F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	1
	ILLINOIS	CONTRACT NO. 61F46		

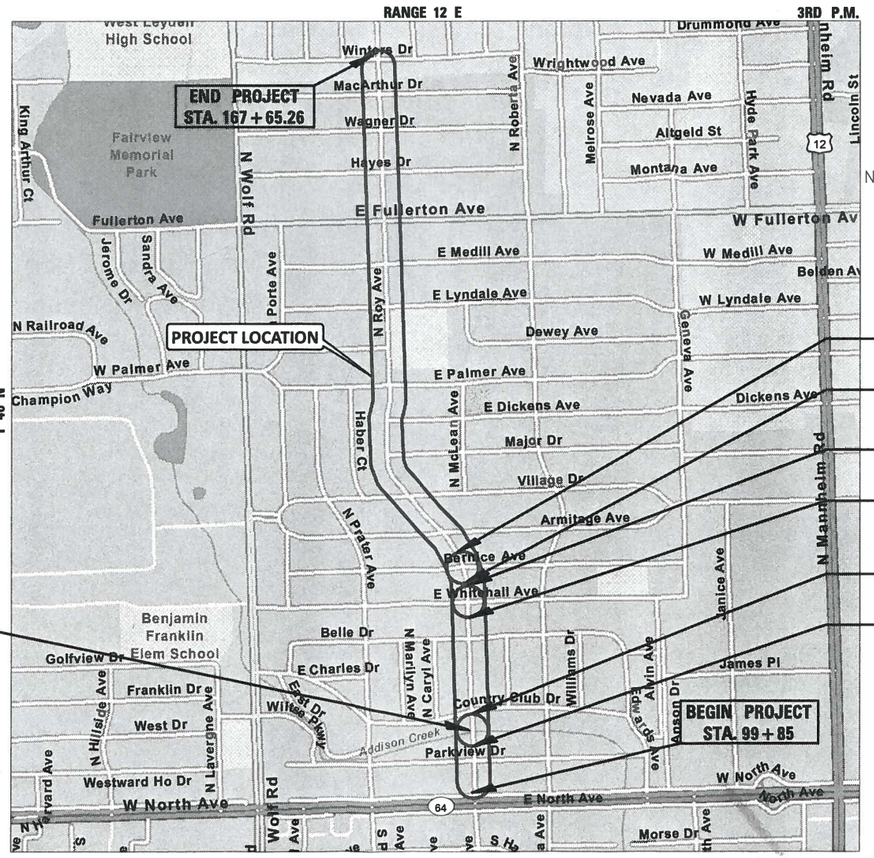
04-26-2019 LETTING ITEM 108

FOR INDEX OF SHEETS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

F.A.U. 2539 (ROY AVENUE)
NORTH AVENUE (F.A.U. 307) TO WINTERS DRIVE (F.A.U. 2910)
RESURFACING
Section No.: 18–00093–00–RS
Project No. 18H2(332)
CITY OF NORTHLAKE
COOK COUNTY
JOB NO.: C–91–233–19



LEYDEN TOWNSHIP

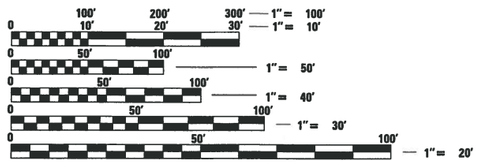
LOCATION MAP
N.T.S.

GROSS LENGTH OF PROJECT = 6,985 LINEAL FEET (1.32 MI.)
NET LENGTH OF PROJECT = 6,756 LINEAL FEET (1.28 MI.)

LOCATION OF SECTION INDICATED THUS: –

DESIGN DESIGNATION ROUTE : MINOR COLLECTOR
DESIGN SPEED = 25 M.P.H.
POSTED SPEED = 25 M.P.H.

TRAFFIC DATA: 2014
ADT = 1,225



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

ENGINEER: CARMEN E. RAMOS SCHAUMBURG, IL

FEDERAL AID PROGRAM

CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PROFESSIONAL DESIGN FIRM NO.: 184-001175
EXPIRATION DATE: APRIL 30, 2018

CONTRACT NO. 61F46

JOHN A. LAPAGLIA 062-070592 LICENSED PROFESSIONAL ENGINEER OF ILLINOIS

ENGINEER — DATE 11/26/18

JOHN A. LAPAGLIA
ILLINOIS REGISTRATION No. 062-070592
EXPIRATION DATE: 11/30/2019

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED
JEFFREY T. SHEEHAN
CITY OF NORTHLAKE, MAYOR

PASSED 2/14/19
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID BASED ON LIMITED REVIEW
FEBRUARY 15, 2019
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

HIGHWAY STANDARDS

000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C & D PATCHES
604001-04	FRAME AND LIDS TYPE 1
606001-07	CONC. CURB TYPE B AND COMB. CONC CURB AND GUTTER
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701301-04	LANE CLOSURES, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2019; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JUNE 2014 SEVENTH EDITION; THE "ILLINOIS URBAN MANUAL " AND THE "ILLINOIS URBAN MANUAL FIELD MANUAL FOR INSPECTION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES"; THE "AMERICANS WITH DISABILITIES ACT OF 1990 ACCESSIBILITY GUIDELINES"; THE "DRAFT" REHABILITATION ACT OF 1973 (SECTION 504); THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES; THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.

CODES OF THE IEPA TITLE 35, AND O.S.H.A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE ADHERENCE TO THESE (NOT THE CITY OF NORTHLAKE OR CHRISTOPHER B. BURKE ENGINEERING).

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHEETING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE AGENCY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE CITY DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE CITY.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATION IS REQUIRED.)

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT WATER DEPARTMENT FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT.

MISCELLANEOUS

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED IN REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

AGGREGATE BASE REPAIR, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLAN QUANTITY.

THE THICKNESS OF EXISTING PAVEMENT TO BE REMOVED HAS BEEN ESTIMATED FROM PAVEMENT CORES. THE PAVEMENT CORING GEOTECHNICAL REPORT IS ATTACHED TO THE PROJECT SPECIFICATIONS. THE CITY DOES NOT GUARANTEE ITS ACCURACY. NO ADJUSTMENTS TO PLAN QUANTITIES SHALL BE MADE FOR VARIATIONS IN PAVEMENT THICKNESS.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO RESIDENTIAL DRIVEWAYS AT ALL TIMES. THIS WORK SHALL BE COMPLETED AND PAID FOR IN ACCORDANCE WITH THE SPECIAL PROVISION FOR "AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS". AGGREGATE RAMPS SHALL BE CONSTRUCTED FROM THE EXISTING ROADWAY AGGREGATE BASE TO THE EXISTING CURB AND GUTTER. THE GRADE OF THE AGGREGATE RAMPS SHALL NOT EXCEED 12%.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

CURB AND GUTTER TO BE REMOVED AT DRIVEWAYS SHALL BE REPLACED WITH DEPRESSED CURB AND GUTTER.

PAVING

HOT-MIX ASPHALT BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED. THE THICKNESSES SHOWN ON THE PLANS ARE THE MINIMUM ACCEPTABLE THICKNESSES.

LANDSCAPING

CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER.

THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR.

CONTRACTOR SHALL BE CAUTIOUS NOT TO DISTURB ANY PARKWAY TREES DURING CONSTRUCTION. ANY TREES THAT ARE DAMAGED OR DISTURBED DURING THE CONSTRUCTION SHALL BE REPLACED IN KINDS BY THE CONTRACTOR.

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	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE
ROY AVENUE
GENERAL NOTES

SCALE: NONE | SHEET NO. OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	2
		CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			

				STP - LAFO 80% FED. 20% LOCAL
				ROY AVENUE
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE
	CODE NO.	ITEM	UNIT	TOTAL QUANTITY
	20101200	TREE ROOT PRUNING	EACH	5
	20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	675
	28000510	INLET FILTERS	EACH	51
	31101100	SUBBASE GRANULAR MATERIAL, TYPE B	CU YD	525
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	15,600
	40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	3,300
	40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	1,980
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	14,600
	42400400	PORTLAND CEMENT CONCRETE SIDEWALK 7 INCH	SQ FT	1,700
*	42400800	DETECTABLE WARNINGS	SQ FT	1,000
	44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	23,000
	44000600	SIDEWALK REMOVAL	SQ FT	16,300
	44201701	CLASS D PATCHES, TYPE I, 5 INCH	SQ YD	1,437
	44201705	CLASS D PATCHES, TYPE II, 5 INCH	SQ YD	1,437
	44201709	CLASS D PATCHES, TYPE III, 5 INCH	SQ YD	1,437
	44201711	CLASS D PATCHES, TYPE IV, 5 INCH	SQ YD	1,437
	44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	23,000
	60253000	CATCH BASINS TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	8
	60258200	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1
	60262900	INLETS TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1
	67100100	MOBILIZATION	L SUM	1
*	70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1
*	70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
	70300100	SHORT TERM PAVEMENT MARKING	FOOT	5,200
	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2,600
**	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3,700
**	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	600
**	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	900
** *	88600600	DETECTOR LOOP REPLACEMENT	FOOT	125
*	X0326862	STRUCTURES TO BE ADJUSTED	EACH	68
*	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	70
*	X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	47
*	X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1
*	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	200
*	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	3,200
*	XX003435	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT REMOVAL AND REPLACEMENT	SQ YD	130
*	XX006947	HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	SQ YD	150

* INDICATES SPECIAL PROVISION
** INDICATES SPECIALTY ITEM

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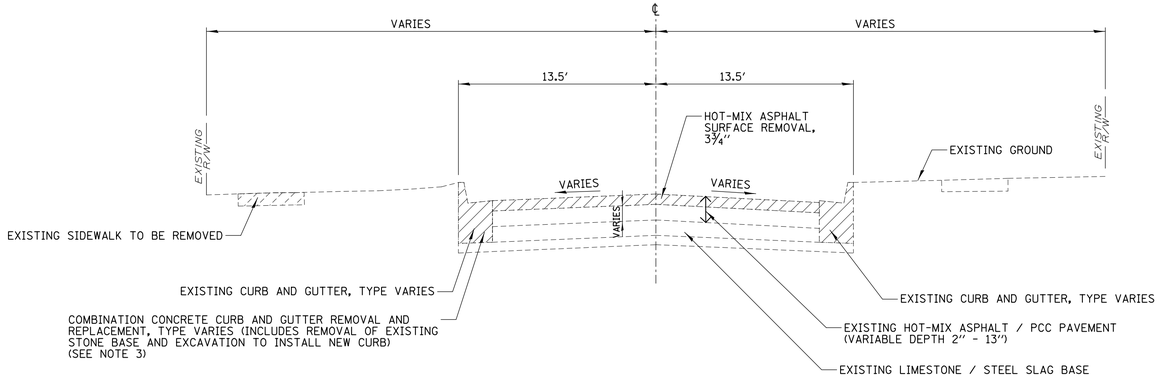
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE
ROY AVENUE
SUMMARY OF QUANTITIES

SCALE: NONE | SHEET NO. OF 1 SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	3
		CONTRACT NO. 61F46		
	ILLINOIS	FED. AID PROJECT		

