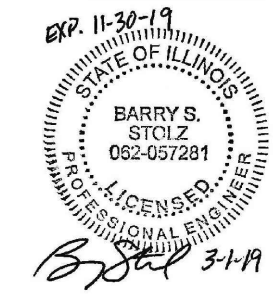


| SUMMARY OF QUANTITIES | | | | |
|-----------------------|-------------------------------------|------|----------------|-------------------|
| ITEM NO. | DESCRIPTION | UNIT | TOTAL QUANTITY | AS-BUILT QUANTITY |
| AR150520 | MOBILIZATION | L.S. | 1 | |
| AR209510 | CRUSHED AGGREGATE BASE COURSE | TON | 284 | |
| AR401613 | BIT. SURF. CSE.-METHOD I, SUPERPAVE | TON | 176 | |
| AR602510 | BITUMINOUS PRIME COAT | GAL. | 267 | |
| AR603510 | BITUMINOUS TACK COAT | GAL. | 115 | |
| AR751920 | REPLACE INLET | EA. | 11 | |
| AR800469 | REMOVE BITUMINOUS & PCC PAVEMENT | S.Y. | 764 | |
| AR800597 | CLEAN EXISTING DRAINAGE STRUCTURE | EA. | 2 | |

| INDEX TO SHEETS | |
|-----------------|---|
| SHEET NUMBER | SHEET TITLE |
| 1 | COVER SHEET |
| 2 | SUMMARY OF QUANTITIES AND INDEX TO SHEETS |
| 3 | PROPOSED SCOPE OF WORK |
| 4 | PROPOSED SAFETY PLAN - NORTH WORK AREA |
| 5 | PROPOSED SAFETY PLAN - SOUTH WORK AREA |
| 6 | CONSTRUCTION SAFETY DETAILS AND NOTES |
| 7 | PROPOSED CONSTRUCTION PLAN - NORTH 1 |
| 8 | PROPOSED CONSTRUCTION PLAN - NORTH 2 |
| 9 | PROPOSED CONSTRUCTION PLAN - SOUTH 1 |
| 10 | PROPOSED CONSTRUCTION PLAN - SOUTH 2 |
| 11 | TYPICAL CONSTRUCTION DETAILS 1 |
| 12 | TYPICAL CONSTRUCTION DETAILS 2 |



**REPAIR EXISTING
INLETS IN MAIN
APRON AREA**

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

GENERAL NOTES:

QUANTITIES

PAYMENT WILL BE MADE UNDER THE ITEM NUMBERS, DESCRIPTIONS AND UNITS NOTED IN THE ABOVE TABLE IN ACCORDANCE WITH THE BASIS OF PAYMENT FOR EACH RESPECTIVE WORK ITEM COMPLETED AND ACCEPTED BY THE ENGINEER.

CERTIFIED PAYROLLS

THE RESIDENT ENGINEER/TECHNICIAN CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS AND EEO STATEMENTS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATIONS

MATERIALS TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL. ALL MATERIALS TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. USE OF MATERIALS WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.

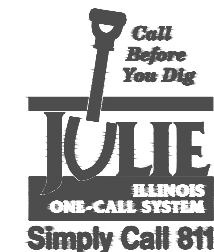
UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY.

CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E. INFORMATION

COUNTY.....LAWRENCE
CITY.....LAWRENCEVILLE
TOWNSHIP.....ALLISON
SECTION NO.....23 & 26
ADDRESS.....LAWRENCEVILLE-VINCENNES AIRPORT
13608 HANGAR ROAD
LAWRENCEVILLE, ILLINOIS 62439



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ISSUE: MARCH 1, 2019

PROJECT NO: 18A0093

CAD FILE: G-002_FLP.DWG

DESIGN BY: BSS 11/26/2018

DRAWN BY: CWS 11/26/2018

REVIEWED BY: BSS 03/01/2019

SHEET TITLE

**SUMMARY OF
QUANTITIES AND
INDEX TO SHEETS**

GENERAL NOTES

1. THE SCOPE OF WORK SHEET IS INTENDED ONLY AS A GENERAL DESCRIPTION OF WORK ITEMS AND THEIR APPROXIMATE LOCATIONS AND LIMITS, FOR THE PURPOSE OF UNDERSTANDING THE SCOPE OF THE PROJECT. THIS SHEET SHALL NOT BE USED AS A CONSTRUCTION PLAN. REFER TO THE FOLLOWING PLAN SHEETS FOR DETAILED CONSTRUCTION REQUIREMENTS, LOCATIONS, AND ITEMS OF WORK.
2. THIS PROJECT CONSISTS OF REPLACEMENT OF EXISTING STORM INLETS AND REPAIRS TO PAVEMENT AREAS LOCATED AROUND THE MAIN AIRCRAFT APRON. THIS PROJECT INCLUDES PAVEMENT REMOVAL, INLET REPLACEMENT, PLACEMENT OF AGGREGATE BASE AND BITUMINOUS PAVEMENT, AND ASSOCIATED ITEMS.
3. THE CONTRACTOR SHALL FURNISH ALL LABOR, MATERIAL, EQUIPMENT, AND TRANSPORTATION NECESSARY TO CONSTRUCT ALL ELEMENTS OF THE PROJECT AS DESCRIBED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS.
4. THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN.
5. THE CONTRACTOR IS NOT PERMITTED TO USE THE AIRPORT AUTO PARKING LOT FOR MATERIAL AND EQUIPMENT STORAGE. THE CONSTRUCTION ENTRANCE AS SHOWN ON THE PROPOSED SAFETY AND PHASING PLAN WILL BE THE PRIMARY ACCESS TO THE PROPOSED CONSTRUCTION SITE. ALL HAULING OF MATERIALS AND EQUIPMENT SHALL BE RESTRICTED TO THE DESIGNATED CONSTRUCTION ENTRANCE.
6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT, PRESERVE AND REPAIR THE EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES AT NO ADDITIONAL COST TO THE CONTRACT. ANY DAMAGE TO FAA CABLES, ACCESS ROADS OR TO FAA FACILITIES DURING CONSTRUCTION WILL REQUIRE THE CONTRACTOR TO REPLACE THE DAMAGED CABLES, ACCESS ROAD OR FAA FACILITIES TO FAA REQUIREMENTS AT THE CONTRACTOR'S EXPENSE. SPLICING OF CABLES IS NOT AN ACCEPTABLE FORM OF REPAIR.
7. NO EQUIPMENT SHALL BE PERMITTED TO CROSS OR USE ANY EXISTING PAVEMENT OUTSIDE THE CONSTRUCTION LIMITS, GENERAL PROJECT AREA OR HAUL ROUTE.
8. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
9. UNLESS OTHERWISE NOTED, ALL DISTURBED AREAS OUTSIDE OF THE PROPOSED CONSTRUCTION LIMITS SHALL BE GRADED, SEEDED AND/OR HYDROMULCH SEEDED AT NO ADDITIONAL COST TO THE CONTRACT.
10. ALL WASTE MATERIAL SHALL BE HAULED FROM THE AIRPORT AND PROPERLY DISPOSED OF UNLESS OTHERWISE SPECIFIED HEREIN.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL.
12. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON THE PROJECT.
13. THE CONTRACTOR SHALL COORDINATE WITH THE RESIDENT ENGINEER/TECHNICIAN SO THEY MAY DEVELOP ONE SET OF REDLINED AS-BUILT DRAWINGS AT THE COMPLETION OF THE PROJECT.
14. CONTRACTOR SHALL NOTE THAT ALL AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO BEGINNING ANY WORK. ALL AREAS WHICH HAVE BEEN FARMED AND OR DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION, AND HAVE BEEN DISTURBED BY CONSTRUCTION ACTIVITY, SHALL BE CHISEL PLOWED (36" MAX.) OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER.)
15. CONTRACTOR SHALL RESTORE TO ORIGINAL CONDITION ALL GRASS, STONE, OR PAVEMENT DISTURBED BY CONTRACTOR'S CONSTRUCTION OPERATIONS, STAGING, AND CONSTRUCTION ACCESS ROUTES. DISTURBED AREAS TO BE REPAIRED, GRADED, AND SEEDED AND MULCHED UNLESS OTHERWISE NOTED. STAGING AREA AND SITE ACCESS RESTORATION SHALL BE INCLUDED IN THE COST OF THE HAUL ROUTE.
16. THE PROJECT PAY ITEMS ARE INTENDED TO BE INCLUSIVE OF ALL WORK TO BE PERFORMED AS SHOWN IN THESE PLANS. ALL INCIDENTAL WORK REQUIRED TO COMPLETE THE PROJECT TO THE SATISFACTION OF THE RESIDENT ENGINEER/TECHNICIAN IS TO BE INCLUDED IN THE COSTS OF PERFORMING THESE ITEMS.
17. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER PERSONS FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
18. THE CONTRACTOR MUST AT ALL TIMES MAINTAIN PROPER DRAINAGE FOR ALL AREAS AFFECTED BY HIS WORK.

