

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	1
		ILLINOIS	CONTRACT NO. 62R75	

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE  
OF DOLTON

PROPOSED  
HIGHWAY PLANS

FAP 397/L 83 (SIBLEY BLVD) OVER UNION PACIFIC RAILROAD  
SECTION FAP 397 22 BRIDGE REHAB  
PROJECT NO: NHPP-BP64(466)  
BRIDGE DECK OVERLAY AND JOINT REPAIR  
COOK COUNTY

TRAFFIC DATA

IL 83  
ADT: 29,100  
POSTED SPEED: 35 MPH  
DESIGN DESIGNATION: OTHER PRINCIPAL ARTERIAL

APPLY TO SHEETS:  
1-17, 37-44

APPLY TO SHEETS:  
18-36



Hugh E. Regan

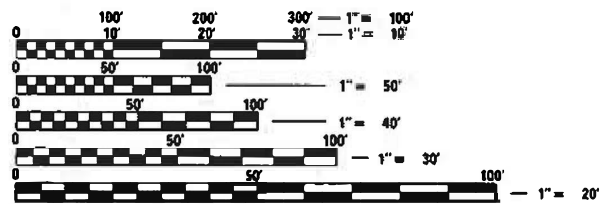
HUGH E. REGAN  
HNTB  
NO.: 062072743  
DATE SIGNED: 1/26/2024  
EXP. DATE: 11/30/2025



Ralph J. Otrembiak

RALPH J. OTREMBIAK  
GKE, LLC  
NO.: 081004812  
DATE SIGNED: 1/26/2024  
EXP. DATE: 11/30/2024

IL 83 OVER UPRR  
BRIDGE LOCATION  
STRUCTURE NO. 016-0955



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

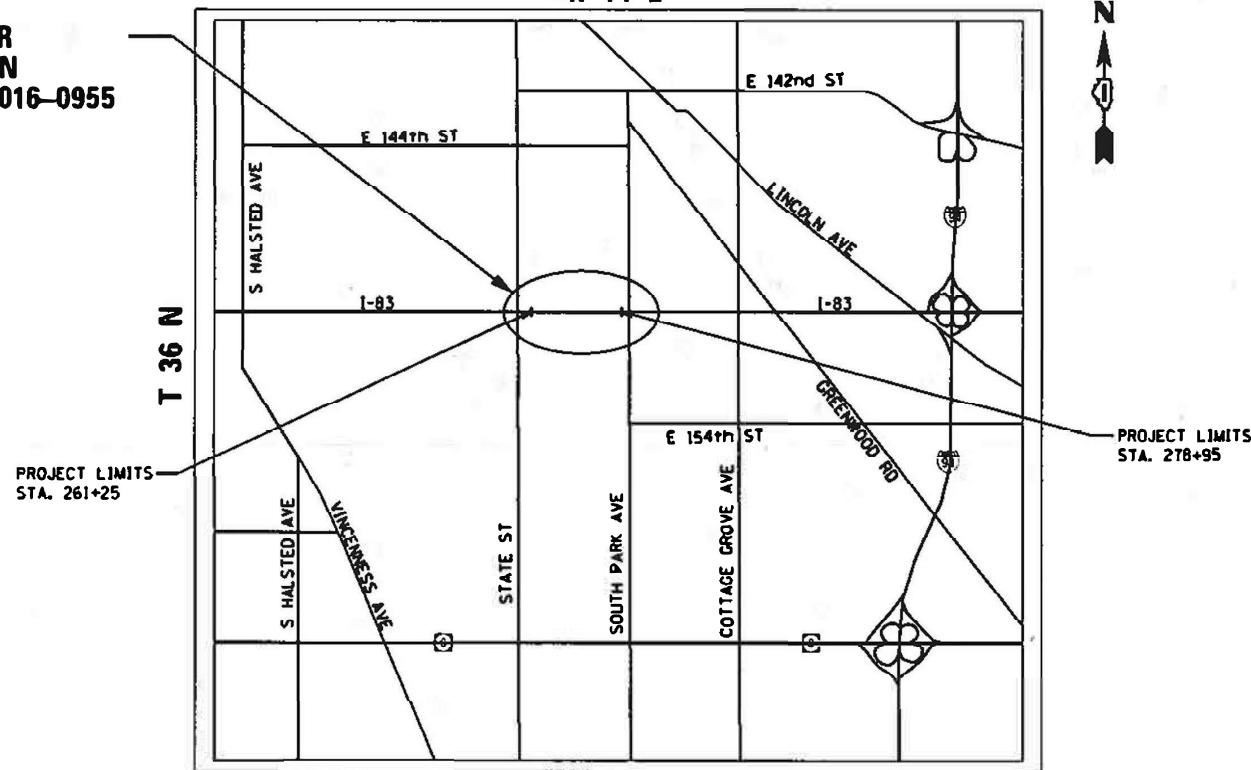
J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS  
1-800-892-0123  
OR 811

PROJECT ENGINEER: PRAVEEN KAINI (847) 705-4237  
PROJECT MANAGER: JEAN ALAIN MIDY (847) 221-3056

CONTRACT NO. 62R75

C-91-231-22

R 14 E



THORNTON TOWNSHIP

PROJECT LENGTH

GROSS LENGTH = 1,770 FT. = 0.32 MILE  
NET LENGTH = 1,770 FT. = 0.32 MILE



LOCATION OF SECTION INDICATED THUS: - [black rectangle symbol]



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED Jan 26, 2024  
Jose Rojas IR  
REGIONAL ENGINEER

March 22, 2024 [Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

March 22, 2024 [Signature]  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

**GENERAL NOTES**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL JULIE AT (800) 892-0123 OR 811 TO DETERMINE WHICH UTILITIES ARE WITHIN THE AREA. 48 HOUR NOTIFICATION IS REQUIRED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AS REQUIRED.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJACENT RESIDENTIAL AREAS.
- THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS WAS DEVELOPED THROUGH AERIAL PHOTOGRAPHY AND IS NOT THE RESULT OF A GROUND SURVEY. THEREFORE, THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS ARE FOR REFERENCE PURPOSES ONLY. THE RELATIVE ACCURACY OF THE INFORMATION IS UNKNOWN AND CANNOT BE GUARANTEED. THE CONTRACTOR MAY BE REQUIRED TO ADJUST LAYOUT TO MATCH ACTUAL FIELD CONDITIONS AND THE INTENT OF THE PLANS.
- THE CONTRACTOR SHALL SWEEP AND CLEAN THE PAVEMENT SURFACE, PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES), ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES AND SHALL BRACE AND SUPPORT THE UTILITIES PROPERLY IN ORDER TO PREVENT SETTLEMENT, DISPLACEMENT, OR DAMAGE TO THE UTILITIES. THE PROTECTION OF THE UTILITIES AS SPECIFIED HEREIN WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT.
- THE CONTRACTOR SHALL CAREFULLY REMOVE OR EXCAVATE NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING DRAINAGE THROUGHOUT THE CONSTRUCTION OF THIS PROJECT. THIS WORK WILL NOT BE PAID FOR SEPARATELY AND SHALL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT.
- NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- RESIDENT ENGINEER SHALL NOTIFY PACE AT [transportation.engineer@pacebus.com](mailto:transportation.engineer@pacebus.com) AT LEAST 2 (TWO) WEEKS IN ADVANCE OF CONSTRUCTION SO THAT PACE CAN NOTIFY RIDERS OF IMPENDING CONGESTION, DELAYS, AND CHANGE IN ROUTING.
- TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS ON IDOT ARTERIAL ROADS, THE ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN AT [Patrice.harris@illinois.gov](mailto:Patrice.harris@illinois.gov).
- THE CONTRACTOR SHALL CONTACT KALPANA KANNAN-HOSADURGA, THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR, AT [Kalpana.Kannan-Hosadurga@illinois.gov](mailto:Kalpana.Kannan-Hosadurga@illinois.gov) A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE CONTRACTOR SHALL REFERENCE 755571 IN THEIR COMMUNICATION WITH THE UPRR WHEN CONSTRUCTION STARTS.

**INDEX OF SHEETS**

1	COVER SHEET
2	GENERAL NOTES, INDEX OF SHEETS, STANDARD DRAWINGS
3 - 5	SUMMARY OF QUANTITIES
6	SCHEDULE OF QUANTITIES
7	ALIGNMENT
8	PAVEMENT PATCHING DETAIL
9	PAVEMENT MARKINGS
10	MOT GENERAL NOTES
11	MOT TYPICAL SECTIONS
12 - 14	MOT STAGING PLANS
15 - 17	DETOUR PLANS
18 - 36	STRUCTURAL PLANS
37 - 44	D1 HIGHWAY STANDARDS

**IDOT STANDARD DRAWINGS**

STANDARD NUMBERS	DESCRIPTION
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-09	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

**DISTRICT 1 STANDARD DRAWINGS**

STANDARD NUMBERS	DESCRIPTION
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAIL
TC-10	TRAFFIC CONTROL AND PROTECTIN FOR SIDE ROAD, INTERSECTIONS, AND DRIVEWAYS
TC-11	RAISED REFLECTIVE PAVEMENT MARKERTS (SNOW PLOW RESISTANT)
TC-13	TYPICAL PAVEMENT MARKINGS
TC-21	DETOUR SIGNING FOR CLOSING STATE HIGHWAYS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES  
INDEX OF SHEETS AND STANDARD DRAWINGS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	2
ILLINOIS			CONTRACT NO. 62R75	
FED. AID PROJECT				

MODEL Path: \\...  
FILE NAME: D:\123122-dht-notes\_01.dgn



USER NAME = Michael.Czerpak	DESIGNED - AMB	REVISED -
	DRAWN - AMB	REVISED -
PLOT SCALE = 40,0000 */ in.	CHECKED - MM	REVISED -
PLOT DATE = 1/26/2024	DATE - 01/26/2024	REVISED -

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				80% FED/20% STATE	
				BRIDGE REHABILITATION	
				0059	URBAN
	SN. 016-0955				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	200		200
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	38		38
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	267		267
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	200.0		200.0
44000600	SIDEWALK REMOVAL	SQ FT	200		200
44004250	PAVED SHOULDER REMOVAL	SQ YD	23		23
48203021	HOT-MIX ASPHALT SHOULDERS, 6"	SQ YD	23		23
50102400	CONCRETE REMOVAL	CU YD	39.9		39.9
50157300	PROTECTIVE SHIELD	SQ YD	2,325		2,325
50300255	CONCRETE SUPERSTRUCTURE	CU YD	35.2		35.2
50300260	BRIDGE DECK GROOVING	SQ YD	2,498		2,498
50300300	PROTECTIVE COAT	SQ YD	3,595.0		3,595.0
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	5,690		5,690
50800515	BAR SPLICERS	EACH	112		112
50800530	MECHANICAL SPLICERS	EACH	32		32
52000110	PREFORMED JOINT STRIP SEAL	FOOT	170.0		170.0
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	16.0		16.0
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	1		1
52100520	ANCHOR BOLTS, 1"	EACH	58		58
58700300	CONCRETE SEALER	SQ FT	638		638
67100100	MOBILIZATION	L SUM	1		1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	294		294
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1		1
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	4,748		4,748
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	14,196		14,196
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,563		1,563
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1,675		1,675

MODEL: D:\64bit  
 FILE NAME: D:\123122-2h-50001.dgn



USER NAME = Michael.Czerpak	DESIGNED -	REVISED -
PLOT SCALE = 2,0000' / in.	DRAWN -	REVISIONS -
PLOT DATE = 2/1/2024	CHECKED -	REVISIONS -
	DATE - 01/26/2024	REVISIONS -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	3
			CONTRACT NO. 62R75	
			ILLINOIS FED. AID PROJECT	





PAY ITEM NUMBER		PAY ITEM NAME					FINAL QUANTITY	38
40604062		HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70					UNIT OF MEASURE TON	
<b>QUANTITY CALCULATIONS</b>								
BEGIN STA.	END STA.	LENGTH (FOOT)	WIDTH (FOOT)	DEPTH (INCHES)	FACTOR (LBS/SYS)	VOLUME (TONS)	QUANTITY	
267+25.60	267+75.60	50	40	1.50	168	19	19	
273+10.60	273+60.60	50	40	1.50	168	19	19	
<b>TOTAL</b>							<b>38</b>	

PAY ITEM NUMBER		PAY ITEM NAME					FINAL QUANTITY	14196
70307120		TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE					UNIT OF MEASURE FOOT	
<b>QUANTITY CALCULATIONS</b>								
LOCATION	STATION	STATION	OFFSET				QUANTITY	
STAGE 1							5063	
STAGE 2							4355	
STAGE 3							4778	
<b>TOTAL</b>							<b>14196</b>	

PAY ITEM NUMBER		PAY ITEM NAME					FINAL QUANTITY	1068
78004620		PERFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 4"					UNIT OF MEASURE FOOT	
<b>QUANTITY CALCULATIONS</b>								
LOCATION	STATION	STATION	OFFSET	LENGTH	LINE TYPE	FACTOR	QUANTITY	
	267+76.00	273+10.00		534.00	DOUBLE CL	2	1068	
<b>TOTAL</b>							<b>1068</b>	

PAY ITEM NUMBER		PAY ITEM NAME					FINAL QUANTITY	3778
78000200		THERMOPLASTIC PAVEMENT MARKING - LINE 4"					UNIT OF MEASURE FOOT	
<b>QUANTITY CALCULATIONS</b>								
LOCATION	STATION	STATION	OFFSET	Length	Line Type	Factor	QUANTITY	
	261+25.00	267+76.00	L	651.00	WHITE 10' DASH, 30' SKIP	0.25	170	
	261+25.00	267+76.00	R	651.00	WHITE 10' DASH, 30' SKIP	0.25	170	
	273+10.00	278+95.00	L	585.00	WHITE 10' DASH, 30' SKIP	0.25	150	
	273+10.00	278+95.00	R	585.00	WHITE 10' DASH, 30' SKIP	0.25	150	
	261+25.00	267+76.00	L	651.00	DOUBLE CL	2	1302	
	261+25.00	263+84.00	R	259.00	DOUBLE CL	2	518	
	273+10.00	278+95.00	L	585.00	DOUBLE CL	2	1170	
	278+21.00	278+95.00	R	74.00	DOUBLE CL	2	148	
<b>TOTAL</b>							<b>3778</b>	

PAY ITEM NUMBER		PAY ITEM NAME					FINAL QUANTITY	280
78004635		PERFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 7"					UNIT OF MEASURE FOOT	
<b>QUANTITY CALCULATIONS</b>								
LOCATION	STATION	STATION	OFFSET	LENGTH	LINE TYPE	FACTOR	QUANTITY	
	267+76.00	273+10.00	L	534.00	WHITE 10' DASH, 30' SKIP	0.25	140	
	267+76.00	273+10.00	R	534.00	WHITE 10' DASH, 30' SKIP	0.25	140	
<b>TOTAL</b>							<b>280</b>	

PAY ITEM NUMBER		PAY ITEM NAME					FINAL QUANTITY	1290
78008210		POLYUREA PAVEMENT MARKING TYPE I - LINE 4"					UNIT OF MEASURE FOOT	
<b>QUANTITY CALCULATIONS</b>								
LOCATION	STATION	STATION	OFFSET	LENGTH	LINE TYPE	FACTOR	QUANTITY	
	267+20.00	273+65.00	L	645.00	EDGE LINE	1	645	
	267+20.00	273+65.00	R	645.00	EDGE LINE	1	645	
<b>TOTAL</b>							<b>1290</b>	

MODEL: D:\p4\h...  
FILE NAME: D:\2\22-2h-arche01.dgn



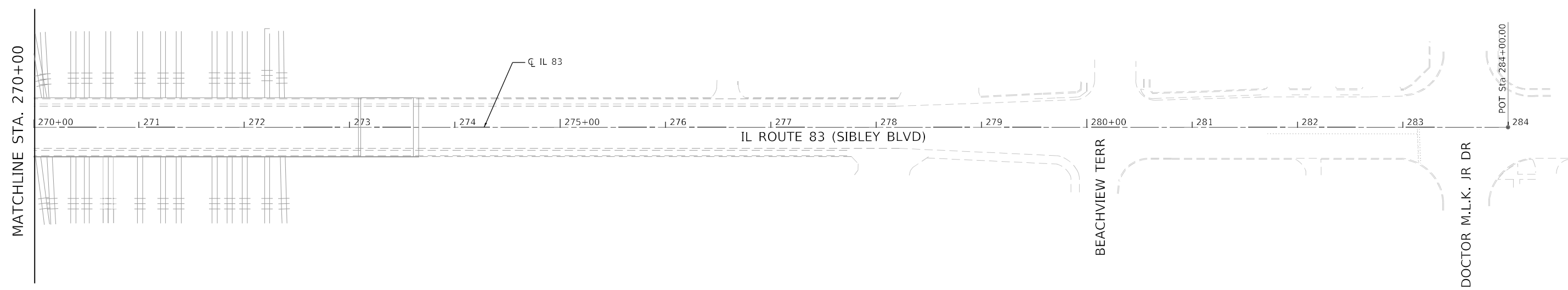
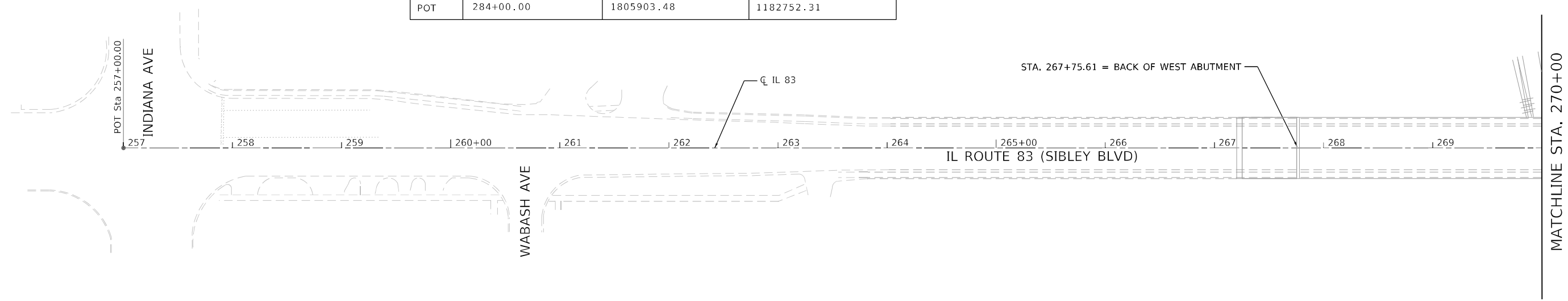
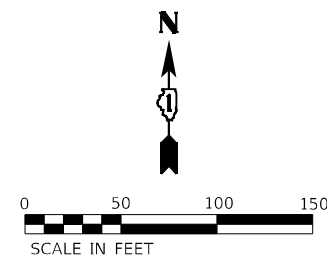
USER NAME = Michael.Czerpak	DESIGNED - MB	REVISED -
	DRAWN - MB	REVISED -
PLOT SCALE = 40,0000 * / in.	CHECKED - AMB	REVISED -
PLOT DATE = 1/26/2024	DATE - 01/26/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>SCHEDULE OF QUANTITIES</b>			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	6
			CONTRACT NO. 62R75	
ILLINOIS FED. AID PROJECT				

ALIGNMENT COORDINATES IL ROUTE 83 (SIBLEY BLVD)			
	STATION	N	E
POT	257+00.00	1805900.82	1180052.31
POT	284+00.00	1805903.48	1182752.31



MODEL: D:\64bit  
FILE NAME: D:\123122-dht-a\brn.dgn



USER NAME = Michael.Czerpak	DESIGNED - JSCHAEFER	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN - LSPARBER	REVISED -
PLOT DATE = 1/26/2024	CHECKED - HREGAN	REVISED -
	DATE - 1/26/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (SIBLEY BLVD) OVER UPRR  
ALIGNMENT, TIES, AND BENCHMARKS**

SCALE: 1"=50'    SHEET 1 OF 1 SHEETS    STA. 257+00 TO STA. 284+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	7
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				





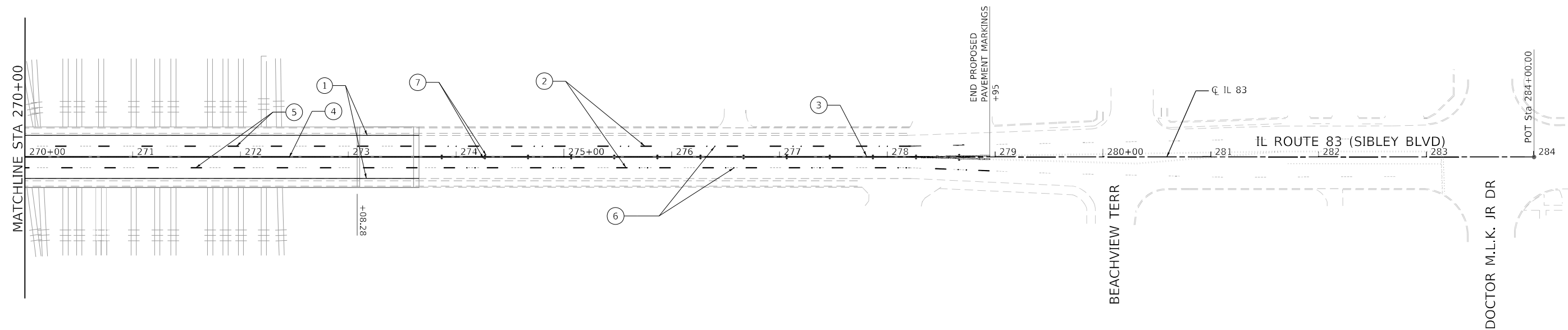
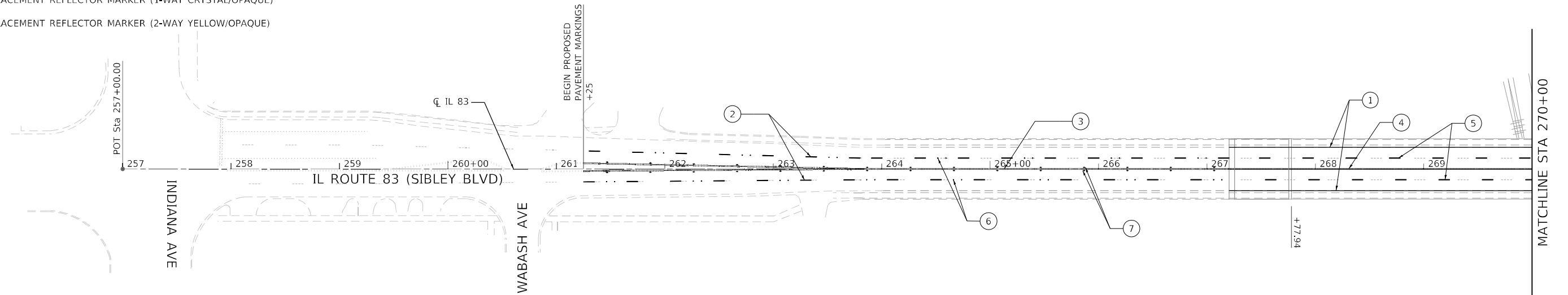


**LEGEND:**

- ① POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" (WHITE EDGE LINES)
- ② THERMOPLASTIC PAVEMENT MARKING, LINE 4" (WHITE 10' DASH, 30' SKIP)
- ③ THERMOPLASTIC PAVEMENT MARKING, DOUBLE, LINE 4" (YELLOW SOLID 2 @ 11")
- ④ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, STANDARD, LINE 4" (YELLOW SOLID 2 @ 11")
- ⑤ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, STANDARD, LINE 7" (10' DASH, 30' SKIP) (1.5" BLACK, 4" WHITE, 1.5" BLACK)
- ⑥ REPLACEMENT REFLECTOR MARKER (1-WAY CRYSTAL/OPAQUE)
- ⑦ REPLACEMENT REFLECTOR MARKER (2-WAY YELLOW/OPAQUE)

**NOTES**

- 1. PREFORMED PLASTIC MARKINGS, TYPE D AND GROOVING FOR RECESSED PAVEMENT MARKINGS SHALL BE USED FOR LANE LINES AND CENTER LINE PAVEMENT MARKINGS WITHIN BRIDGE LIMITS.
- 2. ALL RAISED REFLECTIVE PAVEMENT MARKERS WITHIN BRIDGE LIMITS SHALL BE REMOVED AND NOT REPLACED



MODEL: D:\64\h...  
FILE NAME: D:\2322-2b-rmk



USER NAME = Michael.Czerpak	DESIGNED - MFINDER	REVISED -
	DRAWN - MFINDER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 1/26/2024	DATE - 1/26/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**IL ROUTE 83 (SIBLEY BLVD) OVER UPRR  
PROPOSED PAVEMENT MARKINGS**

SCALE: 1"=50'    SHEET 1 OF 1 SHEETS    STA. 257+00 TO STA. 284+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	9
CONTRACT NO. 62R75			ILLINOIS FED. AID PROJECT	

## MAINTENANCE OF TRAFFIC GENERAL NOTES

1. ALL TRAFFIC CONTROL SIGNS AND DEVICES, WORK ZONE PAVEMENT MARKING, TEMPORARY CONCRETE BARRIERS AND ATTENUATORS NECESSARY FOR THE MAINTENANCE OF TRAFFIC, AS SHOWN ON THE PLANS, SHALL BE IN PLACE BEFORE THE START OF ANY CONSTRUCTION STAGE.
2. ALL CONSTRUCTION WARNING SIGNS SHALL BE BLACK LEGEND ON ORANGE BACKGROUND. ALL CONSTRUCTION WARNING SIGNS SHALL BE 48" X 48".
3. ALL "ROAD CONSTRUCTION AHEAD" WARNING SIGNS SHALL BE EQUIPPED WITH TYPE A MONO-DIRECTIONAL FLASHING LIGHT.
4. EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE MARKERS THAT CONFLICT WITH TEMPORARY MARKINGS SHALL BE REMOVED. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REPLACED DURING THE PLACEMENT OF THE FINAL PAVEMENT MARKINGS.
5. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES AND NIGHTTIME DETOUR EVENTS ON EACH APPROACH OF THE AFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING, AND/OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.
6. ACCESS TO PACE BUS STOP WEST OF BEACHVIEW TERRACE (STOP ID: 2224) SHALL BE MAINTAINED DURING CONSTRUCTION.

## SUGGESTED STAGING AND TRAFFIC CONTROL

### PRESTAGE

INSTALL TEMPORARY SIGNING AND OTHER TRAFFIC DEVICES NECESSARY FOR PRE-STAGE CONFIGURATION. INCLUDING ANY NECESSARY MODIFICATIONS TO THE EXISTING GROUND MOUNTED AND OVERHEAD SIGNS.

#### TRAFFIC CONTROL:

1. DETOUR TRAFFIC USING DETOUR PLAN SHEETS NO. 15-17.
2. HOURS OF DETOUR SHALL BE LIMITED TO THOSE SHOWN IN THE SPECIAL PROVISIONS "TRAFFIC DETOUR FOR NIGHTTIME BRIDGE CLOSURE".

#### CONSTRUCTION

1. JACK AND CRIB SUPERSTRUCTURE FOR BEARING REPLACEMENT

### STAGE 1

INSTALL TEMPORARY SIGNING, STRIPING AND OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE STAGE 1 TRAFFIC CONFIGURATION, INCLUDING ANY NECESSARY MODIFICATIONS TO THE EXISTING GROUND MOUNTED AND OVERHEAD SIGNS.

#### TRAFFIC CONTROL:

1. TRAFFIC WILL BE REDUCED TO ONE EASTBOUND LANE AND ONE WESTBOUND LANE.
2. THE EASTBOUND LANE AND THE WESTBOUND LANE WILL BE MAINTAINED THROUGHOUT THE CONTRACT LIMITS WITH A SHIFT TO THE EASTBOUND LANES OCCURING AT THE BRIDGE STRUCTURE TO ACCOMMODATE THE WORK ZONES.

#### CONSTRUCTION

1. REPAIR BRIDGE DECK.
2. REPAIR APPROACH SLAB.
3. RECONSTRUCT DECK JOINTS AT EACH ABUTMENT WITH PREFORMED JOINT STRIP SEAL.
4. REPAIR PARAPET WITH FORMED CONCRETE REPAIR.
5. CLEAN AND RESEAL RELIEF JOINTS AT THE END OF APPROACH PAVEMENT CONNECTORS.
6. APPLY CONCRETE SEALER TO PARAPETS, AND ABUTMENT SEATS AND BACKWALLS.
7. INSTALL HMA OVERLAY OVER APPROACH PAVEMENT.

### STAGE 2

INSTALL TEMPORARY SIGNING, STRIPING AND OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE STAGE 2 TRAFFIC CONFIGURATION, INCLUDING ANY NECESSARY MODIFICATIONS TO THE EXISTING GROUND MOUNTED AND OVERHEAD SIGNS.

#### TRAFFIC CONTROL:

1. TRAFFIC WILL REMAIN AS ONE EASTBOUND LANE AND ONE WESTBOUND LANE.
2. THE EXISTING EASTBOUND LANE WILL BE USED. THE WESTBOUND LANE CONSTRUCTED IN STAGE 1 WILL BE USED.

#### CONSTRUCTION

1. REPAIR BRIDGE DECK.
2. REPAIR APPROACH SLAB.
3. RECONSTRUCT DECK JOINTS AT EACH ABUTMENT WITH PREFORMED JOINT STRIP SEAL.
4. REPAIR PARAPET WITH FORMED CONCRETE REPAIR.
5. CLEAN AND RESEAL RELIEF JOINTS AT THE END OF APPROACH PAVEMENT CONNECTORS.
6. APPLY CONCRETE SEALER TO PARAPETS, AND ABUTMENT SEATS AND BACKWALLS.
7. INSTALL HMA OVERLAY OVER APPROACH PAVEMENT.

### STAGE 3

INSTALL TEMPORARY SIGNING, STRIPING AND OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE STAGE 2 TRAFFIC CONFIGURATION, INCLUDING ANY NECESSARY MODIFICATIONS TO THE EXISTING GROUND MOUNTED AND OVERHEAD SIGNS.

#### TRAFFIC CONTROL:

1. TRAFFIC WILL REMAIN AS ONE EASTBOUND LANE AND ONE WESTBOUND LANE.
2. ONE EASTBOUND LANE AND ONE WESTBOUND LANE WILL BE MAINTAINED THROUGHOUT THE CONTRACT LIMITS WITH SHIFTS TO THE WESTBOUND LANES PAVED IN STAGE 1 OCCURING AT THE BRIDGE STRUCTURE TO ACCOMMODATE THE WORK ZONES.

#### CONSTRUCTION

1. REPAIR BRIDGE DECK.
2. REPAIR APPROACH SLAB.
3. RECONSTRUCT DECK JOINTS AT EACH ABUTMENT WITH PREFORMED JOINT STRIP SEAL.
4. REPAIR PARAPET WITH FORMED CONCRETE REPAIR.
5. CLEAN AND RESEAL RELIEF JOINTS AT THE END OF APPROACH PAVEMENT CONNECTORS.
6. APPLY CONCRETE SEALER TO PARAPETS, AND ABUTMENT SEATS AND BACKWALLS.
7. INSTALL HMA OVERLAY OVER APPROACH PAVEMENT
8. PLACE PERMANENT PAVEMENT MARKINGS.

MODEL Path: h  
FILE Name: D:\123122-sh-rtb-jhb-ghp-ghenotes.dgn



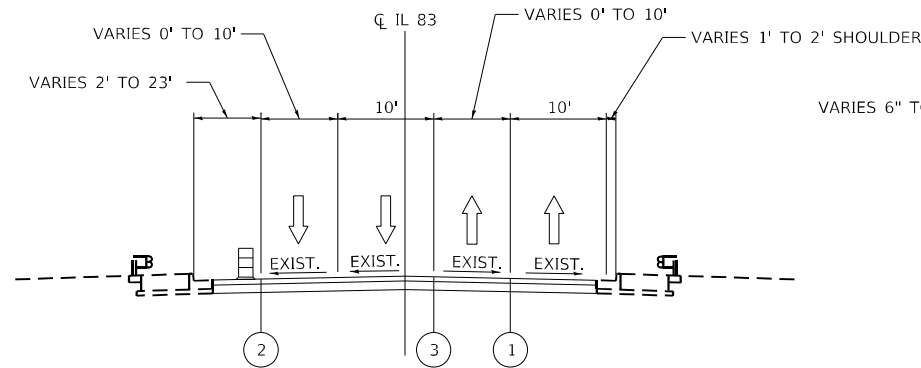
USER NAME = Michael.Czerpak	DESIGNED - JSCHAEFER	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 2,000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 1/26/2024	DATE - 1/26/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

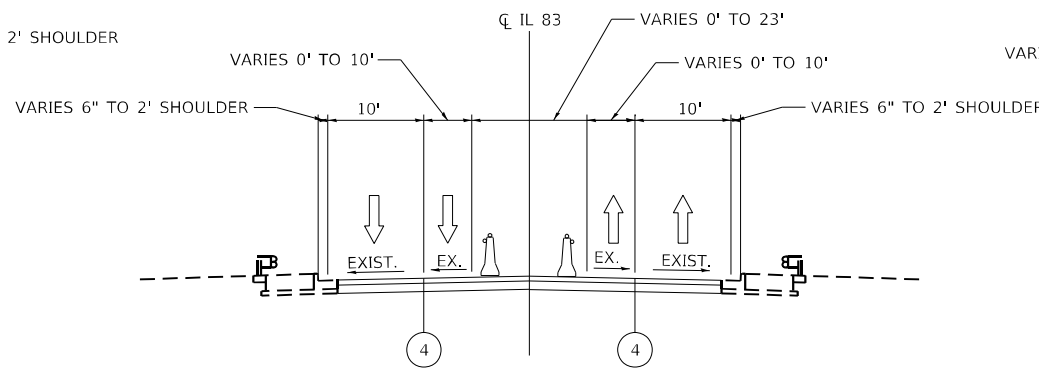
**MAINTENANCE OF TRAFFICE  
GENERAL NOTES AND SEQUENCE OF STAGING**

SCALE: SHEET OF SHEETS STA. TO STA.