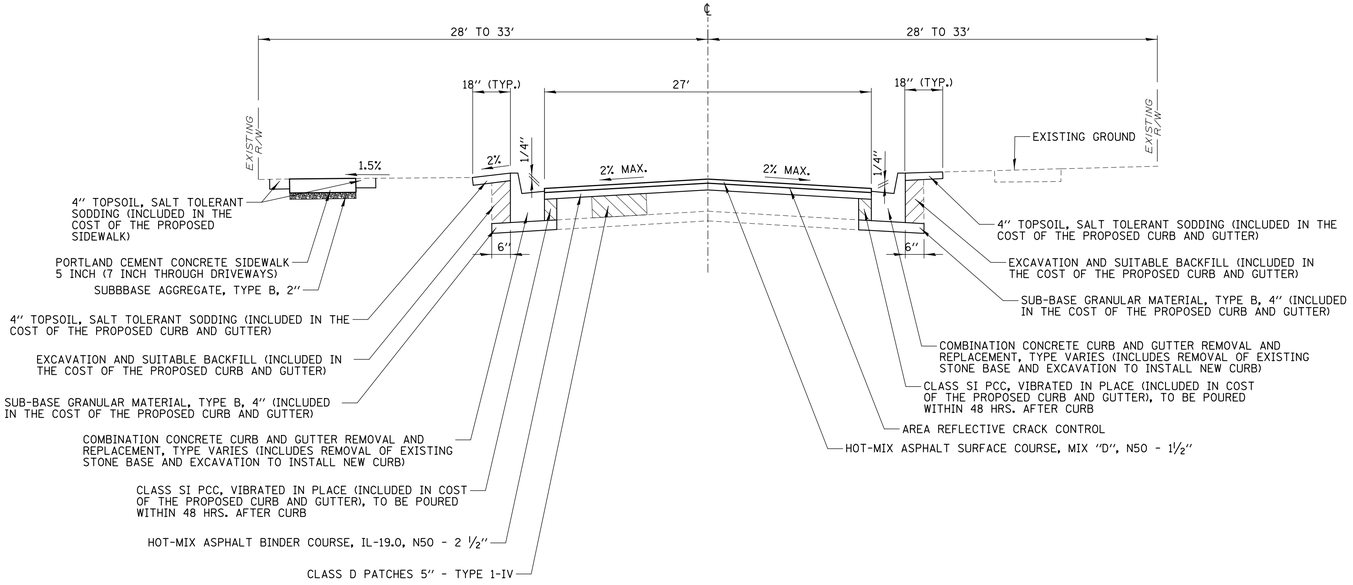


EXISTING TYPICAL SECTION

STA. 100+38.60 TO STA. 167+65.25, ROY AVENUE

NOTES:

1. ADDITIONAL STONE BACK FILLING SHALL BE INCLUDED IN THE COST FOR COMBINATION CONCRETE CURB AND GUTTER.
2. CONTRACTOR SHALL SAWCUT PAVEMENT PRIOR TO REMOVING THE CURB.
3. PAVEMENT REMOVAL ADJACENT TO PROPOSED CURB AND GUTTER (INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER).
4. ALL WORK INCLUDING LANDSCAPE RESTORATION MUST BE COMPLETED AND APPROVED BY ENGINEER PRIOR TO FINAL PAYOUT.
5. CONTRACTOR SHALL MILL PRIOR TO PATCHING.
6. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR PETROMAT (FABRIC) ENCOUNTER DURING GRINDING OPERATIONS. REMOVAL OF PETROMAT SHALL BE CONSIDERED INCIDENTAL TO HOT-MIX ASPHALT SURFACE REMOVAL.
7. ALL LANDSCAPE RESTORATION (4″ TOPSOIL, SALT TOLERANT SODDING) REQUIRED SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
8. TOTAL HOT-MIX ASPHALT TO BE REMOVED (3 ¾″) & REPLACED (4″).



PROPOSED TYPICAL SECTION
GRIND AND OVERLAY

STA. 100+38.60 TO STA. 167+65.25, ROY AVENUE
OMISSION: STA. 120+26–STA. 121+12, ROY AVENUE
OMISSION: STA. 117+01–STA. 117+87, ROY AVENUE
OMISSION: STA. 105+57–STA. 106+14, ROY AVENUE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
ROADWAY ITEM	AIR VOIDS @Ndes
RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX ″D″, N50 (IL-9.5mm) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	4% @ 50 GYR. 4% @ 50 GYR.
CLASS D PATCH (HMA BINDER IL-19mm)	4% @ 50 GYR.
HOT-MIX ASPHALT DRIVEWAY REMOVAL AND REPLACEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX ″D″, N50 (IL-9.5mm)	4% @ 50 GYR.

NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
2. FOR USE OF RECYCLED MATERIAL SEE SPECIAL PROVISIONS.
3. FOR NON-POLYMERIZED HMA THE ″AC TYPE″ SHALL BE ″PG 64-22″ UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
4. PAVEMENT PATCHING TO BE PERFORMED AFTER ROADWAY IS MILLED.

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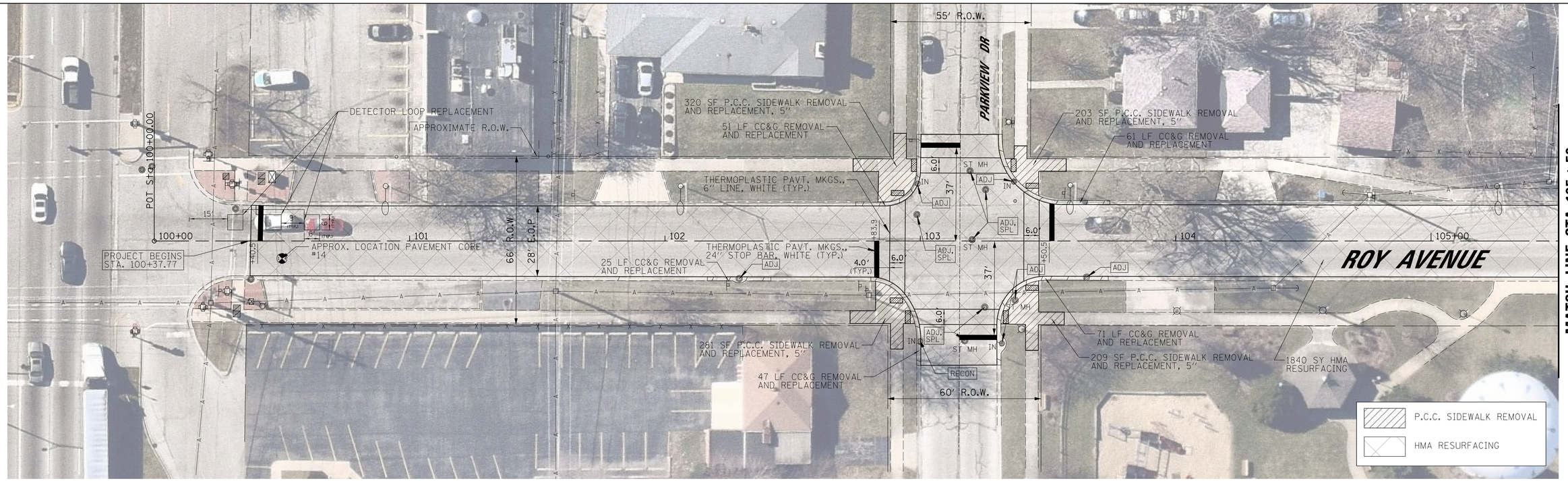
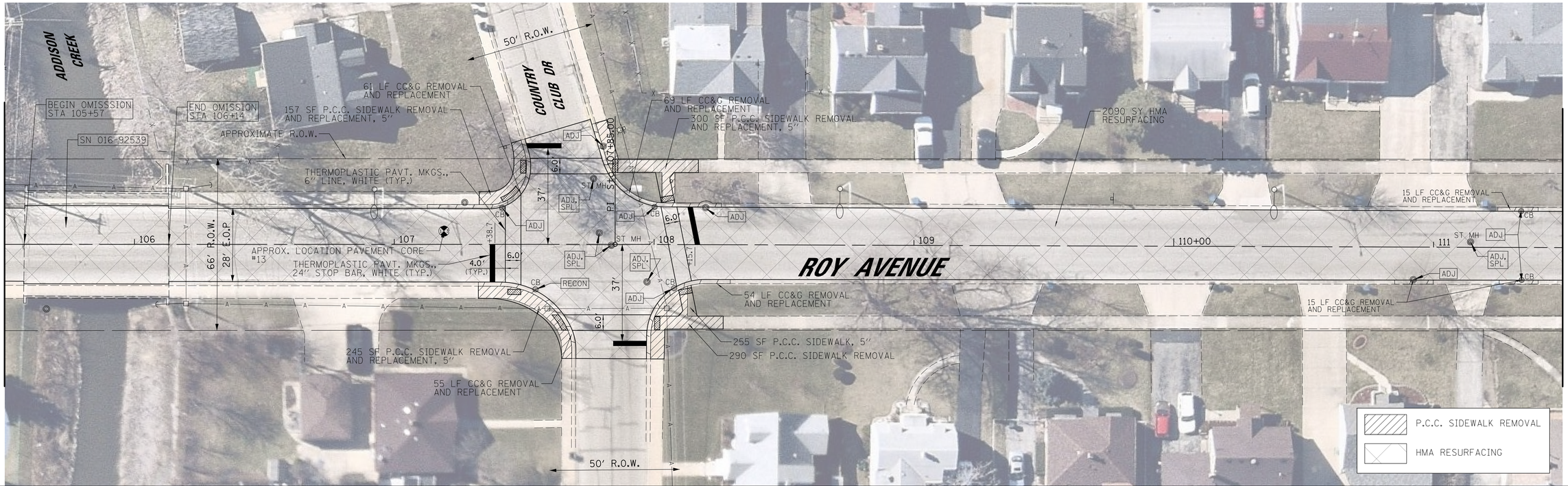
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE
ROY AVENUE
TYPICAL SECTIONS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-RS	COOK	22	4
		CONTRACT NO. 61F46		
		ILLINOIS FED. AID PROJECT		





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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE
ROY AVENUE
ROADWAY PLAN

