04-26-2019 LETTING ITEM 115

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	1
	ILLINOIS	CONTRACT NO. 61F72		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

FAU 2588 (JANES AVENUE)
75th STREET TO CENTER DRIVE
RESURFACING, CURB AND GUTTER, PAVEMENT MARKING
SECTION: 18–00080–00–RS
PROJECT NO. IY60(358)
VILLAGE OF WOODRIDGE
DUPAGE COUNTY
C–91–219–18

FOR INDEX OF SHEETS AND STANDARDS, SEE SHEET NO. 2

LOCATION OF SECTION INDICATED THUS: –■–

TRAFFIC DATA

JANES AVENUE:

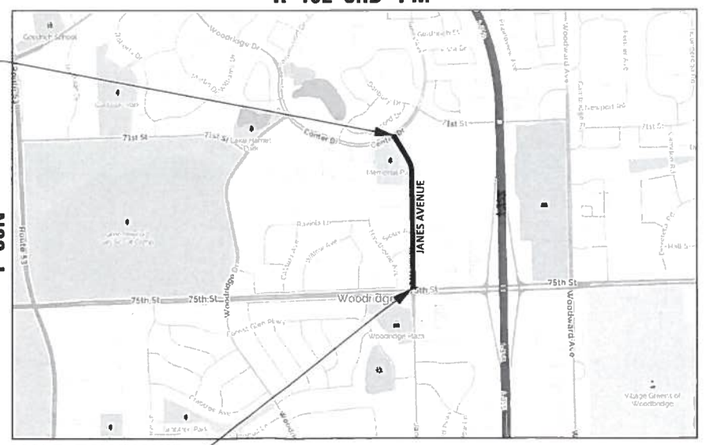
HIGHWAY CLASSIFICATION: MAJOR COLLECTOR

ADT (2016) = 6600

POSTED SPEED LIMIT = 30 MPH

R 10E 3RD PM

T 38N

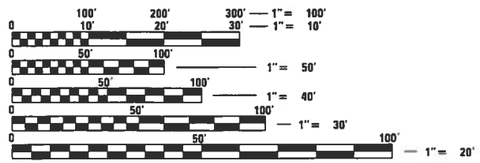


LISLE TOWNSHIP

N

LOCATION MAP
(NOT TO SCALE)

GROSS LENGTH = 2,452 FT. = 0.46 MILE
NET LENGTH = 2,452 FT. = 0.46 MILE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

SCOTT R. KASPER
062-041391
REGISTERED PROFESSIONAL ENGINEER OF ILLINOIS

Scott R. Kasper 1/14/19
Expires 11/30/19

CHASTAIN & ASSOCIATES LLC
CONSULTING ENGINEERS
SERVICE | SOLUTIONS | COMMITMENT™

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED January 11 20 19
VILLAGE OF WOODRIDGE, DIRECTOR OF PUBLIC WORKS

PASSED 2/20/ 20 19
DISTRICT ONE ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW FEBRUARY 20 20 19
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 61F72

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E. SCHAUMBURG, IL

INDEX OF SHEETS

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-04	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-05	DEPRESSED CORNER FOR SIDEWALKS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
424031-02	MEDIAN PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606306-04	CORRUGATED PC CONCRETE MEDIANS
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
720006-04	SIGN PANEL ERECTION DETAILS
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOURS NOTIFICATION REQUIRED). THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNER OF ALL EXISTING UTILITIES FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AND VILLAGE.
3. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE ENGINEER. THE CONTRACTOR SHALL CONTACT THE VILLAGE OF WOODRIDGE AT (630) 719-4753 A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON PUBLIC PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE ENGINEER.
5. THE STORAGE OF EQUIPMENT AND/OR MATERIALS WITHIN THE RIGHT-OF-WAY OF ANY STREET AND/OR PARK PROPERTY SHALL REQUIRE PRIOR APPROVAL OF THE ENGINEER.
6. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
7. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
8. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
9. UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURE AS DETERMINED AND APPROVED IN WRITING BY THE RESIDENT ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.
10. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES WHERE THE SPEED IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1V:3H.
11. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
12. THE CONTRACTOR SHALL VERIFY THAT ALL CRACKS, JOINTS, AND FLANGEWAYS ARE CLEAN AND DRY PRIOR TO PLACEMENT OF MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS.
13. PRIOR TO APPLYING HOT-MIX ASPHALT TACK COAT, THE BASE SURFACE INCLUDING GUTTERS SHALL BE CLEANED OF LOOSE MATERIALS. ALL CRACK FILL MATERIAL SHALL BE REMOVED IN ITS ENTIRETY ALONG THE CURB LINE.
14. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKINGS LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
15. PAVEMENT MARKING TAPE, TYPE 4, SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPLACEMENT OF ANY DETECTOR LOOPS DAMAGED DURING CONSTRUCTION.
17. THE CONTRACTOR SHALL COORDINATE ALL WORK IMPACTING THOMAS JEFFERSON JR. HIGH SCHOOL WITH REPRESENTATIVES OF WOODRIDGE SCHOOL DISTRICT 68 AT (630) 985-7925.
18. THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES, AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
19. THE CONTRACTOR SHALL VERIFY THE RIM & INVERTS OF ALL EXISTING AND PROPOSED STORM SEWER STRUCTURES PRIOR TO CONSTRUCTION.
20. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS THAT WILL NOT BE REMOVED INCLUDING PREVIOUSLY SEEDED AREAS. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE ENGINEER.
21. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER.
22. ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS, AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT, SHALL BE EPOXY COATED, UNLESS NOTED ON THE PLANS.
23. THE MINIMUM THICKNESS OF THE PROPOSED GUTTER FLAG SHALL BE 10 INCHES UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER.
24. THE CONTRACTOR SHALL MAKE FULL DEPTH SAW CUTS AT THE EDGE OF PAVEMENT ADJACENT TO THE REMOVAL OF ALL COMBINATION CURB AND GUTTER. THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTERS, SIDEWALKS, AND DRIVEWAYS OR AS DIRECTED BY THE ENGINEER.
25. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS TO THE SATISFACTION OF THE ENGINEER.
26. THE CONTRACTOR SHALL MAINTAIN THE SITE IN A CLEAN AND ORDERLY MANNER, DEBRIS AND SURPLUS MATERIAL SHALL BE REMOVED AND RESTORATION SHALL PROCEED AS THE WORK PROCEEDS. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION. DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED BY THE CONTRACTOR OFF-SITE.
27. CONTRACTOR SHALL USE CAUTION NOT TO DAMAGE ANY TREES WITHIN THE PROJECT AREA. SHOULD ANY TREES BE DAMAGED OR DISTURBED DUE TO CONSTRUCTION ACTIVITIES, CONTRACTOR SHALL REPLACE THEM IN KIND.
28. UNLESS OTHERWISE APPROVED BY THE ENGINEER, CONTRACTOR SHALL BE REQUIRED TO KEEP ALL DRIVEWAY ENTRANCES OPEN TO TRAFFIC. ALL CONCRETE WORK AT ENTRANCES SHALL BE CONSTRUCTED ½ AT A TIME TO ALLOW ACCESS DURING THE CONCRETE CURING PERIOD.
29. A HIGHWAY PERMIT FROM DU PAGE COUNTY DIVISION OF TRANSPORTATION FOR WORK WITHIN THE 75TH STREET RIGHT-OF-WAY, TRAFFIC SIGNAL DETECTOR LOOP REPLACEMENT AND PLACEMENT OF ANY SIGNAGE, OR PAVEMENT MARKINGS IS REQUIRED FOR THIS PROJECT. THE CONTRACTOR SHALL PROVIDE ANY HIGHWAY PERMIT BONDS OR CERTIFICATE OF INSURANCE REQUIRED BY DU PAGE COUNTY. THE COST TO PROVIDE CERTIFICATE OF INSURANCE SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
30. THE CONTRACTOR SHALL CONTACT DU PAGE COUNTY DIVISION OF TRANSPORTATION (630) 407-6900 A MINIMUM OF 48 HOURS PRIOR TO ANY WORK WITHIN THE 75TH STREET RIGHT-OF-WAY. THE CONTRACTOR SHALL ALSO COORDINATE THE TRAFFIC SIGNAL DETECTOR LOOP REPLACEMENT AND PAVEMENT MARKINGS AT THE 75TH STREET INTERSECTION WITH THE COUNTY.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES

SCALE: | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	2
		CONTRACT NO. 61F72		
		ILLINOIS FED. AID PROJECT		

SUMMARY OF QUANTITIES				CONSTR. CODE FEDERAL 65% LOCAL 35%
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005 URBAN
20200100	EARTH EXCAVATION	CU YD	15	15.0
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	2420	2420
25000210	SEEDING, CLASS 2A	ACRE	0.5	0.5
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	45	45
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	45	45
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	45	45
28000510	INLET FILTERS	EACH	26	26
35101400	AGGREGATE BASE COURSE, TYPE B	TON	10	10
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	96	96
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SQ YD	14	14
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	82	82
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	9727	9727
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	20	20
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	594	594
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	156	156.00
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1625	1625
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	2695	2695

SUMMARY OF QUANTITIES				CONSTR. CODE FEDERAL 65% LOCAL 35%
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005 URBAN
42400800	DETECTABLE WARNINGS	SQ FT	204	204
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	14410	14410
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	96	96
44000600	SIDEWALK REMOVAL	SQ FT	2735	2735
44003100	MEDIAN REMOVAL	SQ FT	240	240
44201741	CLASS D PATCHES, TYPE II, 8 INCH	SQ YD	10	10
44201745	CLASS D PATCHES, TYPE III, 8 INCH	SQ YD	58	58
44201747	CLASS D PATCHES, TYPE IV, 8 INCH	SQ YD	429	429
60108204	PIPE UNDERDRAINS, TYPE 2, 4"	FOOT	150	150
60250200	CATCH BASINS TO BE ADJUSTED	EACH	7	7
60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	5
60624600	CORRUGATED MEDIAN	SQ FT	360	360
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	5	5
* 66901001	REGULATED SUBSTANCE PRE-CONSTRUCTION PLAN	LSUM	1	1
* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	3	3
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1

* = SPECIALTY ITEMS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
SUMMARY OF QUANTITIES

SCALE: | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	3
		CONTRACT NO. 61F72		
	ILLINOIS FED. AID PROJECT			

	SUMMARY OF QUANTITIES			CONSTR. CODE FEDERAL 65% LOCAL 35%
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005 URBAN
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	1	1
67100100	MOBILIZATION	LSUM	1	1
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	LSUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	LSUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1	1
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	30	30
70300100	SHORT TERM PAVEMENT MARKING	FOOT	3969	3969
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1320	1320
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	582	582
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	12150	12150
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1660	1660
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	360	360
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	190	190
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	250	250
72400500	RELOCATE SIGN PANEL ASSEMBLY - TYPE A	EACH	1	1
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	291	291
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	6075	6075

	SUMMARY OF QUANTITIES			CONSTR. CODE FEDERAL 65% LOCAL 35%
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005 URBAN
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	830	830
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	180	180
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	95	95
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	125	125
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	882	882
X4400220	CURB REMOVAL AND REPLACEMENT	FOOT	32	32
X4402805	ISLAND REMOVAL	SQ FT	190	190
X6020710	CATCH BASINS TO BE ADJUSTED WITH SPECIAL FRAME AND GRATE	EACH	5	5
X6022900	CATCH BASINS TO BE RECONSTRUCTED (SPECIAL)	EACH	2	2
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	3	3
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	6392	6392
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1970	1970
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	52	52
Z0076870	UNDERDRAIN CONNECTION TO STRUCTURE	EACH	5	5

* = SPECIALTY ITEMS

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
SUMMARY OF QUANTITIES

SCALE: | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	4
		CONTRACT NO. 61F72		
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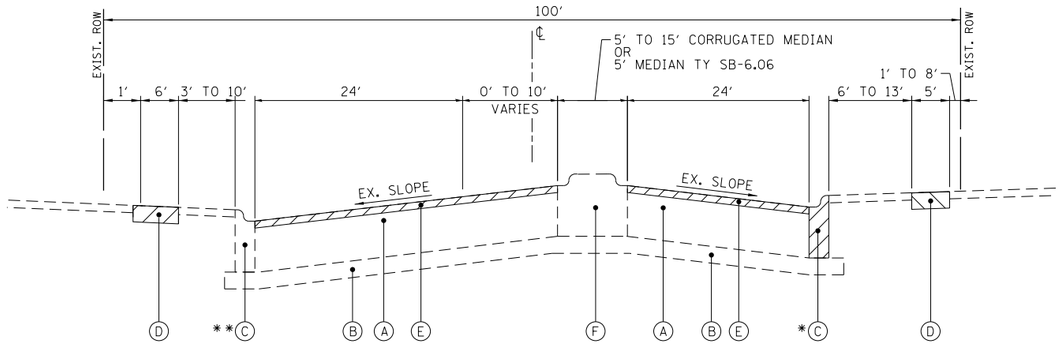
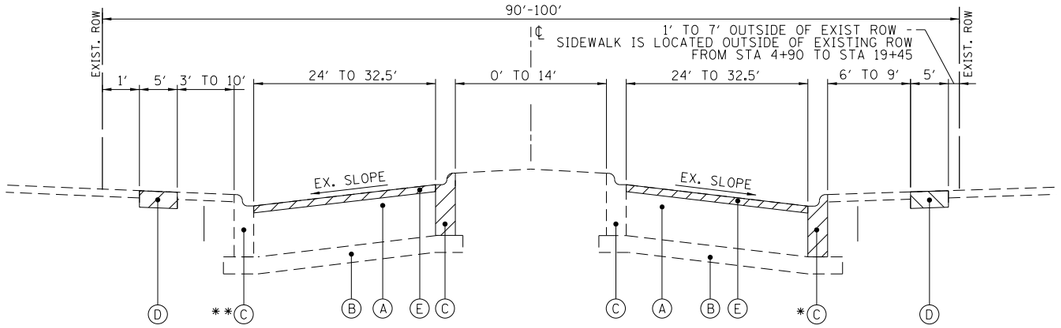