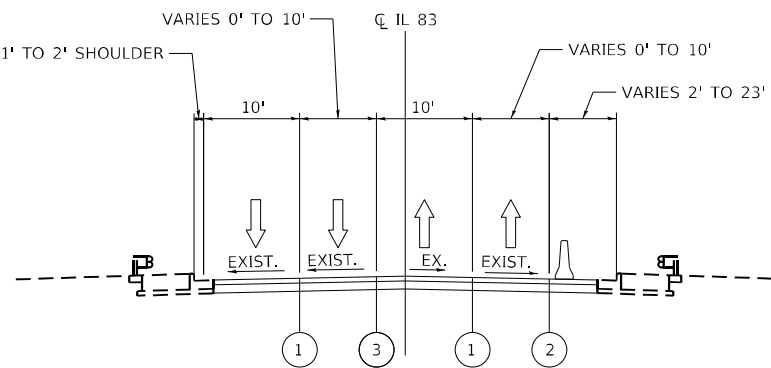
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	10
			CONTRACT NO. 62R75	
		ILLINOIS	FED. AID PROJECT	



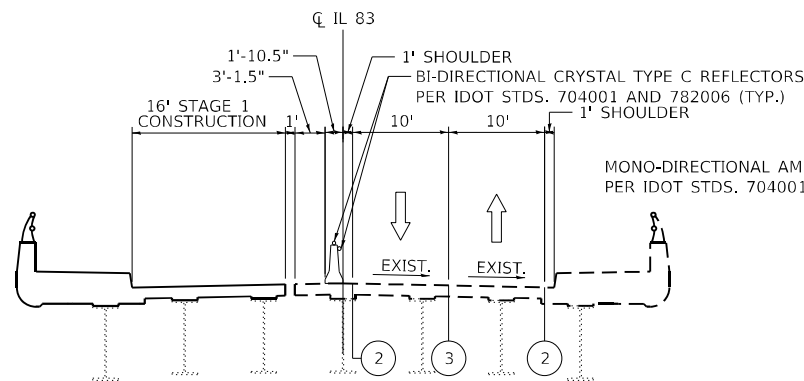
STAGE 1  
IL 83 WEST APPROACH



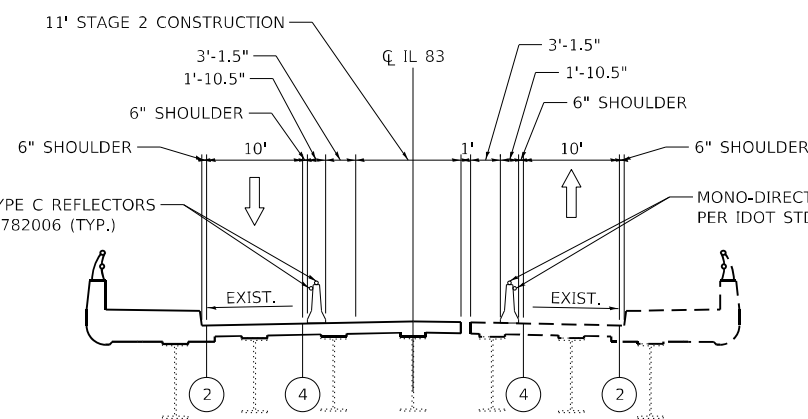
STAGE 2  
IL 83 WEST APPROACH



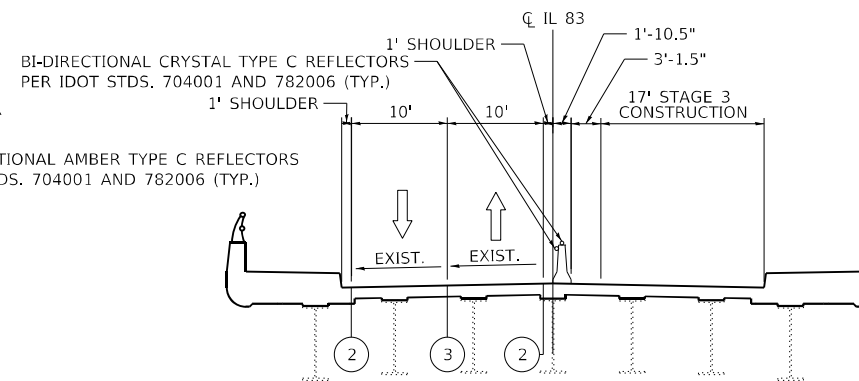
STAGE 3  
IL 83 WEST APPROACH



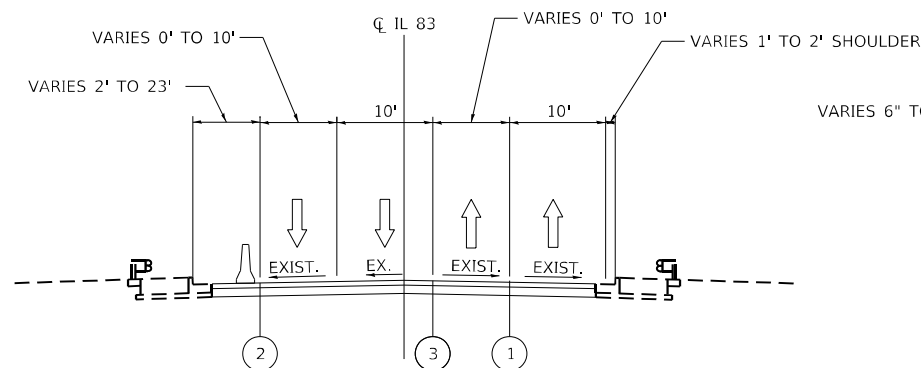
STAGE 1  
IL 83 BRIDGE



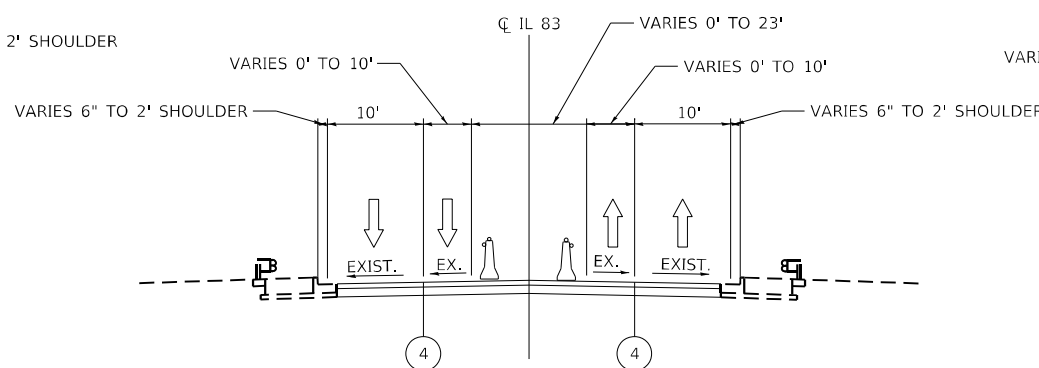
STAGE 2  
IL 83 BRIDGE



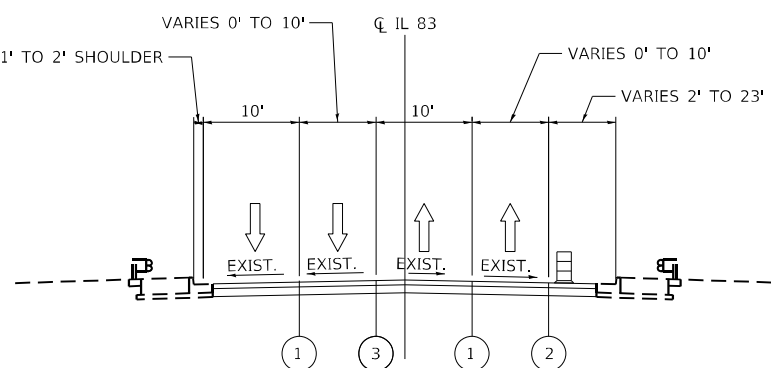
STAGE 3  
IL 83 BRIDGE



STAGE 1  
IL 83 EAST APPROACH



STAGE 2  
IL 83 EAST APPROACH



STAGE 3  
IL 83 EAST APPROACH

LEGEND

- ① EXISTING PAVEMENT MARKING
- ② TEMPORARY PAVEMENT MARKING, TYPE IV TAPE, 4 INCH (WHITE)
- ③ TEMPORARY PAVEMENT MARKING, LINE 4" - TYPE IV TAPE (DOUBLE YELLOW @ 11" C-C)
- ④ TEMPORARY PAVEMENT MARKING, TYPE IV TAPE, 4 INCH (YELLOW)
- ▭ TEMPORARY CONCRETE BARRIER
- ▭ DRUM OR BARRICADE

NOTES

1. SEE MAINTENANCE OF TRAFFIC STAGING PLANS FOR LIMITS OF TEMPORARY CONCRETE BARRIER.
2. TYPE C REFLECTORS SHALL BE CONSIDERED INCLUDED IN THE COST OF TEMPORARY CONCRETE BARRIER.

MODEL: D:\hntb\... FILE NAME: D:\123122-2b-4\123122.dgn



USER NAME = Michael.Czerpak	DESIGNED - JSCHAEFER	REVISED -
	DRAWN - JSCHAEFER	REVISED -
PLOT SCALE = 20,0000 * / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 1/26/2024	DATE - 1/26/2024	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

IL ROUTE 83 (SIBLEY BLVD) OVER UPRR  
MAINTENANCE OF TRAFFIC - IL 83 TYPICAL SECTIONS

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	11
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				

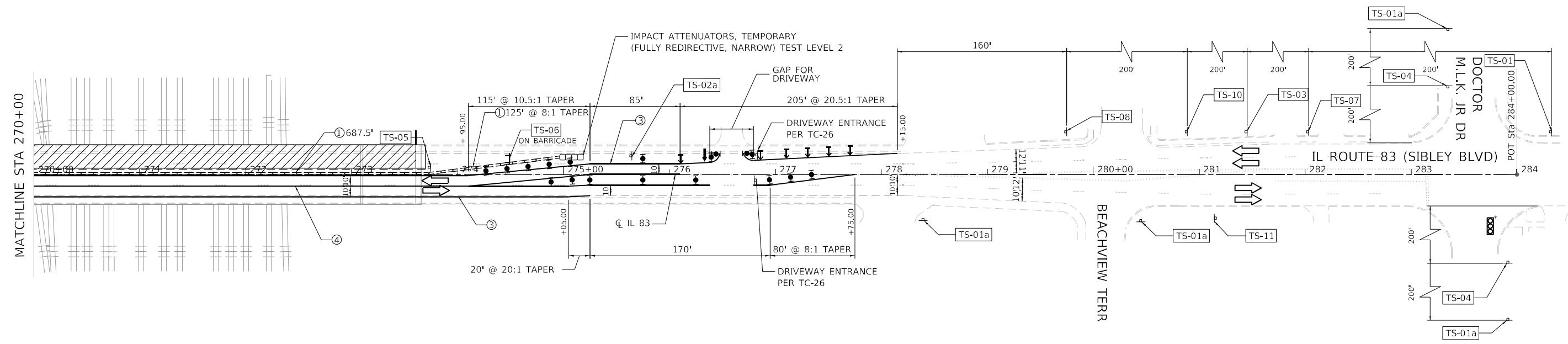
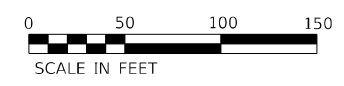
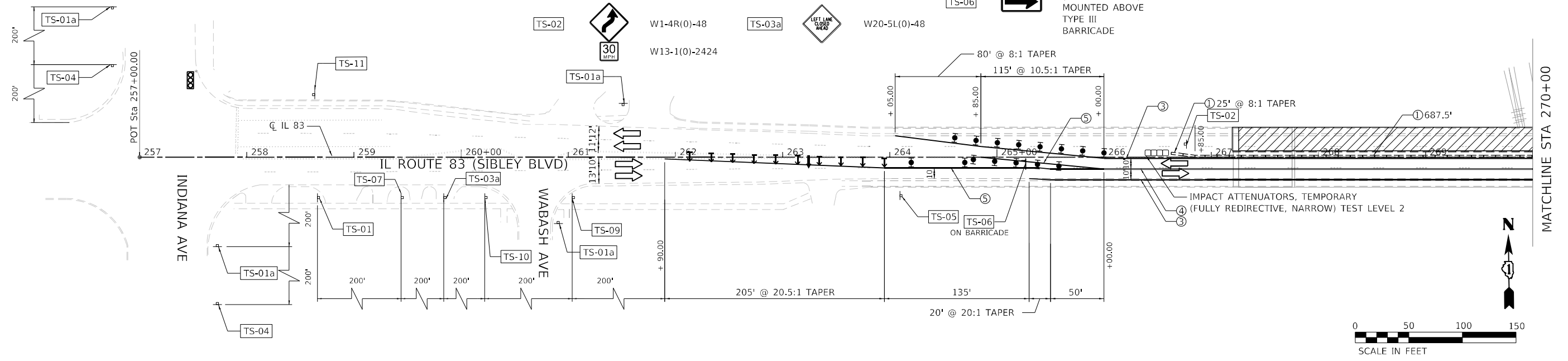
**LEGEND**

- ➔ DIRECTION OF TRAFFIC
- ▨ WORKZONE
- ⊥ TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY IMPACT ATTENUATOR
- ➔ ARROW BOARD
- ⊥ DRUM OR TYPE I OR TYPE II BARRICADE (@ 50' C-C TANGENT, @ 20' C-C TAPER, @ 10' C-C RADII)
- ➔ DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT (@ 20' C-C)

- ① TEMPORARY CONCRETE BARRIER
- ② RELOCATE TEMPORARY CONCRETE BARRIER
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (WHITE)
- ④ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOUBLE YELLOW @ 11 C-C)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (YELLOW)
- ⊥ SIGNALIZED INTERSECTION \*TEMPORARY TRAFFIC SIGNAL TIMING REQUIRED

**MAINTENANCE OF TRAFFIC SIGNS**

- |        |  |  |        |  |               |       |  |  |       |  |   |       |  |  |
|--------|--|--|--------|--|---------------|-------|--|--|-------|--|---|-------|--|--|
| TS-01  |  | W20-1103(0)-48<br>W/ TYPE A<br>FLASHING LIGHT                                    | TS-02a |  | W1-4L(0)-48   | TS-04 |  | W12-1102(0)-48<br>W/ TYPE A<br>FLASHING LIGHT<br>M6-1(0)-2115<br>(LEFT OR RIGHT) | TS-07 |  | W12-1102(0)-48<br>W/ TYPE A<br>FLASHING LIGHT | TS-10 |  | W21-1115(0)-3611<br>R2-1-3648<br>R10-1108p-3618<br>R2-1106p-3618 |
| TS-01a |  | W20-1103(0)-48<br>W/ TYPE A<br>FLASHING LIGHT<br>M6-1(0)-2115<br>(LEFT OR RIGHT) | TS-02  |  | W13-1(0)-2424 | TS-05 |  | W6-3(0)-48   | TS-08 |  | W4-2R(0)-48                                   | TS-09 |  | W4-2L(0)-48  |
| TS-02  |  | W1-4R(0)-48  | TS-03  |  | W20-5R(0)-48  | TS-06 |  | W1-6(0)-6030<br>(LEFT OR RIGHT)<br>MOUNTED ABOVE<br>TYPE III<br>BARRICADE        | TS-11 |  | G20-1103-6036                                 |       |  |  |
| TS-01a |  | W13-1(0)-2424  | TS-03a |  | W20-5L(0)-48  |       |  |  |       |  |   |       |  |  |



MODEL: D:\p4\hntb\1122\1122-2b-rtb\1122.dwg



USER NAME = Michael.Czerpak	DESIGNED - JSCHAEFER	REVISED -
DRAWN - LSPARBER	REVISIONS -	
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISIONS -
PLOT DATE = 1/26/2024	DATE - 1/26/2024	REVISIONS -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC PLAN - STAGE 1</b>			
SCALE: 1"=50'	SHEET 1	OF 3 SHEETS	STA. 257+00.00 TO STA. 284+00.00

F.A.P. RTE. 397	SECTION FAP 397 22 BRIDGE REHAB	COUNTY COOK	TOTAL SHEETS 44	SHEET NO. 12
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R75	

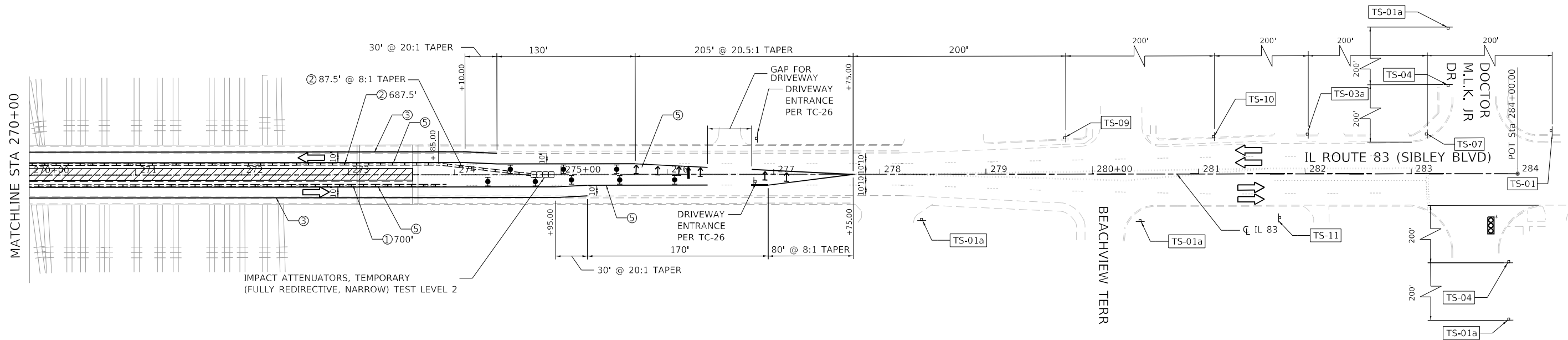
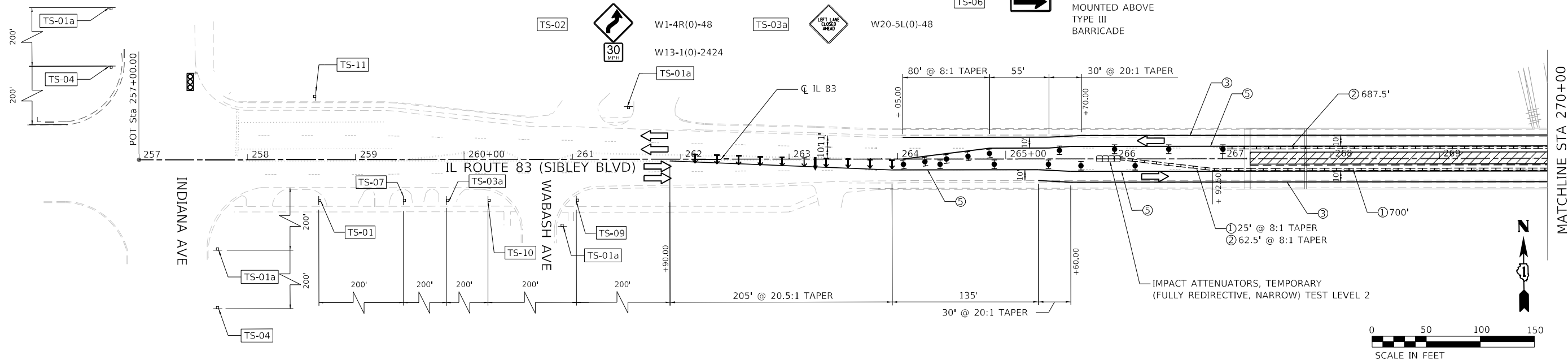
**LEGEND**

- ➔ DIRECTION OF TRAFFIC
- ▨ WORKZONE
- ⊥ TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY IMPACT ATTENUATOR
- ➔ ARROW BOARD
- ⊙ DRUM OR TYPE I OR TYPE II BARRICADE (@ 50' C-C TANGENT, @ 20' C-C TAPER, @ 10' C-C RADII)
- ➔ DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT (@ 20' C-C)

- ① TEMPORARY CONCRETE BARRIER
- ② RELOCATE TEMPORARY CONCRETE BARRIER
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (WHITE)
- ④ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOUBLE YELLOW @ 11 C-C)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (YELLOW)
- ⊙ SIGNALIZED INTERSECTION \*TEMPORARY TRAFFIC SIGNAL TIMING REQUIRED

**MAINTENANCE OF TRAFFIC SIGNS**

- TS-01 W20-1103(0)-48 W/ TYPE A FLASHING LIGHT
- TS-01a W20-1103(0)-48 W/ TYPE A FLASHING LIGHT M6-1(0)-2115 (LEFT OR RIGHT)
- TS-02 W1-4R(0)-48
- TS-02a W1-4L(0)-48
- TS-03 W20-5R(0)-48
- TS-03a W20-5L(0)-48
- TS-04 W12-1102(0)-48 W/ TYPE A FLASHING LIGHT M6-1(0)-2115 (LEFT OR RIGHT)
- TS-05 W6-3(0)-48
- TS-06 W1-6(0)-6030 (LEFT OR RIGHT) MOUNTED ABOVE TYPE III BARRICADE
- TS-07 W12-1102(0)-48 W/ TYPE A FLASHING LIGHT
- TS-08 W4-2R(0)-48
- TS-09 W4-2L(0)-48
- TS-10 W21-1115(0)-3618 R2-1-3648 R10-1108p-3618 R2-1106p-3618
- TS-11 G20-1103-6036



NOTES:  
 DRUMS, BARRICADES AND DIRECTION INDICATOR BARRICADES SHALL BE SPACED AT 20' CENTERS ALONG TAPERS AND 50' CENTERS ALONG TANGENTS.

MODEL: D:\64bit\...  
 FILE NAME: D:\123122-2h-rtb3\m02



USER NAME = lsparber	DESIGNED - JSCHAEFER	REVISED -
	DRAWN - LSPARBER	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - HREGAN	REVISED -
PLOT DATE = 1/26/2024	DATE - 1/26/2024	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>MAINTENANCE OF TRAFFIC          PLAN - STAGE 2</b>			
SCALE: 1"=50'	SHEET 2	OF 3 SHEETS	STA. 257+00.00 TO STA. 284+00.00

F.A.P. RTE. 397	SECTION FAP 397 22 BRIDGE REHAB	COUNTY COOK	TOTAL SHEETS 44	SHEET NO. 13
CONTRACT NO. 62R75				ILLINOIS FED. AID PROJECT

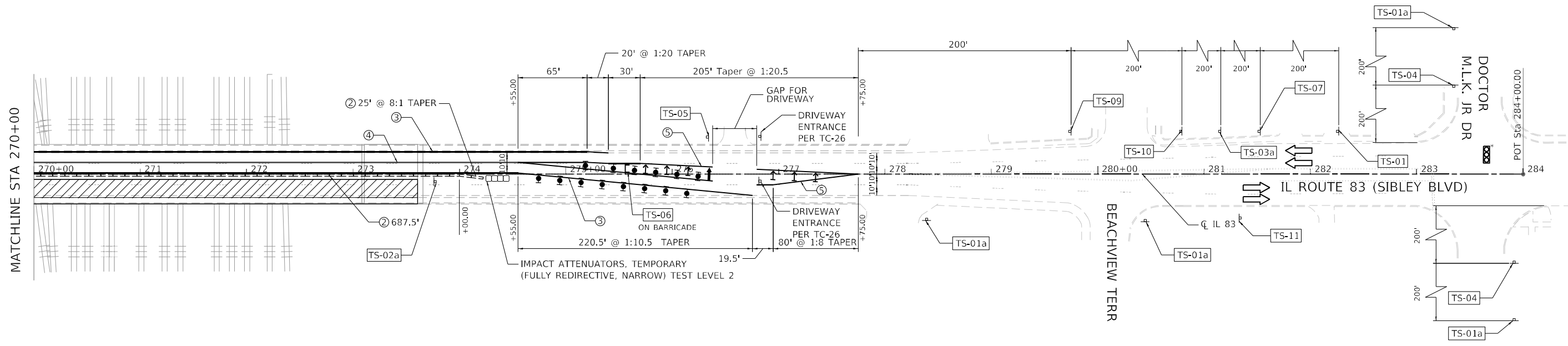
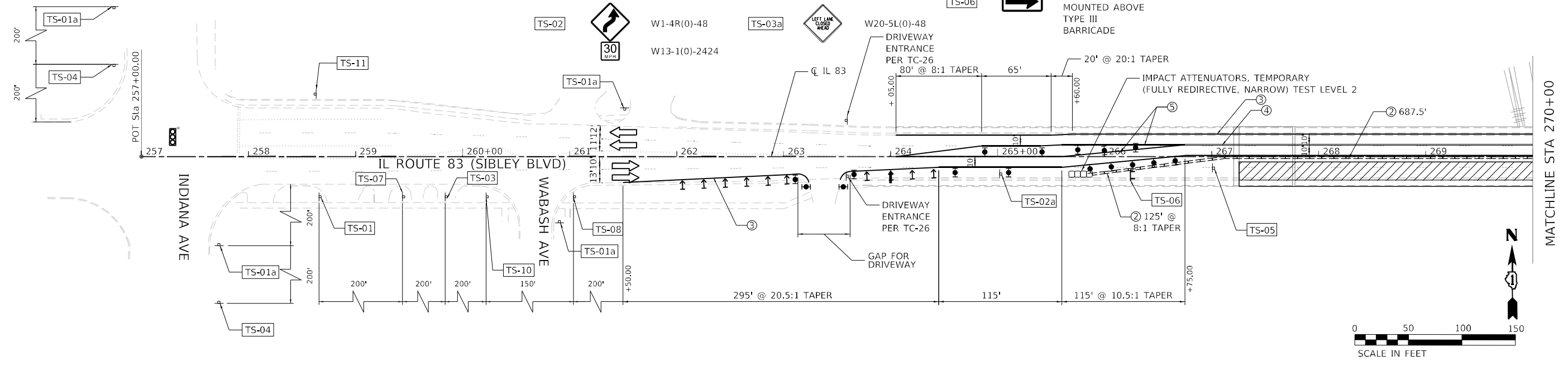
**LEGEND**

- ➔ DIRECTION OF TRAFFIC
- ▨ WORKZONE
- ⊥ TYPE III BARRICADE WITH FLASHING LIGHTS
- TEMPORARY IMPACT ATTENUATOR
- ➔ ARROW BOARD
- ⊥ DRUM OR TYPE I OR TYPE II BARRICADE (@ 50' C-C TANGENT, @ 20' C-C TAPER, @ 10' C-C RADIUS)
- ➔ DIRECTION INDICATOR BARRICADE W/ STEADY BURN LIGHT (@ 20' C-C)

- ① TEMPORARY CONCRETE BARRIER
- ② RELOCATE TEMPORARY CONCRETE BARRIER
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (WHITE)
- ④ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (DOUBLE YELLOW @ 11 C-C)
- ⑤ TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE (YELLOW)
- ⓧ SIGNALIZED INTERSECTION \*TEMPORARY TRAFFIC SIGNAL TIMING REQUIRED

**MAINTENANCE OF TRAFFIC SIGNS**

- TS-01 W20-1103(0)-48 W/ TYPE A FLASHING LIGHT
- TS-01a W20-1103(0)-48 W/ TYPE A FLASHING LIGHT M6-1(0)-2115 (LEFT OR RIGHT)
- TS-02 W1-4R(0)-48
- TS-02a W1-4L(0)-48
- TS-03 W13-1(0)-2424
- TS-03a W13-1(0)-2424
- TS-04 W12-1102(0)-48 W/ TYPE A FLASHING LIGHT M6-1(0)-2115 (LEFT OR RIGHT)
- TS-05 W6-3(0)-48
- TS-06 W1-6(0)-6030 (LEFT OR RIGHT) MOUNTED ABOVE TYPE III BARRICADE
- TS-07 W12-1102(0)-48 W/ TYPE A FLASHING LIGHT
- TS-08 W4-2R(0)-48
- TS-09 W4-2L(0)-48
- TS-10 R2-1-3648
- TS-11 G20-1103-6036



NOTES:  
 DRUMS, BARRICADES AND DIRECTION INDICATOR BARRICADES SHALL BE SPACED AT 20' CENTERS ALONG TAPERS AND 50' CENTERS ALONG TANGENTS.

MODEL: Default  
 FILE NAME: D:\13122-ent-est\mtr03



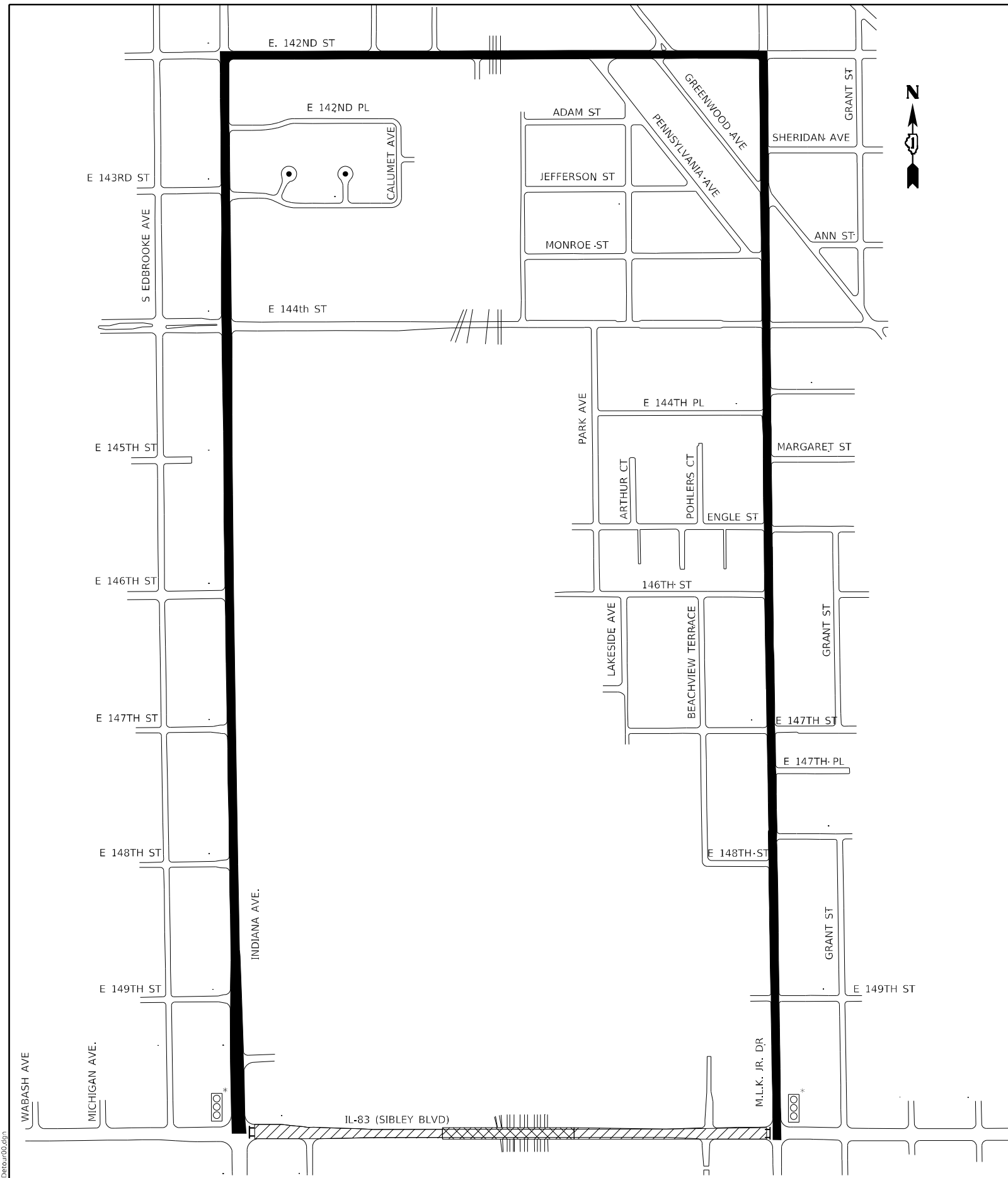
USER NAME = lsparber	DESIGNED - JSCHAEFER	REVISED -
PLOT SCALE = 100.1522' / in.	DRAWN - LSPARBER	REVISED -
PLOT DATE = 1/26/2024	CHECKED - HREGAN	REVISED -
	DATE - 1/26/2024	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC  
 PLAN - STAGE 3**

SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. 257+00.00 TO STA. 284+00.00

F.A.P. RTE. 397	SECTION FAP 397 22 BRIDGE REHAB	COUNTY COOK	TOTAL SHEETS 44	SHEET NO. 14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R75	



**GENERAL NOTES**

- SIGN POSTS SHALL BE INSTALLED AT LEAST TWELVE (12) FEET FROM THE EDGE OF THE MAINLINE ROADWAY (WHITE PAINTED EDGELINE)
- DETOUR SIGN INSTALLATION CONTRACTOR SHALL CALL JULIE FOR UTILITY LOCATES. CONTACT JULIE AT (800) 892-0123.
- FOLLOW DISTRICT ONE DETAIL TC 21 FOR ADDITIONAL INFORMATION.
- DETOUR SIGNING ARE SUPPOSED TO BE ONLY DURING PERIODS WHEN DETOUR IS IN EFFECT. ALL CONFLICTING SIGNING SHALL BE COVERED.
- THIS DETOUR AND ALL DETOUR SIGNS SHOWN HERE, WILL BE PAID AS "TRAFFIC CONTROL AND PROTECTION, (SPECIAL)" UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL DEVELOP AND MAINTAIN APPROPRIATE SIGNAL TIMINGS FOR THE FOLLOWING INTERSECTIONS, OR AS DIRECTED BY THE ENGINEER:  
 A) IL 83 / M.L.K. JR. DR  
 B) IL 83 / INDIANA AVE

**LEGEND**

① M4-8-2412	DETOUR		
② M1-50-2424	IL 83	⑩ W20-2-4848	DETOUR AHEAD
③ M3-1-2412	EAST	⑪ W20-3-4848	ROAD CLOSED AHEAD
④ M3-3-2412	WEST	⑫ W20-3-4848	ROAD CLOSED 500 FT
⑤ M5-1L-2115	↙	⑬ M4-8A-2418	END DETOUR
⑥ M5-1R-2115	↘		
⑦ M6-1-2115	←		
⑧ M6-1-2115	→		
⑨ M6-3-2115	↑		
		⑭ R11-2-4830	ROAD CLOSED
		⑮ M4-10L-4818	← DETOUR
		⑯ M4-10R-4818	DETOUR →
		⑰ R11-4-6030	ROAD CLOSED TO THRU TRAFFIC

DETOUR ROUTE

PARTIALLY CLOSED PORTION

COMPLETELY CLOSED PORTION

TYPE III BARRICADE

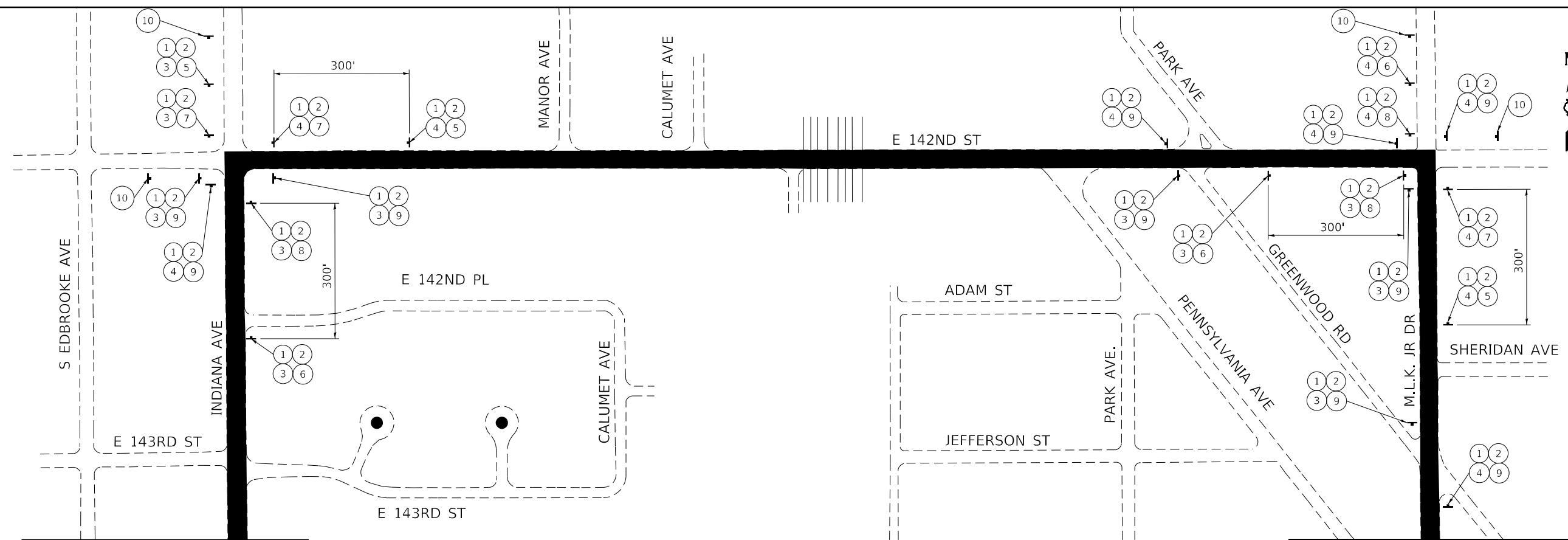
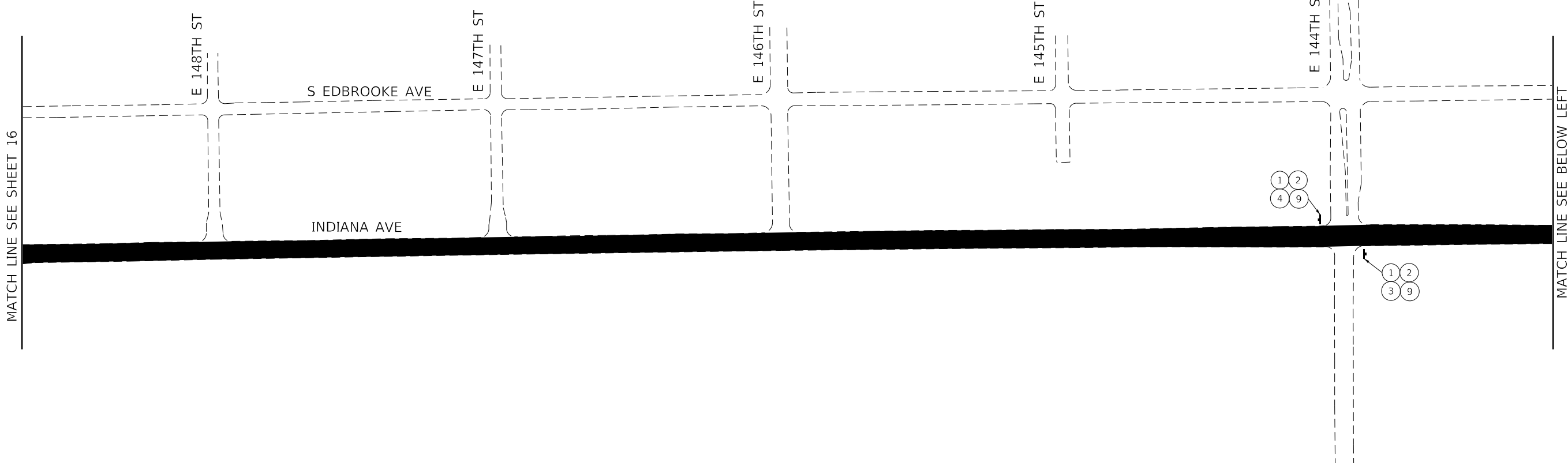
SIGNALIZED INTERSECTION

\* TEMPORARY TRAFFIC SIGNAL TIMING REQUIRED

**ROUTE OVERVIEW**







MODEL: D:\p4\h\p  
 FILE NAME: D:\123122-2h-Detour.dgn



USER NAME = Michael.Czerpak	DESIGNED - JJM	REVISED -
	DRAWN - JJM	REVISED -
PLOT SCALE = 1600,0000' / in.	CHECKED - AMB	REVISED -
PLOT DATE = 1/26/2024	DATE - 01/26/2024	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>IL ROUTE 83 (SIBLEY BLVD) OVER UPRR</b> <b>PROPOSED DETOUR &amp; SIGNING</b>			
SCALE: N.T.S.	SHEET 3	OF 3 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	17
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				

**Existing Structure:**  
 The bridge is an eleven-span, four unit continuous steel structure measuring 535'-0" Back to back abutments and 56'-0" Out to Out width. It has a 7.5-inch reinforced concrete deck and concrete multi-column piers. The original cast-in-place concrete beam bridge was built in 1936 as Section 1010-Vb-BR(82). In 1984, the bridge was rehabilitated with a new superstructure and new bearings, and the concrete bearing seats were reconstructed. In 2011 concrete repairs were made to the abutments and piers.