| CRITICAL POINTS TABLE | | | | | |
|-----------------------|-----------------|------------------|--------------|---------------|--------------|
| POINT # | LATITUDE | LONGITUDE | GROUND ELEV. | EQUIP. HEIGHT | EQUIP. ELEV. |
| CP#1 | 038° 45' 55.73" | -087° 36' 31.59" | 424.00 | 25' | 449.00 |
| CP#2 | 038° 45' 54.81" | -087° 36' 24.37" | 423.00 | 25' | 448.00 |
| CP#3 | 038° 45' 51.62" | -087° 36' 18.34" | 422.00 | 25' | 447.00 |
| CP#4 | 038° 45' 48.58" | -087° 36' 16.90" | 422.00 | 25' | 447.00 |
| CP#5 | 038° 45' 38.14" | -087° 36' 10.53" | 423.00 | 25' | 448.00 |
| CP#6 | 038° 45' 32.63" | -087° 36' 10.90" | 423.00 | 25' | 448.00 |
| CP#7 | 038° 45' 23.58" | -087° 36' 14.23" | 424.00 | 25' | 449.00 |
| CP#8 | 038° 45' 35.35" | -087° 36' 06.85" | 420.00 | 25' | 445.00 |
| CP#9 | 038° 45' 14.76" | -087° 36' 06.64" | 424.00 | 25' | 449.00 |
| CP#10 | 038° 45' 37.19" | -087° 36' 42.19" | 424.00 | 25' | 449.00 |

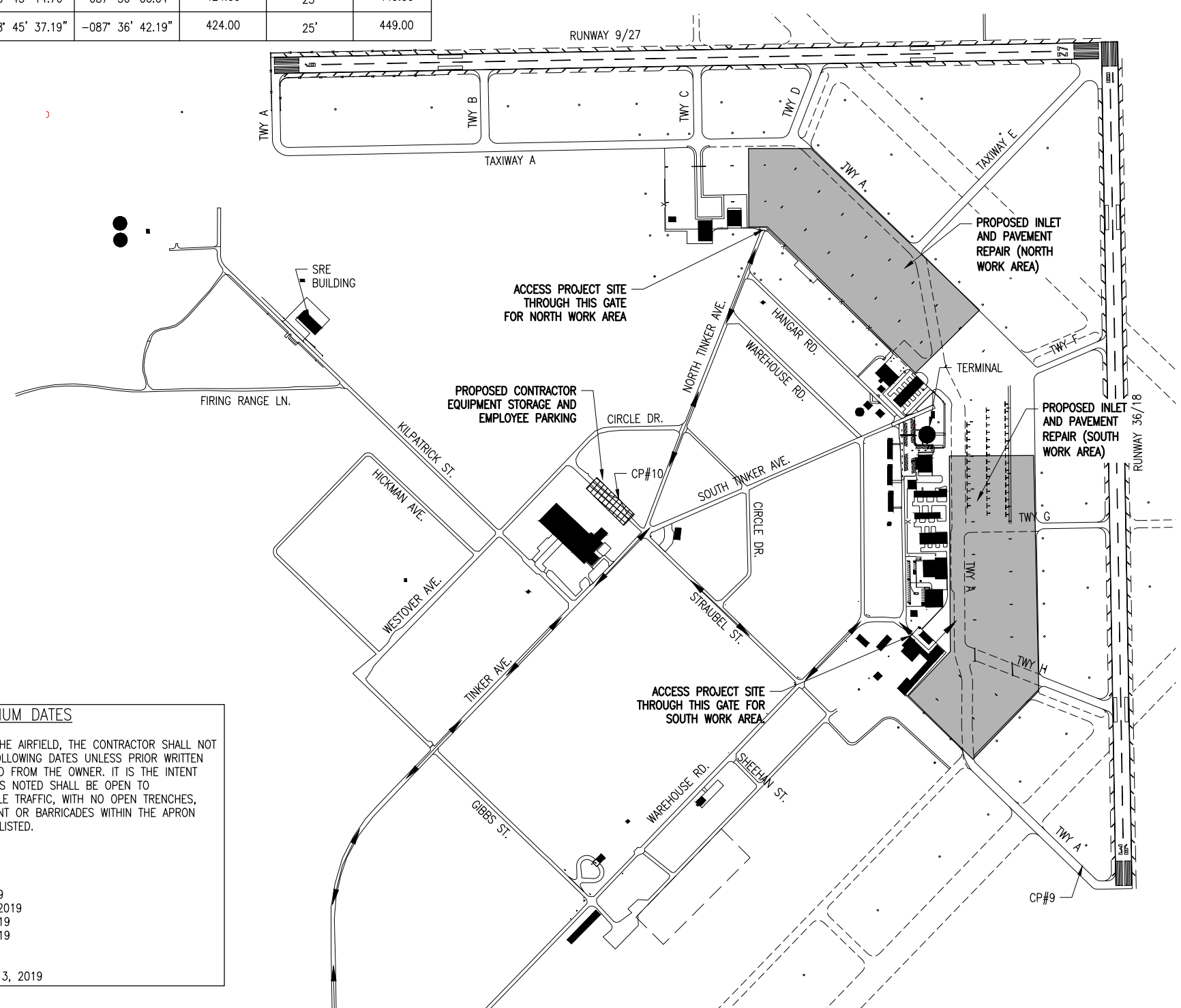
LEGEND

- EXISTING PAVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED EQUIPMENT PARKING AREA
- PROPOSED HAUL ROUTE

0' 200' 400' 800'

HALF SIZE SCALE: 1" = 800'

FULL SIZE SCALE: 1" = 400'



WORK MORATORIUM DATES

DUE TO EVENTS ON THE AIRFIELD, THE CONTRACTOR SHALL NOT WORK DURING THE FOLLOWING DATES UNLESS PRIOR WRITTEN APPROVAL IS RECEIVED FROM THE OWNER. IT IS THE INTENT THAT THE WORK AREAS NOTED SHALL BE OPEN TO UNOBSTRUCTED VEHICLE TRAFFIC, WITH NO OPEN TRENCHES, STOCKPILES, EQUIPMENT OR BARRICADES WITHIN THE APRON AREA ON THE DATES LISTED.

NORTH WORK AREA

- JULY 07, 2019
- JULY 21, 2019
- AUGUST 18, 2019
- SEPTEMBER 15, 2019
- OCTOBER 06, 2019
- OCTOBER 13, 2019

ALL WORK AREAS

- SEPTEMBER 06-13, 2019



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MID-AMERICAN AIR CENTER
 LAWRENCEVILLE-VINCENNES
 INTERNATIONAL AIRPORT

BI-STATE AUTHORITY
 LAWRENCEVILLE, ILLINOIS

EXP. 11-30-19

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REPAIR EXISTING INLETS IN MAIN APRON AREA

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

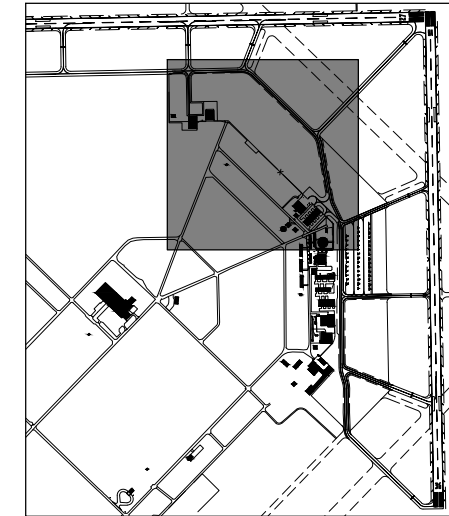
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ISSUE: MARCH 1, 2019
 PROJECT NO: 18A0093
 CAD FILE: G-003-SOW.DWG
 DESIGN BY: BSS 11/26/2018
 DRAWN BY: CWS 11/26/2018
 REVIEWED BY: BSS 03/01/2019

