING AREA. SPILLS SHALL BE IMMEDIATELY CONTAINED AND THE AREA CLEANED AT NO ADDITIONAL COST THE CONTRACT.



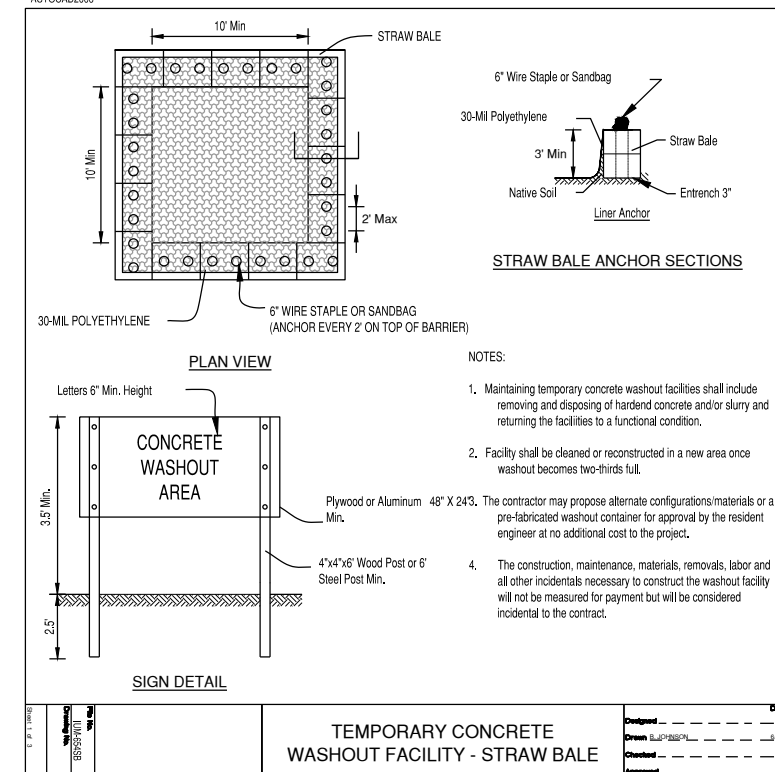
INLET PROTECTION - SILT BASKET (PAVEMENT AND TURF)

NOT TO SCALE

NOTES

1. CONTRACTOR SHALL CLEAR DEBRIS PER THE MANUFACTURER'S RECOMMENDATIONS BUT NOT LESS THAN WHEN THE CAPACITY FOR SEDIMENT STORAGE HAS BEEN REDUCED BY HALF.
2. FILTER FABRIC SHALL MEET THE MATERIAL REQUIREMENTS OF SPECIFICATION 592, TABLE 1, CLASS 2 OF THE ILLINOIS URBAN MANUAL.

AUTOCAD2006



NOTES:

1. Maintaining temporary concrete washout facilities shall include removing and disposing of hardened concrete and/or slurry and returning the facilities to a functional condition.
2. Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.
3. The contractor may propose alternate configurations/materials or a pre-fabricated washout container for approval by the resident engineer at no additional cost to the project.
4. The construction, maintenance, materials, removals, labor and all other incidentals necessary to construct the washout facility will not be measured for payment but will be considered incidental to the contract.



CONSULTANTS

IL PROJECT: UGN-5119
IL LETTING ITEM: 07A
IL CONTRACT: WA080

REHABILITATE MAIN PARKING LOT

FINAL
MARCH 1, 2024

OWNER



MARK DATE DESCRIPTION

CMT PROJECT NO: 22004938.00

CAD DWG FILE:

DESIGNED BY: LB

DRAWN BY: JRO

CHECKED BY: CAL

APPROVED BY: DKP

COPYRIGHT:

SHEET TITLE

EROSION CONTROL NOTES AND DETAILS



SOIL BORING LOG

GEO Job No. 24001
Page 1 of 1
Date 2/28/24

PROJECT: Rehabilitate Main Parking Lot
 LOCATION: Waukegan National Airport
 COUNTY: Cook DRILLING METHOD: Hollow Stem Auger HAMMER TYPE: CME Automatic
 CLIENT: CMT

