04-26-2024 LETTING ITEM 195

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE VILLAGE OF OAK BROOK

TRAFFIC DATA:

IL ROUTE 83
S.N. 022-0047 2018 ADT = 56,800

DESIGN CLASSIFICATION = PRINCIPAL ARTERIAL

DESIGN SPEED = 50 MPH (ASSUMED)

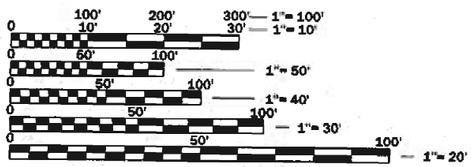
POSTED SPEED = 45 MPH

2018 ADT
INTERSTATE 88 ADT = 172,800
22ND ST ADT= 40,500

DESIGN CLASSIFICATION = I-88: INTERSTATE
22ND ST ADT: MINOR ARTERIAL

DESIGN SPEED = IL-88: 65 MPH (ASSUMED)
22ND ST : 45 MPH (ASSUMED)

POSTED SPEED = IL-88: 60 MPH
22ND ST: 40 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: PRAVEEN KAINI, PE. (847-705-4237)
PROJECT MANAGER: J. ALAIN MIDY, PE. (847-221-3056)

CONTRACT NO. 62M37

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

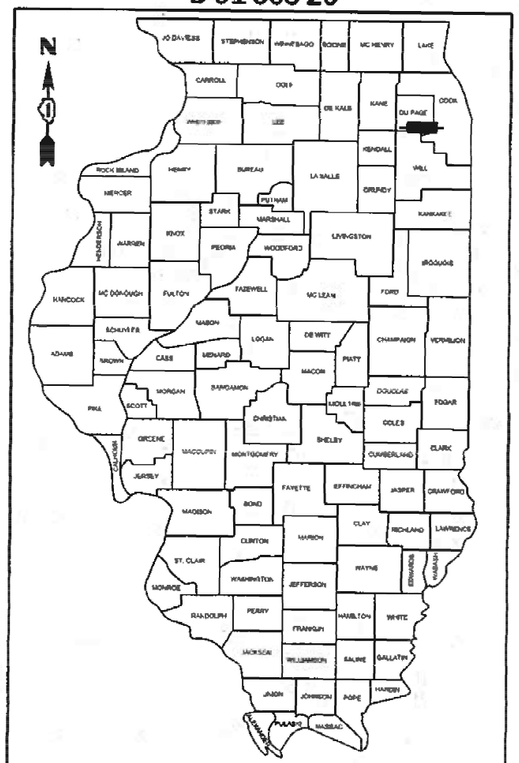
PROPOSED HIGHWAY PLANS

FAP ROUTE 344 (IL ROUTE 83) OVER I-88
SECTION 2020-182-BR
PROJECT NO. NHPP-ARPW(718)
BRIDGE DECK OVERLAY AND JOINT REPAIR
DUPAGE COUNTY

C-91-364-20

F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	1
	ILLINOIS	CONTRACT NO. 62M37		

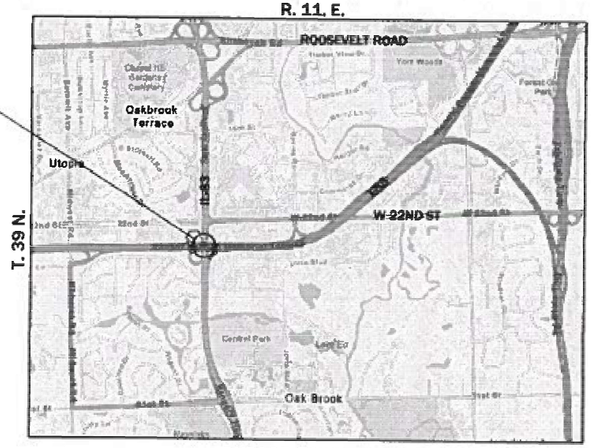
D-91-565-20



LOCATION OF SECTION INDICATED THUS:- ■

3rd P.M.

R. 11. E.



DUPAGE TOWNSHIP

LOCATION MAP

NOT TO SCALE

GROSS LENGTH = 1340 FT. = 0.25 MILES
NET LENGTH = 1014 FT. = 0.21 MILES

SHIRAZ TARIQUE
062-064219
LICENSED PROFESSIONAL ENGINEER
OF
ILLINOIS

1/25/2024
Shiraz Tarique — Date
Illinois Registered Engineer No. 062-064219
Registration Expires Nov. 30, 2025

LIN ENGINEERING,LTD.
Consulting Engineers
Westmont, Illinois

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Jan 29 2024 IR
REGIONAL ENGINEER

March 22, 2024
ENGINEER OF DESIGN AND ENVIRONMENT

March 22, 2024
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

REV-SEP

INDEX OF SHEETS

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606001-08	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
630001-13	STEEL PLATE BEAM GUARDRAIL
630301-09	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631011-10	TRAFFIC BARRIER TERMINAL, TYPE 2
631046-04	TRAFFIC BARRIER TERMINAL, TYPE 10
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TC-12	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS
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TOLLWAY STANDARDS

E1-07	CONSTRUCTION SIGNS
E2-10	LANE CLOSURE DETAILS
E3-09	SHOULDER CLOSURE DETAILS

GENERAL NOTES

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.

SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET, OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED WILL BE DETERMINED BY THE ENGINEER.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER SHOWN IN THE LIST OF STANDARDS OR THE COPY INCLUDED IN THESE PLANS.

THE CONTRACTOR SHALL CALL "J.U.L.I.E" AT (800) 892-0123 OR 811 AT LEAST 48 HOURS PRIOR TO EXCAVATION TO DETERMINE WHICH BURIED ELECTRIC, TELEPHONE, AND GAS UTILITIES ARE IN THE AREA. 48 HOUR NOTIFICATION IS REQUIRED.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS THAT CONFLICT WITH TEMPORARY MARKINGS, IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR PROPOSED STRIPING AT THE COMPLETION OF THIS CONTRACT. EXACT LOCATIONS OF ALL PROPOSED PAVEMENT MARKINGS SHALL BE DIRECTED BY THE RESIDENT ENGINEER.

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH AFFECTED UTILITY COMPANIES AND THE VILLAGE OF OAK BROOK.

THE CONTRACTOR SHALL MAINTAIN ALL ROADWAYS OPEN TO TRAFFIC AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.

THE CONTRACTOR SHALL USE CARE NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S OWN EXPENSE.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

DURING CONSTRUCTION OPERATIONS, IF ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKDAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DUST AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

PERMANENT PAVEMENT MARKINGS SHALL BE AS SPECIFIED IN THE PLANS AND SHALL BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAILS. (TC-12, MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS AND TC-13, DISTRICT ONE TYPICAL PAVEMENT MARKINGS).

THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULL LOADED TANDEM AXLE TRUCK.

ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRACTORS VEHICLES AND/OR EQUIPMENT IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.

ALL STAGE CHANGES REQUIRING THE STOPPING AND/OR THE PACING OF TRAFFIC SHALL TAKE PLACE DURING THE ALLOWABLE HOURS FOR FULL EXPRESSWAY CLOSURES AND SHALL BE APPROVED BY THE DEPARTMENT. THE CONTRACTOR SHALL NOTIFY THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT LEAST 3 WORKING DAYS (WEEKENDS AND HOLIDAYS DO NOT COUNT INTO THIS 72 HOURS NOTIFICATION) IN ADVANCE OF ANY PROPOSED STAGE CHANGE.

A MAINTENANCE OF TRAFFIC PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR 14 DAYS IN ADVANCE OF ANY STAGE CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT BE LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4155. ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV, AND THE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT CARLOSMUNOZ@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING ANY WORK.

GEOTECHNICAL FABRIC FOR GROUND STABILIZATION AND/OR AGGREGATE SUBGRADE IMPROVEMENT (CU YD) HAVE BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSTABLE AND/OR UNSUITABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ABOVE ITEM WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 OF THE SSRBC AND IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE SOILS ARE NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

THE SEEDING DATES FOR BARE EARTH SEEDING OF MIXTURE CLASS 2A SHALL BE FROM APRIL 1 TO JUNE 1 AND FROM AUGUST 15 TO SEPTEMBER 30. ALL SEEDING NOT SOWN ACCORDINGLY TO THE SPECIFIED SEASONAL DATE SHALL REQUIRE PRIOR WRITTEN APPROVAL FROM THE ENGINEER, FAILURE TO SECURE SUCH APPROVAL SHALL RESULT IN THE REJECTION OF THE SEEDING AND REPLACEMENT BY THE CONTRACTOR AT THEIR EXPENSE.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC FIELD ENGINEER, WALTER CZARNY, VIA EMAIL AT WALTERCZARNY@ILLINOIS.GOV AT LEAST (2) WEEKS PRIOR TO PLACING PERMANENT PAVEMENT MARKINGS.

NOMINAL QUANTITIES FOR EARTH EXCAVATION, SEEDING, FERTILIZER NUTRIENTS, MULCH METHOD, AND PERIMETER EROSION BARRIER HAVE BEEN INCLUDED FOR RESEEDING AREAS DISTURBED BY PROPOSED SHOULDER IMPROVEMENTS.

COMMITMENTS

NONE.

HOT- MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ NDES	QUALITY MANAGEMENT PROGRAM (QMP)
BUTT JOINT		
POLY, HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80	3.5% @ 80 GYR.	QC/QA
HOT-MIX ASPHALT SHOULDER, 11 1/4"		
HOT-MIX ASPHALT SURFACE COURSE IL-9.5, MIX "D", N70; 2"	4% @ 70 GYR.	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 9 1/4"	4% @ 70 GYR.	QC/QA
TEMPORARY PAVEMENT		
HOT-MIX ASPHALT SURFACE COURSE IL-9.5, MIX "D", N70; 2"	4% @ 70 GYR.	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 9 1/4"	4% @ 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

MIXTURE TABLE NOTES

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
2. THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE AC TYPE SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATION.
3. PCC TEMPORARY PAVEMENT SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ART. 1020 OF THE STANDARD SPECIFICATIONS; THICKNESS SHALL BE 8". TEMPORARY PCC PAVEMENT DOES NOT REQUIRE DOWEL BARS.
4. TEMPORARY PAVEMENT SHALL BE PLACED OVER 4" SUBBASE GRANULAR MATERIAL TY B.

FACTORS FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

AGGREGATE SHOULDERS	1.60 TON/CU YD
SEEDING, CLASS 2A	200 LB/ACRE
SHORT TERM PAVEMENT MARKING	10 FT/100 FT
NITROGEN FERTILIZER NUTRIENT	90 LB/ACRE
PHOSPHORUS FERTILIZER NUTRIENT	90 LB/ACRE
POTASSIUM FERTILIZER NUTRIENT	90 LB/ACRE
GRANULAR MATERIAL	2.05 TONS/CU YD
MULCH	2 TON/ACRE

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% FED/20% STATE SN 022-0047 0059 BRIDGE
20200100	EARTH EXCAVATION	CU YD	86	86
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	126	126
25000210	SEEDING, CLASS 2A	ACRE	0.25	0.25
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	30	30
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	30	30
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	30	30
25100115	MULCH, METHOD 2	ACRE	0.25	0.25
28000400	PERIMETER EROSION BARRIER	FOOT	1,229	1,229
28000510	INLET FILTERS	EACH	5	5
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	42	42
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	1,534	1,534
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	378	378
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	839	839
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	94	94

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% FED/20% STATE SN 022-0047 0059 BRIDGE
44000100	PAVEMENT REMOVAL	SQ YD	22	22
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	123	123
44004250	PAVED SHOULDER REMOVAL	SQ YD	1,379	1,379
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	80	80
48203042	HOT-MIX ASPHALT SHOULDERS, 11 1/4"	SQ YD	1,379	1,379
48300210	PORTLAND CEMENT CONCRETE SHOULDERS 7 1/2"	SQ YD	43	43
50102400	CONCRETE REMOVAL	CU YD	10.0	10.0
50157300	PROTECTIVE SHIELD	SQ YD	3,757	3,757
50300255	CONCRETE SUPERSTRUCTURE	CU YD	12.5	12.5
50300300	PROTECTIVE COAT	SQ YD	5,383	5,383
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1,480	1,480
50800515	BAR SPLICERS	EACH	24	24
52000030	PREFORMED JOINT SEAL 2 1/2"	FOOT	239	239
52000110	PREFORMED JOINT STRIP SEAL	FOOT	272	272

* SPECIALTY ITEM

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% FED/20% STATE SN 022-0047 0059 BRIDGE
	58700300	CONCRETE SEALER	SQ FT	6,824	6,824
	60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5
	60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	123	123
*	63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	25.0	25.0
*	63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	2	2
*	63100105	TRAFFIC BARRIER TERMINAL, TYPE 10	EACH	2	2
*	63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1	1
	63200310	GUARDRAIL REMOVAL	FOOT	77	77
*	63301210	REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	488	488
*	63301990	REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 1	EACH	1	1
*	63302700	REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 6	EACH	2	2
*	66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	232	232
*	66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2
*	66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1

* SPECIALTY ITEM

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% FED/20% STATE SN 022-0047 0059 BRIDGE
*	66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1
*	66901006	REGULATED SUBSTANCES MONITORING	CAL DA	5	5
	67100100	MOBILIZATION	L SUM	1	1
	70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	65	65
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	390	390
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	1,724	1,724
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	5,744	5,744
	70307100	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE	SQ FT	219	219
	70307120	TEMPORARY PAVEMENT MARKING-LINE 4"-TYPE IV TAPE	FOOT	14,776	14,776
	70307130	TEMPORARY PAVEMENT MARKING-LINE 6"-TYPE IV TAPE	FOOT	500	500
	70307140	TEMPORARY PAVEMENT MARKING-LINE 8"-TYPE IV TAPE	FOOT	1,956	1,956
	70307160	TEMPORARY PAVEMENT MARKING-LINE 12"-TYPE IV TAPE	FOOT	218	218
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	2,025.0	2,025.0
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	2,650.0	2,650.0

* SPECIALTY ITEM

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% FED/20% STATE SN 022-0047 0059 BRIDGE
	70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2
	70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4	4
*	72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	1	1
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	110	110
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2,961	2,961
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	393	393
*	78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	562	562
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	72	72
*	78004235	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 7"	FOOT	208	208
*	78004625	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - STANDARD - LINE 5"	FOOT	1,250	1,250
*	78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1,316	1,316
*	78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	711	711
*	78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	13	13
*	78011030	GROOVING FOR RECESSED PAVEMENT MARKING 6"	FOOT	1,250	1,250

* SPECIALTY ITEM

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% FED/20% STATE SN 022-0047 0059 BRIDGE
*	78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	208	208
*	78011045	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	95	95
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	44	44
*	78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	4	4
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	44	44
	78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	3,121	3,121
	X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	5,179	5,179
	X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12
	X7010005	MAINTENANCE OF TRAFFIC (ILLINOIS TOLLWAY)	L SUM	1	1
	X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1
	X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1
	X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	65	65
	X7040010	TEMPORARY CONCRETE BARRIER (SPECIAL)	FOOT	350.0	350.0
	X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	82	82

* SPECIALTY ITEM

	CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE 80% FED/20% STATE SN 022-0047 0059 BRIDGE
*	X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT	EACH	82	82
	Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	4	4
	Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	5,374	5,374
	Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	5,374	5,374
	Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	20	20
	Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	4,790	4,790
	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	61	61
	Z0041895	POLYMER CONCRETE	CU FT	146	146
	Z0062456	TEMPORARY PAVEMENT	SQ YD	22	22
	Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	4	4
Ø	Z0076600	TRAINEES	HOURS	500	500
Ø	Z0076604	TRAINING PROGRAM GRADUATE	HOURS	500	500

* SPECIALTY ITEM

LIN ENGINEERING,LTD.
Consulting Engineers
Westmont, Illinois

USER NAME = 14nho	DESIGNED - NH	REVISED -
	DRAWN - NH	REVISED -
PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -
PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
SUMMARY OF QUANTITIES

SCALE: N.T.S. | SHEET 7 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	9
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		

REV-SEP

INLET FILTERS

FROM STATION	LT/RT	EACH
526+83.69	LT	1
526+98.83	LT	1
527+00.24	RT	1
527+01.63	RT	1
527+73.57	RT	1
TOTAL		5

SUBBASE GRANULAR MATERIAL 4"

FROM STATION	TO STATION	LT/RT	AREA (SQ YD)
519+98.78	522+01.62	LT	372.97
520+04.16	524+36.43	RT	547.85
523+02.17	524+34.85	LT	133.09
526+73.03	528+07.43	LT	140.63
528+47.86	531+18.08	LT	339.44
ROUNDED TOTAL			1,534

COMBINATION CURB AND GUTTER REMOVAL

FROM STATION	TO STATION	LT/RT	FOOT
526+86.76	528+08.93	RT	122.29
ROUNDED TOTAL			123

PAVED SHOULDER REMOVAL

FROM STATION	TO STATION	LT/RT	AREA (SQ YD)
519+98.78	522+01.62	LT	350.45
520+04.16	524+36.43	RT	499.82
523+02.17	524+34.85	LT	105.80
526+73.03	528+07.43	LT	113.22
528+47.86	531+18.08	LT	309.47
ROUNDED TOTAL			1,379

AGGREGATE WEDGE SHOULDER, TYPE B

FROM STATION	TO STATION	LT/RT	TON
519+98.78	522+01.62	LT	14.85
520+04.16	524+36.43	RT	32.08
523+02.17	524+34.85	LT	7.51
526+73.03	528+57.89	LT	14.01
528+47.86	531+18.08	LT	10.58
ROUNDED TOTAL			80

HOT-MIX ASPHALT SHOULDERS, 11 1/4"

FROM STATION	TO STATION	LT/RT	AREA (SQ YD)
519+98.78	522+01.62	LT	350.45
520+04.16	524+36.43	RT	499.82
523+02.17	524+34.85	LT	105.80
526+73.03	528+07.43	LT	113.22
528+47.86	531+18.08	LT	309.47
ROUNDED TOTAL			1,379

PORTLAND CEMENT CONCRETE SHOULDERS 7 1/2"

FROM STATION	TO STATION	LT/RT	AREA (SQ YD)
524+16.16	524+34.94	LT	12.50
526+73.10	526+91.91	LT	12.52
526+74.66	526+92.60	RT	17.94
ROUNDED TOTAL			43

FRAMES AND GRATES TO BE ADJUSTED

FROM STATION	LT/RT	EACH
526+83.69	LT	1
526+98.83	LT	1
527+00.24	RT	1
527+01.63	RT	1
527+73.57	RT	1
TOTAL		5

COMBINATION CURB AND GUTTER, TYPE B-6.24

FROM STATION	TO STATION	LT/RT	FOOT
526+86.76	528+08.93	RT	122.29
ROUNDED TOTAL			123

STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS

FROM STATION	TO STATION	LT/RT	FOOT
524+10.37	524+22.87	LT	12.50
526+89.46	527+01.96	RT	12.50
ROUNDED TOTAL			25.0

TRAFFIC BARRIER TERMINAL, TYPE 2

FROM STATION	TO STATION	LT/RT	EACH
523+97.87	524+10.37	LT	1
527+01.96	527+14.46	RT	1
TOTAL			2

TRAFFIC BARRIER TERMINAL, TYPE 10

FROM STATION	TO STATION	LT/RT	EACH
524+22.87	524+25.18	LT	1
526+88.17	526+89.46	RT	1
TOTAL			2

GUARDRAIL REMOVAL

FROM STATION	TO STATION	LT/RT	FOOT
526+88.17	527+14.46	RT	26.30
528+03.00	528+53.00	LT	50.00
ROUNDED TOTAL			77

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A

FROM STATION	TO STATION	LT/RT	FOOT
520+69.29	524+38.01	RT	368.76
526+85.66	528+03.00	LT	118.75
ROUNDED TOTAL			488

REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 1

FROM STATION	TO STATION	LT/RT	EACH
520+69.29	521+19.29	RT	1
TOTAL			1

REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 6

FROM STATION	TO STATION	LT/RT	EACH
524+94.26	524+38.01	RT	1
526+85.66	527+29.28	LT	1
TOTAL			2

TEMPORARY PAVEMENT MARKING LINE 6"-TYPE IV TAPE

FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1			
524+57.35	529+51.48	RT	123.54
528+08.89	529+51.48	RT	35.68
STAGE 2			
524+47.04	529+51.48	RT	126.09
528+09.10	529+89.41	RT	45.06
STAGE 3			
523+30.45	526+84.87	LT	88.60
528+08.90	529+50.82	RT	35.99
528+08.90	529+86.20	RT	44.26
ROUNDED TOTAL			500

TEMPORARY PAVEMENT MARKING LINE 4"-TYPE IV TAPE

FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1			
519+92.51	532+16.45	LT	1,224.11
519+92.51	531+51.30	LT	1,159.17
519+92.50	532+16.41	LT	306.04
521+30.53	529+51.48	RT	205.22
521+30.53	529+51.48	RT	821.44
521+30.53	529+51.48	RT	823.97
STAGE 2			
518+76.42	530+19.33	RT	1,143.45
518+76.42	530+19.33	RT	1,143.42
519+19.79	531+79.91	LT	1,265.34
519+19.79	532+16.36	LT	324.28
519+19.79	532+16.41	LT	1,297.26
522+25.67	528+08.91	RT	583.24
522+25.67	528+08.88	RT	582.20
529+51.48	530+19.33	RT	67.86
529+51.48	530+19.33	RT	67.86
529+89.41	530+19.33	RT	29.92
STAGE 3			
519+36.89	530+72.59	RT	1,143.10
519+36.89	530+72.59	RT	1,136.53
519+36.89	530+72.59	RT	284.06
523+30.45	528+45.17	LT	128.70
523+30.45	528+45.17	LT	128.72
523+30.45	528+45.17	LT	514.88
528+08.90	530+72.59	RT	65.96
529+51.48	530+72.59	RT	121.12
529+51.48	530+72.59	RT	121.12
529+86.20	530+72.59	RT	86.39
ROUNDED TOTAL			14,776

TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS-TYPE IV TAPE

STATION	LT/RT	AREA (SQ FT)
STAGE 2		
529+64.40	RT	36.4
529+64.55	RT	36.4
529+99.27	RT	36.4
STAGE 3		
529+64.40	RT	36.4
529+64.55	RT	36.4
529+99.27	RT	36.4
ROUNDED TOTAL		219

TEMPORARY PAVEMENT MARKING
LINE 8"-TYPE IV TAPE

FROM STATION	TO STATION	LT/RT	FOOT
STAGE 2			
518+76.42	522+25.67	RT	698.98
528+08.88	530+19.33	RT	421.52
STAGE 3			
523+30.45	528+41.27	LT	511.99
526+84.87	528+45.17	LT	323.15
		ROUNDED TOTAL	1,956

TEMPORARY PAVEMENT MARKING
LINE 12"-TYPE IV TAPE

FROM STATION	TO STATION	LT/RT	FOOT
STAGE 2			
518+76.42	522+25.67	RT	83.27
528+08.88	530+19.33	RT	100.27
STAGE 3			
526+84.87	528+45.17	LT	33.96
		ROUNDED TOTAL	218

TEMPORARY CONCRETE BARRIER

FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1			
519+92.51	529+66.94	LT	975.00
521+84.13	528+08.90	RT	625.00
STAGE 2			
523+00.53	530+49.86	LT	425.00
		ROUNDED TOTAL	2,025.0

RELOCATE TEMPORARY CONCRETE BARRIER

FROM STATION	TO STATION	LT/RT	FOOT
STAGE 2			
521+71.72	528+08.88	RT	637.50
521+71.72	528+08.91	RT	637.50
523+00.53	530+49.86	LT	325.00
STAGE 3			
520+34.65	528+08.89	RT	775.00
524+35.64	527+10.60	LT	275.00
		ROUNDED TOTAL	2,650.0

TEMPORARY PAVEMENT

FROM STATION	TO STATION	LT/RT	AREA (SQ YD)
528+47.86	529+29.90	LT	21.32
		ROUNDED TOTAL	22

IMPACT ATTENUATORS,TEMPORARY (FULLY REDIRECTIVE,NARROW),TEST LEVEL 3

FROM STATION	LT/RT	EACH
STAGE 1		
521+84.13	RT	1
529+66.94	LT	1
	TOTAL	2

IMPACT ATTENUATORS,RELOCATE (FULLY REDIRECTIVE,NARROW),TEST LEVEL 3

FROM STATION	LT/RT	EACH
STAGE 2		
521+71.72	RT	1
530+49.86	LT	1
STAGE 3		
520+34.65	RT	1
527+10.60	LT	1
	TOTAL	4

GUARDRAIL REFLECTORS, TYPE A

FROM STATION	TO STATION	LT/RT	EACH
524+10.37	524+22.87	LT	2
526+89.46	527+01.96	RT	2
		TOTAL	4

TEMPORARY CONCRETE BARRIER (SPECIAL)

FROM STATION	TO STATION	LT/RT	FOOT
STAGE 1			
ALONG I-88	RAMP CLOSURE	RT	350.0
		ROUNDED TOTAL	350.0

POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE
STONE MATRIX ASPHALT, 9.5, MIX "F", N80

FROM STATION	TO STATION	LT/RT	TON
523+00.67	523+35.67	RT	16.57
523+00.68	523+35.68	LT	23.89
527+73.87	528+08.87	RT	26.75
527+73.93	528+08.93	LT	26.75
		ROUNDED TOTAL	94

PAVEMENT MARKING REMOVAL - WATER BLASTING

FROM STATION	TO STATION	LT/RT	AREA (SQ FT)
518+76.42	523+35.67	RT	153.11
518+76.42	523+35.67	RT	153.11
518+76.42	523+35.67	RT	95.69
519+19.79	522+01.65	LT	93.96
519+19.79	523+35.67	LT	138.63
519+19.79	523+35.68	LT	86.89
522+02.98	523+08.61	LT	143.58
522+02.98	523+08.61	LT	48.64
522+08.79	523+35.68	LT	86.36
523+35.67	527+73.92	RT	146.12
523+35.67	527+73.84	RT	146.17
523+35.67	527+73.92	LT	146.09
523+35.67	527+73.89	RT	146.07
523+35.68	527+73.54	LT	292.01
523+35.68	527+73.92	LT	146.08
526+84.87	527+73.93	LT	118.83
526+84.87	527+73.93	LT	12.51
527+73.54	528+41.27	LT	45.85
527+73.84	530+72.59	RT	99.64
527+73.89	530+72.59	RT	62.27
527+73.91	528+47.87	LT	22.92
527+73.91	528+47.87	LT	98.57
527+73.92	530+72.59	RT	99.61
527+73.92	532+16.45	LT	147.53
527+73.92	532+16.45	LT	93.03
528+47.87	531+51.70	LT	101.30
529+41.86	530+72.59	RT	65.36
529+41.86	530+72.59	RT	65.36
529+41.86	530+72.59	RT	65.36
		ROUNDED TOTAL	3,121

PAVEMENT REMOVAL

FROM STATION	TO STATION	LT/RT	AREA (SQ YD)
528+47.86	529+29.90	LT	21.32
		ROUNDED TOTAL	22

TEMPORARY INFORMATION SIGNING

FROM STATION	LT/RT	AREA (SQ FT)
STAGE 2		
518+76.00	RT	16.00
518+76.00	RT	16.00
520+00.00	RT	16.00
STAGE 3		
519+68.49	RT	12.50
	ROUNDED TOTAL	61

BITUMINOUS MATERIALS (TACK COAT)

FROM STATION	TO STATION	LT/RT	POUND
523+00.67	523+35.67	RT	66.58
523+00.68	523+35.68	LT	95.98
527+73.87	528+08.87	RT	107.49
527+73.93	528+08.93	LT	107.49
		ROUNDED TOTAL	378

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

FROM STATION	TO STATION	LT/RT	AREA (SQ YD)
523+00.67	523+35.67	RT	147.95
523+00.68	523+35.68	LT	213.30
527+73.87	528+08.87	RT	238.87
527+73.93	528+08.93	LT	238.87
		ROUNDED TOTAL	839

TERMINAL MARKER - DIRECT APPLIED

FROM STATION	LT/RT	EACH
528+52.29	LT	1
	TOTAL	1

TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT

FROM STATION	TO STATION	LT/RT	EACH
528+03.98	528+52.29	LT	1
		TOTAL	1

PAVEMENT MARKING SCHEDULE

FROM STATION	TO STATION	LT/RT	THERMOPLASTIC PAVEMENT MARKING					PREFORMED PLASTIC PAVEMENT MARKING, TYPE D	MODIFIED URETHANE PAVEMENT MARKING			RAISED REFLECTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	GROOVING FOR RECESSED PAVEMENT MARKING 6"	GROOVING FOR RECESSED PAVEMENT MARKING 8"	GROOVING FOR RECESSED PAVEMENT MARKING 9"	RAISED REFLECTIVE PAVEMENT MARKER REFLECTOR REMOVAL	RAISED REFLECTIVE PAVEMENT MARKER REFLECTOR REPLACEMENT	PREF PLASTIC PMK TYPE B INLAID LINE 7"
			L&S	LINE 4"	LINE 6"	LINE 8"	LINE 12"	LINE 5"	LINE 4"	LINE 8"	LINE 12"								
			SQ FT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	FOOT	EACH	EACH	FOOT	FOOT	FOOT	EACH	EACH	FOOT
518+76.42	523+35.67	RT		918.66				229.66						229.66			23	23	
519+19.79	522+01.65	LT		281.87															
519+19.79	523+35.67	LT		415.88				208.53						208.53			21	21	
522+02.98	523+08.61	LT				215.37	48.64												
522+08.79	523+35.68	LT				129.53													
523+08.61	526+84.87	LT								94.07						94.07			
523+35.67	527+73.89	RT						219.11	876.88			22	22	219.11					
523+35.67	527+73.92	LT						219.13	438.26					219.13					
523+35.68	527+73.54	LT								438.02		22	22						
523+95.96	529+41.86	RT													136.48				136.48
526+59.12	529+41.86	RT													70.69				70.69
526+84.87	527+73.93	LT								178.24	12.51								
527+73.54	528+41.27	LT				68.77													
527+73.84	530+72.59	RT		298.93				149.44						149.44			15	15	
527+73.91	528+47.87	LT				147.86	22.92												
527+73.92	530+72.59	RT		298.83															
527+73.92	532+16.45	LT		442.60				223.28						223.28			23	23	
528+47.87	531+51.70	LT		303.89															
529+41.86	530+72.59	RT			392.19														
529+54.78		RT	36.4																
529+54.94		RT	72.8																
		ROUNDED TOTAL	110	2,961	393	562	72	1,250	1,316	711	13	44	44	1,250	208	95	82	82	208

POLYMER CONCRETE

FROM STATION	TO STATION	LT/RT	CU FT
523+35.68	523+38.68	RT	25.64
523+35.68	523+38.68	LT	36.97
527+70.93	527+73.93	RT	41.29
527+70.93	527+73.93	LT	41.60
		ROUNDED TOTAL	146

MAINTENANCE OR TRAFFIC GENERAL NOTES

1. THE MAINTENANCE OF TRAFFIC PLANS SHALL SERVE AS A GUIDE FOR THE SAFE DIVERSION OF TRAFFIC DURING THE EXECUTION OF THIS CONTRACT. THE CONTRACTOR MAY MODIFY THE MAINTENANCE OF TRAFFIC PLANS TO MEET CONSTRUCTION NEEDS BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL PLANS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

2. EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PAVEMENT MARKING REMOVAL - WATER BLASTING.

3. ALL EXISTING PAVEMENT MARKING LINES AND EXISTING RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS ALONG ILLINOIS ROUTE 83 THAT ARE REMOVED AS A RESULT OF A CONFLICT WITH THE REVISED TRAFFIC PATTERNS, OUTSIDE OF THE PAVEMENT MARKING LIMITS SHOWN IN THE PLANS, SHALL BE RE-ESTABLISHED FOR PROPOSED STRIPING AT THE COMPLETION OF THIS CONTRACT. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF PAVEMENT MARKING REMOVAL - WATER BLASTING AND RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL. THE EXACT LOCATIONS OF ALL PROPOSED PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.

4. THE REMOVAL OF ALL PAVEMENT MARKING TAPE WILL BE PAID FOR AT THE CONTRACT UNIT PRICE SQUARE FOOT FOR SHORT TERM PAVEMENT MARKING REMOVAL.

5. PAVEMENT MARKING TAPE, TYPE IV SHALL BE USED FOR ANY SHORT TERM PAVEMENT MARKINGS ON FINAL SURFACES.

6. ALL TRAFFIC CONTROL DEVICES SHALL BE REFLECTORIZED PRIOR TO INSTALLATION AND CLEANED AS SPECIFIED IN THE TRAFFIC CONTROL SPECIAL PROVISIONS OR AS DIRECTED BY THE ENGINEER.

7. FOR STABILIZATION, ANY REQUIRED TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SAND BAGS PER BARRICADE.

8. EXISTING SIGNS WITHIN THE LIMITS OF TRAFFIC CONTROL WHICH ARE OBSTRUCTED BY OR OTHERWISE INTERFERED WITH BY CONSTRUCTION OPERATIONS OF DESIGNATED TRAFFIC CONTROL, SHALL BE COVERED OR REMOVED BY THE CONTRACTOR UNLESS SPECIFIED IN THE PLANS OR WHEN DIRECTED BY THE ENGINEER. THIS WORK SHALL BE AS SPECIFIED IN ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.

9. CHANGEABLE MESSAGE BOARDS WILL BE PLACED 2 WEEKS PRIOR TO START OF WORK, AT LOCATIONS DETERMINED BY THE ENGINEER, FOR ADVANCED WARNING.

10. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING ANY WORK ALONG IL-83 OR 22ND ST.

11. CONTRACTOR SHALL REFER TO ILLINOIS TOLLWAY STANDARD DRAWINGS E1-07, E2-10, AND E3-09 FOR MAINTENANCE OF TRAFFIC REQUIRED ALONG I-88. LANE CLOSURE HOURS SHALL BE AS SPECIFIED IN THE SPECIAL PROVISIONS.

12. CONTRACTOR TO PLACE ADVANCED SIGNAGE AT THE APPLICABLE LOCATIONS ALONG THE APPROACH LEGS OF THE ILLINOIS ROUTE 83 INTERSECTIONS WITH 31ST STREET AND 22ND STREET UTILIZING DISTRICT 1 STANDARD TC-10.

SUGGESTED SEQUENCE OF OPERATIONS

STAGE 1

1. INSTALL CHANGEABLE MESSAGE SIGNS 14 DAYS AHEAD OF RAMP CLOSURE. SET UP DETOUR SIGNING AS SHOWN ON THE DETOUR PLAN
2. BEFORE BEGINNING ANY WORK, UTILIZE TEMPORARY LANE CLOSURES, DURING THE ALLOWABLE HOURS LISTED IN THE SPECIAL PROVISIONS, TO INSTALL PROTECTIVE SHIELDING UNDERNEATH S.N. 022-0047 AT LOCATIONS SHOWN ON THE STRUCTURAL PLANS.
3. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS UTILIZING HIGHWAY STANDARDS 701400, 701401, 701411, & 701428 AS WELL AS DISTRICT STANDARDS TC-08, TC-09, TC-14, & TC-17 AND AS SHOWN IN THE STAGE 1 STAGING PLANS.
4. CLOSE NB LANE 3 UTILIZING HIGHWAY STANDARDS 701400, 701401, & 701411 AS WELL AS DISTRICT STANDARD TC-14.
5. CLOSE THE WB I-88 TO SB IL-83 ENTRANCE RAMP AND SB IL-83 TO EB I-88 EXIT RAMP UTILIZING DISTRICT STANDARD TC-08.
6. SHIFT NB AND SB TRAFFIC TO THE INSIDE AND CLOSE NB AND SB OUTSIDE SHOULDERS UTILIZING DISTRICT STANDARD TC-09 AND AS SHOWN IN THE STAGE 1 STAGING PLANS.
7. PERFORM BRIDGE DECK AND BRIDGE PARAPET REPAIRS, OUTSIDE SHOULDER RECONSTRUCTION, AND CURB AND GUTTER REPLACEMENT ALONG OUTSIDE SHOULDER AND LANE 3 AS SHOWN IN THE STAGE 1 STAGING AND STRUCTURAL PLANS.
8. CONSTRUCT TEMPORARY PAVEMENT AT THE LOCATIONS SHOWN IN THE STAGE 1 STAGING PLANS.
9. ADJUST EXISTING DRAINAGE STRUCTURES ALONG OUTSIDE SHOULDER AT LOCATIONS SHOWN IN THE ROADWAY PLAN.
10. REMOVE EXISTING GUARDRAIL AND INSTALL PROPOSED GUARDRAIL AT LOCATIONS SHOWN IN THE ROADWAY PLAN.