SHEET TITLE

PROPOSED SCOPE OF WORK

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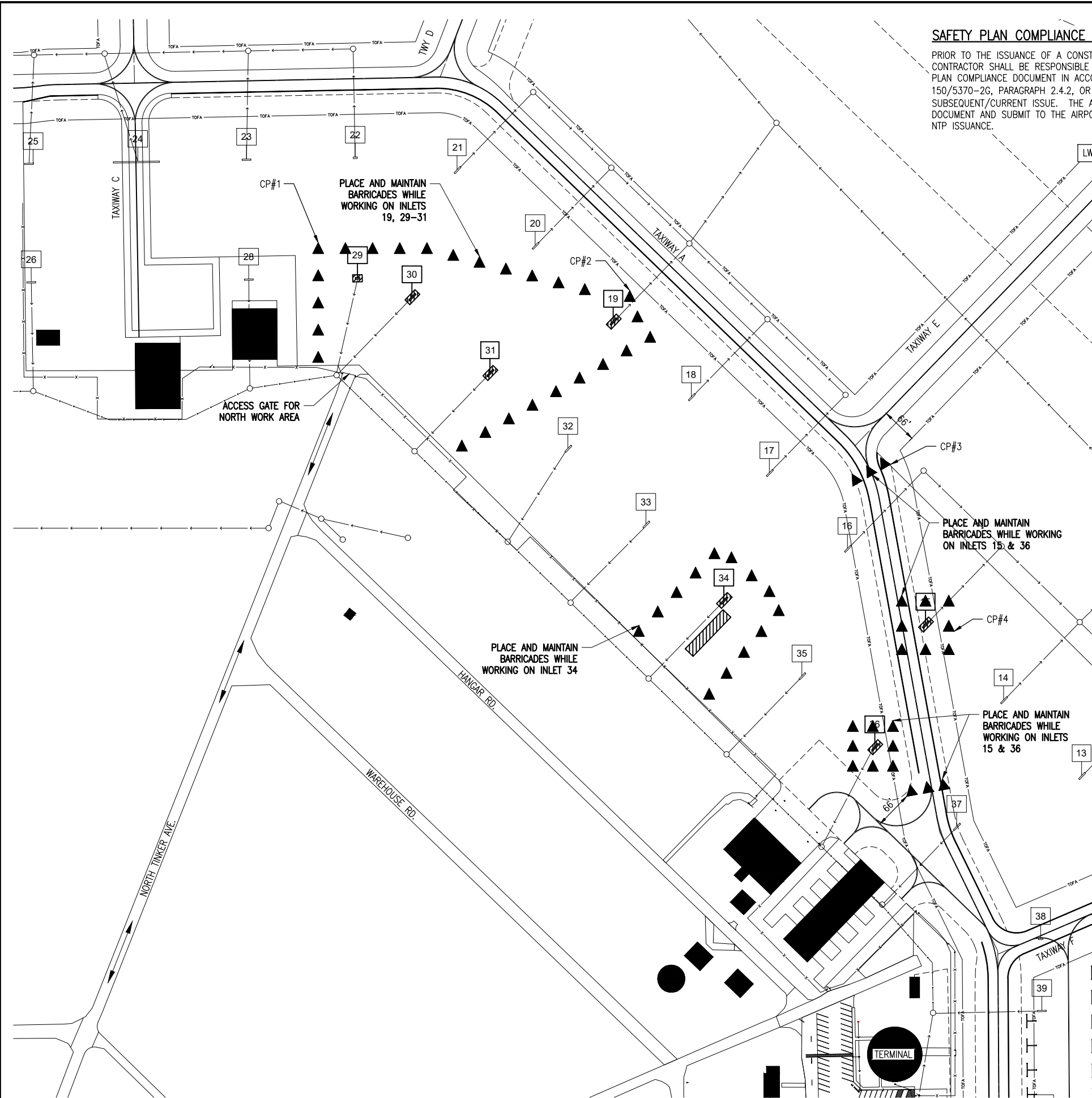


KEY PLAN

SAFETY PLAN COMPLIANCE DOCUMENT

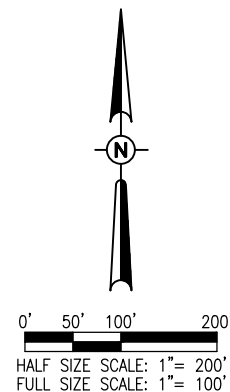
PRIOR TO THE ISSUANCE OF A CONSTRUCTION NOTICE-TO-PROCEED (NTP), THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING AND SUBMITTING A SAFETY PLAN COMPLIANCE DOCUMENT IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G, PARAGRAPH 2.4.2, OR EQUIVALENT SECTION IN SUBSEQUENT/CURRENT ISSUE. THE AIRPORT DIRECTOR SHALL APPROVE THIS DOCUMENT AND SUBMIT TO THE AIRPORT AUTHORITY FOR APPROVAL PRIOR TO THE NTP ISSUANCE.

LWV CTAF/UNICOM FREQUENCY = 122.80 MHz



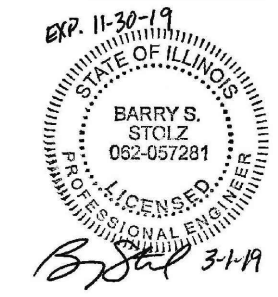
LEGEND

- EXISTING PAVEMENTS
- EXISTING BUILDINGS
- PROPOSED INLET CLEANING
- PROPOSED INLET REPLACEMENT
- PROPOSED PAVEMENT REPAIR
- INLET NUMBER
- EXISTING INLET DRAINAGE PIPE
- EXISTING DRYWELL
- PROPOSED BARRICADES
- PROPOSED HAUL ROUTE
- TAXIWAY OBJECT FREE AREA



NORTH WORK AREA NOTES

1. BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION. CONTRACTOR MAY WORK IN ALL WORK AREAS SIMULTANEOUSLY, HOWEVER THE MAINTENANCE OF TRAFFIC DEVICES AND HAUL ROUTES SHALL REMAIN CONSISTENT WITH THOSE DEPICTED ON THESE SAFETY PLANS.
2. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE DISRUPTION TO AIRCRAFT OPERATIONS.
3. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO AIRPORT VEHICLES AND REMAIN CLEAR AT ALL TIMES.
4. ALL ACTIVE AIRFIELD PAVEMENTS SHALL BE KEPT CLEAN AND FREE OF DEBRIS AT ALL TIMES DURING CONSTRUCTION.
5. FLAGPERSONS AND/OR ESCORTS MAY BE NECESSARY TO ENSURE PROPER CONTROL OF CONSTRUCTION VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. COSTS OF SUCH CONTROL SHALL BE INCIDENTAL TO THE PROJECT.
6. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROUTE AND CONSTRUCTION EQUIPMENT PARKING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS PER THE SPECIFICATIONS.
7. THE COSTS FOR PROVISION, PLACEMENT, MAINTENANCE AND REMOVAL OF BARRICADES/CLOSURE CROSSES ON THE AIRFIELD AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.
8. THE COSTS FOR CONSTRUCTION/MAINTENANCE OF HAUL ROUTE AND EQUIPMENT STAGING AREA AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.



REPAIR EXISTING INLETS IN MAIN APRON AREA

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

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ISSUE: MARCH 1, 2019

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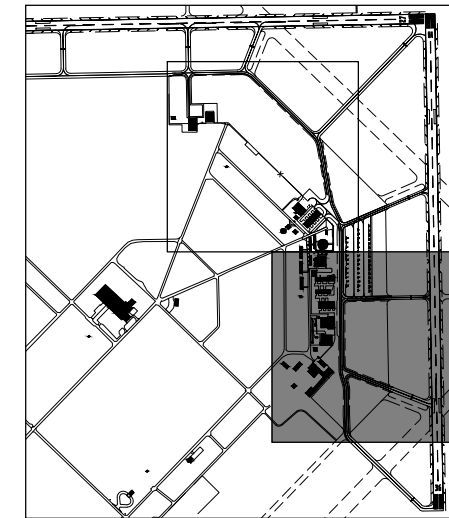
DESIGN BY: BSS 11/26/2018

DRAWN BY: CWS 11/26/2018

REVIEWED BY: BSS 03/01/2019

SHEET TITLE

PROPOSED SAFETY PLAN - NORTH WORK AREA



KEY PLAN

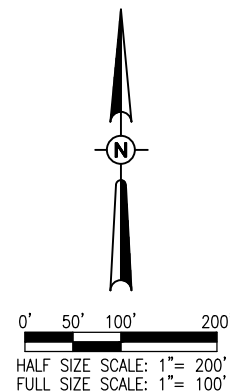
SAFETY PLAN COMPLIANCE DOCUMENT

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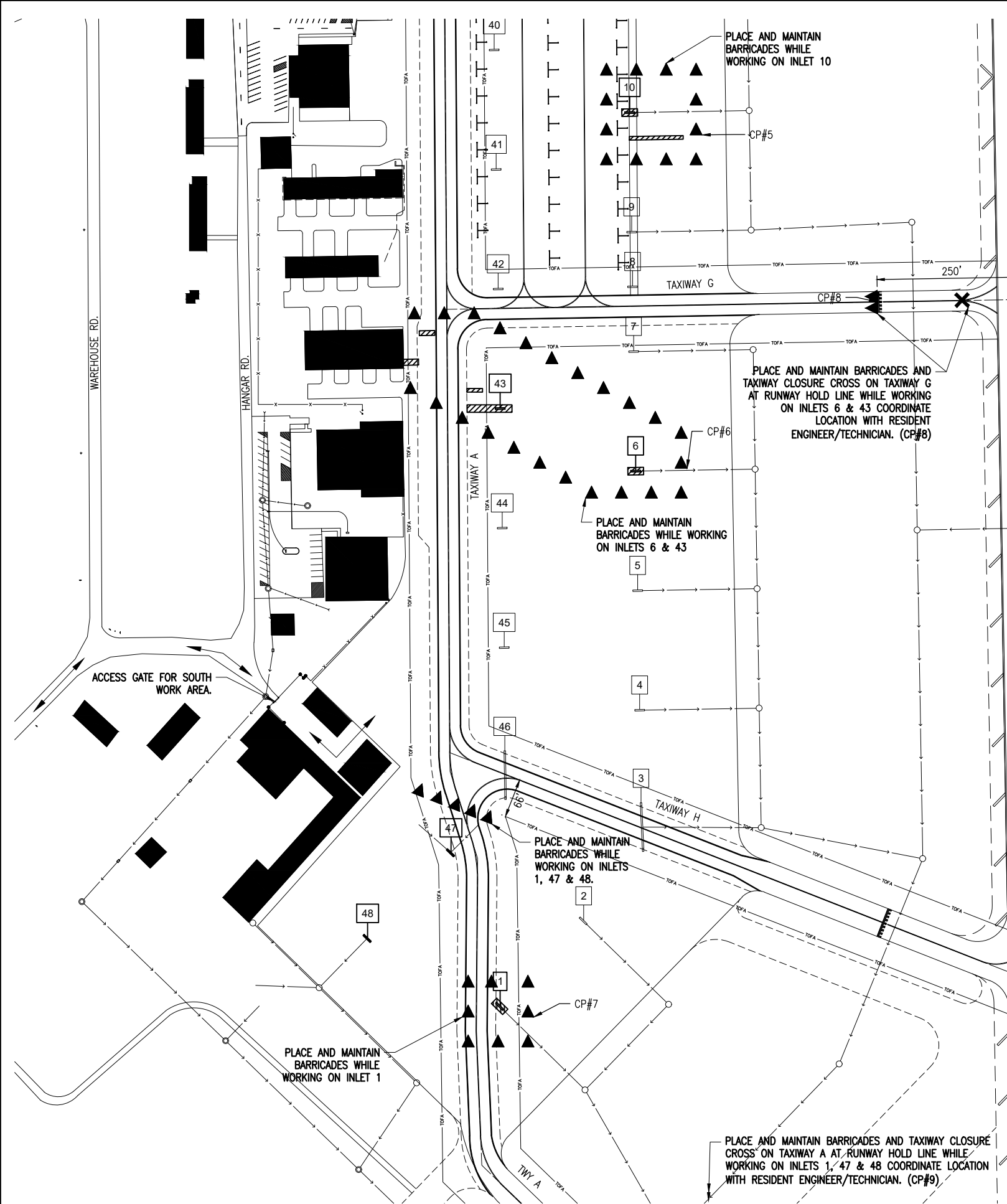
LEGEND