Stage construction shall be utilized to maintain traffic during construction.

No salvage

**DESIGN SPECIFICATIONS**

2020 AASHTO LRFD Bridge Design Specifications, 9th Edition  
 2002 AASHTO Standard Specifications

**DESIGN STRESSES**

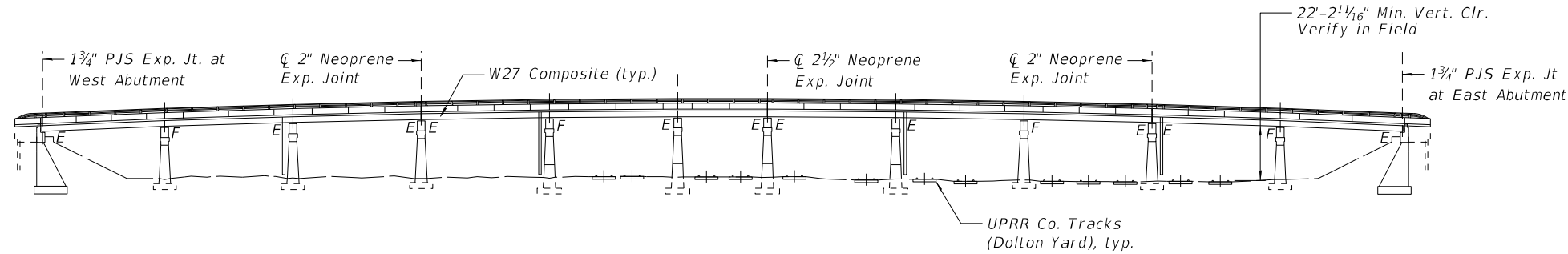
$f'_c = 3,500$  psi  
 $f_y = 60,000$  psi

Structural Steel Plate:  
 $f_y = 50,000$  psi

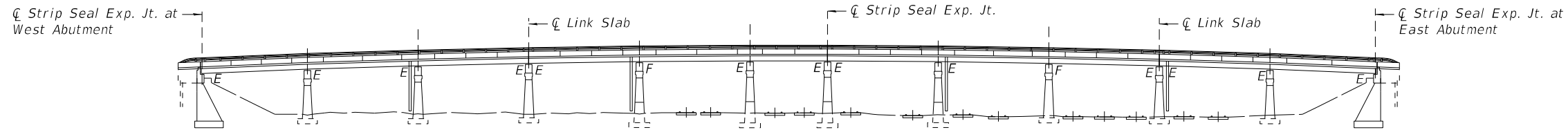
UHPC Stresses:  
 $f'_c = 4,000$  psi (Superstructure)  
 $f_y = 60,000$  psi  
 $f'_c = 21,000$  psi (Concrete Structures (special))

**SCOPE OF WORK**

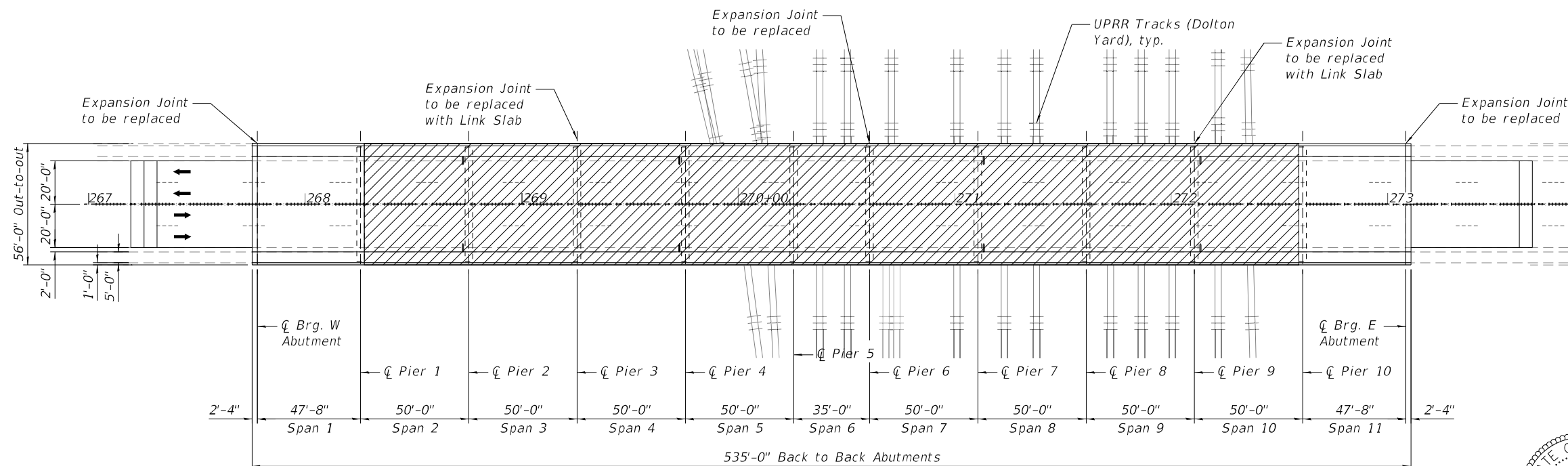
1. Install Protective Shield.
2. Replace elastomeric bearings (3 each), 1 at Pier 3, 2 at Pier 6, replace fixed bearings with elastomeric bearings at Piers 1 & 10, and replace elastomeric bearings at East & West Abutments.
3. Install Temporary Links.
4. Replace joints at Pier 3 & Pier 9 with Link Slab.
5. Replace strip joints at Abutments & replace Neoprene Jt. at Pier 6.
6. Scarify Deck.
7. Deck slab full depth repair & apply new deck overlay.
8. Drain scupper adjustment.
9. Repair handrails & posts.
10. Structural repair of Concrete >5" at Parapet & Structural repair of concrete <5" at piers and abutments.
11. Apply Protective Coat to top of overlay, sidewalk, Top and inside face of parapet.



**EXISTING ELEVATION**  
 FOR INFORMATION ONLY



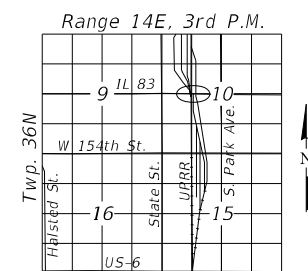
**PROPOSED ELEVATION**



**PLAN**

**LEGEND**

Limits of Protective Shield



**LOCATION SKETCH**

**GENERAL PLAN & ELEVATION**  
**F.A.P. RT. 397 (IL 83)**  
**OVER UPRR (DOLTON YARD)**  
**SECTION FAP 397 22 BRIDGE REHAB**

**COOK COUNTY**  
**SN 016-0955**  
**STA. 270+43.11**

01/26/2024  
 Ralph Otrembiak, PE, SE Date  
 Licensed Structural Engineer  
 State of Illinois 081-004812  
 Expires 11/30/2024



**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION**  
**STRUCTURE NO. 016-0955**

SHEET 1 OF 19 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	18
CONTRACT NO. 62R75				

ILLINOIS FED. AD. PROJECT

MODEL: Default  
 FILE NAME: D:\23122-5\11-016-0955-01-1\Bridges\GPE.dgn  
 1/26/2024 6:16:22 PM



USER NAME =	DESIGNED - EG	REVISED -
PLOT SCALE =	CHECKED - RJO	REVISED -
PLOT DATE =	DRAWN - EG	REVISED -
	CHECKED - RJO	REVISED -

**GENERAL NOTES**

- All new structural steel and bearing assembly shall be hot-dip galvanized. See Special Provisions for "Hot Dip Galvanizing for Structural Steel".
- No field welding is permitted except as specified in the contract documents.
- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.  
  
As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding 1#4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- Concrete Sealer shall be applied to the north abutment backwall. All surfaces to be sealed shall be cleaned thoroughly prior to sealer application.
- The Contractor shall exercise care during removal of existing joints to ensure that the slab, beams and diaphragms' integrity will not be detrimentally impacted. The Contractor shall repair any damage(s) to the slab, beams and diaphragms caused by his operation as directed by the Engineer at no additional cost to the Department.
- Protective Coat shall be applied to areas of overlay, joint repairs and parapet repairs per Section 503.19 of the Standard Specifications.
- Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.


**INDEX OF SHEETS**

- General Plan and Elevation
- General Data
- Construction Staging
- Temporary Concrete Barrier
- Deck Repair and Overlay Plan
- Drainage Scupper Adjustment Details
- Aluminum Railing, Type L
- 8-9. Abutment and Pier Expansion Joint Replacement Plan and Details
- 10-12. Preformed Joint Strip Seal - Sidewalk
- 13-14. UHPC Link Slab Plan and Details
- 15-16. Bearing Replacement Details I and II
- 17-18. Substructure Repairs
19. Bar Splicer Assembly and Mechanical Splicer Details

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	39.9		39.9
Protective Shield	Sq Yd	2,325		2,325
Concrete Superstructure	Cu Yd	35.2		35.2
Bridge Deck Grooving	Sq Yd	2,498		2,498
Protective Coat	Sq Yd	3,595		3,595
Reinforcement Bars, Epoxy Coated	Pound	5,690		5,690
Bar Splicers	Each	112		112
Mechanical Splicers	Each	32		32
Preformed Joint Strip Seal	Foot	170		170
Elastomeric Bearing Assembly, Type I	Each	16		16
Elastomeric Bearing Assembly, Type II	Each	1		1
Anchor Bolts, 1" Dia.	Each	58		58
Concrete Sealer	Sq Ft		638	638
Cleaning Bridge Scuppers And Downspouts	Each	8		8
Concrete Structures (Special)	Cu Yd	7.9		7.9
Repair Bridge Rail	Foot	222		222
Elastomeric Bearing Assembly, Type I (Special)	Each	14		14
Jack And Remove Existing Bearings	Each	31		31
Bridge Deck Latex Concrete Overlay, 2 1/4 Inches	Sq Yd	2,616		2,616
Bridge Deck Scarification, 3/4"	Sq Yd	2,616		2,616
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft	2	638	640
Structural Repair Of Concrete (Depth Greater Than 5 Inches)	Sq Ft	5		5
Deck Slab Repair (Full Depth, Type II)	Sq Yd	3		3
Drainage Scuppers (Special)	Each	2		2
Drainage Scuppers To Be Adjusted	Each	8		8
Polymer Concrete	Cu Ft	26		26

MODEL: Default  
FILE NAME: D:\23122-shl-016-0955-02-GeneralData.dgn

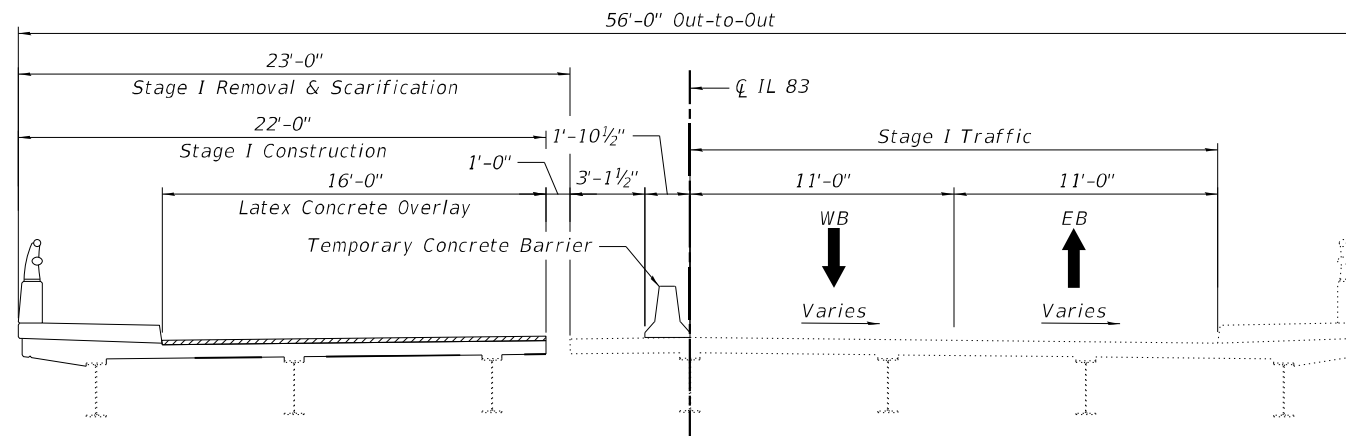
	USER NAME =	DESIGNED - RJO	REVISED -
		CHECKED - JJM & EG	REVISED -
	PLOT SCALE =	DRAWN - EG	REVISED -
	PLOT DATE =	CHECKED - RJO	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

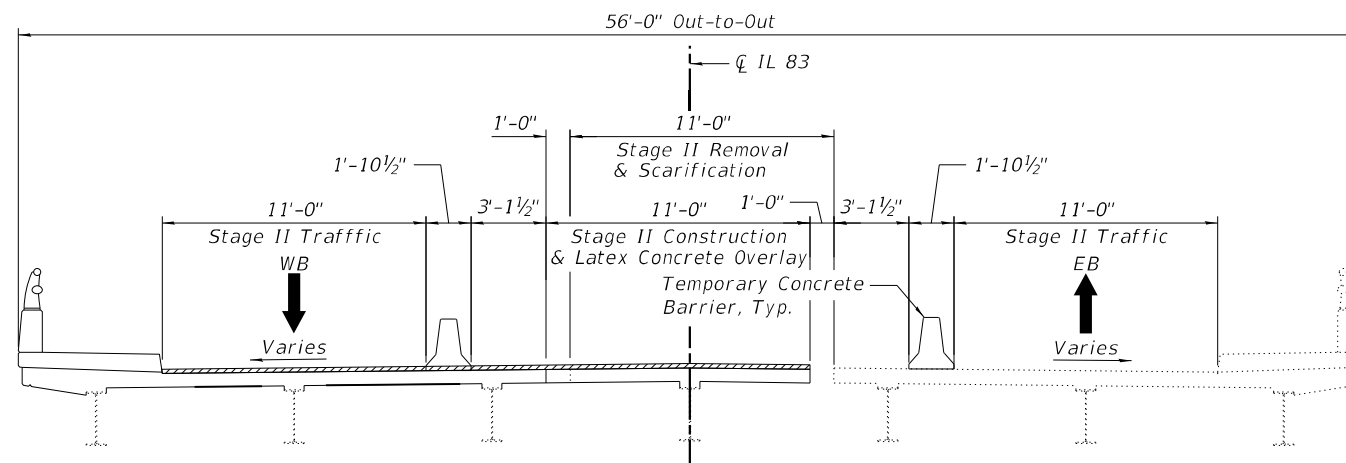
**GENERAL DATA  
STRUCTURE NO. 016-0955**

SHEET 2 OF 19 SHEETS

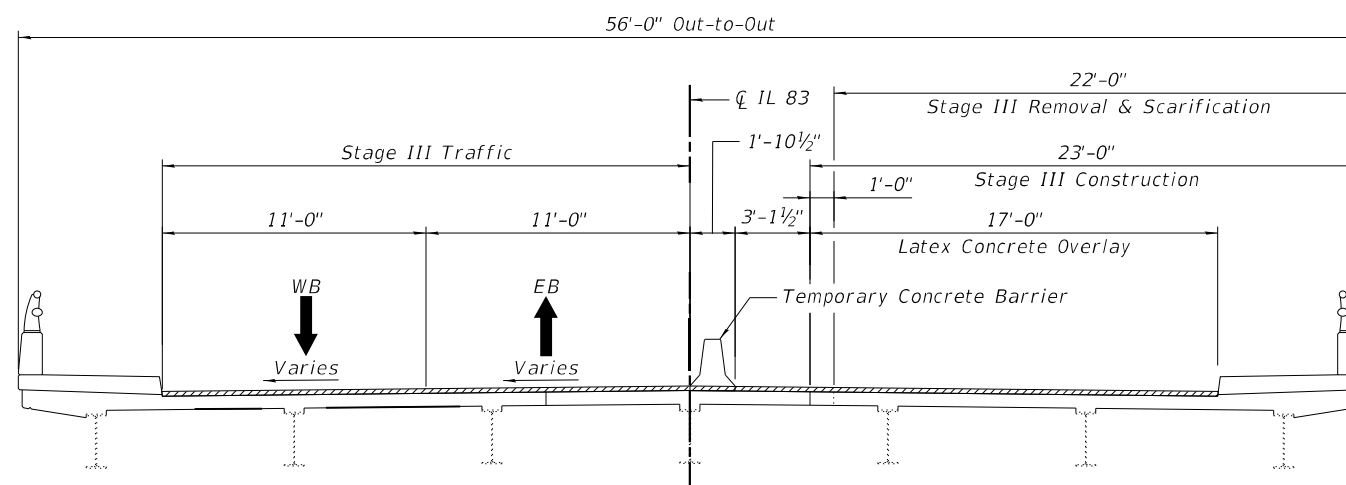
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	19
CONTRACT NO. 62R75				
		ILLINOIS	FED. AID PROJECT	



**STAGE I**  
(Looking East)



**STAGE II**  
(Looking East)



**STAGE III**  
(Looking East)

MODEL: Default  
FILE NAME: D:\23122-shl-016-0955-03-ConstructionStaging.dgn



USER NAME =	DESIGNED - EG	REVISED -
	CHECKED - RJO	REVISED -
PLOT SCALE =	DRAWN - EG	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

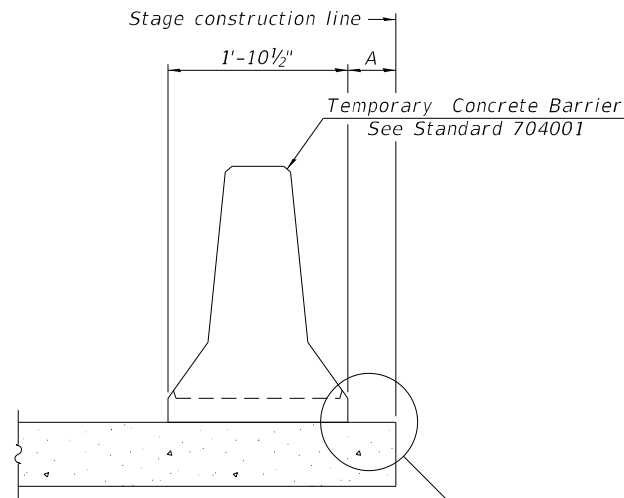
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION STAGING  
STRUCTURE NO. 016-0955**

SHEET 3 OF 19 SHEETS

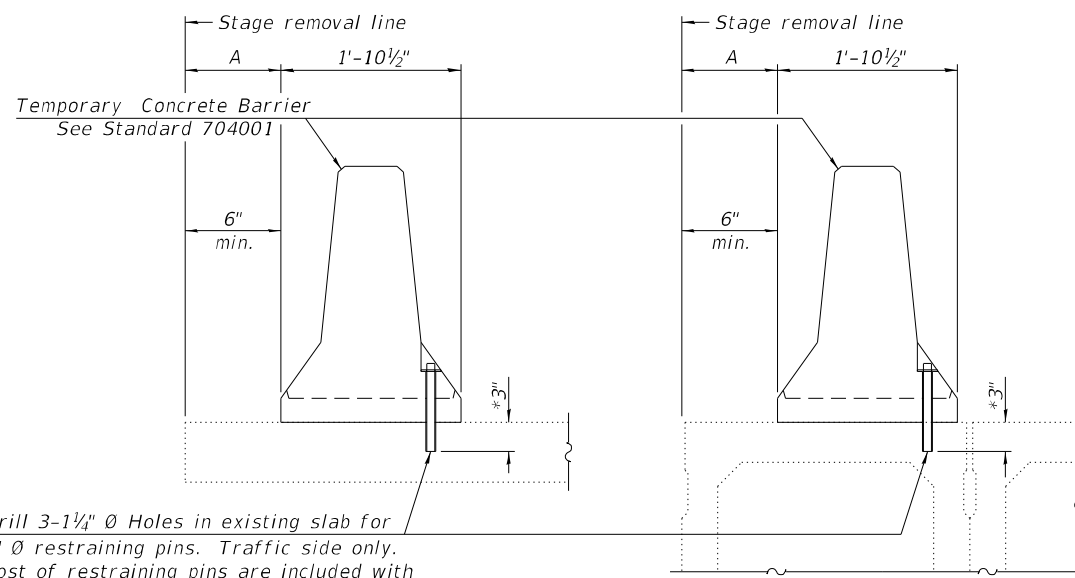
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	20
CONTRACT NO. 62R75				

ILLINOIS FED. AID PROJECT



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

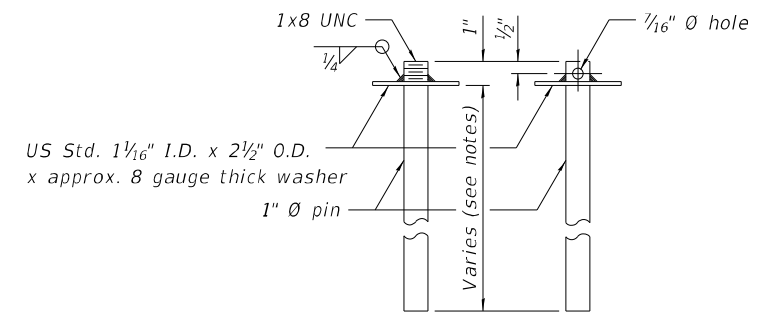


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

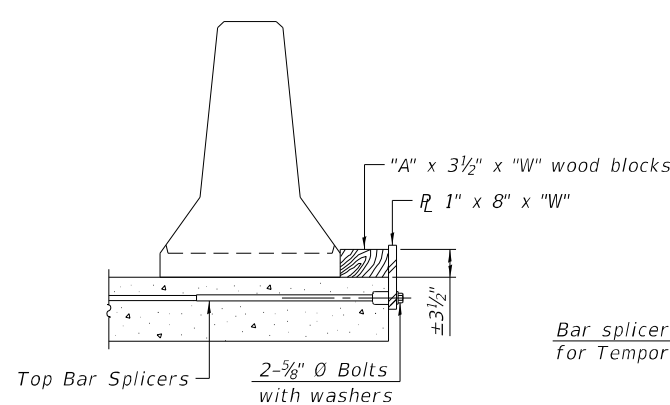
\* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

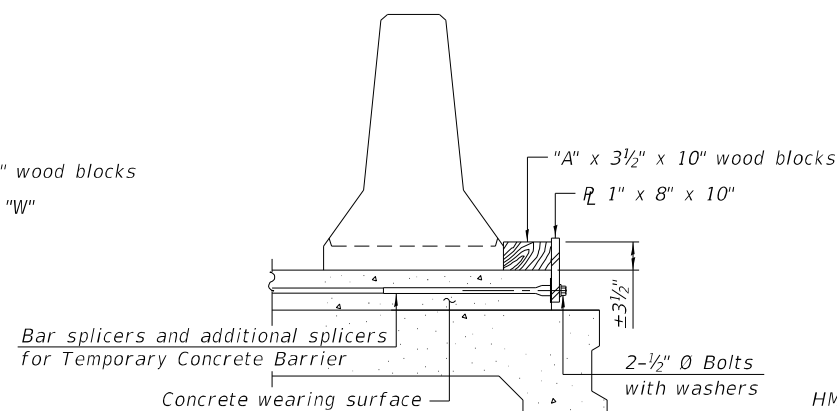


RESTRAINING PIN

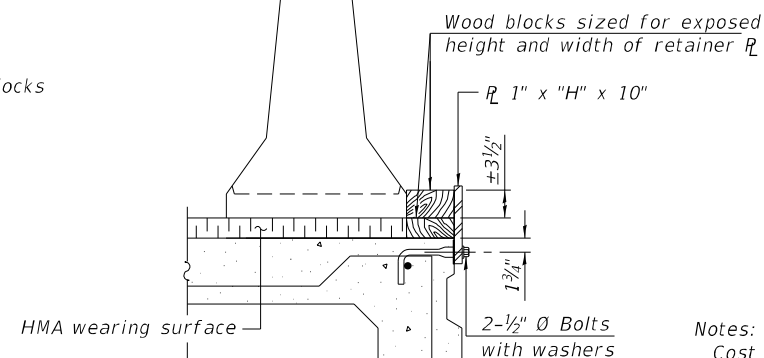
SECTIONS THRU SLAB OR DECK BEAM



DETAIL I

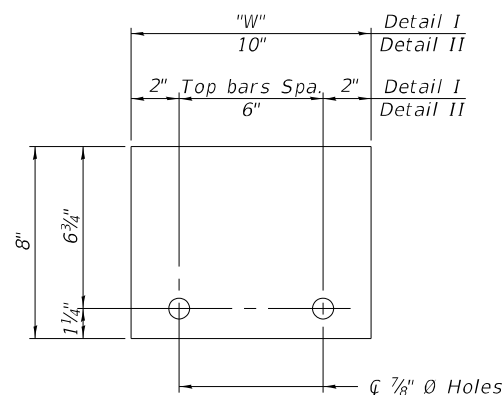


DETAIL II

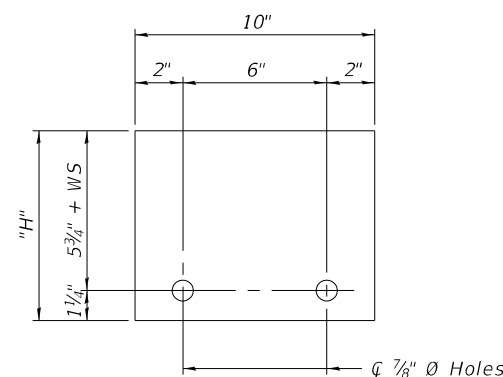


DETAIL III

BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W" (Detail I and II)



STEEL RETAINER R 1" x "H" x 10" (Detail III)

Notes:  
 Cost of retainer assembly is included with Temporary Concrete Barrier.  
 A retainer assembly shall be located at the approximate  $\frac{1}{2}$  of each temporary concrete barrier.  
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.  
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.  
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.  
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27 10-12-2021

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION  
STRUCTURE NO. 016-0955

SHEET 4 OF 19 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	21
CONTRACT NO. 62R75				

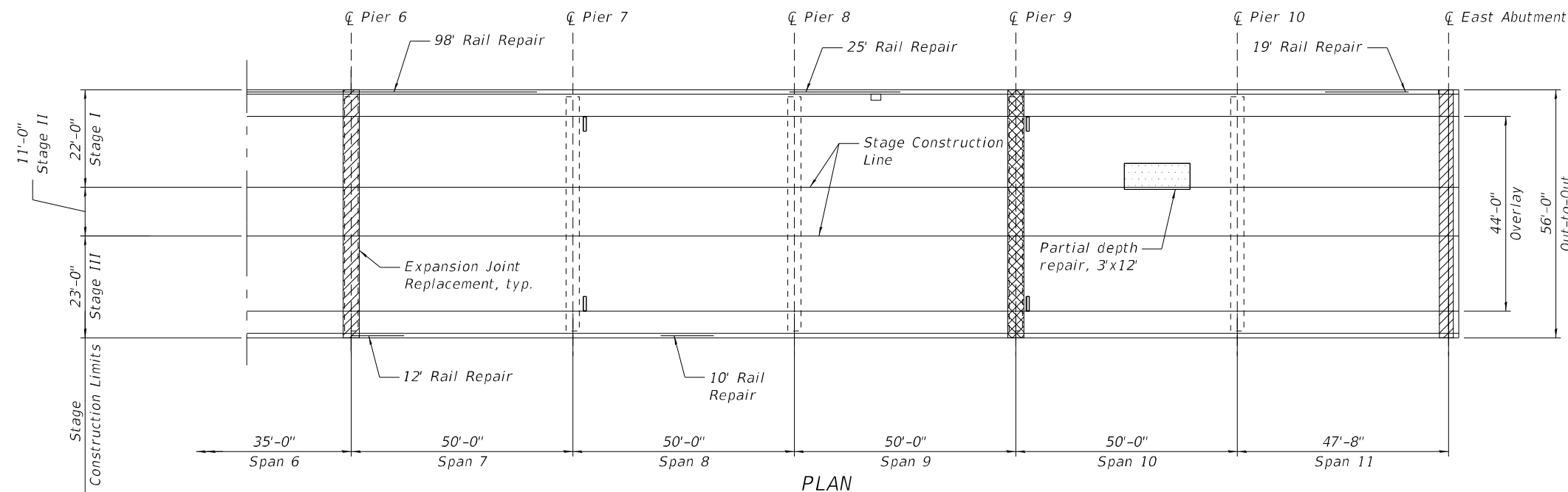
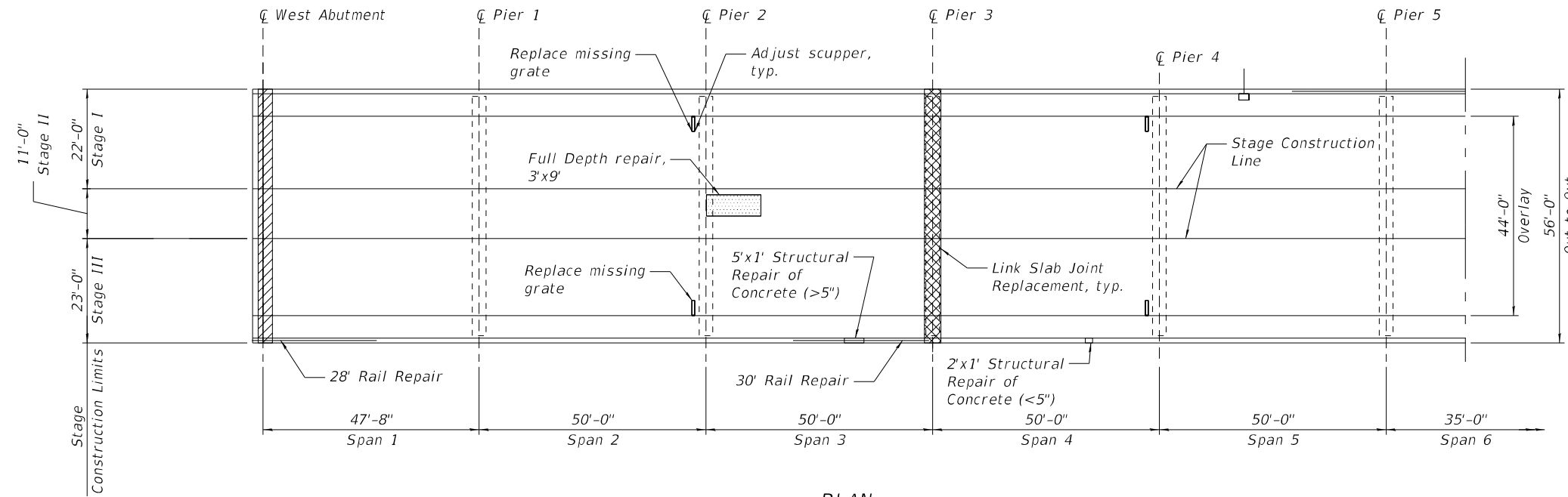
USER NAME =	DESIGNED - EG	REVISED -
PLOT SCALE =	CHECKED - RJO	REVISED -
PLOT DATE =	DRAWN - EG	REVISED -
	CHECKED - RJO	REVISED -

**NOTES:**

- Deck slab repair areas are estimated based on an inspection conducted by IDOT and the visual inspection conducted in October 2022 actual repair areas and locations shall be determined by the Engineer and shown on As-Built plans.
- Areas of Deck Slab Repair (Partial) will not be measured for payment and shall be included with the cost of Bridge Deck Scarification, 3/4 Inches.
- See Sheet 7 of 19 for Aluminum Railing Details.
- See Sheet 8 & 9 of 19 for Expansion Joint Replacement Details.
- See Sheet 13 & 14 of 19 for Link Slab Joint Replacement Details.
- See Sheet 6 of 19 for Drainage Scupper Adjustment details.
- Replace missing scupper grates as shown on the Plan. This work shall be paid as Drainage Scuppers, Special.