BORING NO. B-01
 Northing 2095143.043
 Easting 1109862.200
 Ground Surface Elev. 705.9 ft

DEPTH (ft)	DIAMETER (in.)	SOIL DESCRIPTION	MOISTURE (%)	UNSATURATED WATER CONTENT (%)	LABORATORY
0	7	4" ASPHALT			
1	7	3.0" CRUSHED STONE			
2	5	SANDY LEAN CLAY-dark brown/black-stiff to hard			
3	7		4.50	25	01
4	8				
5	3				
6	3	CLAY-brown & gray-hard	1.50	24	03
7	4				
8	7		4.50	18	14
9	7				
10	6	SILTY CLAY-brown & gray-hard			
11	11		4.50	17	15
12	13				
13		End Of Boring @ -10.0'. Boring backfilled with cuttings.			
14					
15					
16					
17					
18					
19					
20					

Surface Water Elev. n/a ft
 Stream Bed Elev. n/a ft
 Groundwater Elev.:
 First Encounter n/a ft
 Upon Completion n/a ft
 After - Hrs. n/a ft

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (ASTM D1586) - Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)



PAVEMENT CORE SUMMARY

Page: 1 of 3

Project: Rehabilitate Main Parking Lot GSI Job No.: 24001
Location: Waukegan National Airport, Waukegan, IL 60087 Date: 02/28/2024
County: Lake Cored By: ROB
Client: CMT Checked By: SP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C-1	1.25 1.5	Northing - 2095218.091, Easting - 1109904.076 ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium aggregates-Surface mix.
C-2	1.25 1.5 2	Northing - 2095042.481, Easting - 1109760.459 ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium aggregates-Surface mix.
C-3	1.5 1.25 1.5	Northing - 2095103.511, Easting - 1109746.083 ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium aggregates-Surface mix. ASPHALT-well consolidated, fine to medium aggregates-Surface mix.
C-4	2.5	Northing - 2094969.743, Easting - 1109802.984 ASPHALT-well consolidated, fine to medium coarse aggregate-Binder mix.

LEGEND

- NEW PAVEMENT REHABILITATION
- EXISTING BUILDING/HANGAR
- CORING/BORING LOCATION

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

THE GEOTECHNICAL INFORMATION (OR ANY PORTIONS THEREOF) ARE PROVIDED ONLY AS AVAILABLE INFORMATION. THE CONTRACTOR MAY DRAW HIS OWN CONCLUSIONS FROM THE DATA SHOWN. THE SOILS INFORMATION IS NOT REPRESENTATIVE OF ALL SOIL WHICH MIGHT BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL BY HIS OWN MEANS, SATISFY HIMSELF AS TO THE EXISTING SITE AND GEOTECHNICAL CONDITIONS FOR DETERMINING COST, MEANS, METHODS, TECHNIQUES AND SEQUENCES OF CONSTRUCTION.



CONSULTANTS

IL PROJECT: UGN-5119
IL LETTING ITEM: 07A
IL CONTRACT: WA080

REHABILITATE MAIN PARKING LOT

FINAL
MARCH 1, 2024

OWNER



Geo Services, Inc. logo
 PAVEMENT CORE PHOTO LOG
 Page: 2 of 3

Project: Rehabilitate Main Parking Lot GSI Job No.: 24001
 Location: Waukegan National Airport, Waukegan, IL 60087 Date: 02/28/2024
 County: Lake Cored By: ROB
 Client: CMT Checked By: SP

