

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	1
ILLINOIS			CONTRACT NO. 62G96	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PROPOSED HIGHWAY PLANS

**F.A.P. 346: US RTE 41
SOUTH OF CHANTILLY BOULEVARD TO LAKE-COOK ROAD &
RAMPS AT CLAVEY ROAD AND SKOKIE VALLEY ROAD
SECTION: 2018-051-RS&CR
RESURFACING (3P), PEDESTRIAN RAMPS
PROJECT: NHPP-NSJV(793)
LAKE COUNTY
C-91-283-18**

FOR INDEX OF SHEETS SEE SHEET NO.2

THE PROJECT IS LOCATED IN THE CITY OF HIGHLAND PARK

TRAFFIC DATA:
2017 ADT = 53,000 - 67,000
POSTED SPEED LIMIT = 55 MPH

DESIGN DESIGNATION:
OTHER PRINCIPAL ARTERIAL

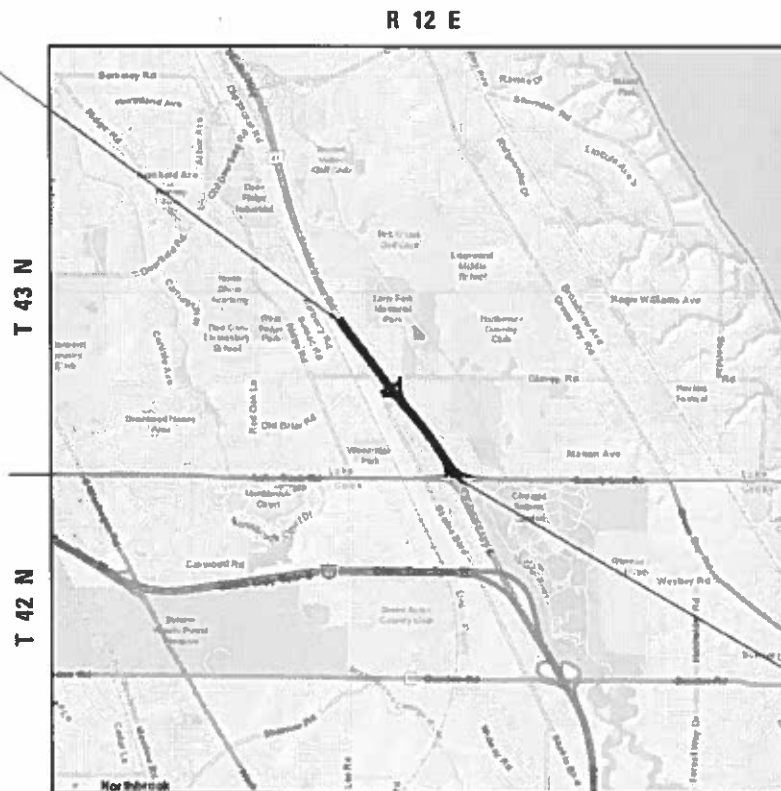


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

**PROJECT ENGINEER: KARI SMITH (847) 705-4437
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247**

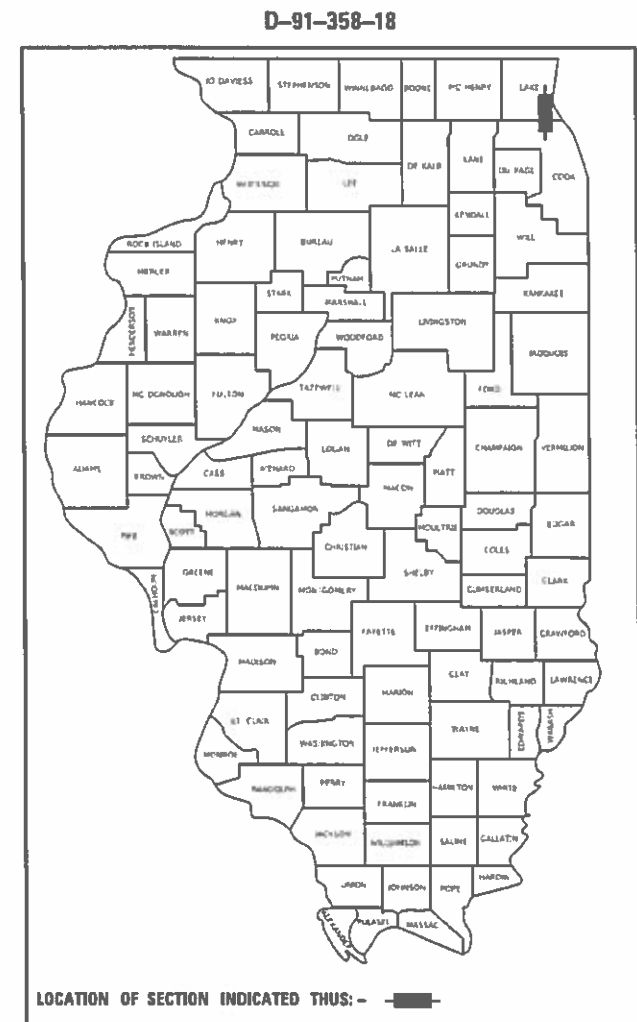
CONTRACT NO. 62G96



US RTE 41
START PROJECT:
STA. 13+00.00

MORaine TOWNSHIP

GROSS LENGTH = 5001.95 FT = 0.947 MI
NET LENGTH = 5001.95 FT = 0.947 MI



LOCATION OF SECTION INDICATED THUS: - [black rectangle] -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 29, 2019

Anthony J. Dingler / **AS**
REGIONAL ENGINEER

March 22, 2019

Paul P. [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

March 20, 2019

Paul P. [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS**

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LIST OF STATE STANDARDS

<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-05	DEPRESSED CORNER FOR SIDEWALKS
424031-02	MEDIAN PEDESTRIAN CROSSING
442001-04	CLASS A PATCHES
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
630001-12	STEEL PLATE BEAM GUARDRAIL
642001-02	SHOULDER RUMBLE STRIPS, 16 IN.
701400-09	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-12	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701428-01	TRAFFIC CONTROL, SETUP AND REMOVAL, FREEWAY/EXPRESSWAY
701446-09	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

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PLOT DATE = 3/18/2019	DATE - 1/31/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

INDEX OF SHEETS			
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. - TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	2
CONTRACT NO. 62G96			ILLINOIS FED. AID PROJECT	

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
2. TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND CITY OF HIGHLAND PARK.
4. THE CONTRACTOR SHALL CONTACT JIM MAYER, ILLINOIS TOLLWAY ENGINEERING MANAGER, AT (312) 402-0984 OR VIA EMAIL AT JMAYER@GETIPASS.COM TO COORDINATE CONSTRUCTION ACTIVITIES WITH NEARBY ILLINOIS TOLLWAY CONSTRUCTION PROJECT.
5. THE CONTRATOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
6. USE #8 EPOXY-COATED TIE BARS, CONFORMING TO ART. 1006.10 OF THE STANDARD SPECIFICATIONS, FOR ALL TIE BARS. USE THE "LONGITUDINAL CONSTRUCTION JOINT (TIE BAR GROUTED IN PLACE)" DETAIL SHOWN ON HIGHWAY STANDARD 420001 FOR ALL LONGITUDINAL JOINTS.
7. ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
8. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
9. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
10. ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
11. LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
12. DRAINAGE ADJUSTEMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
13. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
14. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (874) 705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
15. THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, AREA TRAFFIC FIELD ENGINEER, VIA EMAIL AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
16. THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULATIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MISTURE IS PLACED.
17. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLAN.
18. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPERATE PAY ITEM HAS BEEN PROVIDED.
19. FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
20. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
21. PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
22. WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 40 MPH (80 KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 KM/H). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
23. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX APSHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES	
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD	
SCALE: NONE	SHEET 1 OF 1 SHEETS
STA. -	TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-R5&CR	LAKE	31	3
CONTRACT NO. 62G96				
ILLINOIS		FED. AID PROJECT		

URBAN

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED. 20% STATE		
20200100	EARTH EXCAVATION	CU YD	7	7		
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	13	13		
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	1	1		
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	1	1		
25200110	SODDING, SALT TOLERANT	SQ YD	13	13		
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	46488	46488		
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	103	103		
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	2795	2795		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	295	295		
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	2023	2023		
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	2428	2428		
42001300	PROTECTIVE COAT	SQ YD	153	153		
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	664	664		
42400800	DETECTABLE WARNINGS	SQ FT	62	62		
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SQ YD	3182	3182		*

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED. 20% STATE		
44000600	SIDEWALK REMOVAL	SQ FT	424	424		
44003100	MEDIAN REMOVAL	SQ FT	221	221		
44200553	CLASS A PATCHES, TYPE II, 10 INCH	SQ YD	300	300		
44200557	CLASS A PATCHES, TYPE III, 10 INCH	SQ YD	800	800		
44200559	CLASS A PATCHES, TYPE IV, 10 INCH	SQ YD	1200	1200		
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	96	96		
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	248	248		
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	1195	1195		
44213000	PATCHING REINFORCEMENT	SQ YD	2300	2300		
44213200	SAW CUTS	FOOT	7116	7116		
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	226	226		
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2	2		
60300105	FRAMES AND LIDS TO BE ADJUSTED	EACH	55	55		
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	5	5		
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	13515	13515		
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	7	7		*

* SPECIALTY ITEMS
NON-PARTICIPATING ITEMS: 100% STATE

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES	
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD	
SCALE: NONE	SHEET 1 OF 3 SHEETS STA. - TO STA. -

F.A.P. RTE. 346	SECTION 2018-51-R5&CR	COUNTY LAKE	TOTAL SHEETS 31	SHEET NO. 4
CONTRACT NO. 62G96			ILLINOIS FED. AID PROJECT	

URBAN

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 ROADWAY	80% FED. 20% STATE	
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1		
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1		
* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	1	1		
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1		
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6		
67100100	MOBILIZATION	L SUM	1	1		
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1		
70300100	SHORT TERM PAVEMENT MARKING	FOOT	12336	12336		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	4112	4112		
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	412	412		
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	19119	19119		
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	3503	3503		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 ROADWAY	80% FED. 20% STATE	
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	599	599		
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	9327	9327		
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1419	1419		
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	169	169		
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	2844	2844		
70300530	PAVEMENT MARKING TAPE, TYPE III 5"	FOOT	1268	1268		
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	412	412		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	19119	19119		
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	599	599		
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	9327	9327		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1419	1419		
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	169	169		
* 78004354	PREFORMED PLASTIC PAVEMENT MARKING - TYPED - INLAID - LINE 4"	FOOT	725	725		
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1877	1877		
* 78004355	PREFORMED PLASTIC PAVEMENT MARKING - TYPED - INLAID - LINE 5"	FOOT	3503	3503		
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	814	814		
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	81	81		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	454	454		

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES		
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD		
SCALE: NONE	SHEET 2 OF 3 SHEETS	STA. - TO STA. -

F.A.P. RTE. 346	SECTION 2018-51-R5&CR	COUNTY LAKE	TOTAL SHEETS 31	SHEET NO. 5
			CONTRACT NO. 62G96	
ILLINOIS FED. AID PROJECT				

* SPECIALTY ITEMS
NON-PARTICIPATING ITEMS: 100% STATE

URBAN

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED. 20% STATE		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	400	400		
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	503	503		
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1		
X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1339	1339		
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	100	100		
* X2700012	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 8"	FOOT	266	266		
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	4166	4166		
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SQ YD	967	967		
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	1275	1275		
X4405030	LONGITUDINAL PARTIAL DEPTH REMOVAL 3"	FOOT	15975	15975		
X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	596	596		
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	200	200		
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	2	2		
X6330900	VERTICAL ADJUSTMENT OF GUARDRAIL	FOOT	1045	1045		
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1		

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE		
CODE NO	ITEM	UNIT		0005 ROADWAY 80% FED. 20% STATE		
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	17996	17996		
* X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	266	266		
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	380	380		
# Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	15	15		
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	77	77		

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

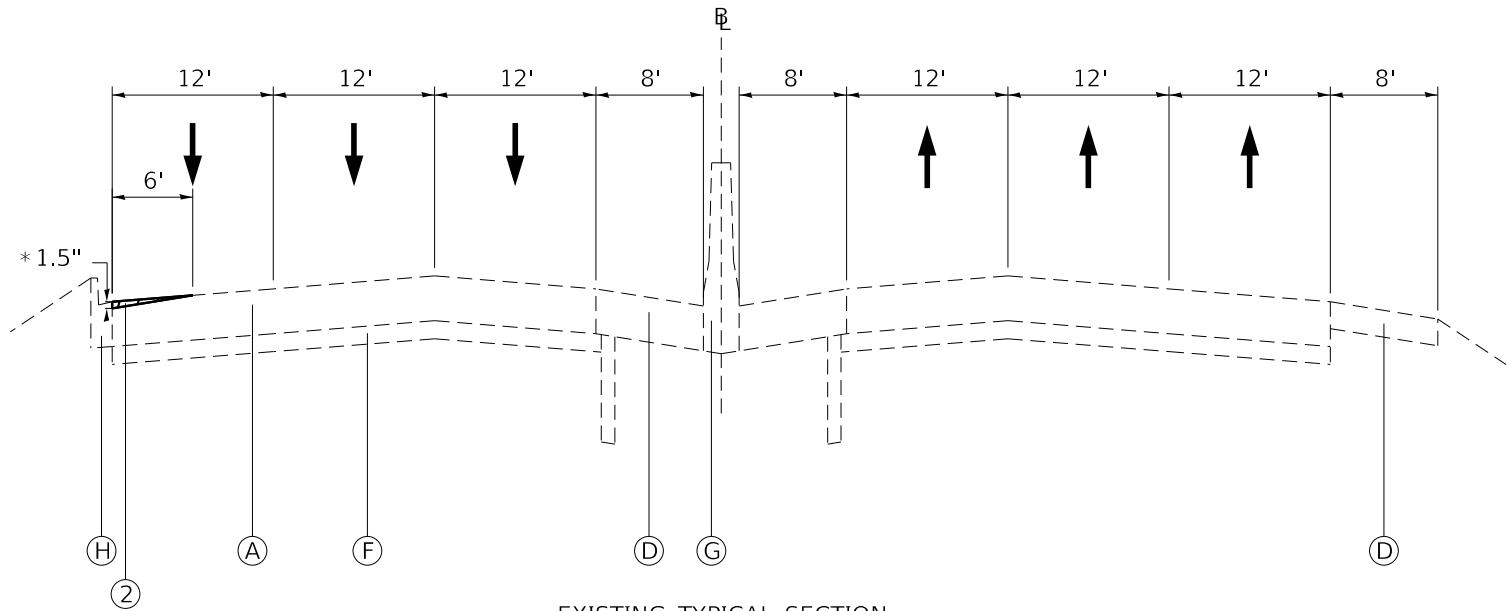
SCHEDULE OF QUANTITIES	
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD	
SCALE: NONE	SHEET 3 OF 3 SHEETS

F.A.P. RTE. 346	SECTION 2018-51-R5&CR	COUNTY LAKE	TOTAL SHEETS 31	SHEET NO. 6
CONTRACT NO. 62G96			ILLINOIS FED. AID PROJECT	

* SPECIALTY ITEMS
NON-PARTICIPATING ITEMS: 100% STATE

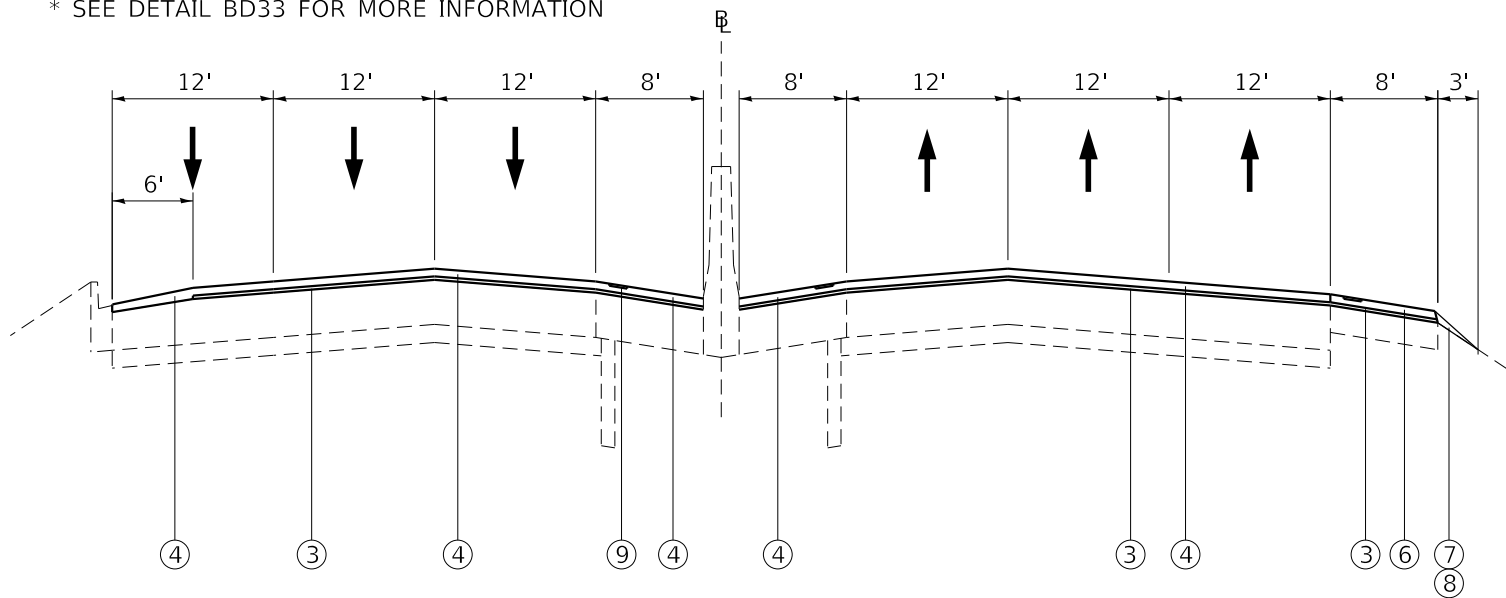
LEGEND

- Ⓐ EXISTING PCC PAVEMENT, CRC, 9.75" (CLASS A PATCHING)
- Ⓑ EXISTING PCC PAVEMENT, JOINTED, 9.75" (CLASS D PATCHING)
- Ⓒ EXISTING HMA PAVEMENT, 10" (CLASS D PATCHING)
- Ⓓ EXISTING PCC SHOULDER
- Ⓔ EXISTING HMA SHOULDER
- Ⓕ EXISTING AGGREGATE SUBBASE
- Ⓖ EXISTING PCC BARRIER
- Ⓗ EXISTING CURB AND GUTTER
- ① PROPOSED HMA SURFACE REMOVAL, 2.25"
- ② PROPOSED CONCRETE SURFACE REMOVAL, VARIABLE DEPTH
- ③ PROPOSED POLYMERIZED LEVELING BINDER, (MM), IL 4.75, N50, 0.75"
- ④ PROPOSED POLYMERIZED HMA SURFACE COURSE STONE MATRIX ASPHALT, 9.5, N80, 1.75"
- ⑤ PROPOSED HMA SURFACE COURSE, MIX D, N70, 1.5"
- ⑥ PROPOSED HMA SURFACE COURSE, MIX D, N70, 1.75"
- ⑦ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑧ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑨ PROPOSED 16" RUMBLE STRIPS



EXISTING TYPICAL SECTION
US RTE 41 STA 13+00.00 TO STA 63+01.95

* SEE DETAIL BD33 FOR MORE INFORMATION



PROPOSED TYPICAL SECTION
US RTE 41 STA 13+00.00 TO STA 63+01.95

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT AND SHOULDER RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, MIX D, N70 (IL 9.5 mm)	4% AT 70 GYR.	QC/QA
POLYMERIZED HMA SURFACE COURSE, SMA., 9.5, N80	3.5% AT 80 GYR.	QCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% AT 50 GYR.	QCP
PATCHING		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA
LONGITUDINAL PARTIAL DEPTH PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/QA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFM)		

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.
- NOTE 4: THE CONTRACTOR SHALL MILL FIRST ON HMA SURFACES.