**BILL OF MATERIAL**

Item	Unit	Total
Protective Coat	Sq Yd	3,595
Repair Bridge Rail	Foot	222
Bridge Deck Latex Concrete Overlay, 2 1/4 Inches	Sq Yd	2,616
Bridge Deck Scarification, 3/4"	Sq Yd	2,616
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq Ft	2
Structural Repair of Concrete (Depth Greater than 5 Inches)	Sq Ft	5
Deck Slab Repair (Full Depth, Type II)	Sq Yd	3
Drainage Scuppers to be Adjusted	Each	8
Drainage Scuppers, Special	Each	2



**LEGEND**

- Railing Repair
- Concrete Removal & Expansion Joint Replacement
- Concrete & Expansion Joint Removal & Replacement with Link Slab
- Partial Depth Deck Repair
- Full Depth Deck Repair

MODEL: Default  
FILE NAME: D:\23122-5\11-016-0955-05-DeckRepair1.dgn



USER NAME =	DESIGNED - EG	REVISED -
CHECKED - RJO	CHECKED - RJO	REVISED -
PLOT SCALE =	DRAWN - EG	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

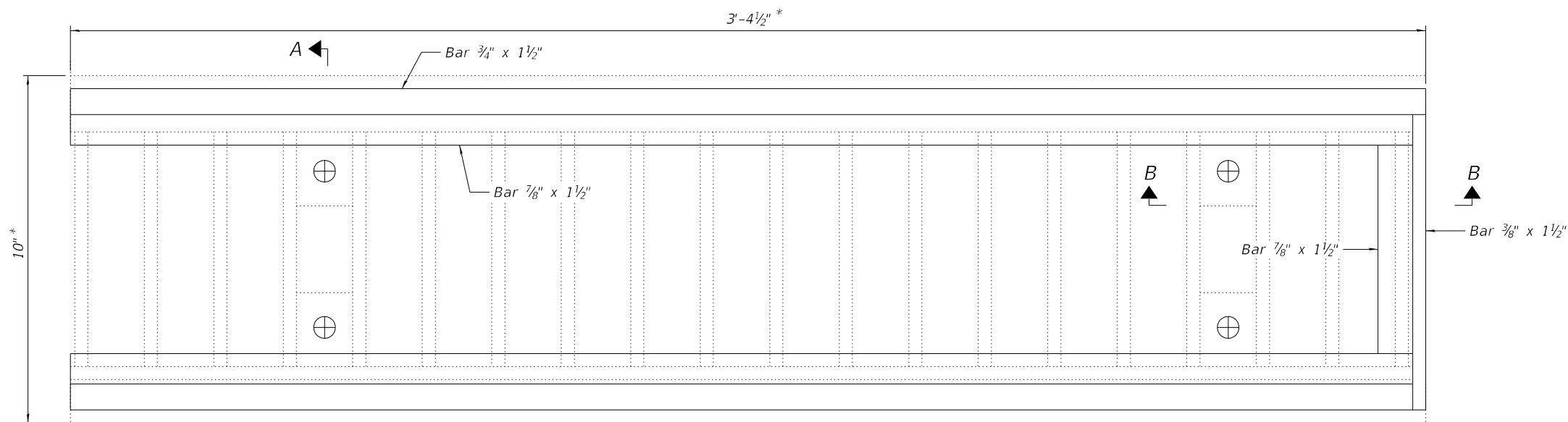
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DECK REPAIR AND OVERLAY PLAN  
STRUCTURE NO. 016-0955**

SHEET 5 OF 19 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	22
CONTRACT NO. 62R75				

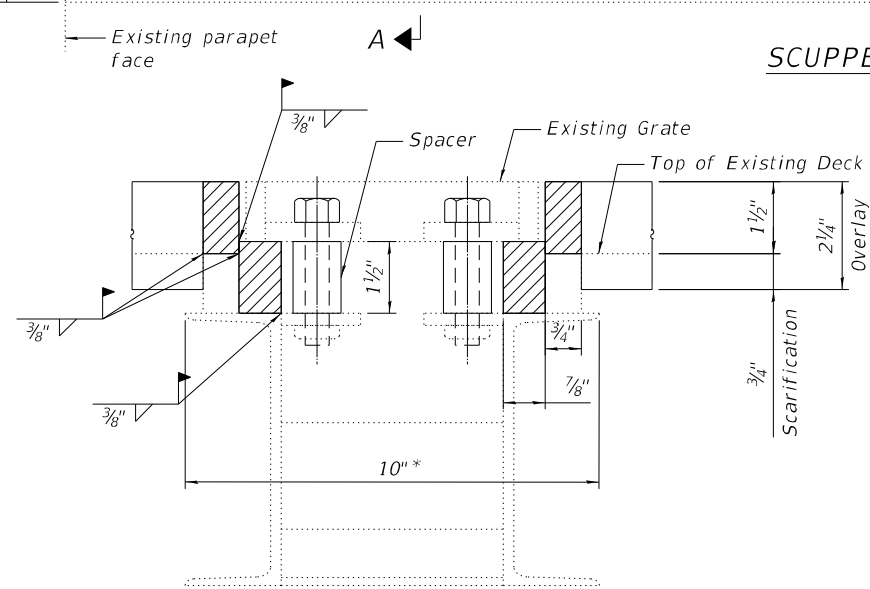
ILLINOIS FED. AID PROJECT



**GENERAL NOTES**

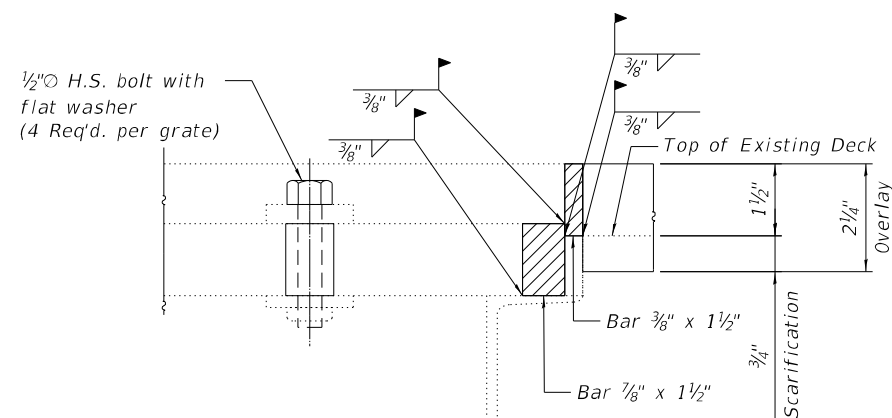
All structural steel shall conform to AASHTO Classification M-270 Gr. 36. The adjusting scupper ring shall be galvanized.  
 Bolts shall be 1/2"Ø, AASHTO M164 Type I, mechanically galvanized.  
 Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.  
 The contractor shall ensure that no damage is done to existing grates to be reused.  
 Shop plans for proposed adjusting scupper ring shall be submitted for approval prior to fabrication.  
 Cost of all labor and materials necessary to remove existing grates, clean existing scuppers, install adjusting scupper rings and reinstalling grates is included in the cost per unit each for "Drainage Scuppers to Be Adjusted".  
 See Sheet 5 of 19 for Bill of Materials.

**SCUPPER PLAN TYPE A**



\*Verify in Field

**SECTION A-A**



**SECTION B-B**

MODEL: Default  
 FILE NAME: D:\23122-shl-016-0955-06-Drainage Scupper.dgn  
 1/26/2024 6:16:38 PM

	USER NAME =	DESIGNED - EG	REVISED -
		CHECKED - RJO	REVISED -
	PLOT SCALE =	DRAWN - EG	REVISED -
	PLOT DATE =	CHECKED - RJO	REVISED -

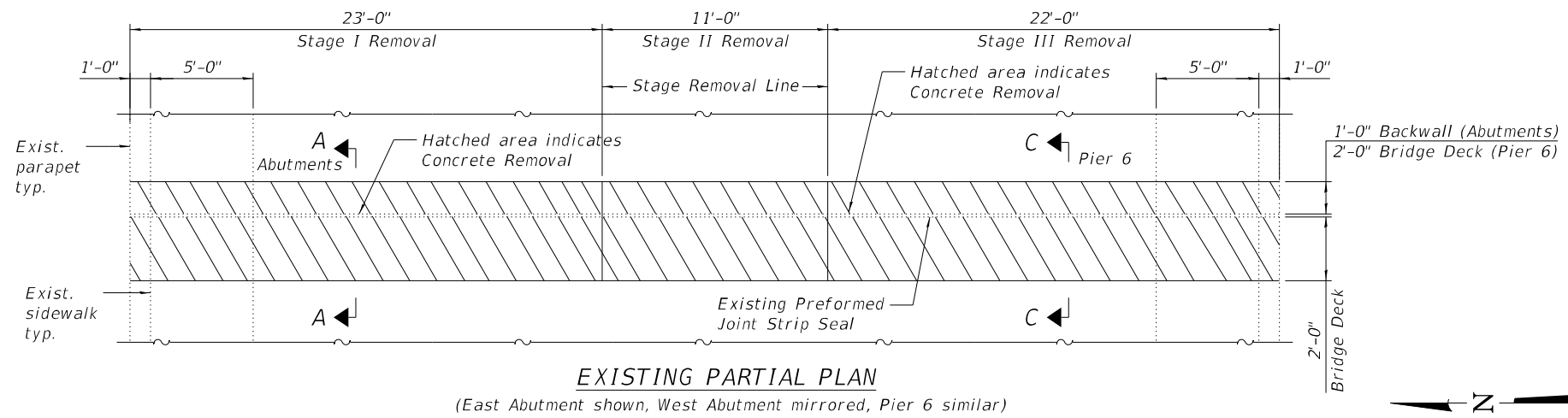
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRAINAGE SCUPPER ADJUSTMENT DETAILS  
 STRUCTURE NO. 016-0955**

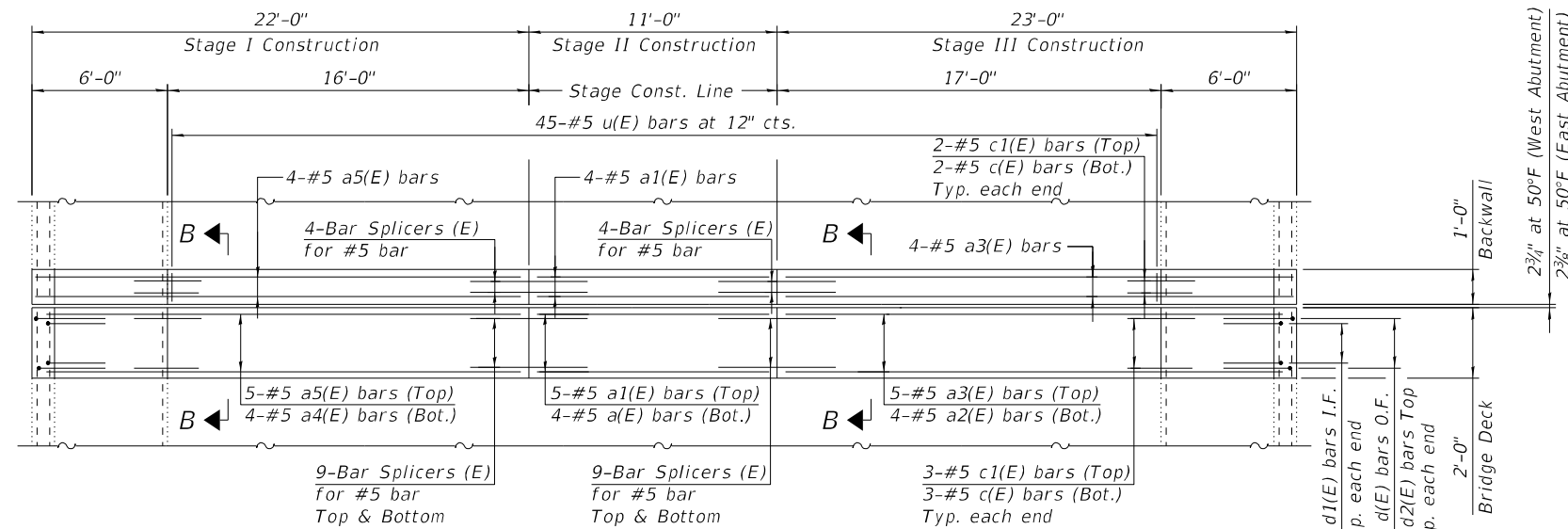
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	23
CONTRACT NO. 62R75				



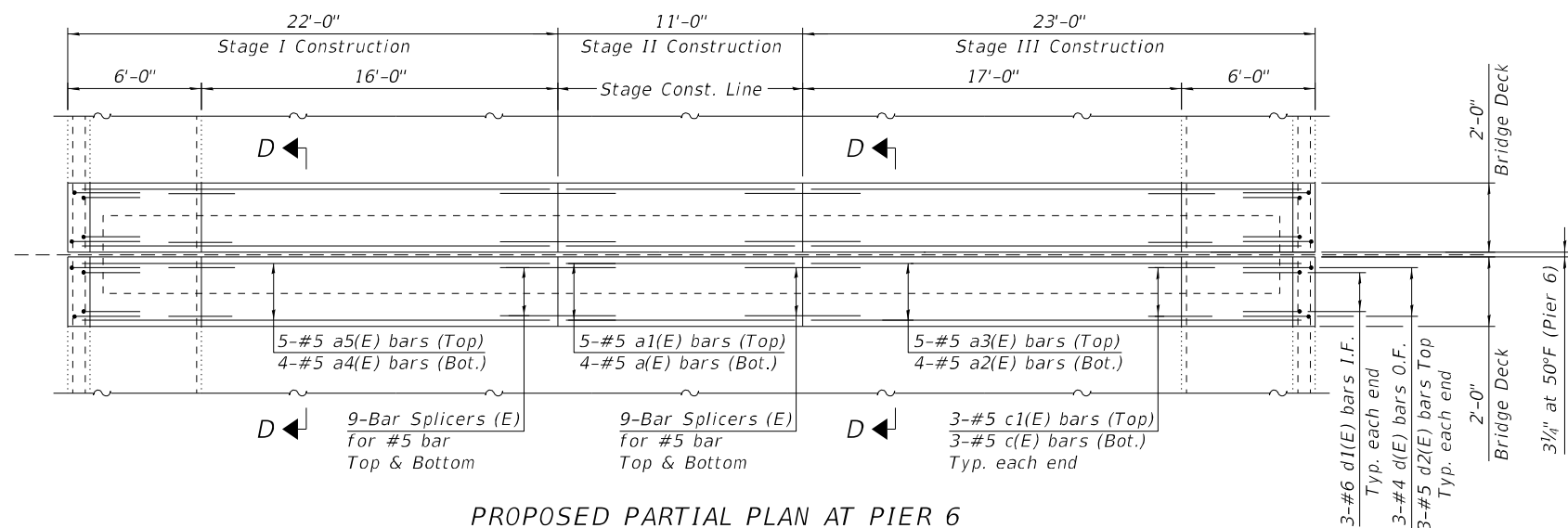




**EXISTING PARTIAL PLAN**  
(East Abutment shown, West Abutment mirrored, Pier 6 similar)



**PROPOSED PARTIAL PLAN AT ABUTMENT**  
(East Abutment shown, West Abutment mirrored)

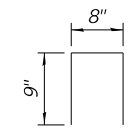


**PROPOSED PARTIAL PLAN AT PIER 6**

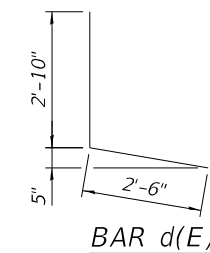
Note: Reinforcement layout is Typical, each side of Expansion Joint

**NOTES:**

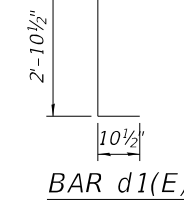
1. I.F. denotes Inside Face. O.F. denotes Outside Face.
2. See Sheet 9 of 19 for Sections A-A, B-B and B'-B'.
3. Existing reinforcement bars extending into the concrete removal area shall be cleaned, straightened and incorporated into the new construction. Cost included with Concrete Removal. Any reinforcement bars that are damaged during concrete removal shall be repaired or replaced with an approved bar splicer or anchorage system at no additional cost to the Department.
4. Existing reinforcement bars in the concrete removal area parallel to the expansion joints shall be removed.
5. Removal and disposal of the existing expansion joints will not be paid for separately, but shall be included with the cost of Concrete Removal.
6. If existing name plate falls within the limits of Concrete Removal, it shall be removed and reinstalled in its original location in accordance with IDOT Std. 515001. Cost included with Concrete Superstructure.
7. If existing guardrail and/or end shoe fall within the limits of Concrete Removal, they shall be removed and reinstalled in their original location in accordance with District 1 Std. BM-21. Cost included with Concrete Superstructure.
8. The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.



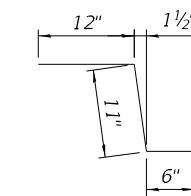
**BAR u(E)**



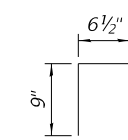
**BAR d(E)**



**BAR d1(E)**



**BAR c(E)**



**BAR d2(E)**

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
a(E)	16	#5	10'-10"	—
a1(E)	28	#5	10'-10"	—
a2(E)	16	#5	21'-9"	—
a3(E)	28	#5	22'-10"	—
a4(E)	16	#5	20'-9"	—
a5(E)	28	#5	21'-10"	—
c(E)	32	#5	2'-5"	┌
c1(E)	32	#5	5'-9"	—
d(E)	32	#4	5'-4"	┌
d1(E)	32	#6	3'-9"	┌
d2(E)	32	#5	2'-1"	┌
u(E)	90	#5	2'-2"	┌
Item		Unit	Total	
Concrete Removal		Cu. Yd.	23.9	
Concrete Superstructure		Cu. Yd.	25.9	
Reinforcement Bars, Epoxy Coated		Pound	3360	

MODEL: Default  
FILE NAME: D:\23122-5\11-016-0955-08-Exp\11Plan.dgn



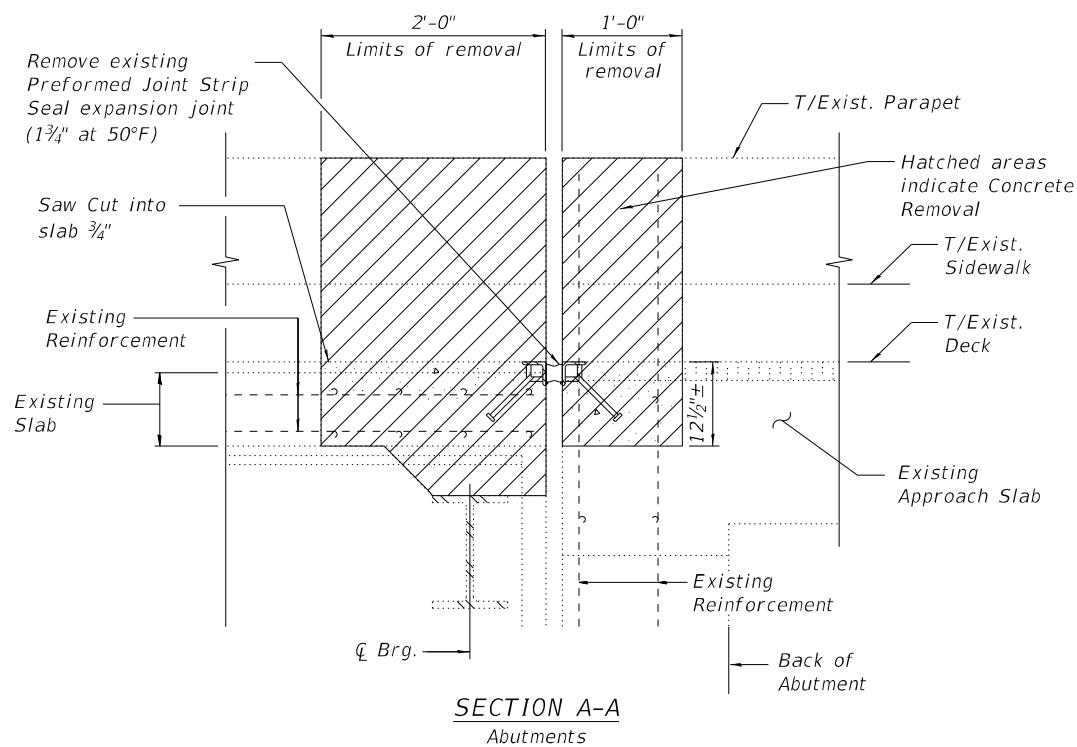
USER NAME =	DESIGNED - RJO	REVISED -
CHECKED - JJM	REVISED -	
PLOT SCALE =	DRAWN - JJM	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

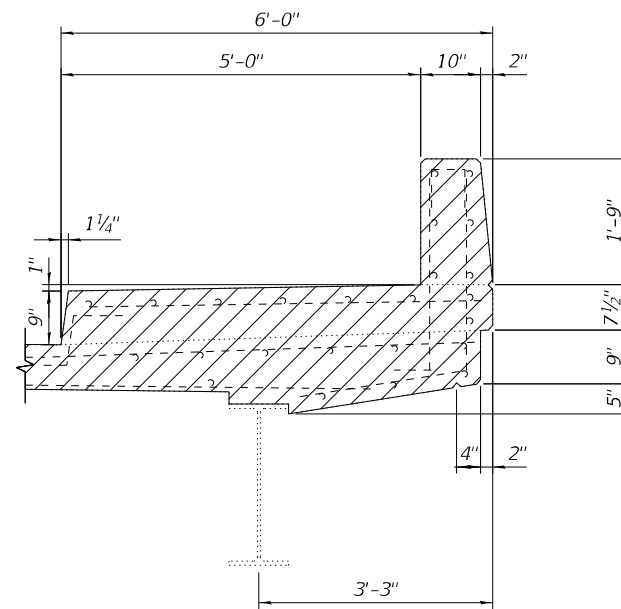
**ABUTMENT AND PIER EXPANSION JOINT REPLACEMENT PLAN  
STRUCTURE NO. 016-0955**

SHEET 8 OF 19 SHEETS

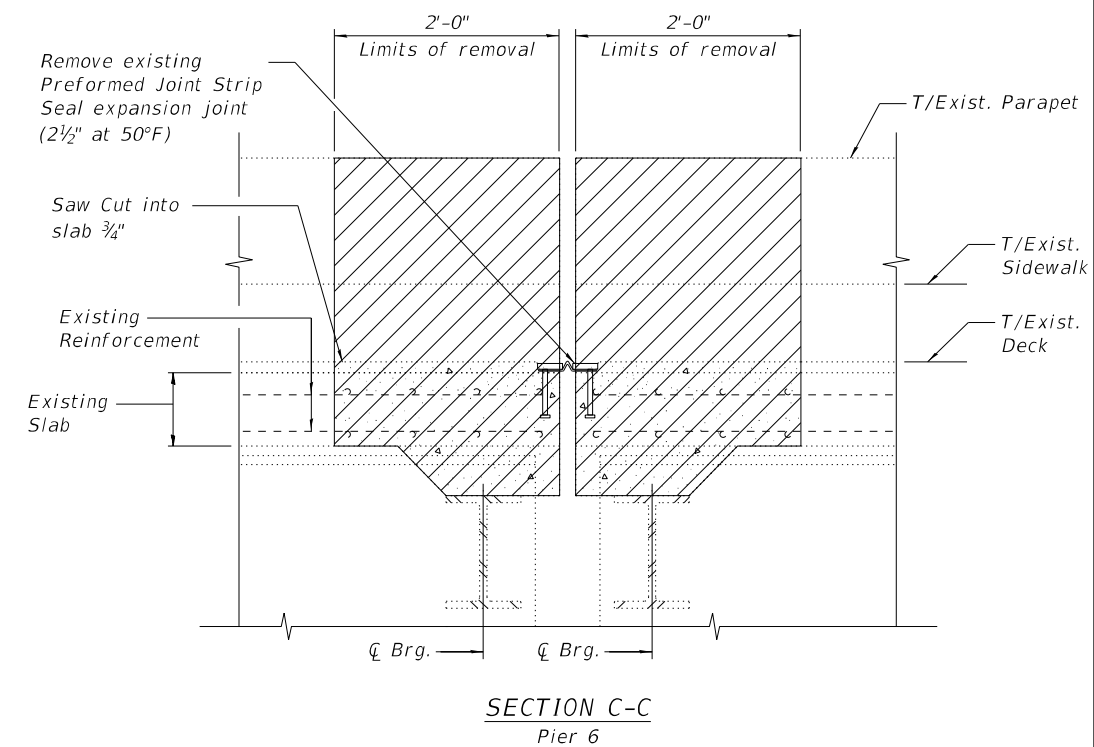
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	25
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				



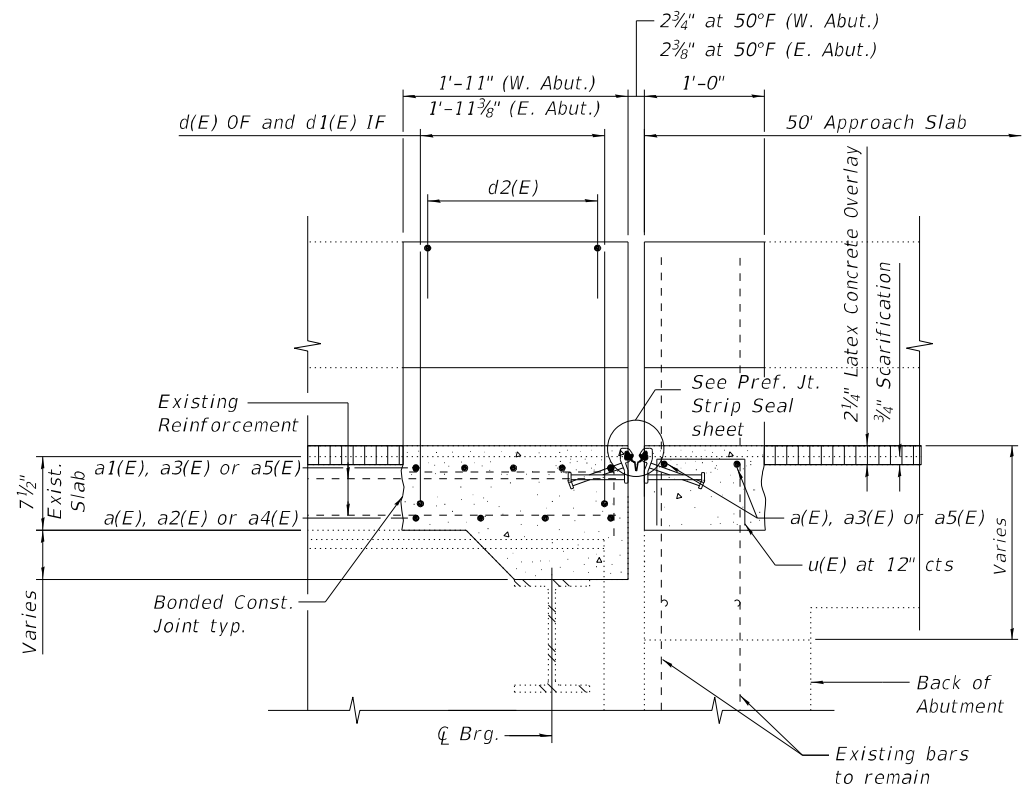
SECTION A-A  
Abutments



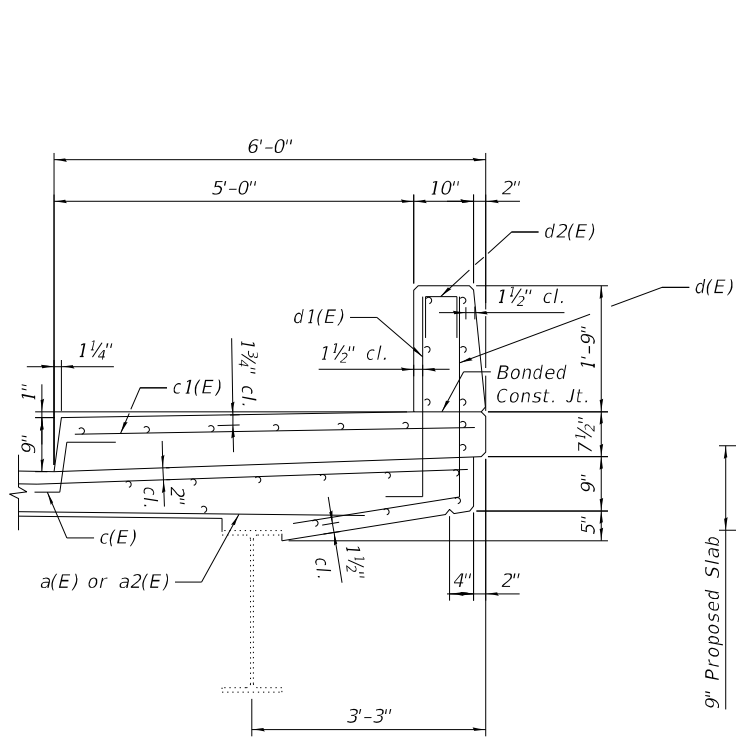
EXISTING PARAPET SECTION



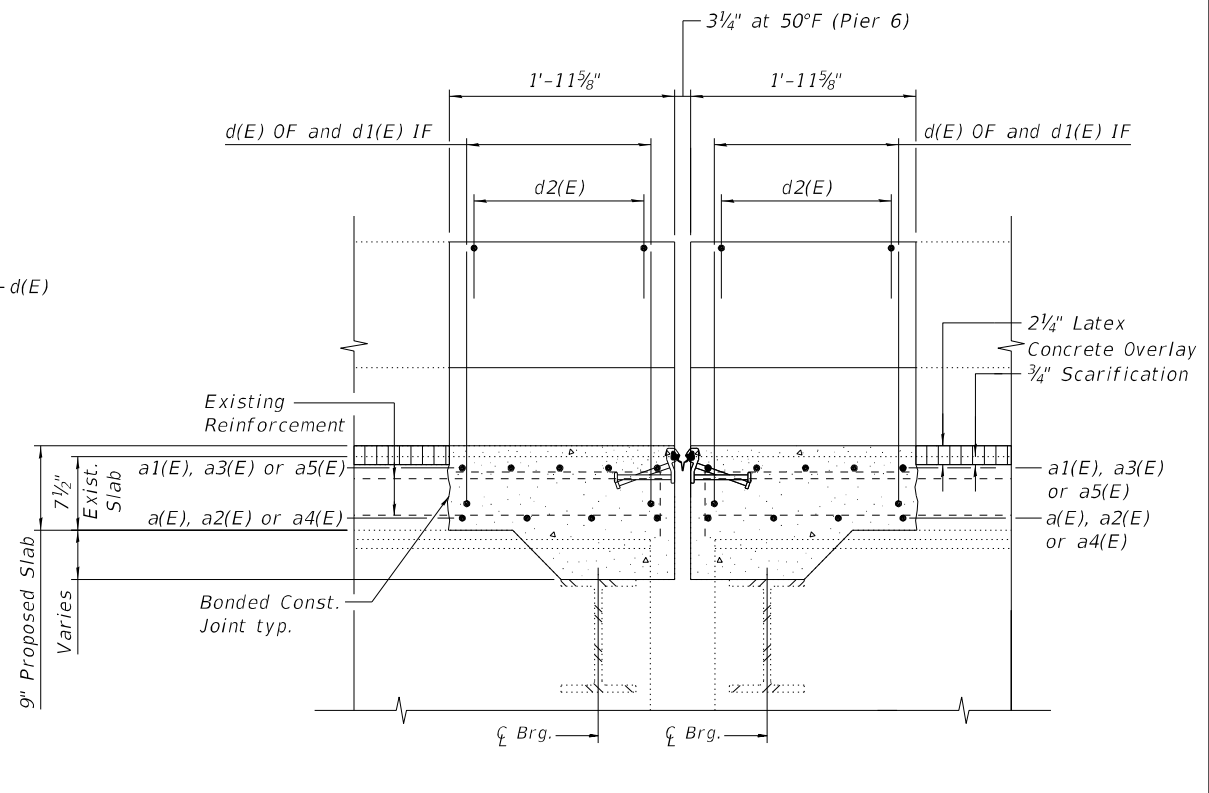
SECTION C-C  
Pier 6



SECTION B-B  
Abutments



PROPOSED PARAPET SECTION



SECTION D-D  
Pier 6

Legend

Concrete Removal

MODEL: Default  
FILE NAME: D:\212122-511-016-0955-09-Exp\11Details.dgn  
1/26/2024 6:16:49 PM



USER NAME =	DESIGNED - JJM	REVISED -
CHECKED - RJO	REVISED -	
PLOT SCALE =	DRAWN - JJM	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

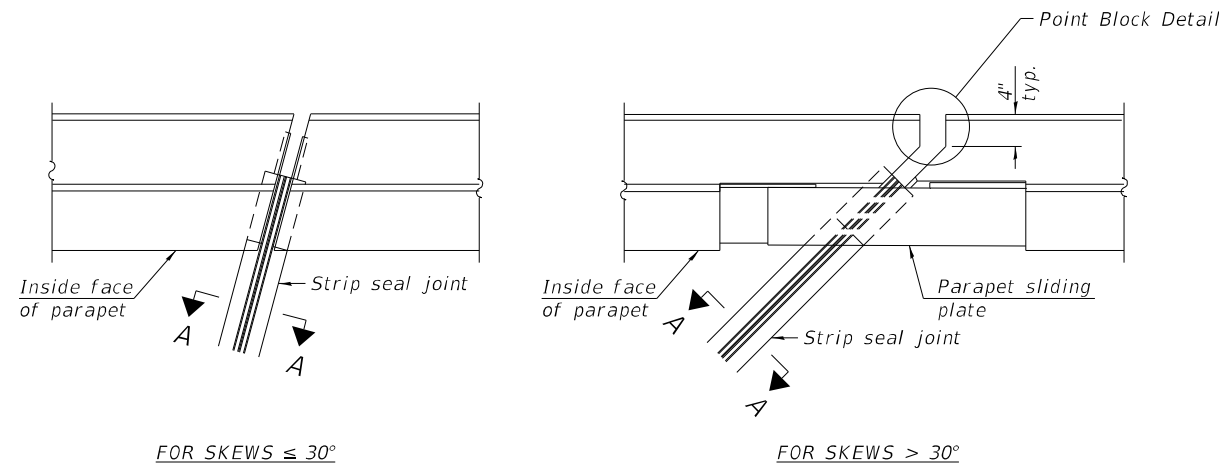
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

