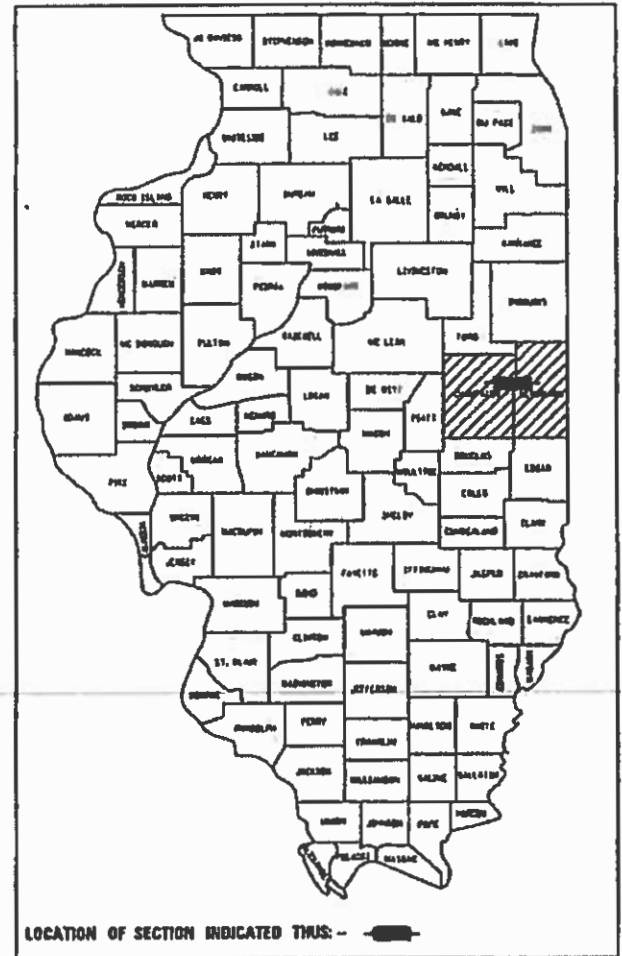


STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

| F.A.I. R.T.C. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------|---------|--------|--------------|-----------|
| 1-74          |         | **     | 142          | 1         |

CONTRACT NO. 70790  
110-7,8;92-8,9)RS-2(92-10)RS-3  
\*\* CHAMPAIGN & VERMILION

D-95-032-11



FUNCTIONAL CLASSIFICATION  
PRINCIPAL ARTERIAL (INTERSTATE)

| ST. JOSEPH TO CH 22 (OGDEN) | CH 22 (OGDEN) TO IL 49 N | IL 49 N TO 0.37 MILES EAST |
|-----------------------------|--------------------------|----------------------------|
| 2018 ADT = 30,700           | 2018 ADT = 29,200        | 2018 ADT = 26,200          |
| P.V. = 74.3%                | P.V. = 74.5%             | P.V. = 70.2%               |
| S.U. = 3.2%                 | S.U. = 2.9%              | S.U. = 3.1%                |
| M.U. = 22.5%                | M.U. = 22.6%             | M.U. = 26.7%               |

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR HIGHWAY STANDARDS LIST, SEE SHEET NO. 2  
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 4-15

OMISSIONS

SN 010-0183  
STA 901+29.00  
2500E /I-74  
EB VC = 15'-5"  
WB VC = 15'-5"

SN 010-0277  
STA 1000+88.67  
IL 49 (S) /I-74  
EB VC = 16'-11"  
WB VC = 17'-0"

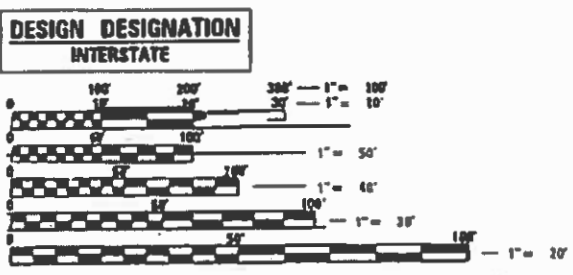
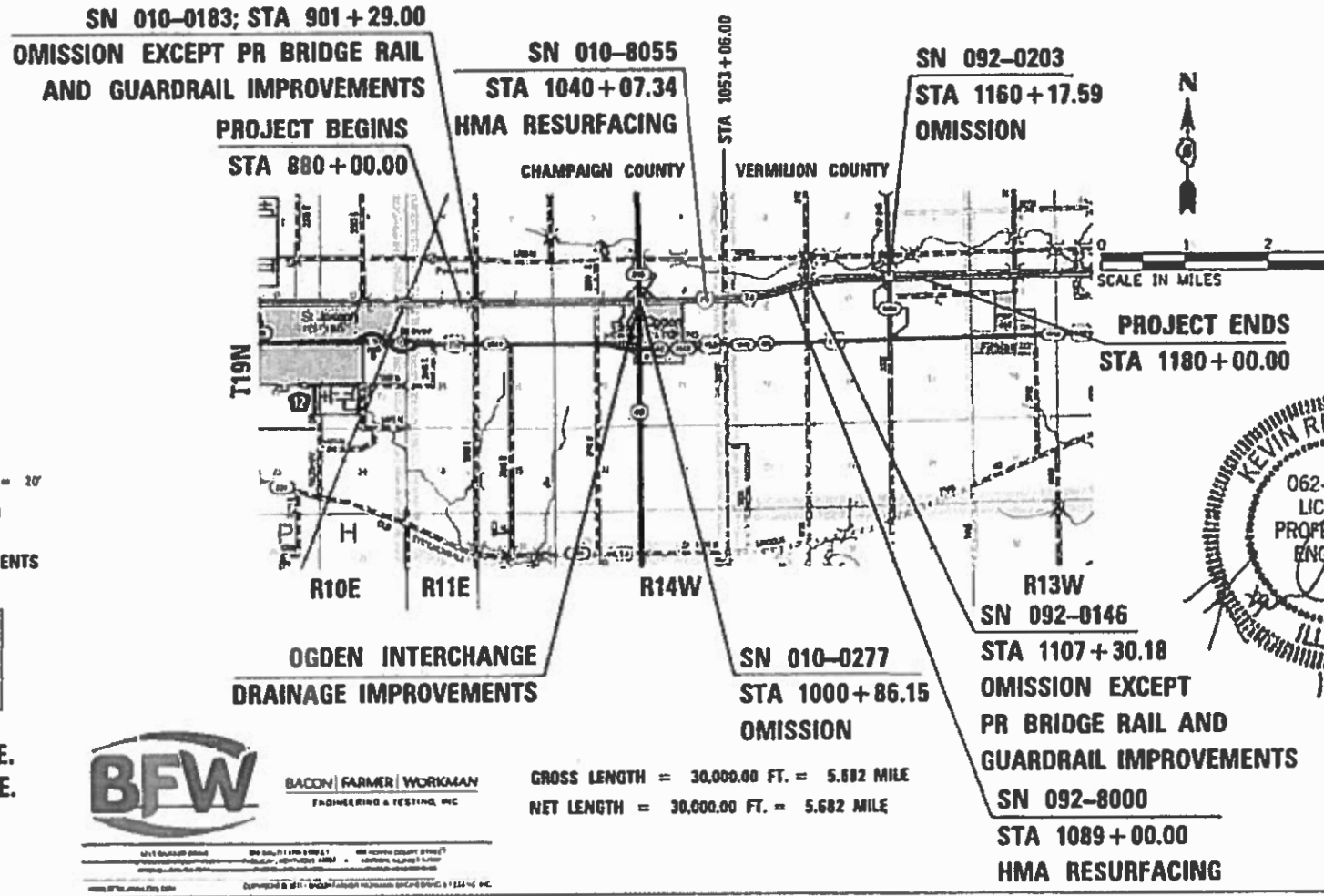
SN 092-0146  
STA 1107+30.18  
070E /I-74  
EB VC = 15'-2"  
WB VC = 15'-3"

SN 092-0203  
STA 1160+20.53  
IL 49 (N) /I-74  
EB VC = 17'-0"  
WB VC = 17'-0"

PROPOSED  
HIGHWAY PLANS

FAI ROUTE 74 (I-74)  
SECTION (10-7,8;92-8,9)RS-2(92-10)RS-3  
PROJECT NHPP-SCB2(239)  
RESURFACING (INT-2ND) RAMP REPAIRS  
CHAMPAIGN & VERMILION COUNTY

C-95-032-11  
F.A.I. 74 FROM 2 MILES EAST OF ST. JOSEPH INTERCHANGE TO  
IL 49 (N)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

TOWNSHIPS: OGDEN & OAKWOOD  
PROJECT ENGINEER JASON STULTS, P.E.  
PROJECT MANAGER RYAN CARROLL, P.E.  
(217) 465-4181

CONTRACT NO. 70790



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED: January 31, 2019  
March 22, 2019  
March 22, 2019

REGIONAL ENGINEER  
ENGINEER OF DESIGN AND ENVIRONMENT  
DIRECTOR OF HIGHWAYS, PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

### INDEX OF SHEETS

| SHEET NO.  | DESCRIPTION   |
|------------|---|
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| 2          | INDEX OF SHEETS & HIGHWAY STANDARDS                               |
| 3          | GENERAL NOTES, COMMITMENTS, & LEGEND                              |
| 4 TO 15    | SUMMARY OF QUANTITIES   |
| 16 TO 19   | TYPICAL SECTIONS  |
| 20 TO 41   | SCHEDULES OF QUANTITIES   |
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| 72 TO 75   | INTERCHANGE ISLANDS   |
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| 83 TO 84   | LIGHTING PLANS  |
| 85 TO 92   | STRUCTURE PLANS   |
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| 110        | DETAIL OF CONCRETE MEDIAN, TYPE SM (DOWELLED)                     |
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| 130        | DETAIL OF RAMP C DETOUR PLAN                                      |
| 131 TO 132 | CULVERT DETAILS AND PROFILES                                      |
| 133 TO 142 | CROSS SECTIONS  |
|            |   |
|            |   |

### LIST OF STANDARDS

|           |  |
|-----------|--|
| 000001-07 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS                                 |
| 001006    | DECIMAL OF AN INCH AND OF A FOOT   |
| 202001-01 | EARTH MEDIAN DITCH CHECK   |
| 280001-07 | TEMPORARY EROSION CONTROL SYSTEMS  |
| 420701-03 | PAVEMENT WELDED WIRE REINFORCEMENT   |
| 442101-09 | CLASS B PATCHES  |
| 542301-03 | PRECAST REINFORCED CONCRETE FLARED END SECTION                               |
| 542306-03 | PRECAST REINFORCED CONCRETE ELLIPTICAL FLARED END SECTION                    |
| 542401-03 | METAL FLARED END SECTION FOR PIPE CULVERTS                                   |
| 601001-05 | PIPE UNDERDRAINS   |
| 601101-02 | CONCRETE HEADWALL FOR PIPE UNDERDRAINS                                       |
| 602301-04 | INLET - TYPE A   |
| 602401-06 | PRECAST MANHOLE TYPE A 4' (1.22 m) DIAMETER                                  |
| 602406-10 | PRECAST MANHOLE TYPE A 6' (1.83 m) DIAMETER                                  |
| 602701-02 | MANHOLE STEPS  |
| 604001-04 | FRAME AND LIDS TYPE 1  |
| 604036-03 | GRATE TYPE 8   |
| 606301-04 | PC CONCRETE ISLANDS AND MEDIANS  |
| 630301-09 | SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS                   |
| 642001-02 | SHOULDER RUMBLE STRIPS, 16 in.   |
| 667101-02 | PERMANENT SURVEY MARKERS   |
| 701101-05 | OFF-RD OPERATIONS MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE  |
| 701106-02 | OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY                     |
| 701201-05 | LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH                          |
| 701400-09 | APPROACH TO LANE CLOSURE, FREEWAY / EXPRESSWAY                               |
| 701401-12 | LANE CLOSURE, FREEWAY / EXPRESSWAY   |
| 701402-12 | LANE CLOSURE, FREEWAY / EXPRESSWAY, WITH BARRIER                             |
| 701406-12 | LANE CLOSURE, FREEWAY / EXPRESSWAY, DAY OPERATIONS ONLY                      |
| 701411-09 | LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXT RAMP, FOR SPEEDS ≥ 45 MPH        |
| 701426-09 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH   |
| 701428-01 | TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY / EXPRESSWAY                       |
| 701456-05 | PARTIAL EXT RAMP CLOSURE FREEWAY/ EXPRESSWAY                                 |
| 701901-08 | TRAFFIC CONTROL DEVICES  |
| 704001-08 | TEMPORARY CONCRETE BARRIER   |
| 725001-01 | OBJECT AND TERMINAL MARKERS  |
| 780001-05 | TYPICAL PAVEMENT MARKINGS  |
| 781001-04 | TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS                      |
| 782006    | GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS                        |
| 821101-02 | LUMINAIRE WIRING DIAGRAM   |
| 825011-04 | LIGHTING CONTROLLER PEDESTAL MOUNTED, 240V                                   |
| 838001-01 | BREAKAWAY DEVICES  |
| 701001-02 | OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5m) AWAY                         |
| 701006-05 | OFF-RD OPERATIONS, 2L, 2W, 15' (4.5m) TO 24" (600 mm) FROM THE PAVEMENT EDGE |

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FILE NAME: ...D:\570790-11c-gemote.dgn

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| PLOT DATE = 1/30/2019 - 1:38:39 PM | DATE -     | REVISED - |  |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS  
AND LISTS OF STANDARDS**

SCALE: N.T.S.    SHEET    OF    SHEETS    STA.    TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| 1-74               | *       | **     | 142          | 2         |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION  
REV. 3/29/19

**GENERAL NOTES**

- G.N.-100 ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.
- G.N.-100B MICROSTATION AND GEOPAK FILES OF THIS PROJECT WILL BE MADE AVAILABLE TO THE CONTRACTOR AFTER CONTRACT AWARD. IF THERE IS A CONFLICT BETWEEN THE ELECTRONIC FILES AND THE PRINTED CONTRACT PLANS AND DOCUMENTS, THE PRINTED CONTRACT PLANS AND DOCUMENTS SHALL TAKE PRECEDENCE OVER THE ELECTRONIC FILES. THE CONTRACTOR SHALL ACCEPT ALL RISK ASSOCIATED WITH USING THE ELECTRONIC FILES AND SHALL HOLD THE DEPARTMENT HARMLESS FOR ANY ERRORS OR OMISSIONS IN THE ELECTRONIC FILES AND THE DATA CONTAINED THEREIN. ERRORS OR DELAYS RESULTING FROM THE USE OF THE ELECTRONIC FILES BY THE CONTRACTOR SHALL NOT RESULT IN AN EXTENSION OF TIME FOR ANY INTERIM OR FINAL COMPLETION DATE OR SHALL NOT BE CONSIDERED CAUSE FOR ADDITIONAL COMPENSATION. THE CONTRACTOR SHALL NOT USE, SHARE, OR DISTRIBUTE THESE ELECTRONIC FILES EXCEPT FOR THE PURPOSE OF CONSTRUCTING THIS CONTRACT. ANY CLAIMS BY THIRD PARTIES DUE TO USE OR ERRORS SHALL INCLUDE THIS DISCLAIMER WITH THE TRANSFER OF THESE ELECTRONIC FILES TO ANY OTHER PARTIES AND SHALL INCLUDE APPROPRIATE LANGUAGE BINDING THEM TO SIMILAR RESPONSIBILITIES.
- G.N.-105.07C EXISTING STATE-OWNED AND MAINTAINED UNDERGROUND UTILITY FACILITIES EXIST WITHIN THE ROW. THE DEPARTMENT IS NOT A MEMBER OF JULIE AND DOES NOT LOCATE IT'S OWN FACILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE AT THEIR OWN EXPENSE FOR SECURING AN APPROVED LOCATING FIRM TO LOCATE ALL EXISTING IDOT UNDERGROUND FACILITIES PRIOR TO COMMENCING ANY EXCAVATION, PER THE REQUIREMENTS OF ARTICLE 803 OF THE STANDARD SPECIFICATIONS. UTILITY LOCATES MAY ALSO BE REQUIRED OUTSIDE THE PROJECT LIMITS, SUCH AS FOR TRAFFIC CONTROL SIGNING AND OTHER ITEMS. THE CONTRACTOR MAY OBTAIN, ON REQUEST, PLANS OF EXISTING ELECTRICAL FACILITIES FROM THE DEPARTMENT. FOR FURTHER INFORMATION, THE CONTRACTOR MAY CONTACT THE DISTRICT TRAFFIC OPERATIONS ENGINEER, GARY SIMS, AT 217-251-4859.  
  
THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR LOCATING AND PROVIDING PROTECTION FOR FACILITIES DURING ALL PHASES OF CONSTRUCTION. IF, AT ANY TIME, THE FACILITIES ARE DAMAGED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE DEPARTMENT AND MAKE ALL NECESSARY ARRANGEMENTS FOR REPAIR TO THE SATISFACTION OF THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE CONTRACT BID PRICE AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- G.N.-105.09A ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988. (NAVD 88)
- G.N.-201 TREES THAT INTERFERE WITH THE CONSTRUCTION OPERATIONS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER. ANY TREE DUE TO ITS LOCATION AND DEEMED SUITABLE FOR SAVING BY THE ENGINEER SHALL BE PROTECTED DURING CLEARING AND SUBSEQUENT CONSTRUCTION OPERATIONS.
- G.N.-250C SEEDING, CLASS 7 AND MULCH, METHOD 2 IS INCLUDED IN THIS CONTRACT TO SEED NEW EARTH SHOULDERS DURING TIME PERIODS WHEN PERMANENT SEEDING IS NOT ALLOWED. SOME OR ALL OF THE CLASS 7 SEEDING AND MULCH WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SEEDING ON EARTH SHOULDERS AT THE TIME OF THEIR COMPLETION.
- G.N.-406 THE QUANTITIES INCLUDED IN THE PLANS FOR HOT-MIX ASPHALT RESURFACING ARE INTENDED TO GIVE THE COVERAGE SHOWN ON THE TYPICAL CROSS SECTIONS. IT IS NOT INTENDED TO INCREASE THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE IN ORDER TO USE ALL OF THE QUANTITIES INCLUDED IN THE CONTRACT.
- G.N.-406H MIXTURE REQUIREMENTS

- G.N.-440M THE HMA BEING MILLED FROM STA. 880+00.00 TO STA. 961+00.00 WITHIN THIS PROJECT CONTAINS A HIGH FRICTION SURFACE TREATMENT CONSISTING OF EPOXY BINDER AND CALCINED BAUXITE SAND PARTICLES WITH AN APPROXIMATE THICKNESS OF 1/4" - 3/8". THIS MATERIAL SHALL BE MILLED AND STOCKPILED SEPERATELY FROM OTHER MAINLINE MILLINGS OR WASTED. THIS MATERIAL SHALL NOT BE USED AS RAP/FRAP IN FUTURE HMA WORK. THIS MATERIAL WILL BE SUITABLE FOR USE AS AGGREGATE WEDGE SHOULDER, TYPE B, ON THIS OR FUTURE IDOT CONTRACTS
- G.N.-442B THE PATCHING SCHEDULES INCLUDED IN THE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AT THE TIME OF COMPLETION OF THE PLANS FOR LETTING. VARIATIONS IN LOCATION AND SIZES OF THE FULL-DEPTH AND PARTIAL-DEPTH PATCHES MAY OCCUR.
- G.N.-482 ALL LOW ESAL MIXTURE PLACED AS HOT-MIX ASPHALT SHOULDERS SHALL BE COMPACTED TO 94.0 - 98.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY. THIS REQUIREMENT SHALL APPLY TO IL 9.5L GRADATION MIXES. THIS MAXIMUM DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE OF FOUR TESTS AS IN OTHER QC/QA TESTING. A NUCLEAR GAUGE DENSITY/CORE CORRELATION SHALL BE PERFORMED FOR THE IL 9.5L USING STANDARD CORRELATION PROCEDURES WHEN MORE THAN 3,000 TONS ARE TO BE PLACED.
- G.N.-542B ALL THE ENTRANCE CULVERT LENGTHS SHOWN IN THE PLANS WERE CALCULATED WITH THE ASSUMPTION THAT METAL PIPES AND METAL END ECTIONS WOULD BE USED. BEFORE ORDERING STORM SEWERS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR THE EXACT LENGTHS.
- G.N.-609 PRIOR TO ROUTING TRAFFIC ONTO THE SHOULDERS AS SHOWN IN THE STAGING PLANS, THE CONTRACTOR SHALL SECURE THE GRATINGS ON SHOULDER INLETS AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR ACCORDING TO ARTICLE 109.04.
- G.N.-667 THE RESIDENT ENGINEER SHALL CONTACT PROGRAM DEVELOPMENT CHIEF OF SURVEYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE FOR INSTRUCTION AS TO SETTING OF TEMPORARY OR PERMANENT TIES FOR CENTERLINE ALIGNMENT CONTROL SURVEY MARKERS (PC'S, PT'S, AND PI'S). PROJECT IMPLEMENTATION PERSONNEL WILL BE RESPONSIBLE FOR LAYOUT OF THESE MARKERS.
- G.N.-703A SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER (MACHING METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).
- G.N.-781 RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH STANDARD 781001, AND THE DETAILS SHOWN IN THE PLANS. IF THERE IS ANY DISCREPANCY BETWEEN THE STANDARD AND THE DETAILS IN THE PLANS, THE DETAILS IN THE PLANS SHALL GOVERN. THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS AND THE RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED MIDWAY IN THE 30 FOOT (9M) SPACE BETWEEN THE DASHED CENTERLINE STRIPES (WHEN APPLICABLE).
- G.N.-1004.01 COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS.

MANUFACTURER'S SPECIFICATIONS FOR THE HTC MEDIAN BARRIER SHALL BE IN COMPLIANCE WITH LINES AND GRADES SHOWN ON THE PLANS. LINE POST FOUNDATIONS SHALL BE A MINIMUM DEPTH OF 42" AND SHALL BE IN COMPLIANCE WITH MANUFACTURER'S SPECIFICATIONS FOR THE SOIL STRENGTH BASED ON SOIL BORINGS. IN ADDITION, THE HTC MEDIAN BARRIER TERMINALS SHALL BE IN COMPLIANCE WITH THE MANUFACTURER'S SPECIFICATIONS FOR THE SOIL STRENGTH BASED ON PLAN SOIL BORINGS. POSTS SHALL BE PLACED ON CENTERS BASED ON THE MANUFACTURER'S RECOMMENDATIONS TO ACHIEVE A DEFLECTION NOT GREATER THAN 9.2 FEET. FOR QUANTITIES, SEE THE QUANTITIES SCHEDULE. REFLECTIVE CAPS SHALL BE PLACED ON THE POSTS AT THE SAME SPACING AS RAISED REFLECTIVE MARKERS ON THE ADJACENT PAVEMENT. LINE POST FOUNDATIONS AND REFLECTIVE CAPS SHALL NOT BE MEASURED FOR PAYMENT AND SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER FOOT FOR "HIGH TENSION CABLE MEDIAN BARRIER". ALL HTC HARDWARE ON THIS PROJECT SHALL MEET TEST LEVEL-4.

**COMMITMENTS**

THERE ARE NO COMMITMENTS FOR THIS CONTRACT

**MATERIAL TRANSFER DEVICE INFORMATION**

BASED ON STRUCTURAL ANALYSIS, MTD USAGE OVER STRUCTURES IS AS FOLLOWS:

ONLY AN EMPTY MTD MAY PASS OVER STRUCTURES:  
010-8055 AND 092-8000

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

| LOCATION                   | MAINLINE - HFST LOCATION | MAINLINE & RAMPS | MAINLINE & RAMPS | BOTTOM 10 3/4" UNDER OVERHEADS | MAINLINE, RAMPS & MOW STRIP | I-74  | RAMPS                |
|----------------------------|--------------------------|------------------|------------------|--------------------------------|-----------------------------|---|----------------------|
| MIXTURE USE                | POLYMER SURFACE          | POLYMER SURFACE  | POLYMER BINDER   | POLYMER BINDER                 | SHOULDER SURFACE            | SHLDR BINDER, HMA SHLDR 8", SHLDR REM & REPL. | POLYMER LEVEL BINDER |
| AC/PG                      | SBS PG 76-22             | SBS PG 76-22     | SBS PG 70-22     | SBS PG 70-22                   | PG 64-22                    | PG 64-22                                      | SBS PG 70-22         |
| DESIGN AIR VOIDS           | 4.0% @ Ndes=80           | 4.0% @ Ndes=80   | 4.0% @ Ndes=90   | 4.0% @ Ndes=90                 | 4.0% @ Ndes=30              | 4.0% @ Ndes=50                                | 4.0% @ Ndes=90       |
| MIX COMP(GRADATION)        | SMA 12.5                 | SMA 12.5         | IL 19.0          | IL 19.0                        | IL 9.5L                     | IL 19.0                                       | IL 9.5 F.G.          |
| FRICTION AGGREGATE         | Mix F                    | Mix E            | N.A.             | N.A.                           | Mix C                       | N.A.  | N.A.                 |
| MIXTURE WEIGHT             | 112                      | 112              | 112              | 112                            | 112                         | 112   | 112                  |
| QUALITY MANAGEMENT PROGRAM | QCP                      | PPF              | PPF              | QC/QA                          | QC/QA                       | QC/QA   | QC/QA                |
| SUBLOT SIZE                | 1000                     | 1000             | 1000             | N.A.                           | N.A.                        | N.A.  | N.A.                 |

POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 LIMITS:  
MIX "F" - STA 880+00.00 TO STA 961+00.00 QUANTITY: 6,027 TONS  
MIX "E" - STA 961+00.00 TO STA 1180+00.00 QUANTITY: 15,196 TONS

MODEL: Default  
FILE: Mch18\_1032170905-hc-gemstone.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
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| PLOT DATE = 1/24/2019 - 3:53:38 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |                              |
|--|------------------------------|
| <b>GENERAL NOTES<br/>AND COMMITMENTS</b> |                              |
| SCALE: N.T.S.                            | SHEET OF SHEETS STA. TO STA. |

|                    |         |        |                           |           |
|--------------------|---------|--------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| I-74               | *       | **     | 142                       | 3         |
| CONTRACT NO. 70790 |         |        | ILLINOIS FED. AID PROJECT |           |

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO. | ITEM                                  | UNIT  | TOTAL QUANTITY |        |       |
|----------|---------------------------------------|-------|----------------|--------|-------|
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT  | 146            | 146    |       |
| 20100210 | TREE REMOVAL (OVER 15 UNITS DIAMETER) | UNIT  | 332            | 332    |       |
| 20200100 | EARTH EXCAVATION                      | CU YD | 10,864         | 10,864 |       |
| 20400800 | FURNISHED EXCAVATION                  | CU YD | 1,153          | 1,153  |       |
| 20700110 | POROUS GRANULAR EMBANKMENT            | TON   | 2,764          | 1,382  | 1,382 |
| 20800150 | TRENCH BACKFILL                       | CU YD | 16             | 16     |       |
| 25000210 | SEEDING, CLASS 2A                     | ACRE  | 3.00           | 3.00   |       |
| 25000312 | SEEDING, CLASS 4A                     | ACRE  | 5.00           | 5.00   |       |
| 25000314 | SEEDING, CLASS 4B                     | ACRE  | 3.50           | 3.50   |       |
| 25000400 | NITROGEN FERTILIZER NUTRIENT          | POUND | 990            | 990    |       |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT        | POUND | 990            | 990    |       |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT         | POUND | 990            | 990    |       |
| 25100115 | MULCH, METHOD 2                       | ACRE  | 11.00          | 11.00  |       |
| 25100630 | EROSION CONTROL BLANKET               | SQ YD | 8,256          | 8,220  | 36    |

\* SPECIALTY ITEM

MODEL: Default  
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| PLOT DATE = 1/29/2019 - 2:16:55 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|             |         |        |                           |           |
|-------------|---------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| I-74        | *       | **     | 142                       | 4         |
|             |         |        | CONTRACT NO. 70790        |           |
|             |         |        | ILLINOIS FED. AID PROJECT |           |

\* (10-7,8,92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION  
 REV. 3/7/19



|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO. | ITEM  | UNIT  | TOTAL QUANTITY |         |        |
|----------|---|-------|----------------|---------|--------|
| 28000250 | TEMPORARY EROSION CONTROL SEEDING                                     | POUND | 4,450          | 4,450   |        |
| 28000305 | TEMPORARY DITCH CHECKS  | FOOT  | 3,435          | 3,435   |        |
| 28000400 | PERIMETER EROSION BARRIER   | FOOT  | 2,726          | 2,726   |        |
| 28000500 | INLET AND PIPE PROTECTION   | EACH  | 5              | 5       |        |
| 28001000 | AGGREGATE (EROSION CONTROL)   | TON   | 8              | 8       |        |
| 28100105 | STONE RIPRAP, CLASS A3  | SQ YD | 8              | 8       |        |
| 31100100 | SUBBASE GRANULAR MATERIAL, TYPE A                                     | TON   | 2,370          | 1,185   | 1,185  |
| 35400510 | PORTLAND CEMENT CONCRETE BASE COURSE WIDENING 11"                     | SQ YD | 1              | 1       |        |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT)                                     | POUND | 18,042         | 9,040   | 9,002  |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT)                                      | POUND | 194,581        | 110,931 | 83,650 |
| 40600847 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-9.5FG, N90           | TON   | 636            | 325     | 311    |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT                          | SQ YD | 2,973          | 2,973   |        |
| 40600990 | TEMPORARY RAMP  | SQ YD | 1,124          | 955     | 169    |
| 40603153 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 | TON   | 21,223         | 12,054  | 9,169  |

\* SPECIALTY ITEM

MODEL: Default  
 FILE NAME: ...CAD\_Sheets\0570790-01-500.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/29/2019 - 2:17:00 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|             |         |        |                    |           |
|-------------|---------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| 1-74        | *       | **     | 142                | 5         |
|             |         |        | CONTRACT NO. 70790 |           |

\* (10-7,8,92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO. | ITEM  | UNIT  | TOTAL QUANTITY |        |        |
|----------|---|-------|----------------|--------|--------|
| 40603240 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 | TON   | 19,330         | 11,388 | 7,942  |
| 40800025 | BITUMINOUS MATERIALS (PRIME COAT)                       | POUND | 98             | 60     | 38     |
| 40800050 | INCIDENTAL HOT-MIX ASPHALT SURFACING                    | TON   | 18             | 11     | 7      |
| 44000100 | PAVEMENT REMOVAL  | SQ YD | 6,419          | 3,217  | 3,202  |
| 44000154 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"                 | SQ YD | 6,609          |        | 6,609  |
| 44000156 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"                 | SQ YD | 1,269          | 612    | 657    |
| 44000157 | HOT-MIX ASPHALT SURFACE REMOVAL, 2"                     | SQ YD | 148,049        | 72,240 | 75,809 |
| 44000159 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"                 | SQ YD | 7,387          |        | 7,387  |
| 44000169 | HOT-MIX ASPHALT SURFACE REMOVAL, 5"                     | SQ YD | 64,671         | 32,430 | 32,241 |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL                     | FOOT  | 7              | 7      |        |
| 44004250 | PAVED SHOULDER REMOVAL                                  | SQ YD | 3,677          | 1,835  | 1,842  |
| 44200050 | WELDED WIRE REINFORCEMENT                               | SQ YD | 359            | 297    | 62     |
| 44201031 | CLASS B PATCHES, TYPE II, 15 INCH                       | SQ YD | 399            | 295    | 104    |
| 44201035 | CLASS B PATCHES, TYPE III, 15 INCH                      | SQ YD | 232            | 200    | 32     |

\* SPECIALTY ITEM

MODEL: Default  
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|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/29/2019 - 2:17:04 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---------------------------|---------|--------|--------------------|-----------|
| I-74                      | *       | **     | 142                | 6         |
|                           |         |        | CONTRACT NO. 70790 |           |
| ILLINOIS FED. AID PROJECT |         |        |                    |           |

\*(10-7,8,92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION  
 REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO.   | ITEM  | UNIT  | TOTAL QUANTITY |       |       |
|------------|---|-------|----------------|-------|-------|
| 44201037   | CLASS B PATCHES, TYPE IV, 15 INCH                   | SQ YD | 127            | 97    | 30    |
| 44201299   | DOWEL BARS 1 1/2"                                   | EACH  | 1,080          | 820   | 260   |
| 44213200   | SAW CUTS  | FOOT  | 2,464          | 1,904 | 560   |
| 44213204   | TIE BARS 3/4"                                       | EACH  | 38             | 27    | 11    |
| 48102100   | AGGREGATE WEDGE SHOULDER, TYPE B                    | TON   | 3,847          | 2,303 | 1,544 |
| 48203029   | HOT-MIX ASPHALT SHOULDERS, 8"                       | SQ YD | 3,143          | 1,568 | 1,575 |
| 48203100   | HOT-MIX ASPHALT SHOULDERS                           | TON   | 14,045         | 9072  | 4973  |
| * 50900200 | STEEL RAILING, TYPE 2399                            | FOOT  | 746.0          | 369.0 | 377.0 |
| 542A0217   | PIPE CULVERTS, CLASS A, TYPE 1 12"                  | FOOT  | 36             | 36    |       |
| 542A0223   | PIPE CULVERTS, CLASS A, TYPE 1 18"                  | FOOT  | 48             | 48    |       |
| 542D0241   | PIPE CULVERTS, CLASS D, TYPE 1 36"                  | FOOT  | 50             | 50    |       |
| 542JA018   | PIPE CULVERTS, CLASS A 18" (JACKED)                 | FOOT  | 175            | 175   |       |
| 54213657   | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12" | EACH  | 2              | 2     |       |
| 54213663   | PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18" | EACH  | 4              | 4     |       |

\* SPECIALTY ITEM

MODEL: Default  
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|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/29/2019 - 2:17:09 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|             |         |        |                    |           |
|-------------|---------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| I-74        | *       | **     | 142                | 7         |
|             |         |        | CONTRACT NO. 70790 |           |

\*(10-7,8,92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO. | ITEM  | UNIT  | TOTAL QUANTITY |       |       |
|----------|---|-------|----------------|-------|-------|
| 54248510 | CONCRETE COLLAR   | CU YD | 1.7            | 1.7   |       |
| 54262712 | METAL FLARED END SECTIONS 12"                           | EACH  | 7              | 7     |       |
| 54262736 | METAL FLARED END SECTIONS 36"                           | EACH  | 2              | 2     |       |
| 55200900 | STORM SEWERS JACKED IN PLACE, 24"                       | FOOT  | 220            | 220   |       |
| 60100060 | CONCRETE HEADWALLS FOR PIPE DRAINS                      | EACH  | 29             | 17    | 12    |
| 60100070 | SHOULDER REMOVAL AND REPLACEMENT                        | FOOT  | 5,097          | 2,590 | 2,507 |
| 60100915 | PIPE DRAINS 6"  | FOOT  | 848            | 508   | 340   |
| 60100945 | PIPE DRAINS 12"   | FOOT  | 108            | 108   |       |
| 60107600 | PIPE UNDERDRAINS 4"                                     | FOOT  | 4,800          | 2,400 | 2,400 |
| 60108100 | PIPE UNDERDRAINS 4" (SPECIAL)                           | FOOT  | 1,875          | 1,134 | 741   |
| 60108206 | PIPE UNDERDRAINS, TYPE 2, 6"                            | FOOT  | 8,975          | 6,244 | 2,731 |
| 60218400 | MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH  | 1              | 1     |       |
| 60223800 | MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID | EACH  | 2              | 2     |       |
| 60236200 | INLETS, TYPE A, TYPE 8 GRATE                            | EACH  | 12             | 7     | 5     |

\* SPECIALTY ITEM

MODEL: Default  
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|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/29/2019 - 2:17:14 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|             |         |        |                           |           |
|-------------|---------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| I-74        | *       | **     | 142                       | 8         |
|             |         |        | CONTRACT NO. 70790        |           |
|             |         |        | ILLINOIS FED. AID PROJECT |           |

\* (10-7,8,92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO.   | ITEM  | UNIT   | TOTAL QUANTITY |         |         |
|------------|---|--------|----------------|---------|---------|
| 60605000   | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24     | FOOT   | 7              | 7       |         |
| * 63000001 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS      | FOOT   | 3,950.0        | 1,950.0 | 2,000.0 |
| * 63100087 | TRAFFIC BARRIER TERMINAL, TYPE 6A                     | EACH   | 8              | 4       | 4       |
| * 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT    | EACH   | 10             | 6       | 4       |
| 63200310   | GUARDRAIL REMOVAL                                     | FOOT   | 4,837.5        | 2,487.5 | 2,350.0 |
| 63301210   | REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A | FOOT   | 950.0          | 475.0   | 475.0   |
| 63301990   | REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 1  | EACH   | 4              | 2       | 2       |
| 63302720   | REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 6B | EACH   | 4              | 2       | 2       |
| 64200116   | SHOULDER RUMBLE STRIPS, 16 INCH                       | FOOT   | 141,381        | 80,245  | 61,136  |
| * 64401100 | HIGH TENSION CABLE MEDIAN BARRIER                     | FOOT   | 5,891          | 5,891   |         |
| * 64401300 | HIGH TENSION CABLE MEDIAN BARRIER TERMINALS           | EACH   | 3              | 3       |         |
| 67000500   | ENGINEER'S FIELD OFFICE, TYPE B                       | CAL MO | 9              | 9       |         |
| 67100100   | MOBILIZATION  | L SUM  | 1              | 0.5     | 0.5     |
| 70100207   | TRAFFIC CONTROL AND PROTECTION, STANDARD 701402       | EACH   | 4              | 2       | 2       |

\* SPECIALTY ITEM

MODEL: Default  
FILE NAME: ...CAD\_Sheets\0570790-sh-500.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/29/2019 - 2:17:18 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|             |         |        |                           |           |
|-------------|---------|--------|---------------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| I-74        | *       | **     | 142                       | 9         |
|             |         |        | CONTRACT NO. 70790        |           |
|             |         |        | ILLINOIS FED. AID PROJECT |           |

\* (10-7,8,92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION  
REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO. | ITEM  | UNIT   | TOTAL QUANTITY | 0005 RURAL | 0005 RURAL |
|----------|---|--------|----------------|------------|------------|
| 70100420 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701411 | EACH   | 8              | 4          | 4          |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM  | 1              | 0.5        | 0.5        |
| 70100700 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701406 | L SUM  | 1              | 0.5        | 0.5        |
| 70100800 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 | L SUM  | 1              | 0.5        | 0.5        |
| 70100825 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701456 | L SUM  | 1              | 0.5        | 0.5        |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE                    | CAL DA | 275            | 138        | 137        |
| 70107025 | CHANGEABLE MESSAGE SIGN                         | CAL DA | 60             | 46         | 14         |
| 70300100 | SHORT TERM PAVEMENT MARKING                     | FOOT   | 11,635         | 6,655      | 4,980      |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL             | SQ FT  | 3,879          | 2,219      | 1,660      |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS  | SQ FT  | 172            | 86         | 86         |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4"            | FOOT   | 155,809        | 88,708     | 67,101     |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6"            | FOOT   | 748            | 401        | 347        |
| 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8"            | FOOT   | 3,198          | 1,610      | 1,588      |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12"           | FOOT   | 592            | 337        | 255        |

\* SPECIALTY ITEM

MODEL: Default  
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|------------------------------------|------------|-----------|
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| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/29/2019 - 2:17:23 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|-------------|---------|--------|---------------------------|-----------|
| I-74        | *       | **     | 142                       | 10        |
|             |         |        | CONTRACT NO. 70790        |           |
|             |         |        | ILLINOIS FED. AID PROJECT |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
|          | **                   | ***                  |
| COUNTY   | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO.   | ITEM  | UNIT  | TOTAL QUANTITY |         |         |
|------------|---|-------|----------------|---------|---------|
| 70300280   | TEMPORARY PAVEMENT MARKING - LINE 24"                                   | FOOT  | 144            | 72      | 72      |
| 70400100   | TEMPORARY CONCRETE BARRIER  | FOOT  | 3,362.5        | 1,687.5 | 1,675.0 |
| 70400200   | RELOCATE TEMPORARY CONCRETE BARRIER                                     | FOOT  | 3,112.5        | 1,562.5 | 1,550.0 |
| 70600260   | IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3 | EACH  | 4              | 2       | 2       |
| 70600332   | IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3  | EACH  | 4              | 2       | 2       |
| * 72501000 | TERMINAL MARKER - DIRECT APPLIED  | EACH  | 10             | 6       | 4       |
| * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS                    | SQ FT | 172            | 86      | 86      |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4"                                | FOOT  | 140,803        | 80,054  | 60,749  |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6"                                | FOOT  | 748            | 401     | 347     |
| * 78000500 | THERMOPLASTIC PAVEMENT MARKING - LINE 8"                                | FOOT  | 3,198          | 1,610   | 1,588   |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12"                               | FOOT  | 592            | 337     | 255     |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24"                               | FOOT  | 144            | 72      | 72      |
| * 78004210 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4"           | FOOT  | 15,006         | 8,654   | 6,352   |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER                                       | EACH  | 1,706          | 969     | 737     |

\* SPECIALTY ITEM

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|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/29/2019 - 2:17:27 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---------------------------|---------|--------|--------------------|-----------|
| I-74                      | *       | **     | 142                | 11        |
|                           |         |        | CONTRACT NO. 70790 |           |
| ILLINOIS FED. AID PROJECT |         |        |                    |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION  
 REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO.   | ITEM  | UNIT  | TOTAL QUANTITY | 0005 RURAL | 0005 RURAL |
|------------|---|-------|----------------|------------|------------|
| * 78200005 | GUARDRAIL REFLECTORS, TYPE A  | EACH  | 54             | 27         | 27         |
| 78300200   | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL   | EACH  | 1,706          | 969        | 737        |
| * 80400100 | ELECTRIC SERVICE INSTALLATION   | EACH  | 2              | 1          | 1          |
| * 81028350 | UNDERGROUND CONDUIT, PVC, 2" DIA.   | FOOT  | 325            | 325        |            |
| * 81603035 | UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE | FOOT  | 1,940          | 1,940      |            |
| 82110008   | LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION H                                     | EACH  | 24             | 14         | 10         |
| * 82500330 | LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240VOLT, 60AMP                             | EACH  | 2              | 1          | 1          |
| * 84500110 | REMOVAL OF LIGHTING CONTROLLER  | EACH  | 2              | 1          | 1          |
| * 84500120 | REMOVAL OF ELECTRIC SERVICE INSTALLATION  | EACH  | 2              | 1          | 1          |
| * 84500130 | REMOVAL OF LIGHTING CONTROLLER FOUNDATION   | EACH  | 2              | 1          | 1          |
| * 89502300 | REMOVE ELECTRIC CABLE FROM CONDUIT  | FOOT  | 5,820          | 5,820      |            |
| * X0320157 | CLEANING UNDERDRAIN OUTLETS   | EACH  | 145            | 87         | 58         |
| * X0325110 | BIAAXIAL GEOGRID  | SQ YD | 6,934          | 3,467      | 3,467      |
| * X0325379 | DIRECTIONAL BORING  | FOOT  | 842            | 502        | 340        |
| X0325667   | WEED CONTROL MOWING STRIP (SPECIAL)   | SQ YD | 2,032          | 2,032      |            |

\* SPECIALTY ITEM

MODEL: Default  
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|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
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| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|-------------|---------|--------|---------------------------|-----------|
| I-74        | *       | **     | 142                       | 12        |
|             |         |        | CONTRACT NO. 70790        |           |
|             |         |        | ILLINOIS FED. AID PROJECT |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION  
REV. 3/7/19



|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
|          | **                   | ***                  |
| COUNTY   | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO.   | ITEM  | UNIT   | TOTAL QUANTITY |        |        |
|------------|---|--------|----------------|--------|--------|
| * X0326677 | REMOVE HIGH TENSION CABLE MEDIAN BARRIER    | FOOT   | 4,571          | 4,571  |        |
| * X0327640 | REAL-TIME TRAFFIC CONTROL CENTRAL BASE UNIT | CAL MO | 9              | 9      |        |
| * X0327641 | REAL-TIME TRAFFIC CONTROL SENSOR UNIT       | CAL MO | 117            | 54     | 63     |
| X0327980   | PAVEMENT MARKING REMOVAL - WATER BLASTING   | SQ FT  | 55,347         | 31,263 | 24,084 |
| X0795800   | COARSE AGGREGATE                            | TON    | 1,363          | 948    | 415    |
| * X1400267 | REMOVAL OF LIGHTING LUMINAIRE, NO SALVAGE   | EACH   | 24             | 14     | 10     |
| X2700001   | TEMPORARY RUMBLE STRIPS (SPECIAL)           | EACH   | 32             | 16     | 16     |
| X4400196   | HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL    | SQ YD  | 204            | 125    | 79     |
| X4403300   | CONCRETE MEDIAN REMOVAL                     | SQ FT  | 176            |        | 176    |
| X6015000   | REMOVE CONCRETE HEADWALLS FOR PIPE DRAINS   | EACH   | 3              |        | 3      |
| X6061702   | CONCRETE MEDIAN, TYPE SM (DOWELLED)         | SQ FT  | 181            |        | 181    |
| X7040125   | PINNING TEMPORARY CONCRETE BARRIER          | EACH   | 1,554          | 780    | 774    |

\* SPECIALTY ITEM

MODEL: Default  
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|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
|-------------|---------|--------|---------------------------|-----------|
| I-74        | *       | **     | 142                       | 13        |
|             |         |        | CONTRACT NO. 70790        |           |
|             |         |        | ILLINOIS FED. AID PROJECT |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION  
 REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO.   | ITEM  | UNIT  | TOTAL QUANTITY |        |        |
|------------|---|-------|----------------|--------|--------|
| X7200201   | WIDTH RESTRICTION SIGNING                                   | L SUM | 1              | 0.5    | 0.5    |
| * X7830060 | GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS | SQ FT | 172            | 86     | 86     |
| * X7830070 | GROOVING FOR RECESSED PAVEMENT MARKING 5"                   | FOOT  | 140,803        | 80,054 | 60,749 |
| * X7830074 | GROOVING FOR RECESSED PAVEMENT MARKING 7"                   | FOOT  | 748            | 401    | 347    |
| * X7830076 | GROOVING FOR RECESSED PAVEMENT MARKING 9"                   | FOOT  | 3,198          | 1,610  | 1,588  |
| * X7830078 | GROOVING FOR RECESSED PAVEMENT MARKING 13"                  | FOOT  | 592            | 337    | 255    |
| * X7830090 | GROOVING FOR RECESSED PAVEMENT MARKING 25"                  | FOOT  | 144            | 72     | 72     |
| * XZ193300 | SURVEY MARKER, TYPE 1 (SPECIAL)                             | EACH  | 9              | 4      | 5      |
| * Z0008758 | AERIAL SPEED CHECK MARKING                                  | FOOT  | 144            | 72     | 72     |
| Z0013798   | CONSTRUCTION LAYOUT   | L SUM | 1              | 0.5    | 0.5    |
| Z0023600   | FILLING EXISTING CULVERTS                                   | EACH  | 1              | 1      |        |
| Z0025615   | FURNISH AND INSTALL METAL SCREEN                            | EACH  | 20             | 12     | 8      |
| Z0033700   | LONGITUDINAL JOINT SEALANT                                  | FOOT  | 64,136         | 36,650 | 27,486 |
| Z0034105   | MATERIAL TRANSFER DEVICE                                    | TON   | 37,129         | 21,463 | 15,666 |

\* SPECIALTY ITEM

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|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|             |         |        |                    |           |
|-------------|---------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| I-74        | *       | **     | 142                | 14        |
|             |         |        | CONTRACT NO. 70790 |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION  
 REV. 3/7/19

|          |                      |                      |
|----------|----------------------|----------------------|
| LOCATION | FAI 74 (I-74)        | FAI 74 (I-74)        |
|          | INTERSTATE           | INTERSTATE           |
| COUNTY   | **                   | ***                  |
|          | CHAMPAIGN            | VERMILION            |
| FUNDING  | 90% FED<br>10% STATE | 90% FED<br>10% STATE |
|          | ROADWAY              | ROADWAY              |
|          | 0005                 | 0005                 |
|          | RURAL                | RURAL                |

\*\* STA 880+00.00 TO STA 1053+06.00  
 \*\*\* STA 1053+06.00 TO STA 1180+00.00

| CODE NO.   | ITEM                               | UNIT | TOTAL QUANTITY | ROADWAY 0005 RURAL | ROADWAY 0005 RURAL |
|------------|------------------------------------|------|----------------|--------------------|--------------------|
| Z0040530   | PIPE UNDERDRAIN REMOVAL            | FOOT | 6,112          | 3,163              | 2,949              |
| ∅ Z0076600 | TRAINEES                           | HOUR | 2000           | 2000               |                    |
| Z0076870   | UNDERDRAIN CONNECTION TO STRUCTURE | EACH | 39             | 25                 | 14                 |
| ∅ Z0076604 | TRAINEES TRAINING PROGRAM GRADUATE | HOUR | 2000           | 2000               |                    |

∅ 0042

\* SPECIALTY ITEM

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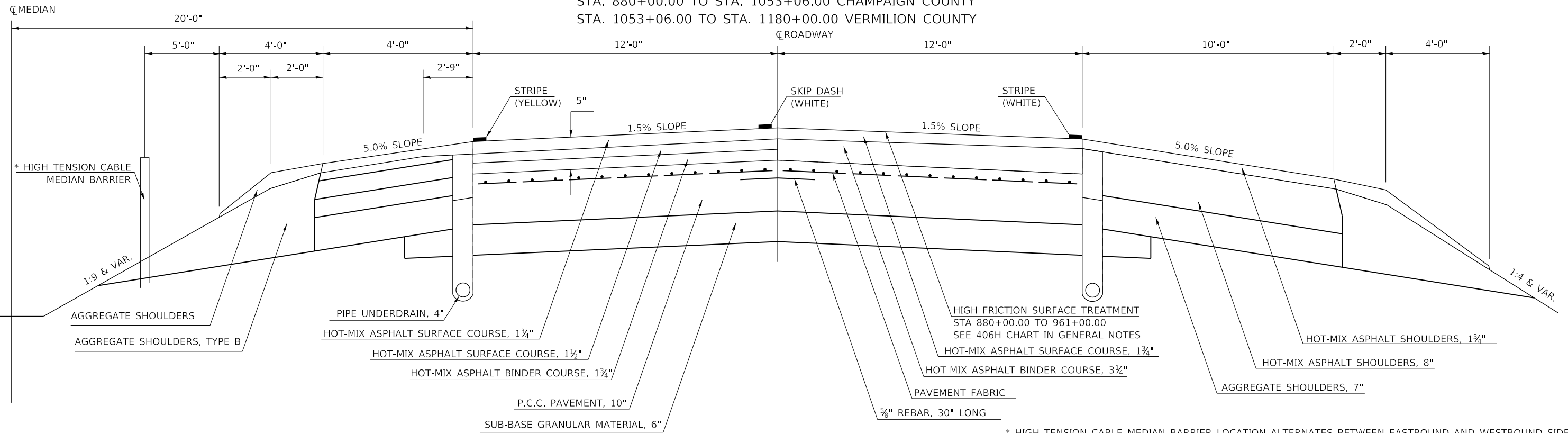
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| USER NAME = bemery                 | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>SUMMARY OF QUANTITIES</b>     |       |    |        | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS              | SHEET NO. |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |   |                                  |       |    |        | I-74        | *       | **     | 142                       | 15        |
| PLOT DATE = 1/29/2019 - 2:17:46 PM | CHECKED -  | REVISED + |   | SCALE: N.T.S.                    | SHEET | OF | SHEETS | STA.        | TO      | STA.   | ILLINOIS FED. AID PROJECT |           |
|                                    | DATE -     | REVISED + |   | * (10-7,8,92-8,9)RS-2(92-10)RS-3 |       |    |        |             |         |        |                           |           |

\*\* CHAMPAIGN & VERMILION  
REV. 3/7/19

## EXISTING TYPICAL CROSS SECTION

### F.A.I. 74

STA. 880+00.00 TO STA. 1053+06.00 CHAMPAIGN COUNTY  
 STA. 1053+06.00 TO STA. 1180+00.00 VERMILION COUNTY



\* HIGH TENSION CABLE MEDIAN BARRIER LOCATION ALTERNATES BETWEEN EASTBOUND AND WESTBOUND SIDE

## PROPOSED TYPICAL CROSS SECTION

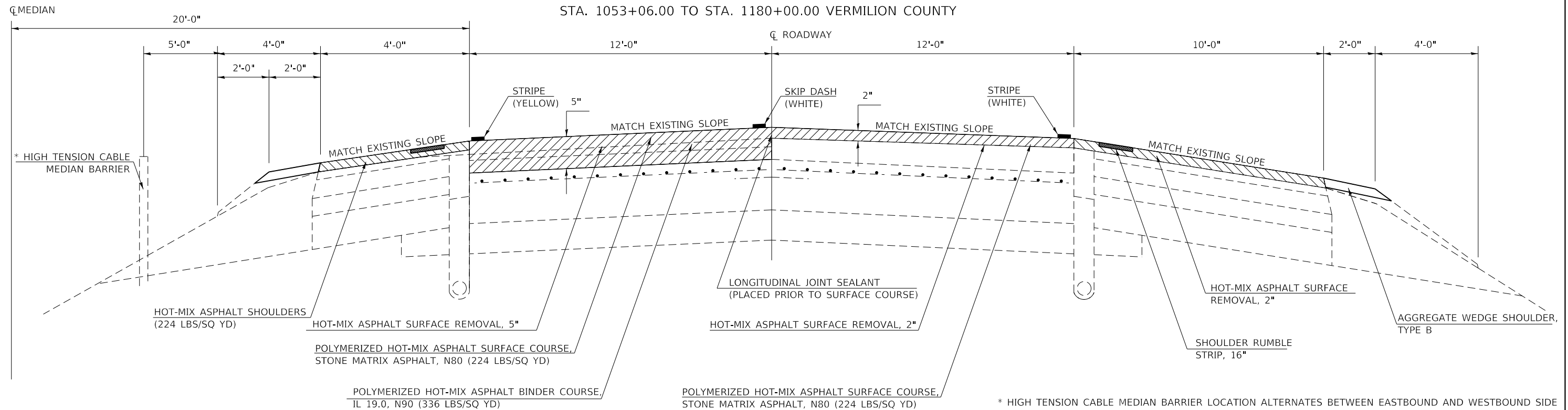
### F.A.I. 74

STA. 880+00.00 TO STA. 979+40.75 CHAMPAIGN COUNTY  
 STA. 1025+15.08 TO STA. 1053+06.00 CHAMPAIGN COUNTY  
 STA. 1053+06.00 TO STA. 1180+00.00 VERMILION COUNTY

①

POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,  
 STONE MATRIX ASPHALT, N80 (224 LBS/SQ YD) LIMITS:  
 MIX "F" - STA 880+00.00 TO STA 961+00.00  
 MIX "E" - STA 961+00.00 TO STA 1180+00.00

FULL-DEPTH HMA OMISSION (SEE DETAIL SHEETS):  
 STA. 898+29.00 TO STA. 904+29.00 CHAMPAIGN COUNTY  
 STA. 1104+30.18 TO STA. 1110+30.18 VERMILION COUNTY



\* HIGH TENSION CABLE MEDIAN BARRIER LOCATION ALTERNATES BETWEEN EASTBOUND AND WESTBOUND SIDE

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL CROSS SECTIONS**

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
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| PLOT DATE = 1/24/2019 - 3:55:26 PM | DATE -     | REVISED - |

|                    |                 |              |                           |           |
|--------------------|-----------------|--------------|---------------------------|-----------|
| F.A.I. RTE.        | SECTION         | COUNTY       | TOTAL SHEETS              | SHEET NO. |
| I-74               | *               | **           | 142                       | 16        |
| CONTRACT NO. 70790 |                 |              |                           |           |
| SCALE: N.T.S.      | SHEET OF SHEETS | STA. TO STA. | ILLINOIS FED. AID PROJECT |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

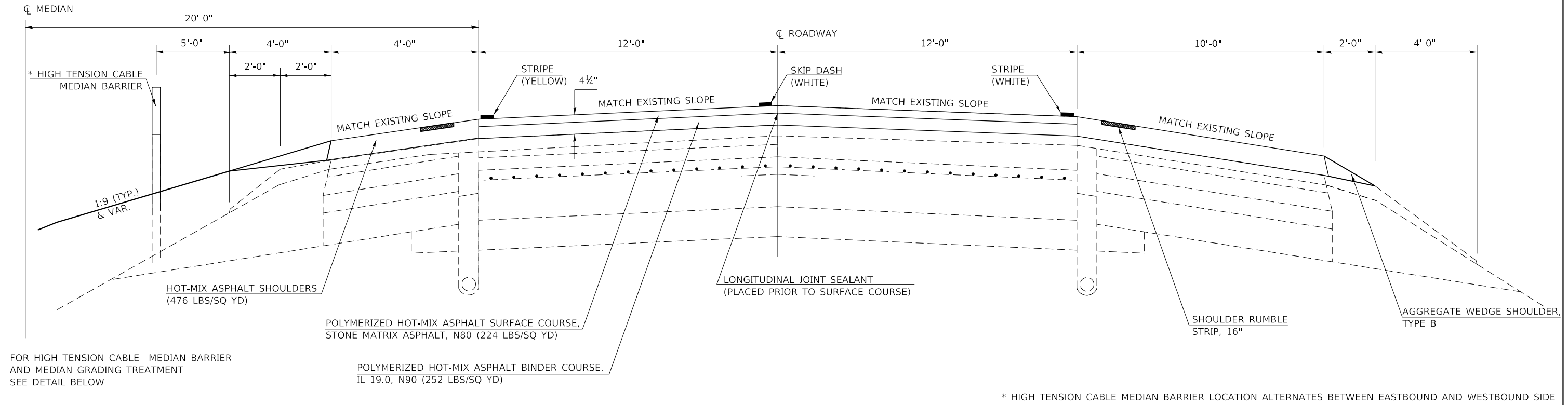
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# PROPOSED TYPICAL CROSS SECTION

## F.A.I. 74

2

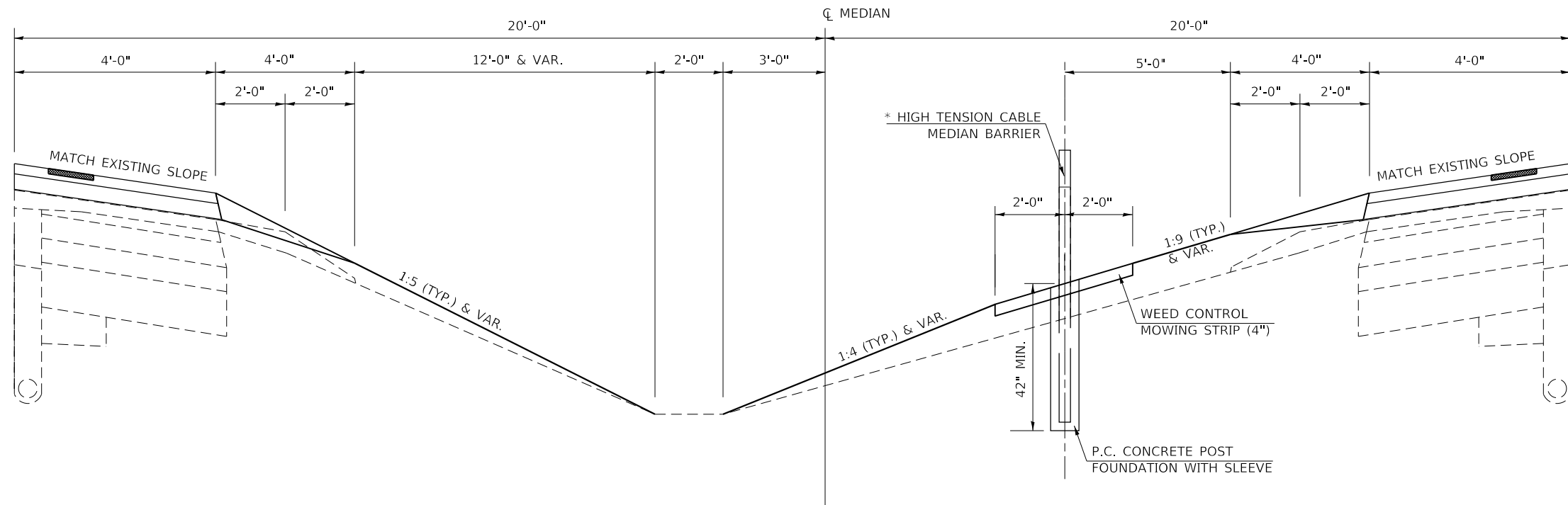
STA. 979+40.75 TO STA. 1025+15.08 CHAMPAIGN COUNTY



# PROPOSED MEDIAN TREATMENT

## F.A.I. 74

STA. 979+40.75 TO STA. 1025+15.08 CHAMPAIGN COUNTY



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|                                    | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

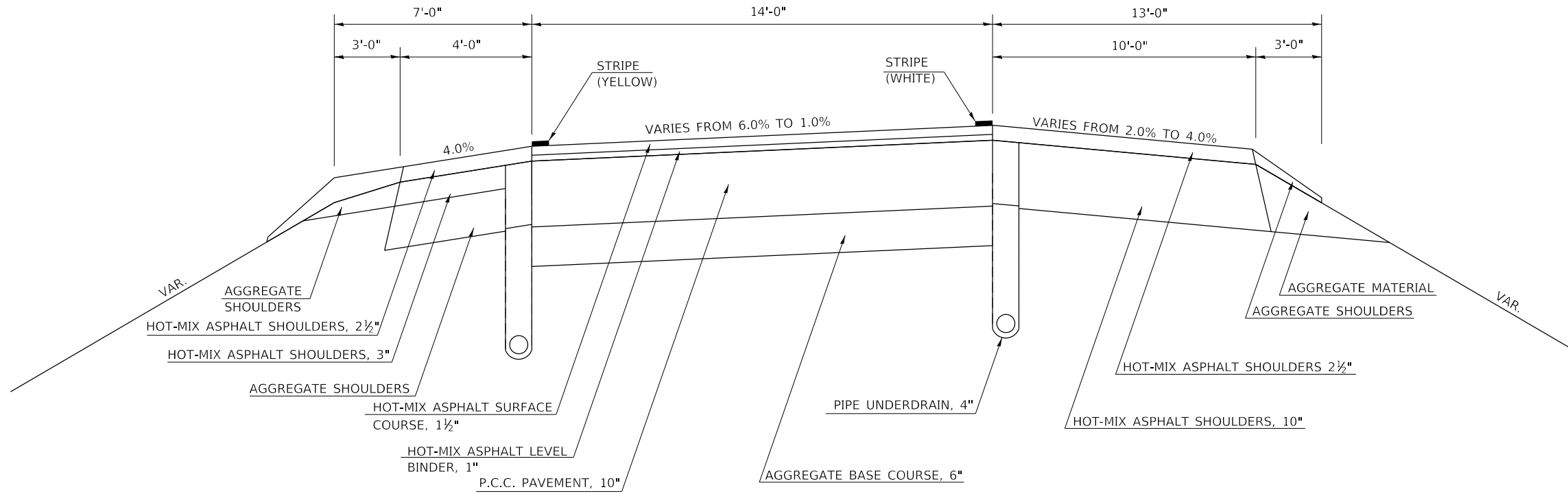
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| SCALE: N.T.S. |  | SHEET OF SHEETS |  | STA. | TO STA. |
|---------------|--|-----------------|--|------|---------|

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 17        |
| CONTRACT NO. 70790 |         |        |              |           |

## EXISTING TYPICAL CROSS SECTION

### F.A.I. 74 RAMPS

F.A.I. 74 & F.A.S. 518/CH 22 INTERCHANGE RAMPS  
F.A.I. 74 & IL 49 INTERCHANGE RAMPS

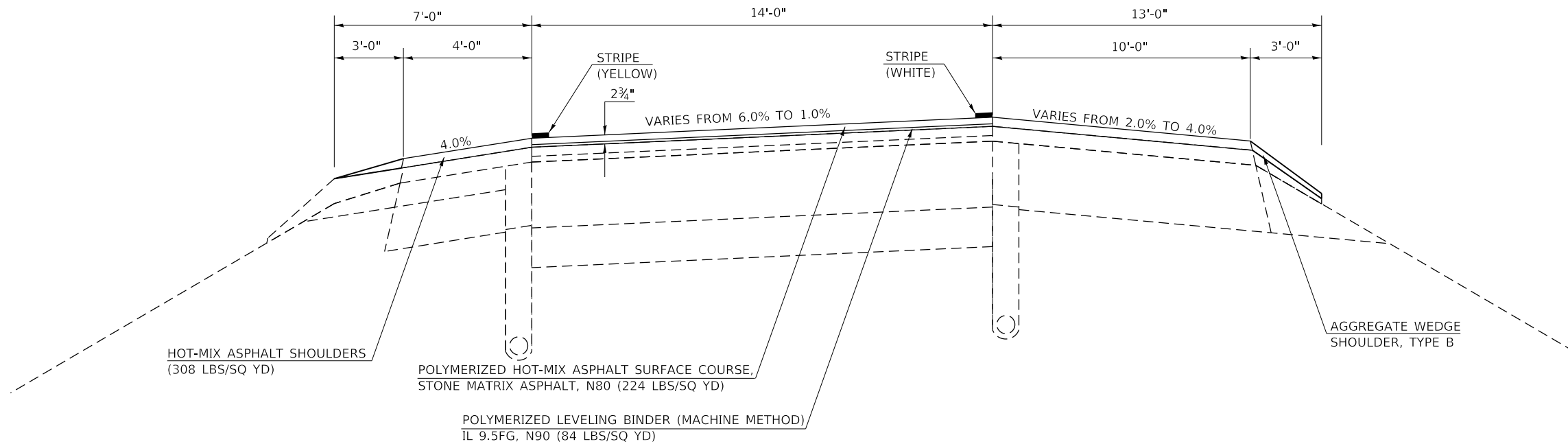


## PROPOSED TYPICAL CROSS SECTION

### F.A.I. 74 RAMPS

3

F.A.I. 74 & F.A.S. 518/CH 22 INTERCHANGE RAMPS  
RAMP A STA. 389+76.72 TO STA. 400+99.56  
RAMP B STA. 299+58.16 TO STA. 312+00.65  
RAMP C STA. 489+58.29 TO STA. 501+01.15  
RAMP D STA. 100+45.96 TO STA. 112+58.99



MODEL: Default  
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| PLOT SCALE = 40.0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:55:26 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                               |                              |
|-------------------------------|------------------------------|
| <b>TYPICAL CROSS SECTIONS</b> |                              |
| SCALE: N.T.S.                 | SHEET OF SHEETS STA. TO STA. |

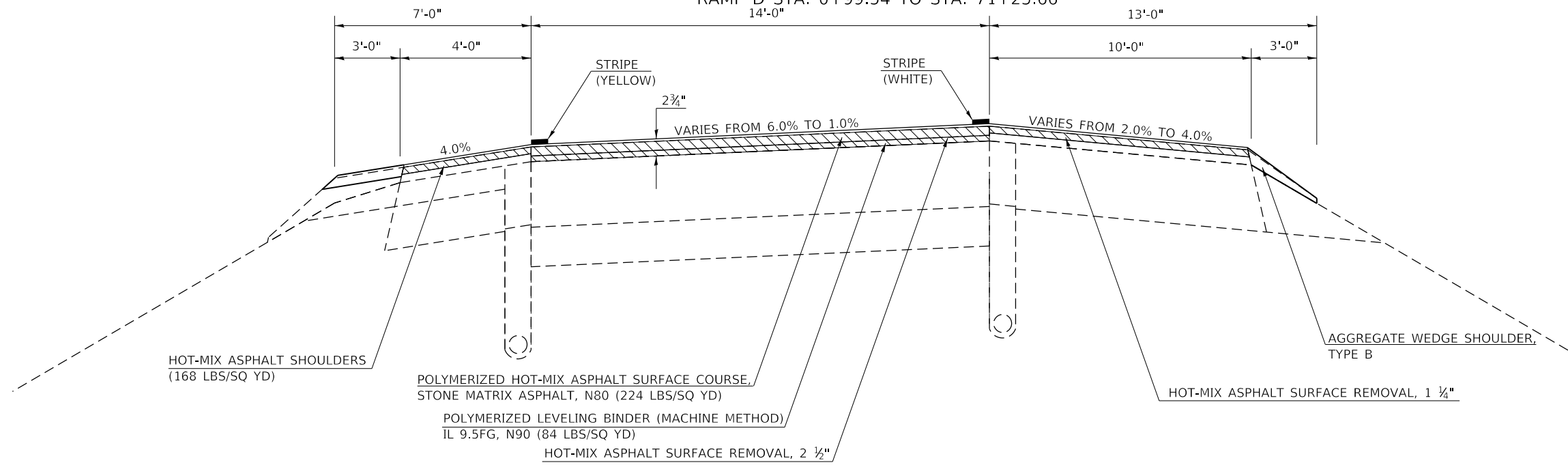
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| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 18        |
| CONTRACT NO. 70790 |         |        |              |           |

# PROPOSED TYPICAL CROSS SECTION

## F.A.I. 74 RAMPS

4

F.A.I. 74 & IL 49 INTERCHANGE RAMPS  
 RAMP A STA. 49+00.76 TO STA. 61+13.65  
 RAMP B STA. 63+35.52 TO STA. 74+99.75  
 RAMP C STA. 48+92.74 TO STA. 60+78.08  
 RAMP D STA. 0+99.54 TO STA. 71+25.66



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| PLOT DATE = 1/24/2019 - 3:55:27 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL CROSS SECTIONS**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|                     |              |              |                     |                 |
|---------------------|--------------|--------------|---------------------|-----------------|
| F.A.I. RTE.<br>I-74 | SECTION<br>* | COUNTY<br>** | TOTAL SHEETS<br>142 | SHEET NO.<br>19 |
|                     |              |              | CONTRACT NO. 70790  |                 |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

# EROSION CONTROL SCHEDULE

| LOCATION<br>STATION TO STATION  | OFFSET | SIDE | EROSION CONTROL<br>BLANKET | TEMPORARY<br>DITCH<br>CHECKS | PERIMETER<br>EROSION<br>BARRIER | INLET & PIPE<br>PROTECTION | AGGREGATE<br>(EROSION<br>CONTROL) |
|---------------------------------|--------|------|----------------------------|------------------------------|---------------------------------|----------------------------|-----------------------------------|
|                                 |        |      | 25100830<br>SQ YD          | 28000305<br>FOOT             | 28000400<br>FOOT                | 28000500<br>EACH           | 28001000<br>TON                   |
| <b>CHAMPAIGN COUNTY</b>         |        |      |                            |                              |                                 |                            |                                   |
| <b>FAS 518/CH 22</b>            |        |      |                            |                              |                                 |                            |                                   |
| 34+17.00 TO 39+65.00            |        | RT   |                            |                              | 617                             |                            |                                   |
| 34+30.00                        | 35     | RT   |                            |                              |                                 | 1                          |                                   |
| 34+80.00                        | 35     | RT   |                            | 25                           |                                 |                            |                                   |
| 35+81.00                        | 34     | RT   |                            | 25                           |                                 |                            |                                   |
| 36+81.00                        | 33     | RT   |                            | 25                           |                                 |                            |                                   |
| 37+81.00                        | 32     | RT   |                            | 25                           |                                 |                            |                                   |
| 38+81.00                        | 31     | RT   |                            | 25                           |                                 |                            |                                   |
| 39+36.00 TO 39+55.00            |        | RT   |                            |                              |                                 |                            | 6                                 |
| 40+01.00 TO 43+31.00            |        | RT   |                            |                              | 541                             |                            |                                   |
| 40+15.00                        | 29     | RT   |                            |                              |                                 | 1                          |                                   |
| 40+52.00                        | 29     | RT   |                            | 25                           |                                 |                            |                                   |
| 41+49.00                        | 52     | RT   |                            | 25                           |                                 |                            |                                   |
| 41+91.00 TO 42+04.00            |        | RT   |                            |                              |                                 |                            | 2                                 |
| 42+00.00                        | 137    | RT   |                            | 25                           |                                 |                            |                                   |
| 42+37.00                        | 231    | RT   |                            | 25                           |                                 |                            |                                   |
| 42+61.00                        | 95     | RT   |                            |                              |                                 | 1                          |                                   |
| 42+64.00                        | 116    | RT   |                            | 25                           |                                 |                            |                                   |
| 42+67.10 TO 49+11.00            |        | RT   |                            | 650                          |                                 |                            |                                   |
| 42+67.26 TO 49+04.00            |        | RT   |                            | 650                          |                                 |                            |                                   |
| 42+89.00 TO 49+36.00            |        | RT   | 3,354                      |                              |                                 |                            |                                   |
| 42+90.00                        | 315    | RT   |                            | 25                           |                                 |                            |                                   |
| 43+01.00                        | 68     | RT   |                            | 25                           |                                 |                            |                                   |
| 43+06.00 TO 44+79.00            |        | LT   |                            |                              | 210                             |                            |                                   |
| 43+12.00                        | 251    | RT   |                            | 25                           |                                 |                            |                                   |
| 43+24.00                        | 44     | LT   |                            | 25                           |                                 |                            |                                   |
| 43+29.00 TO 44+83.00            |        | LT   | 442                        |                              |                                 |                            |                                   |
| 43+48.00 TO 49+19.00            |        | RT   |                            |                              | 908                             |                            |                                   |
| 43+68.00                        | 337    | RT   |                            | 25                           |                                 |                            |                                   |
| 44+00.00                        |        | LT   | 4                          |                              |                                 |                            |                                   |
| 44+02.00                        | 67     | RT   |                            | 25                           |                                 |                            |                                   |
| 44+41.00                        | 405    | RT   |                            | 25                           |                                 |                            |                                   |
| 45+00.00                        | 85     | RT   |                            | 25                           |                                 |                            |                                   |
| 45+18.00                        | 469    | RT   |                            | 25                           |                                 |                            |                                   |
| 45+27.00                        | 479    | RT   |                            |                              |                                 | 1                          |                                   |
| 46+02.00                        | 89     | RT   |                            | 25                           |                                 |                            |                                   |
| 47+00.00                        | 94     | RT   |                            | 25                           |                                 |                            |                                   |
| 47+99.00                        | 96     | RT   |                            | 25                           |                                 |                            |                                   |
| 48+99.00                        | 102    | RT   |                            | 25                           |                                 |                            |                                   |
| 50+56.00 TO 57+23.00            |        | RT   | 4,376                      |                              |                                 |                            |                                   |
| 50+85.00 TO 54+40.00            |        | RT   |                            |                              | 450                             |                            |                                   |
| 50+89.29 TO 57+03.00            |        | RT   |                            | 660                          |                                 |                            |                                   |
| 50+89.64 TO 56+90.00            |        | RT   |                            | 650                          |                                 |                            |                                   |
| 50+91.00                        | 185    | RT   |                            |                              |                                 | 1                          |                                   |
| 51+10.00                        | 348    | RT   |                            | 25                           |                                 |                            |                                   |
| 51+66.00                        | 431    | RT   |                            | 25                           |                                 |                            |                                   |
| 52+53.00                        | 461    | RT   |                            | 25                           |                                 |                            |                                   |
| 53+53.00                        | 461    | RT   |                            | 25                           |                                 |                            |                                   |
| 54+53.00                        | 464    | RT   |                            | 25                           |                                 |                            |                                   |
| 55+38.00                        | 415    | RT   |                            | 25                           |                                 |                            |                                   |
| 55+97.00                        | 330    | RT   |                            | 25                           |                                 |                            |                                   |
| 56+51.00                        | 243    | RT   |                            | 25                           |                                 |                            |                                   |
| 56+84.00                        | 149    | RT   |                            | 25                           |                                 |                            |                                   |
| 57+05.00                        | 51     | RT   |                            | 25                           |                                 |                            |                                   |
| <b>I-74</b>                     |        |      |                            |                              |                                 |                            |                                   |
| 883+77.00                       |        | RT   | 4                          |                              |                                 |                            |                                   |
| 916+08.50                       |        | RT   | 4                          |                              |                                 |                            |                                   |
| 930+19.00                       |        | RT   | 4                          |                              |                                 |                            |                                   |
| 974+81.00                       |        | LT   | 4                          |                              |                                 |                            |                                   |
| 977+02.00                       |        | LT   | 4                          |                              |                                 |                            |                                   |
| 980+06.00                       |        | LT   | 4                          |                              |                                 |                            |                                   |
| 983+06.12                       |        | RT   | 4                          |                              |                                 |                            |                                   |
| 898+29.00 TO 904+29.00          |        | LT   | 8                          |                              |                                 |                            |                                   |
| 898+29.00 TO 904+29.00          |        | RT   | 8                          |                              |                                 |                            |                                   |
| <b>Total (Champaign County)</b> |        |      | <b>8,220</b>               | <b>3,435</b>                 | <b>2,726</b>                    | <b>5</b>                   | <b>8</b>                          |
| <b>VERMILION COUNTY</b>         |        |      |                            |                              |                                 |                            |                                   |
| <b>I-74</b>                     |        |      |                            |                              |                                 |                            |                                   |
| 1056+18.00                      |        | LT   | 4                          |                              |                                 |                            |                                   |
| 1058+96.00                      |        | LT   | 4                          |                              |                                 |                            |                                   |
| 1100+99.00                      |        | LT   | 4                          |                              |                                 |                            |                                   |
| 1129+12.00                      |        | RT   | 4                          |                              |                                 |                            |                                   |
| 1142+78.00                      |        | LT   | 4                          |                              |                                 |                            |                                   |
| 1104+30.18 TO 1110+30.18        |        | RT   | 8                          |                              |                                 |                            |                                   |
| 1104+30.18 TO 1110+30.18        |        | LT   | 8                          |                              |                                 |                            |                                   |
| <b>Total (Vermilion County)</b> |        |      | <b>36</b>                  |                              |                                 |                            |                                   |
| <b>PROJECT TOTAL</b>            |        |      | <b>8,256</b>               | <b>3,435</b>                 | <b>2,726</b>                    | <b>5</b>                   | <b>8</b>                          |