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- EXISTING BUILDINGS
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- PROPOSED INLET REPLACEMENT
- PROPOSED PAVEMENT REPAIR
- INLET NUMBER
- EXISTING INLET DRAINAGE PIPE
- EXISTING DRYWELL
- PROPOSED BARRICADES
- PROPOSED HAUL ROUTE
- TAXIWAY OBJECT FREE AREA

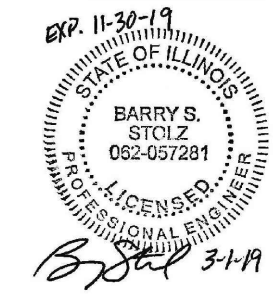


SOUTH WORK AREA NOTES

1. BARRICADES SHALL BE IN PLACE PRIOR TO BEGINNING CONSTRUCTION. CONTRACTOR MAY WORK IN ALL WORK AREAS SIMULTANEOUSLY, HOWEVER THE MAINTENANCE OF TRAFFIC DEVICES AND HAUL ROUTES SHALL REMAIN CONSISTENT WITH THOSE DEPICTED ON THESE SAFETY PLANS.
2. AT ALL TIMES, THE CONTRACTOR'S OPERATIONS SHALL BE SUCH AS TO MINIMIZE DISRUPTION TO AIRCRAFT OPERATIONS.
3. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO AIRPORT VEHICLES AND REMAIN CLEAR AT ALL TIMES.
4. ALL ACTIVE AIRFIELD PAVEMENTS SHALL BE KEPT CLEAN AND FREE OF DEBRIS AT ALL TIMES DURING CONSTRUCTION.
5. FLAGPERSONS AND/OR ESCORTS MAY BE NECESSARY TO ENSURE PROPER CONTROL OF CONSTRUCTION VEHICLE TRAFFIC ACROSS ACTIVE AIRFIELD PAVEMENTS. COSTS OF SUCH CONTROL SHALL BE INCIDENTAL TO THE PROJECT.
6. AT THE COMPLETION OF ALL WORK AREA CONSTRUCTION, THE HAUL ROUTE AND CONSTRUCTION EQUIPMENT PARKING AREA SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS PER THE SPECIFICATIONS.
7. THE COSTS FOR PROVISION, PLACEMENT, MAINTENANCE AND REMOVAL OF BARRICADES/CLOSURE CROSSES ON THE AIRFIELD AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.
8. THE COSTS FOR CONSTRUCTION/MAINTENANCE OF HAUL ROUTE AND EQUIPMENT STAGING AREA AND ALL ASSOCIATED INCIDENTALS SHALL BE PAID FOR UNDER ITEM AR150520 MOBILIZATION.



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REPAIR EXISTING INLETS IN MAIN APRON AREA

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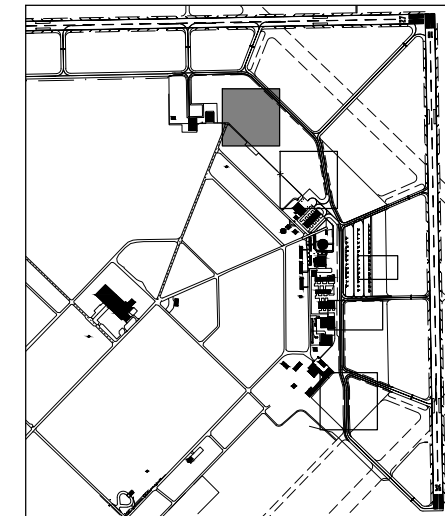
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DESIGN BY: BSS 11/26/2018
DRAWN BY: CWS 11/26/2018
REVIEWED BY: BSS 03/01/2019

PROPOSED SAFETY PLAN - SOUTH WORK AREA



KEY PLAN

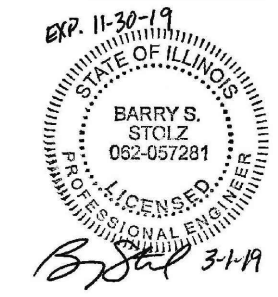
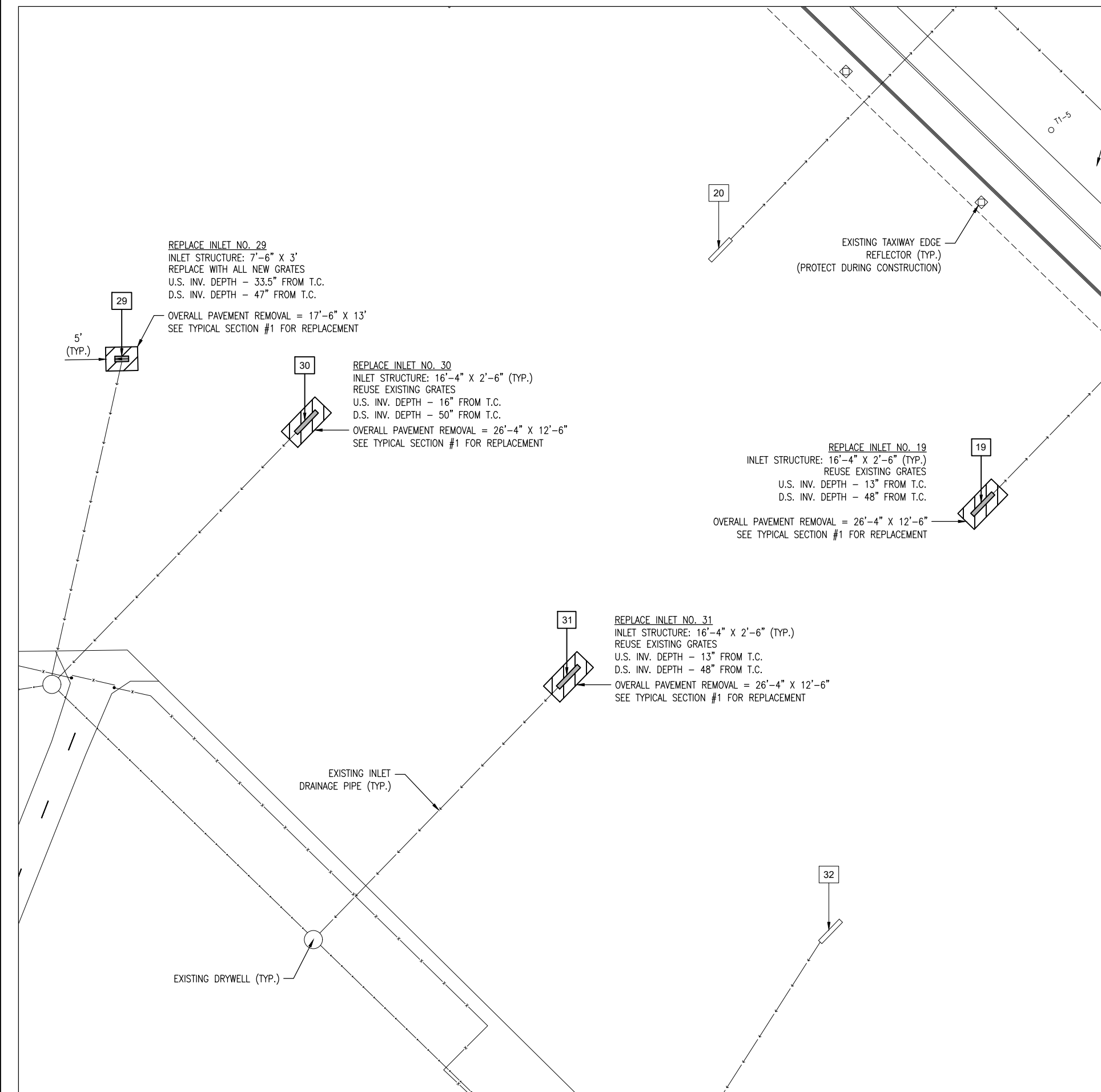