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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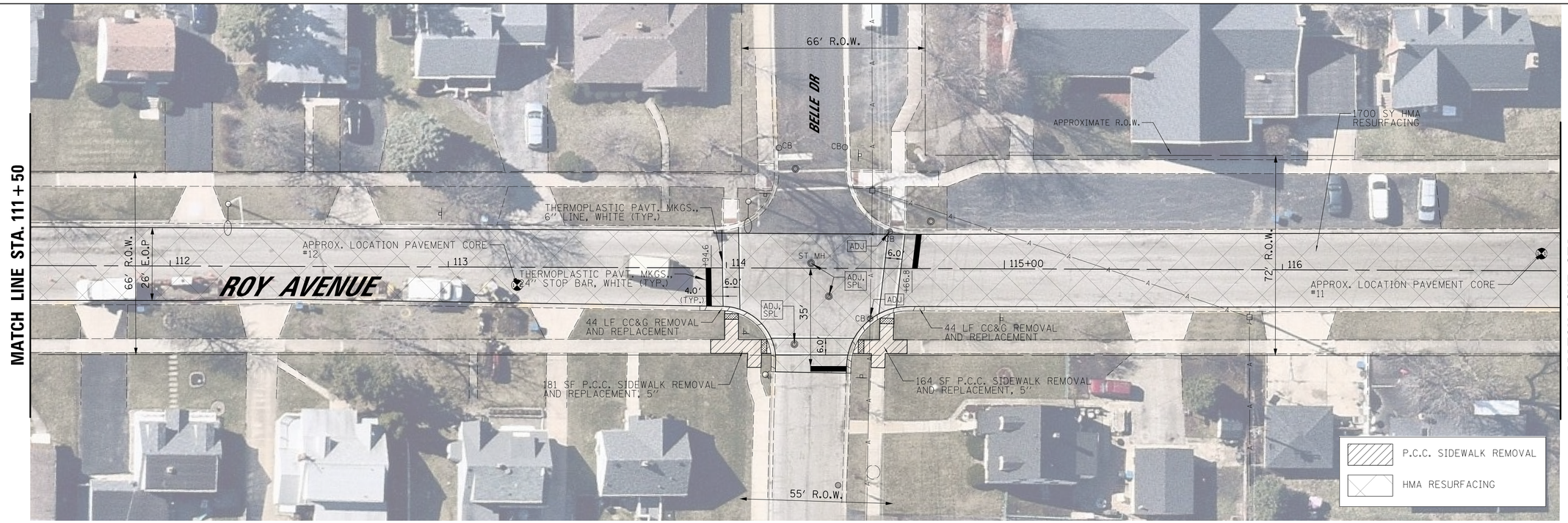
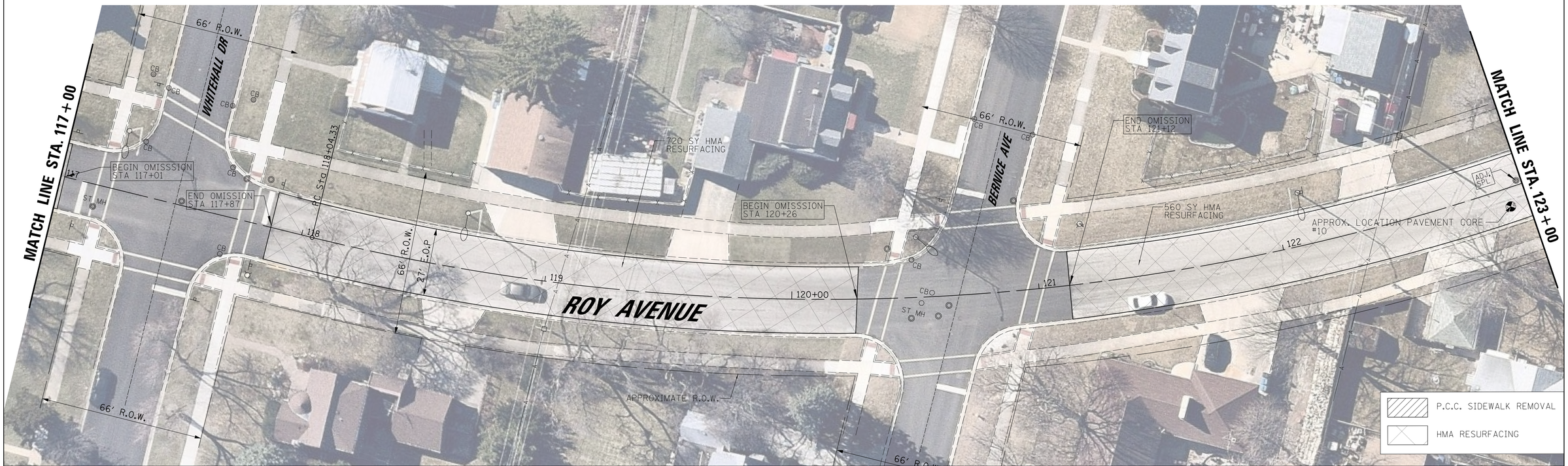
		CONTRACT NO. 61F46		
		ILLINOIS FED. AID PROJECT		





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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE
ROY AVENUE
ROADWAY PLAN

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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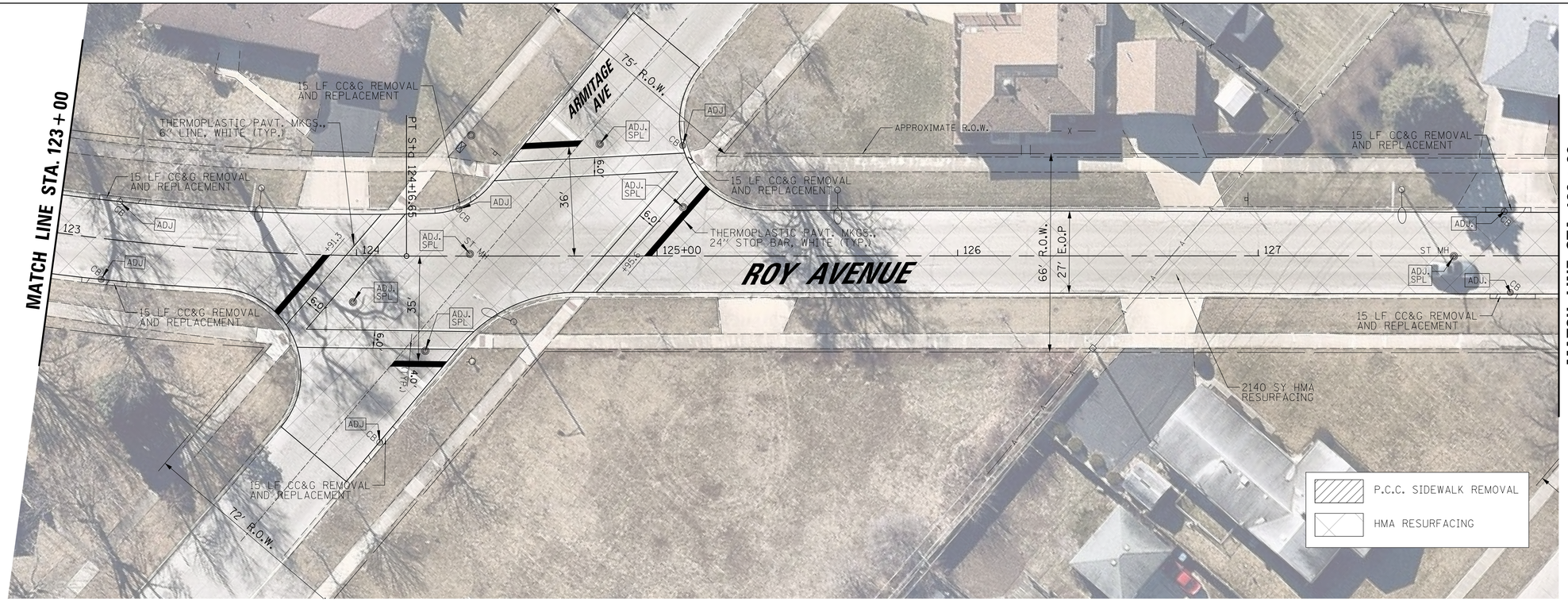
		CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			



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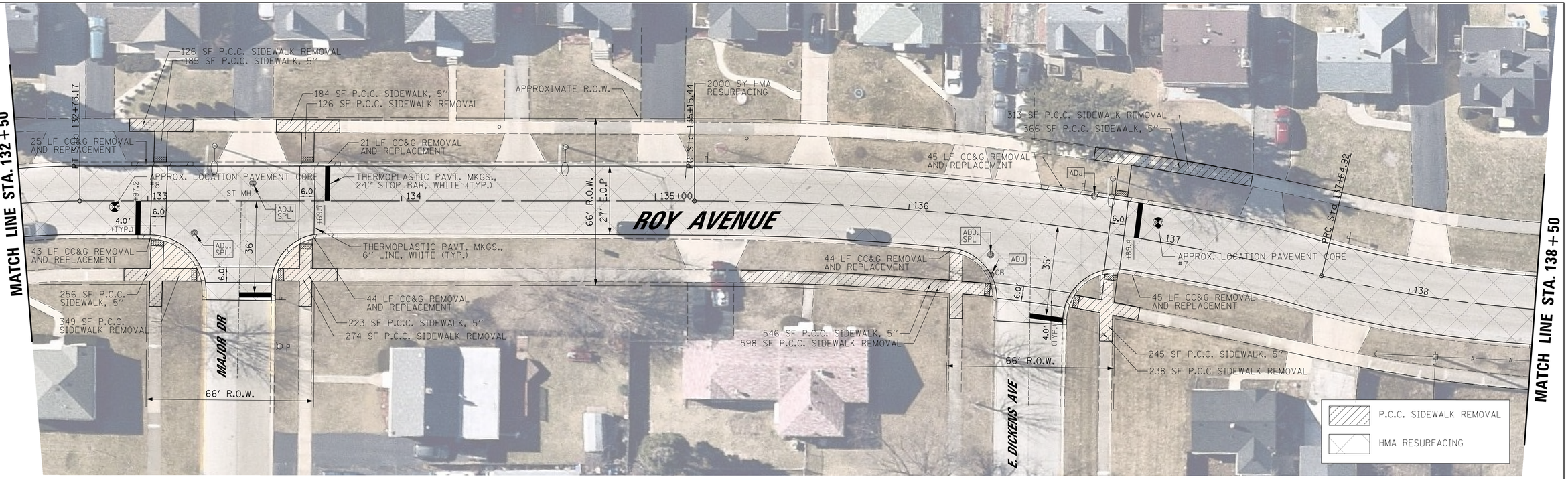
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

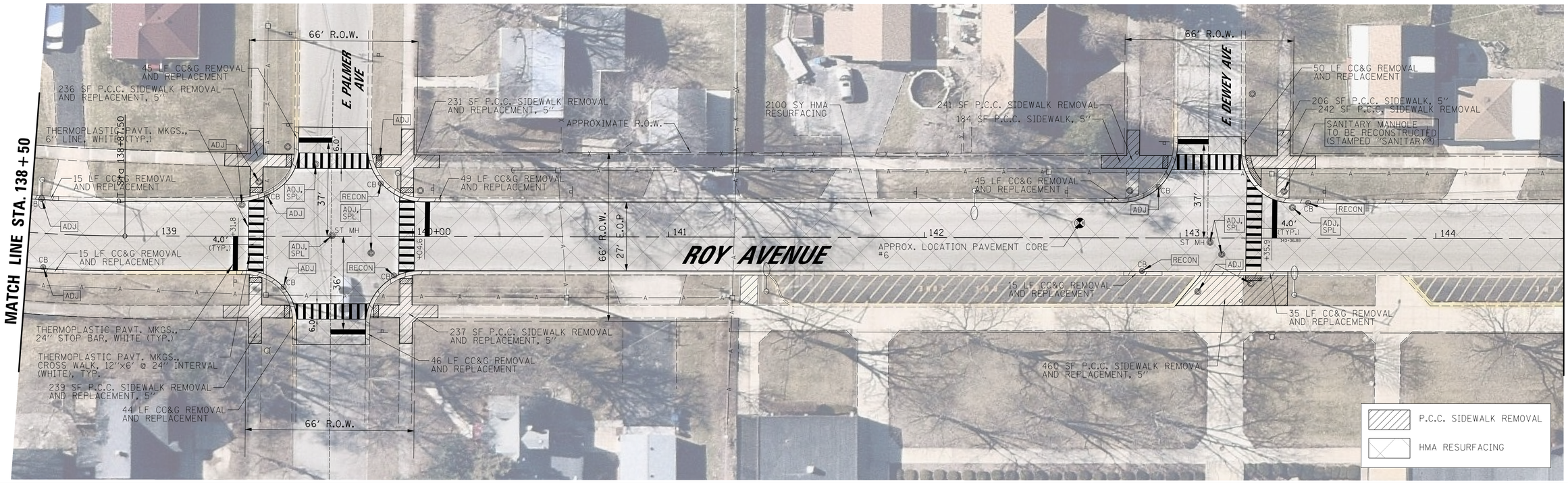
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE
ROY AVENUE
ROADWAY PLAN