# TEMPORARY CONCRETE BARRIER

| LOCATION<br>STATION TO STATION  | COMMENT | DESCRIPTON               | TEMPORARY<br>CONCRETE<br>BARRIER | RELOCATE<br>TEMPORARY<br>CONCRETE<br>BARRIER | IMPACT<br>ATTENUATORS,<br>TEMPORARY<br>(FULLY-REDIRECTIVE<br>NARROW)<br>TEST LEVEL 3 | IMPACT<br>ATTENUATORS,<br>RELOCATE<br>(FULLY-REDIRECTIVE<br>NARROW)<br>TEST LEVEL 3 | PINNING<br>TEMPORARY<br>CONCRETE<br>BARRIER |
|---------------------------------|---------|--------------------------|----------------------------------|--|--|---|---|
|                                 |         |                          | 70400100<br>FOOT                 | 70400200<br>FOOT                             | 70600260<br>EACH   | 7060332<br>EACH   | X7040125<br>EACH                            |
| <b>CHAMPAIGN COUNTY</b>         |         |                          |                                  |  |  |   |   |
| <b>I-74</b>                     |         |                          |                                  |  |  |   |   |
| 896+75.00 TO 904+50.00          | I-74 EB | SN 010-0183, PRE-STAGE B | 775.0                            |  | 1  |   | 186   |
| 898+02.50 TO 905+90.00          | I-74 WB | SN 010-0183, PRE-STAGE B | 787.5                            |  | 1  |   | 189   |
| 896+12.50 TO 904+50.00          | I-74 EB | SN 010-0183, PRE-STAGE C | 62.5                             | 775.0  |  | 1   | 201   |
| 898+02.50 TO 906+52.50          | I-74 WB | SN 010-0183, PRE-STAGE C | 62.5                             | 787.5  |  | 1   | 204   |
| <b>Total (Champaign County)</b> |         |                          | <b>1687.5</b>                    | <b>1562.5</b>                                | <b>2</b>   | <b>2</b>  | <b>780</b>                                  |
| <b>VERMILION COUNTY</b>         |         |                          |                                  |  |  |   |   |
| <b>I-74</b>                     |         |                          |                                  |  |  |   |   |
| 1102+75.00 TO 1110+50.00        | I-74 EB | SN 092-0146, PRE-STAGE B | 775.0                            |  | 1  |   | 186   |
| 1104+05.00 TO 1111+80.00        | I-74 WB | SN 092-0146, PRE-STAGE B | 775.0                            |  | 1  |   | 186   |
| 1102+12.50 TO 1110+50.00        | I-74 EB | SN 092-0146, PRE-STAGE C | 62.5                             | 775.0  |  | 1   | 201   |
| 1104+05.00 TO 1112+42.50        | I-74 WB | SN 092-0146, PRE-STAGE C | 62.5                             | 775.0  |  | 1   | 201   |
| <b>Total (Vermilion County)</b> |         |                          | <b>1675.0</b>                    | <b>1550.0</b>                                | <b>2</b>   | <b>2</b>  | <b>774</b>                                  |
| <b>PROJECT TOTAL</b>            |         |                          | <b>3,362.5</b>                   | <b>3,112.5</b>                               | <b>4</b>   | <b>4</b>  | <b>1,554</b>                                |

# SEEDING

| LOCATION<br>STATION TO STATION | SIDE | SEEDING,<br>CLASS 2A | SEEDING,<br>CLASS 4A | SEEDING,<br>CLASS 4B | SEEDING,<br>CLASS 7 | MULCH,<br>METHOD 2 | TEMPORARY<br>EROSION<br>CONTROL<br>SEEDING | NITROGEN<br>FERTILIZER<br>NUTRIENT | PHOSPHORUS<br>FERTILIZER<br>NUTRIENT | POTASSIUM<br>FERTILIZER<br>NUTRIENT |
|--------------------------------|------|----------------------|----------------------|----------------------|---------------------|--------------------|--|------------------------------------|--------------------------------------|-------------------------------------|
|                                |      | 25000210<br>ACRE     | 25000312<br>ACRE     | 25000314<br>ACRE     | 25000350<br>ACRE    | 25100115<br>ACRE   | 28000250<br>POUND                          | 25000400<br>POUND                  | 25000500<br>POUND                    | 25000600<br>POUND                   |
| <b>CHAMPAIGN COUNTY</b>        |      |                      |                      |                      |                     |                    |  |                                    |                                      |                                     |
| <b>FAS 518/CH 22</b>           |      |                      |                      |                      |                     |                    |  |                                    |                                      |                                     |
| 34+19.00 TO 39+71.00           | RT   | 0.18                 | 0.25                 |                      | 0.50                | 0.43               | 180  | 39                                 | 39                                   | 39                                  |
| 40+02.00 TO 43+29.00           | RT   | 0.21                 | 0.15                 |                      | 0.50                | 0.36               | 150  | 32                                 | 32                                   | 32                                  |
| 42+60.00 TO 48+95.00           | RT   |                      | 3.11                 |                      | 4.25                | 3.11               | 1250                                       | 280                                | 280                                  | 280                                 |
| 42+45.00 TO 45+66.00           | RT   | 0.26                 |                      |                      |                     | 0.26               | 110  | 23                                 | 23                                   | 23                                  |
| 42+67.26 TO 49+04.00           | RT   | 0.70                 |                      |                      |                     | 0.70               | 280  | 63                                 | 63                                   | 63                                  |
| 43+29.00 TO 44+84.00           | LT   | 0.12                 |                      |                      | 0.25                | 0.12               | 50   | 11                                 | 11                                   | 11                                  |
| 48+95.00 TO 49+36.00           | RT   | 0.13                 |                      |                      |                     | 0.13               | 60   | 12                                 | 12                                   | 12                                  |
| 50+56.00 TO 50+95.00           | RT   | 0.03                 |                      |                      |                     | 0.03               | 20   | 3                                  | 3                                    | 3                                   |
| 50+56.00 TO 57+23.00           | RT   | 0.91                 |                      |                      |                     | 0.91               | 370  | 82                                 | 82                                   | 82                                  |
| 50+90.00 TO 57+36.00           | RT   | 0.24                 | 1.43                 | 3.27                 | 6.00                | 4.94               | 1980                                       | 445                                | 445                                  | 445                                 |
| <b>PROJECT TOTAL</b>           |      | <b>3.00</b>          | <b>5.00</b>          | <b>3.50</b>          | <b>11.50</b>        | <b>11.00</b>       | <b>4,450</b>                               | <b>990</b>                         | <b>990</b>                           | <b>990</b>                          |

MODEL: \\MODELS\BAMES  
FILE: \\MODELS\BAMES

|                       |                  |           |
|-----------------------|------------------|-----------|
| USER NAME = \$USERS   | DESIGNED - BCD   | REVISED - |
| PLOT SCALE = \$SCALES | DRAWN - GDC      | REVISED - |
| PLOT DATE = \$DATES   | CHECKED - BCD    | REVISED - |
|                       | DATE - 9/25/2018 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**I-74 SCHEDULES**

SCALE: SHEET OF SHEETS STA. TO STA.

|                           |         |        |                 |              |
|---------------------------|---------|--------|-----------------|--------------|
| F.A.I.<br>RTE.            | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
| I-74                      | *       | **     | 142             | 20           |
| CONTRACT NO.              |         |        |                 |              |
| ILLINOIS FED. AID PROJECT |         |        |                 |              |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



# REMOVAL

| LOCATION<br>STATION TO STATION | COMMENT    | TREE<br>REMOVAL<br>(6 TO 15<br>UNITS OF<br>DIAMETER) | TREE<br>REMOVAL<br>(OVER 15<br>UNITS OF<br>DIAMETER) | PAVEMENT<br>REMOVAL | PAVED<br>SHOULDER<br>REMOVAL | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL-<br>BUTT JOINT | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>1 1/4" | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>1 3/4" | HOT-MIX ASPHALT SURFACE<br>REMOVAL, 2" |          | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>2 1/2" | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>5" | CONCRETE<br>MEDIAN<br>REMOVAL | GUARDRAIL<br>REMOVAL |
|--------------------------------|------------|--|--|---------------------|------------------------------|---|---|---|--|----------|---|---|-------------------------------|----------------------|
|                                |            | 20100110   | 20100210   | 44000100            | 44004250                     | 40600982  | 44000154  | 44000156  | PAVEMENT                               | SHOULDER | 44000159  | 44000169  | X4403300                      | 63200310             |
|                                |            | UNIT   | UNIT   | SQ YD               | SQ YD                        | SQ YD   | SQ YD   | SQ YD   | SQ YD                                  | SQ YD    | SQ YD   | SQ YD   | SQ FT                         | FOOT                 |
| <b>CHAMPAIGN COUNTY</b>        |            |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| <b>I-74 EB</b>                 |            |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 998+68.00                      | RT         | 15.0   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 998+70.00                      | RT         |  | 26.0   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 998+77.00                      | RT         |  | 40.0   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 998+90.00                      | RT         |  | 18.0   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 880+00.00 TO 898+29.00         | EB INSIDE  |  |  |                     |                              |   |   |   |  | 852.0    |   | 2,438.0   |                               |                      |
| 897+74.00 TO 904+84.00         | EB INSIDE  |  |  |                     |                              |   |   | 313.4   |  |          |   |   |                               |                      |
| 898+29.00 TO 904+29.00         | EB INSIDE  |  |  | 802.5               | 264.2                        |   |   |   |  |          |   |   |                               |                      |
| 904+29.00 TO 979+40.75         | EB INSIDE  |  |  |                     |                              |   |   |   |  | 3346.0   |   | 10,038.7  |                               |                      |
| 978+53.97 TO 980+23.97         | EB INSIDE  |  |  |                     |                              | 302.2   |   |   |  |          |   |   |                               |                      |
| 979+40.75 TO 1025+15.08        | EB INSIDE  |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 1023+45.08 TO 1025+15.08       | EB INSIDE  |  |  |                     |                              | 302.2   |   |   |  |          |   |   |                               |                      |
| 1025+15.08 TO 1053+06.00       | EB INSIDE  |  |  |                     |                              |   |   |   |  | 1308.9   |   | 3,854.4   |                               |                      |
| 880+00.00 TO 898+29.00         | EB OUTSIDE |  |  |                     |                              |   |   |   | 2,471.2                                | 1,994.1  |   |   |                               |                      |
| 898+29.00 TO 904+29.00         | EB OUTSIDE |  |  | 806.8               | 665.4                        |   |   |   |  |          |   |   |                               |                      |
| 904+29.00 TO 979+40.75         | EB OUTSIDE |  |  |                     |                              |   |   |   | 10,206.4                               | 8,291.8  |   |   |                               |                      |
| 978+53.97 TO 979+33.97         | EB OUTSIDE |  |  |                     |                              | 195.6   |   |   |  |          |   |   |                               |                      |
| 1001+70.00 TO 1025+15.08       | EB OUTSIDE |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 1024+35.08 TO 1025+15.08       | EB OUTSIDE |  |  |                     |                              | 195.6   |   |   |  |          |   |   |                               |                      |
| 1025+15.08 TO 1053+06.00       | EB OUTSIDE |  |  |                     |                              |   |   |   | 3,762.2                                | 3,099.0  |   |   |                               |                      |
| <b>I-74 WB</b>                 |            |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 880+00.00 TO 898+29.00         | WB INSIDE  |  |  |                     |                              |   |   |   |  | 815.7    |   | 2,430.5   |                               |                      |
| 897+74.00 TO 904+84.00         | WB INSIDE  |  |  |                     |                              |   |   | 298.5   |  |          |   |   |                               |                      |
| 898+29.00 TO 904+29.00         | WB INSIDE  |  |  | 806.4               | 251.8                        |   |   |   |  |          |   |   |                               |                      |
| 904+29.00 TO 979+40.75         | WB INSIDE  |  |  |                     |                              |   |   |   |  | 3267.7   |   | 9,961.3   |                               |                      |
| 978+53.97 TO 980+23.97         | WB INSIDE  |  |  |                     |                              | 302.2   |   |   |  |          |   |   |                               |                      |
| 979+40.75 TO 1025+15.08        | WB INSIDE  |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 1023+45.08 TO 1025+15.08       | WB INSIDE  |  |  |                     |                              | 302.2   |   |   |  |          |   |   |                               |                      |
| 1025+15.08 TO 1053+06.00       | WB INSIDE  |  |  |                     |                              |   |   |   |  | 1183.2   |   | 3,706.7   |                               |                      |
| 880+00.00 TO 898+29.00         | WB OUTSIDE |  |  |                     |                              |   |   |   | 2,455.3                                | 1,970.6  |   |   |                               |                      |
| 898+29.00 TO 904+29.00         | WB OUTSIDE |  |  | 801.3               | 653.2                        |   |   |   |  |          |   |   |                               |                      |
| 904+29.00 TO 979+40.75         | WB OUTSIDE |  |  |                     |                              |   |   |   | 10,172.9                               | 8,038.4  |   |   |                               |                      |
| 978+12.00 TO 1000+38.00        | WB OUTSIDE |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 978+53.97 TO 979+33.97         | WB OUTSIDE |  |  |                     |                              | 195.6   |   |   |  |          |   |   |                               |                      |
| 1024+35.08 TO 1025+15.08       | WB OUTSIDE |  |  |                     |                              | 195.6   |   |   |  |          |   |   |                               |                      |
| 1025+15.08 TO 1053+06.00       | WB OUTSIDE |  |  |                     |                              |   |   |   | 3,820.5                                | 3,061.7  |   |   |                               |                      |
| <b>CH 22</b>                   |            |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 42+99.50                       | LT         |  |  |                     |                              |   |   |   |  |          |   |   |                               | 112.5                |
| 44+27.00                       | RT         | 14   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 44+86.00                       | RT         |  | 18   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 45+13.00                       | RT         |  | 16   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 45+76.00                       | RT         | 10   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 47+93.00                       | RT         | 12   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 48+13.00                       | RT         |  | 16   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 48+34.00                       | RT         | 12   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 48+65.00                       | RT         |  | 16   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 48+78.00                       | RT         | 14   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 48+86.00                       | RT         | 12   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 51+59.00                       | RT         |  | 48   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 52+32.00                       | RT         |  | 32   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 52+53.00                       | RT         | 15   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 52+80.00                       | RT         |  | 34   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 53+63.00                       | RT         | 12   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 55+76.00                       | RT         |  |  |                     |                              |   |   |   |  |          |   |   |                               | 125.0                |

SCHEDULE CONTINUES ON NEXT SHEET

MODEL: Default  
FILE: Model - 10/27/2019 3:55:54 PM

|                                     |            |           |
|-------------------------------------|------------|-----------|
| USER NAME = bemory                  | DESIGNED - | REVISED - |
|                                     | DRAWN -    | REVISED - |
| PLOT SCALE = 1/4" = 100.0000' / in. | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:55:54 PM  | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |       |      |        |
|------------------|-------|------|--------|
| <b>SCHEDULES</b> |       |      |        |
| SCALE: N.T.S.    | SHEET | OF   | SHEETS |
| STA.             | TO    | STA. | TO     |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 21        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

# REMOVAL

| LOCATION<br>STATION TO STATION        | COMMENT      | TREE<br>REMOVAL<br>(6 TO 15<br>UNITS OF<br>DIAMETER) | TREE<br>REMOVAL<br>(OVER 15<br>UNITS OF<br>DIAMETER) | PAVEMENT<br>REMOVAL | PAVED<br>SHOULDER<br>REMOVAL | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL-<br>BUTT JOINT | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>1 1/4" | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>1 3/4" | HOT-MIX ASPHALT SURFACE<br>REMOVAL, 2" |          | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>2 1/2" | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>5" | CONCRETE<br>MEDIAN<br>REMOVAL | GUARDRAIL<br>REMOVAL |
|---------------------------------------|--------------|--|--|---------------------|------------------------------|---|---|---|--|----------|---|---|-------------------------------|----------------------|
|                                       |              | 20100110   | 20100210   | 44000100            | 44004250                     | 40600982  | 44000154  | 44000156  | PAVEMENT                               | SHOULDER | 44000159  | 44000169  | X4403300                      | 63200310             |
|                                       |              | UNIT   | UNIT   | SQ YD               | SQ YD                        | SQ YD   | SQ YD   | SQ YD   | SQ YD                                  | SQ YD    | SQ YD   | SQ YD   | SQ FT                         | FOOT                 |
| <b>CH 22 RAMP A</b>                   |              |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 396+76.00                             | LT           |  | 20   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 397+08.00                             | LT           |  | 18   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 397+24.00                             | LT           | 15   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 397+46.00                             | LT           | 15   |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 401+29.56 TO 402+09.56                | BUTT JOINT   |  |  |                     |                              | 241.6   |   |   |  |          |   |   |                               |                      |
| 402+09.56 TO 403+14.01                | INTERSECTION |  |  |                     |                              |   |   |   | 542.7                                  |          |   |   |                               |                      |
| <b>CH 22 RAMP B</b>                   |              |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 299+58.16 TO 300+38.16                | BUTT JOINT   |  |  |                     |                              | 238.6   |   |   |  |          |   |   |                               |                      |
| 298+41.15 TO 299+58.16                | INTERSECTION |  |  |                     |                              |   |   |   | 540.7                                  |          |   |   |                               |                      |
| <b>CH 22 RAMP C</b>                   |              |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 500+71.00                             | RT           |  | 30   |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 501+31.15 TO 502+11.15                | BUTT JOINT   |  |  |                     |                              | 257.5   |   |   |  |          |   |   |                               |                      |
| 502+11.15 TO 503+21.60                | INTERSECTION |  |  |                     |                              |   |   |   | 509.8                                  |          |   |   |                               |                      |
| <b>CH 22 RAMP D</b>                   |              |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 99+35.96 TO 100+15.96                 | BUTT JOINT   |  |  |                     |                              | 244.2   |   |   |  |          |   |   |                               |                      |
| 98+50.69 TO 99+35.96                  | INTERSECTION |  |  |                     |                              |   |   |   | 528.3                                  |          |   |   |                               |                      |
| <b>COUNTY RD 2500 E (SN 010-0183)</b> |              |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| NW QUAD                               | APPROACH     |  |  |                     |                              |   |   |   |  |          |   |   |                               | 562.5                |
| NE QUAD                               | DEPART       |  |  |                     |                              |   |   |   |  |          |   |   |                               | 562.5                |
| SW QUAD                               | DEPART       |  |  |                     |                              |   |   |   |  |          |   |   |                               | 562.5                |
| SE QUAD                               | APPROACH     |  |  |                     |                              |   |   |   |  |          |   |   |                               | 562.5                |
| <b>VERMILION COUNTY</b>               |              |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| <b>I-74 EB</b>                        |              |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 1053+06.00 TO 1104+30.18              | EB INSIDE    |  |  |                     |                              |   |   | 328.8   |  | 2376.1   |   | 6,871.7   |                               |                      |
| 1103+75.00 TO 1110+85.00              | EB INSIDE    |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 1104+30.18 TO 1110+30.18              | EB INSIDE    |  |  | 795.1               | 278.8                        |   |   |   |  |          |   |   |                               |                      |
| 1110+30.18 TO 1180+00.00              | EB INSIDE    |  |  |                     |                              |   |   |   |  | 3027.9   |   | 9,265.6   |                               |                      |
| 1053+06.00 TO 1104+30.18              | EB OUTSIDE   |  |  |                     |                              |   |   |   | 7,054.1                                | 5,492.9  |   |   |                               |                      |
| 1104+30.18 TO 1110+30.18              | EB OUTSIDE   |  |  | 789.4               | 648.3                        |   |   |   |  |          |   |   |                               |                      |
| 1110+30.18 TO 1143+50.45              | EB OUTSIDE   |  |  |                     |                              |   |   |   |  | 3,671.4  |   |   |                               |                      |
| 1110+30.18 TO 1180+00.00              | EB OUTSIDE   |  |  |                     |                              |   |   |   | 9,545.0                                |          |   |   |                               |                      |
| 1148+64.18 TO 1172+55.10              | EB OUTSIDE   |  |  |                     |                              |   |   |   |  | 2,652.2  |   |   |                               |                      |
| <b>I-74 WB</b>                        |              |  |  |                     |                              |   |   |   |  |          |   |   |                               |                      |
| 1053+06.00 TO 1104+30.18              | WB INSIDE    |  |  |                     |                              |   |   |   |  | 2230.8   |   | 6,765.3   |                               |                      |
| 1103+75.00 TO 1110+85.00              | WB INSIDE    |  |  |                     |                              |   |   | 327.6   |  |          |   |   |                               |                      |
| 1104+30.18 TO 1110+30.18              | WB INSIDE    |  |  | 826.7               | 277.7                        |   |   |   |  |          |   |   |                               |                      |
| 1110+30.18 TO 1180+00.00              | WB INSIDE    |  |  |                     |                              |   |   |   |  | 3047.5   |   | 9,338.4   |                               |                      |
| 1053+06.00 TO 1104+30.18              | WB OUTSIDE   |  |  |                     |                              |   |   |   | 6,996.8                                | 5,563.1  |   |   |                               |                      |
| 1104+30.18 TO 1110+30.18              | WB OUTSIDE   |  |  | 789.9               | 636.7                        |   |   |   |  |          |   |   |                               |                      |
| 1110+30.18 TO 1139+78.38              | WB OUTSIDE   |  |  |                     |                              |   |   |   |  | 3,180.9  |   |   |                               |                      |
| 1110+30.18 TO 1180+00.00              | WB OUTSIDE   |  |  |                     |                              |   |   |   | 9,427.8                                |          |   |   |                               |                      |
| 1147+51.26 TO 1171+76.12              | WB OUTSIDE   |  |  |                     |                              |   |   |   |  | 2,566.6  |   |   |                               |                      |
| 1176+84.65 TO 1180+00.00              | WB OUTSIDE   |  |  |                     |                              |   |   |   |  | 351.1    |   |   |                               |                      |

SCHEDULE CONTINUES ON NEXT SHEET

MODEL Default  
FILE Name: ...03/27/2019-3:55:59 PM

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:55:59 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |       |      |        |
|------------------|-------|------|--------|
| <b>SCHEDULES</b> |       |      |        |
| SCALE: N.T.S.    | SHEET | OF   | SHEETS |
| STA.             | TO    | STA. |        |

|                           |         |        |                 |              |
|---------------------------|---------|--------|-----------------|--------------|
| F.A.I.<br>RTE.            | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
| I-74                      | *       | **     | 142             | 22           |
| CONTRACT NO. 70790        |         |        |                 |              |
| ILLINOIS FED. AID PROJECT |         |        |                 |              |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION

# REMOVAL

| LOCATION<br>STATION TO STATION   | COMMENT      | TREE<br>REMOVAL<br>(6 TO 15<br>UNITS OF<br>DIAMETER) | TREE<br>REMOVAL<br>(OVER 15<br>UNITS OF<br>DIAMETER) | PAVEMENT<br>REMOVAL | PAVED<br>SHOULDER<br>REMOVAL | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL-<br>BUTT JOINT | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>1 1/4" | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>1 3/4" | HOT-MIX ASPHALT SURFACE<br>REMOVAL, 2" |               | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>2 1/2" | HOT-MIX<br>ASPHALT<br>SURFACE<br>REMOVAL,<br>5" | CONCRETE<br>MEDIAN<br>REMOVAL | GUARDRAIL<br>REMOVAL |
|----------------------------------|--------------|--|--|---------------------|------------------------------|---|---|---|--|---------------|---|---|-------------------------------|----------------------|
|                                  |              | 20100110   | 20100210   | 44000100            | 44004250                     | 40600982  | 44000154  | 44000156  | PAVEMENT                               | SHOULDER      | 44000159  | 44000169  | X4403300                      | 63200310             |
|                                  |              | UNIT   | UNIT   | SQ YD               | SQ YD                        | SQ YD   | SQ YD   | SQ YD   | SQ YD                                  | SQ YD         | SQ YD   | SQ YD   | SQ YD                         | SQ FT                |
| <b>IL 49 RAMP A</b>              |              |  |  |                     |                              |   |   |   |  |               |   |   |                               |                      |
| 43+43.54 TO 61+48.14             | RT SHOULDER  |  |  |                     |                              |   | 1,145.6   |   |  | 518.8         |   |   |                               |                      |
| 44+55.13 TO 49+00.76             | RAMP TAPER   |  |  |                     |                              |   |   |   | 535.6                                  |               |   |   |                               |                      |
| 48+06.33 TO 61+13.77             | LT SHOULDER  |  |  |                     |                              |   | 539.9   |   |  | 65.4          |   |   |                               |                      |
| 49+00.76 TO 61+03.65             | RAMP         |  |  |                     |                              |   |   |   |  |               | 1,861.0   |   |                               |                      |
| 61+03.65 TO 61+96.89             | INTERSECTION |  |  |                     |                              |   |   |   | 493.0                                  |               |   |   |                               |                      |
| 61+80.00                         | ISLAND       |  |  |                     |                              |   |   |   |  |               |   |   | 106                           |                      |
| <b>IL 49 RAMP B</b>              |              |  |  |                     |                              |   |   |   |  |               |   |   |                               |                      |
| 63+06.04 TO 75+29.29             | LT SHOULDER  |  |  |                     |                              |   | 552.4   |   |  | 12.0          |   |   |                               |                      |
| 74+99.75 TO 83+75.01             | RAMP TAPER   |  |  |                     |                              |   |   |   | 1,254.1                                |               |   |   |                               |                      |
| 63+35.52 TO 74+99.75             | RAMP         |  |  |                     |                              |   |   |   |  |               | 1,796.3   |   |                               |                      |
| 63+25.52 TO 83+75.01             | RT SHOULDER  |  |  |                     |                              |   | 1,083.7   |   |  | 934.7         |   |   |                               |                      |
| 62+20.57 TO 63+35.52             | INTERSECTION |  |  |                     |                              |   |   |   |  | 502.8         |   |   |                               |                      |
| <b>IL 49 RAMP C</b>              |              |  |  |                     |                              |   |   |   |  |               |   |   |                               |                      |
| 39+71.02 TO 60+93.42             | LT SHOULDER  |  |  |                     |                              |   | 1,111.3   |   |  | 981.7         |   |   |                               |                      |
| 40+44.56 TO 48+92.74             | RAMP TAPER   |  |  |                     |                              |   |   |   | 1,278.4                                |               |   |   |                               |                      |
| 48+25.92 TO 60+90.22             | RT SHOULDER  |  |  |                     |                              |   | 536.4   |   |  | 32.0          |   |   |                               |                      |
| 48+92.74 TO 60+78.08             | RAMP         |  |  |                     |                              |   |   |   |  |               | 1,835.8   |   |                               |                      |
| 60+78.08 TO 61+81.84             | INTERSECTION |  |  |                     |                              |   |   |   |  | 460.8         |   |   |                               |                      |
| <b>IL 49 RAMP D</b>              |              |  |  |                     |                              |   |   |   |  |               |   |   |                               |                      |
| 0+99.54 TO 71+25.66              | RAMP         |  |  |                     |                              |   |   |   |  |               | 1,893.6   |   |                               |                      |
| 0+61.51 TO 76+77.88              | LT SHOULDER  |  |  |                     |                              |   | 1,099.1   |   |  | 537.4         |   |   |                               |                      |
| 71+25.66 TO 75+27.49             | RAMP TAPER   |  |  |                     |                              |   |   |   | 506.6                                  |               |   |   |                               |                      |
| 0+89.26 TO 72+25.78              | RT SHOULDER  |  |  |                     |                              |   | 539.6   |   |  | 49.2          |   |   |                               |                      |
| 0+13.34 TO 0+99.54               | INTERSECTION |  |  |                     |                              |   |   |   | 461.6                                  |               |   |   |                               |                      |
| 0+25.00                          | ISLAND       |  |  |                     |                              |   |   |   |  |               |   |   | 70                            |                      |
| <b>100 EAST RD (SN 092-0146)</b> |              |  |  |                     |                              |   |   |   |  |               |   |   |                               |                      |
| NW QUAD                          | APPROACH     |  |  |                     |                              |   |   |   |  |               |   |   |                               | 712.5                |
| NE QUAD                          | DEPART       |  |  |                     |                              |   |   |   |  |               |   |   |                               | 612.5                |
| SW QUAD                          | DEPART       |  |  |                     |                              |   |   |   |  |               |   |   |                               | 512.5                |
| SE QUAD                          | APPROACH     |  |  |                     |                              |   |   |   |  |               |   |   |                               | 512.5                |
| <b>PROJECT TOTAL</b>             |              | <b>146</b>   | <b>332</b>   | <b>6,419</b>        | <b>3,677</b>                 | <b>2,973</b>  | <b>6,609</b>  | <b>1,269</b>  | <b>70,442</b>                          | <b>77,607</b> | <b>7,387</b>  | <b>64,671</b>                                   | <b>176</b>                    | <b>4,837.5</b>       |

MODEL: Default  
FILE: Model - ...03270790-shc-schedule.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:56:05 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |                              |
|------------------|------------------------------|
| <b>SCHEDULES</b> |                              |
| SCALE: N.T.S.    | SHEET OF SHEETS STA. TO STA. |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 23        |
| CONTRACT NO. 70790 |         |        |              |           |

## EARTHWORK

| LOCATION<br>STATION TO STATION | EARTH<br>EXCAVATION<br>20200100<br>CU YD | FOR INFORMATION ONLY                |  |                     |        | EARTHWORK<br>BALANCE<br>WASTE (+)<br>SHORTAGE (-)<br>CU YD | FURNISHED<br>EXCAVATION<br>20400800<br>CU YD |
|--------------------------------|--|-------------------------------------|--|---------------------|--------|--|--|
|                                |  | AVERAGE<br>SHRINKAGE<br>FACTOR<br>% | EARTH<br>EXCAVATION<br>(ADJUSTED)<br>CU YD | EMBANKMENT<br>CU YD |        |  |  |
|                                |  |                                     |  |                     |        |  |  |
| <b>CHAMPAIGN COUNTY</b>        |  |                                     |  |                     |        |  |  |
| <b>I 74</b>                    |  |                                     |  |                     |        |  |  |
| 978+20.43 TO 1025+15.08        | 0  | 0                                   | 0  | 1,185               | -1,185 | 1,185  |  |
| <b>CH 22 SW QUADRANT</b>       |  |                                     |  |                     |        |  |  |
|                                | 4,776                                    | 25                                  | 3,562                                      | 3,564               | 18     | -18  |  |
| <b>CH 22 NW QUADRANT</b>       |  |                                     |  |                     |        |  |  |
|                                | 5,884                                    | 25                                  | 4,413                                      | 4,410               | 3      | -3   |  |
| <b>CH 22 NE QUADRANT</b>       |  |                                     |  |                     |        |  |  |
|                                | 15                                       | 25                                  | 11   | 117                 | -106   | 106  |  |
| <b>CH 22</b>                   |  |                                     |  |                     |        |  |  |
|                                | 189                                      | 25                                  | 142  | 25                  | 117    | -117   |  |
| <b>PROJECT TOTAL</b>           | <b>10,864</b>                            |                                     |  |                     |        | <b>1,153</b>   |  |

## MATERIAL TRANSFER DEVICE

| LOCATION<br>STATION TO STATION | COMMENT    | MATERIAL<br>TRANSFER<br>DEVICE |
|--------------------------------|------------|--------------------------------|
| <b>CHAMPAIGN COUNTY</b>        |            |                                |
| <b>I-74 EB</b>                 |            | Z0034105<br>TON                |
| 880+00.00 TO 898+29.00         |            | 961.3                          |
| 898+29.00 TO 904+29.00         |            | 1,464.4                        |
| 904+29.00 TO 979+40.75         |            | 3,941.7                        |
| 979+33.97 TO 980+23.97         | BUTT JOINT | 17.0                           |
| 979+40.75 TO 1025+15.08        |            | 2,874.8                        |
| 1023+45.08 TO 1024+35.08       | BUTT JOINT | 17.0                           |
| 1025+15.08 TO 1053+06.00       |            | 1,503.4                        |
| <b>I-74 WB</b>                 |            |                                |
| 880+00.00 TO 898+29.00         |            | 957.4                          |
| 898+29.00 TO 904+29.00         |            | 1,463.1                        |
| 904+29.00 TO 979+40.75         |            | 3,917.8                        |
| 979+33.97 TO 980+23.97         | BUTT JOINT | 16.7                           |
| 979+40.75 TO 1025+15.08        |            | 2,842.6                        |
| 1023+45.08 TO 1024+35.08       | BUTT JOINT | 16.8                           |
| 1025+15.08 TO 1053+06.00       |            | 1,468.6                        |
| <b>VERMILION COUNTY</b>        |            |                                |
| <b>I-74 EB</b>                 |            |                                |
| 1053+06.00 TO 1104+30.18       |            | 2,719.4                        |
| 1104+30.18 TO 1110+30.18       |            | 1,443.3                        |
| 1110+30.18 TO 1180+00.00       |            | 3,670.7                        |
| <b>I-74 WB</b>                 |            |                                |
| 1053+06.00 TO 1104+30.18       |            | 2,683.6                        |
| 1104+30.18 TO 1110+30.18       |            | 1,471.0                        |
| 1110+30.18 TO 1180+00.00       |            | 3,677.9                        |
| <b>PROJECT TOTAL</b>           |            | <b>37,129</b>                  |

## INCIDENTAL HMA SCHEDULE

| LOCATION<br>STATION TO STATION | COMMENT | BITUMINOUS<br>MATERIALS<br>(PRIME COAT)<br>40800025<br>POUND | INCIDENTAL<br>HMA<br>SURFACING<br>40800050<br>TON | HMA SURFACE<br>REMOVAL<br>SPECIAL<br>X4400196<br>SQ YD |
|--------------------------------|---------|--|---|--|
| <b>CHAMPAIGN COUNTY</b>        |         |  |   |  |
| <b>I-74 MEDIAN CROSS-OVER</b>  |         |  |   |  |
| 888+12.03 TO 888+58.55         |         | 49.2   | 8.8   | 104.4  |
| 1052+92.88 TO 1053+06.00       |         | 10.7   | 1.7   | 19.7   |
| <b>VERMILION COUNTY</b>        |         |  |   |  |
| <b>I-74 MEDIAN CROSS-OVER</b>  |         |  |   |  |
| 1053+06.00 TO 1053+36.69       |         | 37.3   | 6.6   | 78.7   |
| <b>PROJECT TOTAL</b>           |         | <b>98</b>  | <b>18</b>   | <b>204</b>   |

MODEL: Default  
FILE: hma19\_03021905-hs-schedule.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:56:09 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |                              |
|------------------|------------------------------|
| <b>SCHEDULES</b> |                              |
| SCALE: N.T.S.    | SHEET OF SHEETS STA. TO STA. |

| F.A.I.<br>RTE.            | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
|---------------------------|---------|--------|-----------------|--------------|
| I-74                      | *       | **     | 142             | 24           |
| CONTRACT NO. 70790        |         |        |                 |              |
| ILLINOIS FED. AID PROJECT |         |        |                 |              |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



## HOT-MIX ASPHALT RESURFACING - EB DRIVING LANE

| LOCATION<br>STATION TO STATION | COMMENT                | LENGTH<br>FOOT | LANE<br>WIDTH<br>FOOT | *<br>LANE<br>AREA<br>SQ YD | SHLDR<br>WIDTH<br>FOOT | *<br>SHLDR<br>AREA<br>SQ YD | BITUMINOUS<br>MATERIALS<br>(TACK COAT)<br>40600290<br>POUND | TEMPORARY<br>RAMP<br>40600990<br>SQ YD | POLYMERIZED<br>HOT-MIX ASPHALT<br>SURFACE<br>COURSE, STONE<br>MATRIX ASPHALT,<br>N80<br>40603153<br>TON | POLYMERIZED<br>HOT-MIX<br>ASPHALT BINDER<br>COURSE, IL-19.0,<br>N90<br>40603240<br>TON | AGGREGATE<br>WEDGE<br>SHOULDERS,<br>TYPE B<br>48102100<br>TON | HOT-MIX<br>ASPHALT<br>SHOULDERS<br>48203100<br>TON | SHOULDER<br>RUMBLE<br>STRIPS, 16"<br>64200116<br>FOOT | LONGITUDINAL<br>JOINT SEALANT<br>Z0033700<br>FOOT |
|--------------------------------|------------------------|----------------|-----------------------|----------------------------|------------------------|-----------------------------|---|--|---|--|---|--|---|---|
|                                |                        |                |                       |                            |                        |                             |   |  | TON   | TON  | TON   | TON  | FOOT  | FOOT  |
| <b>CHAMPAIGN COUNTY</b>        |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |   |
| <b>I-74 EB DRIVING LANE</b>    |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |   |
| 880+00.00 TO 898+29.00         | 2" pvt & shldr surface | 1,829.00       | 12                    | 2,471.2                    | 10                     | 1,994.1                     | 2,375.1   | 16.3                                   | 277.7   |  | 47.2  | 223.3  | 1,829.0   | 1,829.0   |
| 898+29.00 TO 904+29.00         | 2" pvt & shldr surface | 600.00         | 12                    | 873.4                      | 10                     | 598.7                       | 391.2   |  | 98.1  |  | 7.6   | 67.1   | 600.0   | 600.0   |
| 904+29.00 TO 979+40.75         | 2" pvt & shldr surface | 7,511.75       | 12                    | 10,206.4                   | 10                     | 8,291.8                     | 9,826.5   | 16.3                                   | 1,147.0   |  | 193.9   | 928.7  | 7,511.8   | 7,511.8   |
| 979+40.75 TO 983+81.38         | 2.25" shldr bottom     | 440.63         | -                     | -                          | 10                     | 486.8                       | 171.0   |  |   |  |   | 61.3   |   |   |
| 979+40.75 TO 983+81.38         | 2" shldr surface       | 440.63         | -                     | -                          | 10                     | 486.5                       | 167.2   |  |   |  |   | 54.5   |   |   |
| 979+73.97 TO 980+23.97         | TRANSITION             | 50.00          | 12                    | 213.3                      | -                      | -                           | 35.7  |  |   | 7.7  |   |  |   |   |
| 980+23.97 TO 1023+45.08        | 2.25" pvt bottom       | 4,321.11       | 12                    | 5,719.9                    | 10                     | 3,749.7                     | 5,058.1   |  |   | 720.7  |   |  |   |   |
| 979+40.75 TO 1025+15.08        | 2" pvt surface         | 4,574.33       | 12                    | 6,061.8                    | -                      | -                           | 1,592.6   |  | 681.3   |  | 83.4  |  | 3,360.6   | 4,574.3   |
| 985+18.95 TO 988+01.28         | EB Outside EOP         | -              | -                     | -                          | -                      | -                           | -   |  | -   |  | -   |  | -   | 282.3   |
| 989+22.22 TO 1013+78.69        | 2.25" shldr bottom     | 2,456.47       | -                     | -                          | 10                     | 2,869.8                     |   |  |   |  |   | 342.7  |   |   |
| 989+22.22 TO 1013+78.69        | 2" shldr surface       | 2,456.47       | -                     | -                          | 10                     | 2,719.9                     | 740.8   |  |   |  |   | 304.6  |   |   |
| 1013+78.69 TO 1021+02.58       | EB Outside EOP         | -              | -                     | -                          | -                      | -                           | -   |  | -   |  | -   |  | -   | 723.9   |
| 1021+72.50 TO 1025+15.08       | 2.25" shldr bottom     | 342.58         | -                     | -                          | 10                     | 393.1                       |   |  |   |  |   | 49.5   |   |   |
| 1021+72.50 TO 1025+15.08       | 2" shldr surface       | 342.58         | -                     | -                          | 10                     | 393.1                       | 105.6   |  |   |  |   | 44.0   |   |   |
| 1023+45.08 TO 1024+35.08       | TRANSITION             | 90.00          | 12                    | 213.3                      | -                      | -                           | 35.3  |  |   | 7.6  |   |  |   |   |
| 1025+15.08 TO 1053+06.00       | 2" pvt & shldr surface | 2,790.92       | 12                    | 3,762.2                    | 10                     | 3,099.0                     | 3,645.7   | 16.3                                   | 422.8   |  | 72.0  | 347.1  | 2,790.9   | 2,790.9   |
| <b>VERMILION COUNTY</b>        |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |   |
| <b>I-74 EB DRIVING LANE</b>    |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |   |
| 1053+06.00 TO 1104+30.18       | 2" pvt surface         | 5,124.18       | 12                    | 7,054.1                    | -                      | -                           | 3,686.8   |  | 792.7   |  | 132.3   |  | 5,124.2   | 5,124.2   |
| 1053+06.00 TO 1104+30.18       | 2" shldr surface       | 5,124.18       | -                     | -                          | 10                     | 5,492.9                     | 2,984.2   |  |   |  |   | 615.2  |   |   |
| 1104+30.18 TO 1110+30.18       | 2" pvt surface         | 600.00         | 12                    | 855.8                      | -                      | -                           | 445.1   |  | 96.2  |  | 7.6   |  | 600.0   | 600.0   |
| 1104+30.18 TO 1110+30.18       | 2" shldr surface       | 600.00         | 12                    | 855.8                      | 10                     | 581.9                       | 160.9   |  |   |  |   | 65.2   |   |   |
| 1110+30.18 TO 1180+00.00       | 2" pvt surface         | 6,969.82       | 12                    | 9,545.0                    | -                      | -                           | 4,992.2   |  | 1,072.7   |  | 142.1   |  | 5,711.2   | 6,969.8   |
| 1110+30.18 TO 1143+50.45       | 2" shldr surface       | 3,320.27       | -                     | -                          | 10                     | 3,671.4                     | 3,542.6   |  |   |  |   | 411.2  |   |   |
| 1144+62.05 TO 1148+13.32       | EB Outside EOP         | -              | -                     | -                          | -                      | -                           | -   |  | -   |  | -   |  | -   | 351.3   |
| 1148+64.18 TO 1172+55.10       | 2" shldr surface       | 2,390.92       | -                     | -                          | 10                     | 2,652.2                     | 1,432.6   |  |   |  |   | 297.0  |   |   |
| 1172+55.10 TO 1179+98.58       | EB Outside EOP         | -              | -                     | -                          | -                      | -                           | -   | 16.3                                   |   |  |   |  |   | 743.5   |
| <b>EB DRIVING LANE TOTAL</b>   |                        |                |                       |                            |                        |                             | <b>41,390</b>   | <b>65</b>                              | <b>4,589</b>  | <b>736</b>   | <b>686</b>  | <b>3,812</b>                                       | <b>27,528</b>   | <b>32,102</b>                                     |

**Notes:**

\* AREAS MEASURED IN CADD

Bituminous Materials calculations:

1 application at 0.05 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders except in Full Depth Pavement locations.

Additional 1 application for Typical #1 at 0.025 lb/sq ft for the Passing Lane

Additional 1 application for Typical #2 at 0.025 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders

1 application at 0.25 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavement locations.

Additional 3 applications at 0.025 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavment locations.

Additional 2 applications at 0.025 lb/sq ft for the Shoulders in Full Depth Pavment locations.

MODEL: Default  
FILE: N:\m11e...03\210790-shr-schedule.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/29/2019 - 2:19:06 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |         |    |        |
|------------------|---------|----|--------|
| <b>SCHEDULES</b> |         |    |        |
| SCALE: N.T.S.    | SHEET   | OF | SHEETS |
| STA.             | TO STA. |    |        |

|                           |         |        |                 |              |
|---------------------------|---------|--------|-----------------|--------------|
| F.A.I.<br>RTE.            | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
| I-74                      | *       | **     | 142             | 26           |
| CONTRACT NO. 70790        |         |        |                 |              |
| ILLINOIS FED. AID PROJECT |         |        |                 |              |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

## HOT-MIX ASPHALT RESURFACING - EB PASSING LANE

| LOCATION<br>STATION TO STATION | COMMENT                | LENGTH<br>FOOT | LANE<br>WIDTH<br>FOOT | *<br>LANE<br>AREA<br>SQ YD | SHLDR<br>WIDTH<br>FOOT | *<br>SHLDR<br>AREA<br>SQ YD | BITUMINOUS<br>MATERIALS<br>(TACK COAT)<br>40600290<br>POUND | TEMPORARY<br>RAMP<br>40600990<br>SQ YD | POLYMERIZED<br>HOT-MIX ASPHALT<br>SURFACE<br>COURSE, STONE<br>MATRIX ASPHALT,<br>N80<br>40603153<br>TON | POLYMERIZED<br>HOT-MIX<br>ASPHALT BINDER<br>COURSE, IL-19.0,<br>N90<br>40603240<br>TON | HOT-MIX<br>ASPHALT<br>SHOULDERS<br>48203100<br>TON | AGGREGATE<br>WEDGE<br>SHOULDERS,<br>TYPE B<br>48102100<br>TON | SHOULDER<br>RUMBLE<br>STRIPS, 16"<br>64200116<br>FOOT |
|--------------------------------|------------------------|----------------|-----------------------|----------------------------|------------------------|-----------------------------|---|--|---|--|--|---|---|
|                                |                        |                |                       |                            |                        |                             |   |  |   |  |  |   |   |
| <b>CHAMPAIGN COUNTY</b>        |                        |                |                       |                            |                        |                             |   |  |   |  |  |   |   |
| <b>I-74 EB PASSING LANE</b>    |                        |                |                       |                            |                        |                             |   |  |   |  |  |   |   |
| 880+00.00 TO 898+29.00         | 3" bottom pvt          | 1,829.00       | 12                    | 2,468.0                    | -                      | -                           | 1,280.0   | 11.9                                   |   | 409.6  |  |   |   |
| 880+00.00 TO 898+29.00         | 2" pvt & shldr surface | 1,829.00       | 12                    | 2,438.0                    | 4                      | 852.0                       | 1,206.3   |  | 274.0   |  | 95.4   | 46.2  | 1,829.0   |
| 898+29.00 TO 904+29.00         | 2" pvt & shldr surface | 600.00         | 12                    | 869.1                      | 4                      | 197.6                       | 374.5   |  | 97.7  |  | 22.1   | 15.5  | 600.0   |
| 904+29.00 TO 979+40.75         | 3" bottom pvt          | 7,511.75       | 12                    | 9,919.4                    | -                      | -                           | 5,214.9   |  |   | 1,666.5  |  |   |   |
| 904+29.00 TO 979+40.75         | 2" pvt & shldr surface | 7,511.75       | 12                    | 10,038.7                   | 4                      | 3,346.0                     | 4,891.2   | 11.9                                   | 1,128.2   |  | 374.8  | 193.9   | 7,511.8   |
| 979+33.97 TO 980+23.98         | TRANSITION             | 90.01          | 12                    | 213.3                      | -                      | -                           | 35.9  |  |   | 7.8  |  |   |   |
| 980+23.97 TO 1023+45.08        | 2.25" bottom pvt       | 4,321.11       | 12                    | 6,013.3                    | 4                      | 2,156.4                     | 4,565.9   |  |   | 757.7  | 271.7  |   |   |
| 979+40.75 TO 1025+15.08        | 2" pvt & shldr surface | 4,574.33       | 12                    | 6,363.6                    | 4                      | 2,156.4                     | 2,374.4   |  | 715.1   |  | 241.5  | 245.9   | 4,574.3   |
| 1023+45.08 TO 1024+35.08       | TRANSITION             | 90.00          | 12                    | 213.3                      | -                      | -                           | 36.2  |  |   | 7.8  |  |   |   |
| 1025+15.08 TO 1053+06.00       | 3" bottom pvt          | 2,790.92       | 12                    | 3,854.4                    | -                      | -                           | 2,013.6   | 11.9                                   |   | 647.5  |  |   |   |
| 1025+15.08 TO 1053+06.00       | 2" pvt & shldr surface | 2,790.92       | 12                    | 3,854.4                    | 4                      | 1,308.9                     | 1,874.9   |  | 433.1   |  | 146.6  | 71.8  | 2,790.9   |
| <b>VERMILION COUNTY</b>        |                        |                |                       |                            |                        |                             |   |  |   |  |  |   |   |
| <b>I-74 EB PASSING LANE</b>    |                        |                |                       |                            |                        |                             |   |  |   |  |  |   |   |
| 1053+06.00 TO 1104+30.18       | 3" bottom pvt          | 5,124.18       | 12                    | 6,871.7                    | -                      | -                           | 3,604.7   |  |   | 1,154.4  |  |   |   |
| 1053+06.00 TO 1104+30.18       | 2" pvt & shldr surface | 5,124.18       | 12                    | 6,871.7                    | 4                      | 2,376.1                     | 3,384.0   |  | 772.3   |  | 266.1  | 131.6   | 5,124.2   |
| 1104+30.18 TO 1110+30.18       | 2" pvt & shldr surface | 600.00         | 12                    | 861.6                      | 4                      | 212.3                       | 301.7   |  | 96.8  |  | 23.8   | 15.5  | 600.0   |
| 1110+30.18 TO 1180+00.00       | 3" bottom pvt          | 6,969.82       | 12                    | 9,265.6                    | -                      | -                           | 4,866.5   |  |   | 1,556.6  |  |   |   |
| 1110+30.18 TO 1180+00.00       | 2" pvt & shldr surface | 6,969.82       | 12                    | 9,265.6                    | 4                      | 3,027.9                     | 4,492.8   | 11.9                                   | 1,041.4   |  | 339.1  | 179.9   | 6,969.8   |
| <b>EB PASSING LANE TOTAL</b>   |                        |                |                       |                            |                        |                             | <b>40,518</b>   | <b>48</b>                              | <b>4,559</b>  | <b>6,208</b>   | <b>1,781</b>                                       | <b>900</b>  | <b>30,000</b>   |

**Notes:**

\* AREAS MEASURED IN CADD

Bituminous Materials calculations:

1 application at 0.05 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders except in Full Depth Pavement locations.

Additional 1 application for Typical #1 at 0.025 lb/sq ft for the Passing Lane

Additional 1 application for Typical #2 at 0.025 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders

1 application at 0.25 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavement locations.

Additional 3 applications at 0.025 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavment locations.

Additional 2 applications at 0.025 lb/sq ft for the Shoulders in Full Depth Pavment locations.

MODEL: Default  
FILE: N:\m\15-03-03\70790-shr-schedule.dgn

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|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/29/2019 - 2:19:11 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |         |    |        |
|------------------|---------|----|--------|
| <b>SCHEDULES</b> |         |    |        |
| SCALE: N.T.S.    | SHEET   | OF | SHEETS |
| STA.             | TO STA. |    |        |

|                        |              |                  |                        |                    |
|------------------------|--------------|------------------|------------------------|--------------------|
| F.A.I.<br>RTE.<br>I-74 | SECTION<br>* | COUNTY<br>**     | TOTAL<br>SHEETS<br>142 | SHEET<br>NO.<br>27 |
| CONTRACT NO. 70790     |              |                  |                        |                    |
| ILLINOIS               |              | FED. AID PROJECT |                        |                    |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

## HOT-MIX ASPHALT RESURFACING - WB DRIVING LANE

| LOCATION<br>STATION TO STATION | COMMENT                | LENGTH   | LANE<br>WIDTH | *<br>LANE<br>AREA | SHLDR<br>WIDTH | *<br>SHLDR<br>AREA | BITUMINOUS<br>MATERIALS<br>(TACK COAT) | TEMPORARY<br>RAMP | POLYMERIZED<br>HOT-MIX ASPHALT<br>SURFACE<br>COURSE, STONE<br>MATRIX ASPHALT,<br>N80 | POLYMERIZED<br>HOT-MIX<br>ASPHALT BINDER<br>COURSE, IL-19.0,<br>N90 | AGGREGATE<br>WEDGE<br>SHOULDERS,<br>TYPE B | HOT-MIX<br>ASPHALT<br>SHOULDERS | SHOULDER<br>RUMBLE<br>STRIPS, 16" | LONGITUDINAL<br>JOINT SEALANT |
|--------------------------------|------------------------|----------|---------------|-------------------|----------------|--------------------|--|-------------------|--|---|--|---------------------------------|-----------------------------------|-------------------------------|
|                                |                        | FOOT     | FOOT          | SQ YD             | FOOT           | SQ YD              | 40600290<br>POUND                      | 40600990<br>SQ YD | 40603153<br>TON  | 40603240<br>TON   | 48102100<br>TON                            | 48203100<br>TON                 | 64200116<br>FOOT                  | Z0033700<br>FOOT              |
| <b>CHAMPAIGN COUNTY</b>        |                        |          |               |                   |                |                    |  |                   |  |   |  |                                 |                                   |                               |
| <b>I-74 WB DRIVING LANE</b>    |                        |          |               |                   |                |                    |  |                   |  |   |  |                                 |                                   |                               |
| 880+00.00 TO 898+29.00         | 2" pvt & shldr surface | 1,829.00 | 12            | 2,455.3           | 10             | 1,970.6            | 2,357.5                                | 16.3              | 275.9  |   | 47.2                                       | 220.7                           | 1,829.0                           | 1,829.0                       |
| 898+29.00 TO 904+29.00         | 2" pvt & shldr surface | 600.00   | 12            | 867.9             | 10             | 586.5              | 387.3                                  |                   | 97.5   |   | 7.6  | 65.7                            | 600.0                             | 600.0                         |
| 904+29.00 TO 979+40.75         | 2" pvt & shldr surface | 7,511.75 | 12            | 10,172.9          | 10             | 8,038.4            | 9,697.4                                | 16.3              | 1,143.3  |   | 193.9                                      | 900.3                           | 7,511.8                           | 7,511.8                       |
| 979+33.97 TO 980+23.97         | TRANSITION             | 90.00    | 12            | 213.3             | -              | -                  | 36.0                                   |                   |  | 7.8   |  |                                 |                                   |                               |
| 979+40.75 TO 1025+15.08        | 2" pvt surface         | 4,574.33 | 12            | 6,174.9           | -              | -                  | 1,080.0                                |                   | 694.0  |   |  |                                 |                                   |                               |
| 979+40.75 TO 1023+45.08        | 2.25" pvt bottom       | 4,404.33 | 12            | 5,828.7           | 10             | 3,584.1            | 3,056.1                                |                   |  | 734.4   |  |                                 |                                   |                               |
| 980+53.25 TO 987+69.83         | WB Outside EOP         | -        | -             | -                 | -              | -                  |  |                   |  |   |  |                                 |                                   | 716.6                         |
| 988+65.55 TO 1013+45.32        | 2.25" shldr bottom     | 2,479.77 | -             | -                 | 10             | 3,492.9            | 857.0                                  |                   |  |   |  | 354.2                           |                                   |                               |
| 988+65.55 TO 1013+45.32        | 2" shldr surface       | 2,479.77 | -             | -                 | 10             | 2,810.9            | 787.3                                  |                   |  |   | 83.3                                       | 314.8                           | 3,276.4                           | 4,574.3                       |
| 1013+45.32 TO 1016+60.24       | WB Outside EOP         | -        | -             | -                 | -              | -                  |  |                   |  |   |  |                                 |                                   | 314.9                         |
| 1018+01.69 TO 1025+15.08       | 2.25" shldr bottom     | 713.39   | -             | -                 | 10             | 773.2              | 712.8                                  |                   |  |   |  | 97.4                            |                                   |                               |
| 1018+01.69 TO 1025+15.08       | 2" shldr surface       | 713.39   | -             | -                 | 10             | 773.2              | 475.2                                  |                   |  |   |  | 86.6                            |                                   |                               |
| 1023+45.08 TO 1024+35.08       | TRANSITION             | 90.00    | 12            | 213.3             | -              | -                  | 35.7                                   |                   |  | 7.7   |  |                                 |                                   |                               |
| 1025+15.08 TO 1053+06.00       | 2" pvt & shldr surface | 2,790.92 | 12            | 3,820.5           | 10             | 3,061.7            | 3,655.1                                | 16.3              | 429.3  |   | 72.0                                       | 342.9                           | 2,790.9                           | 2,790.9                       |
| <b>VERMILION COUNTY</b>        |                        |          |               |                   |                |                    |  |                   |  |   |  |                                 |                                   |                               |
| <b>I-74 WB DRIVING LANE</b>    |                        |          |               |                   |                |                    |  |                   |  |   |  |                                 |                                   |                               |
| 1053+06.00 TO 1104+30.18       | 2" pvt & shldr surface | 5,124.18 | 12            | 6,996.8           | 10             | 5,563.1            | 6,676.8                                |                   | 786.3  |   | 132.3                                      | 623.1                           | 5,124.2                           | 5,124.2                       |
| 1104+30.18 TO 1110+30.18       | 2" pvt & shldr surface | 600.00   | 12            | 856.8             | 10             | 569.8              | 603.8                                  |                   | 96.3   |   | 7.6  | 63.8                            | 600.0                             | 600.0                         |
| 1110+30.18 TO 1180+00.00       | 2" pvt & shldr surface | 6,969.82 | 12            | 9,427.8           | 10             | 6,098.6            | 8,380.8                                | 16.3              | 1,059.5  |   | 141.0                                      | 683.1                           | 5,688.5                           | 6,969.8                       |
| 1140+51.98 TO 1147+51.26       | WB Outside EOP         | -        | -             | -                 | -              | -                  |  |                   |  |   |  |                                 |                                   | 699.3                         |
| 1172+31.03 TO 1175+34.24       | WB Outside EOP         | -        | -             | -                 | -              | -                  |  |                   |  |   |  |                                 |                                   | 303.2                         |
| <b>WB DRIVING LANE TOTAL</b>   |                        |          |               |                   |                |                    | <b>38,799</b>                          | <b>65</b>         | <b>4,582</b>   | <b>750</b>  | <b>685</b>                                 | <b>3,753</b>                    | <b>27,421</b>                     | <b>32,034</b>                 |

**Notes:**

\* AREAS MEASURED IN CADD

Bituminous Materials calculations:

1 application at 0.05 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders except in Full Depth Pavement locations.

Additional 1 application for Typical #1 at 0.025 lb/sq ft for the Passing Lane

Additional 1 application for Typical #2 at 0.025 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders

1 application at 0.25 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavement locations.

Additional 3 applications at 0.025 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavment locations.

Additional 2 applications at 0.025 lb/sq ft for the Shoulders in Full Depth Pavment locations.

MODEL: Default  
FILE: \\mch\p\0570790-shr-schedule.dgn

|                                    |            |           |  |
|------------------------------------|------------|-----------|--|
| USER NAME = bemery                 | DESIGNED - | REVISED - |  |
|                                    | DRAWN -    | REVISED - |  |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |  |
| PLOT DATE = 1/29/2019 - 2:19:14 PM | DATE -     | REVISED - |  |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |                 |      |         |
|------------------|-----------------|------|---------|
| <b>SCHEDULES</b> |                 |      |         |
| SCALE: N.T.S.    | SHEET OF SHEETS | STA. | TO STA. |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 28        |
| CONTRACT NO. 70790 |         |        |              |           |



## HOT-MIX ASPHALT RESURFACING - WB PASSING LANE

| LOCATION<br>STATION TO STATION | COMMENT                | LENGTH<br>FOOT | LANE<br>WIDTH<br>FOOT | *<br>LANE<br>AREA<br>SQ YD | SHLDR<br>WIDTH<br>FOOT | *<br>SHLDR<br>AREA<br>SQ YD | BITUMINOUS<br>MATERIALS<br>(TACK COAT)<br>40600290<br>POUND | TEMPORARY<br>RAMP<br>40600990<br>SQ YD | POLYMERIZED<br>HOT-MIX ASPHALT<br>SURFACE<br>COURSE, STONE<br>MATRIX ASPHALT,<br>N80<br>40603153<br>TON | POLYMERIZED<br>HOT-MIX<br>ASPHALT BINDER<br>COURSE, IL-19.0,<br>N90<br>40603240<br>TON | AGGREGATE<br>WEDGE<br>SHOULDERS,<br>TYPE B<br>48102100<br>TON | HOT-MIX<br>ASPHALT<br>SHOULDERS<br>48203100<br>TON | SHOULDER<br>RUMBLE<br>STRIPS, 16"<br>64200116<br>FOOT |
|--------------------------------|------------------------|----------------|-----------------------|----------------------------|------------------------|-----------------------------|---|--|---|--|---|--|---|
|                                |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |
| <b>CHAMPAIGN COUNTY</b>        |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |
| <b>I-74 WB PASSING LANE</b>    |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |
| 880+00.00 TO 898+29.00         | 2" pvt & shldr surface | 1,829.00       | 12                    | 2,430.5                    | 4                      | 815.7                       | 1,188.2   | 11.9                                   | 273.2   |  | 46.2  | 91.4   | 1,829.0   |
| 880+00.00 TO 898+29.00         | 3" bottom pvt          | 1,829.00       | 12                    | 2,430.5                    | -                      | -                           | 1,276.6   |  |   | 408.3  |   |  |   |
| 898+29.00 TO 904+29.00         | 2" pvt & shldr surface | 600.00         | 12                    | 873.1                      | 4                      | 185.1                       | 369.7   | 11.9                                   | 98.1  |  | 15.5  | 20.7   | 600   |
| 904+29.00 TO 979+40.75         | 2" pvt & shldr surface | 7,511.75       | 12                    | 9,961.3                    | 4                      | 3,267.1                     | 4,838.6   |  | 1,119.6   |  | 193.9   | 366.0  | 7,511.8   |
| 904+29.00 TO 979+40.75         | 3" bottom pvt          | 7,511.75       | 12                    | 9,850.7                    | -                      | -                           | 5,184.0   |  |   | 1,654.9  |   |  |   |
| 979+33.97 TO 980+23.97         | BUTT JOINT             | 90.00          | 12                    | 213.3                      | -                      | -                           | 34.2  |  |   | 7.3  |   |  |   |
| 979+40.75 TO 1025+15.08        | 2" pvt & shldr surface | 4,574.33       | 12                    | 6,109.4                    | 4                      | 2,002.9                     | 2,287.2   |  | 686.6   |  | 245.9   | 224.3  | 4,574.3   |
| 980+23.97 TO 1023+45.08        | 3" bottom pvt          | 4,321.11       | 12                    | 5,774.2                    | 4                      | 2,002.9                     | 4,389.2   |  |   | 727.6  |   | 252.4  |   |
| 1023+45.08 TO 1024+35.08       | BUTT JOINT             | 90.00          | 12                    | 213.3                      | -                      | -                           | 34.2  |  |   | 7.6  |   |  |   |
| 1025+15.08 TO 1053+06.00       | 2" pvt & shldr surface | 2,790.92       | 12                    | 3,706.7                    | 4                      | 1,183.2                     | 1,785.1   |  | 416.6   |  | 71.8  | 132.5  | 2,790.9   |
| 1025+15.08 TO 1053+06.00       | 3" bottom pvt          | 2,790.92       | 12                    | 3,706.7                    | -                      | -                           | 1,947.1   | 11.9                                   |   | 622.7  |   |  |   |
| <b>VERMILION COUNTY</b>        |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |
| <b>I-74 WB PASSING LANE</b>    |                        |                |                       |                            |                        |                             |   |  |   |  |   |  |   |
| 1053+06.00 TO 1104+30.18       | 3" bottom pvt          | 5,124.18       | 12                    | 6,765.3                    | -                      | -                           | 3,556.8   |  |   | 1,136.6  |   |  |   |
| 1053+06.00 TO 1104+30.18       | 2" pvt & shldr surface | 5,124.18       | 12                    | 6,765.3                    | 4                      | 2,230.8                     | 3,294.7   |  | 760.4   |  | 131.6   | 249.9  | 5,124.2   |
| 1104+30.18 TO 1110+30.18       | 2" pvt & shldr surface | 600.00         | 12                    | 893.5                      | 4                      | 210.9                       | 308.5   |  | 100.4   |  | 15.5  | 23.6   | 600.0   |
| 1110+30.18 TO 1180+00.00       | 2" pvt & shldr surface | 6,969.82       | 12                    | 9,338.4                    | 4                      | 3,047.5                     | 4,517.9   |  | 1,049.5   |  | 179.9   | 341.3  | 6,969.8   |
| 1110+30.18 TO 1180+00.00       | 3" bottom pvt          | 6,969.82       | 12                    | 9,338.4                    | -                      | -                           | 4,668.3   | 11.9                                   |   | 1,568.9  |   |  |   |
| <b>WB PASSING LANE TOTAL</b>   |                        |                |                       |                            |                        |                             | <b>39,681</b>   | <b>48</b>                              | <b>4,504</b>  | <b>6,134</b>   | <b>901</b>  | <b>1,702</b>                                       | <b>30,000</b>   |

Notes:

\* AREAS MEASURED IN CADD

Bituminous Materials calculations:

1 application at 0.05 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders except in Full Depth Pavement locations.

Additional 1 application for Typical #1 at 0.025 lb/sq ft for the Passing Lane

Additional 1 application for Typical #2 at 0.025 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders

1 application at 0.25 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavement locations.

Additional 3 applications at 0.025 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavment locations.

Additional 2 applications at 0.025 lb/sq ft for the Shoulders in Full Depth Pavment locations.

MODEL: Default  
FILE: h:\m\p\...03\201905\hrc-schedule.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/29/2019 - 2:19:18 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |         |    |        |
|------------------|---------|----|--------|
| <b>SCHEDULES</b> |         |    |        |
| SCALE: N.T.S.    | SHEET   | OF | SHEETS |
| STA.             | TO STA. |    |        |

| F.A.I.<br>RTE.     | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
|--------------------|---------|--------|-----------------|--------------|
| I-74               | *       | **     | 142             | 29           |
| CONTRACT NO. 70790 |         |        |                 |              |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

# HOT-MIX ASPHALT RESURFACING - RAMPS I-74/CH22

| LOCATION<br>STATION TO STATION  | COMMENT                   | LENGTH<br>FOOT | LANE<br>WIDTH<br>FOOT | *<br>LANE<br>AREA<br>SQ YD | SHLDR<br>WIDTH<br>FOOT | *<br>SHLDR<br>AREA<br>SQ YD | BITUMINOUS<br>MATERIALS<br>(TACK COAT)<br>40600290 | POLYMERIZED<br>LEVELING<br>BINDER<br>(MACHINE<br>METHOD),<br>IL-9.5FG, N90<br>40600847 | TEMPORARY<br>RAMP<br>40600990 | POLYMERIZED<br>HOT-MIX ASPHALT<br>SURFACE<br>COURSE, STONE<br>MATRIX ASPHALT,<br>N80<br>40603153 | POLYMERIZED<br>HOT-MIX<br>ASPHALT BINDER<br>COURSE, IL-19.0,<br>N90<br>40603240 | AGGREGATE<br>WEDGE<br>SHOULDERS,<br>TYPE B<br>48102100 | HOT-MIX<br>ASPHALT<br>SHOULDERS<br>48203100 | SHOULDER<br>RUMBLE<br>STRIPS, 16"<br>64200116 |
|---------------------------------|---------------------------|----------------|-----------------------|----------------------------|------------------------|-----------------------------|--|--|-------------------------------|--|---|--|---|---|
|                                 |                           |                |                       |                            |                        |                             | POUND  | TON  | SQ YD                         | TON  | TON   | TON  | TON   | TON   |
| <b>CHAMPAIGN COUNTY</b>         |                           |                |                       |                            |                        |                             |  |  |                               |  |   |  |   |   |
| <b>CH 22 RAMP A</b>             |                           |                |                       |                            |                        |                             |  |  |                               |  |   |  |   |   |
| 383+77.99 TO 389+16.72          | 2.25" RT shldr bottom     | 538.73         | -                     | -                          | 10                     | 618.2                       | 307.4  |  |                               |  |   |  | 71.0  |   |
| 383+77.99 TO 389+16.72          | 2.00" RT shldr surface    | 538.73         | -                     | -                          | 10                     | 618.2                       | 153.7  |  |                               |  |   |  | 63.1  |   |
| 385+15.52 TO 389+16.72          | 2.25" pvt bottom          | 401.20         | VAR                   | 453.0                      | -                      | -                           | 122.0  |  |                               |  | 57.1  |  |   |   |
| 385+15.52 TO 403+14.01          | 2" pvt surface            | 1,798.49       | 14                    | -                          | -                      | -                           | 1,032.8  | 176.4  | 336.1                         |  |   |  |   |   |
| 387+97.15 TO 389+16.72          | gore bottom 2.25"         | 119.57         | -                     | -                          | -                      | 149.9                       | 79.4   |  |                               |  |   |  | 18.9  |   |
| 387+97.15 TO 389+16.72          | gore surface 2.00"        | 119.57         | -                     | -                          | -                      | 149.9                       | 39.7   |  |                               |  |   |  | 16.8  |   |
| 389+16.72 TO 400+99.56          | 0.75" pvt binder          | 1,182.84       | 14                    | 1,804.6                    | -                      | -                           | 465.2  | 79.4   |                               |  |   |  |   |   |
| 389+16.72 TO 389+76.72          | Transition 4.25" to 2.75" | 60.00          | -                     | -                          | -                      | -                           |  | 15.6   |                               |  |   |  |   |   |
| 389+16.72 TO 389+76.72          | Transition LT shldr       | 60.00          | -                     | -                          | 4                      | 26.8                        | 9.0  |  |                               |  |   |  | 6.4   |   |
| 389+16.72 TO 389+76.72          | Transition RT shldr       | 60.00          | -                     | -                          | 10                     | 54.8                        | 18.0   |  |                               |  |   |  | 13.0  |   |
| 389+76.72 TO 402+09.79          | 2.75" RT shldr surface    | 1,233.07       | -                     | -                          | 10                     | 1,739.9                     | 652.7  |  |                               |  |   | 47.7   | 172.7                                       | 1,831.8                                       |
| 389+76.72 TO 402+09.97          | 2.75" LT shldr surface    | 1,233.25       | -                     | -                          | 4                      | 461.3                       | 363.9  |  |                               |  |   | 33.8   | 82.3  | 1,293.3                                       |
| <b>CH 22 RAMP B</b>             |                           |                |                       |                            |                        |                             |  |  |                               |  |   |  |   |   |
| 298+41.15 TO 320+96.42          | 2" pvt surface            | 2,255.27       | 14                    | -                          | -                      | -                           | 975.1  |  |                               |  | 429.3   |  |   |   |
| 299+57.44 TO 312+00.65          | 2.75" LT shldr surface    | 1,243.21       | -                     | -                          | 4                      | 568.8                       | 389.2  |  |                               |  |   | 34.1   | 81.9  | 1,331.9                                       |
| 299+57.99 TO 312+00.65          | 2.75" RT shldr surface    | 1,242.66       | -                     | -                          | 10                     | 2,466.6                     | 749.9  |  |                               |  |   | 57.6   | 214.1                                       | 2,208.3                                       |
| 300+68.16 TO 312+60.65          | 0.75" pvt binder          | 1,192.49       | 14                    | 1,828.3                    | -                      | -                           | 941.9  | 80.5   | 150.5                         |  |   |  |   |   |
| 312+00.65 TO 312+89.37          | 2.25" LT shldr bottom     | 88.72          | -                     | -                          | 4                      | 36.9                        | 12.7   |  |                               |  |   |  | 4.7   |   |
| 312+00.65 TO 321+66.25          | 2.25" RT shldr bottom     | 965.60         | -                     | -                          | 10                     | 1,076.4                     | 290.5  |  |                               |  |   |  | 135.6                                       |   |
| 312+00.65 TO 321+66.25          | 2.00" RT shldr surface    | 965.60         | -                     | -                          | 10                     | 1,076.4                     |  |  |                               |  |   |  | 120.6                                       |   |
| 312+00.65 TO 312+89.37          | 2.00" LT shldr surface    | 88.72          | -                     | -                          | 4                      | 36.9                        |  |  |                               |  |   |  | 4.1   |   |
| 312+60.65 TO 320+96.42          | 2.25" pvt bottom          | 835.77         | VAR                   | 1,263.3                    | -                      | -                           | 652.1  |  |                               |  | 159.2   |  |   |   |
| <b>CH 22 RAMP C</b>             |                           |                |                       |                            |                        |                             |  |  |                               |  |   |  |   |   |
| 480+20.79 TO 488+98.29          | 2.25" pvt bottom          | 877.50         | VAR                   | 1,284.7                    | -                      | -                           | 665.9  |  |                               |  | 161.9   |  |   |   |
| 480+20.79 TO 489+58.29          | 2.25" LT shldr bottom     | 937.50         | -                     | -                          | 10                     | 1,005.7                     | 546.3  |  |                               |  |   |  | 126.7                                       |   |
| 480+20.79 TO 489+58.29          | 2.00" LT shldr surface    | 937.50         | -                     | -                          | 10                     | 2,279.5                     | 698.5  |  |                               |  |   | 57.1   | 112.6                                       | 2,190.3                                       |
| 480+20.79 TO 503+21.60          | 2" pvt surface            | 2,300.81       | 14                    | -                          | -                      | -                           | 979.4  |  |                               | 432.0  |   |  |   |   |
| 487+66.74 TO 489+58.29          | 2.25" RT shldr bottom     | 191.55         | -                     | -                          | 4                      | 105.5                       | 15.0   |  |                               |  |   |  | 13.3  |   |
| 487+66.74 TO 489+58.29          | 2.00" RT shldr surface    | 191.55         | -                     | -                          | 4                      | 690.6                       | 51.0   |  |                               |  |   | 34.1   | 11.8  | 1,444.5                                       |
| 488+98.29 TO 501+01.18          | 0.75" pvt binder          | 1,202.89       | 14                    | 1,863.1                    | -                      | -                           | 958.7  | 82.1   | 148.3                         |  |   |  |   |   |
| 489+58.29 TO 502+11.09          | 2.75" LT shldr surface    | 1,252.80       | -                     | -                          | 4                      | 1,273.8                     | 273.2  |  |                               |  |   |  | 196.2                                       |   |
| 489+58.29 TO 502+11.09          | 2.75" RT shldr surface    | 1,252.80       | -                     | -                          | 10                     | 585.1                       | 30.0   |  |                               |  |   |  | 90.1  |   |
| <b>CH 22 RAMP D</b>             |                           |                |                       |                            |                        |                             |  |  |                               |  |   |  |   |   |
| 98+50.69 TO 111+98.99           | 2" pvt surface            | 1,348.30       | 14                    | -                          | -                      | -                           | 761.2  |  |                               |  | 347.2   |  |   |   |
| 99+35.67 TO 111+98.99           | 2.75" RT shldr surface    | 1,263.32       | -                     | -                          | 4                      | 601.3                       | 368.9  |  |                               |  |   | 34.7   | 84.7  | 1,369.6                                       |
| 99+35.36 TO 111+98.99           | 2.75" LT shldr surface    | 1,263.63       | -                     | -                          | 10                     | 1,719.5                     | 487.2  |  |                               |  |   | 48.5   | 174.8                                       | 1,862.5                                       |
| 100+45.96 TO 111+98.99          | 0.75" pvt binder          | 1,153.03       | 14                    | 1,870.2                    | -                      | -                           | 962.9  | 82.4   | 182.4                         |  |   |  |   |   |
| 111+98.99 TO 113+05.24          | 2.00" RT shldr surface    | 106.25         | -                     | -                          | 4                      | 51.3                        | 5.0  |  |                               |  |   |  | 5.7   |   |
| 111+98.99 TO 117+97.88          | 2.00" LT shldr surface    | 598.89         | -                     | -                          | 10                     | 584.4                       | 150.0  |  |                               |  |   |  | 65.4  |   |
| 111+98.99 TO 113+05.24          | 2.25" RT shldr bottom     | 106.25         | -                     | -                          | 4                      | 51.3                        | 16.9   |  |                               |  |   |  | 6.5   |   |
| 111+98.99 TO 117+97.88          | 2.25" LT shldr bottom     | 598.89         | -                     | -                          | 10                     | 584.4                       | 161.4  |  |                               |  |   |  | 73.6  |   |
| 112+58.59 TO 116+56.48          | RAMP TAPER                | 397.89         | VAR                   | 495.2                      | -                      | -                           | 262.6  |  |                               |  | 62.4  |  |   |   |
| <b>I-74 / CH 22 RAMPS TOTAL</b> |                           |                |                       |                            |                        |                             | <b>14,649</b>                                      | <b>325</b>   | <b>673</b>                    | <b>1,545</b>   | <b>441</b>  | <b>348</b>   | <b>1,967</b>                                | <b>13,532</b>                                 |

Notes:  
 \* AREAS MEASURED IN CADD  
 Bituminous Materials calculations:  
 1 application at 0.05 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders except in Full Depth Pavement locations.  
 Additional 1 application for Typical #1 at 0.025 lb/sq ft for the Passing Lane  
 Additional 1 application for Typical #2 at 0.025 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders  
 1 application at 0.25 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavement locations.  
 Additional 3 applications at 0.025 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavment locations.  
 Additional 2 applications at 0.025 lb/sq ft for the Shoulders in Full Depth Pavment locations.