EXISTING TYPICAL SECTION

STA 0+80 TO STA 3+40, JANES AVENUE

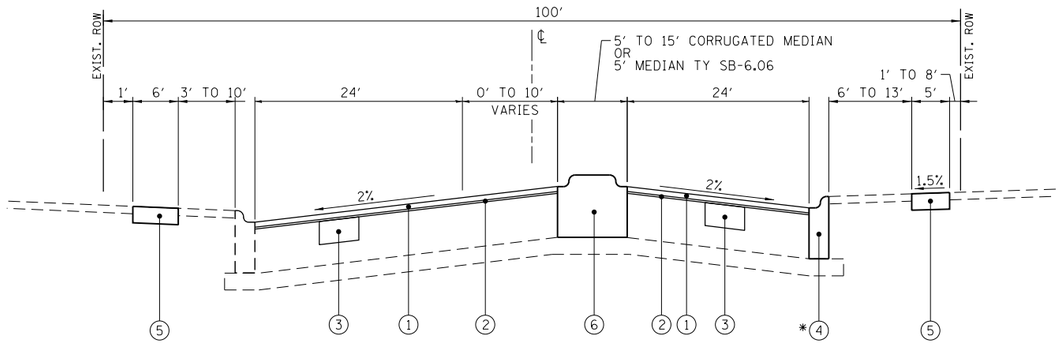


EXISTING TYPICAL SECTION

STA 3+40 TO STA 18+21, JANES AVENUE
STA 22+66 TO STA 25+32, JANES AVENUE

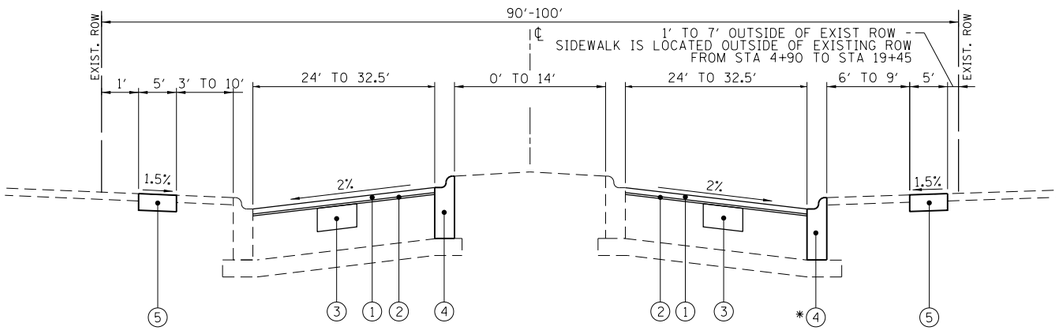
*B-6.24 C&G: STA 0+80 TO STA 1+21
B-6.12 C&G: STA 1+21 TO STA 10+84
B-6.24 C&G: STA 10+84 TO STA 25+32

**B-6.24 C&G: STA 0+80 TO STA 1+58
B-6.12 C&G: STA 1+58 TO STA 17+25
B-6.24 C&G: STA 17+25 TO STA 25+32



PROPOSED TYPICAL SECTION

STA 0+80 TO STA 3+40, JANES AVENUE



PROPOSED TYPICAL SECTION

STA 3+40 TO STA 18+21, JANES AVENUE
STA 22+66 TO STA 25+32, JANES AVENUE

EXISTING LEGEND

- (A) HMA PAVEMENT, 11 ½"
- (B) SUB-BASE GRANULAR MATERIAL, 4"
- (C) COMBINATION CONCRETE CURB & GUTTER
- (D) PCC SIDEWALK
- (E) HMA SURFACE REMOVAL, 2 ¾"
- (F) CONCRETE MEDIAN
  -SB -6.06
  -CORRUGATED
- REMOVAL ITEMS

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50, 2"
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, ¾"
- (3) CLASS D PATCHES, 8"
- (4) COMBINATION CONCRETE CURB & GUTTER, B-6.12
- (5) PCC SIDEWALK 5"
- (6) CONCRETE MEDIAN
  -SB -6.06
  -CORRUGATED

NOTES:
1. CONTRACTOR SHALL MILL PRIOR TO PATCHING OPERATIONS.
2. LIMITS OF PAVEMENT PATCHING AND COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT WILL BE DETERMINED IN FIELD BY THE ENGINEER.
3. COMBINATION CURB & GUTTER WILL BE REPLACED WITH THE SAME TYPE AS REMOVED.

HOT–MIX ASPHALT MIXTURE REQUIREMENTS

OPERATION	MIXTURE TYPE	AIR VOIDS @NDES
PAVEMENT RESURFACING	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2" POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; ¾"	4% @ 50 GYR. 3.5% @ 50 GYR.
COMMERCIAL ENTRANCE	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2" HMA BASE COURSE (HMA BINDER IL-19.0 mm); 8"	4% @ 50 GYR. 4% @ 50 GYR.
PRIVATE DRIVEWAY	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2" HMA BASE COURSE (HMA BINDER IL-19.0 mm); 6"	4% @ 50 GYR. 4% @ 50 GYR.
PATCHING	CLASS D PATCHES (HMA BINDER IL-19.0 mm); 8"	4% @ 70 GYR.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

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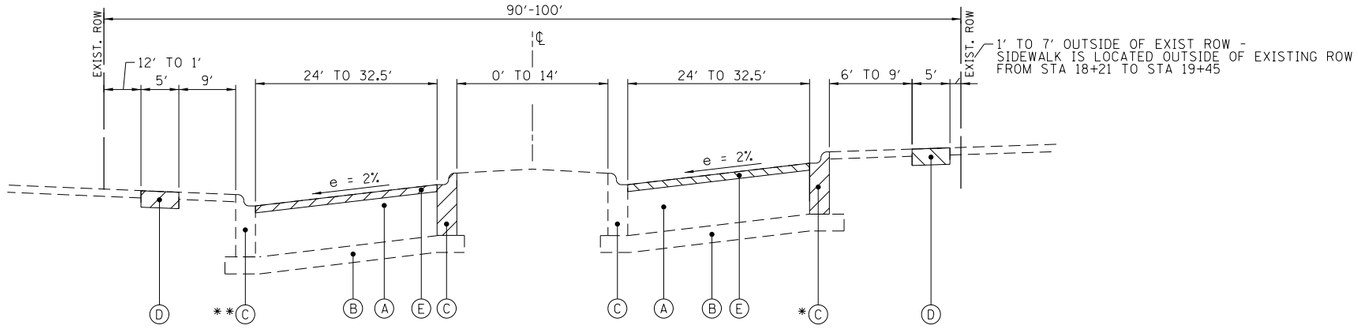
	PLOT DATE = 2/11/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
TYPICAL SECTIONS

SCALE: | SHEET 1 OF 2 SHEETS | STA. 0+80 | TO STA. 25+25

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	5
		CONTRACT NO. 61F72		
		ILLINOIS FED. AID PROJECT		

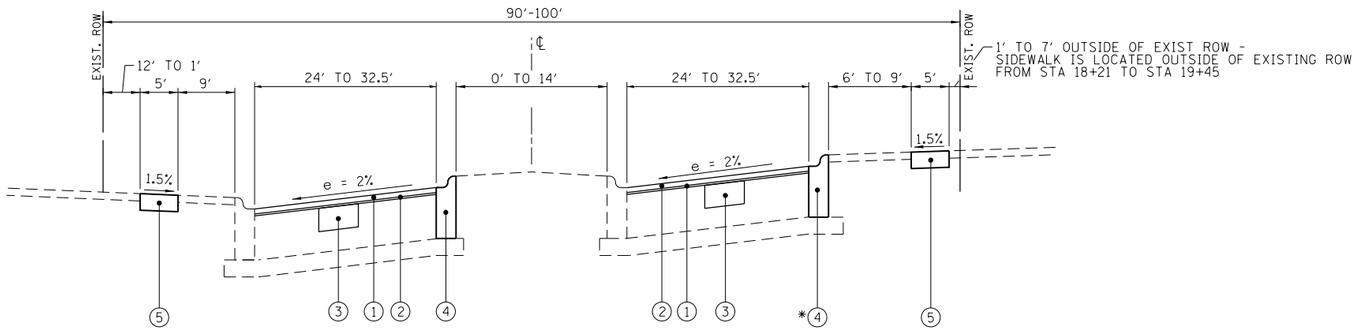


EXISTING TYPICAL SECTION

SUPERELEVATION STA 18+21 TO STA 22+66, JANES AVENUE

*B-6.24 C&G: STA 10+84 TO STA 25+32

**B-6.24 C&G: STA 17+25 TO STA 25+32



PROPOSED TYPICAL SECTION

SUPERELEVATION STA 18+21 TO STA 22+66, JANES AVENUE

EXISTING LEGEND

- (A) HMA PAVEMENT, 11 ½''
- (B) SUB-BASE GRANULAR MATERIAL, 4''
- (C) COMBINATION CONCRETE CURB & GUTTER, B-6.12
- (D) PCC SIDEWALK
- (E) HMA SURFACE REMOVAL, 2 ¾''
- (F) CONCRETE MEDIAN
  -SB -6.06
  -CORRUGATED
- REMOVAL ITEMS

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX ''D'' N50, 2''
- (2) POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50, ¾''
- (3) CLASS D PATCHES, 8''
- (4) COMBINATION CONCRETE CURB & GUTTER, B-6.12
- (5) PCC SIDEWALK 5''
- (6) CONCRETE MEDIAN
  -SB -6.06
  -CORRUGATED

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	PLOT DATE = 2/11/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
TYPICAL SECTIONS

SCALE: | SHEET 2 OF 2 SHEETS | STA. 18+21 | TO STA. 22+66

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	6
		CONTRACT NO. 61F72		
		ILLINOIS FED. AID PROJECT		

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COMBINATION CONCRETE CURB AND GUTTER

STATION	STATION	OFFSET (LT/RT)	LOCATION	COMBINATION CURB AND GUTTER REMOVAL (FOOT)	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (FOOT)	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (FOOT)	CONCRETE CURB, TYPE B (FOOT)
00+86	01+06	LT	OUTSIDE	20.0		20.0	
01+08	01+25	RT	OUTSIDE	17.0		17.0	
01+33	01+55	LT	OUTSIDE	22.0		22.0	
01+55	01+91	LT	OUTSIDE	36.0	36.0		
01+63	02+06	RT	OUTSIDE	43.0	43.0		
02+36	02+49	LT	OUTSIDE	13.0	13.0		
02+58	02+88	RT	OUTSIDE	30.0	30.0		
02+80	02+98	RT	OUTSIDE	18.0			18.0
02+98	03+41	LT	OUTSIDE	43.0	43.0		
03+27	03+41	RT	OUTSIDE	14.0			14.0
03+31	03+48	RT	OUTSIDE	17.0	17.0		
03+56	03+74	RT	MEDIAN	18.0	18.0		
03+61	04+34	LT	OUTSIDE	73.0	73.0		
03+61	03+82	LT	MEDIAN	21.0	21.0		
03+69	03+87	RT	OUTSIDE	18.0	18.0		
03+73	03+88	RT	OUTSIDE	15.0	15.0		
03+98	04+03	LT	MEDIAN	5.0	5.0		
04+03	04+18	RT	OUTSIDE	15.0	15.0		
04+05	04+15	RT	OUTSIDE	10.0	10.0		
04+48	04+54	RT	OUTSIDE	6.0	6.0		
04+54	04+61	LT	OUTSIDE	7.0	7.0		
04+83	04+95	RT	MEDIAN	12.0	12.0		
04+89	05+00	RT	OUTSIDE	11.0	11.0		
05+00	05+08	RT	MEDIAN	8.0	8.0		
05+08	05+13	RT	OUTSIDE	5.0	5.0		
05+16	05+22	LT	OUTSIDE	6.0	6.0		
05+48	05+53	LT	OUTSIDE	5.0	5.0		
05+57	05+67	RT	MEDIAN	10.0	10.0		
05+61	05+79	RT	OUTSIDE	18.0	18.0		
05+92	05+99	LT	OUTSIDE	7.0	7.0		
06+20	06+48	LT	OUTSIDE	28.0	28.0		
06+36	06+43	LT	MEDIAN	7.0	7.0		
06+71	06+83	RT	OUTSIDE	12.0	12.0		
06+86	06+93	LT	MEDIAN	7.0	7.0		
06+88	07+00	LT	OUTSIDE	12.0	12.0		
07+00	07+07	LT	MEDIAN	7.0	7.0		
07+23	07+33	RT	OUTSIDE	10.0	10.0		
07+40	07+55	LT	OUTSIDE	15.0	15.0		
07+45	07+65	RT	MEDIAN	20.0	20.0		
07+82	07+94	LT	OUTSIDE	12.0	12.0		
07+95	08+05	RT	OUTSIDE	10.0	10.0		
07+98	08+03	LT	MEDIAN	5.0	5.0		
08+07	08+13	LT	MEDIAN	6.0	6.0		
08+28	08+41	LT	OUTSIDE	13.0	13.0		
08+32	08+37	RT	OUTSIDE	5.0	5.0		
08+68	08+74	RT	OUTSIDE	6.0	6.0		
08+84	08+97	LT	OUTSIDE	13.0	13.0		
08+90	09+05	RT	OUTSIDE	15.0	15.0		
09+07	09+15	LT	OUTSIDE	8.0	8.0		
09+18	09+36	RT	OUTSIDE	18.0	18.0		
09+38	09+66	LT	OUTSIDE	28.0	28.0		
09+42	09+72	LT	MEDIAN	30.0	30.0		
09+46	09+72	RT	MEDIAN	26.0	26.0		
09+93	10+11	LT	OUTSIDE	18.0	18.0		
10+14	10+34	LT	OUTSIDE	20.0	20.0		
10+14	10+31	RT	MEDIAN	17.0	17.0		