Core No.: C-1 Core Location: Northing - 2095218.091, Easting - 1109904.076

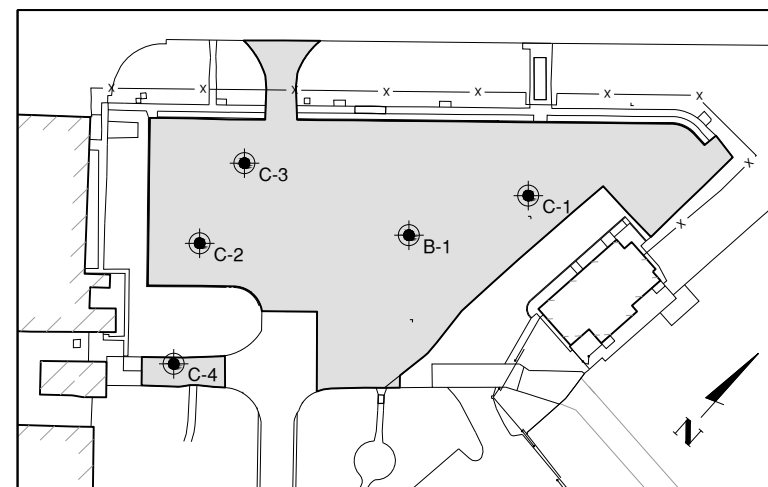
Core No.: C-2 Core Location: Northing - 2095042.481, Easting - 1109760.459

Geo Services, Inc. logo
 PAVEMENT CORE PHOTO LOG
 Page: 3 of 3

Project: Rehabilitate Main Parking Lot GSI Job No.: 24001
 Location: Waukegan National Airport, Waukegan, IL 60087 Date: 02/28/2024
 County: Lake Cored By: ROB
 Client: CMT Checked By: SP

Core No.: C-3 Core Location: Northing - 2095103.511, Easting - 1109746.083

Core No.: C-4 Core Location: Northing - 2094969.743, Easting - 1109802.984



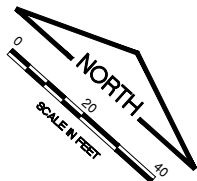
LOCATION MAP

MARK DATE DESCRIPTION

CMT PROJECT NO:	22004938.00
CAD DWG FILE:	
DESIGNED BY:	LB
DRAWN BY:	JRO
CHECKED BY:	CAL
APPROVED BY:	DKP
COPYRIGHT:	