MODEL: Default  
FILE: Model - 10/27/2019 2:19:22 PM

|                                    |            |           |   |                           |  |  |  |                                |              |                    |                        |                    |
|------------------------------------|------------|-----------|---|---------------------------|--|--|--|--------------------------------|--------------|--------------------|------------------------|--------------------|
| USER NAME = bemery                 | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>SCHEDULES</b>          |  |  |  | F.A.I.<br>RTE.<br>I-74         | SECTION<br>* | COUNTY<br>**       | TOTAL<br>SHEETS<br>142 | SHEET<br>NO.<br>30 |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |   | SCALE: N.T.S.             |  |  |  | SHEET OF SHEETS STA. TO STA.   |              | CONTRACT NO. 70790 |                        |                    |
| PLOT DATE = 1/29/2019 - 2:19:22 PM | DATE -     | REVISED - |   | ILLINOIS FED. AID PROJECT |  |  |  | (10-7.8;92-8.9)RS-2(92-10)RS-3 |              |                    |                        |                    |
| ** CHAMPAIGN & VERMILION           |            |           |   |                           |  |  |  |                                |              |                    |                        |                    |

# HOT-MIX ASPHALT RESURFACING - RAMPS I-74/IL49

| LOCATION<br>STATION TO STATION  | COMMENT            | LENGTH<br>FOOT | LANE<br>WIDTH<br>FOOT | *<br>LANE<br>AREA<br>SQ YD | SHLDR<br>WIDTH<br>FOOT | *<br>SHLDR<br>AREA<br>SQ YD | BITUMINOUS<br>MATERIALS<br>(TACK COAT)<br>40600290 | POLYMERIZED<br>LEVELING<br>BINDER<br>(MACHINE<br>METHOD),<br>IL-9.5FG, N90<br>40600847 | POLYMERIZED<br>HOT-MIX ASPHALT<br>SURFACE<br>COURSE, STONE<br>MATRIX ASPHALT,<br>N80<br>40603153 | AGGREGATE<br>WEDGE<br>SHOULDERS,<br>TYPE B<br>48102100 | HOT-MIX<br>ASPHALT<br>SHOULDERS<br>48203100 | SHOULDER<br>RUMBLE<br>STRIPS, 16"<br>64200116 |
|---------------------------------|--------------------|----------------|-----------------------|----------------------------|------------------------|-----------------------------|--|--|--|--|---|---|
|                                 |                    |                |                       |                            |                        |                             | POUND  | TON  | TON  | TON  | TON   | FOOT  |
| <b>VERMILION COUNTY</b>         |                    |                |                       |                            |                        |                             |  |  |  |  |   |   |
| <b>IL 49 RAMP A</b>             |                    |                |                       |                            |                        |                             |  |  |  |  |   |   |
| 43+43.54 TO 49+00.76            | 2" RT shldr        | 557.22         | -                     | -                          | 10                     | 518.8                       | 289.2  |  |  |  | 58.1  |   |
| 43+43.54 TO 61+96.89            | 2" pvt surface     | 1,853.35       | 14                    | 2,923.2                    | -                      | -                           | 311.9  |  | 324.1  |  |   |   |
| 48+06.33 TO 49+00.76            | 2" LT shldr (gore) | 94.43          | -                     | -                          | 10                     | 65.4                        | 38.9   |  |  |  | 7.3   |   |
| 49+00.76 TO 61+03.65            | 0.75" pvt binder   | 1,202.89       | 14                    | 1,861.0                    | -                      | -                           | 750.4  | 78.2   |  |  |   |   |
| 49+00.76 TO 61+13.77            | 1.5" LT shldr      | 1,213.01       | -                     | -                          | 10                     | 539.9                       | 1,331.2  |  |  | 34.9   | 45.3  | 1,307.4                                       |
| 49+00.76 TO 61+48.14            | 1.5" RT shldr      | 1,247.38       | -                     | -                          | 10                     | 1,145.6                     | 640.3  |  |  | 42.7   | 96.2  | 1,804.6                                       |
| <b>IL 49 RAMP B</b>             |                    |                |                       |                            |                        |                             |  |  |  |  |   |   |
| 62+20.57 TO 83+74.97            | 2" pvt surface     | 2,154.40       | 14                    | 3,584.6                    | -                      | -                           | 914.3  |  | 398.4  |  |   |   |
| 63+06.04 TO 74+99.75            | 1.5" LT shldr      | 1,193.71       | -                     | -                          | 10                     | 552.4                       | 368.0  |  |  | 28.5   | 46.4  | 1,223.3                                       |
| 63+25.52 TO 74+99.75            | 1.5" RT shldr      | 1,174.23       | -                     | -                          | 10                     | 1,083.7                     | 605.1  |  |  | 58.3   | 91.0  | 2,049.5                                       |
| 63+35.52 TO 74+99.75            | 0.75" pvt binder   | 1,164.23       | 14                    | 1,796.3                    | -                      | -                           | 924.8  | 75.4   |  |  |   |   |
| 74+99.75 TO 75+29.29            | 2" LT shldr (gore) | 29.54          | -                     | -                          | 10                     | 12.0                        | 8.4  |  |  |  | 1.3   |   |
| 74+99.75 TO 83+75.01            | 2" RT shldr        | 875.26         | -                     | -                          | 10                     | 934.7                       | 508.1  |  |  |  | 104.7                                       |   |
| <b>IL 49 RAMP C</b>             |                    |                |                       |                            |                        |                             |  |  |  |  |   |   |
| 39+71.02 TO 48+92.74            | 2" LT shldr        | 921.72         | -                     | -                          | 10                     | 981.7                       | 534.0  |  |  |  | 110.0                                       |   |
| 39+71.02 TO 61+84.84            | 2" pvt surface     | 2,213.82       | 14                    | 3,607.6                    | -                      | -                           | 762.5  |  | 400.8  |  |   |   |
| 48+25.92 TO 48+92.74            | 2" RT shldr (gore) | 66.82          | -                     | -                          | 10                     | 32.0                        | 21.1   |  |  |  | 3.6   |   |
| 48+92.74 TO 60+78.08            | 0.75" pvt binder   | 1,185.34       | 14                    | 1,835.8                    | -                      | -                           | 922.4  | 77.1   |  |  |   |   |
| 48+92.74 TO 60+93.42            | 1.5" LT shldr      | 1,200.68       | -                     | -                          | 10                     | 111.3                       | 620.2  |  |  | 50.2   | 93.4  | 2,122.4                                       |
| 48+92.74 TO 60+90.22            | 1.5" RT shldr      | 1,197.48       | -                     | -                          | 10                     | 536.4                       | 1,305.7  |  |  | 34.3   | 45.1  | 1,264.3                                       |
| <b>IL 49 RAMP D</b>             |                    |                |                       |                            |                        |                             |  |  |  |  |   |   |
| 0+13.34 TO 76+77.88             | 2" pvt surface     | 7,664.54       | 14                    | 2,898.1                    | -                      | -                           | 1,035.3  |  | 321.0  |  |   |   |
| 0+61.51 TO 71+25.66             | 1.5" LT shldr      | 7,064.15       | -                     | -                          | 10                     | 1,099.1                     | 1,201.0  |  |  | 42.7   | 92.3  | 1,803.9                                       |
| 0+89.26 TO 71+25.66             | 1.5" RT shldr      | 7,036.40       | -                     | -                          | 10                     | 539.9                       | 946.6  |  |  | 35.2   | 45.3  | 1,324.1                                       |
| 0+99.54 TO 71+25.66             | 0.75" pvt binder   | 7,026.12       | 14                    | 1,893.6                    | -                      | -                           | 1,618.6  | 79.5   |  |  |   |   |
| 71+25.66 TO 72+25.78            | 2" RT shldr        | 100.12         | -                     | -                          | 10                     | 49.2                        | 32.2   |  |  |  | 5.5   |   |
| 71+25.66 TO 76+77.88            | 2" LT shldr        | 552.22         | -                     | -                          | 10                     | 537.4                       | 233.4  |  |  |  | 60.2  |   |
| <b>I-74 / IL 49 RAMPS TOTAL</b> |                    |                |                       |                            |                        |                             | <b>15,924</b>                                      | <b>311</b>   | <b>1,444</b>   | <b>327</b>   | <b>906</b>                                  | <b>12,900</b>                                 |

Notes:  
 \* AREAS MEASURED IN CADD  
 Bituminous Materials calculations:  
 1 application at 0.05 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders except in Full Depth Pavement locations.  
 Additional 1 application for Typical #1 at 0.025 lb/sq ft for the Passing Lane  
 Additional 1 application for Typical #2 at 0.025 lb/sq ft for the Passing Lane, Driving Lane, and Shoulders  
 1 application at 0.25 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavement locations.  
 Additional 3 applications at 0.025 lb/sq ft for the Passing Lane and Driving Lane in Full Depth Pavment locations.  
 Additional 2 applications at 0.025 lb/sq ft for the Shoulders in Full Depth Pavment locations.

MODEL: Default  
FILE: Model - ...03270790-shr-schedule.dgn

|  |            |           |   |                           |  |  |  |                              |              |                    |                        |                    |
|--|------------|-----------|---|---------------------------|--|--|--|------------------------------|--------------|--------------------|------------------------|--------------------|
| USER NAME = bemery   | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>SCHEDULES</b>          |  |  |  | F.A.I.<br>RTE.<br>I-74       | SECTION<br>* | COUNTY<br>**       | TOTAL<br>SHEETS<br>142 | SHEET<br>NO.<br>31 |
| PLOT SCALE = 100,0000' / in.                                   | CHECKED -  | REVISED - |   | SCALE: N.T.S.             |  |  |  | SHEET OF SHEETS STA. TO STA. |              | CONTRACT NO. 70790 |                        |                    |
| PLOT DATE = 1/29/2019 - 2:19:27 PM                             | DATE -     | REVISED - |   | ILLINOIS FED. AID PROJECT |  |  |  |                              |              |                    |                        |                    |
| * (10-7,8;92-8,9)RS-2(92-10)RS-3      ** CHAMPAIGN & VERMILION |            |           |   |                           |  |  |  |                              |              |                    |                        |                    |

## PAVEMENT PATCHES

| LOCATION STATION        | PATCH NO. | SIDE | LENGTH | WIDTH | AREA  | CLASS B PATCHES, TYPE II, 15 INCH                            | CLASS B PATCHES, TYPE III, 15 INCH | CLASS B PATCHES, TYPE IV, 15 INCH | WELDED WIRE REINFORCEMENT | SAW CUTS | DOWEL BARS 1 1/2" | TIE BARS 3/4" |
|-------------------------|-----------|------|--------|-------|-------|--|------------------------------------|-----------------------------------|---------------------------|----------|-------------------|---------------|
|                         |           |      | FOOT   | FOOT  | SQ YD | 4421031  | 44201035                           | 44201037                          | 44200050                  | 44213200 | 44201299          | 44213204      |
| <b>CHAMPAIGN COUNTY</b> |           |      |        |       |       |  |                                    |                                   |                           |          |                   |               |
| <b>I-74 EB</b>          |           |      |        |       |       |  |                                    |                                   |                           |          |                   |               |
| 895+20.64               | 1         | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 895+20.64               | 2         | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 902+80.96               | 3         | DL   | 6      | 12    | 8.0   | 8.0  |                                    |                                   |                           | 36       | 20                |               |
| 907+87.84               | 4         | DL   | 6      | 12    | 8.0   | 8.0  |                                    |                                   |                           | 36       | 20                |               |
| 918+91.36               | 5         | DL   | 18     | 12    | 24.0  |  | 24.0                               |                                   | 24.0                      | 60       | 20                |               |
| 921+92.32               | 6         | DL   | 18     | 12    | 24.0  |  | 24.0                               |                                   | 24.0                      | 60       | 20                |               |
| 939+34.72               | 7         | DL   | 12     | 12    | 16.0  |  | 16.0                               |                                   | 16.0                      | 48       | 20                |               |
| 944+20.48               | 8         | DL   | 10     | 12    | 13.3  | 13.3   |                                    |                                   |                           | 44       | 20                |               |
| 986+39.20               | 9         | DL   | 20     | 12    | 26.7  |  |                                    | 26.7                              | 26.7                      | 64       | 20                |               |
| 990+08.80               | 10        | DL   | 10     | 12    | 13.3  | 13.3   |                                    |                                   |                           | 44       | 20                |               |
| 990+08.80               | 11        | PL   | 10     | 12    | 13.3  | 13.3   |                                    |                                   |                           | 44       | 20                |               |
| 1001+75.68              | 12        | DL   | 6      | 12    | 8.0   | 8.0  |                                    |                                   |                           | 36       | 20                |               |
| 1001+75.68              | 13        | PL   | 6      | 12    | 8.0   | 8.0  |                                    |                                   |                           | 36       | 20                |               |
| 1002+54.88              | 14        | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 1002+54.88              | 15        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 1006+56.16              | 16        | DL   | 6      | 12    | 8.0   | 8.0  |                                    |                                   |                           | 36       | 20                |               |
| 1008+25.12              | 17        | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| <b>I-74 WB</b>          |           |      |        |       |       |  |                                    |                                   |                           |          |                   |               |
| 893+30.56               | 18        | DL   | 28     | 12    | 37.3  |  |                                    | 37.3                              | 37.3                      | 80       | 20                | 14            |
| 894+73.12               | 19        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 894+73.12               | 20        | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 895+62.88               | 21        | DL   | 10     | 12    | 13.3  | 13.3   |                                    |                                   |                           | 44       | 20                |               |
| 897+58.24               | 22        | DL   | 16     | 12    | 21.3  |  | 21.3                               |                                   | 21.3                      | 56       | 20                |               |
| 908+93.44               | 23        | DL   | 18     | 12    | 24.0  |  | 24.0                               |                                   | 24.0                      | 60       | 20                |               |
| 917+11.84               | 24        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 917+11.84               | 25        | DL   | 12     | 12    | 16.0  |  | 16.0                               |                                   | 16.0                      | 48       | 20                |               |
| 922+76.80               | 26        | PL   | 16     | 12    | 21.3  |  | 21.3                               |                                   | 21.3                      | 56       | 20                |               |
| 922+76.80               | 27        | DL   | 16     | 12    | 21.3  |  | 21.3                               |                                   | 21.3                      | 56       | 20                |               |
| 972+29.44               | 28        | PL   | 6      | 12    | 8.0   | 8.0  |                                    |                                   |                           | 36       | 20                |               |
| 972+29.44               | 29        | DL   | 6      | 12    | 8.0   | 8.0  |                                    |                                   |                           | 36       | 20                |               |
| 973+61.44               | 30        | PL   | 10     | 12    | 13.3  | 13.3   |                                    |                                   |                           | 44       | 20                |               |
| 973+61.44               | 31        | DL   | 25     | 12    | 33.3  |  |                                    | 33.3                              | 33.3                      | 74       | 20                | 13            |
| 982+22.08               | 32        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 982+22.08               | 33        | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 995+79.04               | 34        | PL   | 10     | 12    | 13.3  | 13.3   |                                    |                                   |                           | 44       | 20                |               |
| 995+79.04               | 35        | DL   | 10     | 12    | 13.3  | 13.3   |                                    |                                   |                           | 44       | 20                |               |
| 1048+06.24              | 36        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 1048+06.24              | 37        | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           | 40       | 20                |               |
| 1051+91.68              | 38        | PL   | 12     | 12    | 16.0  |  | 16.0                               |                                   | 16.0                      | 48       | 20                |               |
| 1051+91.68              | 39        | DL   | 12     | 12    | 16.0  |  | 16.0                               |                                   | 16.0                      | 48       | 20                |               |
|                         |           |      |        |       |       | <b>PROJECT TOTAL</b>   |                                    |                                   |                           |          |                   |               |
|                         |           |      |        |       |       | 399      232      127      359      2,464      1,080      38 |                                    |                                   |                           |          |                   |               |

## PAVEMENT PATCHES

| LOCATION STATION        | PATCH NO. | SIDE | LENGTH | WIDTH | AREA  | CLASS B PATCHES, TYPE II, 15 INCH                            | CLASS B PATCHES, TYPE III, 15 INCH | CLASS B PATCHES, TYPE IV, 15 INCH | WELDED WIRE REINFORCEMENT | SAW CUTS | DOWEL BARS 1 1/2" | TIE BARS 3/4" |
|-------------------------|-----------|------|--------|-------|-------|--|------------------------------------|-----------------------------------|---------------------------|----------|-------------------|---------------|
|                         |           |      | FOOT   | FOOT  | SQ YD | 4421031  | 44201035                           | 44201037                          | 44200050                  | 44213200 | 44201299          | 44213204      |
| <b>RAMP C</b>           |           |      |        |       |       |  |                                    |                                   |                           |          |                   |               |
| 497+25.00               | 40        |      | 3      | 28    | 9.3   | 9.3  |                                    |                                   |                           |          | 62                | 20            |
| 502+25.00               | 41        |      | 4      | 18    | 8.0   | 8.0  |                                    |                                   |                           |          | 44                | 20            |
| <b>VERMILION COUNTY</b> |           |      |        |       |       |  |                                    |                                   |                           |          |                   |               |
| <b>I-74 EB</b>          |           |      |        |       |       |  |                                    |                                   |                           |          |                   |               |
| 1077+04.96              | 42        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
| 1136+66.08              | 43        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
| 1175+52.16              | 44        | DL   | 6      | 12    | 8.0   | 8.0  |                                    |                                   |                           |          | 36                | 20            |
| 1175+52.16              | 45        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
| <b>I-74 WB</b>          |           |      |        |       |       |  |                                    |                                   |                           |          |                   |               |
| 1056+51.04              | 46        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
| 1056+51.04              | 47        | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
| 1072+08.64              | 48        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
| 1072+08.64              | 49        | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
| 1098+69.76              | 50        | PL   | 12     | 12    | 16.0  |  | 16.0                               |                                   | 16.0                      |          | 48                | 20            |
| 1098+69.76              | 51        | DL   | 12     | 12    | 16.0  |  | 16.0                               |                                   | 16.0                      |          | 48                | 20            |
| 1128+26.56              | 52        | DL   | 22     | 12    | 29.3  |  |                                    | 29.3                              | 29.3                      |          | 68                | 20            |
| 1151+76.16              | 53        | PL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
| 1151+76.16              | 54        | DL   | 8      | 12    | 10.7  | 10.7   |                                    |                                   |                           |          | 40                | 20            |
|                         |           |      |        |       |       | <b>PROJECT TOTAL</b>   |                                    |                                   |                           |          |                   |               |
|                         |           |      |        |       |       | 399      232      127      359      2,464      1,080      38 |                                    |                                   |                           |          |                   |               |

NOTES: DL = DRIVING LANE, PL = PASSING LANE

MODEL: Default  
FILE: Model - 10/27/2019 3:56:42 PM

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 3:56:42 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |                              |
|------------------|------------------------------|
| <b>SCHEDULES</b> |                              |
| SCALE: N.T.S.    | SHEET OF SHEETS STA. TO STA. |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 32        |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION

## PIPE CULVERTS & STORM SEWERS

| LOCATION<br>STATION TO STATION | SIDE  | TRENCH<br>BACKFILL<br>20800150<br>CU YD | PIPE CULVERTS,<br>CLASS A,<br>TYPE 1 |                         | PIPE<br>CULVERTS,<br>CLASS A 18"<br>(JACKED)<br>542JA018<br>FOOT | PIPE<br>CULVERTS,<br>CLASS D,<br>TYPE 1 36"<br>542D0241<br>FOOT | PRECAST REINFORCED<br>CONCRETE FLARED<br>END SECTIONS |                         | METAL<br>FLARED END<br>SECTIONS<br>36"<br>5426736<br>EACH | STORM<br>SEWERS<br>JACKED IN<br>PLACE, 24"<br>55200900<br>FOOT | FILLING<br>EXISTING<br>CULVERTS<br>Z0023600<br>EACH | MANHOLES, TYPE<br>A, 4'-DIAMETER,<br>TYPE 1 FRAME,<br>CLOSED LID<br>60218400<br>EACH | MANHOLES, TYPE<br>A, 6'-DIAMETER,<br>TYPE 1 FRAME,<br>CLOSED LID<br>60223800<br>EACH |
|--------------------------------|-------|---|--------------------------------------|-------------------------|--|---|---|-------------------------|---|--|---|--|--|
|                                |       |   | 12"<br>542A0217<br>FOOT              | 18"<br>542A0223<br>FOOT |  |   | 12"<br>54213657<br>EACH                               | 18"<br>54213663<br>EACH |   |  |   |  |  |
|                                |       |   |                                      |                         |  |   |   |                         |   |  |   |  |  |
| <b>CHAMPAIGN COUNTY</b>        |       |   |                                      |                         |  |   |   |                         |   |  |   |  |  |
| <b>I-74</b>                    |       |   |                                      |                         |  |   |   |                         |   |  |   |  |  |
| 999+00.00                      | LT    |   |                                      |                         | 93   |   |   | 1                       |   |  |   | 1  |  |
| 999+00.00                      | RT    |   |                                      |                         | 82   |   |   | 1                       |   |  |   |  |  |
| 1014+05.00                     | LT/RT |   |                                      |                         |  |   |   |                         | 220   |  |   |  | 2  |
| <b>CH 22 RAMP C</b>            |       |   |                                      |                         |  |   |   |                         |   |  |   |  |  |
| 497+25.00                      | LT/RT | 4                                       | 36                                   |                         |  |   | 2   |                         |   |  |   |  |  |
| 502+25.00                      | LT/RT | 12                                      |                                      | 48                      |  |   |   | 2                       |   |  |   |  |  |
| 502+32.00                      | LT/RT |   |                                      |                         |  |   |   |                         |   | 1  |   |  |  |
| <b>CH 22</b>                   |       |   |                                      |                         |  |   |   |                         |   |  |   |  |  |
| 39+59.15 TO 40+09.14           | FER   |   |                                      |                         |  | 50  |   |                         | 2   |  |   |  |  |
| <b>PROJECT TOTAL</b>           |       | <b>16</b>                               | <b>36</b>                            | <b>48</b>               | <b>175</b>   | <b>50</b>   | <b>2</b>  | <b>4</b>                | <b>2</b>  | <b>220</b>   | <b>1</b>  | <b>1</b>   | <b>2</b>   |

## TEMPORARY RUMBLE STRIPS (SPECIAL)

| LOCATION<br>STATION TO STATION | COMMENTS     | TEMPORARY<br>RUMBLE STRIPS<br>(SPECIAL)<br>X2700001 |
|--------------------------------|--------------|---|
|                                |              | FOOT  |
| <b>CHAMPAIGN COUNTY</b>        |              |   |
| <b>I-74 EB</b>                 | DRIVING LANE | 8   |
| <b>I-74 EB</b>                 | PASSING LANE | 8   |
| <b>VERMILION COUNTY</b>        |              |   |
| <b>I-74 WB</b>                 | DRIVING LANE | 8   |
| <b>I-74 WB</b>                 | PASSING LANE | 8   |
| <b>PROJECT TOTAL</b>           |              | <b>32</b>   |

SEE SPECIAL PROVISION AND DETAIL SHEET.

## PIPE DRAIN EXTENSION

| LOCATION<br>STATION TO STATION | SIDE | CONCRETE<br>COLLAR<br>54248510 | * PIPE<br>DRAINS, 6"<br>60100915 | PIPE DRAINS,<br>12"<br>60100945 | * CONCRETE<br>HEADWALLS<br>FOR PIPE<br>DRAINS<br>60100060 | METAL<br>FLARED END<br>SECTIONS<br>12"<br>54262712 | * STONE<br>RIPRAP,<br>CLASS A3<br>28100105 |
|--------------------------------|------|--------------------------------|----------------------------------|---------------------------------|---|--|--|
|                                |      | CU YD                          | FOOT                             | FOOT                            | EACH  | EACH   | SQ YD                                      |
|                                |      |                                |                                  |                                 |   |  |  |
| <b>CHAMPAIGN COUNTY</b>        |      |                                |                                  |                                 |   |  |  |
| <b>CH 22</b>                   |      |                                |                                  |                                 |   |  |  |
| 43+01.00                       | RT   | 0.21                           |                                  | 6                               |   | 1  | 1.0  |
| 44+00.00                       | LT   | 0.21                           | 6                                |                                 | 1   |  | 1.0  |
| 44+99.00                       | RT   | 0.21                           |                                  | 25                              |   | 1  | 1.0  |
| 46+76.00                       | RT   | 0.21                           |                                  | 21                              |   | 1  | 1.0  |
| 48+77.00                       | RT   | 0.21                           |                                  | 21                              |   | 1  | 1.0  |
| 51+25.00                       | RT   | 0.21                           |                                  | 18                              |   | 1  | 1.0  |
| 53+24.00                       | RT   | 0.21                           |                                  | 12                              |   | 1  | 1.0  |
| 55+02.00                       | RT   | 0.21                           |                                  | 5                               |   | 1  | 1.0  |
| <b>PROJECT TOTAL</b>           |      | <b>1.7</b>                     | <b>6</b>                         | <b>108</b>                      | <b>1</b>  | <b>7</b>   | <b>8</b>                                   |

\* SEE OTHER SCHEDULES FOR ADDITIONAL QUANTITIES.

## GUARDRAIL

| LOCATION<br>STATION TO STATION        | SIDE      | STEEL<br>PLATE BEAM<br>GUARDRAIL,<br>TYPE A, 6<br>FOOT POSTS<br>63000001 | TRAFFIC<br>BARRIER<br>TERMINAL,<br>TYPE 1<br>(SPECIAL)<br>TANGENT<br>63100167 | TRAFFIC<br>BARRIER<br>TERMINAL,<br>TYPE 6A<br>63100087 | GUARDRAIL<br>REFLECTORS,<br>TYPE A<br>78200005 | TERMINAL<br>MARKER -<br>DIRECT<br>APPLIED<br>72501000 |
|---------------------------------------|-----------|--|---|--|--|---|
|                                       |           | FOOT   | EACH  | EACH   | EACH   | EACH  |
|                                       |           |  |   |  |  |   |
| <b>CHAMPAIGN COUNTY</b>               |           |  |   |  |  |   |
| <b>COUNTY RD 2500 E (SN 010-0183)</b> |           |  |   |  |  |   |
| NW QUAD                               | APPROACH  | 525.0  | 1   | 1  | 7  | 1   |
| NE QUAD                               | DEPARTURE | 475.0  | 1   | 1  | 6  | 1   |
| SW QUAD                               | DEPARTURE | 475.0  | 1   | 1  | 6  | 1   |
| SE QUAD                               | APPROACH  | 475.0  | 1   | 1  | 6  | 1   |
| <b>CH 22</b>                          |           |  |   |  |  |   |
| 43+62.00 TO 44+12.00                  | LT        |  | 1   |  | 1  | 1   |
| 55+76.00 TO 56+26.00                  | RT        |  | 1   |  | 1  | 1   |
| <b>VERMILION COUNTY</b>               |           |  |   |  |  |   |
| <b>100 EAST RD (SN 092-0146)</b>      |           |  |   |  |  |   |
| NW QUAD                               | APPROACH  | 625.0  | 1   | 1  | 8  | 1   |
| NE QUAD                               | DEPARTURE | 525.0  | 1   | 1  | 7  | 1   |
| SW QUAD                               | DEPARTURE | 425.0  | 1   | 1  | 6  | 1   |
| SE QUAD                               | APPROACH  | 425.0  | 1   | 1  | 6  | 1   |
| <b>PROJECT TOTAL</b>                  |           | <b>3,950.0</b>   | <b>10</b>   | <b>8</b>   | <b>54</b>                                      | <b>10</b>   |

## ISLANDS

| LOCATION<br>STATION TO STATION | PORTLAND CEMENT<br>CONCRETE BASE<br>COURSE WIDENING<br>11"<br>35400510 | CONCRETE<br>MEDIAN, TYPE SM<br>(DOWELLED)<br>X6061702 |
|--------------------------------|--|---|
|                                | SQ YD  | SQ FT   |
|                                |  |   |
| <b>VERMILION COUNTY</b>        |  |   |
| <b>IL 49 RAMP A</b>            |  |   |
| 61+80.00 LT                    | 0.2  | 107   |
| <b>IL 49 RAMP D</b>            |  |   |
| 0+25.00 RT                     | 0.3  | 73.2  |
| <b>PROJECT TOTAL</b>           |  | <b>181</b>  |

MODEL: Default  
FILE: Model - 10/27/2019 3:56:46 PM

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:56:46 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |          |        |              |
|------------------|----------|--------|--------------|
| <b>SCHEDULES</b> |          |        |              |
| SCALE: N.T.S.    | SHEET OF | SHEETS | STA. TO STA. |

| F.A.I.<br>RTE.            | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
|---------------------------|---------|--------|-----------------|--------------|
| I-74                      | *       | **     | 142             | 33           |
| CONTRACT NO. 70790        |         |        |                 |              |
| ILLINOIS FED. AID PROJECT |         |        |                 |              |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

# MEDIAN DRAINAGE & UNDERDRAINS

| LOCATION STATION TO STATION | SIDE       | PIPE UNDERDRAINS 4" 60107600 | PIPE UNDERDRAINS 4", (SPECIAL) 60108100 | PIPE UNDERDRAINS, TYPE 2, 6" 60108206 | * PIPE DRAINS 6" 60100915 | * CONCRETE HEADWALLS FOR PIPE DRAINS 60100060 | COARSE AGGREGATE X0795800 | INLETS, TYPE A, TYPE 8 GRATE 60236200 | UNDERDRAIN CONNECTION TO STRUCTURE Z0076870 | DIRECTIONAL BORING X0325379 | CLEANING UNDERDRAIN OUTLETS X0320157 | PIPE UNDERDRAIN REMOVAL Z0040530 | REMOVE CONCRETE HEADWALLS FOR PIPE DRAINS X6015000 | SHOULDER REMOVAL AND REPLACEMENT 60100070 |
|-----------------------------|------------|------------------------------|---|---------------------------------------|---------------------------|---|---------------------------|---------------------------------------|---|-----------------------------|--------------------------------------|----------------------------------|--|---|
|                             |            | FOOT                         | FOOT                                    | FOOT                                  | FOOT                      | EACH  | TON                       | EACH                                  | EACH  | FOOT                        | EACH                                 | FOOT                             | EACH   | FOOT                                      |
| <b>CHAMPAIGN COUNTY</b>     |            |                              |   |                                       |                           |   |                           |                                       |   |                             |                                      |                                  |  |   |
| <b>I-74</b>                 |            |                              |   |                                       |                           |   |                           |                                       |   |                             |                                      |                                  |  |   |
| 880+00.00 TO 898+29.00      | EB INSIDE  |                              |   |                                       |                           |   |                           |                                       |   |                             | 1                                    |                                  |  |   |
| 880+00.00 TO 898+29.00      | EB OUTSIDE |                              |   |                                       |                           |   |                           |                                       |   |                             | 5                                    |                                  |  |   |
| 880+00.00 TO 898+30.00      | WB INSIDE  |                              |   |                                       |                           |   |                           |                                       |   |                             |                                      |                                  |  |   |
| 880+00.00 TO 898+30.00      | WB OUTSIDE |                              |   |                                       |                           |   |                           |                                       |   |                             | 5                                    |                                  |  |   |
| 883+76.74                   | WB INSIDE  |                              | 16                                      |                                       | 72                        | 1   |                           | 1                                     | 1   | 72                          |                                      | 13.5                             |  | 4   |
| 885+97.61                   | EB INSIDE  |                              | 19                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 13.5                             |  | 4   |
| 885+97.61 TO 888+02.55      |            |                              |   | 207                                   |                           |   | 31.3                      |                                       | 1   |                             |                                      |                                  |  |   |
| 904+29.00 TO 979+40.75      | EB INSIDE  |                              |   |                                       |                           |   |                           |                                       |   |                             |                                      |                                  |  |   |
| 904+29.00 TO 979+40.75      | EB OUTSIDE |                              |   |                                       |                           |   |                           |                                       |   |                             | 16                                   |                                  |  |   |
| 904+29.00 TO 979+41.75      | WB INSIDE  |                              |   |                                       |                           |   |                           |                                       |   |                             | 2                                    |                                  |  |   |
| 904+29.00 TO 979+41.75      | WB OUTSIDE |                              |   |                                       |                           |   |                           |                                       |   |                             | 17                                   |                                  |  |   |
| 907+63.45                   | EB INSIDE  |                              | 24                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 20.0                             |  | 4   |
| 907+63.45 TO 908+01.30      |            |                              |   | 38                                    |                           |   | 5.8                       |                                       | 1   |                             |                                      |                                  |  |   |
| 907+72.41                   | WB INSIDE  |                              | 16                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 11.0                             |  | 4   |
| 916+05.87                   | EB INSIDE  |                              | 23                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 22.0                             |  | 4   |
| 916+08.50                   | EB         |                              |   |                                       | 80                        | 1   |                           | 1                                     | 2   | 80                          |                                      |                                  |  |   |
| 916+10.33                   | WB INSIDE  |                              | 16                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 14.0                             |  | 4   |
| 919+05.57 TO 922+15.59      |            |                              |   | 311                                   |                           |   | 47.2                      |                                       | 1   |                             |                                      |                                  |  |   |
| 922+09.29                   | WB INSIDE  |                              | 16                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 13.5                             |  | 4   |
| 922+15.57                   | EB INSIDE  |                              | 24                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 23.5                             |  | 4   |
| 930+15.03                   | EB INSIDE  |                              | 23                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 21.0                             |  | 4   |
| 930+19.00                   | EB         |                              |   |                                       | 76                        | 1   |                           | 1                                     | 2   | 76                          |                                      |                                  |  |   |
| 930+20.44                   | WB INSIDE  |                              | 16                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 14.0                             |  | 4   |
| 934+99.99                   | EB INSIDE  |                              | 24                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 21.0                             |  | 4   |
| 935+00.00 TO 939+01.91      |            |                              |   | 402                                   |                           |   | 61.0                      |                                       | 1   |                             |                                      |                                  |  |   |
| 935+04.30                   | WB INSIDE  |                              | 16                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 13.5                             |  | 4   |
| 940+06.46                   | WB INSIDE  |                              | 17                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 14.0                             |  | 4   |
| 940+06.46 TO 946+01.87      |            |                              |   | 596                                   |                           |   | 90.5                      |                                       | 1   |                             |                                      |                                  |  |   |
| 944+99.35                   | EB INSIDE  |                              | 24                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 22.5                             |  | 4   |
| 945+11.70                   | WB INSIDE  |                              | 17                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 14.0                             |  | 4   |
| 950+54.43                   | WB INSIDE  |                              | 17                                      |                                       |                           |   |                           |                                       |   |                             |                                      |                                  |  | 4   |
| 950+54.43 TO 958+91.80      |            |                              |   | 838                                   |                           |   | 127.1                     |                                       | 1   |                             |                                      | 17.0                             |  |   |
| 954+82.09                   | WB INSIDE  |                              | 17                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 13.5                             |  | 4   |
| 955+00.50                   | EB INSIDE  |                              | 23                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 22.5                             |  | 4   |
| 958+38.87                   | EB INSIDE  |                              | 24                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 23.0                             |  | 4   |
| 958+57.19                   | WB INSIDE  |                              | 17                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 14.0                             |  | 4   |
| 962+05.52 TO 967+13.15      |            |                              |   | 513                                   |                           |   | 77.7                      |                                       | 1   |                             |                                      |                                  |  |   |
| 967+09.83                   | EB INSIDE  |                              | 23                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 21.0                             |  | 4   |
| 967+13.00                   | WB INSIDE  |                              | 18                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 11.5                             |  | 4   |
| 974+89.99                   | EB INSIDE  |                              | 22                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 22.0                             |  | 4   |
| 974+91.00                   | WB         |                              |   |                                       | 61                        | 1   |                           | 1                                     | 1   | 61                          |                                      |                                  |  |   |
| 977+02.00                   | WB         |                              |   |                                       | 66                        | 1   |                           | 1                                     | 1   | 66                          |                                      |                                  |  |   |
| 977+02.40                   | WB INSIDE  |                              | 17                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 14.5                             |  | 4   |
| 979+40.75 TO 1025+15.08     | EB INSIDE  |                              |   |                                       |                           |   |                           |                                       |   |                             |                                      |                                  |  |   |
| 979+40.75 TO 1025+15.08     | EB OUTSIDE |                              |   |                                       |                           |   |                           |                                       |   |                             | 8                                    |                                  |  |   |
| 979+40.75 TO 1025+16.08     | WB INSIDE  |                              |   |                                       |                           |   |                           |                                       |   |                             |                                      |                                  |  |   |
| 979+40.75 TO 1025+16.08     | WB OUTSIDE |                              |   |                                       |                           |   |                           |                                       |   |                             | 9                                    |                                  |  |   |
| 980+05.11                   | WB INSIDE  |                              | 22                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 20.5                             |  | 4   |
| 980+06.00                   | WB         |                              |   |                                       | 78                        | 1   |                           | 1                                     | 1   | 78                          |                                      |                                  |  |   |
| 983+04.80                   | WB INSIDE  |                              | 22                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 19.0                             |  | 4   |
| 983+06.12                   | EB         |                              |   |                                       | 69                        | 1   |                           | 1                                     | 2   | 69                          |                                      |                                  |  |   |
| 983+22.29                   | EB INSIDE  |                              | 23                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 13.0                             |  | 4   |
| 988+04.56                   | EB INSIDE  |                              | 16                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 13.0                             |  | 4   |
| 988+04.56 TO 992+01.64      |            |                              |   | 398                                   |                           |   | 60.3                      |                                       | 1   |                             |                                      |                                  |  |   |
| 988+08.30                   | WB INSIDE  |                              | 24                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 20.0                             |  | 4   |
| 995+89.04                   | WB INSIDE  |                              | 24                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 20.5                             |  | 4   |
| 995+89.04 TO 998+98.00      |            |                              |   | 309                                   |                           |   | 46.9                      |                                       | 1   |                             |                                      |                                  |  |   |
| 997+07.24                   | EB INSIDE  |                              | 18                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 14.0                             |  | 4   |
| 1003+53.79                  | WB INSIDE  |                              | 18                                      |                                       |                           |   |                           |                                       |   |                             |                                      | 15.0                             |  | 4   |

**SCHEDULE CONTINUES ON NEXT SHEET**

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemory                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:56:51 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |       |      |        |
|------------------|-------|------|--------|
| <b>SCHEDULES</b> |       |      |        |
| SCALE: N.T.S.    | SHEET | OF   | SHEETS |
| STA.             | TO    | STA. | TO     |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 34        |
| CONTRACT NO. 70790 |         |        |              |           |

MODEL: Default  
FILE: Mchamc...03270905-hc-schedule.dgn

# MEDIAN DRAINAGE & UNDERDRAINS

| LOCATION<br>STATION TO STATION                      | SIDE       | PIPE<br>UNDERDRAINS<br>4"<br>60107600 | PIPE<br>UNDERDRAINS<br>4", (SPECIAL)<br>60108100 | PIPE<br>UNDERDRAINS,<br>TYPE 2, 6"<br>60108206 | * PIPE DRAINS<br>6"<br>60100915 | * CONCRETE<br>HEADWALLS<br>FOR PIPE<br>DRAINS<br>60100060 | COARSE<br>AGGREGATE<br>X0795800 | INLETS, TYPE A,<br>TYPE 8 GRATE<br>60236200 | UNDERDRAIN<br>CONNECTION<br>TO STRUCTURE<br>Z0076870 | DIRECTIONAL<br>BORING<br>X0325379 | CLEANING<br>UNDERDRAIN<br>OUTLETS<br>X0320157 | PIPE<br>UNDERDRAIN<br>REMOVAL<br>Z0040530 | REMOVE<br>CONCRETE<br>HEADWALLS FOR<br>PIPE DRAINS<br>X6015000 | SHOULDER<br>REMOVAL AND<br>REPLACEMENT<br>60100070 |
|---|------------|---------------------------------------|--|--|---------------------------------|---|---------------------------------|---|--|-----------------------------------|---|---|--|--|
|   |            | FOOT                                  | FOOT   | FOOT   | FOOT                            | EACH  | TON                             | EACH  | EACH   | FOOT                              | EACH  | FOOT                                      | EACH   | FOOT   |
| 1003+53.79 TO 1011+52.59                            |            |                                       |  | 799  |                                 |   | 121.3                           |   | 1  |                                   |   |   |  |  |
| 1003+55.78  | EB INSIDE  |                                       | 22   |  |                                 |   |                                 |   |  |                                   |   | 23.0                                      |  | 4  |
| 1011+56.48 TO 1015+15.63                            |            |                                       |  | 360  |                                 |   | 54.5                            |   | 1  |                                   |   |   |  |  |
| 1012+22.72  | EB INSIDE  |                                       | 24   |  |                                 |   |                                 |   |  |                                   |   | 20.0                                      |  | 4  |
| 1015+08.01  | EB INSIDE  |                                       | 24   |  |                                 |   |                                 |   |  |                                   |   | 21.0                                      |  | 4  |
| 1015+15.40  | WB INSIDE  |                                       | 17   |  |                                 |   |                                 |   |  |                                   |   | 16.0                                      |  | 4  |
| 1019+05.92 TO 1022+14.78                            |            |                                       |  | 309  |                                 |   | 46.9                            |   | 1  |                                   |   |   |  |  |
| 1022+14.77  | WB INSIDE  |                                       | 17   |  |                                 |   |                                 |   |  |                                   |   | 17.0                                      |  | 4  |
| 1025+15.08 TO 1053+06.00                            | EB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 1025+15.08 TO 1053+06.00                            | EB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   | 7   |   |  |  |
| 1025+15.08 TO 1053+06.00                            | WB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 1025+15.08 TO 1053+06.00                            | WB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   | 6   |   |  |  |
| 1025+55.78  | EB INSIDE  |                                       | 24   |  |                                 |   |                                 |   |  |                                   |   | 20.0                                      |  | 4  |
| 1025+55.78 TO 1030+05.80                            |            |                                       |  | 499  |                                 |   | 75.6                            |   | 1  |                                   |   |   |  |  |
| 1026+84.51  | WB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 1029+91.83  | WB INSIDE  |                                       | 17   |  |                                 |   |                                 |   |  |                                   |   | 13.0                                      |  | 4  |
| 1030+05.41  | EB INSIDE  |                                       | 23   |  |                                 |   |                                 |   |  |                                   |   | 21.0                                      |  | 4  |
| 1042+82.80  | WB INSIDE  |                                       | 16   |  |                                 |   |                                 |   |  |                                   |   | 13.5                                      |  | 4  |
| 1042+82.80 TO 1045+95.46                            |            |                                       |  | 313  |                                 |   | 47.5                            |   | 1  |                                   |   |   |  |  |
| 1042+89.46  | EB INSIDE  |                                       | 24   |  |                                 |   |                                 |   |  |                                   |   | 20.0                                      |  | 4  |
| 1046+20.03 TO 1049+71.42                            |            |                                       |  | 352  |                                 |   | 53.3                            |   | 1  |                                   |   |   |  |  |
| 1049+60.57  | WB INSIDE  |                                       | 16   |  |                                 |   |                                 |   |  |                                   |   | 14.5                                      |  | 4  |
| 1049+71.31  | EB INSIDE  |                                       | 24   |  |                                 |   |                                 |   |  |                                   |   | 19.0                                      |  | 4  |
| <b>OGDEN INTERCHANGE RAMPS</b>                      |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| <b>RAMP A</b>                                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 384+00.00 TO 403+00.00                              |            |                                       |  |  |                                 |   |                                 |   |  |                                   | 3   |   |  |  |
| <b>RAMP B</b>                                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 300+00.00 TO 321+00.00                              |            |                                       |  |  |                                 |   |                                 |   |  |                                   | 2   |   |  |  |
| <b>RAMP C</b>                                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 480+21.00 TO 503+00.00                              |            |                                       |  |  |                                 |   |                                 |   |  |                                   | 4   |   |  |  |
| 500+00.00   |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  | 14   |
| <b>RAMP D</b>                                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 100+00.00 TO 117+97.00                              |            |                                       |  |  |                                 |   |                                 |   |  |                                   | 2   |   |  |  |
| <b>FULL DEPTH RESURFACING UNDERNEATH STRUCTURES</b> |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| <b>SN 010-0183</b>                                  |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 898+29.00 TO 904+29.00                              | EB INSIDE  | 600                                   | 87   |  |                                 | 2   |                                 |   |  |                                   |   | 600.0                                     |  | 600  |
| 898+29.00 TO 904+29.00                              | WB INSIDE  | 600                                   | 83   |  |                                 | 2   |                                 |   |  |                                   |   | 600.0                                     |  | 600  |
| 898+29.00 TO 904+29.00                              | EB OUTSIDE | 600                                   | 41   |  |                                 | 2   |                                 |   |  |                                   |   | 600.0                                     |  | 600  |
| 898+29.00 TO 904+29.00                              | WB OUTSIDE | 600                                   | 39   |  |                                 | 2   |                                 |   |  |                                   |   | 600.0                                     |  | 600  |
| <b>VERMILION COUNTY</b>                             |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| <b>I-74</b>   |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 1053+06.00 TO 1104+30.18                            | EB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   | 4   |   |  |  |
| 1053+06.00 TO 1104+30.18                            | EB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   | 7   |   |  |  |
| 1053+06.00 TO 1104+30.18                            | WB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   | 2   |   |  |  |
| 1053+06.00 TO 1104+30.18                            | WB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   | 13  |   |  |  |
| 1056+17.86  | WB INSIDE  |                                       | 19   |  |                                 |   |                                 |   |  |                                   |   | 14.0                                      |  | 4  |
| 1056+18.00  | WB         |                                       |  |  | 66                              | 1   |                                 | 1   | 2  | 66                                |   |   |  |  |
| 1056+30.08  | EB INSIDE  |                                       | 23   |  |                                 |   |                                 |   |  |                                   |   | 20.5                                      |  | 4  |
| 1058+96.00  | WB         |                                       |  |  | 62                              | 1   |                                 | 1   | 2  | 62                                |   |   |  |  |
| 1058+96.21  | WB INSIDE  |                                       | 16   |  |                                 |   |                                 |   |  |                                   |   | 14.5                                      |  | 4  |
| 1059+09.69  | EB INSIDE  |                                       | 27   |  |                                 |   |                                 |   |  |                                   |   | 20.5                                      |  | 4  |
| 1071+03.27  | WB INSIDE  |                                       | 16   |  |                                 |   |                                 |   |  |                                   |   | 13.5                                      |  | 4  |
| 1071+03.27 TO 1080+03.61                            |            |                                       |  | 905  |                                 |   | 137.3                           |   | 1  |                                   |   |   |  |  |
| 1071+12.57  | EB INSIDE  |                                       | 26   |  |                                 |   |                                 |   |  |                                   |   | 19.5                                      |  | 4  |
| 1074+05.78  | EB INSIDE  |                                       | 24   |  |                                 |   |                                 |   |  |                                   |   | 18.0                                      |  | 4  |
| 1074+11.73  | WB INSIDE  |                                       | 16   |  |                                 |   |                                 |   |  |                                   |   | 15.5                                      |  | 4  |
| 1076+83.06  | WB INSIDE  |                                       | 12   |  |                                 |   |                                 |   |  |                                   |   | 17.5                                      |  | 4  |
| 1076+93.97  | EB INSIDE  |                                       | 28   |  |                                 |   |                                 |   |  |                                   |   | 17.5                                      |  | 4  |

**SCHEDULE CONTINUES ON NEXT SHEET**

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:56:58 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |       |      |        |
|------------------|-------|------|--------|
| <b>SCHEDULES</b> |       |      |        |
| SCALE: N.T.S.    | SHEET | OF   | SHEETS |
| STA.             | TO    | STA. | TO     |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 35        |
| CONTRACT NO. 70790 |         |        |              |           |

MODEL Default  
FILE Name: ...03270905ch-schedule.dgn

# MEDIAN DRAINAGE & UNDERDRAINS

| LOCATION<br>STATION TO STATION                      | SIDE       | PIPE<br>UNDERDRAINS<br>4"<br>60107600 | PIPE<br>UNDERDRAINS<br>4", (SPECIAL)<br>60108100 | PIPE<br>UNDERDRAINS,<br>TYPE 2, 6"<br>60108206 | * PIPE DRAINS<br>6"<br>60100915 | * CONCRETE<br>HEADWALLS<br>FOR PIPE<br>DRAINS<br>60100060 | COARSE<br>AGGREGATE<br>X0795800 | INLETS, TYPE A,<br>TYPE 8 GRATE<br>60236200 | UNDERDRAIN<br>CONNECTION<br>TO STRUCTURE<br>Z0076870 | DIRECTIONAL<br>BORING<br>X0325379 | CLEANING<br>UNDERDRAIN<br>OUTLETS<br>X0320157 | PIPE<br>UNDERDRAIN<br>REMOVAL<br>Z0040530 | REMOVE<br>CONCRETE<br>HEADWALLS FOR<br>PIPE DRAINS<br>X6015000 | SHOULDER<br>REMOVAL AND<br>REPLACEMENT<br>60100070 |
|---|------------|---------------------------------------|--|--|---------------------------------|---|---------------------------------|---|--|-----------------------------------|---|---|--|--|
|   |            | FOOT                                  | FOOT   | FOOT   | FOOT                            | EACH  | TON                             | EACH  | EACH   | FOOT                              | EACH  | FOOT                                      | EACH   | FOOT   |
| 1100+92.01  | WB INSIDE  |                                       | 25   |  |                                 |   |                                 |   |  |                                   |   | 22.0                                      |  | 4  |
| 1100+98.73  | EB INSIDE  |                                       | 15   |  |                                 |   |                                 |   |  |                                   |   | 13.5                                      |  | 4  |
| 1100+99.00  | WB         |                                       |  |  | 69                              | 1   |                                 | 1   | 2  | 69                                |   |   |  |  |
| 1104+04.08  | WB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  | 4  |
| 1104+19.32  | EB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  | 4  |
| 1109+08.74 TO 1114+10.93                            |            |                                       |  | 513  |                                 |   | 77.7                            |   | 1  |                                   |   | 36.5                                      |  |  |
| 1110+30.18 TO 1180+00.00                            | EB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   | 1   |   |  |  |
| 1110+30.18 TO 1180+00.00                            | EB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   | 11  |   |  |  |
| 1110+30.18 TO 1180+00.00                            | WB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   | 1   |   |  |  |
| 1110+30.18 TO 1180+00.00                            | WB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   | 7   |   |  |  |
| 1114+05.17  | EB INSIDE  |                                       | 24   |  |                                 |   |                                 |   |  |                                   |   | 14.0                                      |  | 4  |
| 1114+10.98  | WB INSIDE  |                                       | 17   |  |                                 |   |                                 |   |  |                                   |   | 20.0                                      |  | 4  |
| 1123+98.09  | EB INSIDE  |                                       | 17   |  |                                 |   |                                 |   |  |                                   |   | 13.0                                      |  | 4  |
| 1123+98.09 TO 1129+11.02                            |            |                                       |  | 513  |                                 |   | 77.9                            |   | 1  |                                   |   | 21.5                                      |  |  |
| 1129+00.52  | WB INSIDE  |                                       | 23   |  |                                 |   |                                 |   |  |                                   |   |   |  | 4  |
| 1129+11.58  | EB INSIDE  |                                       | 17   |  |                                 |   |                                 |   |  |                                   |   | 14.0                                      |  | 4  |
| 1129+12.00  | EB         |                                       |  |  | 65                              | 1   |                                 | 1   | 1  | 65                                |   |   |  |  |
| 1142+79.00  | WB         |                                       |  |  | 78                              | 1   |                                 | 1   | 1  | 78                                |   |   |  |  |
| 1142+79.34  | WB INSIDE  |                                       | 22   |  |                                 |   |                                 |   |  |                                   |   | 20.5                                      |  | 4  |
| 1150+93.42  | WB INSIDE  |                                       | 23   |  |                                 |   |                                 |   |  |                                   |   | 22.5                                      |  | 4  |
| 1150+93.42 TO 1153+93.69                            |            |                                       |  | 301  |                                 |   | 45.6                            |   | 1  |                                   |   |   |  |  |
| 1151+04.90  | EB INSIDE  |                                       | 17   |  |                                 |   |                                 |   |  |                                   |   | 13.0                                      |  | 4  |
| 1156+09.47  | WB INSIDE  |                                       | 22   |  |                                 |   |                                 |   |  |                                   |   | 19.5                                      |  | 4  |
| 1156+09.41 TO 1159+54.39                            |            |                                       |  | 345  |                                 |   | 52.4                            |   | 1  |                                   |   |   |  |  |
| 1156+18.88  | EB INSIDE  |                                       | 19   |  |                                 |   |                                 |   |  |                                   |   | 14.5                                      |  | 4  |
| 1166+01.53  | WB INSIDE  |                                       | 16   |  |                                 |   |                                 |   |  |                                   |   | 13.5                                      |  | 4  |
| 1166+01.53 TO 1167+55.35                            |            |                                       |  | 154  |                                 |   | 23.4                            |   | 1  |                                   |   |   |  |  |
| 1166+09.63  | EB INSIDE  |                                       | 24   |  |                                 |   |                                 |   |  |                                   |   | 20.5                                      |  | 4  |
| <b>IL 49 N INTERCH. RAMPS</b>                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| <b>RAMP A</b>                                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 43+44.00 TO 60+00.00                                |            |                                       |  |  |                                 |   |                                 |   |  |                                   | 2   |   |  |  |
| <b>RAMP B</b>                                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 63+00.00 TO 83+00.00                                |            |                                       |  |  |                                 |   |                                 |   |  |                                   | 6   |   |  |  |
| <b>RAMP C</b>                                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 40+00.00 TO 60+00.00                                |            |                                       |  |  |                                 |   |                                 |   |  |                                   | 2   |   |  |  |
| <b>RAMP D</b>                                       |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 60+00.00 TO 73+00.00                                |            |                                       |  |  |                                 |   |                                 |   |  |                                   | 2   |   |  |  |
| <b>FULL DEPTH RESURFACING UNDERNEATH STRUCTURES</b> |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| <b>SN 092-0146</b>                                  |            |                                       |  |  |                                 |   |                                 |   |  |                                   |   |   |  |  |
| 1104+04.08  | WB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   |   | 23.0                                      |  |  |
| 1104+04.07 TO 1110+30.18                            | WB INSIDE  | 600                                   | 84   |  |                                 | 2   |                                 |   |  |                                   |   | 609.0                                     |  | 609  |
| 1104+19.22  | EB INSIDE  |                                       |  |  |                                 |   |                                 |   |  |                                   |   | 12.0                                      |  |  |
| 1104+19.33 TO 1110+30.18                            | EB INSIDE  | 600                                   | 88   |  |                                 | 2   |                                 |   |  |                                   |   | 594.0                                     |  | 594  |
| 1104+30.18 TO 1110+30.18                            | EB OUTSIDE | 600                                   | 43   |  |                                 | 2   |                                 |   |  |                                   |   | 600.0                                     |  | 600  |
| 1104+30.18 TO 1110+30.18                            | WB OUTSIDE | 600                                   | 38   |  |                                 | 2   |                                 |   |  |                                   |   | 600.0                                     |  | 600  |
| 1108+80.02  | WB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   |   | 19.0                                      | 1  |  |
| 1108+98.13  | EB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   |   | 21.0                                      | 1  |  |
| 1108+99.78  | EB OUTSIDE |                                       |  |  |                                 |   |                                 |   |  |                                   |   | 21.0                                      | 1  |  |
| <b>PROJECT TOTAL</b>                                |            | <b>4,800</b>                          | <b>1,875</b>                                     | <b>8,975</b>                                   | <b>842</b>                      | <b>28</b>   | <b>1,363</b>                    | <b>12</b>                                   | <b>39</b>  | <b>842</b>                        | <b>145</b>                                    | <b>6,112</b>                              | <b>3</b>   | <b>5,097</b>                                       |

\* SEE OTHER SCHEDULES FOR ADDITIONAL QUANTITIES

MODEL: Default  
FILE: Model - 10/27/2019 3:57:05 PM - schedule.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemyer                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 3:57:05 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |                              |
|------------------|------------------------------|
| <b>SCHEDULES</b> |                              |
| SCALE: N.T.S.    | SHEET OF SHEETS STA. TO STA. |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 36        |
| CONTRACT NO. 70790 |         |        |              |           |



# TEMPORARY PAVEMENT MARKING

| LOCATION<br>STATION TO STATION | DESCRIPTION      | TEMPORARY PAVEMENT MARKING |        |          |          |          |          | SHORT TERM PAVEMENT MARKING |       | SHORT TERM PAVEMENT MARKING REMOVAL |        | PAVEMENT MARKING REMOVAL - WATER BLASTING |        |          |        |
|--------------------------------|------------------|----------------------------|--------|----------|----------|----------|----------|-----------------------------|-------|-------------------------------------|--------|---|--------|----------|--------|
|                                |                  | 7030220                    |        | 70300240 | 70300250 | 70300260 | 70300280 | 70300210                    |       | 70300100                            |        | 70300150                                  |        | X0327980 |        |
|                                |                  | LINE 4"                    |        | LINE 6"  | LINE 8"  | LINE 12" | LINE 24" | LETTERS & SYMBOLS           |       | LINE 4"                             |        | LINE 4"                                   |        | WHITE    | YELLOW |
|                                |                  | WHITE                      | YELLOW | WHITE    | WHITE    | WHITE    | WHITE    | WHITE                       | WHITE | WHITE                               | YELLOW | WHITE                                     | YELLOW | SQ FT    | SQ FT  |
| FOOT                           |                  | FOOT                       | FOOT   | FOOT     | FOOT     | FOOT     | FOOT     | SQ FT                       | FOOT  | FOOT                                | SQ FT  | SQ FT                                     | SQ FT  | SQ FT    |        |
| <b>CHAMPAIGN COUNTY</b>        |                  |                            |        |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| <b>I-74 EB</b>                 |                  |                            |        |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 880+00.00 TO 1053+06.00        |                  |                            | 17,307 |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 880+00.00 TO 1053+06.00        | CENTERLINE       | 4,327                      |        |          |          |          |          |                             | 1,731 | 693                                 | 231.0  |   |        | 5,769.0  |        |
| 880+00.00 TO 985+18.95         |                  | 10,520                     |        |          |          |          |          |                             | 421   |                                     | 140.3  |   |        | 3,506.7  |        |
| 988+01.28 TO 989+78.96         |                  |                            |        |          | 178      |          |          |                             |       |                                     |        |   |        | 118.7    |        |
| 989+78.96 TO 1011+65.08        |                  | 2,187                      |        |          |          |          |          |                             | 88    |                                     | 29.3   |   |        | 729.0    |        |
| 1011+65.08 TO 1013+78.69       |                  |                            |        |          | 214      |          |          |                             |       |                                     |        |   |        | 142.7    |        |
| 1021+02.61 TO 1053+06.00       |                  | 3,204                      |        |          |          |          |          |                             | 129   |                                     | 43.0   |   |        | 1,068.0  |        |
| <b>I-74 WB</b>                 |                  |                            |        |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 880+00.00 TO 1053+06.00        |                  |                            | 17,306 |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 880+00.00 TO 1053+06.00        | CENTERLINE       | 4,327                      |        |          |          |          |          |                             | 1,731 | 693                                 | 231.0  |   |        | 5,768.7  |        |
| 880+00.00 TO 980+53.49         |                  | 10,053                     |        |          |          |          |          |                             | 403   |                                     | 134.3  |   |        | 3,351.0  |        |
| 987+69.83 TO 990+11.89         |                  |                            |        |          | 243      |          |          |                             |       |                                     |        |   |        | 162.0    |        |
| 990+11.89 TO 1011+89.36        |                  | 2,178                      |        |          |          |          |          |                             | 88    |                                     | 29.3   |   |        | 726.0    |        |
| 1011+89.36 TO 1013+45.34       |                  |                            |        |          | 156      |          |          |                             |       |                                     |        |   |        | 104.0    |        |
| 1016+60.24 TO 1053+06.00       |                  | 3,645                      |        |          |          |          |          |                             | 129   |                                     | 43.0   |   |        | 1,068.0  |        |
| <b>CH 22 RAMP A</b>            |                  |                            |        |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 385+15.52 TO 403+25.66         |                  | 1,857                      |        |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 387+96.70 TO 389+75.08         |                  |                            |        |          | 184      |          |          |                             | 75    |                                     | 25.0   |   |        | 619.0    |        |
| 389+75.08 TO 402+90.89         |                  | 1,359                      |        |          |          |          |          |                             | 55    |                                     | 18.3   |   |        | 453.0    |        |
| 401+28.41                      | THROUGH ARROW    |                            |        |          |          |          |          |                             | 11.5  |                                     |        |   |        | 11.5     |        |
| 402+66.36                      | LEFT TURN ARROW  |                            |        |          |          |          |          |                             | 15.6  |                                     |        |   |        | 15.6     |        |
| 402+70.87 TO 403+00.62         | ISLAND           |                            |        | 49       |          |          |          |                             |       |                                     |        |   |        | 24.5     |        |
| 402+71.76                      | RIGHT TURN ARROW |                            |        |          |          |          |          |                             | 15.6  |                                     |        |   |        | 15.6     |        |
| 402+70.87 TO 403+21.58         | ISLAND           |                            |        | 65       |          |          |          |                             |       |                                     |        |   |        | 32.5     |        |
| 402+73.00                      | STOP BAR         |                            |        |          |          |          |          | 18                          |       |                                     |        |   |        | 36.0     |        |
| 402+93.00                      | STOP BAR         |                            |        |          |          |          |          | 18                          |       |                                     |        |   |        | 36.0     |        |
| 403+00.62 TO 403+21.58         | ISLAND           |                            |        | 80       |          | 160      |          |                             |       |                                     |        |   |        | 200.0    |        |
| <b>CH 22 RAMP B</b>            |                  |                            |        |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 298+29.90 TO 320+96.40         |                  | 2,287                      |        |          |          |          |          |                             | 92    |                                     | 30.7   |   |        | 762.3    |        |
| 298+59.02 TO 311+59.55         |                  | 1,321                      |        |          |          |          |          |                             | 53    |                                     | 17.7   |   |        | 440.3    |        |
| 311+59.55 TO 313+72.34         |                  |                            |        |          | 222      |          |          |                             |       |                                     |        |   |        | 148.0    |        |
| <b>CH 22 RAMP C</b>            |                  |                            |        |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 480+50.04 TO 503+30.61         |                  | 2,298                      |        |          |          |          |          |                             | 92    |                                     | 30.7   |   |        | 766.0    |        |
| 487+66.87 TO 490+08.69         |                  |                            |        |          | 250      |          |          |                             |       |                                     |        |   |        | 166.7    |        |
| 490+08.69 TO 503+04.38         |                  | 1,314                      |        |          |          |          |          |                             | 53    |                                     | 17.7   |   |        | 438.0    |        |
| <b>CH 22 RAMP D</b>            |                  |                            |        |          |          |          |          |                             |       |                                     |        |   |        |          |        |
| 98+39.60 TO 116+56.49          |                  | 1,873                      |        |          |          |          |          |                             | 75    |                                     | 25.0   |   |        | 624.3    |        |
| 98+41.03 TO 98+93.68           | ISLAND           |                            |        | 72       |          |          |          |                             |       |                                     |        |   |        | 36.0     |        |
| 98+41.03 TO 98+63.23           | ISLAND           |                            |        | 86       |          | 177      |          |                             |       |                                     |        |   |        | 220.0    |        |
| 98+63.23 TO 98+93.68           | ISLAND           |                            |        | 49       |          |          |          |                             |       |                                     |        |   |        | 24.5     |        |
| 98+69.29 TO 111+86.12          |                  | 1,345                      |        |          |          |          |          |                             | 54    |                                     | 18.0   |   |        | 448.3    |        |
| 98+71.83                       | STOP BAR         |                            |        |          |          |          |          | 18                          |       |                                     |        |   |        | 36.0     |        |
| 98+93.18                       | RIGHT TURN ARROW |                            |        |          |          |          |          |                             | 11.5  |                                     |        |   |        | 15.6     |        |
| 98+97.62                       | STOP BAR         |                            |        |          |          |          |          | 18                          |       |                                     |        |   |        | 36.0     |        |
| 98+98.18                       | LEFT TURN ARROW  |                            |        |          |          |          |          |                             | 15.6  |                                     |        |   |        | 15.6     |        |
| 100+10.47                      | THROUGH ARROW    |                            |        |          |          |          |          |                             | 15.6  |                                     |        |   |        | 11.5     |        |
| 111+86.12 TO 113+42.92         |                  |                            |        |          | 163      |          |          |                             |       |                                     |        |   |        | 108.7    |        |

SCHEDULE CONTINUES ON NEXT SHEET

MODEL: Default  
FILE: \\bmr\p\2019\0790\ch-schedule.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:57:11 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |       |      |        |
|------------------|-------|------|--------|
| <b>SCHEDULES</b> |       |      |        |
| SCALE: N.T.S.    | SHEET | OF   | SHEETS |
| STA.             | TO    | STA. |        |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 37        |
| CONTRACT NO. 70790 |         |        |              |           |

# TEMPORARY PAVEMENT MARKING

| LOCATION<br>STATION TO STATION | DESCRIPTION      | TEMPORARY PAVEMENT MARKING |            |              |            |            |            | SHORT TERM PAVEMENT MARKING |              | SHORT TERM PAVEMENT MARKING REMOVAL |          | PAVEMENT MARKING REMOVAL - WATER BLASTING |          |        |
|--------------------------------|------------------|----------------------------|------------|--------------|------------|------------|------------|-----------------------------|--------------|-------------------------------------|----------|---|----------|--------|
|                                |                  | 7030220                    |            | 70300240     | 70300250   | 70300260   | 70300280   | 70300210                    | 70300100     |                                     | 70300150 |   | X0327980 |        |
|                                |                  | LINE 4"                    |            | LINE 6"      | LINE 8"    | LINE 12"   | LINE 24"   | LETTERS & SYMBOLS           | LINE 4"      |                                     | LINE 4"  |   |          |        |
|                                |                  | WHITE                      | YELLOW     | WHITE        | WHITE      | WHITE      | WHITE      | WHITE                       | WHITE        | YELLOW                              | WHITE    | YELLOW                                    | WHITE    | YELLOW |
| FOOT                           |                  | FOOT                       | FOOT       | FOOT         | FOOT       | FOOT       | SQ FT      | FOOT                        | FOOT         | SQ FT                               | SQ FT    | SQ FT                                     | SQ FT    |        |
| <b>VERMILION COUNTY</b>        |                  |                            |            |              |            |            |            |                             |              |                                     |          |   |          |        |
| <b>I-74 EB</b>                 |                  |                            |            |              |            |            |            |                             |              |                                     |          |   |          |        |
| 1053+06.00 TO 1180+00.00       |                  |                            | 12,695     |              |            |            |            |                             | 508          |                                     | 169.3    |   | 4,231.7  |        |
| 1053+06.00 TO 1180+00.00       | CENTERLINE       | 3,176                      |            |              |            |            |            | 1,271                       |              | 423.7                               |          |   | 1,058.7  |        |
| 1053+06.00 TO 1144+62.05       |                  | 9,157                      |            |              |            |            |            | 367                         |              | 122.3                               |          |   | 3,052.3  |        |
| 1148+13.32 TO 1149+72.44       |                  |                            |            | 160          |            |            |            | 83                          |              | 27.7                                |          |   | 106.7    |        |
| 1149+72.44 TO 1170+35.89       |                  | 2,064                      |            |              |            |            |            |                             |              |                                     |          |   | 688.0    |        |
| 1170+35.89 TO 1172+55.10       |                  |                            |            | 220          |            |            |            |                             |              |                                     |          |   | 146.7    |        |
| <b>I-74 WB</b>                 |                  |                            |            |              |            |            |            |                             |              |                                     |          |   |          |        |
| 1053+06.00 TO 1180+00.00       |                  |                            | 12,695     |              |            |            |            |                             | 508          |                                     | 169.3    |   | 4,231.7  |        |
| 1053+06.00 TO 1180+00.00       | CENTERLINE       | 3,176                      |            |              |            |            |            | 1,271                       |              | 423.7                               |          |   | 1,058.7  |        |
| 1053+06.00 TO 1140+51.98       |                  | 8,749                      |            |              |            |            |            | 350                         |              | 116.7                               |          |   | 2,916.3  |        |
| 1147+51.19 TO 1149+92.28       |                  |                            |            | 242          |            |            |            |                             |              |                                     |          |   | 161.3    |        |
| 1149+92.28 TO 1170+70.63       |                  | 2,079                      |            |              |            |            |            | 84                          |              | 28.0                                |          |   | 693.0    |        |
| 1170+70.63 TO 1172+31.03       |                  |                            |            | 161          |            |            |            |                             |              |                                     |          |   | 107.3    |        |
| 1175+34.24 TO 1180+00.00       |                  | 473                        |            |              |            |            |            | 19                          |              | 6.3                                 |          |   | 157.7    |        |
| <b>IL 49 RAMP A</b>            |                  |                            |            |              |            |            |            |                             |              |                                     |          |   |          |        |
| 44+55.13 TO 62+08.99           |                  | 1,789                      |            |              |            |            |            | 72                          |              | 24.0                                |          |   | 596.3    |        |
| 48+05.10 TO 49+65.19           |                  |                            |            | 166          |            |            |            |                             |              |                                     |          |   | 110.7    |        |
| 49+65.19 TO 61+74.90           |                  | 1,251                      |            |              |            |            |            | 51                          |              | 17.0                                |          |   | 417.0    |        |
| 60+22.48                       | THROUGH ARROW    |                            |            |              |            |            | 11.5       |                             |              |                                     |          |   | 11.5     |        |
| 61+53.33                       | LEFT TURN ARROW  |                            |            |              |            |            | 15.6       |                             |              |                                     |          |   | 15.6     |        |
| 61+54.52 TO 61+84.25           | ISLAND           |                            | 48         |              |            |            |            |                             |              |                                     |          |   | 24.0     |        |
| 61+54.52 TO 62+02.63           | ISLAND           |                            | 60         |              | 135        |            |            |                             |              |                                     |          |   | 165.0    |        |
| 61+56.49                       | STOP BAR         |                            |            |              |            | 18         |            |                             |              |                                     |          |   | 36.0     |        |
| 61+59.29                       | RIGHT TURN ARROW |                            |            |              |            |            | 15.6       |                             |              |                                     |          |   | 15.6     |        |
| 61+76.06                       | STOP BAR         |                            |            |              |            | 18         |            |                             |              |                                     |          |   | 36.0     |        |
| 62+02.63 TO 61+54.52           | ISLAND           |                            | 72         |              |            |            |            |                             |              |                                     |          |   | 36.0     |        |
| <b>IL 49 RAMP B</b>            |                  |                            |            |              |            |            |            |                             |              |                                     |          |   |          |        |
| 62+09.00 TO 83+80.97           |                  | 2,202                      |            |              |            |            |            | 89                          |              | 29.7                                |          |   | 734.0    |        |
| 62+37.40 TO 74+11.71           |                  | 1,200                      |            |              |            |            |            | 48                          |              | 16.0                                |          |   | 400.0    |        |
| 74+11.71 TO 76+29.99           |                  |                            |            | 226          |            |            |            |                             |              |                                     |          |   | 150.7    |        |
| <b>IL 49 RAMP C</b>            |                  |                            |            |              |            |            |            |                             |              |                                     |          |   |          |        |
| 40+44.56 TO 61+93.96           |                  | 2,180                      |            |              |            |            |            | 88                          |              | 29.3                                |          |   | 726.7    |        |
| 47+44.04 TO 49+84.26           |                  |                            |            | 247          |            |            |            |                             |              |                                     |          |   | 164.7    |        |
| 49+84.26 TO 61+65.29           |                  | 1,203                      |            |              |            |            |            | 49                          |              | 16.3                                |          |   | 401.0    |        |
| <b>IL 49 RAMP D</b>            |                  |                            |            |              |            |            |            |                             |              |                                     |          |   |          |        |
| 0+00.00 TO 75+27.49            |                  | 1,758                      |            |              |            |            |            | 71                          |              | 23.7                                |          |   | 586.0    |        |
| 0+06.67 TO 0+24.77             | ISLAND           |                            | 68         |              | 120        |            |            |                             |              |                                     |          |   | 154.0    |        |
| 0+06.67 TO 0+51.07             | ISLAND           |                            | 57         |              |            |            |            |                             |              |                                     |          |   | 28.5     |        |
| 0+17.28                        | RIGHT TURN ARROW |                            |            |              |            |            | 15.6       |                             |              |                                     |          |   | 15.6     |        |
| 0+24.77 TO 0+51.07             | ISLAND           |                            | 42         |              |            |            |            |                             |              |                                     |          |   | 21.0     |        |
| 0+32.19                        | STOP BAR         |                            |            |              |            | 18         |            |                             |              |                                     |          |   | 36.0     |        |
| 0+34.30 TO 70+64.48            |                  | 1,254                      |            |              |            |            |            | 51                          |              | 17.0                                |          |   | 418.0    |        |
| 0+49.70                        | LEFT TURN ARROW  |                            |            |              |            |            | 15.6       |                             |              |                                     |          |   | 15.6     |        |
| 0+51.18                        | STOP BAR         |                            |            |              |            | 18         |            |                             |              |                                     |          |   | 36.0     |        |
| 60+12.37                       | THROUGH ARROW    |                            |            |              |            |            | 11.5       |                             |              |                                     |          |   | 11.5     |        |
| 70+64.48 TO 72+25.51           |                  |                            |            | 166          |            |            |            |                             |              |                                     |          |   | 110.7    |        |
| <b>PROJECT TOTAL</b>           |                  | <b>155,809</b>             | <b>748</b> | <b>3,198</b> | <b>592</b> | <b>144</b> | <b>172</b> | <b>11,635</b>               | <b>3,879</b> | <b>55,347</b>                       |          |   |          |        |

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|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemyer                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:57:16 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |       |      |        |
|------------------|-------|------|--------|
| <b>SCHEDULES</b> |       |      |        |
| SCALE: N.T.S.    | SHEET | OF   | SHEETS |
| STA.             | TO    | STA. |        |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 38        |
| CONTRACT NO. 70790 |         |        |              |           |

### HIGH TENSION CABLE MEDIAN BARRIER

| LOCATION STATION TO STATION | SIDE | HIGH TENSION CABLE MEDIAN BARRIER<br>64401100<br>FOOT | HIGH TENSION CABLE MEDIAN BARRIER TERMINALS<br>64401300<br>EACH | WEED CONTROL MOWING STRIP (SPECIAL)<br>X0325677<br>SQ YD | REMOVE HIGH TENSION CABLE MEDIAN BARRIER<br>X0326677<br>FOOT |
|-----------------------------|------|---|---|--|--|
| <b>CHAMPAIGN COUNTY</b>     |      |   |   |  |  |
| <b>I-74</b>                 |      |   |   |  |  |
| 978+12.00 TO 978+62.00      | LT   |   | 1   |  |  |
| 978+12.00 TO 1000+38.00     | LT   |   |   | 989  | 2,226  |
| 978+62.00 TO 999+88.00      | LT   | 2,126   |   |  |  |
| 999+88.00 TO 1000+38.00     | LT   |   | 1   |  |  |
| 1001+70.00 TO 1002+20.00    | RT   |   | 1   |  |  |
| 1001+70.00 TO 1025+15.08    | RT   |   |   | 1,042  | 2,345  |
| 1002+20.00 TO 1039+85.00    | RT   | 3,765   |   |  |  |
| <b>PROJECT TOTAL</b>        |      | <b>5,891</b>  | <b>3</b>  | <b>2,032</b>   | <b>4,571</b>   |