COMBINATION CONCRETE CURB AND GUTTER

STATION	STATION	OFFSET (LT/RT)	LOCATION	COMBINATION CURB AND GUTTER REMOVAL (FOOT)	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (FOOT)	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (FOOT)	CONCRETE CURB, TYPE B (FOOT)
10+23	10+51	RT	OUTSIDE	28.0	28.0		
10+61	10+69	RT	OUTSIDE	8.0	8.0		
10+75	10+81	RT	MEDIAN	6.0	6.0		
10+81	10+92	RT	OUTSIDE	11.0	11.0		
10+86	10+99	LT	OUTSIDE	13.0	13.0		
11+67	11+73	RT	OUTSIDE	6.0	6.0		
11+78	11+98	LT	OUTSIDE	20.0	20.0		
12+18	12+48	LT	OUTSIDE	30.0	30.0		
12+23	12+29	LT	MEDIAN	6.0	6.0		
12+27	12+65	RT	MEDIAN	38.0	38.0		
12+75	13+05	LT	OUTSIDE	30.0	30.0		
12+97	13+03	RT	MEDIAN	6.0	6.0		
13+19	13+26	LT	MEDIAN	7.0	7.0		
13+19	13+27	LT	OUTSIDE	8.0	8.0		
13+26	13+31	RT	MEDIAN	5.0	5.0		
13+28	13+28	RT	OUTSIDE	7.0		7.0	
13+36	13+51	LT	MEDIAN	15.0	15.0		
13+43	13+83	LT	OUTSIDE	40.0	40.0		
13+86	13+86	RT	OUTSIDE	25.0		25.0	
14+25	14+31	LT	OUTSIDE	6.0	6.0		
14+33	14+38	RT	MEDIAN	5.0	5.0		
14+48	14+54	RT	MEDIAN	6.0	6.0		
14+48	14+54	LT	OUTSIDE	6.0	6.0		
14+53	14+59	LT	MEDIAN	6.0	6.0		
14+73	14+80	RT	MEDIAN	7.0	7.0		
14+78	14+83	LT	MEDIAN	5.0	5.0		
14+87	14+94	LT	OUTSIDE	7.0	7.0		
15+00	15+06	LT	OUTSIDE	6.0	6.0		
15+28	15+35	LT	OUTSIDE	7.0	7.0		
15+49	15+49	RT	OUTSIDE	6.0		6.0	
15+54	15+61	RT	MEDIAN	7.0	7.0		
15+57	15+63	LT	OUTSIDE	6.0	6.0		
15+64	15+64	RT	OUTSIDE	24.0		24.0	
15+72	15+98	RT	MEDIAN	26.0	26.0		
15+90	15+96	LT	OUTSIDE	6.0	6.0		
16+13	16+13	RT	OUTSIDE	6.0		6.0	
16+14	16+20	LT	MEDIAN	6.0	6.0		
16+15	16+20	LT	OUTSIDE	5.0	5.0		
16+38	16+38	RT	OUTSIDE	6.0		6.0	
16+45	17+13	LT	OUTSIDE	68.0	68.0		
16+59	16+65	LT	MEDIAN	6.0	6.0		
16+83	16+83	RT	OUTSIDE	6.0		6.0	
17+23	17+28	RT	MEDIAN	5.0	5.0		
17+43	17+49	RT	MEDIAN	6.0	6.0		
17+43	17+43	LT	OUTSIDE	6.0		6.0	
17+58	17+63	RT	MEDIAN	5.0	5.0		
17+61	17+61	LT	OUTSIDE	7.0		7.0	
17+87	17+87	LT	OUTSIDE	35.0		35.0	
17+95	17+95	RT	OUTSIDE	10.0		10.0	
18+43	18+43	LT	OUTSIDE	6.0		6.0	
19+04	19+09	LT	MEDIAN	5.0	5.0		
19+18	19+23	LT	MEDIAN	5.0	5.0		
19+28	19+28	LT	OUTSIDE	10.0		10.0	
19+28	19+34	RT	MEDIAN	6.0	6.0		
19+46	19+46	LT	OUTSIDE	10.0		10.0	
19+47	19+53	RT	MEDIAN	6.0	6.0		

COMBINATION CONCRETE CURB AND GUTTER

STATION	STATION	OFFSET (LT/RT)	LOCATION	COMBINATION CURB AND GUTTER REMOVAL (FOOT)	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12 (FOOT)	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24 (FOOT)	CONCRETE CURB, TYPE B (FOOT)
19+52	19+58	LT	MEDIAN	6.0	6.0		
19+64	19+70	LT	MEDIAN	6.0	6.0		
19+96	19+96	RT	OUTSIDE	68.0		68.0	
20+05	20+05	LT	OUTSIDE	10.0		10.0	
20+72	20+72	LT	OUTSIDE	10.0		10.0	
20+21	20+21	LT	OUTSIDE	5.0		5.0	
20+63	20+99	RT	MEDIAN	36.0	36.0		
20+94	20+94	RT	OUTSIDE	6.0		6.0	
21+30	21+40	RT	MEDIAN	10.0	10.0		
21+43	21+48	LT	MEDIAN	5.0	5.0		
21+60	21+60	RT	OUTSIDE	5.0		5.0	
21+82	21+88	LT	MEDIAN	6.0	6.0		
21+83	21+93	RT	MEDIAN	10.0	10.0		
22+15	22+15	LT	OUTSIDE	6.0		6.0	
22+20	22+25	LT	MEDIAN	5.0	5.0		
22+38	22+38	RT	OUTSIDE	36.0		36.0	
22+56	22+62	LT	MEDIAN	6.0	6.0		
22+61	22+61	LT	OUTSIDE	6.0		6.0	
22+96	22+96	RT	OUTSIDE	6.0		6.0	
22+98	22+98	LT	OUTSIDE	5.0		5.0	
23+27	23+27	LT	OUTSIDE	6.0		6.0	
23+31	23+31	RT	OUTSIDE	48.0		48.0	
23+43	23+49	LT	MEDIAN	6.0	6.0		
23+83	23+83	LT	OUTSIDE	28.0		28.0	
23+94	24+00	LT	MEDIAN	6.0	6.0		
23+94	24+00	RT	MEDIAN	6.0	6.0		
23+94	23+94	RT	OUTSIDE	30.0		30.0	
24+34	24+39	LT	MEDIAN	5.0	5.0		
24+59	24+59	LT	OUTSIDE	8.0		8.0	
24+67	24+73	LT	MEDIAN	6.0	6.0		
24+87	24+87	RT	OUTSIDE	8.0		8.0	
24+89	24+89	LT	OUTSIDE	5.0		5.0	
25+18	25+24	LT	MEDIAN	6.0	6.0		
TOTAL B R&R =				32.0			
TOTAL B-6.12 R&R =				1451.0			
TOTAL B-6.24 R&R =				519.0			
ROUNDED TOTAL				2002.0	1451.0	519.0	32.0

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
SCHEDULE OF QUANTITIES

SCALE: | SHEET 1 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	7
		CONTRACT NO. 61F72		
	ILLINOIS	FED. AID PROJECT		

SIDEWALK & CURB RAMP REMOVAL & REPLACEMENT						
STATION	STATION	OFFSET (LT/RT)	WIDTH (FOOT)	SIDEWALK REMOVAL (SQ FT)	PCC SIDEWALK 5 INCH (SQ FT)	DETECTABLE WARNINGS (SQ FT)
01+05	01+20	RT	5	75	75	10
01+50	01+70	RT	5	100	100	10
02+68	02+93	RT	5	125	125	10
02+93	03+08	LT	6	90	90	12
03+30	03+43	LT	6	75	75	12
03+30	03+45	RT	5	75	75	10
03+57	03+77	RT	5	100	100	10
04+07	04+17	RT	5	50	50	10
04+90	05+10	RT	5	100	100	
06+00	06+15	LT	6	90	90	
07+54	07+59	LT	6	30	30	
07+96	08+01	LT	6	30	30	
08+68	08+73	LT	6	30	30	
09+11	09+16	LT	6	30	30	
09+44	09+57	RT	5	150	150	20
09+49	09+54	LT	5	75	75	10
09+49	09+64	CL	5	75	75	20
09+50	09+65	RT	5	75	75	10
09+92	10+17	LT	6	220	180	12
10+27	10+40	RT	5	175	175	20
11+10	11+20	LT	6	60	60	
12+50	12+55	LT	6	30	30	
17+62	17+67	LT	5	25	25	
18+27	18+37	LT	5	50	50	
19+72	19+82	RT	5	50	50	14
20+45	20+55	RT	5	50	50	14
20+67	20+97	RT	5	150	150	
22+08	22+33	LT	5	125	125	
22+46	22+96	LT	5	250	250	
23+23	23+28	LT	5	25	25	
23+47	23+67	RT	5	100	100	
24+04	24+14	RT	5	50	50	
ROUNDED TOTAL				2735	2695	204

APRONS				
STATION	OFFSET (LT/RT)	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, TNS	HOT-MIX ASPHALT BASE COURSE 6" SY (Private)	HOT-MIX ASPHALT BASE COURSE 8" SY (Commercial)
3+00.00	LT	2.49		22.2
7+83.00	LT	0.41	3.7	
8+88.00	LT	0.45	4.0	
10+89.00	LT	0.45	4.0	
11+10.00	LT	0.19	1.7	
13+80.00	RT	0.71		6.3
19+95.00	RT	2.50		22.3
22+36.00	RT	0.63		5.7
23+40.00	RT	2.81		25.1
ROUNDED TOTAL		11.0	14.0	82.0

CLASS D PATCHES				
STATION	OFFSET (LT/RT)	CLASS D PATCHES, TYPE II, 8 INCH (SQ YD)	CLASS D PATCHES, TYPE III, 8 INCH (SQ YD)	CLASS D PATCHES, TYPE IV, 8 INCH (SQ YD)
1+06.00	RT		16	
2+21.00	RT			34
3+85.00	RT		24	
3+98.00	LT			44
9+57.00	RT	9		
10+02.00	RT		17	
19+88.00	LT/RT			154
20+90.00	LT			87
24+52.00	LT			109
ROUNDED TOTAL		10.0	58.0	429.0

MEDIAN REMOVAL & REPLACEMENT				
STATION	STATION	OFFSET (LT/RT)	MEDIAN REMOVAL (SQ FT)	CORRUGATED MEDIAN (SQ FT)
02+95	03+05	LT/RT	105	105
03+25	03+35	LT/RT	135	135
09+48	09+65	LT/RT	0	120
ROUNDED TOTAL			240	360

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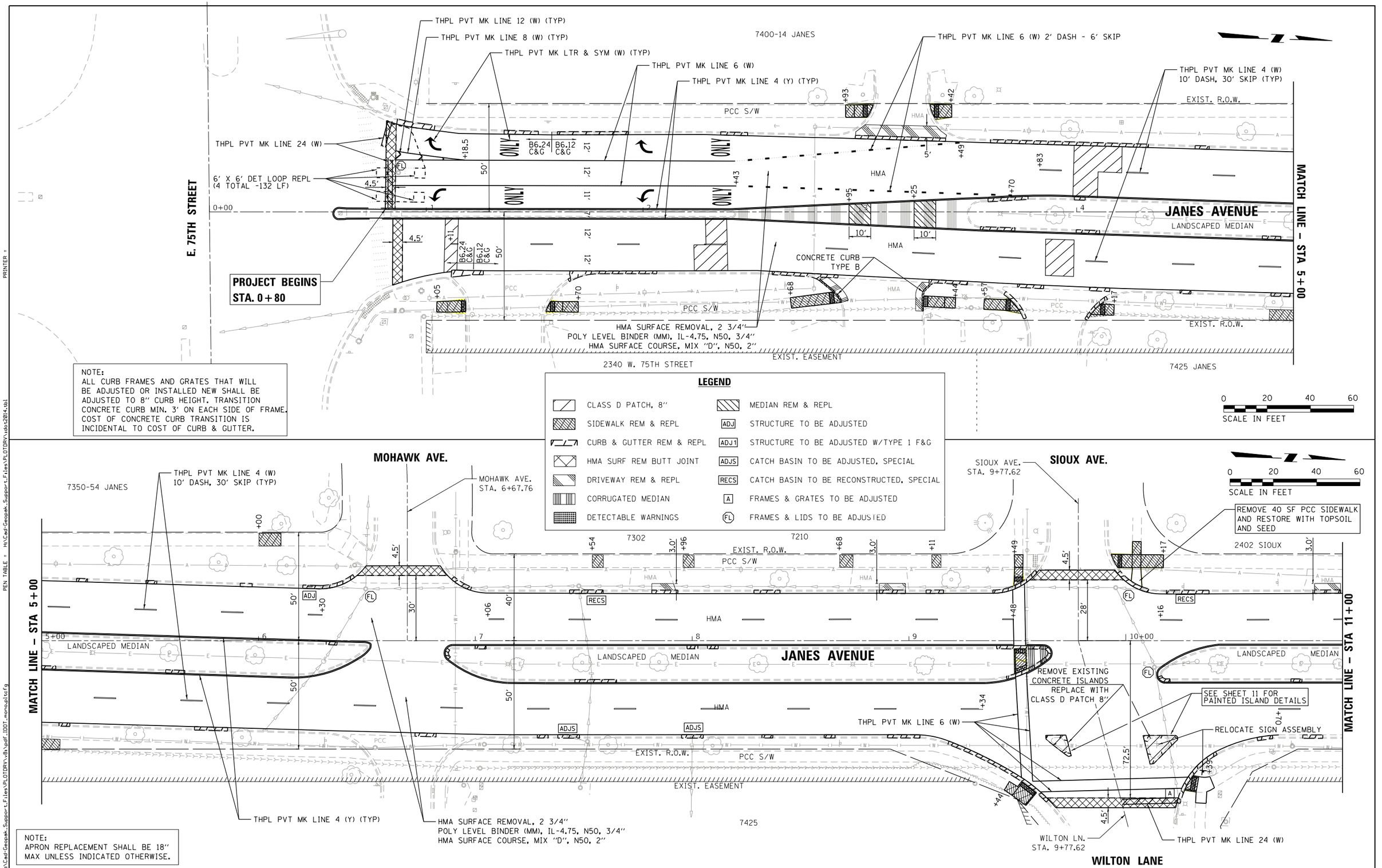
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
SCHEDULE OF QUANTITIES