STAGE 2

1. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS UTILIZING HIGHWAY STANDARDS 701400, 701401, 701411, & 701428 AS WELL AS DISTRICT STANDARDS TC-08, TC-09, TC-14, TC-17, & TC-25 AND AS SHOWN IN THE STAGE 2 STAGING PLANS.
2. CLOSE NB AND SB LANE 1 UTILIZING HIGHWAY STANDARDS 701400 AND 701401.
3. SHIFT NB TRAFFIC IN LANE 2 TO THE INSIDE AND SHIFT NB TRAFFIC IN LANE 3 ONTO THE OUTSIDE SHOULDER. SHIFT SB TRAFFIC IN LANES 2 AND 3 ONTO THE OUTSIDE SHOULDER AND LANE 3. CLOSE NB LANE 2 UTILIZING DISTRICT STANDARD TC-25 AND AS SHOWN IN THE STAGE 2 STAGING PLANS AND CLOSE SB LANE 2 UTILIZING DISTRICT STANDARD TC-09 AND AS SHOWN IN THE STAGE 2 STAGING PLANS.
4. PERFORM BRIDGE DECK REPAIRS ALONG NB LANE 2 AND SB LANES 1 AND 2 AND RE-SEAL THE SB LONGITUDINAL OPEN JOINT IN THE MEDIAN AS SHOWN IN THE STAGE 2 STAGING AND STRUCTURAL PLANS.
5. ADJUST EXISTING SB MEDIAN DRAINAGE STRUCTURES AS REQUIRED.
6. REMOVE DETOUR SIGNAGE AND OPEN RAMPS TO TRAFFIC.

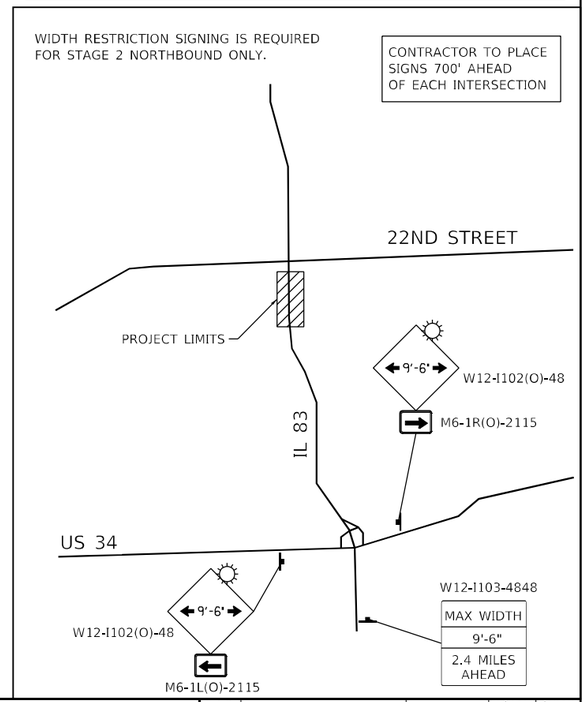
STAGE 3

1. INSTALL TRAFFIC CONTROL DEVICES AND TEMPORARY PAVEMENT MARKINGS UTILIZING HIGHWAY STANDARDS 701400, 701401, 701411, & 701428 AS WELL AS DISTRICT STANDARDS TC-09, & TC-17 AND AS SHOWN IN THE STAGE 3 STAGING PLANS.
2. CLOSE NB LANE 1 UTILZING HIGHWAY STANDARDS 701400 AND 701401.
3. SHIFT NB TRAFFIC ONTO THE OUTSIDE SHOULDER UTILIZING DISTRICT STANDARD TC-09 AND AS SHOWN IN THE STAGE 3 STAGING PLANS.
4. SHIFT SB TRAFFIC SLIGHTLY TO THE OUTSIDE UTILIZING HIGHWAY STANDARD 701428 AS WELL AS DISTRICT STANDARD TC-09 AND AS SHOWN IN THE STAGE 3 STAGING PLANS.
5. PERFORM BRIDGE DECK REPAIRS ALONG NB LANE 1 AND RE-SEAL THE NB LONGITUDINAL OPEN JOINT IN THE MEDIAN AS SHOWN IN THE STAGE 3 STAGING AND STRUCTURAL PLANS.
6. ADJUST EXISTING NB MEDIAN DRAINAGE STRUCTURES AS REQUIRED.

POST-STAGE

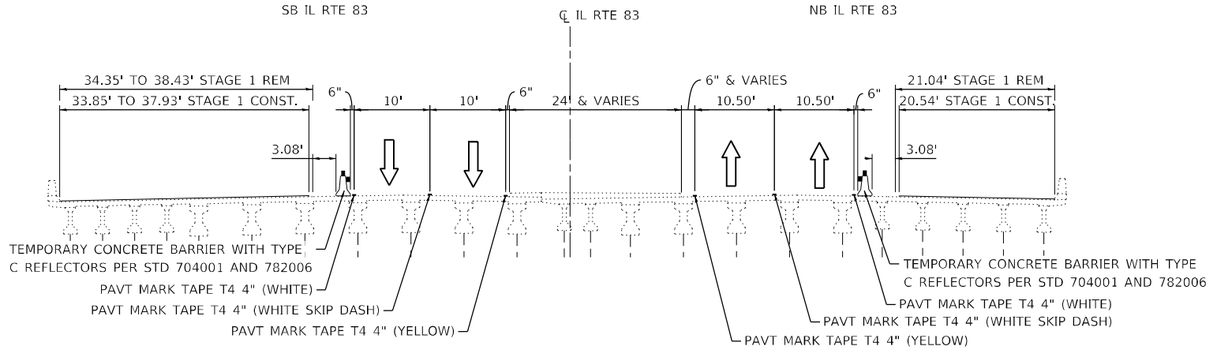
1. PLACE PERMANENT PAVEMENT MARKINGS AT THE LOCATIONS SHOWN AND DESCRIBED IN THE PLANS UTILIZING HIGHWAY STANDARD 701400, 701401, 701428, AND 701446 AS WELL AS DISTRICT STANDARD TC-09 & TC-17 DURING THE ALLOWABLE LANE CLOSURE HOURS.

ADVANCED WIDTH RESTRICTION SIGNING DETAIL - STAGE 2 NB



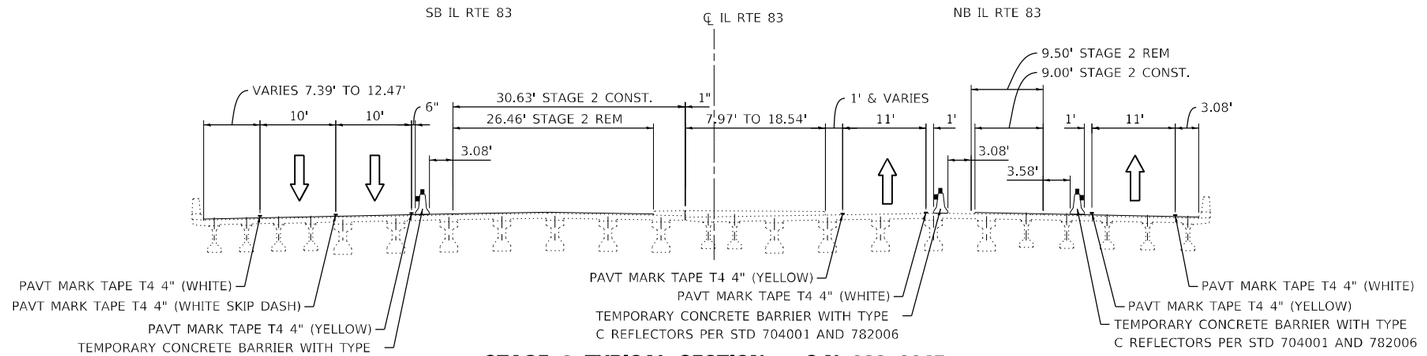
LIN ENGINEERING,LTD. Consulting Engineers Westmont, Illinois	USER NAME = 14nho	DESIGNED - RC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88 STAGING GENERAL NOTES & SUGGESTED SEQUENCE
		DRAWN - IS	REVISED -		
	PLOT SCALE = 100.0000 ' / in.	CHECKED - ST	REVISED -		SCALE: N.T.S. SHEET 1 OF 7 SHEETS STA. TO STA.
	PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	13
		CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	



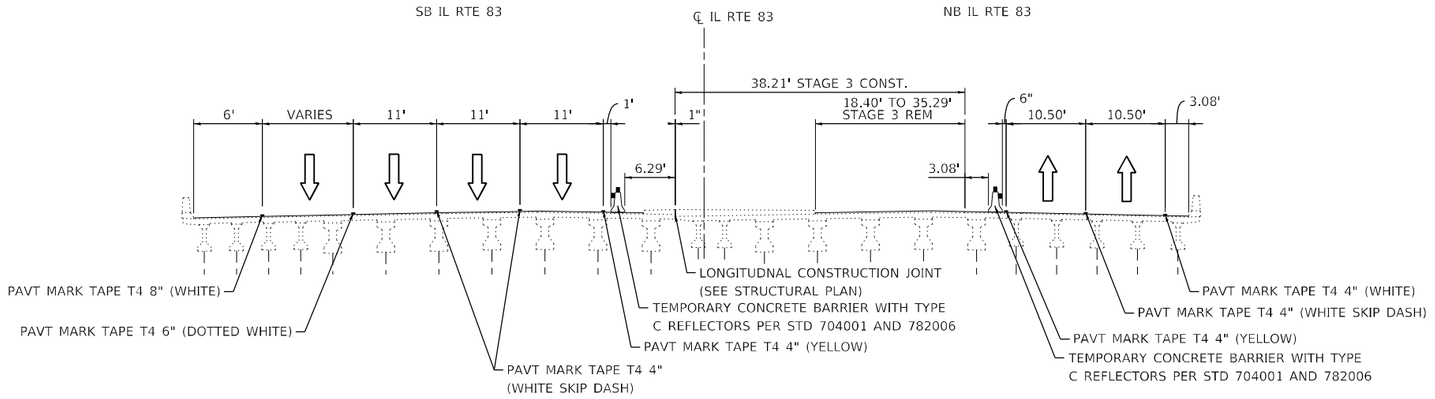
STAGE 1 TYPICAL SECTION – S.N. 022–0047

FAP-344 OVER I-88
LOOKING NORTH



STAGE 2 TYPICAL SECTION – S.N. 022–0047

FAP-344 OVER I-88
LOOKING NORTH

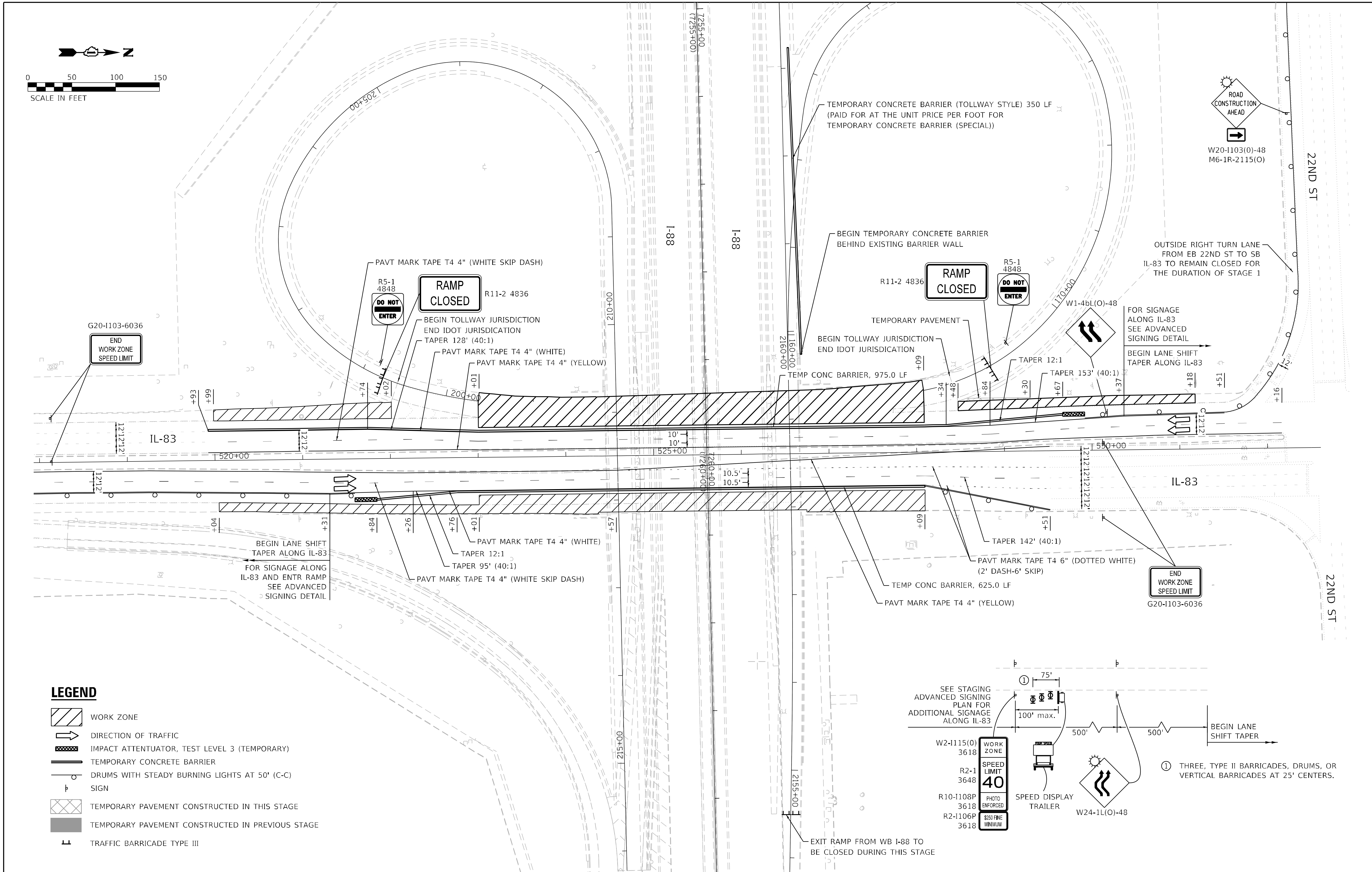


STAGE 3 TYPICAL SECTION – S.N. 022–0047

FAP-344 OVER I-88
LOOKING NORTH

LIN ENGINEERING,LTD. Consulting Engineers Westmont, Illinois	USER NAME = 14nho	DESIGNED - RC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88 STAGING TYPICAL SECTIONS
		DRAWN - IS	REVISED -		
	PLOT SCALE = 20.0000 ' / in.	CHECKED - ST	REVISED -		SCALE: N.T.S. SHEET 2 OF 7 SHEETS STA. TO STA.
	PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	14
		CONTRACT NO. 62M37		
		ILLINOIS FED. AID PROJECT		



LIN ENGINEERING,LTD.
Consulting Engineers
Westmont, Illinois

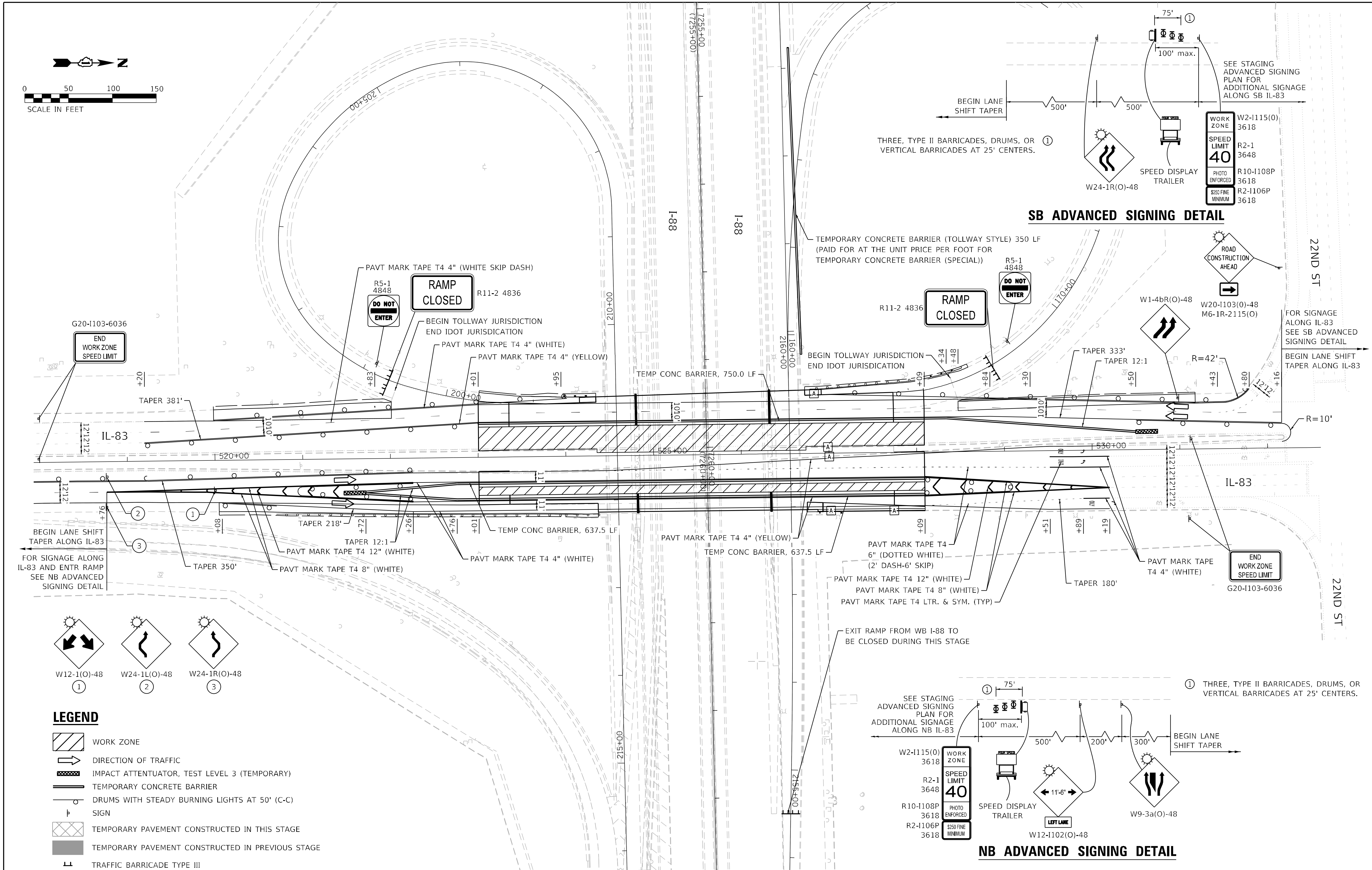
USER NAME = 14nho	DESIGNED - RC	REVISED -	
	DRAWN - IS	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED - ST	REVISED -	
PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
STAGING PLAN - STAGE 1

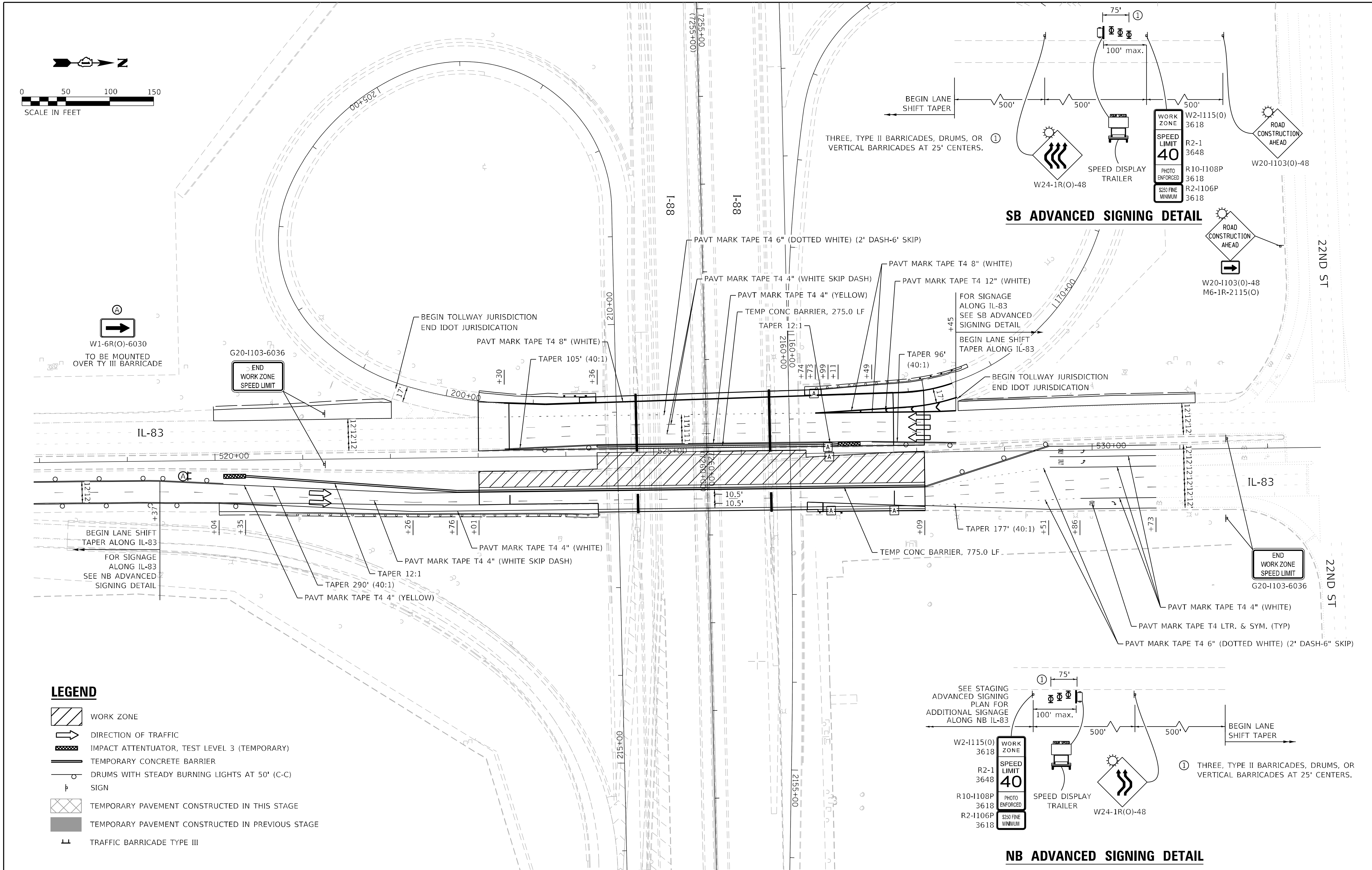
SCALE: 1"=50' | SHEET 3 OF 7 SHEETS | STA. 521+30.52 TO STA. 532+16.45

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	15
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		



0 50 100 150
SCALE IN FEET
I-88
I-88
IL-83
IL-83
22ND ST
22ND ST
SB ADVANCED SIGNING DETAIL
NB ADVANCED SIGNING DETAIL
LEGEND
WORK ZONE
DIRECTION OF TRAFFIC
IMPACT ATTENTUATOR, TEST LEVEL 3 (TEMPORARY)
TEMPORARY CONCRETE BARRIER
DRUMS WITH STEADY BURNING LIGHTS AT 50' (C-C)
SIGN
TEMPORARY PAVEMENT CONSTRUCTED IN THIS STAGE
TEMPORARY PAVEMENT CONSTRUCTED IN PREVIOUS STAGE
TRAFFIC BARRICADE TYPE III
W12-1(O)-48 ①
W24-1L(O)-48 ②
W24-1R(O)-48 ③
G20-I103-6036
END WORK ZONE SPEED LIMIT
R5-1 4848
DO NOT ENTER
RAMP CLOSED
R11-2 4836
PAVT MARK TAPE T4 4" (WHITE SKIP DASH)
BEGIN TOLLWAY JURISDICTION
END IDOT JURISDICATION
PAVT MARK TAPE T4 4" (WHITE)
PAVT MARK TAPE T4 4" (YELLOW)
TAPER 381'
TEMP CONC BARRIER, 750.0 LF
BEGIN TOLLWAY JURISDICTION
END IDOT JURISDICTION
TEMPORARY CONCRETE BARRIER (TOLLWAY STYLE) 350 LF
(PAID FOR AT THE UNIT PRICE PER FOOT FOR
TEMPORARY CONCRETE BARRIER (SPECIAL))
TAPER 333'
TAPER 12:1
R=42'
R=10'
W1-4bR(O)-48
ROAD CONSTRUCTION AHEAD
W20-I103(O)-48
M6-1R-2115(O)
FOR SIGNAGE ALONG IL-83 SEE SB ADVANCED SIGNING DETAIL
BEGIN LANE SHIFT TAPER ALONG IL-83
520+00
525+00
530+00
BEGIN LANE SHIFT TAPER ALONG IL-83
FOR SIGNAGE ALONG IL-83 AND ENTR RAMP SEE NB ADVANCED SIGNING DETAIL
TAPER 218'
TAPER 12:1
TAPER 350'
PAVT MARK TAPE T4 12" (WHITE)
PAVT MARK TAPE T4 8" (WHITE)
PAVT MARK TAPE T4 4" (WHITE)
TEMP CONC BARRIER, 637.5 LF
PAVT MARK TAPE T4 4" (YELLOW)
TEMP CONC BARRIER, 637.5 LF
PAVT MARK TAPE T4 6" (DOTTED WHITE) (2' DASH-6' SKIP)
PAVT MARK TAPE T4 12" (WHITE)
PAVT MARK TAPE T4 8" (WHITE)
PAVT MARK TAPE T4 LTR. & SYM. (TYP)
TAPER 180'
PAVT MARK TAPE T4 4" (WHITE)
EXIT RAMP FROM WB I-88 TO BE CLOSED DURING THIS STAGE
75'
100' max.
SEE STAGING ADVANCED SIGNING PLAN FOR ADDITIONAL SIGNAGE ALONG SB IL-83
BEGIN LANE SHIFT TAPER
500'
500'
THREE, TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES AT 25' CENTERS. ①
W24-1R(O)-48
SPEED DISPLAY TRAILER
WORK ZONE
SPEED LIMIT 40
PHOTO ENFORCED
$250 FINE MINIMUM
W2-I115(O) 3618
R2-1 3648
R10-I108P 3618
R2-I106P 3618
SEE STAGING ADVANCED SIGNING PLAN FOR ADDITIONAL SIGNAGE ALONG NB IL-83
500'
200'
300'
BEGIN LANE SHIFT TAPER
11'-6"
LEFT LANE
W12-I102(O)-48
W9-3a(O)-48
① THREE, TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES AT 25' CENTERS.

LIN ENGINEERING,LTD. Consulting Engineers Westmont, Illinois	USER NAME = 14nho	DESIGNED - RC	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88 STAGING PLAN - STAGE 2	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN - IS	REVISED -			344	2020-182-BR	DUPAGE	51	16
	PLOT SCALE = 100.0000 ' / in.	CHECKED - ST	REVISED -			CONTRACT NO. 62M37				
	PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -		SCALE: 1"=50' SHEET 4 OF 7 SHEETS STA. 518+76.42 TO STA. 532+16.45	ILLINOIS FED. AID PROJECT				



0 50 100 150
SCALE IN FEET
N
W1-6R(O)-6030
TO BE MOUNTED OVER TY III BARRICADE
G20-I103-6036
END WORK ZONE SPEED LIMIT
BEGIN TOLLWAY JURISDICTION
END IDOT JURISDICTION
PAVT MARK TAPE T4 8" (WHITE)
TAPER 105' (40:1)
PAVT MARK TAPE T4 6" (DOTTED WHITE) (2' DASH-6' SKIP)
PAVT MARK TAPE T4 4" (WHITE SKIP DASH)
PAVT MARK TAPE T4 4" (YELLOW)
TEMP CONC BARRIER, 275.0 LF
TAPER 12:1
PAVT MARK TAPE T4 8" (WHITE)
PAVT MARK TAPE T4 12" (WHITE)
TAPER 96' (40:1)
FOR SIGNAGE ALONG IL-83 SEE SB ADVANCED SIGNING DETAIL
BEGIN LANE SHIFT TAPER ALONG IL-83
BEGIN TOLLWAY JURISDICTION
END IDOT JURISDICTION
I-88
I-88
IL-83
IL-83
22ND ST
22ND ST
200+00
205+00
210+00
215+00
520+00
525+00
530+00
2155+00
2160+00
170+00
7255+00
BEGIN LANE SHIFT TAPER ALONG IL-83
FOR SIGNAGE ALONG IL-83 SEE NB ADVANCED SIGNING DETAIL
PAVT MARK TAPE T4 4" (WHITE)
PAVT MARK TAPE T4 4" (WHITE SKIP DASH)
TAPER 12:1
TAPER 290' (40:1)
PAVT MARK TAPE T4 4" (YELLOW)
10.5'
10.5'
TEMP CONC BARRIER, 775.0 LF
TAPER 177' (40:1)
END WORK ZONE SPEED LIMIT
G20-I103-6036
PAVT MARK TAPE T4 4" (WHITE)
PAVT MARK TAPE T4 LTR. & SYM. (TYP)
PAVT MARK TAPE T4 6" (DOTTED WHITE) (2' DASH-6" SKIP)
BEGIN LANE SHIFT TAPER
500'
500'
500'
75'
100' max.
THREE, TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES AT 25' CENTERS.
W24-1R(O)-48
SPEED DISPLAY TRAILER
WORK ZONE
SPEED LIMIT 40
PHOTO ENFORCED
$250 FINE MINIMUM
W2-I115(O) 3618
R2-1 3648
R10-I108P 3618
R2-I106P 3618
ROAD CONSTRUCTION AHEAD
W20-I103(O)-48
SB ADVANCED SIGNING DETAIL
ROAD CONSTRUCTION AHEAD
W20-I103(O)-48
M6-1R-2115(O)
LEGEND
WORK ZONE
DIRECTION OF TRAFFIC
IMPACT ATTENTUATOR, TEST LEVEL 3 (TEMPORARY)
TEMPORARY CONCRETE BARRIER
DRUMS WITH STEADY BURNING LIGHTS AT 50' (C-C)
SIGN
TEMPORARY PAVEMENT CONSTRUCTED IN THIS STAGE
TEMPORARY PAVEMENT CONSTRUCTED IN PREVIOUS STAGE
TRAFFIC BARRICADE TYPE III
SEE STAGING ADVANCED SIGNING PLAN FOR ADDITIONAL SIGNAGE ALONG NB IL-83
75'
100' max.
500'
500'
BEGIN LANE SHIFT TAPER
W2-I115(O) 3618
R2-1 3648
R10-I108P 3618
R2-I106P 3618
WORK ZONE
SPEED LIMIT 40
PHOTO ENFORCED
$250 FINE MINIMUM
SPEED DISPLAY TRAILER
W24-1R(O)-48
THREE, TYPE II BARRICADES, DRUMS, OR VERTICAL BARRICADES AT 25' CENTERS.
NB ADVANCED SIGNING DETAIL

LIN ENGINEERING, LTD.
Consulting Engineers
Westmont, Illinois

USER NAME = 14nho	DESIGNED - RC	REVISED -	
	DRAWN - IS	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED - ST	REVISED -	
PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

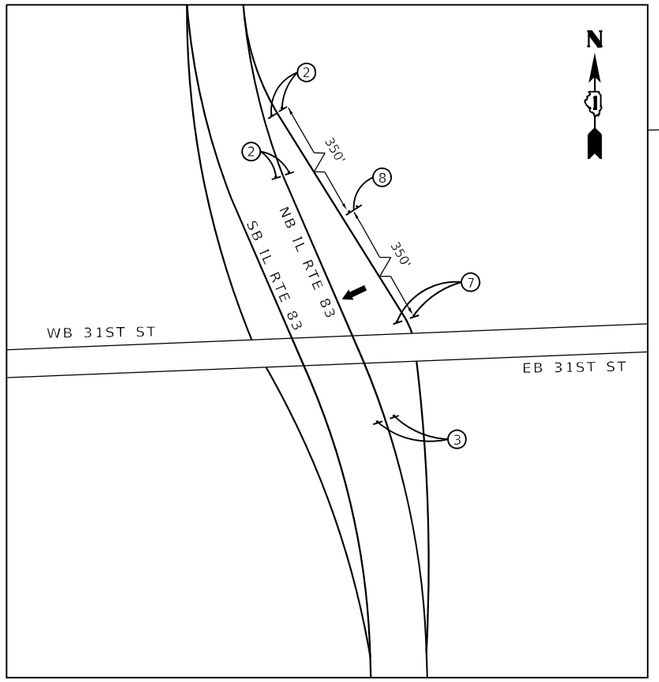
F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
STAGING PLAN - STAGE 3

SCALE: 1"=50' SHEET 5 OF 7 SHEETS STA. 519+36.89 TO STA. 530+72.59

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	17

CONTRACT NO. 62M37

ILLINOIS FED. AID PROJECT

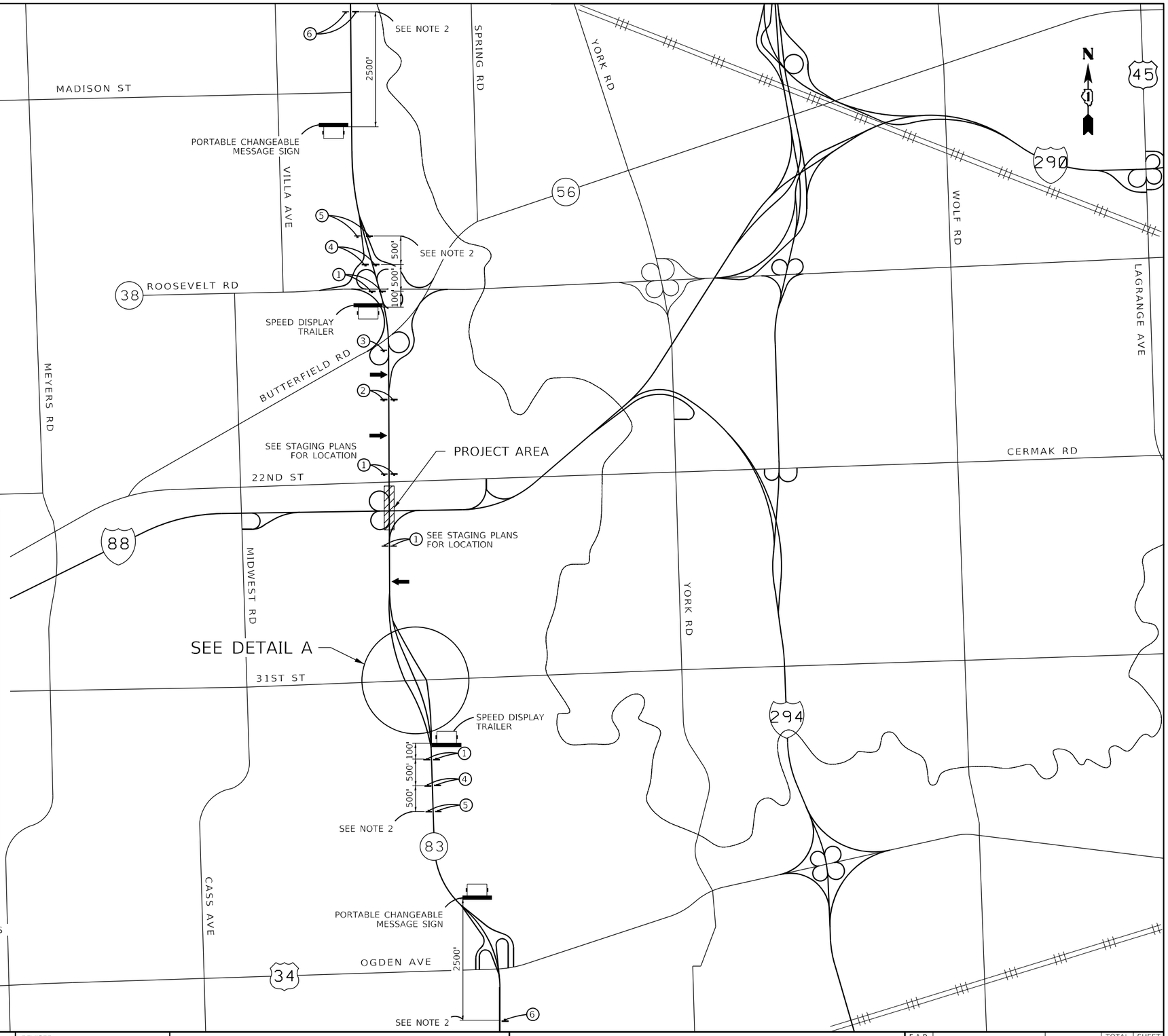


DETAIL A

SIGN LEGEND

NOTES:

1. ENGINEER WILL CONFIRM THE MESSAGES AND LOCATIONS OF PORTABLE CHANGEABLE MESSAGE SIGNS.
2. ALL ADVANCED WORK ZONE SIGNAGE SHALL BE LOCATED AND PLACED PER HIGHWAY STANDARDS 701400, 701401, 701411, & 701601 (SOUTHBOUND IL 83), DISTRICT STANDARDS TC-08, TC-10 AND TC-14, AS SHOWN IN THE PLANS, & AS DETERMINED BY THE ENGINEER UNLESS OTHERWISE NOTED ON THE PLANS.
3. ALL EXISTING SIGNS THAT CONFLICT WITH ADVANCED WORK ZONE SIGNAGE SHALL BE COVERED FOR THE DURATION OF CONSTRUCTION. THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS TRAFFIC CONTROL ITEMS.