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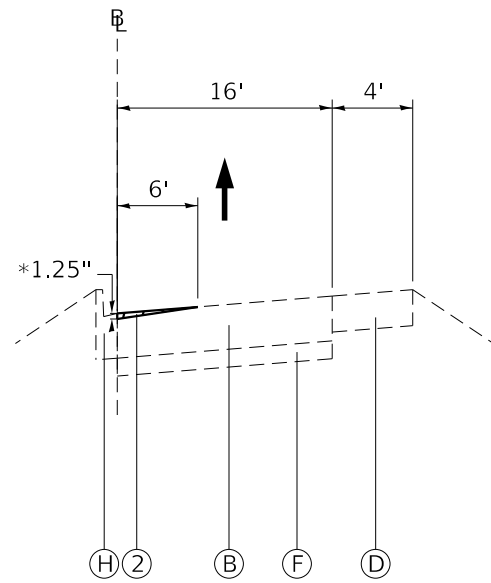
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS	
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD	
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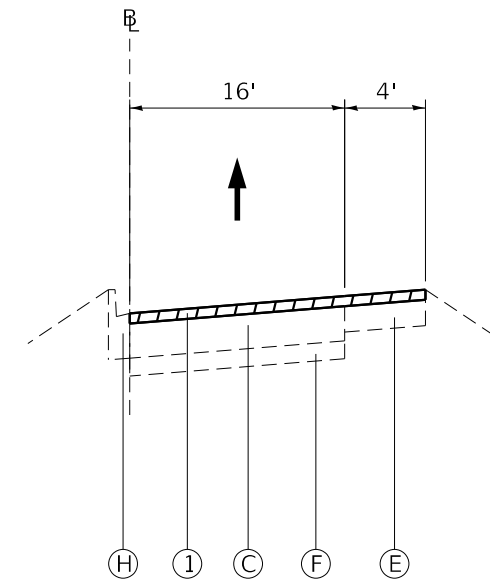
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	7
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				

LEGEND

- Ⓐ EXISTING PCC PAVEMENT, CRC, 9.75"
(CLASS A PATCHING)
- Ⓑ EXISTING PCC PAVEMENT, JOINTED, 9.75"
(CLASS D PATCHING)
- Ⓒ EXISTING HMA PAVEMENT, 10"
(CLASS D PATCHING)
- Ⓓ EXISTING PCC SHOULDER
- Ⓔ EXISTING HMA SHOULDER
- Ⓕ EXISTING AGGREGATE SUBBASE
- Ⓖ EXISTING PCC BARRIER
- Ⓗ EXISTING CURB AND GUTTER
- ① PROPOSED HMA SURFACE REMOVAL, 2.25"
- ② PROPOSED CONCRETE SURFACE REMOVAL, VARIABLE DEPTH
- ③ PROPOSED POLYMERIZED LEVELING BINDER, (MM), IL 4.75, N50, 0.75"
- ④ PROPOSED POLYMERIZED HMA SURFACE COURSE STONE MATRIX ASPHALT, 9.5, N80, 1.75"
- ⑤ PROPOSED HMA SURFACE COURSE, MIX D, N70, 1.5"
- ⑥ PROPOSED HMA SURFACE COURSE, MIX D, N70, 1.75"
- ⑦ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑧ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑨ PROPOSED 16" RUMBLE STRIPS

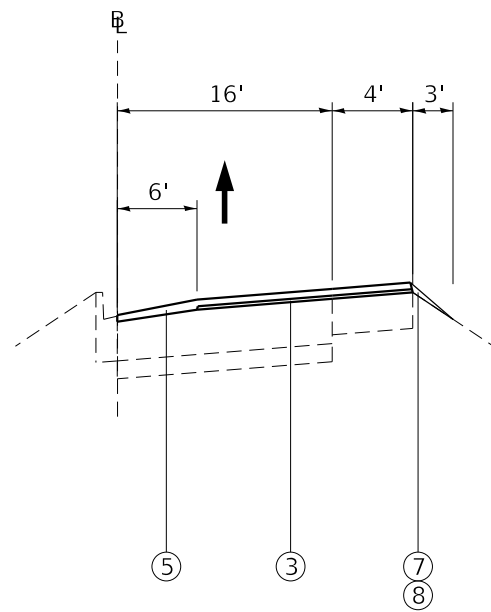


EXISTING TYPICAL SECTION
CLAVEY ROAD AND SKOKIE VALLEY ROAD RAMP

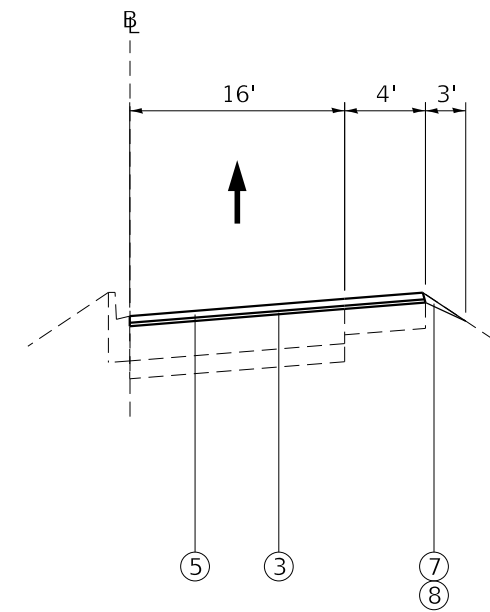


EXISTING TYPICAL SECTION
LAKE COOK ROAD RAMP

* SEE DETAIL BD33 FOR MORE INFORMATION



PROPOSED TYPICAL SECTION
CLAVEY ROAD AND SKOKIE VALLEY ROAD RAMP



PROPOSED TYPICAL SECTION
LAKE COOK ROAD RAMP

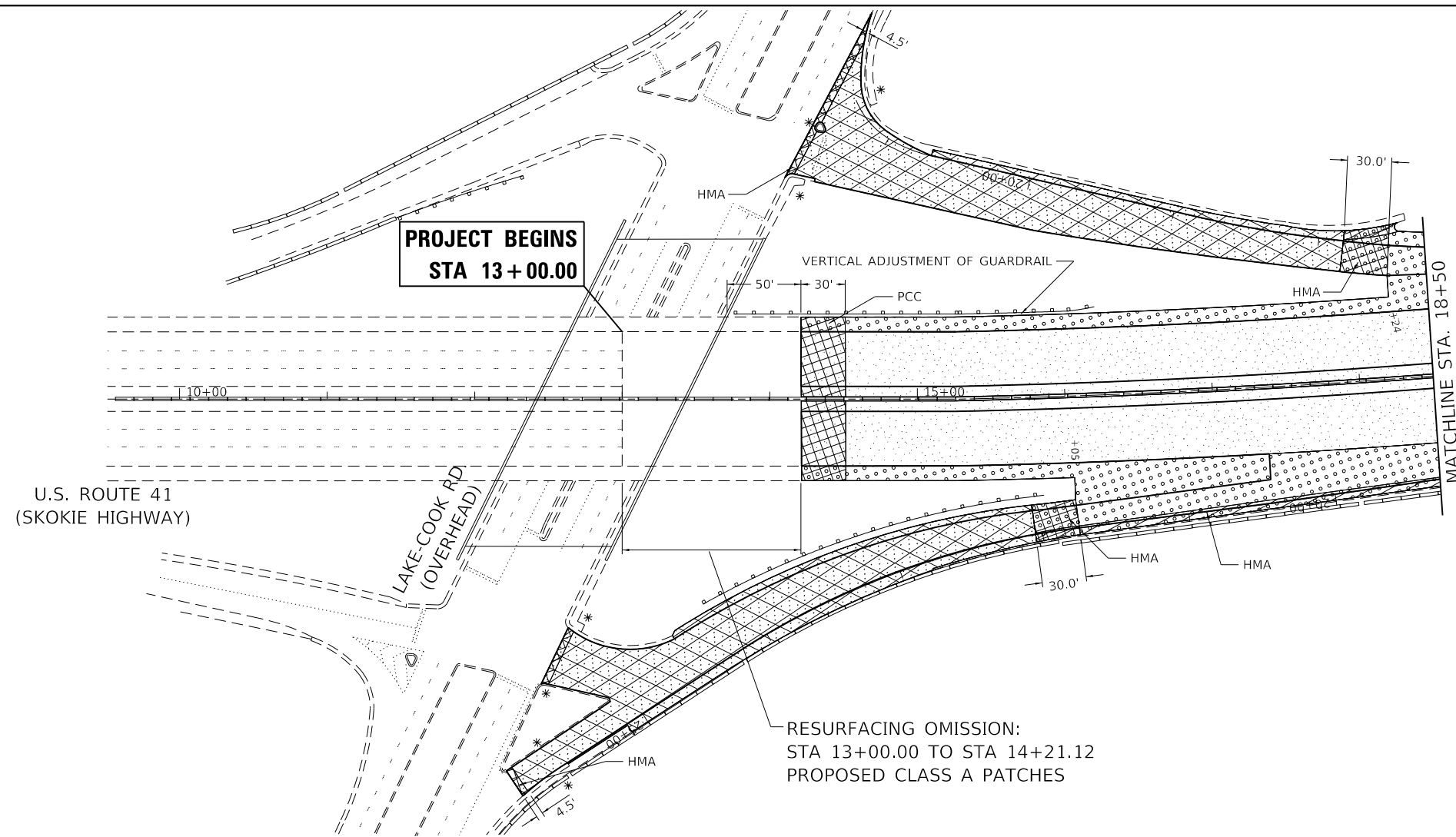
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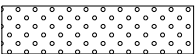

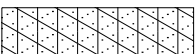
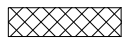
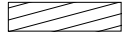
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

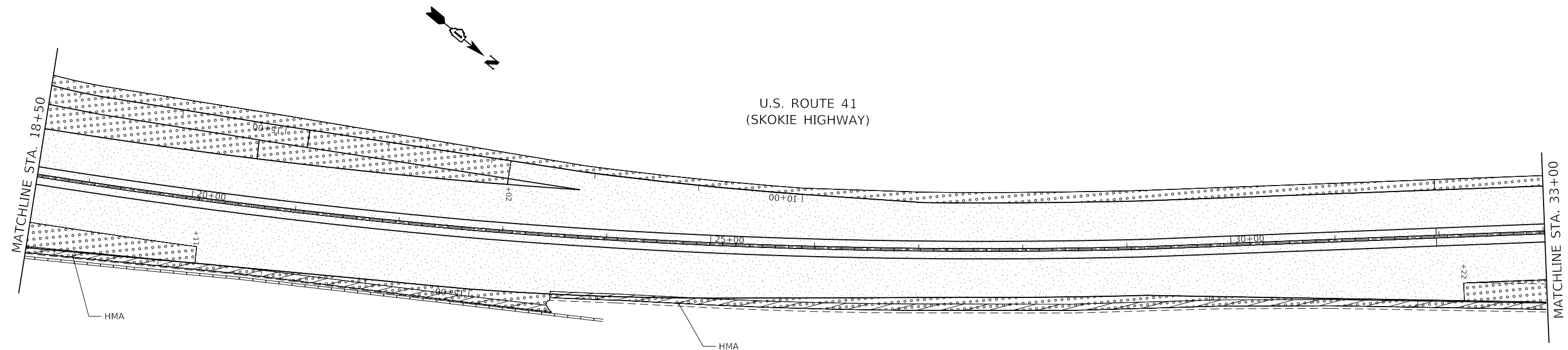
TYPICAL SECTIONS	
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD	
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STA. -	TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	8
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				



- LEGEND**
-  PROPOSED POLYMERIZED HOT-MIX SURFACE COURSE, SMA, 9.5, N80, 1.75"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
 -  PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.75"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
 -  PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.5"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
 -  PROPOSED HMA SURFACE REMOVAL 2.25"
PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.5"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
 -  SURFACE REMOVAL - BUTT JOINT
 -  SURFACE REMOVAL - VARIABLE DEPTH (0.0" - 1.5")

NOTES:
* PROPOSED ADA SIDEWALK RAMP



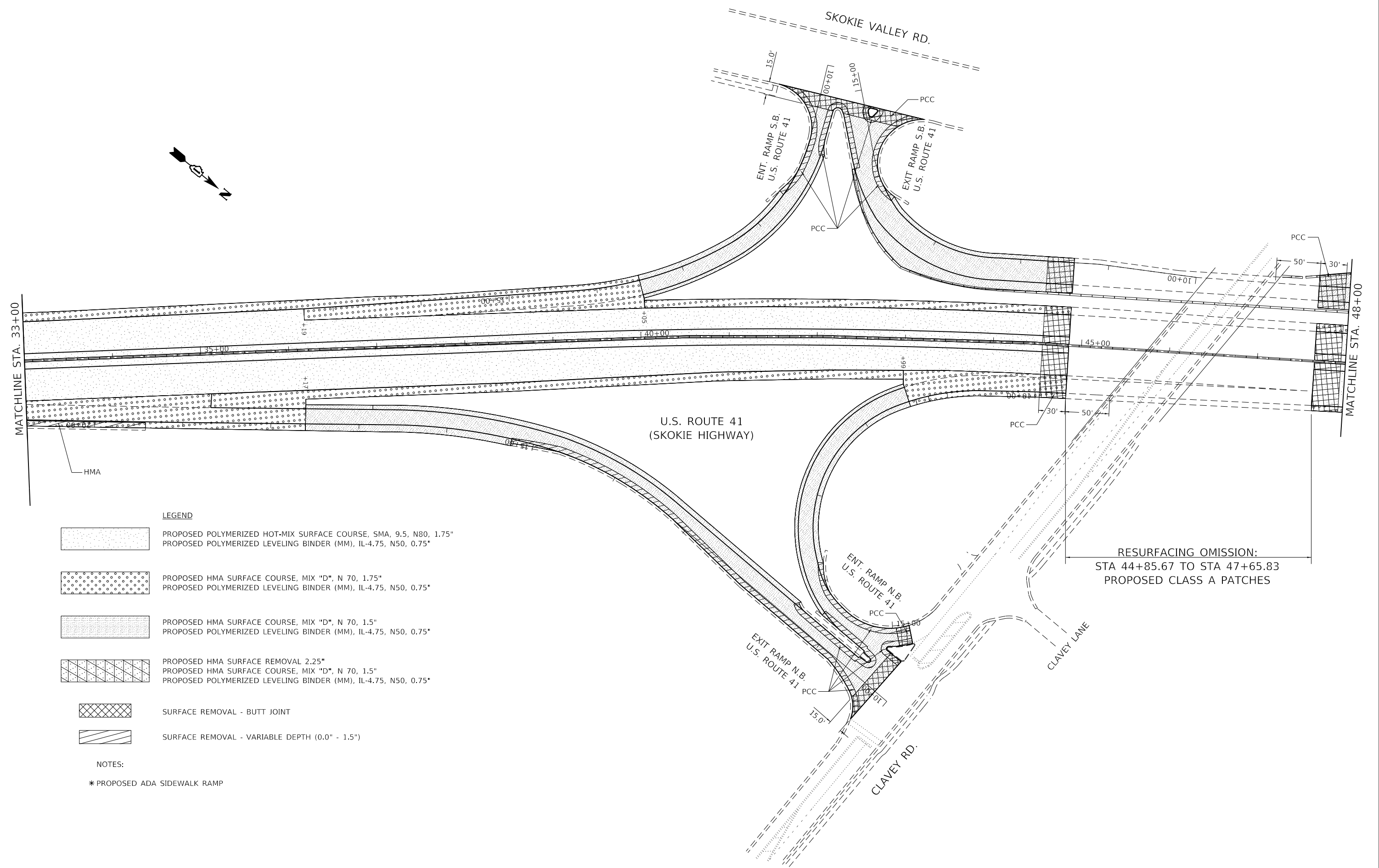
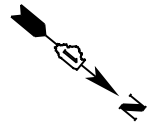
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD
 SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. - TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	9
CONTRACT NO. 62G96			ILLINOIS FED. AID PROJECT	



LEGEND

	PROPOSED POLYMERIZED HOT-MIX SURFACE COURSE, SMA, 9.5, N80, 1.75" PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
	PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.75" PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
	PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.5" PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
	PROPOSED HMA SURFACE REMOVAL 2.25" PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.5" PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
	SURFACE REMOVAL - BUTT JOINT
	SURFACE REMOVAL - VARIABLE DEPTH (0.0" - 1.5")

NOTES:
* PROPOSED ADA SIDEWALK RAMP

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

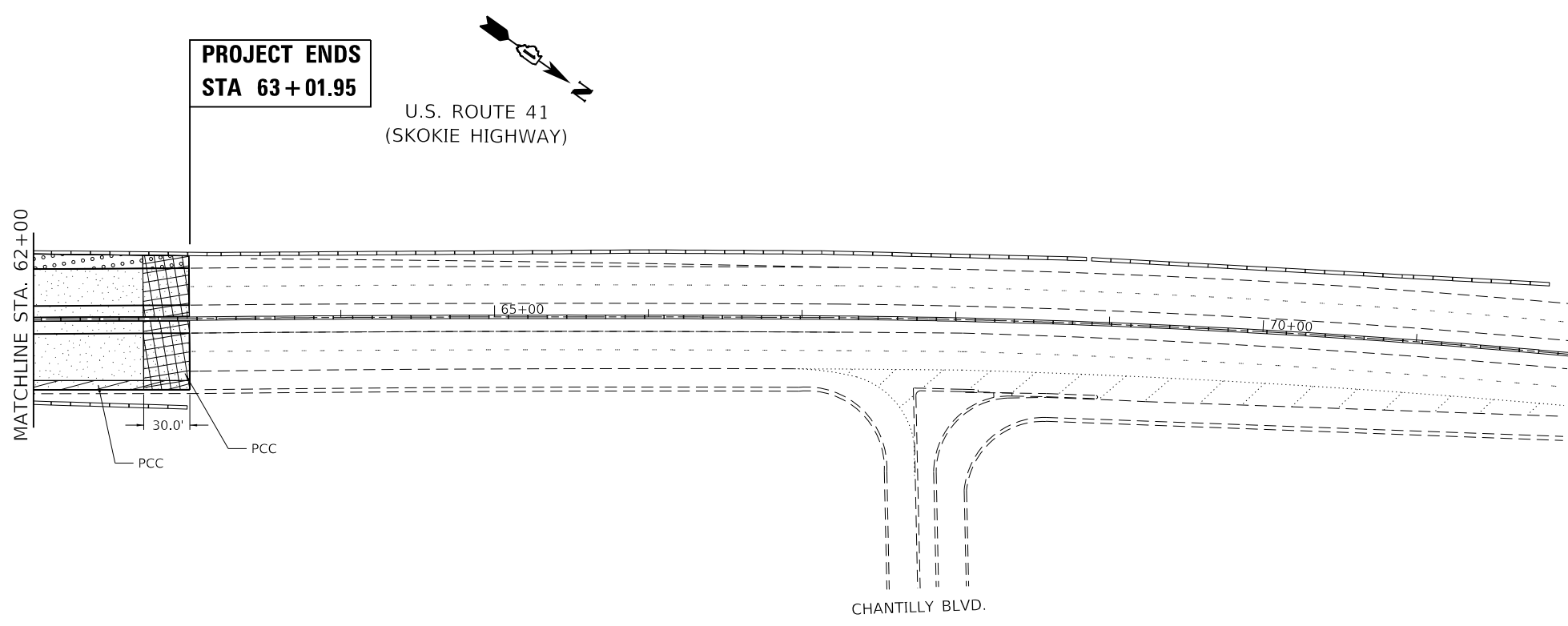
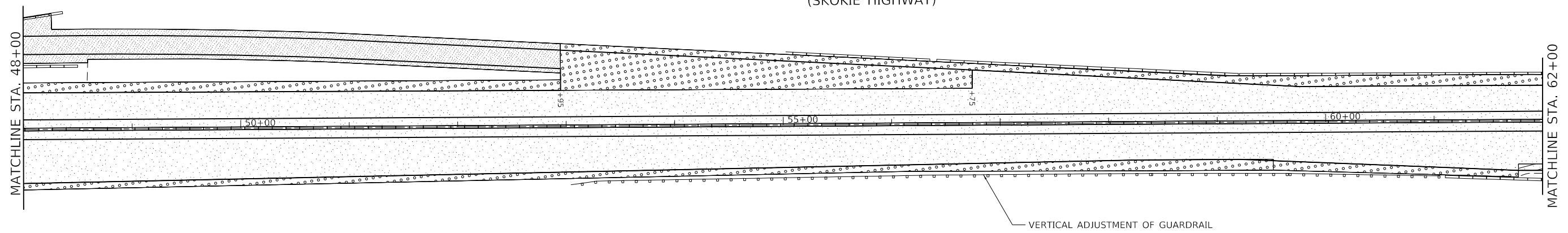
ROADWAY PLAN

US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD


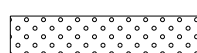




SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. - TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	10
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				

U.S. ROUTE 41
(SKOKIE HIGHWAY)



LEGEND

-  PROPOSED POLYMERIZED HOT-MIX SURFACE COURSE, SMA, 9.5, N80, 1.75"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
-  PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.75"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
-  PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.5"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
-  PROPOSED HMA SURFACE REMOVAL 2.25"
PROPOSED HMA SURFACE COURSE, MIX "D", N 70, 1.5"
PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 0.75"
-  SURFACE REMOVAL - BUTT JOINT
-  SURFACE REMOVAL - VARIABLE DEPTH (0.0" - 1.5")

NOTES:
* PROPOSED ADA SIDEWALK RAMP

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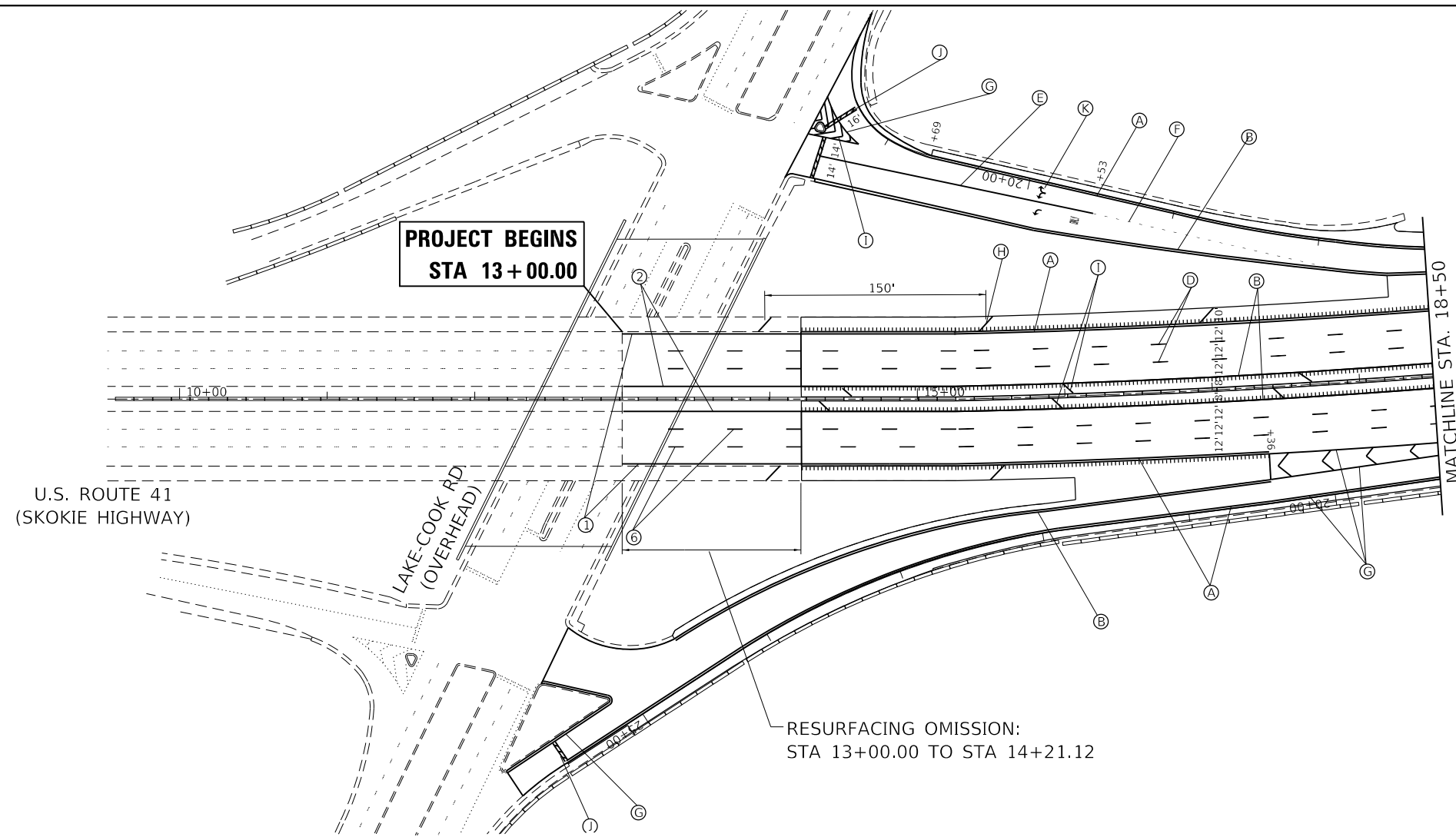
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD
SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. - TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	11
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 62G96