SCALE: | SHEET 2 OF 2 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	8
		CONTRACT NO. 61F72		
	ILLINOIS FED. AID PROJECT			



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$MODELNAME$	PLOT DATE = 2/11/2019	DATE -	REVISED -

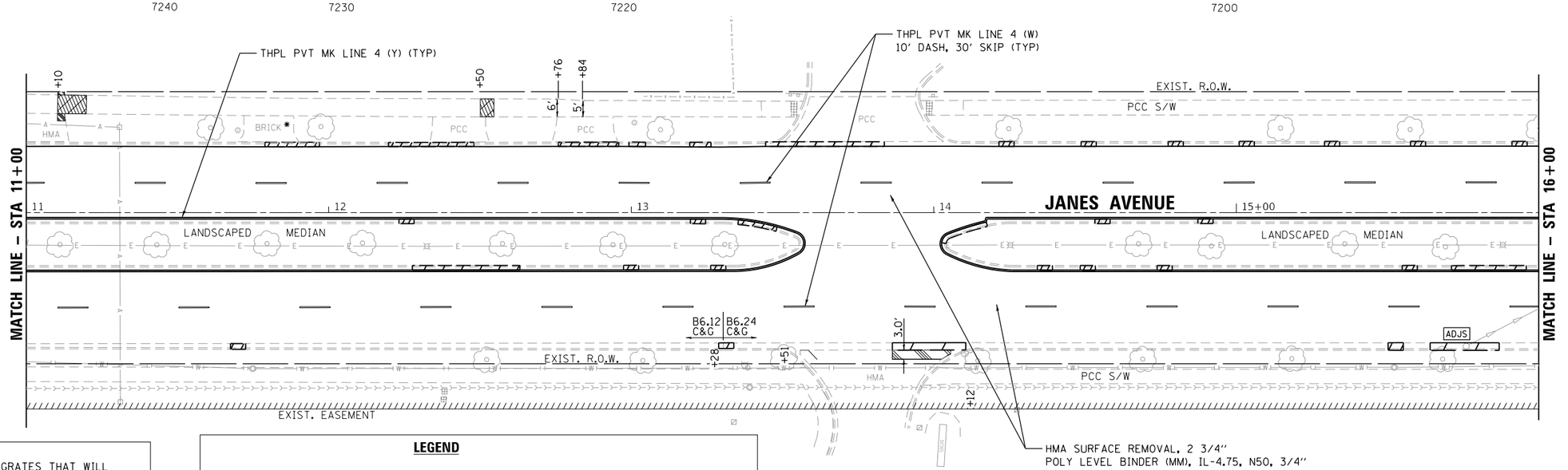
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: 1" = 20' | SHEET 1 OF 3 SHEETS | STA. 0+00 TO STA. 11+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	9
		CONTRACT NO. 61F72		
	ILLINOIS	FED. AID PROJECT		

* CONTRACTOR SHALL PROPERLY RESET ANY APRON BRICK PAVERS DISTURBED DURING CURB REPLACEMENT

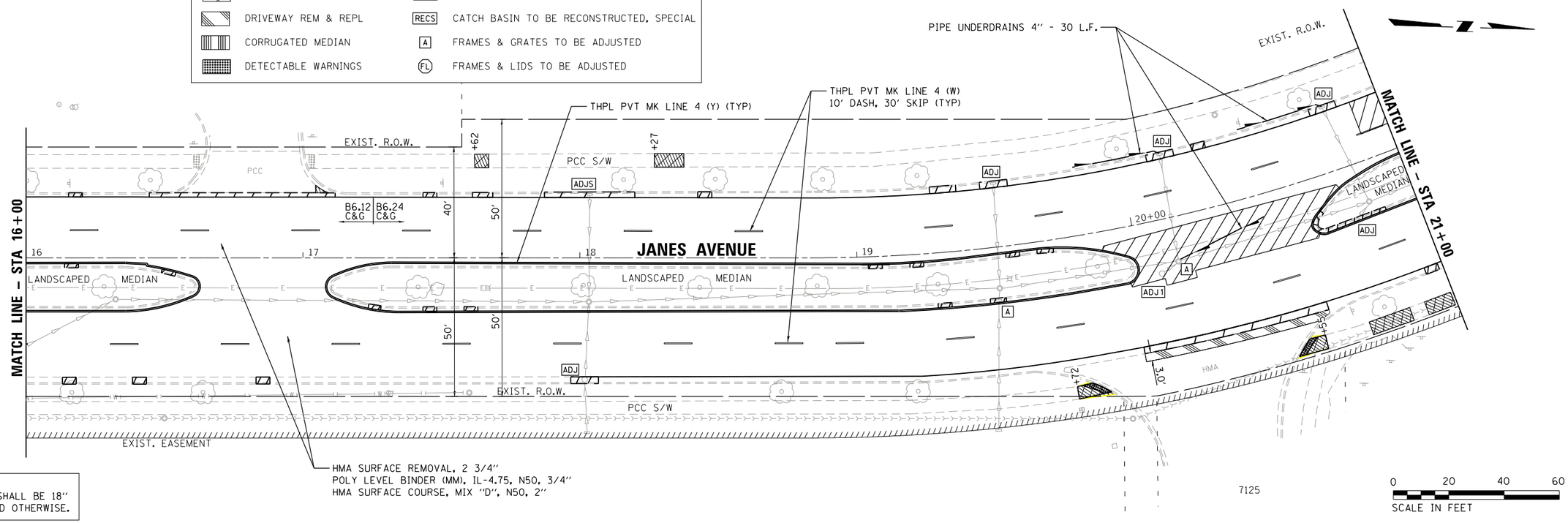


NOTE:
ALL CURB FRAMES AND GRATES THAT WILL BE ADJUSTED OR INSTALLED NEW SHALL BE ADJUSTED TO 8" CURB HEIGHT. TRANSITION CONCRETE CURB MIN. 3' ON EACH SIDE OF FRAME. COST OF CONCRETE CURB TRANSITION IS INCIDENTAL TO COST OF CURB & GUTTER.

LEGEND

	CLASS D PATCH, 8"		MEDIAN REM & REPL
	SIDEWALK REM & REPL	ADJ	STRUCTURE TO BE ADJUSTED
	CURB & GUTTER REM & REPL	ADJ1	STRUCTURE TO BE ADJUSTED W/TYPE 1 F&G
	HMA SURF REM BUTT JOINT	ADJS	CATCH BASIN TO BE ADJUSTED, SPECIAL
	DRIVEWAY REM & REPL	RECS	CATCH BASIN TO BE RECONSTRUCTED, SPECIAL
	CORRUGATED MEDIAN	A	FRAMES & GRATES TO BE ADJUSTED
	DETECTABLE WARNINGS	FL	FRAMES & LIDS TO BE ADJUSTED

0 20 40 60
SCALE IN FEET



NOTE:
APRON REPLACEMENT SHALL BE 18" MAX UNLESS INDICATED OTHERWISE.

0 20 40 60
SCALE IN FEET

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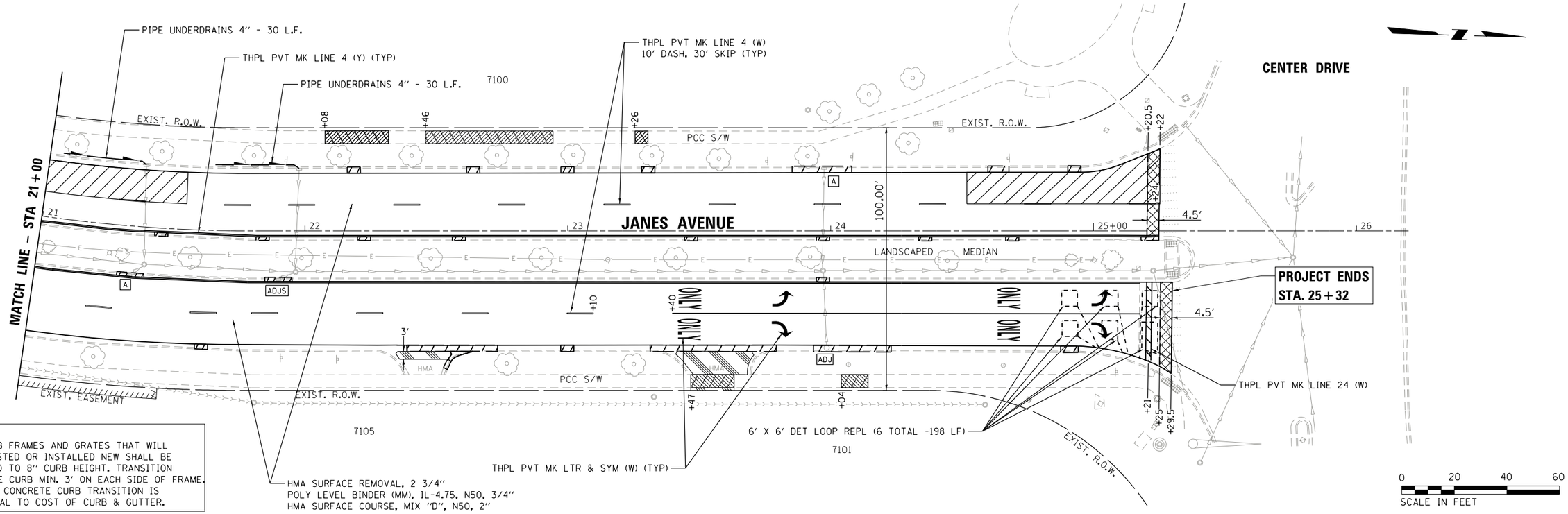
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: 1" = 20' | SHEET 2 OF 3 SHEETS | STA. 11+00 TO STA. 21+50

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	10
		CONTRACT NO. 61F72		
		ILLINOIS FED. AID PROJECT		

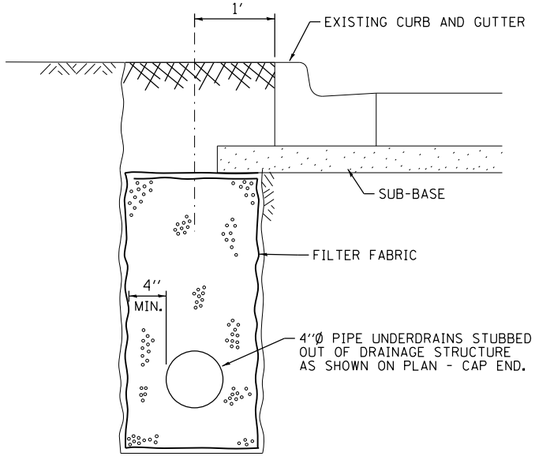


NOTE:
ALL CURB FRAMES AND GRATES THAT WILL BE ADJUSTED OR INSTALLED NEW SHALL BE ADJUSTED TO 8" CURB HEIGHT. TRANSITION CONCRETE CURB MIN. 3' ON EACH SIDE OF FRAME. COST OF CONCRETE CURB TRANSITION IS INCIDENTAL TO COST OF CURB & GUTTER.

LEGEND

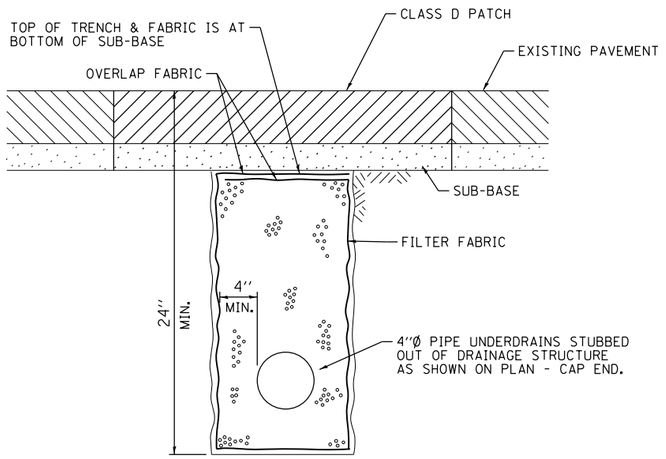
- CLASS D PATCH, 8"
- SIDEWALK REM & REPL
- CURB & GUTTER REM & REPL
- HMA SURF REM BUTT JOINT
- DRIVEWAY REM & REPL
- CORRUGATED MEDIAN
- DETECTABLE WARNINGS
- MEDIAN REM & REPL
- ADJ — STRUCTURE TO BE ADJUSTED
- ADJ1 — STRUCTURE TO BE ADJUSTED W/TYPE 1 F&G
- ADJS — CATCH BASIN TO BE ADJUSTED, SPECIAL
- RECS — CATCH BASIN TO BE RECONSTRUCTED, SPECIAL
- A — FRAMES & GRATES TO BE ADJUSTED
- FL — FRAMES & LIDS TO BE ADJUSTED

NOTE:
APRON REPLACEMENT SHALL BE 18" MAX UNLESS INDICATED OTHERWISE.



FABRIC LINING OF TRENCH, BACKFILL AND BEDDING ARE INCIDENTAL TO THE PIPE UNDERDRAIN TYPE 2 PAY ITEM.