ABUTMENT AND PIER EXPANSION JOINT REPLACEMENT DETAILS  
STRUCTURE NO. 016-0955

SHEET 9 OF 19 SHEETS

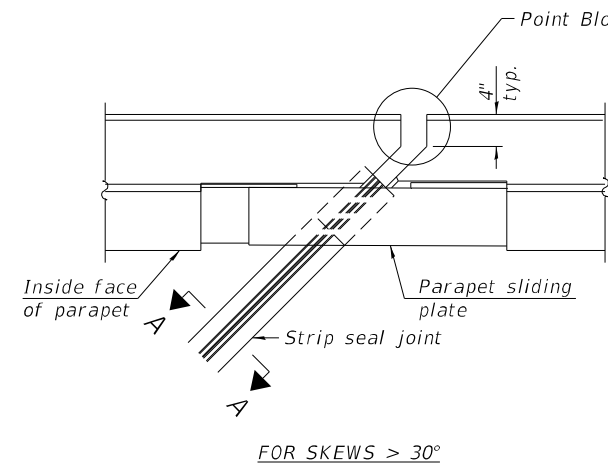
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	26
CONTRACT NO. 62R75				

ILLINOIS FED. AID PROJECT

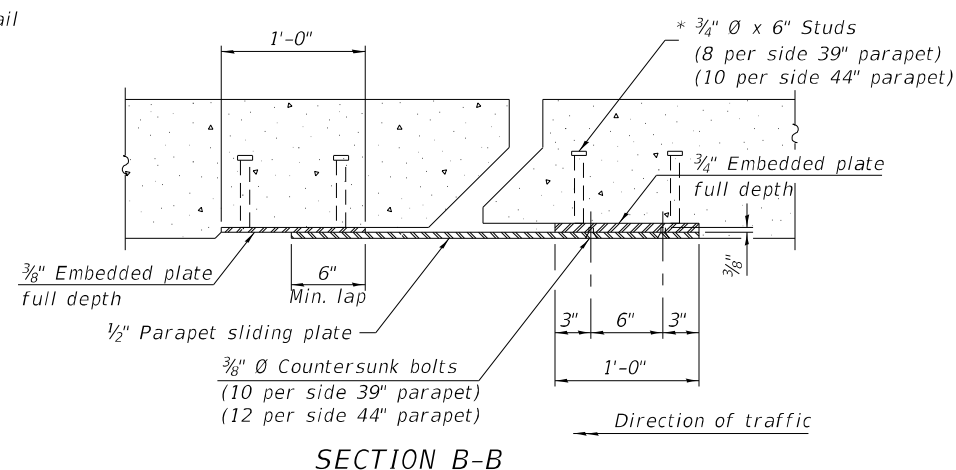


FOR SKEWS  $\leq 30^\circ$

PLAN AT PARAPET

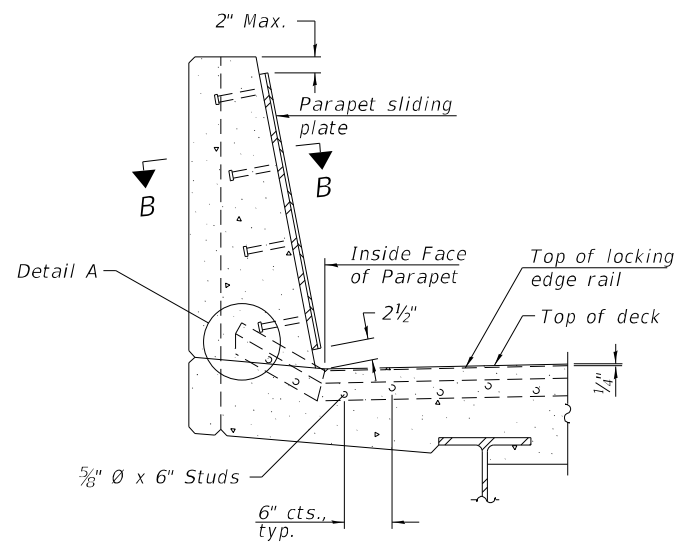


FOR SKEWS  $> 30^\circ$



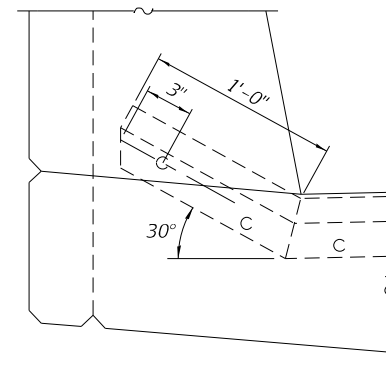
SECTION B-B

**Notes:**  
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.  
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.  
 The manufacturer's recommended installation methods shall be followed.  
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.  
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.  
 The top surface of sidewalk sliding plates shall have a raised pattern according to ASTM A786.  
 Cost of parapet sliding plates, sidewalk sliding plates, embedded plates, anchorage studs, and expansion anchors included with Preformed Joint Strip Seal.  
 39" constant slope barrier shown, 44" constant slope barrier similar as noted.  
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

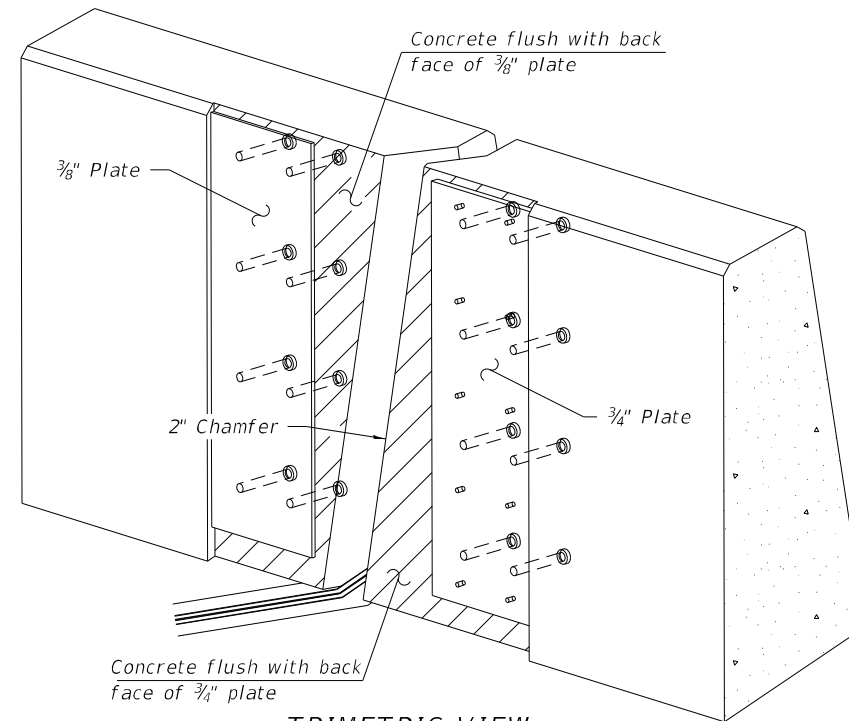


SECTION AT PARAPET

(Skews  $> 30^\circ$  shown. Skews  $\leq 30^\circ$  similar except as shown in plan view.)

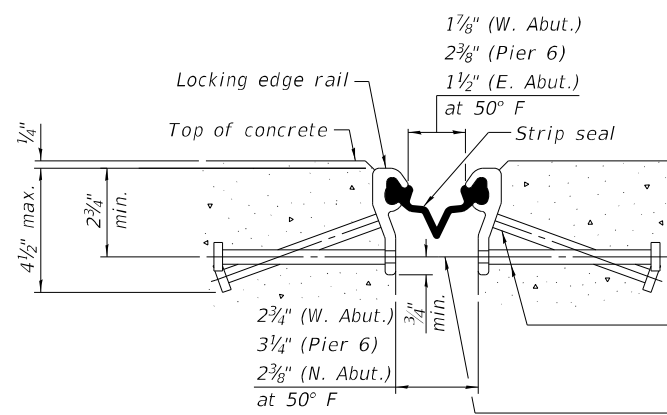


DETAIL A



TRIMETRIC VIEW

(Showing embedded plates only)



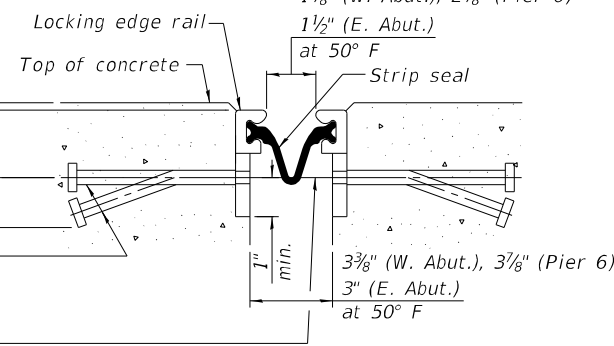
SHOWING ROLLED RAIL JOINT

\* 5/8"  $\phi$  x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

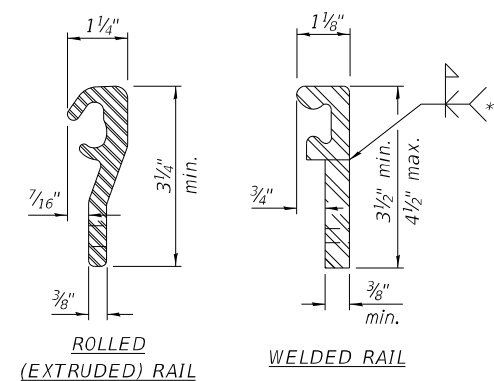
3/8"  $\phi$  threaded rods in 1/16"  $\phi$  holes at  $\pm 4'-0"$  cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

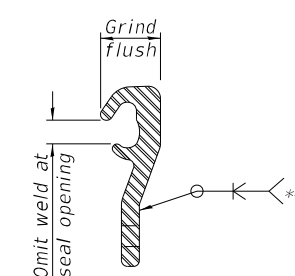


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

\*\* Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	170

MODEL: Default  
 FILE NAME: D:\23122-sht-016-0955-10-ProfStripSeal\_1.dgn  
 1/26/2024 6:16:53 PM

EJ-SS-S

1-1-2020

(Sheet 1 of 3)



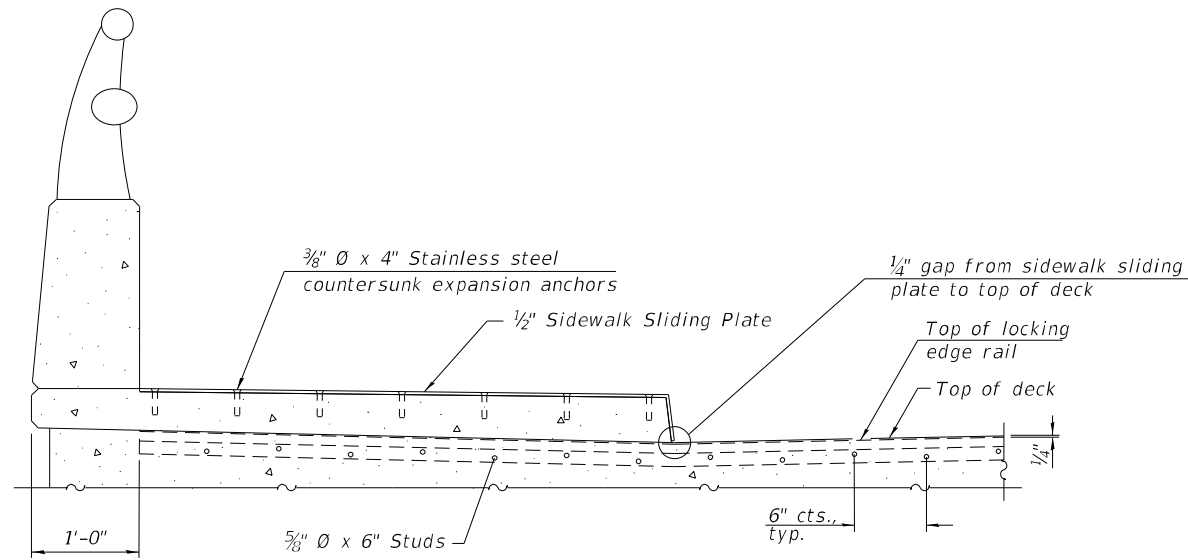
USER NAME =	DESIGNED - EG	REVISED -
PLOT SCALE =	CHECKED - RJO	REVISED -
PLOT DATE =	DRAWN - EG	REVISED -
	CHECKED - RJO	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

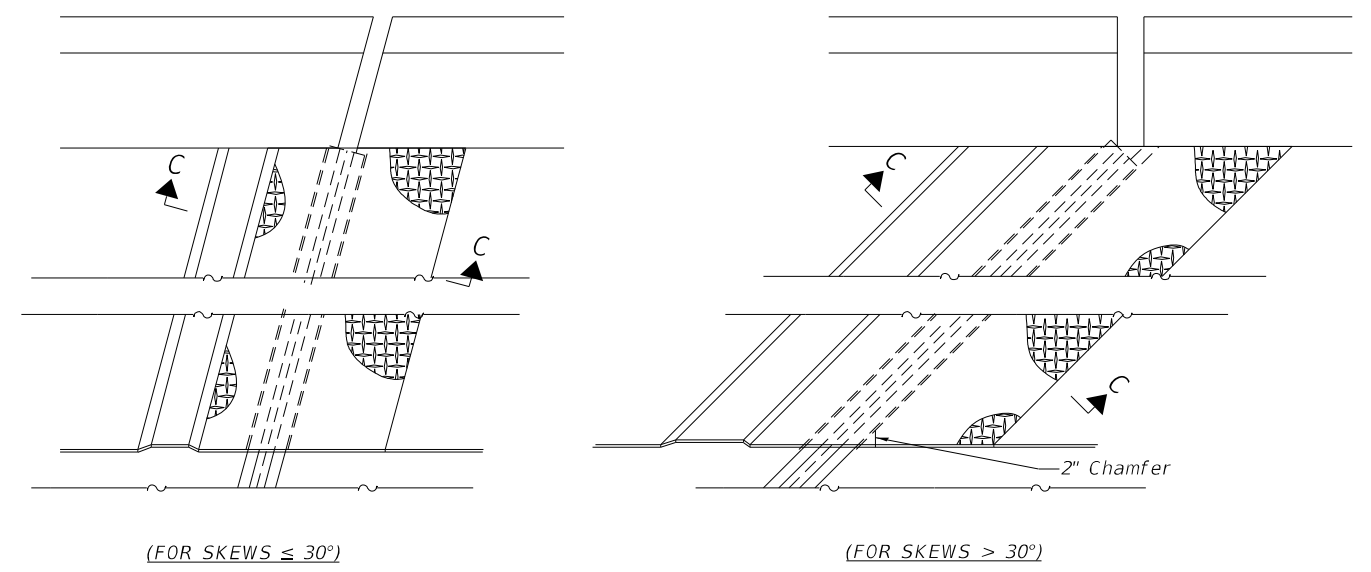
PREFORMED JOINT STRIP SEAL - SIDEWALK  
 STRUCTURE NO. 016-0955

SHEET 10 OF 19 SHEETS

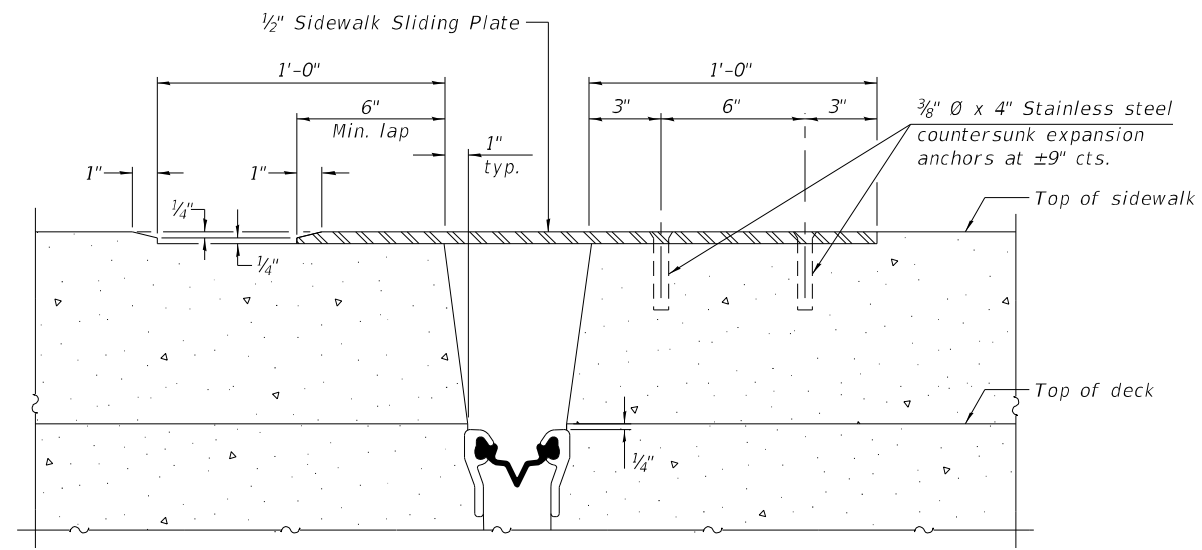
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	27
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				



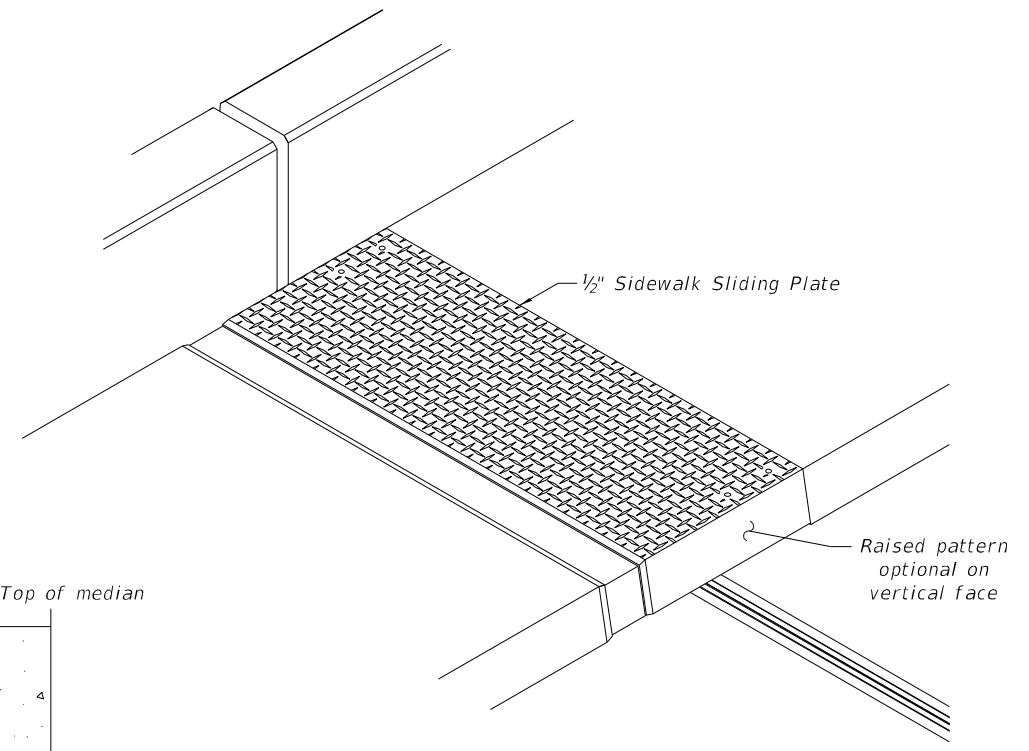
**SECTION AT RAISED SIDEWALK**



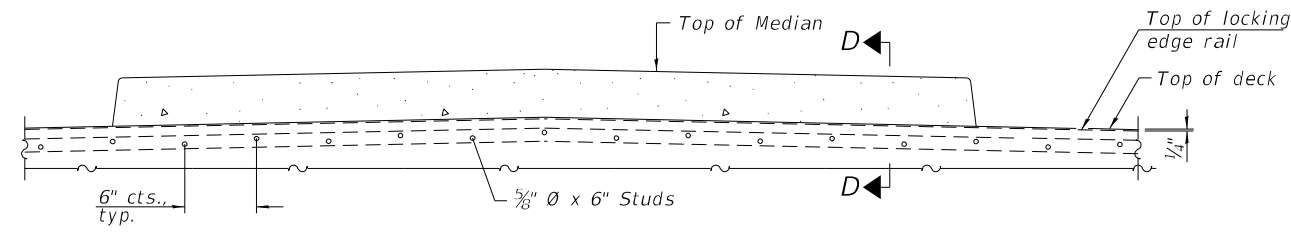
**PLAN AT RAISED SIDEWALK**



**SECTION C-C**

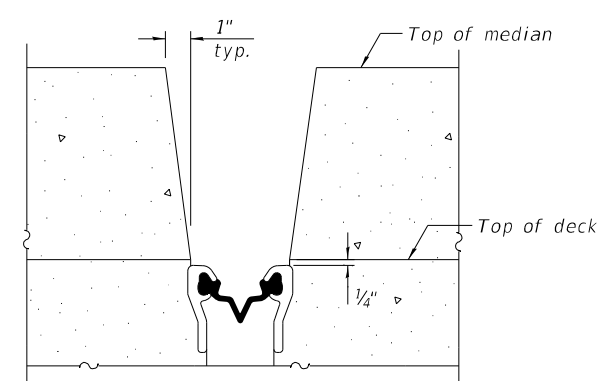


**TRIMETRIC VIEW**



**SECTION AT MEDIAN**

For skews > 30°, chamfer acute corners 2" similar to sidewalk.



**SECTION D-D  
(at Rt. L's)**

(Sheet 2 of 3)

MODEL: Default  
FILE NAME: D:\23122-shl-016-0955-1-1-ProfileStripSeal\_2.dgn  
1/26/2024 6:16:56 PM

EJ-SS-S

1-1-2020



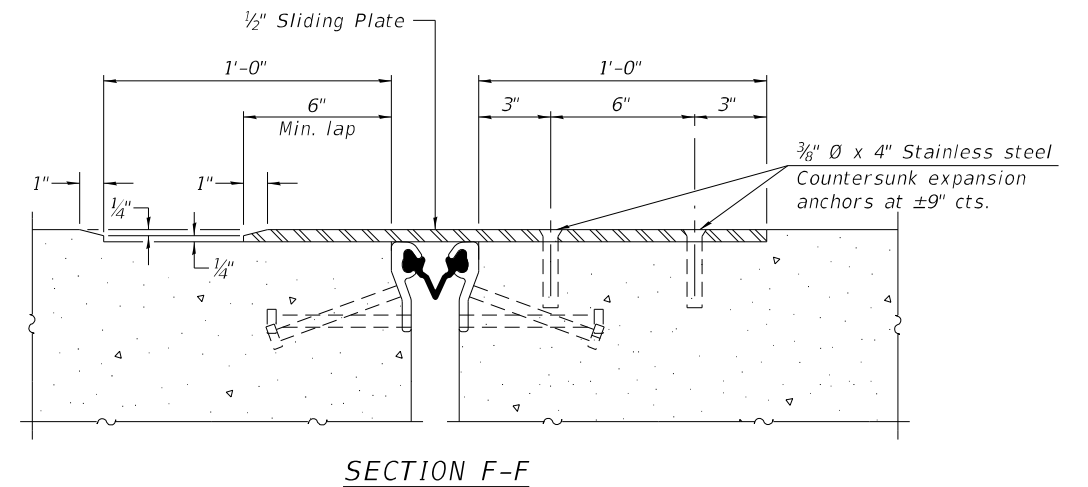
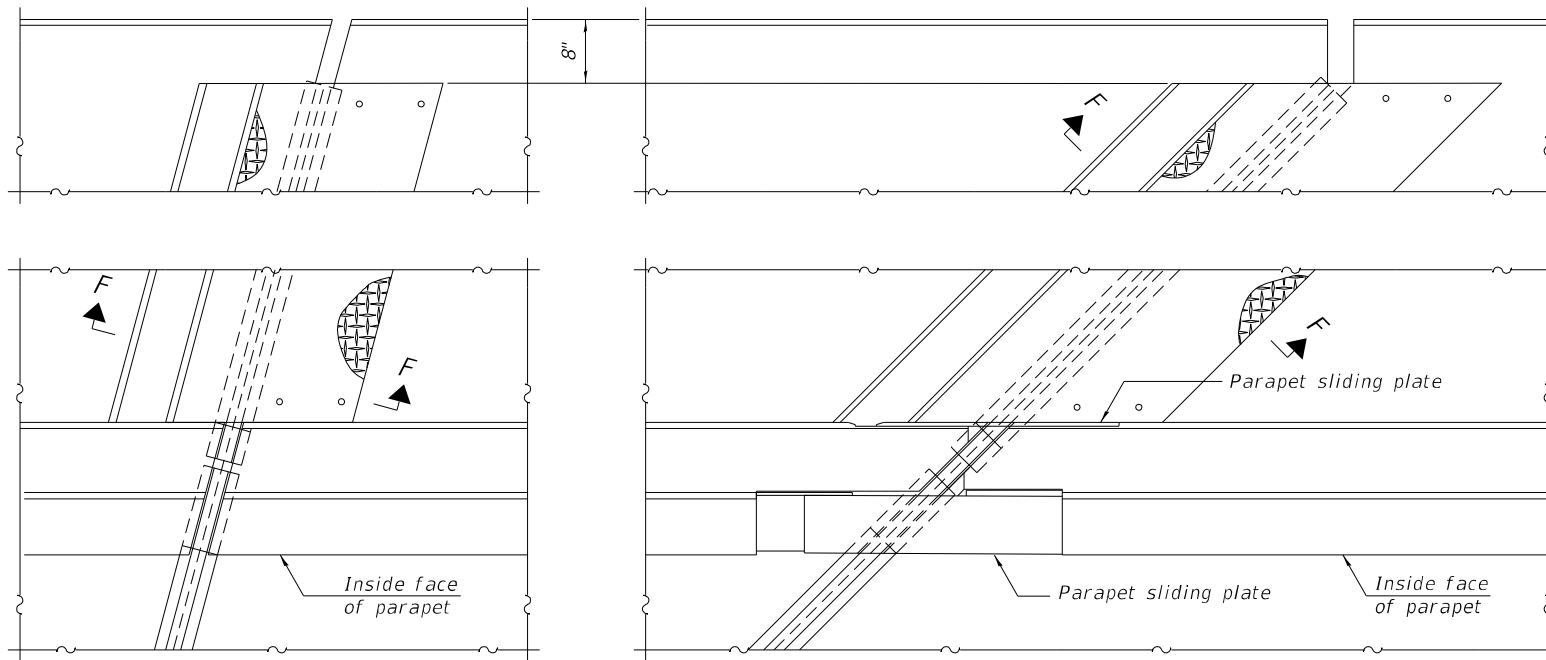
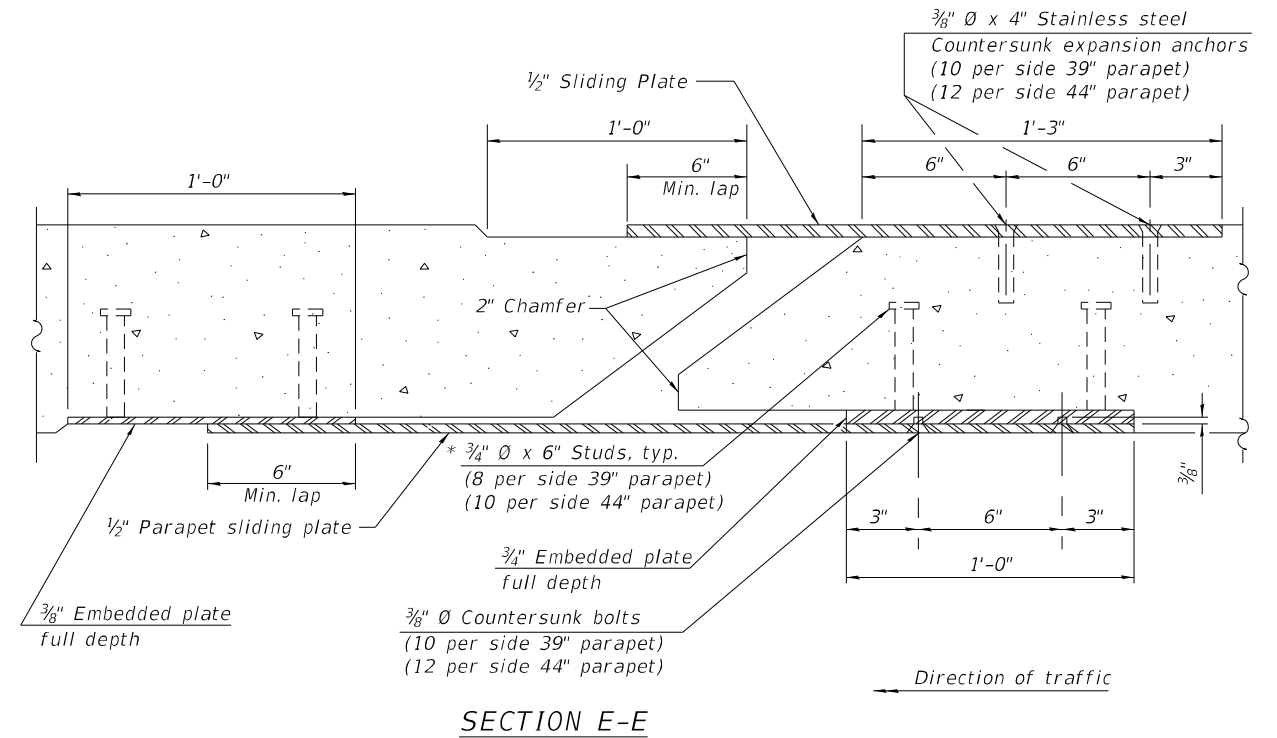
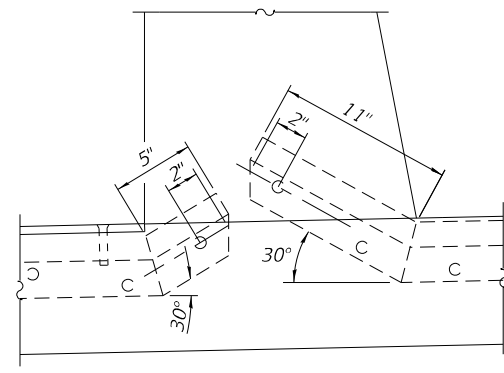
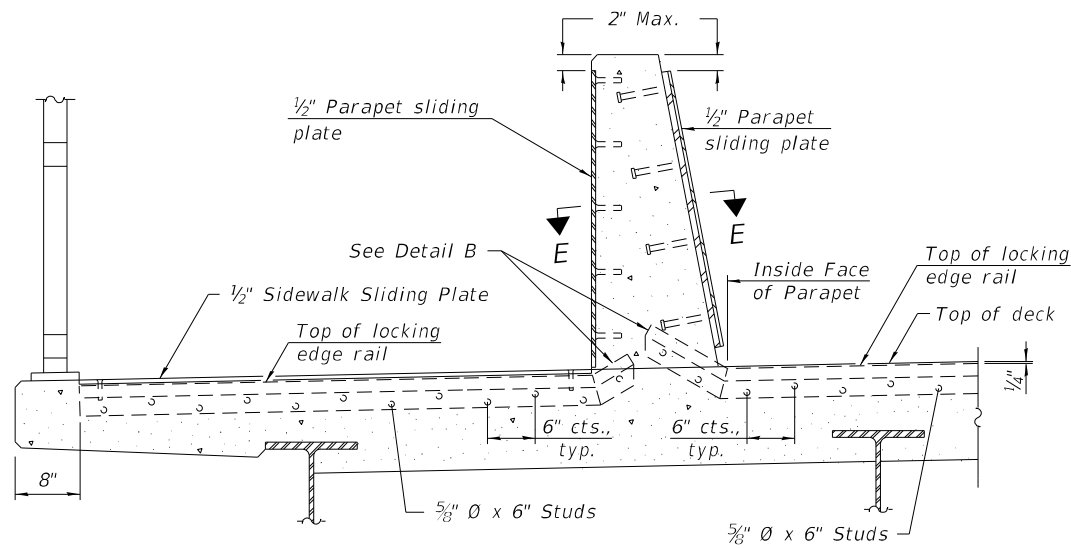
USER NAME =	DESIGNED - JM	REVISED -
PLOT SCALE =	CHECKED - RJO	REVISED -
PLOT DATE =	DRAWN - JM	REVISED -
	CHECKED - RJO	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL - SIDEWALK  
STRUCTURE NO. 016-0955**

SHEET 11 OF 19 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	28
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				



MODEL: Default  
FILE NAME: D:\23122-shl-016-0955-12-ProfileStripSeal\_3.dgn

EJ-SS-S

1-1-2020

(Sheet 3 of 3)



USER NAME =	DESIGNED - JM	REVISED -
CHECKED - RJO	CHECKED - RJO	REVISED -
PLOT SCALE =	DRAWN - JM	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

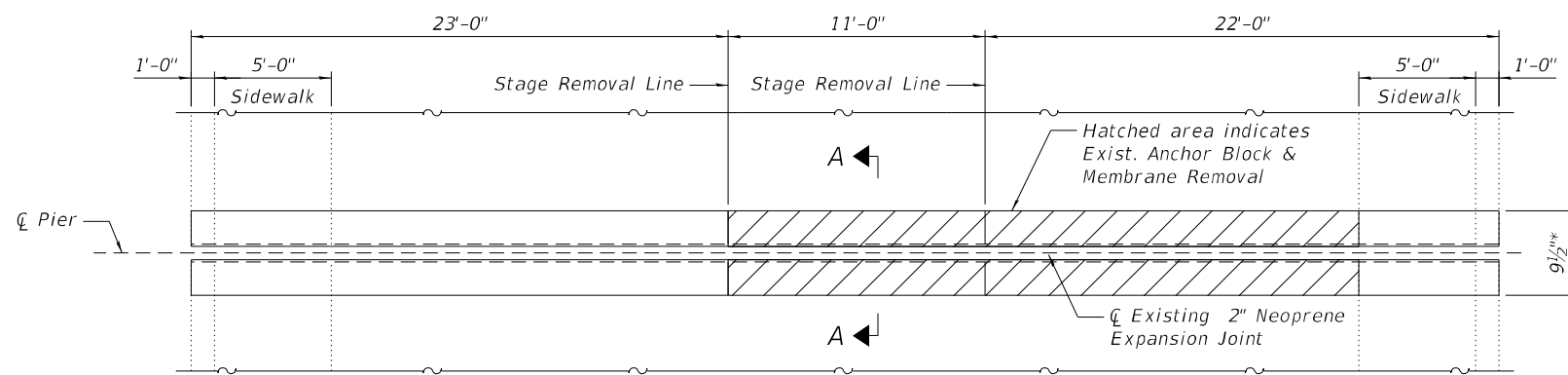
PREFORMED JOINT STRIP SEAL - SIDEWALK  
STRUCTURE NO. 016-0955

SHEET 12 OF 19 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	29
CONTRACT NO. 62R75				

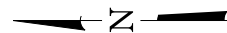
ILLINOIS FED. AID PROJECT

1/26/2024 6:16:58 PM

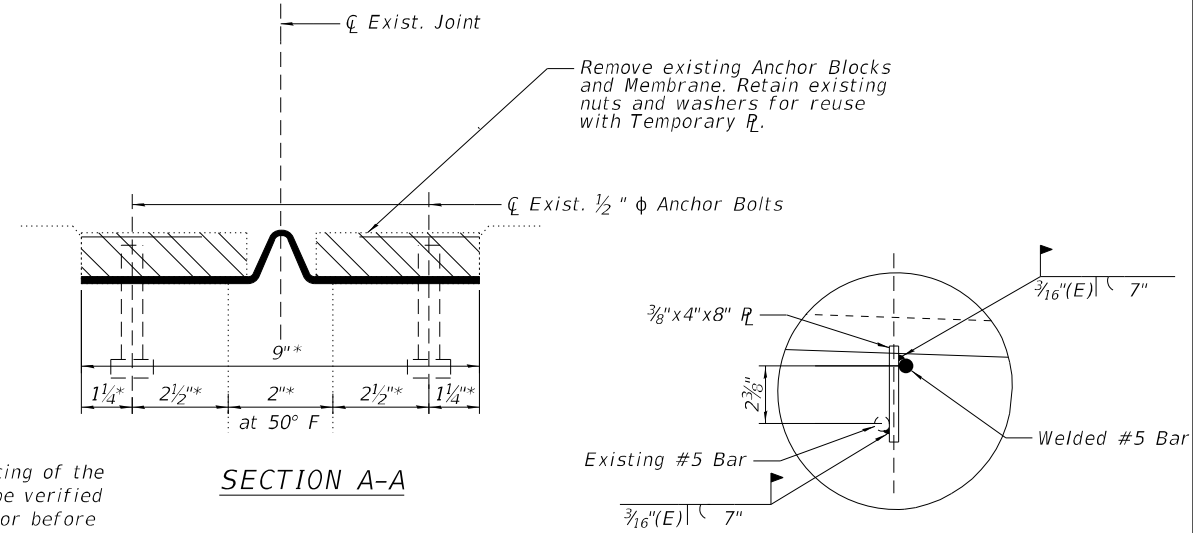


**EXISTING PARTIAL PLAN**

(At Pier 3 and Pier 9)

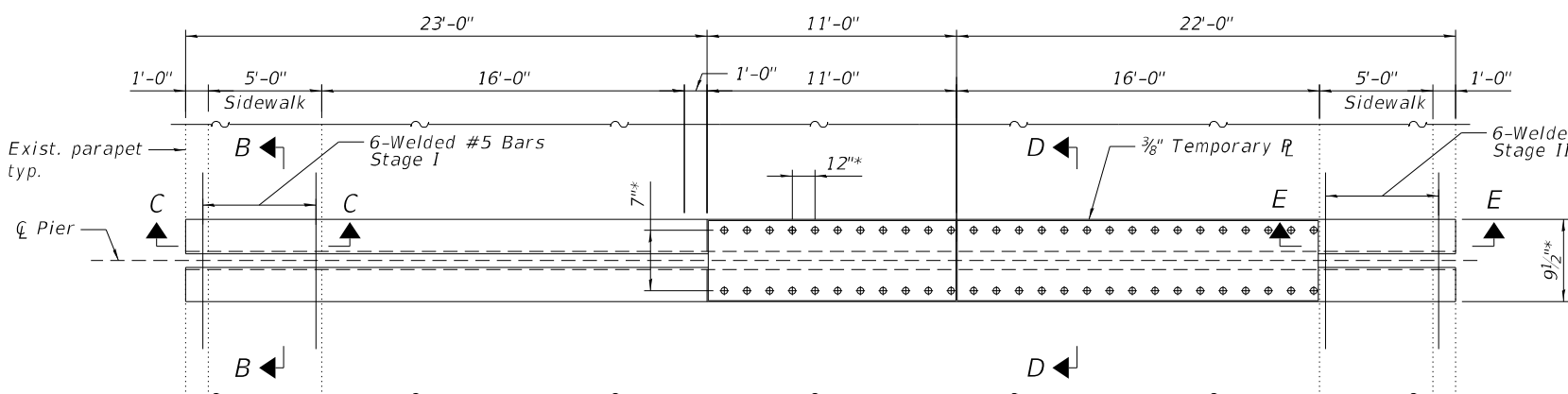


\*The exact location and spacing of the existing anchor bolts shall be verified in the field by the Contractor before fabrication of the Temporary Plates.