PAVEMENT MARKING LEGEND

- (A) THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" SOLID
- (B) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID
- (C) PREFORMED PLASTIC PAVEMENT MARKING TYPE D INLAID, WHITE, 4" DASHED, 2' DASH 6' SKIP
- (D) PREFORMED PLASTIC PAVEMENT MARKING TYPE D INLAID, WHITE, 5" DASHED, 10' DASH 30' SKIP
- (E) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" SOLID
- (F) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" DASHED, 2' DASH 6' SKIP
- (G) THERMOPLASTIC PAVEMENT MARKING, WHITE, 8" SOLID
- (H) THERMOPLASTIC PAVEMENT MARKING, WHITE, 12" SOLID
- (I) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 12" SOLID
- (J) THERMOPLASTIC PAVEMENT MARKING, WHITE, 24" SOLID
- (K) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- (1) MODIFIED URETHANE PAVEMENT MARKING, WHITE, 4" SOLID
- (2) MODIFIED URETHANE PAVEMENT MARKING, YELLOW, 4" SOLID
- (3) MODIFIED URETHANE PAVEMENT MARKING, WHITE, 8" SOLID
- (4) MODIFIED URETHANE PAVEMENT MARKING, WHITE, 12" SOLID
- (5) MODIFIED URETHANE PAVEMENT MARKING, YELLOW, 12" SOLID
- (6) PREFORMED PLASTIC PAVEMENT MARKING, TYPE D CONTRAST, WHITE, 7" DASHED, 10' DASH, 30' SKIP

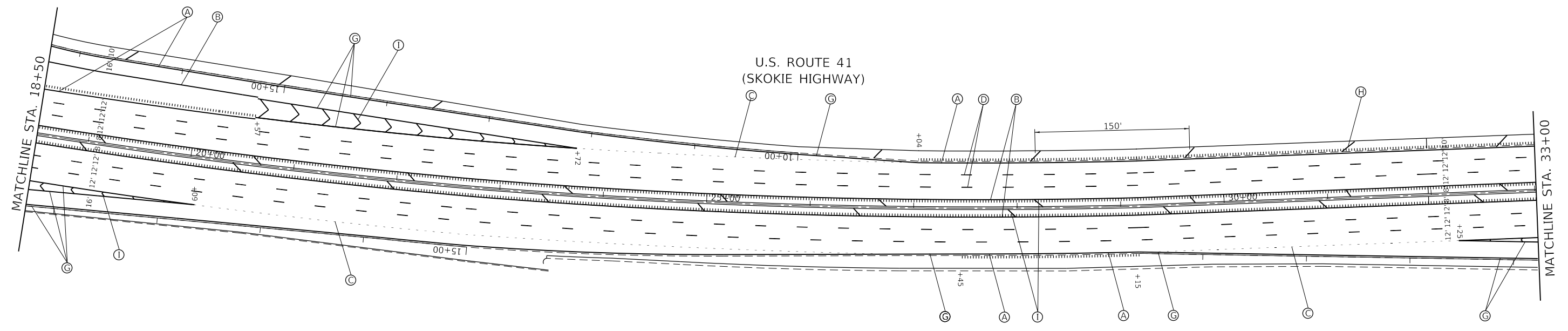
NOTES:

ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE STANDARD DETAILS TC-12 AND TC-13

ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE STANDARDS DETAILS TC-11 AND TC-12

EXISTING PAVEMENT MARKINGS INSIDE RESURFACING OMISSIONS ARE TO BE REMOVED USING PAVEMENT MARKING REMOVAL - WATER BLASTING

..... SHOULDER RUMBLE STRIPS, 16"



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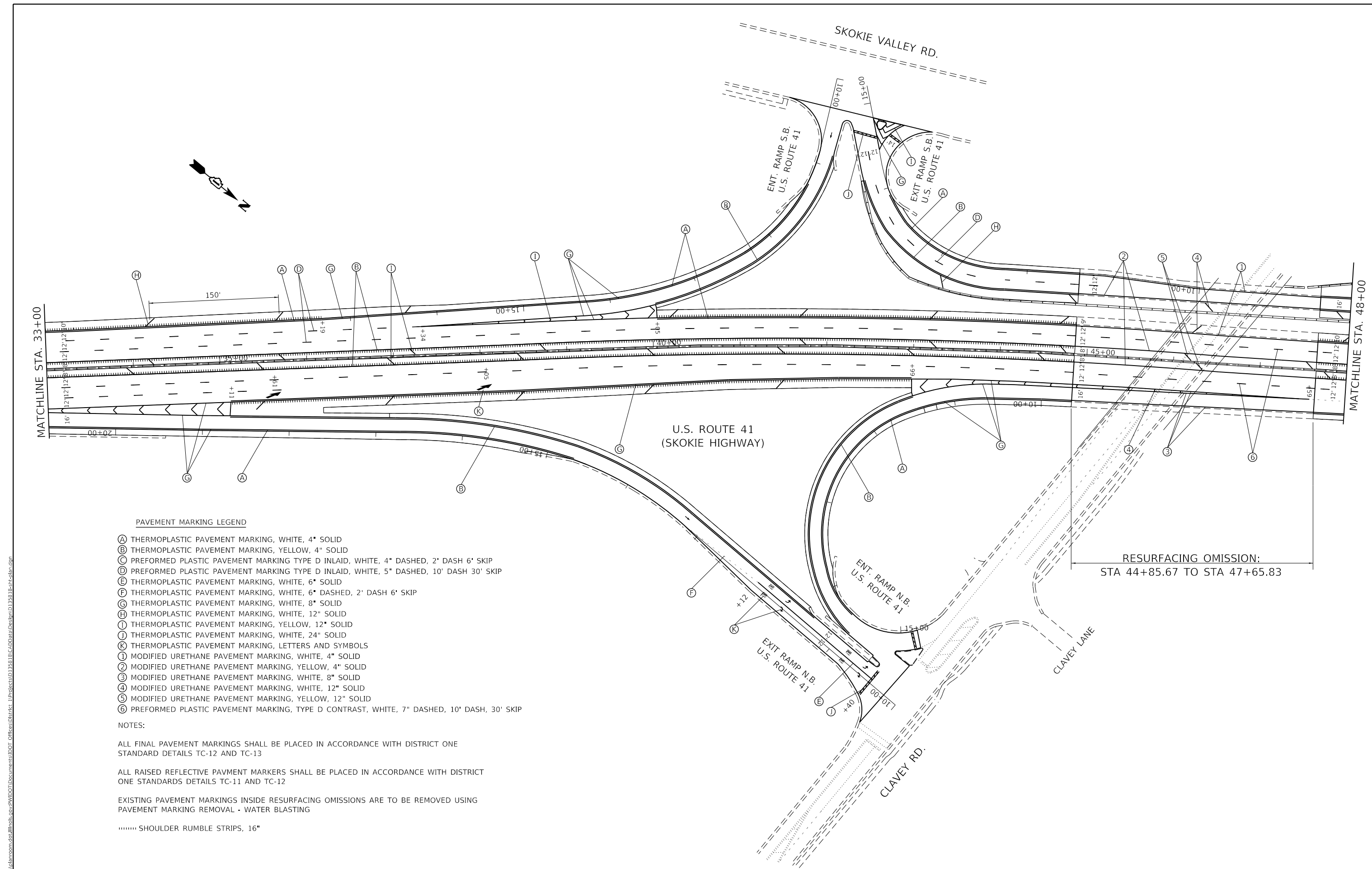
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-R5&CR	LAKE	31	12
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				

SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. - TO STA. -



PAVEMENT MARKING LEGEND

- (A) THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" SOLID
- (B) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID
- (C) PREFORMED PLASTIC PAVEMENT MARKING TYPE D INLAID, WHITE, 4" DASHED, 2' DASH 6' SKIP
- (D) PREFORMED PLASTIC PAVEMENT MARKING TYPE D INLAID, WHITE, 5" DASHED, 10' DASH 30' SKIP
- (E) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" SOLID
- (F) THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" DASHED, 2' DASH 6' SKIP
- (G) THERMOPLASTIC PAVEMENT MARKING, WHITE, 8" SOLID
- (H) THERMOPLASTIC PAVEMENT MARKING, WHITE, 12" SOLID
- (I) THERMOPLASTIC PAVEMENT MARKING, YELLOW, 12" SOLID
- (J) THERMOPLASTIC PAVEMENT MARKING, WHITE, 24" SOLID
- (K) THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- (1) MODIFIED URETHANE PAVEMENT MARKING, WHITE, 4" SOLID
- (2) MODIFIED URETHANE PAVEMENT MARKING, YELLOW, 4" SOLID
- (3) MODIFIED URETHANE PAVEMENT MARKING, WHITE, 8" SOLID
- (4) MODIFIED URETHANE PAVEMENT MARKING, WHITE, 12" SOLID
- (5) MODIFIED URETHANE PAVEMENT MARKING, YELLOW, 12" SOLID
- (6) PREFORMED PLASTIC PAVEMENT MARKING, TYPE D CONTRAST, WHITE, 7" DASHED, 10' DASH, 30' SKIP

NOTES:

ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE STANDARD DETAILS TC-12 AND TC-13

ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE STANDARDS DETAILS TC-11 AND TC-12

EXISTING PAVEMENT MARKINGS INSIDE RESURFACING OMISSIONS ARE TO BE REMOVED USING PAVEMENT MARKING REMOVAL - WATER BLASTING

..... SHOULDER RUMBLE STRIPS, 16"

RESURFACING OMISSION:
STA 44+85.67 TO STA 47+65.83

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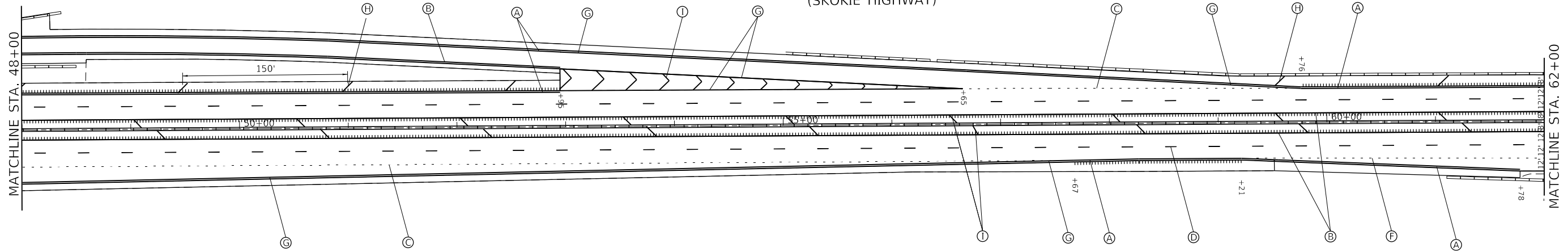
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN	
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD	
SCALE: 1"=50'	SHEET 2 OF 3 SHEETS
STA. -	TO STA. -

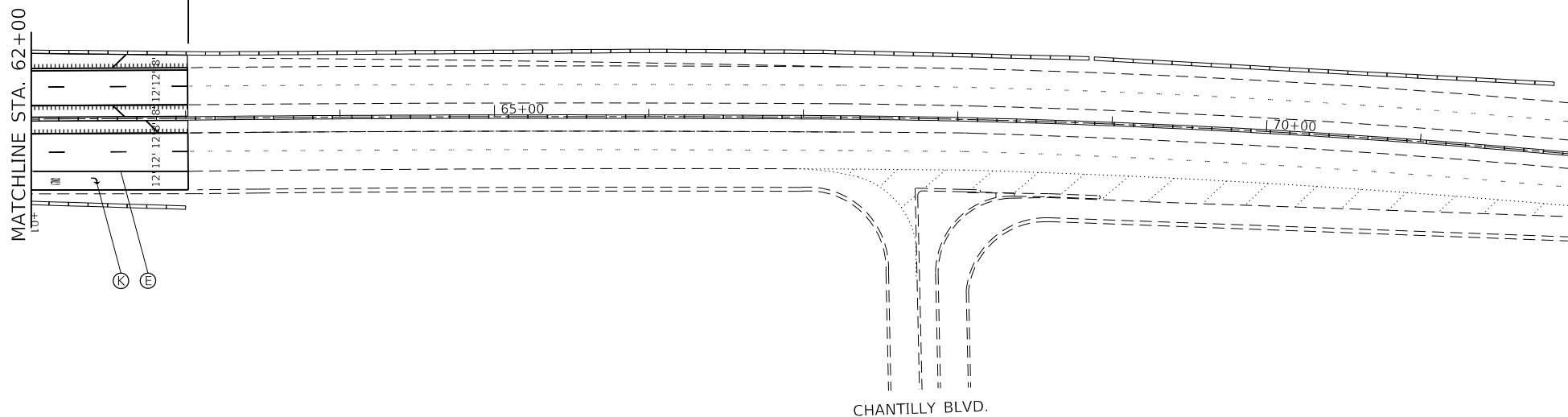
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346	2018-51-RS&CR	LAKE	31	13
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				

U.S. ROUTE 41
(SKOKIE HIGHWAY)



U.S. ROUTE 41
(SKOKIE HIGHWAY)

PROJECT ENDS
STA 63 + 01.95



PAVEMENT MARKING LEGEND

- Ⓐ THERMOPLASTIC PAVEMENT MARKING, WHITE, 4" SOLID
- Ⓑ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 4" SOLID
- Ⓒ PREFORMED PLASTIC PAVEMENT MARKING TYPE D INLAID, WHITE, 4" DASHED, 2' DASH 6' SKIP
- Ⓓ PREFORMED PLASTIC PAVEMENT MARKING TYPE D INLAID, WHITE, 5" DASHED, 10' DASH 30' SKIP
- Ⓔ THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" SOLID
- Ⓕ THERMOPLASTIC PAVEMENT MARKING, WHITE, 6" DASHED, 2' DASH 6' SKIP
- Ⓖ THERMOPLASTIC PAVEMENT MARKING, WHITE, 8" SOLID
- Ⓗ THERMOPLASTIC PAVEMENT MARKING, WHITE, 12" SOLID
- Ⓘ THERMOPLASTIC PAVEMENT MARKING, YELLOW, 12" SOLID
- Ⓡ THERMOPLASTIC PAVEMENT MARKING, WHITE, 24" SOLID
- Ⓚ THERMOPLASTIC PAVEMENT MARKING, LETTERS AND SYMBOLS
- ① MODIFIED URETHANE PAVEMENT MARKING, WHITE, 4" SOLID
- ② MODIFIED URETHANE PAVEMENT MARKING, YELLOW, 4" SOLID
- ③ MODIFIED URETHANE PAVEMENT MARKING, WHITE, 8" SOLID
- ④ MODIFIED URETHANE PAVEMENT MARKING, WHITE, 12" SOLID
- ⑤ MODIFIED URETHANE PAVEMENT MARKING, YELLOW, 12" SOLID
- ⑥ PREFORMED PLASTIC PAVEMENT MARKING, TYPE D CONTRAST, WHITE, 7" DASHED, 10' DASH, 30' SKIP

NOTES:

ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE STANDARD DETAILS TC-12 AND TC-13

ALL RAISED REFLECTIVE PAVMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH DISTRICT ONE STANDARDS DETAILS TC-11 AND TC-12

EXISTING PAVEMENT MARKINGS INSIDE RESURFACING OMISSIONS ARE TO BE REMOVED USING PAVEMENT MARKING REMOVAL - WATER BLASTING

..... SHOULDER RUMBLE STRIPS, 16"

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING PLAN		
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD		
SCALE: 1"=50'	SHEET 3 OF 3 SHEETS	STA. - TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	14
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				

ADA SIDEWALK SCHEDULE OF QUANTITIES

LOCATION			20200100	21101615	25200110	42001300	42400200	44000600	42400800	44003100	Z0004562	85000200
MAIN ROAD	CROSS ROAD	CORNER	EARTH EXCAVATION (CUYD)	TOPSOIL FURNISH AND PLACE, 4"	SODDING, SALT TOLERANT (SQYD)	PROTECTIVE COAT (SQYD)	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (SQFT)	SIDEWALK REMOVAL (SQFT)	DETECTABLE WARNINGS (SQFT)	MEDIAN REMOVAL (SQFT)	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (FT)	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION (EA)
		TOTALS	7	13	13	109	664	424	62	221	157	-
US RTE 41 SB EXIT RAMP	LAKE-COOK ROAD	W	0.93	4	4	15	93	80	11		23	-
		N ISLAND	0.25			10	25			25	33	
		E	1.38	4	4	23	138	137	11		35	
US RTE 41 NB ENTRANCE RAMP	LAKE-COOK ROAD	W	1.53	5	5	26	153	138	10		40	-
		N ISLAND	1.96			26	196		20	196	17	
		E	0.59			9	59	69	10		9	

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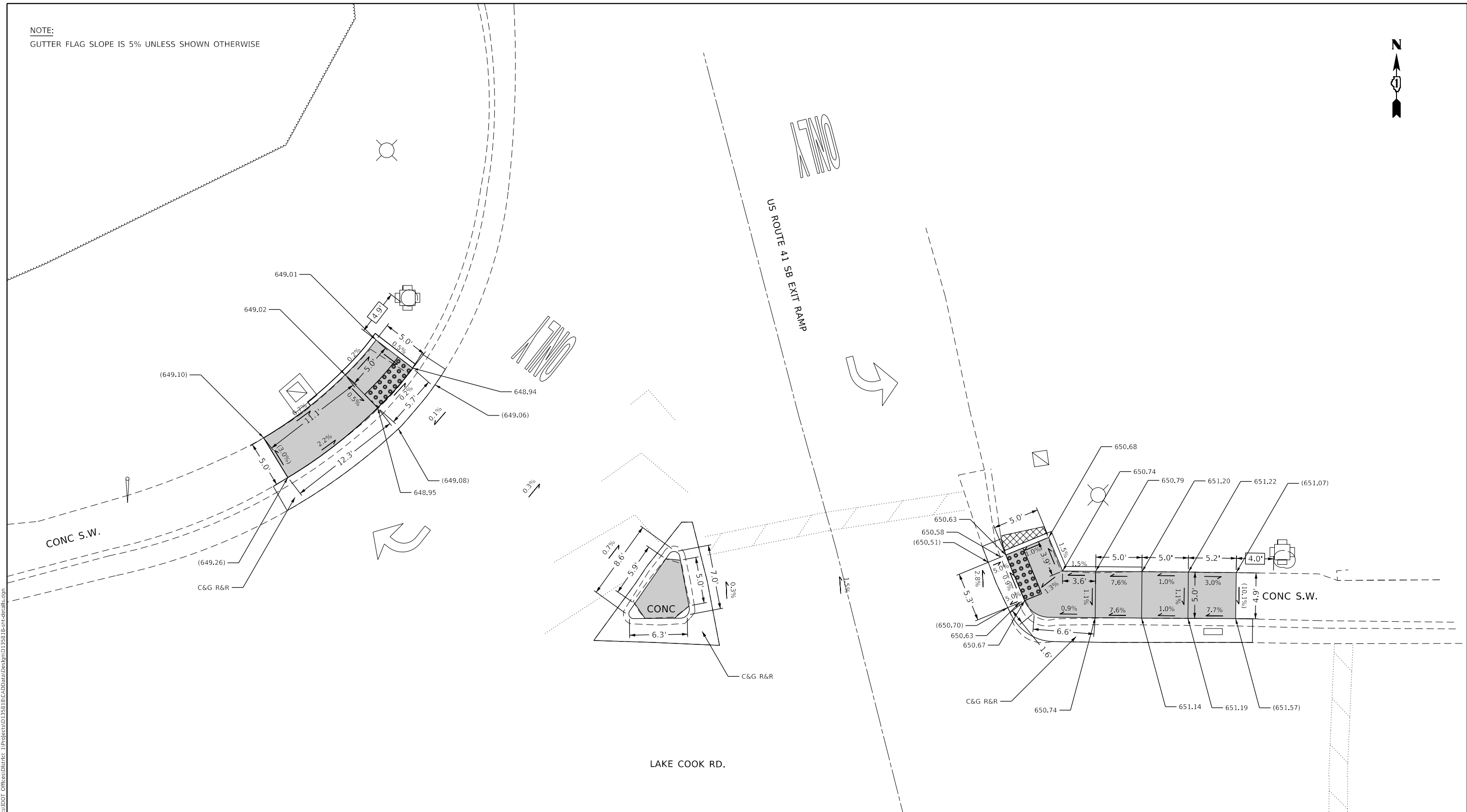
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA SIDEWALK SCHEDULE OF QUANTITIES			
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD			
SCALE: 1"=50'	SHEET 1	OF 1 SHEETS	STA. - TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	14A
CONTRACT NO. 62G96			ILLINOIS FED. AID PROJECT	