### SURVEY MARKERS

| LOCATION STATION TO STATION | SIDE | OFFSET | DESC. | SURVEY MARKER, TYPE 1 (SPECIAL)<br>XZ193300<br>EACH |
|-----------------------------|------|--------|-------|---|
| <b>CHAMPAIGN COUNTY</b>     |      |        |       |   |
| <b>I-74</b>                 |      |        |       |   |
| 956+44.82                   | CL   |        | PC    | 1   |
| 962+69.00                   | RT   | 3.78   | PI    | 1   |
| 968+93.12                   | CL   |        | PT    | 1   |
| 1051+80.04                  | CL   |        | PC    | 1   |
| <b>VERMILION COUNTY</b>     |      |        |       |   |
| <b>I-74</b>                 |      |        |       |   |
| 1066+32.73                  | RT   | 86.15  | PI    | 1   |
| 1080+71.81                  | CL   |        | PT    | 1   |
| 1102+70.78                  | CL   |        | PC    | 1   |
| 1117+50.83                  | LT   | 86.98  | PI    | 1   |
| 1132+17.27                  | CL   |        | PT    | 1   |
| <b>PROJECT TOTAL</b>        |      |        |       | <b>9</b>  |

### GUARDRAIL REMOVAL AND REERECT

| LOCATION STATION TO STATION | SIDE   | REMOVE AND REERECT STEEL PLATE BEAM GUARDRAIL, TYPE A<br>63301210<br>FOOT | REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 1<br>63301990<br>EACH | REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 6B<br>63302720<br>EACH |
|-----------------------------|--------|---|--|---|
| <b>CHAMPAIGN COUNTY</b>     |        |   |  |   |
| <b>I-74 SN 010-0183</b>     |        |   |  |   |
| 897+87.09 TO 898+37.09      | EB OUT |   | 1  |   |
| 898+37.09 TO 900+74.59      | EB OUT | 237.5   |  |   |
| 900+74.59                   | EB OUT |   |  | 1   |
| 901+83.41                   | WB OUT |   |  | 1   |
| 901+83.41 TO 904+20.91      | WB OUT | 237.5   |  |   |
| 904+20.91 TO 904+70.91      | WB OUT |   | 1  |   |
| <b>I-74 SN 092-0146</b>     |        |   |  |   |
| 1103+76.36 TO 1104+26.36    | EB OUT |   | 1  |   |
| 1104+26.36 TO 1106+63.86    | EB OUT | 237.5   |  |   |
| 1106+63.86                  | EB OUT |   |  | 1   |
| 1107+96.50                  | WB OUT |   |  | 1   |
| 1107+96.50 TO 1110+34.00    | WB OUT | 237.5   |  |   |
| 1110+34.00 TO 1110+84.00    | WB OUT |   | 1  |   |
| <b>PROJECT TOTAL</b>        |        | <b>950.0</b>  | <b>4</b>   | <b>4</b>  |

### CHANGEABLE MESSAGE SIGNS

| LOCATION                       | COMMENT                 | # OF CMS BOARDS | DAYS UTILIZED | CALENDAR DAYS |
|--------------------------------|-------------------------|-----------------|---------------|---------------|
| <b>CHAMPAIGN COUNTY</b>        |                         |                 |               |               |
| <b>I-74 EB</b>                 |                         |                 |               |               |
| BEGIN PROJECT                  | PUBLIC INFORMATION PLAN | 1               | 7             | 7             |
| WEST OF ST. JOSEPH INTERCHANGE | PUBLIC INFORMATION PLAN | 1               | 7             | 7             |
| <b>CH 22 NB</b>                |                         |                 |               |               |
| PRIOR TO I-74 EB ENTRANCE RAMP | RAMP C CLOSURE          | 1               | 8             | 8             |
| PRIOR TO RAMP CLOSED           | RAMP C CLOSURE          | 1               | 8             | 8             |
| <b>CH 22 SB</b>                |                         |                 |               |               |
| PRIOR TO RAMP CLOSED           | RAMP C CLOSURE          | 1               | 8             | 8             |
| PRIOR TO I-74 EB ENTRANCE RAMP | RAMP C CLOSURE          | 1               | 8             | 8             |
| <b>VERMILION COUNTY</b>        |                         |                 |               |               |
| <b>I-74 WB</b>                 |                         |                 |               |               |
| BEGIN PROJECT                  | PUBLIC INFORMATION PLAN | 1               | 7             | 7             |
| EAST OF OAKWOOD INTERCHANGE    | PUBLIC INFORMATION PLAN | 1               | 7             | 7             |
| <b>PROJECT TOTAL</b>           |                         |                 |               | <b>60</b>     |

### RAISED REFLECTIVE PAVEMENT MARKERS

| LOCATION STATION TO STATION | SIDE | RAISED REFLECTIVE PAVEMENT MARKER<br>78100100 |                         | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL<br>78300200<br>EACH |
|-----------------------------|------|---|-------------------------|---|
|                             |      | ONE-WAY AMBER<br>EACH                         | ONE-WAY CRYSTAL<br>EACH |   |
| <b>CHAMPAIGN COUNTY</b>     |      |   |                         |   |
| <b>I-74 EB</b>              |      |   |                         |   |
| 880+00.69 TO 1053+06.00     | CL   |   | 434                     | 434   |
| 994+09.67 TO 989+78.96      | OS   |   | 5                       | 5   |
| <b>I-74 WB</b>              |      |   |                         |   |
| 880+00.69 TO 1053+06.00     | CL   |   | 434                     | 434   |
| 1011+89.36 TO 1013+45.34    | OS   |   | 4                       | 4   |
| <b>CH 22 RAMP A</b>         |      |   |                         |   |
| 385+15.52 TO 389+75.08      | RT   |   | 23                      | 23  |
| 387+97.15 TO 389+75.08      | LT   |   | 5                       | 5   |
| 389+75.08 TO 395+92.06      | LT   | 16  |                         | 16  |
| <b>CH 22 RAMP D</b>         |      |   |                         |   |
| 111+86.12 TO 116+56.48      | LT   |   | 24                      | 24  |
| 105+74.09 TO 111+86.12      | RT   | 16  |                         | 16  |
| 111+86.12 TO 113+42.78      | RT   |   | 8                       | 8   |
| <b>VERMILION COUNTY</b>     |      |   |                         |   |
| <b>I-74 EB</b>              |      |   |                         |   |
| 1053+06.00 TO 1180+00.00    | CL   |   | 318                     | 318   |
| 1148+13.32 TO 1149+72.44    | OS   |   | 4                       | 4   |
| <b>I-74 WB</b>              |      |   |                         |   |
| 1053+06.00 TO 1180+00.00    | CL   |   | 318                     | 318   |
| 1170+70.63 TO 1172+31.03    | OS   |   | 4                       | 4   |
| <b>IL 49 RAMP A</b>         |      |   |                         |   |
| 44+55.19 TO 49+65.19        | RT   |   | 26                      | 26  |
| 48+06.33 TO 49+65.19        | LT   |   | 8                       | 8   |
| 49+65.19 TO 54+72.81        | LT   | 13  |                         | 13  |
| <b>IL 49 RAMP D</b>         |      |   |                         |   |
| 65+48.61 TO 70+64.48        | RT   | 13  |                         | 13  |
| 70+64.48 TO 72+25.78        | RT   |   | 9                       | 9   |
| 70+64.48 TO 75+27.49        | LT   |   | 24                      | 24  |
| <b>PROJECT TOTAL</b>        |      |   | <b>1,706</b>            | <b>1,706</b>  |

NOTES: LT/RT SIDES ARE GIVEN WITH RESPECT TO THE DIRECTION OF TRAVEL

### COMBINATION CURB AND GUTTER

| LOCATION STATION TO STATION | SIDE | COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24<br>60605000<br>FOOT | COMBINATION CURB AND GUTTER REMOVAL<br>44000500<br>FOOT |
|-----------------------------|------|---|---|
| <b>CHAMPAIGN COUNTY</b>     |      |   |   |
| <b>CH 22 RAMP C</b>         |      |   |   |
| 502+23.25 TO 502+26.75      | LT   | 3.5   | 3.5   |
| 502+23.25 TO 502+26.75      | RT   | 3.5   | 3.5   |
| <b>PROJECT TOTAL</b>        |      | <b>7</b>  | <b>7</b>  |

MODEL: Default  
FILE: Model - 10/27/2019 9:50:58 AM - sch-schedule.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/29/2019 - 2:20:10 PM | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

|                  |       |      |        |
|------------------|-------|------|--------|
| <b>SCHEDULES</b> |       |      |        |
| SCALE: N.T.S.    | SHEET | OF   | SHEETS |
| STA.             | TO    | STA. |        |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 39        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

# PAVEMENT MARKING

| LOCATION STATION TO STATION | DESCRIPTION      | THERMOPLASTIC PAVEMENT MARKING |             |            |            |            |            | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID | GROOVING FOR RECESSED PAVEMENT MARKING | GROOVING FOR RECESSED PAVEMENT MARKING | GROOVING FOR RECESSED PAVEMENT MARKING | GROOVING FOR RECESSED PAVEMENT MARKING | GROOVING FOR RECESSED PAVEMENT MARKING | GROOVING FOR RECESSED PAVEMENT MARKING |                     |
|-----------------------------|------------------|--------------------------------|-------------|------------|------------|------------|------------|---|--|--|--|--|--|--|---------------------|
|                             |                  | 78000200                       | 78000400    | 78000500   | 78000600   | 78000650   | 78000100   | 78004210  | X7830070                               | X7830074                               | X7830076                               | X7830078                               | X7830090                               | X7830060                               |                     |
|                             |                  | LINE 4"                        |             | LINE 6"    | LINE 8"    | LINE 12"   | LINE 24"   | LETTERS AND SYMBOLS                                 | LINE 4"                                | 5"                                     | 7"                                     | 9"                                     | 13"                                    | 25"                                    | LETTERS AND SYMBOLS |
|                             |                  | WHITE FOOT                     | YELLOW FOOT | WHITE FOOT | WHITE FOOT | WHITE FOOT | WHITE FOOT | WHITE SQ FT   | WHITE FOOT                             | WHITE FOOT                             | YELLOW FOOT                            | WHITE FOOT                             | WHITE FOOT                             | WHITE FOOT                             | WHITE SQ FT         |
| <b>CHAMPAIGN COUNTY</b>     |                  |                                |             |            |            |            |            |   |  |  |  |  |  |  |                     |
| <b>I-74 EB</b>              |                  |                                |             |            |            |            |            |   |  |  |  |  |  |  |                     |
| 880+00.00 TO 1053+06.00     |                  |                                |             |            |            |            |            |   |  |  |  |  |  |  |                     |
| 880+00.00 TO 1053+06.00     | CENTERLINE       |                                | 17,307      |            |            |            |            | 4,327   |  | 17,307                                 |  |  |  |  |                     |
| 880+00.00 TO 985+18.95      |                  | 10,520                         |             |            |            |            |            |   | 10,520                                 |  |  |  |  |  |                     |
| 988+01.28 TO 989+78.96      |                  |                                |             |            | 178        |            |            |   |  |  | 178                                    |  |  |  |                     |
| 989+78.96 TO 1011+65.08     |                  | 2,187                          |             |            |            |            |            | 2,187   |  |  |  |  |  |  |                     |
| 1011+65.08 TO 1013+78.69    |                  |                                |             |            | 214        |            |            |   |  |  | 214                                    |  |  |  |                     |
| 1021+02.61 TO 1053+06.00    |                  | 3,204                          |             |            |            |            |            | 3,204   |  |  |  |  |  |  |                     |
| <b>I-74 WB</b>              |                  |                                |             |            |            |            |            |   |  |  |  |  |  |  |                     |
| 880+00.00 TO 1053+06.00     |                  |                                | 17,306      |            |            |            |            |   |  | 17,306                                 |  |  |  |  |                     |
| 880+00.00 TO 1053+06.00     | CENTERLINE       |                                |             |            |            |            |            | 4,327   |  |  |  |  |  |  |                     |
| 880+00.00 TO 980+53.49      |                  | 10,053                         |             |            |            |            |            |   | 10,053                                 |  |  |  |  |  |                     |
| 987+69.83 TO 990+11.89      |                  |                                |             |            | 243        |            |            |   |  |  | 243                                    |  |  |  |                     |
| 990+11.89 TO 1011+89.36     |                  | 2,178                          |             |            |            |            |            | 2,178   |  |  |  |  |  |  |                     |
| 1011+89.36 TO 1013+45.34    |                  |                                |             |            | 156        |            |            |   |  |  | 156                                    |  |  |  |                     |
| 1021+02.61 TO 1053+06.00    |                  | 3,645                          |             |            |            |            |            | 3,645   |  |  |  |  |  |  |                     |
| <b>CH 22 RAMP A</b>         |                  |                                |             |            |            |            |            |   |  |  |  |  |  |  |                     |
| 385+15.52 TO 403+25.66      |                  | 1,857                          |             |            |            |            |            |   | 1,857                                  |  |  |  |  |  |                     |
| 387+96.70 TO 389+75.08      |                  |                                |             |            | 184        |            |            |   |  |  | 184                                    |  |  |  |                     |
| 389+75.08 TO 402+90.89      |                  | 1,359                          |             |            |            |            |            |   | 1,359                                  |  |  |  |  |  |                     |
| 401+28.41                   | THROUGH ARROW    |                                |             |            |            |            | 11.5       |   |  |  |  |  |  | 11.5                                   |                     |
| 402+66.36                   | LEFT TURN ARROW  |                                |             |            |            |            | 15.6       |   |  |  |  |  |  | 15.6                                   |                     |
| 402+70.87 TO 403+00.62      | ISLAND           |                                | 49          |            |            |            |            |   |  | 49                                     |  |  |  |  |                     |
| 402+71.76                   | RIGHT TURN ARROW |                                |             |            |            |            | 15.6       |   |  |  |  |  |  | 15.6                                   |                     |
| 402+70.87 TO 403+21.58      | ISLAND           |                                | 65          |            |            |            |            |   |  | 65                                     |  |  |  |  |                     |
| 402+73.00                   | STOP BAR         |                                |             |            |            | 18         |            |   |  |  |  |  | 18                                     |  |                     |
| 402+93.00                   | STOP BAR         |                                |             |            |            | 18         |            |   |  |  |  |  | 18                                     |  |                     |
| 403+00.62 TO 403+21.58      | ISLAND           |                                | 80          |            | 160        |            |            |   |  | 80                                     |  | 160                                    |  |  |                     |
| <b>CH 22 RAMP B</b>         |                  |                                |             |            |            |            |            |   |  |  |  |  |  |  |                     |
| 298+29.90 TO 320+96.40      |                  | 2,287                          |             |            |            |            |            |   | 2,287                                  |  |  |  |  |  |                     |
| 298+59.02 TO 311+59.55      |                  | 1,321                          |             |            |            |            |            |   | 1,321                                  |  |  |  |  |  |                     |
| 311+59.55 TO 313+72.34      |                  |                                |             |            | 222        |            |            |   |  |  | 222                                    |  |  |  |                     |
| <b>CH 22 RAMP C</b>         |                  |                                |             |            |            |            |            |   |  |  |  |  |  |  |                     |
| 480+50.04 TO 503+30.61      |                  | 2,298                          |             |            |            |            |            |   | 2,298                                  |  |  |  |  |  |                     |
| 487+66.87 TO 490+08.69      |                  |                                |             |            | 250        |            |            |   |  |  | 250                                    |  |  |  |                     |
| 490+08.69 TO 503+04.38      |                  | 1,314                          |             |            |            |            |            |   | 1,314                                  |  |  |  |  |  |                     |
| <b>CH 22 RAMP D</b>         |                  |                                |             |            |            |            |            |   |  |  |  |  |  |  |                     |
| 98+39.60 TO 116+56.49       |                  | 1,873                          |             |            |            |            |            |   | 1,873                                  |  |  |  |  |  |                     |
| 98+41.03 TO 98+93.68        | ISLAND           |                                | 72          |            |            |            |            |   |  | 72                                     |  |  |  |  |                     |
| 98+41.03 TO 98+63.23        | ISLAND           |                                | 86          |            | 177        |            |            |   |  | 86                                     |  | 177                                    |  |  |                     |
| 98+63.23 TO 98+93.68        | ISLAND           |                                | 49          |            |            |            |            |   |  | 49                                     |  |  |  |  |                     |
| 98+69.29 TO 111+86.12       |                  | 1,345                          |             |            |            |            |            |   | 1,345                                  |  |  |  |  |  |                     |
| 98+71.83                    | STOP BAR         |                                |             |            |            | 18         |            |   |  |  |  |  | 18                                     |  |                     |
| 98+93.18                    | RIGHT TURN ARROW |                                |             |            |            |            | 11.5       |   |  |  |  |  |  | 11.5                                   |                     |
| 98+97.62                    | STOP BAR         |                                |             |            |            | 18         |            |   |  |  |  |  | 18                                     |  |                     |
| 98+98.18                    | LEFT TURN ARROW  |                                |             |            |            |            | 15.6       |   |  |  |  |  |  | 15.6                                   |                     |
| 100+10.47                   | THROUGH ARROW    |                                |             |            |            |            | 15.6       |   |  |  |  |  |  | 15.6                                   |                     |
| 111+86.12 TO 113+42.92      |                  |                                |             |            | 163        |            |            |   |  |  |  |  | 163                                    |  |                     |

SCHEDULE CONTINUES ON NEXT SHEET

MODEL: Default  
FILE: \\miller\...032707905-chr-schedule.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:57:27 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |       |      |        |
|------------------|-------|------|--------|
| <b>SCHEDULES</b> |       |      |        |
| SCALE: N.T.S.    | SHEET | OF   | SHEETS |
| STA.             | TO    | STA. |        |

|                    |         |                  |              |                          |
|--------------------|---------|------------------|--------------|--------------------------|
| F.A.I. RTE.        | SECTION | COUNTY           | TOTAL SHEETS | SHEET NO.                |
| I-74               | *       | **               | 142          | 40                       |
| CONTRACT NO. 70790 |         |                  |              |                          |
| ILLINOIS           |         | FED. AID PROJECT |              | ** CHAMPAIGN & VERMILION |

# PAVEMENT MARKING

| LOCATION<br>STATION TO STATION | DESCRIPTION      | THERMOPLASTIC PAVEMENT MARKING |            |              |            |            |            | PREFORMED<br>PLASTIC PAVEMENT<br>MARKING, TYPE B -<br>INLAID | GROOVING FOR<br>RECESSED<br>PAVEMENT<br>MARKING | GROOVING FOR<br>RECESSED<br>PAVEMENT<br>MARKING | GROOVING FOR<br>RECESSED<br>PAVEMENT<br>MARKING | GROOVING FOR<br>RECESSED<br>PAVEMENT<br>MARKING | GROOVING FOR<br>RECESSED<br>PAVEMENT<br>MARKING | GROOVING FOR<br>RECESSED<br>PAVEMENT<br>MARKING |       |                        |
|--------------------------------|------------------|--------------------------------|------------|--------------|------------|------------|------------|--|---|---|---|---|---|---|-------|------------------------|
|                                |                  | 78000200                       | 78000400   | 78000500     | 78000600   | 78000650   | 78000100   | 78004210   | X7830070  | X7830074  | X7830076  | X7830078  | X7830090  | X7830060  |       |                        |
|                                |                  | LINE 4"                        |            | LINE 6"      | LINE 8"    | LINE 12"   | LINE 24"   | LETTERS AND<br>SYMBOLS                                       | LINE 4"   | 5"  |   | 7"  | 9"  | 13"   | 25"   | LETTERS AND<br>SYMBOLS |
|                                |                  | WHITE                          | YELLOW     | WHITE        | WHITE      | WHITE      | WHITE      | WHITE  | WHITE   | WHITE   | YELLOW  | WHITE   | WHITE   | WHITE   | WHITE | WHITE                  |
|                                |                  | FOOT                           | FOOT       | FOOT         | FOOT       | FOOT       | FOOT       | FOOT   | FOOT  | FOOT  | FOOT  | FOOT  | FOOT  | FOOT  | SQ FT |                        |
| <b>VERMILION COUNTY</b>        |                  |                                |            |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| <b>I-74 EB</b>                 |                  |                                |            |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| 1053+06.00 TO 1180+00.00       |                  |                                | 12,695     |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| 1053+06.00 TO 1180+00.00       | CENTERLINE       |                                |            |              |            |            | 3,176      |  |   |   |   |   |   |   |       |                        |
| 1053+06.00 TO 1144+62.05       |                  | 9,157                          |            |              |            |            |            | 9,157  |   |   |   |   |   |   |       |                        |
| 1148+13.32 TO 1149+72.44       |                  |                                |            | 160          |            |            |            |  |   |   | 160   |   |   |   |       |                        |
| 1149+72.44 TO 1170+35.89       |                  | 2,064                          |            |              |            |            |            | 2,064  |   |   |   |   |   |   |       |                        |
| 1170+35.89 TO 1172+55.10       |                  |                                |            | 220          |            |            |            |  |   |   | 220   |   |   |   |       |                        |
| <b>I-74 WB</b>                 |                  |                                |            |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| 1053+06.00 TO 1180+00.00       |                  |                                | 12,695     |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| 1053+06.00 TO 1180+00.00       | CENTERLINE       |                                |            |              |            |            | 3,176      |  |   |   |   |   |   |   |       |                        |
| 1053+06.00 TO 1140+51.98       |                  | 8,749                          |            |              |            |            |            | 8,749  |   |   |   |   |   |   |       |                        |
| 1147+51.19 TO 1149+92.28       |                  |                                |            | 242          |            |            |            |  |   |   | 242   |   |   |   |       |                        |
| 1149+92.28 TO 1170+70.63       |                  | 2,079                          |            |              |            |            |            | 2,079  |   |   |   |   |   |   |       |                        |
| 1170+70.63 TO 1172+31.03       |                  |                                |            | 161          |            |            |            |  |   |   | 161   |   |   |   |       |                        |
| 1175+34.24 TO 1180+07.19       |                  | 473                            |            |              |            |            |            | 473  |   |   |   |   |   |   |       |                        |
| <b>IL 49 RAMP A</b>            |                  |                                |            |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| 44+55.13 TO 62+08.99           |                  | 1,789                          |            |              |            |            |            | 1,789  |   |   |   |   |   |   |       |                        |
| 48+05.10 TO 49+65.19           |                  |                                |            | 166          |            |            |            |  |   |   | 166   |   |   |   |       |                        |
| 49+65.19 TO 61+74.90           |                  | 1,251                          |            |              |            |            |            | 1,251  |   |   |   |   |   |   |       |                        |
| 60+22.48                       | THROUGH ARROW    |                                |            |              |            |            | 11.5       |  |   |   |   |   |   |   | 11.5  |                        |
| 61+53.33                       | LEFT TURN ARROW  |                                |            |              |            |            | 15.6       |  |   |   |   |   |   |   | 15.6  |                        |
| 61+54.52 TO 61+84.25           | ISLAND           |                                | 48         |              |            |            |            |  |   | 48  |   |   |   |   |       |                        |
| 61+54.52 TO 62+02.63           | ISLAND           |                                | 60         |              | 135        |            |            |  |   | 60  |   | 135   |   |   |       |                        |
| 61+56.49                       | STOP BAR         |                                |            |              |            | 18         |            |  |   |   |   |   | 18  |   |       |                        |
| 61+59.29                       | RIGHT TURN ARROW |                                |            |              |            |            | 15.6       |  |   |   |   |   |   |   | 15.6  |                        |
| 61+76.06                       | STOP BAR         |                                |            |              |            | 18         |            |  |   |   |   |   | 18  |   |       |                        |
| 62+02.63 TO 61+54.52           | ISLAND           |                                | 72         |              |            |            |            |  |   | 72  |   |   |   |   |       |                        |
| <b>IL 49 RAMP B</b>            |                  |                                |            |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| 62+09.00 TO 83+80.97           |                  | 2,202                          |            |              |            |            |            | 2,202  |   |   |   |   |   |   |       |                        |
| 62+37.40 TO 74+11.71           |                  | 1,200                          |            |              |            |            |            | 1,200  |   |   |   |   |   |   |       |                        |
| 74+11.71 TO 76+29.99           |                  |                                |            | 226          |            |            |            |  |   |   | 226   |   |   |   |       |                        |
| <b>IL 49 RAMP C</b>            |                  |                                |            |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| 40+44.56 TO 61+93.96           |                  | 2,180                          |            |              |            |            |            | 2,180  |   |   |   |   |   |   |       |                        |
| 47+44.04 TO 49+84.26           |                  |                                |            | 247          |            |            |            |  |   |   | 247   |   |   |   |       |                        |
| 49+84.26 TO 61+65.29           |                  | 1,203                          |            |              |            |            |            | 1,203  |   |   |   |   |   |   |       |                        |
| <b>IL 49 RAMP D</b>            |                  |                                |            |              |            |            |            |  |   |   |   |   |   |   |       |                        |
| 0+00.00 TO 75+27.49            |                  | 1,758                          |            |              |            |            |            | 1,758  |   |   |   |   |   |   |       |                        |
| 0+06.67 TO 0+24.77             | ISLAND           |                                | 68         |              | 120        |            |            |  |   | 68  |   | 120   |   |   |       |                        |
| 0+06.67 TO 0+51.07             | ISLAND           |                                | 57         |              |            |            |            |  |   | 57  |   |   |   |   |       |                        |
| 0+17.28                        | RIGHT TURN ARROW |                                |            |              |            |            | 15.6       |  |   |   |   |   |   |   | 15.6  |                        |
| 0+24.77 TO 0+51.07             | ISLAND           |                                | 42         |              |            |            |            |  |   | 42  |   |   |   |   |       |                        |
| 0+32.19                        | STOP BAR         |                                |            |              |            | 18         |            |  |   |   |   |   | 18  |   |       |                        |
| 0+34.30 TO 70+64.48            |                  | 1,254                          |            |              |            |            |            | 1,254  |   |   |   |   |   |   |       |                        |
| 0+49.70                        | LEFT TURN ARROW  |                                |            |              |            |            | 15.6       |  |   |   |   |   |   |   | 15.6  |                        |
| 0+51.18                        | STOP BAR         |                                |            |              |            | 18         |            |  |   |   |   |   | 18  |   |       |                        |
| 60+12.37                       | THROUGH ARROW    |                                |            |              |            |            | 11.5       |  |   |   |   |   |   |   | 11.5  |                        |
| 70+64.48 TO 72+25.51           |                  |                                |            | 166          |            |            |            |  |   |   | 166   |   |   |   |       |                        |
| <b>PROJECT TOTAL</b>           |                  | <b>140,803</b>                 | <b>748</b> | <b>3,198</b> | <b>592</b> | <b>144</b> | <b>172</b> | <b>15,006</b>  | <b>140,803</b>                                  | <b>748</b>                                      | <b>3,198</b>                                    | <b>592</b>                                      | <b>144</b>                                      | <b>172</b>                                      |       |                        |

MODEL Default  
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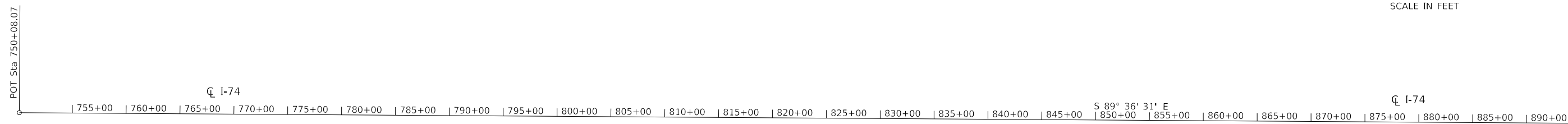
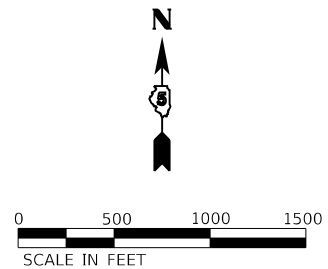
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|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000 ' / in.      | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:57:33 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                  |         |    |        |
|------------------|---------|----|--------|
| <b>SCHEDULES</b> |         |    |        |
| SCALE: N.T.S.    | SHEET   | OF | SHEETS |
| STA.             | TO STA. |    |        |

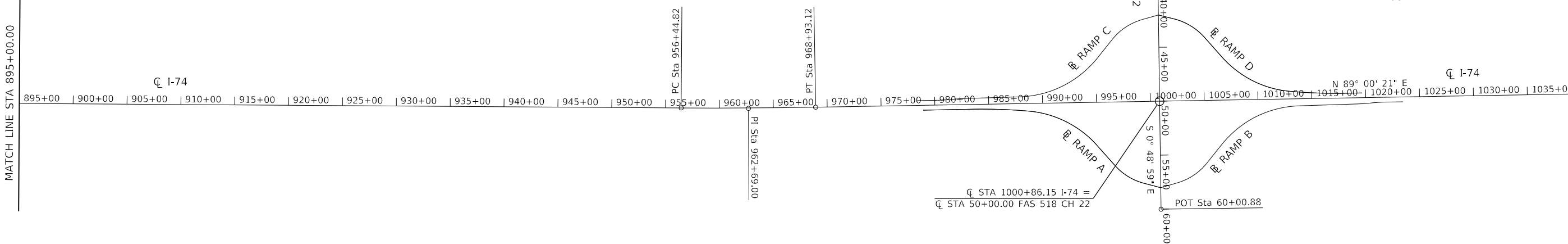
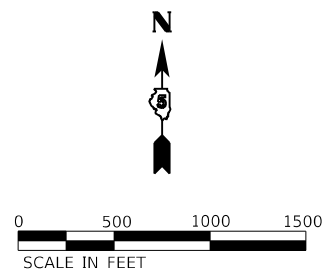
|                    |         |                  |                 |                          |
|--------------------|---------|------------------|-----------------|--------------------------|
| F.A.I.<br>RTE.     | SECTION | COUNTY           | TOTAL<br>SHEETS | SHEET<br>NO.             |
| I-74               | *       | **               | 142             | 41                       |
| CONTRACT NO. 70790 |         |                  |                 |                          |
| ILLINOIS           |         | FED. AID PROJECT |                 | ** CHAMPAIGN & VERMILION |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3



EXIST. CURVE 174\_3  
 PI STA. = 962+69.00  
 $\Delta = 1^\circ 23' 10''$  (LT)  
 $D = 0^\circ 06' 40''$   
 $R = 51,599.32'$   
 $T = 624.18'$   
 $L = 1,248.30'$   
 $E = 3.78'$   
 $e =$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. = 956+44.82  
 P.T. STA. = 968+93.12

SEE INTERCHANGE ALIGNMENT SHEETS



MODEL: D:\p1\11 FILE NAME: P:\11\1007.01\_17410\_CADD\CADD\_S\RES\DS70790-pts-ATB-174-Align.dgn

|                               |                  |           |
|-------------------------------|------------------|-----------|
| USER NAME = gdc               | DESIGNED - BCD   | REVISED - |
| DRAWN - RJO                   | REVISIONS -      |           |
| PLOT SCALE = 1000.0000' / in. | CHECKED - BCD    | REVISED - |
| PLOT DATE = 1/17/2019         | DATE - 9/21/2018 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-74  
 ALIGNMENTS, TIE POINTS & BENCHMARKS**

SCALE: 1" = 500' SHEET OF SHEETS STA. 750+08.07 TO STA. 1040+00

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 42        |
| CONTRACT NO. 70790 |         |        |              |           |

SEE INTERCHANGE ALIGNMENT SHEETS

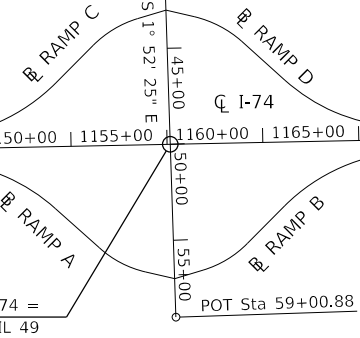
EXIST. CURVE 174.6  
 PI STA. = 1066+32.73  
 $\Delta = 13^\circ 34' 33''$  (LT)  
 $D = 0^\circ 28' 10''$   
 $R = 12,204.43'$   
 $T = 1,452.68'$   
 $L = 2,891.76'$   
 $E = 86.15'$   
 $e =$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. = 1051+80.04  
 P.T. STA. = 1080+71.81

EXIST. CURVE 174.9  
 PI STA. = 1117+50.83  
 $\Delta = 13^\circ 27' 10''$  (RT)  
 $D = 0^\circ 27' 24''$   
 $R = 12,549.28'$   
 $T = 1,480.05'$   
 $L = 2,946.49'$   
 $E = 86.98'$   
 $e =$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. = 1102+70.78  
 P.T. STA. = 1132+17.27

MATCH LINE STA 1040+00.00

MATCH LINE STA 1185+00.00

1040+00 | 1045+00 | 1050+00 | 1055+00 | 1060+00 | 1065+00 | 1070+00 | 1075+00 | 1080+00 | 1085+00 | 1090+00 | 1095+00 | 1100+00 | 1105+00 | 1110+00 | 1115+00 | 1120+00 | 1125+00 | 1130+00 | 1135+00 | 1140+00 | 1145+00 | 1150+00 | 1155+00 | 1160+00 | 1165+00 | 1170+00 | 1175+00 | 1180+00



MATCH LINE STA 1185+00.00

POT Sta 1224+77.59

1185+00 | 1190+00 | 1195+00 | 1200+00 | 1205+00 | 1210+00 | 1215+00 | 1220+00

MODEL: D:\p1\18-1007-01\_I-74\10\_CADD\CADD\_S\RES\0570790-pts-ATB-I74-Align.dgn

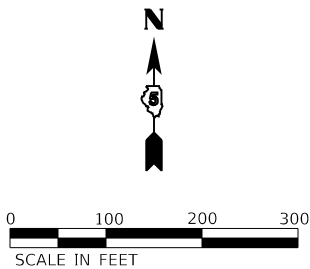
|                               |                  |           |
|-------------------------------|------------------|-----------|
| USER NAME = gdc               | DESIGNED - BCD   | REVISED - |
| DRAWN - RJO                   | REVISIONS -      |           |
| PLOT SCALE = 1000.0000' / in. | CHECKED - BCB    | REVISED - |
| PLOT DATE = 1/17/2019         | DATE - 9/21/2018 | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

|  |              |    |                 |
|--|--------------|----|-----------------|
| <b>I-74</b>                                    |              |    |                 |
| <b>ALIGNMENTS, TIE POINTS &amp; BENCHMARKS</b> |              |    |                 |
| SCALE: 1" = 500'                               | SHEET        | OF | SHEETS          |
|  | STA. 1040+00 | TO | STA. 1224+77.59 |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 43        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION



EXIST. CURVE C271  
 PI STA. = 480+88.89  
 $\Delta = 5^{\circ} 04' 28''$  (LT)  
 $D = 3^{\circ} 43' 41''$   
 $R = 1,536.86'$   
 $T = 68.10'$   
 $L = 136.11'$   
 $E = 1.51'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 480+20.79  
 P.T. STA. = 481+56.90

EXIST. CURVE C269  
 PI STA. = 482+13.93  
 $\Delta = 4^{\circ} 15' 33''$  (RT)  
 $D = 3^{\circ} 44' 23''$   
 $R = 1,532.06'$   
 $T = 56.97'$   
 $L = 113.89'$   
 $E = 1.06'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 481+56.96  
 P.T. STA. = 482+70.84

EXIST. CURVE C267  
 PI STA. = 492+29.95  
 $\Delta = 49^{\circ} 27' 21''$  (LT)  
 $D = 6^{\circ} 00' 08''$   
 $R = 954.60'$   
 $T = 439.63'$   
 $L = 823.98'$   
 $E = 96.37'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 487+90.32  
 P.T. STA. = 496+14.30

EXIST. CURVE C36  
 PI STA. = 500+13.26  
 $\Delta = 35^{\circ} 28' 03''$  (RT)  
 $D = 10^{\circ} 22' 31''$   
 $R = 552.24'$   
 $T = 176.60'$   
 $L = 341.85'$   
 $E = 27.55'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 498+36.66  
 P.T. STA. = 501+78.51

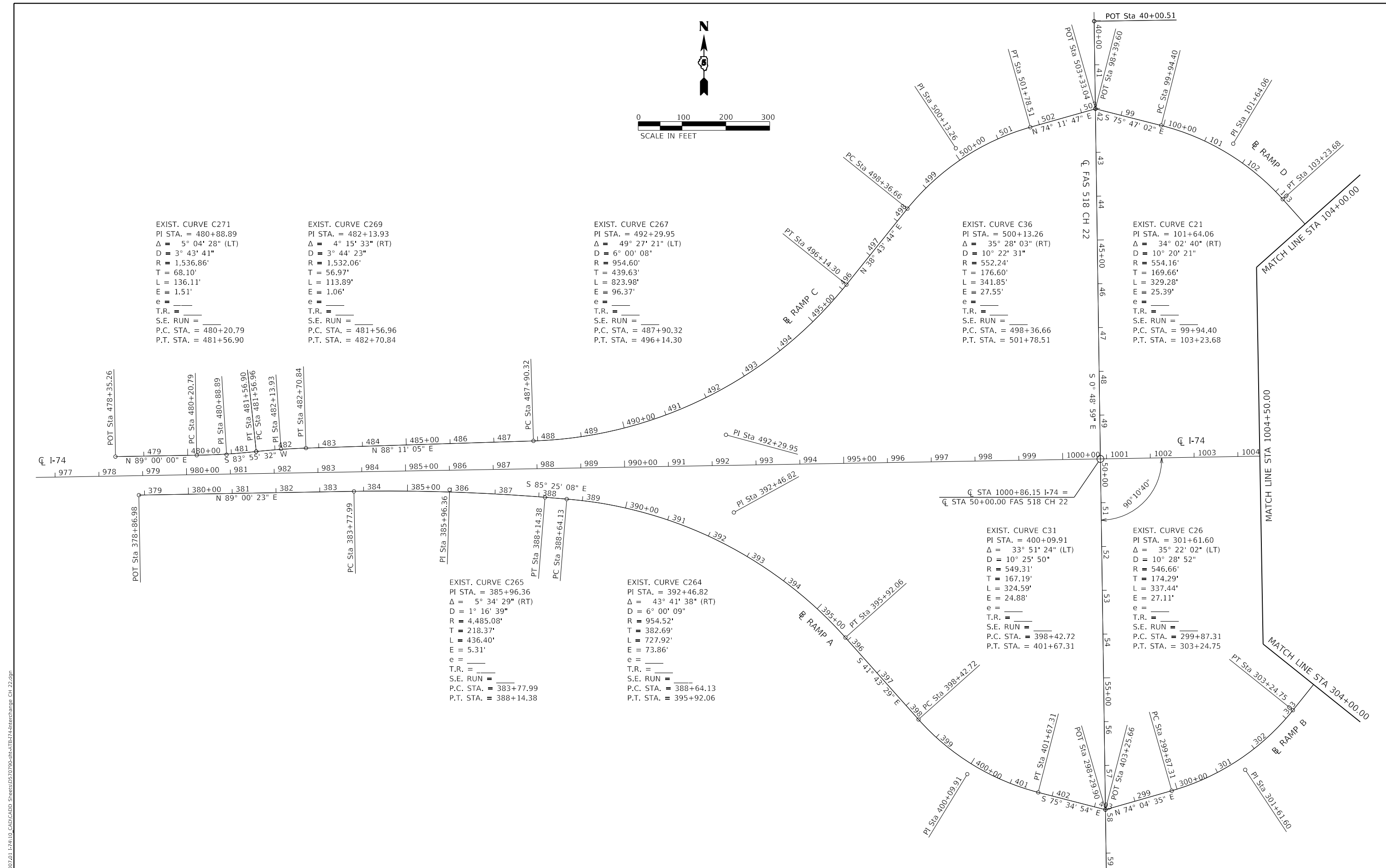
EXIST. CURVE C21  
 PI STA. = 101+64.06  
 $\Delta = 34^{\circ} 02' 40''$  (RT)  
 $D = 10^{\circ} 20' 21''$   
 $R = 554.16'$   
 $T = 169.66'$   
 $L = 329.28'$   
 $E = 25.39'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 99+94.40  
 P.T. STA. = 103+23.68

EXIST. CURVE C265  
 PI STA. = 385+96.36  
 $\Delta = 5^{\circ} 34' 29''$  (RT)  
 $D = 1^{\circ} 16' 39''$   
 $R = 4,485.08'$   
 $T = 218.37'$   
 $L = 436.40'$   
 $E = 5.31'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 383+77.99  
 P.T. STA. = 388+14.38

EXIST. CURVE C264  
 PI STA. = 392+46.82  
 $\Delta = 43^{\circ} 41' 38''$  (RT)  
 $D = 6^{\circ} 00' 09''$   
 $R = 954.52'$   
 $T = 382.69'$   
 $L = 727.92'$   
 $E = 73.86'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 388+64.13  
 P.T. STA. = 395+92.06

EXIST. CURVE C31  
 PI STA. = 400+09.91  
 $\Delta = 33^{\circ} 51' 24''$  (LT)  
 $D = 10^{\circ} 25' 50''$   
 $R = 549.31'$   
 $T = 167.19'$   
 $L = 324.59'$   
 $E = 24.88'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 398+42.72  
 P.T. STA. = 401+67.31

EXIST. CURVE C26  
 PI STA. = 301+61.60  
 $\Delta = 35^{\circ} 22' 02''$  (LT)  
 $D = 10^{\circ} 28' 52''$   
 $R = 546.66'$   
 $T = 174.29'$   
 $L = 337.44'$   
 $E = 27.11'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 299+87.31  
 P.T. STA. = 303+24.75



MODEL: D:\dft\11  
 FILE NAME: P:\1007.01\_17410\_CAD\CADD\_Series\0570790-pts-ATB-I74-Interchange\_C11\_22.dgn

|                              |                  |           |
|------------------------------|------------------|-----------|
| USER NAME = gdc              | DESIGNED - BCD   | REVISED - |
|                              | DRAWN - RJO      | REVISED - |
| PLOT SCALE = 200.0000' / in. | CHECKED - BCD    | REVISED - |
| PLOT DATE = 1/17/2019        | DATE - 9/21/2018 | REVISED - |

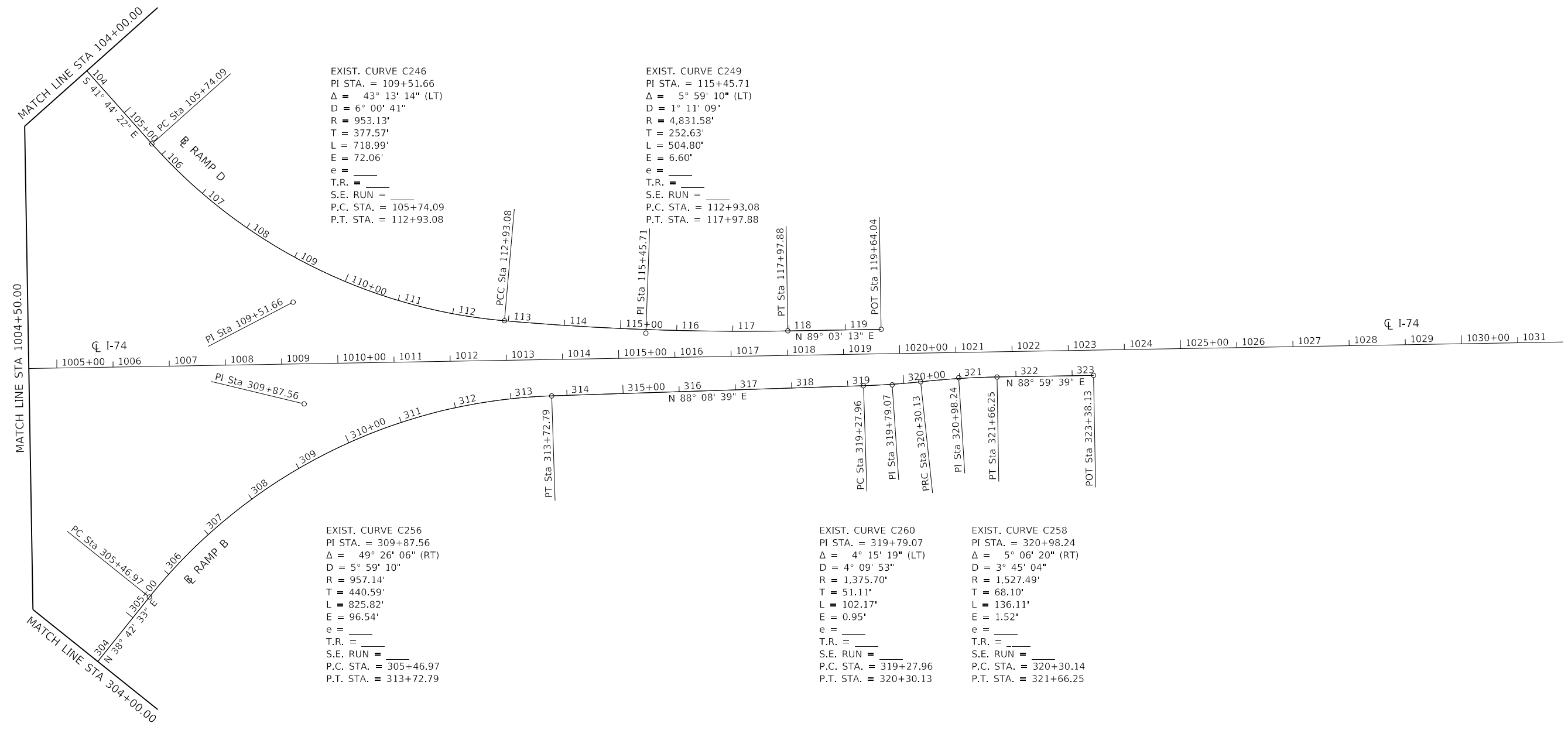
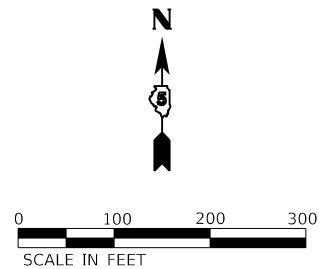
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

|  |       |           |                             |
|--|-------|-----------|-----------------------------|
| <b>INTERCHANGE ALIGNMENT</b>           |       |           |                             |
| <b>I-74 &amp; FAS 518 CH 22, RAMPS</b> |       |           |                             |
| SCALE: 1" = 100'                       | SHEET | OF SHEETS | STA. 977+00 TO STA. 1004+50 |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 44        |
| CONTRACT NO. 70790 |         |        |              |           |

ILLINOIS FED. AID PROJECT  
 (10-7.8;92-8.9)RS-2(92-10)RS-3  
 \*\* CHAMPAIGN & VERMILION





MODEL: D:\p1\107\01\_17410\_CAD\CADD\_Series\0570790-pts-ATB-I74-Interchange\_C1\_22.dgn  
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|                              |                  |           |
|------------------------------|------------------|-----------|
| USER NAME = gdc              | DESIGNED - BCD   | REVISED - |
| DRAWN - RJO                  | REVISIONS -      |           |
| PLOT SCALE = 200.0000' / in. | CHECKED - BCD    | REVISED - |
| PLOT DATE = 1/17/2019        | DATE - 9/21/2018 | REVISED - |

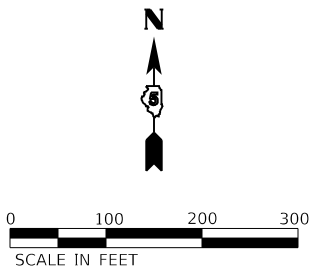
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INTERCHANGE ALIGNMENT  
I-74 & FAS 518 CH 22, RAMPS**

SCALE: 1" = 100'    SHEET    OF    SHEETS    STA. 1004+50    TO STA. 1004+50

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 45        |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3    \*\* CHAMPAIGN & VERMILION



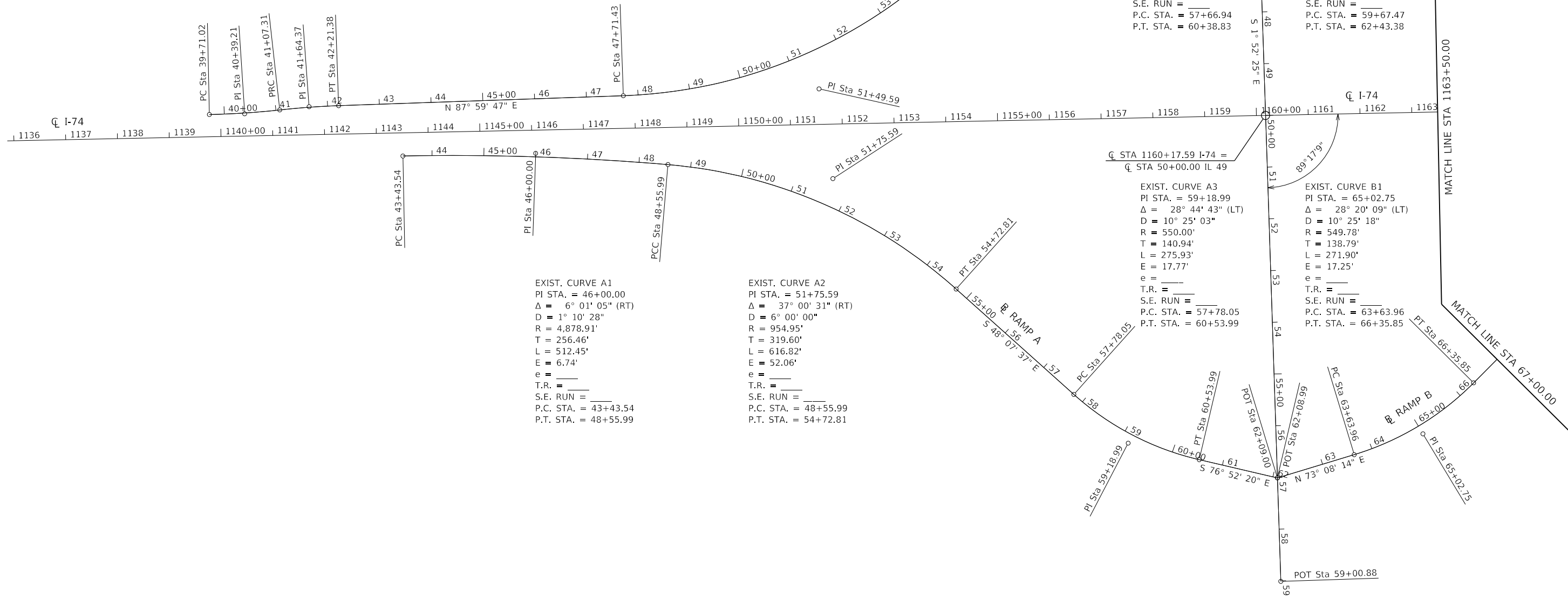
EXIST. CURVE C1  
 PI STA. = 40+39.21  
 $\Delta = 5^\circ 06' 44''$  (LT)  
 $D = 3^\circ 45' 04''$   
 $R = 1,527.48'$   
 $T = 68.19'$   
 $L = 136.29'$   
 $E = 1.52'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 39+71.02  
 P.T. STA. = 41+07.31

EXIST. CURVE C2  
 PI STA. = 41+64.37  
 $\Delta = 4^\circ 16' 44''$  (RT)  
 $D = 3^\circ 45' 04''$   
 $R = 1,527.40'$   
 $T = 57.06'$   
 $L = 114.07'$   
 $E = 1.07'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 41+07.31  
 P.T. STA. = 42+21.38

EXIST. CURVE C3  
 PI STA. = 51+49.59  
 $\Delta = 43^\circ 12' 30''$  (LT)  
 $D = 6^\circ 00' 00''$   
 $R = 954.92'$   
 $T = 378.16'$   
 $L = 720.13'$   
 $E = 72.15'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 47+71.43  
 P.T. STA. = 54+91.57

EXIST. CURVE C4  
 PI STA. = 59+05.73  
 $\Delta = 28^\circ 20' 49''$  (RT)  
 $D = 10^\circ 25' 33''$   
 $R = 549.56'$   
 $T = 138.79'$   
 $L = 271.89'$   
 $E = 17.25'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 57+66.94  
 P.T. STA. = 60+38.83

EXIST. CURVE D1  
 PI STA. = 61+08.39  
 $\Delta = 28^\circ 44' 33''$  (RT)  
 $D = 10^\circ 25' 03''$   
 $R = 550.00'$   
 $T = 140.92'$   
 $L = 275.91'$   
 $E = 17.77'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 59+67.47  
 P.T. STA. = 62+43.38



EXIST. CURVE A1  
 PI STA. = 46+00.00  
 $\Delta = 6^\circ 01' 05''$  (RT)  
 $D = 1^\circ 10' 28''$   
 $R = 4,878.91'$   
 $T = 256.46'$   
 $L = 512.45'$   
 $E = 6.74'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 43+43.54  
 P.T. STA. = 48+55.99

EXIST. CURVE A2  
 PI STA. = 51+75.59  
 $\Delta = 37^\circ 00' 31''$  (RT)  
 $D = 6^\circ 00' 00''$   
 $R = 954.95'$   
 $T = 319.60'$   
 $L = 616.82'$   
 $E = 52.06'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 48+55.99  
 P.T. STA. = 54+72.81

EXIST. CURVE A3  
 PI STA. = 59+18.99  
 $\Delta = 28^\circ 44' 43''$  (LT)  
 $D = 10^\circ 25' 03''$   
 $R = 550.00'$   
 $T = 140.94'$   
 $L = 275.93'$   
 $E = 17.77'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 57+78.05  
 P.T. STA. = 60+53.99

EXIST. CURVE B1  
 PI STA. = 65+02.75  
 $\Delta = 28^\circ 20' 09''$  (LT)  
 $D = 10^\circ 25' 18''$   
 $R = 549.78'$   
 $T = 138.79'$   
 $L = 271.90'$   
 $E = 17.25'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 63+63.96  
 P.T. STA. = 66+35.85

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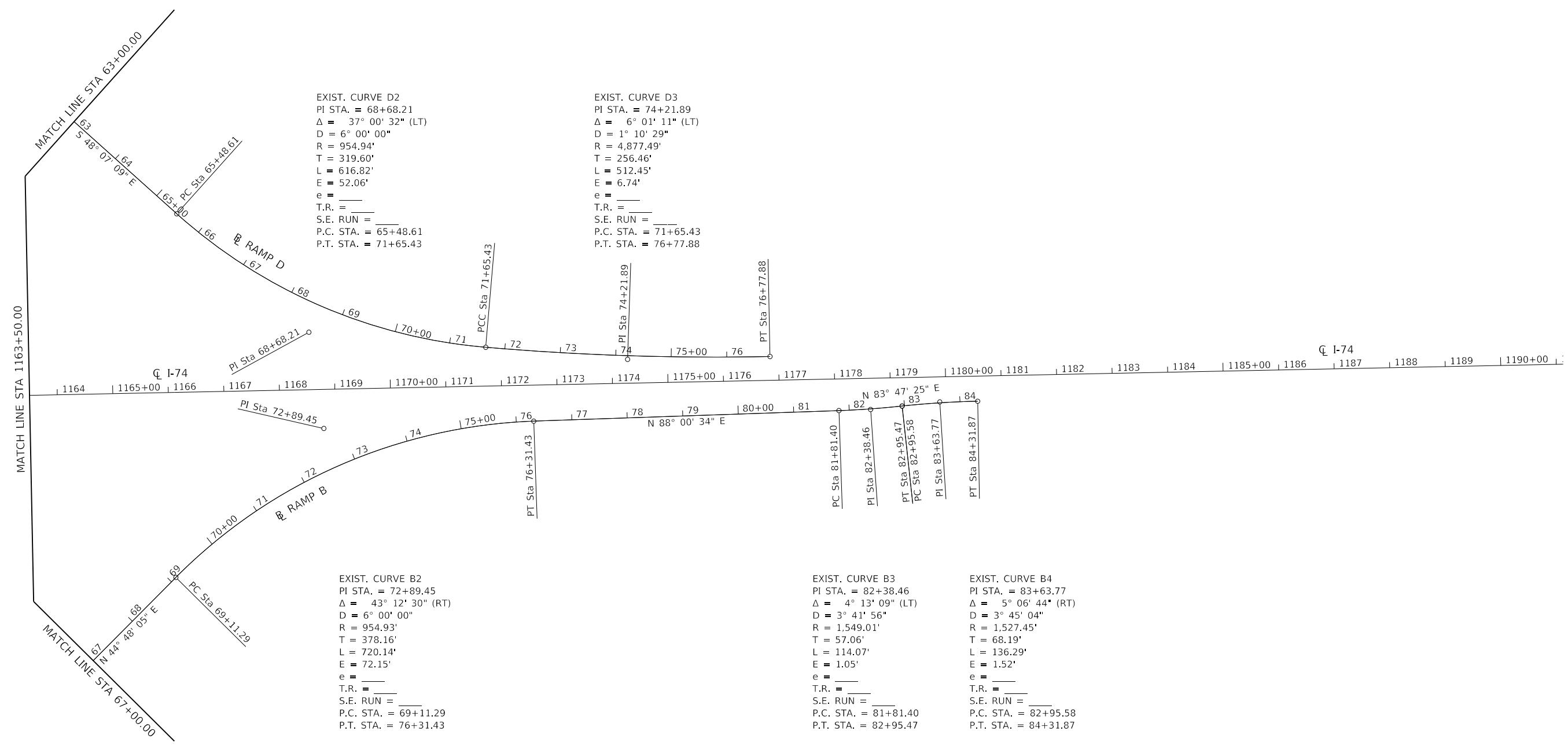
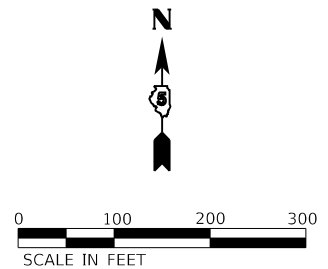
|                              |                  |           |
|------------------------------|------------------|-----------|
| USER NAME = gdc              | DESIGNED - BCD   | REVISED - |
| DRAWN - RJO                  | REVISED -        |           |
| PLOT SCALE = 200.0000' / in. | CHECKED - BCD    | REVISED - |
| PLOT DATE = 1/17/2019        | DATE - 9/21/2018 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|  |  |
|--|--|
| <b>INTERCHANGE ALIGNMENT<br/>I-74 &amp; IL 49, RAMPS</b> |  |
| SCALE: 1" = 100'   | SHEET OF SHEETS STA. 1136+80 TO STA. 1163+50 |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 46        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION



EXIST. CURVE D2  
 PI STA. = 68+68.21  
 $\Delta = 37^\circ 00' 32''$  (LT)  
 $D = 6^\circ 00' 00''$   
 $R = 954.94'$   
 $T = 319.60'$   
 $L = 616.82'$   
 $E = 52.06'$   
 $e =$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. = 65+48.61  
 P.T. STA. = 71+65.43

EXIST. CURVE D3  
 PI STA. = 74+21.89  
 $\Delta = 6^\circ 01' 11''$  (LT)  
 $D = 1^\circ 10' 29''$   
 $R = 4,877.49'$   
 $T = 256.46'$   
 $L = 512.45'$   
 $E = 6.74'$   
 $e =$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. = 71+65.43  
 P.T. STA. = 76+77.88

EXIST. CURVE B2  
 PI STA. = 72+89.45  
 $\Delta = 43^\circ 12' 30''$  (RT)  
 $D = 6^\circ 00' 00''$   
 $R = 954.93'$   
 $T = 378.16'$   
 $L = 720.14'$   
 $E = 72.15'$   
 $e =$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. = 69+11.29  
 P.T. STA. = 76+31.43

EXIST. CURVE B3  
 PI STA. = 82+38.46  
 $\Delta = 4^\circ 13' 09''$  (LT)  
 $D = 3^\circ 41' 56''$   
 $R = 1,549.01'$   
 $T = 57.06'$   
 $L = 114.07'$   
 $E = 1.05'$   
 $e =$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. = 81+81.40  
 P.T. STA. = 82+95.47

EXIST. CURVE B4  
 PI STA. = 83+63.77  
 $\Delta = 5^\circ 06' 44''$  (RT)  
 $D = 3^\circ 45' 04''$   
 $R = 1,527.45'$   
 $T = 68.19'$   
 $L = 136.29'$   
 $E = 1.52'$   
 $e =$   
 T.R. =  
 S.E. RUN =  
 P.C. STA. = 82+95.58  
 P.T. STA. = 84+31.87

MODEL: D:\dxf\it  
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|                              |                  |           |
|------------------------------|------------------|-----------|
| USER NAME = gdc              | DESIGNED - BCD   | REVISED - |
|                              | DRAWN - RJO      | REVISED - |
| PLOT SCALE = 200.0000' / in. | CHECKED - BCD    | REVISED - |
| PLOT DATE = 1/17/2019        | DATE - 9/21/2018 | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|  |              |         |         |
|--|--------------|---------|---------|
| <b>INTERCHANGE ALIGNMENT<br/>I-74 &amp; IL 49, RAMPS</b> |              |         |         |
| SCALE: 1" = 100'   | SHEET        | OF      | SHEETS  |
|  | STA. 1163+50 | TO STA. | 1190+00 |

|             |         |        |                    |           |
|-------------|---------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| I-74        | *       | **     | 142                | 47        |
|             |         |        | CONTRACT NO. 70790 |           |

**CONTROL POINTS**

| CENTERLINE OF I-74             |               |               |            |          |      |             |
|--------------------------------|---------------|---------------|------------|----------|------|-------------|
| POINT NO.                      | NORTHING      | EASTING       | STATION    | OFFSET   | CODE | DESCRIPTION |
| I741                           | 1,257,896.331 | 1,063,907.862 | 750+08.07  | 0.00'    | -    | POT         |
|                                | 1,257,807.553 | 1,076,899.489 | 880+00.00  | 0.00'    |      | POB         |
| PC I74_3                       | 1,257,755.313 | 1,084,544.134 | 956+44.82  | 0.00'    |      | PC          |
| PI I74_3                       | 1,257,751.048 | 1,085,168.299 | 962+69.00  | 3.775'   |      | PI          |
| PT I74_3                       | 1,257,761.882 | 1,085,792.385 | 968+93.12  | 0.00'    |      | PT          |
| RP119                          | 1,257,817.278 | 1,088,984.935 | 1000+86.15 | 0.00'    |      | I-74/CH 22  |
| PC I74_6                       | 1,257,905.651 | 1,094,078.059 | 1051+80.04 | 0.00'    |      | PC          |
| PI I74_6                       | 1,257,930.843 | 1,095,530.526 | 1066+32.73 | 86.152'  |      | PI          |
| PT I74_6                       | 1,258,296.273 | 1,096,936.496 | 1080+71.81 | 0.00'    |      | PT          |
| PC I74_9                       | 1,258,849.356 | 1,099,064.776 | 1102+70.78 | 0.00'    |      | PC          |
| PI I74_9                       | 1,259,222.923 | 1,100,496.906 | 1117+50.83 | -86.976' |      | PI          |
| PT I74_9                       | 1,259,253.066 | 1,101,976.649 | 1132+17.27 | 0.00'    |      | PT          |
| RP117                          | 1,259,309.736 | 1,104,776.401 | 1160+17.59 | 0.00'    |      | I-74/IL 49  |
|                                | 1,259,349.854 | 1,106,758.402 | 1180+00.00 | 0.00'    |      | POE         |
| I7411                          | 1,259,440.466 | 1,111,235.076 | 1224+77.59 | 0.00'    |      | POT         |
| FAS 518 CH22 (Chain 3350IL49S) |               |               |            |          |      |             |
| POINT NO.                      | NORTHING      | EASTING       | STATION    | OFFSET   | CODE | DESCRIPTION |
| 41                             | 1,259,466.605 | 1,088,961.437 | 33+50.51   | 0.00'    | -    | POT         |
| 42                             | 1,258,816.671 | 1,088,970.696 | 40+00.51   | 0.00'    |      | POT         |
| FAS 518 CH22 (Chain IL49S)     |               |               |            |          |      |             |
| POINT NO.                      | NORTHING      | EASTING       | STATION    | OFFSET   | CODE | DESCRIPTION |
| 43                             | 1,258,816.671 | 1,088,970.696 | 40+00.51   | 0.00'    | -    | POT         |
| 44                             | 1,256,816.504 | 1,088,999.194 | 60+00.88   | 0.00'    |      | POT         |
| IL49 (Chain IL49N)             |               |               |            |          |      |             |
| POINT NO.                      | NORTHING      | EASTING       | STATION    | OFFSET   | CODE | DESCRIPTION |
| RP115                          | 1,260,208.480 | 1,104,746.999 | 41+00.78   | 0.00'    | -    | POT         |
| RP116                          | 1,258,409.341 | 1,104,805.857 | 59+00.88   | 0.00'    |      | POT         |
| I-74 & CH 22 RAMP A STATIONS   |               |               |            |          |      |             |
| POINT NO.                      | NORTHING      | EASTING       | STATION    | OFFSET   | CODE | DESCRIPTION |
| R268                           | 1,257,734.708 | 1,086,790.254 | 378+86.98  | 0.00'    | -    | POT         |
| PC C265                        | 1,257,743.223 | 1,087,281.187 | 383+77.99  | 0.00'    |      | PC          |
| PI C265                        | 1,257,747.010 | 1,087,499.524 | 385+96.36  | -5.313'  |      | PI          |
| PT C265                        | 1,257,729.568 | 1,087,717.196 | 388+14.38  | 0.00'    |      | PT          |
| PC C264                        | 1,257,725.594 | 1,087,766.789 | 388+64.13  | 0.00'    |      | PC          |
| PI C264                        | 1,257,695.028 | 1,088,148.257 | 392+46.82  | -73.857' |      | PI          |
| PT C264                        | 1,257,409.407 | 1,088,402.958 | 395+92.06  | 0.00'    |      | PT          |
| PC C31                         | 1,257,222.324 | 1,088,569.788 | 398+42.72  | 0.00'    |      | PC          |
| PI C31                         | 1,257,097.542 | 1,088,681.062 | 400+09.91  | 24.880'  |      | PI          |
| PT C31                         | 1,257,055.912 | 1,088,842.986 | 401+67.31  | 0.00'    |      | PT          |
| R263                           | 1,257,016.483 | 1,088,996.345 | 403+25.66  | 0.00'    |      | POT         |
| I-74 & CH 22 RAMP B STATIONS   |               |               |            |          |      |             |
| POINT NO.                      | NORTHING      | EASTING       | STATION    | OFFSET   | CODE | DESCRIPTION |
| R15                            | 1,257,016.483 | 1,088,996.345 | 298+29.90  | 0.00'    | -    | POT         |
| PC C26                         | 1,257,059.670 | 1,089,147.717 | 299+87.31  | 0.00'    |      | PC          |
| PI C26                         | 1,257,107.488 | 1,089,315.319 | 301+61.60  | 27.112'  |      | PI          |
| PT C26                         | 1,257,243.492 | 1,089,424.314 | 303+24.75  | 0.00'    |      | PT          |
| PC C256                        | 1,257,416.891 | 1,089,563.279 | 305+46.97  | 0.00'    |      | PC          |
| PI C256                        | 1,257,760.697 | 1,089,838.810 | 309+87.56  | -96.538' |      | PI          |
| PT C256                        | 1,257,774.965 | 1,090,279.168 | 313+72.79  | 0.00'    |      | PT          |
| PC C260                        | 1,257,792.944 | 1,090,834.045 | 319+27.96  | 0.00'    |      | PC          |
| PI C260                        | 1,257,794.599 | 1,090,885.128 | 319+79.07  | 0.949'   |      | PI          |
| PT C260                        | 1,257,800.040 | 1,090,935.947 | 320+30.13  | 0.00'    |      | PT          |
| PC C258                        | 1,257,800.040 | 1,090,935.943 | 320+30.14  | 0.00'    |      | PC          |
| PI C258                        | 1,257,807.290 | 1,091,003.656 | 320+98.24  | -1.517'  |      | PI          |
| PT C258                        | 1,257,808.485 | 1,091,071.746 | 321+66.25  | 0.00'    |      | PT          |
| R262                           | 1,257,811.502 | 1,091,243.599 | 323+38.13  | 0.00'    |      | POT         |

| I-74 & CH 22 RAMP C STATIONS |               |               |           |          |      |             |
|------------------------------|---------------|---------------|-----------|----------|------|-------------|
| POINT NO.                    | NORTHING      | EASTING       | STATION   | OFFSET   | CODE | DESCRIPTION |
| R273                         | 1,257,822.473 | 1,086,736.808 | 478+35.26 | 0.00'    | -    | POT         |
| PC C271                      | 1,257,825.711 | 1,086,922.311 | 480+20.79 | 0.00'    |      | PC          |
| PI C271                      | 1,257,826.900 | 1,086,990.401 | 480+88.89 | 1.508'   |      | PI          |
| PT C271                      | 1,257,834.106 | 1,087,058.119 | 481+56.90 | 0.00'    |      | PT          |
| PC C269                      | 1,257,834.100 | 1,087,058.065 | 481+56.96 | 0.00'    |      | PC          |
| PI C269                      | 1,257,840.129 | 1,087,114.715 | 482+13.93 | -1.058'  |      | PI          |
| PT C269                      | 1,257,841.934 | 1,087,171.657 | 482+70.85 | 0.00'    |      | PT          |
| PC C267                      | 1,257,858.389 | 1,087,690.875 | 487+90.32 | 0.00'    |      | PC          |
| PI C267                      | 1,257,872.315 | 1,088,130.285 | 492+29.95 | 96.369'  |      | PI          |
| PT C267                      | 1,258,215.277 | 1,088,405.333 | 496+14.30 | 0.00'    |      | PT          |
| PC C36                       | 1,258,388.744 | 1,088,544.449 | 498+36.66 | 0.00'    |      | PC          |
| PI C36                       | 1,258,526.512 | 1,088,654.936 | 500+13.26 | -27.550' |      | PI          |
| PT C36                       | 1,258,574.607 | 1,088,824.861 | 501+78.51 | 0.00'    |      | PT          |
| R274                         | 1,258,616.691 | 1,088,973.546 | 503+33.04 | 0.00'    |      | POT         |
| I-74 & CH 22 RAMP D STATIONS |               |               |           |          |      |             |
| POINT NO.                    | NORTHING      | EASTING       | STATION   | OFFSET   | CODE | DESCRIPTION |
| R14                          | 1,258,616.691 | 1,088,973.546 | 98+39.60  | 0.00'    | -    | POT         |
| PC C21                       | 1,258,578.675 | 1,089,123.610 | 99+94.40  | 0.00'    |      | PC          |
| PI C21                       | 1,258,537.010 | 1,089,288.074 | 101+64.06 | -25.389' |      | PI          |
| PT C21                       | 1,258,410.413 | 1,089,401.024 | 103+23.68 | 0.00'    |      | PT          |
| PC C246                      | 1,258,223.566 | 1,089,567.731 | 105+74.09 | 0.00'    |      | PC          |
| PI C246                      | 1,257,941.831 | 1,089,819.097 | 109+51.66 | 72.061'  |      | PI          |
| PT C246                      | 1,257,908.663 | 1,090,195.207 | 112+93.08 | 0.00'    |      | PT          |
| PC C249                      | 1,257,908.663 | 1,090,195.208 | 112+93.08 | 0.00'    |      | PC          |
| PI C249                      | 1,257,886.470 | 1,090,446.862 | 115+45.71 | 6.600'   |      | PI          |
| PT C249                      | 1,257,890.643 | 1,090,699.457 | 117+97.88 | 0.00'    |      | PT          |
| R255                         | 1,257,893.388 | 1,090,865.594 | 119+64.04 | 0.00'    |      | POT         |
| I-74 & IL 49 RAMP A STATIONS |               |               |           |          |      |             |
| POINT NO.                    | NORTHING      | EASTING       | STATION   | OFFSET   | CODE | DESCRIPTION |
| PC A1                        | 1,259,231.276 | 1,103,110.500 | 43+43.54  | 0.00'    | -    | PC          |
| PI A1                        | 1,259,236.439 | 1,103,366.908 | 46+00.00  | -6.736'  |      | PI          |
| PT A1/PC A2                  | 1,259,214.692 | 1,103,622.444 | 48+55.99  | 0.00'    |      | PCC         |
| PI A2                        | 1,259,187.590 | 1,103,940.893 | 51+75.59  | -52.062' |      | PI          |
| PT A2                        | 1,258,974.263 | 1,104,178.876 | 54+72.81  | 0.00'    |      | PT          |
| PC A3                        | 1,258,770.520 | 1,104,406.167 | 57+78.05  | 0.00'    |      | PC          |
| PI A3                        | 1,258,676.448 | 1,104,511.112 | 59+18.99  | 17.770'  |      | PI          |
| PT A3                        | 1,258,644.438 | 1,104,648.365 | 60+53.99  | 0.00'    |      | PT          |
| RP113                        | 1,258,609.234 | 1,104,799.318 | 62+08.99  | 0.00'    |      | POT         |
| I-74 & IL 49 RAMP B STATIONS |               |               |           |          |      |             |
| POINT NO.                    | NORTHING      | EASTING       | STATION   | OFFSET   | CODE | DESCRIPTION |
| RP114                        | 1,258,609.234 | 1,104,799.318 | 62+09.00  | 0.00'    | -    | POT         |
| PC B1                        | 1,258,654.185 | 1,104,947.617 | 63+63.96  | 0.00'    |      | PC          |
| PI B1                        | 1,258,694.445 | 1,105,080.437 | 65+02.75  | 17.248'  |      | PI          |
| PT B1                        | 1,258,792.923 | 1,105,178.234 | 66+35.85  | 0.00'    |      | PT          |
| PC B2                        | 1,258,988.361 | 1,105,372.321 | 69+11.29  | 0.00'    |      | PC          |
| PI B2                        | 1,259,256.690 | 1,105,638.794 | 72+89.46  | -72.153' |      | PI          |
| PT B2                        | 1,259,269.825 | 1,106,016.730 | 76+31.43  | 0.00'    |      | PT          |
| PC B3                        | 1,259,288.927 | 1,106,566.368 | 81+81.40  | 0.00'    |      | PC          |
| PI B3                        | 1,259,290.909 | 1,106,623.394 | 82+38.46  | 1.051'   |      | PI          |
| PT B3                        | 1,259,297.081 | 1,106,680.119 | 82+95.47  | 0.00'    |      | PT          |
| PC B4                        | 1,259,297.093 | 1,106,680.229 | 82+95.58  | 0.00'    |      | PC          |
| PI B4                        | 1,259,304.469 | 1,106,748.019 | 83+63.77  | -1.521'  |      | PI          |
| PT B4                        | 1,259,305.775 | 1,106,816.196 | 84+31.87  | 0.00'    |      | PT          |

HORIZONTAL & GPS CONTROL - GRID COORDINATES WERE USED  
 NAD 83 DATUM WITH A 2011 ADJUSTMENT FOR HORIZONTAL CONTROL AND  
 NAVD 88 DATUM WITH A GEOID 12A WAS USED FOR THE VERTICAL CONTROL

VERTICAL CONTROL - NAVD 88 DATUM

| I-74 & IL 49 RAMP C STATIONS |               |               |          |          |      |             |
|------------------------------|---------------|---------------|----------|----------|------|-------------|
| POINT NO.                    | NORTHING      | EASTING       | STATION  | OFFSET   | CODE | DESCRIPTION |
| PC C1                        | 1,259,311.446 | 1,102,736.733 | 39+71.02 | 0.00'    | -    | PC          |
| PI C1                        | 1,259,312.839 | 1,102,804.908 | 40+39.21 | 1.521'   |      | PI          |
| PT C1/PC C2                  | 1,259,320.301 | 1,102,872.689 | 41+07.31 | 0.00'    |      | PRC         |
| PI C2                        | 1,259,326.545 | 1,102,929.406 | 41+64.37 | -1.065'  |      | PI          |
| PT C2                        | 1,259,328.540 | 1,102,986.431 | 42+21.38 | 0.00'    |      | PT          |
| PC C3                        | 1,259,347.770 | 1,103,536.153 | 47+71.43 | 0.00'    |      | PC          |
| PI C3                        | 1,259,360.991 | 1,103,914.082 | 51+49.59 | 72.152'  |      | PI          |
| PT C3                        | 1,259,629.377 | 1,104,180.491 | 54+91.57 | 0.00'    |      | PT          |
| PC C4                        | 1,259,824.817 | 1,104,374.491 | 57+66.94 | 0.00'    |      | PC          |
| PI C4                        | 1,259,923.317 | 1,104,472.265 | 59+05.73 | -17.254' |      | PI          |
| PT C4                        | 1,259,963.582 | 1,104,605.084 | 60+38.83 | 0.00'    |      | PT          |
| RP112                        | 1,260,008.587 | 1,104,753.539 | 61+93.96 | 0.00'    |      | POT         |
| I-74 & IL 49 RAMP D STATIONS |               |               |          |          |      |             |
| POINT NO.                    | NORTHING      | EASTING       | STATION  | OFFSET   | CODE | DESCRIPTION |
| RP111                        | 1,260,008.587 | 1,104,753.539 | 0+00.00  | 0.00'    | -    | POT         |
| STA EQU/PC D1                |               |               | 1+55.02  |          |      | STA EQU     |
| PC D1                        | 1,259,973.350 | 1,104,904.499 | 59+67.47 | 0.00'    |      | PC          |
| PI D1                        | 1,259,941.318 | 1,105,041.732 | 61+08.39 | -17.767' |      | PI          |
| PT D1                        | 1,259,847.241 | 1,105,146.653 | 62+43.38 | 0.00'    |      | PT          |
| PC D2                        | 1,259,643.468 | 1,105,373.913 | 65+48.61 | 0.00'    |      | PC          |
| PI D2                        | 1,259,430.108 | 1,105,611.866 | 68+68.21 | 52.063'  |      | PI          |
| PT D2/PC D3                  | 1,259,402.964 | 1,105,930.311 | 71+65.43 | 0.00'    |      | PCC         |
| PI D3                        | 1,259,381.183 | 1,106,185.845 | 74+21.89 | 6.738'   |      | PI          |
| PT D3                        | 1,259,386.320 | 1,106,442.253 | 76+77.88 | 0.00'    |      | PT          |

**BENCHMARKS**

NO PERMANENT BENCHMARKS WERE SET ALONG THIS PROJECT

THE MAJORITY OF THE PROJECT WAS SURVEYED BY MEANS OF REAL-TIME GPS. THE ELEVATIONS THAT ARE LEFT ON THE PROJECT ARE GPS ELEVATIONS ON THE TRAVERSE STATIONS.