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-RS	COOK	22	7
		CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			





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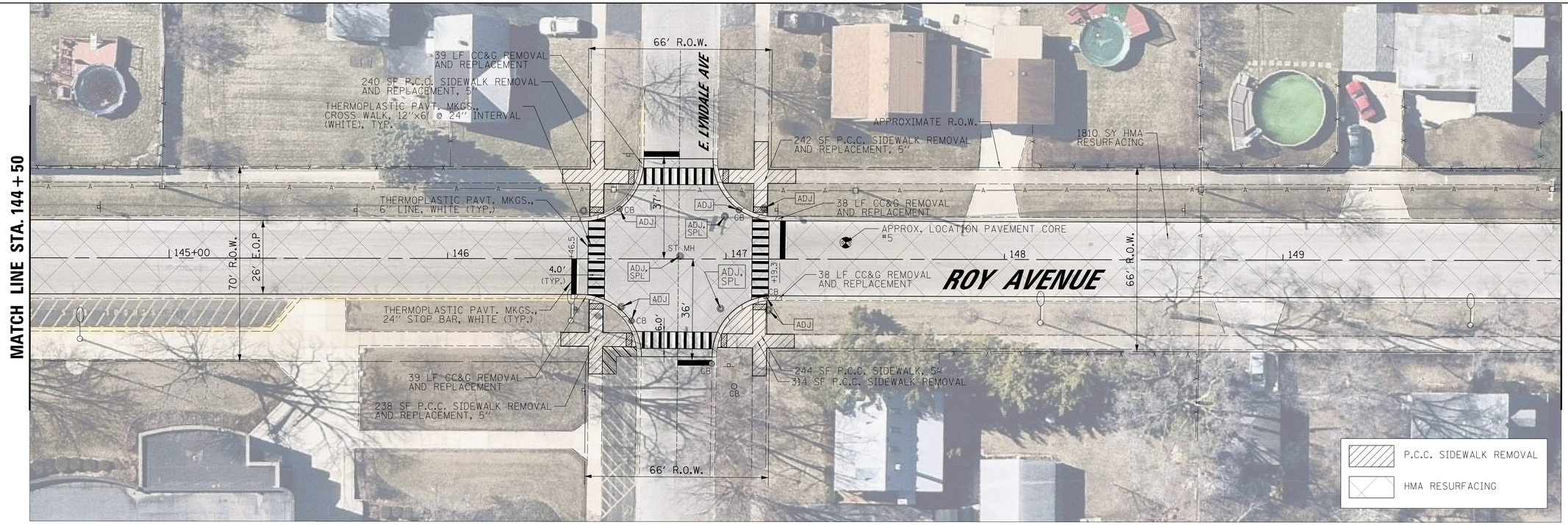
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

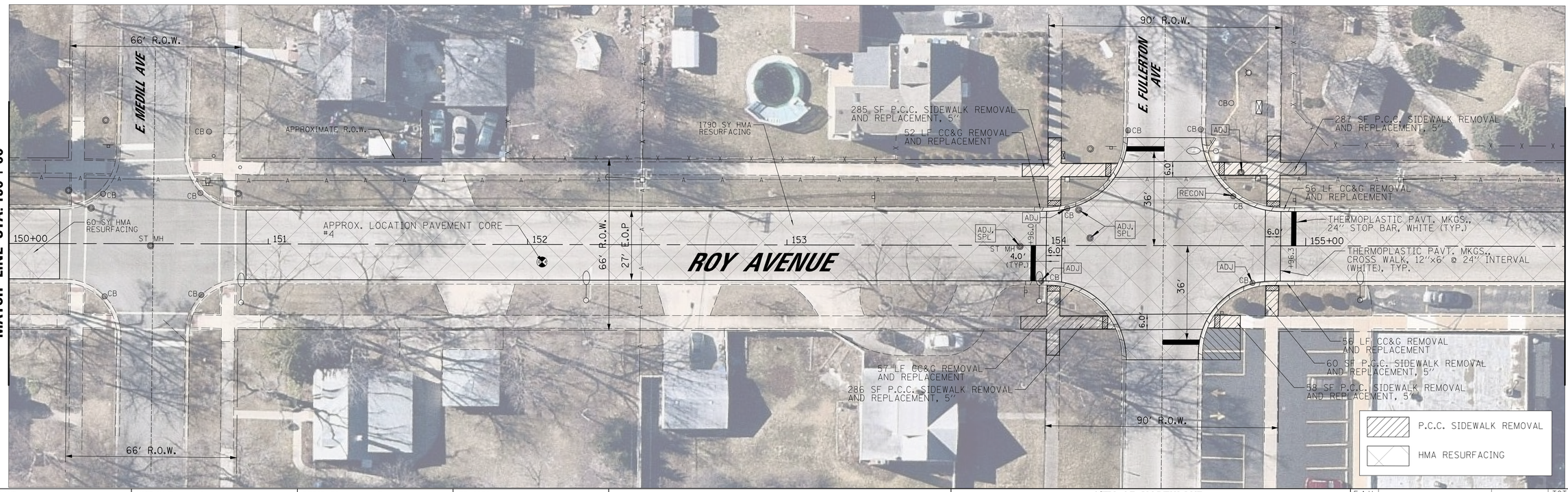
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE
ROY AVENUE
ROADWAY PLAN

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-RS	COOK	22	8
		CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			





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STATE OF ILLINOIS
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CITY OF NORTHLAKE
ROY AVENUE
ROADWAY PLAN

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

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2539	18-00093-RS	COOK	22	9
		CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			

MATCH LINE STA. 156+00

MATCH LINE STA. 162+00

66' R.O.W.

HAYES DR

244 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

46 LF CC&G REMOVAL AND REPLACEMENT

246 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

APPROXIMATE R.O.W.

2000 SY HMA RESURFACING

PI Sta 156+77.23

15 LF CC&G REMOVAL AND REPLACEMENT

THERMOPLASTIC PAVT. MKGS., 6" LINE, WHITE (TYP.)

47 LF CC&G REMOVAL AND REPLACEMENT

15 LF CC&G REMOVAL AND REPLACEMENT

APPROX. LOCATION PAVEMENT CORE #3

66' R.O.W.

27' E.O.P.

ROY AVENUE

15 LF CC&G REMOVAL AND REPLACEMENT

THERMOPLASTIC PAVT. MKGS., 24" STOP BAR, WHITE (TYP.)

15 LF CC&G REMOVAL AND REPLACEMENT

45 LF CC&G REMOVAL AND REPLACEMENT

244 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

47 LF CC&G REMOVAL AND REPLACEMENT

247 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

100 SF P.C.C. SIDEWALK R&R, 7'

66' R.O.W.

P.C.C. SIDEWALK REMOVAL

HMA RESURFACING

60' R.O.W.

WAGNER DR

237 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

47 LF CC&G REMOVAL AND REPLACEMENT

189 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

46 LF CC&G REMOVAL AND REPLACEMENT

2100 SY HMA RESURFACING

223 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

APPROXIMATE R.O.W.

60' R.O.W.

MACARTHUR DR

227 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

APPROX. LOCATION PAVEMENT CORE #2

THERMOPLASTIC PAVT. MKGS., 24" STOP BAR, WHITE (TYP.)

15 LF CC&G REMOVAL AND REPLACEMENT

44 LF CC&G REMOVAL AND REPLACEMENT

44 LF CC&G REMOVAL AND REPLACEMENT

APPROX. LOCATION PAVEMENT CORE #1

POT Sta 168+00.00

66' R.O.W.

27' E.O.P.

ROY AVENUE

THERMOPLASTIC PAVT. MKGS., 6" LINE, WHITE (TYP.)

15 LF CC&G REMOVAL AND REPLACEMENT

43 LF CC&G REMOVAL AND REPLACEMENT

49 LF CC&G REMOVAL AND REPLACEMENT

PROJECT ENDS STA. 167+65.26

199 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

38 LF CC&G REMOVAL AND REPLACEMENT

38 LF CC&G REMOVAL AND REPLACEMENT

219 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

222 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

227 SF P.C.C. SIDEWALK REMOVAL AND REPLACEMENT, 5"

60' R.O.W.

60' R.O.W.

P.C.C. SIDEWALK REMOVAL

HMA RESURFACING

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	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE
ROY AVENUE
ROADWAY PLAN

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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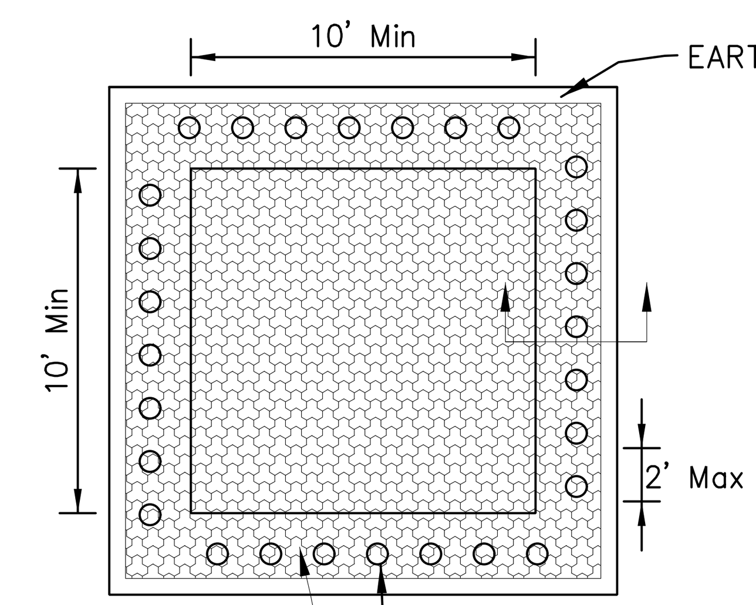
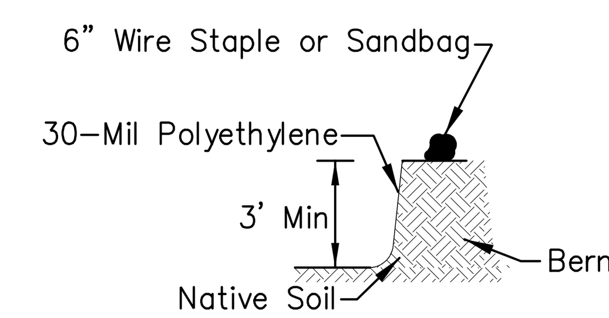
		CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			