LIN ENGINEERING,LTD. Consulting Engineers Westmont, Illinois	USER NAME = 14nho	DESIGNED - RC	REVISED -
		DRAWN - IS	REVISED -
	PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -
	PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
STAGING ADVANCED SIGNING PLAN

SCALE: N.T.S. | SHEET 6 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	18
		CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			

SIGN LEGEND

1	DETOUR M4-8 36X18	8	INTERSTATE 88 M1-1 36X36
2	SOUTH M3-3 (BL) 36X18	9	ILLINOIS 83 M1-5 36X36
3	EAST M3-2 (BL) 36X18	10	DETOUR AHEAD W20-2-4848
4	M6-3 (3021) (BL)	11	END DETOUR M4-8a 30X15
5	M6-2R (3021) (BL)	12	PORTABLE CHANGEABLE MESSAGE SIGN
5A	M5-2R (3021) (BL)	13	RAMP CLOSED AHEAD W20-3-4848
6	M6-1L (3021) (BL)		
6A	M6-1R (3021) (BL)		
7	M5-1R (3021) (BL)		
7A	M5-1L (3021) (BL)		

IL-83 SOUTHBOUND DETOUR

IL-83 NORTHBOUND DETOUR

PCMS MESSAGE BEFORE CLOSURE

PHASE 1
RAMP TO
EB I-88
PHASE 2
CLOSED
FROM
XX-XX-XX

PCMS MESSAGE AFTER START OF CLOSURE

PHASE 1
RAMP TO
EB I-88
CLOSED
PHASE 2
FOLLOW
DETOUR

PCMS MESSAGE BEFORE CLOSURE

PHASE 1
RAMP TO
SB IL-83
PHASE 2
CLOSED
FROM
XX-XX-XX

PCMS MESSAGE AFTER START OF CLOSURE

PHASE 1
RAMP TO
SB IL-83
CLOSED
PHASE 2
FOLLOW
DETOUR

VILLA AVE
BUTTERFIELD RD
16TH ST
SPRING RD
22ND ST
S MEYERS RD
YORK RD
MIDWEST RD
31ST ST
35TH ST
S HIGHLAND AVE
38
56
88
83
294
N

NOTES:

1. ENGINEER WILL CONFIRM THE MESSAGES AND THE LOCATIONS OF THE PORTABLE CHANGEABLE MESSAGE SIGNS.
2. CONTRACTOR SHALL PLACE "RAMP CLOSED" PLAQUE OVER ALL EXISTING ADVANCED SIGNAGE FOR SB IL-83 EXIT RAMP (EXIT 137) FROM WB I-88.
3. ADD PORTABLE CHANGEABLE MESSAGE SIGN ALONG WB I-88 ENTRANCE RAMP FROM YORK ROAD AT 22ND ST ADVANCED SIGN TRUSS (EXIT 138).
4. ADD PORTABLE CHANGEABLE MESSAGE SIGNS ALONG ENTRANCE RAMP FROM NB I-294 AT ADVANCED GUIDE SIGNS FOR 22ND ST EXIT (EXIT 138).
5. ADD PORTABLE CHANGEABLE MESSAGE SIGNS AT ½MILE AND ¾MILE 22ND ST ADVANCED SIGN TRUSSES ALONG WB I-88.
6. CHANGEABLE MESSAGE SIGNS TO BE PLACED 14 DAYS PRIOR TO CLOSURE.
7. THE IL-83 SOUTHBOUND DETOUR WILL BE IN PLACE FOR 6 WEEKS. THE IL-83 NORTHBOUND DETOUR WILL BE IN PLACE FOR 6 WEEKS.

LIN ENGINEERING,LTD.
Consulting Engineers
Westmont, Illinois

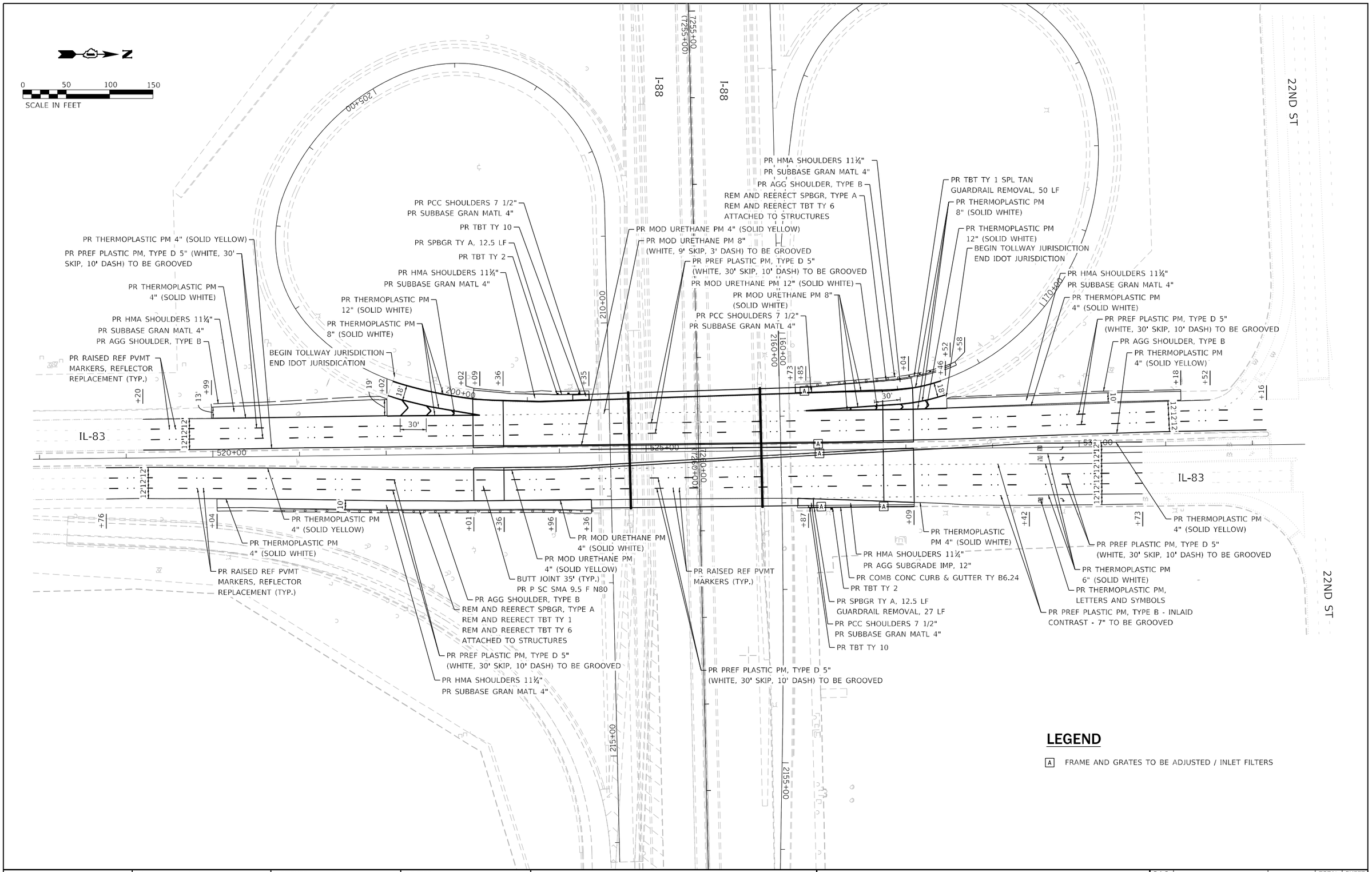
USER NAME = 14nho	DESIGNED - RC	REVISED -
	DRAWN - IS	REVISED -
PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -
PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
DETOUR PLAN

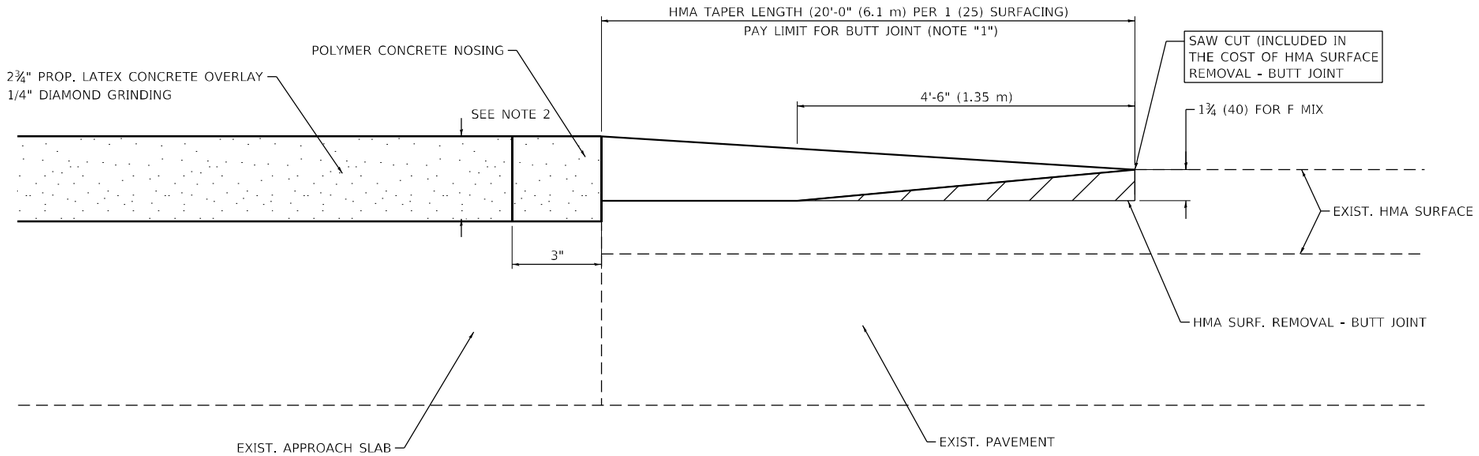
SCALE: N.T.S. | SHEET 7 OF 7 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	19
		CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			



LIN ENGINEERING,LTD. Consulting Engineers Westmont, Illinois	USER NAME = 14nho	DESIGNED - IS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88 ROADWAY PLAN
		DRAWN - IS	REVISED -		
	PLOT SCALE = 100.0000 ' / in.	CHECKED - ST	REVISED -		SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. 518+76.42 TO STA. 532+16.45
	PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	20
		CONTRACT NO. 62M37		
		ILLINOIS FED. AID PROJECT		



BUTT JOINT AND HMA TAPER FOR SCARIFICATION AND RESURFACING

NOTES:

1. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
2. SEE BRIDGE PLANS FOR SCARIFICATION THICKNESS.
3. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

LIN ENGINEERING,LTD. Consulting Engineers Westmont, Illinois	USER NAME = 14nho	DESIGNED - RC	REVISED -
		DRAWN - RC	REVISED -
	PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -
	PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: N.T.S. | SHEET 2 OF 2 SHEETS | STA. | TO STA.

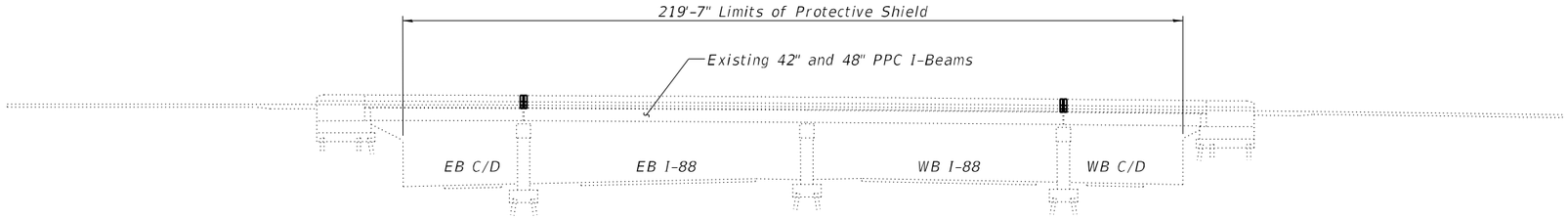
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	21
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		

Existing Structure: SN 022-0047 built in 1958 as FAP Rte. 872, Section 543-R-3 (82) at Sta. 525+60.80. In 1986, the deck was replaced and the bridge was widened in kind. The structure is a 4-span bridge with a 7½" deck on PPC I-beams, measuring 239'-3" back to back abutments with a 0°39'20" right ahead skew. The out to out of the northbound deck is 69'-4" while the southbound deck varies 65'-0⅞" to 70'-1⅞". The concrete substructure units are comprised of stub abutments and multi-column piers. Stage construction will be utilized to maintain two lanes of traffic in each direction.

INDEX OF SHEETS

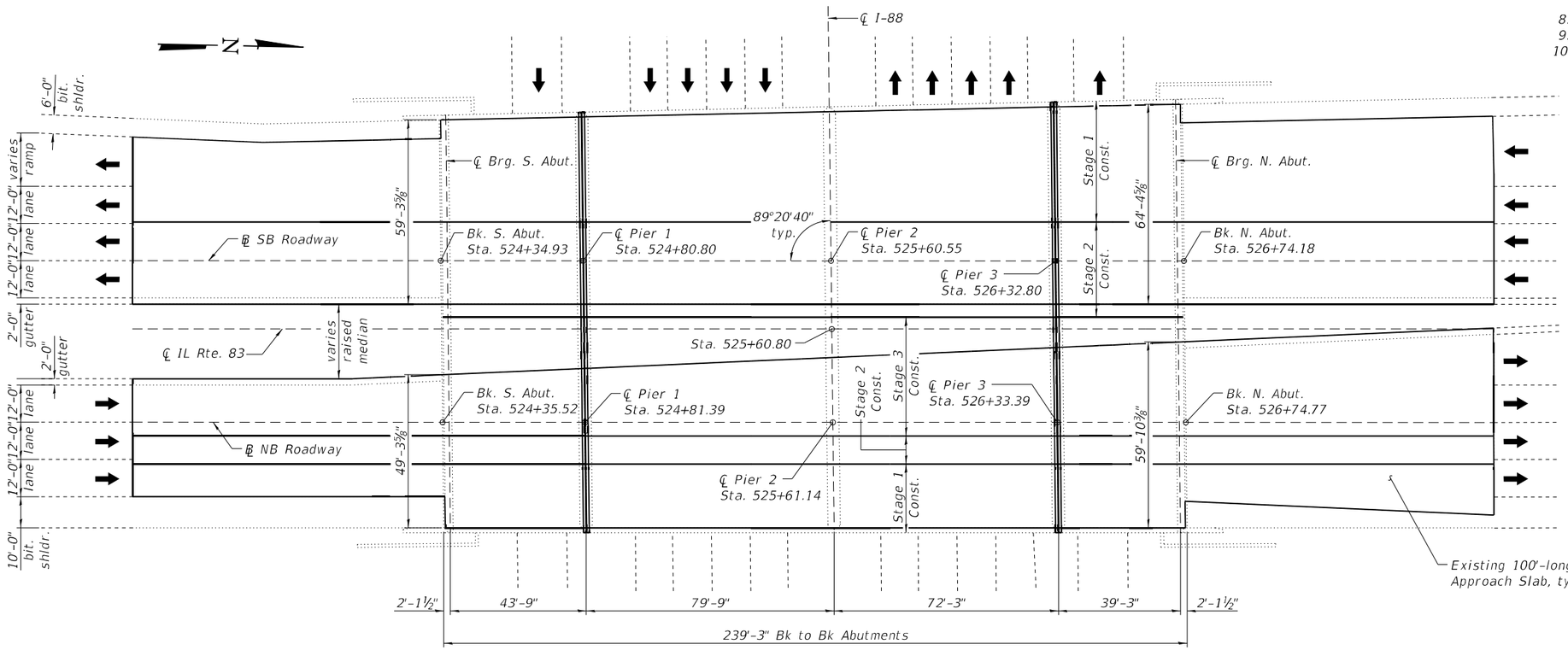
SCOPE OF WORK

1. Remove portions of existing concrete deck and parapets as required to replace expansion joints over Piers 1 and 3.
2. Perform ¾" scarification to top of existing bridge deck and approach slabs.
3. Complete concrete repairs to the bridge deck, approach slabs and parapets.
4. Provide new strip seal expansion joints and adjacent superstructure concrete over Piers 1 and 3.
5. Place 2¾" latex concrete overlay on deck and approach slabs.
6. Perform ¼" diamond grinding on new overlay.
7. Perform bridge deck grooving on new overlay and apply protective coat to new overlay and the top/inside surface of new parapet and median concrete.
8. Replace the longitudinal open joint seal in median.
9. Cut and seal bolts in parapets from removed signage.
10. Apply Concrete Sealer to existing median surface and the top/inside surface of existing parapets.



ELEVATION

Note:
Up to ¼" may be ground off the bridge deck and approach slab overlays.



LOADING HS-20

No allowance for future wearing surface.

PLAN

DESIGN SPECIFICATIONS

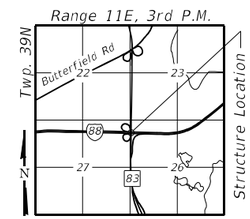
(New Construction)
2002 AASHTO Standard Specifications for Highway Bridges

DESIGN STRESSES

FIELD UNITS - NEW CONSTRUCTION
f'c = 4,000 psi (Superstructure)
fy = 60,000 psi (Reinforcement)

MICHAEL T. HALEY 081-005991 CHATHAM
LICENSED STRUCTURAL ENGINEER ★ STATE OF ILLINOIS ★

Michael T. Haley 01/23/2024
Michael T. Haley Date
Licensed Structural Engineer
State of Illinois No. 081-005991
Expires 11/30/2024



LOCATION SKETCH

GENERAL PLAN & ELEVATION
IL RTE. 83 OVER I-88
F.A.P. RTE. 344 - SEC. 2020-182-BR
DUPAGE COUNTY
STATION 525+60.80
STRUCTURE NO. 022-0047

LIN ENGINEERING, LTD. Consulting Engineers Springfield, Illinois	USER NAME =	DESIGNED - MTH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
		CHECKED - RJM	REVISED -	
	PLOT SCALE =	DRAWN - AJF	REVISED -	
	PLOT DATE = 1/23/2024	CHECKED - MTH	REVISED -	

SHEET 1 OF 8 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	22
		CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	

MODEL: Default
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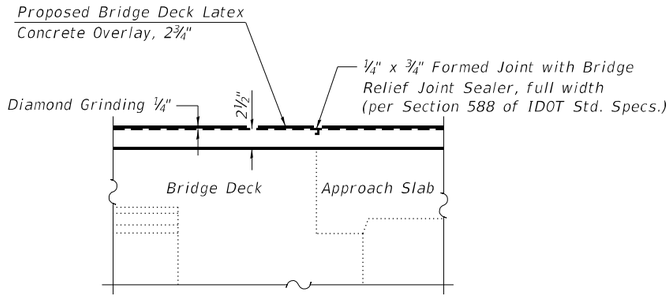
GENERAL NOTES

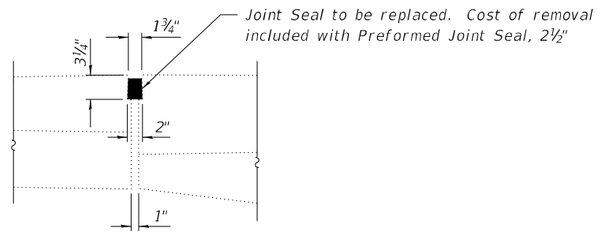
1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
4. Cost of removal and disposal of existing expansion joints shall be included in the cost of Concrete Removal.
5. Protective Coat shall be applied to the inside and top faces of parapets, new overlay and top surface of new concrete adjacent to joints.
6. Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the concrete adjacent to joints is poured at an ambient temperature other than 50°F.
7. Expansion joints shall be fabricated to conform to the existing cross slope of the bridge.
8. The Contractor shall exercise care during removal of existing joints to ensure that the slab, beams, and diaphragms' integrity will not be detrimentally impacted. The Contractor shall repair any damage(s) to the slab, beams and diaphragms caused by their operation as directed by the Engineer at no additional cost to the Department.



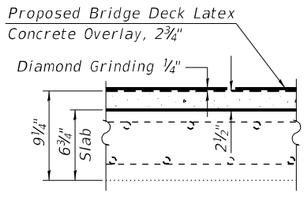
OVERLAY AT ABUTMENTS

TOTAL BILL OF MATERIAL

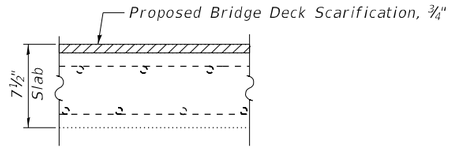
ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	10.0	–	10.0
Protective Shield	Sq. Yd.	3,757	–	3,757
Concrete Superstructure	Cu. Yd.	12.5	–	12.5
Protective Coat	Sq. Yd.	5,383	–	5,383
Reinforcement Bars, Epoxy Coated	Pound	1,480	–	1,480
Bar Splicers	Each	24	–	24
Preformed Joint Seal, 2 1/2"	Foot	239	–	239
Preformed Joint Strip Seal	Foot	272	–	272
Concrete Sealer	Sq. Ft.	6,824	–	6,824
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	5,179	–	5,179
Approach Slab Repair (Full Depth)	Sq. Yd.	4	–	4
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq. Yd.	5,374	–	5,374
Bridge Deck Scarification, 3/4 Inch	Sq. Yd.	5,374	–	5,374
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	20	–	20
Diamond Grinding (Bridge Section)	Sq. Yd.	4,790	–	4,790



LONGITUDINAL JOINT DETAIL



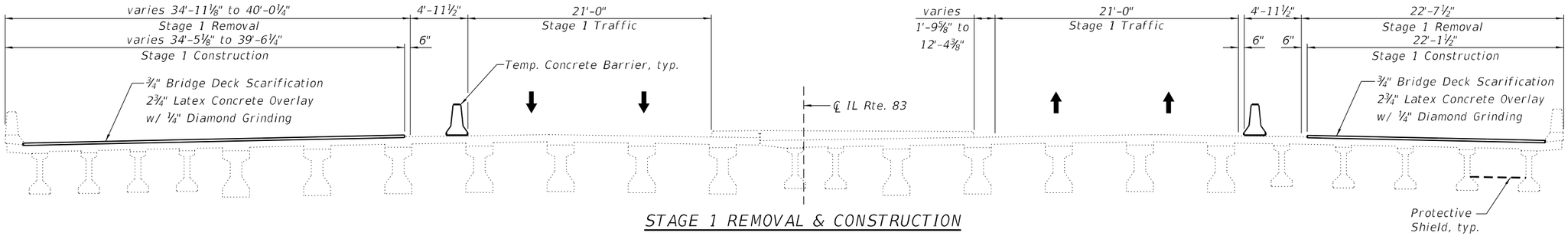
PROPOSED BRIDGE DECK CROSS SECTION



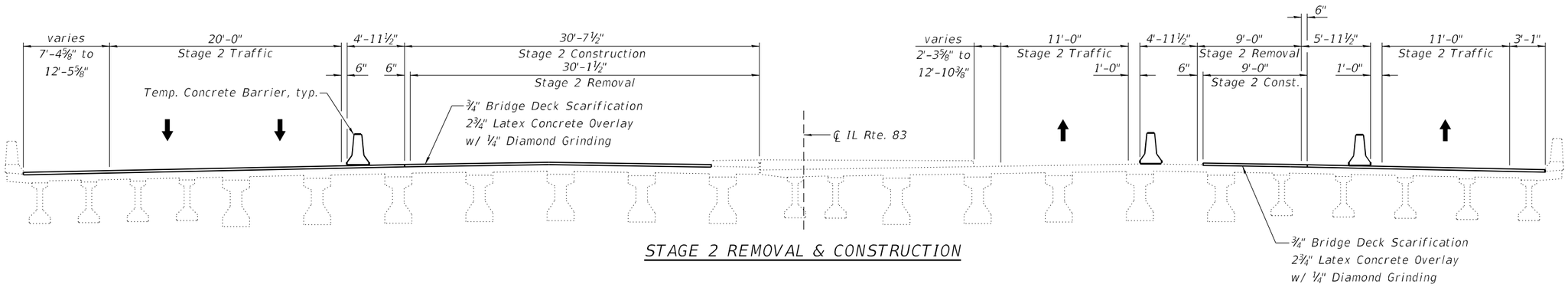
EXISTING BRIDGE DECK CROSS SECTION

LIN ENGINEERING, LTD. Consulting Engineers Springfield, Illinois	USER NAME =	DESIGNED - MTH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL DATA STRUCTURE NO. 022-0047
		CHECKED - RJM	REVISED -		
	PLOT SCALE =	DRAWN - AJF	REVISED -		
	PLOT DATE = 1/23/2024	CHECKED - MTH	REVISED -		SHEET 2 OF 8 SHEETS

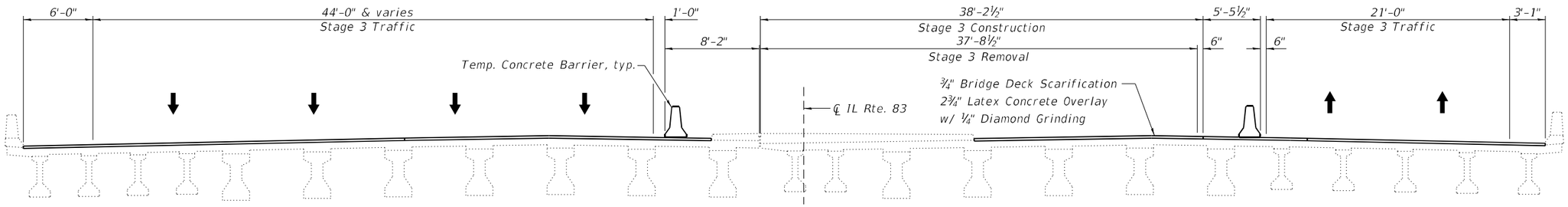
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	23
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		



STAGE 1 REMOVAL & CONSTRUCTION



STAGE 2 REMOVAL & CONSTRUCTION



STAGE 3 REMOVAL & CONSTRUCTION

Notes:
All sections are looking north.
See Roadway Plans for Temporary Concrete Barrier quantities.

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Springfield, Illinois

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS
STRUCTURE NO. 022-0047
SHEET 3 OF 8 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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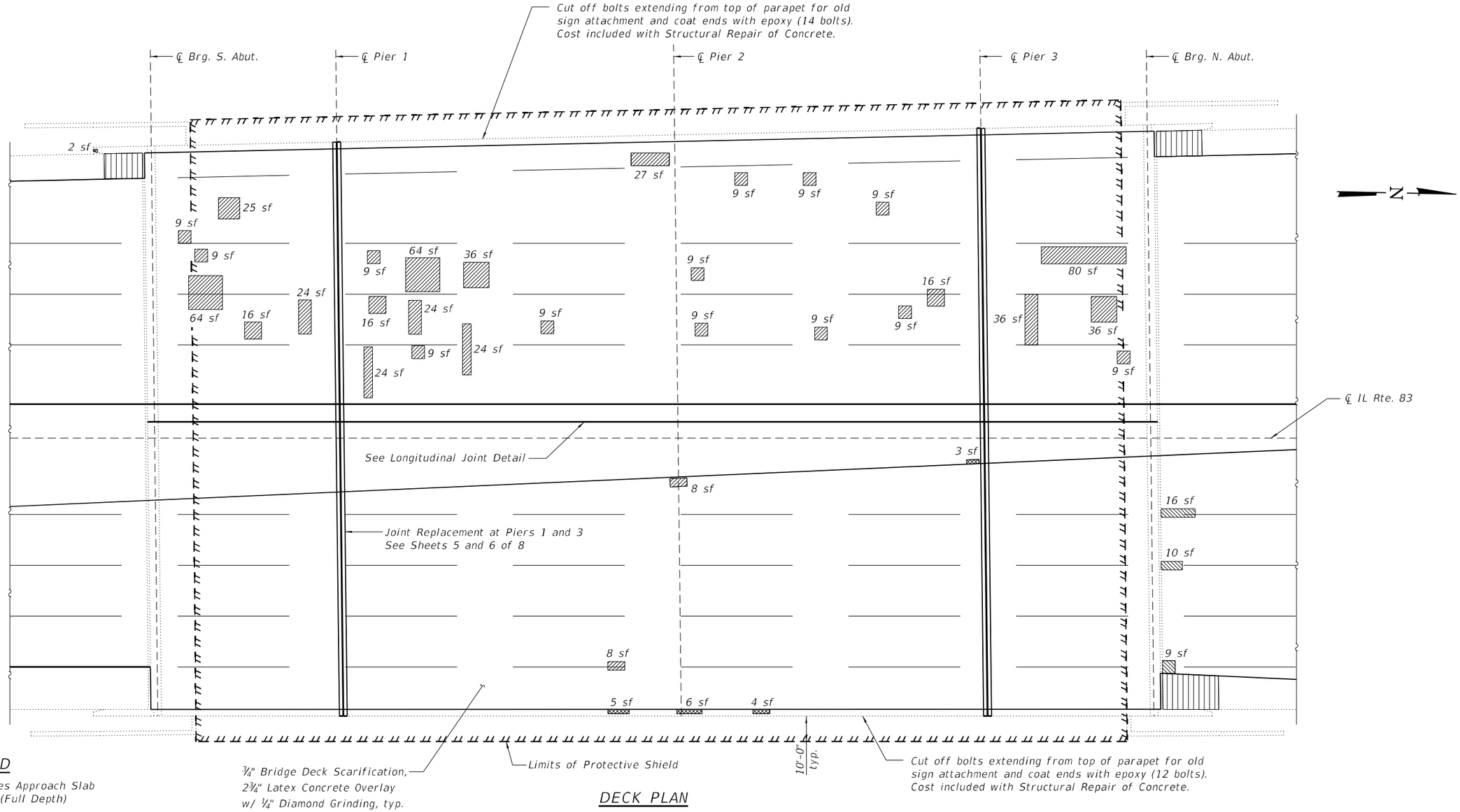
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		



DECK PLAN

LEGEND

- Indicates Approach Slab Repair (Full Depth)
- Indicates Deck Slab Repair (Partial) (For information only)
- Indicates Structural Repair of Concrete (Depth ≤ 5")
- Indicates Approach Shoulder Pavement (See Roadway Plans)
- Indicates limits of Protective Shield
- sf – Square Feet

Notes:
Repair areas shown are estimated. The Engineer shall document actual locations of repairs on As-Built Plans.
Quantity listed for Deck Slab Repair (Partial) is for information only. See Bridge Deck Latex Concrete Overlay Special Provisions.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Approach Slab Repair (Full Depth)	Sq. Yd.	4
Structural Repair of Concrete (Depth ≤ 5")	Sq. Ft.	20

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PLOT DATE = 1/23/2024	CHECKED - MTH	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK SLAB REPAIR PLAN
STRUCTURE NO. 022-0047
SHEET 4 OF 8 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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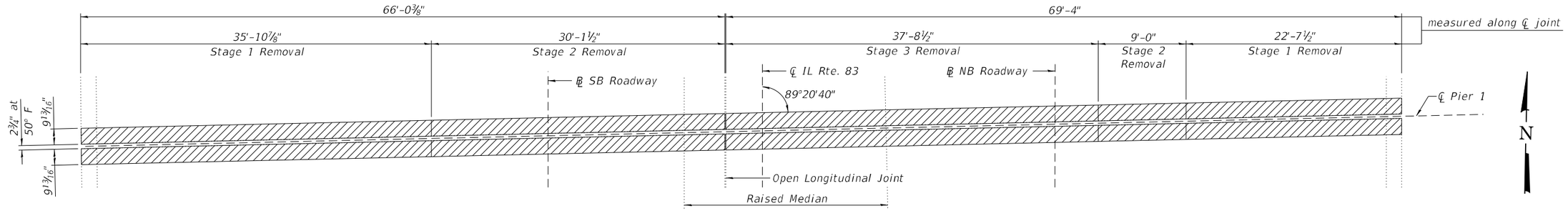
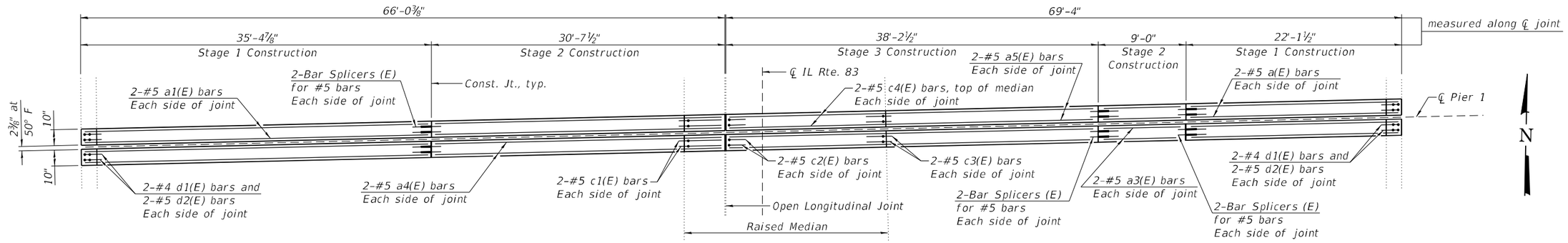
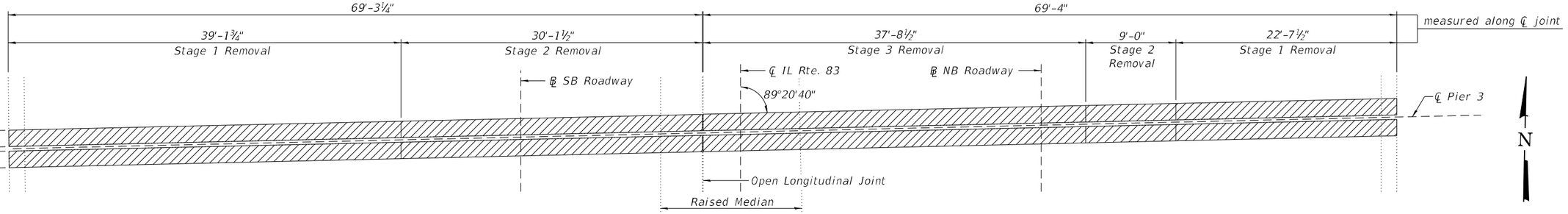
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		



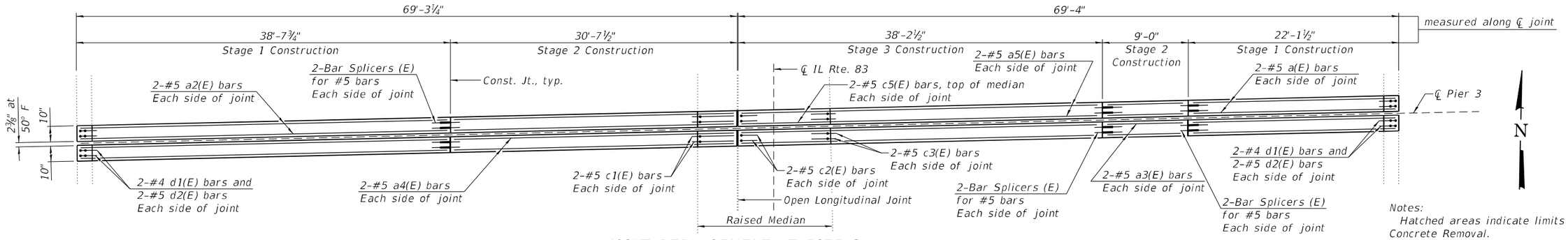
JOINT REMOVAL AT PIER 1



JOINT REPLACEMENT AT PIER 1



JOINT REMOVAL AT PIER 3



JOINT REPLACEMENT AT PIER 3

Notes:
Hatched areas indicate limits of Concrete Removal.
See Sheet 6 of 8 for Sections.
Dimensions at right angles unless noted otherwise.