NOTE:
GUTTER FLAG SLOPE IS 5% UNLESS SHOWN OTHERWISE



LEGEND

- xx.xx' EXISTING LENGTH
- ==== PROPOSED SIDE CURB
- () EXISTING ELEVATION/SLOPE
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- ▨ SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 650.737
 BENCHMARK: CUT SQUARE ON SE CORNER OF TRAFFIC SIGNAL HANDHOLE
 LOCATION: TRAFFIC SIGNAL HANDHOLE ON PCC MEDIAN, WEST LEG OF INTERSECTION

MODEL: Default
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DRAWN - RR	REVISIONS -	
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PLOT DATE = 3/15/2019	DATE - 1/31/2019	REVISIONS -

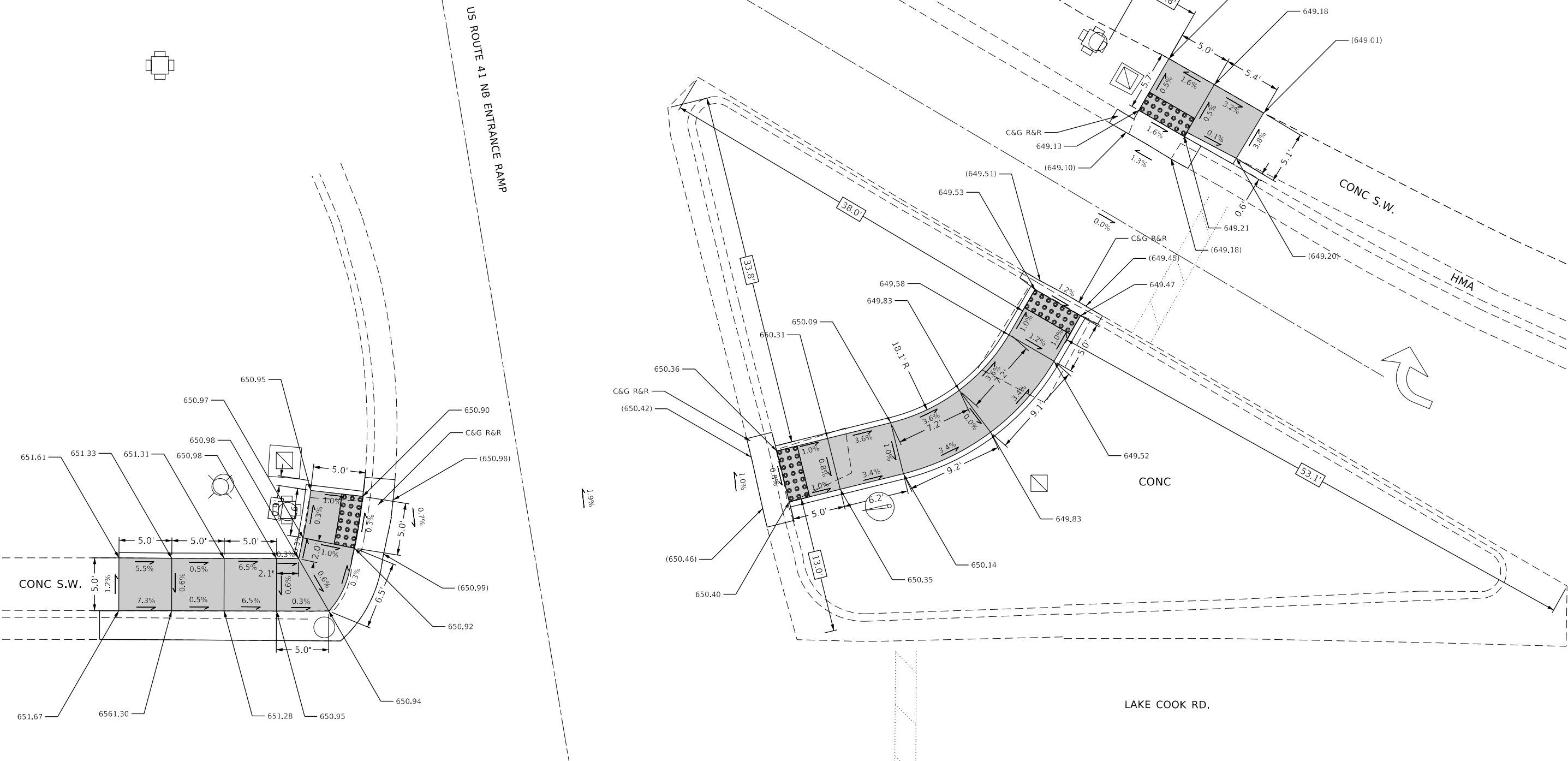
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA SIDEWALK RAMP DETAILS
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD**

SCALE: 1"=5' SHEET 1 OF 2 SHEETS STA. - TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	15
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				

NOTE:
GUTTER FLAG SLOPE IS 5% UNLESS SHOWN OTHERWISE



LEGEND

xx.xx'

EXISTING LENGTH

==

PROPOSED SIDE CURB

()

EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



DETECTABLE WARNINGS



SIDEWALK REMOVAL
REPLACE W/TOPSOIL & SOD

REFERENCE BENCH MARK ELEV 649.517

BENCHMARK: CUT SQUARE ON NW CORNER OF TRAFFIC CONTROLLER FOUNDATION

LOCATION: TRAFFIC CONTROLLER FOUNDATION ON SE CORNER OF INTERSECTION

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PROJECT: \\harc...
DRAWING: \\harc...
DATE: 3/15/2019

USER NAME = rostkowskir	DESIGNED - RR	REVISED -
	DRAWN - RR	REVISED -
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PLOT DATE = 3/15/2019	DATE - 1/31/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

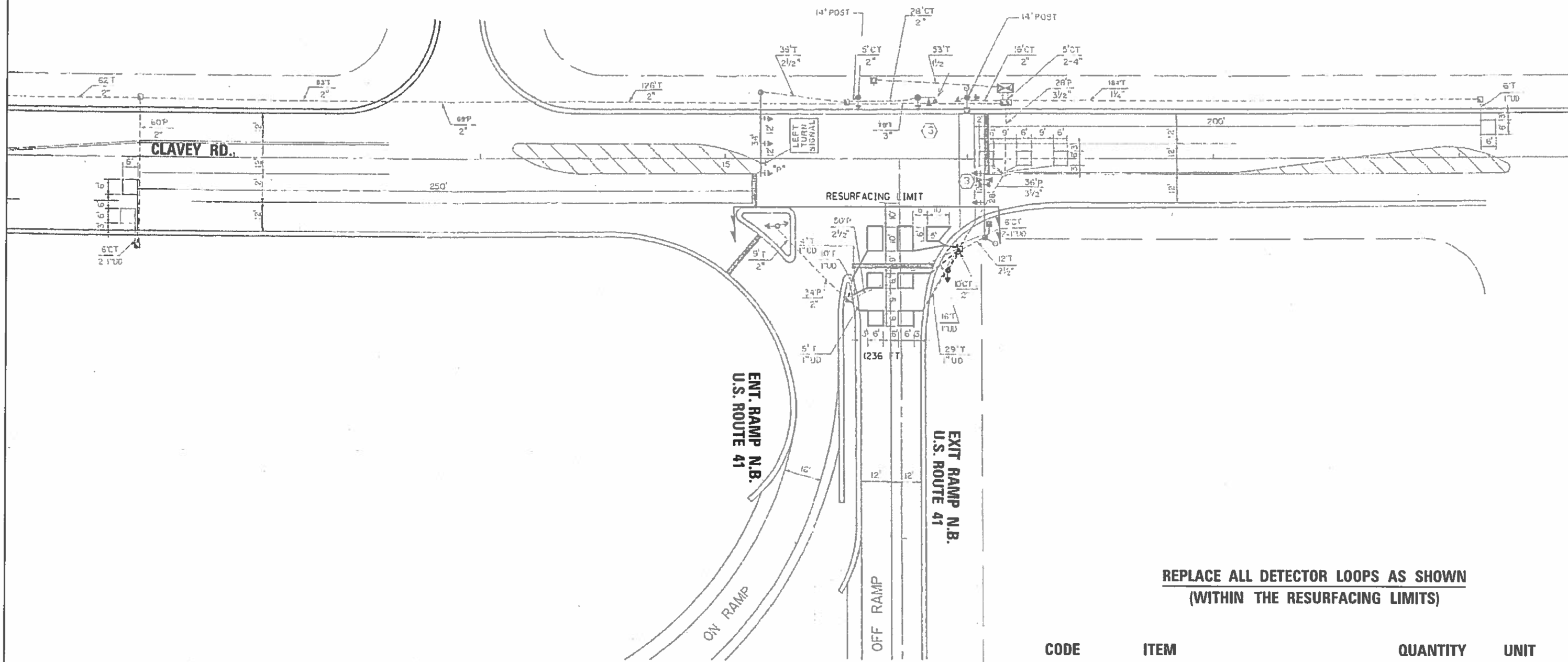
**ADA SIDEWALK RAMP DETAILS
US RTE 41 (SKOKIE HIGHWAY); S OF CHANTILLY BLVD TO LAKE-COOK RD**

SCALE: 1"=5' SHEET 2 OF 2 SHEETS STA. - TO STA. -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	16
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION.
 *DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING,
 RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	236	FOOT

FILE NAME * 62096 -US RTE 41 @ Clavey Rd.dgn	USER NAME * va-gata	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
		CHECKED - LP	REVISED -
		DATE - 08/08/2018	REVISED -

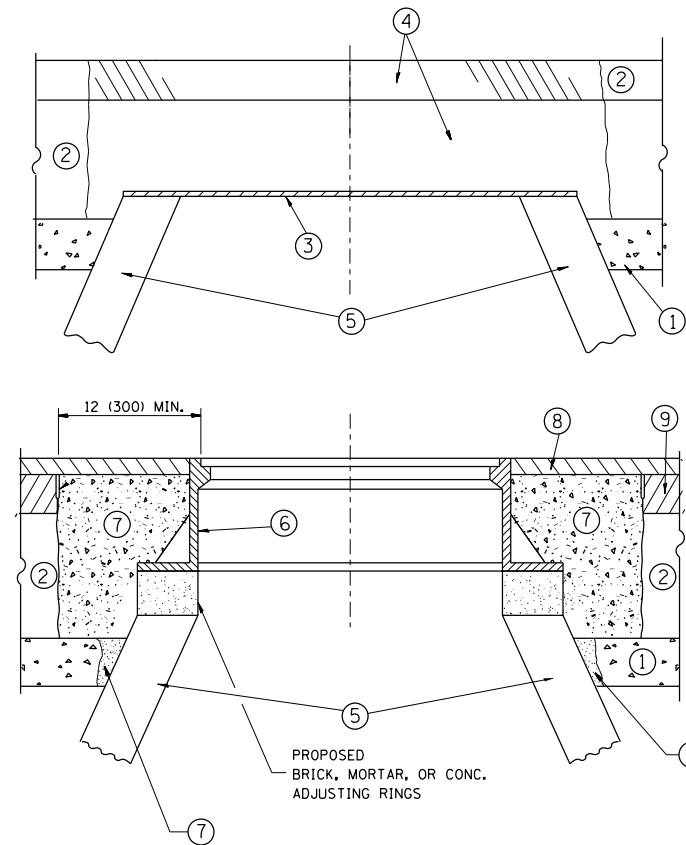
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN
U.S. ROUTE 41 (SKOKIE HWY) AT CLAVEY RD.**

F.A.P. RTE. 346	SECTION 2018-051-RS&CR	COUNTY LAKE	TOTAL SHEETS 31	SHEET NO. 18
CONTRACT NO. 62G96				
ILLINOIS FED. AID PROJECT				

Default

SCALE: SHEET OF SHEETS STA. TO STA.



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

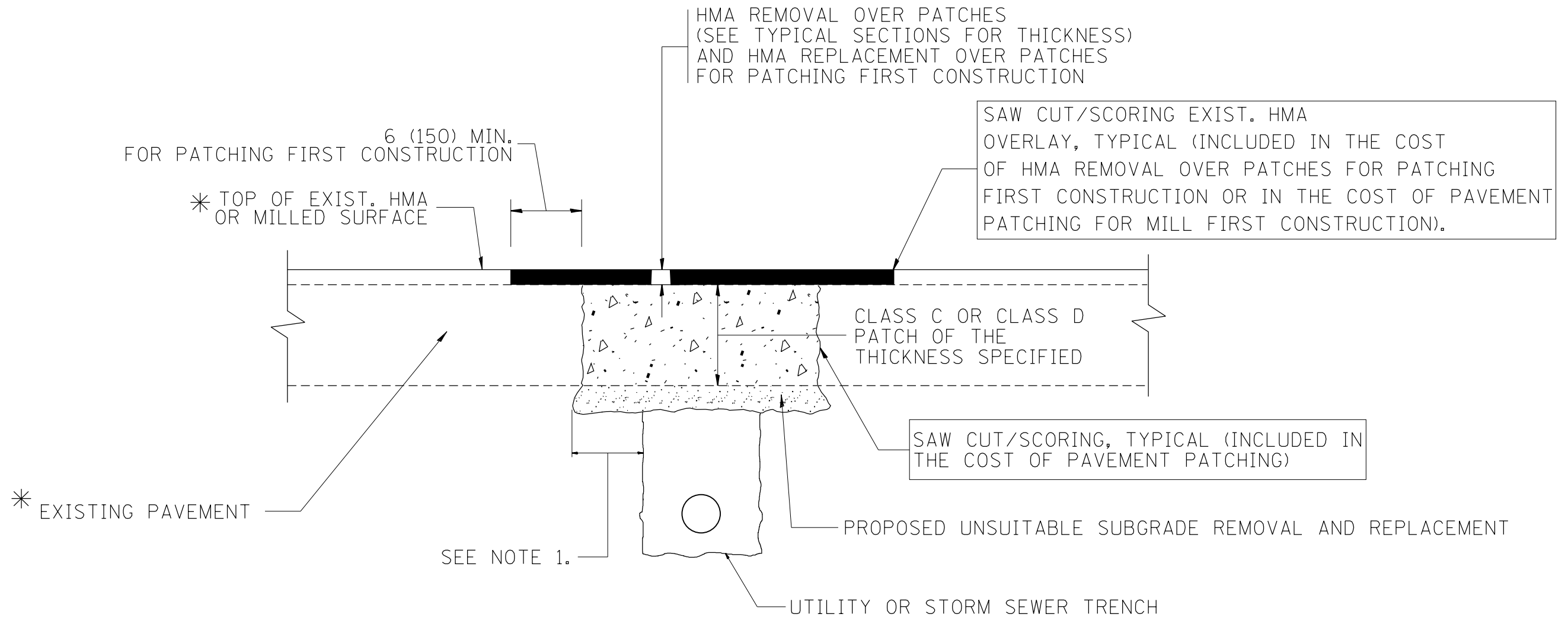
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	PLOT DATE = 2/5/2019	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RILE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	19
BD600-03 (BD-8)		CONTRACT NO. 62G96		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT DATE = 2/5/2019		REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RILE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	20
BD400-04 (BD-22)		CONTRACT NO. 62G96		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

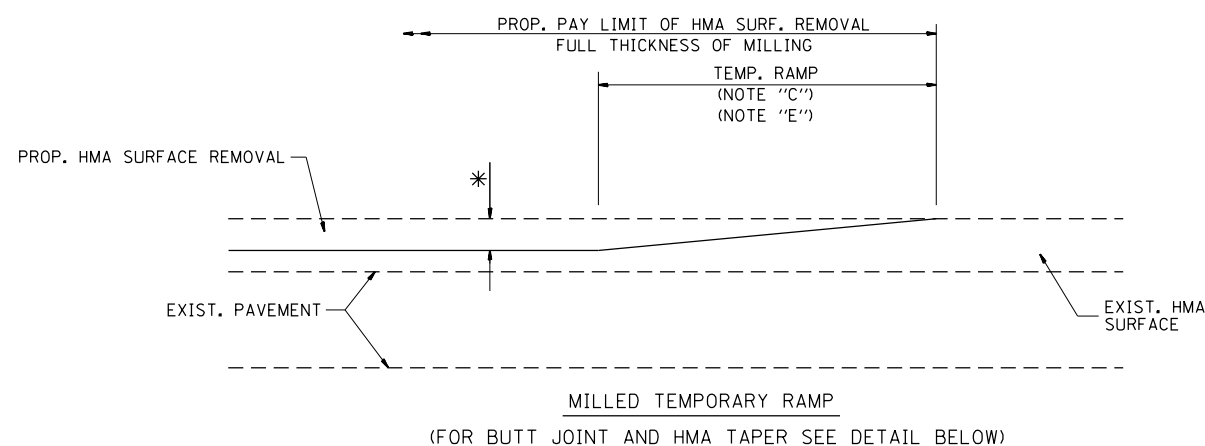
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

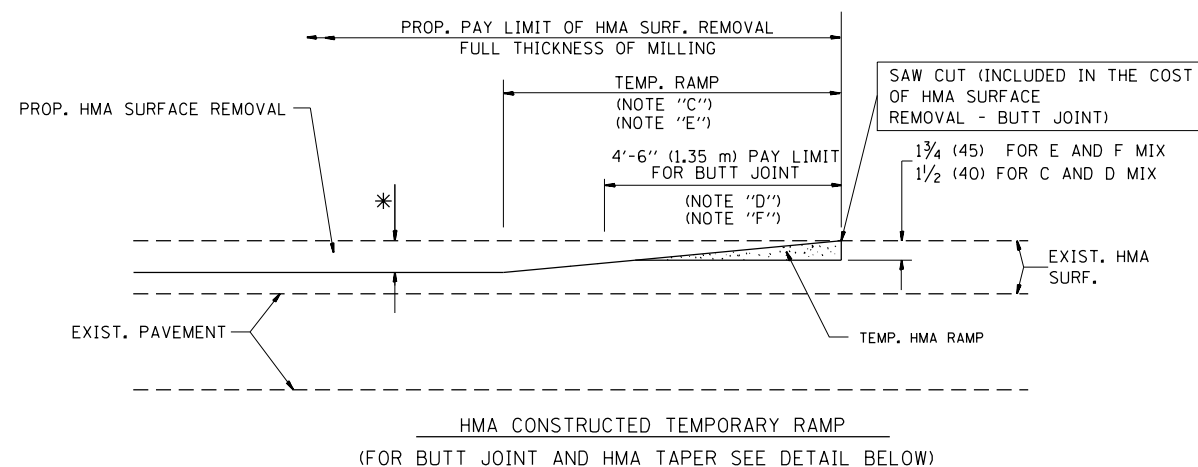
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

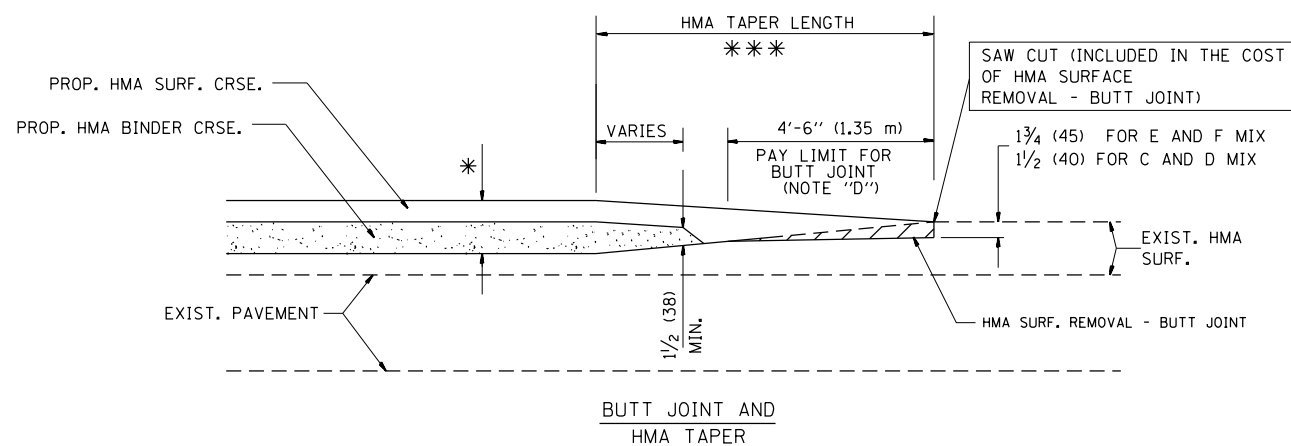
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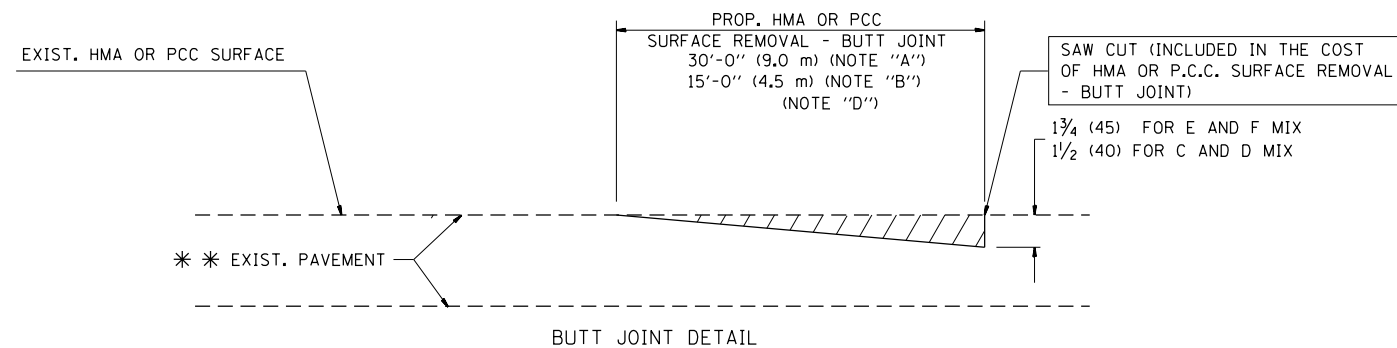
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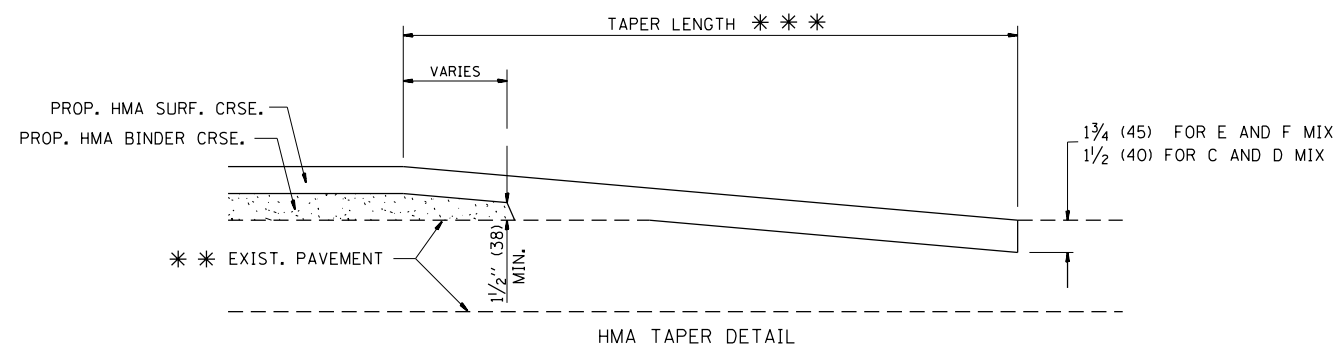
**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