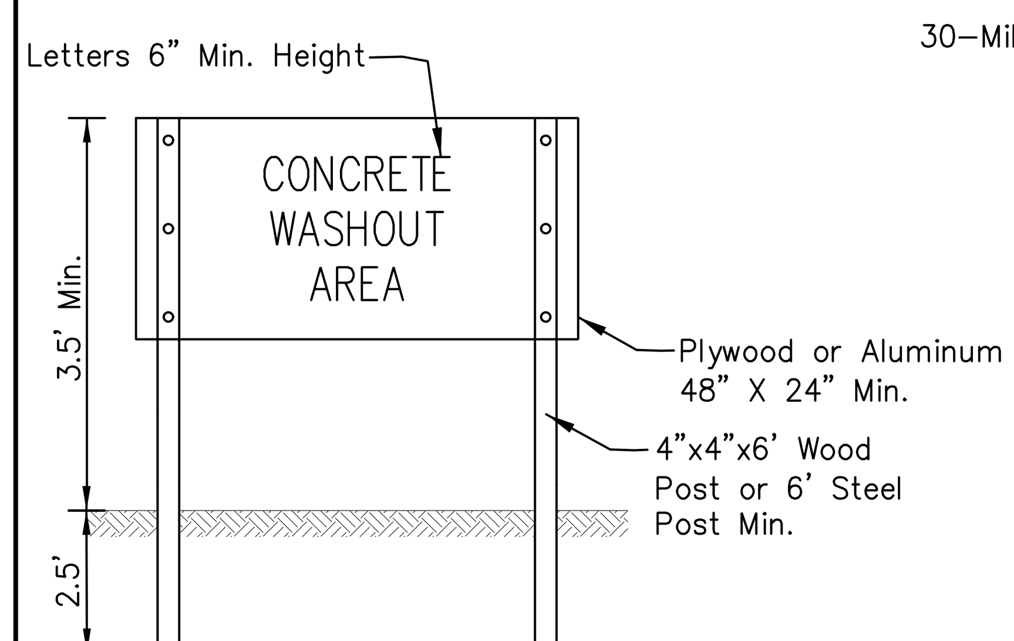


PLAN VIEW

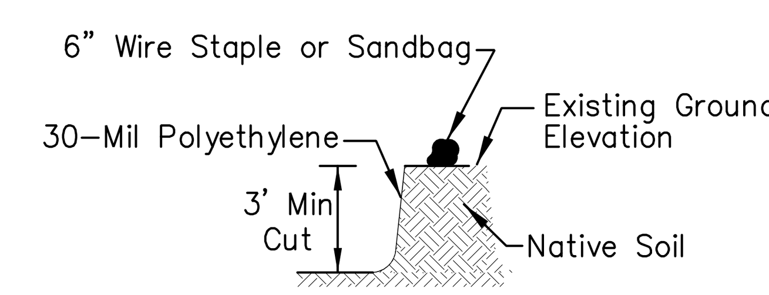


Sandbag Anchor

EARTHEN BERM ANCHOR SECTIONS



SIGN DETAIL



Sandbag Anchor

SUBGRADE ANCHOR SECTIONS

NOTES:

1. Maintaining temporary concrete washout facilities shall include removing and disposing of hardend concrete and/or slurry and returning the facilities to a functional condition.

2. Facility shall be cleaned or reconstructed in a new area once washout becomes two-thirds full.

Sheet 2 of 3 / Drawing No. / File No. IUM-654ET	TEMPORARY CONCRETE WASHOUT FACILITY – EARTHEN TYPE	Designed / Drawn B. JOHNSON, Date 6/08 / Checked / Approved

FILE NAME = …\Civil\DET_1_940032HR286.sht	USER NAME = jdefrenza	DESIGNED - VMR	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CITY OF NORTHLAKE ROY AVENUE CONSTRUCTION DETAILS
		DRAWN - ES	REVISED -		
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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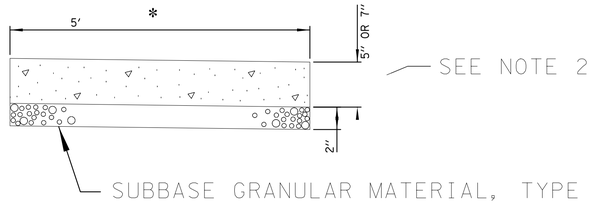
		ILLINOIS FED. AID PROJECT		

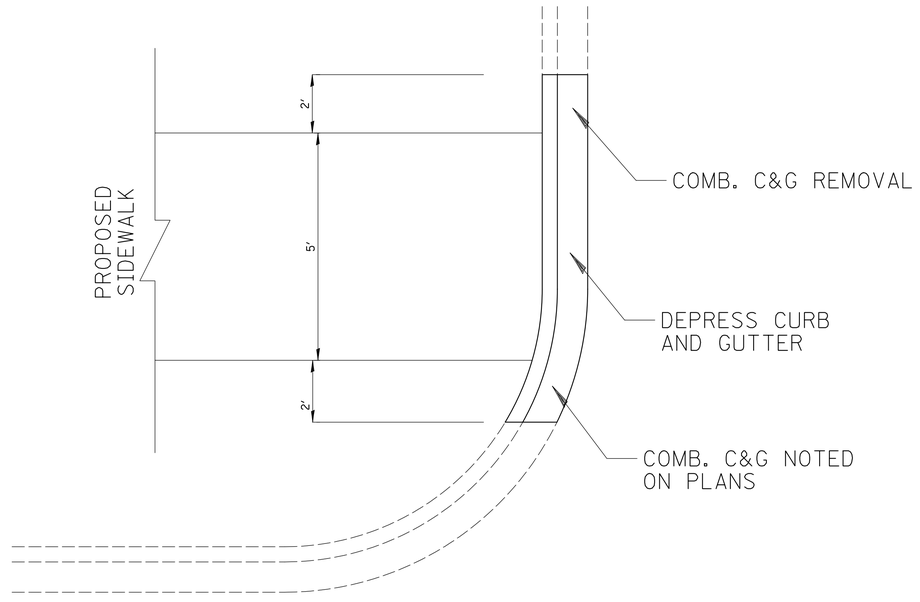


NOTES:

1. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.

2. ALL LANDSCAPE RESTORATION (4" TOPSOIL, SALT TOLERANT SODDING) SHALL BE INCLUDED IN COST FOR P.C.C. SIDEWALK.

P.C.C. SIDEWALK DETAIL



PROPOSED DEPRESSED CURB & GUTTER
EXISTING CURB & GUTTER NOT DEPRESSED

NOT TO SCALE

P.C.C. SIDEWALK DETAIL

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		DRAWN - ES	REVISED -
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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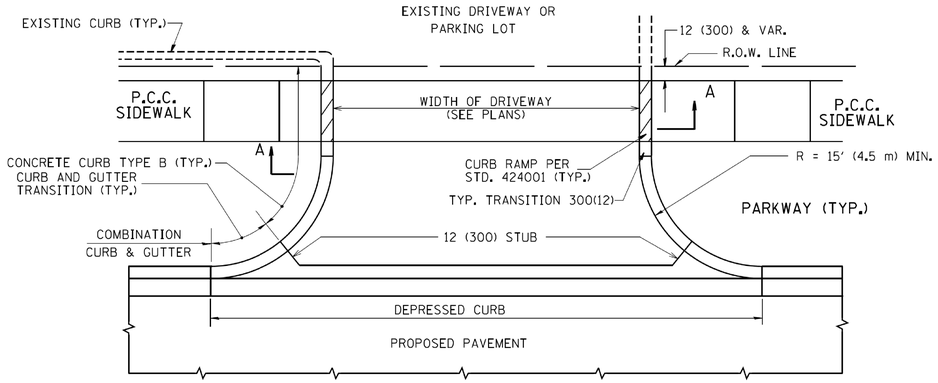
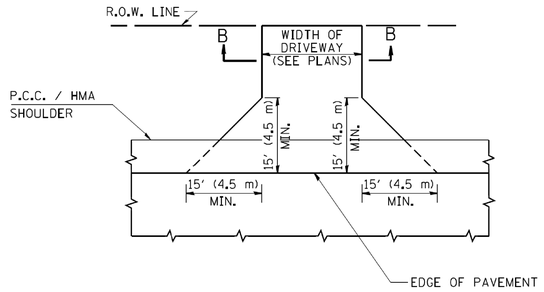
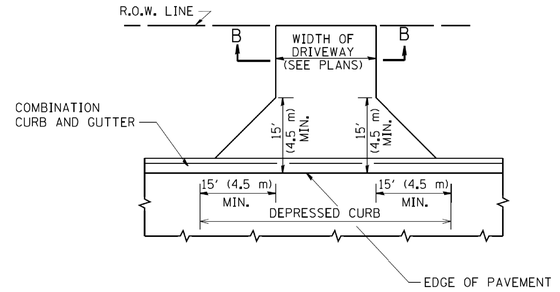


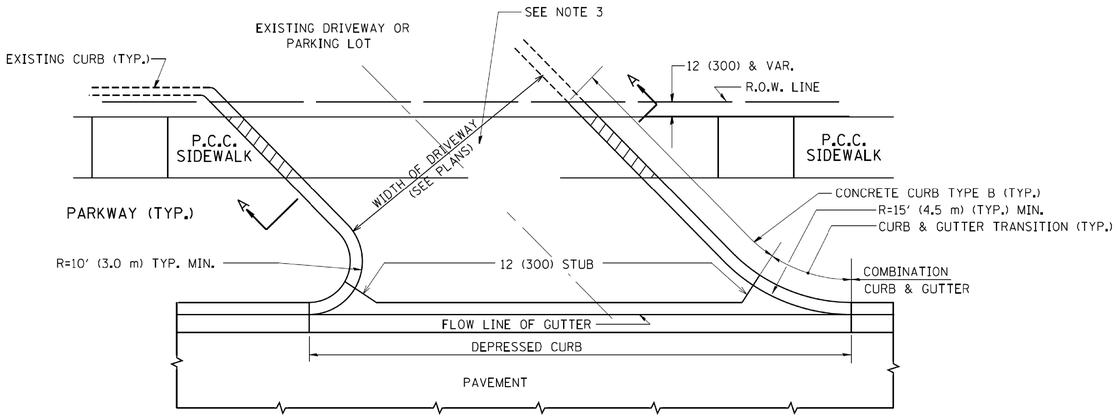
WITH CONCRETE CURB, TYPE B



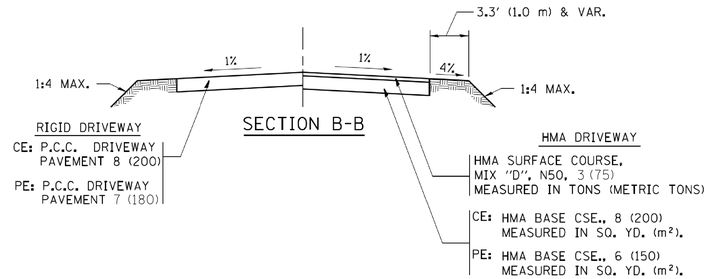
ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



WITH CONCRETE CURB, TYPE B

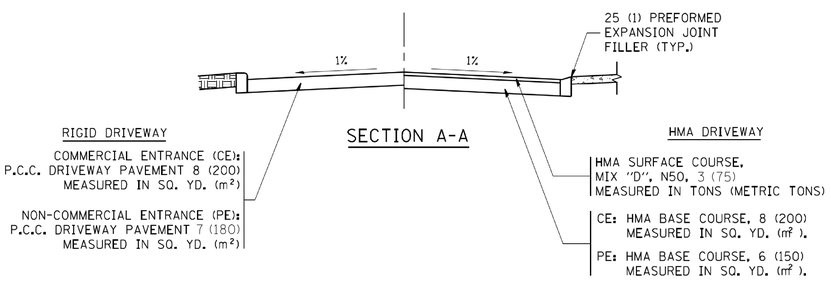


SECTION B-B

RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE,
MIX "D", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200)
MEASURED IN SQ. YD. (m²).