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PLOT SCALE =		DRAWN - AJF	REVISED -	
PLOT DATE =	1/23/2024	CHECKED - MTH	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JOINT REPLACEMENT DETAILS
STRUCTURE NO. 022-0047
SHEET 5 OF 8 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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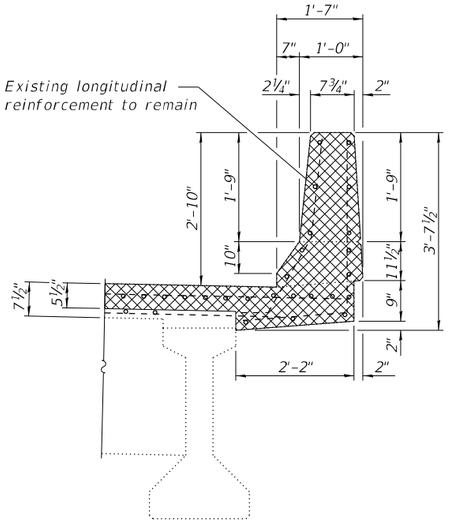
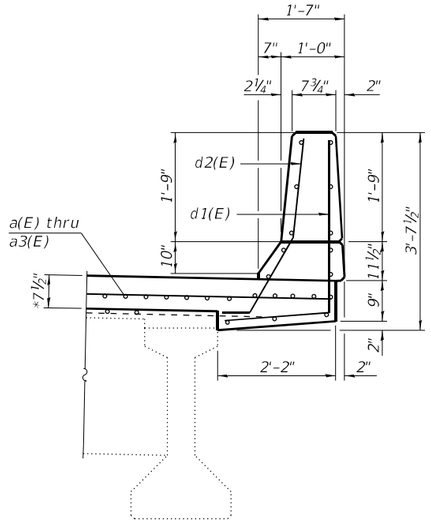
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		



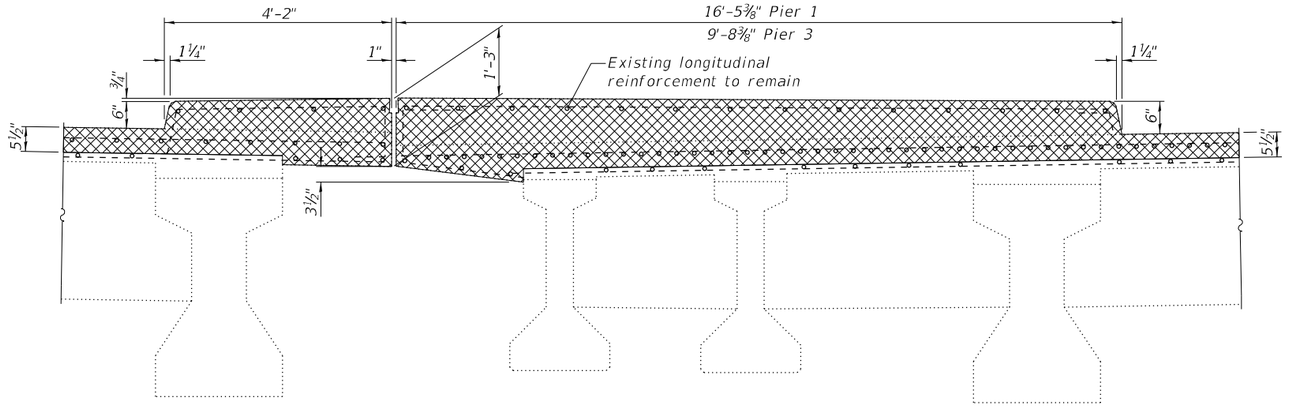
SECTION THRU PARAPET AT PIERS 1 & 3

(Showing Removal)



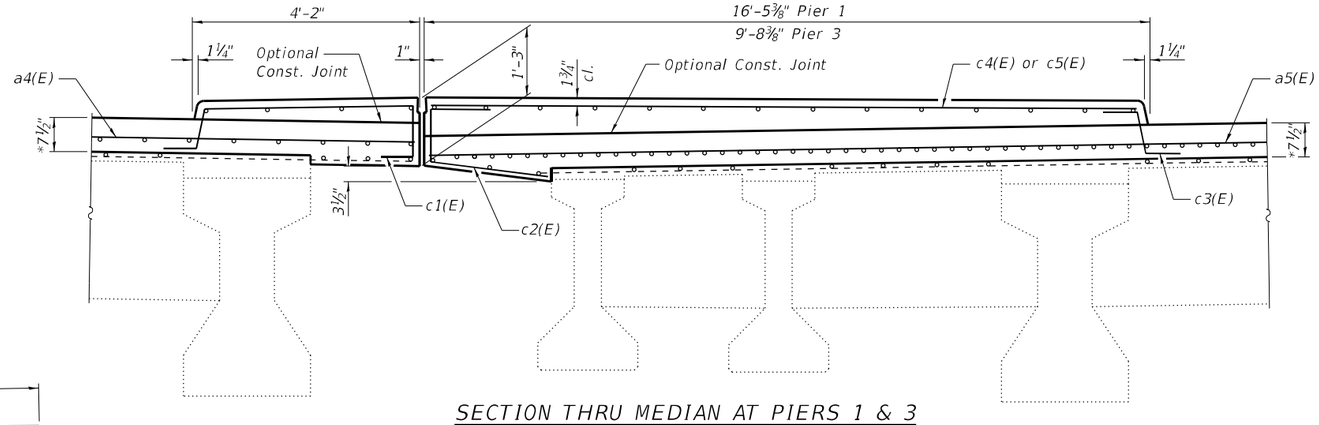
SECTION THRU PARAPET AT PIERS 1 & 3

(Showing Proposed)



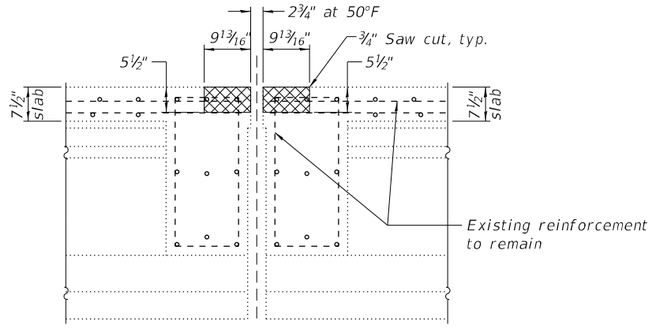
SECTION THRU MEDIAN AT PIERS 1 & 3

(Showing Removal)



SECTION THRU MEDIAN AT PIERS 1 & 3

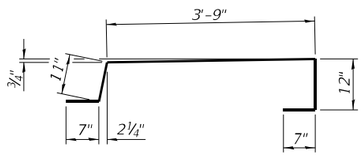
(Showing Proposed)



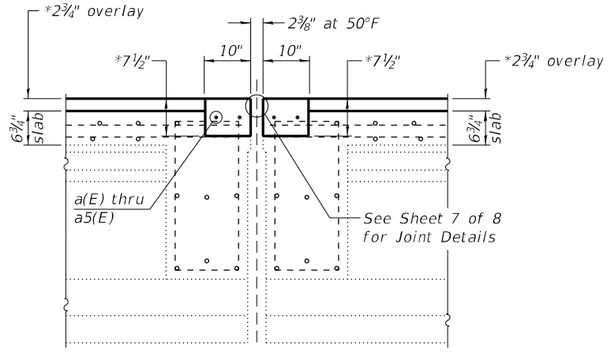
SECTION THRU JOINT AT PIERS 1 & 3

(Showing Removal; Dimensions at right angles)

*prior to grinding

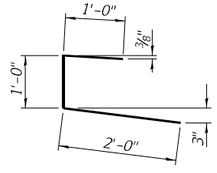


BAR c1(E)

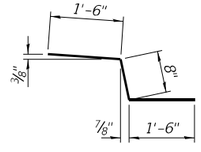


SECTION THRU JOINT AT PIERS 1 & 3

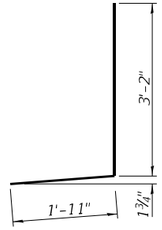
(Showing Proposed; Dimensions at right angles)



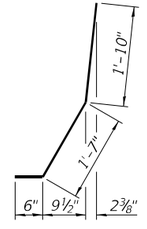
BAR c2(E)



BAR c3(E)



BAR d1(E)



BAR d2(E)

Notes:
Cross-hatched areas indicate limits of Concrete Removal.

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	8	#5	21'-8"	
a1(E)	4	#5	34'-11"	
a2(E)	4	#5	38'-2"	
a3(E)	8	#5	8'-9"	
a4(E)	8	#5	30'-2"	
a5(E)	8	#5	37'-9"	
c1(E)	8	#5	6'-10"	
c2(E)	8	#5	4'-0"	
c3(E)	8	#5	3'-8"	
c4(E)	4	#5	16'-0"	
c5(E)	4	#5	9'-3"	
d1(E)	16	#4	5'-1"	
d2(E)	16	#5	3'-11"	
Concrete Removal			Cu. Yd.	10.0
Concrete Superstructure			Cu. Yd.	12.5
Reinforcement Bars, Epoxy Coated			Pound	1,480

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JOINT REPLACEMENT DETAILS
STRUCTURE NO. 022-0047
SHEET 6 OF 8 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	27
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		

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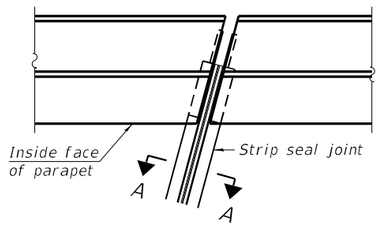
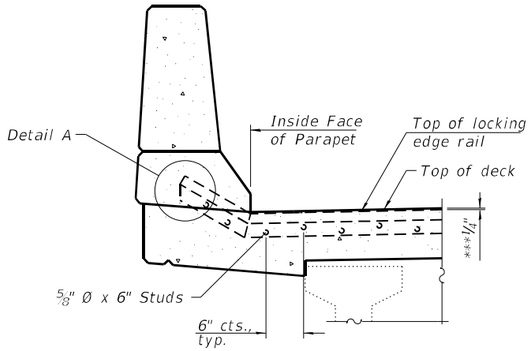
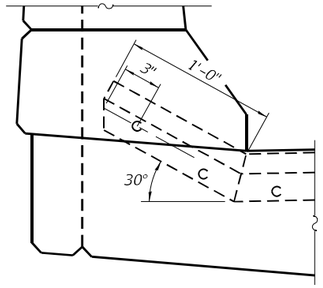


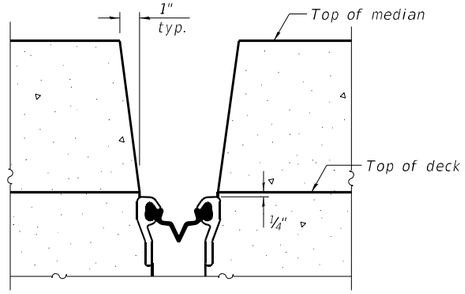
PLAN AT PARAPET



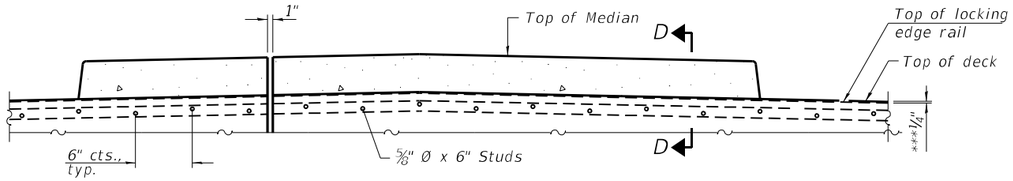
SECTION AT PARAPET



DETAIL A



SECTION D-D
(at Rt. ℄'s)



SECTION AT MEDIAN

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of ¼". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

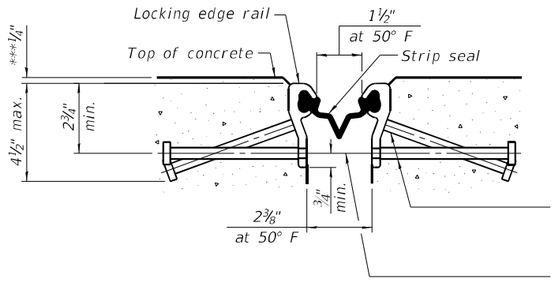
The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4½" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

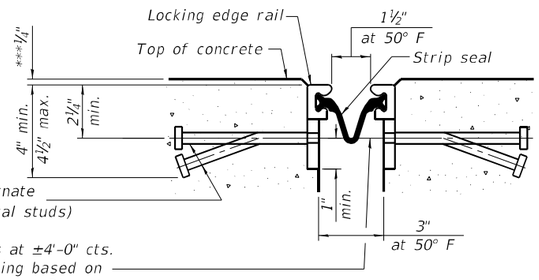
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required.



SHOWING ROLLED RAIL JOINT



SHOWING WELDED RAIL JOINT

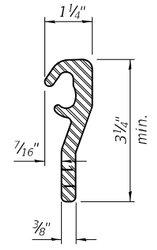
* ⅝" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

⅜" ϕ threaded rods in 7/16" ϕ holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

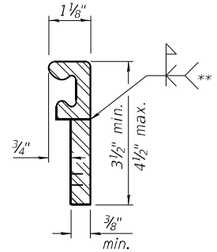
SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

***after grinding



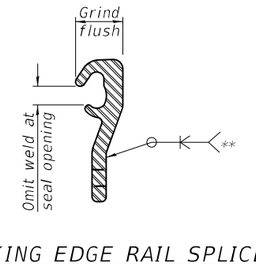
ROLLED
(EXTRUDED) RAIL



WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.

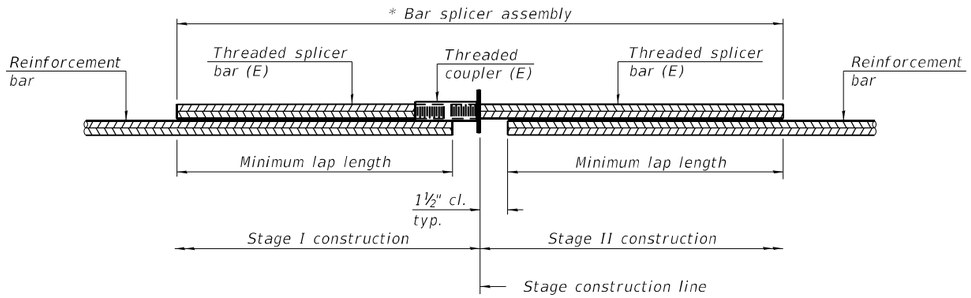


LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	275



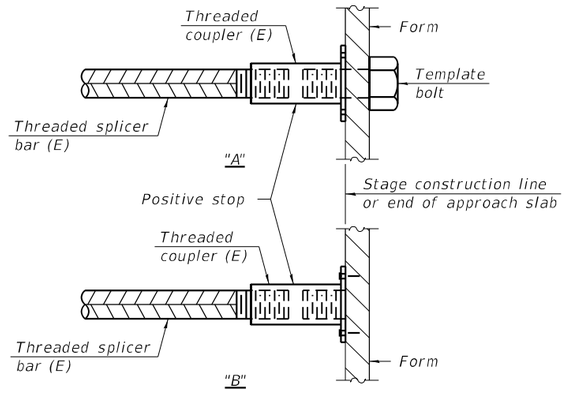
STANDARD BAR SPLICER ASSEMBLY PLAN

(All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1½" + thread length

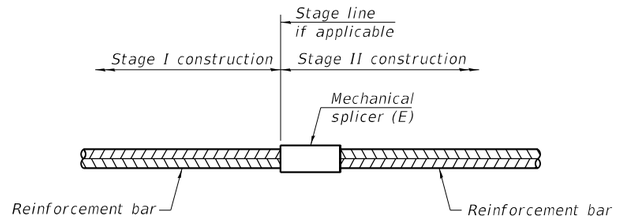
** Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.*

Location	Bar size	No. assemblies required	Minimum lap length
Joint	#5	24	3'-0"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 1-1-2020

LIN ENGINEERING,LTD.
Consulting Engineers
Springfield, Illinois

USER NAME =	DESIGNED - MTH	REVISED -	
	CHECKED - RJM	REVISED -	
PLOT SCALE =	DRAWN - AJF	REVISED -	
PLOT DATE = 1/23/2024	CHECKED - MTH	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 022-0047
SHEET 8 OF 8 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	29
		CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		

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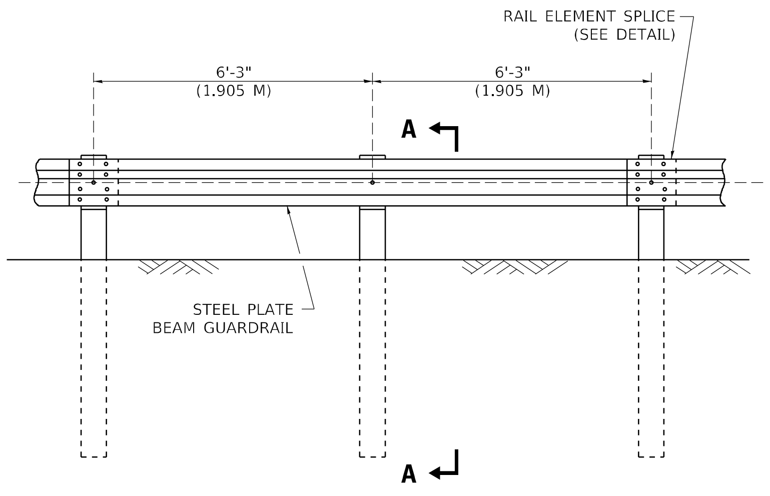
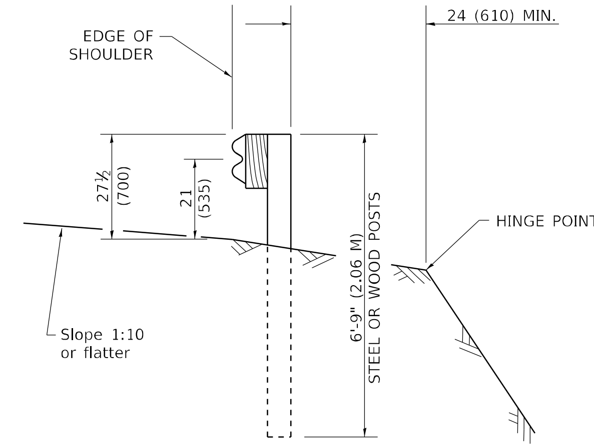


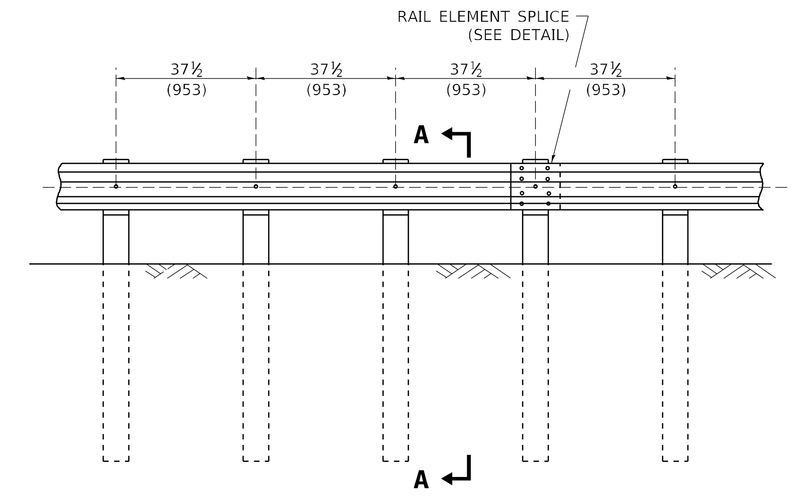
ELEVATION

TYPE A

6'-3" (1.905 M) TYPICAL POST SPACING



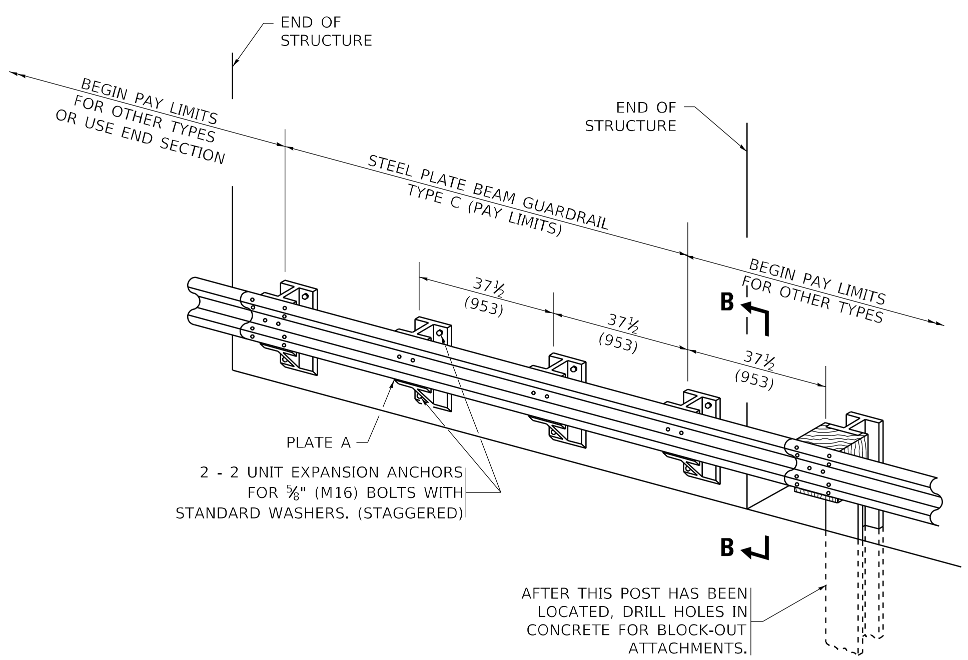
SECTION A–A



ELEVATION

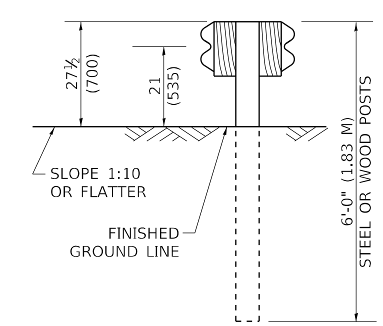
TYPE A

37½ (953) CLOSED POST SPACING

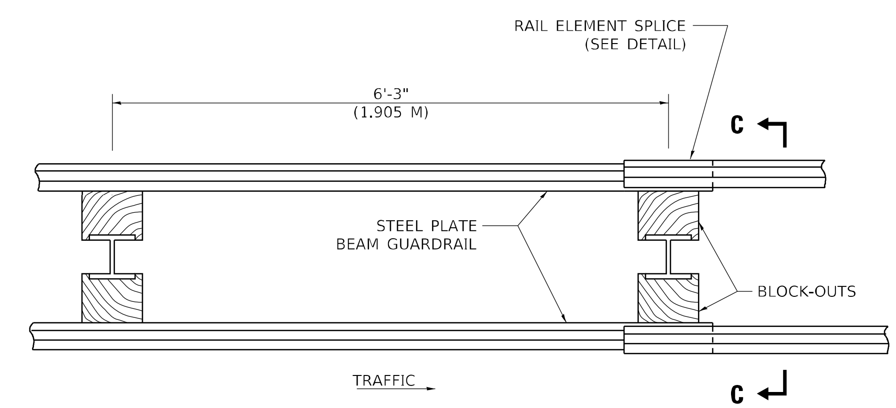


TYPE C

37½ (953) BLOCK-OUT SPACING



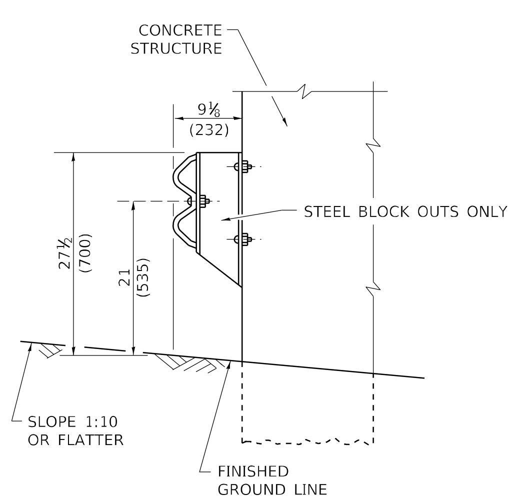
SECTION C–C



PLAN

TYPE D

DOUBLE STEEL PLATE BEAM GUARDRAIL
6'-3" (1.905 M) TYPICAL POST SPACING



SECTION B–B

GENERAL NOTES

ALL SLOPE RATIOS ARE EXPRESSED AS UNITS OF VERTICAL DISPLACEMENT TO UNITS OF HORIZONTAL DISPLACEMENT (V:H).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

THE EXISTING STEEL POSTS MAY BE DRILLED TO MATCH THE BOLT PATTERN SHOWN HEREIN FOR THE WOOD BLOCK-OUT, OR A NEW STEEL POST SHALL BE PROVIDED.

THIS DETAIL IS APPLICABLE TO THE GUARDRAIL SYSTEM USED PRIOR TO JANUARY 1, 2007. FOR DETAILS ON THE MIDWEST GUARDRAIL SYSTEM, SEE STANDARD 630001.

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	DRAWN -	REVISED -
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/11/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL

SCALE: NONE | SHEET 1 OF 4 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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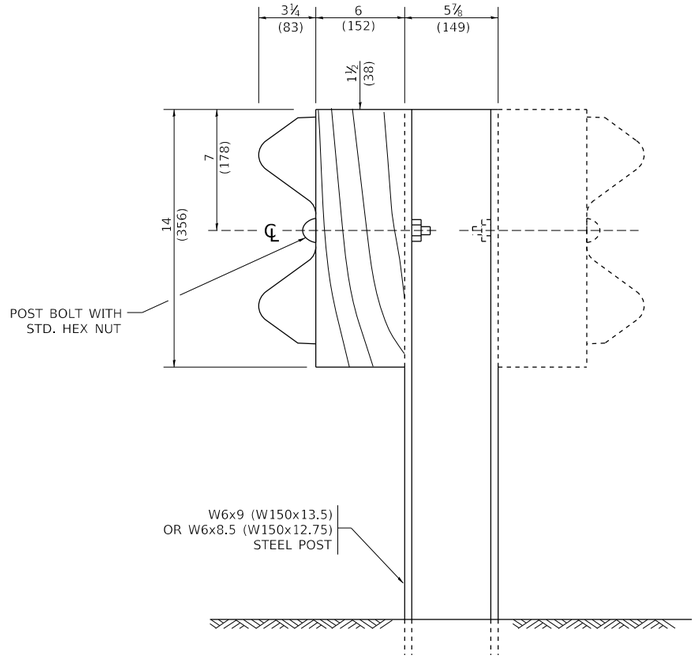
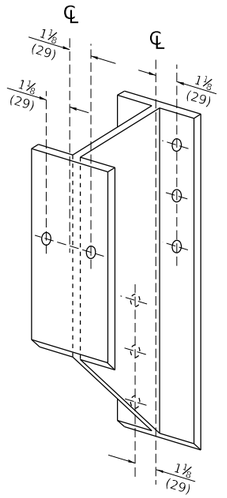
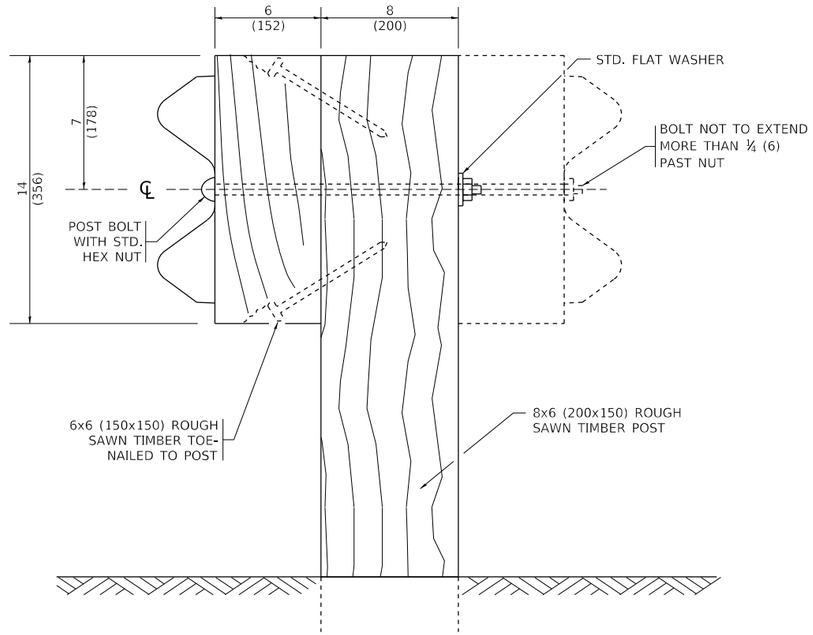
	BM-21	CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	



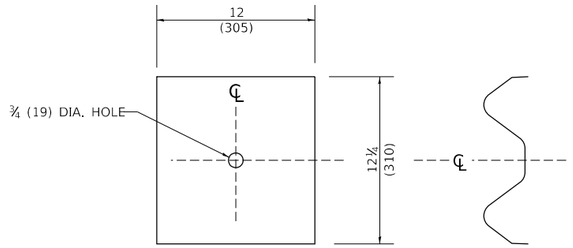
STEEL POST CONSTRUCTION



STEEL BLOCK–OUT DETAIL



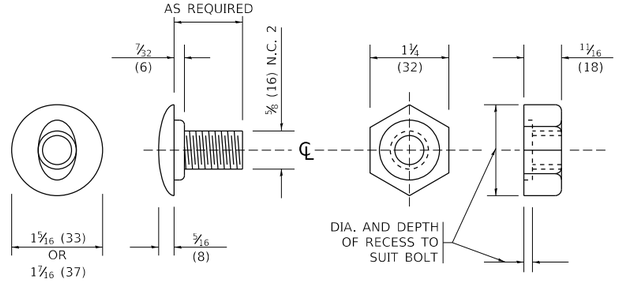
WOOD POST CONSTRUCTION



NOTE:

PLATE A SHALL BE PLACED BETWEEN RAIL ELEMENT AND BLOCK-OUT AT NON-SPLICE MOUNTING POINTS ONLY WHEN STEEL BLOCK-OUTS ARE USED.