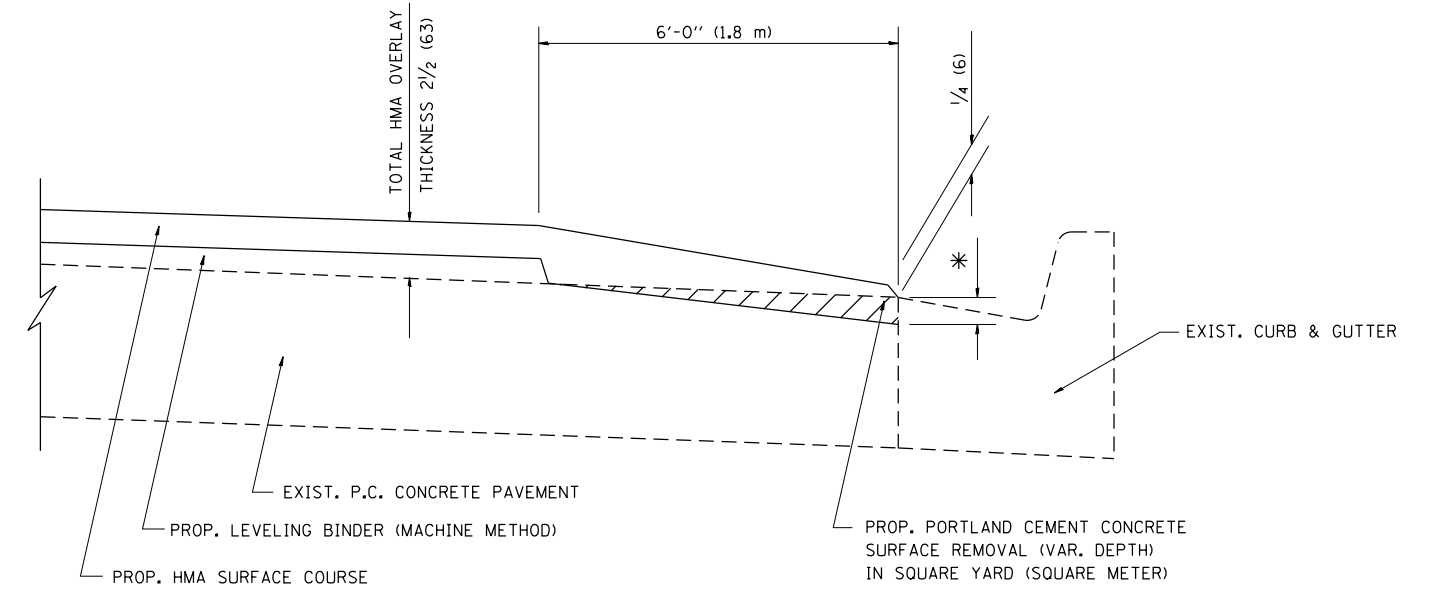
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	PLOT DATE = 2/5/2019	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	22
BD400-05 BD32		CONTRACT NO. 62G96		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

HMA SURFACE	THICKNESS	LEVELING BINDER THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1 1/2 (38)	1 (25)	1/4 (33)
E	1 3/4 (44)	3/4 (19)	1/2 (38)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

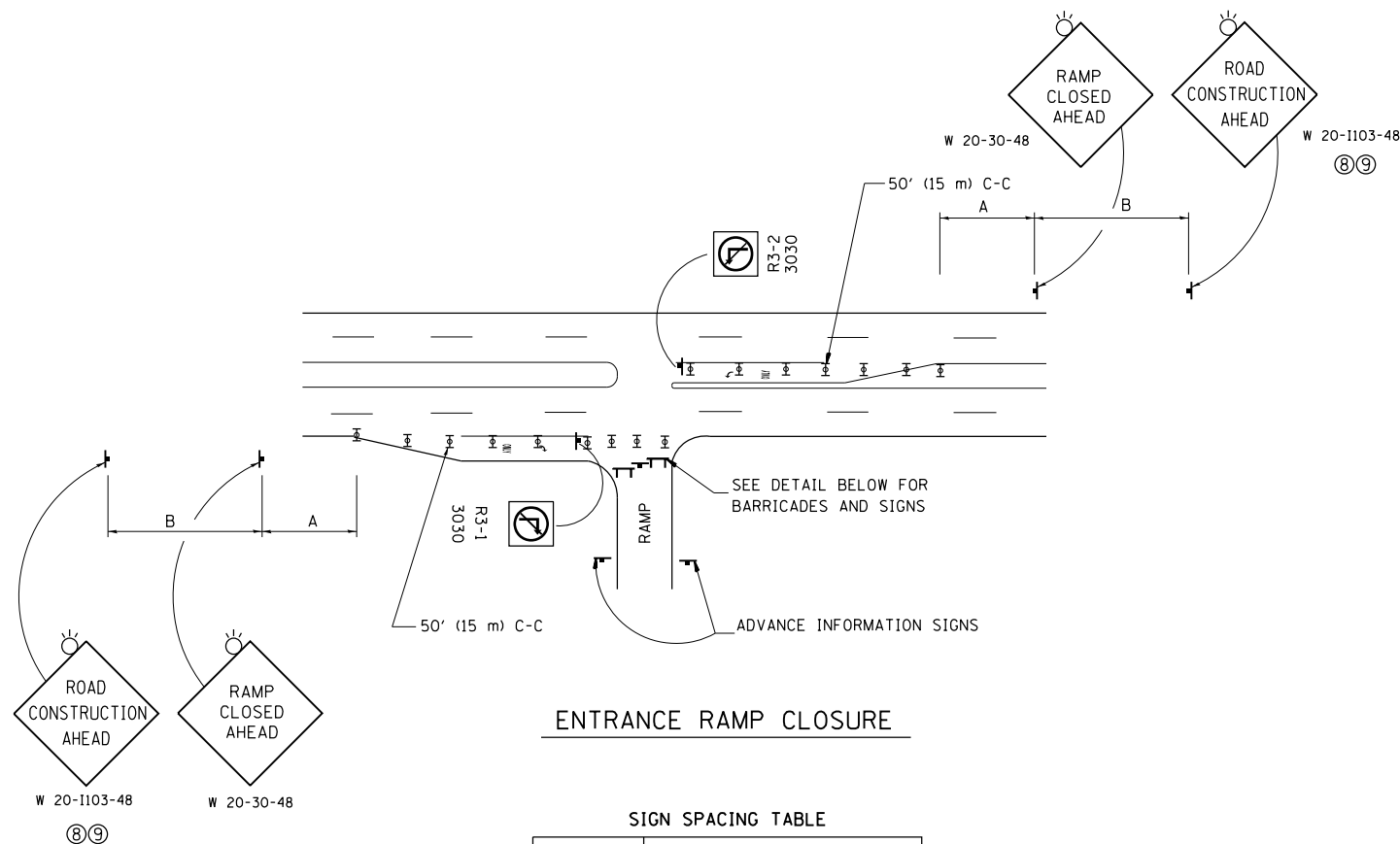
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	PLOT DATE = 2/5/2019		REVISED - JP CHANG 07-08-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT
EDGE OF P.C.C. PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	23
BD400-06 (BD33)		CONTRACT NO. 62G96		
ILLINOIS FED. AID PROJECT				

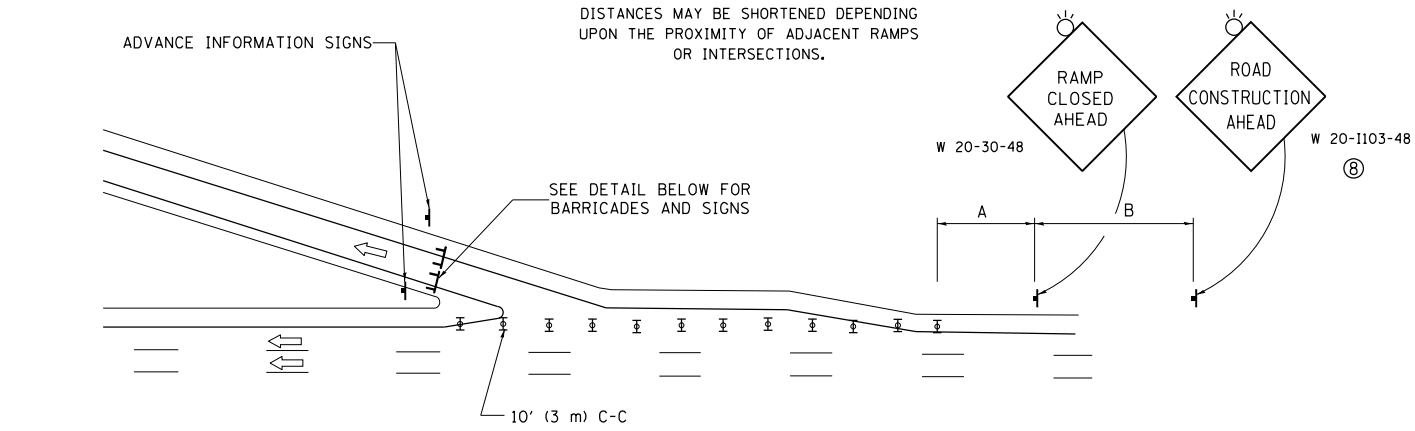


ENTRANCE RAMP CLOSURE

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY <24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

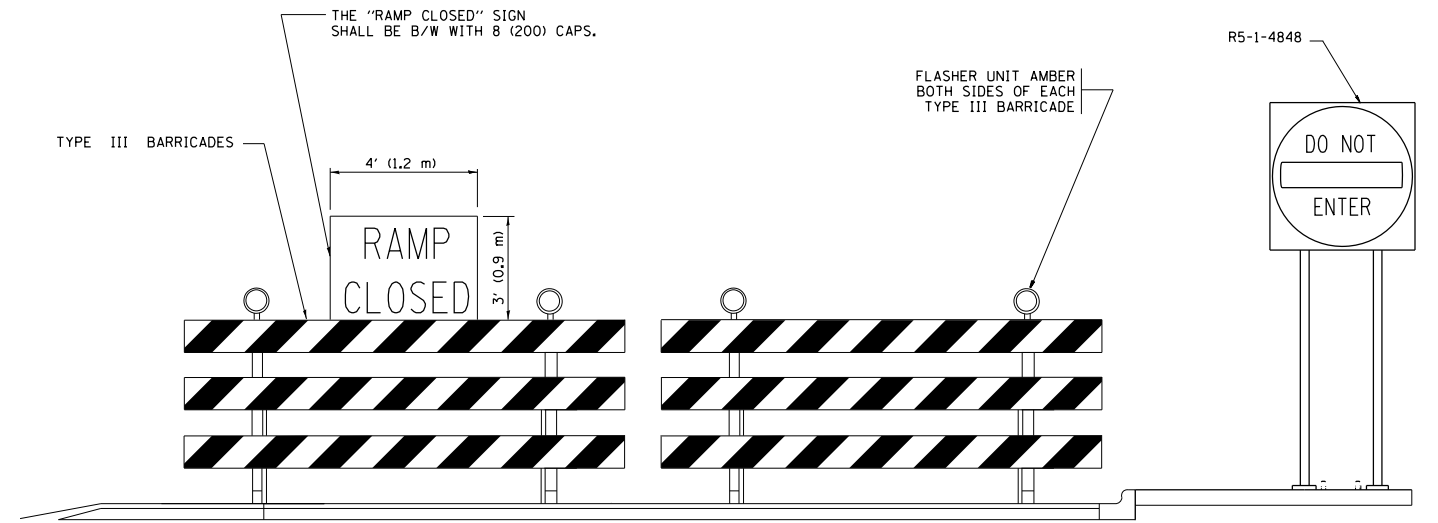
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



EXIT RAMP CLOSURE

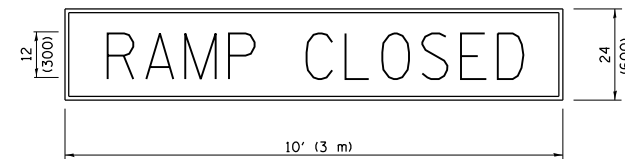
SYMBOLS

- ⊥ TYPE II BARRICADE OR DRUM
- ⊥ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



DETAIL FOR REQUIRED BARRICADES & SIGNS

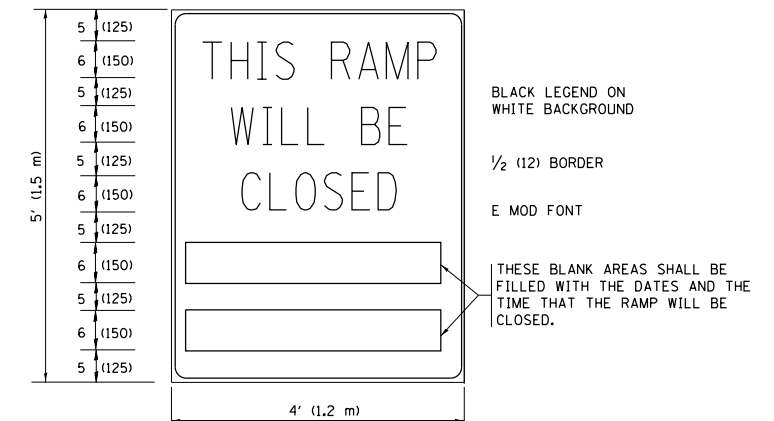
RAMP CLOSURE ADVANCE WARNING SIGN



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY
E MOD FONT
1 (25) BORDER

THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

RAMP CLOSURE ADVANCE INFORMATION SIGN



BLACK LEGEND ON WHITE BACKGROUND

1/2 (12) BORDER

E MOD FONT

THESE BLANK AREAS SHALL BE FILLED WITH THE DATES AND THE TIME THAT THE RAMP WILL BE CLOSED.

THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.

THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

GENERAL NOTES:

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = rostkowski	DESIGNED - D.W.S.	REVISED - S.P.B. 01-07
pw:\planroom.dot\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\0135818\DRAWING\Design\DistStd.dgn		CHECKED -	REVISED - S.P.B. 12-09
Default	PLOT SCALE = 100.0000' / 1in.	DATE - 02-83	REVISED - M.D. 06-13
	PLOT DATE = 3/15/2019		REVISED - M.D. 01-18

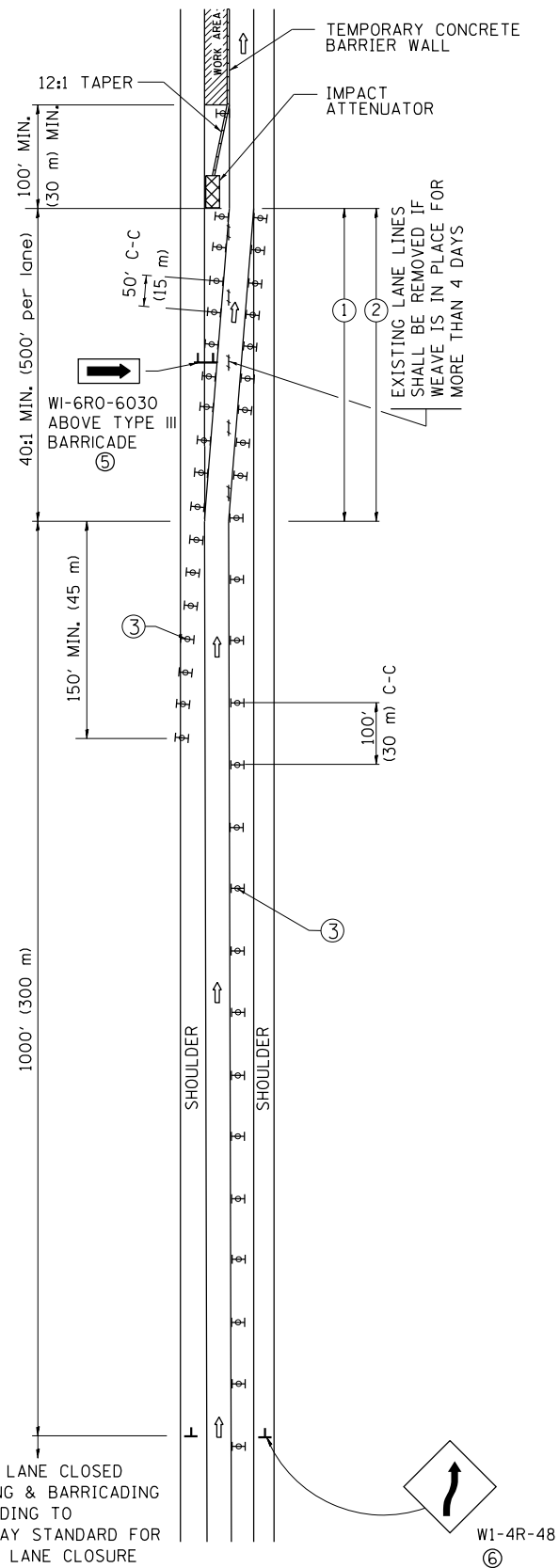
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE AND EXIT RAMP
CLOSURE DETAILS**

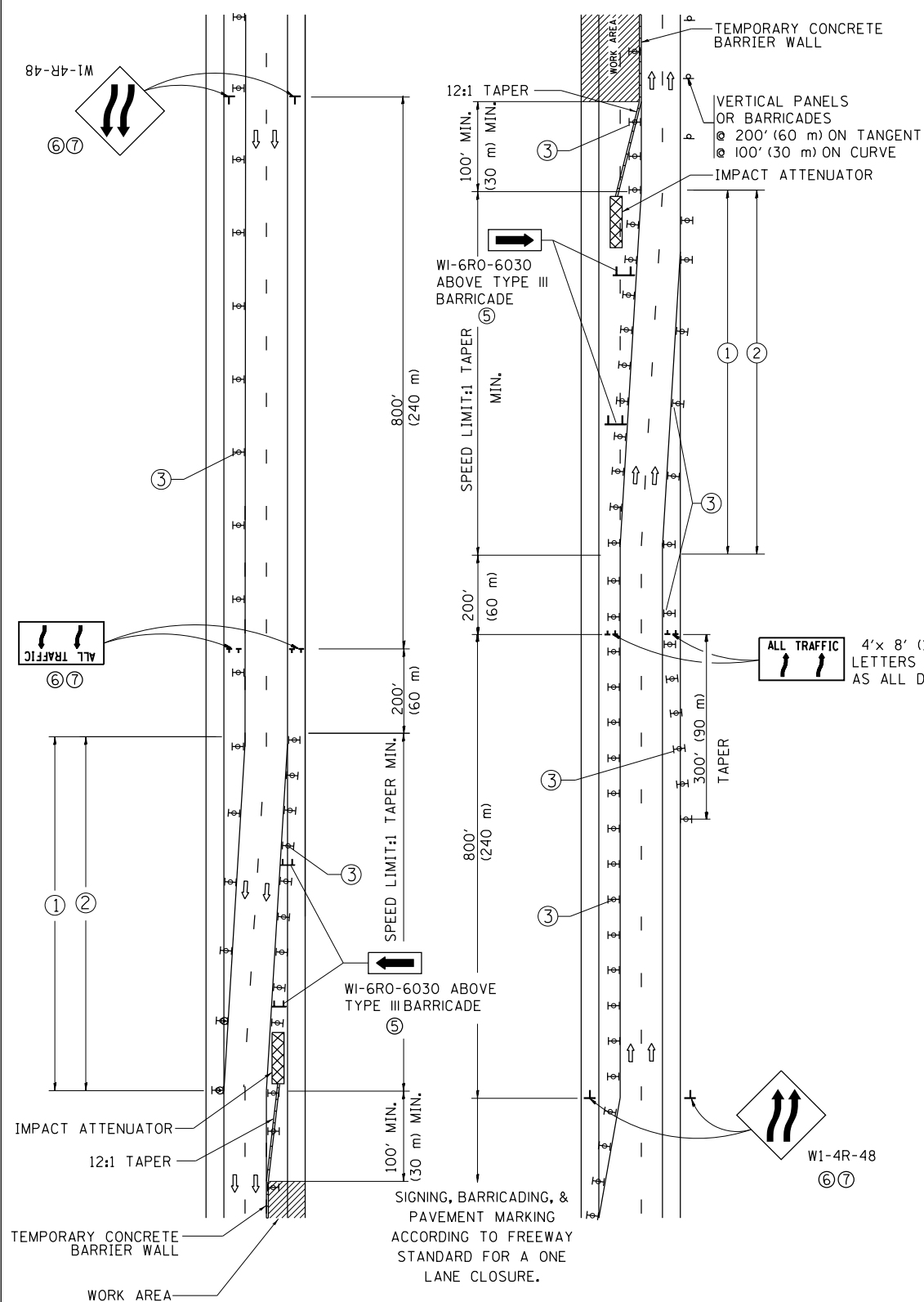
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	23A
TC-08		CONTRACT NO. 62G96		
ILLINOIS FED. AID PROJECT				