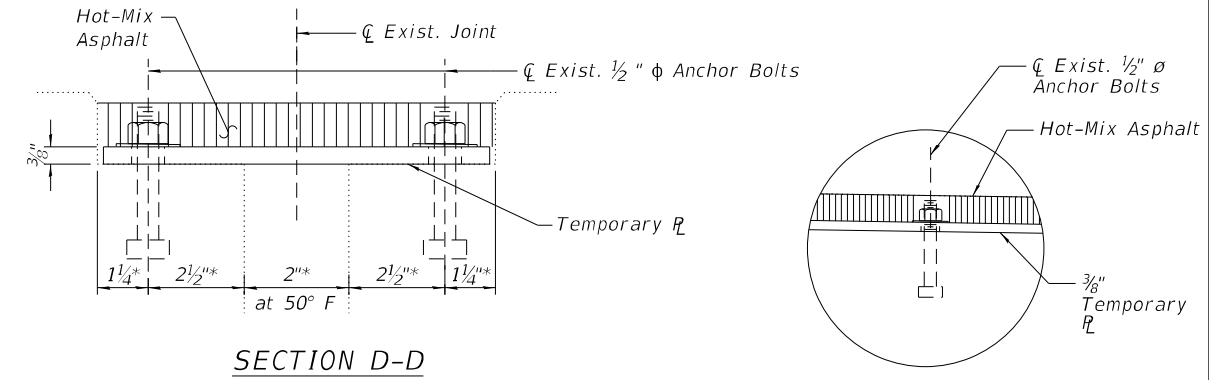
**SECTION A-A**

**DETAIL A**



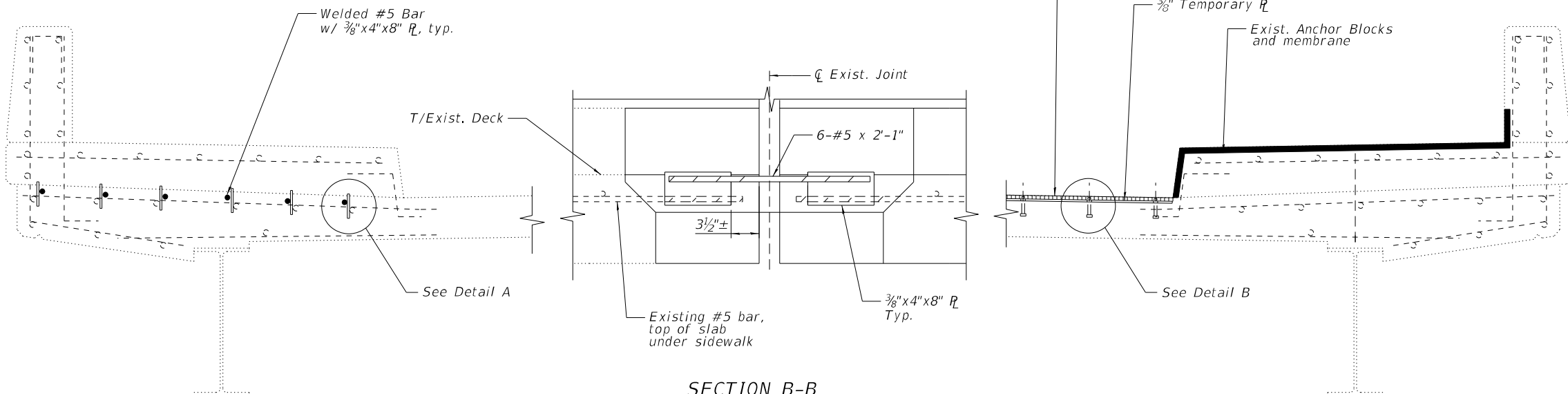
**PROPOSED PARTIAL PLAN**

(At Pier 3 and Pier 9)



**SECTION D-D**

**DETAIL B**



**SECTION C-C**

**SECTION B-B**

**SECTION E-E**

- Notes:
1. Installation of the Temporary Plates shall be completed during overnight closure. This work shall be done simultaneously with the installation of the expansion bearings at W. Abut., Pier 1, Pier 10, and E. Abut.
  2. For installation of 3/8" Temporary Plate, use existing bolts and washers. If existing anchor bolt is damaged, cut off existing bolt flush with concrete and install replacement 3/4" diameter anchor bolt.
  3. All steel plates to be Grade 50.
  4. All work required for installing & removing Temporary Plates shall be included with the cost of Concrete Structures (Special).
  5. Hot Mix Asphalt shall be installed over the Temporary Plate according to Section 442.08 of the Standard Specifications. See "Approach Pavement Resurfacing and Pavement Patching Detail" Hot-Mix Asphalt Requirements table, for mix design. The cost of the Hot Mix Asphalt shall be included with the cost of Concrete Structures (Special).
  6. All work required for the installation of Welded #5 Bars shall be included with the cost of Concrete Structure (Special).

MODEL: Default  
FILE NAME: D:\23122-shl-016-0955-13-PierUHP\_C\_Temporary Links



USER NAME =	DESIGNED - RJO	REVISED -
CHECKED - JUM & EG	REVISED -	
PLOT SCALE =	DRAWN - EG	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**UHP LINK SLAB PLAN AND DETAILS (1 OF 2)  
STRUCTURE NO. 016-0955**

SHEET 13 OF 19 SHEETS

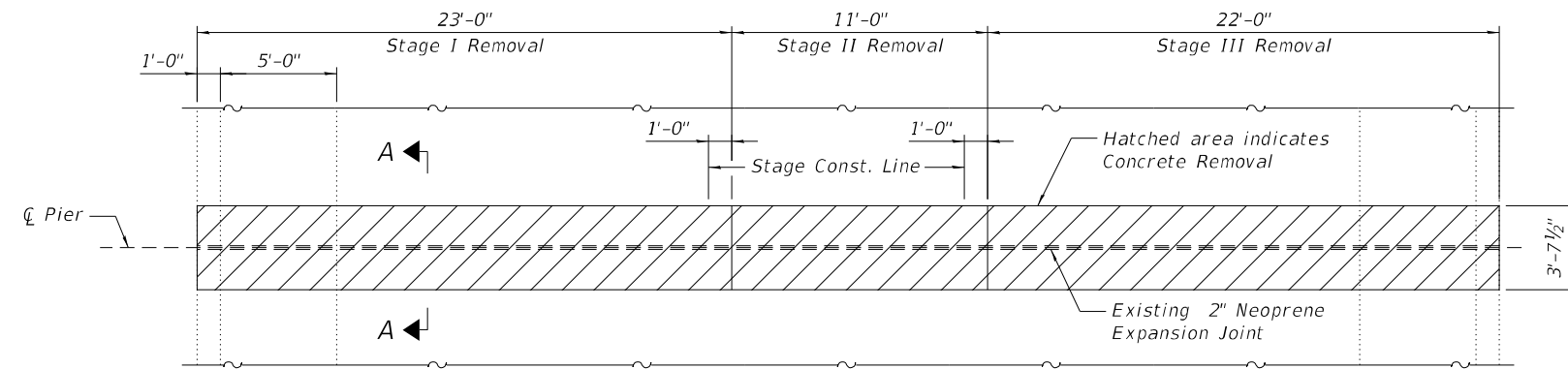
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	30
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				

**BILL OF MATERIAL**

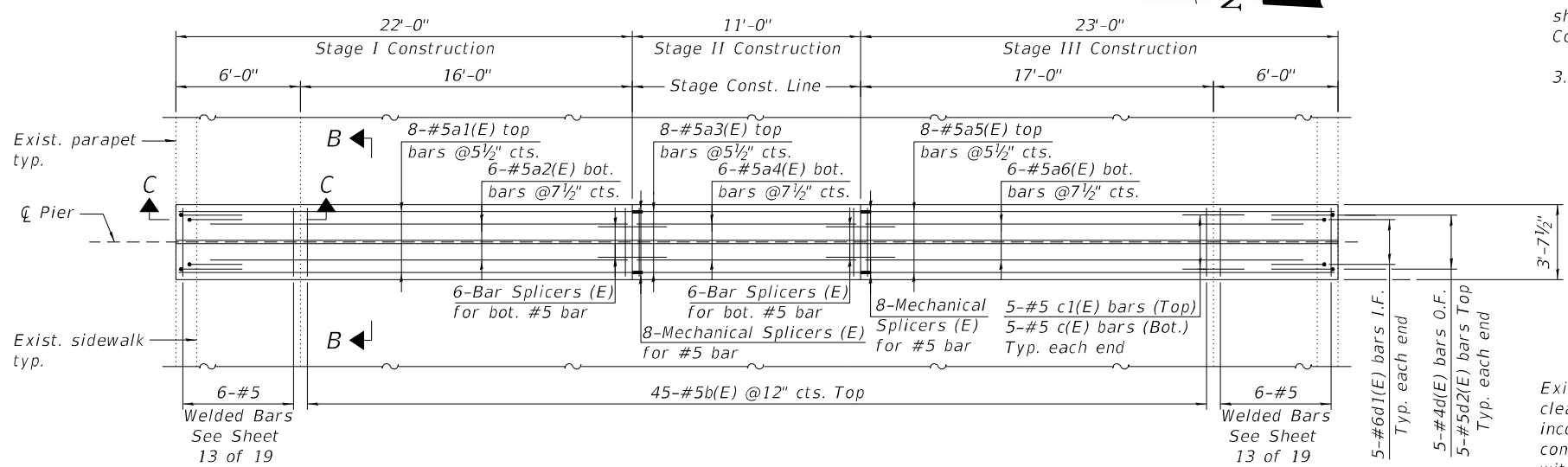
Bar	No.	Size	Length	Shape	
a1(E)	16	#5	21'-10"	—	
a2(E)	12	#5	21'-6"	—	
a3(E)	16	#5	11'-0"	—	
a4(E)	12	#5	10'-8"	—	
a5(E)	16	#5	22'-10"	—	
a6(E)	12	#5	22'-6"	—	
b(E)	90	#5	3'-4"	—	
c(E)	20	#5	2'-5"	┌	
c1(E)	20	#5	5'-9"	┌	
d(E)	20	#4	5'-4"	┌	
d1(E)	20	#6	3'-9"	┌	
d2(E)	20	#5	2'-1"	┐	
Item				Unit	Total
Concrete Removal				Cu. Yd.	16.0
Concrete Superstructure				Cu. Yd.	9.3
Reinforcement Bars				Pound	2330
Epoxy Coated					
Concrete Structures (Special)				Cu. Yd.	7.9
Polymer Concrete				Cu. Ft.	26

**Notes:**

- Ultra High Performance Concrete (UHPC) shall be paid for as Concrete Structures (Special).
- Compressed Synthetic Sheet gasket (two 1/16" thick sheets, treated both sides). Cost to be included with Concrete Structures (Special).
- See Sheet 8 of 19 for bar bending diagrams.

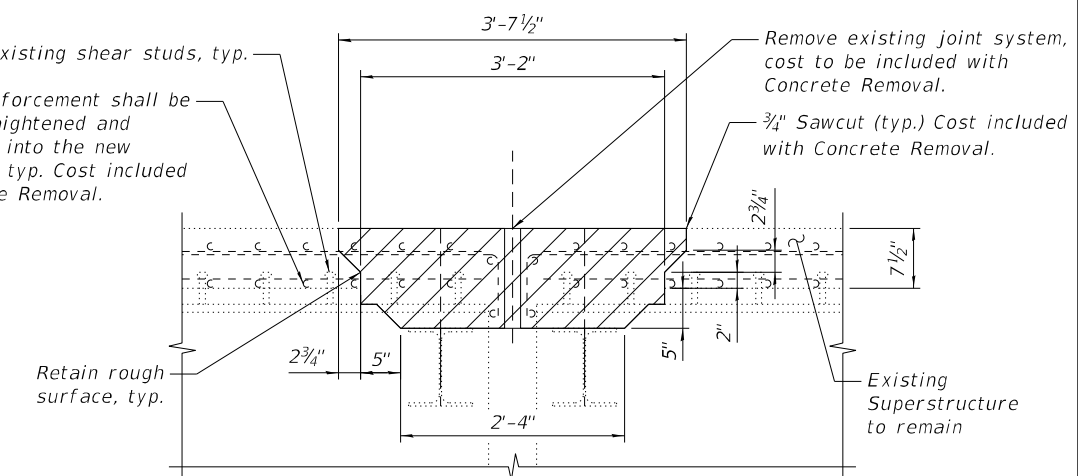


**EXISTING PARTIAL PLAN**

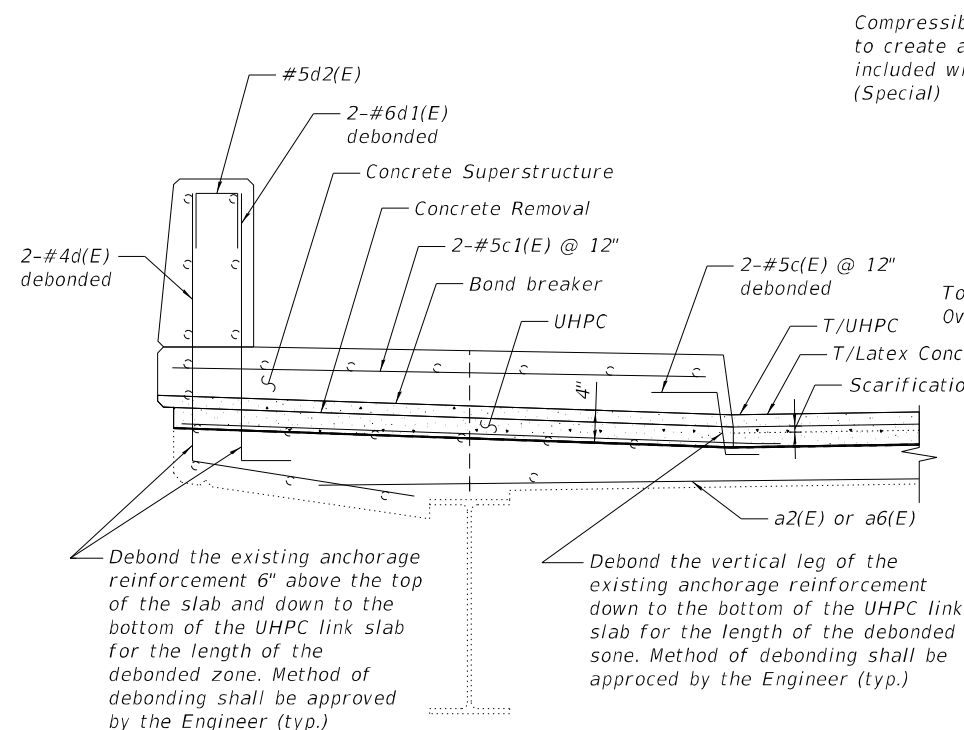


**PROPOSED PARTIAL PLAN**

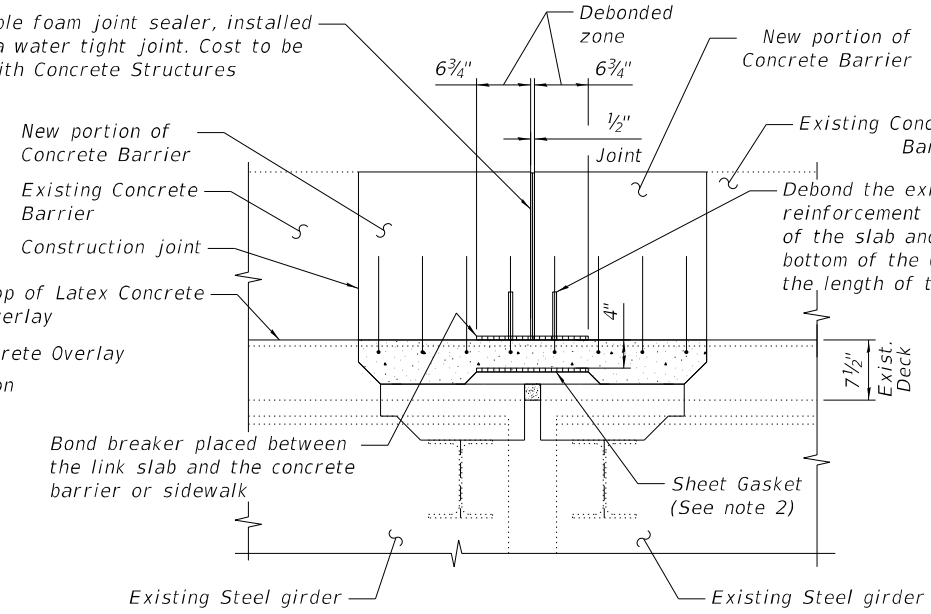
Existing shear studs, typ.  
Existing reinforcement shall be cleaned, straightened and incorporated into the new construction, typ. Cost included with Concrete Removal.



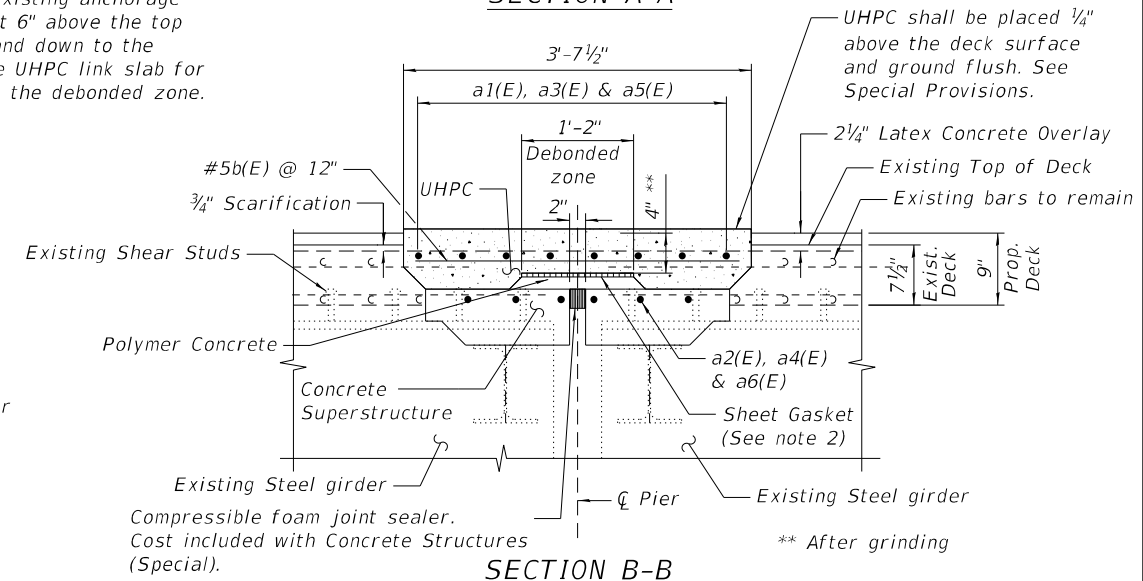
**SECTION A-A**



**SECTION C-C**



**ELEVATION**  
**RELIEF JOINT OVER UHPC LINK SLAB AT SIDEWALK**  
(Sidewalk not shown for clarity)



**SECTION B-B**

MODEL: Default  
FILE NAME: D:\23122-5\14-Pier\UHPC\_LinkSlab

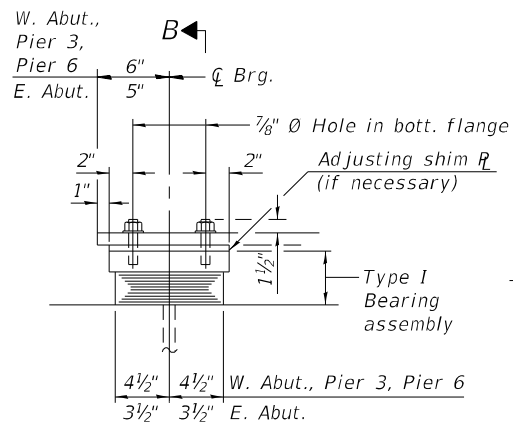
**Garza Karhoff Engineering, LLC**  
1/26/2024 6:17:04 PM

USER NAME =	DESIGNED - RJO	REVISIONS -
PLOT SCALE =	CHECKED - JUM & EG	REVISIONS -
PLOT DATE =	DRAWN - EG	REVISIONS -
	CHECKED - RJO	REVISIONS -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**UHPC LINK SLAB PLAN AND DETAILS (2 OF 2)**  
**STRUCTURE NO. 016-0955**  
SHEET 14 OF 19 SHEETS

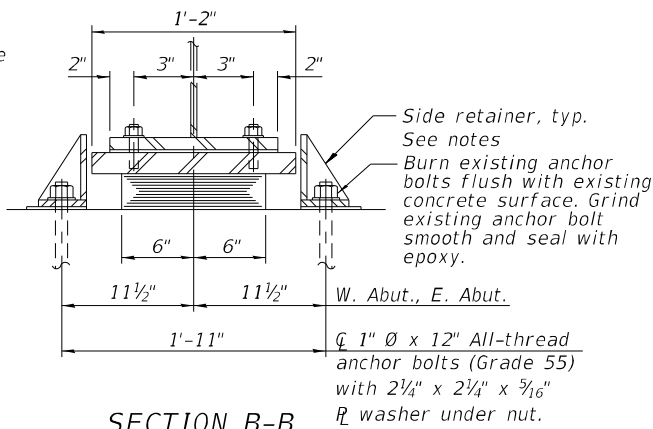
F.A.P. RTE. 397	SECTION FAP 397 22 BRIDGE REHAB	COUNTY COOK	TOTAL SHEETS 44	SHEET NO. 31
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				



ELEVATION

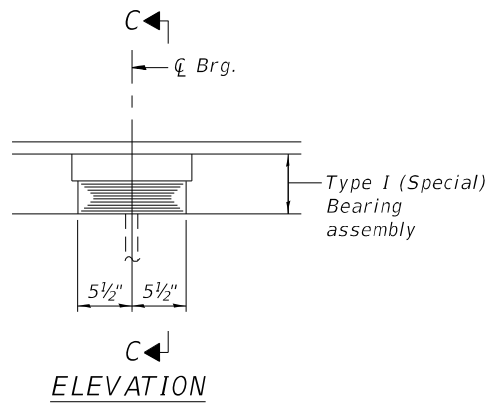
(W. Abut., Pier 3, & Pier 6 looking North  
E. Abut. looking South)

**TYPE I ELASTOMERIC EXP. BRG.**



SECTION B-B

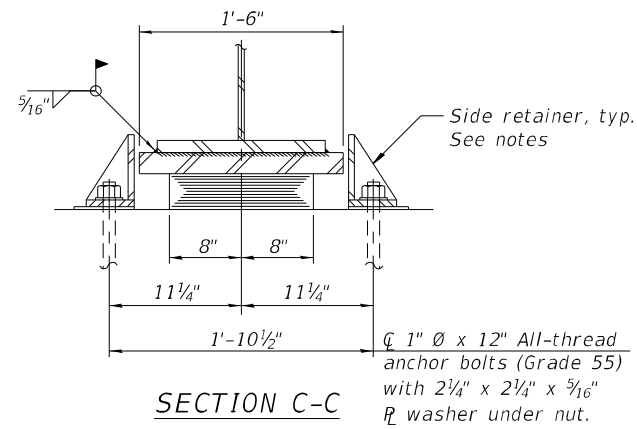
Side Retainers not required at Pier 3 & Pier 6



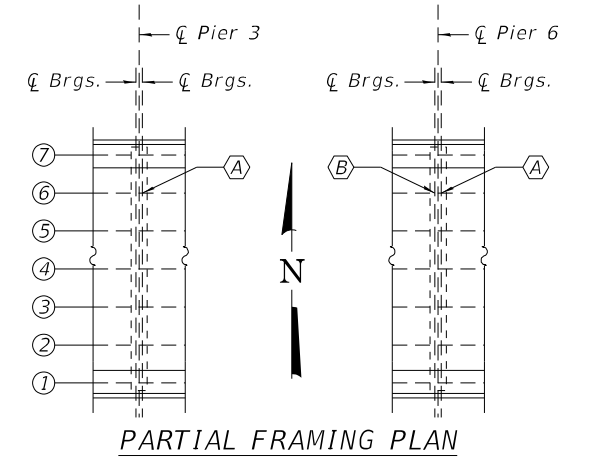
ELEVATION

**TYPE I (SPECIAL) ELASTOMERIC BRG.**

(Pier 1 & 10)

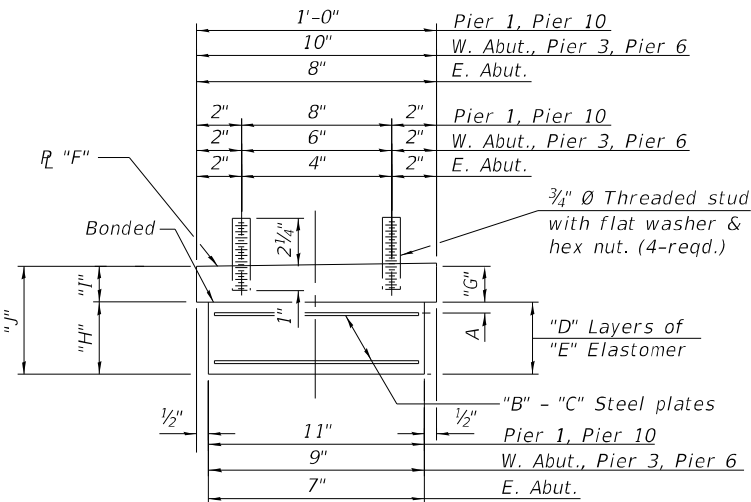


SECTION C-C



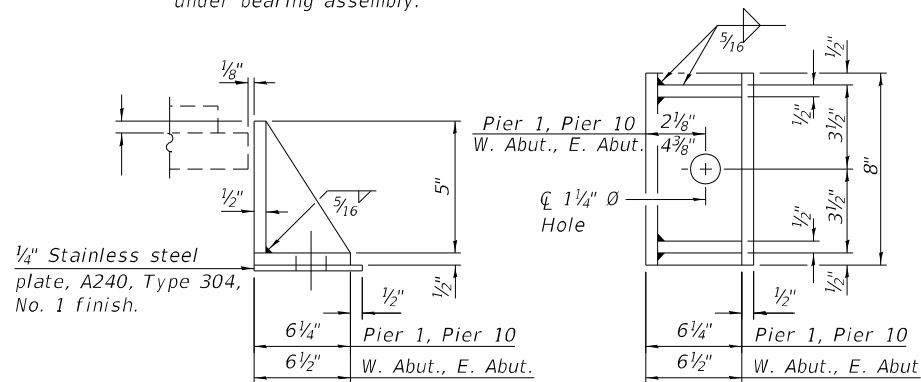
PARTIAL FRAMING PLAN

- (A) Remove & Replace Existing Bearing, Type I Elastomeric Exp. Brg.
  - (B) Remove & Replace Existing Bearing, Type II Elastomeric Exp. Brg.
- See sheet 14 of 19.



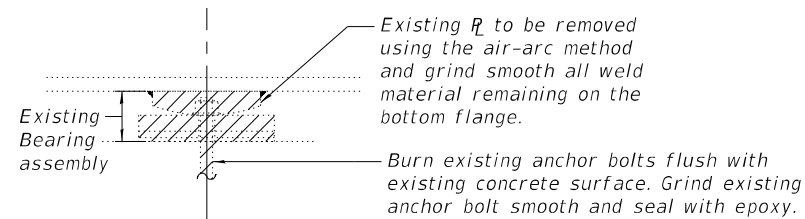
BEARING ASSEMBLY

Note:  
Shim plates shall not be placed under bearing assembly.



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



EXISTING FIXED BEARING REMOVAL DETAIL

Cost included with Jack & Remove Existing Bearings

Notes:

Side retainers and stainless steel plates shall be included in the cost of Elastomeric Bearing Assembly, Type I, and Elastomeric Bearing Assembly, Type I (Special).

Anchor bolts and side retainers at all supports shall be installed as each member is erected unless an equivalent temporary means of lateral restraint is used.

Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM 1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.

Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.

Welding procedures shall be established by the contractor to restrict the maximum temperature reached by surfaces in contact with elastomer to 200°F (93°C). Temperatures shall be controlled by temperature indicating wax pencils or other suitable means approved by the engineer.

No adjustment shims shall be used with Type I (Special) Bearing Assemblies. The Contractor shall verify the height of the existing bearings including any adjustment shims in the field before ordering materials. If any adjustment to the bearing heights shown in the table are necessary to maintain the existing heights, that adjustment shall be added to the thickness of R "F".

Installation of Expansion Bearings at W. Abut., Pier 1, Pier 10, and E. Abut. shall be completed during overnight closure. This work shall be done simultaneously with the Installation of the Temporary Plates at Piers 3 and 9. See Sheet 13 of 19.

Installation of Expansion Bearings at Pier 3 and Pier 6 may be complete during Stage Construction.

Minimum Jack Capacity = 65 Tons.

	INTERIOR BEAM REACTIONS			
	DL (kips)	LL (kips)	Imp (kips)	TOTAL (kips)
W. Abut	26.2	42.0	12.2	80.4
Pier 1	72.6	49.8	14.3	136.7
Pier 3 (Span 4)	26.7	42.3	12.1	81.1
Pier 6 (Span 6)	17.3	37.7	11.3	66.3
Pier 6 (Span 7)	27.2	42.4	12.2	81.8
Pier 10	81.4	50.1	14.5	146.0
E. Abut	24.7	42.0	12.2	78.9

While developing shoring details at this location the contractor must consider the Railroad's limitations for clearances.