PIPE UNDERDRAINS BEHIND CURB DETAIL



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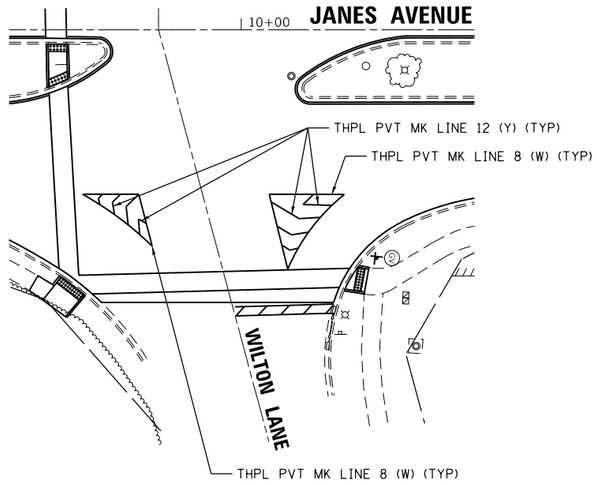
PIPE UNDERDRAINS IN PAVEMENT DETAIL



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
ROADWAY AND PAVEMENT MARKING PLAN

SCALE: 1" = 20' | SHEET 3 OF 3 SHEETS | STA. 21+50 TO STA. 26+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	11
		CONTRACT NO. 61F72		
		ILLINOIS FED. AID PROJECT		

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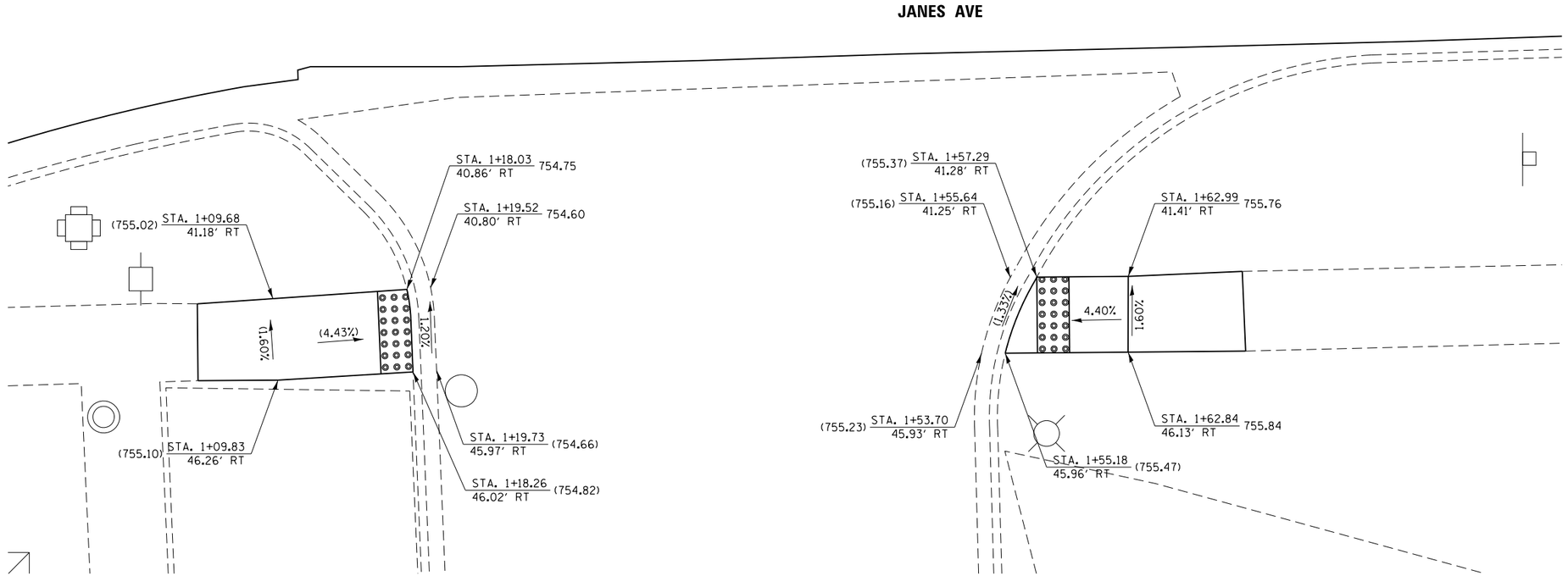
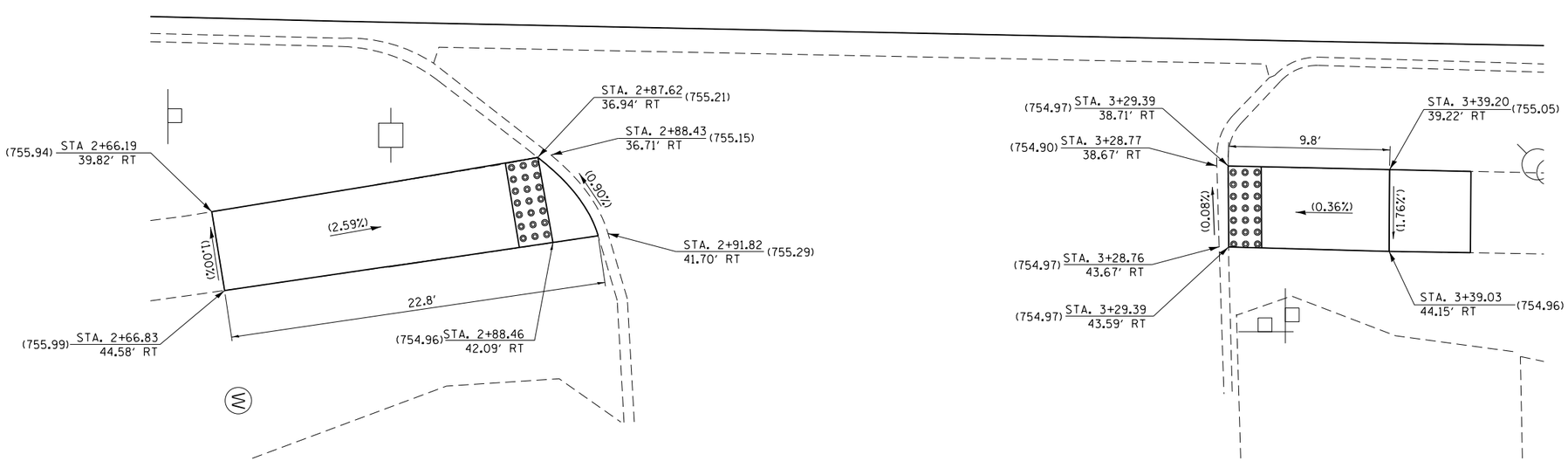


MARATHON PARKING LOT ENTRANCE
STA 1+36.72 RT



McDONALDS PARKING LOT ENTRANCE
STA 3+10.29 RT

N

0 5 10 15
SCALE IN FEET

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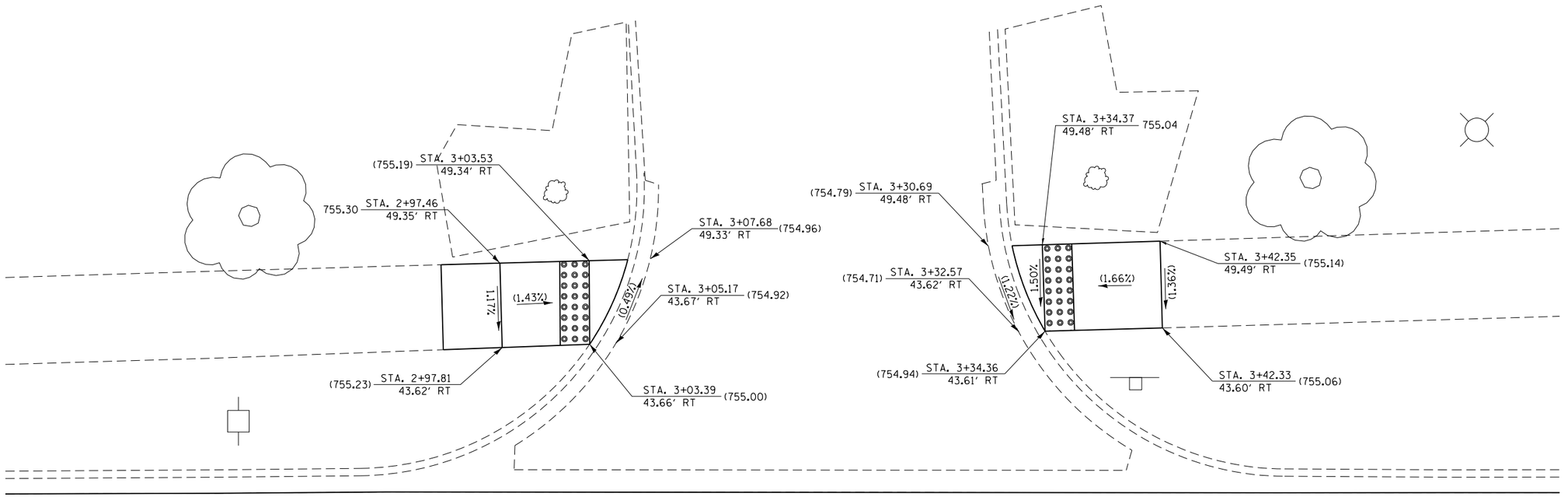
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

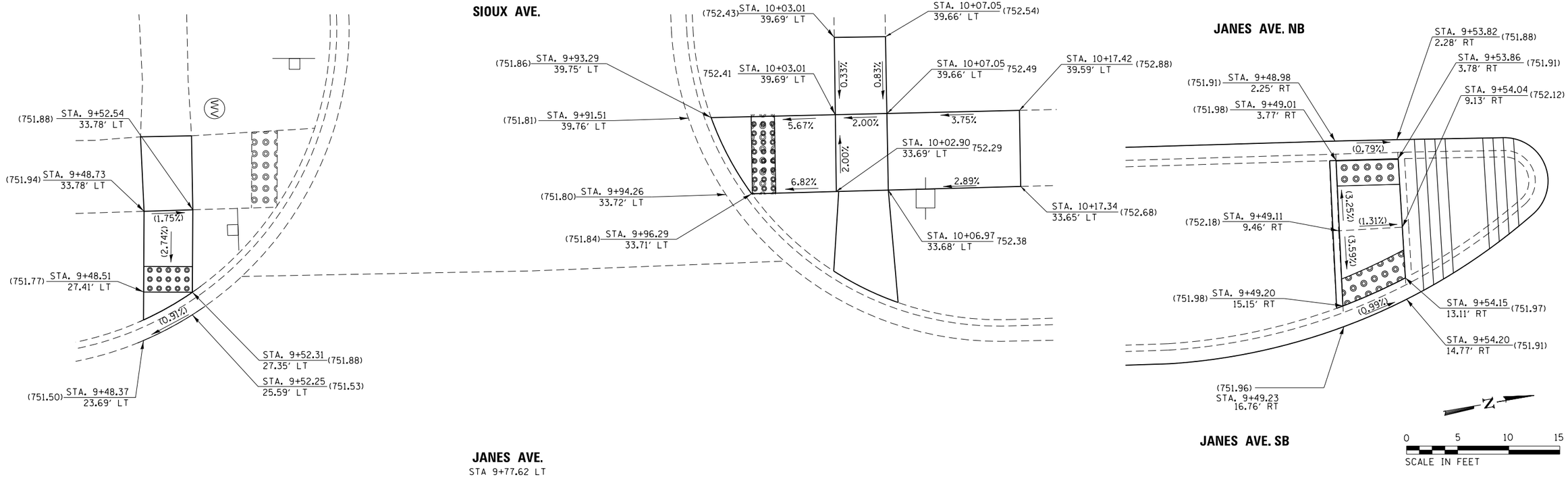
JANES AVENUE – 75th STREET TO CENTER DRIVE
ADA RAMP DETAILS – E. 75TH AND MOHAWK

SCALE: 1"=5' | SHEET 1 OF 3 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	12
		CONTRACT NO. 61F72		
		ILLINOIS FED. AID PROJECT		



JANES AVENUE
STA 3+18.87 LT



JANES AVE.
STA 9+77.62 LT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
ADA RAMP DETAILS – 7414–7400 AND SIOUX AVENUE