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

- EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. WI-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

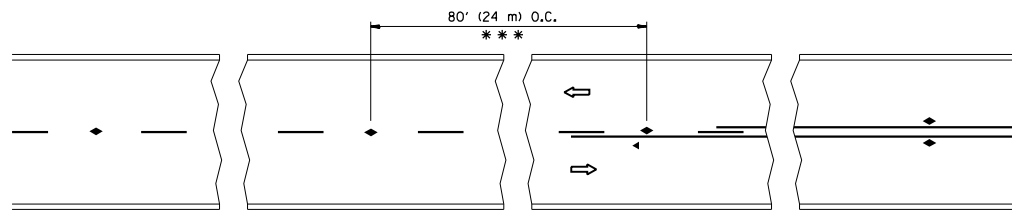
4'x 8' (1.2 m x 2.4 m); 1 (25) BORDER; 10 (250) CAPITAL LETTERS BACKGROUND SHEETING SHALL BE THE SAME AS ALL DIAMOND SHAPED CONSTRUCTION SIGNS.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48 (6, 7)
- W24-1-48 (7)

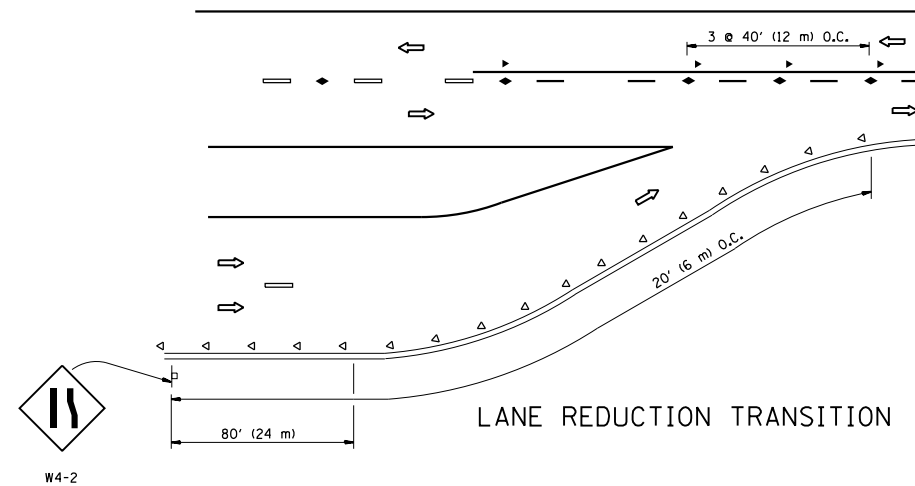
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = rostkowski	DESIGNED - DWS	REVISED - JAF 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE			F.A.P. RILEY	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\013581\Drawings\Design\DistStd.dgn			REVISED - SPB 01-07		346	2018-51-RS&CR	LAKE	31	23B			
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - SPB 12-09			TC-09		CONTRACT NO. 62G96					
PLOT DATE = 3/15/2019	DATE - 02-87	REVISED - MD 06-13			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

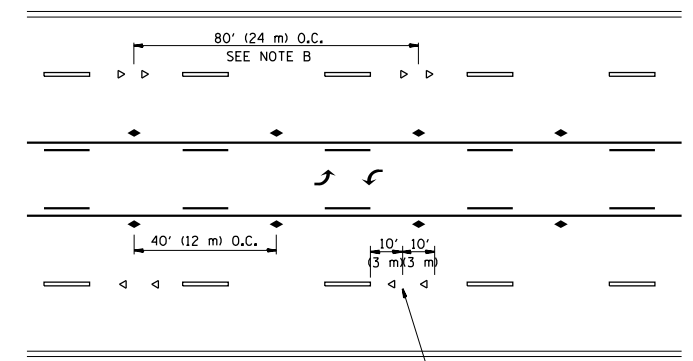


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

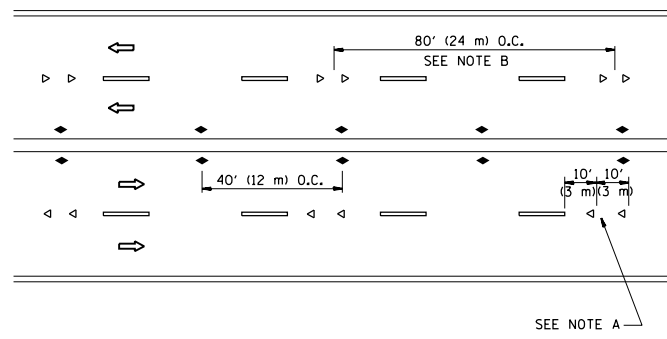
TWO-LANE/TWO-WAY



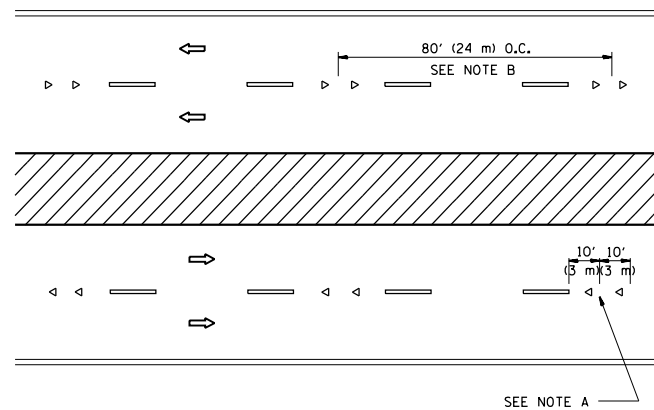
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

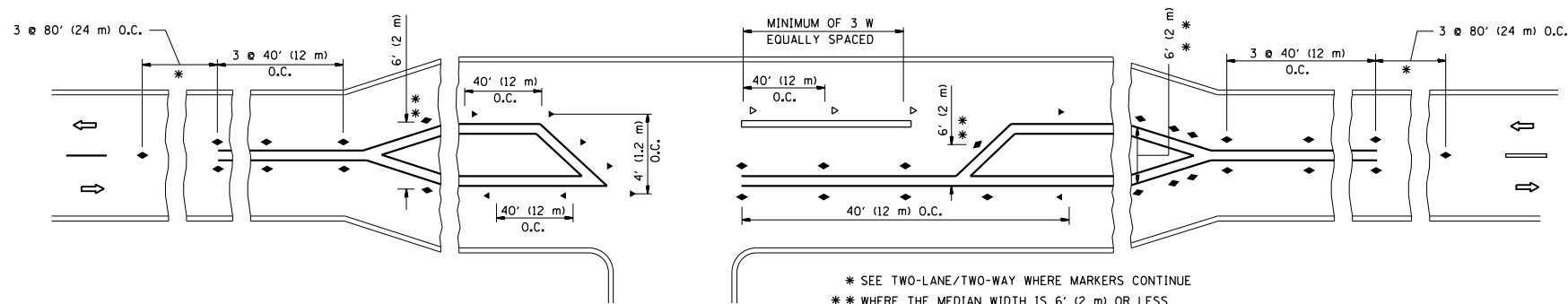
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

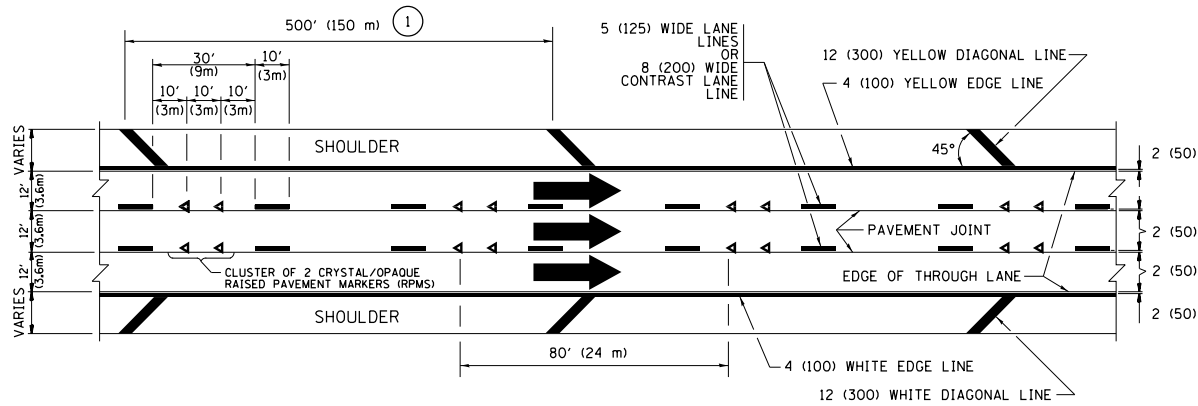
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = rostkowski	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI358\BROWN\Meta\Design\DistStd.dgn		CHECKED -	REVISED - T. RAMMACHER 03-12-99
		DATE -	REVISED - T. RAMMACHER 01-06-00
			REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

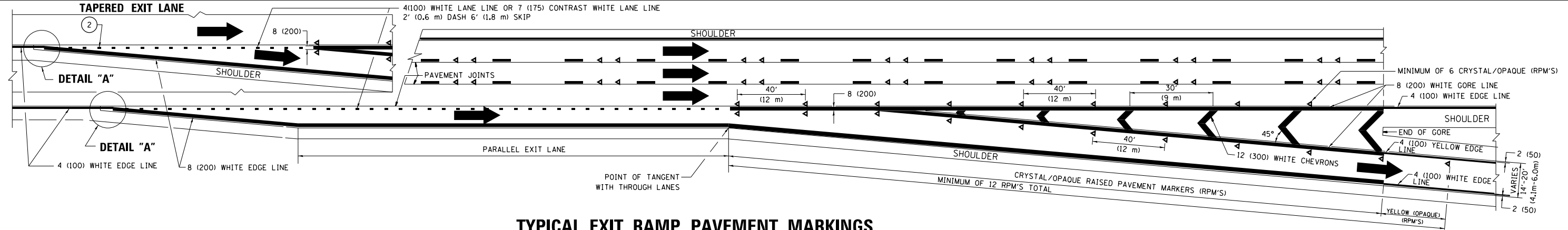
F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11		CONTRACT NO. 62G96		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



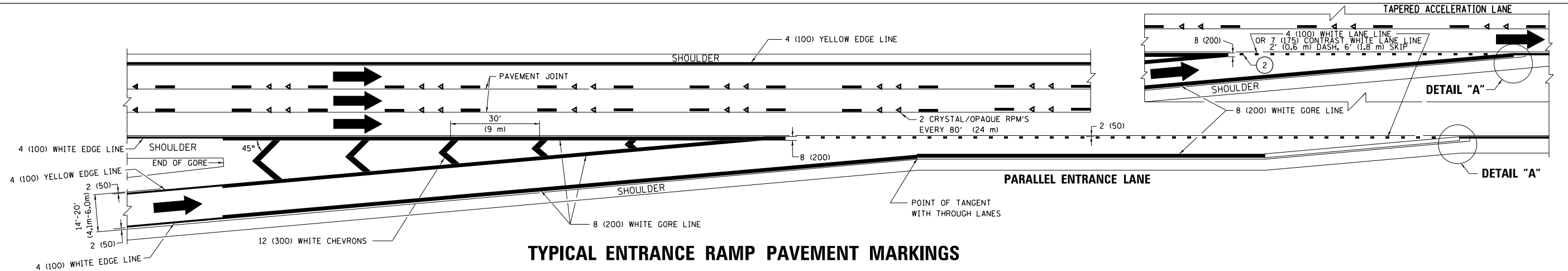
TYPICAL EDGE LINES & LANE LINES

PAVEMENT MARKING MATERIALS

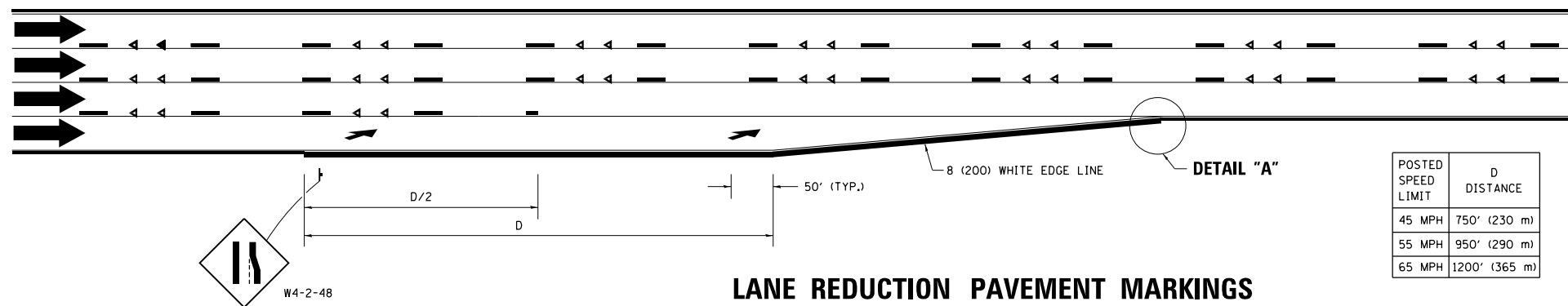
1. THERMOPLASTIC PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON HMA PAVEMENTS.
2. POLYUREA OR MODIFIED URETHANE PAVEMENT MARKING LINE SHALL BE USED FOR ALL EDGE LINES, GORE LINES, AND DIAGONAL LINES ON PCC PAVEMENTS.
3. PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, INLAID OR GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON HMA PAVEMENTS.
4. CONTRAST PREFORMED PLASTIC PAVEMENT MARKING LINE TYPE B, GROOVE IN, SHALL BE USED FOR ALL LANE LINES ON PCC PAVEMENT.



TYPICAL EXIT RAMP PAVEMENT MARKINGS

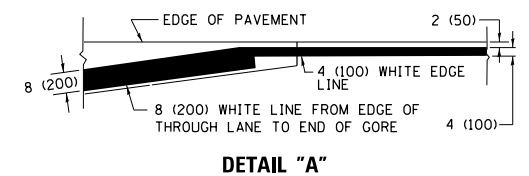


TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS



LANE REDUCTION PAVEMENT MARKINGS

POSTED SPEED LIMIT	D DISTANCE
45 MPH	750' (230 m)
55 MPH	950' (290 m)
65 MPH	1200' (365 m)



- NOTES:**
- ① THE DIAGONAL LINES SHALL BE SPACED AT 40' (12 m) C-C ACROSS ALL STRUCTURES WHICH ARE 500' (150 m) OR LESS IN LENGTH. THE DIAGONAL LINES ARE NOT REQUIRED ON SHOULDERS WHICH ARE 6' (1.8 m) OR LESS IN WIDTH.
 - ② 4" (2' DASH, 6' SKIP) MARKING ON TAPERED ENTRANCE AND EXIT RAMP SHALL BE OMITTED ON TANGENT SECTIONS.

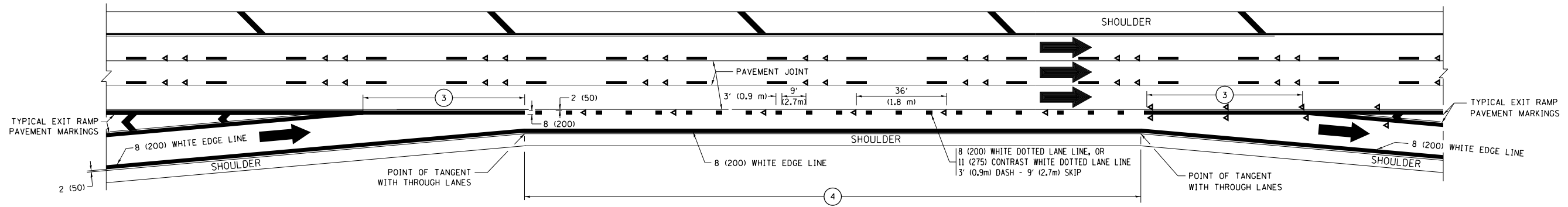
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Default	PLOT SCALE = 100.0000' / in.	DATE - 01-90	REVISED - M.D. 05-13
	PLOT DATE = 2/5/2019		REVISED - M.D. 09-17

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

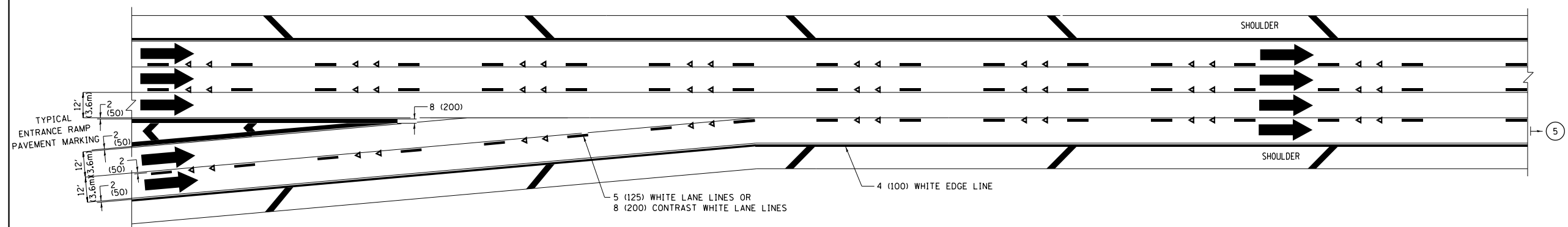
**MULTI-LANE FREEWAY
PAVEMENT MARKING DETAILS**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

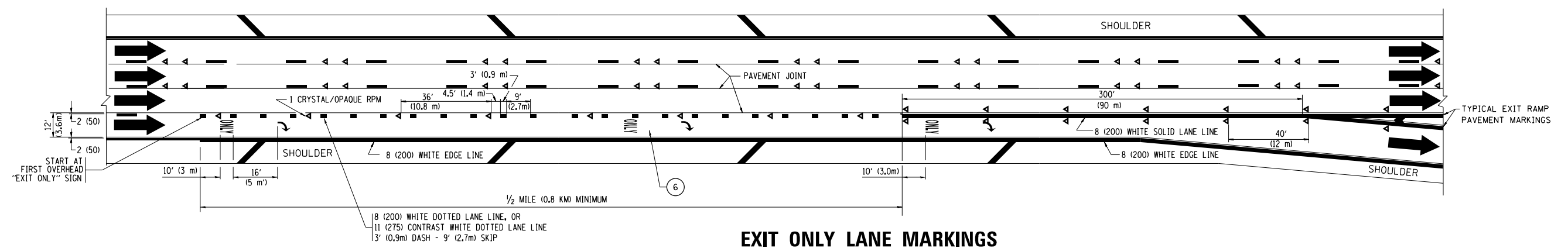
F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	25
TC-12		CONTRACT NO. 62G96		
ILLINOIS FED. AID PROJECT				



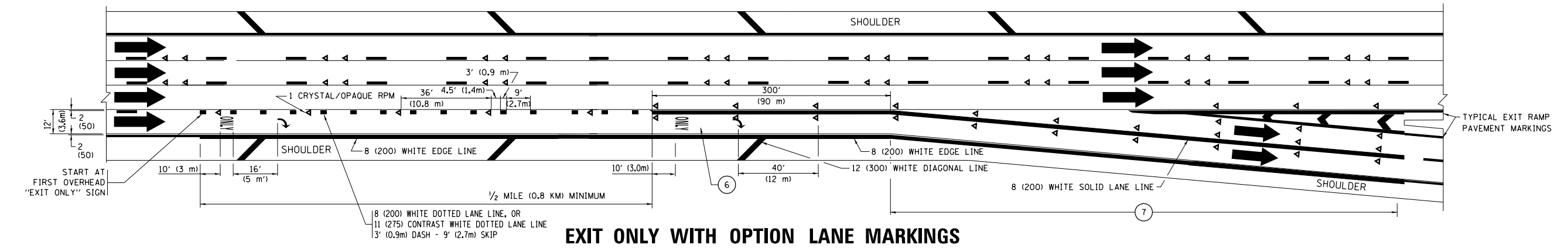
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



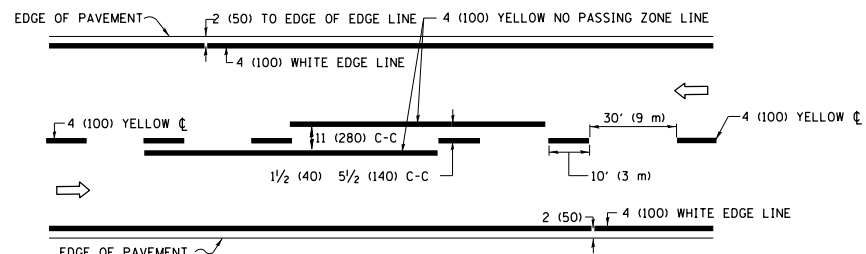
EXIT ONLY LANE MARKINGS



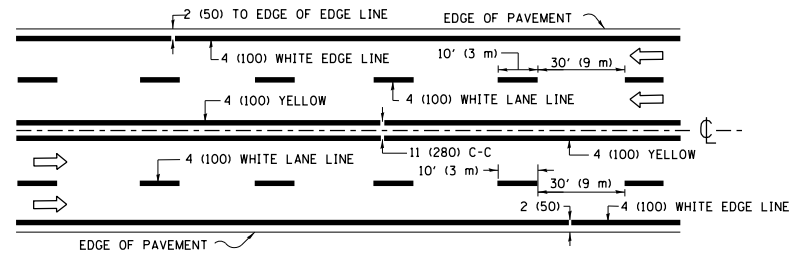
EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES:**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