SHEET TITLE

GEOTECHNICAL LAYOUT



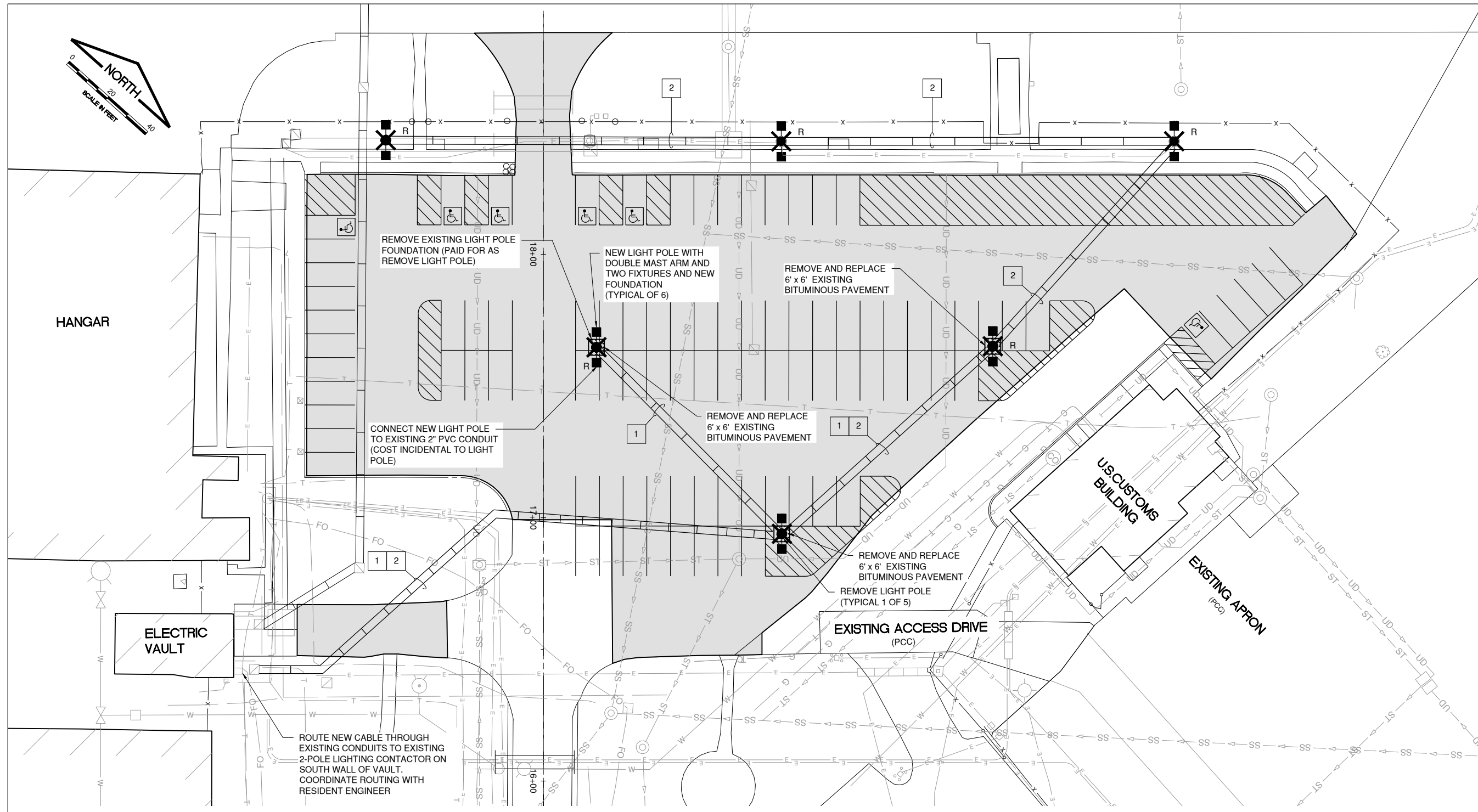
CONSULTANTS
 IL PROJECT: UGN-5119
 IL LETTING ITEM: 07A
 IL CONTRACT: WA080

REHABILITATE MAIN
 PARKING LOT

FINAL
 MARCH 1, 2024



Path: K:\Waukegan\AP\22004938_00_RehabMainParkingLot\Draw\Sheets\UGN-20222601 - Lighting Plan.dwg
 Date: Wednesday, March 27, 2024 3:12:46 PM



KEYED LEGEND

- 1 2 #10 XLP-USE, 1 #10 GND. IN EXISTING DUCT
- 2 2 #8 XLP-USE, 1 #10 GND. IN EXISTING DUCT

NOTES

1. AN ASSUMED QUANTITY OF 20 LF OF REMOVE AND REPLACE 2" PVC DUCT AND 12 SY OF REMOVE AND REPLACE EXISTING BITUMINOUS PAVEMENT IS INCLUDED TO ACCOUNT FOR POTENTIAL BLOCKAGES OF EXISTING CONDUIT.
2. COORDINATE EXACT LIGHT LAYOUT AND FIXTURE AIMING WITH RESIDENT ENGINEER.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

LEGEND

- BITUMINOUS PAVEMENT MILLING AND OVERLAY (BASE BID)
- REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE B (ADDITIVE ALTERNATE #1)
- NEW LIGHT POLE AND FOUNDATION
- REMOVAL ITEM
- EXISTING 2" PVC CONDUIT

ALL PARKING LOT LIGHTING IMPROVEMENTS AND ASSOCIATED PAVEMENT RESTORATION IS ADDITIVE ALTERNATE #1

MARK	DATE	DESCRIPTION

CMT PROJECT NO: 22004938.00
 CAD DWG FILE:
 DESIGNED BY: LB
 DRAWN BY: JRO
 CHECKED BY: CAL
 APPROVED BY: DKP
 COPYRIGHT:

**LIGHTING PLAN -
 ADDITIVE ALTERNATE
 NO. 1**



CONSULTANTS

IL PROJECT: UGN-5119
 IL LETTING ITEM: 07A
 IL CONTRACT: WA080

REHABILITATE MAIN
 PARKING LOT

FINAL
 MARCH 1, 2024

OWNER

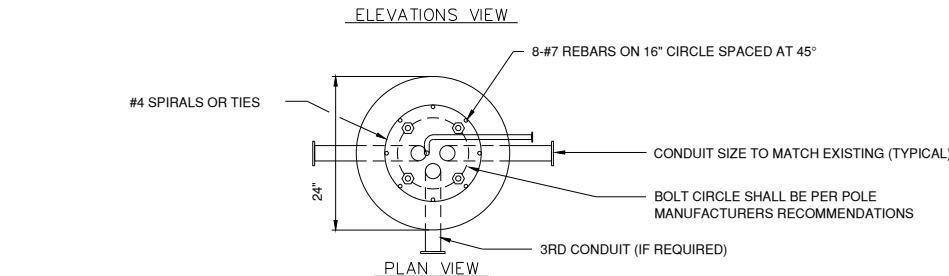
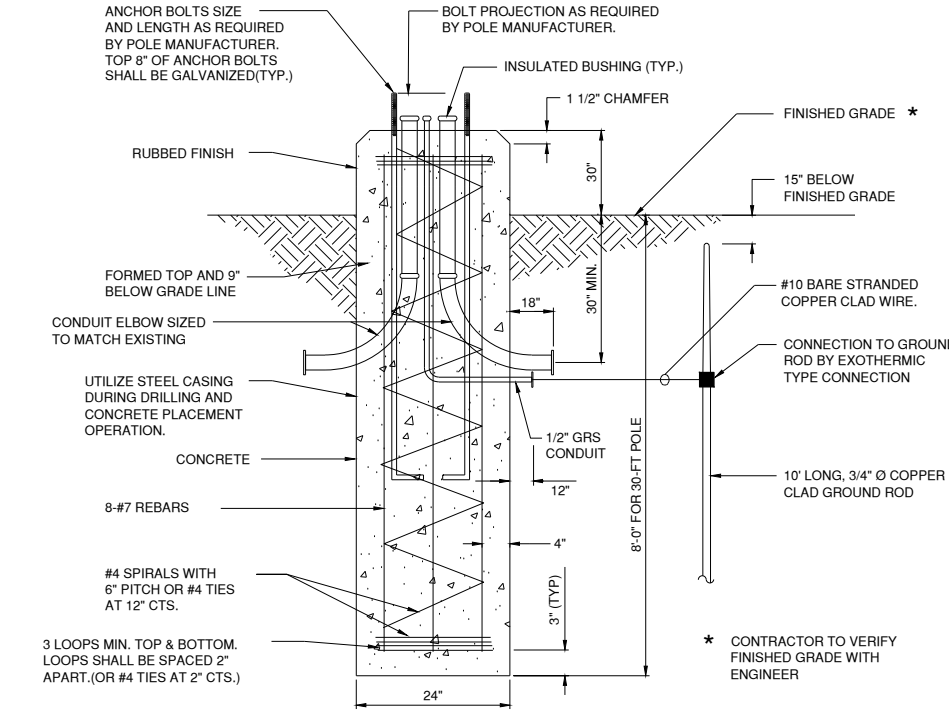


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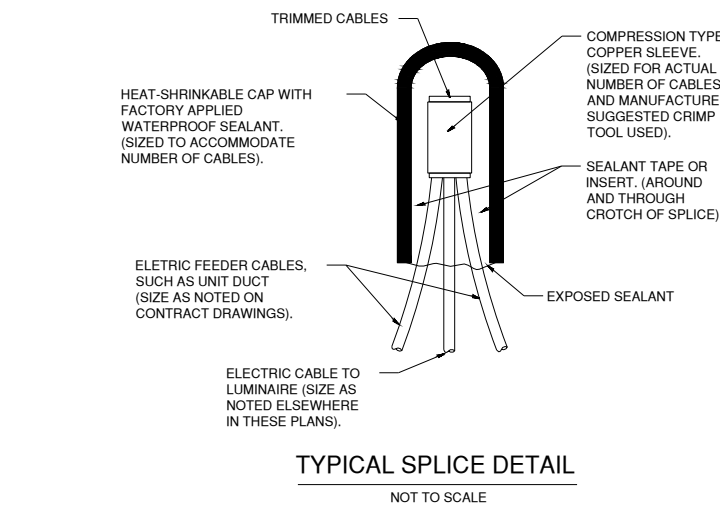
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SHEET TITLE