SECTION A-A

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

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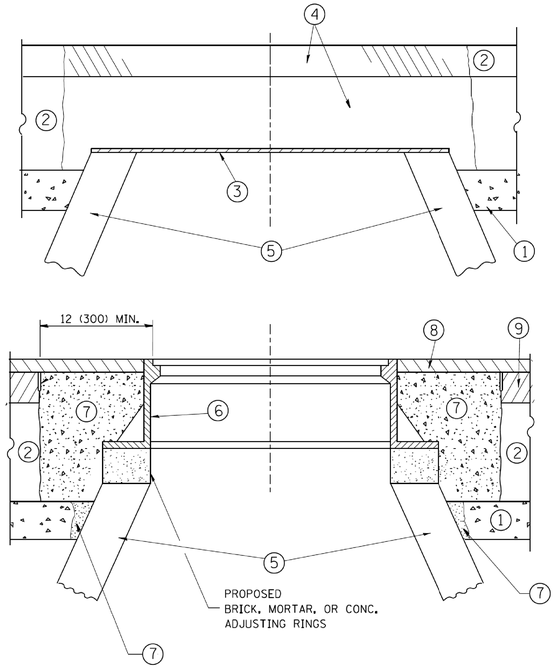
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF NORTHLAKE – ROY AVENUE
DRIVEWAY DETAILS – DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER > = 15'(4.5m)

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	13
	BD0156-07 (BD-01)	CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS PP-1* CONCRETE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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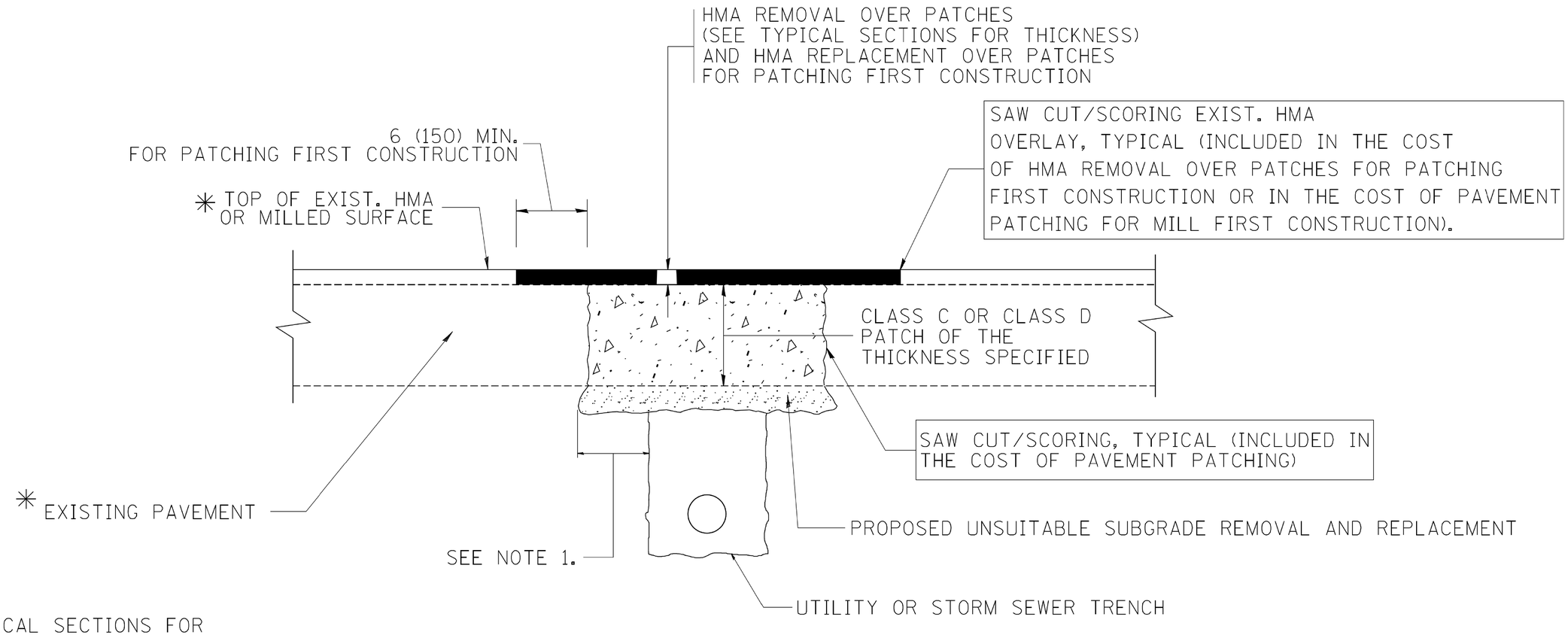
	BD600-03 (BD-8)	CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			



✳ SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST $4\frac{1}{2}$ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	BD400-04 (BD-22)	CONTRACT NO. 61F46		
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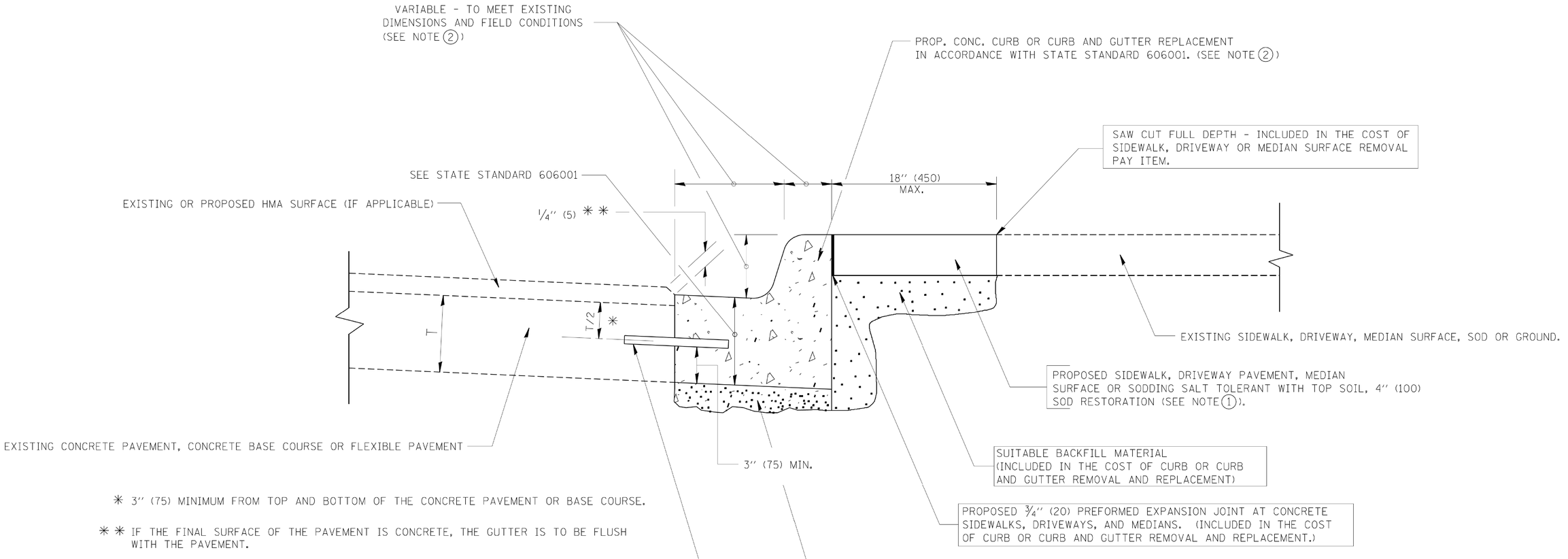


* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

(2) FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

(3) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(4) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(5) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(7) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(8) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

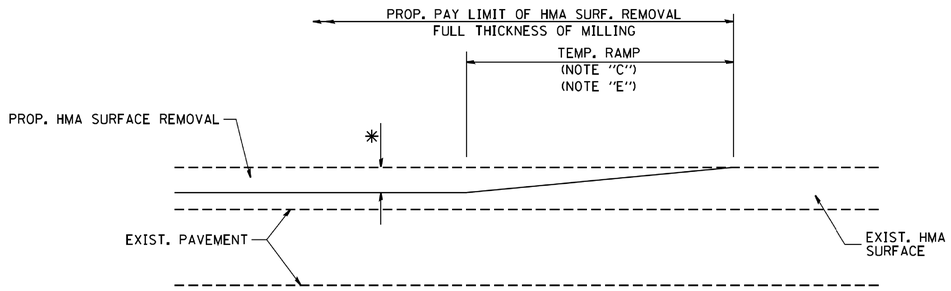
FILE NAME = ...\Civil\DET_6_940032HR286.SHT	USER NAME = jdefrenza	DESIGNED - VMR	REVISED -
		DRAWN - ES	REVISED -
	PLOT SCALE = 20'	CHECKED - JGS	REVISED -
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

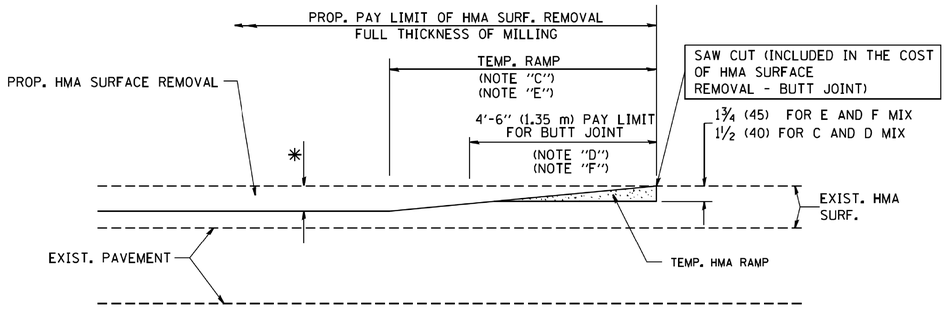
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	16
	BD600-06 (BD-24)	CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