SCALE: 1"=5' | SHEET 2 OF 3 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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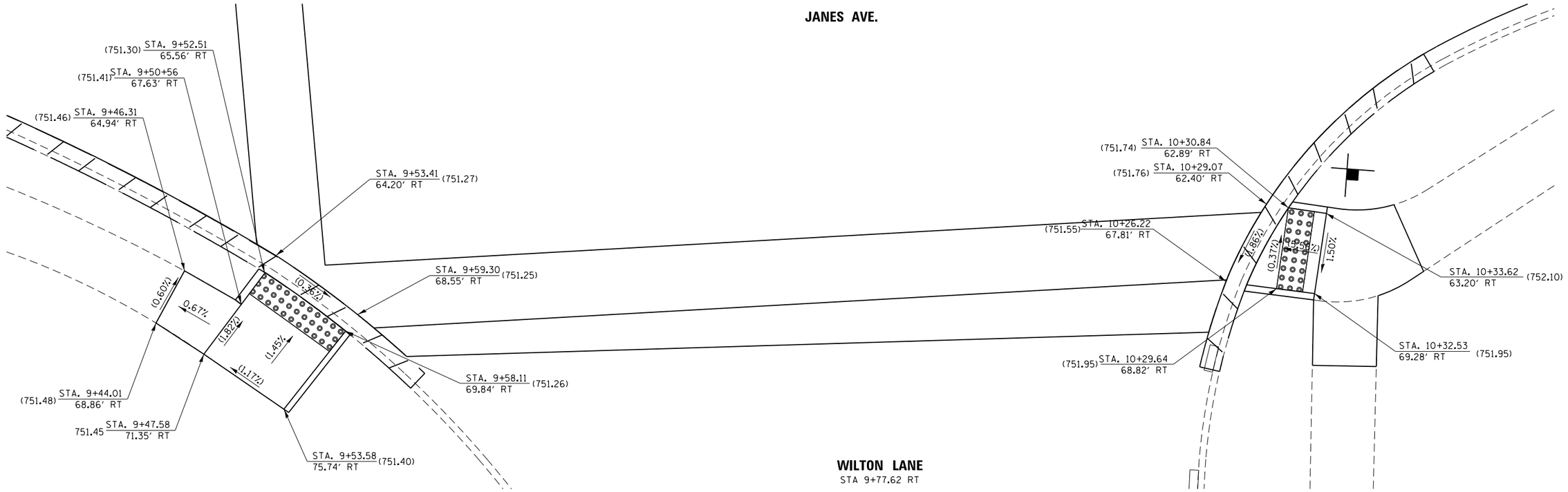
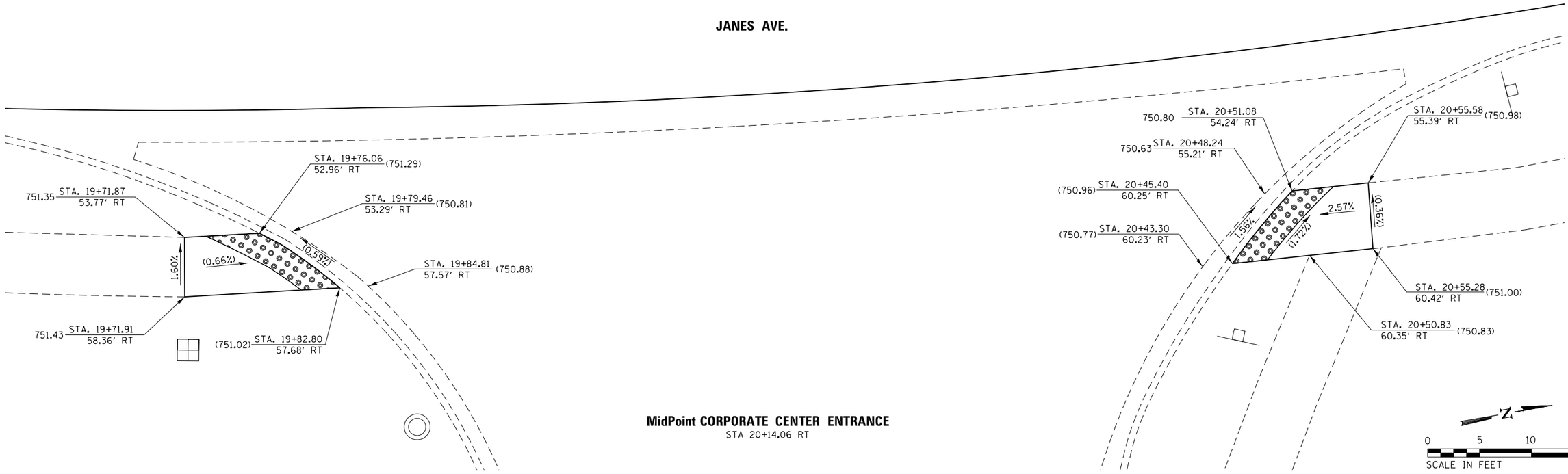
		ILLINOIS FED. AID PROJECT		





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Default	PLOT DATE = 2/11/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE – 75th STREET TO CENTER DRIVE
ADA RAMP DETAILS – WILTON LANE AND MID POINT CORPORATE CENTER

SCALE: 1"=5' | SHEET 3 OF 3 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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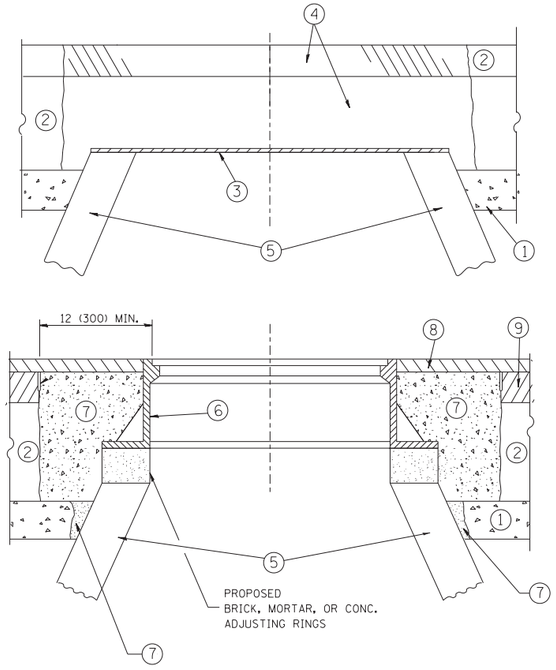
		CONTRACT NO. 61F72		
		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

(1) SUB-BASE GRANULAR MATERIAL
(2) EXISTING PAVEMENT
(3) 36 (900) DIAMETER METAL PLATE
(4) PROPOSED CRUSHED STONE AND HMA SURFACE MIX
(5) EXISTING STRUCTURE
(6) FRAME AND LID (SEE NOTES)
(7) CLASS PP-1* CONCRETE
(8) PROPOSED HMA SURFACE COURSE
(9) PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	15
	BD600-03 (BD-8)		CONTRACT NO. 61F72	
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

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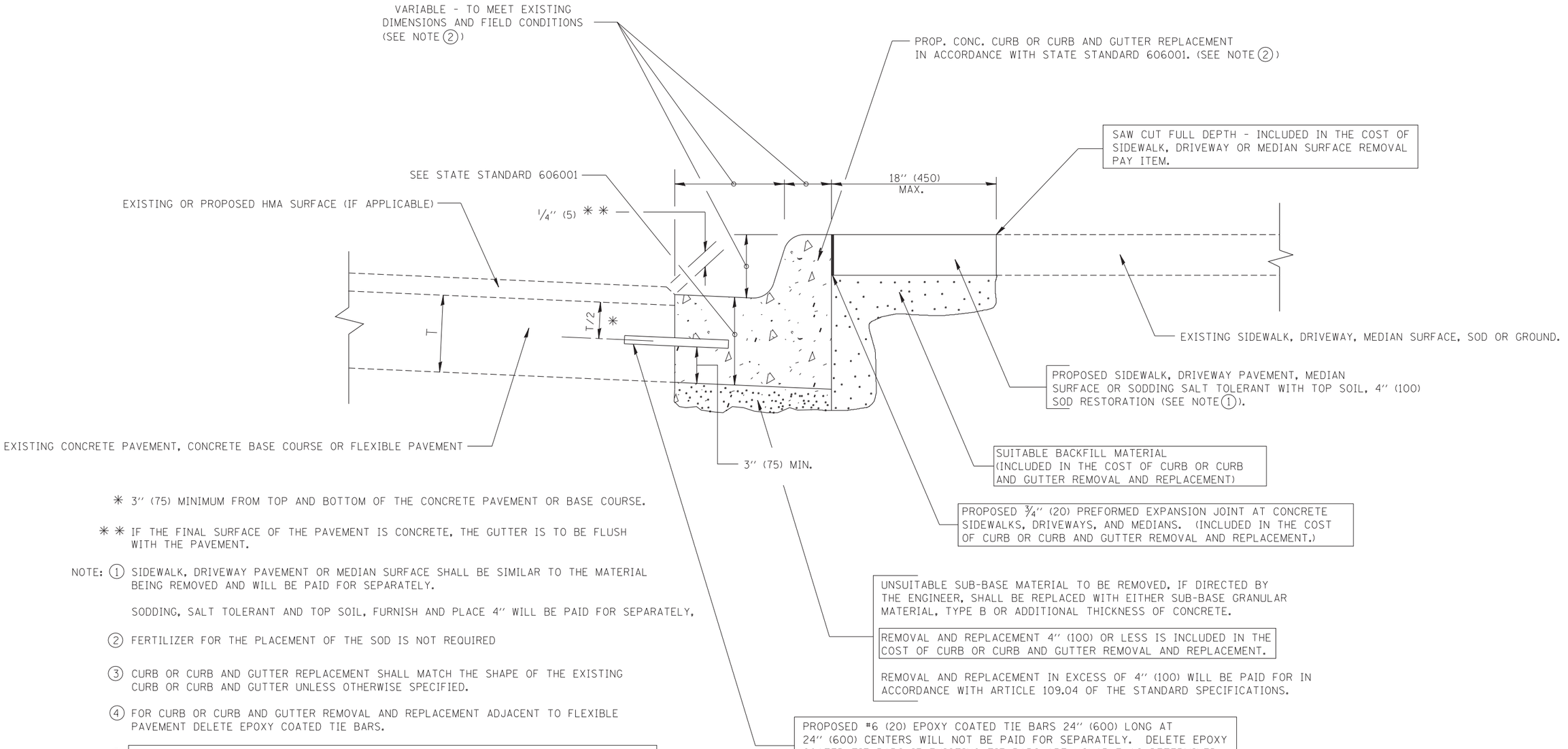

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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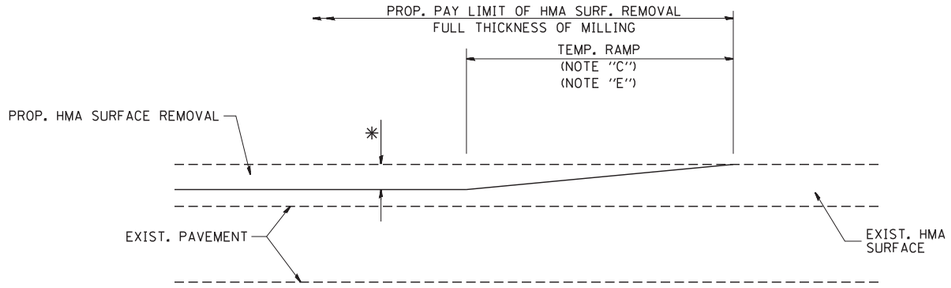
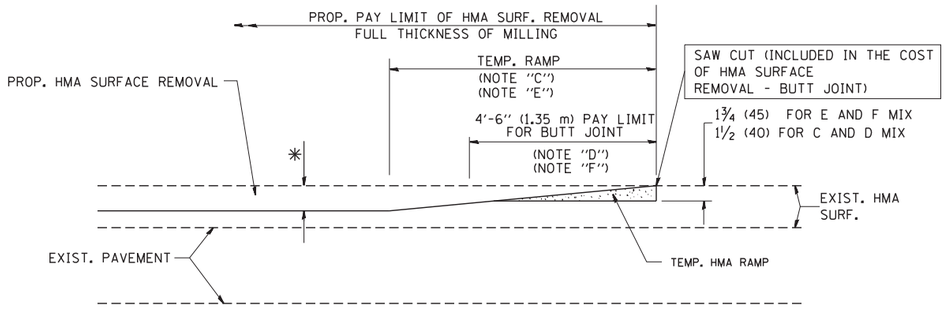
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