PLATE A



POST OR SPLICE BOLT & NUT

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	DRAWN -	REVISED -	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/11/2019	DATE -	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REMOVE AND REERECT
STEEL PLATE BEAM GUARDRAIL

SCALE: NONE | SHEET 2 OF 4 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	31
	BM-21	CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	

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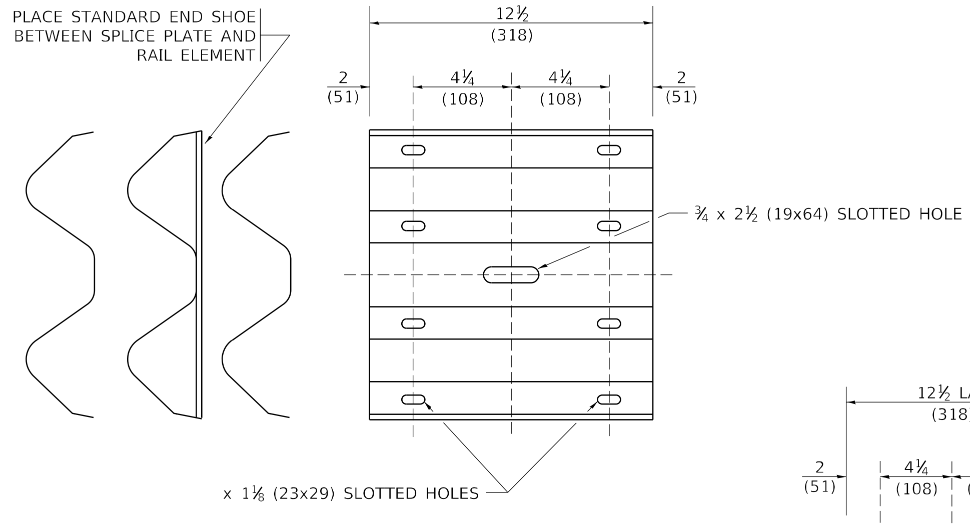
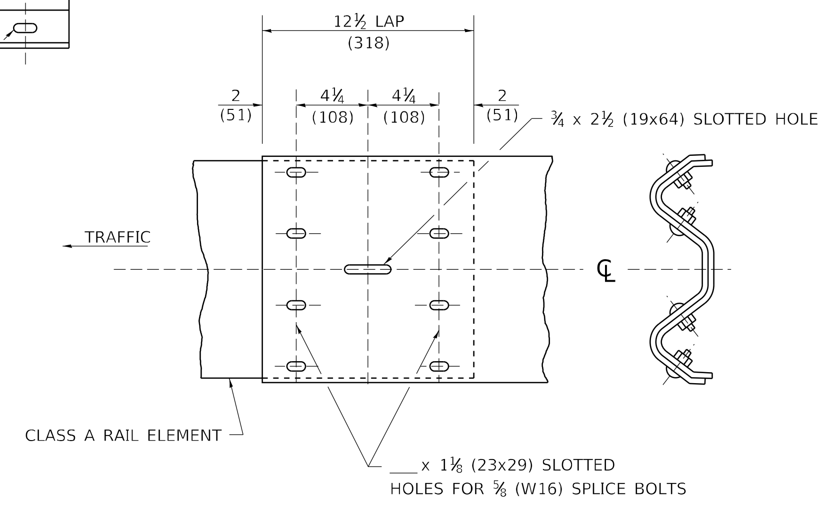
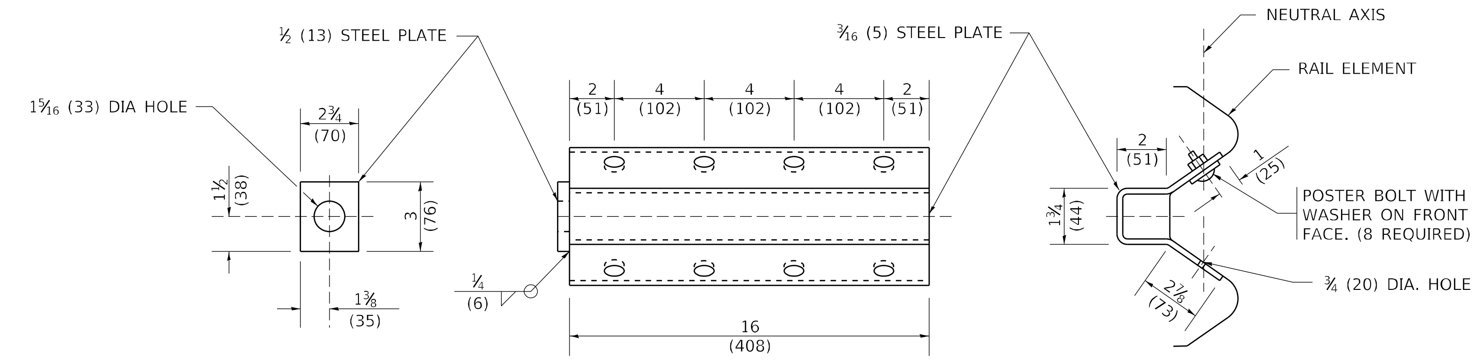


SPLICE PLATE



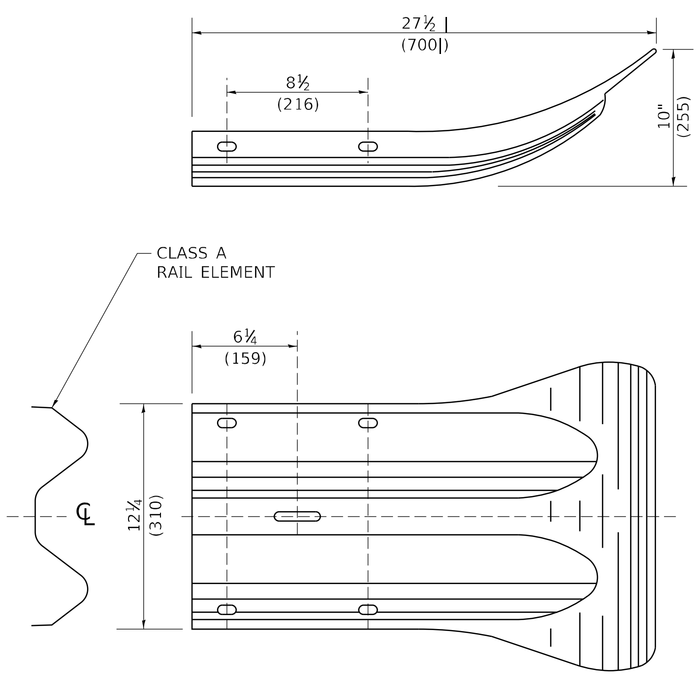
RAIL ELEMENT SPLICE



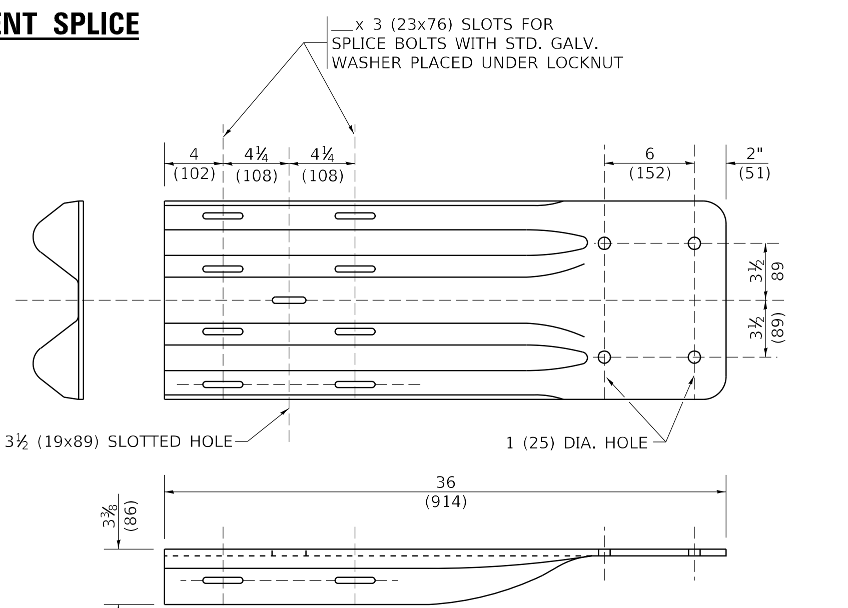
NOTE:

ANCHOR PLATE T SHALL BE USED TO ATTACH CABLE ASSEMBLY TO GUARDRAIL WHEN REQUIRED ON TRAFFIC BARRIER TERMINALS.

ANCHORE PLATE T DETAILS



END SECTION



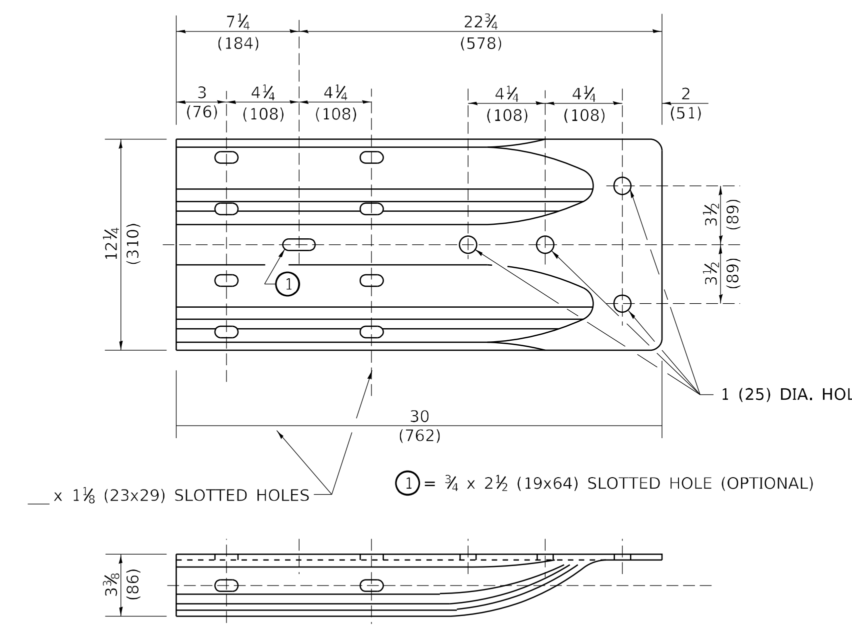
END SHOE

NOTE:

WHEN END SHOE IS ATTACHED TO A BRIDGE PARAPET WHICH HAS AN EXPANSION JOINT, THE BOLTS SHALL BE PROVIDED WITH A LOCKNUT OR DOUBLE NUT AND SHALL BE TIGHTENED ONLY TO A POINT THAT WILL ALLOW GUARDRAIL MOVEMENT.

THE STANDARD END SHOE SHALL BE ATTACHED TO THE CONCRETE WITH PRE-DRILLED OR SELF-DRILLING ANCHOR BOLTS. THE ANCHOR CONE SHALL BE SET FLUSH WITH THE SURFACE OF THE CONCRETE.

EXTERNALLY THREADED STUDS PROTRUDING FROM THE SURFACE OF THE CONCRETE WILL NOT BE PERMITTED.



ALTERNATE END SHOE

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	DRAWN -	REVISED -	
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PLOT DATE = 3/11/2019	DATE -	REVISED -	

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL

SCALE: NONE | SHEET 3 OF 4 SHEETS | STA. | TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	32
	BM-21	CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	

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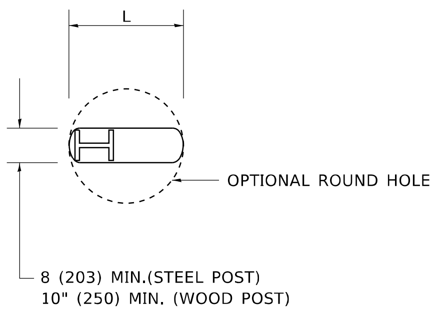
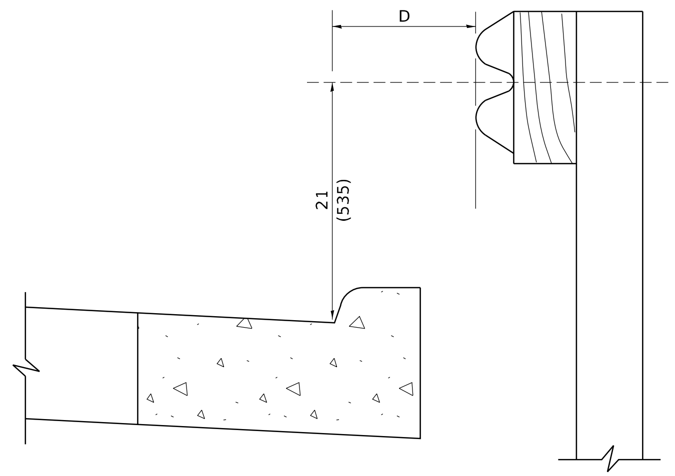


PLAN

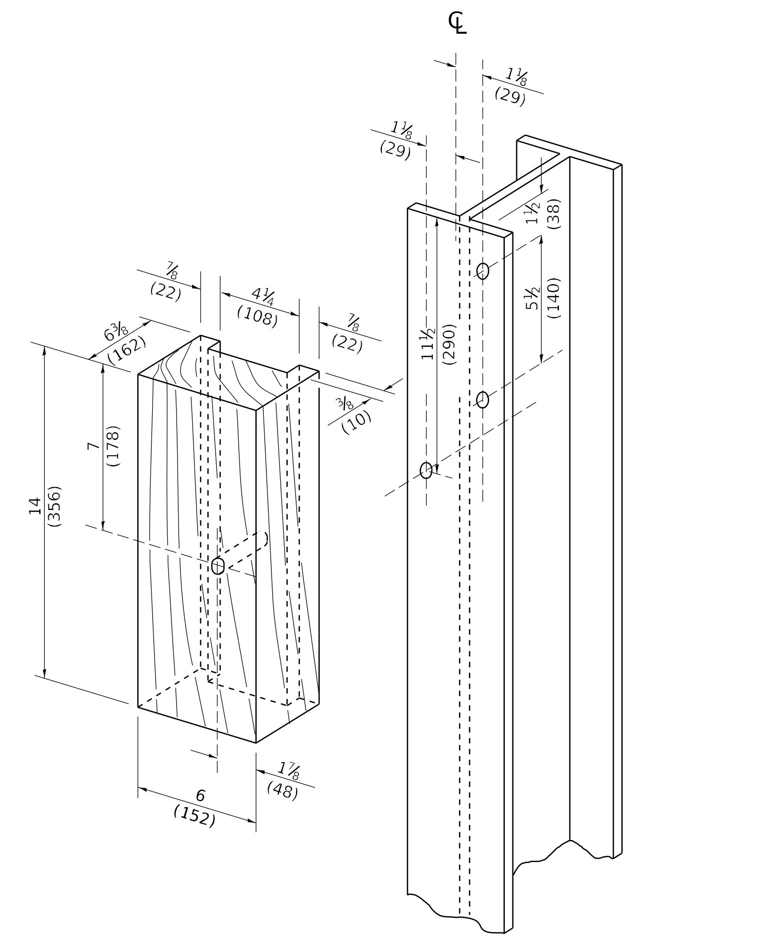


NOTE:

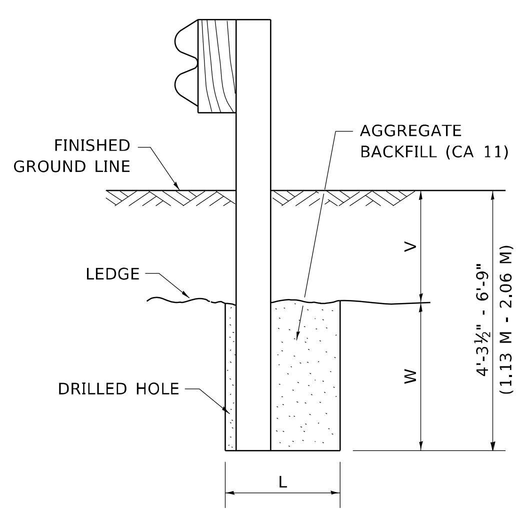
IF IT IS NECESSARY FOR D TO BE MORE THAN 12 (300) AND LESS THAN 10'-0" (3.0 M) TYPE M-2 (M-5) CURB AND GUTTER (STD. 606001) SHALL BE USED IN FRONT OF AND IN ADVANCE OF THE GUARDRAIL.

GUARDRAIL PLACED BEHIND CURB

(D = O DESIRABLE TO 12 (300) MAXIMUM)



WOOD BLOCK – OUT AND STEEL POST DETAILS



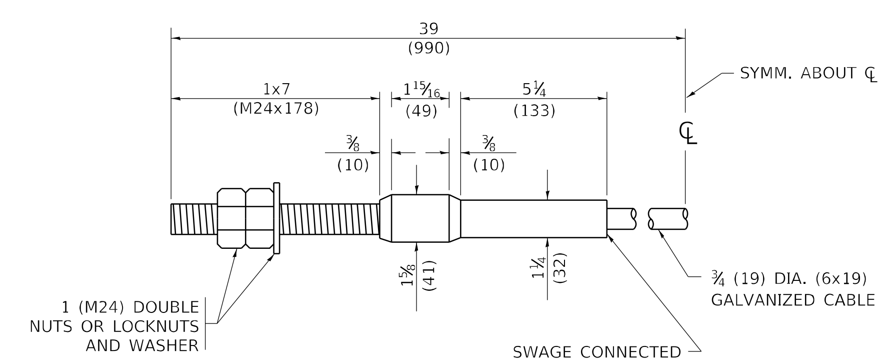
NOTE:

LEDGE LINE IS TOP OF ROCK LEDGE OR HARD SLAG FILL.

ELEVATION

FOOTING FOR POST WHEN IMPERVIOUS MATERIAL IS ENCOUNTERED

V	W	L	
		STEEL POST	WOOD POST
0 - 18 (0 - 460)	24 (610)	21 (530)	23 (580)
>18 - 41.5 (> 460 - 825)	12 (305)	8 (203)	10 (250)
>41.5 - 53.5 (> 825 - 1.13 M)	12 - 0 (350 - 0)	8 (203)	10 (250)



CABLE ASSEMBLY

(40,000 LBS (18,100 KG) MIN. BREAKING STRENGTH)
TIGHTEN TO TAUT TENSION

MODEL: Default

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	DRAWN -	REVISED -	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 3/11/2019	DATE -	REVISED -	

REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL

SCALE: NONE | SHEET 4 OF 4 SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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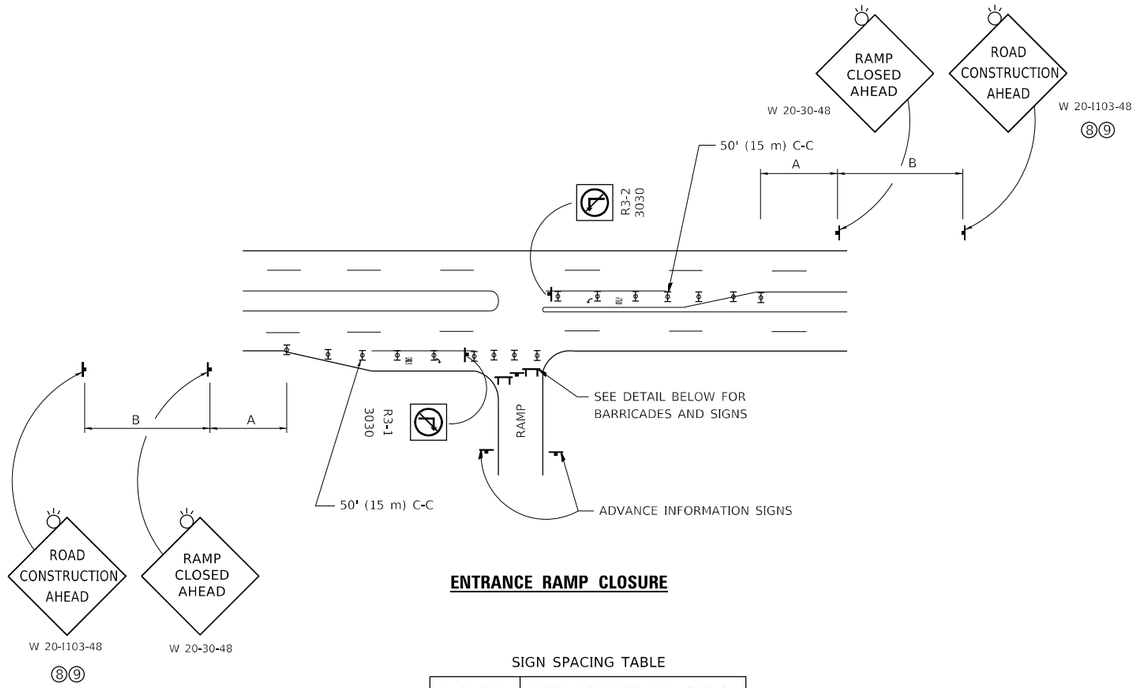
BM-21		CONTRACT NO. 62M37		
		ILLINOIS FED. AID PROJECT		

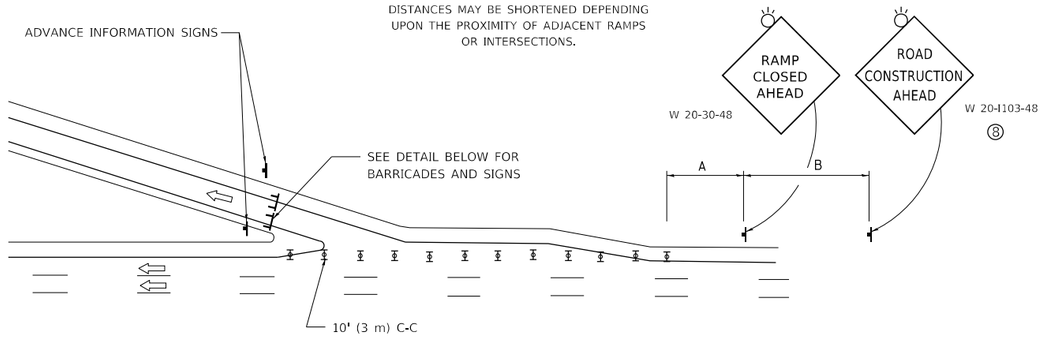


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.

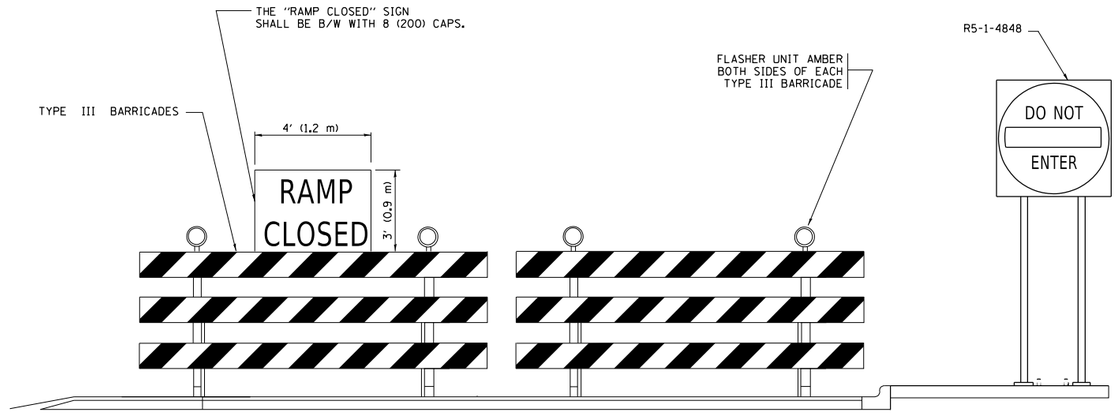


EXIT RAMP CLOSURE

SYMBOLS

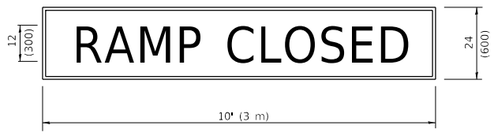
TYPE II BARRICADE OR DRUM

TYPE III BARRICADE WITH 2 FLASHING LIGHTS



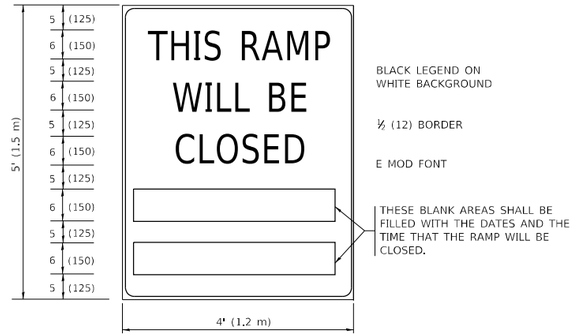
DETAIL FOR REQUIRED BARRICADES & SIGNS

RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

½ (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

1. CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
2. VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
3. A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEEDED BY A W20-7 FLAGGER WARNING SIGN.
4. ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
5. THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
7. THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH
8. ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
9. ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footem}	DESIGNED - D.W.S.	REVISED - S.P.B._01-07	**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**	**ENTRANCE_AND_EXIT_RAMP CLOSURE_DETAILS**	
	DRAWN -	REVISED - S.P.B._12-09			
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PLOT DATE = 3/4/2019	DATE - 02-83	REVISED - M.D._01-18		SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.	

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	34
	TC-08	CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			

MODEL: Default
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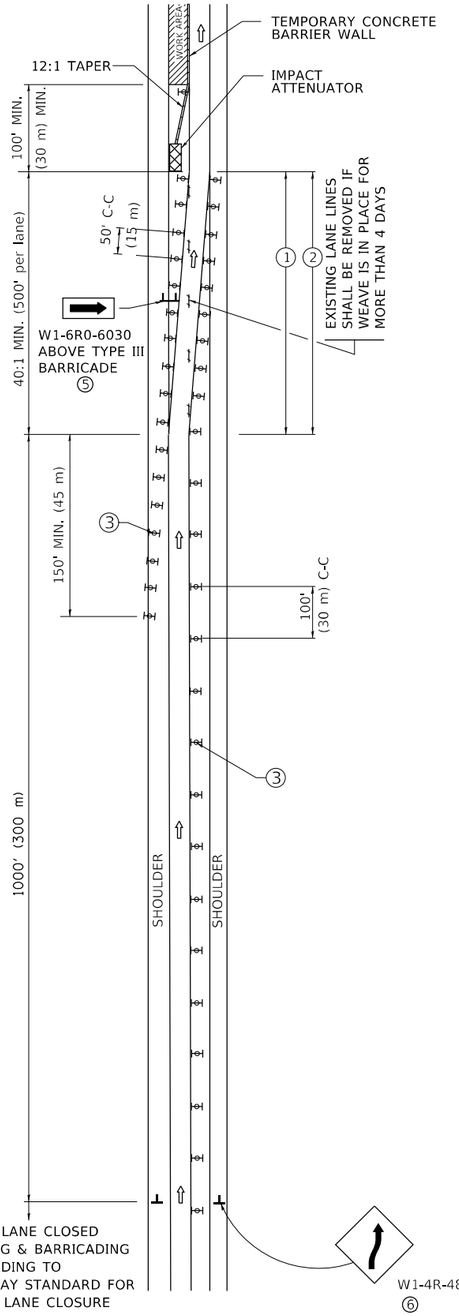
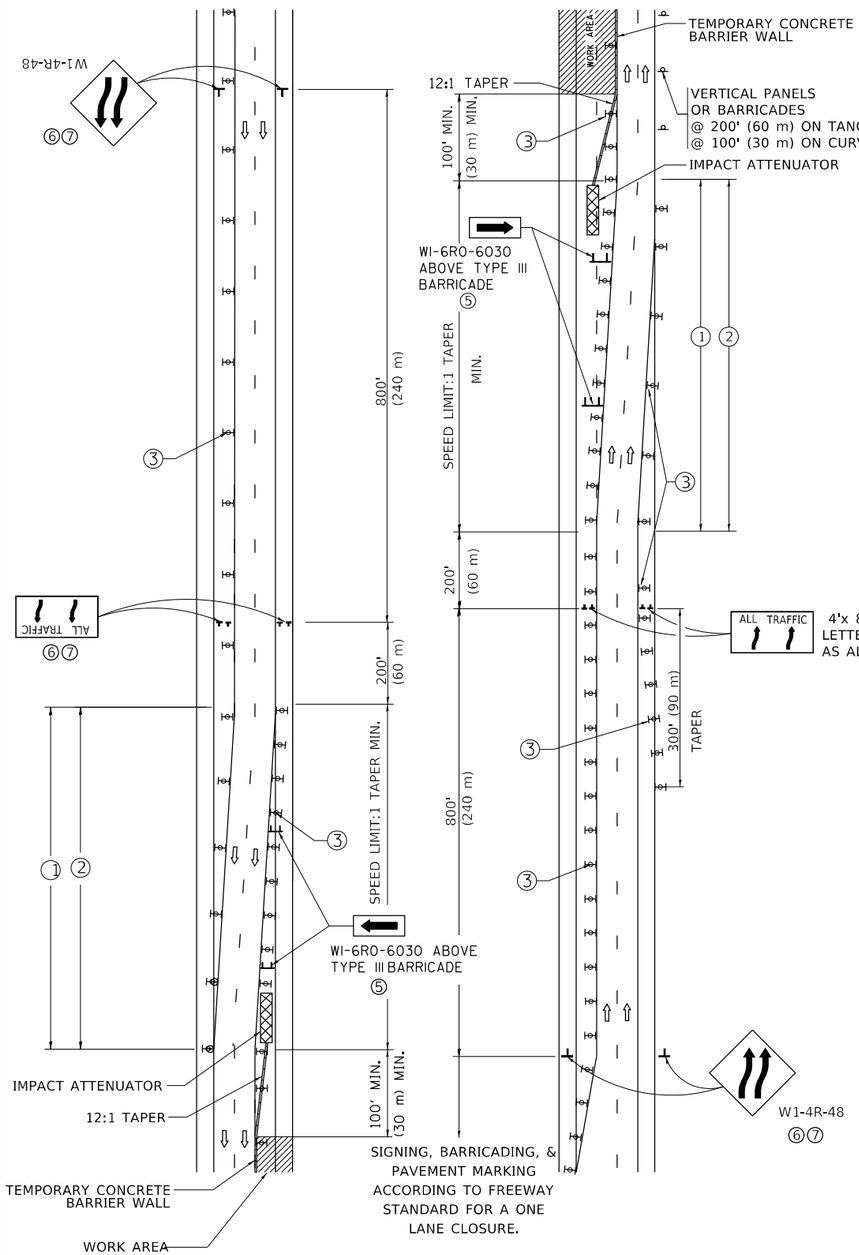
SINGLE LANE WEAVE



MULTI–LANE WEAVE



GENERAL NOTES:

① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.

② CONTINOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.

③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.

④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.

⑤ TYPE III BARRICADES MAY BE OMMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.

⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.

⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W24-1-48 ⑦

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME = footemj	DESIGNED - D.W.S.	REVISED - J.A.F. 02-06	
	DRAWN -	REVISED - S.P.B. 01-07	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - S.P.B. 12-09	
PLOT DATE = 3/4/2019	DATE - 02-87	REVISED - M.D. 06-13	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI–LANE WEAVE

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	35
	TC-09	CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			

MODEL: Default
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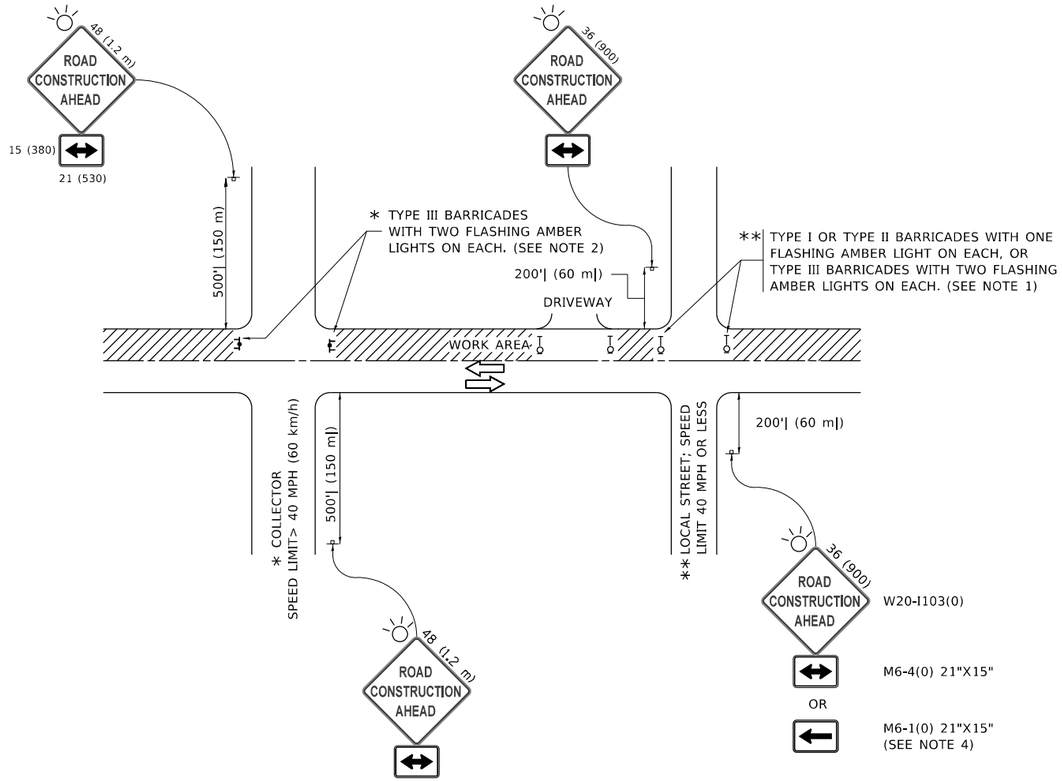

NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footem}	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	36
	TC-10	CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			

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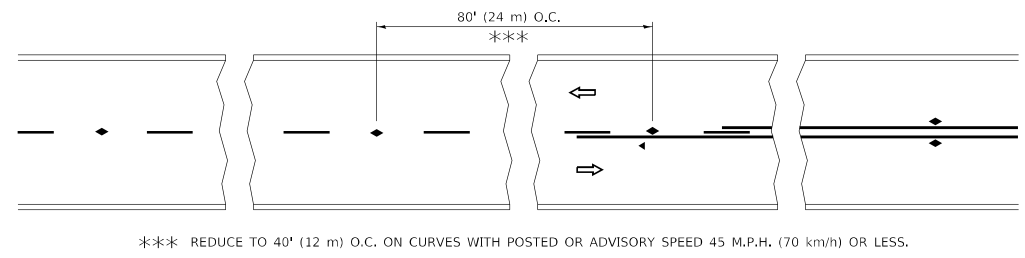
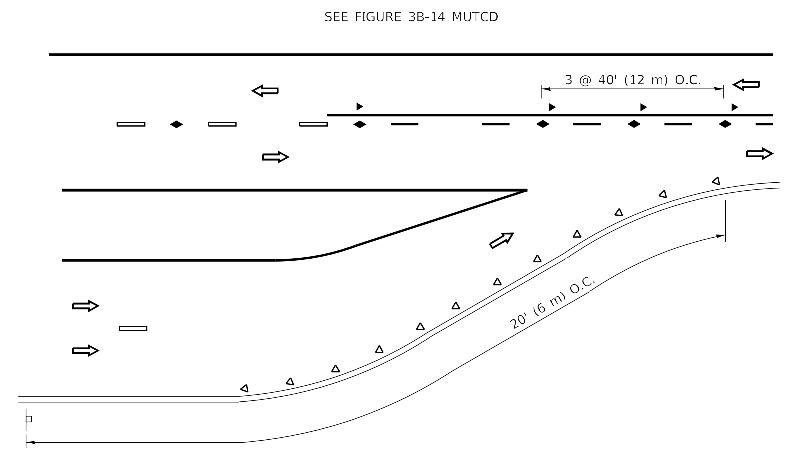
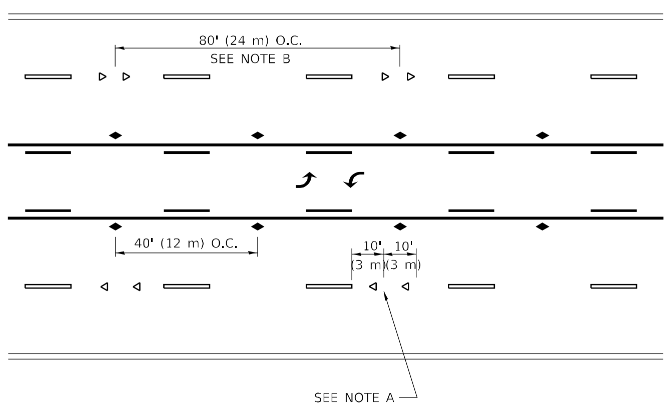


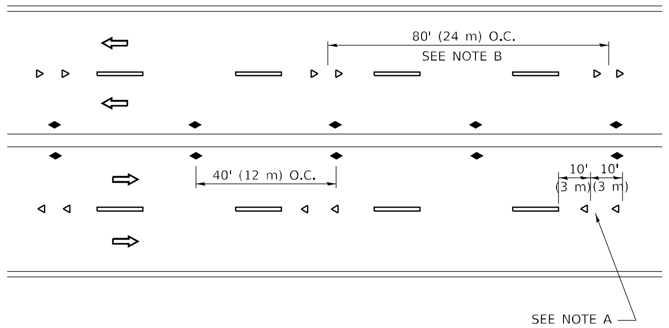
TWO–LANE/TWO–WAY



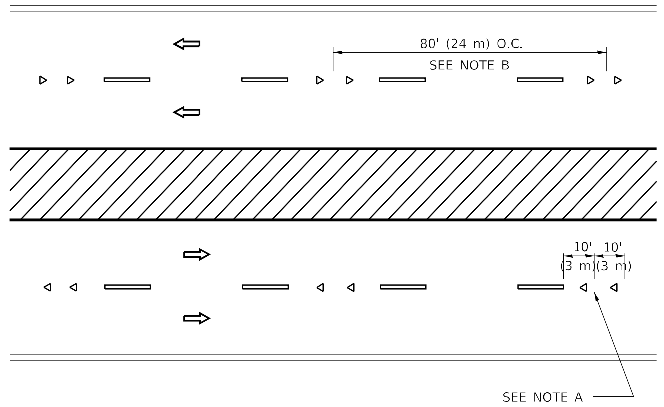
LANE REDUCTION TRANSITION



TWO–WAY LEFT TURN



MULTI–LANE/UNDIVIDED



MULTI–LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

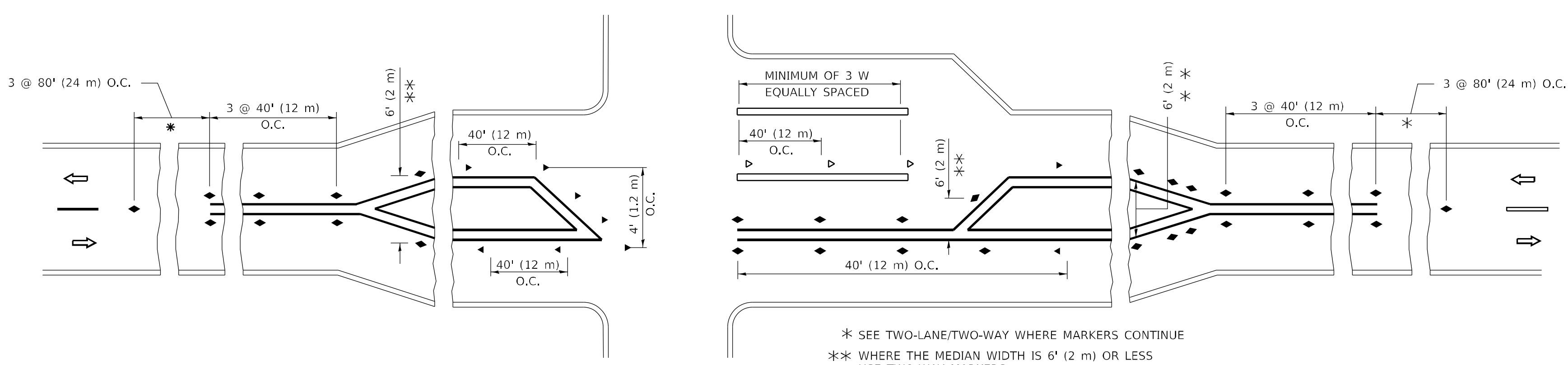
LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