0' 15' 30' 60'
HALF SIZE SCALE: 1" = 60'
FULL SIZE SCALE: 1" = 30'

- LEGEND**
- EXISTING PAVEMENTS
 - EXISTING BUILDINGS
 - PROPOSED INLET CLEANING
 - PROPOSED INLET REPLACEMENT
 - PROPOSED PAVEMENT REPAIR
 - EXISTING INLET
 - INLET NUMBER
 - EXISTING INLET DRAINAGE PIPE
 - EXISTING DRYWELL
 - EXISTING TAXIWAY LIGHT/REFLECTOR

INLET REPLACEMENT NOTES:

1. THE CONTRACTOR SHALL REPLACE INLETS AT LOCATIONS SHOWN ON THIS SHEET AND IN ACCORDANCE WITH DETAILS AND SPECIFICATIONS.
2. ALL INVERT DEPTH MEASUREMENTS ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY PRIOR TO REPLACEMENT OF INLETS.
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REPAIR EXISTING INLETS IN MAIN APRON AREA

IDA No: LWV-4674

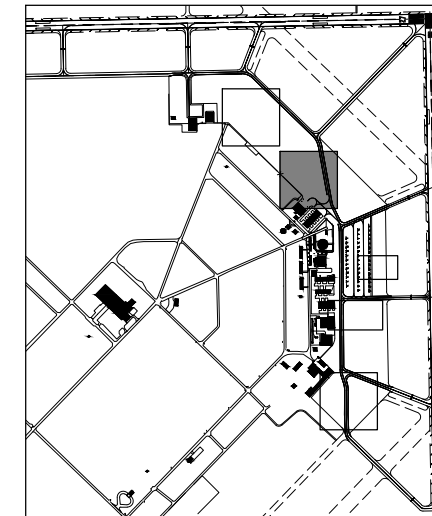
SBG No: 3-17-SBGP-144

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ISSUE: MARCH 1, 2019
PROJECT NO: 18A0093
CAD FILE: C-101-CON.DWG
DESIGN BY: JRH 02/06/2019
DRAWN BY: JRH 02/06/2019
REVIEWED BY: BSS 03/01/2019

SHEET TITLE

PROPOSED CONSTRUCTION PLAN - NORTH 1



KEY PLAN



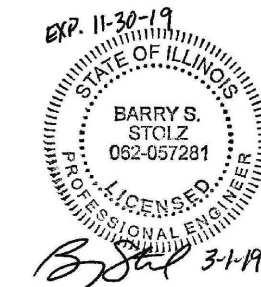
0' 15' 30' 60'
HALF SIZE SCALE: 1" = 60'
FULL SIZE SCALE: 1" = 30'

LEGEND

- EXISTING PAVEMENTS
- EXISTING BUILDINGS
- PROPOSED INLET CLEANING
- PROPOSED INLET REPLACEMENT
- PROPOSED PAVEMENT REPAIR
- EXISTING INLET
- INLET NUMBER
- EXISTING INLET DRAINAGE PIPE
- EXISTING DRYWELL
- EXISTING TAXIWAY LIGHT/REFLECTOR

INLET REPLACEMENT NOTES:

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REPAIR EXISTING INLETS IN MAIN APRON AREA

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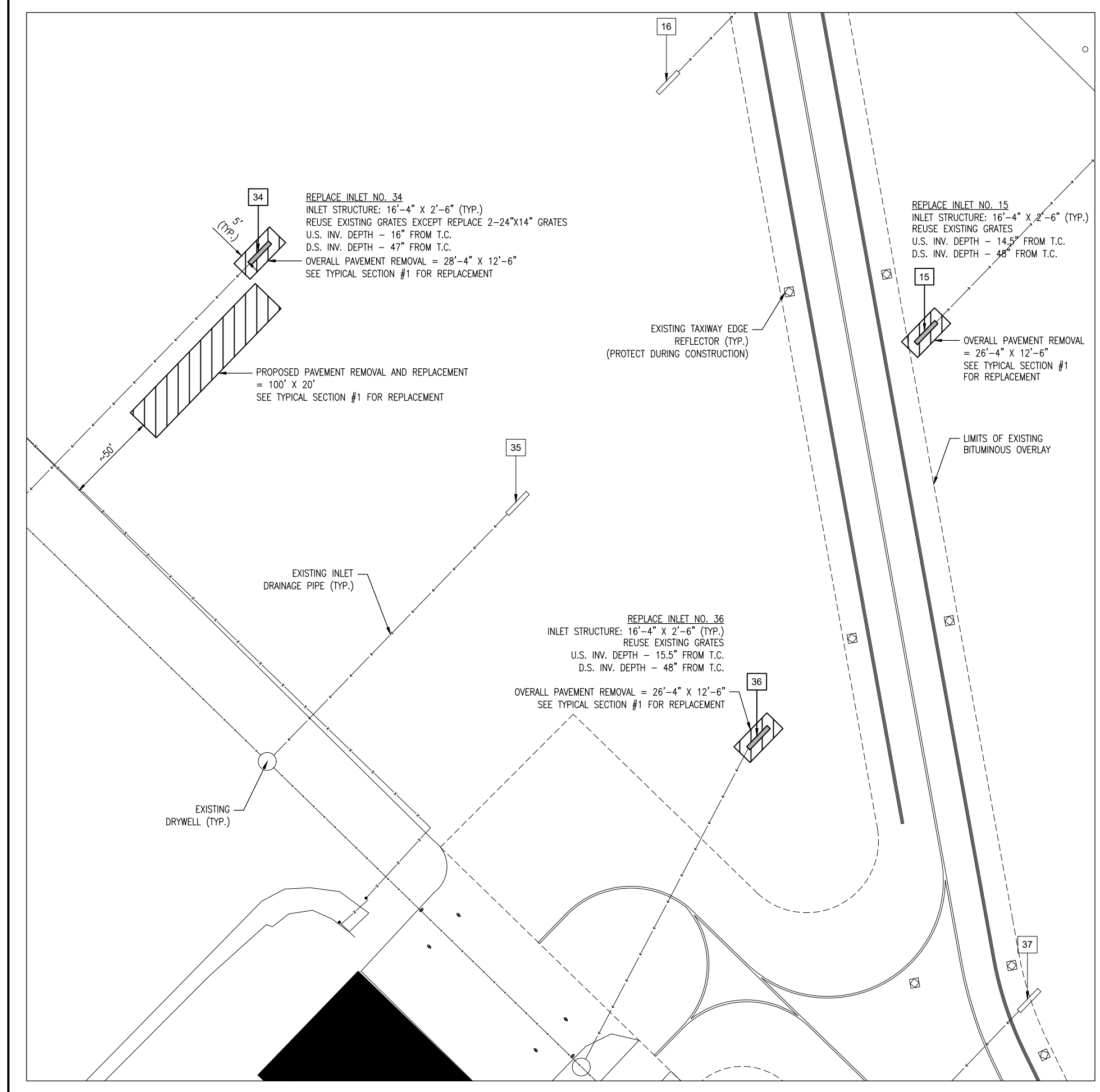
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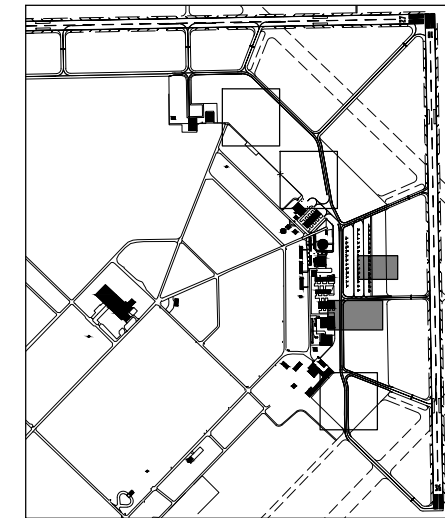
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SHEET TITLE