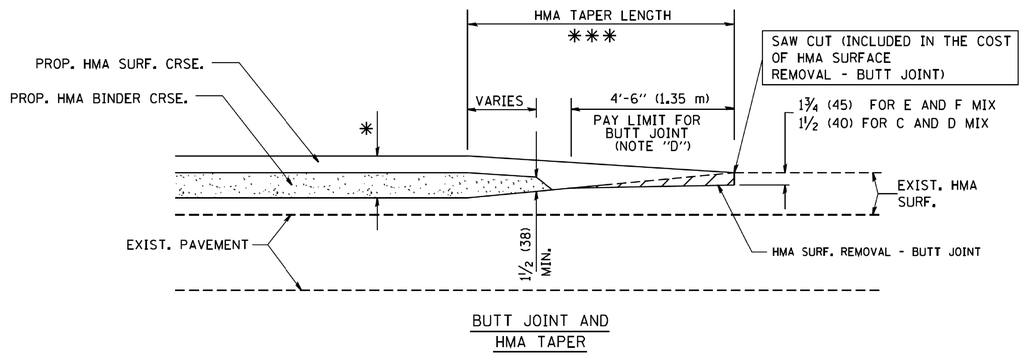


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

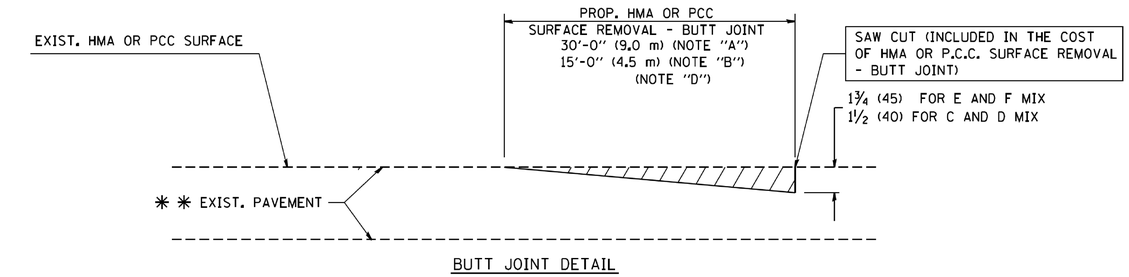
OPTION 2

TYPICAL TEMPORARY RAMP

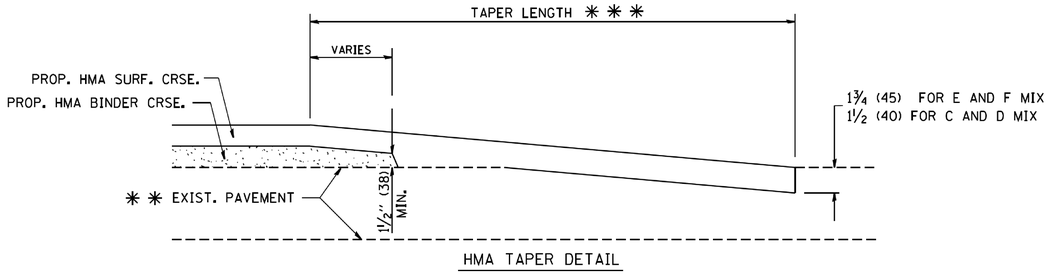


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

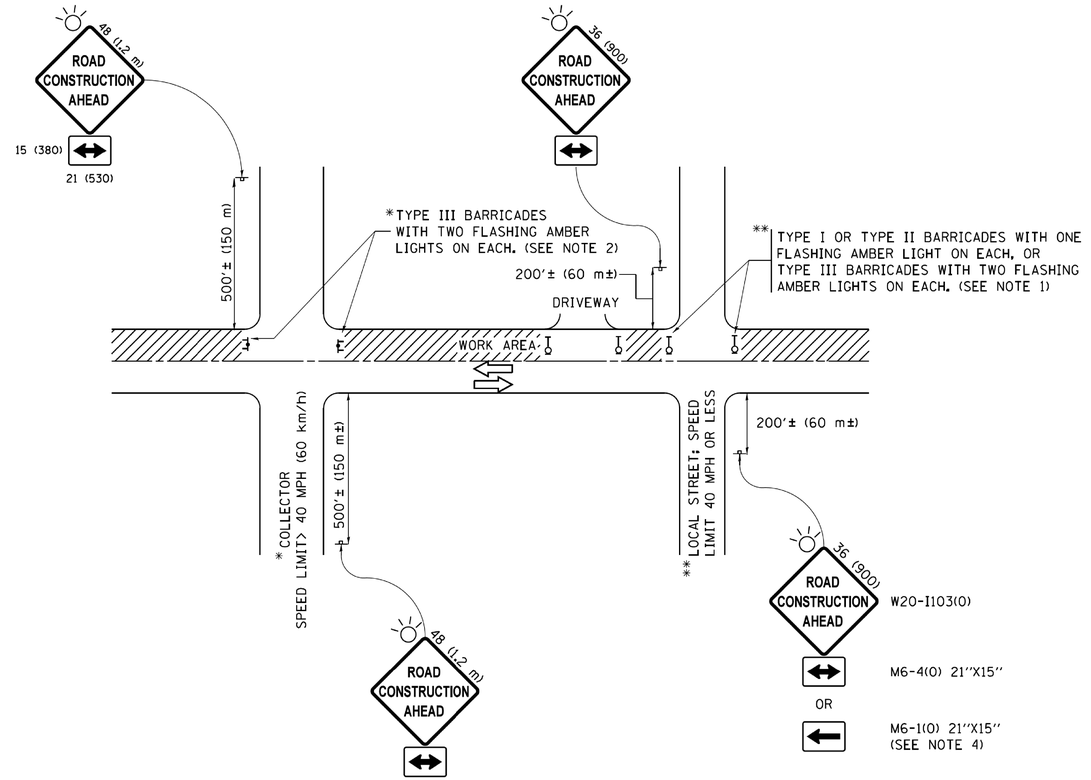
FILE NAME = ...\Civil\DET_9_940032HR286.SHT	USER NAME = jdefrenza	DESIGNED - VMR	REVISED -
		DRAWN - ES	REVISED -
	PLOT SCALE = 20'	CHECKED - JGS	REVISED -
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINTS AND HMA TAPER DETAILS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	17
	BD400-05 (BD-32)	CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

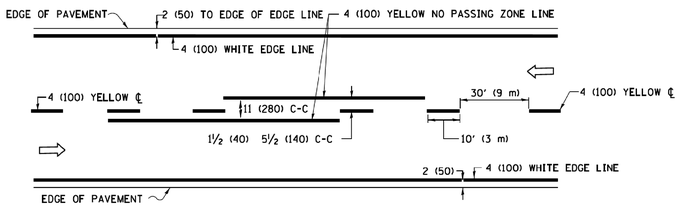
FILE NAME = ...\Civil\DET_2_940032HR286.sht	USER NAME = jdefrenza	DESIGNED - VMR	REVISED -
		DRAWN - ES	REVISED -
	PLOT SCALE = 20'	CHECKED - JGS	REVISED -
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

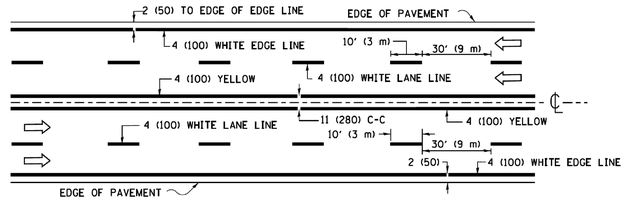
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

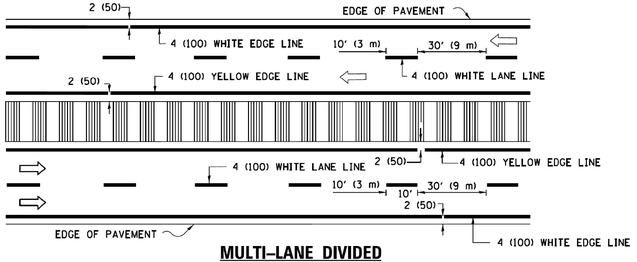
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	18
	TC-10	CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			



2–LANE ROADWAY

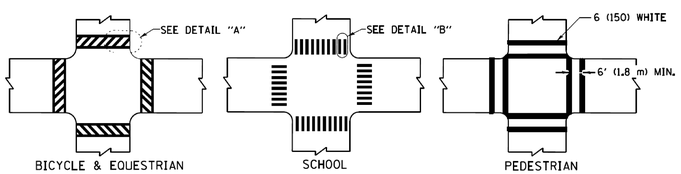


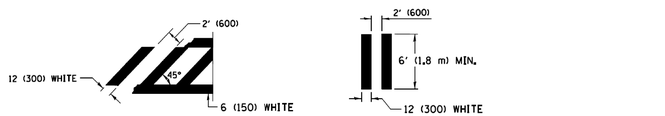
MULTI–LANE UNDIVIDED



MULTI–LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

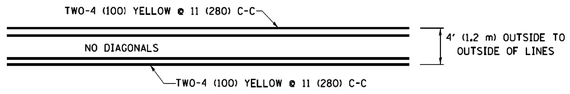




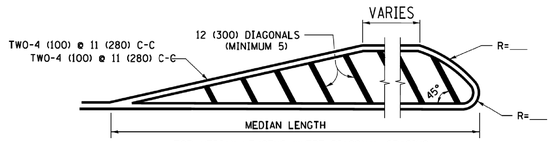
DETAIL "A" **DETAIL "B"**

TYPICAL CROSSWALK MARKING

✳ MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



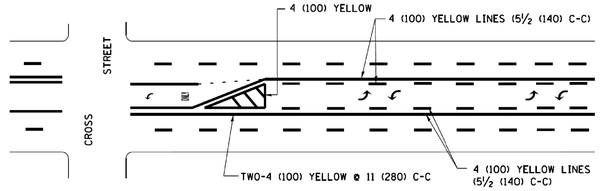
4' (1.2 m) WIDE MEDIANS ONLY



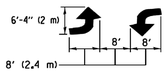
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

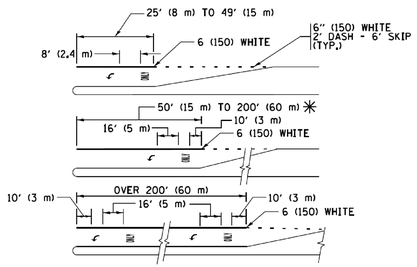


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO–WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

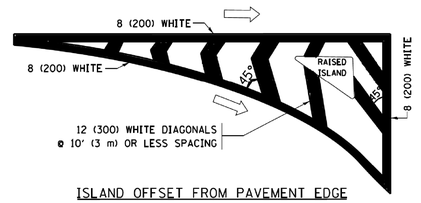


FULL SIZE LETTERS 8' (2,4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) **ONLY** AREA = 20.8 SQ. FT. (1.9 m^2)

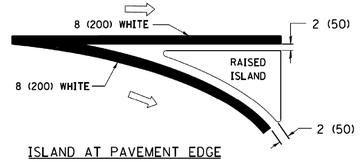
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

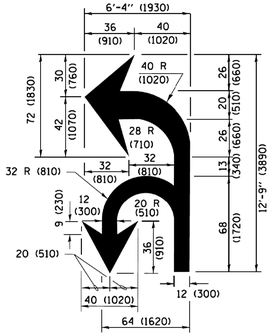


ISLAND OFFSET FROM PAVEMENT EDGE

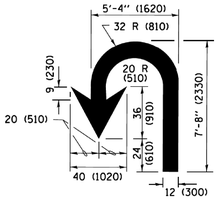


ISLAND AT PAVEMENT EDGE

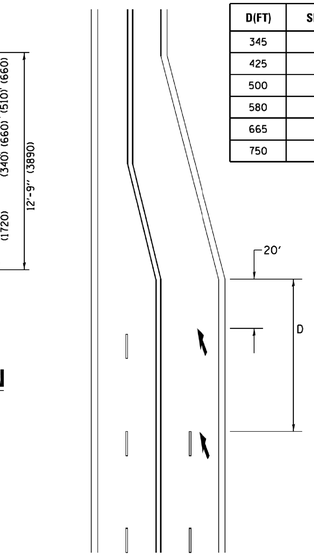
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U–TURN



U–TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

✳ LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 ⊠ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 ⊠ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2,4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 ⊠ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 ⊠ 6 (150) 12 (300) ⊠ 45° 12 (300) ⊠ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 ⊠ 4 (100) WITH 12 (300) DIAGONALS ⊠ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS ⊠ 45°	SOLID	WHITE	DIAGONALS: 15' (4,5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3,6 SQ. FT. (0.33 m^2) EACH "X"=54,0 SQ. FT. (5,0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) ⊠ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