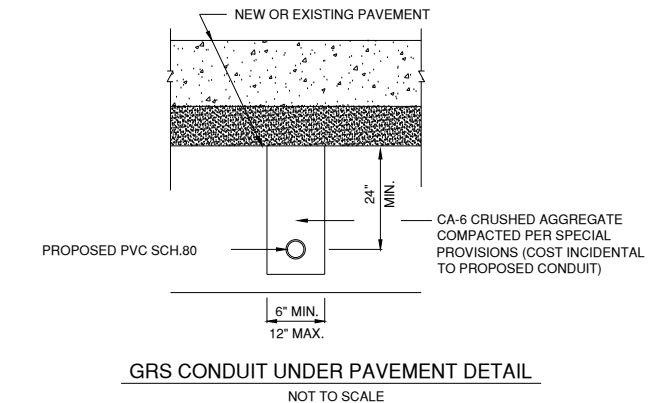
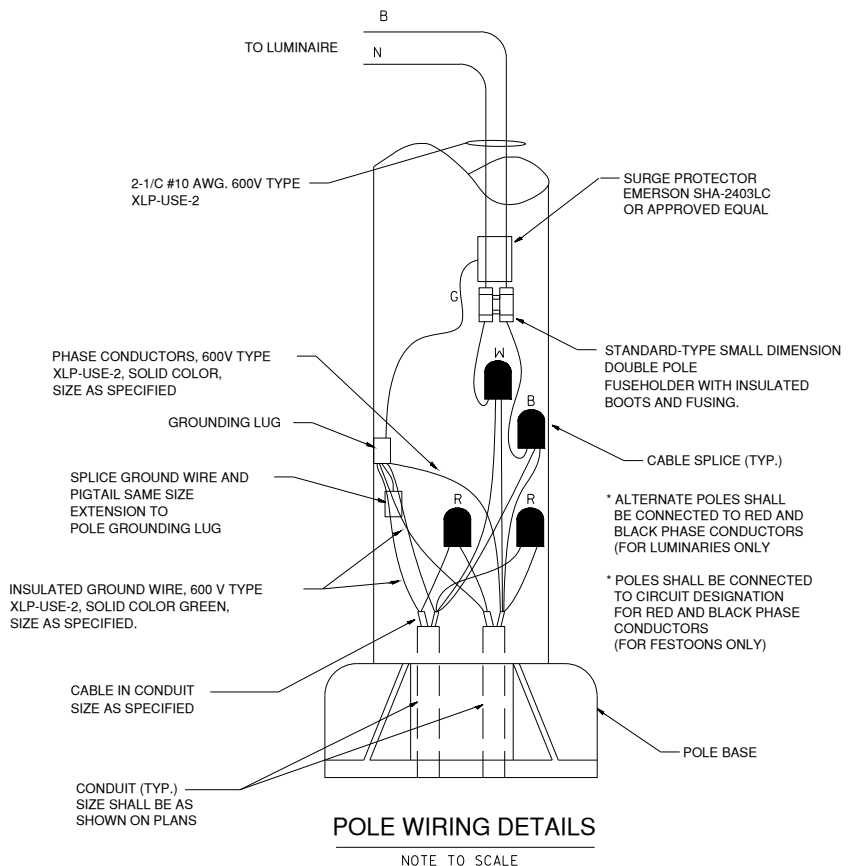
**ELECTRICAL DETAILS
 ADDITIVE
 ALTERNATE NO. 1**