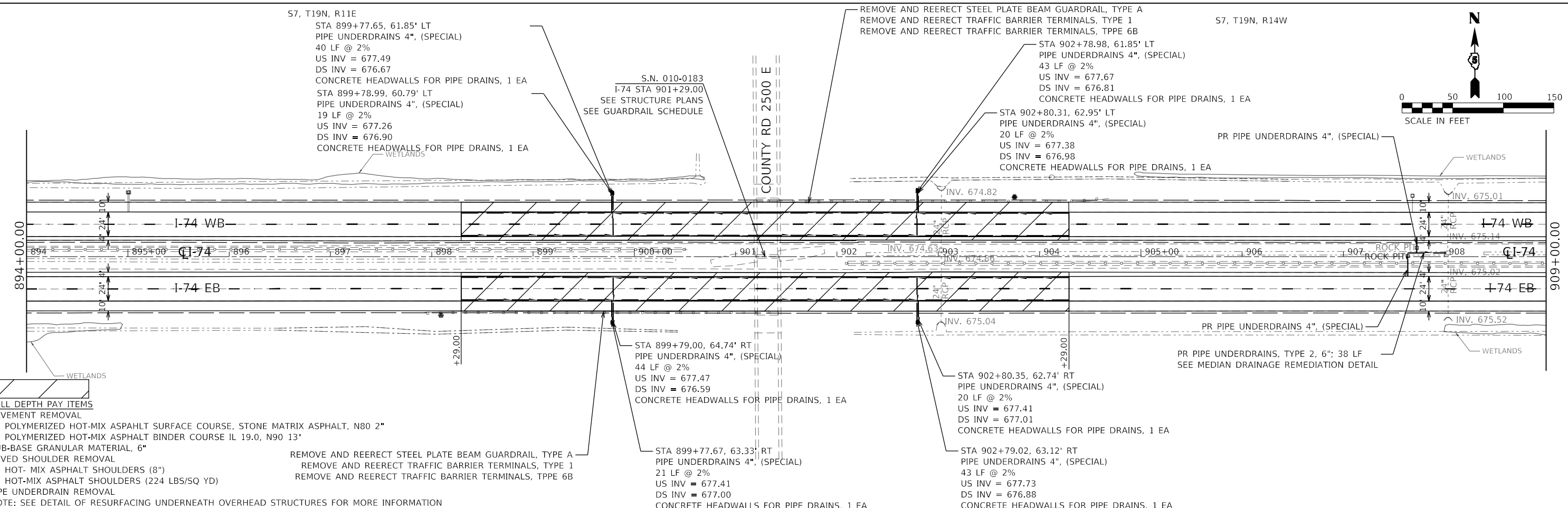
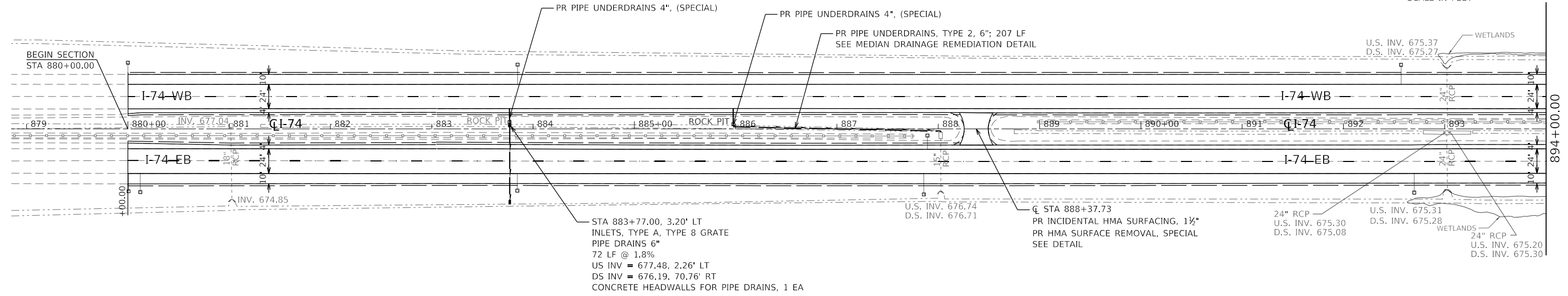
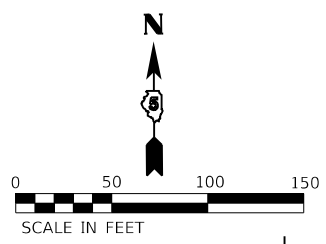
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|                 |                |             |
|-----------------|----------------|-------------|
| USER NAME = gdc | DESIGNED - BCD | REVISED -</ |
|-----------------|----------------|-------------|

**LEGEND**

PR PIPE UNDERDRAIN  
(SEE SCHEDULE AND DETAILS FOR MORE INFORMATION)

S7, T19N, R11E



**FULL DEPTH PAY ITEMS**

PAVEMENT REMOVAL  
PR POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80 2"  
PR POLYMERIZED HOT-MIX ASPHALT BINDER COURSE IL 19.0, N90 13"  
SUB-BASE GRANULAR MATERIAL, 6"  
PAVED SHOULDER REMOVAL  
PR HOT-MIX ASPHALT SHOULDERS (8")  
PR HOT-MIX ASPHALT SHOULDERS (224 LBS/SQ YD)  
PIPE UNDERDRAIN REMOVAL

NOTE: SEE DETAIL OF RESURFACING UNDERNEATH OVERHEAD STRUCTURES FOR MORE INFORMATION

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemory                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100,0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:44 PM | DATE -     | REVISED - |

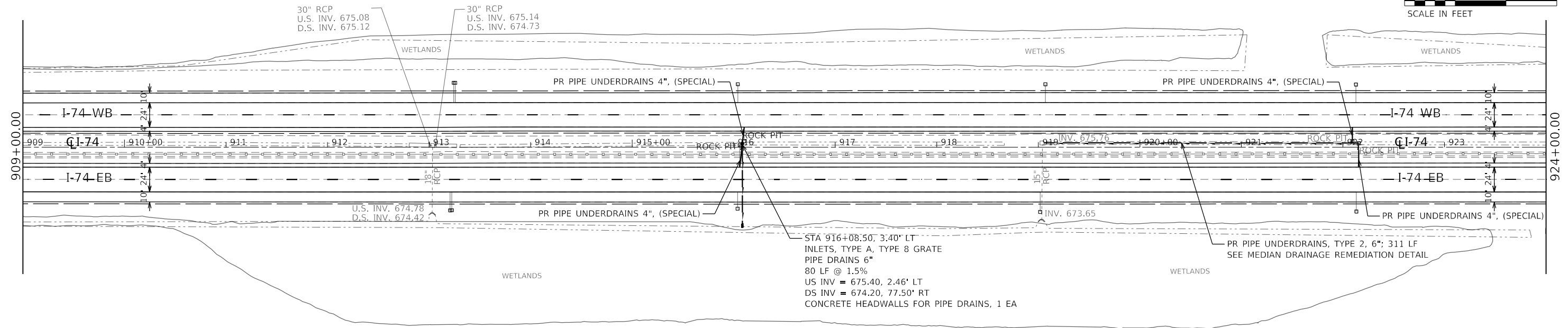
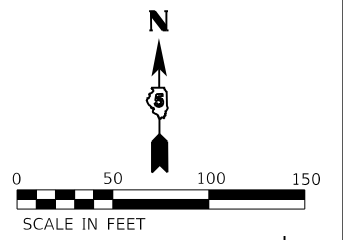
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                            |                   |
|----------------------------|-------------------|
| <b>I-74<br/>PLAN SHEET</b> |                   |
| SCALE: 1" = 50'            | SHEET OF SHEETS   |
| STA. 879+00.00             | TO STA. 909+00.00 |

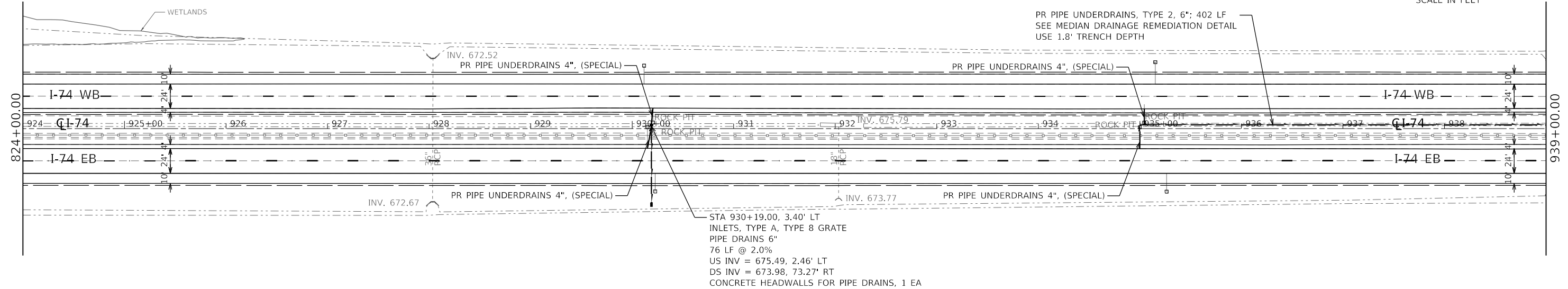
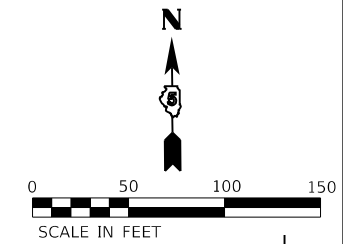
|                    |           |           |                           |              |
|--------------------|-----------|-----------|---------------------------|--------------|
| F.A.I. RTE. I-74   | SECTION * | COUNTY ** | TOTAL SHEETS 142          | SHEET NO. 49 |
| CONTRACT NO. 70790 |           |           | ILLINOIS FED. AID PROJECT |              |

MODEL: Default  
FILE: \\s:\m\15707905-shr-2019-07-14.dwg

S7, T19N, R14W



S7, T19N, R14W



MODEL: Default  
FILE: h:\m\19-03\201905\ch-camp\i74.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:44 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

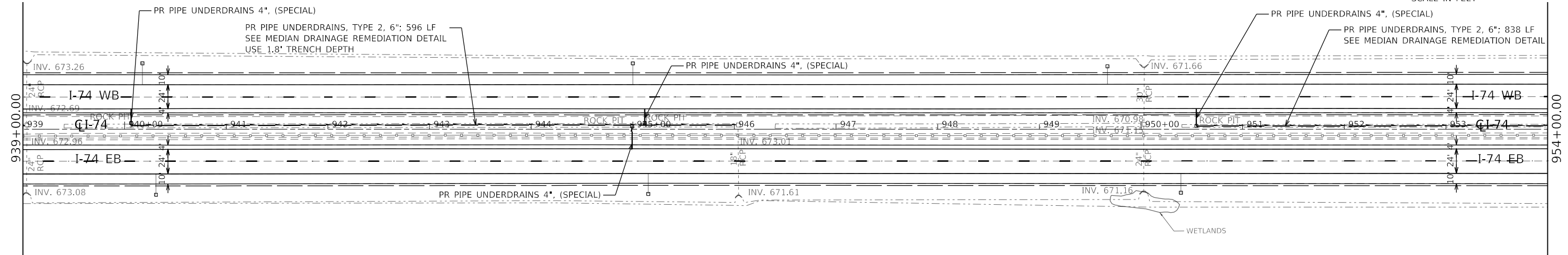
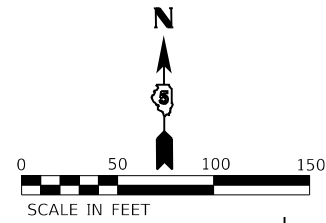
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                 |  |                   |  |                    |  |
|-----------------|--|-------------------|--|--------------------|--|
| SCALE: 1" = 50' |  | SHEET OF SHEETS   |  | I-74               |  |
|                 |  |                   |  | PLAN SHEET         |  |
| STA. 909+00.00  |  | TO STA. 939+00.00 |  | CONTRACT NO. 70790 |  |

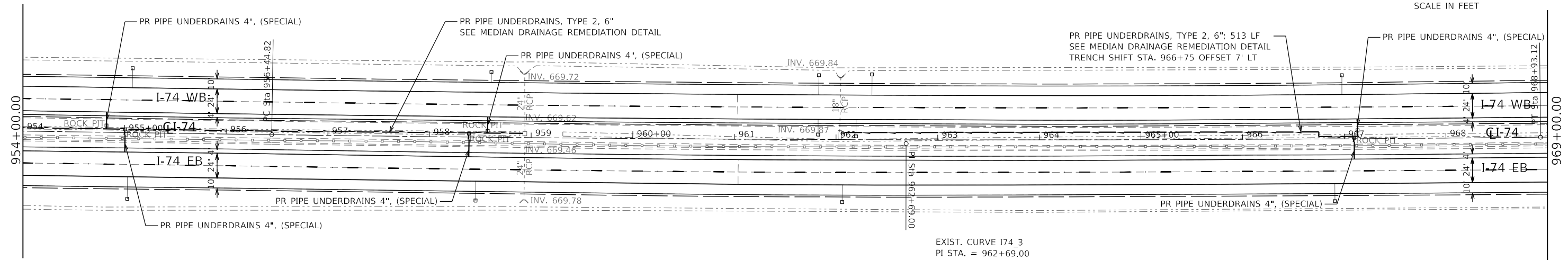
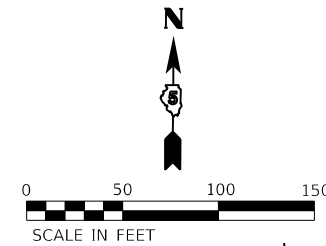
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|-------------|---------|--------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74        | *       | **     | 142          | 50        |

S7, T19N, R14W

S8, T19N, R14W



S8, T19N, R14W



EXIST. CURVE I74\_3  
 PI STA. = 962+69.00  
 $\Delta = 1^\circ 23' 10''$  (LT)  
 $D = 0^\circ 06' 40''$   
 $R = 51,599.32'$   
 $T = 624.18'$   
 $L = 1,248.30'$   
 $E = 3.78'$   
 $e = \text{---}$   
 $T.R. = \text{---}$   
 $S.E. \text{ RUN} = \text{---}$   
 $P.C. \text{ STA.} = 956+44.82$   
 $P.T. \text{ STA.} = 968+93.12$

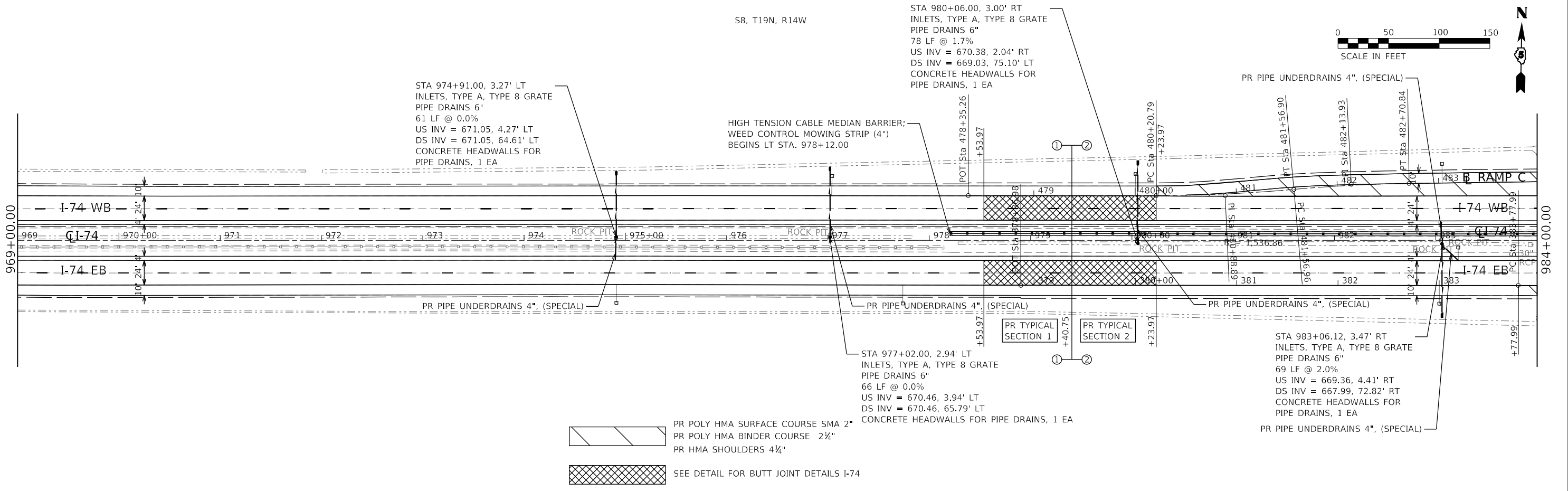
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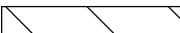



|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:45 PM | DATE -     | REVISED - |

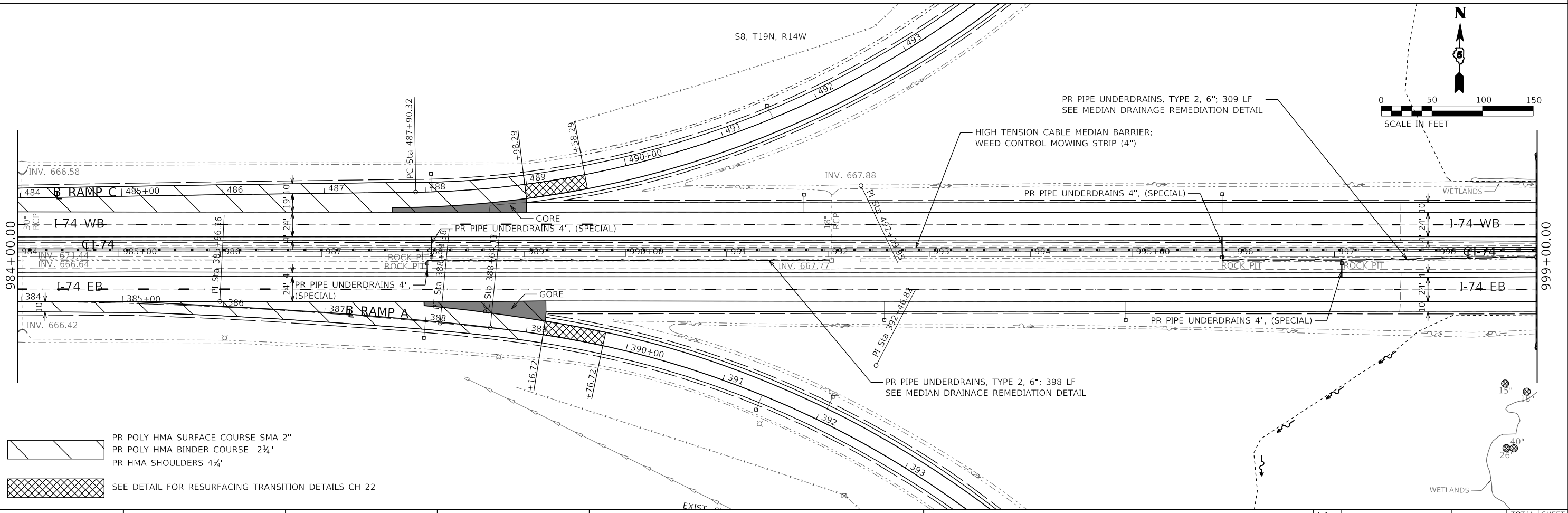
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

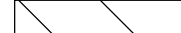



|                            |                 |                |                   |        |              |           |
|----------------------------|-----------------|----------------|-------------------|--------|--------------|-----------|
| <b>I-74<br/>PLAN SHEET</b> |                 | F.A.I. RTE.    | SECTION           | COUNTY | TOTAL SHEETS | SHEET NO. |
| SCALE: 1" = 50'            | SHEET OF SHEETS | STA. 939+00.00 | TO STA. 969+00.00 | **     | 142          | 51        |

|                           |  |                    |  |
|---------------------------|--|--------------------|--|
| ILLINOIS FED. AID PROJECT |  | CONTRACT NO. 70790 |  |
|---------------------------|--|--------------------|--|



-  PR POLY HMA SURFACE COURSE SMA 2"
-  PR POLY HMA BINDER COURSE 2 1/4"
-  PR HMA SHOULDERS 4 1/2"
-  SEE DETAIL FOR BUTT JOINT DETAILS I-74



-  PR POLY HMA SURFACE COURSE SMA 2"
-  PR POLY HMA BINDER COURSE 2 1/4"
-  PR HMA SHOULDERS 4 1/2"
-  SEE DETAIL FOR RESURFACING TRANSITION DETAILS CH 22

MODEL: Default  
FILE: Model5\_0327201905-ch22-rs-2-92-10-rs-3.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:46 PM | DATE -     | REVISED - |

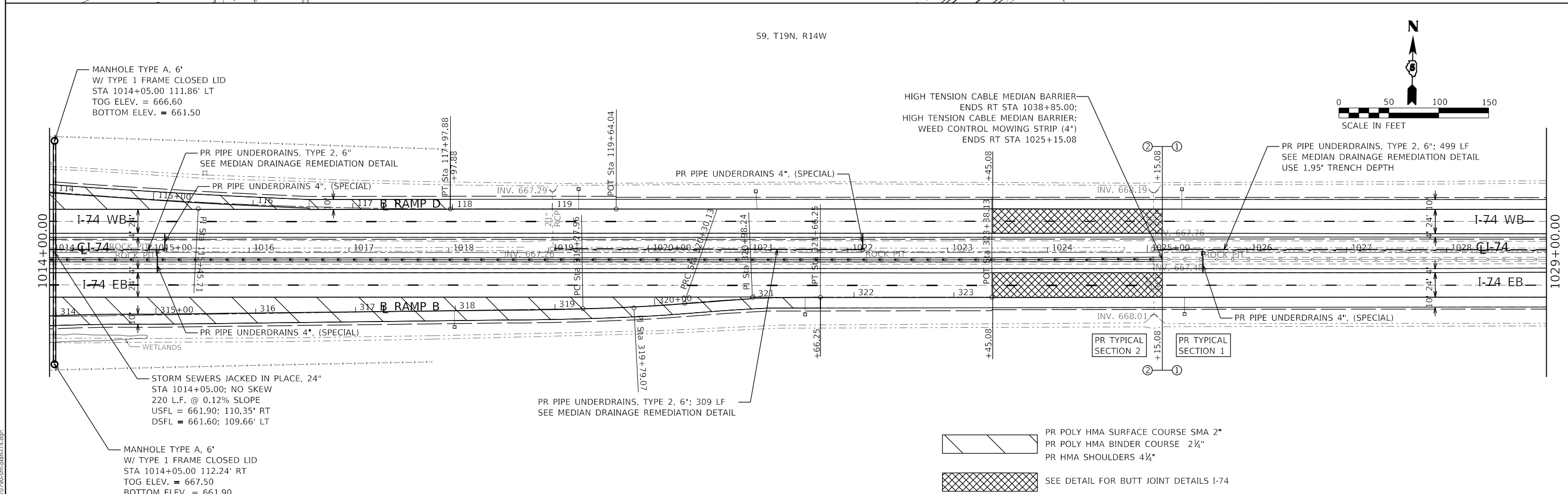
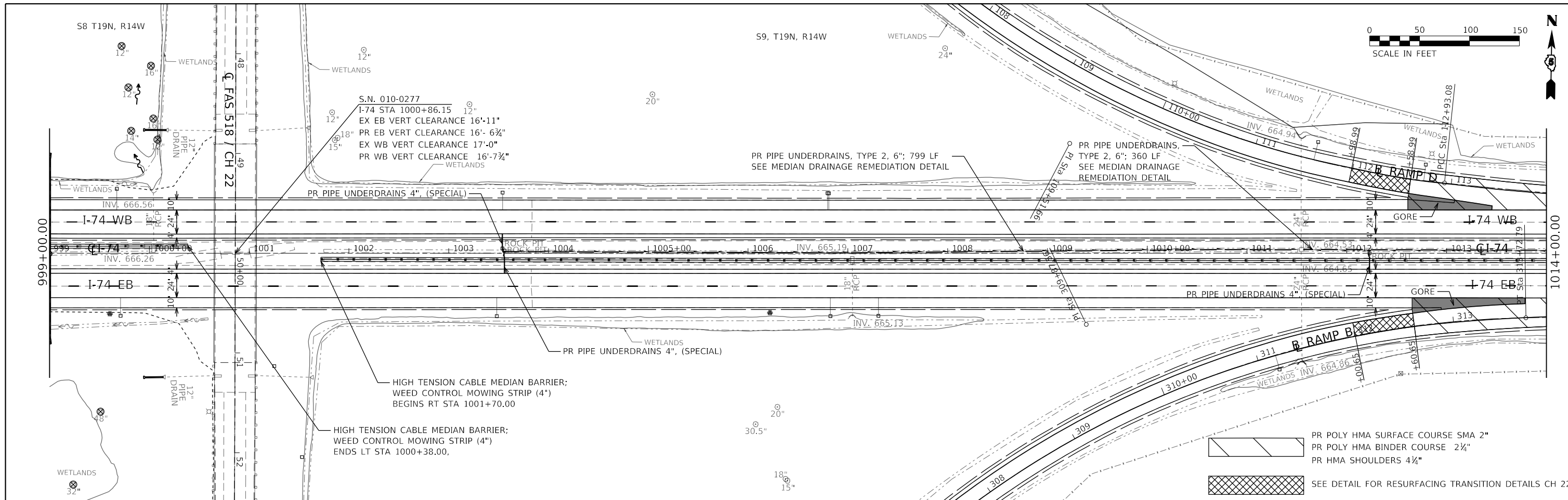
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                            |                   |
|----------------------------|-------------------|
| <b>I-74<br/>PLAN SHEET</b> |                   |
| SCALE: 1" = 50'            | SHEET OF SHEETS   |
| STA. 969+00.00             | TO STA. 999+00.00 |

|                           |           |           |                  |              |
|---------------------------|-----------|-----------|------------------|--------------|
| F.A.I. RTE. I-74          | SECTION * | COUNTY ** | TOTAL SHEETS 142 | SHEET NO. 52 |
| CONTRACT NO. 70790        |           |           |                  |              |
| ILLINOIS FED. AID PROJECT |           |           |                  |              |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION





MODEL: Default  
 FILE: M:\1029+00.00\1029+00.00\1029+00.00.dwg

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemory                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:46 PM | DATE -     | REVISED - |

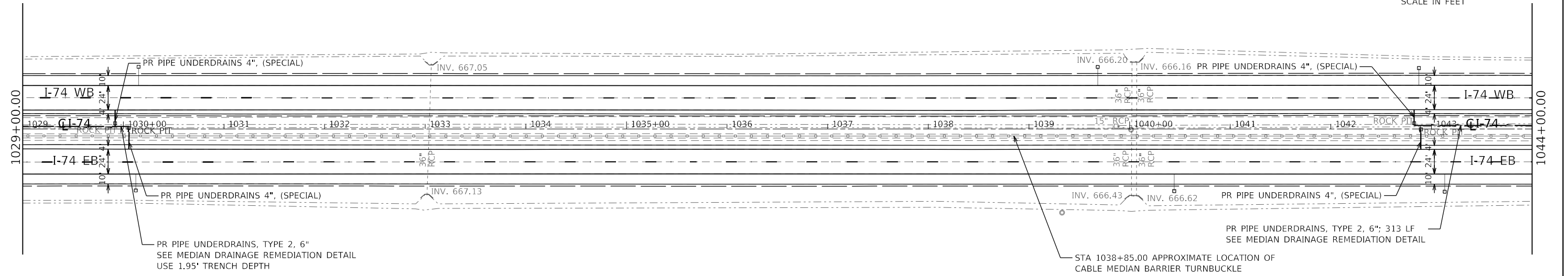
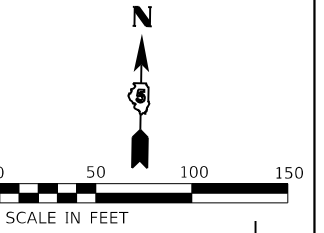
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

|                                  |  |                 |  |                |                    |
|----------------------------------|--|-----------------|--|----------------|--------------------|
| SCALE: 1" = 50'                  |  | SHEET OF SHEETS |  | STA. 999+00.00 | TO STA. 1029+00.00 |
| <b>I-74</b><br><b>PLAN SHEET</b> |  |                 |  |                |                    |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 53        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

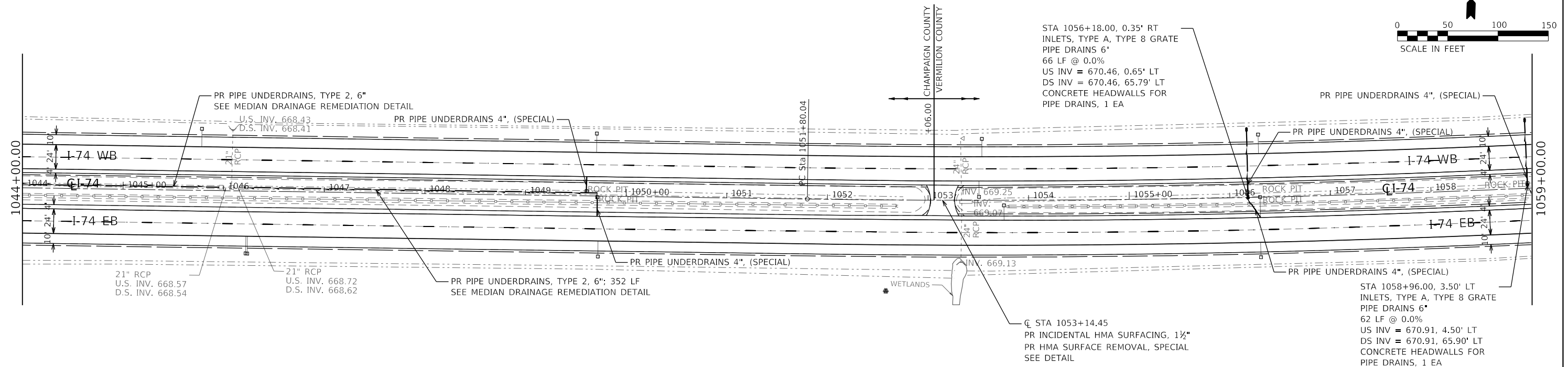
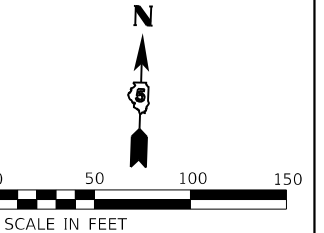
(10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION

S9, T19N, R14W



S9, T19N, R14W

S10, T19N, R14W



MODEL: Default  
FILE: Model5\_1029+00.00-1059+00.00.dwg

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:47 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                            |                    |
|----------------------------|--------------------|
| <b>I-74<br/>PLAN SHEET</b> |                    |
| SCALE: 1" = 50'            | SHEET OF SHEETS    |
| STA. 1029+00.00            | TO STA. 1059+00.00 |

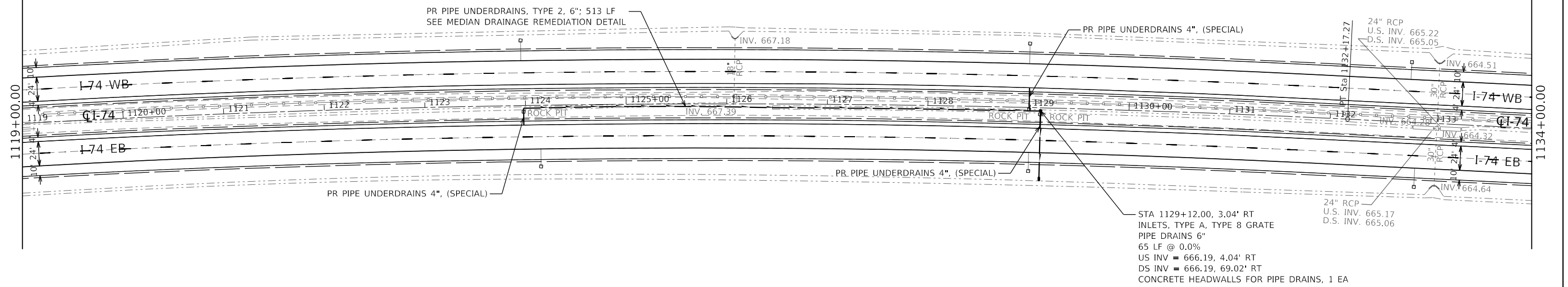
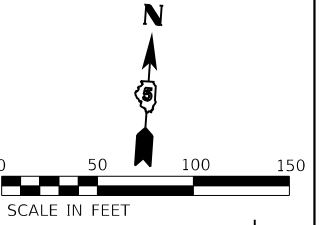
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 54        |
| CONTRACT NO. 70790 |         |        |              |           |

ILLINOIS FED. AID PROJECT \* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

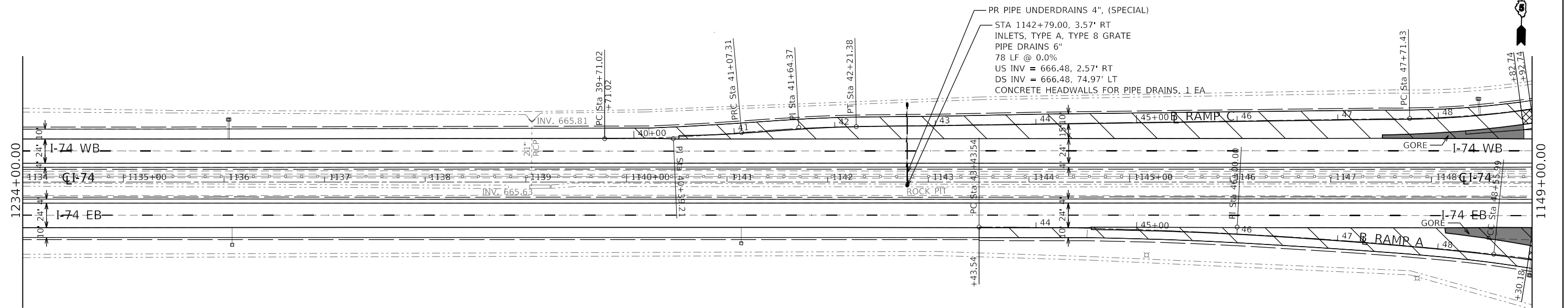
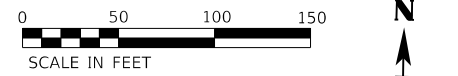




S11, T19N, R14W



S11, T19N, R14W



- PR HMA SURFACE REMOVAL 2"
- PR POLY HMA SURFACE COURSE SMA 2"
- PR HMA SHOULDERS 2"
- SEE DETAIL FOR RESURFACING TRANSITION DETAILS IL 49

MODEL: Default  
FILE: h:\m\19-03\20190505-shc-2019-07-14.dwg

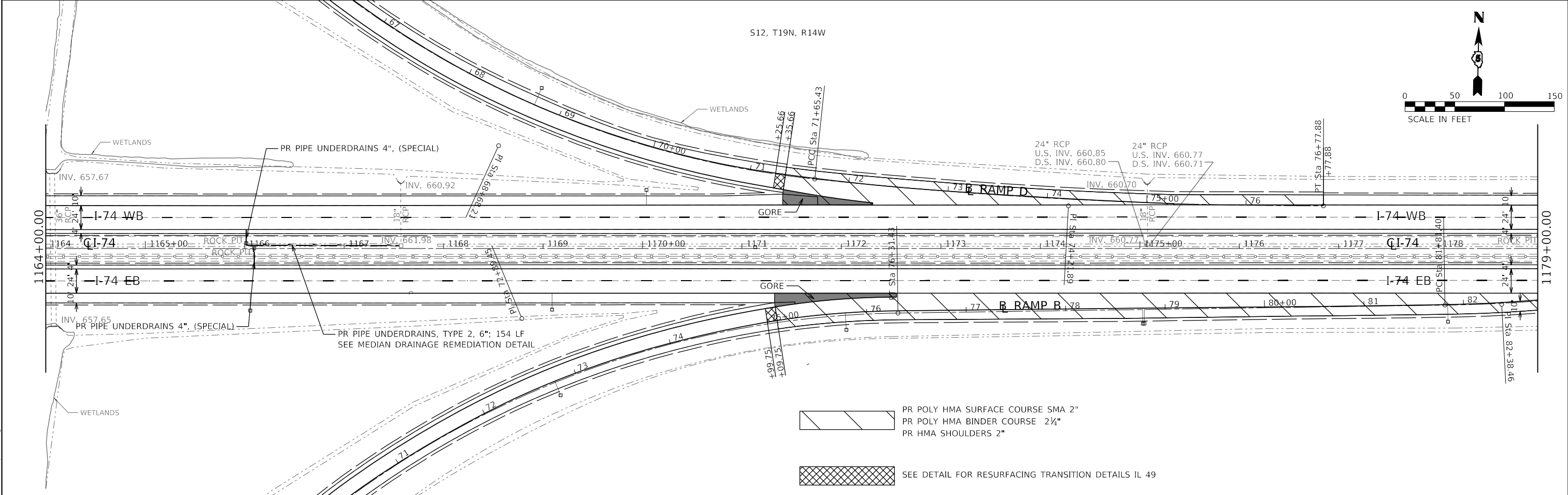
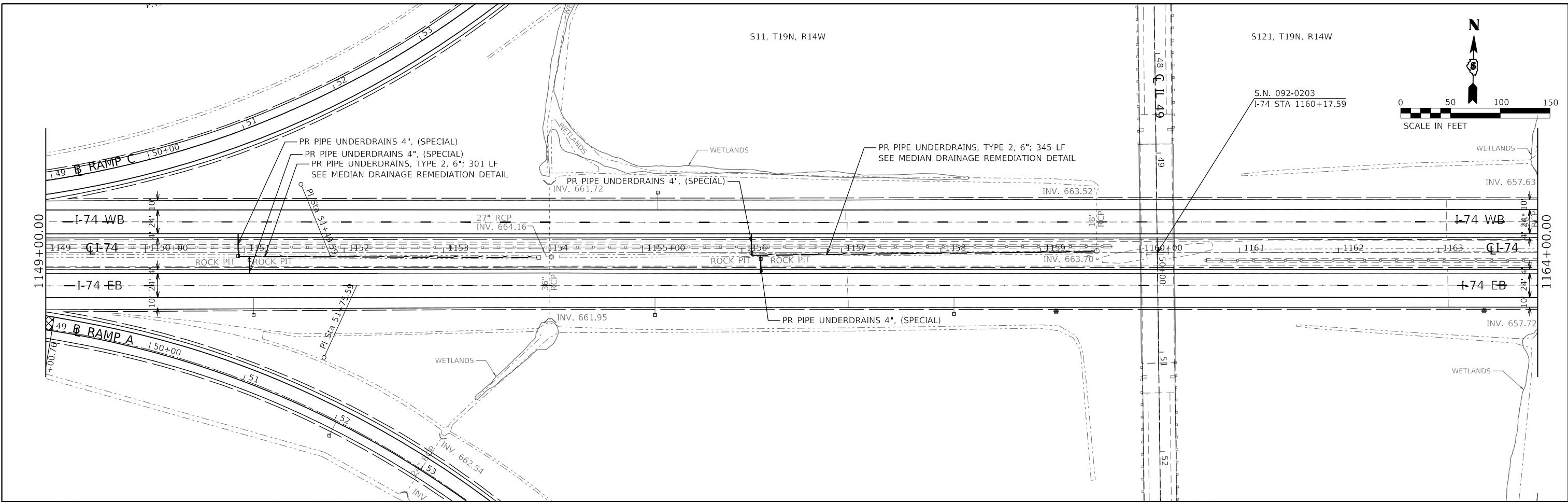
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:49 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                            |                    |
|----------------------------|--------------------|
| <b>I-74<br/>PLAN SHEET</b> |                    |
| SCALE: 1" = 50'            | SHEET OF SHEETS    |
| STA. 1119+00.00            | TO STA. 1149+00.00 |

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 57        |
| CONTRACT NO. 70790 |         |        |              |           |

ILLINOIS FED. AID PROJECT  
\* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION



- PR POLY HMA SURFACE COURSE SMA 2"
- PR POLY HMA BINDER COURSE 2 1/4"
- PR HMA SHOULDERS 2"
- SEE DETAIL FOR RESURFACING TRANSITION DETAILS IL 49

MODEL: Default  
 FILE: Model1...03201905-shr-abbr-174.dgn

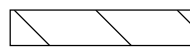
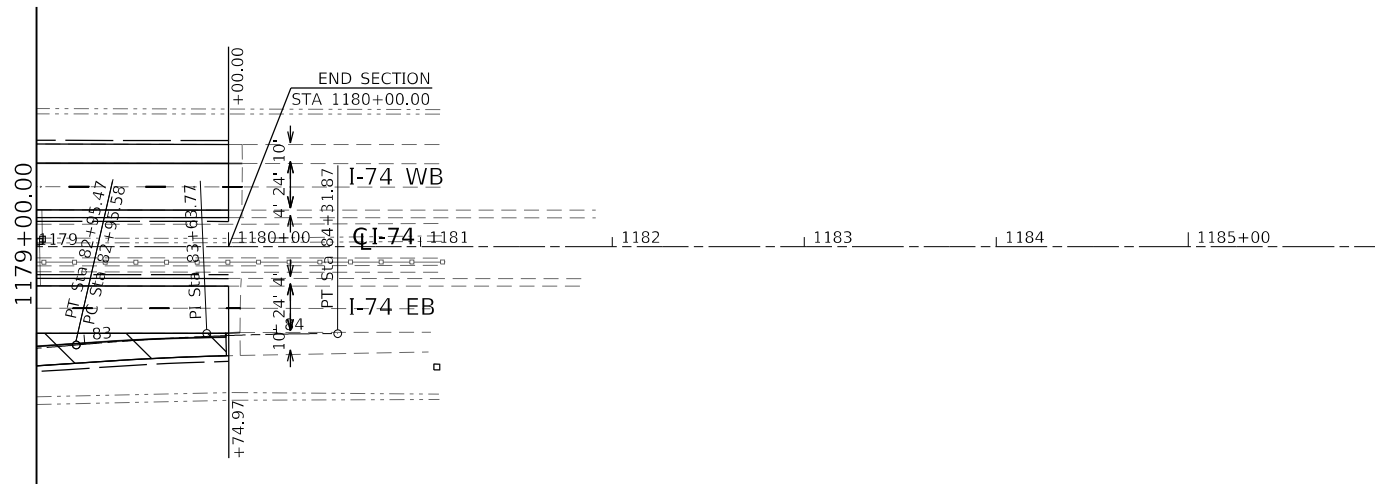
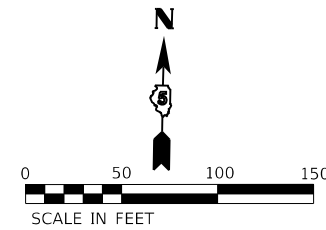
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:50 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

|                   |                 |                 |                    |                    |              |           |
|-------------------|-----------------|-----------------|--------------------|--------------------|--------------|-----------|
| <b>I-74</b>       |                 | F.A.I. RTE.     | SECTION            | COUNTY             | TOTAL SHEETS | SHEET NO. |
| <b>PLAN SHEET</b> |                 | I-74            | *                  | **                 | 142          | 58        |
| SCALE: 1" = 50'   | SHEET OF SHEETS | STA. 1149+00.00 | TO STA. 1179+00.00 | CONTRACT NO. 70790 |              |           |

|          |                  |                                |                          |
|----------|------------------|--------------------------------|--------------------------|
| ILLINOIS | FED. AID PROJECT | (10-7.8;92-8.9)RS-2(92-10)RS-3 | ** CHAMPAIGN & VERMILION |
|----------|------------------|--------------------------------|--------------------------|

S12, T19N, R14W



PR POLY HMA SURFACE COURSE SMA 2"  
 PR POLY HMA BINDER COURSE 2 1/2"  
 PR HMA SHOULDERS

MODEL: Default  
 FILE: Model5...030570790-shc-ahb-174.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 3:59:51 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

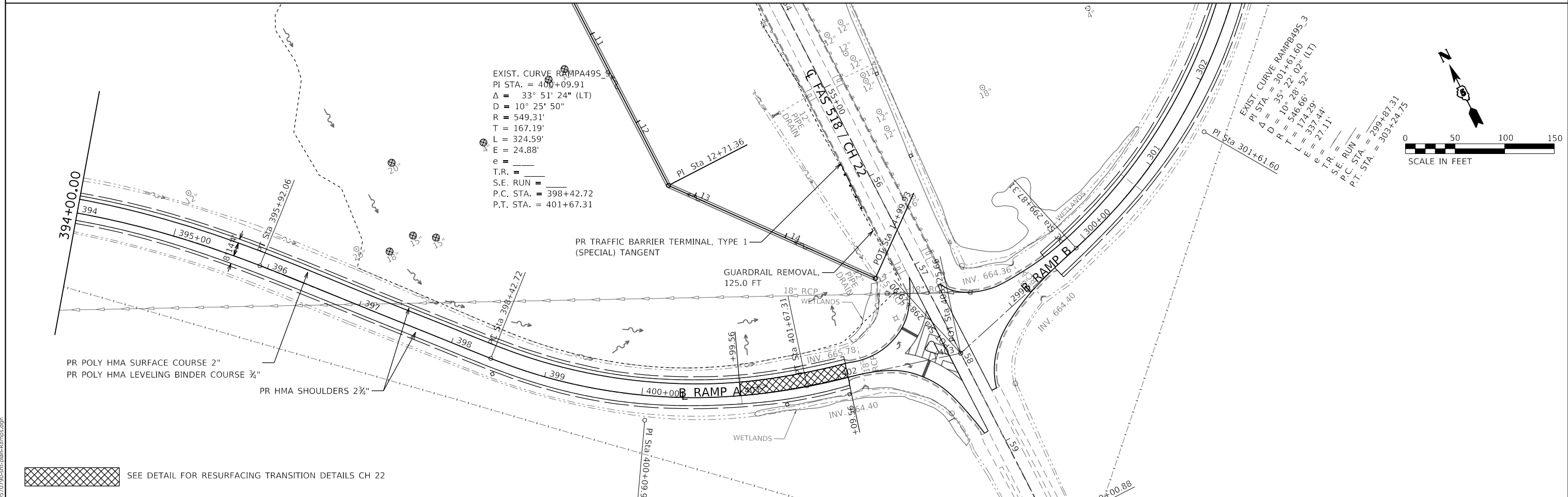
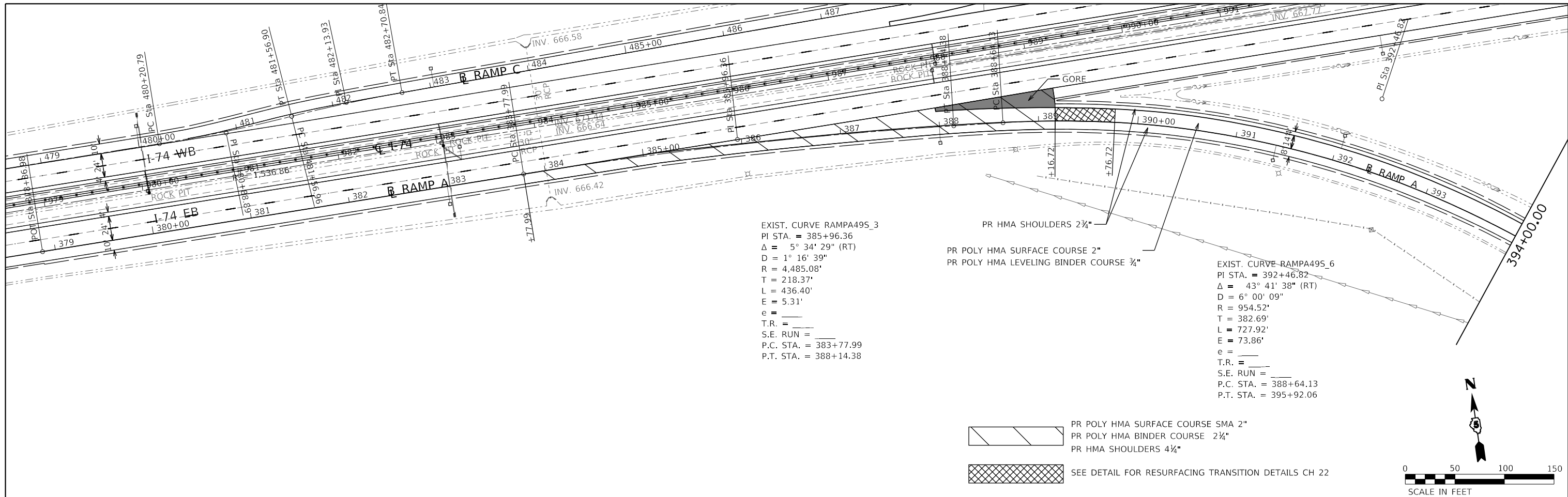
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|                            |  |                |         |        |                 |                    |
|----------------------------|--|----------------|---------|--------|-----------------|--------------------|
| <b>I-74<br/>PLAN SHEET</b> |  | F.A.I.<br>RTE. | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO.       |
|                            |  | I-74           | *       | **     | 142             | 59                 |
| SCALE: 1" = 50'            |  | SHEET          | OF      | SHEETS | STA. 1179+00.00 | TO STA. 1185+00.00 |

|                    |                  |
|--------------------|------------------|
| CONTRACT NO. 70790 |                  |
| ILLINOIS           | FED. AID PROJECT |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



MODEL: Default  
 FILE: N:\mfr\_03\2019\70790-8\ch-abn-ramps.dgn

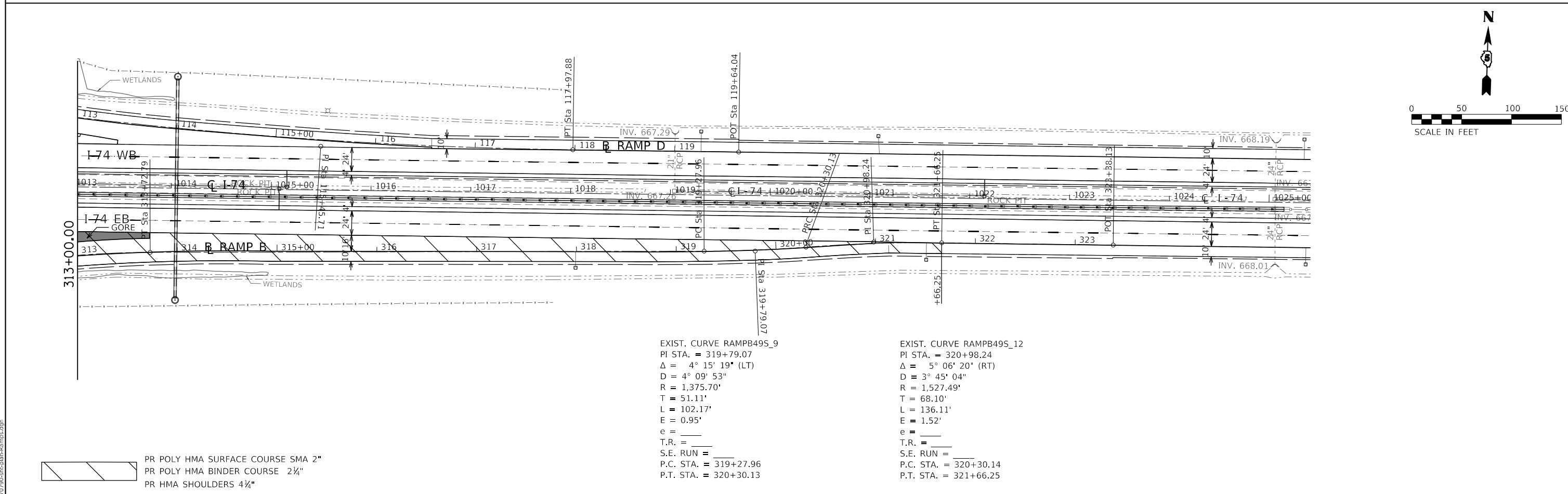
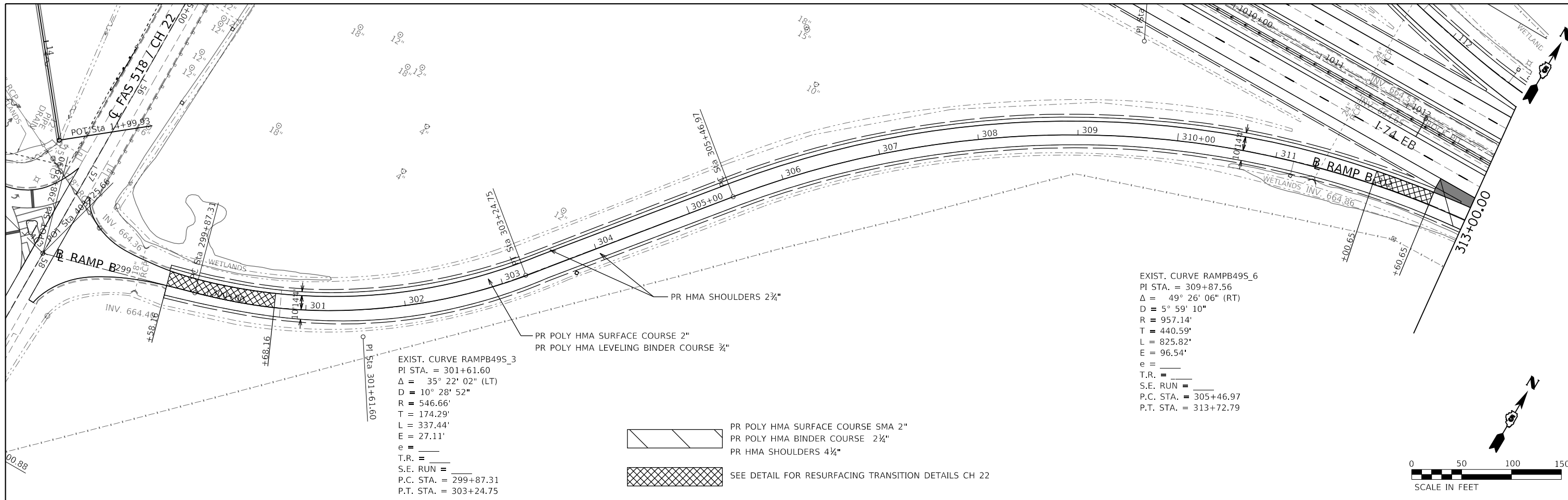
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:01:21 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|  |                   |
|--|-------------------|
| <b>I-74 / CH 22 RAMP A<br/>   PLAN SHEET</b> |                   |
| SCALE: 1" = 50'                              | SHEET OF SHEETS   |
| STA. 378+86.98                               | TO STA. 403+25.66 |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 60        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |





MODEL: Default  
 FILE: \\miller\...0570790-sh-rampb-rampas.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:01:22 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

|                     |                   |
|---------------------|-------------------|
| I-74 / CH 22 RAMP B |                   |
| PLAN SHEET          |                   |
| SCALE: 1" = 50'     | SHEET OF SHEETS   |
| STA. 298+29.90      | TO STA. 323+38.13 |

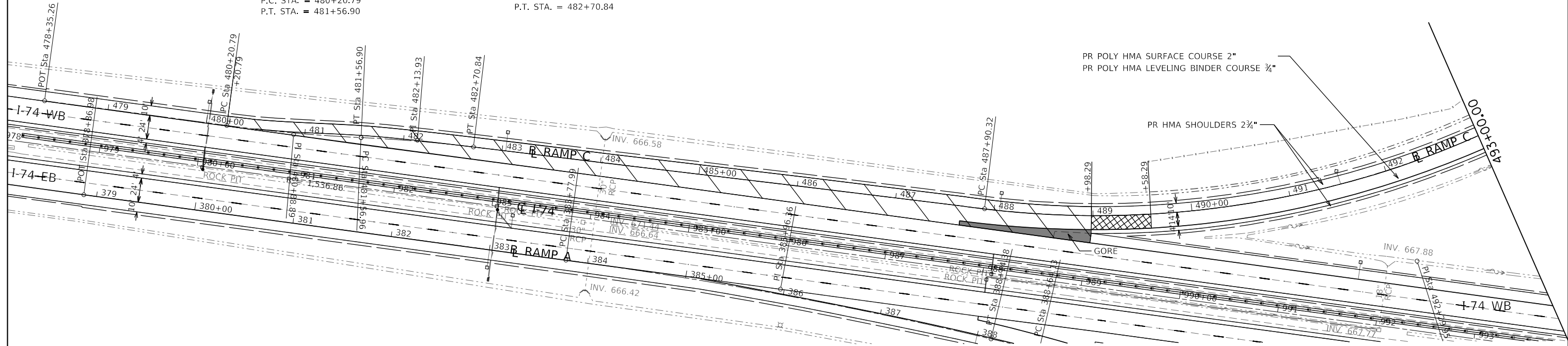
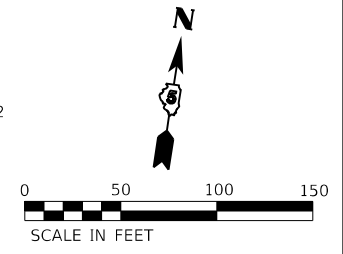
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------|--------|--------------|-----------|
| I-74                      | *       | **     | 142          | 61        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

EXIST. CURVE RAMPC49S\_3  
 PI STA. = 480+88.89  
 $\Delta = 5^\circ 04' 28''$  (LT)  
 $D = 3^\circ 43' 41''$   
 T = 68.10'  
 L = 136.11'  
 E = 1.51'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 480+20.79  
 P.T. STA. = 481+56.90

EXIST. CURVE RAMPC49S\_6  
 PI STA. = 482+13.93  
 $\Delta = 4^\circ 15' 33''$  (RT)  
 $D = 3^\circ 44' 23''$   
 R = 1,532.06'  
 T = 56.97'  
 L = 113.89'  
 E = 1.06'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 481+56.96  
 P.T. STA. = 482+70.84

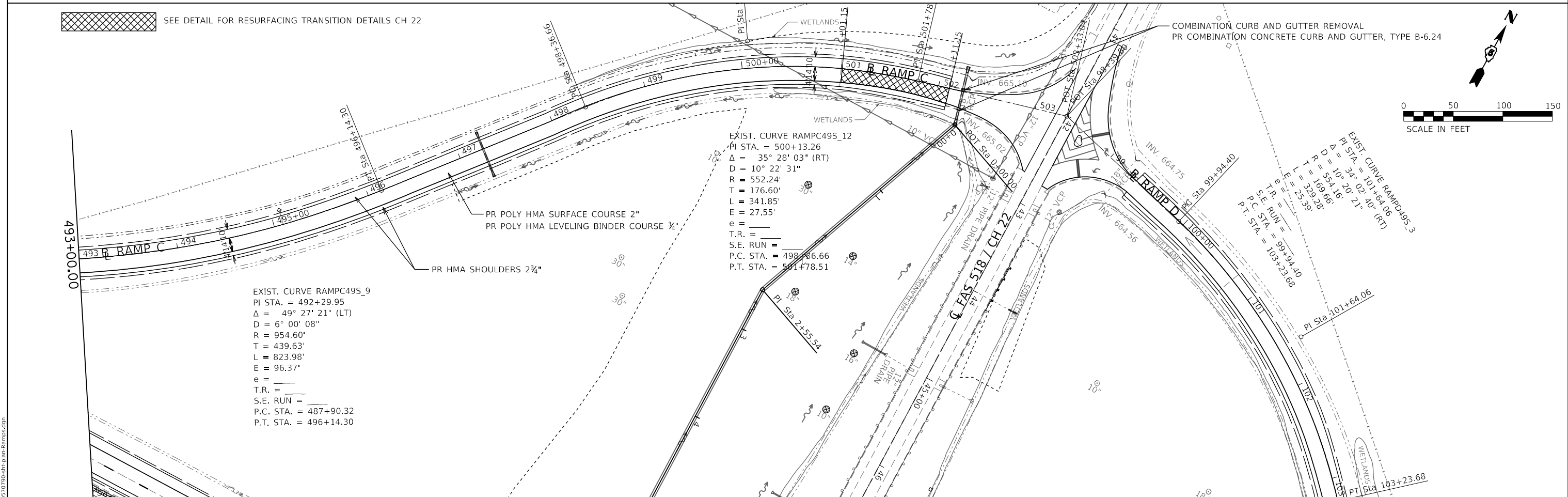
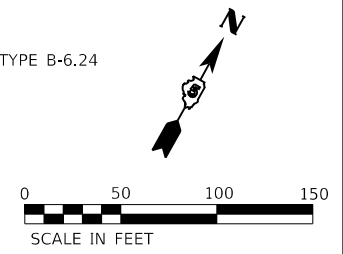
PR POLY HMA SURFACE COURSE SMA 2"  
 PR POLY HMA BINDER COURSE 2 1/4"  
 PR HMA SHOULDERS 4 1/4"  
 SEE DETAIL FOR RESURFACING TRANSITION DETAILS CH 22



SEE DETAIL FOR RESURFACING TRANSITION DETAILS CH 22

EXIST. CURVE RAMPC49S\_12  
 PI STA. = 500+13.26  
 $\Delta = 35^\circ 28' 03''$  (RT)  
 $D = 10^\circ 22' 31''$   
 R = 552.24'  
 T = 176.60'  
 L = 341.85'  
 E = 27.55'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 498+86.66  
 P.T. STA. = 501+78.51

COMBINATION CURB AND GUTTER REMOVAL  
 PR COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24



EXIST. CURVE RAMPC49S\_9  
 PI STA. = 492+29.95  
 $\Delta = 49^\circ 27' 21''$  (LT)  
 $D = 6^\circ 00' 08''$   
 R = 954.60'  
 T = 439.63'  
 L = 823.98'  
 E = 96.37'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 487+90.32  
 P.T. STA. = 496+14.30

EXIST. CURVE RAMPC49S\_3  
 PI STA. = 101+64.06  
 $\Delta = 34^\circ 02' 40''$  (RT)  
 $D = 10^\circ 20' 21''$   
 R = 554.16'  
 T = 329.28'  
 L = 169.66'  
 E = 25.39'  
 e = \_\_\_\_\_  
 T.R. = \_\_\_\_\_  
 S.E. RUN = \_\_\_\_\_  
 P.C. STA. = 99+94.40  
 P.T. STA. = 103+23.68

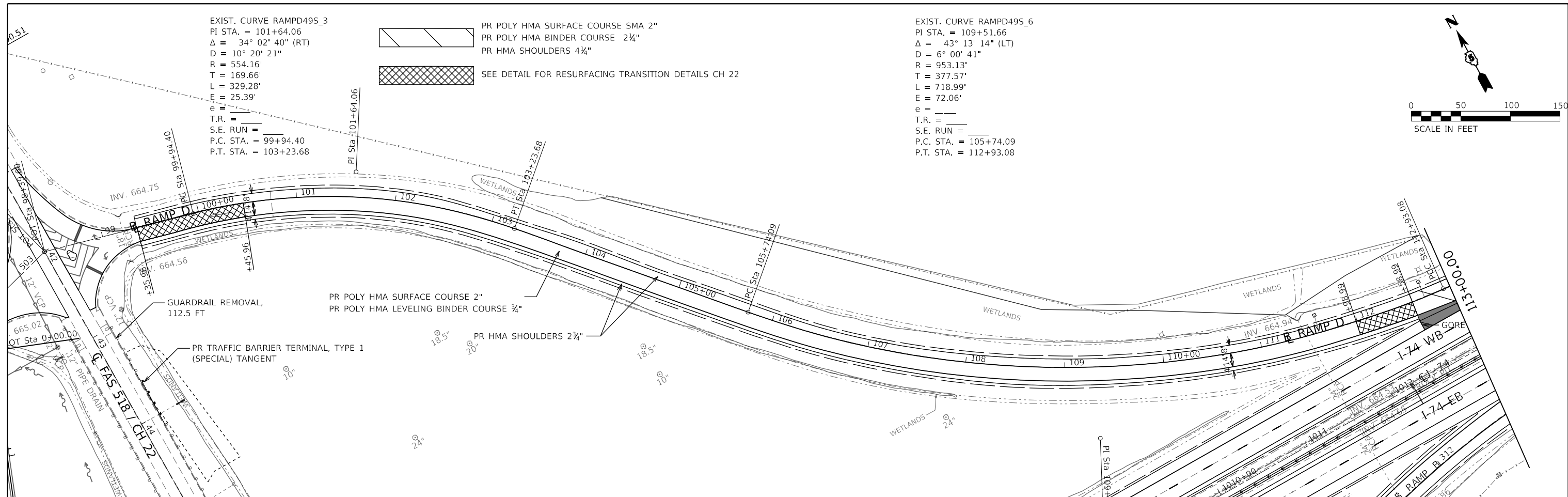
MODEL: Default  
 FILE: I-74-CH 22-Ramp-3.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:01:23 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

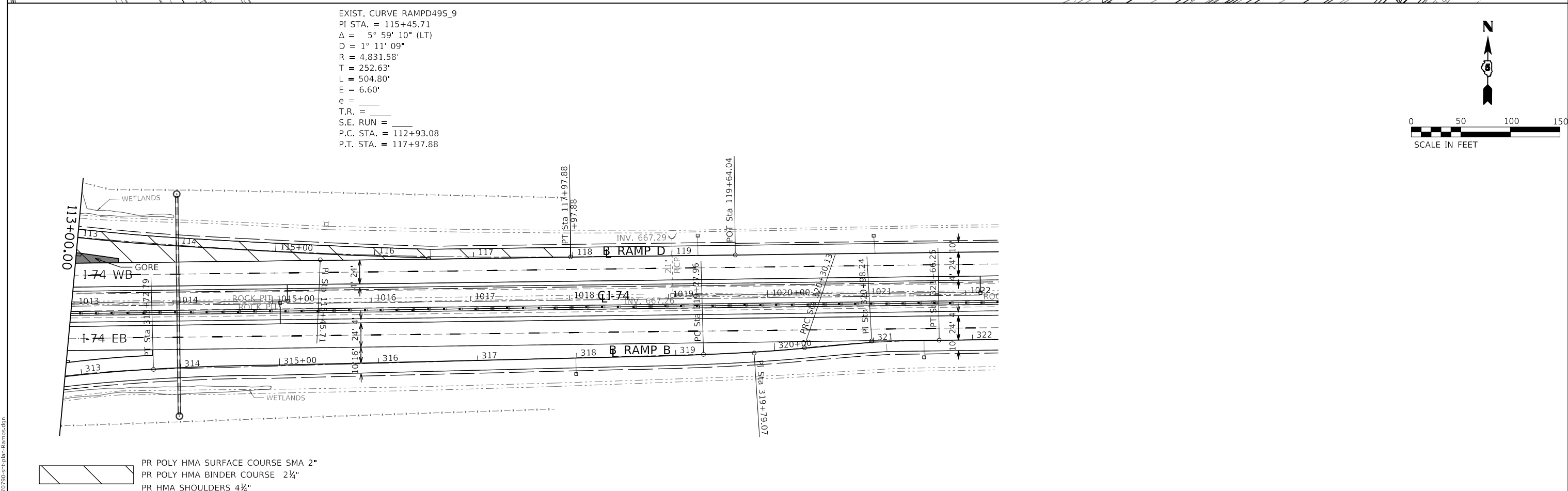
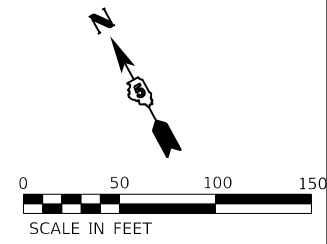
|   |  |
|---|--|
| <b>I-74 / CH 22 RAMP C<br/>PLAN SHEET</b> |  |
| SCALE: 1" = 50'                           | SHEET OF SHEETS STA. 478+35.26 TO STA. 503+33.04 |

|                    |             |             |                    |                |
|--------------------|-------------|-------------|--------------------|----------------|
| F.A.I. RTE. = I-74 | SECTION = * | COUNTY = ** | TOTAL SHEETS = 142 | SHEET NO. = 62 |
| CONTRACT NO. 70790 |             |             |                    |                |

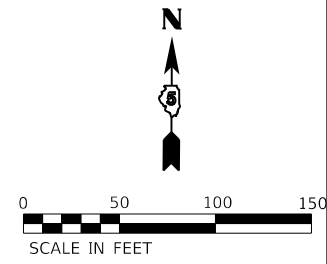


EXIST. CURVE RAMPD49S\_3  
 PI STA. = 101+64.06  
 $\Delta = 34^\circ 02' 40''$  (RT)  
 $D = 10^\circ 20' 21''$   
 $R = 554.16'$   
 $T = 169.66'$   
 $L = 329.28'$   
 $E = 25.39'$   
 $e = \_\_\_\_\_\_$   
 $T.R. = \_\_\_\_\_\_$   
 $S.E. RUN = \_\_\_\_\_\_$   
 $P.C. STA. = 99+94.40$   
 $P.T. STA. = 103+23.68$

EXIST. CURVE RAMPD49S\_6  
 PI STA. = 109+51.66  
 $\Delta = 43^\circ 13' 14''$  (LT)  
 $D = 6^\circ 00' 41''$   
 $R = 953.13'$   
 $T = 377.57'$   
 $L = 718.99'$   
 $E = 72.06'$   
 $e = \_\_\_\_\_\_$   
 $T.R. = \_\_\_\_\_\_$   
 $S.E. RUN = \_\_\_\_\_\_$   
 $P.C. STA. = 105+74.09$   
 $P.T. STA. = 112+93.08$



EXIST. CURVE RAMPD49S\_9  
 PI STA. = 115+45.71  
 $\Delta = 5^\circ 59' 10''$  (LT)  
 $D = 1^\circ 11' 09''$   
 $R = 4,831.58'$   
 $T = 252.63'$   
 $L = 504.80'$   
 $E = 6.60'$   
 $e = \_\_\_\_\_\_$   
 $T.R. = \_\_\_\_\_\_$   
 $S.E. RUN = \_\_\_\_\_\_$   
 $P.C. STA. = 112+93.08$   
 $P.T. STA. = 117+97.88$



PR POLY HMA SURFACE COURSE SMA 2"  
 PR POLY HMA BINDER COURSE 2 1/4"  
 PR HMA SHOULDERS 4 1/4"

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemory                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:01:24 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

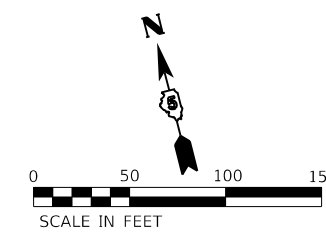
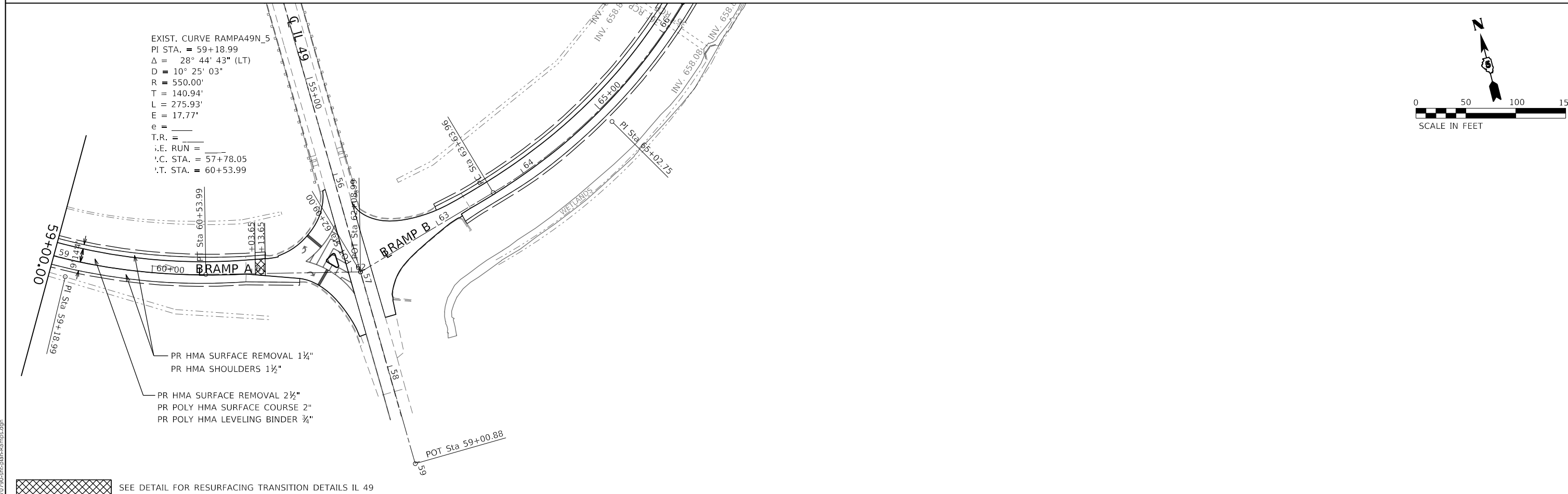
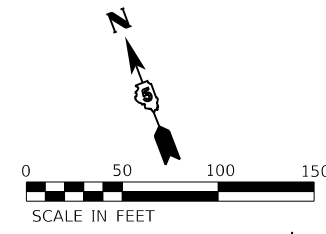
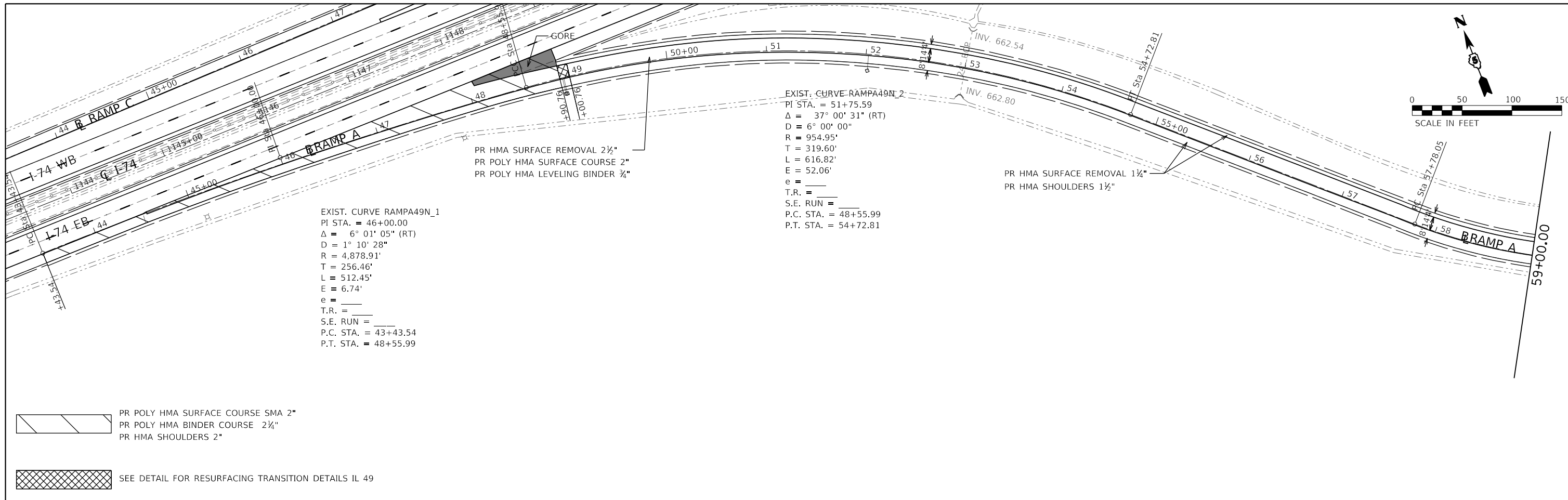
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**I-74 / CH 22 RAMP D  
 PLAN SHEET**