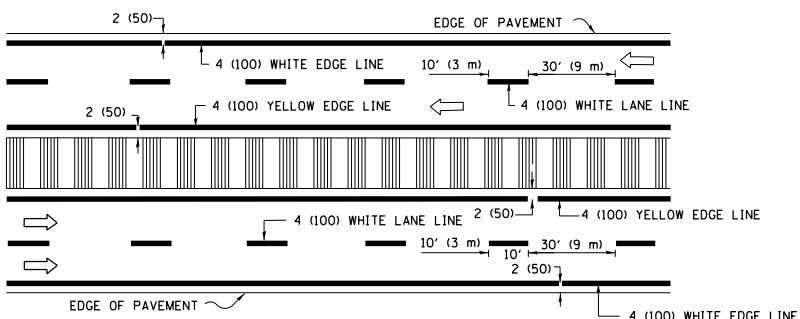
FILE NAME =	USER NAME = rostowski	DESIGNED - D.W.S.	REVISED - J.A.F. 02-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI358\Meta\Design\DistStd.dgn			REVISED - S.P.B. 01-07		346	2018-51-R5&CR	LAKE	31	26			
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - S.P.B. 01-10		TC-12			CONTRACT NO. 62G96				
	PLOT DATE = 2/5/2019	DATE - 01-90	REVISED - M.D. 09-17		SCALE: NONE	SHEET 2	OF 2 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



2-LANE ROADWAY

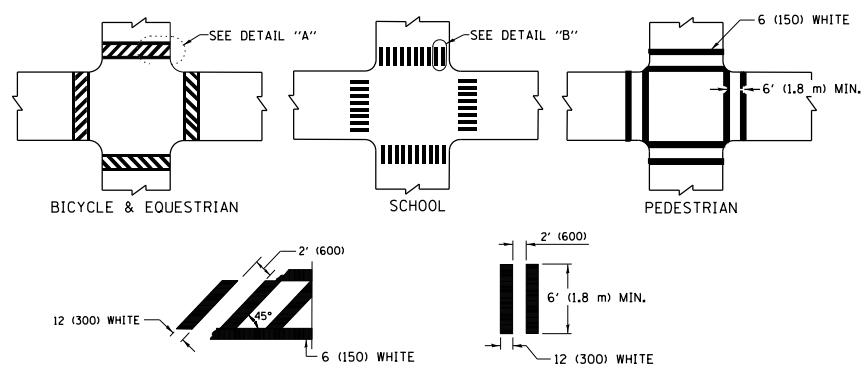


MULTI-LANE UNDIVIDED



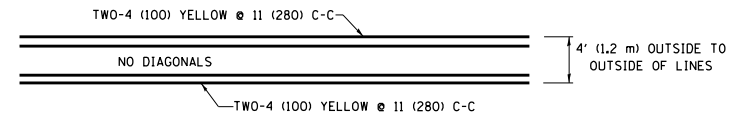
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

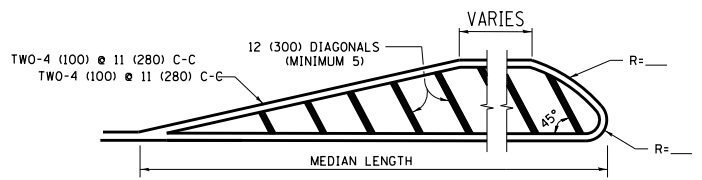


TYPICAL CROSSWALK MARKING

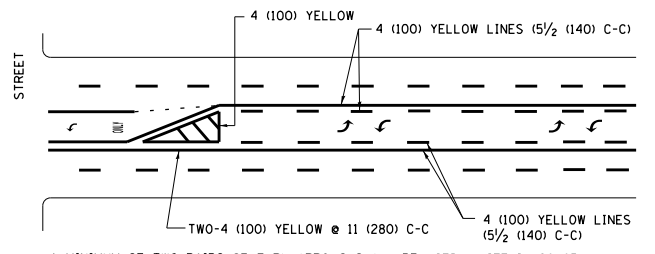
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



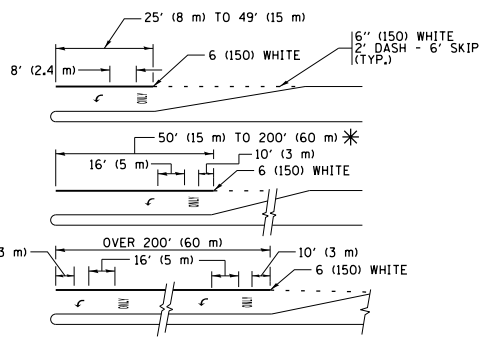
4' (1.2 m) WIDE MEDIANS ONLY



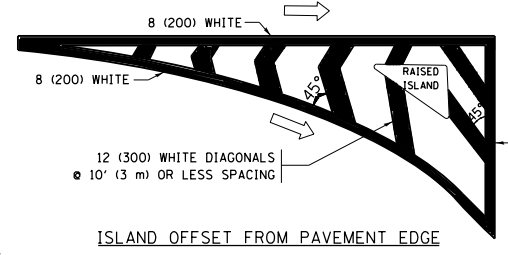
MEDIANS OVER 4' (1.2 m) WIDE



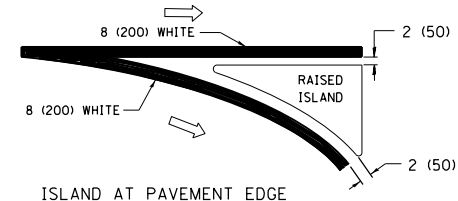
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



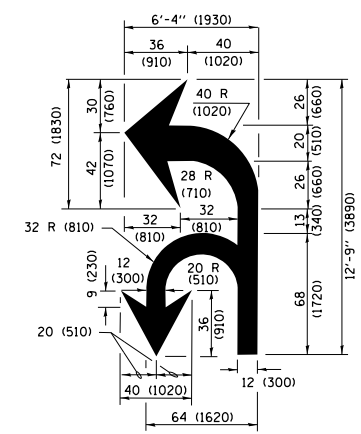
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING



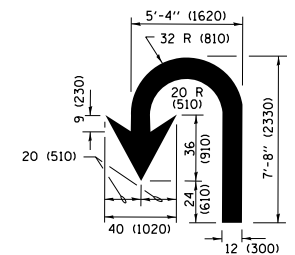
ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (22.5 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

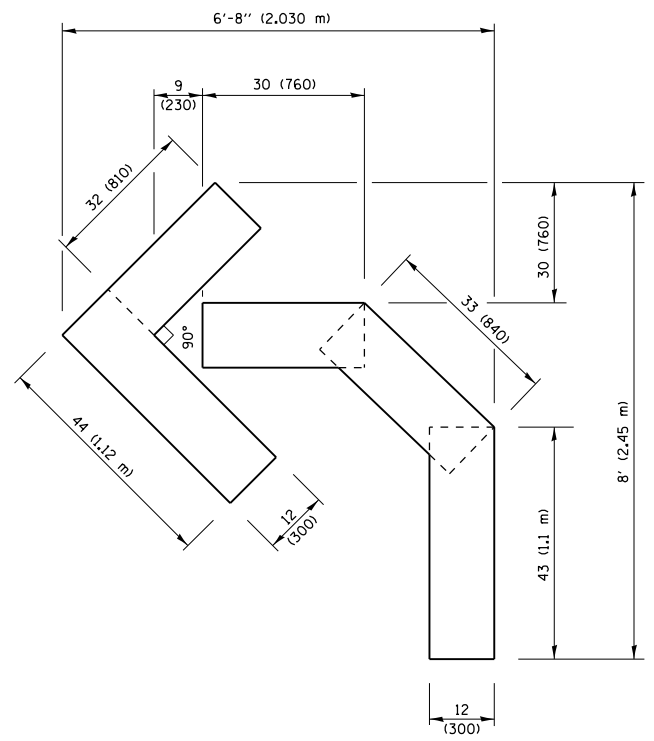
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = rostkowski	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\01358\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 2/5/2019		REVISED - C. JUCIUS 04-12-16

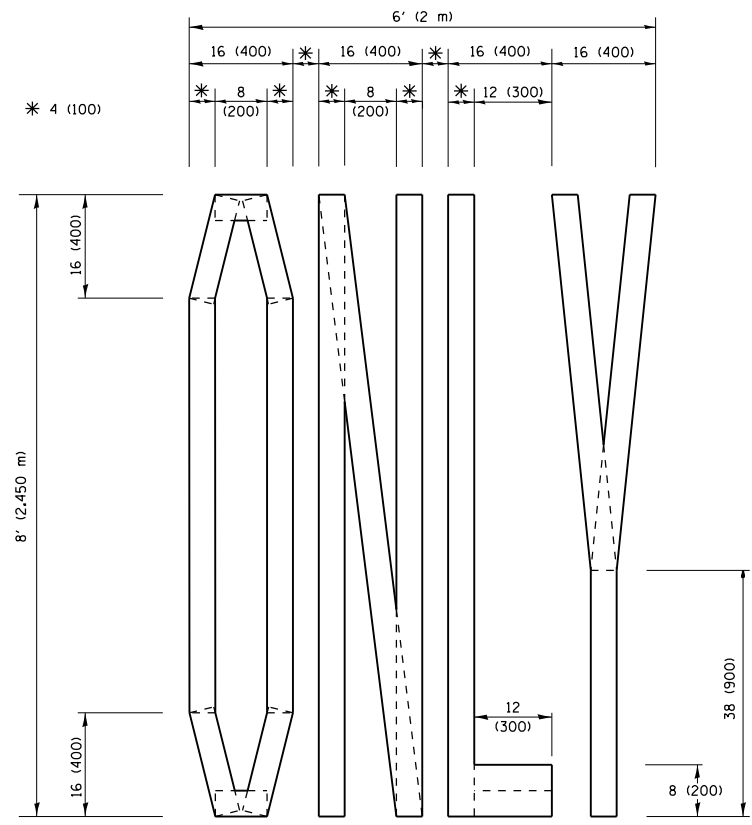
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

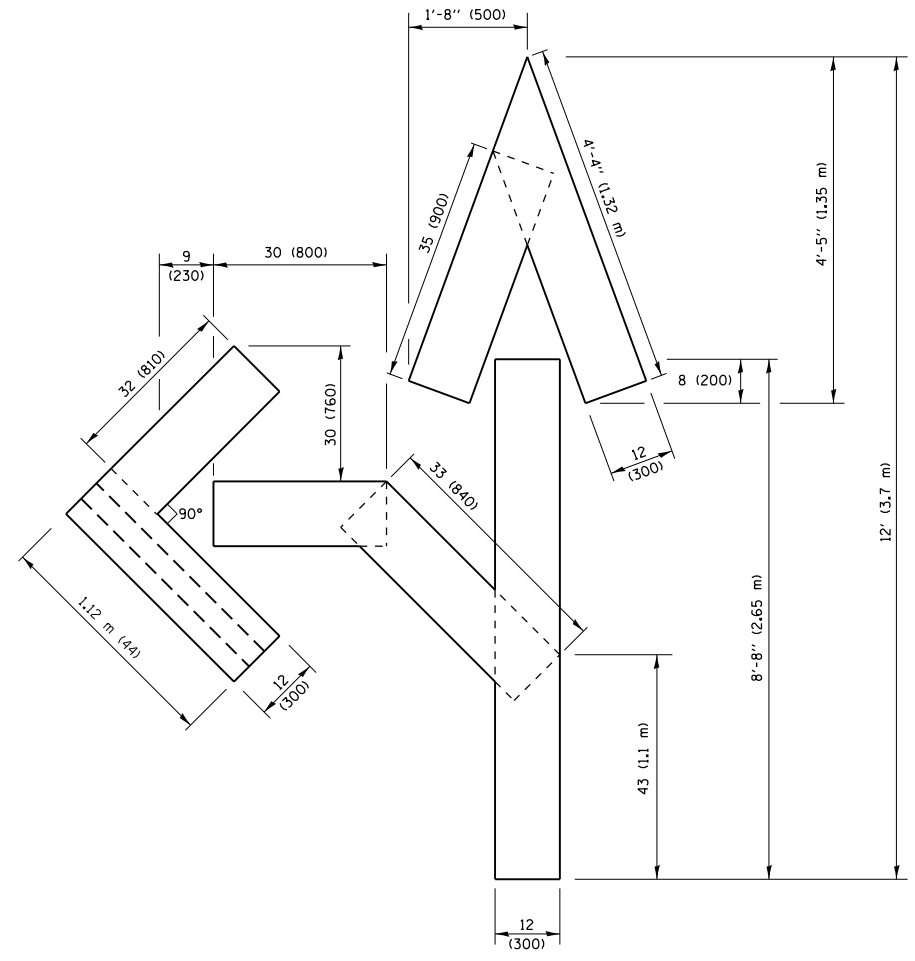
F.A.P. RILEY	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-R5&CR	LAKE	31	27
TC-13		CONTRACT NO. 62G96	ILLINOIS FED. AID PROJECT	



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

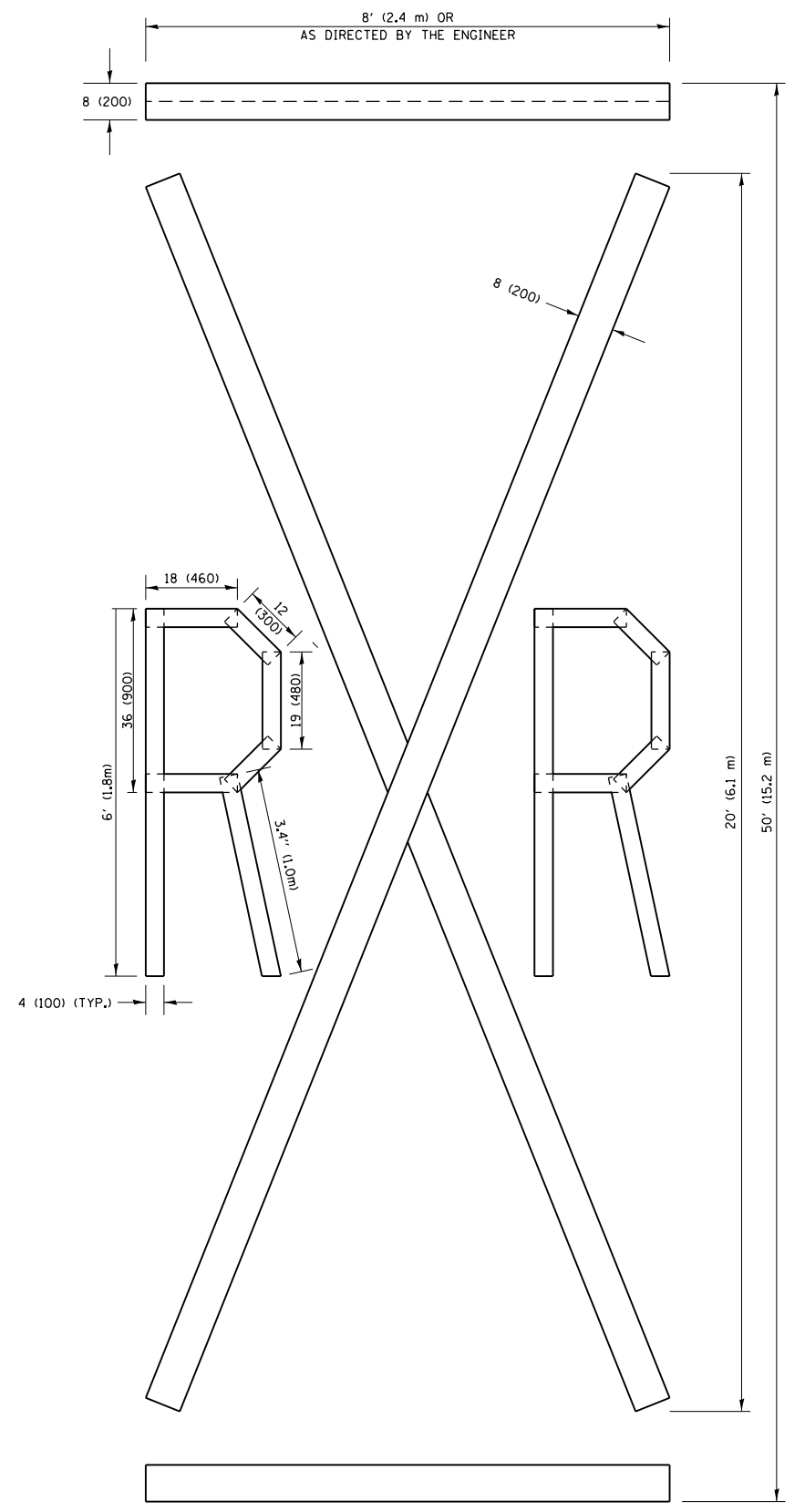


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

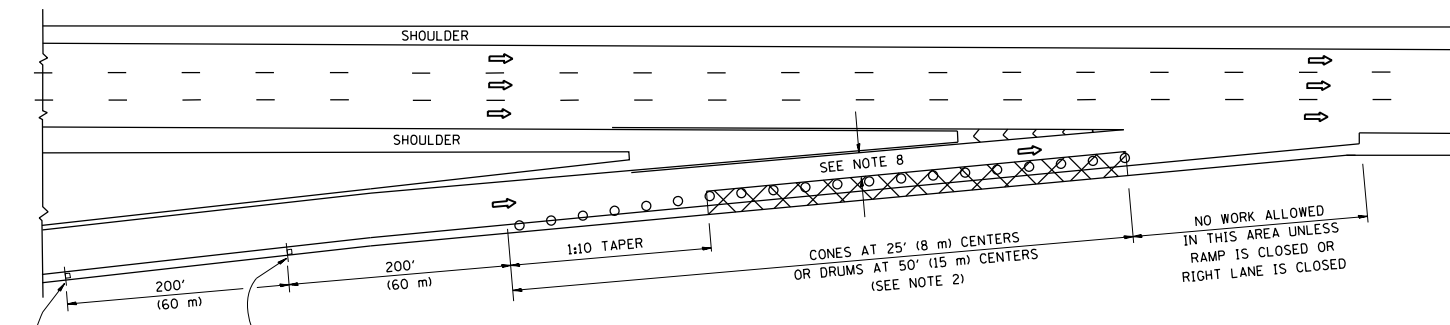
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		DATE -	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

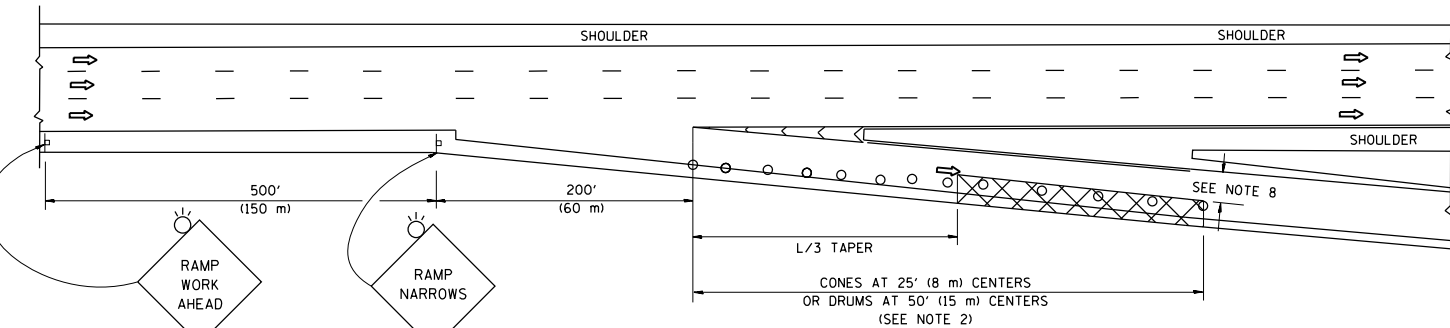
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	28
TC-16		CONTRACT NO. 62G96		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

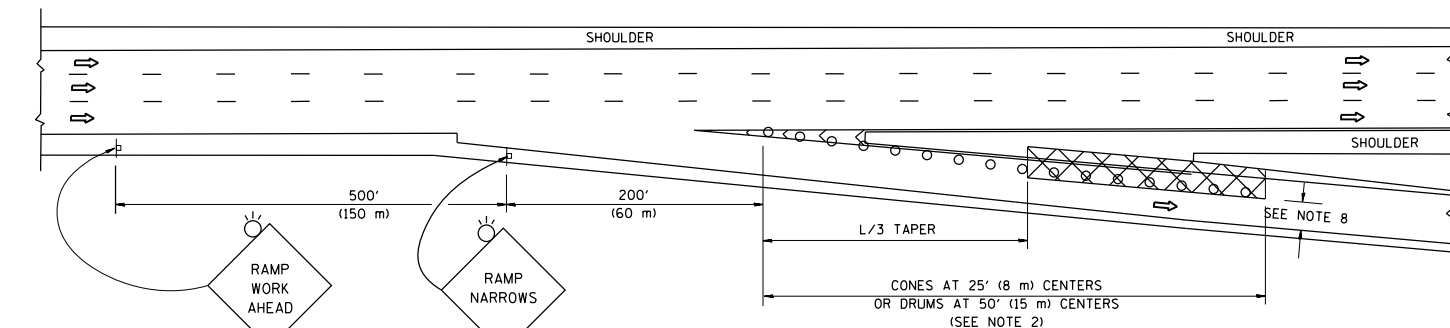
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

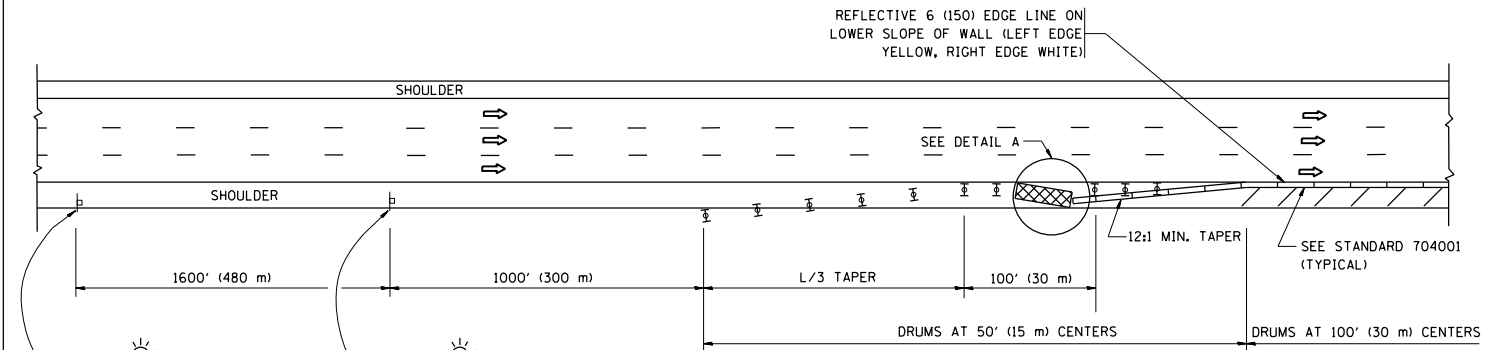
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