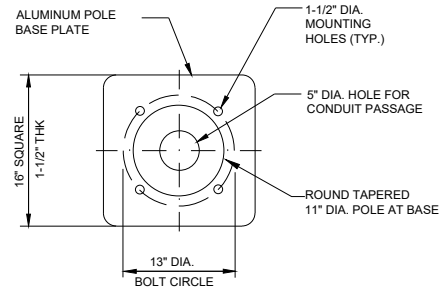
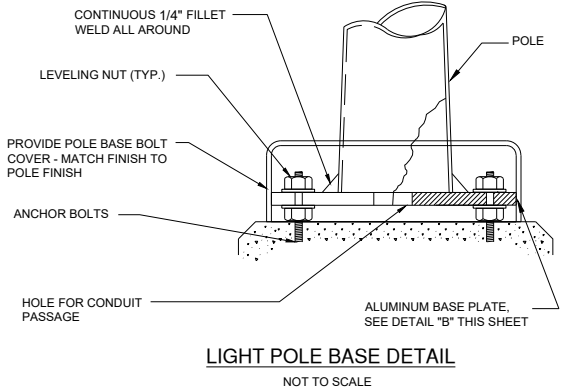
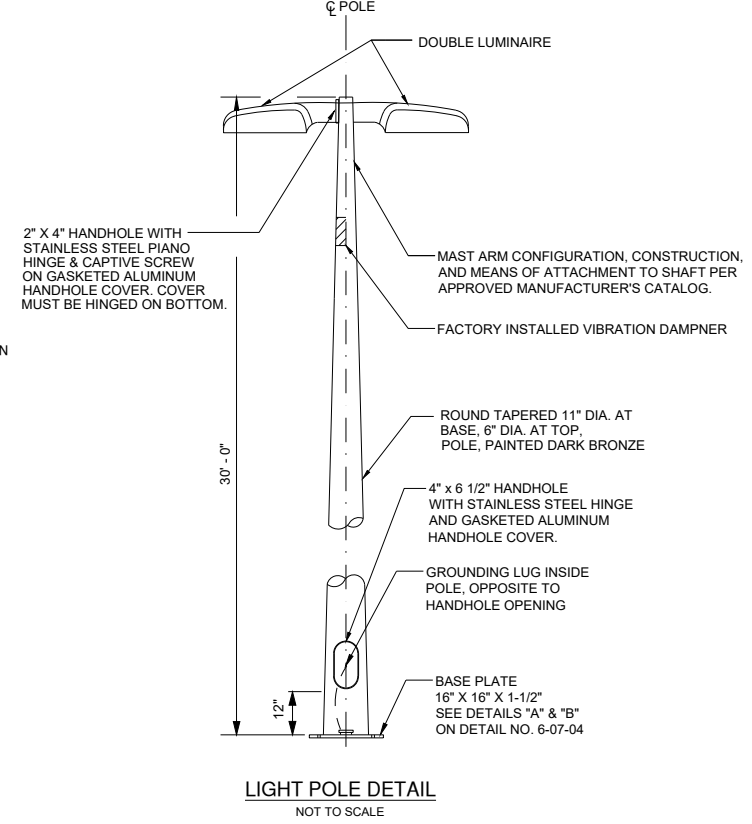
LIGHT POLE FOUNDATION DETAIL
 NOT TO SCALE



- NOTES:**
1. ALL TAPED SPLICES SHALL USE 2 LAYERS OF ELECTRICAL TAPE OVER 3 LAYERS OF RUBBER TAPE AS REQUIRED BY THE STANDARD SPECIFICATION. COAT THE FINISH TAPED SPLICE WITH BONDING COMPOUND.
 2. ALL CABLE SPLICES SHALL BE TAPED UNLESS ANOTHER METHOD HAS BEEN SPECIFICALLY APPROVED BY THE ENGINEER.
 3. THE NUMBER OF CABLES IN SPLICE MAY VARY



NEW DUCT BANK/CONDUIT SHALL BE INSTALLED AT AN ELEVATION THAT WILL NOT CONFLICT WITH EXISTING OR NEW UTILITIES INCLUDING STORM SEWER, UNDERDRAIN, CONDUIT, DUCT, GAS, WATERMAIN, PHONE, ELECTRICAL AT NO ADDITIONAL COST TO THE CONTRACT.



LIGHT POLE BASE PLAN
 NOT TO SCALE