SCALE: 1" = 50' SHEET OF SHEETS STA. 98+39.60 TO STA. 119+64.04

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 63        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

MODEL: Default  
 FILE: \\bms\p\2019\98+39.60-Ch22-RampD.dgn



MODEL: Default  
 FILE: \\miller\0305707990\champs\champs\champs.dgn

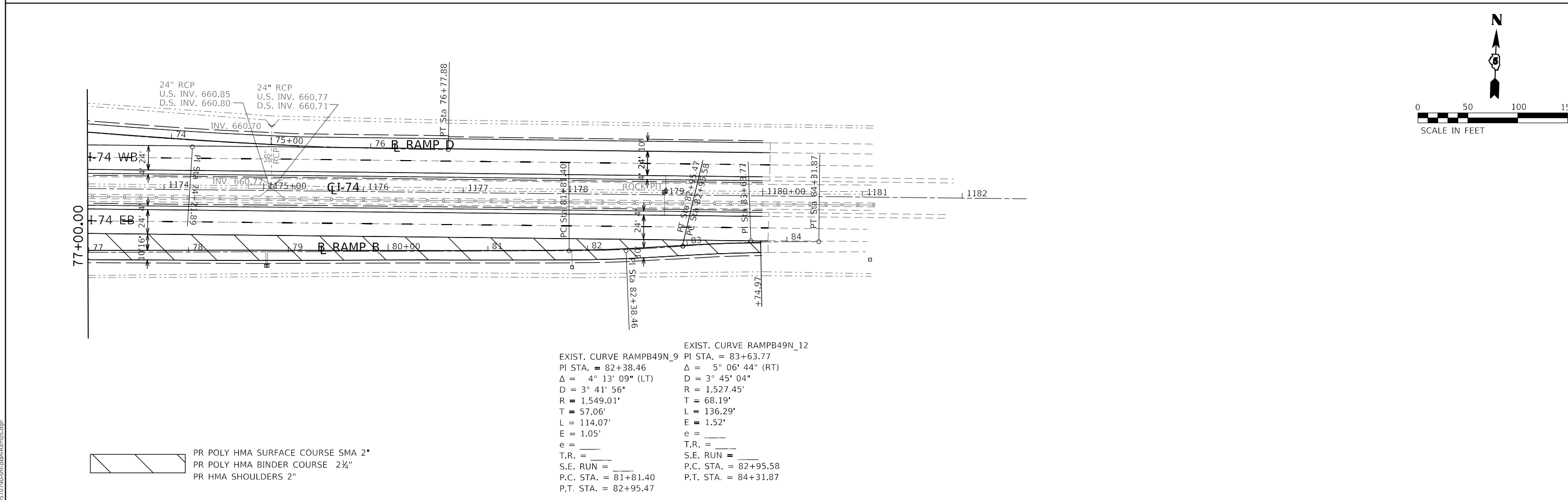
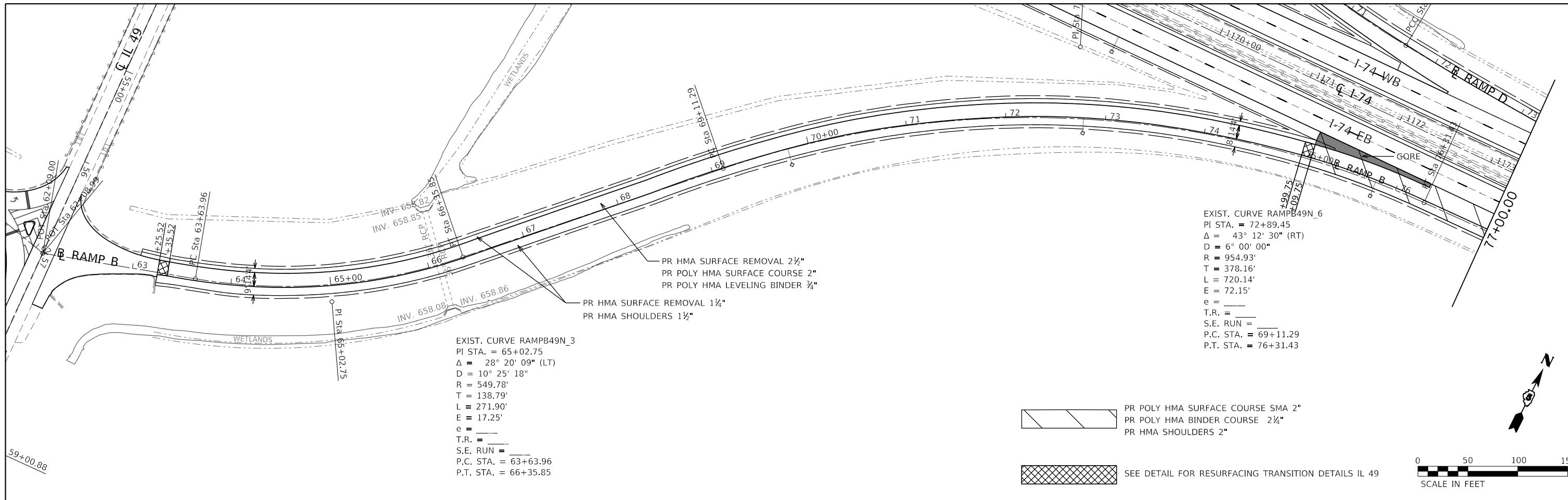
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:01:24 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|   |  |             |         |        |               |                  |
|---|--|-------------|---------|--------|---------------|------------------|
| <b>I-74 /IL 49 RAMP A<br/>   PLAN SHEET</b> |  | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS  | SHEET NO.        |
|   |  | I-74        | *       | **     | 142           | 64               |
| SCALE: 1" = 50'                             |  | SHEET       | OF      | SHEETS | STA. 43+43.54 | TO STA. 62+09.00 |

|          |                  |                    |
|----------|------------------|--------------------|
| ILLINOIS | FED. AID PROJECT | CONTRACT NO. 70790 |
|----------|------------------|--------------------|

\* (10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION



MODEL: Default  
 FILE: h:\m\15\_03\2019\09\shc\cham-rampb.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:01:25 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

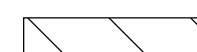
**I-74 / IL 49 RAMP B  
 PLAN SHEET**

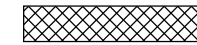
SCALE: 1" = 50' SHEET OF SHEETS STA. 62+08.99 TO STA. 84+31.87

| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------|--------|--------------|-----------|
| I-74                      | *       | **     | 142          | 65        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

EXIST. CURVE RAMPC49N\_1  
 PI STA. = 40+39.21  
 $\Delta = 5^{\circ} 06' 44''$  (LT)  
 $D = 3^{\circ} 45' 04''$   
 $R = 1,527.48'$   
 $T = 68.19'$   
 $L = 136.29'$   
 $E = 1.52'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 39+71.02  
 P.T. STA. = 41+07.31

EXIST. CURVE RAMPC49N\_2  
 PI STA. = 41+64.37  
 $\Delta = 4^{\circ} 16' 44''$  (RT)  
 $D = 3^{\circ} 45' 04''$   
 $R = 1,527.40'$   
 $T = 57.06'$   
 $L = 114.07'$   
 $E = 1.07'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 41+07.31  
 P.T. STA. = 42+21.38

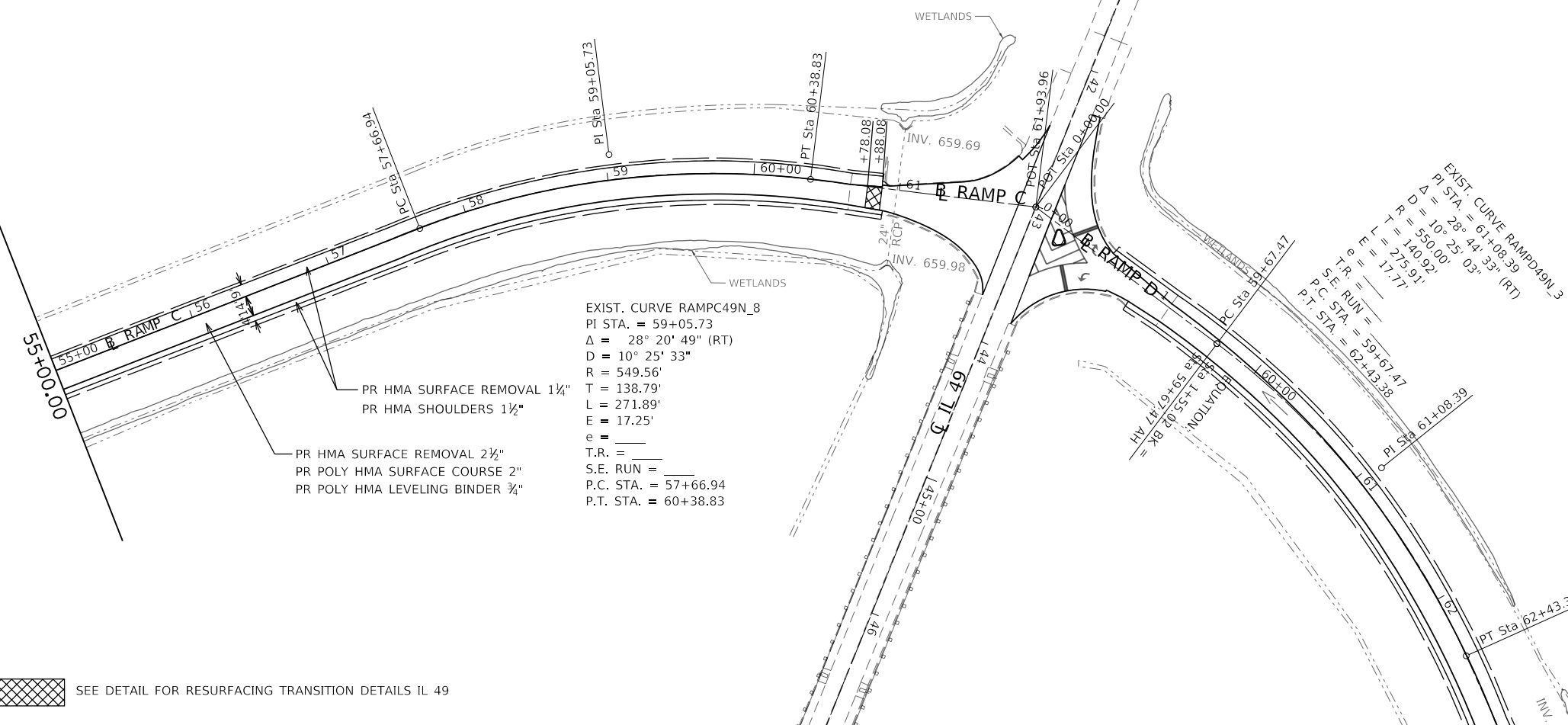
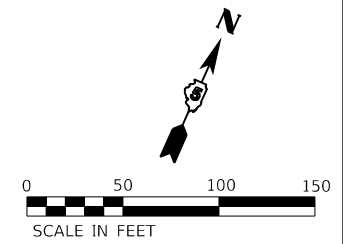
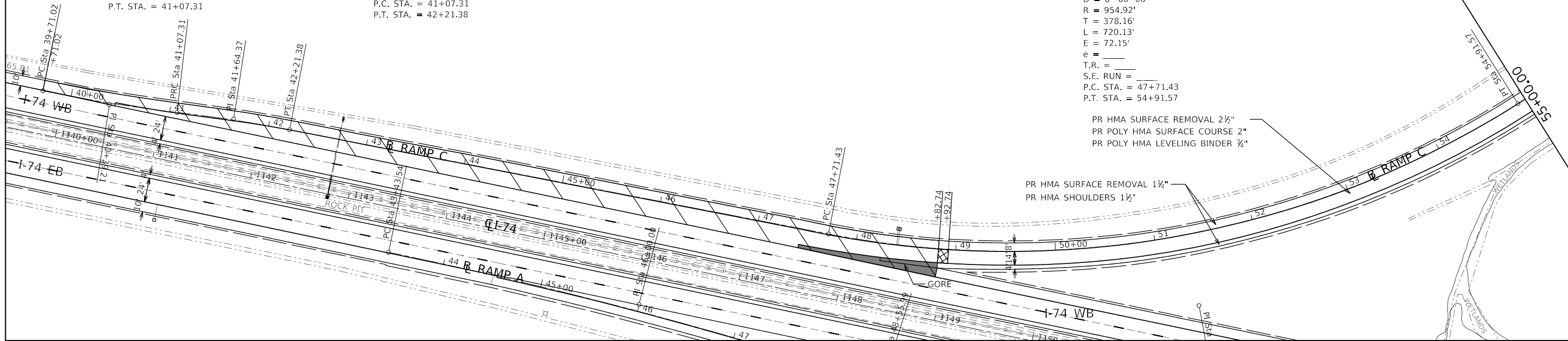
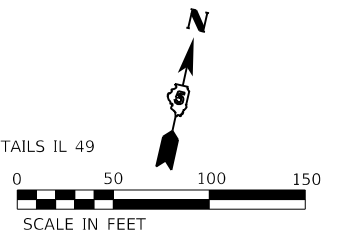
 PR POLY HMA SURFACE COURSE SMA 2"  
 PR POLY HMA BINDER COURSE 2"  
 PR HMA SHOULDERS 2"

 SEE DETAIL FOR RESURFACING TRANSITION DETAILS IL 49

EXIST. CURVE RAMPC49N\_5  
 PI STA. = 51+49.59  
 $\Delta = 43^{\circ} 12' 30''$  (LT)  
 $D = 6^{\circ} 00' 00''$   
 $R = 954.92'$   
 $T = 378.16'$   
 $L = 720.13'$   
 $E = 72.15'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 47+71.43  
 P.T. STA. = 54+91.57

PR HMA SURFACE REMOVAL 2 1/2"  
 PR POLY HMA SURFACE COURSE 2"  
 PR POLY HMA LEVELING BINDER 3/4"

PR HMA SURFACE REMOVAL 1 1/4"  
 PR HMA SHOULDERS 1 1/2"



EXIST. CURVE RAMPC49N\_8  
 PI STA. = 59+05.73  
 $\Delta = 28^{\circ} 20' 49''$  (RT)  
 $D = 10^{\circ} 25' 33''$   
 $R = 549.56'$   
 $T = 138.79'$   
 $L = 271.89'$   
 $E = 17.25'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 57+66.94  
 P.T. STA. = 60+38.83

EXIST. CURVE RAMPC49N\_3  
 PI STA. = 61+08.29  
 $\Delta = 28^{\circ} 44' 33''$  (RT)  
 $D = 10^{\circ} 25' 03''$   
 $R = 550.00'$   
 $T = 140.92'$   
 $L = 275.91'$   
 $E = 17.77'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 59+67.47  
 P.T. STA. = 62+43.38

PR HMA SURFACE REMOVAL 1 1/4"  
 PR HMA SHOULDERS 1 1/2"

PR HMA SURFACE REMOVAL 2 1/2"  
 PR POLY HMA SURFACE COURSE 2"  
 PR POLY HMA LEVELING BINDER 3/4"

 SEE DETAIL FOR RESURFACING TRANSITION DETAILS IL 49

MODEL: Default  
 FILE: \\mtr\p0570790\ch\c49a\I-74-Ramps.dgn

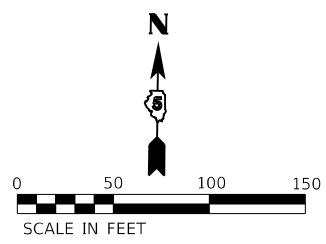
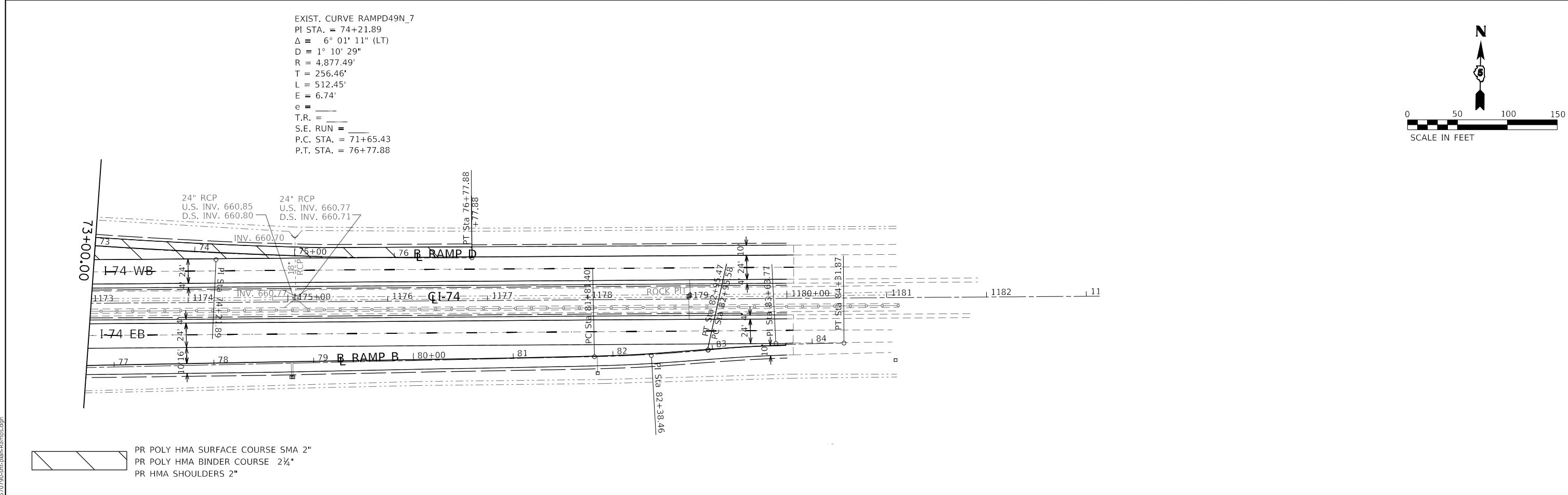
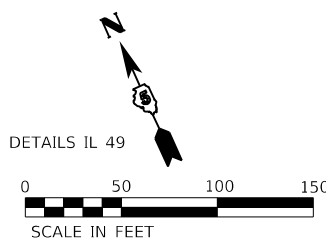
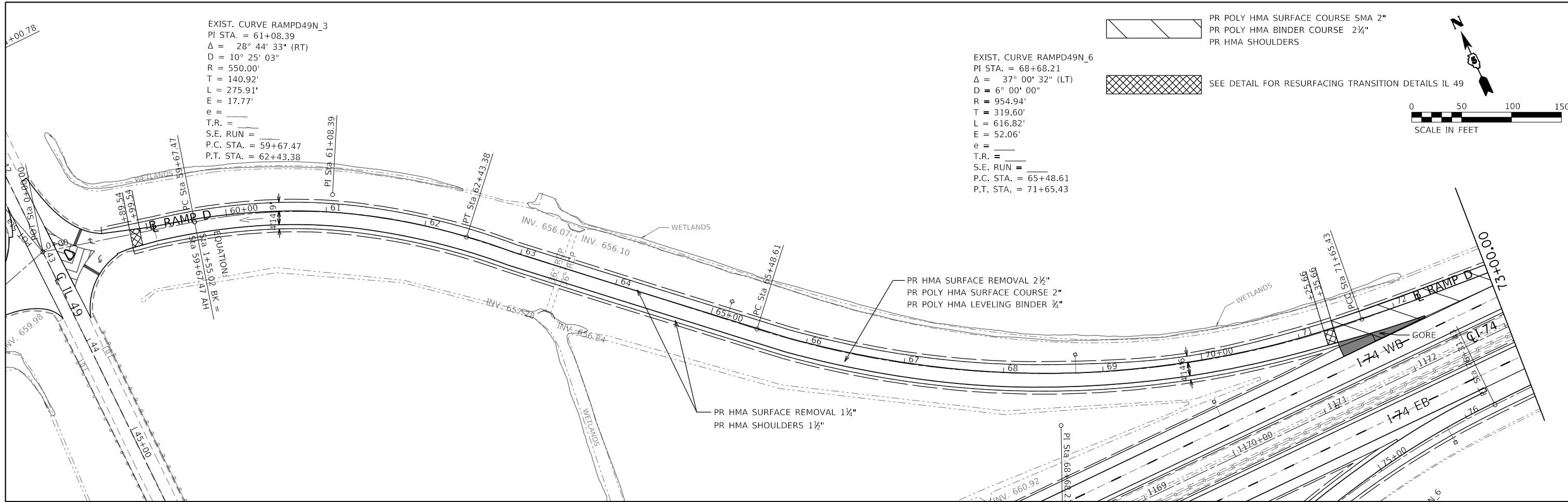
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|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:01:26 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|   |                  |
|---|------------------|
| <b>I-74 / IL 49 RAMP C<br/>PLAN SHEET</b> |                  |
| SCALE: 1" = 50'                           | SHEET OF SHEETS  |
| STA. 39+71.02                             | TO STA. 61+93.96 |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 66        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION



MODEL: Default  
 FILE: I174\_IL49\_RAMP\_D.dwg

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:01:26 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

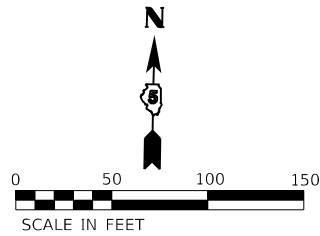
|                                   |   |
|-----------------------------------|---|
| I-74 / IL 49 RAMP D<br>PLAN SHEET |   |
| SCALE: 1" = 50'                   | SHEET OF SHEETS STA. 0+00.00 TO STA. 76+77.88 |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 174                | *       | **     | 142          | 67        |
| CONTRACT NO. 70790 |         |        |              |           |



# OGDEN INTERCHANGE RAMP C NW QUAD

NOTES:  
 ⊗ DENOTES TREE REMOVAL. SEE SCHEDULE.



| LOCATION<br>OGDEN INTERCHANGE | EARTH<br>EXCAVATION<br>(CUT)<br>CU YD | EARTH<br>EXCAVATION<br>ADJUSTED FOR<br>SHRINKAGE (25%)<br>CU YD | EMBANKMENT<br>(FILL)<br>CU YD | EARTHWORK<br>BALANCE<br>WASTE (+) OR<br>SHORTAGE (-)<br>CU YD |
|-------------------------------|---------------------------------------|---|-------------------------------|---|
| NW QUAD                       | 5,346                                 | 4,009   | 4,359                         | -350  |
| Lt Ditch Ramp C CH 22         | 516                                   | 387   | 17                            | 371   |
| Rt Ditch Ramp C CH 22         | 21                                    | 16  | 34                            | -18   |
| TOTAL                         | 5,884                                 | 4,413   | 4,410                         | 2   |

PIPE CULVERTS, CLASS A, TYPE 1 12"  
 STA 497+25.00; NO SKEW  
 36 L.F. @ 0.24% SLOPE  
 W/ PRECAST REINFORCED CONCRETE  
 FLARED END SECTIONS 12", 2-EACH  
 USFL = 666.06; 23.99' RT  
 DSFL = 665.97; 12.01' LT

PIPE CULVERTS, CLASS A, TYPE 1 18"  
 STA 502+25.00; NO SKEW  
 48 L.F. @ 0.10% SLOPE  
 W/ PRECAST REINFORCED CONCRETE  
 FLARED END SECTIONS 18", 2-EACH  
 USFL = 664.39; 28.34' RT  
 DSFL = 664.35; 19.66' LT

PIPE CULVERTS, CLASS A 18" (JACKED)  
 STA 999+00.00; NO SKEW  
 93 L.F. @ 0.05% SLOPE  
 W/ PRECAST REINFORCED CONCRETE  
 FLARED END SECTIONS 18", 1-EACH  
 USFL = 664.55; 1.11' LT  
 DSFL = 664.50; 94.19' LT

MANHOLE TYPE A, 4'  
 W/ TYPE 1 FRAME CLOSED LID  
 STA 999+00.00 0.55' RT  
 TOG ELEV. = 667.55  
 BOTTOM ELEV. = 664.32

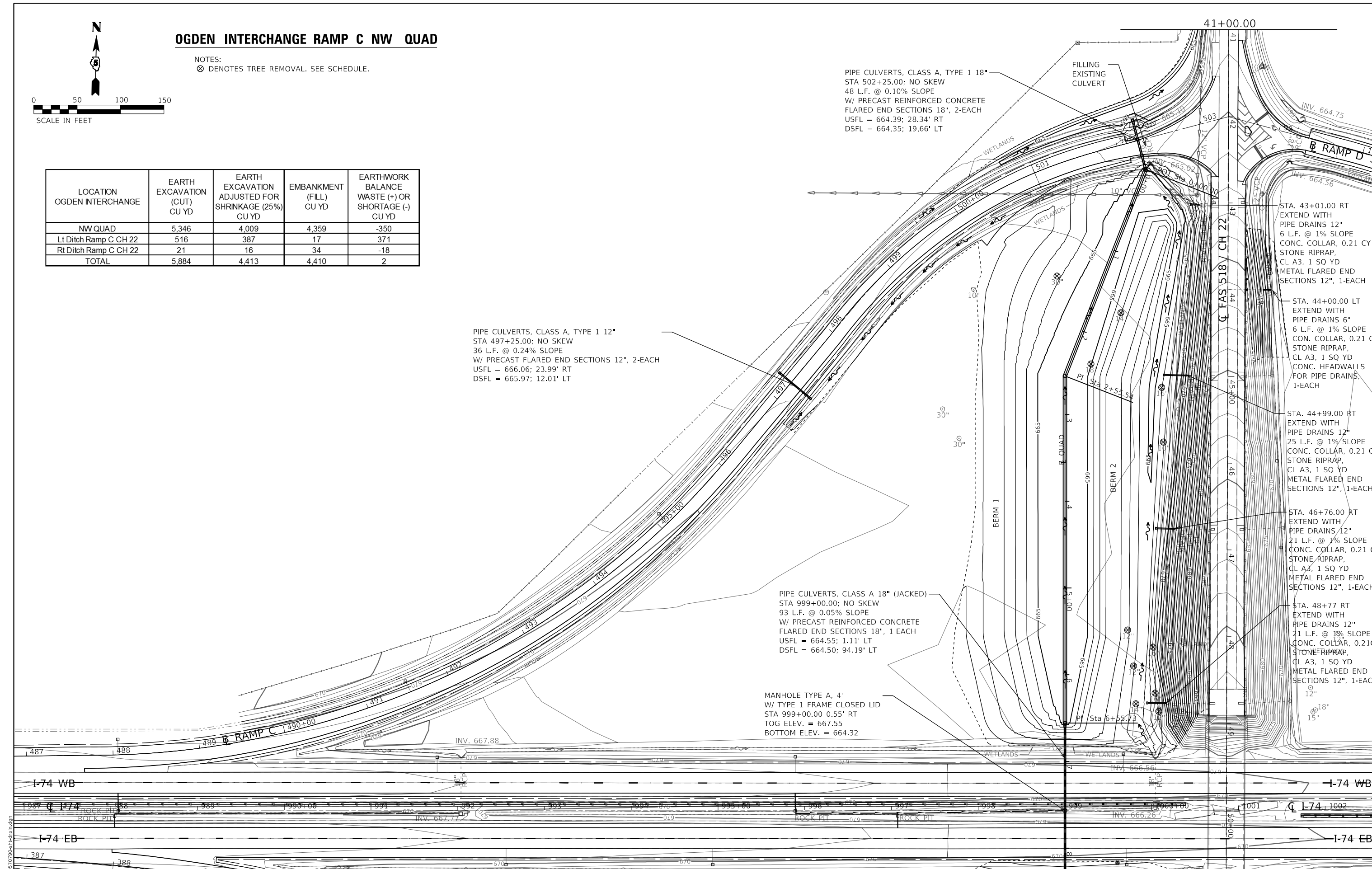
STA. 43+01.00 RT  
 EXTEND WITH  
 PIPE DRAINS 12"  
 6 L.F. @ 1% SLOPE  
 CONC. COLLAR, 0.21 CY  
 STONE RIPRAP,  
 CL A3, 1 SQ YD  
 METAL FLARED END  
 SECTIONS 12", 1-EACH

STA. 44+00.00 LT  
 EXTEND WITH  
 PIPE DRAINS 6"  
 6 L.F. @ 1% SLOPE  
 CON. COLLAR, 0.21 CY  
 STONE RIPRAP,  
 CL A3, 1 SQ YD  
 CONC. HEADWALLS  
 FOR PIPE DRAINS,  
 1-EACH

STA. 44+99.00 RT  
 EXTEND WITH  
 PIPE DRAINS 12"  
 25 L.F. @ 1% SLOPE  
 CONC. COLLAR, 0.21 CY  
 STONE RIPRAP,  
 CL A3, 1 SQ YD  
 METAL FLARED END  
 SECTIONS 12", 1-EACH

STA. 46+76.00 RT  
 EXTEND WITH  
 PIPE DRAINS 12"  
 21 L.F. @ 1% SLOPE  
 CONC. COLLAR, 0.21 CY  
 STONE RIPRAP,  
 CL A3, 1 SQ YD  
 METAL FLARED END  
 SECTIONS 12", 1-EACH

STA. 48+77 RT  
 EXTEND WITH  
 PIPE DRAINS 12"  
 21 L.F. @ 1% SLOPE  
 CONC. COLLAR, 0.21 CY  
 STONE RIPRAP,  
 CL A3, 1 SQ YD  
 METAL FLARED END  
 SECTIONS 12", 1-EACH



MODEL: Default  
 FILE: M:\11111111\11111111\11111111.dwg

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:02:16 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

|                                  |                              |
|----------------------------------|------------------------------|
| DRAINAGE PLANS                   |                              |
| I-74 / CH 22 (OGDEN INTERCHANGE) |                              |
| SCALE: 1" = 50'                  | SHEET OF SHEETS STA. TO STA. |

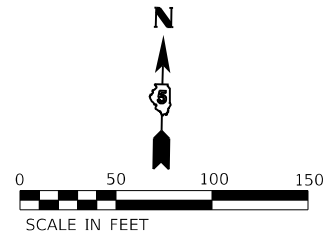
|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 68        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3  
 \*\* CHAMPAIGN & VERMILION



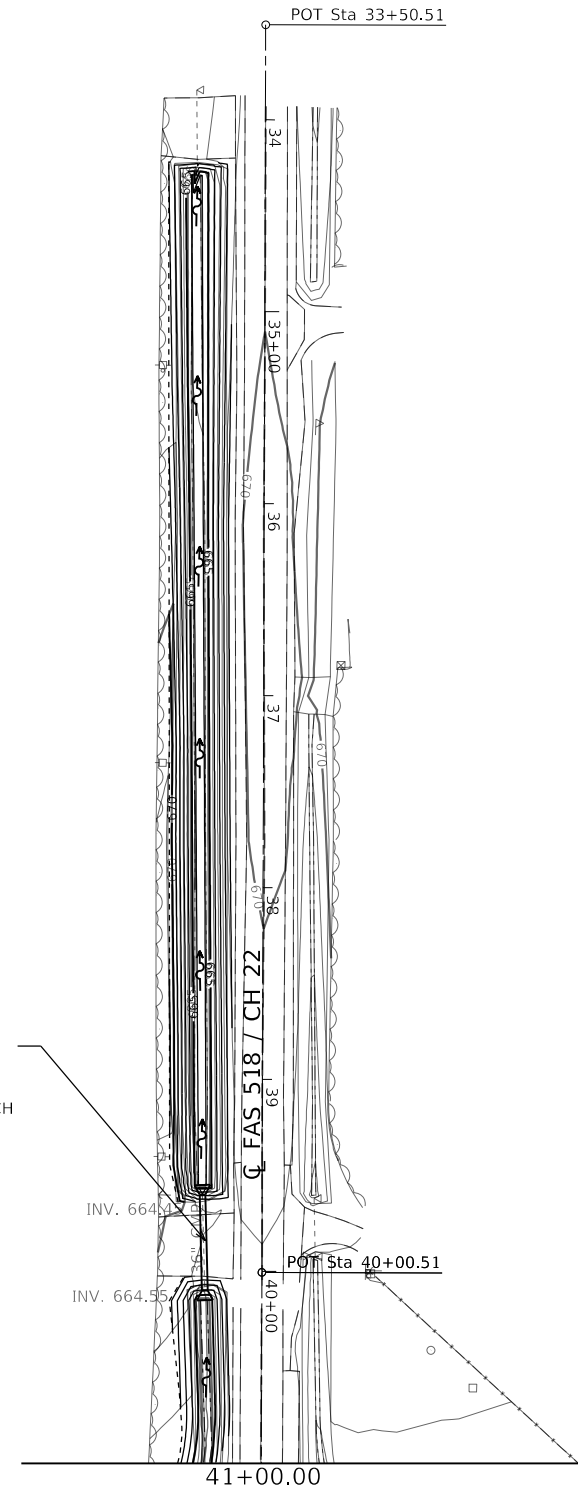
# OGDEN INTERCHANGE RAMP C NW QUAD

NOTES:  
 ⊗ DENOTES TREE REMOVAL. SEE SCHEDULE.



| LOCATION<br>OGDEN INTERCHANGE | EARTH<br>EXCAVATION<br>(CUT)<br>CU YD | EARTH<br>EXCAVATION<br>ADJUSTED FOR<br>SHRNKAGE (25%)<br>CU YD | EMBANKMENT<br>(FILL)<br>CU YD | EARTHWORK<br>BALANCE<br>WASTE (+) OR<br>SHORTAGE (-)<br>CU YD |
|-------------------------------|---------------------------------------|--|-------------------------------|---|
| Right Ditch CH 22             | 189                                   | 142  | 25                            | 117   |

FER STA 39+84.17  
 PIPE CULVERT, CLASS D, TYPE 1 36"  
 50 L.F. @ 0.10% SLOPE  
 W/ METAL FLARED END SECTIONS 36", 2-EACH  
 USFL = 664.05; STA 40+09.14, 29.41' RT  
 DSFL = 664.00; STA 39+59.15, 29.19' RT



MODEL: Default  
 FILE: Model - ...030790-oh-drain.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 100.0000' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:02:17 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

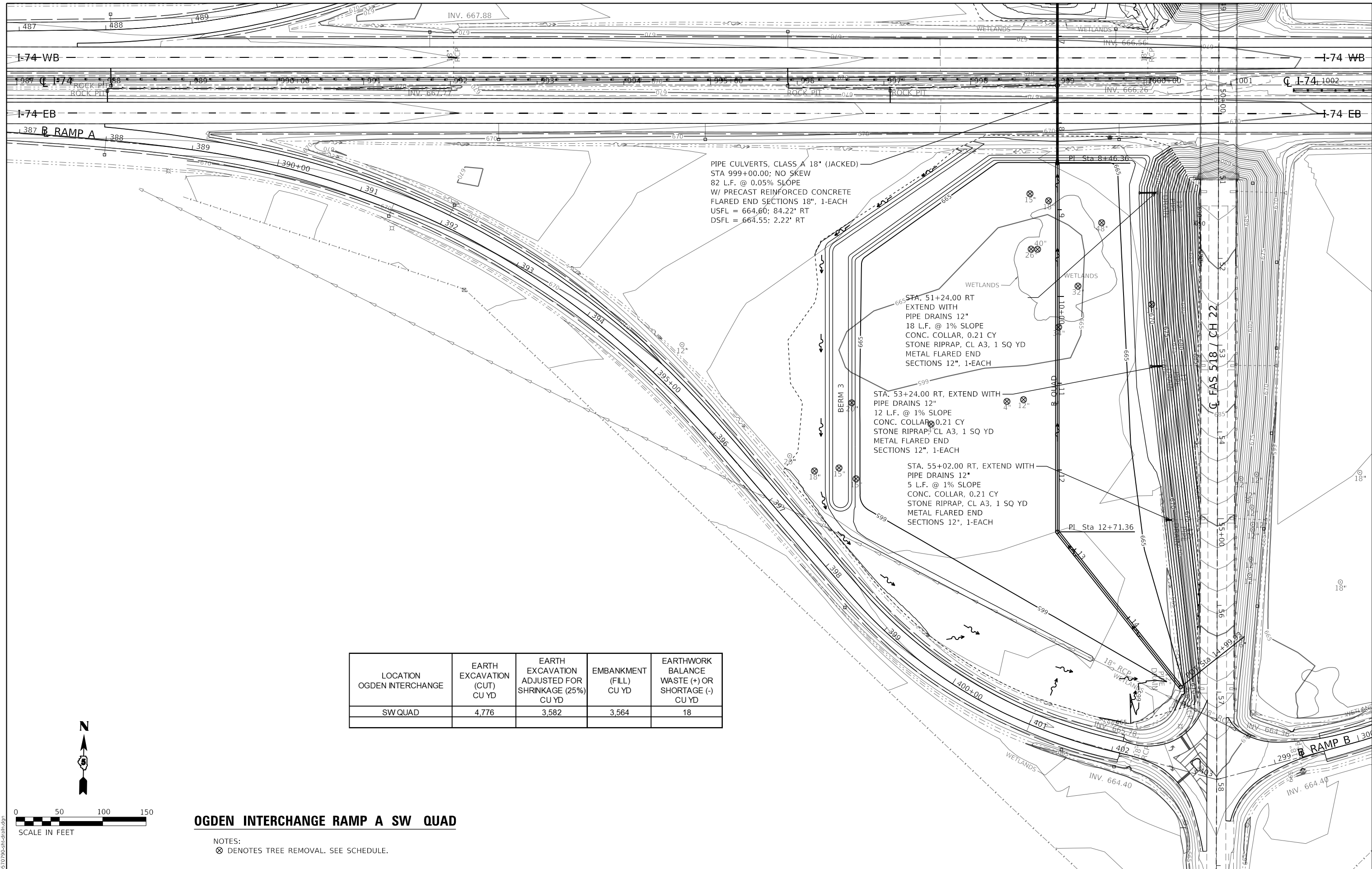
**DRAINAGE PLANS  
 I-74 /CH 22 (OGDEN INTERCHANGE)**

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO STA.

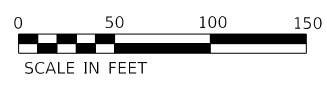
| F.A.I.<br>RTE.     | SECTION | COUNTY | TOTAL<br>SHEETS | SHEET<br>NO. |
|--------------------|---------|--------|-----------------|--------------|
| I-74               | *       | **     | 142             | 69           |
| CONTRACT NO. 70790 |         |        |                 |              |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



| LOCATION<br>OGDEN INTERCHANGE | EARTH<br>EXCAVATION<br>(CUT)<br>CU YD | EARTH<br>EXCAVATION<br>ADJUSTED FOR<br>SHRINKAGE (25%)<br>CU YD | EMBANKMENT<br>(FILL)<br>CU YD | EARTHWORK<br>BALANCE<br>WASTE (+) OR<br>SHORTAGE (-)<br>CU YD |
|-------------------------------|---------------------------------------|---|-------------------------------|---|
| SW QUAD                       | 4,776                                 | 3,582   | 3,564                         | 18  |



**OGDEN INTERCHANGE RAMP A SW QUAD**

NOTES:  
 ⊗ DENOTES TREE REMOVAL. SEE SCHEDULE.

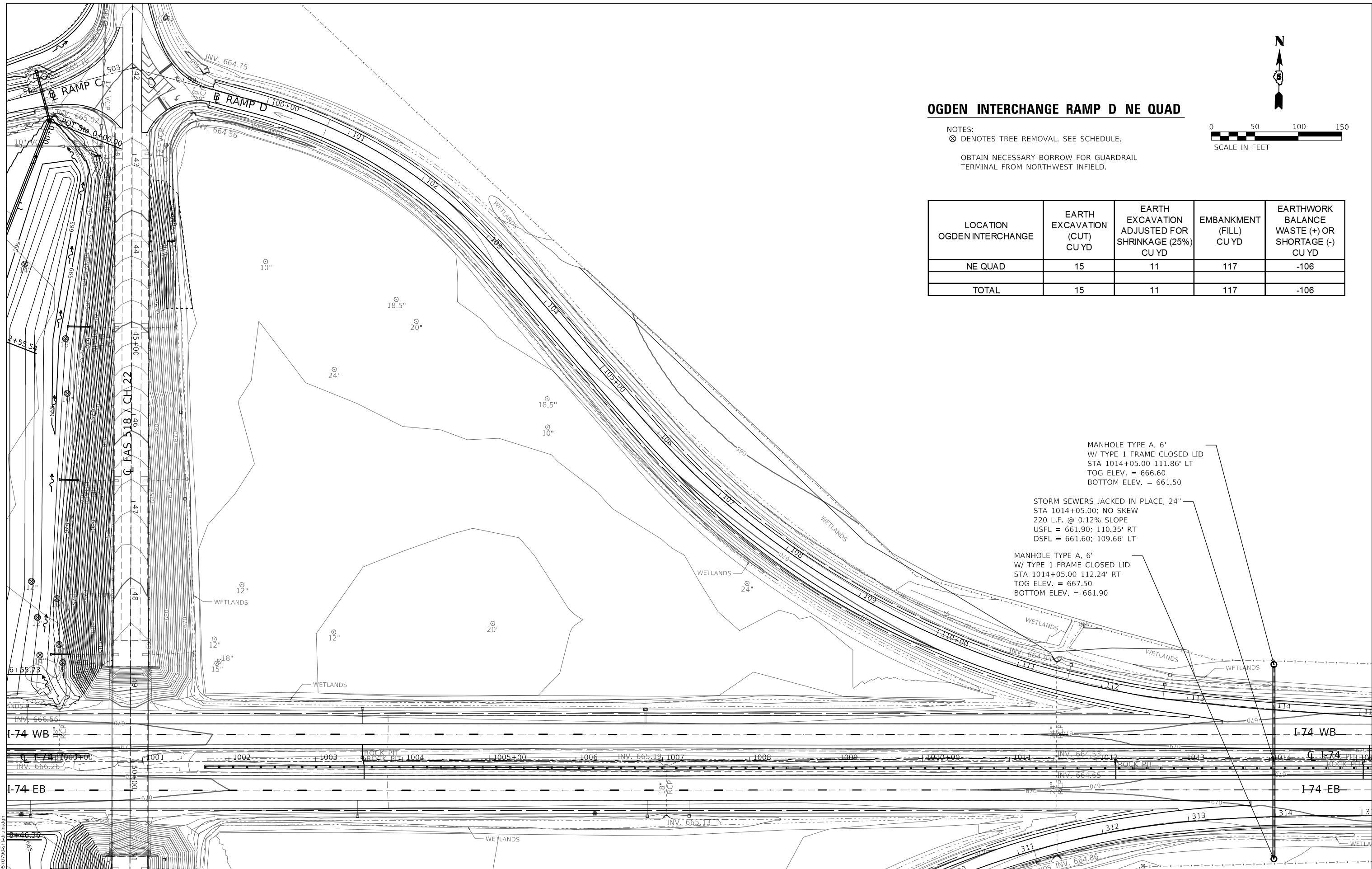
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|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100,0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:02:18 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|   |                              |
|---|------------------------------|
| <b>DRAINAGE PLANS</b>                   |                              |
| <b>I-74 / CH 22 (OGDEN INTERCHANGE)</b> |                              |
| SCALE: 1" = 50'                         | SHEET OF SHEETS STA. TO STA. |

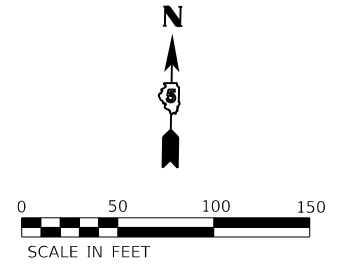
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|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 70        |
| CONTRACT NO. 70790 |         |        |              |           |

ILLINOIS FED. AID PROJECT \*\* CHAMPAIGN & VERMILION  
 (10-7.8;92-8.9)RS-2(92-10)RS-3



**OGDEN INTERCHANGE RAMP D NE QUAD**

NOTES:  
 ⊗ DENOTES TREE REMOVAL. SEE SCHEDULE.  
 OBTAIN NECESSARY BORROW FOR GUARDRAIL  
 TERMINAL FROM NORTHWEST INFIELD.



| LOCATION<br>OGDEN INTERCHANGE | EARTH<br>EXCAVATION<br>(CUT)<br>CU YD | EARTH<br>EXCAVATION<br>ADJUSTED FOR<br>SHRINKAGE (25%)<br>CU YD | EMBANKMENT<br>(FILL)<br>CU YD | EARTHWORK<br>BALANCE<br>WASTE (+) OR<br>SHORTAGE (-)<br>CU YD |
|-------------------------------|---------------------------------------|---|-------------------------------|---|
| NE QUAD                       | 15                                    | 11  | 117                           | -106  |
| TOTAL                         | 15                                    | 11  | 117                           | -106  |

MANHOLE TYPE A, 6'  
 W/ TYPE 1 FRAME CLOSED LID  
 STA 1014+05.00 111.86' LT  
 TOG ELEV. = 666.60  
 BOTTOM ELEV. = 661.50

STORM SEWERS JACKED IN PLACE, 24"  
 STA 1014+05.00; NO SKEW  
 220 L.F. @ 0.12% SLOPE  
 USFL = 661.90; 110.35' RT  
 DSFL = 661.60; 109.66' LT

MANHOLE TYPE A, 6'  
 W/ TYPE 1 FRAME CLOSED LID  
 STA 1014+05.00 112.24' RT  
 TOG ELEV. = 667.50  
 BOTTOM ELEV. = 661.90

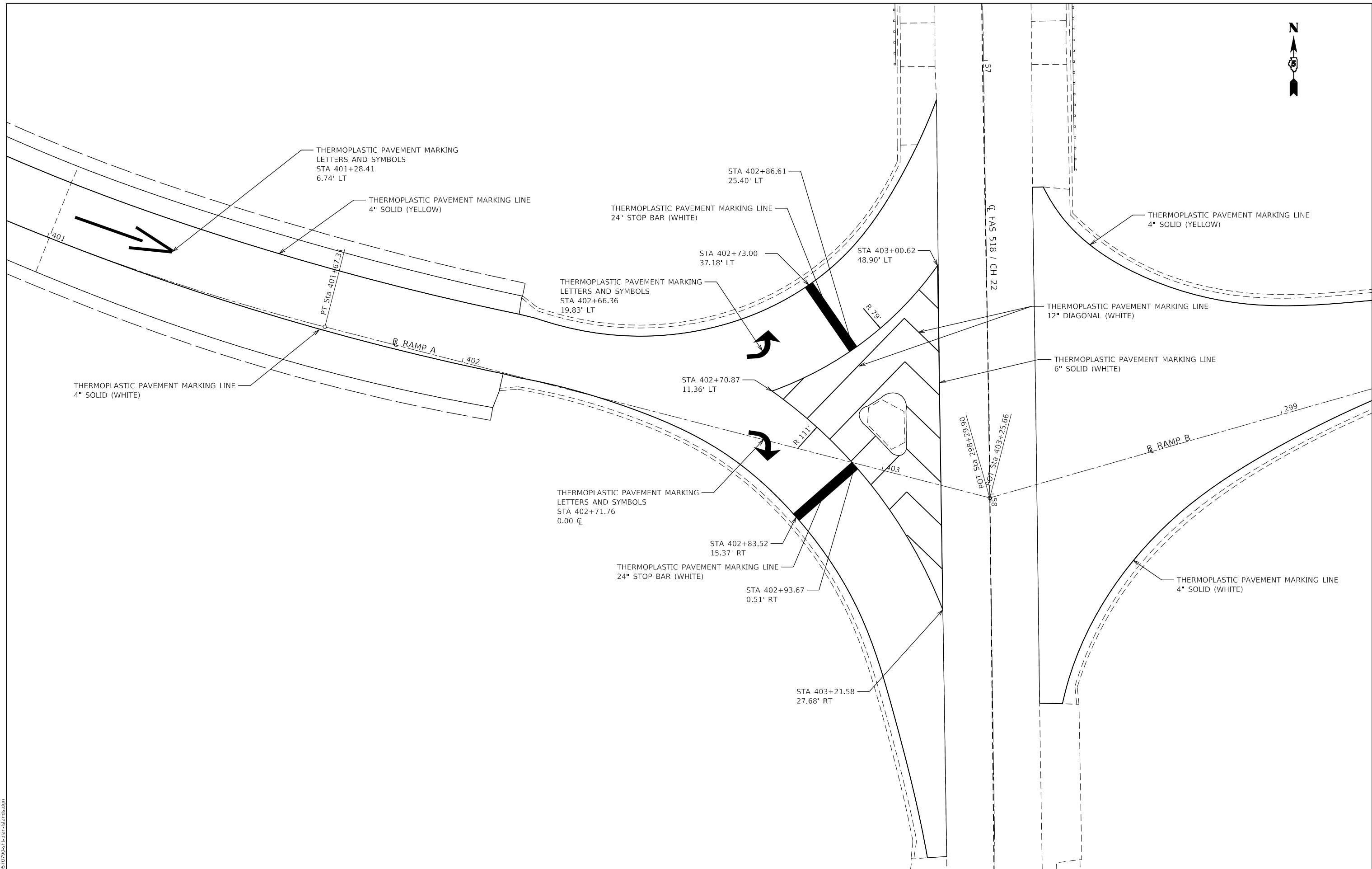
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|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 100.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:02:19 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRAINAGE PLANS  
 I-74 / CH 22 (OGDEN INTERCHANGE)**

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 71        |
| CONTRACT NO. 70790 |         |        |              |           |



MODEL: Default  
 FILE: ModelE\_...03070790-shr-abr-ldandc.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
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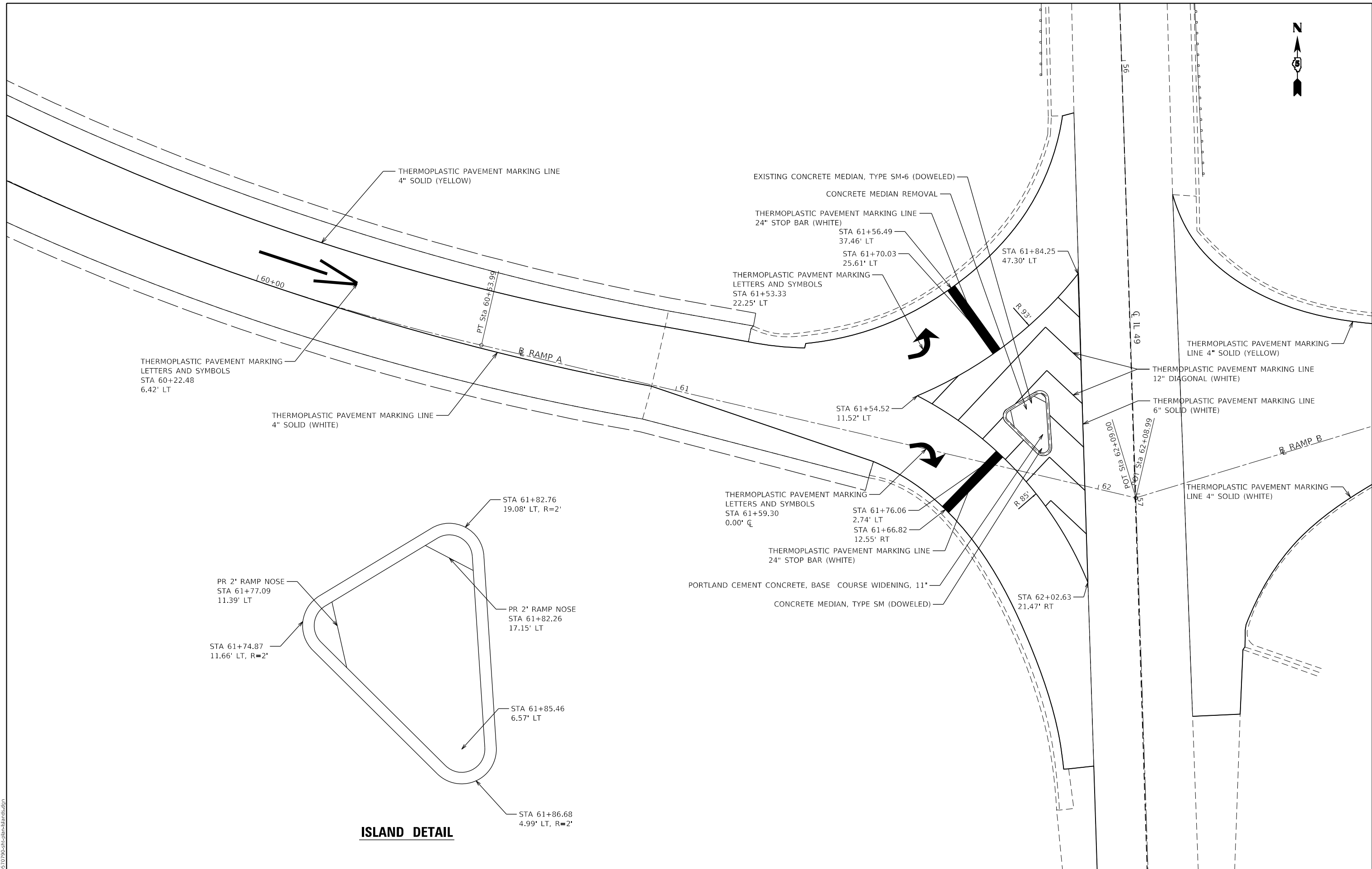
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

|                                      |       |           |              |
|--------------------------------------|-------|-----------|--------------|
| <b>I-74 / CH 22 RAMP A</b>           |       |           |              |
| <b>INTERSECTION STRIPING DETAILS</b> |       |           |              |
| SCALE: N.T.S.                        | SHEET | OF SHEETS | STA. TO STA. |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 72        |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION





**ISLAND DETAIL**

MODEL: Default  
 FILE: ModelE...03027905chr-abn-ldandc.dgn

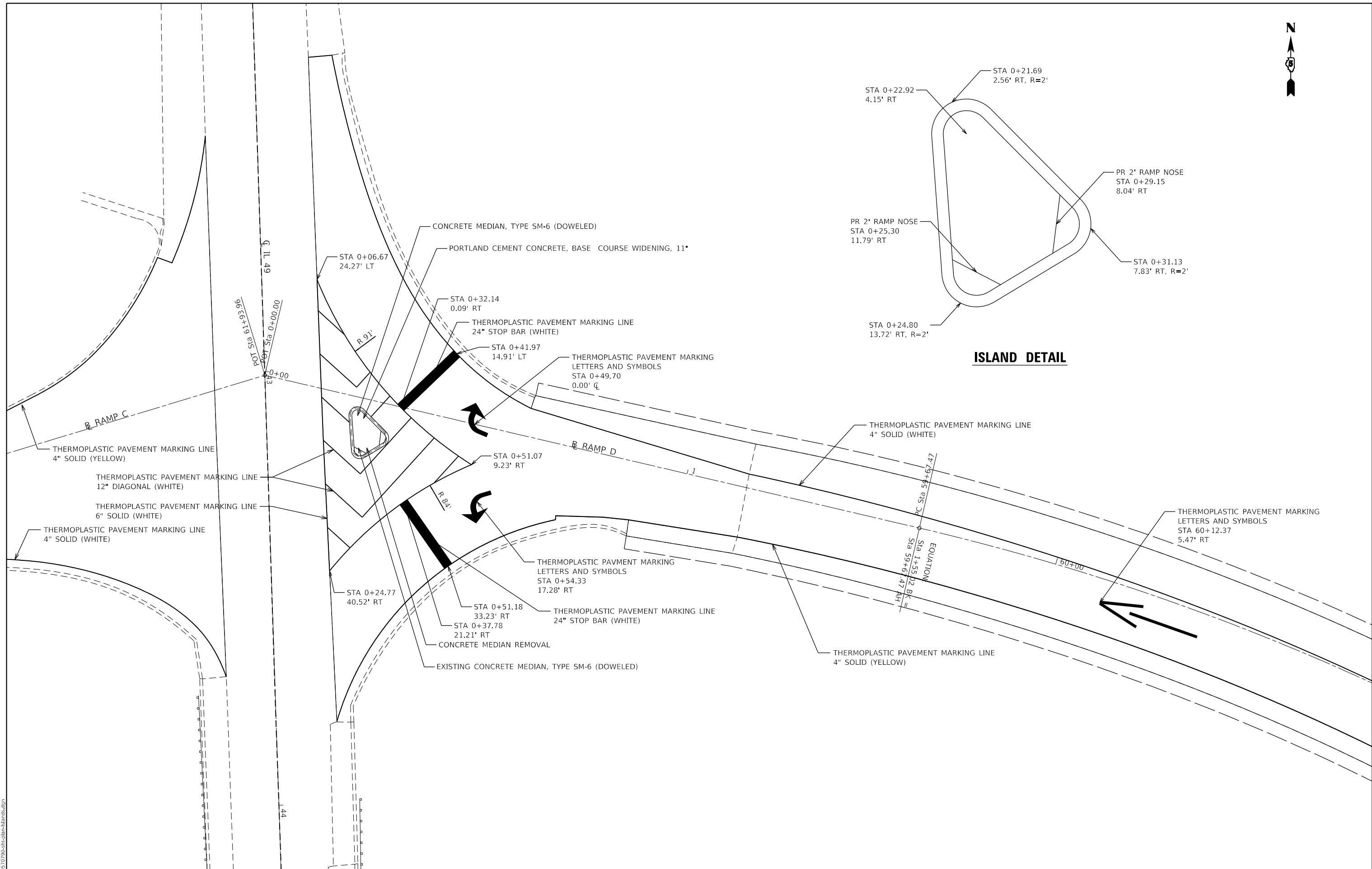
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 20.0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:03:00 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|   |       |           |              |
|---|-------|-----------|--------------|
| <b>I-74 /IL49 RAMP A<br/>INTERSECTION ISLAND &amp; STRIPING DETAILS</b> |       |           |              |
| SCALE: N.T.S.   | SHEET | OF SHEETS | STA. TO STA. |

|                    |           |                  |                  |                          |
|--------------------|-----------|------------------|------------------|--------------------------|
| F.A.I. RTE. I-74   | SECTION * | COUNTY **        | TOTAL SHEETS 142 | SHEET NO. 74             |
| CONTRACT NO. 70790 |           |                  |                  |                          |
| ILLINOIS           |           | FED. AID PROJECT |                  | ** CHAMPAIGN & VERMILION |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION



**ISLAND DETAIL**

MODEL: Default  
 FILE: Model1 - 10/27/2019 4:03:00 PM

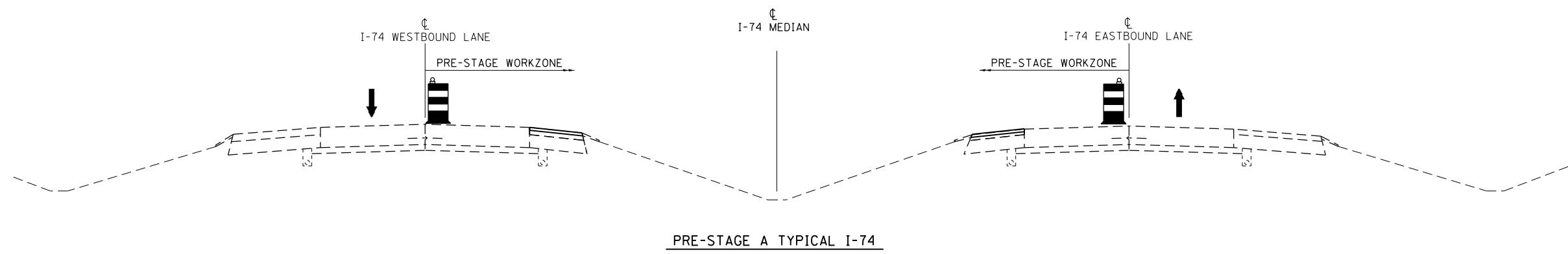
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|------------------------------------|------------|-----------|
| USER NAME = bemyery                | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 20.0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:03:00 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

|   |       |           |              |
|---|-------|-----------|--------------|
| <b>I-74 / IL49 RAMP D<br/>         INTERSECTION ISLAND &amp; STRIPING DETAILS</b> |       |           |              |
| SCALE: N.T.S.   | SHEET | OF SHEETS | STA. TO STA. |

|                           |           |           |                          |              |
|---------------------------|-----------|-----------|--------------------------|--------------|
| F.A.I. RTE. I-74          | SECTION * | COUNTY ** | TOTAL SHEETS 142         | SHEET NO. 75 |
| CONTRACT NO. 70790        |           |           |                          |              |
| ILLINOIS FED. AID PROJECT |           |           | ** CHAMPAIGN & VERMILION |              |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION



PRE-STAGE A TYPICAL I-74

**PRE-STAGE**

TRAFFIC

MAINTAIN ONE LANE OF TRAFFIC IN I-74 EB DRIVING LANE AND MAINTAIN ONE LANE OF TRAFFIC IN I-74 WB DRIVING LANE AT ALL TIMES.

ROADWAY CONSTRUCTION

HMA INLAY OF I-74 EB AND I-74 WB INSIDE SHOULDER (1 3/4") AT FULL DEPTH REPLACEMENT AREAS.

PRIMARY IDOT STANDARDS THIS STAGE  
(SEE SPECIAL PROVISIONS FOR ADDITIONAL APPLICABLE STANDARDS)

701400, 701401, 701406, 701411, 701456, 701901

MODEL: D:\p1\18-1007-01\_I-7410\_CAD\CADD\_Series\0570790-pts\stage\PreStage1.dgn  
 FILE NAME: P1\18-1007-01\_I-7410\_CAD\CADD\_Series\0570790-pts\stage\PreStage1.dgn

|                              |                   |           |
|------------------------------|-------------------|-----------|
| USER NAME = gdc              | DESIGNED - BCD    | REVISED - |
|                              | DRAWN - GDC       | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED - BCD     | REVISED - |
| PLOT DATE = 1/17/2019        | DATE - 09/21/2018 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

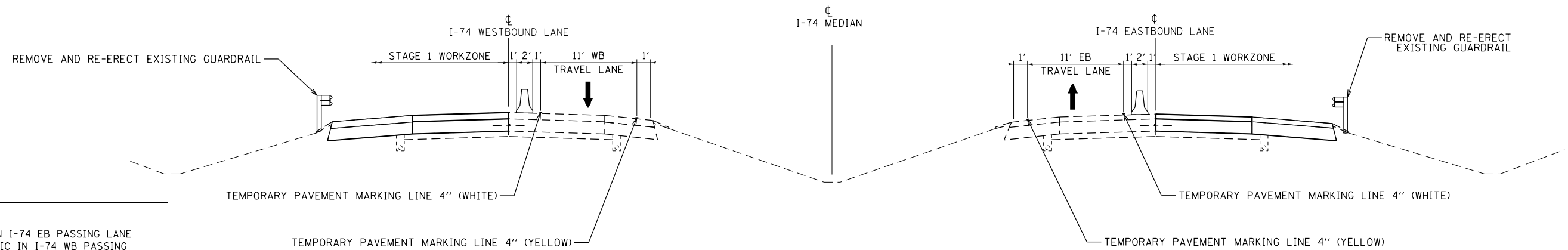
**I-74 SUGGESTED MAINTENANCE OF TRAFFIC  
PRE-STAGE TYPICAL SECTIONS**

SCALE: N.T.S.    SHEET    OF    SHEETS    STA.    TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 76        |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3    \*\* CHAMPAIGN & VERMILION





**PRE-STAGE B**

**TRAFFIC**

MAINTAIN ONE LANE OF TRAFFIC IN I-74 EB PASSING LANE AND MAINTAIN ONE LANE OF TRAFFIC IN I-74 WB PASSING LANE AT ALL TIMES.

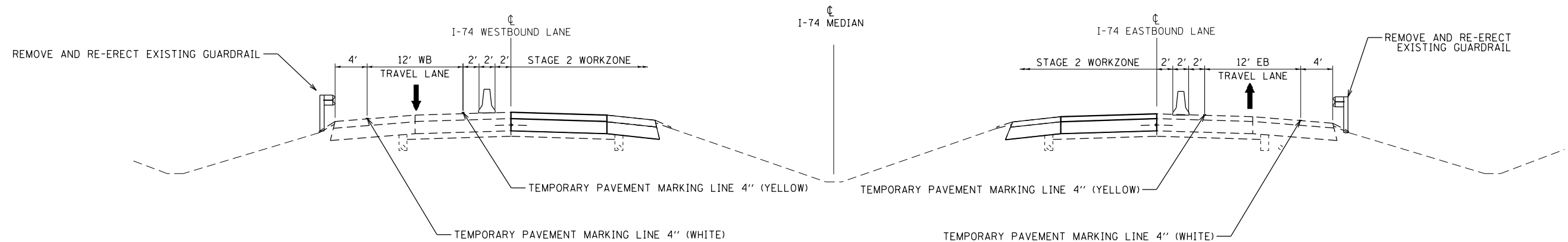
**ROADWAY CONSTRUCTION**

CONSTRUCT I-74 EB AND I-74 WB FULL DEPTH DRIVING LANE AND SHOULDER (EXCEPT SURFACE).

PRIMARY IDOT STANDARDS THIS STAGE  
(SEE SPECIAL PROVISIONS FOR ADDITIONAL APPLICABLE STANDARDS)  
701400, 701402, 701428, 701901

**PRE-STAGE B TYPICAL I-74**

NOTES:  
TO BE USED AT THE FOLLOWING FULL-DEPTH  
REPLACEMENT LOCATIONS UNDERNEATH STRUCTURES:  
SN 010-0183  
SN 092-0146



**PRE-STAGE C**

**TRAFFIC**

MAINTAIN ONE LANE OF TRAFFIC IN I-74 EB DRIVING LANE AND MAINTAIN ONE LANE OF TRAFFIC IN I-74 WB DRIVING LANE AT ALL TIMES.

**ROADWAY CONSTRUCTION**

CONSTRUCT I-74 EB AND I-74 WB FULL DEPTH PASSING LANE AND SHOULDER (EXCEPT SURFACE).

PRIMARY IDOT STANDARDS THIS STAGE  
(SEE SPECIAL PROVISIONS FOR ADDITIONAL APPLICABLE STANDARDS)  
701400, 701402, 701428, 701901

**PRE-STAGE C TYPICAL I-74**

NOTES:  
TO BE USED AT THE FOLLOWING FULL-DEPTH  
REPLACEMENT LOCATIONS UNDERNEATH STRUCTURES:  
SN 010-0183  
SN 092-0146

MODEL: D:\p1\1710\_CAD\CADD\_S\RES\0570790-pts\stageb.dgn

|                              |                   |           |
|------------------------------|-------------------|-----------|
| USER NAME = gdc              | DESIGNED - BCD    | REVISED - |
| PLOT SCALE = 100,0000' / in. | DRAWN - GDC       | REVISED - |
| PLOT DATE = 1/17/2019        | CHECKED - BCD     | REVISED - |
|                              | DATE - 09/21/2018 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**I-74 SUGGESTED MAINTENANCE OF TRAFFIC  
PRE-STAGE TYPICAL SECTIONS**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 77        |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION





**STAGE 1**

**TRAFFIC**

MAINTAIN A MINIMUM OF ONE LANE OF TRAFFIC IN BOTH I-74 EB AND I-74 WB DIRECTIONS AT ALL TIMES.

**ROADWAY CONSTRUCTION**

CONSTRUCT I-74 EB & WB PASSING LANES AND SHOULDER. MILL AND PAVE HMA BINDER COURSE IN PASSING LANES. MILL PASSING LANE SHOULDER TO MATCH. MILL AND PAVE HMA BINDER COURSE IN PASSING LANE AND SHOULDER WITHIN OGDEN INTERCHANGE (I-74 AT FAS 518/CH 22)

SEE PROPOSED TYPICAL SECTION SHEETS FOR DETAILED INFORMATION.

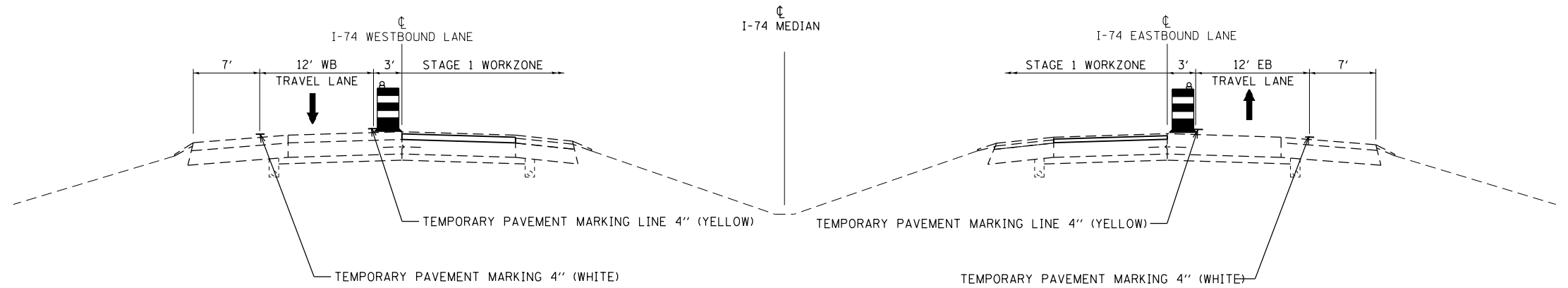
**MEDIAN CONSTRUCTION**

CONSTRUCT PASSING LANE MEDIAN DRAINAGE ITEMS. CONSTRUCT MEDIAN CABLE BARRIER.

SEE DETAILS AND PLAN SHEETS.

PRIMARY IDOT STANDARDS THIS STAGE  
(SEE SPECIAL PROVISIONS FOR ADDITIONAL APPLICABLE STANDARDS)

701400, 701401, 701406, 701411, 701456, 701901



**STAGE 1 TYPICAL I-74**

**STAGE 2**

**TRAFFIC**

MAINTAIN A MINIMUM OF ONE LANE OF TRAFFIC IN BOTH I-74 EB AND I-74 WB DIRECTIONS AT ALL TIMES.

**ROADWAY CONSTRUCTION**

CONSTRUCT I-74 EB & WB DRIVE LANES AND SHOULDER. MILL AND PAVE HMA BINDER COURSE IN DRIVING LANES. MILL DRIVE LANE SHOULDER TO MATCH. MILL AND PAVE HMA BINDER COURSE IN DRIVING LANE AND SHOULDER WITHIN OGDEN INTERCHANGE (I-74 AT FAS 518/CH 22) MILL AND/OR PAVE BINDER ON ALL INTERCHANGE RAMPS

SEE PROPOSED TYPICAL SECTION SHEETS FOR DETAILED INFORMATION.

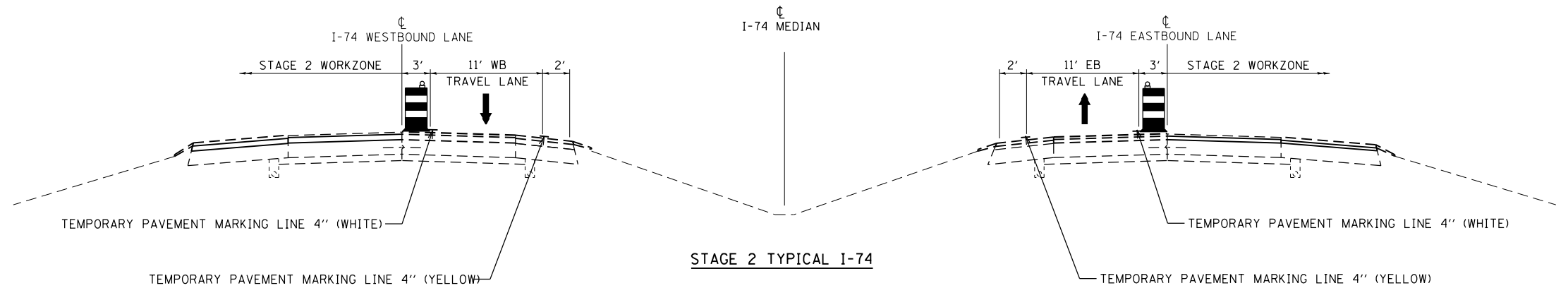
**DRAINAGE CONSTRUCTION**

CONSTRUCT DRIVE LANE AND OUTSIDE DRAINAGE ITEMS.

SEE DETAILS AND PLAN SHEETS.

PRIMARY IDOT STANDARDS THIS STAGE  
(SEE SPECIAL PROVISIONS FOR ADDITIONAL APPLICABLE STANDARDS)

701400, 701401, 701406, 701411, 701456, 701901



**STAGE 2 TYPICAL I-74**

**STAGE 3**

I-74 EB & WB, ALL LANES INCLUDING SHOULDERS, HMA SURFACE TO BE CONSTRUCTED IN SIMILAR METHOD PER IDOT STANDARD 701406.