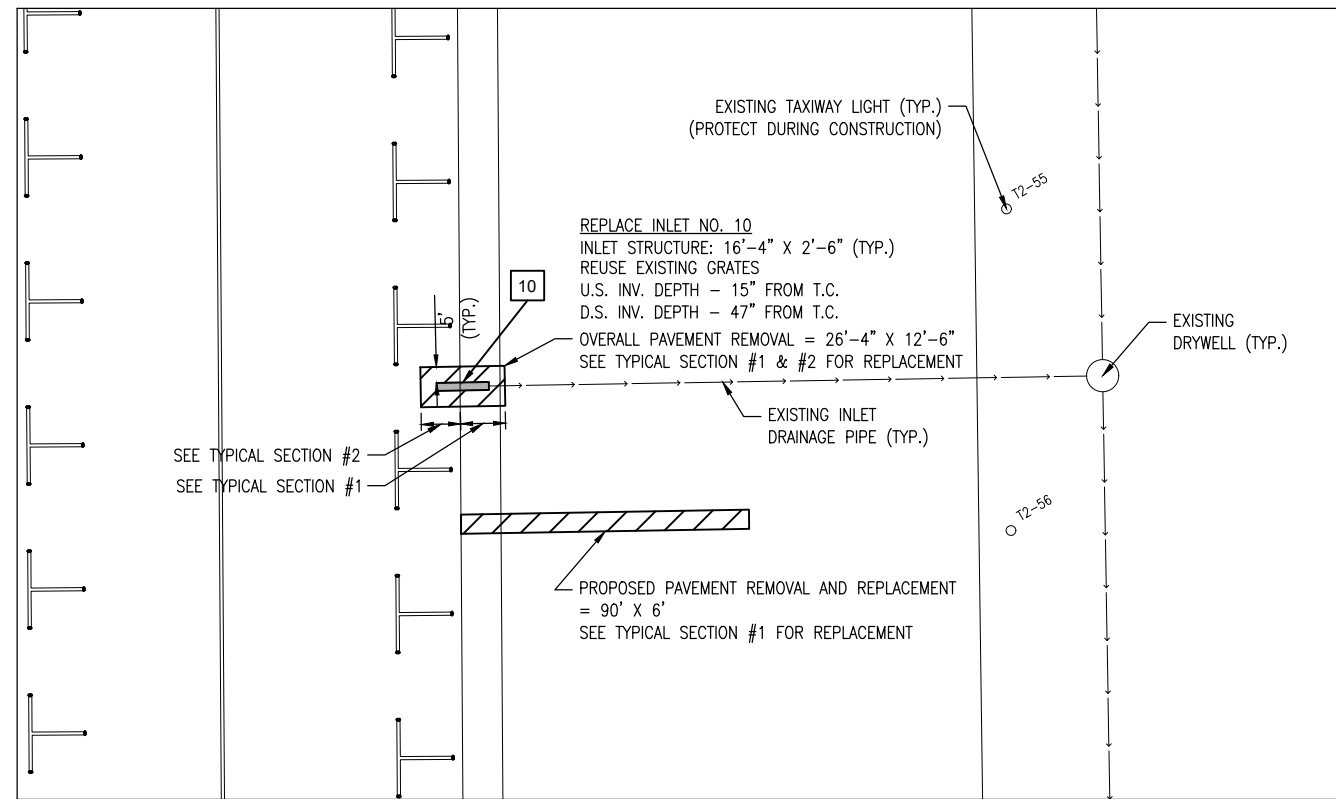
PROPOSED CONSTRUCTION PLAN - NORTH 2

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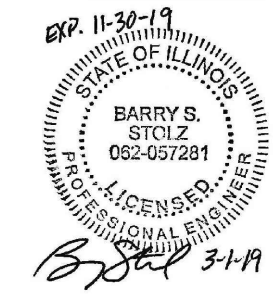
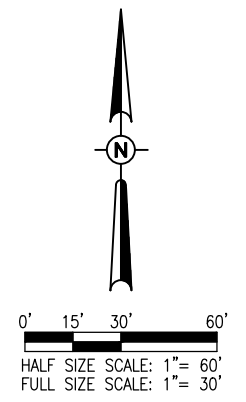




KEY PLAN



- LEGEND**
- EXISTING PAVEMENTS
 - EXISTING BUILDINGS
 - PROPOSED INLET CLEANING
 - PROPOSED INLET REPLACEMENT
 - PROPOSED PAVEMENT REPAIR
 - EXISTING INLET
 - INLET NUMBER
 - EXISTING INLET DRAINAGE PIPE
 - EXISTING DRYWELL
 - EXISTING TAXIWAY LIGHT/REFLECTOR



REPAIR EXISTING
INLETS IN MAIN
APRON AREA

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

INLET REPLACEMENT NOTES:

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ISSUE: MARCH 1, 2019

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CAD FILE: C-101-CON.DWG

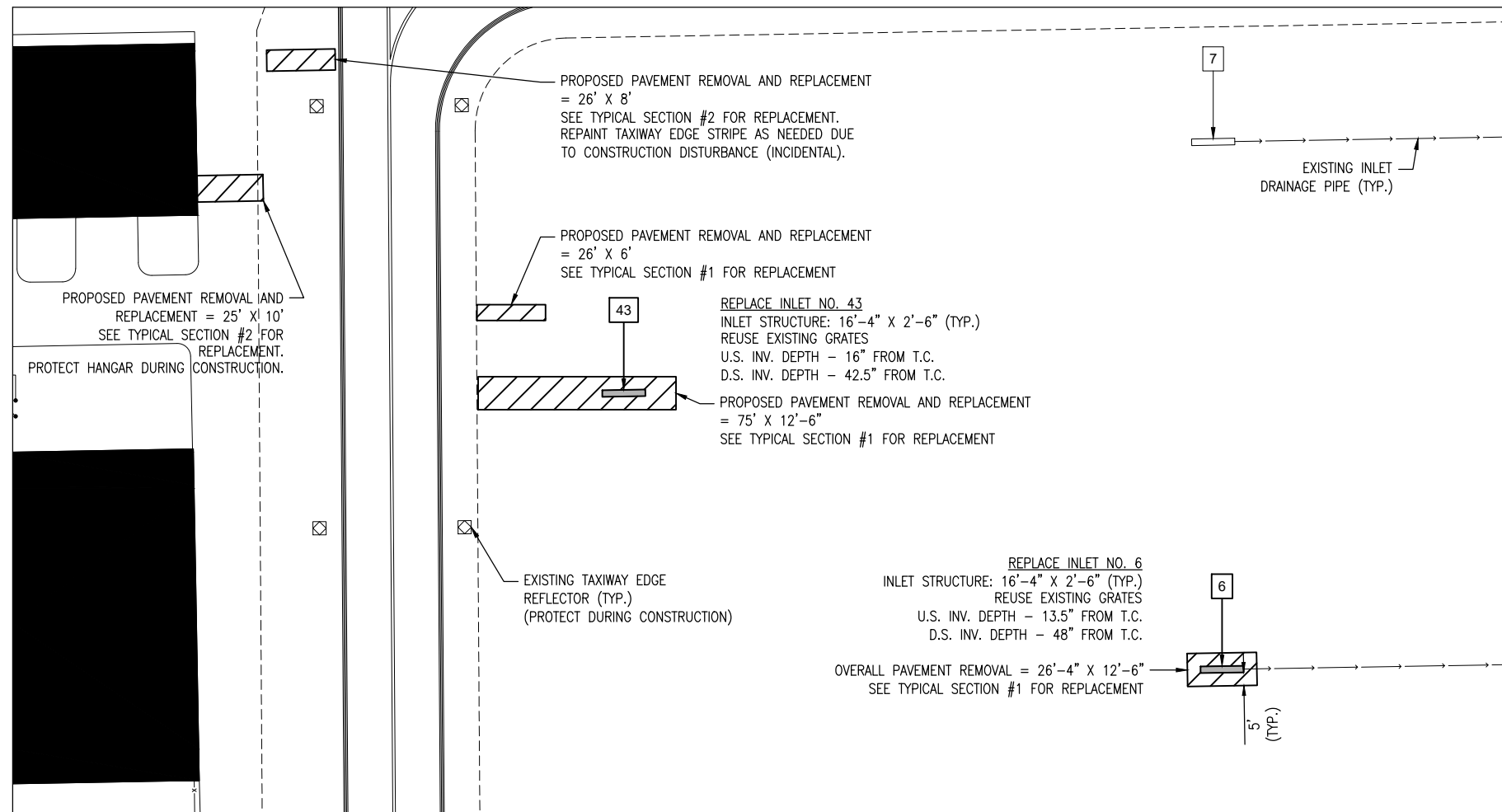
DESIGN BY: JRH 02/06/2019

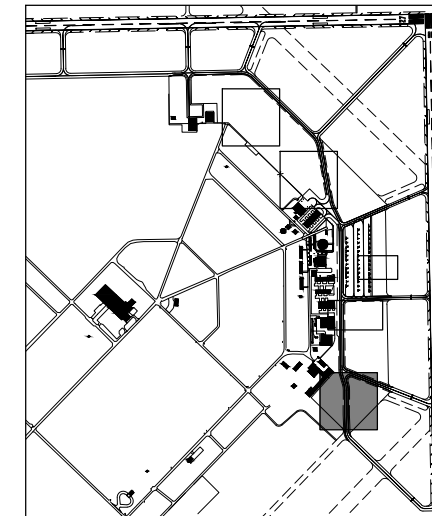
DRAWN BY: JRH 02/06/2019

REVIEWED BY: BSS 03/01/2019

SHEET TITLE

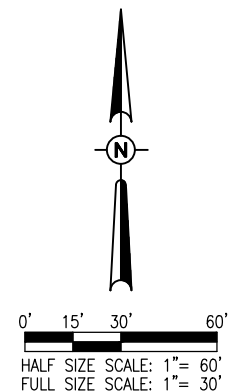
PROPOSED
CONSTRUCTION
PLAN - SOUTH 1





KEY PLAN

- LEGEND**
- EXISTING PAVEMENTS
 - EXISTING BUILDINGS
 - PROPOSED INLET CLEANING
 - PROPOSED INLET REPLACEMENT
 - PROPOSED PAVEMENT REPAIR
 - EXISTING INLET
 - INLET NUMBER
 - EXISTING INLET DRAINAGE PIPE
 - EXISTING DRYWELL
 - EXISTING TAXIWAY LIGHT/REFLECTOR