**TYPE I ELASTOMERIC EXP. BRG.**

Location	Bearing Assembly	No.	"A"	"B"	"C"	"D"	"E"	"F"	"G"	"H"	"I"	"J"
W. Abut	Type I	7	3/8"	5	3/32"	6	3/8"	1 13/16" x 10" x 1'-2"	1 13/16"	2 3/4"	1 1/2"	2 1/4"
Pier 1	Type I (Special)	7	1/2"	3	1/8"	4	1/2"	1 3/8" x 12" x 1'-6"	1 3/8"	2 3/8"	1 1/8"	3 1/2"
Pier 3 (Span 4)	Type I	1	3/8"	4	3/32"	5	3/8"	1 3/4" x 10" x 1'-2"	1 3/4"	2 1/4"	1 1/16"	3 1/16"
Pier 6 (Span 7)	Type I	1	3/8"	6	3/32"	7	3/8"	1 3/16" x 10" x 1'-2"	1 3/16"	3 1/16"	1 1/16"	4 7/8"
Pier 10	Type I (Special)	7	1/2"	3	1/8"	4	1/2"	1 3/8" x 12" x 1'-6"	1 3/8"	2 3/8"	1 1/16"	3 1/16"
E. Abut	Type I	7	3/8"	4	3/32"	5	3/8"	1 13/16" x 8" x 1'-2"	1 13/16"	2 1/4"	1 1/16"	3 1/16"

**BILL OF MATERIAL**

Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	16
Anchor Bolts, 1" Dia.	Each	56
Jack & Remove Existing Bearings	Each	30
Elastomeric Bearing Assembly, Type I (Special)	Each	14

MODEL: Default  
FILE NAME: D:\23122-5\11-016-0955-15-BearingRepair1.dgn



USER NAME =	DESIGNED - RJO	REVISED -
	CHECKED - EG	REVISED -
PLOT SCALE =	DRAWN - EG	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

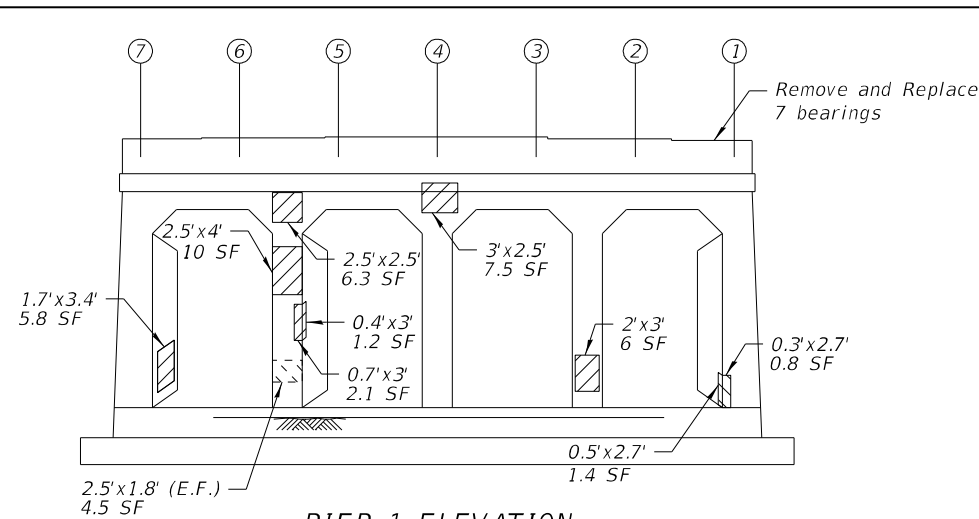
BEARING REPLACEMENT DETAILS I  
STRUCTURE NO. 016-0955

SHEET 15 OF 19 SHEETS

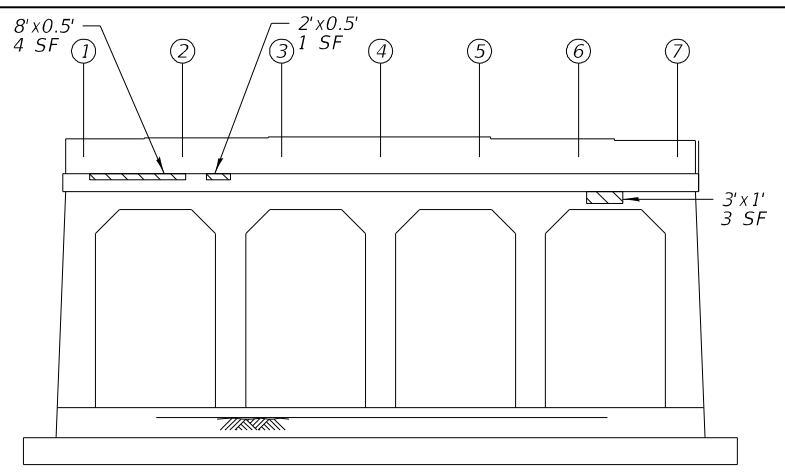
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	32
ILLINOIS FED. AID PROJECT				



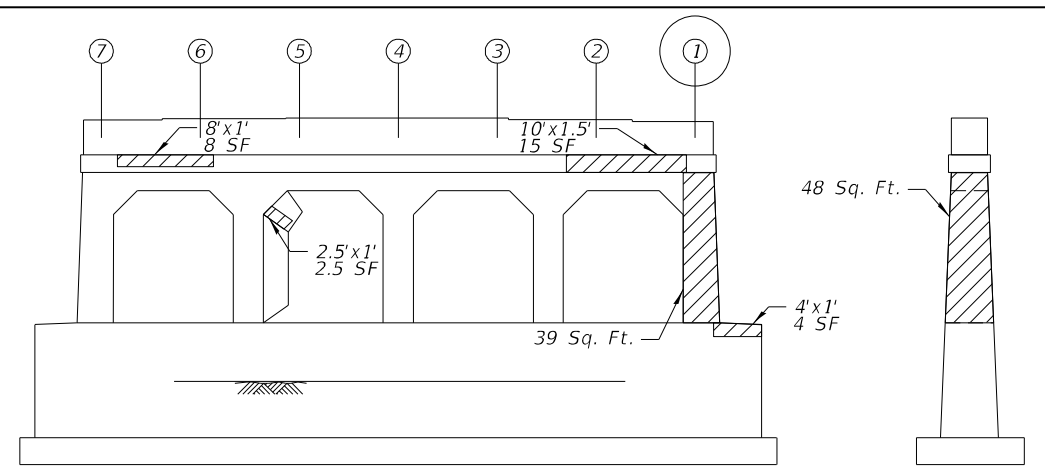




**PIER 1 ELEVATION**  
LOOKING EAST

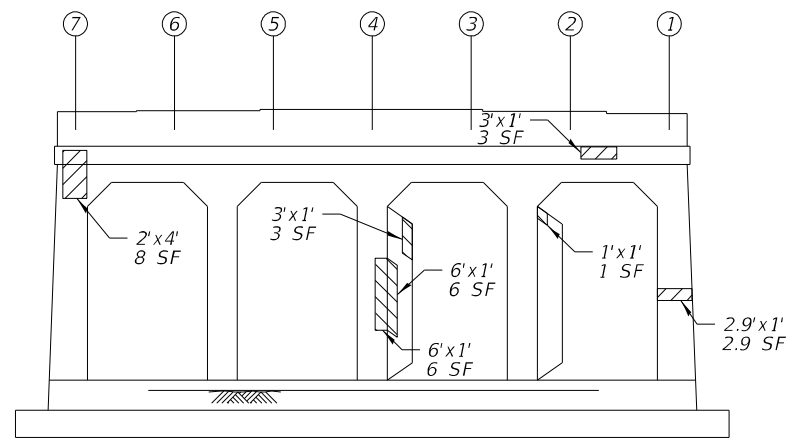


**PIER 3 ELEVATION**  
LOOKING WEST

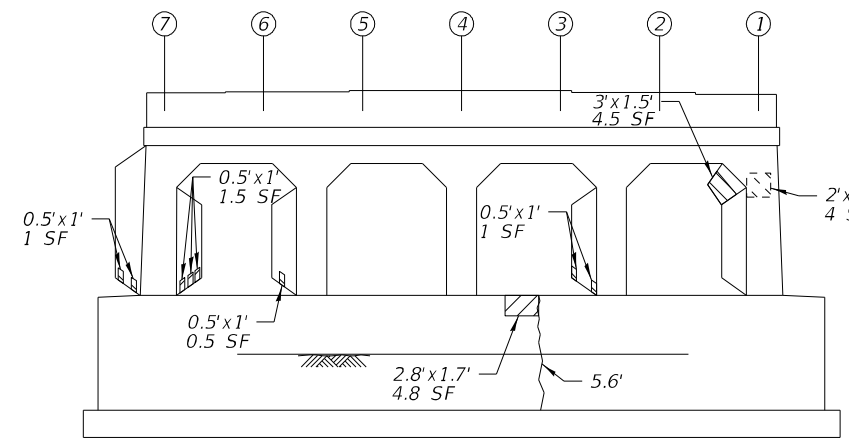


**PIER 6 ELEVATION**  
LOOKING EAST

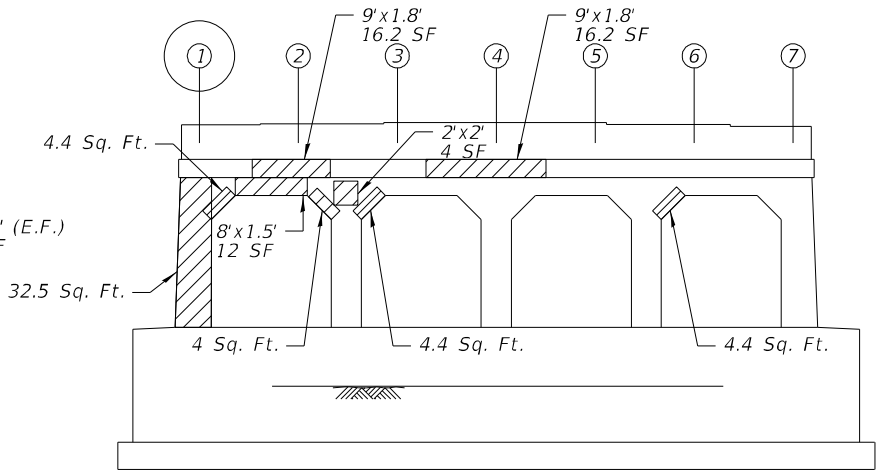
**PIER 6 - SOUTH FACE**



**PIER 2 ELEVATION**  
LOOKING EAST



**PIER 4 ELEVATION**  
LOOKING EAST

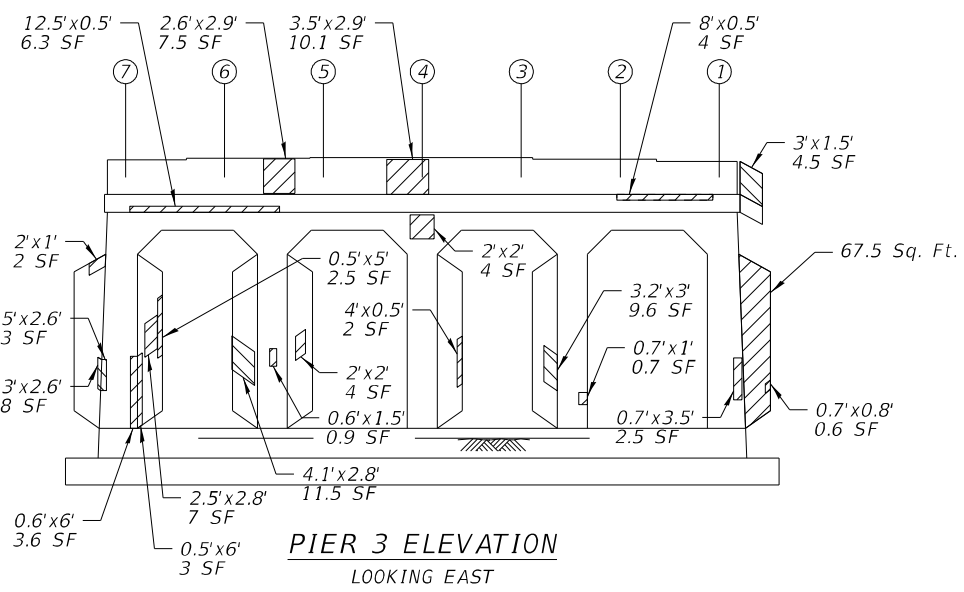


**PIER 6 ELEVATION**  
LOOKING WEST

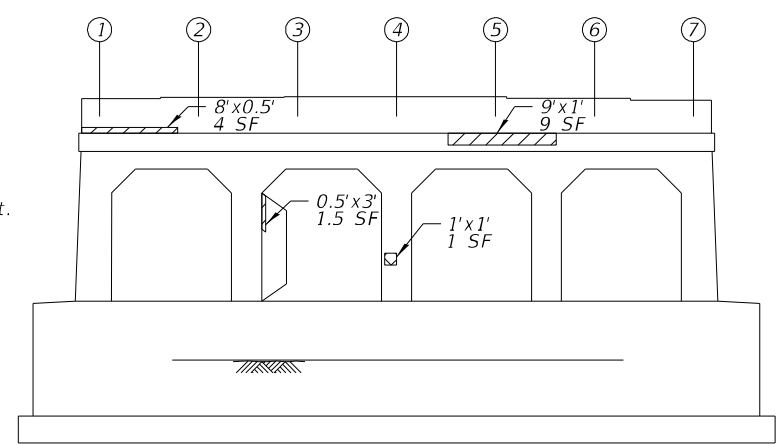
**NOTE**  
1. See sheet 18 of 19 for Bill of Material

**LEGEND**

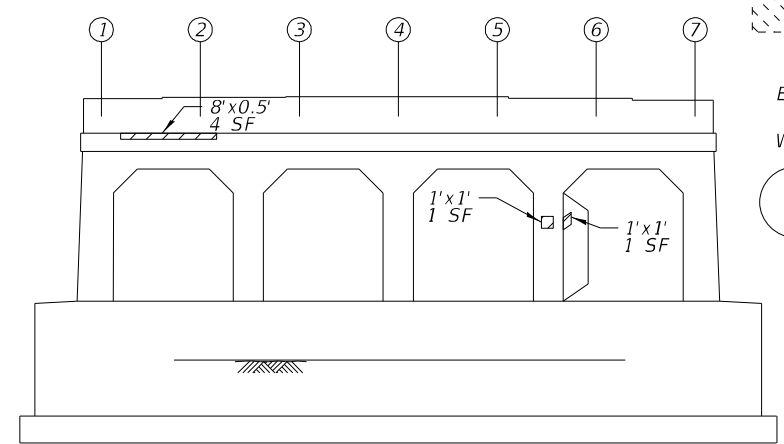
- Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)
- Structural Repair of Concrete (Depth Equal to or Less than 5 Inches) (Opposite Face)
- E.F. East Face
- W.F. West Face
- Temporary Shoring and Cribbing



**PIER 3 ELEVATION**  
LOOKING EAST



**PIER 5 ELEVATION**  
LOOKING WEST



**PIER 7 ELEVATION**  
LOOKING WEST

MODEL: Default  
FILE NAME: D:\23122-5\16-0955-17-SubRepairs1.dgn



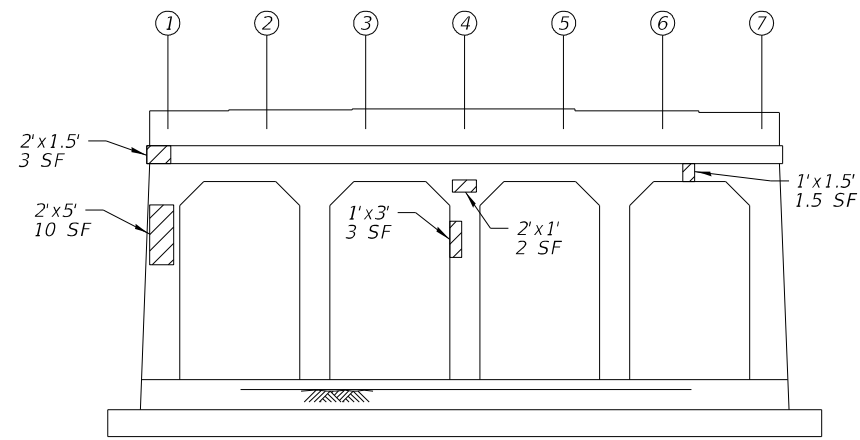
USER NAME =	DESIGNED - JJM	REVISED -
PLOT SCALE =	CHECKED - RJO	REVISED -
PLOT DATE =	DRAWN - JJM	REVISED -
	CHECKED - RJO	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

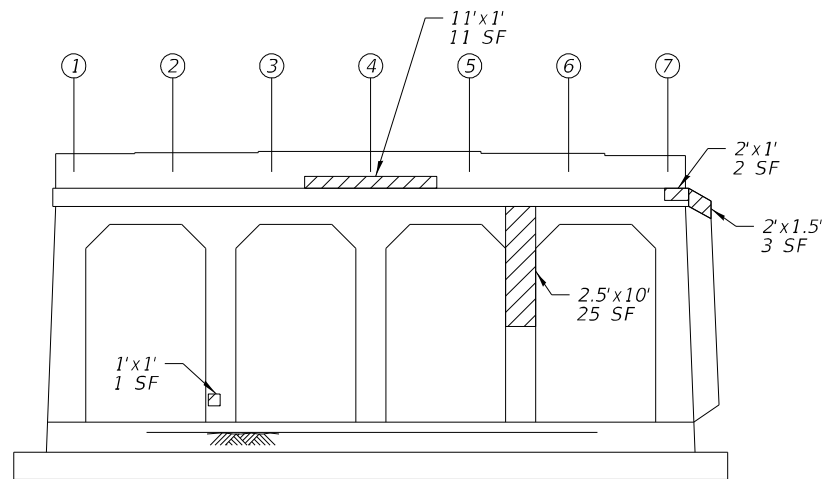
**SUBSTRUCTURE REPAIRS I**  
**STRUCTURE NO. 016-0955**

SHEET 17 OF 19 SHEETS

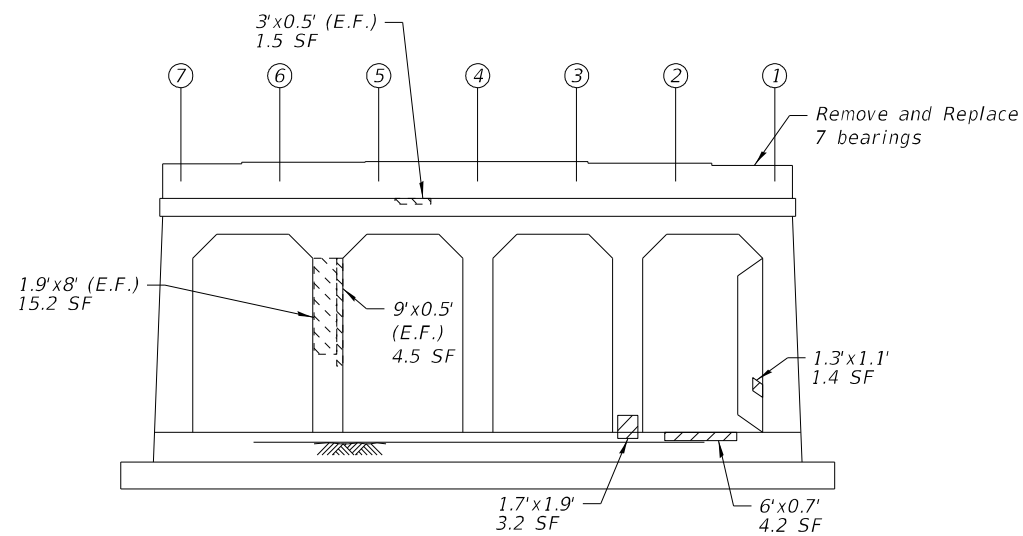
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	34
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				



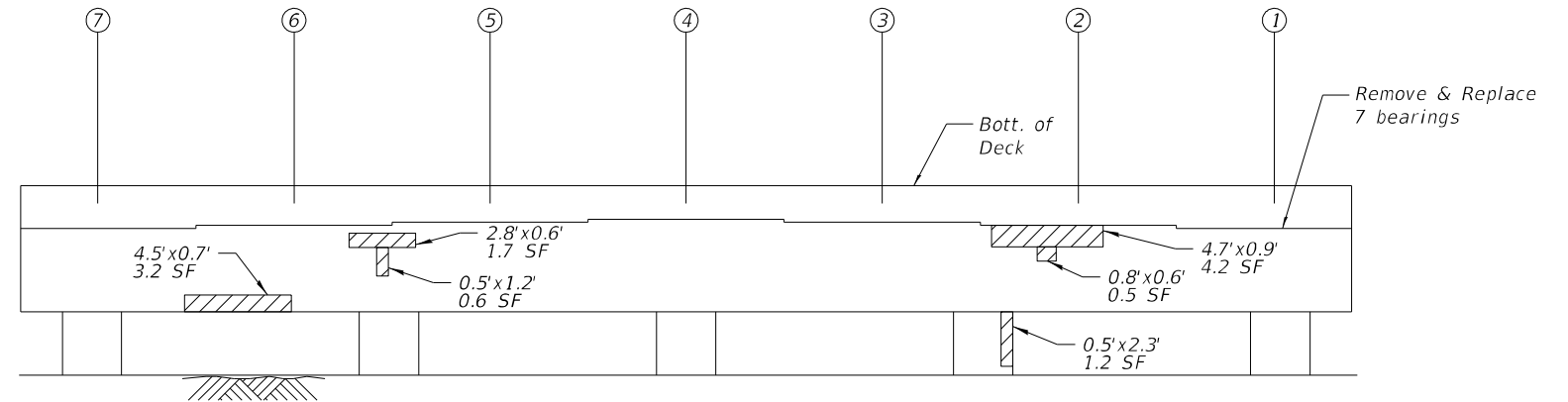
**PIER 8 ELEVATION**  
LOOKING WEST



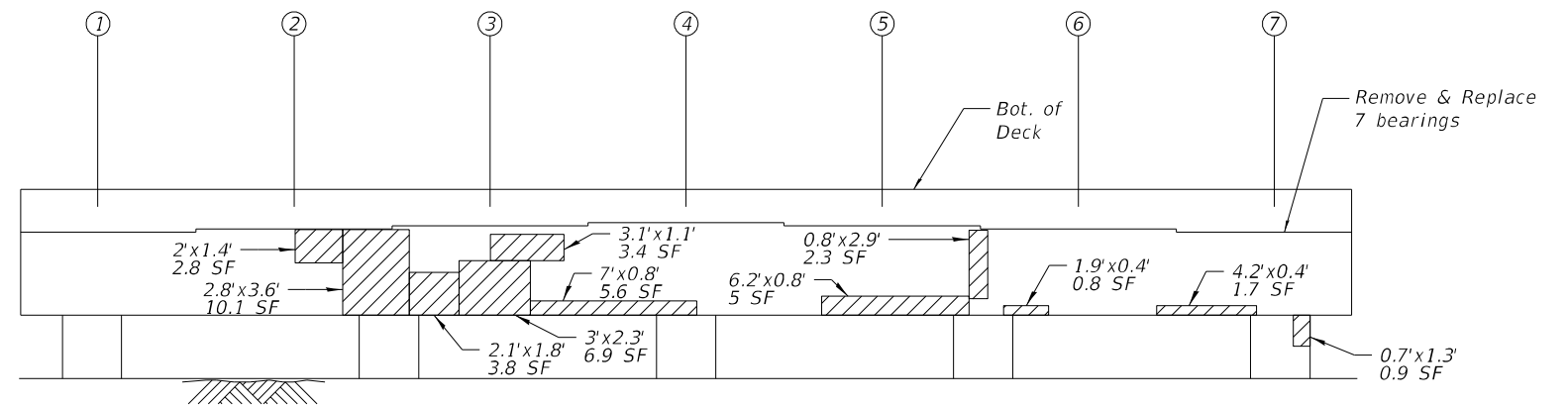
**PIER 9 ELEVATION**  
LOOKING WEST



**PIER 10 ELEVATION**  
LOOKING EAST



**EAST ABUTMENT ELEVATION**  
LOOKING EAST



**WEST ABUTMENT ELEVATION**  
LOOKING WEST

**LEGEND**



Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)



Structural Repair of Concrete (Depth Equal to or Less than 5 Inches) (Opposite Face)

E.F. East Face

W.F. West Face

**BILL OF MATERIAL**

Item	Unit	Total
Concrete Sealer	Sq Ft	638
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq Ft	638

MODEL: Default  
FILE NAME: D:\23122-5\11-016-0955-18-SubRepairs2.dgn



USER NAME =	DESIGNED - JJM	REVISED -
	CHECKED - RJO	REVISED -
PLOT SCALE =	DRAWN - JJM	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

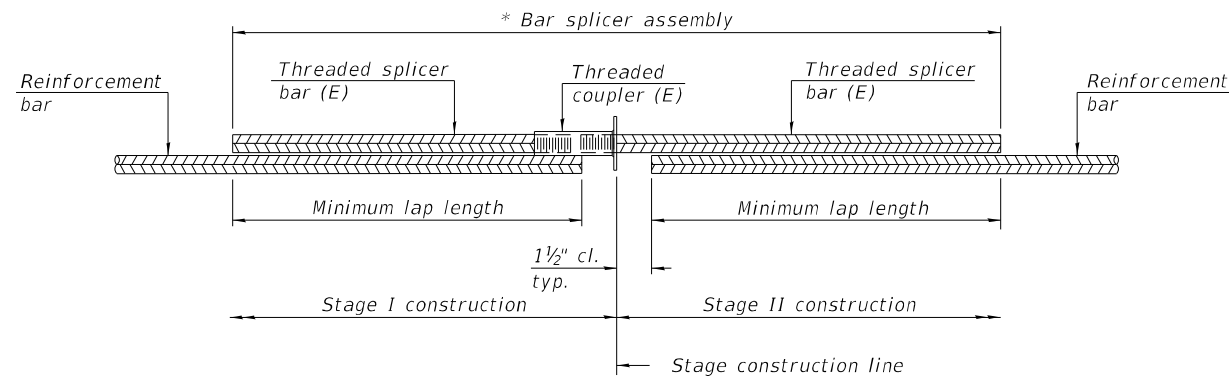
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUBSTRUCTURE REPAIRS II**  
**STRUCTURE NO. 016-0955**

SHEET 18 OF 19 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	35
CONTRACT NO. 62R75				

ILLINOIS FED. AID PROJECT

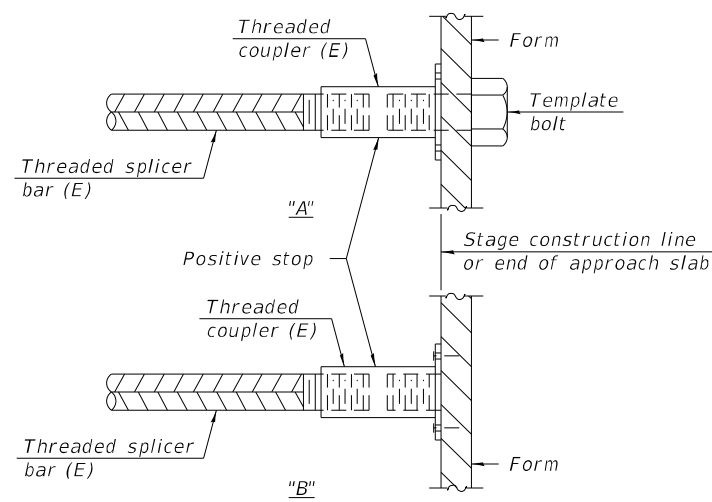


**STANDARD BAR SPLICER ASSEMBLY PLAN**  
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

\* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Deck @ W. Abut	#5	26	3'-2"
Bot. of Deck @ Pier 3	#5	12	3'-4"
Deck @ Pier 6	#5	36	3'-4"
Bot. of Deck @ Pier 9	#5	12	3'-4"
Deck @ E. Abut	#5	26	3'-2"

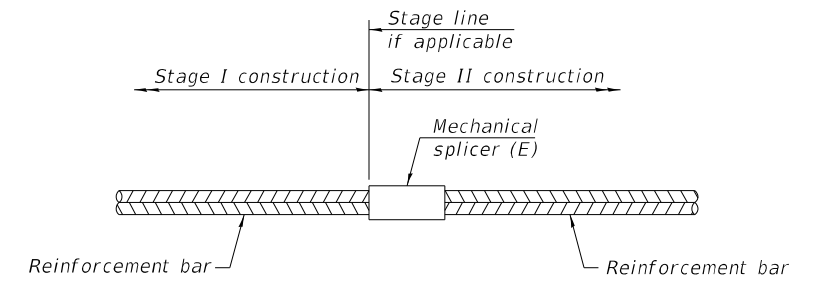


**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



**STANDARD MECHANICAL SPLICER**

Location	Bar size	No. assemblies required
UHPC @ Pier 3	#5	16
UHPC @ Pier 9	#5	16

**Notes:**

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default  
 FILE NAME: D:\23122-shl-016-0955-19-BarSplicer.dgn

BSD-1

1-1-2020



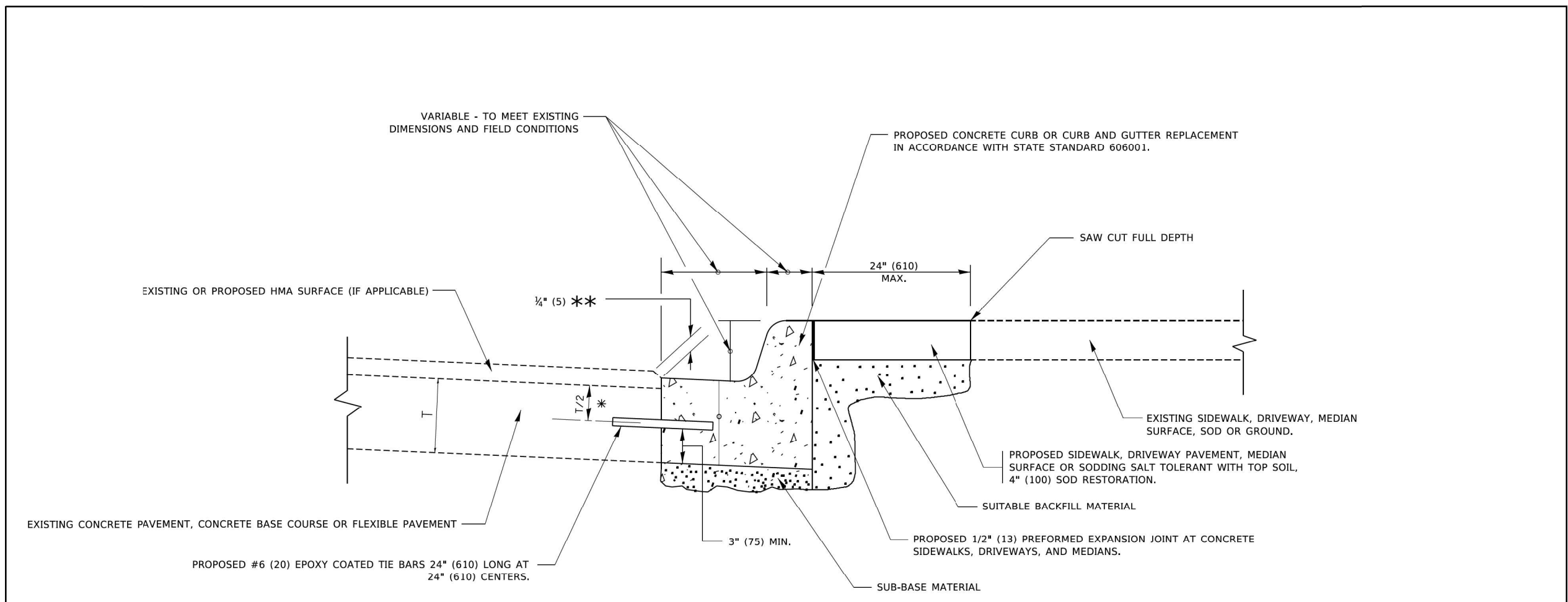
USER NAME =	DESIGNED - JJM	REVISED -
	CHECKED - RJO	REVISED -
PLOT SCALE =	DRAWN - JJM	REVISED -
PLOT DATE =	CHECKED - RJO	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS  
 STRUCTURE NO. 016-0955

SHEET 19 OF 19 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	36
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				



\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

## CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = foctemj PLOT SCALE = 30,0000' / in. PLOT DATE = 7/11/2019	DESIGNED - A. HOUSEH DRAWN - CHECKED - DATE - 03-11-94	REVISED - A. ABBAS 03-21-97 REVISED - M. GOMEZ 01-22-01 REVISED - R. BCRO 12-15-09 REVISED - K. SMITH 07-11-19	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A. RTE. _____ SECTION _____ COUNTY _____ <b>BD600-06 (BD-24)</b> CONTRACT NO. _____ <small>ILLINOIS FED. AID PROJECT</small>
SCALE: NONE				SHEET 1 OF 1 SHEETS STA. _____ TO STA. _____	

MODEL: Default  
 FILE NAME: D:\2024\24-01-01\24-01-01.dgn

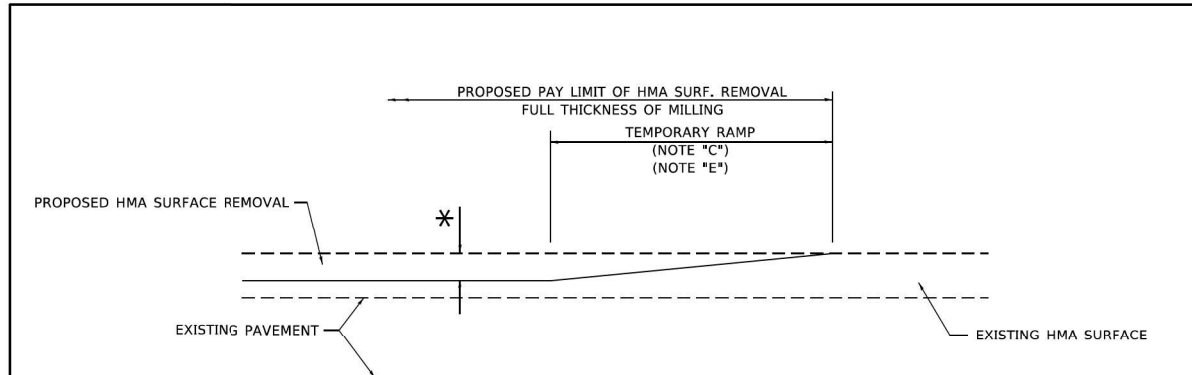


USER NAME = Michael.Czerpak PLOT SCALE = 0.2000' / in. PLOT DATE = 1/26/2024	DESIGNED - AMB DRAWN - AMB CHECKED - MM DATE - 01/26/2024	REVISED - REVISED - REVISED - REVISED -
--	--	--

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

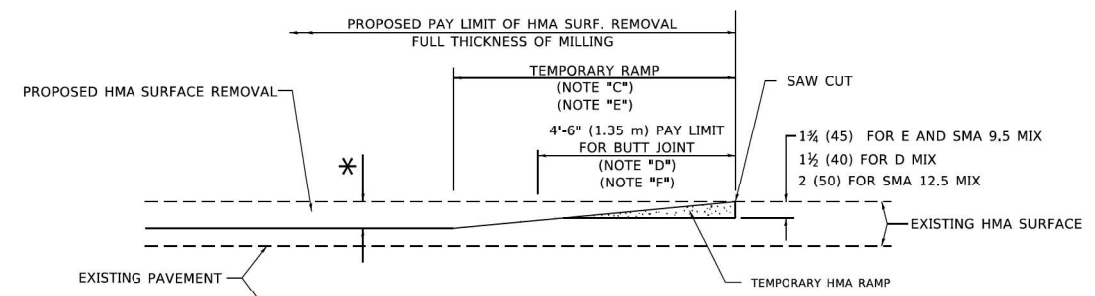
<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	
SCALE: N.T.S.	SHEET 1 OF 8 SHEETS STA. _____ TO STA. _____

F.A.P. RTE. 397	SECTION FAP 397 22 BRIDGE REHAB	COUNTY COOK	TOTAL SHEETS 44	SHEET NO. 37
ILLINOIS FED. AID PROJECT				



**MILLED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

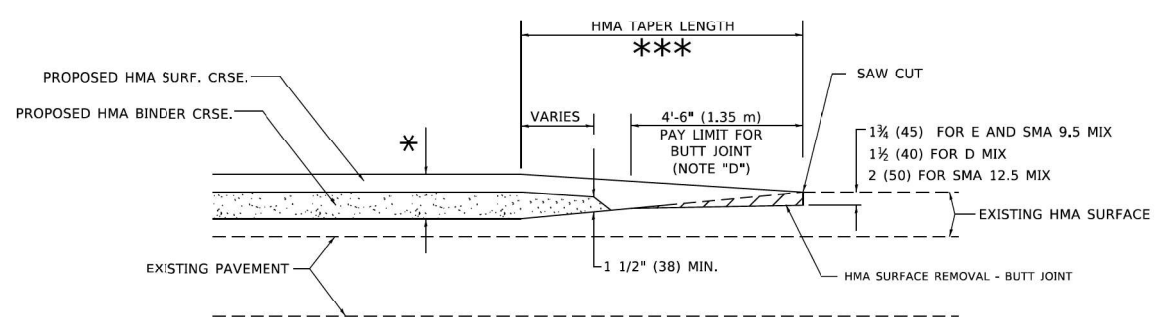
**OPTION 1**



**HMA CONSTRUCTED TEMPORARY RAMP**  
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

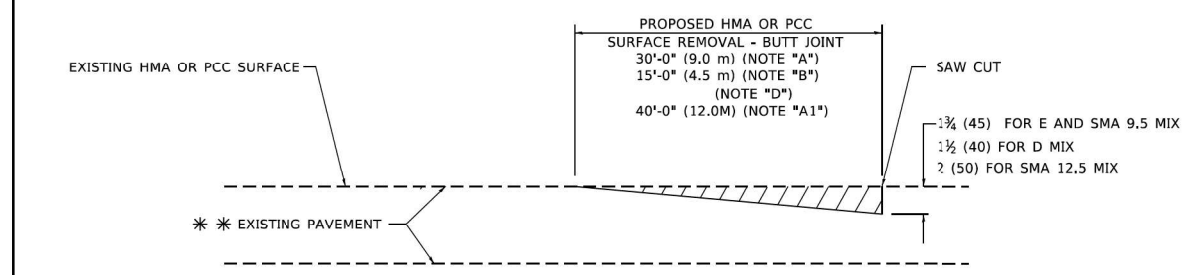
**OPTION 2**

**TYPICAL TEMPORARY RAMP**

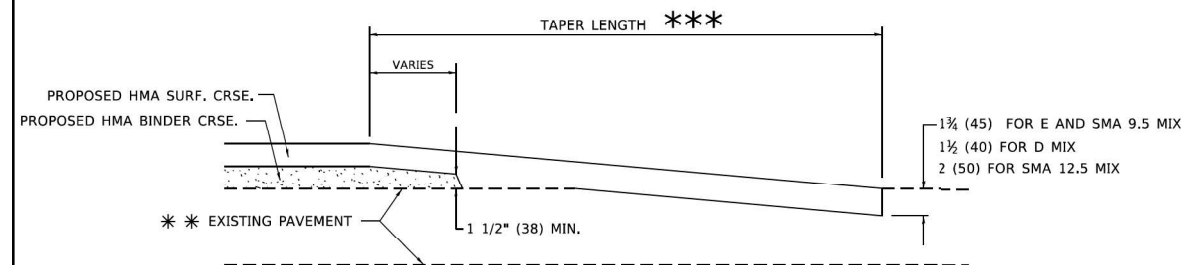


**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

**GENERAL NOTES**

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: D:\p\h\... FILE NAME: D:\p\h\... 02.dgn