MODEL: Default  
FILE NAME: P:\18-1007-01 I-7410 CAD\CADD Sheets\DS70790-rt-ctb.dgn

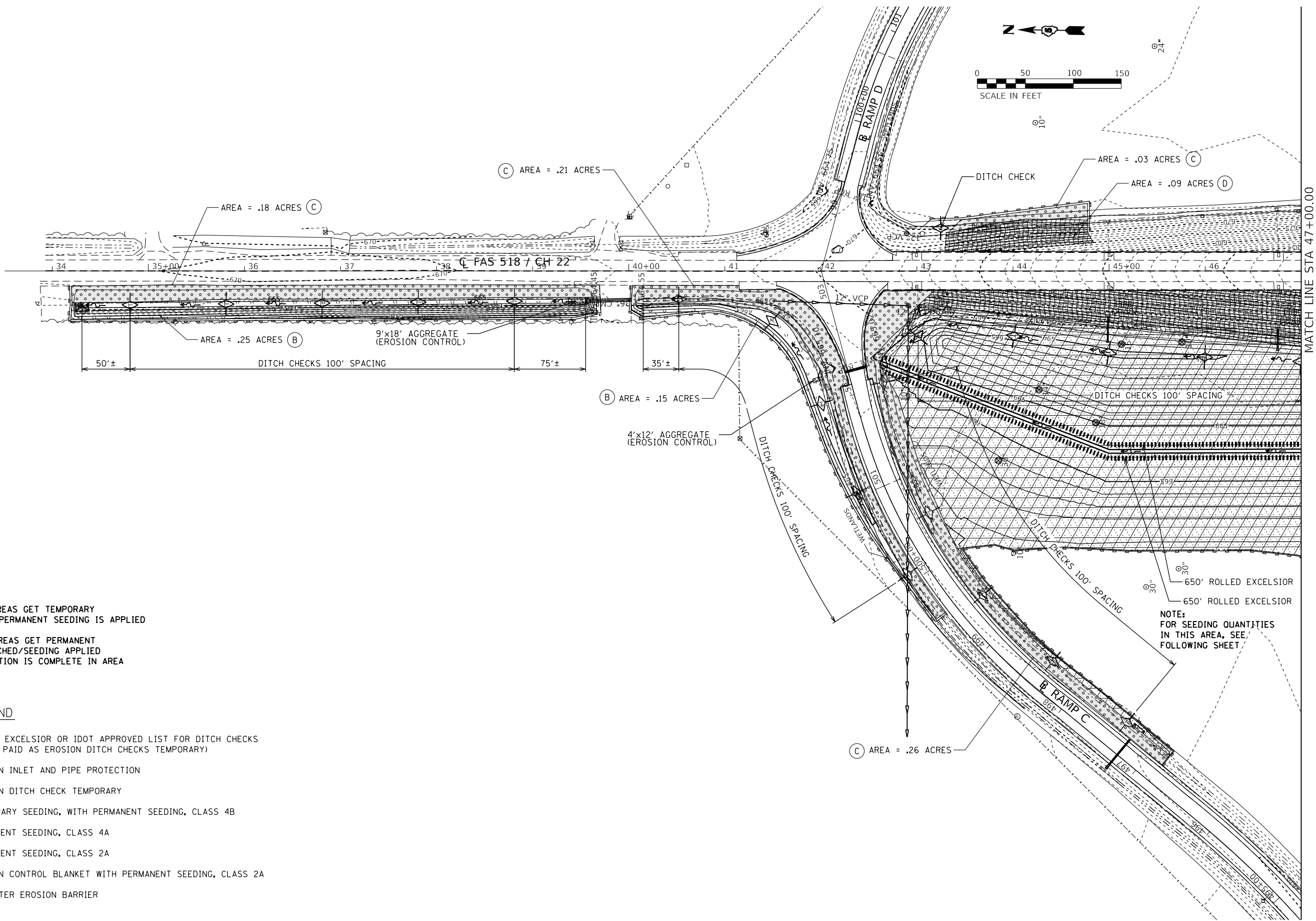
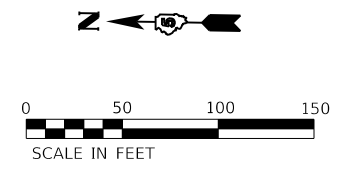
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|                              | DRAWN - GDC       | REVISED - |
| PLOT SCALE = 100,0000' / in. | CHECKED - BCD     | REVISED - |
| PLOT DATE = 1/17/2019        | DATE - 09/21/2018 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |       |           |              |
|--|-------|-----------|--------------|
| <b>I-74 SUGGESTED MAINTENANCE OF TRAFFIC</b> |       |           |              |
| <b>STAGE 1, 2, AND 3</b>                     |       |           |              |
| SCALE: N.T.S.                                | SHEET | OF SHEETS | STA. TO STA. |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 80        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION



- NOTE:**
1. ALL DISTURBED AREAS GET TEMPORARY SEEDING UNTIL PERMANENT SEEDING IS APPLIED
  2. ALL DISTURBED AREAS GET PERMANENT NUTRIENTS/MULCHED/SEEDING APPLIED ONCE CONSTRUCTION IS COMPLETE IN AREA

**LEGEND**

- ROLLED EXCELSIOR OR IDOT APPROVED LIST FOR DITCH CHECKS (TO BE PAID AS EROSION DITCH CHECKS TEMPORARY)
- ⊠ EROSION INLET AND PIPE PROTECTION
- ◊ EROSION DITCH CHECK TEMPORARY
- (A) TEMPORARY SEEDING, WITH PERMANENT SEEDING, CLASS 4B
- (B) PERMANENT SEEDING, CLASS 4A
- (C) PERMANENT SEEDING, CLASS 2A
- (D) EROSION CONTROL BLANKET WITH PERMANENT SEEDING, CLASS 2A
- PERIMETER EROSION BARRIER

**NOTE:**  
FOR SEEDING QUANTITIES  
IN THIS AREA, SEE  
FOLLOWING SHEET

MODEL: D:\m\h\1  
 FILE NAME: P:\18-1007-01-17410-CAD\CADD\_Series\0570790-Plan\EROS\_001.dgn

|                              |                  |           |
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| PLOT DATE = 1/17/2019        | CHECKED - BCD    | REVISED - |
|                              | DATE - 9/21/2018 | REVISED - |

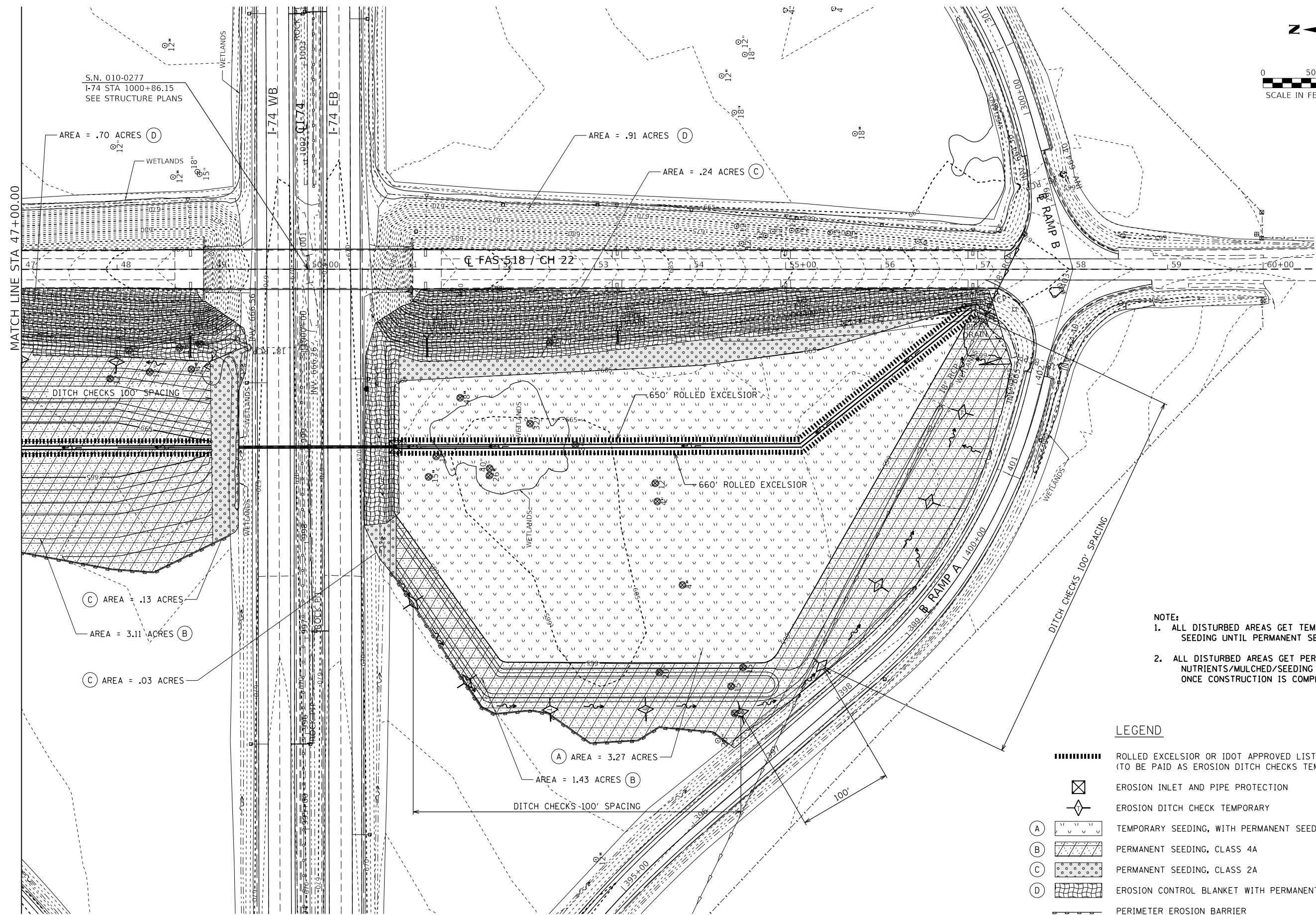
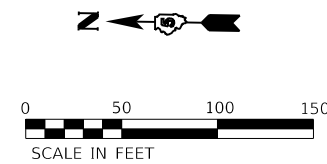
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN  
I-74 AT FAS 518 / CH 22**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

|                    |           |           |                  |              |
|--------------------|-----------|-----------|------------------|--------------|
| F.A.I. RTE. I-74   | SECTION * | COUNTY ** | TOTAL SHEETS 142 | SHEET NO. 81 |
| CONTRACT NO. 70790 |           |           |                  |              |

(10-7.8;92-8.9)RS-2(92-10)RS-3 ILLINOIS FED. AID PROJECT \*\* CHAMPAIGN & VERMILION



S.N. 010-0277  
I-74 STA 1000+86.15  
SEE STRUCTURE PLANS

AREA = .70 ACRES (D)

AREA = .91 ACRES (D)

AREA = .24 ACRES (C)

AREA = .13 ACRES (C)

AREA = 3.11 ACRES (B)

AREA = .03 ACRES (C)

AREA = 3.27 ACRES (A)

AREA = 1.43 ACRES (B)

- NOTE:**
1. ALL DISTURBED AREAS GET TEMPORARY SEEDING UNTIL PERMANENT SEEDING IS APPLIED
  2. ALL DISTURBED AREAS GET PERMANENT NUTRIENTS/MULCHED/SEEDING APPLIED ONCE CONSTRUCTION IS COMPLETE IN AREA

**LEGEND**

- ROLLED EXCELSIOR OR IDOT APPROVED LIST FOR DITCH CHECKS (TO BE PAID AS EROSION DITCH CHECKS TEMPORARY)
- EROSION INLET AND PIPE PROTECTION
- EROSION DITCH CHECK TEMPORARY
- TEMPORARY SEEDING, WITH PERMANENT SEEDING, CLASS 4B
- PERMANENT SEEDING, CLASS 4A
- PERMANENT SEEDING, CLASS 2A
- EROSION CONTROL BLANKET WITH PERMANENT SEEDING, CLASS 2A
- PERIMETER EROSION BARRIER

MODEL: D:\p1\17410\_CAD\CADD\_Series\0570790-PL-EROS\_002.dgn  
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|                              |                  |           |
|------------------------------|------------------|-----------|
| USER NAME = gdc              | DESIGNED - BCD   | REVISED - |
|                              | DRAWN - RJO      | REVISED - |
| PLOT SCALE = 100,0000' / in. | CHECKED - BCD    | REVISED - |
| PLOT DATE = 1/17/2019        | DATE - 9/21/2018 | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN  
I-74 AT FAS 518 / CH 22**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 82        |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

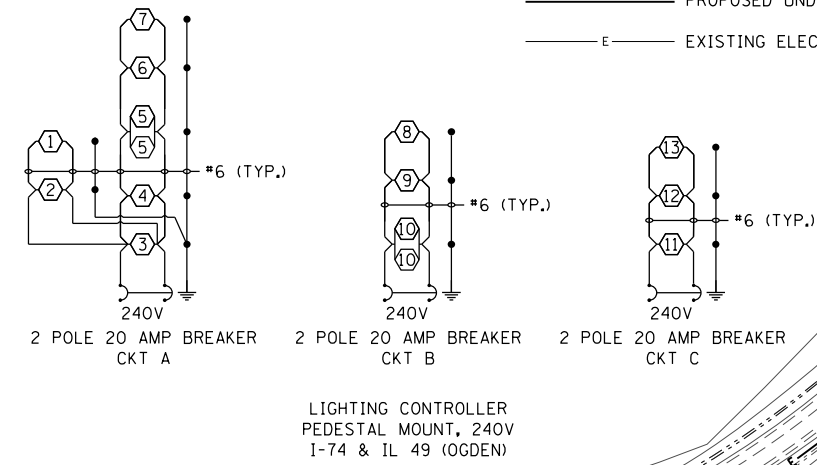
(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION



**NOTES:**  
 1. ALL NECESSARY REVISIONS TO THE WIRING SHOWN ON THIS SHEET SHALL BE MADE AT NO ADDITIONAL COST TO THE DEPARTMENT AND TO THE SATISFACTION OF THE ENGINEER.

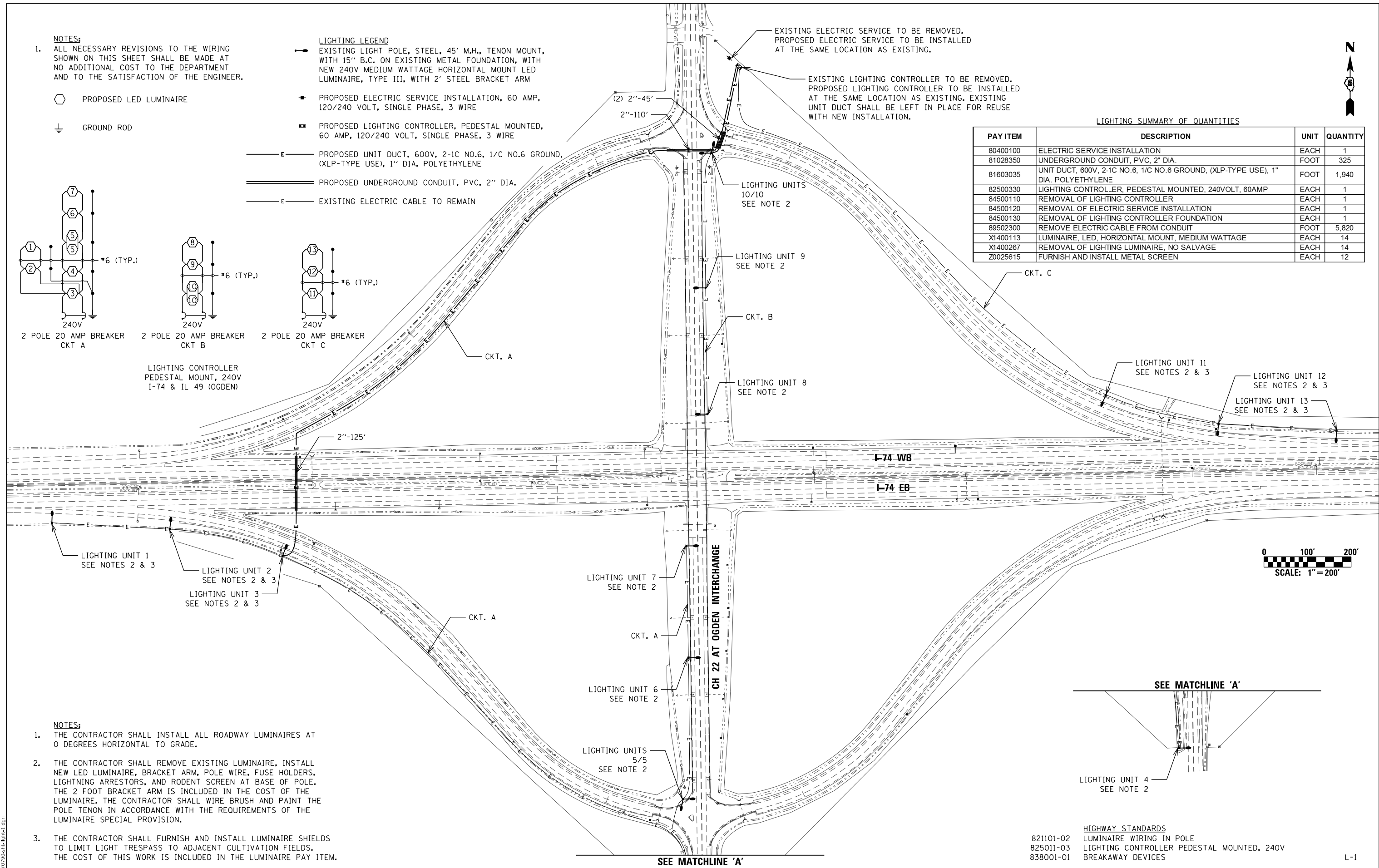
- PROPOSED LED LUMINAIRE
- ⊕ GROUND ROD

- LIGHTING LEGEND**
- EXISTING LIGHT POLE, STEEL, 45' M.H., TENON MOUNT, WITH 15" B.C. ON EXISTING METAL FOUNDATION, WITH NEW 240V MEDIUM WATTAGE HORIZONTAL MOUNT LED LUMINAIRE, TYPE III, WITH 2' STEEL BRACKET ARM
  - PROPOSED ELECTRIC SERVICE INSTALLATION, 60 AMP, 120/240 VOLT, SINGLE PHASE, 3 WIRE
  - PROPOSED LIGHTING CONTROLLER, PEDESTAL MOUNTED, 60 AMP, 120/240 VOLT, SINGLE PHASE, 3 WIRE
  - E— PROPOSED UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE
  - E— PROPOSED UNDERGROUND CONDUIT, PVC, 2" DIA.
  - E— EXISTING ELECTRIC CABLE TO REMAIN



**LIGHTING SUMMARY OF QUANTITIES**

| PAY ITEM | DESCRIPTION   | UNIT | QUANTITY |
|----------|---|------|----------|
| 80400100 | ELECTRIC SERVICE INSTALLATION   | EACH | 1        |
| 81028350 | UNDERGROUND CONDUIT, PVC, 2" DIA.   | FOOT | 325      |
| 81603035 | UNIT DUCT, 600V, 2-1C NO.6, 1/C NO.6 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE | FOOT | 1,940    |
| 82500330 | LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240VOLT, 60AMP                             | EACH | 1        |
| 84500110 | REMOVAL OF LIGHTING CONTROLLER  | EACH | 1        |
| 84500120 | REMOVAL OF ELECTRIC SERVICE INSTALLATION  | EACH | 1        |
| 84500130 | REMOVAL OF LIGHTING CONTROLLER FOUNDATION   | EACH | 1        |
| 89502300 | REMOVE ELECTRIC CABLE FROM CONDUIT  | FOOT | 5,820    |
| X1400113 | LUMINAIRE, LED, HORIZONTAL MOUNT, MEDIUM WATTAGE                                  | EACH | 14       |
| X1400267 | REMOVAL OF LIGHTING LUMINAIRE, NO SALVAGE   | EACH | 14       |
| Z0025615 | FURNISH AND INSTALL METAL SCREEN  | EACH | 12       |



**NOTES:**  
 1. THE CONTRACTOR SHALL INSTALL ALL ROADWAY LUMINAIRES AT 0 DEGREES HORIZONTAL TO GRADE.  
 2. THE CONTRACTOR SHALL REMOVE EXISTING LUMINAIRE, INSTALL NEW LED LUMINAIRE, BRACKET ARM, POLE WIRE, FUSE HOLDERS, LIGHTNING ARRESTORS, AND RODENT SCREEN AT BASE OF POLE. THE 2 FOOT BRACKET ARM IS INCLUDED IN THE COST OF THE LUMINAIRE. THE CONTRACTOR SHALL WIRE BRUSH AND PAINT THE POLE TENON IN ACCORDANCE WITH THE REQUIREMENTS OF THE LUMINAIRE SPECIAL PROVISION.  
 3. THE CONTRACTOR SHALL FURNISH AND INSTALL LUMINAIRE SHIELDS TO LIMIT LIGHT TRESPASS TO ADJACENT CULTIVATION FIELDS. THE COST OF THIS WORK IS INCLUDED IN THE LUMINAIRE PAY ITEM.

- HIGHWAY STANDARDS**
- 821101-02 LUMINAIRE WIRING IN POLE
  - 825011-03 LIGHTING CONTROLLER PEDESTAL MOUNTED, 240V
  - 838001-01 BREAKAWAY DEVICES

MODEL: Default  
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| PLOT SCALE = 200.0000' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:03:30 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**I-74 / CH 22  
LIGHTING PLAN**

SCALE: 1"=100' SHEET OF SHEETS STA. TO STA.

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 83        |
| CONTRACT NO. 70790 |         |        |              |           |



LIGHTING SUMMARY OF QUANTITIES

| PAY ITEM | DESCRIPTION   | UNIT | QUANTITY |
|----------|---|------|----------|
| 80400100 | ELECTRIC SERVICE INSTALLATION                         | EACH | 1        |
| 82500330 | LIGHTING CONTROLLER, PEDESTAL MOUNTED, 240VOLT, 60AMP | EACH | 1        |
| 84500110 | REMOVAL OF LIGHTING CONTROLLER                        | EACH | 1        |
| 84500120 | REMOVAL OF ELECTRIC SERVICE INSTALLATION              | EACH | 1        |
| 84500130 | REMOVAL OF LIGHTING CONTROLLER FOUNDATION             | EACH | 1        |
| X1400113 | LUMINAIRE, LED, HORIZONTAL MOUNT, MEDIUM WATTAGE      | EACH | 10       |
| X1400267 | REMOVAL OF LIGHTING LUMINAIRE, NO SALVAGE             | EACH | 10       |
| Z0025615 | FURNISH AND INSTALL METAL SCREEN                      | EACH | 8        |



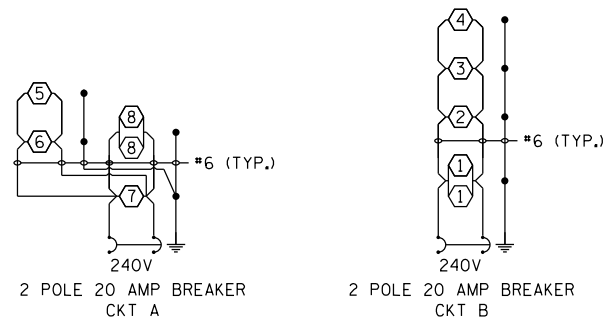
NOTES:

- ALL NECESSARY REVISIONS TO THE WIRING SHOWN ON THIS SHEET SHALL BE MADE AT NO ADDITIONAL COST TO THE DEPARTMENT AND TO THE SATISFACTION OF THE ENGINEER.

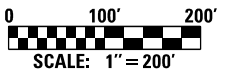
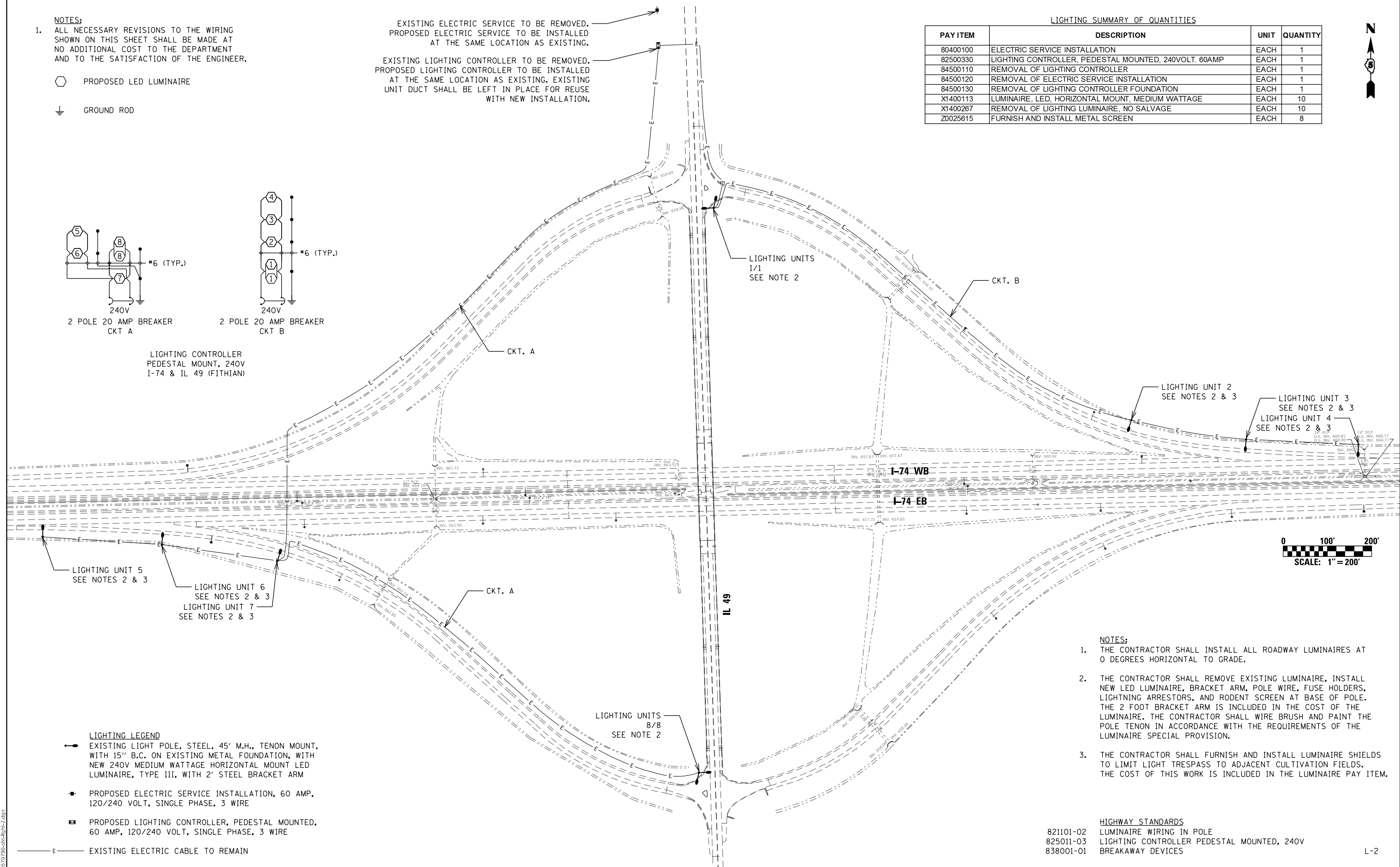
- PROPOSED LED LUMINAIRE
- GROUND ROD

EXISTING ELECTRIC SERVICE TO BE REMOVED.  
PROPOSED ELECTRIC SERVICE TO BE INSTALLED  
AT THE SAME LOCATION AS EXISTING.

EXISTING LIGHTING CONTROLLER TO BE REMOVED.  
PROPOSED LIGHTING CONTROLLER TO BE INSTALLED  
AT THE SAME LOCATION AS EXISTING. EXISTING  
UNIT DUCT SHALL BE LEFT IN PLACE FOR REUSE  
WITH NEW INSTALLATION.



LIGHTING CONTROLLER  
PEDESTAL MOUNT, 240V  
I-74 & IL 49 (FITHIAN)



NOTES:

- THE CONTRACTOR SHALL INSTALL ALL ROADWAY LUMINAIRES AT 0 DEGREES HORIZONTAL TO GRADE.
- THE CONTRACTOR SHALL REMOVE EXISTING LUMINAIRE, INSTALL NEW LED LUMINAIRE, BRACKET ARM, POLE WIRE, FUSE HOLDERS, LIGHTNING ARRESTORS, AND RODENT SCREEN AT BASE OF POLE. THE 2 FOOT BRACKET ARM IS INCLUDED IN THE COST OF THE LUMINAIRE. THE CONTRACTOR SHALL WIRE BRUSH AND PAINT THE POLE TENON IN ACCORDANCE WITH THE REQUIREMENTS OF THE LUMINAIRE SPECIAL PROVISION.
- THE CONTRACTOR SHALL FURNISH AND INSTALL LUMINAIRE SHIELDS TO LIMIT LIGHT TRESPASS TO ADJACENT CULTIVATION FIELDS. THE COST OF THIS WORK IS INCLUDED IN THE LUMINAIRE PAY ITEM.

HIGHWAY STANDARDS

- 821101-02 LUMINAIRE WIRING IN POLE
- 825011-03 LIGHTING CONTROLLER PEDESTAL MOUNTED, 240V
- 838001-01 BREAKAWAY DEVICES

L-2

MODEL: Default  
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| PLOT DATE = 1/24/2019 - 4:03:59 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

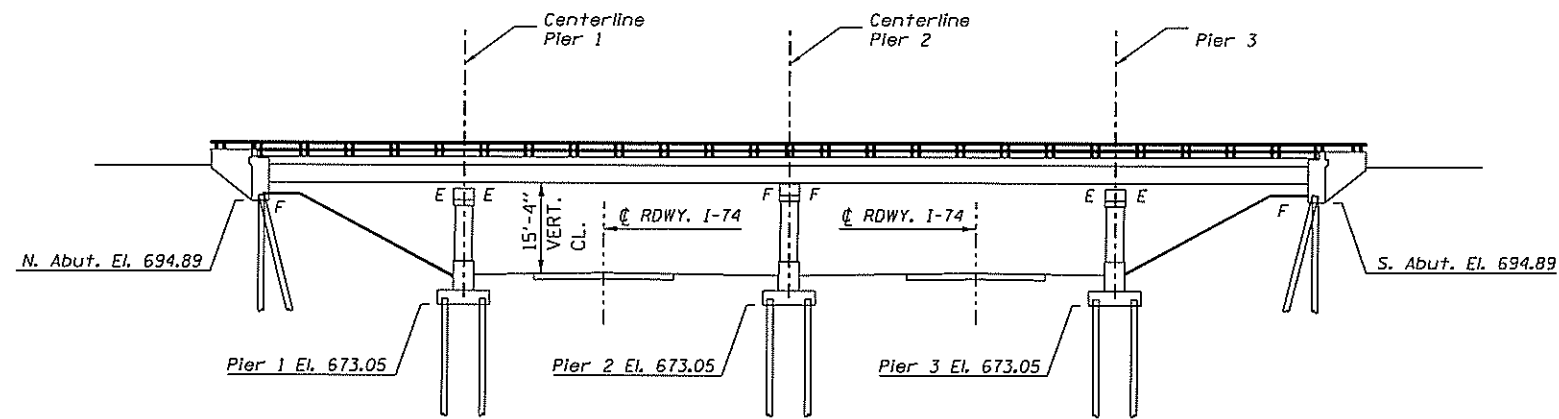
I-74 /IL 49  
LIGHTING PLAN

SCALE: 1"=100' SHEET OF SHEETS STA. TO STA.

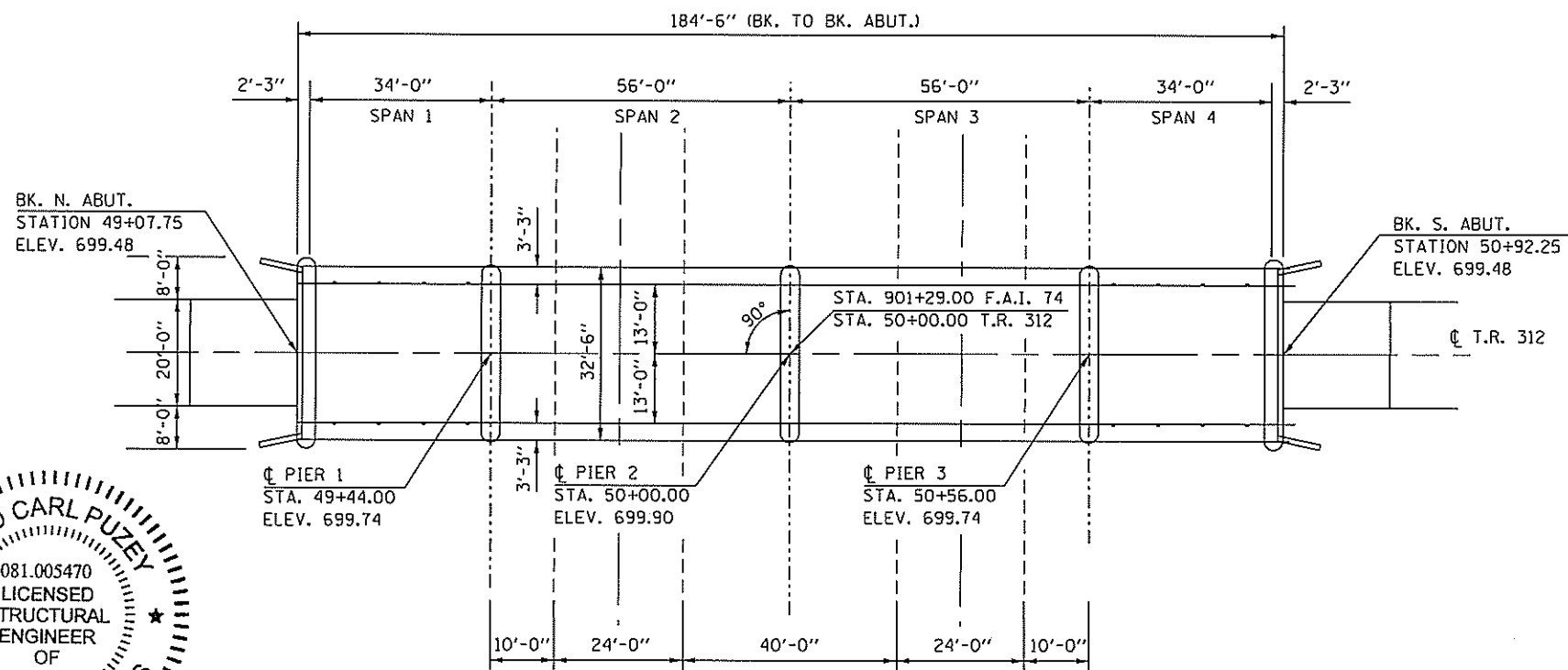
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 84        |
| CONTRACT NO. 70790 |         |        |              |           |

ILLINOIS FED. AID PROJECT (10-7;8;92-8;9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION





ELEVATION VIEW



PLAN VIEW

S.N. 010-0183 WAS BUILT IN 1959 AS FAI 5, SECTION 10.92-BHB AT STATION 901+29 IN CHAMPAIGN COUNTY. THE STRUCTURE IS A FOUR (4) SPAN, 7" THICK REINFORCED DECK WITH PRESTRESSED CONCRETE BEAMS ON STEEL CONCRETE PILE SUPPORTED PIERS AND ABUTMENTS.

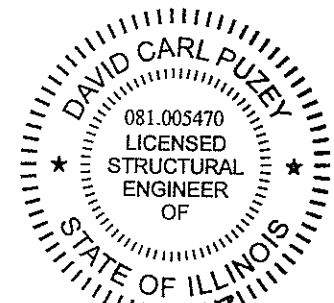
THE STRUCTURE HAS A BACK-TO-BACK ABUTMENT LENGTH OF 184'6", OUT-TO-OUT WIDTH OF 32'-6", BRIDGE ROADWAY WIDTH OF 26'-0", AND AN APPROACH ROADWAY WIDTH OF 20'-0".

PROPOSED IMPROVEMENTS

1. PLACE STEEL BRIDGE RAILING, TYPE 2399

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.



*David Carl Puzey* 3/18/19  
Expires 1/30/20

TOTAL BILL OF MATERIALS

| ITEM                     | UNITS | TOTAL |
|--------------------------|-------|-------|
| STEEL RAILING, TYPE 2399 | FOOT  | 369.0 |

MODEL: Default  
FILE NAME: ...0510799\_Bridge Sheets.dwg

|                                    |                |           |
|------------------------------------|----------------|-----------|
| USER NAME = bernery                | DESIGNED - JMS | REVISED - |
| PLOT SCALE = 40.0000' / in.        | DRAWN - JMS    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:04:54 PM | CHECKED - TJB  | REVISED - |
|                                    | DATE -         | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION  
S.N. 010-0183

SCALE: N.T.S. SHEET 1 OF 4 SHEETS STA. TO STA.

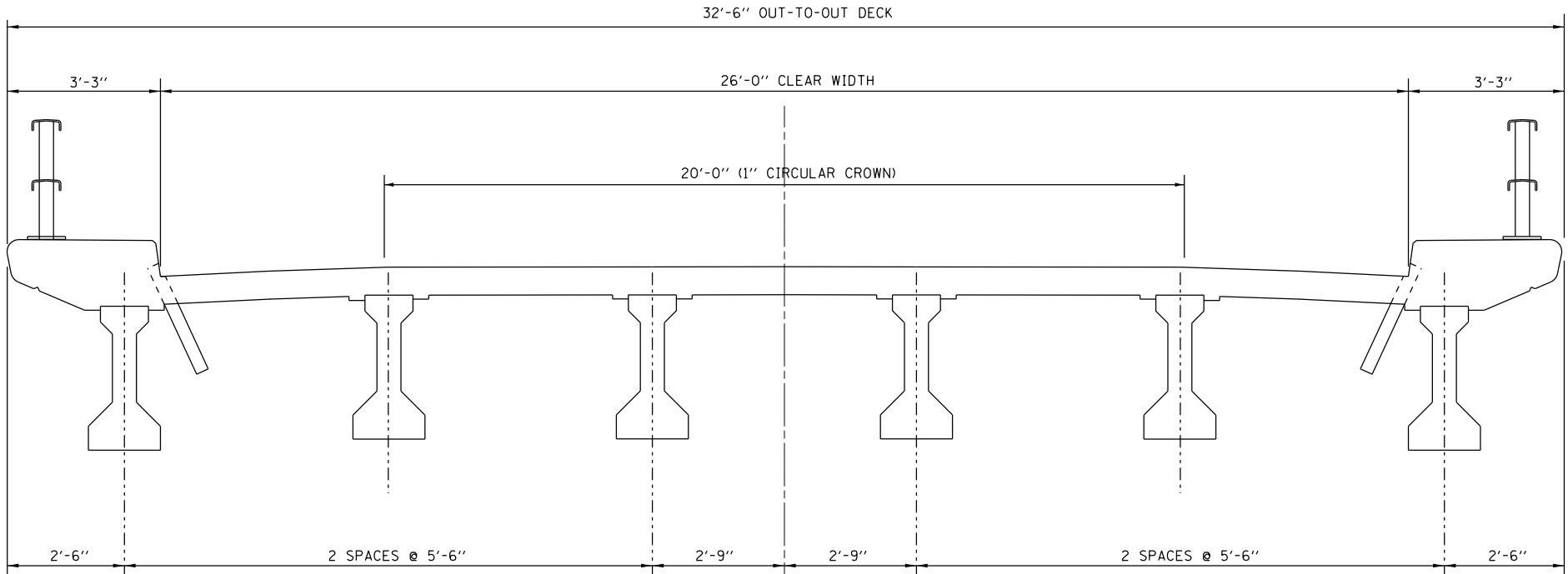
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS                | SHEET NO. |
|--------------------|---------|--------|-----------------------------|-----------|
| I-74               | *       | **     | 142                         | 85        |
| CONTRACT NO. 70790 |         |        | ILLINOIS   FED. AID PROJECT |           |

(10-7 8:92-8 9)R5-7(92-10)R5-3

\*\* CHAMPAIGN & VERMILION

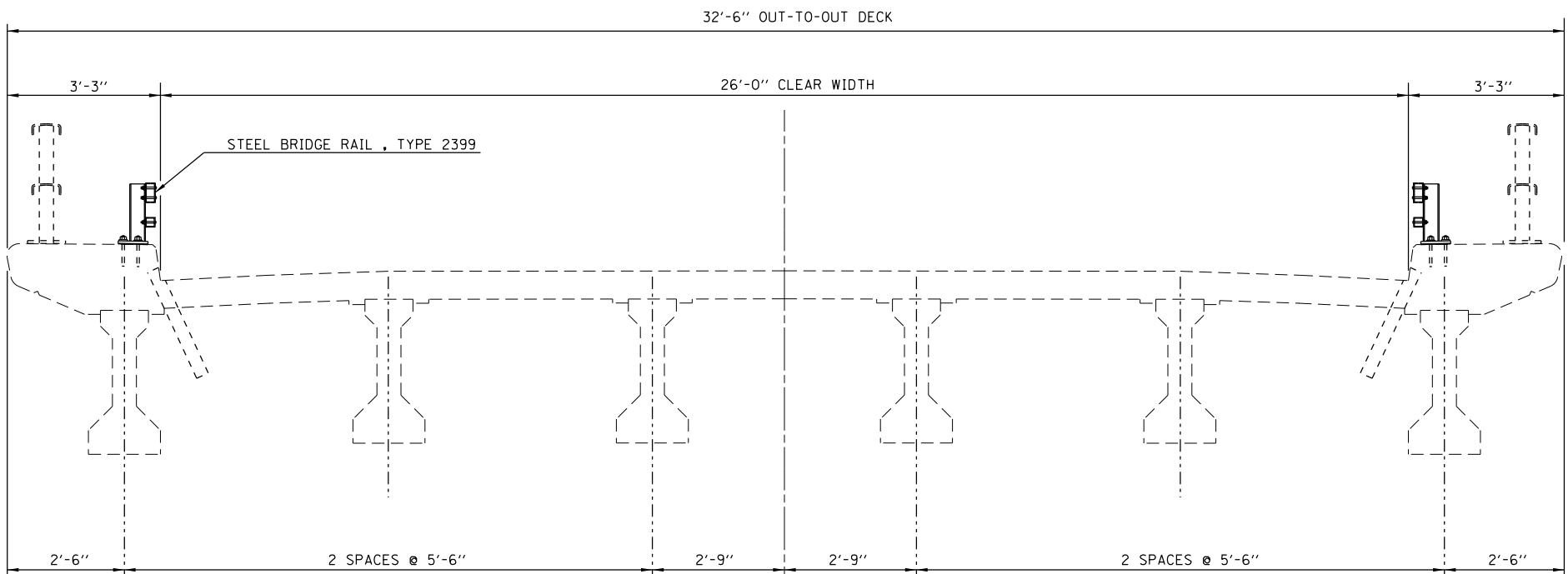
# EXISTING DECK CROSS SECTION

S.N. 010-0183



# PROPOSED DECK CROSS SECTION

S.N. 010-0183



1-74

MODEL: Default  
FILE: Model - 10/27/2019 - Bridge - Sheets.dgn

|                                    |                |               |
|------------------------------------|----------------|---------------|
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|                                    | DRAWN - JMS    | REVISED - JMS |
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| PLOT DATE = 1/24/2019 - 4:04:55 PM | DATE -         | REVISED -     |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

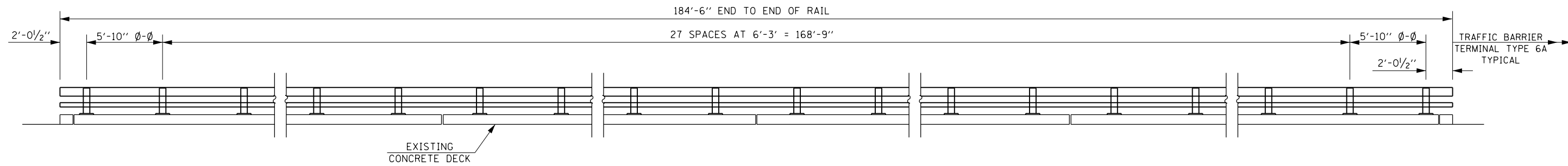
|                            |         |             |              |
|----------------------------|---------|-------------|--------------|
| <b>DECK CROSS SECTIONS</b> |         |             |              |
| <b>S.N. 010-0183</b>       |         |             |              |
| SCALE: N.T.S.              | SHEET 2 | OF 4 SHEETS | STA. TO STA. |

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| 1-74               | *       | **     | 142          | 86        |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

# STEEL RAILING, PLAN

## S.N. 010-0183

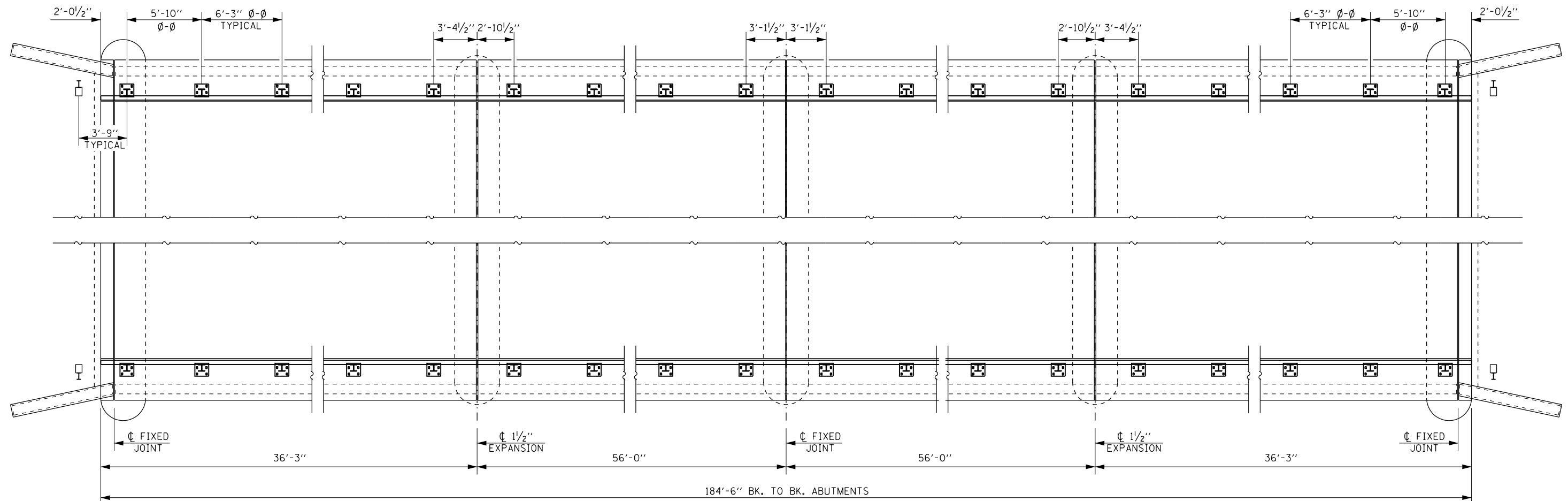


### ELEVATION VIEW EAST STEEL RAIL

WEST STEEL RAIL TYPICAL



FOR ADDITIONAL DETAILS FOR STEEL RAILING SEE NEXT SHEET.



### PLAN VIEW STEEL RAILING

MODEL: Default  
FILE: Model - 10/27/2019 - Bridge - Sheets.dgn

|                                    |                |           |
|------------------------------------|----------------|-----------|
| USER NAME = bemery                 | DESIGNED - ESS | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | DRAWN - ESS    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:04:55 PM | CHECKED - TJB  | REVISED - |
|                                    | DATE -         | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

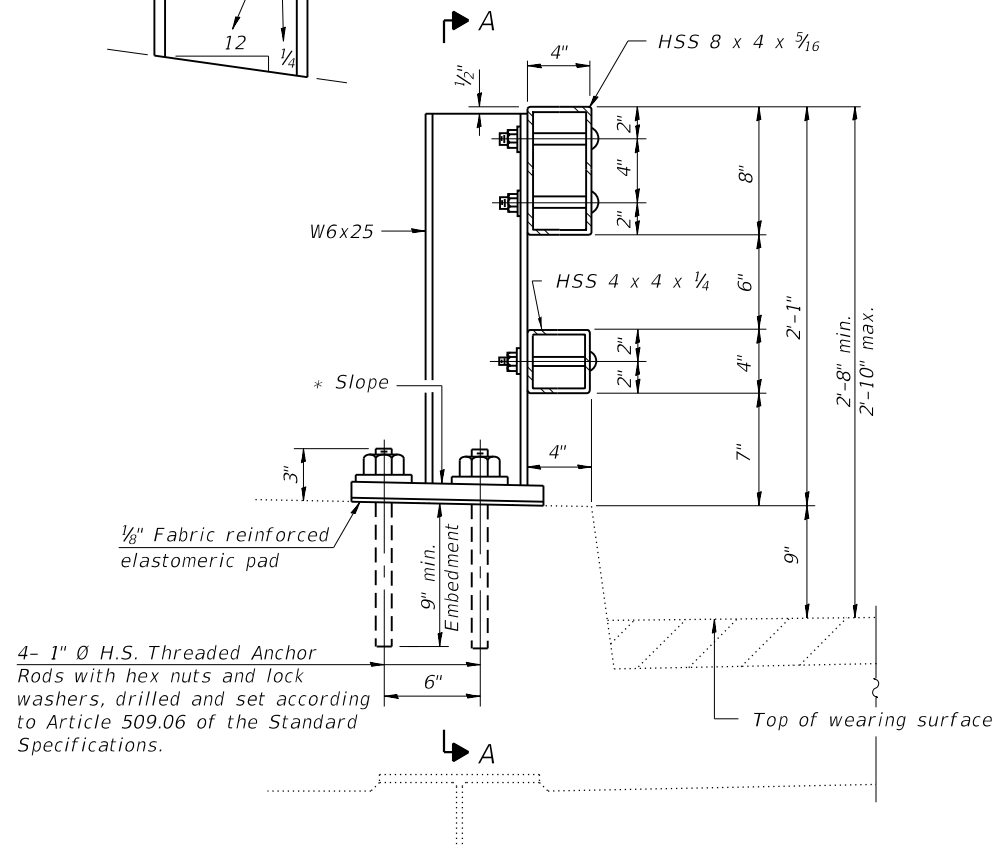
**STEEL RAILING, PLAN  
S.N. 010-0183**

SCALE: N.T.S.    SHEET 3 OF 4 SHEETS    STA.    TO STA.

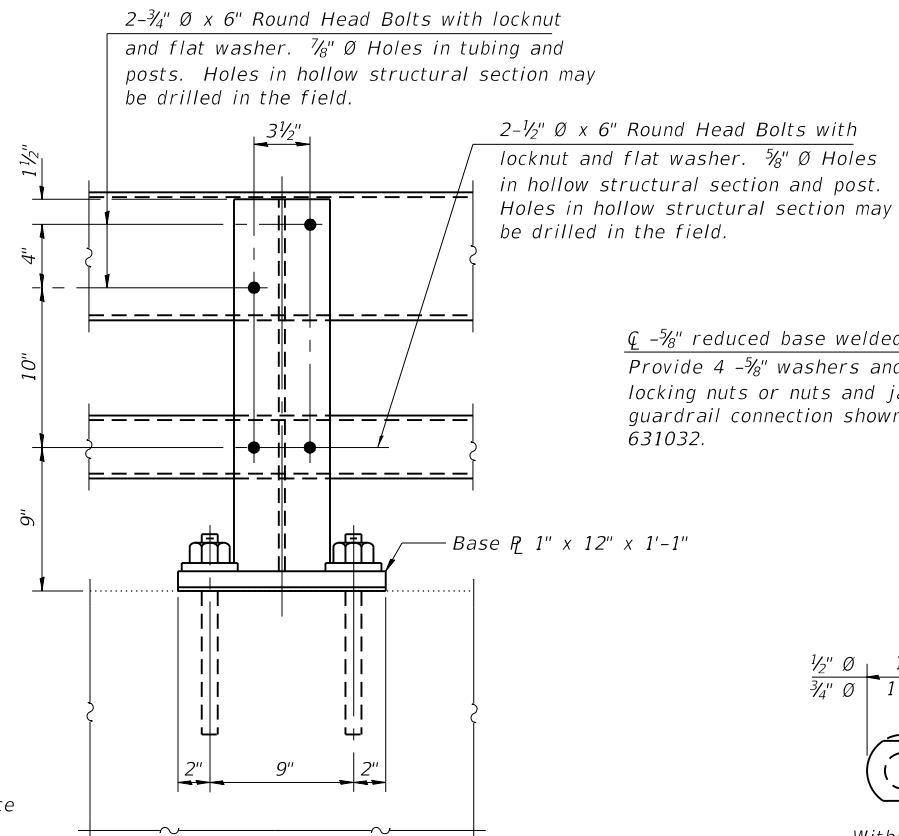
|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 87        |
| CONTRACT NO. 70790 |         |        |              |           |

ILLINOIS FED. AID PROJECT    \* (10-7.8;92-8.9)RS-2(92-10)RS-3    \*\* CHAMPAIGN & VERMILION

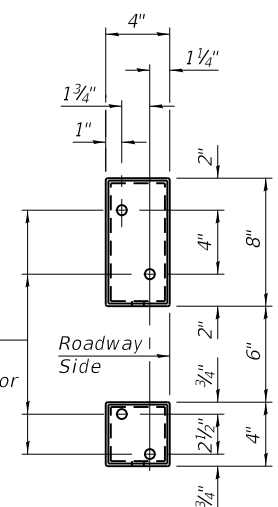
\* Cut bottom end of post to curb slope.



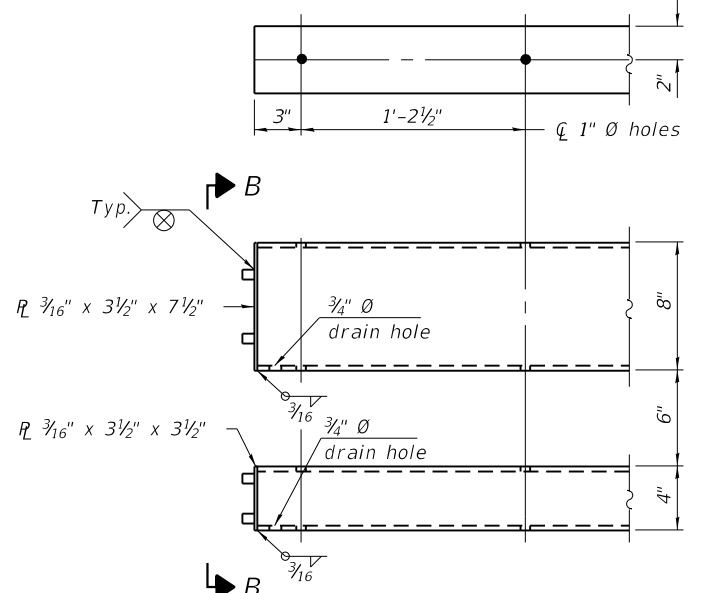
SECTION AT RAIL POST



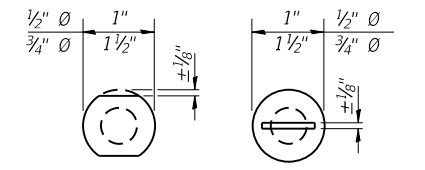
SECTION A-A



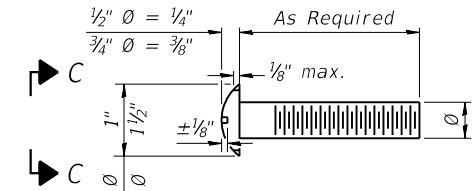
VIEW B-B



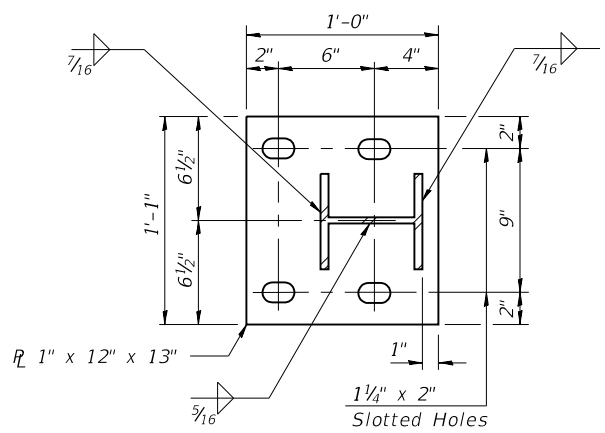
END OF RAIL DETAILS



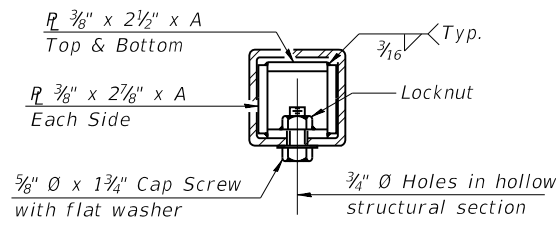
VIEW C-C



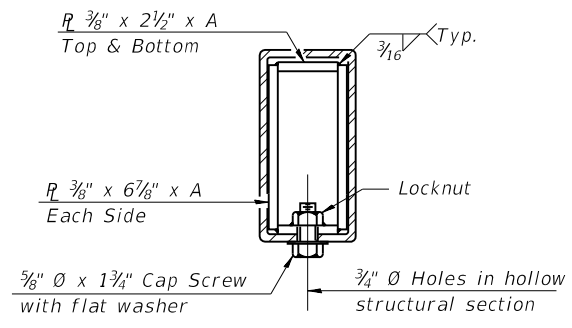
DETAIL OF 1/2" Ø & 3/4" Ø ROUND HEAD BOLTS



BASE PLATE DETAIL

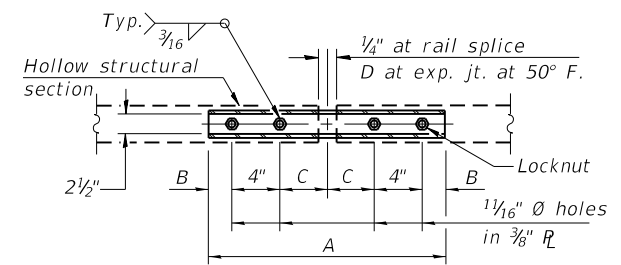


BOTTOM RAIL

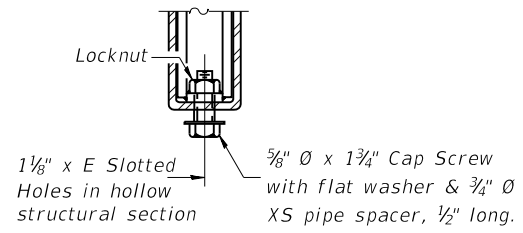


TOP RAIL

SECTIONS AT RAIL SPLICE



PLAN-BOTT. SPLICE TYPICAL



RAIL SPLICE CONNECTION AT EXPANSION JT.

Notes:  
 Posts shall not be located closer than 1'-3" to an existing bridge expansion joint or end of bridge.  
 Steel Bridge Rail expansion joint shall be provided between any two (2) posts which span a bridge expansion joint. Bolts located at expansion joint shall be provided with locknuts and shall be tightened only to a point that will allow railing movement.  
 Provide one 1/8" and two 1/16" steel shims for 25% of the posts. Shims shall be similar to base plates in size and holes.  
 All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

SPLICE DIMENSIONS

| T             | D      | A      | B      | C      | E      |
|---------------|--------|--------|--------|--------|--------|
| ≤ 4"          | 2 1/2" | 1'-8"  | 2"     | 4"     | 2 1/2" |
| > 4" ≤ 6 1/2" | 3 3/4" | 2'-0"  | 2 1/2" | 5 1/2" | 3 1/2" |
| > 6 1/2" ≤ 9" | 5"     | 2'-4"  | 3 1/2" | 6 1/2" | 9"     |
| > 9" ≤ 13"    | 7"     | 2'-10" | 4 1/2" | 8 1/2" | 11"    |
| Rail Splice   | 1/4"   | 1'-8"  | 2"     | 4"     | —      |

T = Total movement at expansion joint as shown on the design plans.

BILL OF MATERIAL

| Item                     | Unit | Quantity |
|--------------------------|------|----------|
| Steel Railing, Type 2399 | Foot | 369.0    |

R-31

8-11-2017

(6'-3" Maximum Post Spacing)

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

Steel Railing, Type 2399  
 S.N. 010-0183

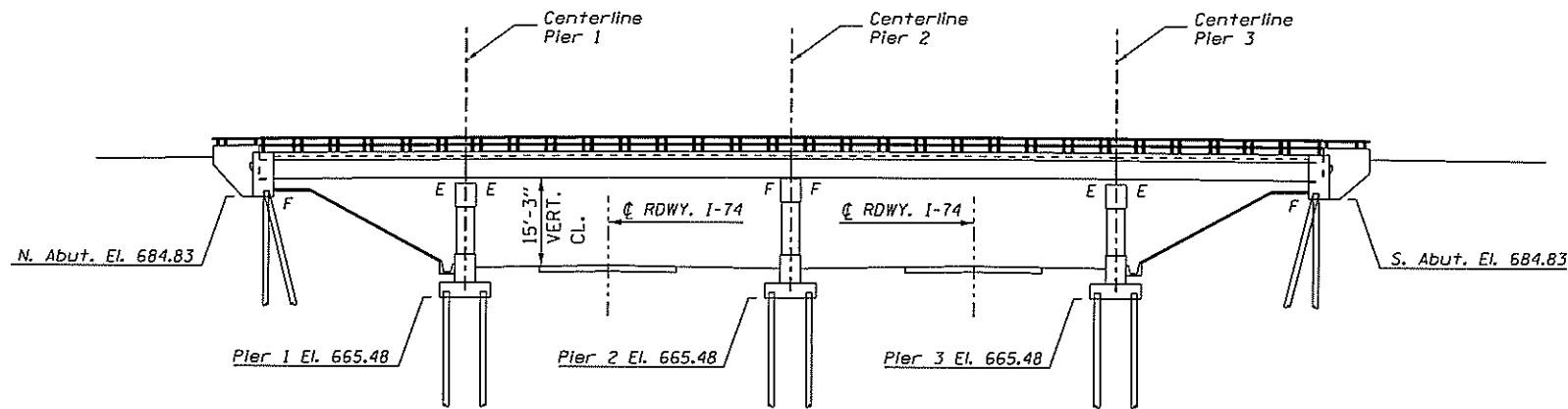
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|--------|--------------|-----------|
| I-74        | *       | **     | 142          | 88        |

SCALE: N.T.S. SHEET 4 OF 4 SHEETS STA. TO STA.

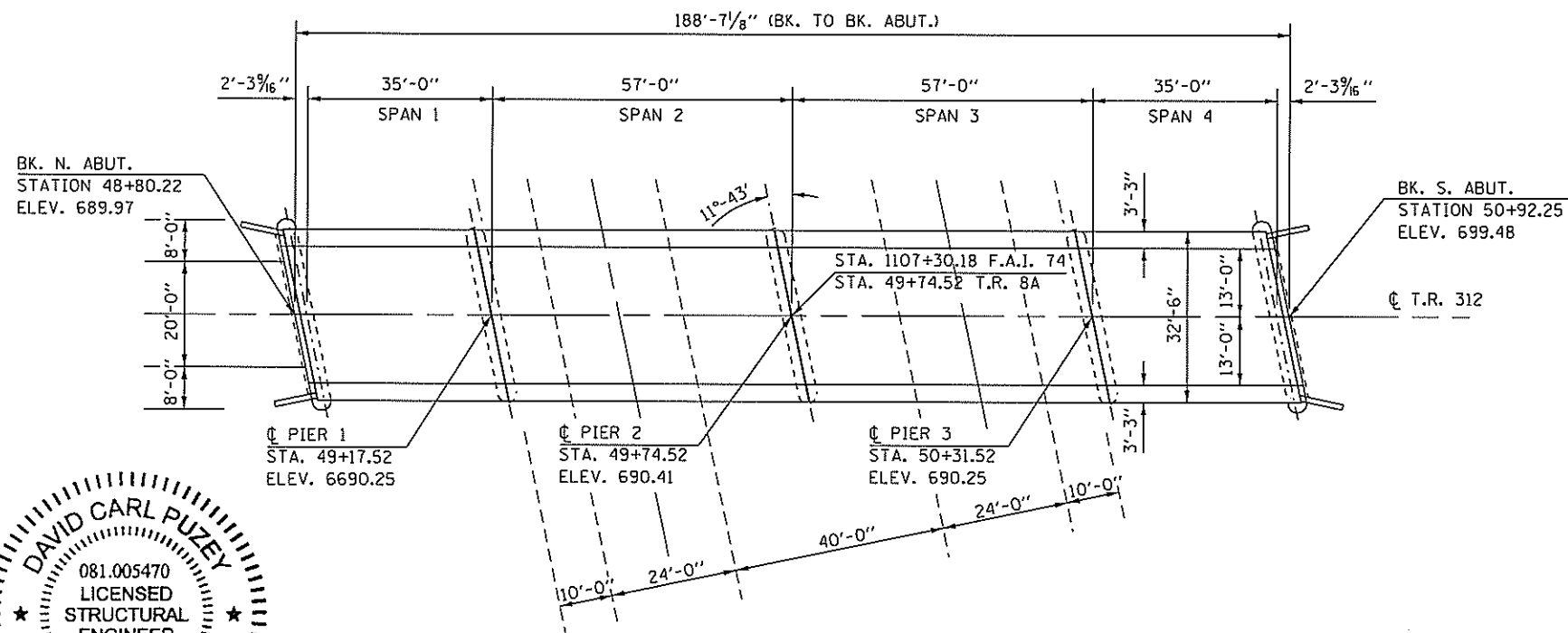
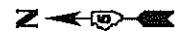
ILLINOIS FED. AID PROJECT CONTRACT NO. 70790

MODEL: Default  
 FILE: Model: 03/27/2019 Bridge-Sheets.dgn

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION



ELEVATION VIEW



PLAN VIEW

S.N. 092-0146 WAS BUILT IN 1959 AS FAI 5, SECTION 10,92-HB3 AT STATION 1107+30.18 IN VERMILION COUNTY. THE STRUCTURE IS A FOUR (4) SPAN, 7" THICK REINFORCED DECK WITH PRESTRESSED CONCRETE BEAMS ON STEEL CONCRETE PILE SUPPORTED PIERS AND ABUTMENTS.

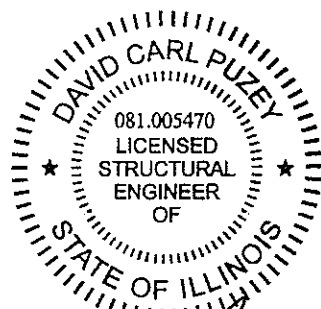
THE STRUCTURE HAS A BACK-TO-BACK ABUTMENT LENGTH OF 188'-7 1/8", OUT-TO-OUT WIDTH OF 32'-6", BRIDGE ROADWAY WIDTH OF 26'-0", AND AN APPROACH ROADWAY WIDTH OF 20'-0".

PROPOSED IMPROVEMENTS

- 1. PLACE STEEL BRIDGE RAILING, TYPE 2399

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.



*Dr. Carl Puzey* 3/18/19  
Expires 11/30/20

TOTAL BILL OF MATERIALS

| ITEM                     | UNITS | TOTAL |
|--------------------------|-------|-------|
| STEEL RAILING, TYPE 2399 | FOOT  | 377.0 |

MODEL: Default  
FILE NAME: ...0510790 Bridge Sheets.dgn

|                                    |                |           |
|------------------------------------|----------------|-----------|
| USER NAME = bemory                 | DESIGNED - ESS | REVISED - |
| PLOT SCALE = 40.0000' / in.        | DRAWN - ESS    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:04:56 PM | CHECKED - TJB  | REVISED - |
|                                    | DATE -         | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION  
S.N. 092-0146

SCALE: N.T.S. SHEET 1 OF 4 SHEETS STA. TO STA.

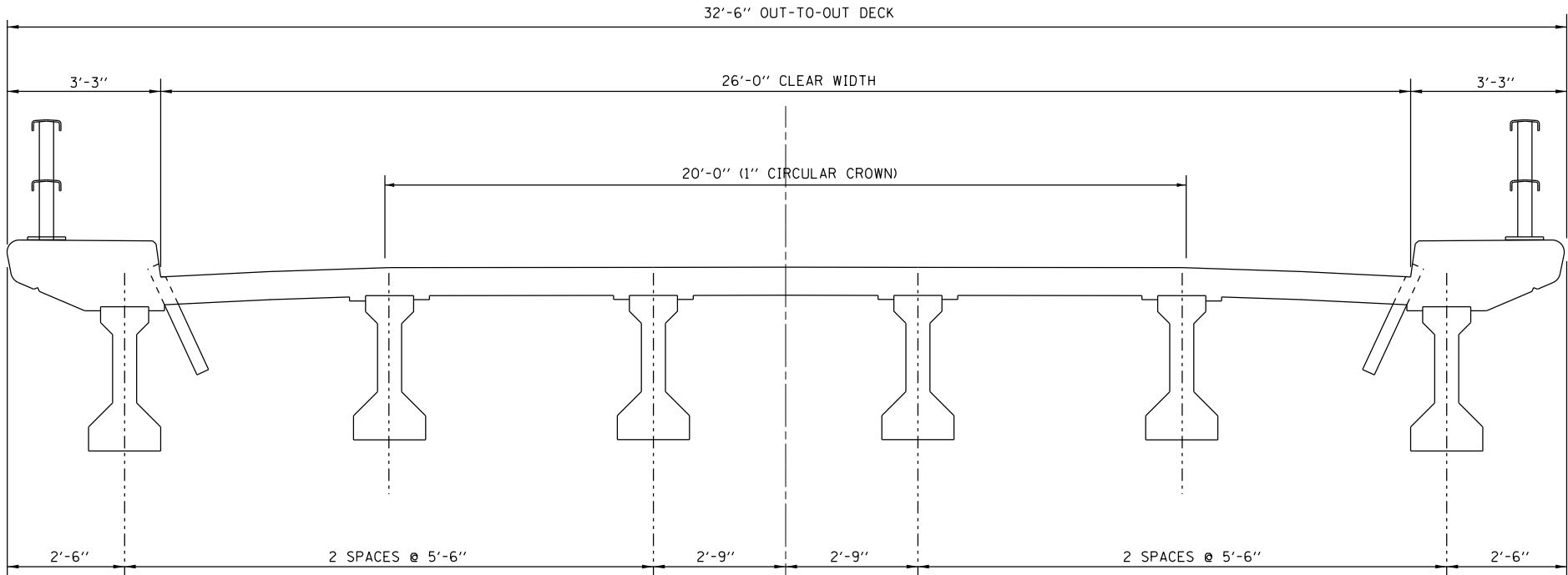
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|--------|--------------|-----------|
| I-74        |         | **     | 142          | 89        |

CONTRACT NO. 70790  
ILLINOIS FED. AID PROJECT  
\*\* CHAMPAIGN & VERMILION

(10-7,8;92-8.9)RS-2(92-10)RS-3

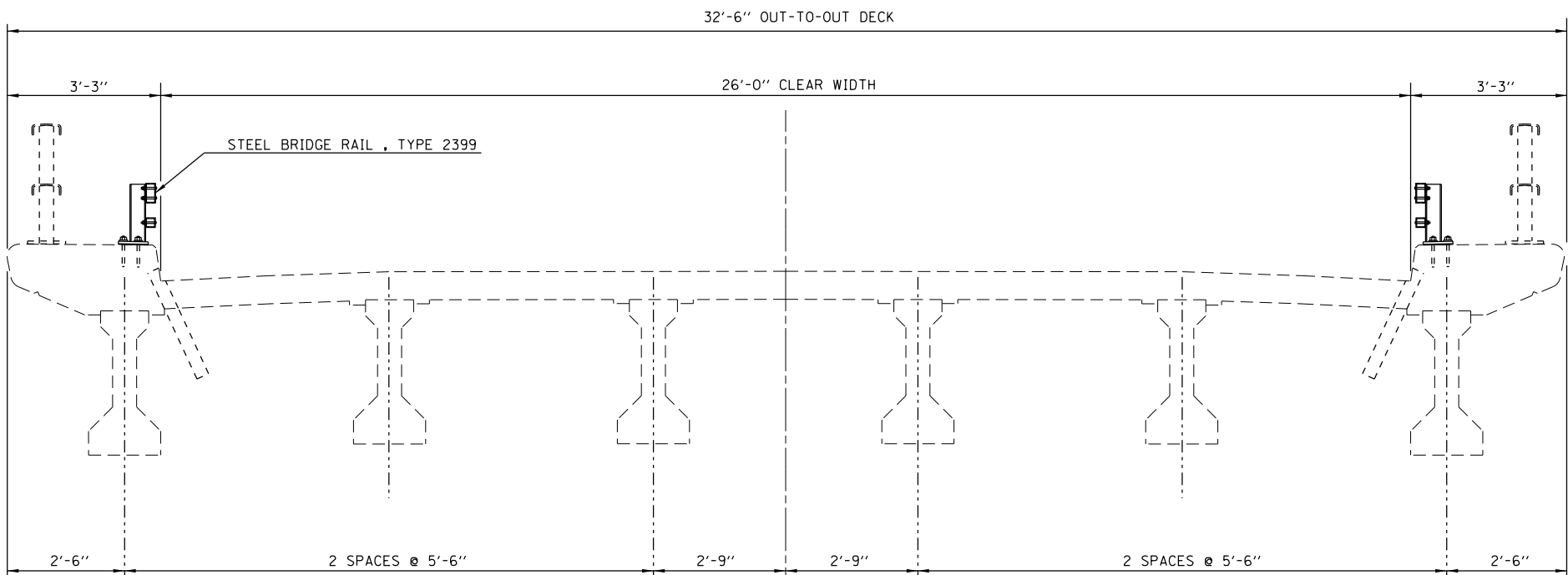
# EXISTING DECK CROSS SECTION

S.N. 092-0146



# PROPOSED DECK CROSS SECTION

S.N. 092-0146



MODEL: Default  
FILE: Model - 100570790\_Bridge\_Sheets.dgn

|                                    |                |           |
|------------------------------------|----------------|-----------|
| USER NAME = bemery                 | DESIGNED - JMS | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | DRAWN - JMS    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:04:56 PM | CHECKED - TJB  | REVISED - |
|                                    | DATE -         | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

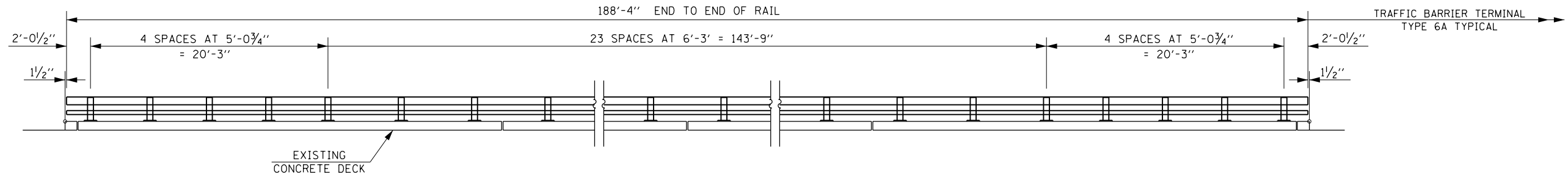
|                            |         |             |              |
|----------------------------|---------|-------------|--------------|
| <b>DECK CROSS SECTIONS</b> |         |             |              |
| <b>S.N. 092-0146</b>       |         |             |              |
| SCALE: N.T.S.              | SHEET 2 | OF 4 SHEETS | STA. TO STA. |

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| 1-74               | *       | **     | 142          | 90        |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

# STEEL RAILING, PLAN

## S.N. 092-0146



### ELEVATION VIEW EAST STEEL RAIL

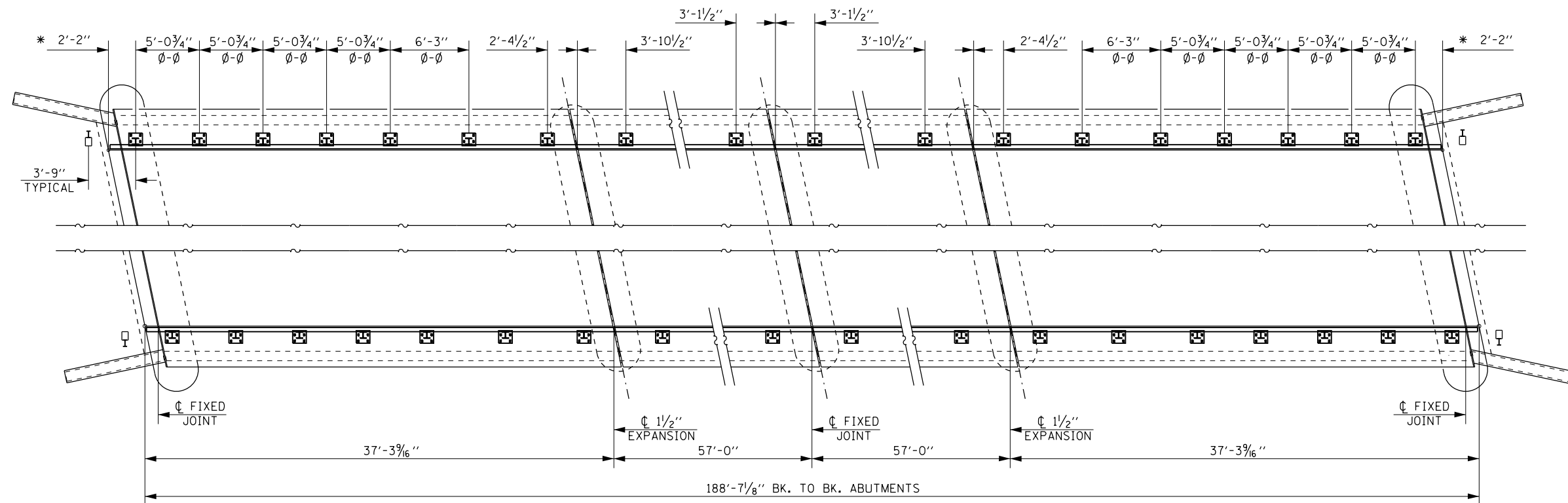
WEST STEEL RAIL TYPICAL

BASE PLATE

FOR ADDITIONAL DETAILS FOR  
STEEL RAILING SEE NEXT SHEET.

TRAFFIC BARRIER TERMINAL  
TYP 6A POST & BLOCKOUT.

\* CORNER OF CURB  
TO CENTER OF POST.