CLEAN INLET NO. 47 (AR800597)
EXISTING STRUCTURE TO REMAIN
INLET STRUCTURE: 16'-4" X 2'-6" (TYP.)
U.S. INV. DEPTH - 12.5" FROM T.C.
D.S. INV. DEPTH - ~46.5" FROM T.C. (NOT VISIBLE)
CLEAN EXISTING INLET AND DOWNSTREAM PIPE

CLEAN INLET NO. 48 (AR800597)
EXISTING STRUCTURE TO REMAIN
INLET STRUCTURE: 16'-4" X 2'-6" (TYP.)
U.S. INV. DEPTH - 14.5" FROM T.C.
D.S. INV. DEPTH - ~48" FROM T.C. (NOT VISIBLE)
CLEAN EXISTING INLET AND DOWNSTREAM PIPE

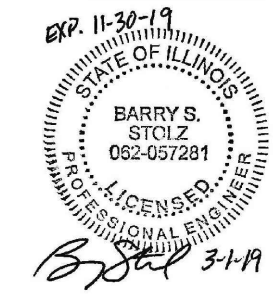
REPLACE INLET NO. 1
INLET STRUCTURE: 16'-4" X 2'-6" (TYP.)
REUSE EXISTING GRATES EXCEPT REPLACE 1-24"X14" GRATE
U.S. INV. DEPTH - 16" FROM T.C.
D.S. INV. DEPTH - 48" FROM T.C.

OVERALL PAVEMENT REMOVAL
= 26'-4" X 12'-6"
SEE TYPICAL SECTION #1
FOR REPLACEMENT

EXISTING TAXIWAY EDGE
REFLECTOR (TYP.)
(PROTECT DURING CONSTRUCTION)

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REPAIR EXISTING INLETS IN MAIN APRON AREA

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

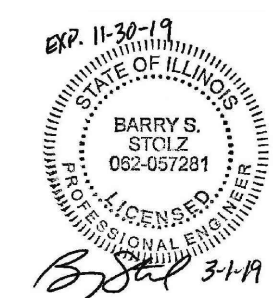
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ISSUE: MARCH 1, 2019

PROJECT NO: 18A0093
CAD FILE: C-101-CON.DWG
DESIGN BY: JRH 02/06/2019
DRAWN BY: JRH 02/06/2019
REVIEWED BY: BSS 03/01/2019

SHEET TITLE

PROPOSED CONSTRUCTION PLAN - SOUTH 2



**REPAIR EXISTING
INLETS IN MAIN
APRON AREA**

IDA No: LWV-4674

SBG No: 3-17-SBGP-144

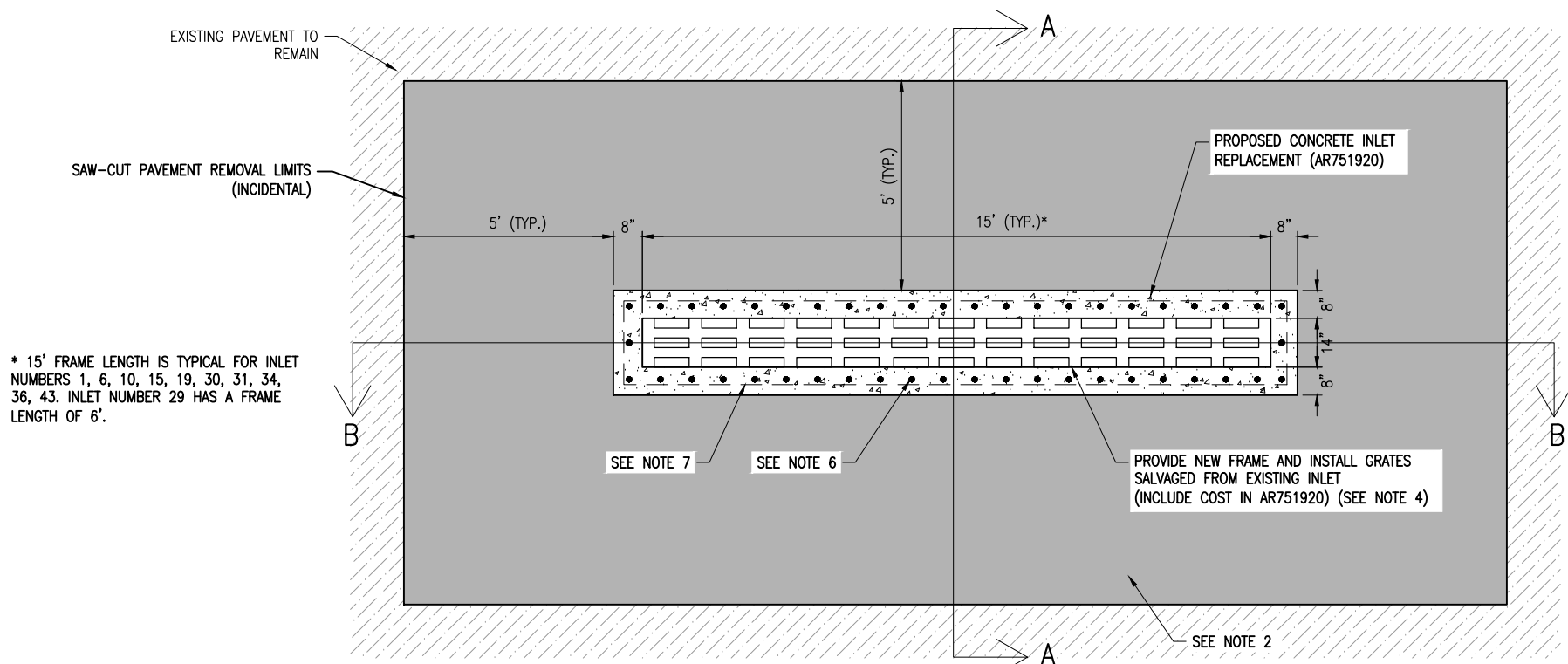
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ISSUE: MARCH 1, 2019
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DESIGN BY: BSS 02/06/2019
DRAWN BY: CWS 02/06/2019
REVIEWED BY: BSS 03/01/2019

SHEET TITLE

**TYPICAL
CONSTRUCTION
DETAILS 1**

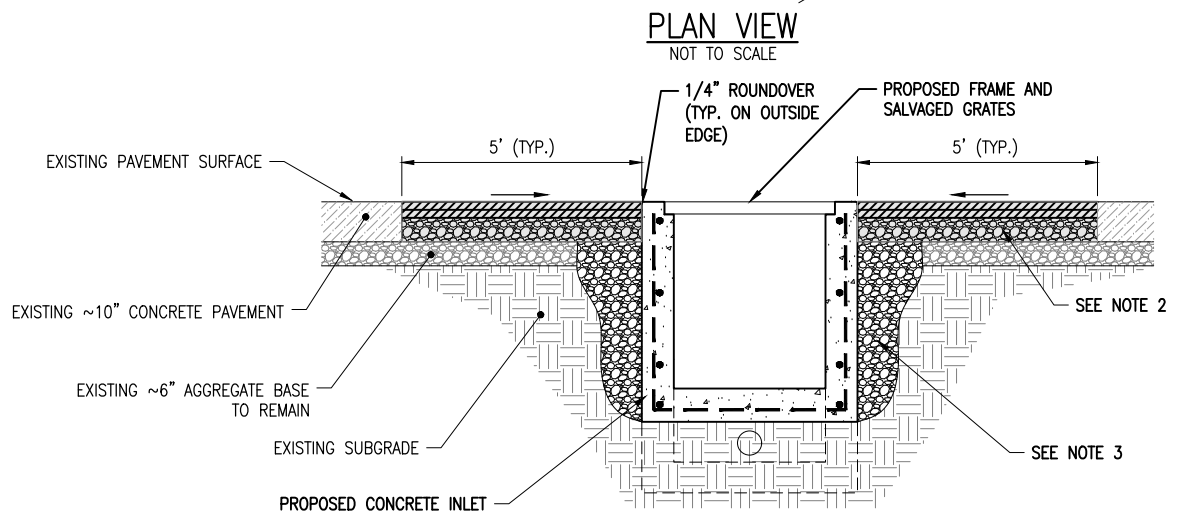


* 15' FRAME LENGTH IS TYPICAL FOR INLET NUMBERS 1, 6, 10, 15, 19, 30, 31, 34, 36, 43. INLET NUMBER 29 HAS A FRAME LENGTH OF 6'.