USER NAME = Lawrence.DeManche	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97
	DRAWN -	REVISED - M. GOMEZ 04-06-01
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED - R. BCRO 01-01-07
PLOT DATE = 11/18/2022	DATE - 06-13-90	REVISED - K. SMITH 11-18-22

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				BD400-05 BD-32	CONTRACT NO.		
				ILLINOIS	FED. AID PROJECT		

MODEL: D:\p\h\... FILE NAME: D:\p\h\... 02.dgn



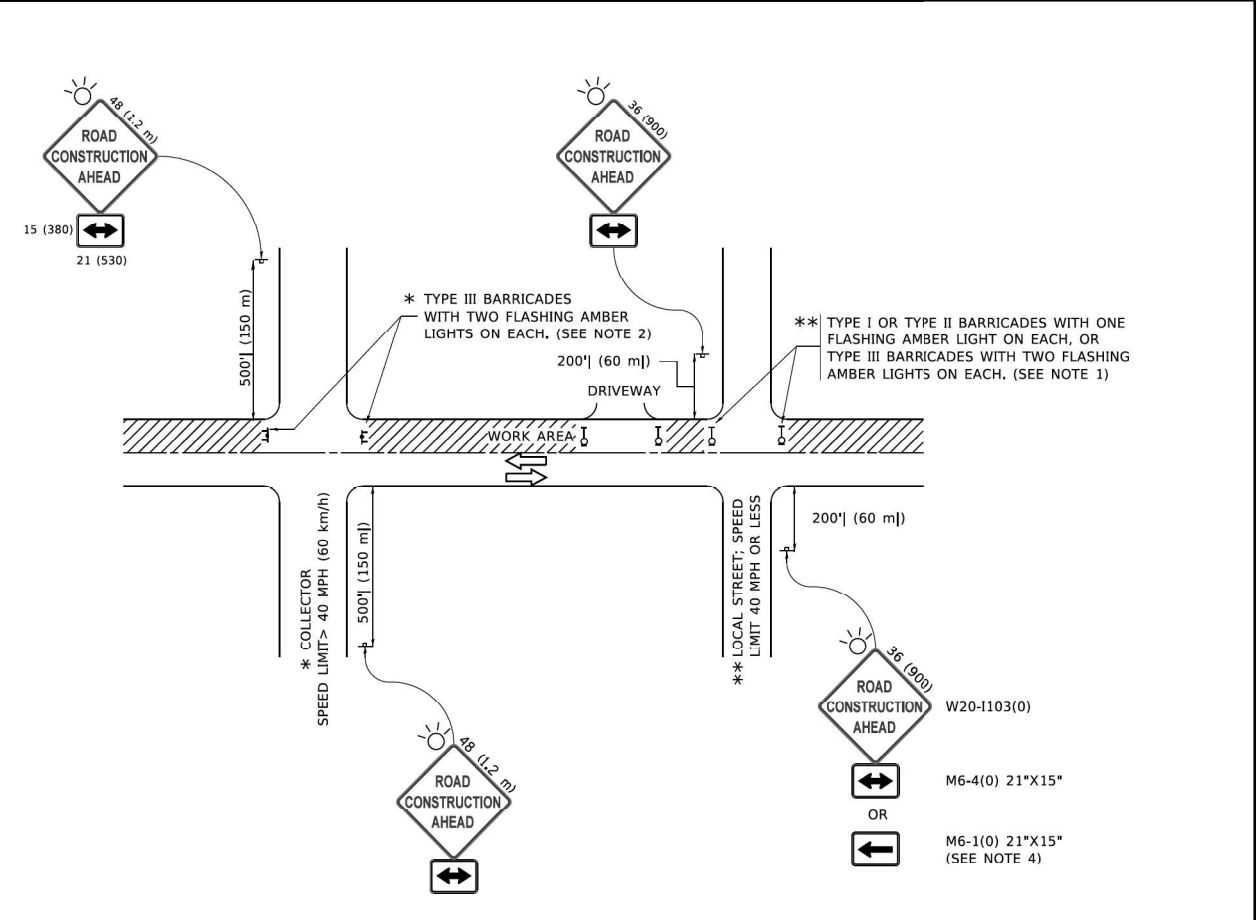
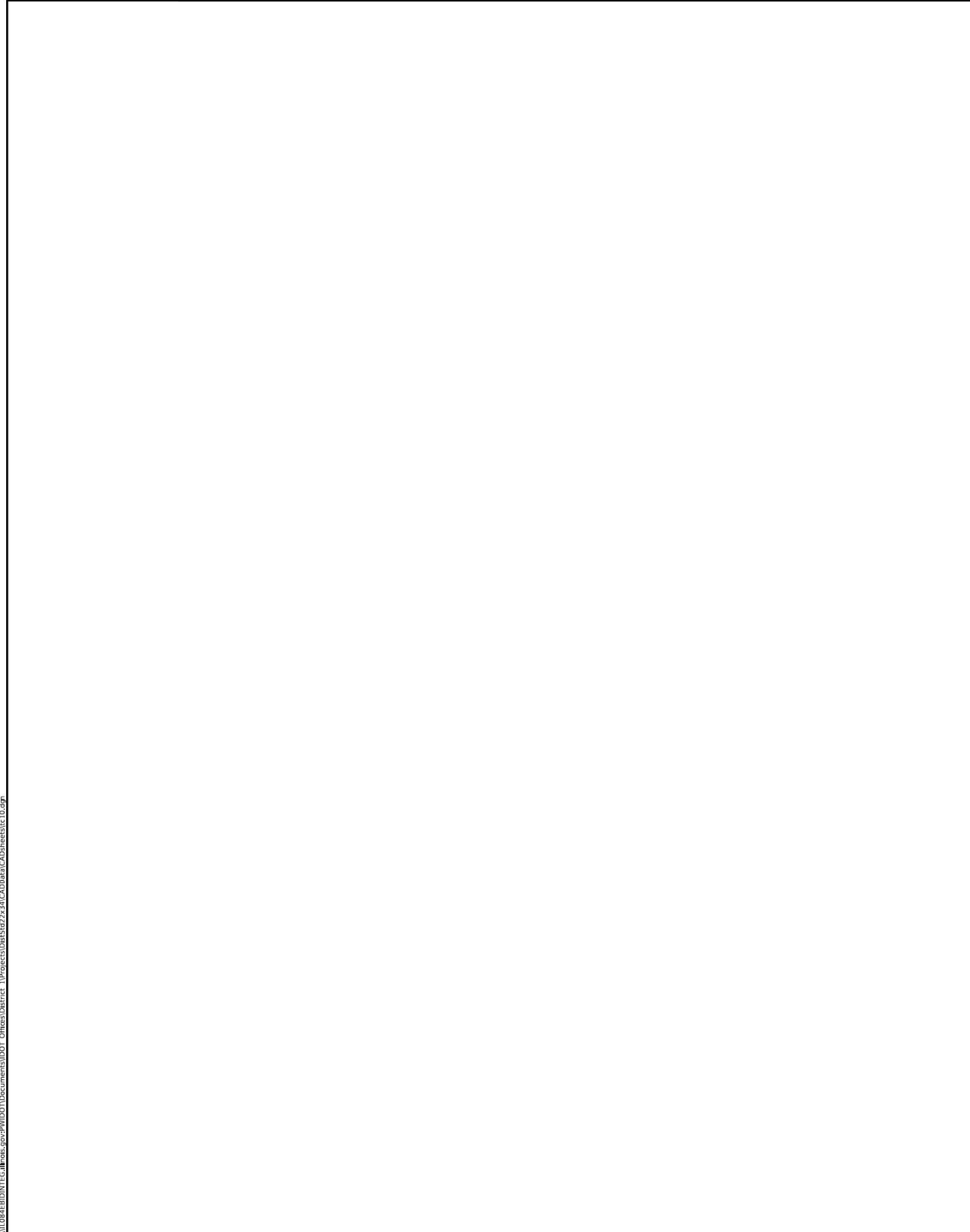
USER NAME = Michael.Czerpak	DESIGNED - AMB	REVISED -
	DRAWN - AMB	REVISED -
PLOT SCALE = 0.2000' / in.	CHECKED - MM	REVISED -
PLOT DATE = 1/26/2024	DATE - 01/26/2024	REVISED -

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND HMA TAPER DETAILS**

SCALE: N.T.S.	SHEET 2 OF 8 SHEETS	STA. TO STA.	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
				397 FAP 397 22 BRIDGE REHAB	COOK	44	38
				ILLINOIS	FED. AID PROJECT		

MODEL: D:\p\h\...  
 FILE NAME: ...



**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footej	DESIGNED - L.H.A.	REVISED - A. HOUSE 10-15-96
DRAWN -	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 30,0000' / in.	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-10			
ILLINOIS FED. AID PROJECT CONTRACT NO.				

MODEL: D:\p\h\...  
 FILE NAME: ...

**garza karhoff  
ENGINEERING, LLC**

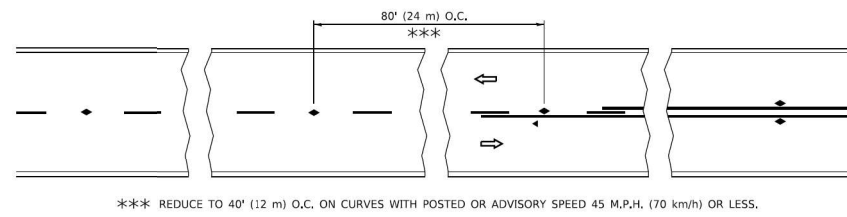
USER NAME = Michael.Czerpak	DESIGNED - AMB	REVISED -
DRAWN - AMB	CHECKED - MM	REVISED -
PLOT SCALE = 0.2000' / in.	DATE - 01/26/2024	REVISED -
PLOT DATE = 1/26/2024		

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

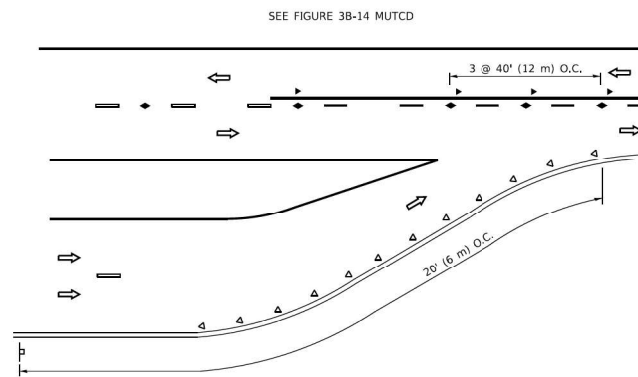
**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: N.T.S. SHEET 3 OF 8 SHEETS STA. TO STA.

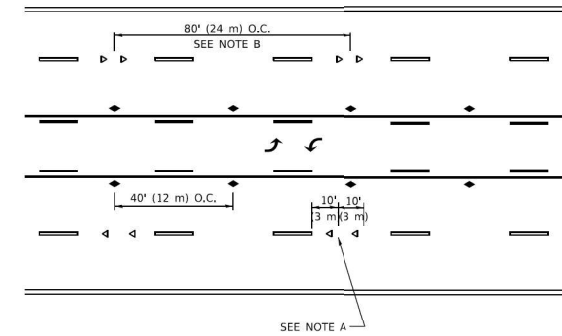
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	39
ILLINOIS FED. AID PROJECT CONTRACT NO. 62R75				



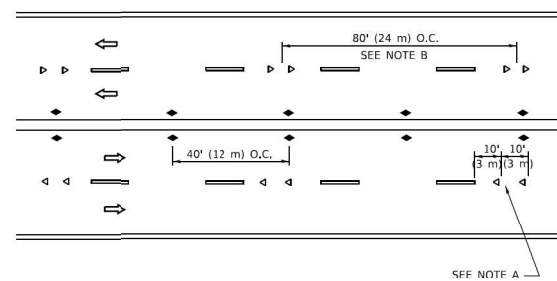
**TWO-LANE/TWO-WAY**



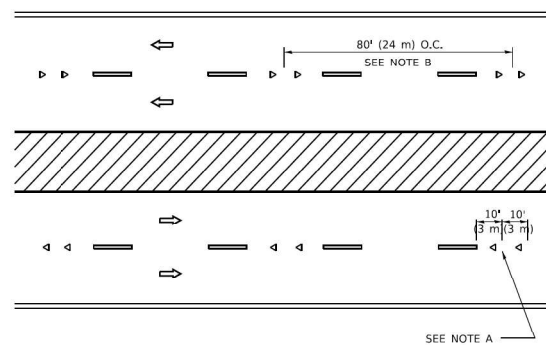
**LANE REDUCTION TRANSITION**



**TWO-WAY LEFT TURN**



**MULTI-LANE/UNDIVIDED**



**MULTI-LANE/DIVIDED**

**GENERAL NOTES**

- MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
- MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES.

**SYMBOLS**

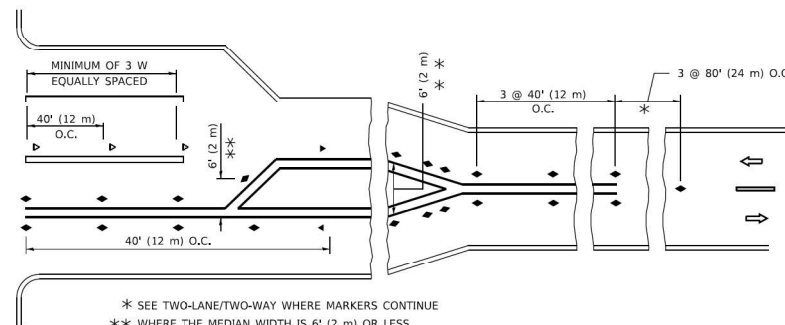
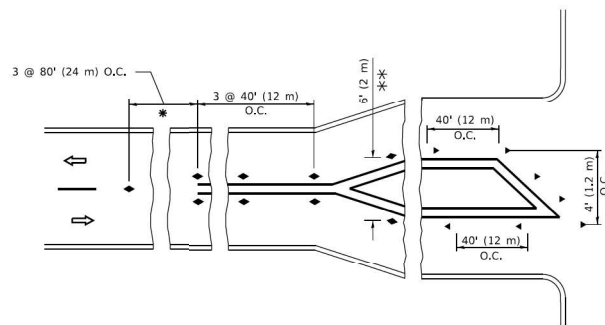
- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (WO)
- TWO-WAY AMBER MARKER

**LANE MARKER NOTES**

- USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

**DESIGN NOTES**

- DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



**TURN LANES**

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = michael.czerpak	DESIGNED - AMB	REVISOR - T. RAMMACHER 03-12-99
DRAWN - AMB	CHECKED - MM	REVISOR - T. RAMMACHER 01-06-00
PLOT SCALE = 0.2000' / in.	DATE - 01/26/2024	REVISOR - C. JUCIUS 09-09-09
PLOT DATE = 3/4/2019		REVISOR - C. JUCIUS 07-01-13

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS  
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
TC-11		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

MODEL: Default  
FILE NAME: D:\123122-2h-d1\2024\04.dgn



USER NAME = michael.czerpak	DESIGNED - AMB	REVISOR -
DRAWN - AMB	CHECKED - MM	REVISOR -
PLOT SCALE = 0.2000' / in.	DATE - 01/26/2024	REVISOR -
PLOT DATE = 1/26/2024		REVISOR -

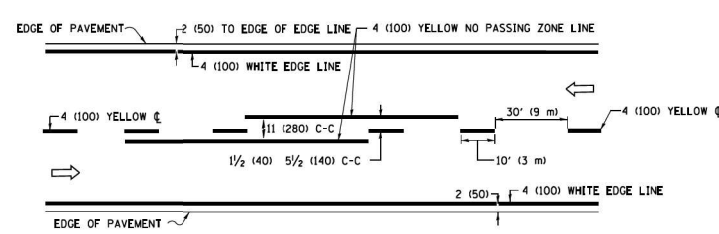
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

RAISED REFLECTIVE  
PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

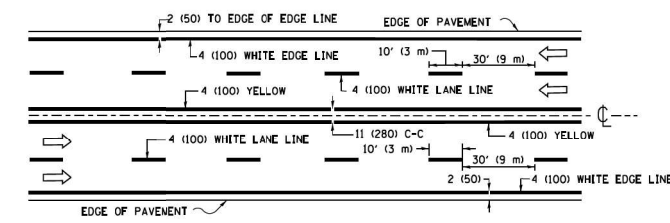
SCALE: N.T.S.	SHEET 4 OF 8 SHEETS	STA. TO STA.
---------------	---------------------	--------------

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	40
		CONTRACT NO. 62R75		
ILLINOIS		FED. AID PROJECT		

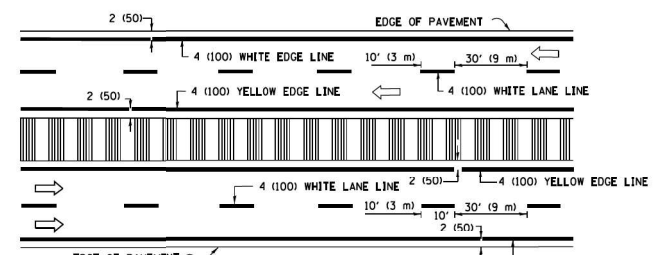




**2-LANE ROADWAY**

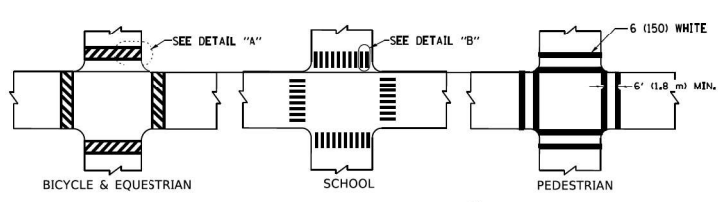


**MULTI-LANE UNDIVIDED**



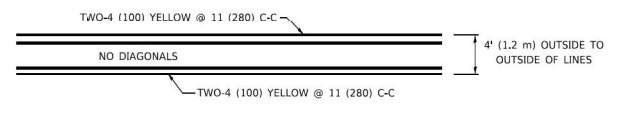
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

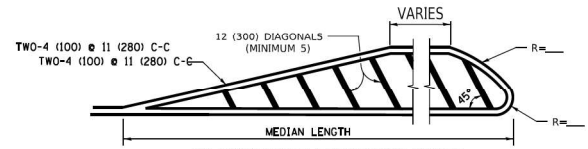


**TYPICAL CROSSWALK MARKING**

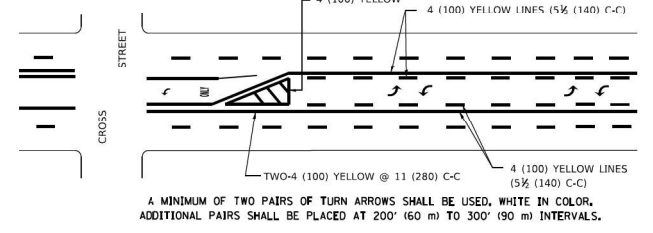
\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



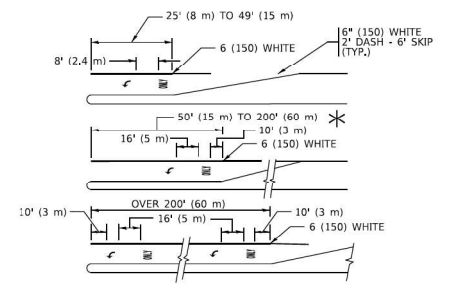
**4' (1.2 m) WIDE MEDIANS ONLY**



**MEDIANS OVER 4' (1.2 m) WIDE**

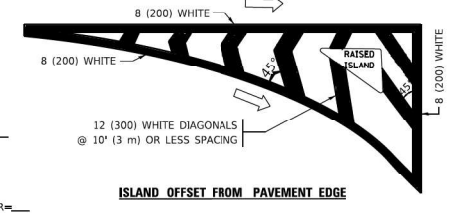


**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

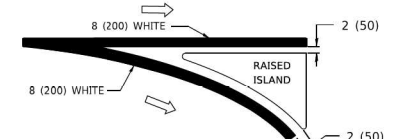


**TYPICAL LEFT (OR RIGHT) TURN LANE**

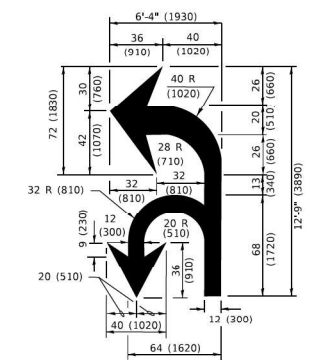
**TYPICAL TURN LANE MARKING**



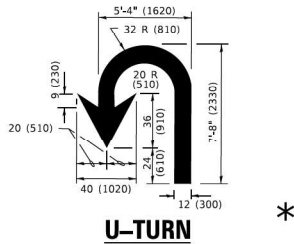
**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS 8' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h)) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m²) EACH *X*=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h)) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in Inches (millimeters) unless otherwise shown.

USER NAME = footem	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
DRAWN -	CHECKED -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 30,0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-13	ILLINOIS		
CONTRACT NO.				

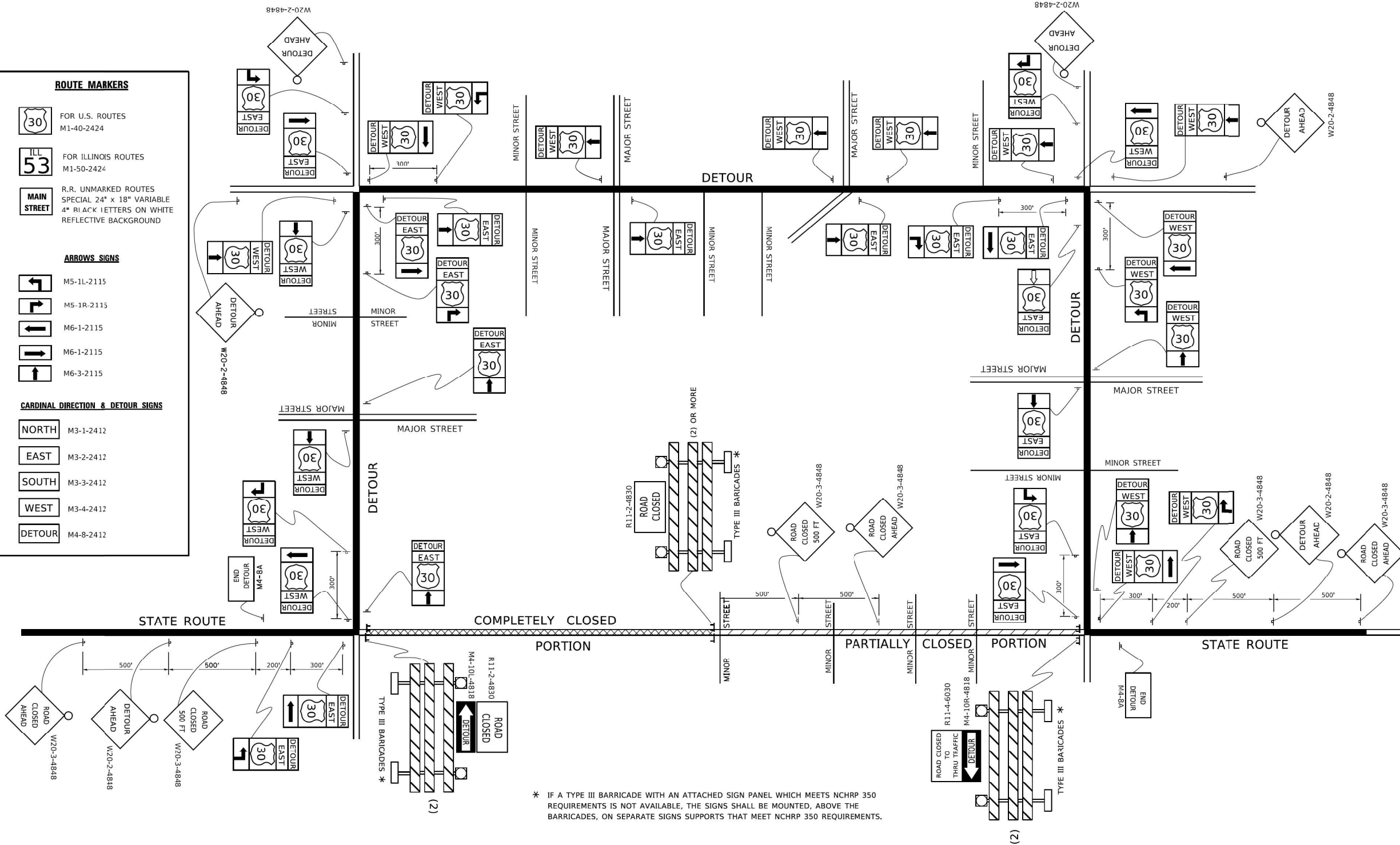
MODEL: Defaul  
FILE NAME: D:\123272-2b-2b-D123272-05.dwg  
1/26/2024 10:26:37 AM User:edward

USER NAME = Michael.Czerpak	DESIGNED - AMB	REVISED -
DRAWN - AMB	CHECKED - MM	REVISED -
PLOT SCALE = 0.2000' / in.	DATE - 01/26/2024	REVISED -
PLOT DATE = 1/26/2024	DATE - 01/26/2024	REVISED -

SCALE: N.T.S. SHEET 5 OF 8 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	41
CONTRACT NO. 6275				
ILLINOIS FED. AID PROJECT				

ROUTE MARKERS	
	FOR U.S. ROUTES M1-40-2424
	FOR ILLINOIS ROUTES M1-50-2424
	R.R. UNMARKED ROUTES SPECIAL 24" x 18" VARIABLE 4" BLACK LETTERS ON WHITE REFLECTIVE BACKGROUND
ARROWS SIGNS	
	M5-1L-2115
	M5-1R-2115
	M6-1-2115
	M6-1-2115
	M6-3-2115
	M6-3-2115
CARDINAL DIRECTION & DETOUR SIGNS	
	NORTH M3-1-2412
	EAST M3-2-2412
	SOUTH M3-3-2412
	WEST M3-4-2412
	DETOUR M4-8-2412



\* IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.

USER NAME = footem	DESIGNED -	REVISED - 10-18-02
PLOT SCALE = 30,0000' / in.	DRAWN -	REVISED - R. BCRO 09-14-09
PLOT DATE = 3/4/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: NONE	SHEET 1	OF 1	SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-21			
ILLINOIS FED. AID PROJECT			CONTRACT NO.	

MODEL: Defaul  
FILE NAME: D:\123122-2h-d1\220503.dwg, 05.dgn

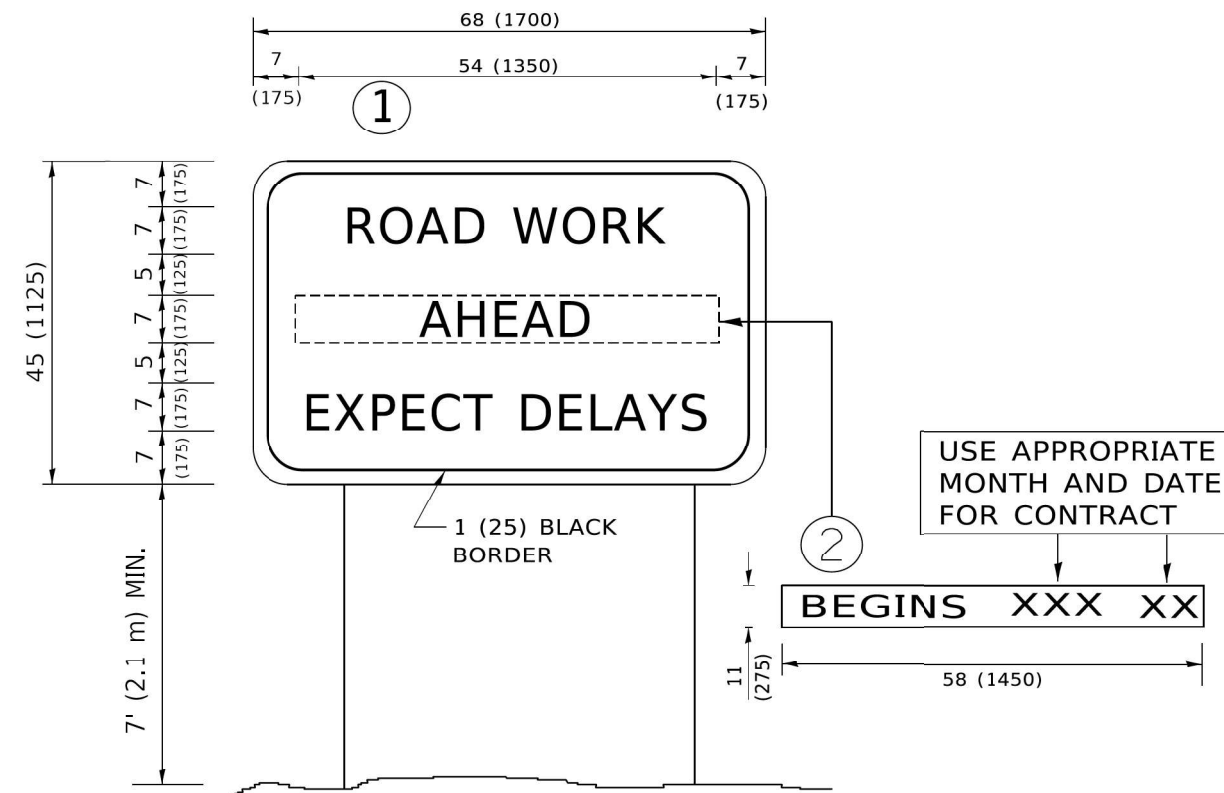
GARZA KARHOFF  
ENGINEERING, LLC

USER NAME = Michael.Czerpak	DESIGNED - AMB	REVISED -
PLOT SCALE = 0.2000' / in.	DRAWN - AMB	REVISED -
PLOT DATE = 1/26/2024	CHECKED - MM	REVISED -
	DATE - 01/26/2024	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

DETOUR SIGNING FOR CLOSING STATE HIGHWAYS			
SCALE: N.T.S.	SHEET 6	OF 8	SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	42
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R75	



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

USER NAME = footejm	DESIGNED -	REVISED - R. MIRS 09-15-97
PLOT SCALE = 50,0000' / in.	CHECKED -	REVISED - R. MIRS 12-11-97
PLOT DATE = 3/4/2019	DATE -	REVISED - T. RAMMACHER 02-02-99
		REVISED - C. JUCIUS 01-31-07

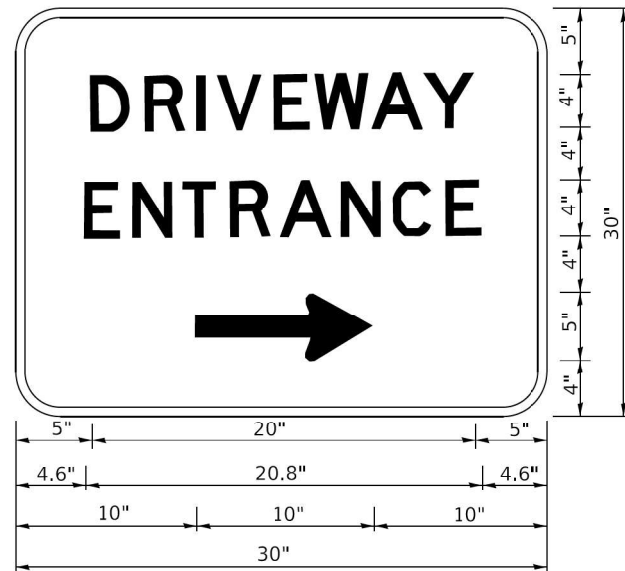
<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>		<b>ARTERIAL ROAD INFORMATION SIGN</b>		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE		SHEET 1 OF 1 SHEETS		TC-22		CONTRACT NO.		
				ILLINOIS		FED. AID PROJECT		

USER NAME = Michael.Czerpak	DESIGNED - AMB	REVISED -
PLOT SCALE = 0.2000' / in.	CHECKED - MM	REVISED -
PLOT DATE = 1/26/2024	DATE - 01/26/2024	REVISED -

<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>		<b>ARTERIAL ROAD INFORMATION SIGN</b>		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: N.T.S.		SHEET 7 OF 8 SHEETS		397 FAP 397 22 BRIDGE REHAB		COOK 44 43		
				ILLINOIS		FED. AID PROJECT		

MODEL: D:\p4\h... FILE NAME: D:\2024\24-01-26\24-01-26.dwg





3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: Default  
 FILE NAME: D:\123122-2h-d1-123122.dwg  
 PROJECT: 123122-2h-d1-123122.dwg  
 USER: Michael.Czerpak  
 DATE: 1/26/2024

USER NAME = leysa	DESIGNED -	REVISED -	C. JUCIUS 02-15-07
PLOT SCALE = 50,0000' / in.	DRAWN -	REVISED -	
PLOT DATE = 8/6/2021	CHECKED -	REVISED -	
	DATE -	REVISED -	

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
<b>TC-26</b>		CONTRACT NO.		
ILLINOIS FED. AID PROJECT				

MODEL: Default  
 FILE NAME: D:\123122-2h-d1-123122.dwg  
 PROJECT: 123122-2h-d1-123122.dwg  
 USER: Michael.Czerpak  
 DATE: 1/26/2024



USER NAME = Michael.Czerpak	DESIGNED -	REVISED -	
PLOT SCALE = 0.2000' / in.	DRAWN -	REVISED -	
PLOT DATE = 1/26/2024	CHECKED -	REVISED -	
	DATE -	REVISED -	

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

SCALE: N.T.S. SHEET 8 OF 8 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
397	FAP 397 22 BRIDGE REHAB	COOK	44	44
CONTRACT NO. 62R75				
ILLINOIS FED. AID PROJECT				