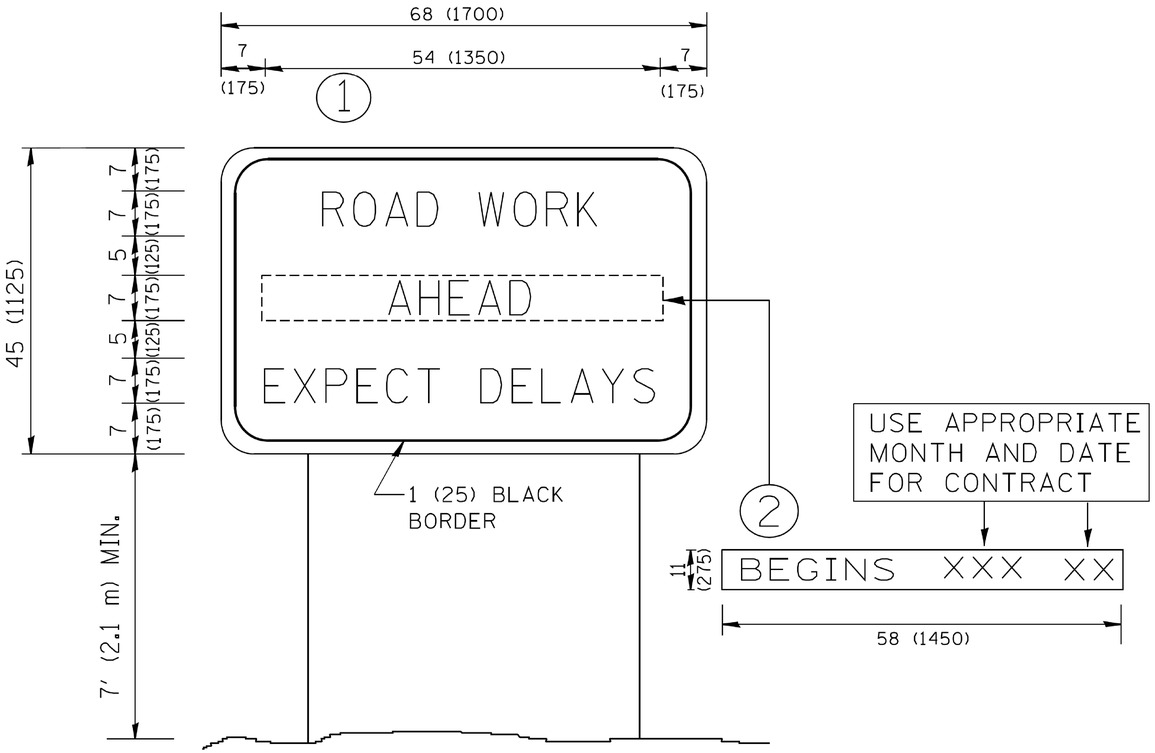
FILE NAME = ...\Civil\DET_3_940032HR286.sht	USER NAME = jdefrenza	DESIGNED - VMR	REVISED -
		DRAWN - ES	REVISED -
	PLOT SCALE = 20'	CHECKED - JGS	REVISED -
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT DETAILS

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	19
	TC-13	CONTRACT NO. 61F46		
		ILLINOIS FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE ''ROAD CONSTRUCTION AHEAD'' SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR ''TEMPORARY INFORMATION SIGNING'' FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = ...\Civil\DET_10_940032HR286.SHT	USER NAME = jdefrenza	DESIGNED - VMR	REVISED -
		DRAWN - ES	REVISED -
	PLOT SCALE = 20'	CHECKED - JGS	REVISED -
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	20
	TC-22	CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

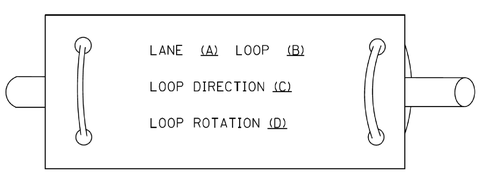
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

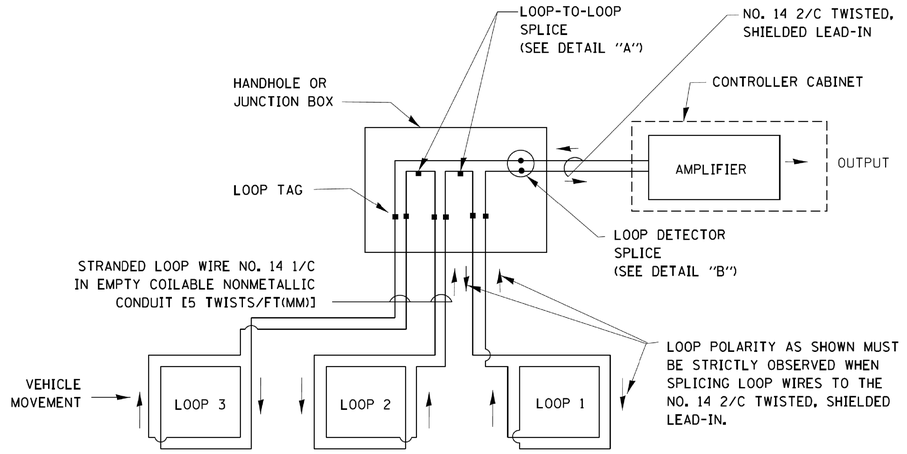


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

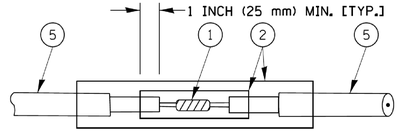
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

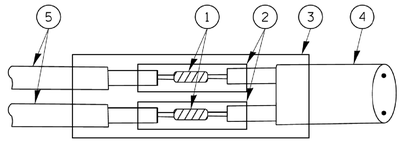


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

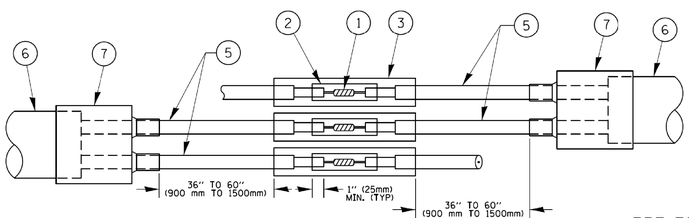


DETAIL "A"
LOOP-TO-LOOP SPLICE



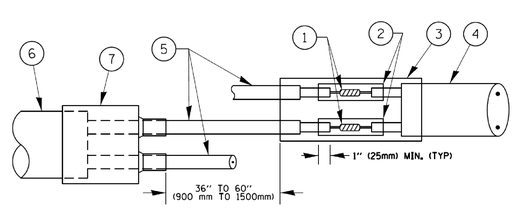
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE

PRE-FORMED LOOP



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

(6) PRE-FORMED LOOP

(7) XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME = ...\Civil\DET_11B_940032HR286.SHT	USER NAME = jdefrenza	DESIGNED - VMR	REVISED -
		DRAWN - ES	REVISED -
	PLOT SCALE = 20'	CHECKED - JGS	REVISED -
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

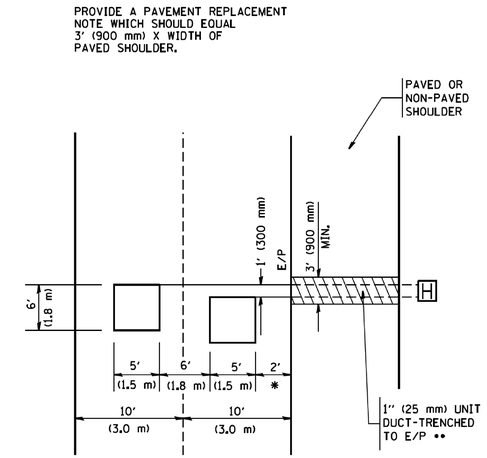
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: | SHEET NO. OF SHEETS | STA. | TO STA.

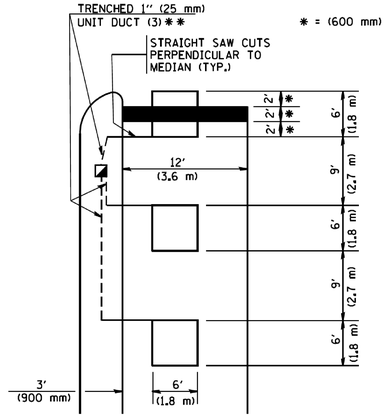
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	21
	TS-05	CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			

LOOPS NEXT TO SHOULDERS



LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

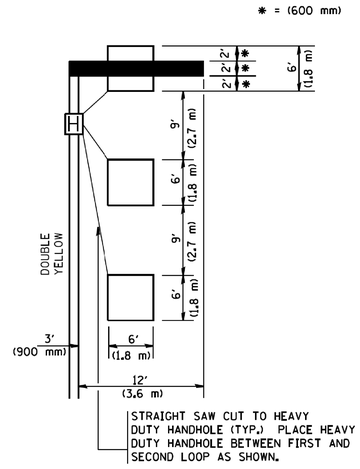
(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

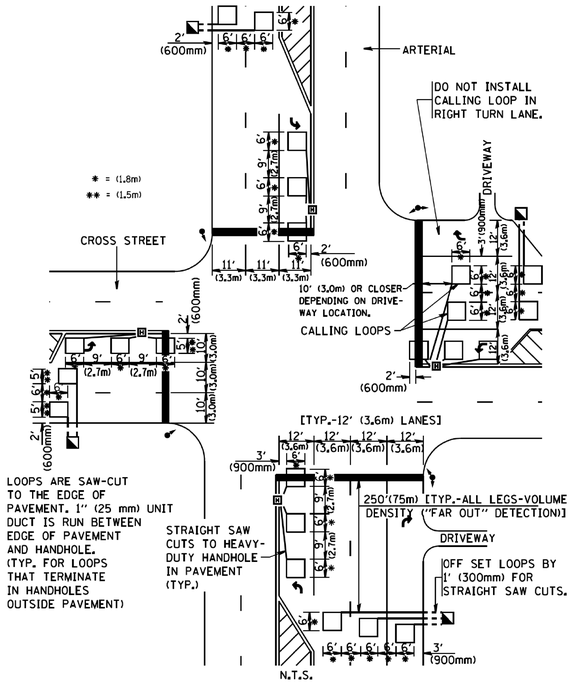
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



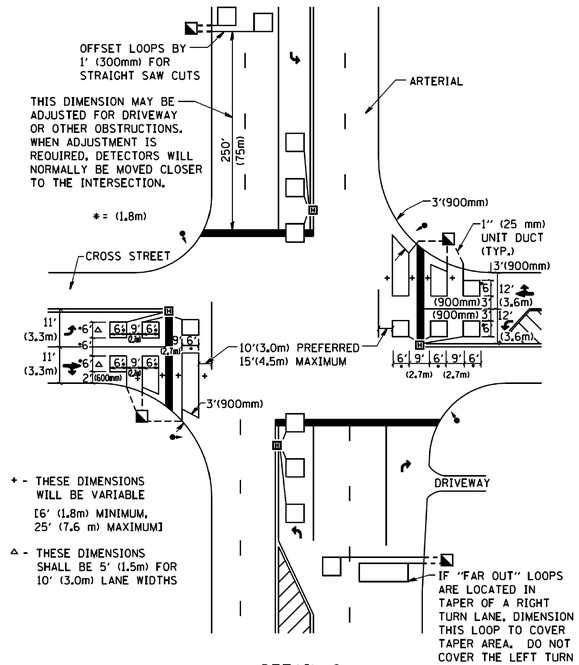
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = ...\Civil\DET_11_94003ZHR286.SHT	USER NAME = jdefrenza	DESIGNED - VMR	REVISED -
		DRAWN - ES	REVISED -
	PLOT SCALE = 20'	CHECKED - JGS	REVISED -
	PLOT DATE = 12/26/2018	DATE - 06/22/07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2539	18-00093-00-RS	COOK	22	22
	TS-07	CONTRACT NO. 61F46		
	ILLINOIS FED. AID PROJECT			