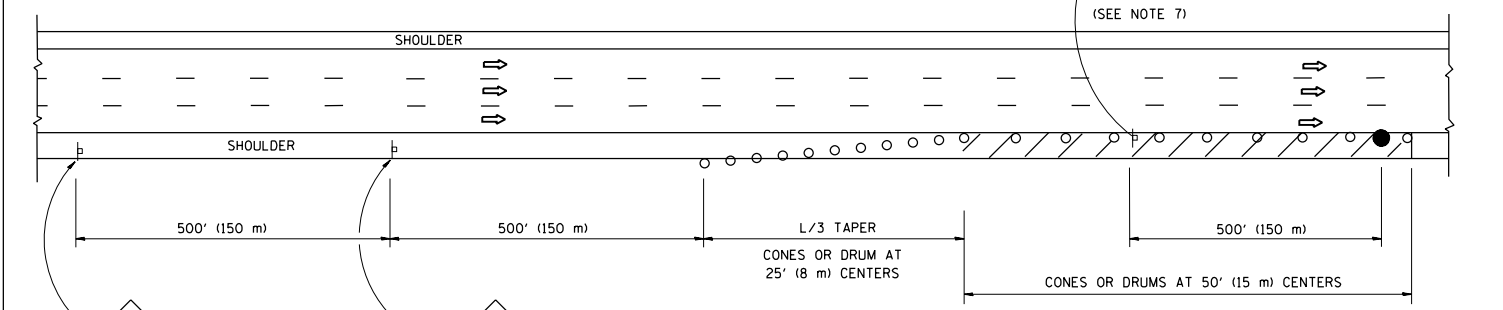
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC: $L=0.65(W)(S)$ ENGLISH: $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

SHOULDER CLOSURE DETAILS

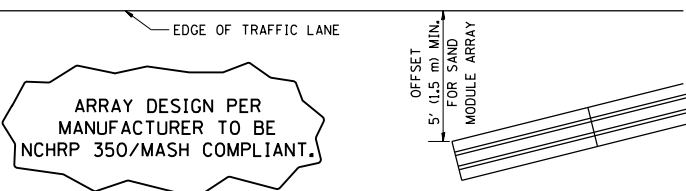


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

- THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

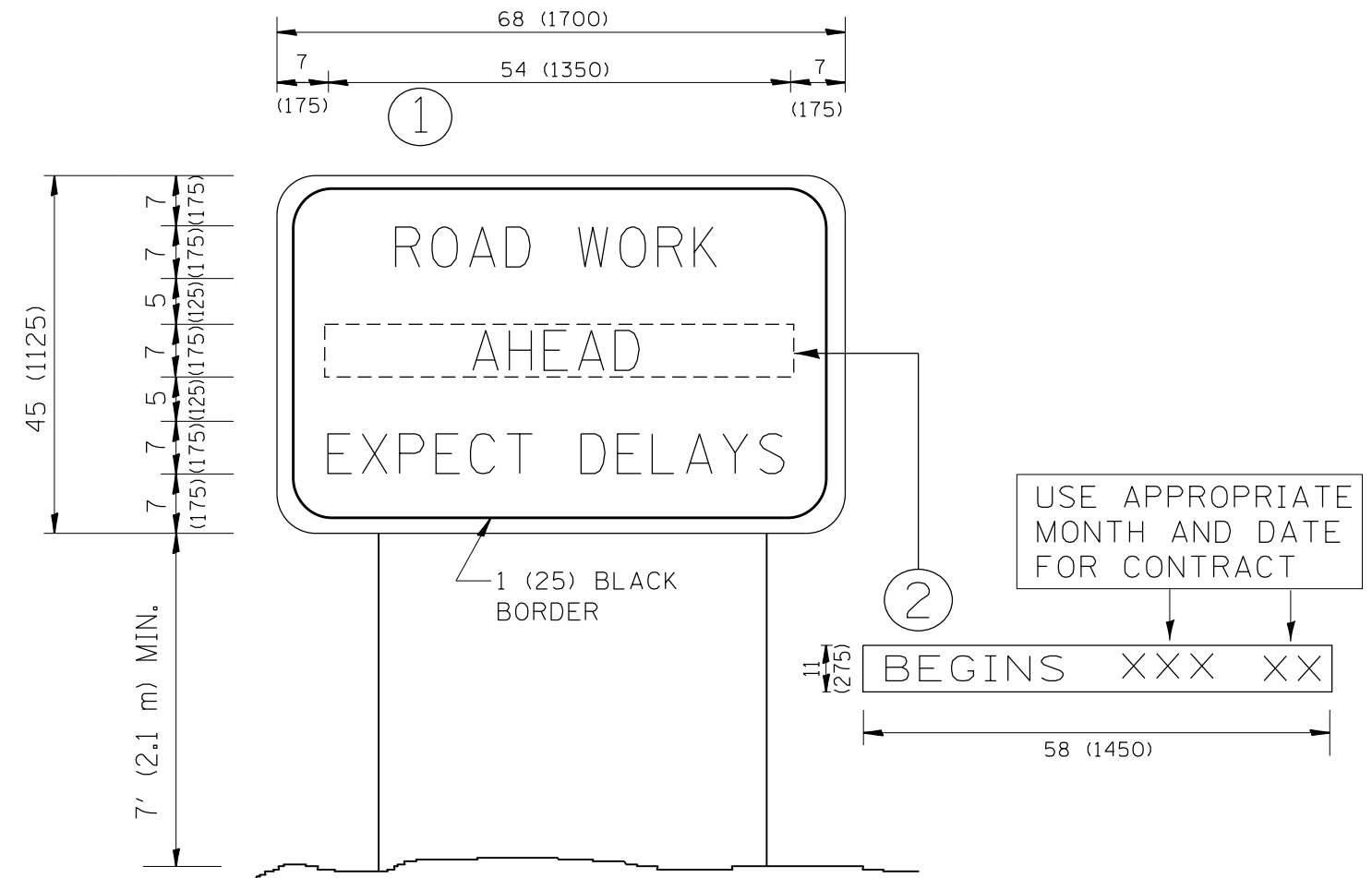
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Default		DATE - 11-96	REVISED - M.D. 06-13
			REVISED - M.D. 01-18

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. R.I.L.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-R5&CR	LAKE	31	29
TC-17		CONTRACT NO. 62G96		
ILLINOIS FED. AID PROJECT				



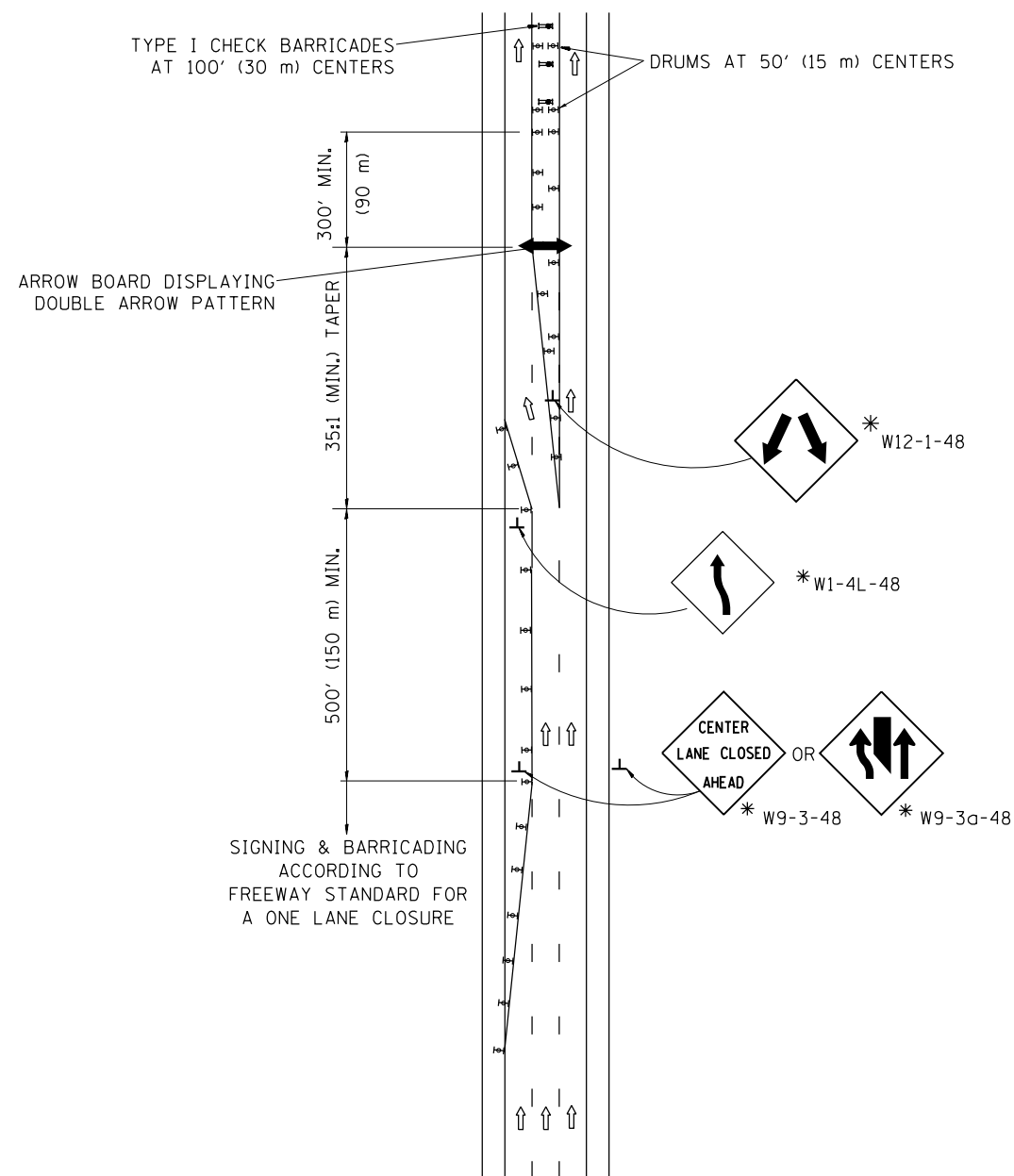
NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

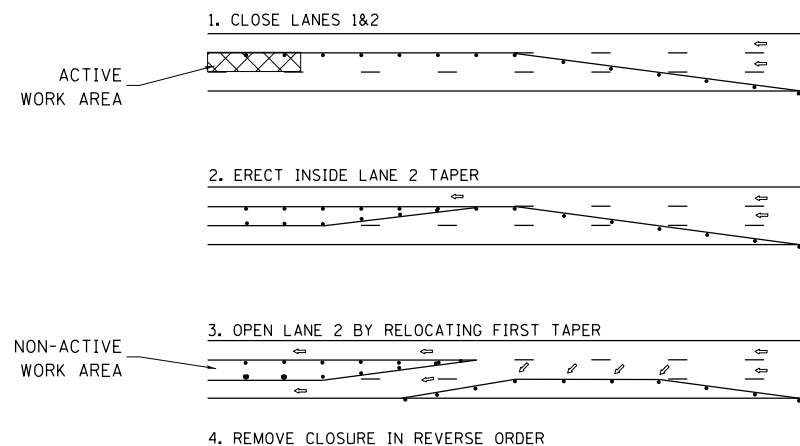
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = rostkowski	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI358\Drawings\Design\DistStd.dgn		REVISED - R. MIRS 12-11-97				346	2018-51-RS&CR	LAKE	31	30
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99				TC-22		CONTRACT NO. 62G96		
PLOT DATE = 2/5/2019	DATE -	REVISED - C. JUCIUS 01-31-07				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

CENTER LANE CLOSURE



INSTALLATION SEQUENCE

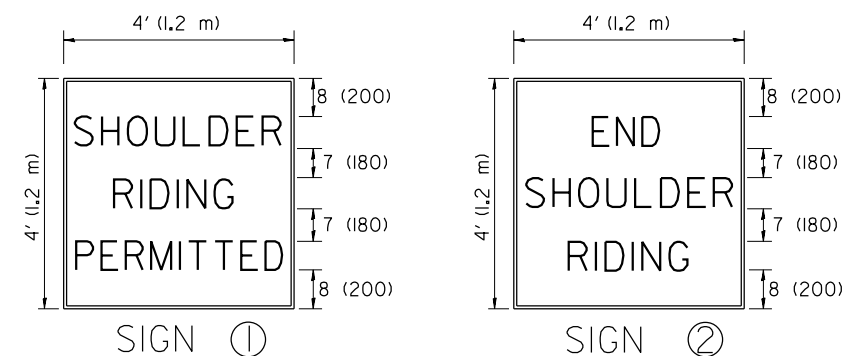
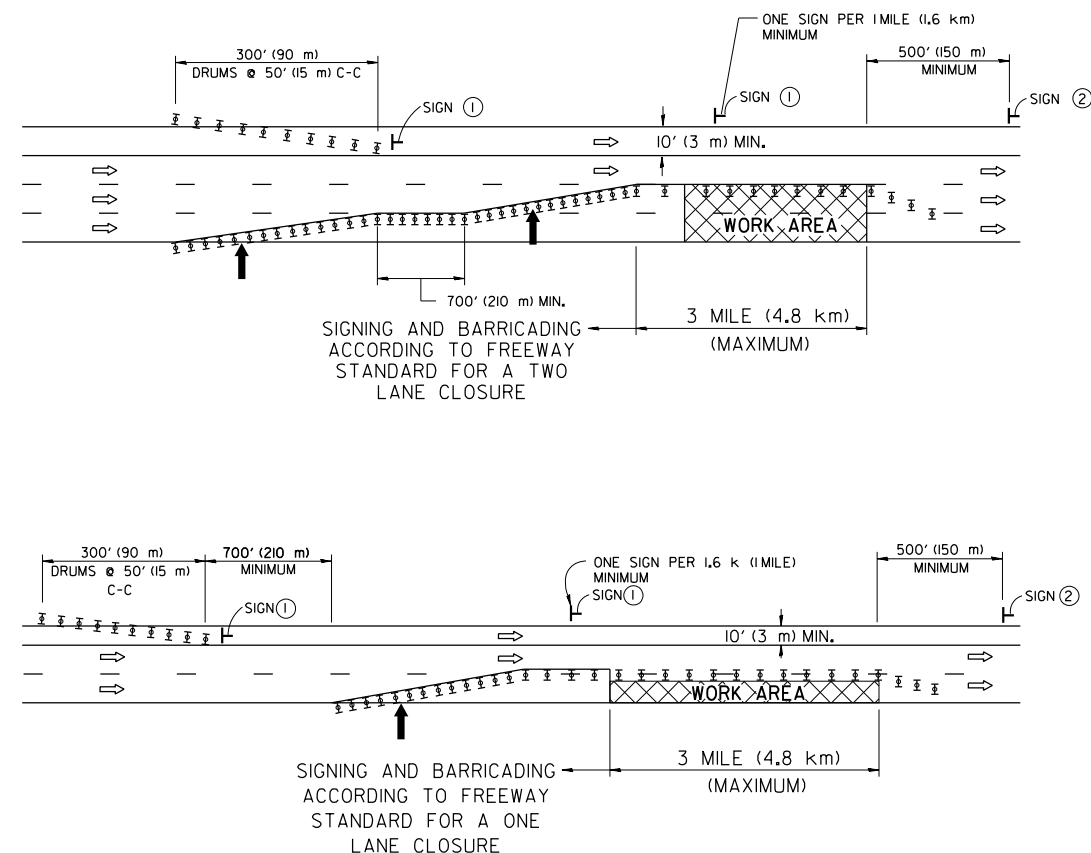


NOTES

1. DRUMS WITH STEADY BURN LIGHTS SHALL BE USED AT 50' (15 m) CENTERS ON ALL TAPERS AND TANGENTS IN ADVANCE OF WORK AREA.
2. CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.
3. CENTER LANE CLOSURE CONFIGURATION IS NOT TO BE USED WITH WORKERS PRESENT.

SHOULDER LANE

NOTE: CLOSURE SHALL BE USED ONLY FOR OPERATIONS LASTING 72 HOURS OR LESS.



6 (150) SERIES "C" LEGEND
BLACK LEGEND
WHITE REFLECT. BACKGROUND
1(25) BORDER

SYMBOLS

- ↑ DIRECTION OF TRAFFIC
- ➔ ARROWBOARD
- ▣ ACTIVE WORK AREA
- ⊥ SIGN ON PORTABLE OR PERMANENT SUPPORT *
- ⊥ TYPE II BARRICADE, OR DRUM WITH MONO-DIRECTIONAL STEADY BURN LIGHT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

* ALL SIGNS SHALL BE MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).

FILE NAME =	USER NAME = rostkowski	DESIGNED -	REVISED - J.A.F. 04-03
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		DATE -	REVISED - S.P.B. 12-09
			REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY
CENTER LANE CLOSURE SHOULDER LANE

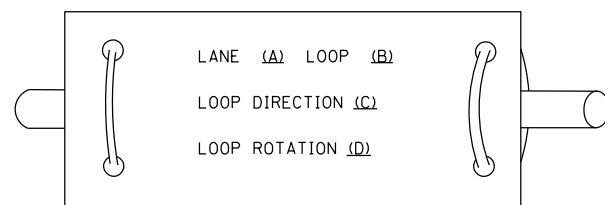
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-25		CONTRACT NO. 62G96		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

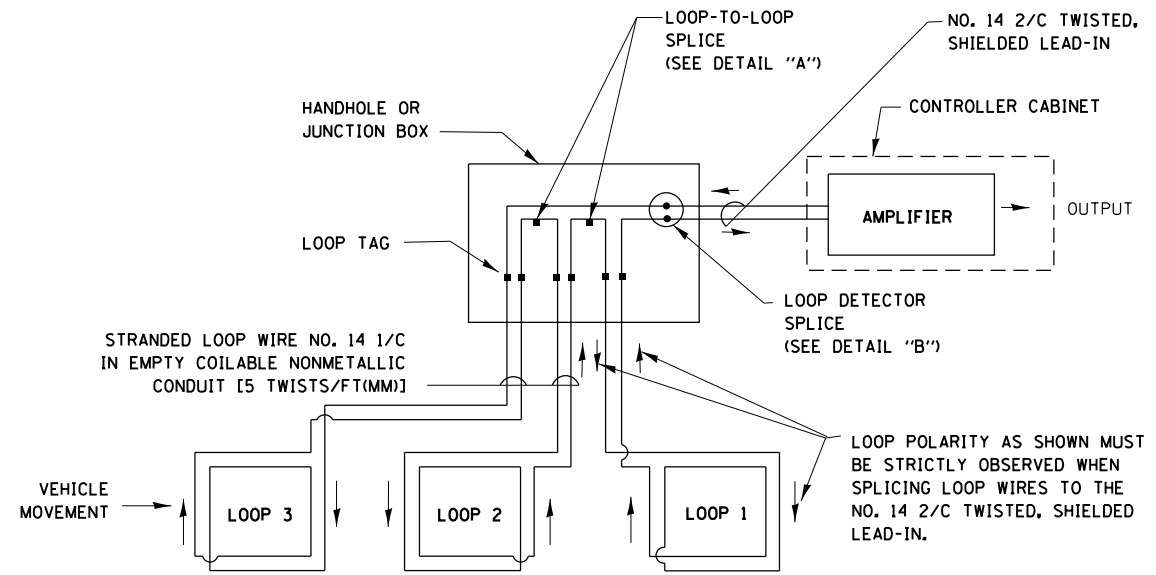
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

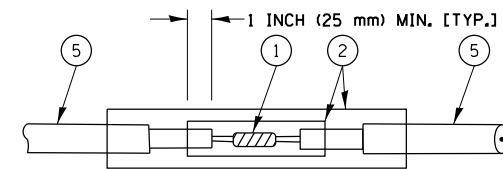


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

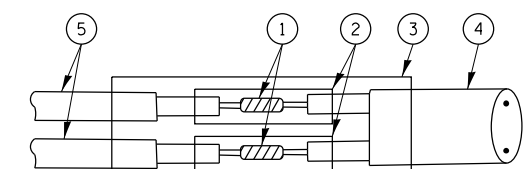


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

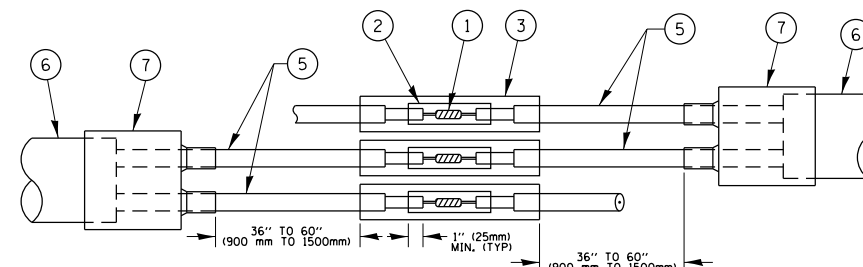


DETAIL "A"
LOOP-TO-LOOP SPLICE

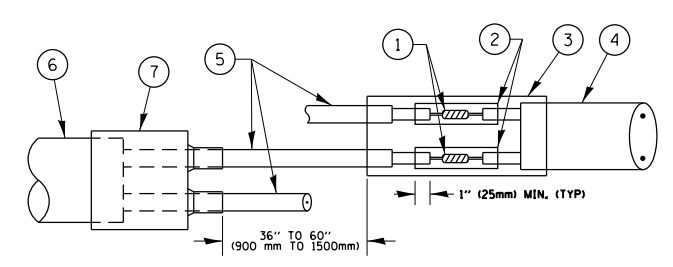


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH, THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = rostkowski	DESIGNED -	REVISED -
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Default	PLOT DATE = 2/5/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.P. R.I.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
346	2018-51-RS&CR	LAKE	31	31
TS-05		CONTRACT NO. 62G96		
ILLINOIS FED. AID PROJECT				