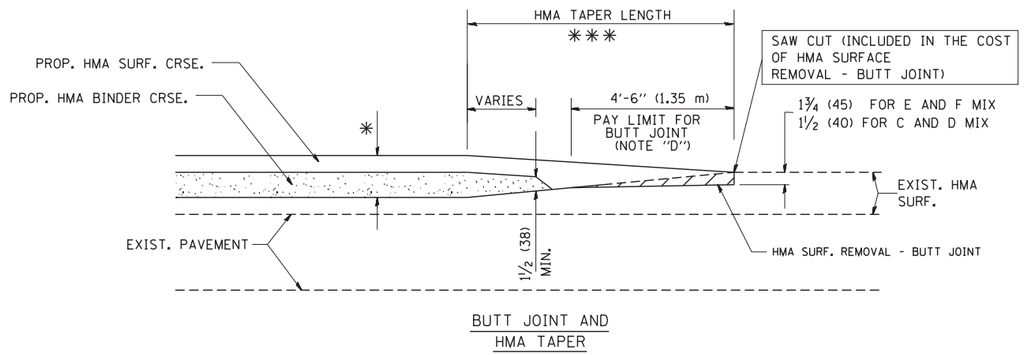


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

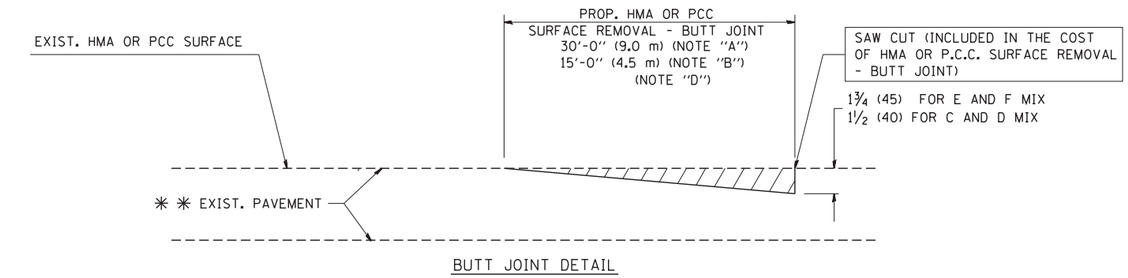
OPTION 2

TYPICAL TEMPORARY RAMP

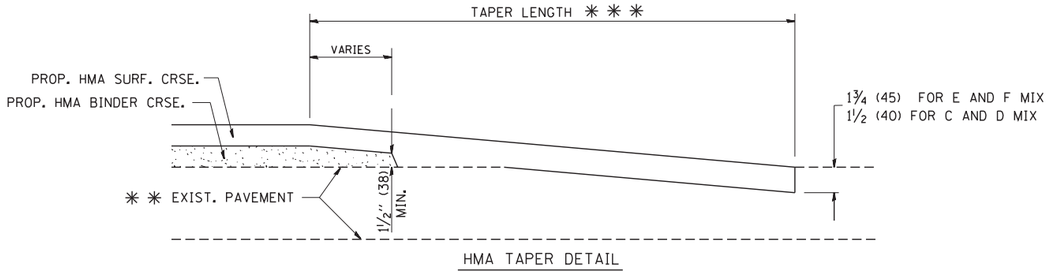


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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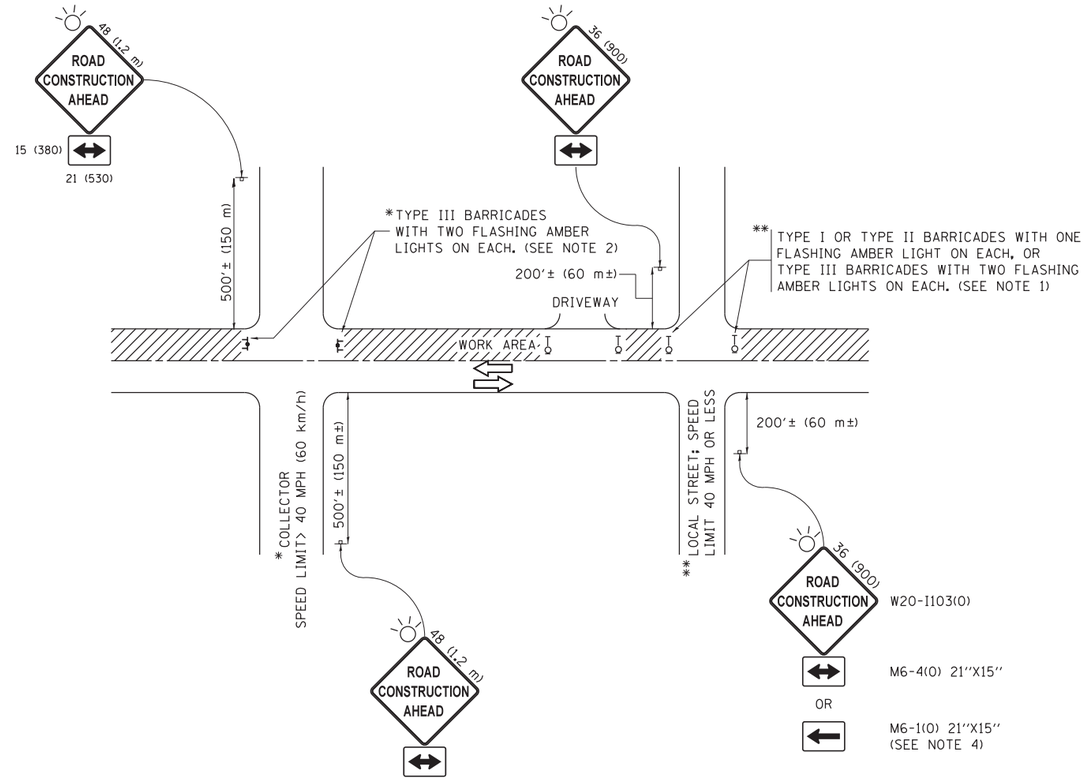


NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE ''ROAD CONSTRUCTION AHEAD'' SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

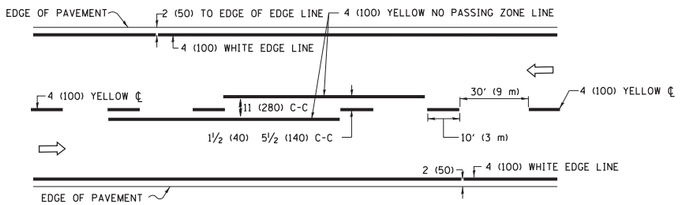
FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\DistStd\DRAWN\CADData\CADsheets\tc10.dgn	USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
			REVISED -T. RAMMACHER 01-06-00
	PLOT SCALE = 50.000 '/ in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
Default	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

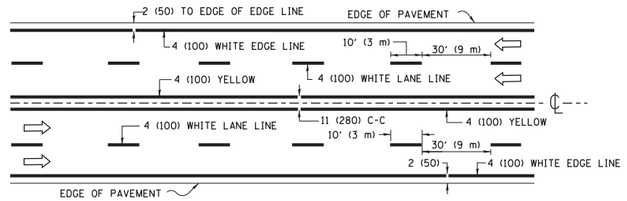
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

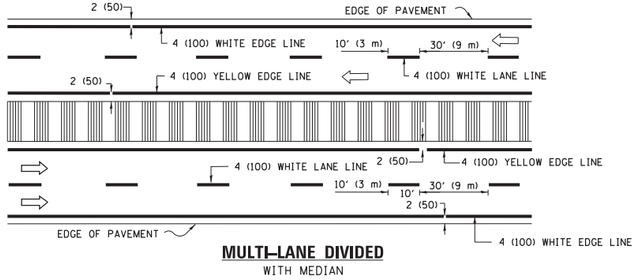
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	18
	TC-10	CONTRACT NO. 61F72		
	ILLINOIS FED. AID PROJECT			



2–LANE ROADWAY

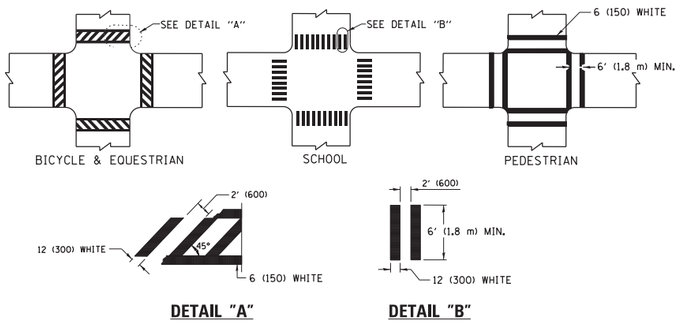


MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

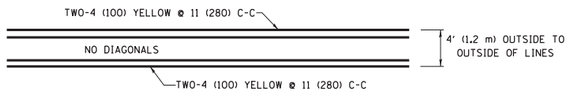
TYPICAL LANE AND EDGE LINE MARKING



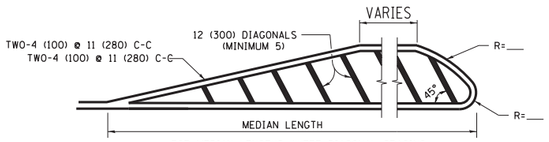
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



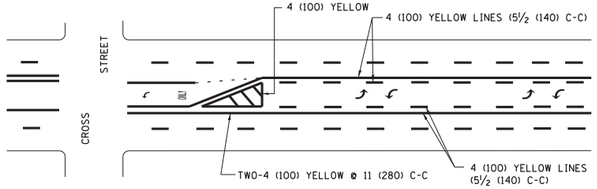
4' (1.2 m) WIDE MEDIANS ONLY



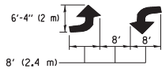
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

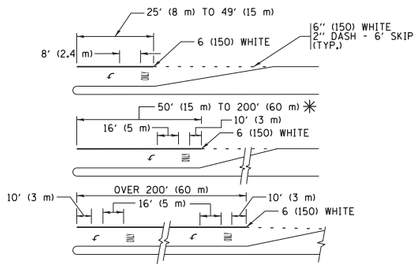


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

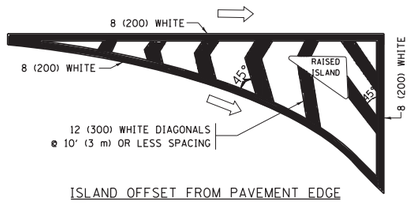


FULL SIZE LETTERS 8' (2,4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

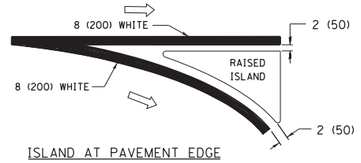
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

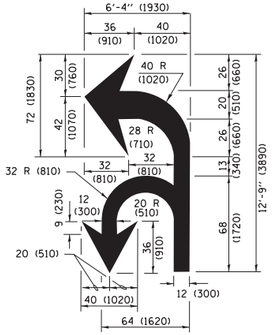


ISLAND OFFSET FROM PAVEMENT EDGE

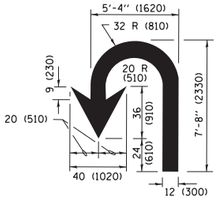


ISLAND AT PAVEMENT EDGE

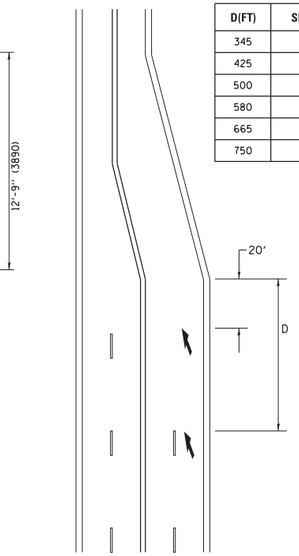
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⊠ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⊠ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⊠ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⊠ 6 (150) 12 (300) ⊠ 45° 12 (300) ⊠ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⊠ 4 (100) WITH 12 (300) DIAGONALS ⊠ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⊠ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ⊠ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\DistStds\WIP\CADData\CADsheets\tc13.dgn Default	USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
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	PLOT DATE = 4/13/2016	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
			REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	19
	TC–13	CONTRACT NO. 61F72		
		ILLINOIS FED. AID PROJECT		

tc13.dgn 4/13/2016 2:04:04 PM User=footemj

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

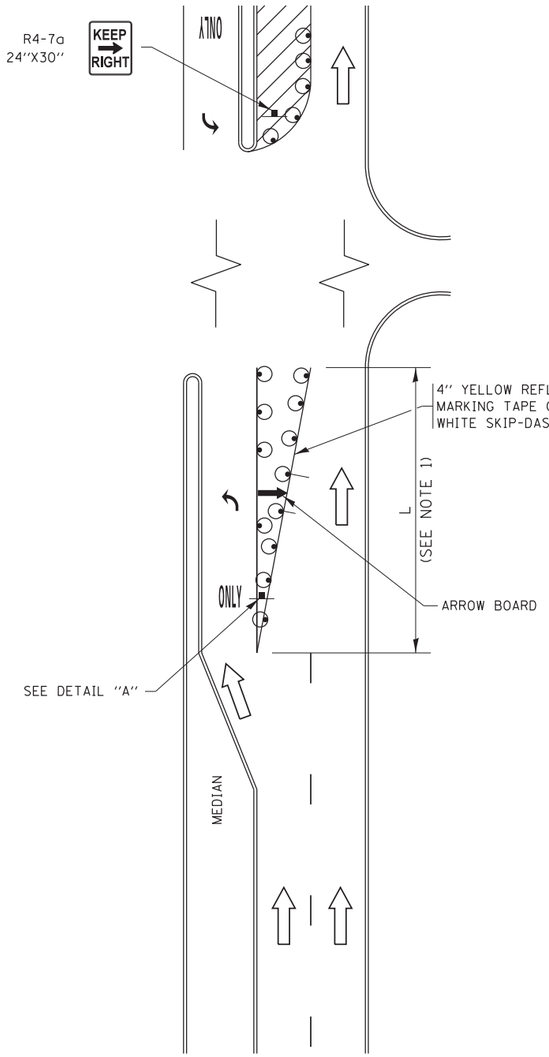


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I10OR 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

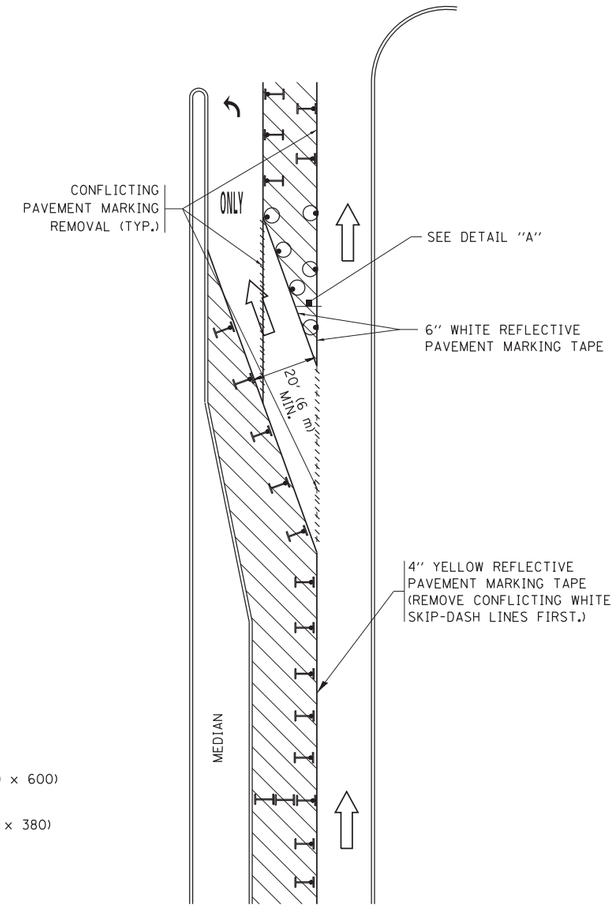
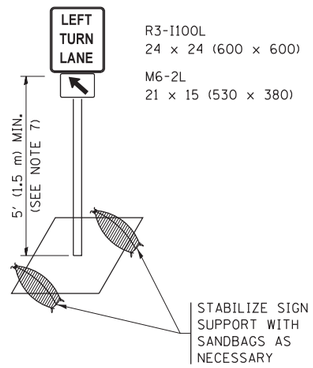