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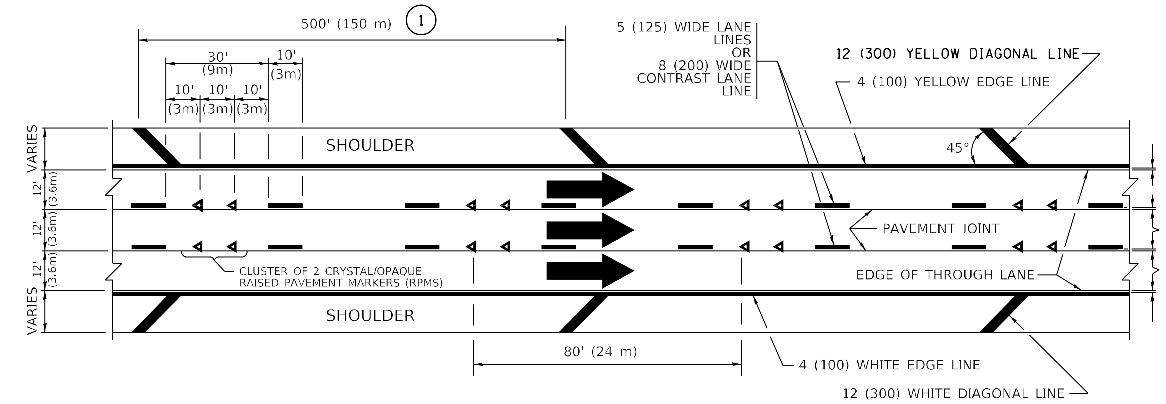
	DRAWN -	REVISED - T. RAMMACHER 01-06-00	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09	
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13	

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW–PLOW RESISTANT)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	37
	TC–11	CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		

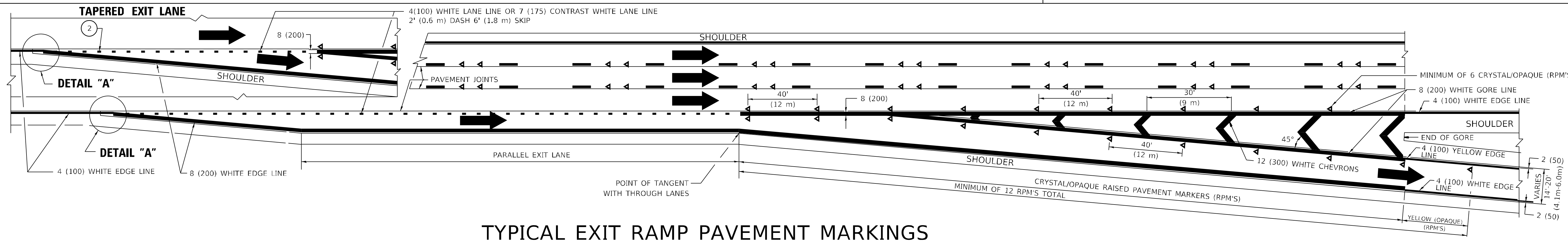
tc11.dgn 3/4/2019 10:29:08 AM User=footemj



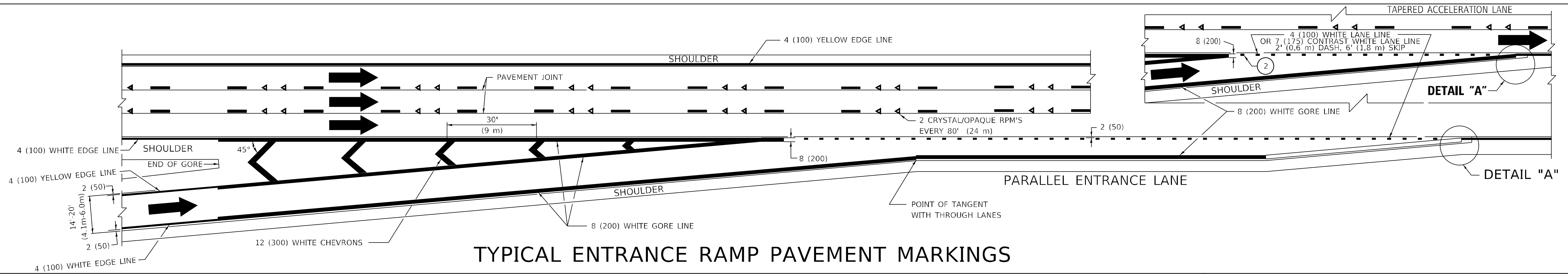
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

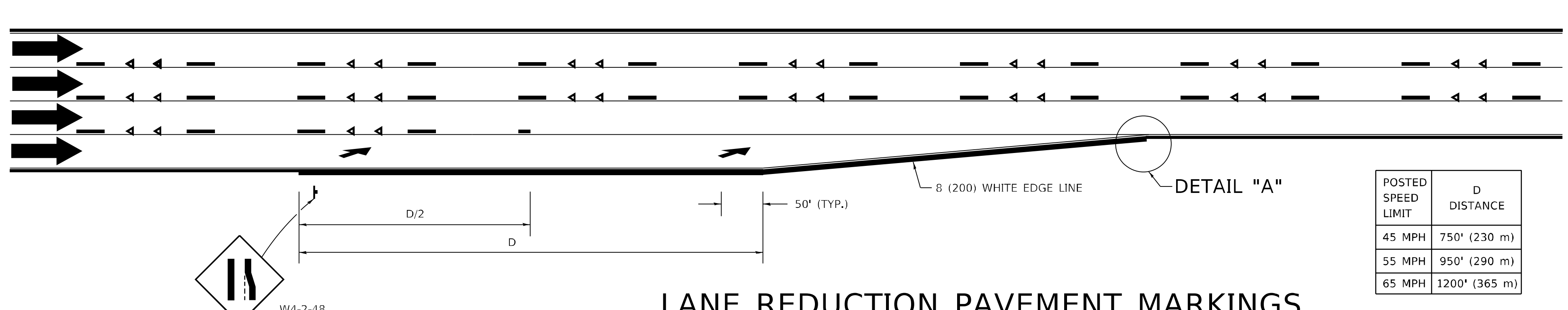
1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE D, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

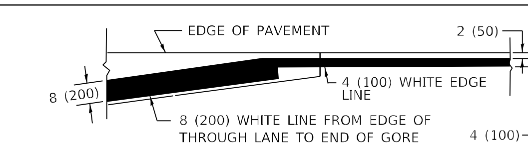


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)

LANE REDUCTION PAVEMENT MARKINGS



DETAIL "A"

NOTES:

1. THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
2. 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

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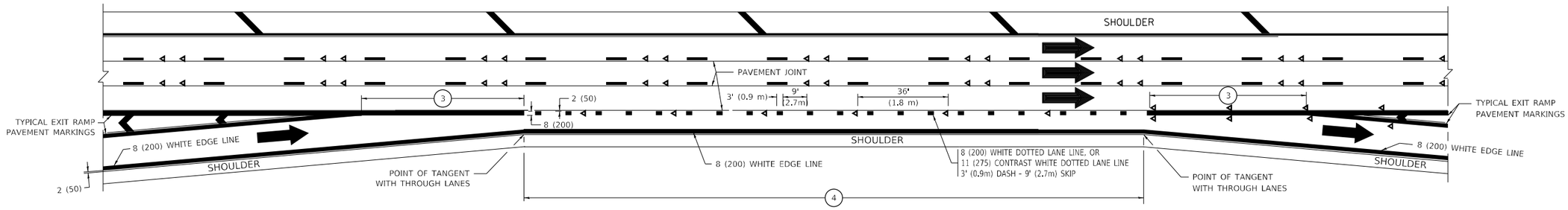
USER NAME = Lawrence,DeManche	DESIGNED - D.W.S.	REVISED - S.P.B. 01-10
	DRAWN -	REVISED - M.D. 05-13
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - M.D. 09-17
PLOT DATE = 11/18/2022	DATE - 01-90	REVISED - K. SMITH 11-18-22

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

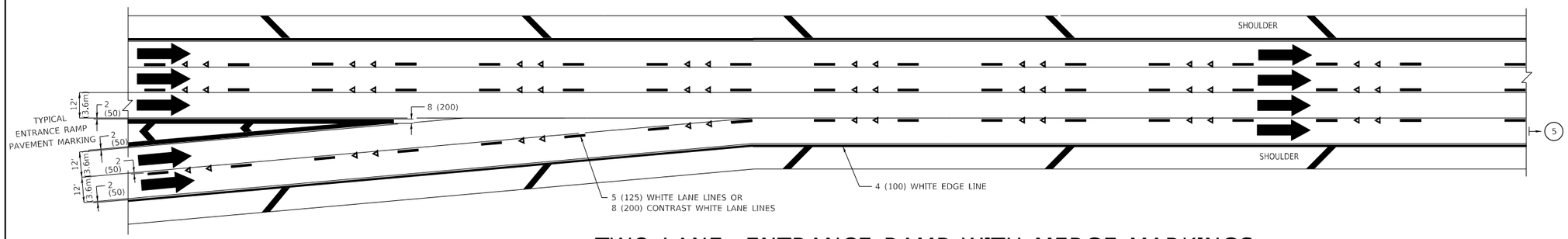
MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS

SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. TO STA.

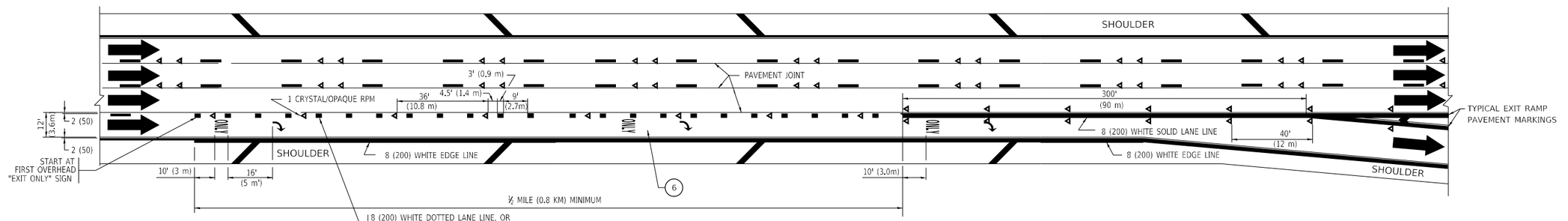
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	38
	TC-12	CONTRACT NO. 62M37		
	ILLINOIS	FED. AID PROJECT		



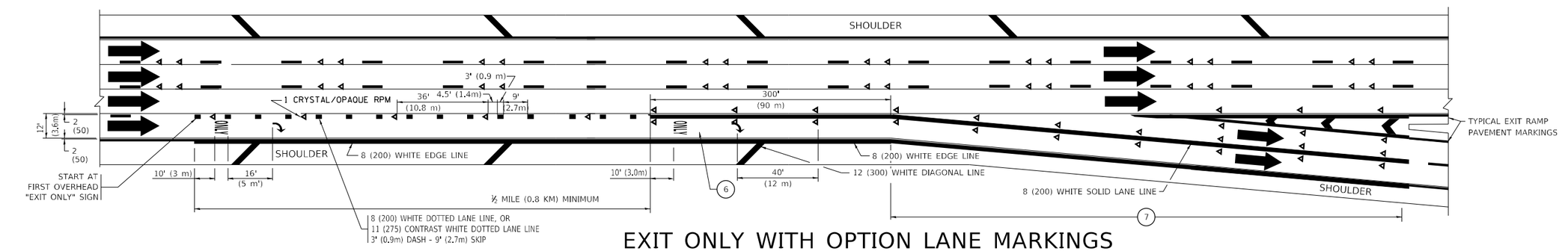
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

NOTES:

3. OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
4. 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
5. FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
6. ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED..
7. CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

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	DRAWN -	REVISED - S.P.B. 01-07	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - S.P.B. 01-10	
PLOT DATE = 3/4/2019	DATE - 01-90	REVISED - M.D. 09-17	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MULTI–LANE FREEWAY
PAVEMENT MARKING DETAILS

SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	39
	TC–12	CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			

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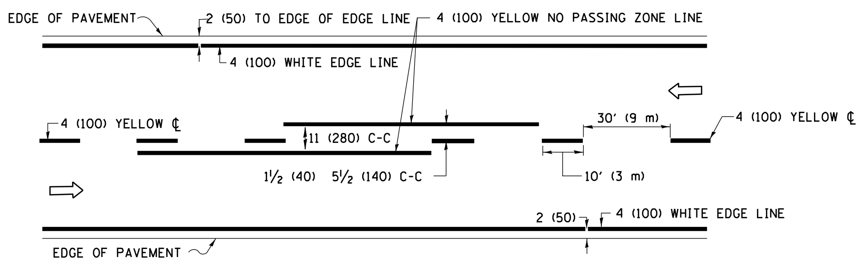
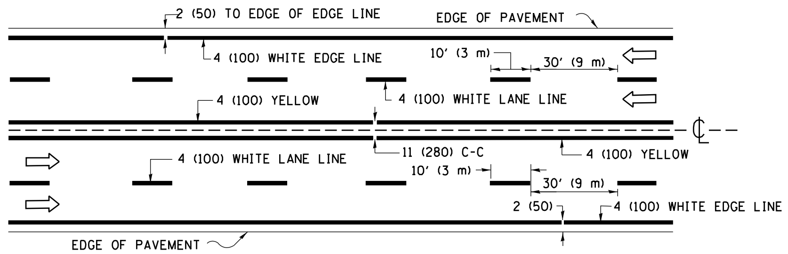
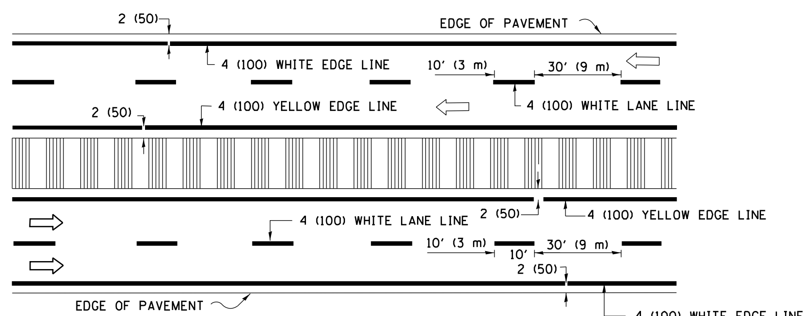


2–LANE ROADWAY



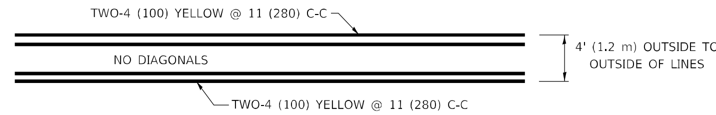
MULTI–LANE UNDIVIDED



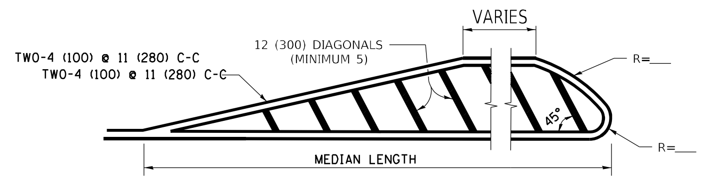
MULTI–LANE DIVIDED

WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING



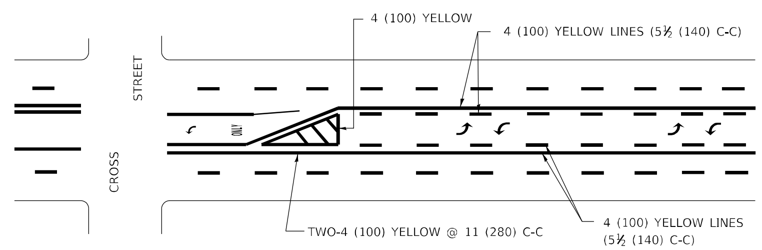
4' (1.2 m) WIDE MEDIANS ONLY



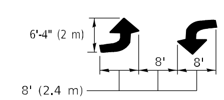
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

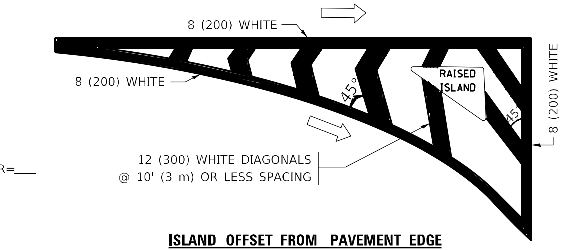


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

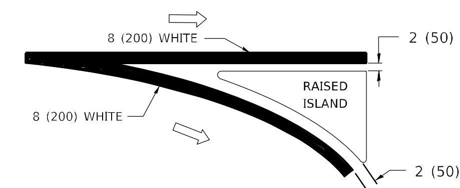


MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

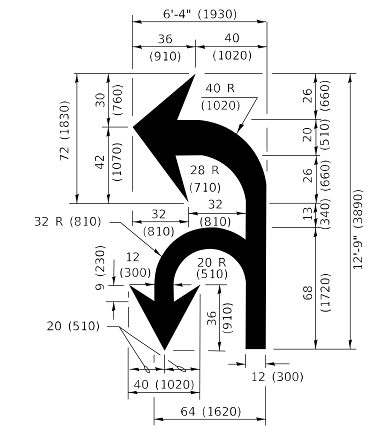


ISLAND OFFSET FROM PAVEMENT EDGE

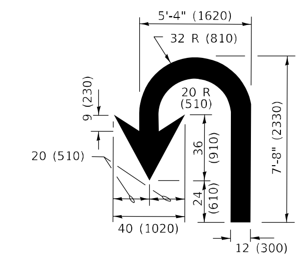


ISLAND AT PAVEMENT EDGE

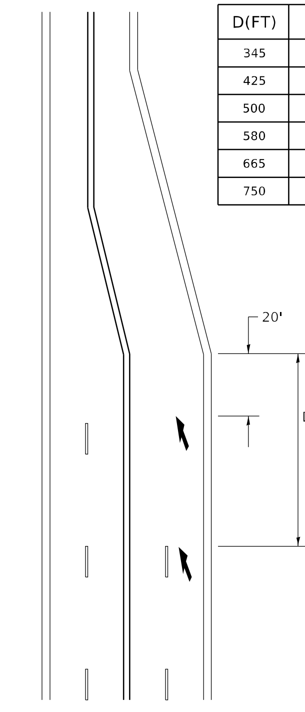
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

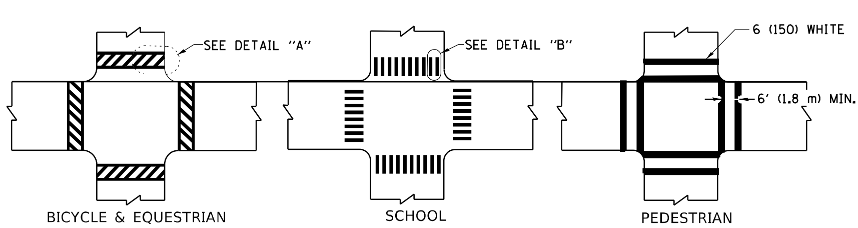
LANE REDUCTION TRANSITION

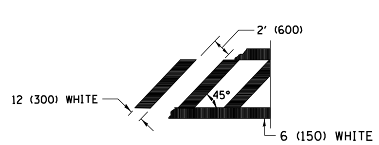
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

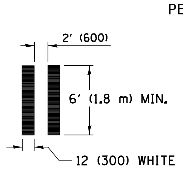
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.





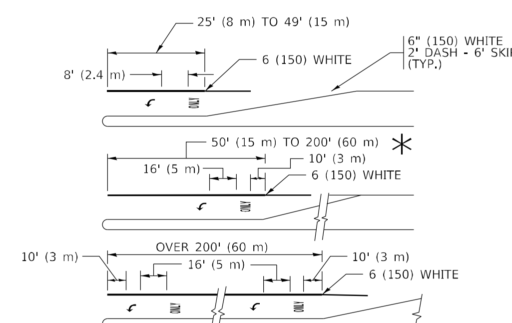
DETAIL "A"



DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
	DRAWN -	REVISED - C. JUCIUS 07-01-13	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15	
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16	

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	40
TC–13		CONTRACT NO. 62M37		
		ILLINOIS FED. AID PROJECT		

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tc13.dgn 3/4/2019 10:34:37 AM User=footemj

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

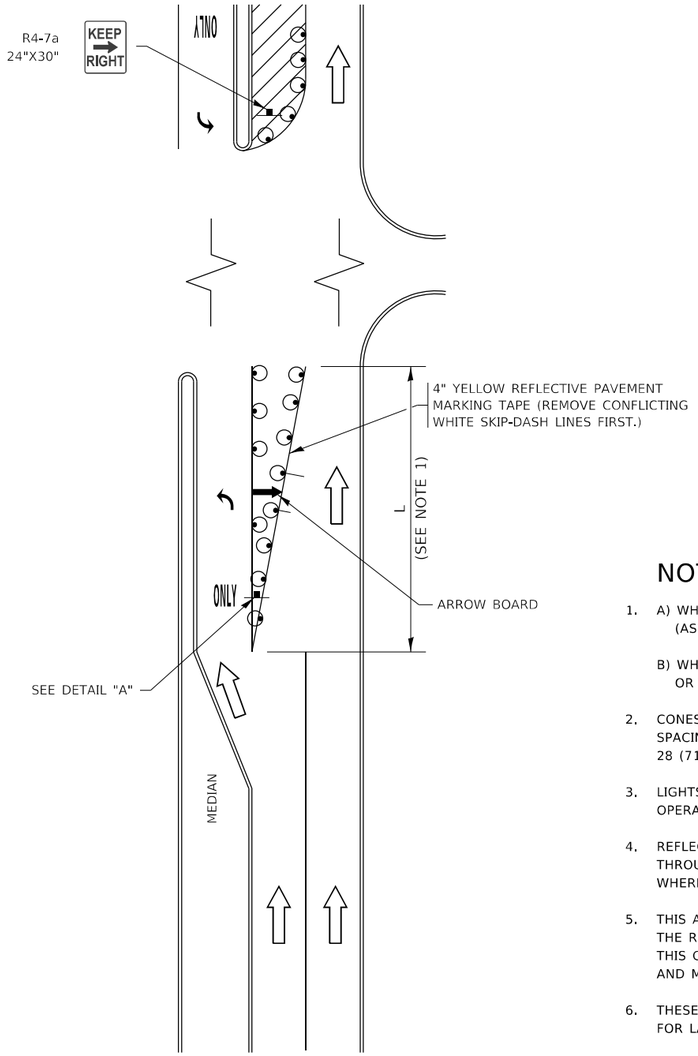


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

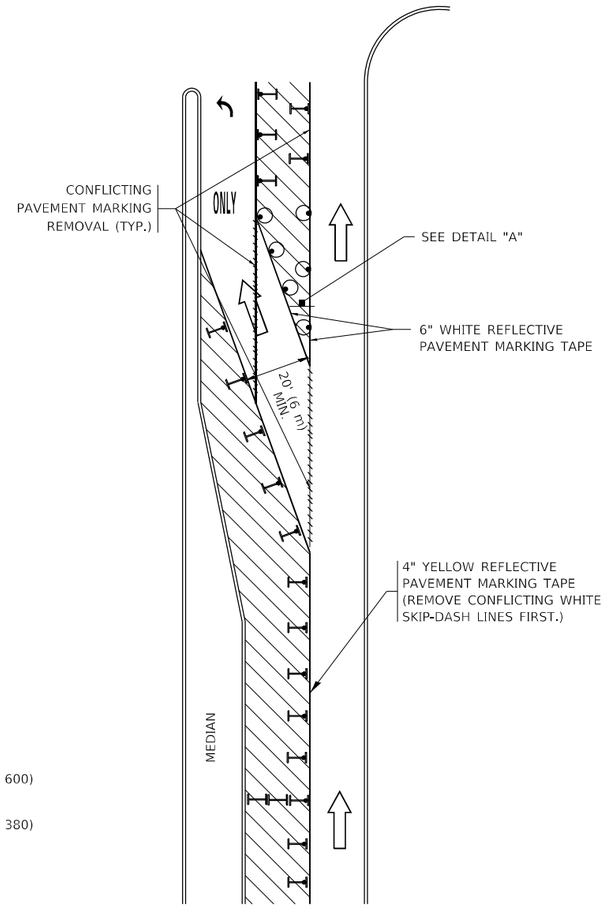
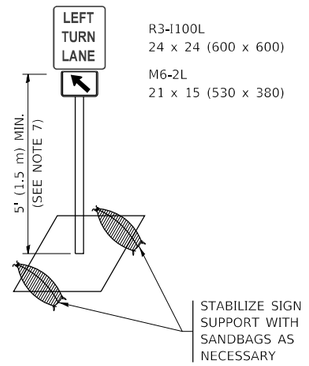


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footem]	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	
PLOT SCALE = 50.0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16	
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	41
	TC-14	CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			

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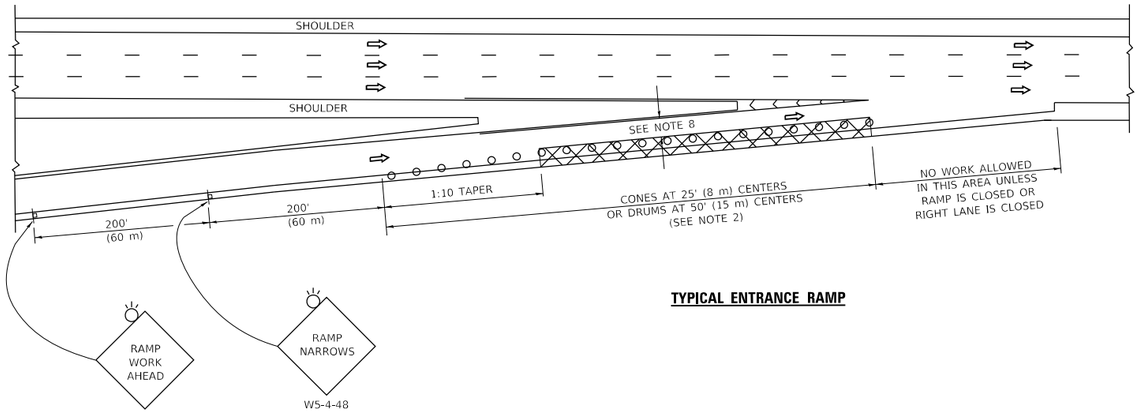
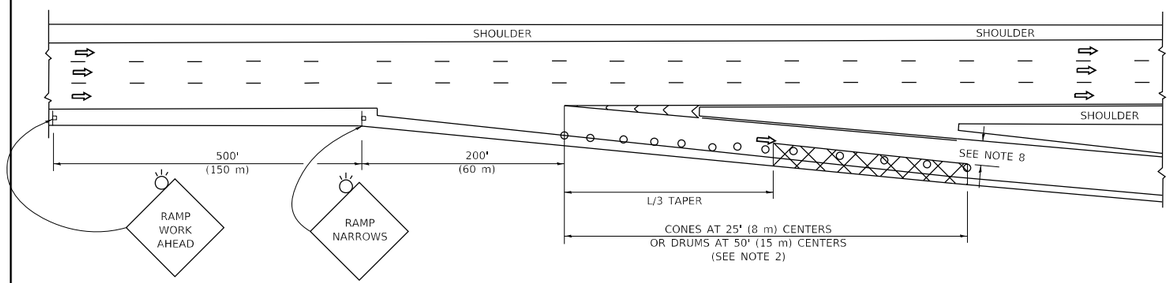
tc14.dgn 3/4/2019 10:36:01 AM User=footemi

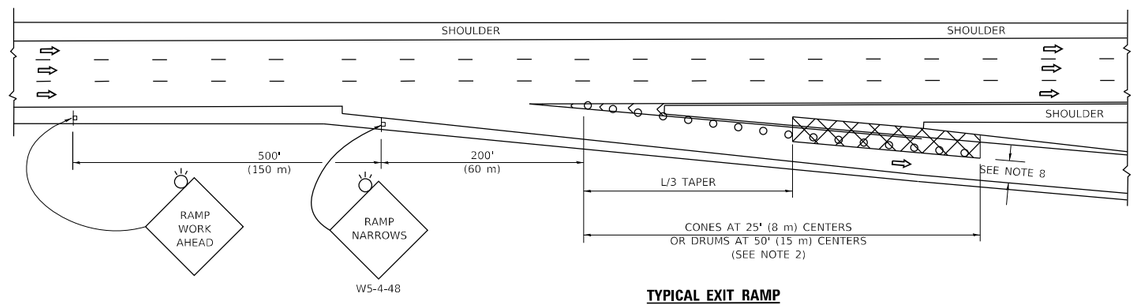
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP

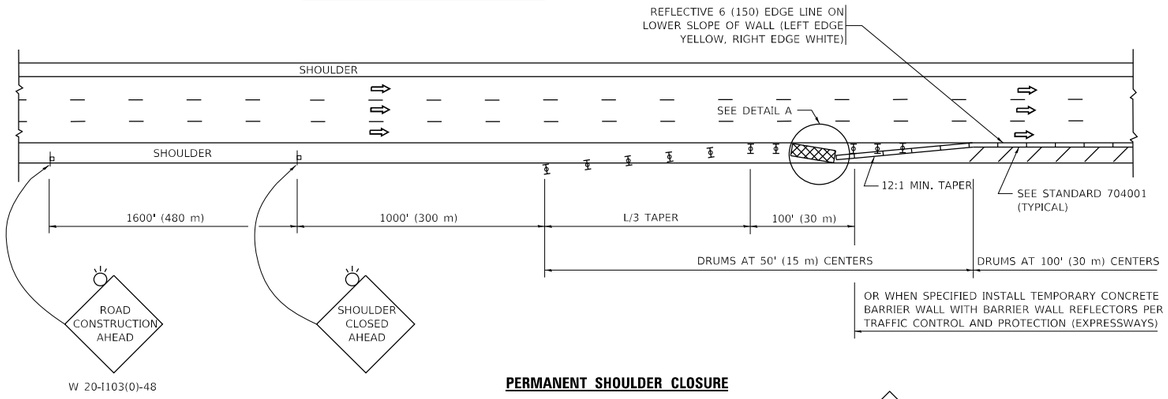


TYPICAL EXIT RAMP

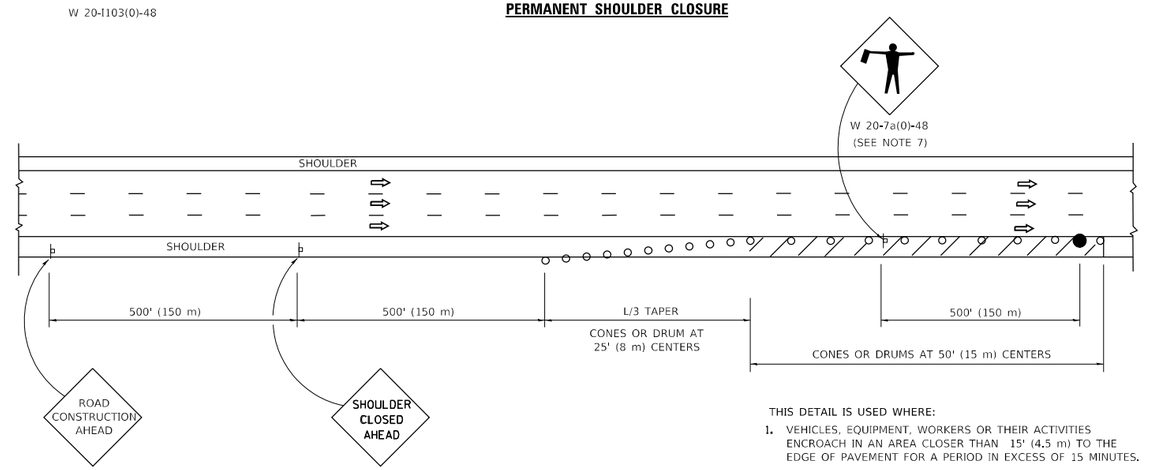


TYPICAL EXIT RAMP

SHOULDER CLOSURE DETAILS



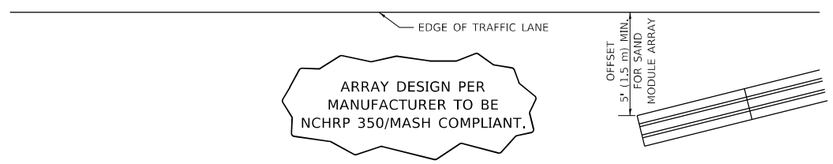
PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

THIS DETAIL IS USED WHERE:

1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENTUATOR, TEMPORARY
(SEE NOTE 5)

SYMBOLS

- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES:

1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS	
45 mph (80 km/h) OR GREATER:	METRIC	ENGLISH
	L=0.65(W)(S)	L=(W)(S)

W = WIDTH OF OFFSET IN FEET (METERS)
S = NORMAL POSTED SPEED MPH (KM/H)

2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTICLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
   a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
   b. THE WORK AVTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.

   THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
   16' MIN. WIDTH CURVE SECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN - D.W.S.	REVISED -	S.P.B. 12-09
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -	M.D. 06-13
PLOT DATE = 3/4/2019	DATE - 11-96	REVISED -	M.D. 01-18

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	42
	TC-17	CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	

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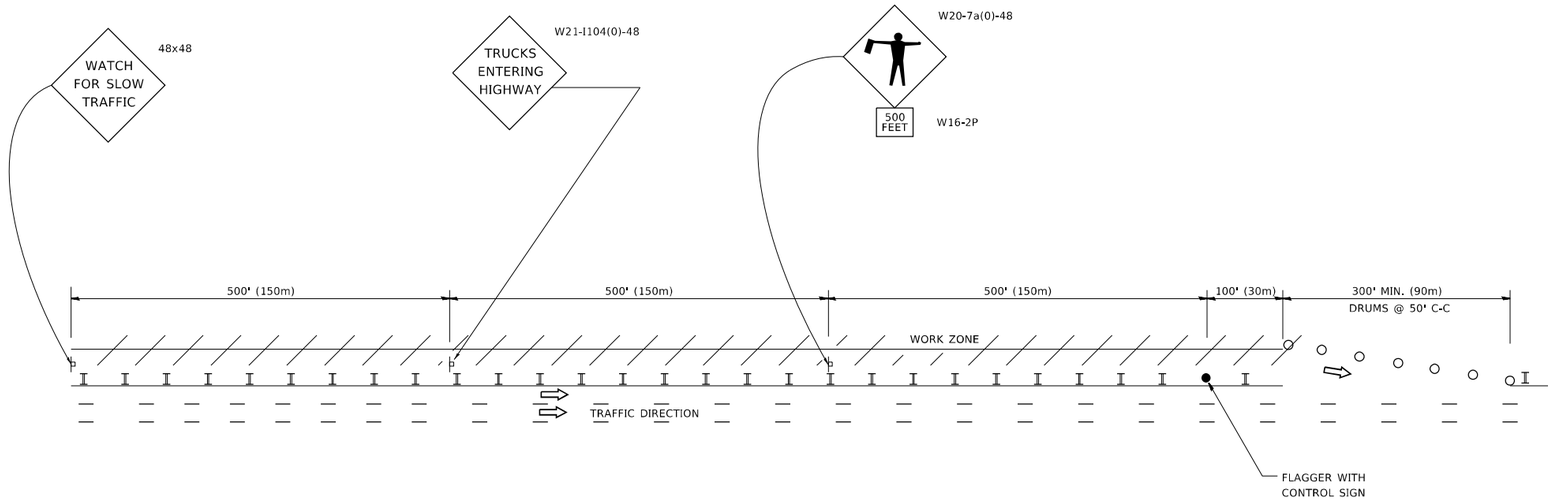
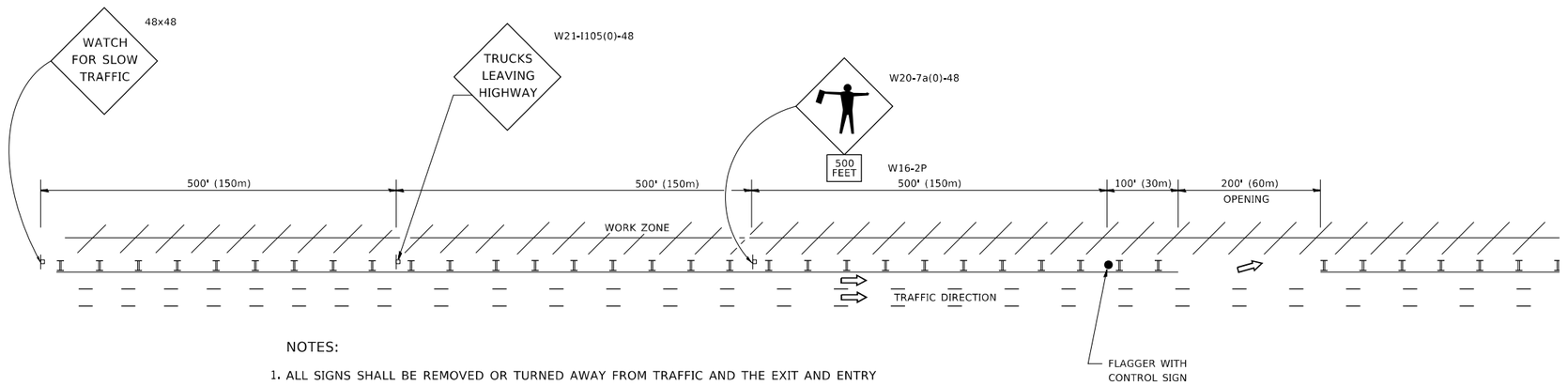
SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMPS.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME = footem}	DESIGNED -	REVISED - J.A.F. 02-06
	DRAWN -	REVISED - S.P.B. 01-07
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - S.P.B. 12-09
PLOT DATE = 3/4/2019	DATE -	REVISED - M.D.06-13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FREEWAY /EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS ON FREEWAYS /EXPRESSWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-180-BR	DUPAGE	51	43
	TC-18	CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	

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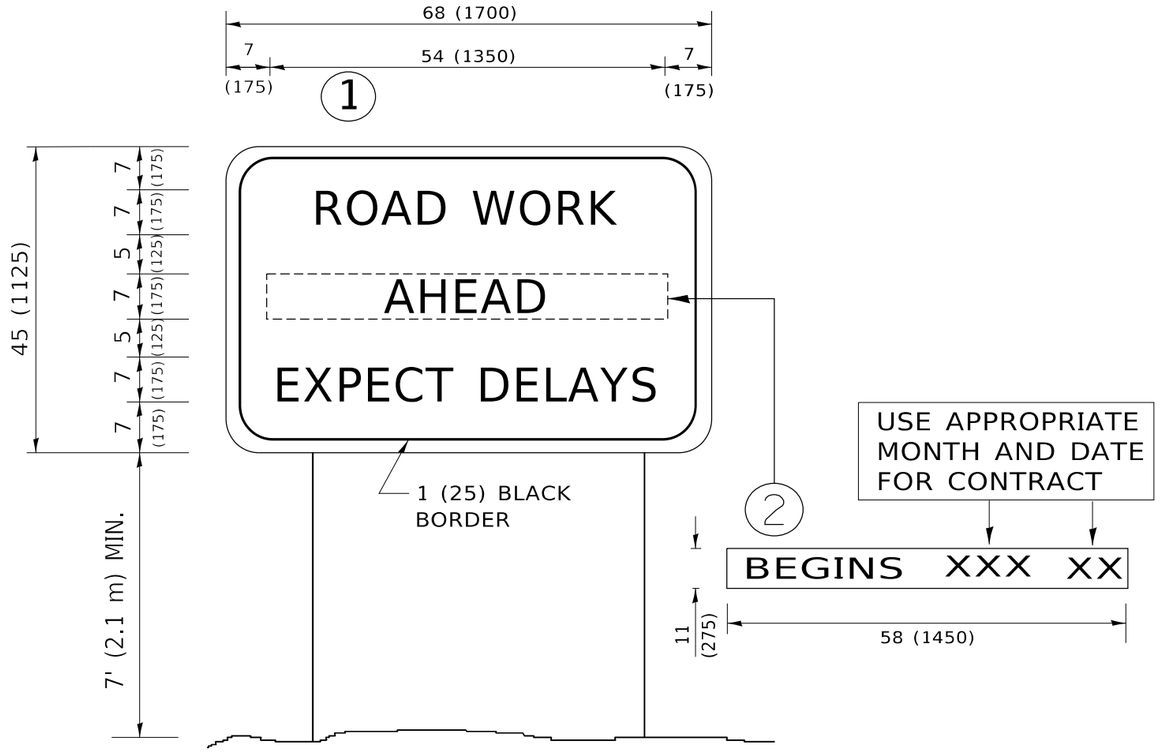
tc18.dgn 3/4/2019 10:44:51 AM User=footemu



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN -	REVISED - R. MIRS 12-11-97	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -T. RAMMACHER 02-02-99	
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 01-31-07	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

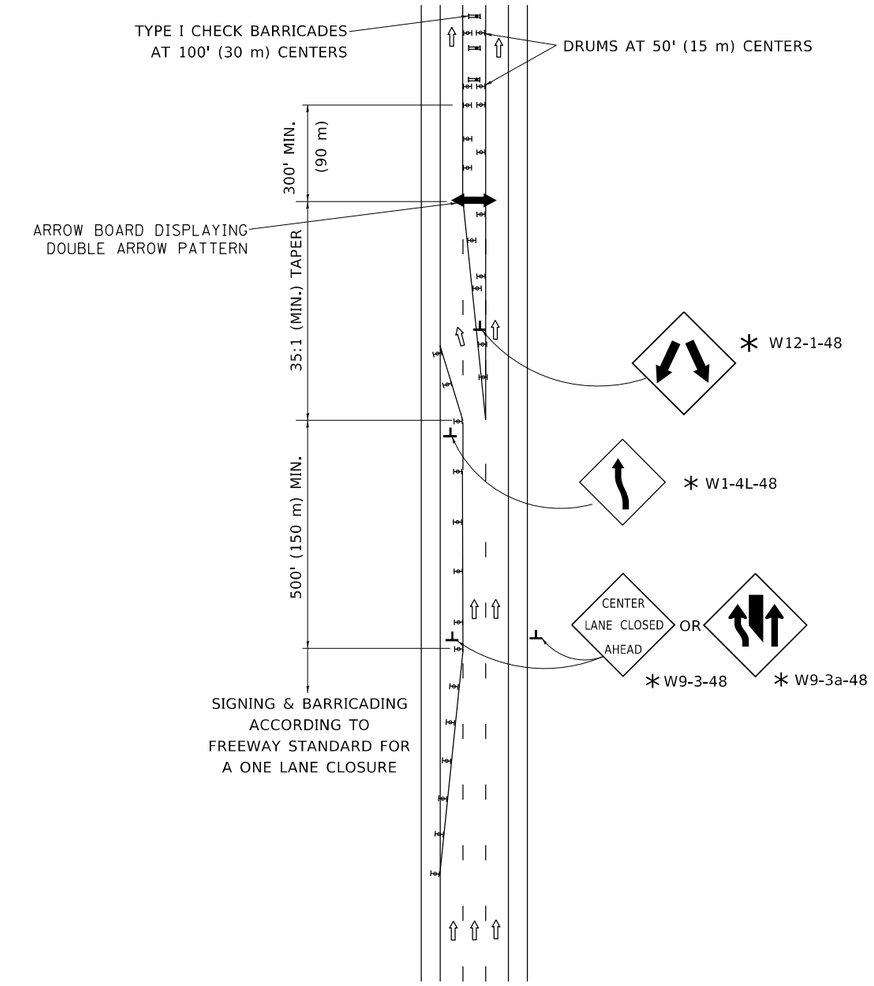
ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

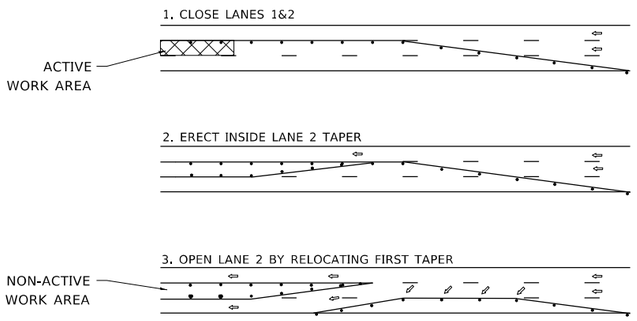
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	44
	TC–22	CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			

tc22.dgn 3/4/2019 10:48:10 AM User=footemj

CENTER LANE CLOSURE



INSTALLATION SEQUENCE



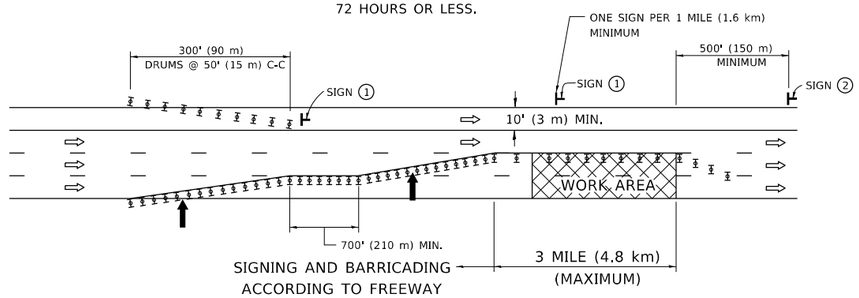
4. REMOVE CLOSURE IN REVERSE ORDER

NOTES:

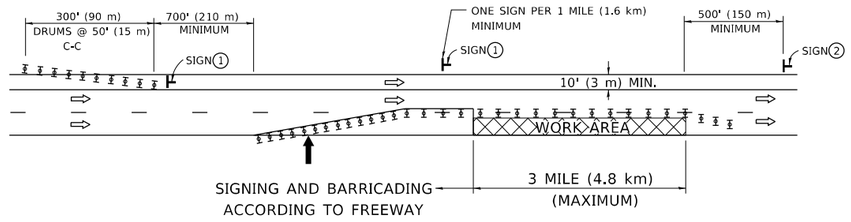
1. DRUMS WITH STEADY BURN LIGHTS SHALL BE USED AT 50' (15 m) CENTERS ON ALL TAPERS AND TANGENTS IN ADVANCE OF WORK AREA.
2. CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.
3. CENTER LANE CLOSURE CONFIGURATION IS NOT TO BE USED WITH WORKERS PRESENT.

SHOULDER LANE

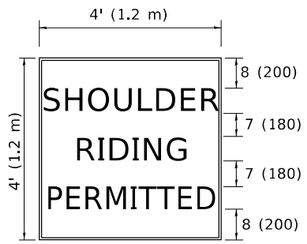
NOTE:
CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.

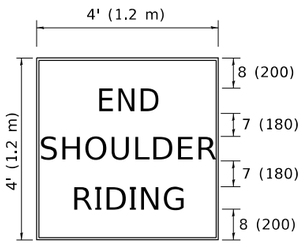


SIGNING AND BARRICADING ACCORDING TO FREEWAY STANDARD FOR A TWO LANE CLOSURE



SIGNING AND BARRICADING ACCORDING TO FREEWAY STANDARD FOR A ONE LANE CLOSURE





6 (150) SERIES "C" LEGEND
BLACK LEGEND
WHITE REFLECT. BACKGROUND
1 (25) BORDER

SYMBOLS

- DIRECTION OF TRAFFIC
- ARROWBOARD
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT *
- TYPE II BARRICADE, OR DRUM WITH MONO-DIRECTIONAL STEADY BURN LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

* ALL SIGNS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).

USER NAME = footemj	DESIGNED -	REVISED - J.A.F. 04-03
	DRAWN -	REVISED - S.P.B. 01-07
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - S.P.B. 12-09
PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
CENTER LANE CLOSURE SHOULDER LANE

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	45
	TC-25	CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	

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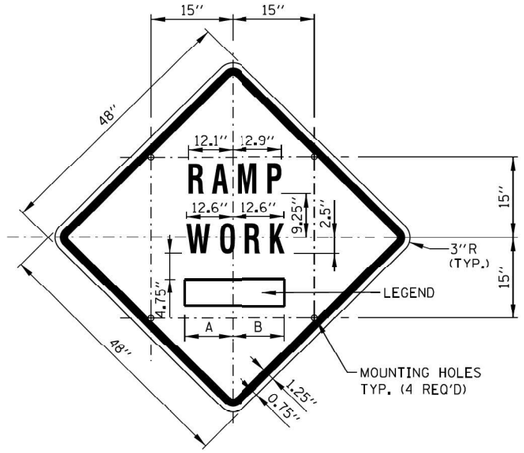
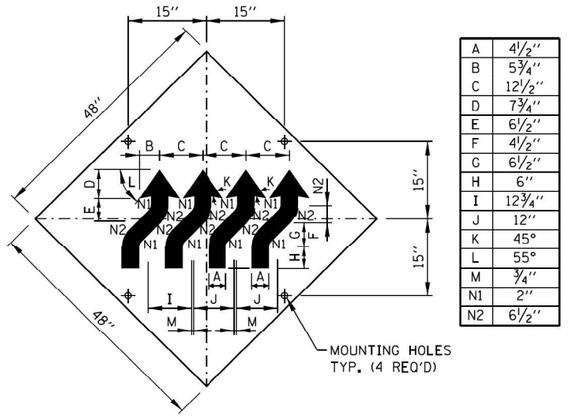


SIGN NO.	LEGEND	A	B
TS-2A	AHEAD	15.50''	15.50''
TS-2B	500 FT	14.25''	15.13''
TS-2C	1000 FT	14.88'' L2	15.75'' L2
TS-2D	1500 FT	14.88'' L2	15.75'' L2
TS-2E	1/2 MILE	15.75'' L3	15.75'' L3
TS-2F	1 MILE	13.06''	13.06''

SIGN TS-2 (O)

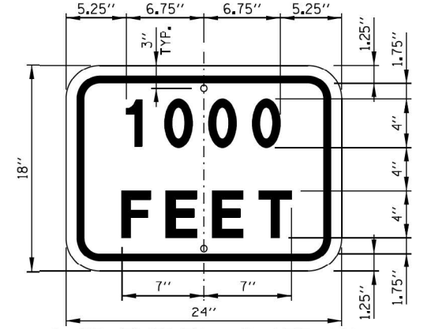
COLOR: BACKGROUND - FLUORESCENT ORANGE (O)
BORDER AND SYMBOL - BLACK
SIZE: 48''x48''
LETTERING: 7'' FEDERAL SERIES D
MOUNTING HOLES: 7/16'' DIA., 4 HOLES SPACED AS SHOWN



A	4 1/2''
B	5 3/4''
C	12 1/2''
D	7 3/4''
E	6 1/2''
F	4 1/2''
G	6 1/2''
H	6''
I	12 3/4''
J	12''
K	45°
L	55°
M	3/4''
N1	2''
N2	6 1/2''

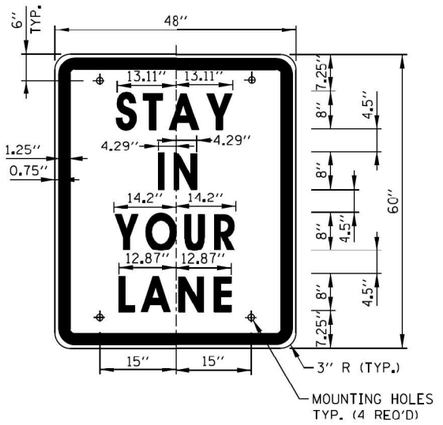
SIGN W1-4dR (O)

COLOR: BACKGROUND-FLUORESCENT ORANGE (O)
TYPE A REFLECTIVE
SHEETING PER STANDARD
SPECIFICATIONS (* A)
BORDER AND LETTERS-BLACK
SIZE: 48''x48''
MOUNTING HOLES: 7/16'' DIA., 4 HOLES SPACED AS SHOWN.



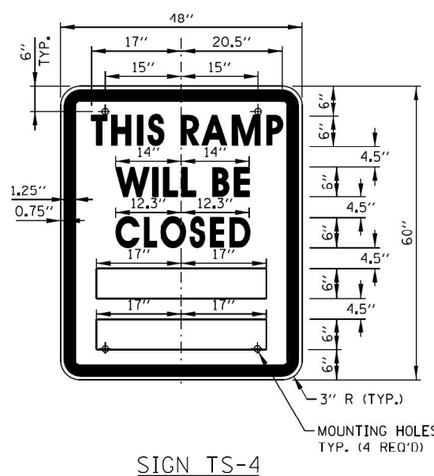
SUPPLEMENTAL PLATE (O)

COLOR: BACKGROUND - FLUORESCENT ORANGE (O)
BORDER AND LETTERS - BLACK
SIZE: 24''x18''
LETTERING: 4'' FEDERAL SERIES D
MOUNTING HOLES: 7/16'' DIA., 2 HOLES SPACED AS SHOWN



SIGN TS-3

COLOR: BACKGROUND - WHITE (REFLECTORIZED) (*A)
BORDER AND LETTERS - BLACK
SIZE: 48''x60''
LETTERING: LEGEND - 8'' FEDERAL SERIES D
MOUNTING HOLES: 7/16'' DIA., 4 HOLES, SPACED AS SHOWN

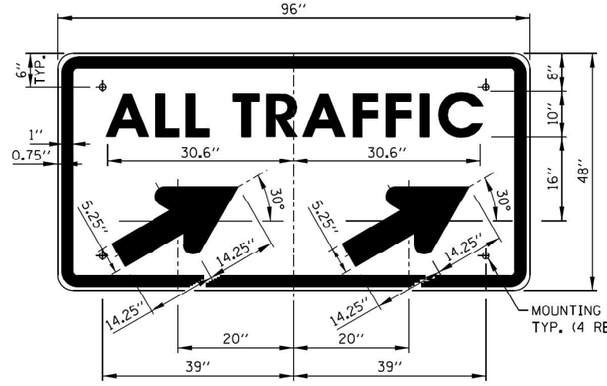


SIGN TS-4

COLOR: BACKGROUND - WHITE (REFLECTORIZED)(*A)
BORDER AND LETTERS - BLACK
SIZE: 48''x60''
LETTERING: LEGEND - 6'' FEDERAL SERIES C
MOUNTING HOLES: 7/16'' DIA., 4 HOLES, SPACED AS SHOWN

RAMP CLOSURE ADVANCE INFORMATION SIGN

THE VARIABLE MESSAGE WITH DATES FOR THE BOTTOM TWO LINES SHALL BE DETERMINED BY THE ENGINEER AND GIVEN TO THE CONTRACTOR BEFORE THE REQUIRED FIELD ERECTION DATE.



SIGN TS-5a & TS-5b

COLOR: BACKGROUND - WHITE (REFLECTORIZED)(*A)
BORDER AND LETTERS - BLACK
ARROW - BLACK
SIZE: 96''x48''
LETTERING: 10'' FEDERAL SERIES D
MOUNTING HOLES: 7/16'' DIA., 4 HOLES, SPACED AS SHOWN
NOTE: SIGN TS-5a IS SHOWN, SUBSTITUTE LEGEND "↙" FOR "↙↙" FOR SIGN TS-5b

NOTES:

1. ALL LETTERING IS DESIGNATED BY SIZE AND SERIES IN ACCORDANCE WITH THE LATEST EDITION OF "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS" AS PUBLISHED BY THE U.S. DEPARTMENT OF TRANSPORTATION. LETTERING SPACING SHALL BE IN ACCORDANCE WITH THIS GUIDE EXCEPT WHERE NOTED.
2. SYMBOLS AND ARROWS SHALL CONFORM TO THE DETAILS SHOWN IN THE LATEST EDITION OF "STANDARD HIGHWAY SIGNS" AS PUBLISHED BY THE U.S. DEPARTMENT OF TRANSPORTATION.
3. SEE THE CONTRACT REQUIREMENTS FOR ADDITIONAL NOTES AND SPECIFICATIONS.
   (O) FLUORESCENT ORANGE REFLECTIVE SHEETING PER THE STANDARD SPECIFICATIONS.
   (*A) - REFLECTIVE SHEETING PER THE STANDARD SPECIFICATIONS.
4. DIMENSIONS INDICATED THUS L ARE BASED ON A REDUCTION IN STANDARD LETTERING SPACING AS SHOWN BELOW:
   L1 SPACING REDUCED BY 25%
   L2 SPACING REDUCED BY 40%
   L3 SPACING REDUCED BY 50%

APPROVED BY: Paul Kovacs CHIEF ENGINEERING OFFICER DATE: 05/01/2009

DATE	REVISIONS
3-01-2019	REMOVED STANDARD IDOT SIGNS.
	REVISED WZSL ASSEMBLY, ADDED
	WZSL TRANSITION
3-31-2017	REVISED END WZSL SIGN COLOR
3-11-2015	REVISED NOTES

Illinois Tollway

CONSTRUCTION SIGNS

STANDARD E1-07

LIN ENGINEERING,LTD.
Consulting Engineers
Westmont, Illinois

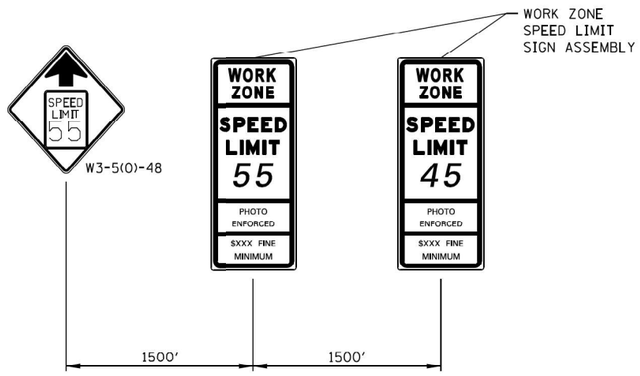
USER NAME = 14nho	DESIGNED - NH	REVISED -	
	DRAWN - NH	REVISED -	
PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -	
PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

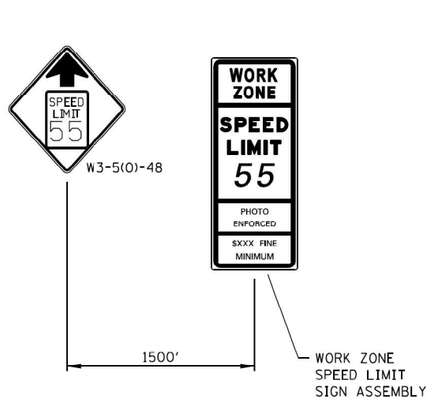
F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
ILLINOIS TOLLWAY STANDARDS

SCALE: N.T.S. | SHEET 1 OF 6 SHEETS | STA. | TO STA.

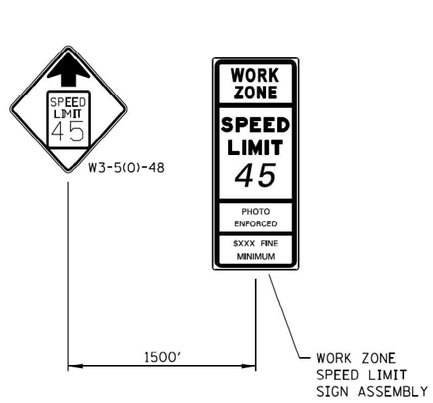
F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	46
		CONTRACT NO. 62M37		
		ILLINOIS FED. AID PROJECT		



70 MPH TO 45 MPH

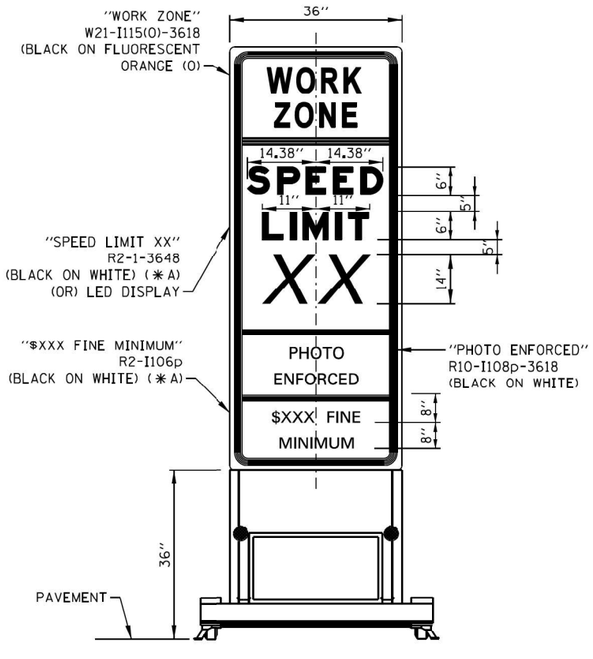


70 MPH TO 55 MPH

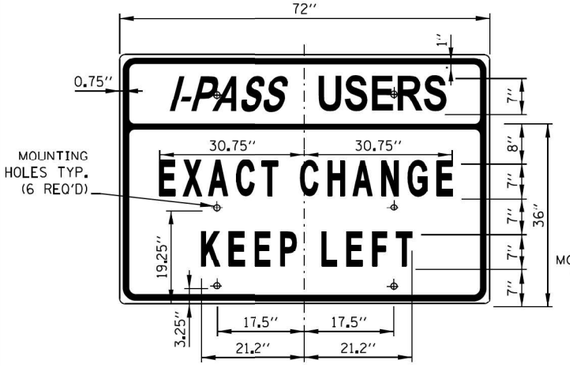


60 MPH TO 45 MPH

WORK ZONE SPEED LIMIT TRANSITION SIGNAGE

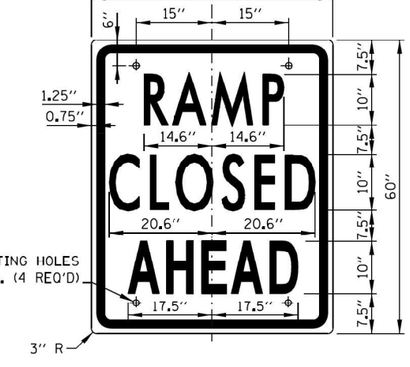


WORK ZONE SPEED LIMIT SIGN ASSEMBLY



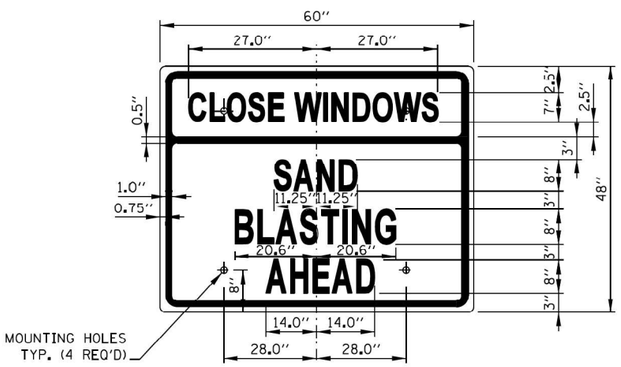
SIGN TS-7

COLOR: BACKGROUND - WHITE (REFLECTORIZED) (* A)
BORDER AND LETTERS - BLACK
SIZE: 72"x36"
LETTERING: 7" FEDERAL SERIES C
MOUNTING HOLES: 7/16" DIA., 4 HOLES SPACED AS SHOWN



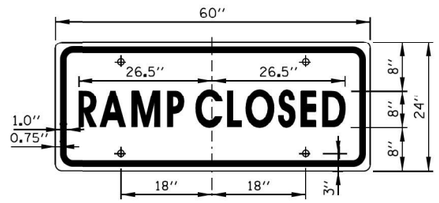
SIGN TS-9

COLOR: BACKGROUND - WHITE (REFLECTORIZED) (* A)
BORDER AND LETTERS - BLACK
SIZE: 48"x60"
LETTERING: 10" FEDERAL SERIES C
MOUNTING HOLES: 7/16" DIA., 4 HOLES SPACED AS SHOWN



SIGN TS-10 (O)

COLOR: BACKGROUND - FLUORESCENT ORANGE (O)
BORDER AND LETTERS - BLACK
SIZE: 60"x48"
LETTERING: 8" FEDERAL SERIES C, 7" FEDERAL SERIES B
MOUNTING HOLES: 7/16" DIA., 4 HOLES SPACED AS SHOWN



SIGN TS-6

COLOR: BACKGROUND - WHITE (REFLECTORIZED) (* A)
BORDER AND LETTERS - BLACK
SIZE: 60"x24"
LETTERING: 8" FEDERAL SERIES C
MOUNTING HOLES: 7/16" DIA., 4 HOLES SPACED AS SHOWN

APPROVED BY: Paul Kovacs
CHIEF ENGINEERING OFFICER
DATE: 05/01/2009

NOTE:
SEE SHEET 1 OF THIS SERIES FOR NOTES.

SHEET 2 OF 2

Illinois Tollway

CONSTRUCTION SIGNS

STANDARD E1-07

LIN ENGINEERING,LTD.
Consulting Engineers
Westmont, Illinois

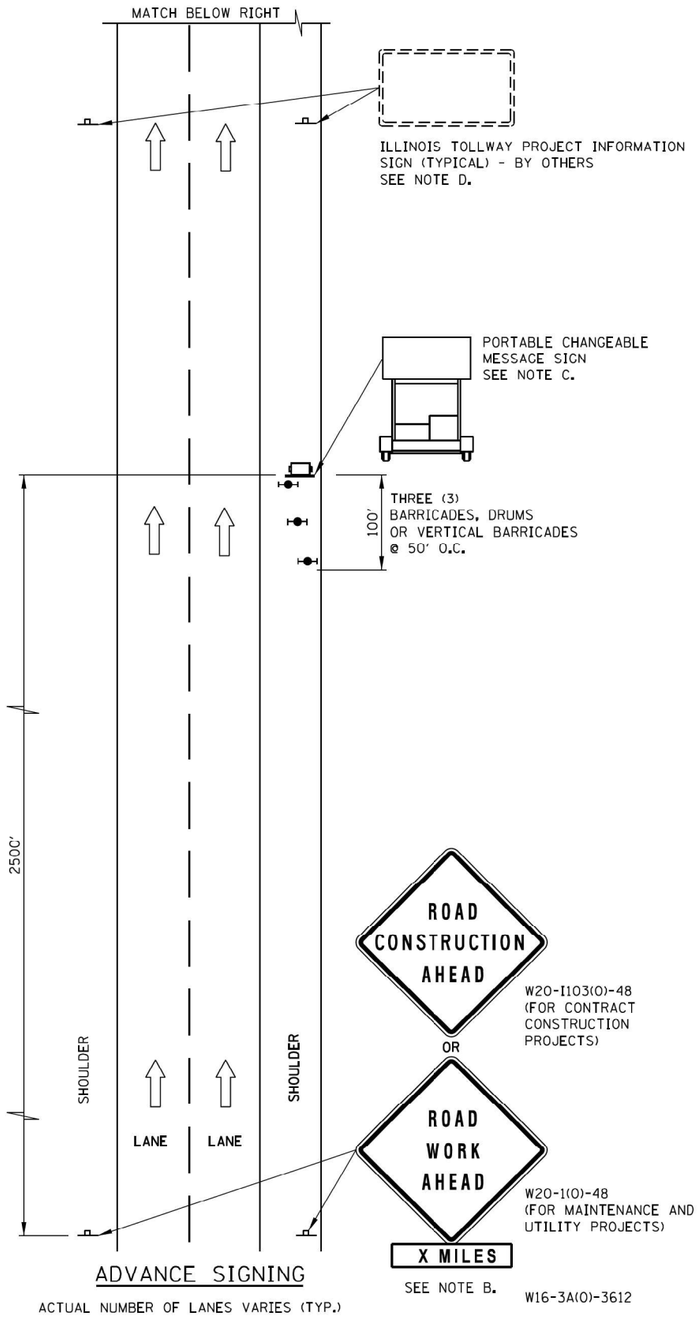
USER NAME = 14nha	DESIGNED - NH	REVISED -	
	DRAWN - NH	REVISED -	
PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -	
PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
ILLINOIS TOLLWAY STANDARDS

SCALE: N.T.S. | SHEET 2 OF 6 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	47
		CONTRACT NO. 62M37		
		ILLINOIS FED. AID PROJECT		



ADVANCE SIGNING

ACTUAL NUMBER OF LANES VARIES (TYP.)

ADVANCE SIGNING NOTES:

A. THE ADVANCE SIGNING SHOWN ON THIS STANDARD SHALL APPLY ANY TIME THE CONTRACTOR CLOSES ONE OR MORE LANES, OR IS REQUIRED TO SHIFT THE LANE ALIGNMENT. THE "ROAD WORK AHEAD" OR "ROAD CONSTRUCTION AHEAD" SIGNS, WORK ZONE PUBLIC INFORMATION SIGNS AND PORTABLE CHANGEABLE MESSAGE ARE STATIONARY.

B. THE ROAD CONSTRUCTION AHEAD SIGN (W20-1A, WITH W16-3a SUPPLEMENTAL PLATE) OR ROAD WORK AHEAD SIGN (W20-1, WITH W16-3A SUPPLEMENTAL PLATE) SHALL BE LOCATED UP TO 5 MILES IN ADVANCE OF THE PROJECT LIMITS, WITH THE LOCATION BEING DETERMINED BY THE ENGINEER.

C. THE PORTABLE CHANGEABLE MESSAGE SIGN SHALL BE USED TO DISPLAY THE STATUS OF LANE WITHIN THE CONTRACT LIMITS. THE PRIMARY MESSAGES SHALL BE: "RIGHT LANE(S) CLOSED" / "x MILES AHEAD", "LEFT LANE(S) CLOSED" / "x MILES AHEAD", "LANE(S) SHIFT" / "x MILES AHEAD", "ALL LANES OPEN". THE PORTABLE CHANGEABLE MESSAGE SIGN MAY BE MOVED TO THE MEDIAN SHOULDER WHEN THE LANE CLOSURES ARE ON THE LEFT, PROVIDED THE EXISTING SHOULDER WIDTH IS ADEQUATE.