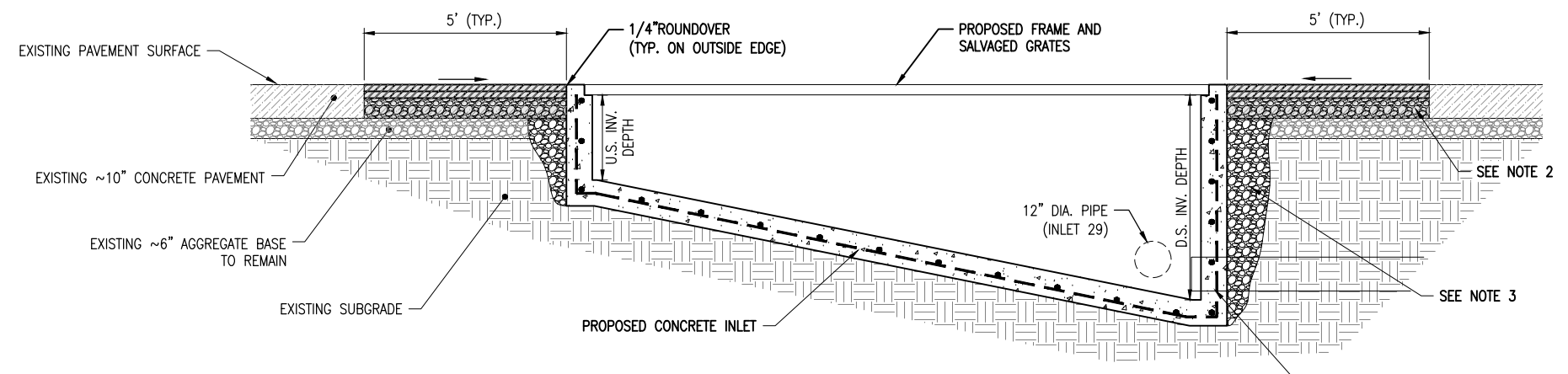
INLET REPLACEMENT NOTES:

- REPLACEMENT OF INLET STRUCTURES MAY OCCUR BY:
 - COMPLETE REMOVAL OF EXISTING STRUCTURE AND REPLACEMENT WITH A NEW PRE-CAST STRUCTURE,
 - COMPLETE REMOVAL OF EXISTING STRUCTURE AND REPLACEMENT WITH A CAST-IN-PLACE STRUCTURE, OR
 - PARTIAL REMOVAL OF THE EXISTING STRUCTURE AND RECONSTRUCTION WITH CAST-IN-PLACE COMPONENTS.

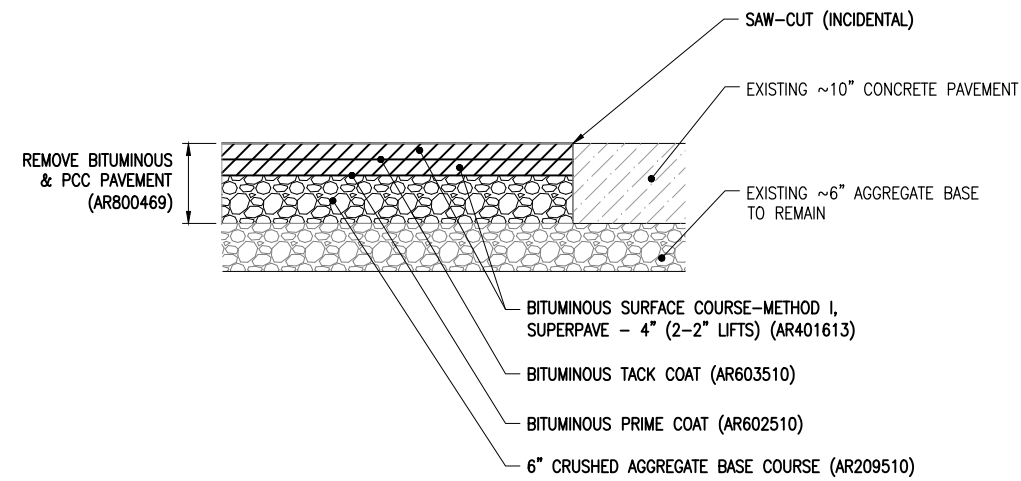
ANY PARTIAL REMOVAL/RECONSTRUCTION SHALL BE APPROVED BY THE RESIDENT ENGINEER/TECHNICIAN PRIOR TO REMOVAL, AND ALL OTHER DIMENSIONS AND CONSTRUCTION STANDARDS SHALL BE ADHERED TO IN ORDER TO MEET THE INTENT OF THE PROJECT SCOPE.
- REMOVE EXISTING PAVEMENT TO BOTTOM OF CONCRETE (~10" FOR TYPICAL SECTION #1; ~14" FOR TYPICAL SECTION #2) AND REPLACE WITH PROPOSED PAVEMENT AS SHOWN ON NEXT SHEET. NEW PAVEMENT SECTION SHALL BE PAID FOR UNDER THE ITEMS LISTED ON THE PROPOSED DETAIL.
- PROPOSED EARTH OR AGGREGATE BACKFILL, AS REQUIRED TO FILL VOID FROM INLET REPLACEMENT, SHALL NOT BE MEASURED FOR PAYMENT AND SHALL BE INCIDENTAL TO INLET REPLACEMENT, AR751920.
- THE CONTRACTOR SHALL OBTAIN A NEW INLET FRAME AND RE-USE THE EXISTING GRATES. THE FRAME SHALL BE NEENAH R4990-DX, TYPE C; EAST JORDAN V-7344, OR AN APPROVED EQUAL. CONTRACTOR SHALL VERIFY DIMENSIONS OF EXISTING GRATES WILL BE COMPATIBLE WITH NEW FRAMES PRIOR TO ORDERING.
- THE CONCRETE USED FOR THE REPLACEMENT INLET SHALL CONFORM TO IDOT AERONAUTICS STANDARD SPECIFICATION ITEM 610, STRUCTURAL PORTLAND CEMENT CONCRETE.
- VERTICAL REINFORCING BARS (NO. 4) SHALL BE SPACED 12" ON CENTER AROUND THE INLET PERIMETER. AS APPLICABLE, THE VERTICAL BARS SHALL BE CUT TO FIT IN LENGTH WITH A MINIMUM OF 3" OF THE BAR DRILLED AND GROUTED INTO THE EXISTING STRUCTURE.
- A HORIZONTAL STRESS RING (NO. 4 BAR) SHALL BE SPACED 6" MAX. AND RUN CONTINUOUSLY AROUND THE PERIMETER OF THE STRUCTURE AND SHALL BE TIED TO THE VERTICAL REINFORCING BARS.
- ALL REINFORCING STEEL SHALL HAVE A MINIMUM 2" COVER FROM THE CONCRETE WALL EDGES.
- THE PROPOSED INLET SHALL BE FLUSH WITH THE SURROUNDING PAVEMENT. THE REPLACEMENT PAVEMENT SHALL HAVE POSITIVE DRAINAGE TO THE NEW INLET.
- THICKNESS OF EXISTING FEATURES SHOWN MAY VARY BY LOCATION. NO ADJUSTMENTS WILL BE MADE TO PAY ITEMS OR UNIT PRICES DUE TO THICKNESS VARIATIONS.



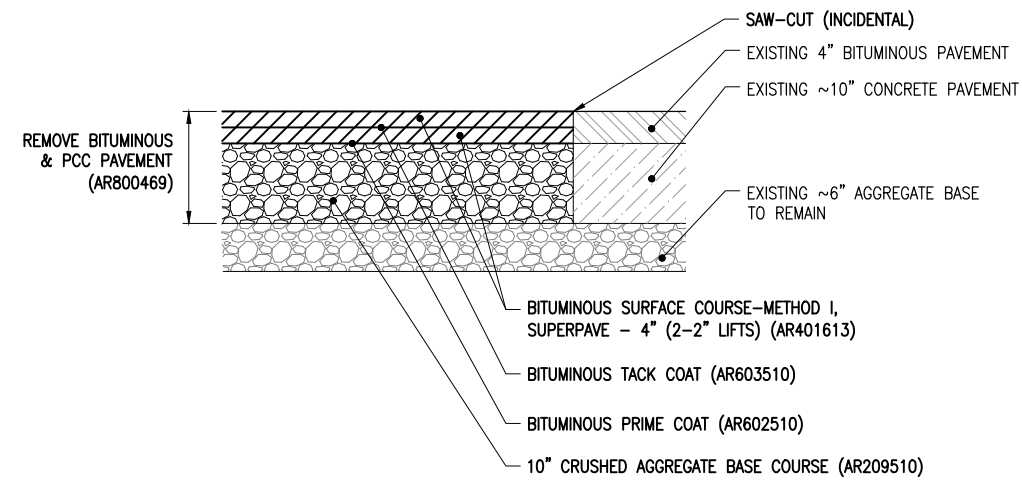
TYPICAL SECTION A-A
NOT TO SCALE



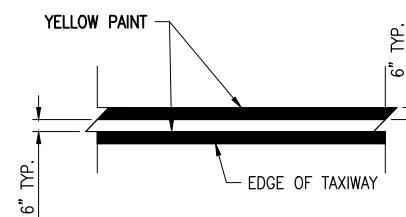
TYPICAL SECTION B-B
NOT TO SCALE



TYPICAL PAVEMENT REPLACEMENT SECTION #1
NOT TO SCALE

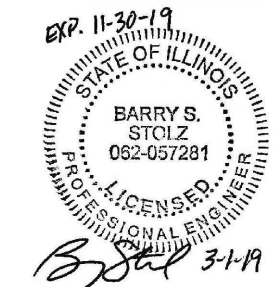


TYPICAL PAVEMENT REPLACEMENT SECTION #2
NOT TO SCALE



TAXIWAY EDGE STRIPE DETAIL
NOT TO SCALE

PAVEMENT MARKING SHALL BE INCIDENTAL TO OTHER PAY ITEMS.



REPAIR EXISTING
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SHEET TITLE

TYPICAL
CONSTRUCTION
DETAILS 2