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 50.0000 ' / in.	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 07-01-13					
	PLOT DATE = 9/15/2016	REVISED -T. RAMMACHER 01-06-00	REVISED - A. SCHUETZE 09-15-16		SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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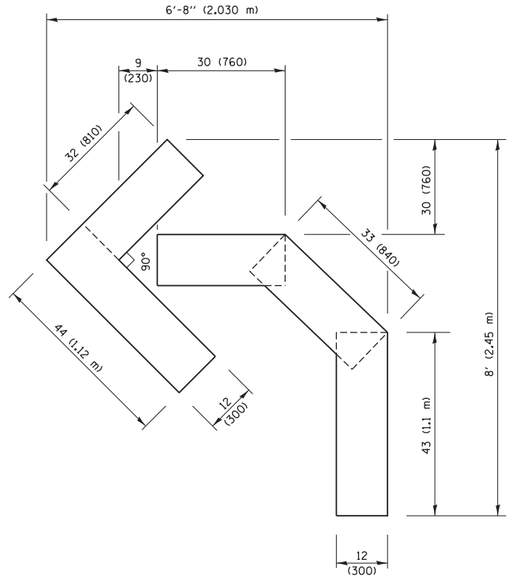
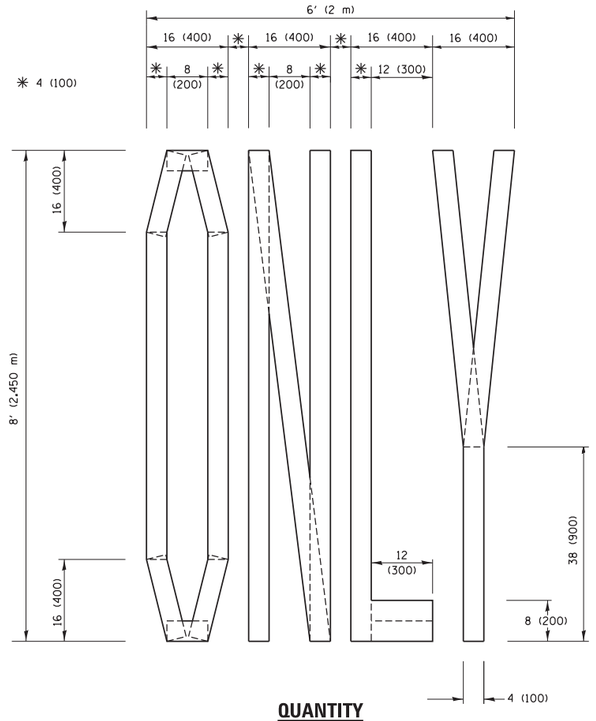
	TC-14	CONTRACT NO. 61F72		
	ILLINOIS FED. AID PROJECT			



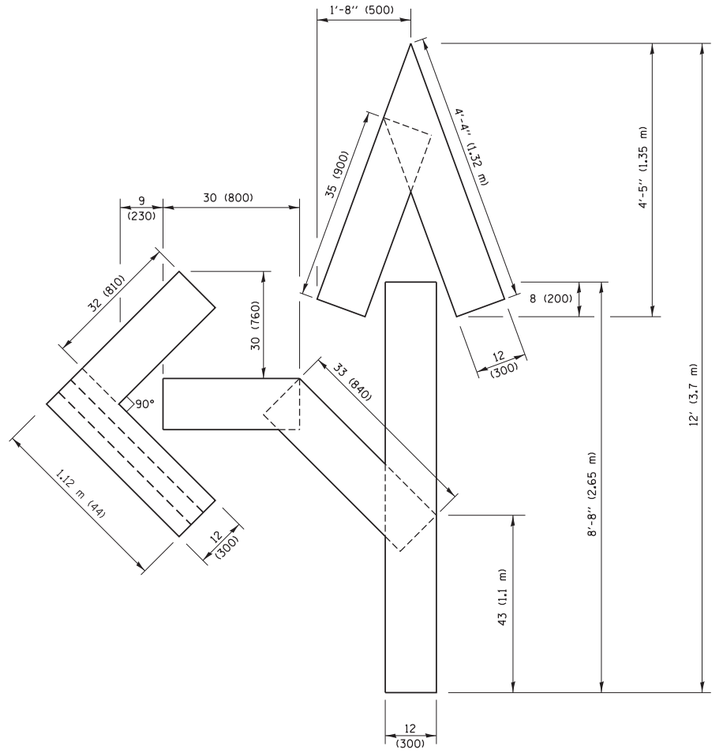
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

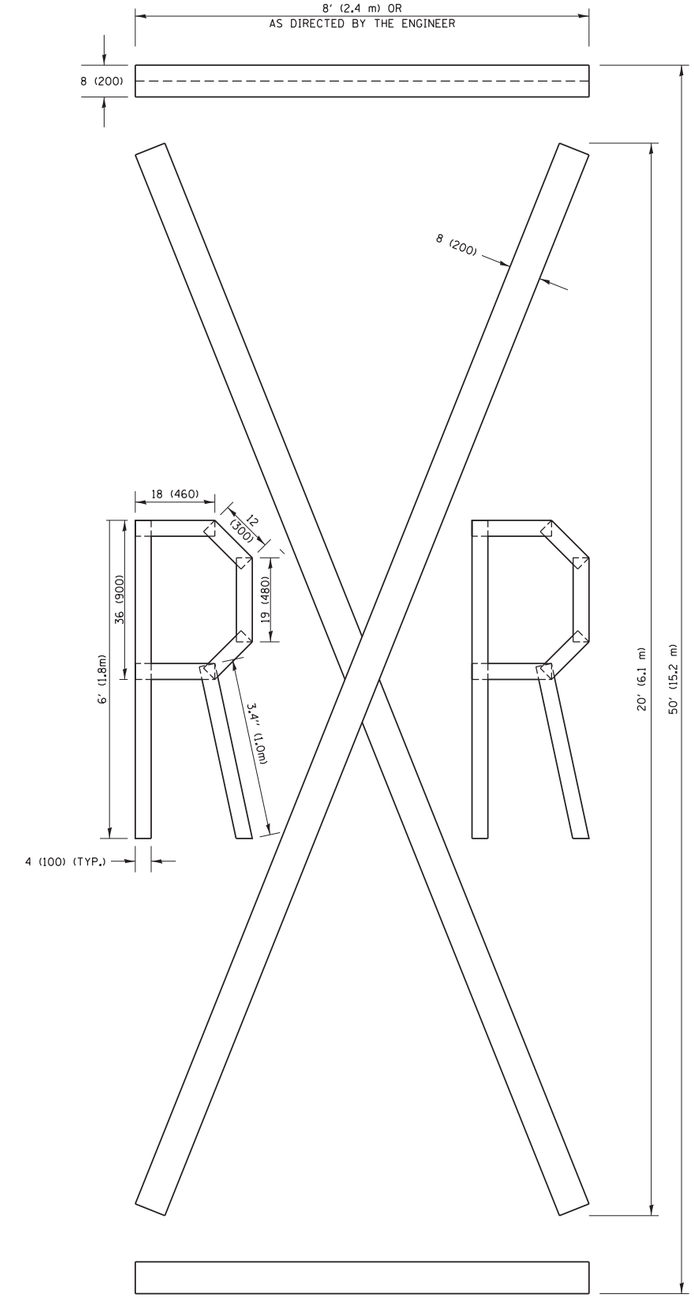


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = pw:\\IL084EBIDINTEG.illinois.gov:PWIDOT\Documents\IDOT Offices\District 1\Projects\Dist\DRAWN\CADData\CADsheets\tc16.dgn	USER NAME = footemj	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
			REVISED -E. GOMEZ 08-28-00
	PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -E. GOMEZ 08-28-00
	PLOT DATE = 9/15/2016	DATE - 09-18-94	REVISED -A. SCHUETZE 09-15-16

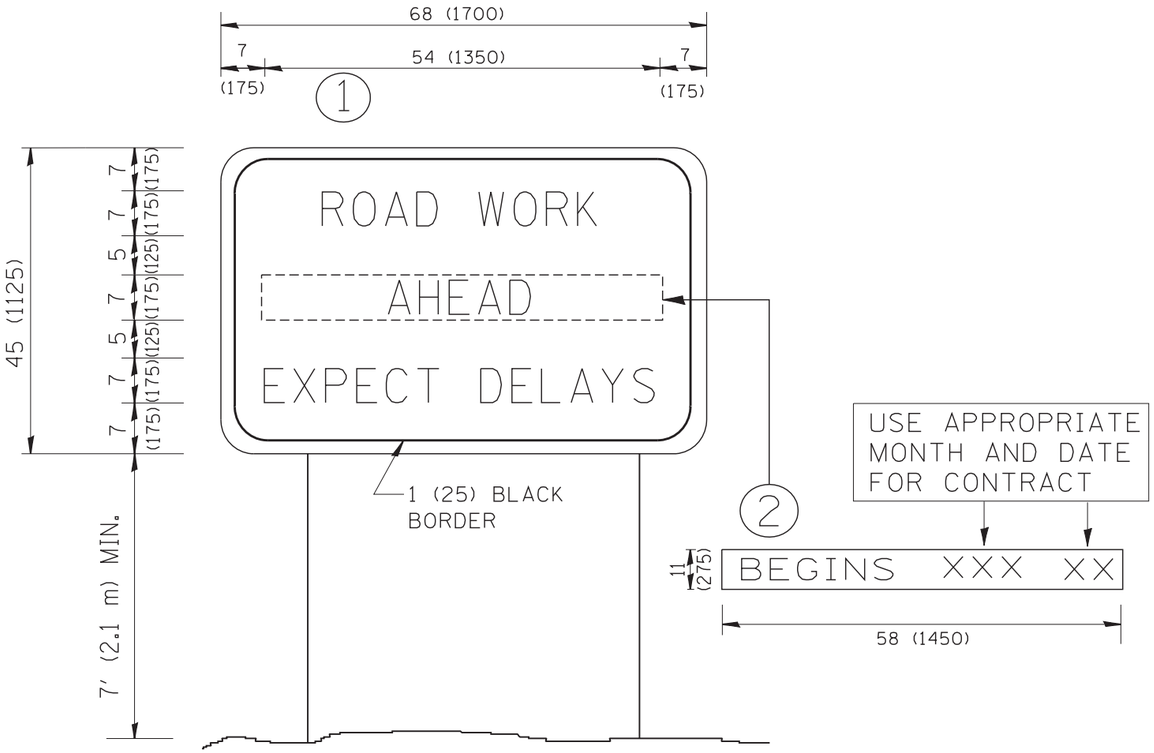
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	21
	TC-16	CONTRACT NO. 61F72		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

tc16.dgn 9/15/2016 12:45:20 PM User=footemj



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gaglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

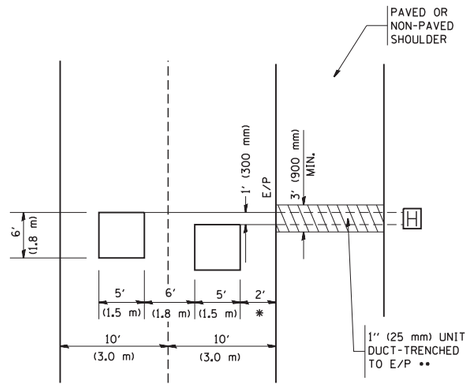
ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	22
	TC-22	CONTRACT NO. 61F72		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

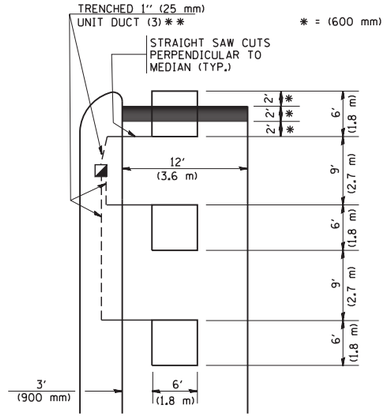


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

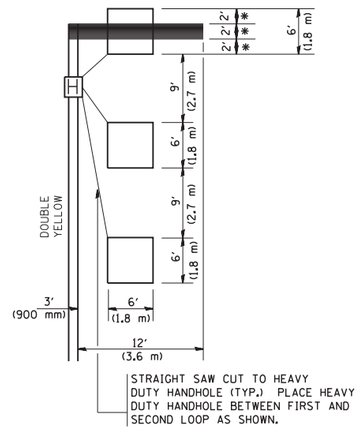


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

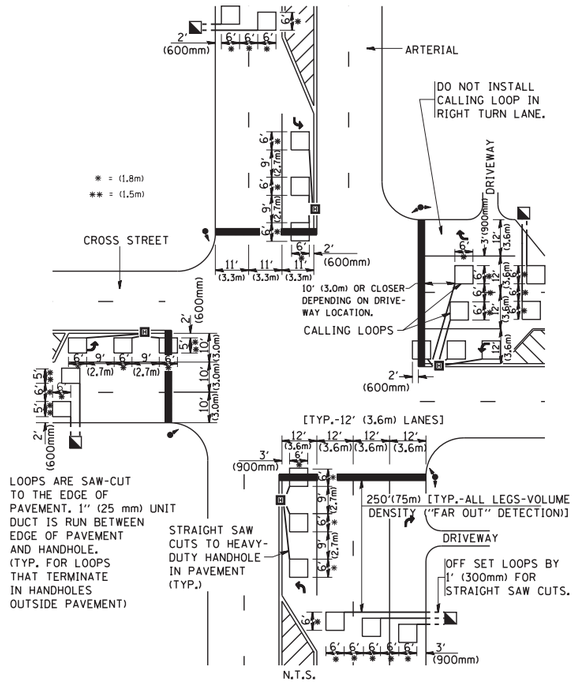
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)



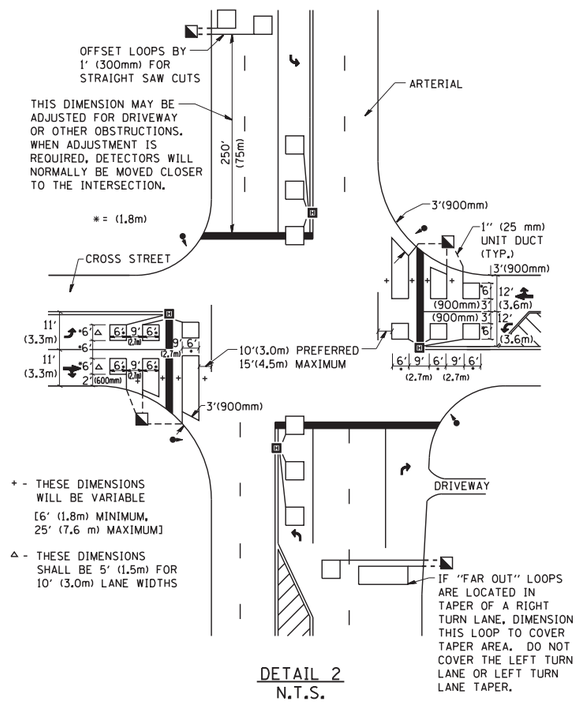
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION) CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/4/2008	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2588	18-00080-00-RS	DU PAGE	23	23
	TS-07	CONTRACT NO. 61F72		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		