D. THE ILLINOIS TOLLWAY WILL FURNISH AND INSTALL STATIC PROJECT INFORMATION SIGNS IN ADVANCE, THROUGH AND AT THE END OF THE WORK ZONE. THESE SIGNS WILL BE INSTALLED ALONG THE OUTSIDE SHOULDER WITH THE ADVANCE SIGNS LOCATED BEYOND THE PORTABLE CHANGEABLE MESSAGE SIGN. THE ENGINEER AND CONTRACTOR SHALL COORDINATE WITH THE ILLINOIS TOLLWAY REGARDING THE LOCATION OF THESE SIGNS AND NOTIFY THE ILLINOIS TOLLWAY OF ANY DAMAGE TO THE SIGNS OR SUPPORTS.

LEGEND

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH SEQUENTIAL FLASHING WARNING LIGHT
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH LIGHT IF REQUIRED. SEE ARTICLE 701.05(a)(5)
- FLAGGER WITH TRAFFIC CONTROL SIGN
- WORKER
- LANE CLOSED
- CHECK BARRICADE
- TRUCK MOUNTED ATTENUATOR

APPROVED BY: Paul Kovacs, CHIEF ENGINEERING OFFICER — DATE: 05/01/2009

DATE	REVISIONS
3-01-2021	DELETED WORK ZONE PUBLIC INFORMATION SIGN.
3-01-2020	CLARIFIED TMA REQUIREMENTS, UPDATED BARRICADE LIGHT CALL-OUTS
3-01-2019	REARRANGED DETAILS, REVISED NOTE 17, ADDED NOTES 18 & 19, ADDED TMA

Illinois Tollway

LANE CLOSURE DETAILS

STANDARD E2-10

LIN ENGINEERING,LTD. Consulting Engineers, Westmont, Illinois

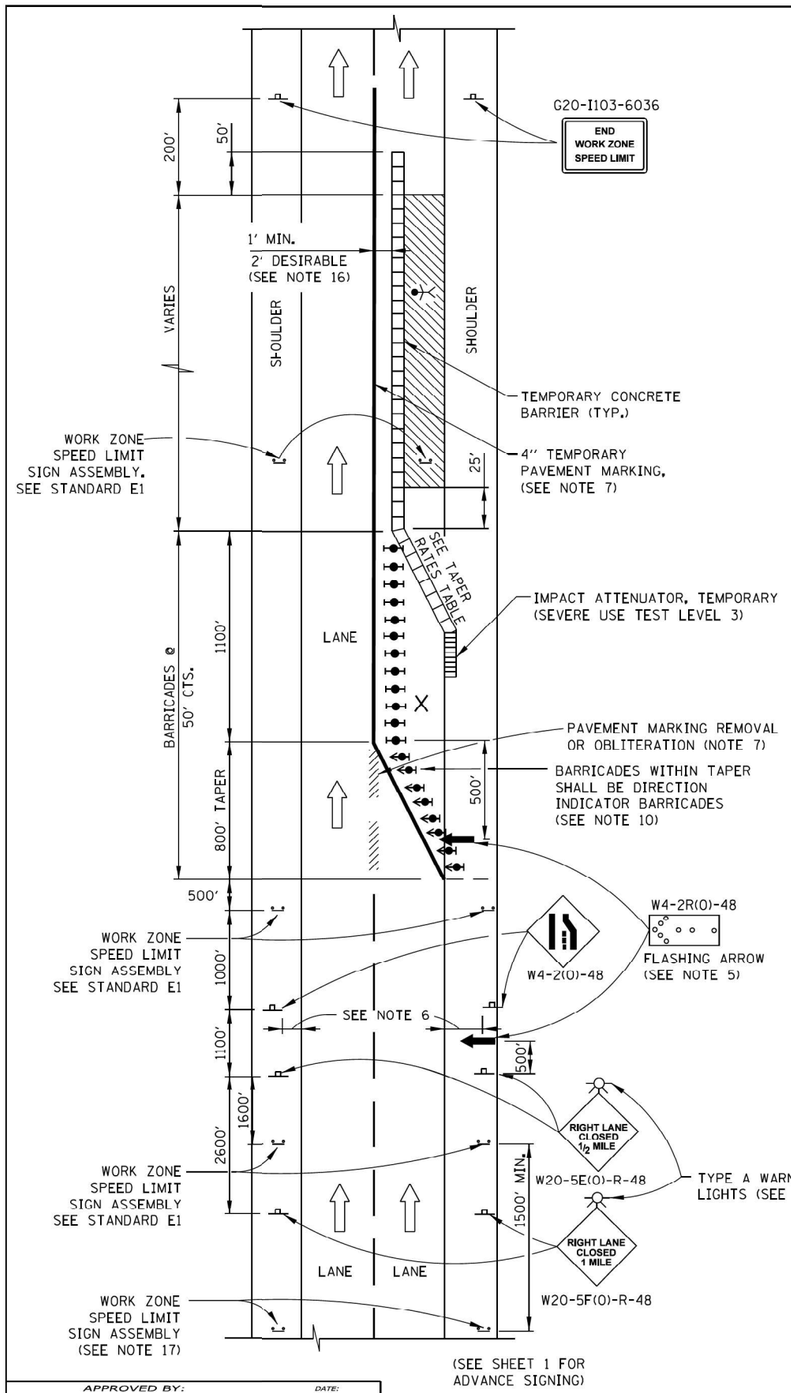
USER NAME = 14nho	DESIGNED - NH	REVISED -	
	DRAWN - NH	REVISED -	
PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -	
PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

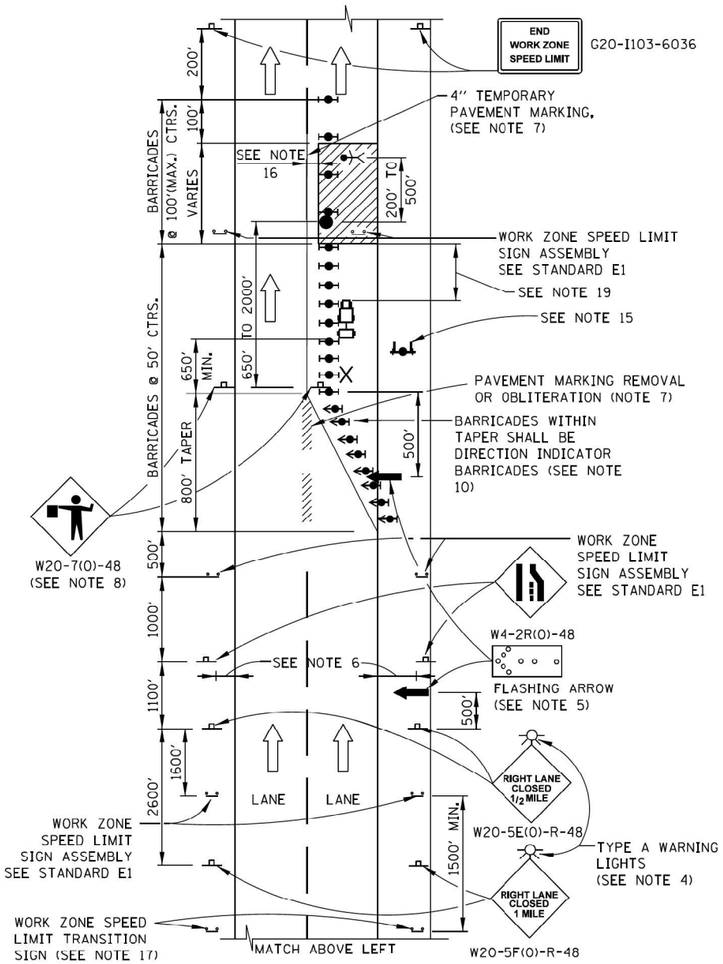
F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
ILLINOIS TOLLWAY STANDARDS

SCALE: N.T.S. | SHEET 3 OF 6 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	48
		CONTRACT NO. 62M37		
		ILLINOIS FED. AID PROJECT		



ONE - LANE CLOSURE WITH BARRIER



ONE - LANE CLOSURE WITH BARRICADE

TAPER RATES

WORK ZONE SPEED (mph)	SHY LINE (ft.)	BARRIER INSIDE SHY LINE	BARRIER AT OR BEYOND SHY LINE
65	8.5	28:1	19:1
60	8	26:1	18:1
55	7	24:1	16:1
50	6.5	21:1	14:1
45	6	18:1	12:1
40	5	16:1	10:1
35	4.5	15:1	9:1
30	4	13:1	8:1

LEGEND

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH SEQUENTIAL FLASHING WARNING LIGHT
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH LIGHT IF REQUIRED. SEE ARTICLE 701.05(a)(5)
- FLAGGER WITH TRAFFIC CONTROL SIGN
- WORKER
- LANE CLOSED
- CHECK BARRICADE
- TRUCK MOUNTED ATTENUATOR

LANE CLOSURE NOTES:

1. IF CLOSURES ARE EXPECTED TO PRODUCE TRAFFIC BACKUPS EXTENDING BEYOND THE FIRST WARNING SIGN SHOWN ON THE DETAILS, ADDITIONAL UPSTREAM SIGNS SHALL BE PLACED SO THAT THE TRAFFIC CONTROL ZONE ENCOMPASSES THE ANTICIPATED BACKUP ZONE.
2. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
3. THESE DETAILS ALSO APPLY TO OPPOSITE HAND LANE CLOSURES BY CHANGING SIGN LEGENDS AND ARROW DIRECTIONS TO INDICATE THE APPROPRIATE CLOSURE.
4. FOR NIGHT TIME CLOSURES, ONE TYPE A WARNING LIGHT SHALL BE INSTALLED ABOVE EACH OF THE 1 MILE AND ½ MILE ADVANCE WARNING SIGNS. FOR DAYLIGHT - ONLY CLOSURES, THE LIGHTS MAY BE OMITTED.
5. FOR ANY LANE CLOSURE, FLASHING ARROW BOARDS SHALL BE REQUIRED AND IN OPERATION AT ALL TIMES. THE FLASHING ARROW BOARD IN ADVANCE OF THE TAPER SHALL BE PROTECTED WITH THREE TYPE II BARRICADES AT 50' O.C.
6. CONSTRUCTION SIGNS SHALL GENERALLY BE POST - MOUNTED OR ATTACHED TO PORTABLE SUPPORTS AND SHALL BE INSTALLED 8' TO 12' FROM ADJACENT TRAVEL LANE WHEREVER POSSIBLE. IN NO CASE SHALL SIGNS BE LOCATED TO PROVIDE LESS THAN 2' CLEARANCE BETWEEN EDGE OF SIGN AND ADJACENT TRAVEL LANE.
7. PAVEMENT MARKING TAPE AND REMOVAL OR OBLITERATION OF EXISTING MARKINGS SHALL BE REQUIRED WHEN THE CLOSURE TIME EXCEEDS FOUR DAYS. THIS WORK SHALL BE MEASURED AND PAID FOR SEPARATELY.
8. WHEN A FLAGGER IS NOT ON STATION, THE FLAGGER SIGN SHALL BE PROMPTLY REMOVED, COVERED OR TURNED TO FACE AWAY FROM TRAFFIC. FLAGGER SIGNS SHALL BE MOVED AS NECESSARY TO MAINTAIN THE REQUIRED SPACING BETWEEN THE SIGNS AND THE WORKERS IN EACH SEPARATE WORK ACTIVITY, PER THE ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS.
9. WORK ZONE SPEED LIMIT SIGN ASSEMBLIES, SHALL BE PLACED ADJACENT TO THE OPEN TRAFFIC LANE(S). WORK ZONE SPEED SIGNS SHALL BE MOVED AS NECESSARY TO MAINTAIN THE REQUIRED SPACING BETWEEN SIGNS AND THE WORKERS IN EACH SEPARATE WORK ACTIVITY PER THE ILLINOIS TOLLWAY SUPPLEMENTAL SPECIFICATIONS.
10. DIRECTION INDICATOR BARRICADES SHALL BE USED IN LANE TAPERS.
11. FOR CLOSURES OTHER THAN SHORT TERM (SUNRISE TO ONE HOUR BEFORE SUNSET), THE MINIMUM HEIGHT OF THE SIGN FROM SHOULDER ELEVATION SHALL BE 7'-0".
12. CONES MAY BE USED IN LIEU OF BARRICADES IN THE BUFFER AND WORK AREAS. WHEN THE CLOSURE IS FOR MAINTENANCE OPERATIONS.
13. BARRICADES ARE TO BE LOCATED AT JOINT LINE WHEN WORK AREA EXTENDS UP TO JOINT UNLESS OTHERWISE SHOWN ON THE PLANS.
14. SEE MAINTENANCE OF TRAFFIC DRAWINGS FOR ADDITIONAL SIGNING IN THIS AREA.
15. CHECK BARRICADES SHALL BE PLACED IN EACH CLOSED LANE AND SHOULDER AT 1000 FOOT CENTERS.
16. A 1'-0" MINIMUM/2'-0" DESIRABLE SHY DISTANCE SHALL BE PROVIDED, MEASURED BETWEEN EDGE OF PAVEMENT LANE MARKING TO THE EDGE OF THE TRAFFIC CONTROL DEVICE.
17. SEE STANDARD E1 FOR ADDITIONAL SIGNAGE REQUIRED WHEN WORK ZONE SPEED LIMIT IS REDUCED BY MORE THAN 10 MPH. THE SPEED LIMIT SHALL BE TRANSITIONED TO THE SPECIFIED WORK ZONE SPEED LIMIT 2600 FEET BEFORE THE FIRST W4-2 SIGN.
18. WHEN NO POSITIVE PROTECTION IS PROVIDED AND WORKERS OR EQUIPMENT ENCROACH WITHIN 2'-0" OR LESS FROM THE EDGE OF TRAVELED WAY, THE LANE OPEN TO TRAFFIC SHALL BE TEMPORARILY CLOSED OR SHIFTED DURING WORK ACTIVITIES.
19. IN WORK ZONES WITH NO POSITIVE PROTECTION, A TRUCK MOUNTED ATTENUATOR (TMA) SHALL BE PROVIDED WITH A BUFFER AREA BETWEEN THE FRONT OF THE TMA AND WORKERS OR EQUIPMENT. THE BUFFER AREA SHALL BE 200' UNLESS OTHERWISE DETERMINED. WHERE WORKERS OR EQUIPMENT ARE PRESENT BEYOND THE WORK AREA, AN ADDITIONAL TMA SHALL BE PROVIDED TO EACH WORK AREA. A WORK AREA IS DEFINED AS STARTING AT THE END OF THE BUFFER AREA, EXTENDING 1000 FEET BEYOND THIS POINT.

APPROVED BY: Paul Kovacs, CHIEF ENGINEERING OFFICER — DATE: 05/01/2009

Illinois Tollway

LANE CLOSURE DETAILS

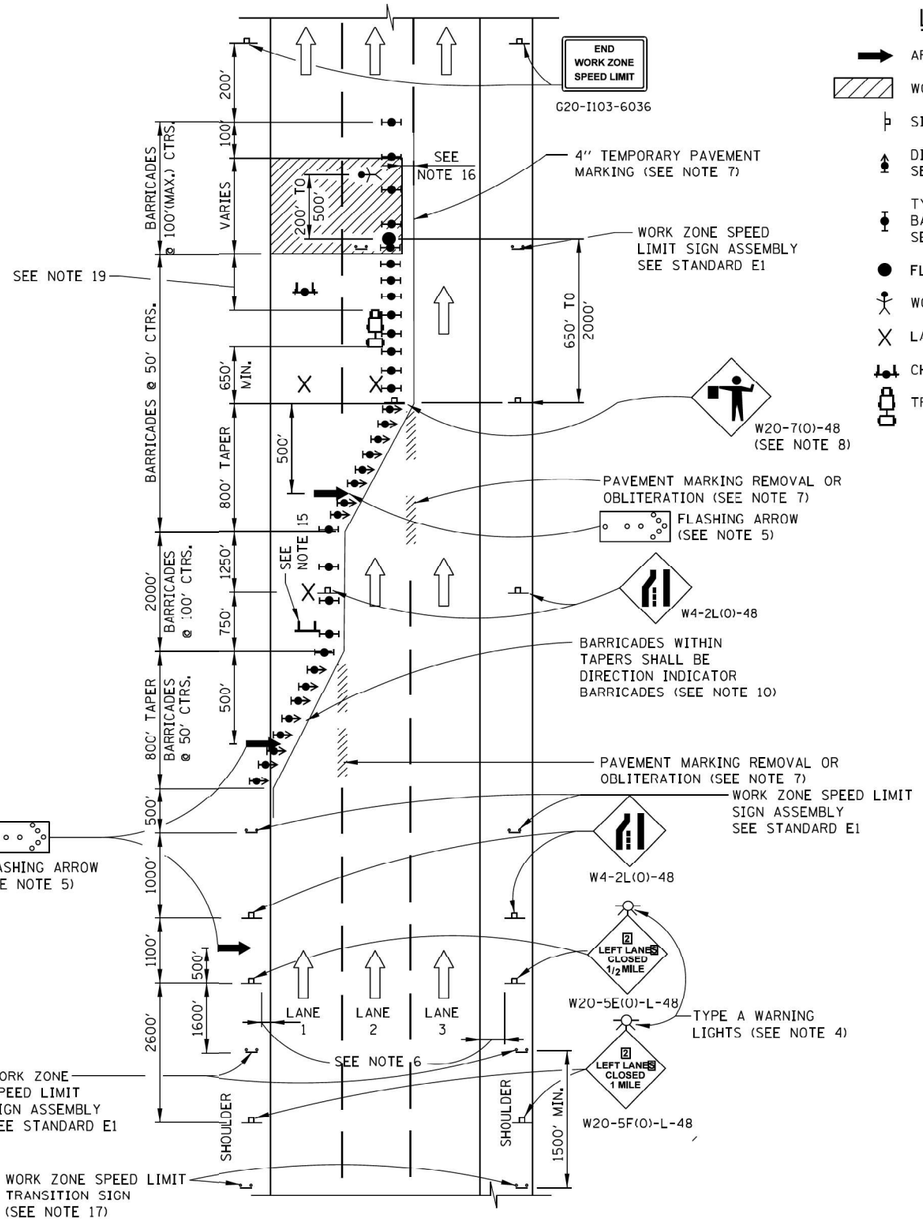
STANDARD E2-10

LIN ENGINEERING,LTD. Consulting Engineers, Westmont, Illinois	USER NAME = 14nho	DESIGNED - NH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
		DRAWN - NH	REVISED -	
	PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -	
	PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -	

F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
ILLINOIS TOLLWAY STANDARDS

SCALE: N.T.S. | SHEET 4 OF 6 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	49
		CONTRACT NO. 62M37		
		ILLINOIS	FED. AID PROJECT	



MATCH ADVANCED SIGNING ON SHEET 1

TWO - LANE CLOSURE WITH BARRICADE

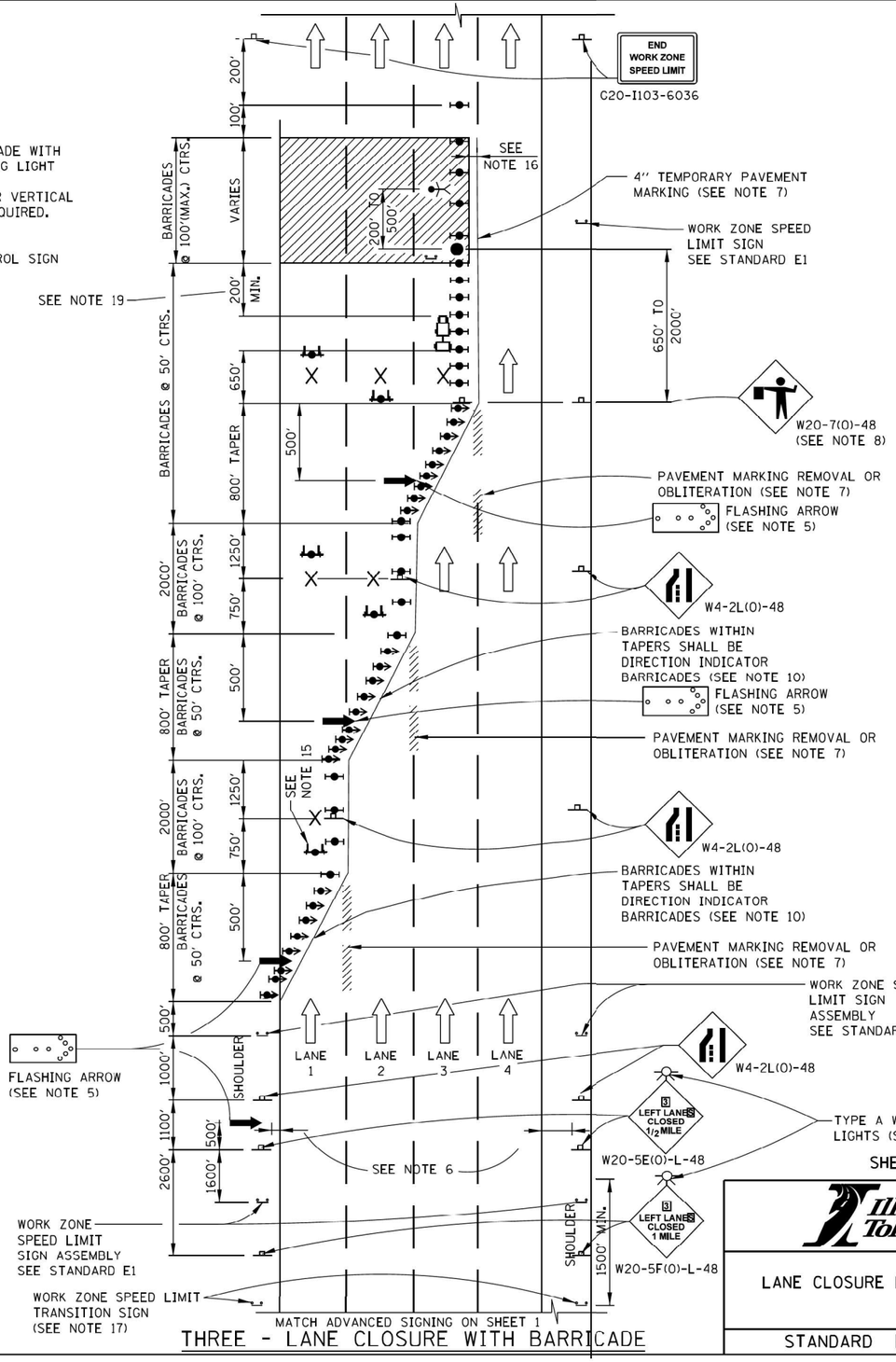
ACTUAL NUMBER OF LANES VARIES (TYP.)

APPROVED BY: Paul Kovacs, CHIEF ENGINEERING OFFICER — DATE: 05/01/2009

SEE SHEET 1 IN THIS SERIES FOR NOTES

LEGEND

- ARROW BOARD
- WORK AREA
- SIGN
- DIRECTION INDICATOR BARRICADE WITH SEQUENTIAL FLASHING WARNING LIGHT
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE WITH LIGHT IF REQUIRED. SEE ARTICLE 701.05(a)(5)
- FLAGGER WITH TRAFFIC CONTROL SIGN
- WORKER
- LANE CLOSED
- CHECK BARRICADE
- TRUCK MOUNTED ATTENUATOR



MATCH ADVANCED SIGNING ON SHEET 1

THREE - LANE CLOSURE WITH BARRICADE

SHEET 3 OF 3

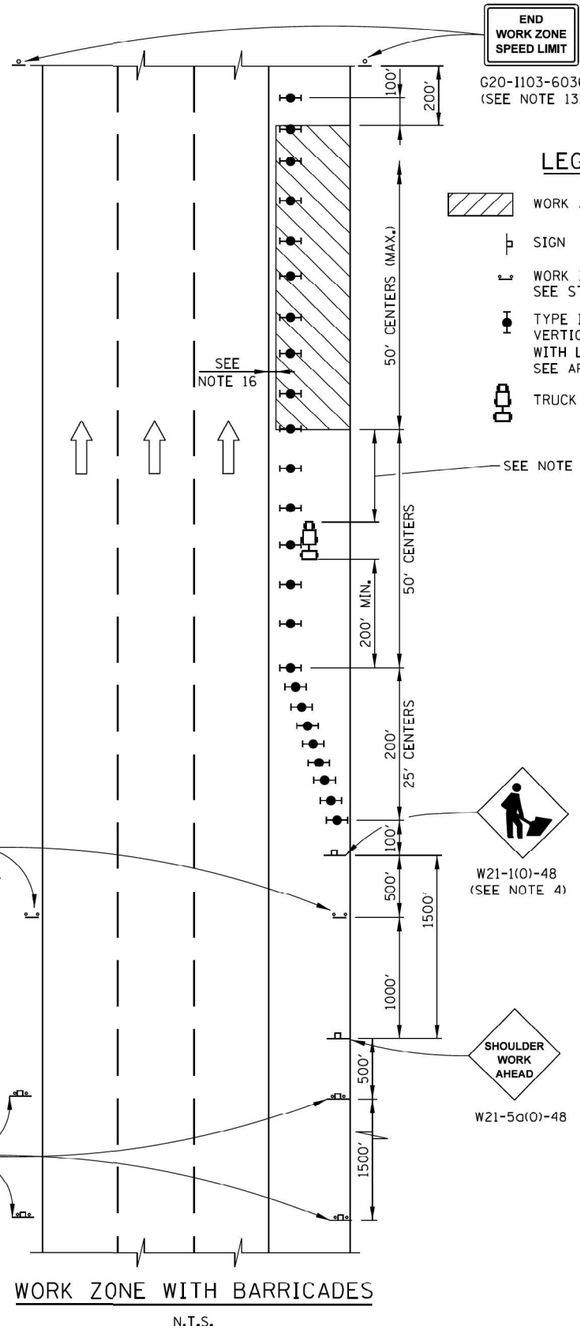
Illinois Tollway

LANE CLOSURE DETAILS

STANDARD E2-10

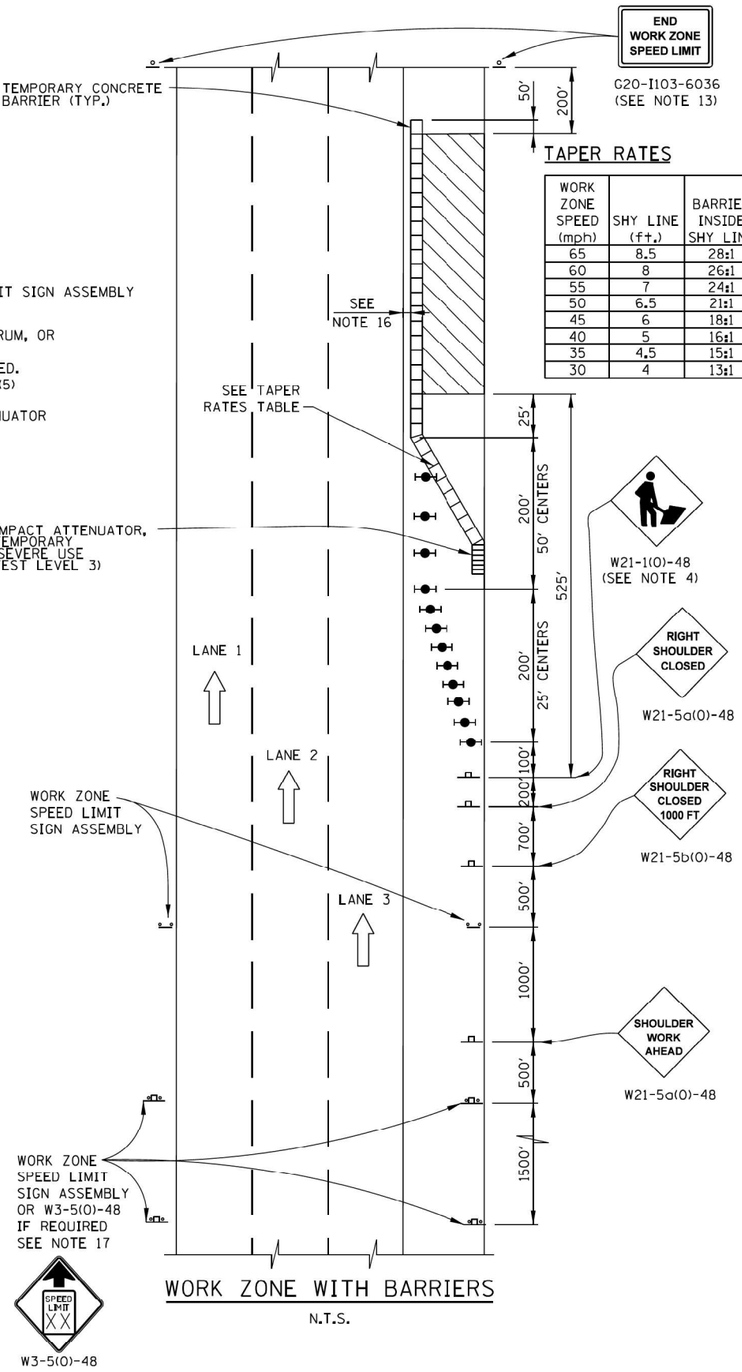
LIN ENGINEERING,LTD. Consulting Engineers Westmont, Illinois	USER NAME = 14nho	DESIGNED - NH	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88 ILLINOIS TOLLWAY STANDARDS
		DRAWN - NH	REVISED -		
	PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -		SCALE: N.T.S. SHEET 5 OF 6 SHEETS STA. TO STA.
	PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	50
		CONTRACT NO. 62M37		
		ILLINOIS FED. AID PROJECT		



WORK ZONE WITH BARRICADES

N.T.S.



WORK ZONE WITH BARRIERS

N.T.S.

TAPER RATES

WORK ZONE SPEED (mph)	SHY LINE (ft.)	BARRIER INSIDE SHY LINE	BARRIER AT OR BEYOND SHY LINE
65	8.5	28:1	19:1
60	8	26:1	18:1
55	7	24:1	16:1
50	6.5	21:1	14:1
45	6	18:1	12:1
40	5	16:1	10:1
35	4.5	15:1	9:1
30	4	13:1	8:1

GENERAL NOTES:

1. THE SHOULDER SHALL BE CLOSED WHEN A WORK ACTIVITY REQUIRING 15 OR MORE MINUTES IS PERFORMED AT A DISTANCE WHICH IS LESS THAN 15 FEET BUT NO CLOSER THAN 2 FEET FROM THE EDGE OF PAVEMENT.
2. THE ADJACENT EXTERIOR LANE SHALL BE CLOSED WHEN WORK IS PERFORMED WITHIN 2 FEET FROM THE EDGE OF PAVEMENT.
3. THE CHANNELIZING DEVICES WHICH SEPARATE THE WORK SPACE FROM THE ADJACENT TRAVEL LANE SHALL BE SPACED AT 25' FOR (200 FEET) AND AT A MAXIMUM OF 50' FOR ALL ADDITIONAL DEVICES.
4. WHEN THE WORK SITE IS UNATTENDED, SUBSTITUTE - "SHOULDER WORK AHEAD" SIGN.
5. WORKER SIGNS OR SHOULDER WORK SIGNS AND CHANNELIZATION DEVICES ARE PLACED ONLY ON THE SIDE OF THE ROADWAY ON WHICH THE ACTIVITY IS PERFORMED.
6. FOR SHOULDER CLOSURE EXTENDING OVERNIGHT, BARRICADE TYPE II SHALL BE USED. SEE ARTICLE 701.05(a)(5) FOR BARRICADE LIGHT REQUIREMENTS
7. FOR SHORT TERM CLOSURE (SUNRISE TO ONE HOUR BEFORE SUNSET) NOT EXTENDING INTO DARKNESS, CONES MAY BE USED.
8. ONE WORK ZONE SPEED LIMIT SIGN ASSEMBLY SHALL BE PLACED AT A DISTANCE OF 500' TO 2,500' MAXIMUM IN ADVANCE OF WORKERS THROUGHOUT THE SHOULDER CLOSURE. MOVING OPERATIONS MAY REQUIRE CONTINUOUS ADJUSTMENT OF THE SIGN ASSEMBLY LOCATION TO MAINTAIN THE ABOVE INTERVAL.
9. AN ADDITIONAL SIGN ASSEMBLY SHALL BE PLACED 500' BEYOND THE LAST ENTRANCE RAMP FOR EACH INTERCHANGE THAT FALLS WITHIN THE 2,500'.
10. THE SIGN ASSEMBLY SHALL BE PLACED NO CLOSER THAN 500' TO ANY OTHER SIGN.
11. THE WORK ZONE SPEED LIMIT SIGNS AND SIGN ASSEMBLY SHALL BE PROMPTLY REMOVED OR COVERED WHEN SHOULDER CLOSURE IS REMOVED.
12. ALL CONFLICTING SPEED LIMIT SIGNS SHALL BE COVERED OR REMOVED.
13. "END WORK ZONE SPEED LIMIT" SIGNS SHALL BE IN PLACE ONLY WHEN THE EXISTING POSTED SPEED > 55MPH.
14. FOR SHOULDER REPAIRS OR REPLACEMENT THE CHANNELIZING DEVICES SHALL BE PLACED AT THE EDGE OF PAVEMENT WHENEVER THE WORK ACTIVITIES RESULT IN A DROP OFF AT THE EDGE OF PAVEMENT.
15. ANY UNATTENDED OBSTACLE OR EXCAVATION LEFT ON THE SHOULDER OVERNIGHT SHALL BE IN COMPLIANCE WITH THE ROADWAY TRAFFIC CONTROL AND COMMUNICATIONS MANUAL.
16. A 1'-0" MINIMUM/2'-0" DESIRABLE SHY DISTANCE SHALL BE PROVIDED, MEASURED BETWEEN EDGE OF PAVEMENT LANE MARKING TO THE EDGE OF THE TRAFFIC CONTROL DEVICE.
17. SEE STANDARD E1 FOR ADDITIONAL SIGNAGE REQUIRED WHEN WORK ZONE SPEED LIMIT IS REDUCED BY MORE THAN 10 MPH.
18. IN WORK ZONES WITH NO POSITIVE PROTECTION, A TRUCK MOUNTED ATTENUATOR SHALL BE PROVIDED WITH A BUFFER AREA BETWEEN THE FRONT OF THE TMA AND WORKERS OR EQUIPMENT. THE BUFFER AREA SHALL BE 200' UNLESS OTHERWISE DETERMINED. WHERE WORKERS OR EQUIPMENT ARE PRESENT BEYOND THE WORK AREA, AN ADDITIONAL TMA SHALL BE PROVIDED FOR EACH WORK AREA IS DEFINED AS STARTING AT THE END OF THE BUFFER AREA, EXTENDING 1000 FEET BEYOND THIS POINT.

APPROVED BY: Paul Kovacs, CHIEF ENGINEERING OFFICER — DATE: 05/01/2009

DATE	REVISIONS
3-01-2021	DELETED WORK ZONE PUBLIC INFORMATION SIGN
3-01-2020	CLARIFIED TMA RQMTS & UPDATED BARRICADE LIGHT RQMTS
3-01-2019	DELETED W21-1a, ADJUSTED SIGN SPACING, ADDED TMA, REVISED NOTES

Illinois Tollway

SHOULDER CLOSURE DETAILS

STANDARD E3-09

LIN ENGINEERING, LTD. Consulting Engineers, Westmont, Illinois

USER NAME = 14nho	DESIGNED - NH	REVISED -	
	DRAWN - NH	REVISED -	
PLOT SCALE = 2.0000 ' / in.	CHECKED - ST	REVISED -	
PLOT DATE = 1/29/2024	DATE - 02/2024	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. ROUTE 344 (IL ROUTE 83) OVER I-88
ILLINOIS TOLLWAY STANDARDS

SCALE: N.T.S. | SHEET 6 OF 6 SHEETS | STA. | TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2020-182-BR	DUPAGE	51	51
		CONTRACT NO. 62M37		
	ILLINOIS FED. AID PROJECT			