### PLAN VIEW STEEL RAILING

MODEL: Default  
FILE: I:\bldg\030570790\_Bldg\_Sheets.dwg

|                                    |                |           |
|------------------------------------|----------------|-----------|
| USER NAME = bemery                 | DESIGNED - ESS | REVISED - |
|                                    | DRAWN - ESS    | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | CHECKED - TJB  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:04:56 PM | DATE -         | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

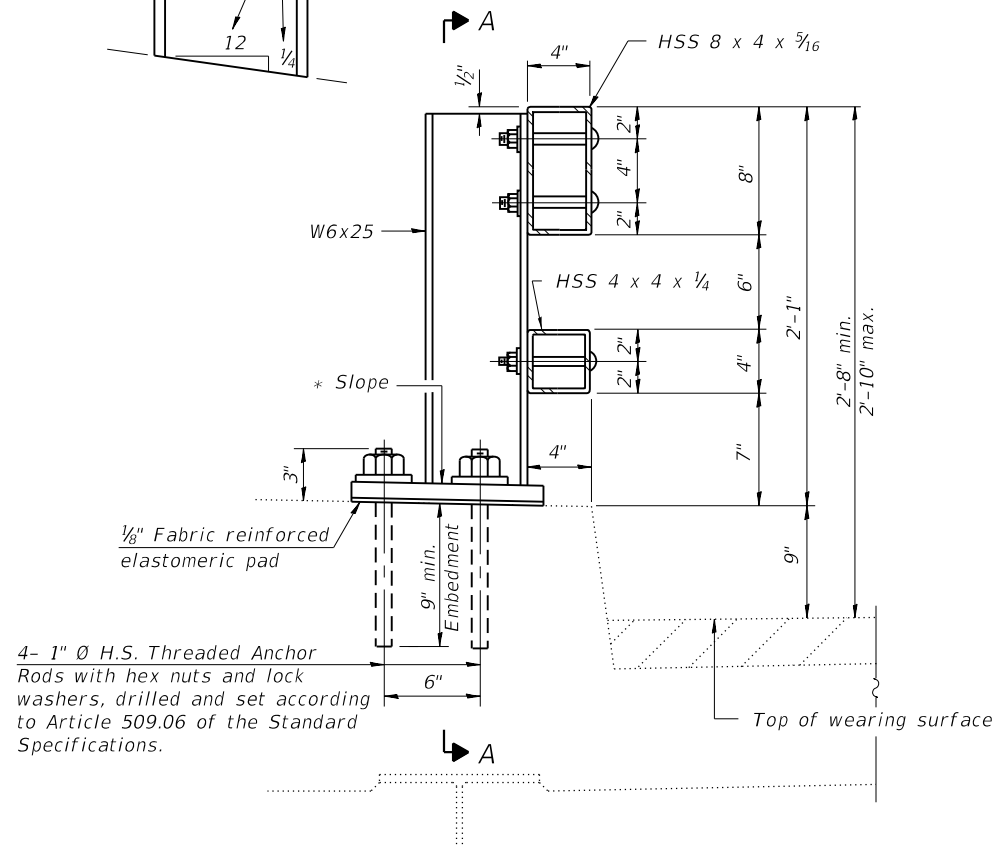
**STEEL RAILING PLAN  
S.N. 092-0146**

SCALE: N.T.S. SHEET 3 OF 4 SHEETS STA. TO STA.

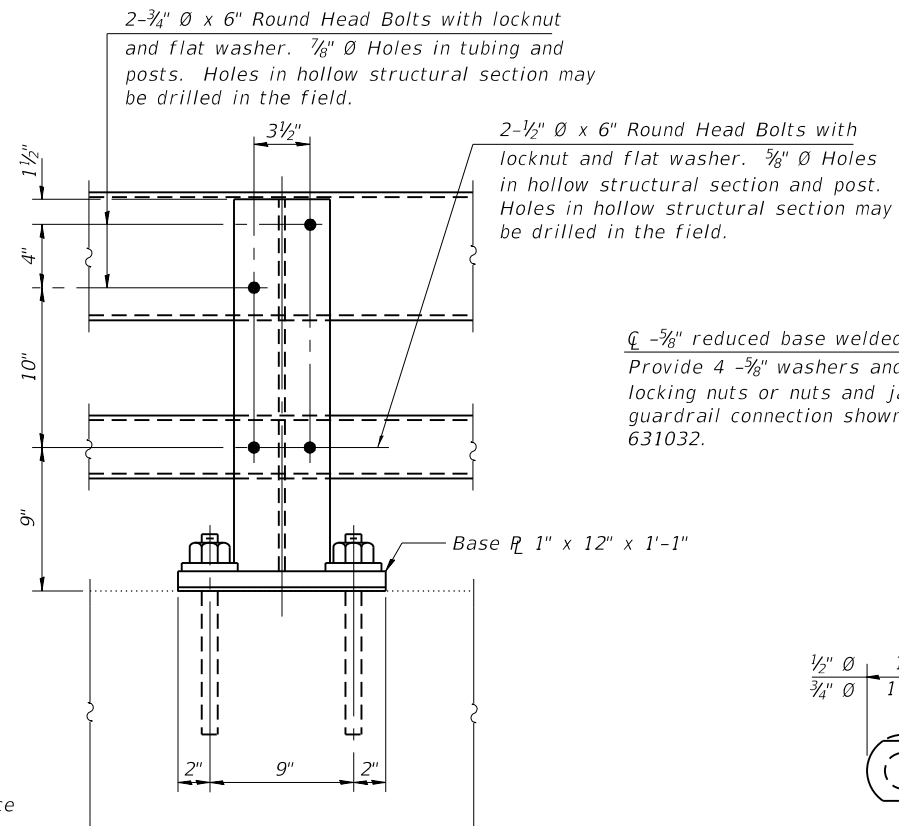
|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 91        |
| CONTRACT NO. 70790 |         |        |              |           |

ILLINOIS FED. AID PROJECT \* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

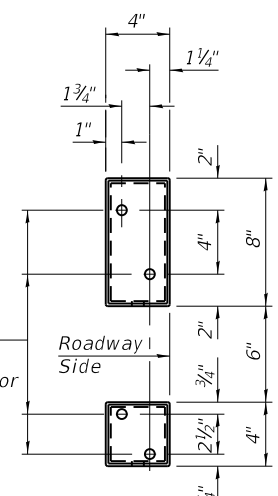
\* Cut bottom end of post to curb slope.



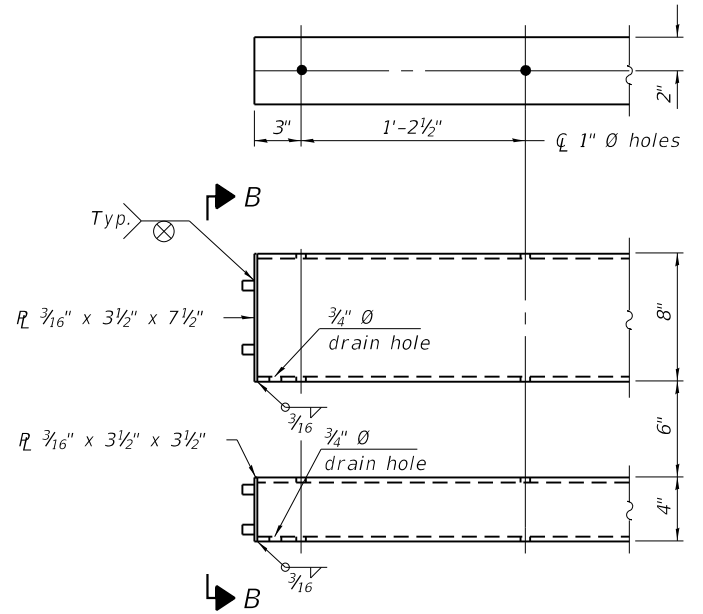
SECTION AT RAIL POST



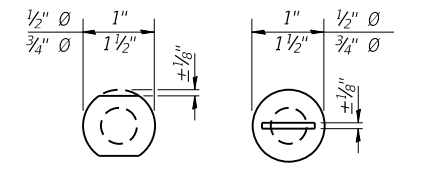
SECTION A-A



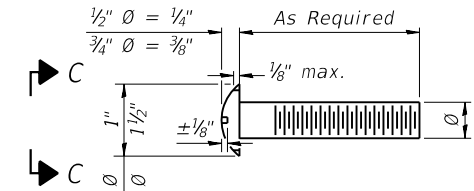
VIEW B-B



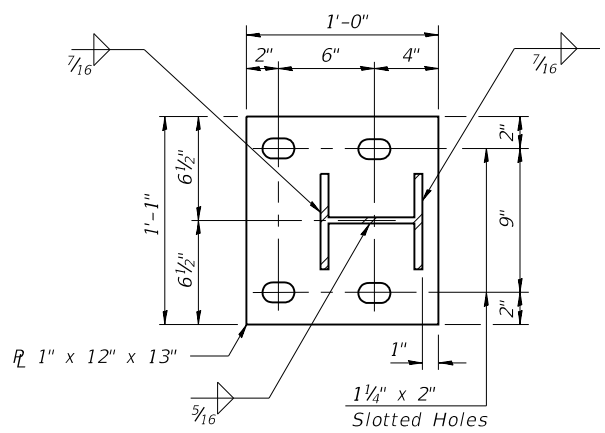
END OF RAIL DETAILS



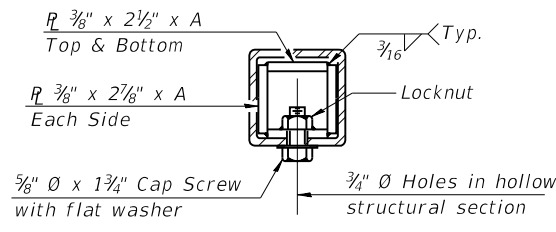
VIEW C-C



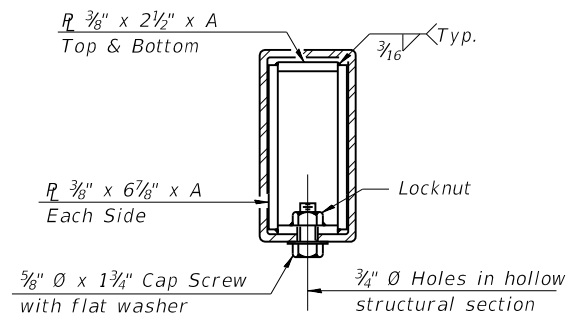
DETAIL OF 1/2" Ø & 3/4" Ø ROUND HEAD BOLTS



BASE PLATE DETAIL

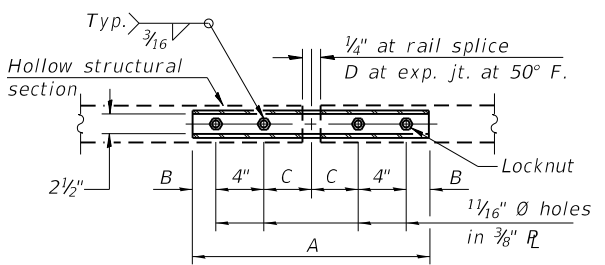


BOTTOM RAIL

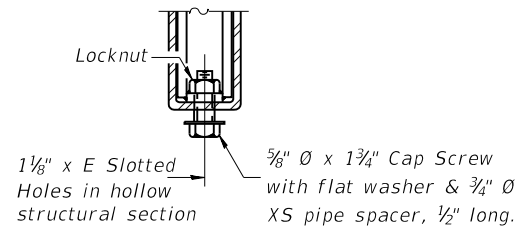


TOP RAIL

SECTIONS AT RAIL SPLICE



PLAN-BOTT. SPLICE TYPICAL



RAIL SPLICE CONNECTION AT EXPANSION JT.

Notes:  
 Posts shall not be located closer than 1'-3" to an existing bridge expansion joint or end of bridge.  
 Steel Bridge Rail expansion joint shall be provided between any two (2) posts which span a bridge expansion joint. Bolts located at expansion joint shall be provided with locknuts and shall be tightened only to a point that will allow railing movement.  
 Provide one 1/8" and two 1/16" steel shims for 25% of the posts. Shims shall be similar to base plates in size and holes.  
 All steel rail elements shall be galvanized according to Article 509.05 of the Standard Specifications.

SPLICE DIMENSIONS

| T             | D      | A      | B      | C      | E      |
|---------------|--------|--------|--------|--------|--------|
| ≤ 4"          | 2 1/2" | 1'-8"  | 2"     | 4"     | 2 1/2" |
| > 4" ≤ 6 1/2" | 3 3/4" | 2'-0"  | 2 1/2" | 5 1/2" | 3 1/2" |
| > 6 1/2" ≤ 9" | 5"     | 2'-4"  | 3 1/2" | 6 1/2" | 9"     |
| > 9" ≤ 13"    | 7"     | 2'-10" | 4 1/2" | 8 1/2" | 11"    |
| Rail Splice   | 1/4"   | 1'-8"  | 2"     | 4"     | —      |

T = Total movement at expansion joint as shown on the design plans.

BILL OF MATERIAL

| Item                     | Unit | Quantity |
|--------------------------|------|----------|
| Steel Railing, Type 2399 | Foot | 377.0    |

R-31

8-11-2017

(6'-3" Maximum Post Spacing)

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

STEEL RAILING, TYPE 2399  
 S.N. 092-0146

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|---------|--------|--------------|-----------|
| I-74        | *       | **     | 142          | 92        |

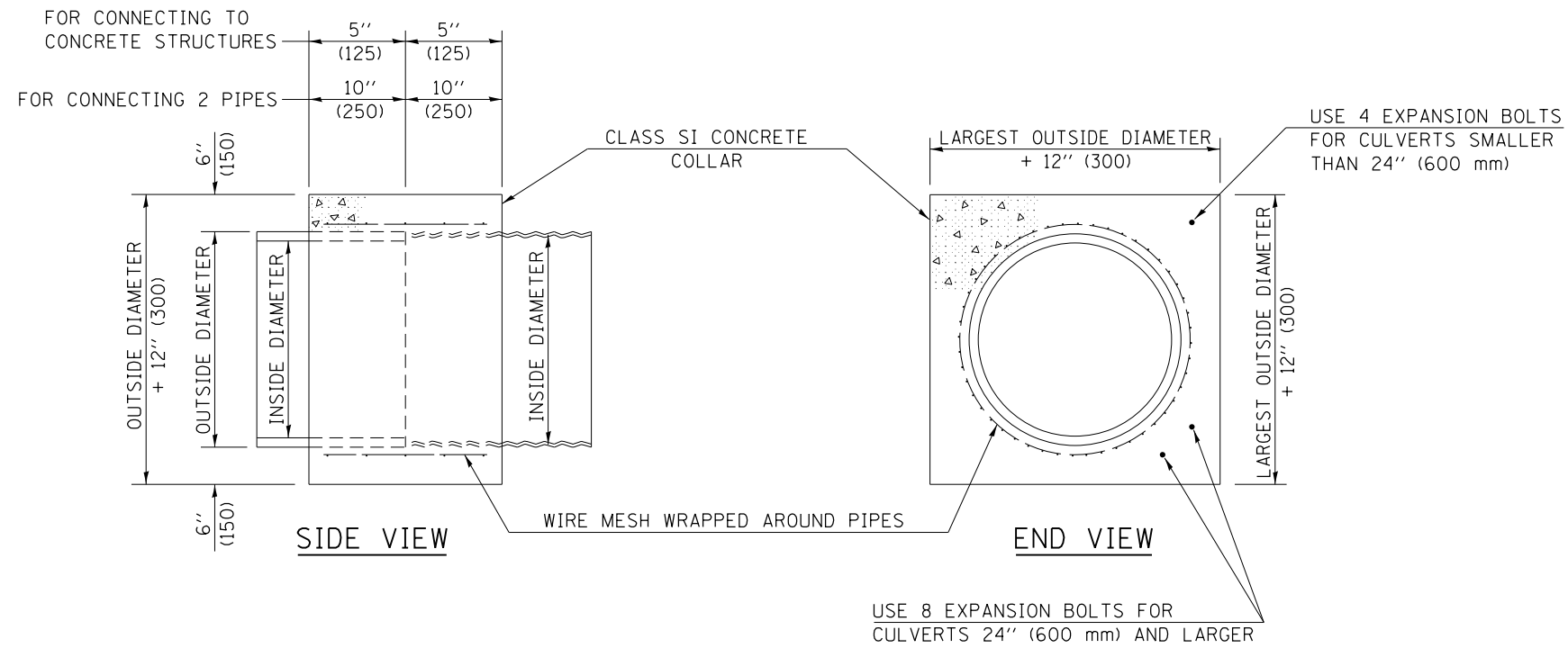
SCALE: N.T.S. SHEET 4 OF 4 SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT CONTRACT NO. 70790

MODEL: Default  
 FILE: Model: 03/27/2019 Bridge-Sheets.dgn

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION





**GENERAL NOTES**

1. CLASS SI CONCRETE SHALL BE USED THROUGHOUT.
2. WHEN CONCRETE COLLARS ARE USED TO CONNECT PIPES OF DIFFERENT OUTSIDE DIAMETERS, THE CONCRETE COLLAR SHALL BE FORMED USING THE LARGEST OUTSIDE DIAMETER (SEE END VIEW).
3. THE WIRE MESH SHALL WEIGH NOT LESS THAN 54#/100 SQ. FT. (2.63 kg/m<sup>2</sup>).
4. WHEN CONCRETE COLLARS ARE CONSTRUCTED ADJACENT TO AN EXISTING CONCRETE STRUCTURE (HEADWALLS, ETC.) EXPANSION BOLTS, SHALL BE USED AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE, EACH, FOR EXPANSION BOLTS OF THE SIZE SPECIFIED IN THE PLANS.
5. CONCRETE COLLARS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE, PER CUBIC YARD (CUBIC METER), FOR CONCRETE COLLARS INCLUDING ALL MATERIAL AND LABOR SPECIFIED TO COMPLETE THE WORK IN PLACE.

| QUANTITIES FOR CONCRETE PIPES |   |
|-------------------------------|---|
| INSIDE DIAMETER OF PIPE       | ESTIMATED CLASS SI CONCRETE REQUIRED            |
| INCH (mm)                     | 20" (500 mm) WIDTH<br>CU. YD. (m <sup>3</sup> ) |
| 4" (100)                      | 0.14 (0.11)                                     |
| 6" (150)                      | 0.16 (0.12)                                     |
| 8" (200)                      | 0.19 (0.14)                                     |
| 10" (250)                     | 0.22 (0.17)                                     |
| 12" (300)                     | 0.25 (0.19)                                     |
| 15" (375)                     | 0.30 (0.23)                                     |
| 18" (450)                     | 0.35 (0.27)                                     |
| 24" (600)                     | 0.45 (0.35)                                     |
| 30" (750)                     | 0.57 (0.43)                                     |
| 36" (900)                     | 0.69 (0.53)                                     |
| 42" (1050)                    | 0.83 (0.63)                                     |
| 48" (1200)                    | 0.97 (0.74)                                     |
| 54" (1350)                    | 1.12 (0.86)                                     |
| 60" (1500)                    | 1.28 (0.98)                                     |

| QUANTITIES FOR METAL PIPES |   |
|----------------------------|---|
| INSIDE DIAMETER OF PIPE    | ESTIMATED CLASS SI CONCRETE REQUIRED            |
| INCH (mm)                  | 20" (500 mm) WIDTH<br>CU. YD. (m <sup>3</sup> ) |
| 4" (100)                   | 0.12 (0.09)                                     |
| 6" (150)                   | 0.14 (0.11)                                     |
| 8" (200)                   | 0.16 (0.12)                                     |
| 10" (250)                  | 0.19 (0.14)                                     |
| 12" (300)                  | 0.21 (0.16)                                     |
| 15" (375)                  | 0.25 (0.19)                                     |
| 18" (450)                  | 0.29 (0.22)                                     |
| 24" (600)                  | 0.38 (0.29)                                     |
| 30" (750)                  | 0.47 (0.36)                                     |
| 36" (900)                  | 0.59 (0.45)                                     |
| 42" (1050)                 | 0.69 (0.53)                                     |
| 48" (1200)                 | 0.81 (0.62)                                     |
| 54" (1350)                 | 0.93 (0.71)                                     |
| 60" (1500)                 | 1.05 (0.81)                                     |

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 54248510**

MODEL: Default  
FILE: Model\_54248510-54248510.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40.0000 * / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:57 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CONCRETE COLLAR**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

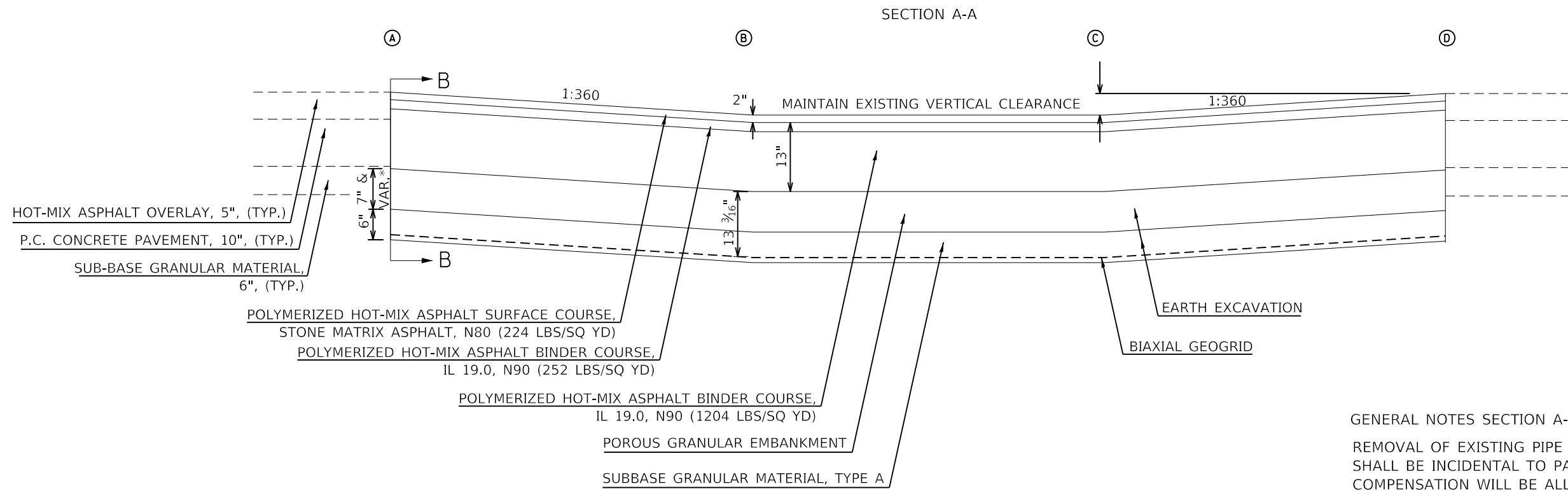
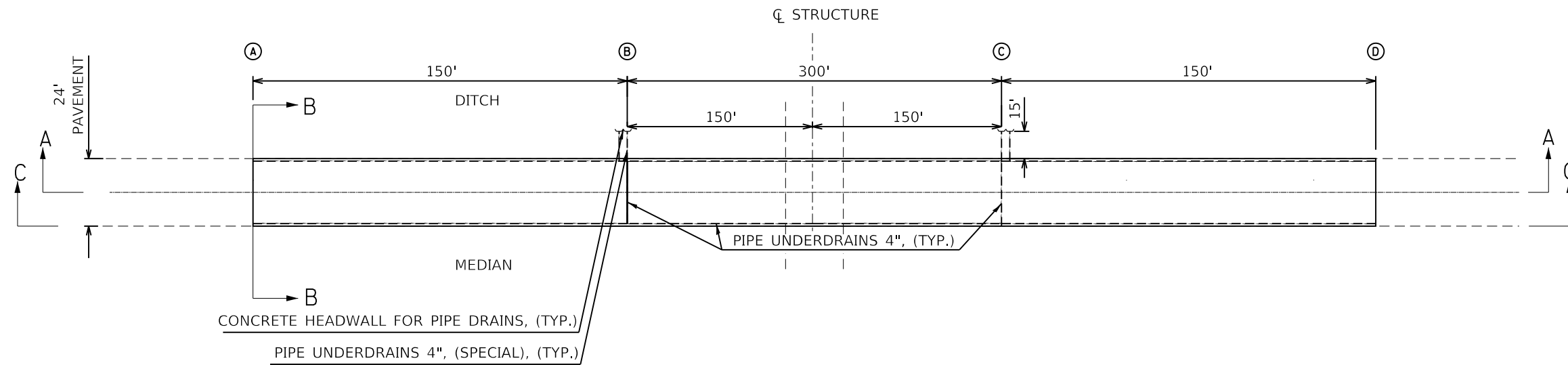
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 93        |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

# DETAIL OF RESURFACING UNDERNEATH

SN 010-0183  
SN 092-0146



**GENERAL NOTES SECTION A-A**

REMOVAL OF EXISTING PIPE UNDERDRAINS AND EARTH EXCAVATION SHALL BE INCIDENTAL TO PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED

\*POROUS GRANULAR EMBANKMENT DEPTH WILL VARY BASED ON CROSS SECTION. SEE SECTION B-B. THE AGGREGATE SHALL BE A CA7 CRUSHED STONE OF QUALITY D OR BETTER.

EXISTING VERTICAL CLEARANCE SHALL NOT BE REDUCED.

| S.N.     | A          | B          | Q STATION  | C          | D          |
|----------|------------|------------|------------|------------|------------|
| 010-0183 | 898+29.00  | 899+79.00  | 901+29.00  | 902+79.00  | 904+29.00  |
| 092-0146 | 1104+30.18 | 1105+80.18 | 1107+30.18 | 1108+80.18 | 1110+30.18 |

MODEL: Default  
FILE: Model.dwg  
PLOT DATE: 1/24/2019 4:05:57 PM

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|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40.0000' / in.        | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:57 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAIL OF RESURFACING  
UNDERNEATH OVERHEAD STRUCTURES**

SCALE: N.T.S.      SHEET OF SHEETS      STA. TO STA.

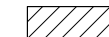

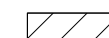
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 94        |
| CONTRACT NO. 70790 |         |        |              |           |

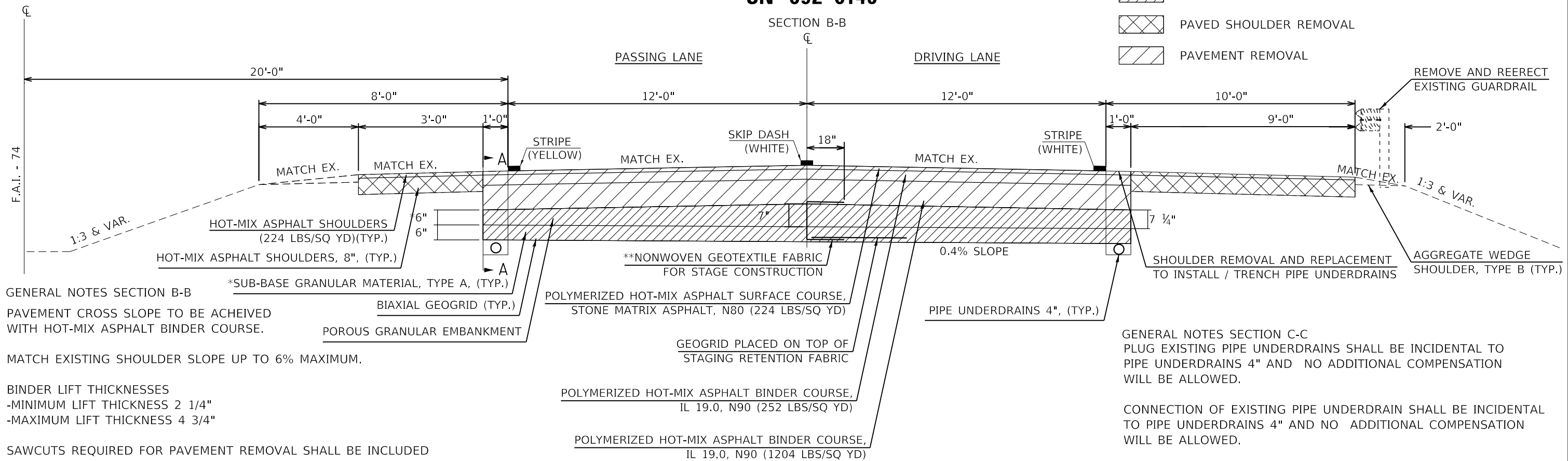
\* (10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

# DETAIL OF RESURFACING UNDERNEATH

SN 010-0183  
SN 092-0146

-  EARTH EXCAVATION
-  PAVED SHOULDER REMOVAL
-  PAVEMENT REMOVAL



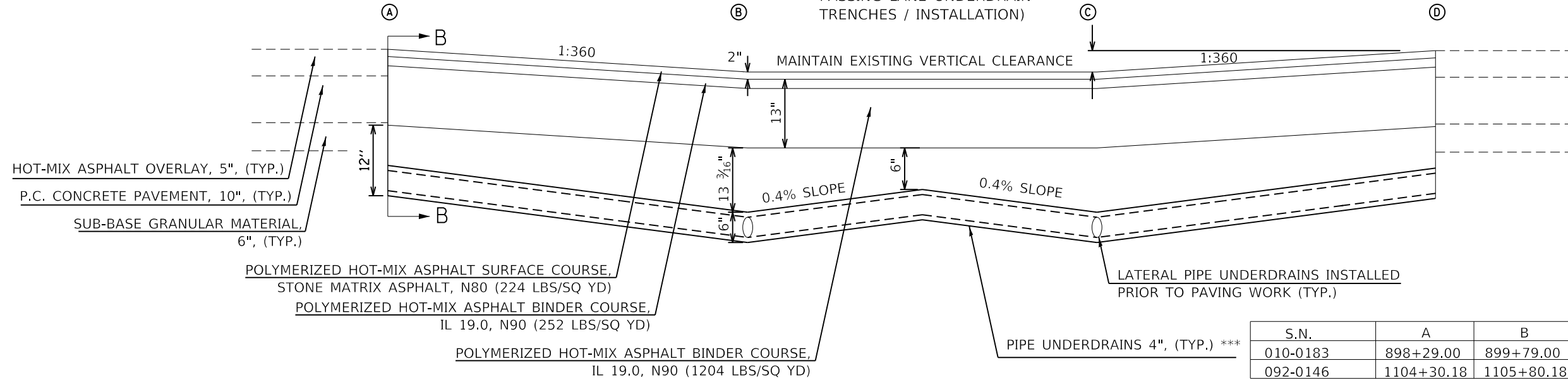
**GENERAL NOTES SECTION B-B**

- PAVEMENT CROSS SLOPE TO BE ACHIEVED WITH HOT-MIX ASPHALT BINDER COURSE.
- MATCH EXISTING SHOULDER SLOPE UP TO 6% MAXIMUM.
- BINDER LIFT THICKNESSES
  - MINIMUM LIFT THICKNESS 2 1/4"
  - MAXIMUM LIFT THICKNESS 4 3/4"
- SAWCUTS REQUIRED FOR PAVEMENT REMOVAL SHALL BE INCLUDED IN THE COST OF PAVEMENT REMOVAL.
- \* EXISTING SUBGRADE SHALL BE EVALUATED BY THE ENGINEER TO ASSESS THE NECESSARY DEPTH OF SUB-BASE GRANULAR MATERIAL FOR STABILITY.
- \*\* A NONWOVEN GEOTEXTILE FABRIC MEETING THE REQUIREMENTS OF ARTICLE 1080.02 SHALL BE PLACED TO RETAIN THE AGGREGATE SUBGRADE MATERIAL. THE MATERIAL SHALL BE PLACED BELOW THE GEOGRID. COST INCLUDED IN VARIOUS AGGREGATE PAY ITEMS.

**GENERAL NOTES SECTION C-C**

- PLUG EXISTING PIPE UNDERDRAINS SHALL BE INCIDENTAL TO PIPE UNDERDRAINS 4" AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- CONNECTION OF EXISTING PIPE UNDERDRAIN SHALL BE INCIDENTAL TO PIPE UNDERDRAINS 4" AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- REMOVAL OF EXISTING PIPE UNDERDRAINS AND EARTH EXCAVATION SHALL BE INCIDENTAL TO PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- EXISTING VERTICAL CLEARANCE SHALL NOT BE REDUCED.
- \*\*\*THE FM4 OR FM4 SPECIAL AGGREGATE FOR BACKFILLING THE PIPE UNDERDRAIN TRENCH SHALL BE INCLUDED IN THE PIPE UNDERDRAINS, 4" UNIT PRICE.

**SECTION C-C**  
(SHOWING DRIVING LANE & PASSING LANE UNDERDRAIN TRENCHES / INSTALLATION)



| S.N.     | A          | B          | CL. STATION | C          | D          |
|----------|------------|------------|-------------|------------|------------|
| 010-0183 | 898+29.00  | 899+79.00  | 901+29.00   | 902+79.00  | 904+29.00  |
| 092-0146 | 1104+30.18 | 1105+80.18 | 1107+30.18  | 1108+80.18 | 1110+30.18 |

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| PLOT SCALE = 40.0000 ' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:57 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DETAIL OF RESURFACING**  
**UNDERNEATH OVERHEAD STRUCTURES**

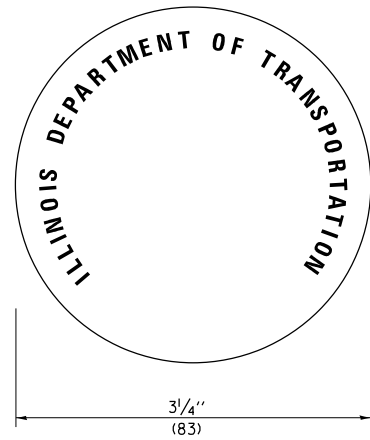
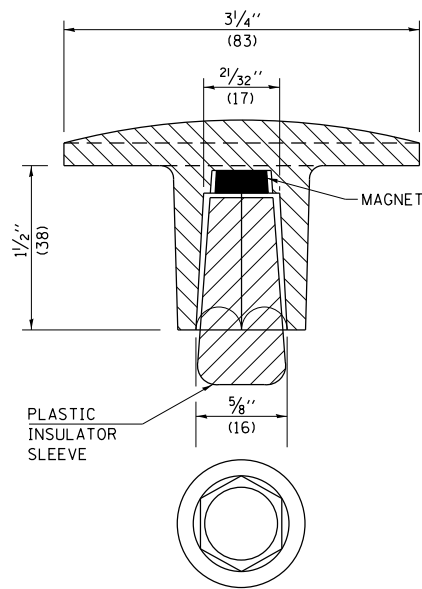
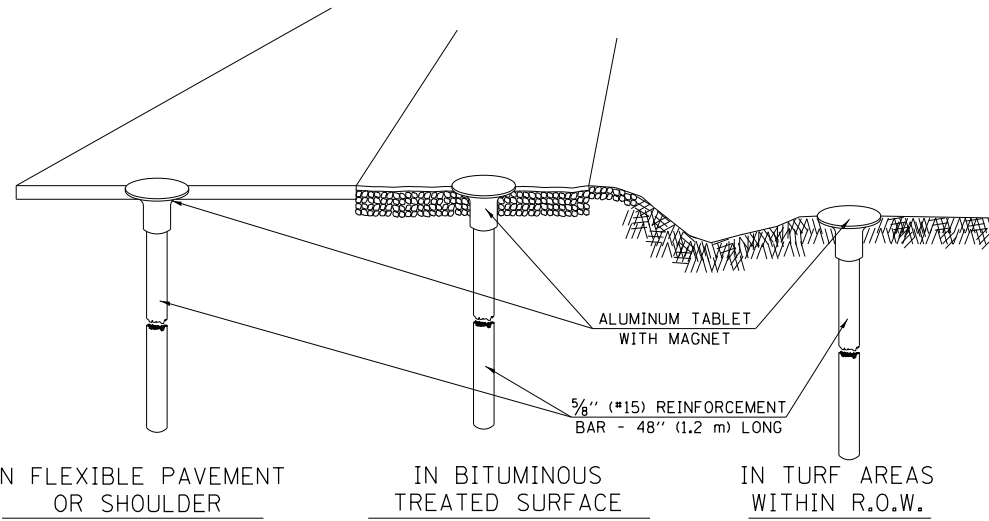
SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|                    |           |           |                           |              |
|--------------------|-----------|-----------|---------------------------|--------------|
| F.A.I. RTE. I-74   | SECTION * | COUNTY ** | TOTAL SHEETS 142          | SHEET NO. 95 |
| CONTRACT NO. 70790 |           |           | ILLINOIS FED. AID PROJECT |              |

DESIGNER NOTE: BDE 58-8-02 "PLACE MARKERS AT THE PT'S AND PC'S OF ALL HORIZONTAL CURVES AND SPACE THEM ALONG TANGENTS SO THAT TWO MARKERS ARE ALWAYS INTERVISIBLE."

## XZ193300 – SURVEY MARKER, TYPE 1 (SPECIAL)

TO BE INSTALLED IN FLEXIBLE PAVEMENT OR SHOULDER, BITUMINOUS TREATED SURFACE AND TURF AREAS WITHIN THE RIGHT-OF-WAY FOR PRESERVING PERMANENT SURVEY MARKERS (PI'S, PT'S, PC'S, POC'S, & POT'S)



THE DIMENSIONS SHOWN SHALL BE EXACT, OTHERS MAY VARY, BUT SHALL BE SHOWN ON SHOP DRAWINGS.

### GENERAL NOTES

1. THE CONTRACT UNIT PRICE, EACH, FOR SURVEY MARKER, TYPE 1 (SPECIAL) SHALL BE PAYMENT IN FULL FOR FURNISHING THE REINFORCEMENT BAR AND ALUMINUM TABLET AND FOR ALL LABOR AND MATERIAL REQUIRED TO SET THE MARKER IN PLACE.
2. ALL SURVEY MARKERS, TYPE 1 (SPECIAL) SHALL BE PLACED ± 1/4" (6 mm) BELOW THE FINAL SURFACE.
3. WHEN THE TABLET AND REBAR ARE PLACED AS PART OF A SURVEY MARKER VAULT, THEY SHALL BE CONSIDERED AS INCLUDED IN THAT PAY ITEM AND THERE WILL BE NO PAYMENT FOR THE SURVEY MARKER, TYPE 1 (SPECIAL).

### SPECIFICATIONS FOR ALUMINUM TABLET

SURVEY CAP FOR REBAR. 3/4" (83 mm) CONVEX SURVEY CAP FOR 5/8" (15 mm) REBAR WITH ILLINOIS DEPARTMENT OF TRANSPORTATION LOGO. THIS LOGO SHALL PROVIDE LETTERS RECESSED INTO THE SURFACE A MINIMUM OF 1/32" (0.8 mm) FOR EASY AND LONG-TERM LEGIBILITY. THE ALUMINUM CAP FOR REBAR SHALL BE PRODUCED BY THE PROCESS OF ORBITAL FORGING TO PRODUCE A HIGH-STRENGTH AND DURABLE MARKER CAP WHICH WILL NOT CHIP OR BREAK AND PROVIDE A SMOOTH FINISH FOR STAMPING OF DATA IN THE FIELD. THE ALUMINUM CAP FOR REBAR SHALL BE TAPERED FOR A PERFECT COMPRESSION FIT. A SPECIAL PLASTIC INSULATOR SHALL BE INSTALLED TO PREVENT DISSIMILAR METAL CONTACT AND CORROSION. THE PLASTIC INSULATOR SHALL FORM READILY TO THE OUTER SHAPE OF THE REBAR AND TO THE INNER SHAPE OF THE ALUMINUM CAP SOCKET. THE PLASTIC INSULATOR SHALL BE LOW DENSITY POLYETHYLENE, A MINIMUM 1 1/2" (38 mm) LONG AND CONFORM TO FEDERAL SPECIFICATION L-P 390.

COMPOSITION: ALUMINUM 98.3-98.7%; OTHER 1.3-1.7%; STRENGTH: YIELD 28 KSI (193 MPa), ULTIMATE 32 KSI (221 MPa). ELONGATION 15% [IN 2" (50 mm)]. SPECIFICATIONS: ALUMINUM ALLOY 6101-0; ASTM B317-83 (EXCEPT TEMPER) AS FORGED. NO EXCEPTIONS.

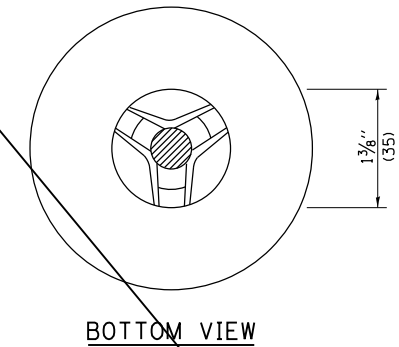
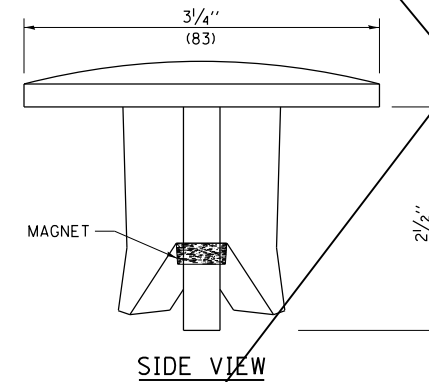
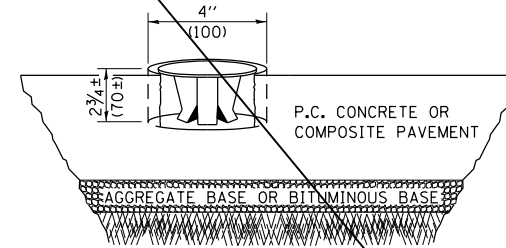
### SPECIFICATIONS FOR REBAR

REBAR FOR ALUMINUM TABLET. REINFORCEMENT BAR SHALL BE 5/8" (#15) X 48" (1.2 m) (DEFORMED).

INSPECTION OF REINFORCEMENT BAR 5/8" (#15) SHALL BE DONE BY DISTRICT PERSONNEL OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF HIGHWAYS.

## XZ193400 – SURVEY MARKER, TYPE 2 (SPECIAL)

TO BE INSTALLED IN RIGID OR COMPOSITE PAVEMENT FOR PRESERVING PERMANENT SURVEY MARKERS (PI'S, PT'S, PC'S, POC'S, & POT'S)



THE DIMENSIONS SHOWN SHALL BE EXACT, OTHERS MAY VARY, BUT SHALL BE SHOWN ON SHOP DRAWINGS.

### GENERAL NOTES

1. WORK ON THIS ITEM SHALL NOT START UNTIL THE FINAL SURFACE IS COMPLETED.
2. THE ALUMINUM TABLET (FORKED) SHALL REST UPON THE BOTTOM OF THE 4" (100 mm) CORE HOLE. IF THE HOLE IS TOO DEEP, EPOXY GROUT MUST BE USED TO DECREASE THE DEPTH AND ALLOWED TO HARDEN BEFORE PROCEEDING.
3. THE ALUMINUM TABLET SHALL BE ANCHORED IN THE 4" (100 mm) DIAMETER HOLE IN THE NEW PAVEMENT WITH TWO-COMPONENT EPOXY CONFORMING TO APPLICABLE PORTIONS OF ARTICLE 1025.01 OF THE STANDARD SPECIFICATIONS.
4. THE 4" (100 mm) CORE HOLE SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.
5. THE CONTRACT PRICE, EACH, FOR SURVEY MARKER, TYPE 2 (SPECIAL) SHALL BE PAYMENT IN FULL FOR FURNISHING THE ALUMINUM TABLET AND FOR ALL LABOR AND MATERIAL REQUIRED TO SET THE MARKER IN PLACE, AS SPECIFIED, INCLUDING CORING THE NEW PAVEMENT.
6. ALL SURVEY MARKERS, TYPE 2 (SPECIAL) SHALL BE PLACED ± 1/4" (6 mm) BELOW THE FINAL SURFACE.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

### DISTRICT 5 DETAIL NO. XZ193AAA

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:58 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

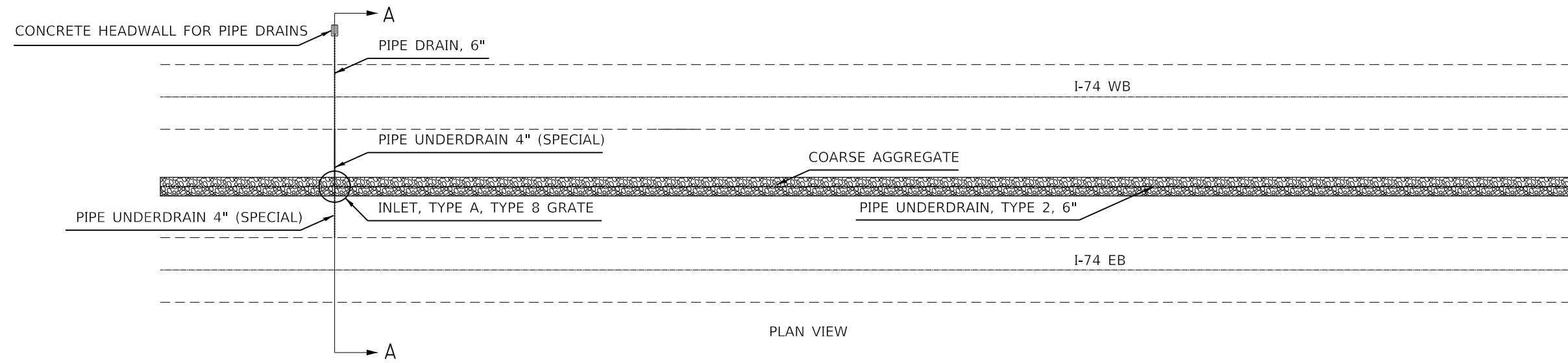
SURVEY MARKERS TYPE 1 & 2 (SPECIAL)

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

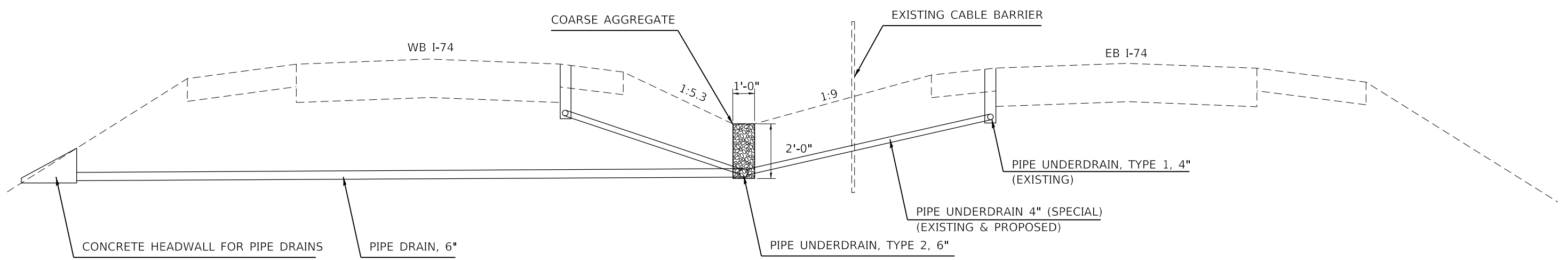
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 96        |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

# MEDIAN DRAINAGE REMEDIATION



PLAN VIEW



SECTION A-A

MODEL: Default  
FILE: Median\_03051905-shc-drt-1111.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:58 PM | DATE -     | REVISED - |

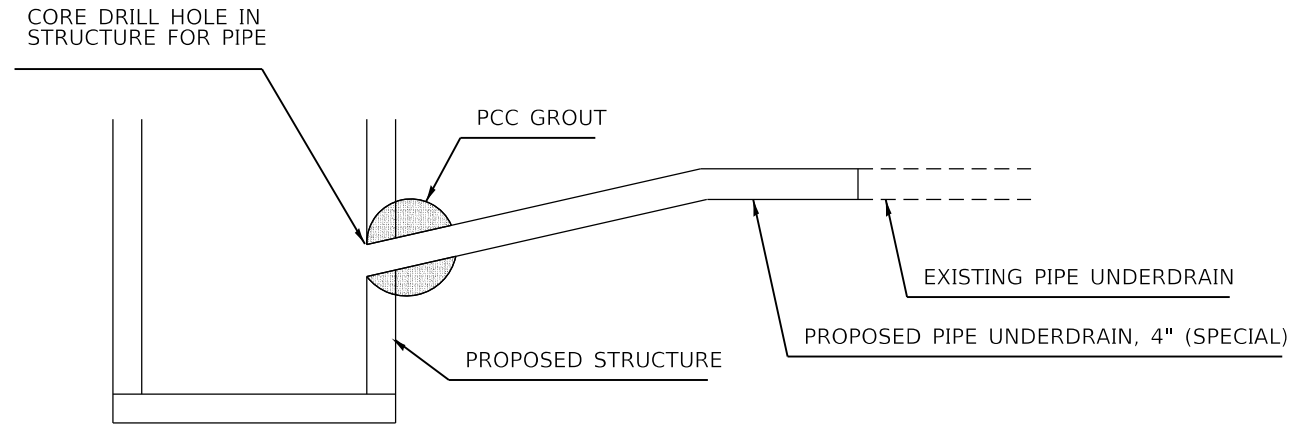
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                                    |       |           |              |
|------------------------------------|-------|-----------|--------------|
| <b>MEDIAN DRAINAGE REMEDIATION</b> |       |           |              |
| SCALE: N.T.S.                      | SHEET | OF SHEETS | STA. TO STA. |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 97        |
| CONTRACT NO. 70790 |         |        |              |           |

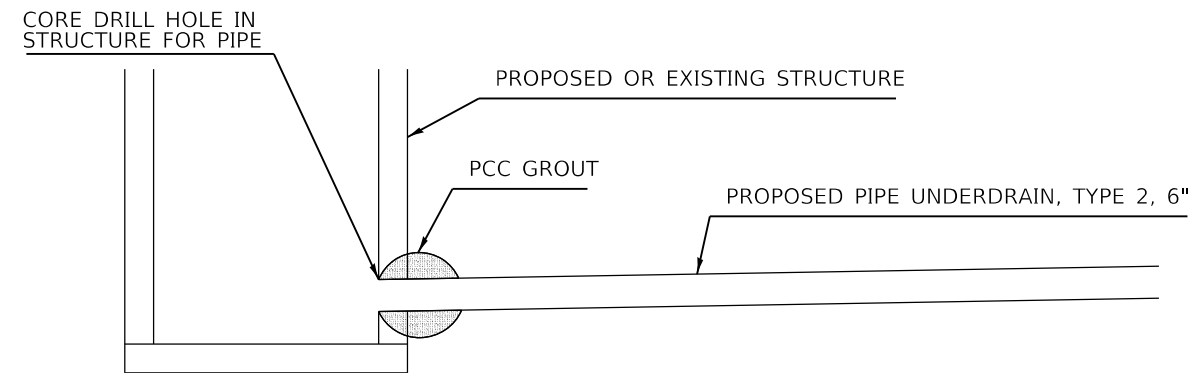
\* (10-7,8;92-8,9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION

## MEDIAN DRAINAGE REMEDIATION



NOTE:  
 ALL ITEMS REQUIRED FOR CONNECTING PROPOSED  
 PIPE UNDERDRAINS 4" (SPECIAL) TO STRUCTURE  
 WILL BE PAID FOR AT THE CONTRACT UNIT PRICE  
 EACH FOR UNDERDRAIN CONNECTION TO STRUCTURE.

## DETAIL OF CONNECTING PIPE UNDERDRAIN 4" (SPECIAL) TO STRUCTURE



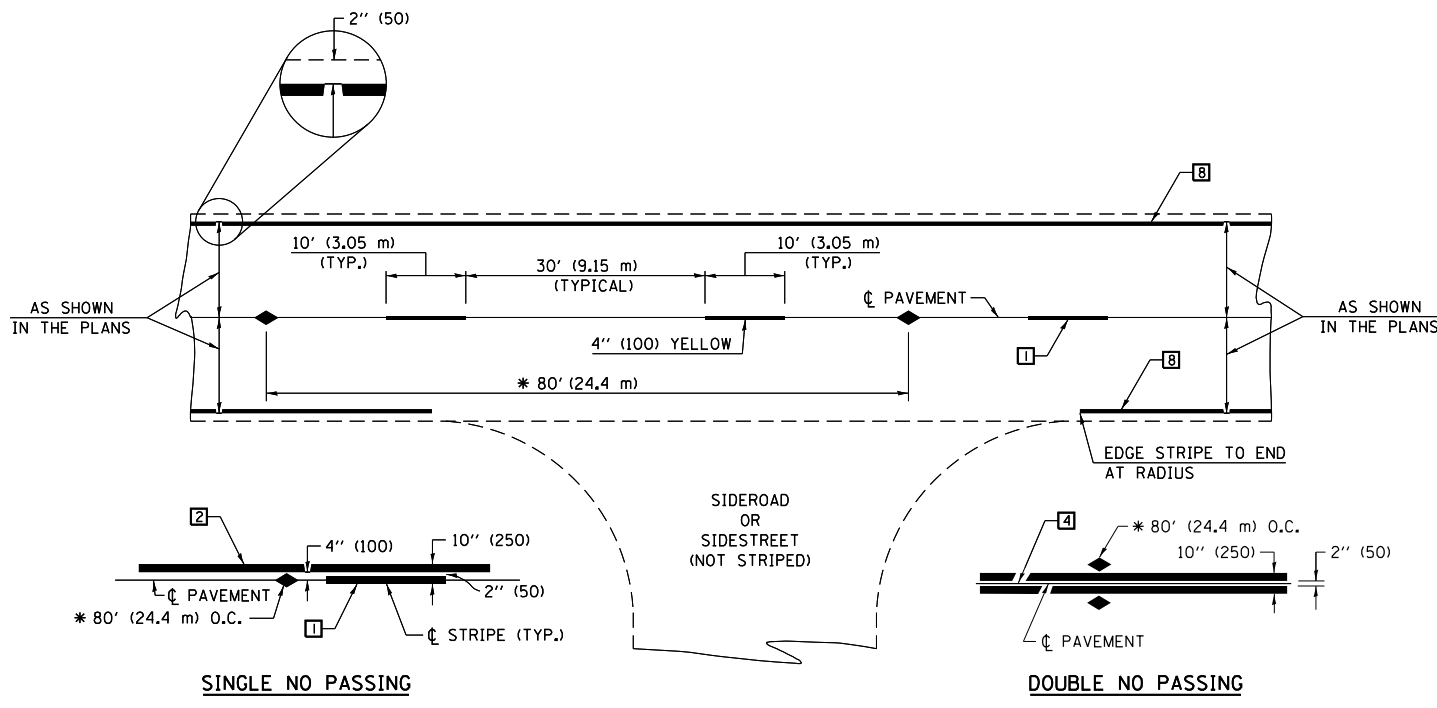
NOTE:  
 ALL ITEMS REQUIRED FOR CONNECTING PROPOSED  
 PIPE UNDERDRAINS, TYPE 2, 6" TO STRUCTURE  
 WILL BE PAID FOR AT THE CONTRACT UNIT PRICE  
 EACH FOR UNDERDRAIN CONNECTION TO STRUCTURE.

MODEL: Default  
FILE: Median\_03/27/2019-shc-drt-111.dgn

|  |                                    |            |           |   |                                    |                |         |                    |                 |              |
|--|------------------------------------|------------|-----------|---|------------------------------------|----------------|---------|--------------------|-----------------|--------------|
|  | USER NAME = bemery                 | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>MEDIAN DRAINAGE REMEDIATION</b> | F.A.I.<br>RTE. | SECTION | COUNTY             | TOTAL<br>SHEETS | SHEET<br>NO. |
|  | PLOT SCALE = 40.0000' / in.        | DRAWN -    | REVISED - |   |                                    | I-74           | *       | **                 | 142             | 98           |
|  | PLOT DATE = 1/24/2019 - 4:05:58 PM | CHECKED -  | REVISED - |   |                                    |                |         | CONTRACT NO. 70790 |                 |              |
|  |                                    | DATE -     | REVISED - |   | SCALE: N.T.S.                      | SHEET          | OF      | SHEETS             | STA.            | TO STA.      |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



\* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

**TWO LANE/TWO WAY**

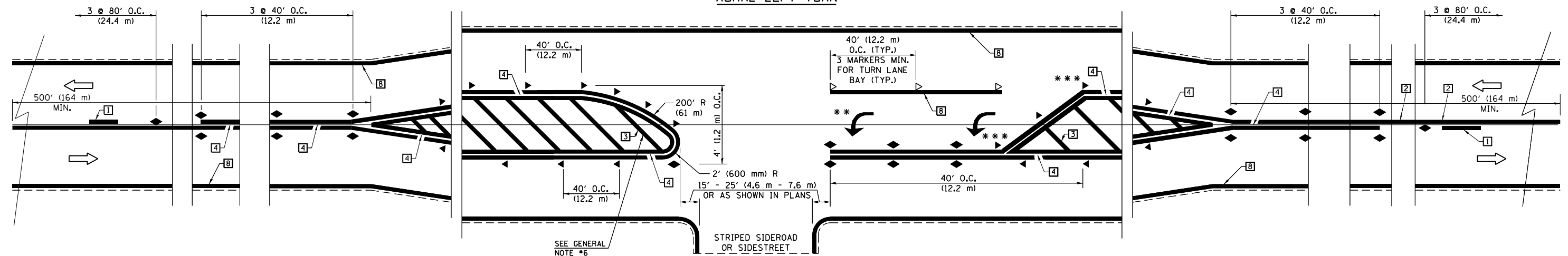
**TYPICAL PAVEMENT MARKING LEGEND**

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 4" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

**TYPICAL PAVEMENT MARKERS LEGEND**

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

**RURAL LEFT TURN**



\*\*\* REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.  
 \*\* TURN ARROWS SHALL BE PLACED AS SHOWN ON SHEET #2.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 7800AAAA**

MODEL: Default  
FILE: h:\m\p\030519\90-shc-detailed.dgn

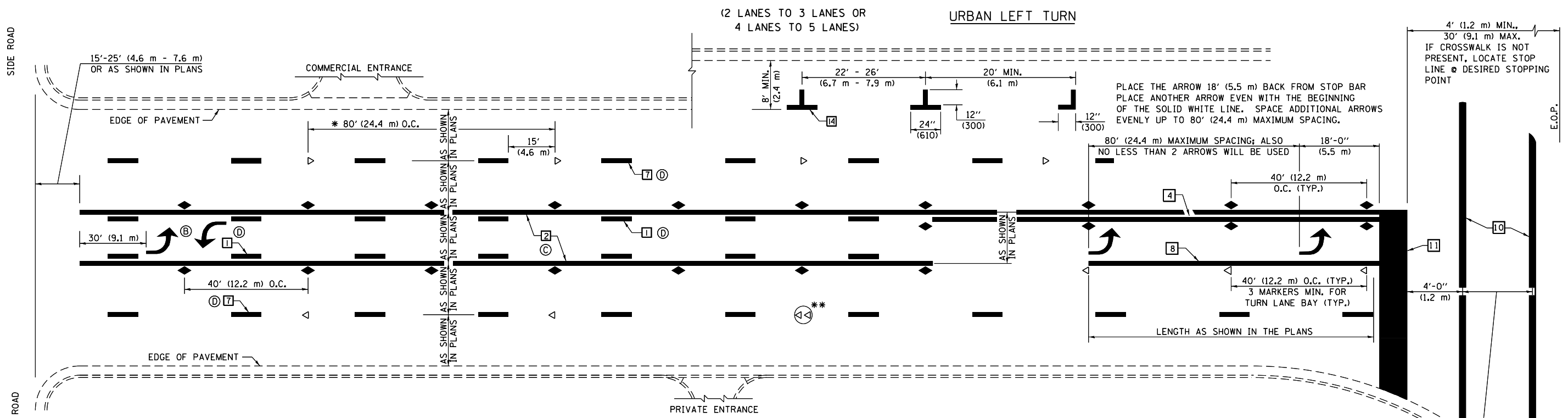
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:58 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS  
(RURAL & URBAN APPLICATIONS)**

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 99        |
| CONTRACT NO. 70790 |         |        |              |           |

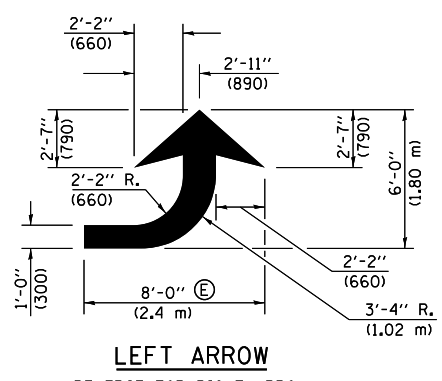
SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.



\* REDUCE TO 40 FEET (12.2 METERS) ON CENTER ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

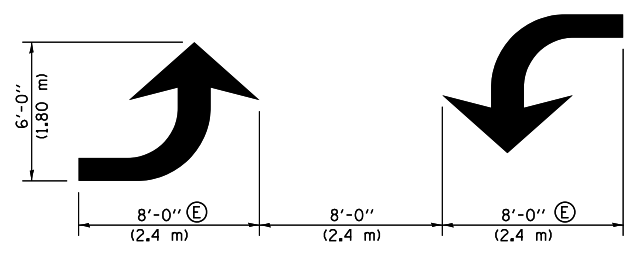
\*\* DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

- GENERAL NOTES:**
- ⓑ TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE.
  - ⓒ THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
  - ⓓ THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER. SEE EXAMPLE ON SHEET 2 OF 3.
  - ⓔ USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)



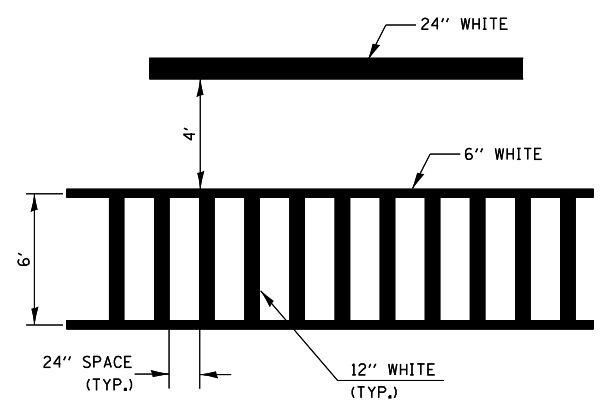
**LEFT ARROW**

REVERSE FOR RIGHT ARROW  
 AREA = 15.6 SQ. FT. (1.47 m<sup>2</sup>)  
 (WHITE)

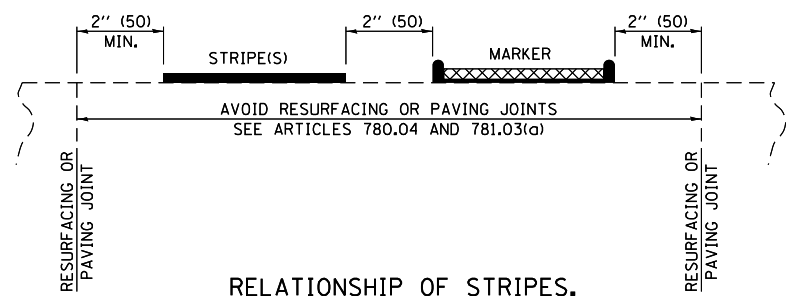


**TYPICAL DOUBLE TURN ARROWS (WHITE)**

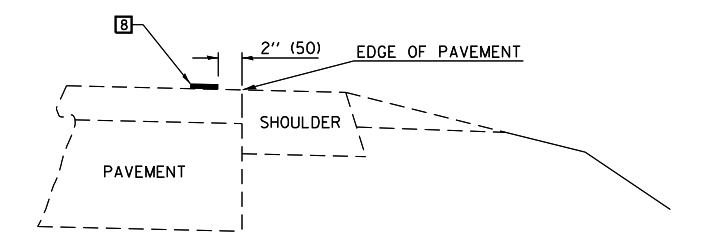
**BLOOMINGTON-NORMAL CITY LIMITS ONLY**



**TYPICAL SPACING FOR CROSSWALKS & STOP BARS**



**RELATIONSHIP OF STRIPES, MARKERS AND JOINTS**



**RELATIONSHIP OF EDGE LINE TO EDGE OF PAVEMENT (SAFETY SHOULDER OR PAVED SURFACE) SEE ARTICLE 780.04**

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 7800AAA**

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:59 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS  
 (RURAL & URBAN APPLICATIONS)**

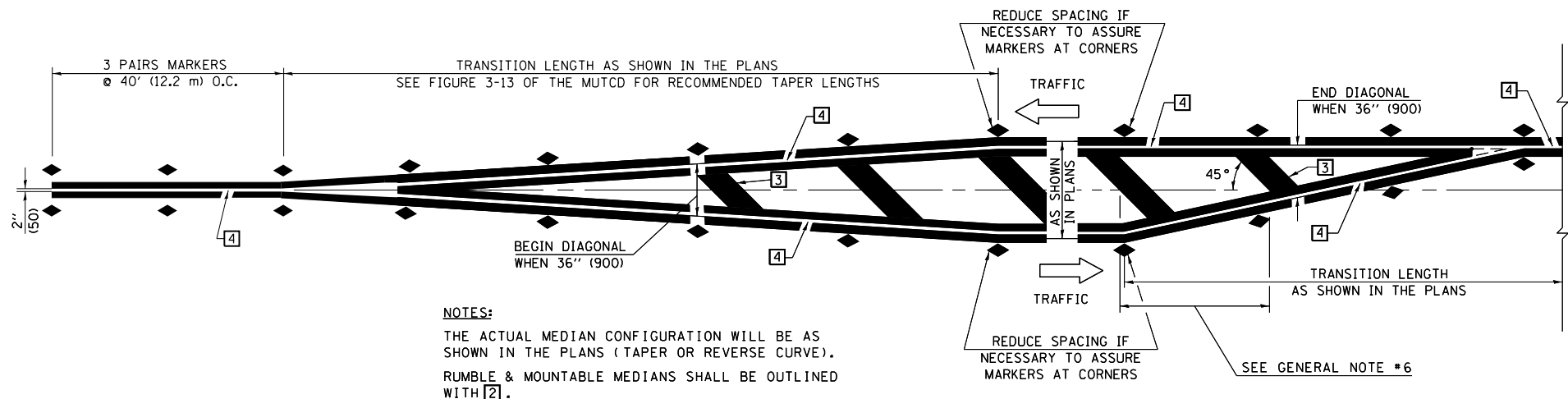
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 100       |
| CONTRACT NO. 70790 |         |        |              |           |

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

MODEL: Default FILE: h:\m\p\03\0790-shc-detailed.dgn



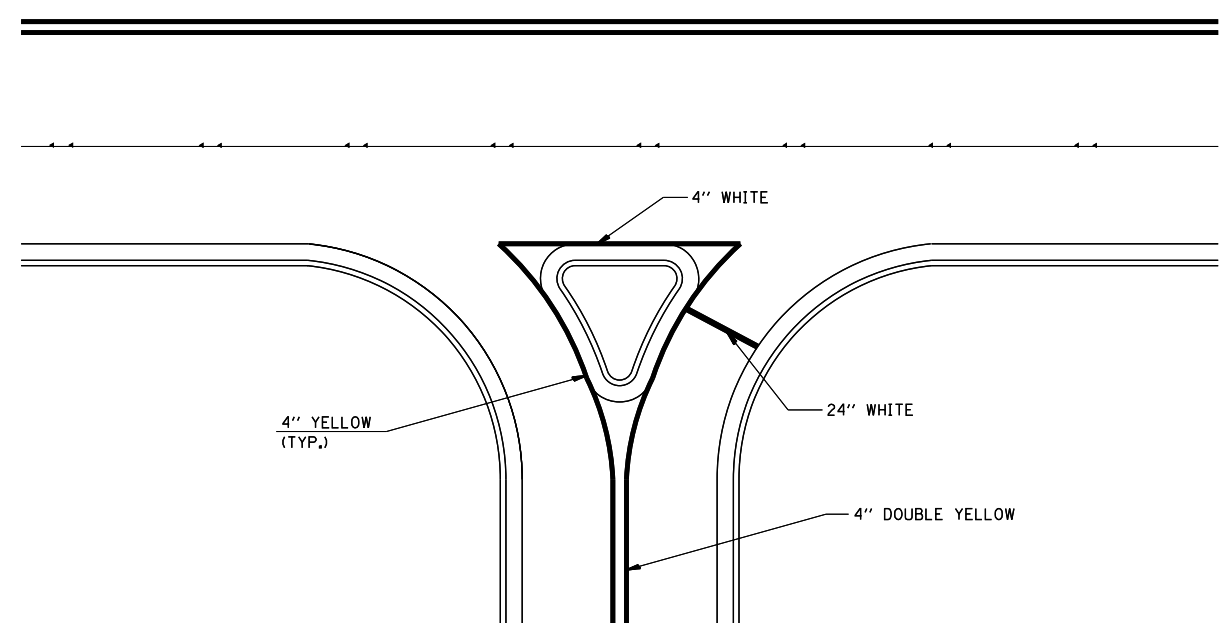


**NOTES:**  
 THE ACTUAL MEDIAN CONFIGURATION WILL BE AS SHOWN IN THE PLANS (TAPER OR REVERSE CURVE).  
 RUMBLE & MOUNTABLE MEDIANS SHALL BE OUTLINED WITH [2].

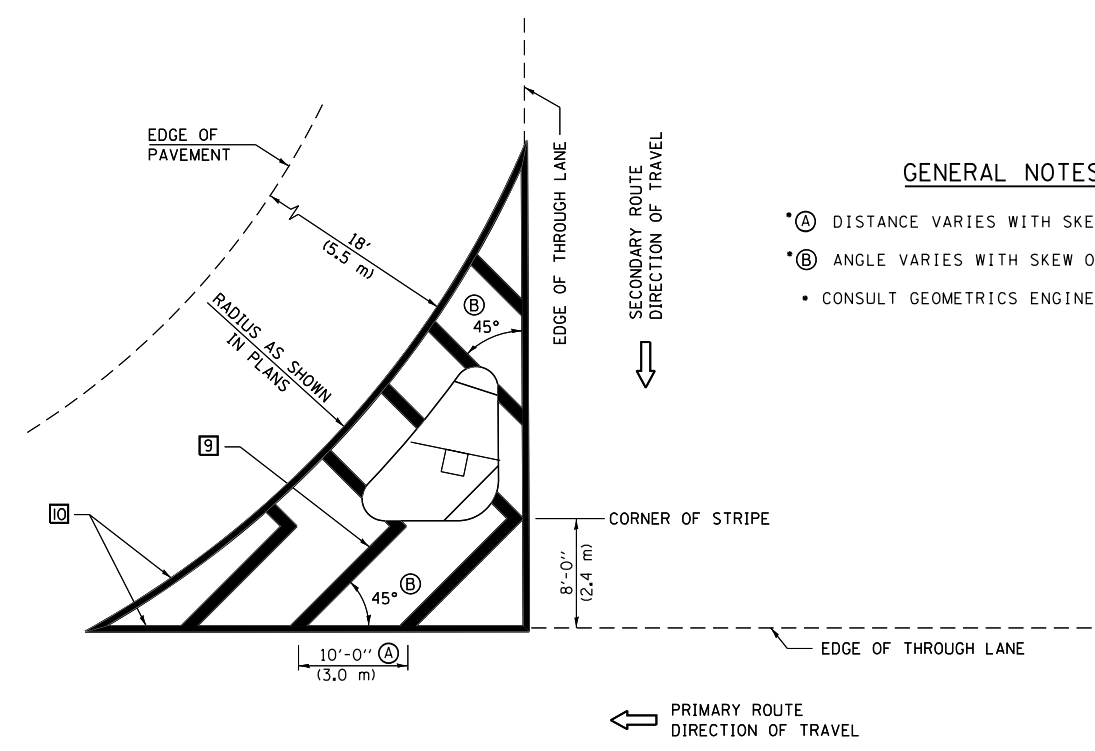
**TYPICAL MEDIAN TRANSITIONS**

**GENERAL NOTES**

1. WHEN MEDIANS ARE PRESENT, PAVEMENT MARKINGS ARE TO BE PLACED ADJACENT TO MEDIANS.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. A STRIPING KEY IS AVAILABLE ELSEWHERE AND SHALL BE SHOWN WHERE THE QUANTITIES ARE LISTED.
5. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
6. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING,  
 < 30 MPH USE 15' (< 50 km/h USE 4.5 m)  
 30-45 MPH USE 20' (50-75 km/h USE 6.0 m)  
 > 45 MPH USE 30' (> 75 km/h USE 9.0 m)



**RIGHT IN - RIGHT OUT ACCESS**



**ISLAND**

**GENERAL NOTES**

- (A) DISTANCE VARIES WITH SKEW OF INTERSECTION.
- (B) ANGLE VARIES WITH SKEW OF INTERSECTION.
- CONSULT GEOMETRICS ENGINEER

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 7800AAA**

MODEL: Default  
FILE: h:\mde\030527090-shc-detailed.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 * / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:59 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

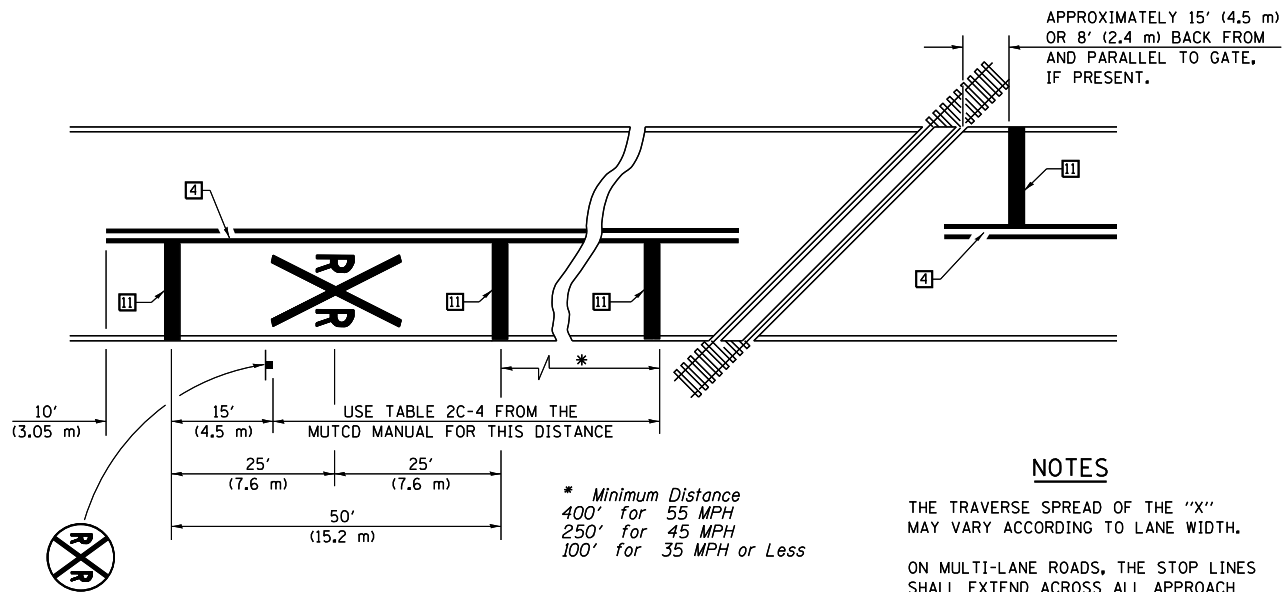
**PAVEMENT MARKING AND MARKERS  
 (RURAL & URBAN APPLICATIONS)**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 101       |
| CONTRACT NO. 70790 |         |        |              |           |

RAILROAD CROSSING WITH INTERCONNECT ONLY

RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



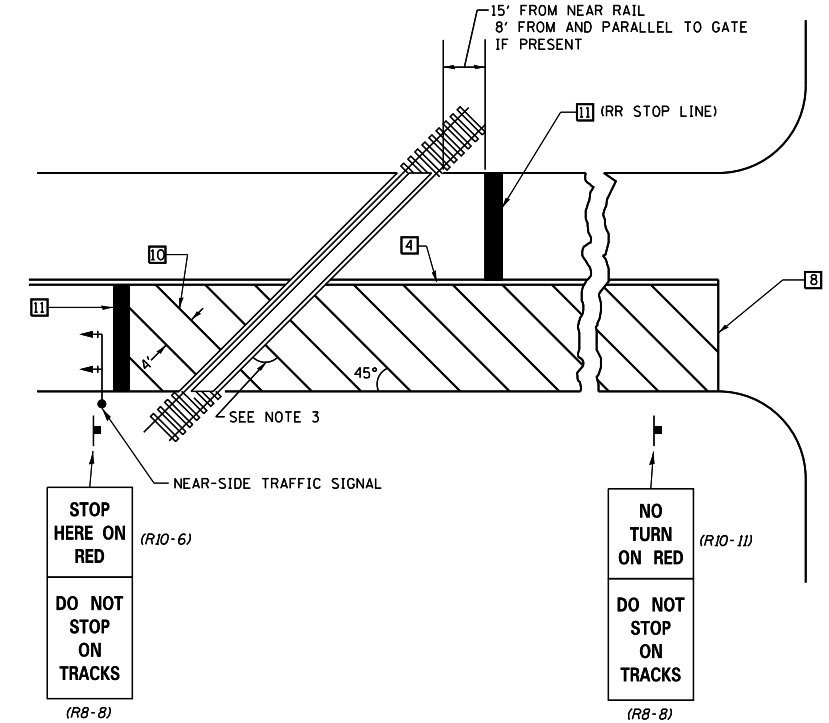
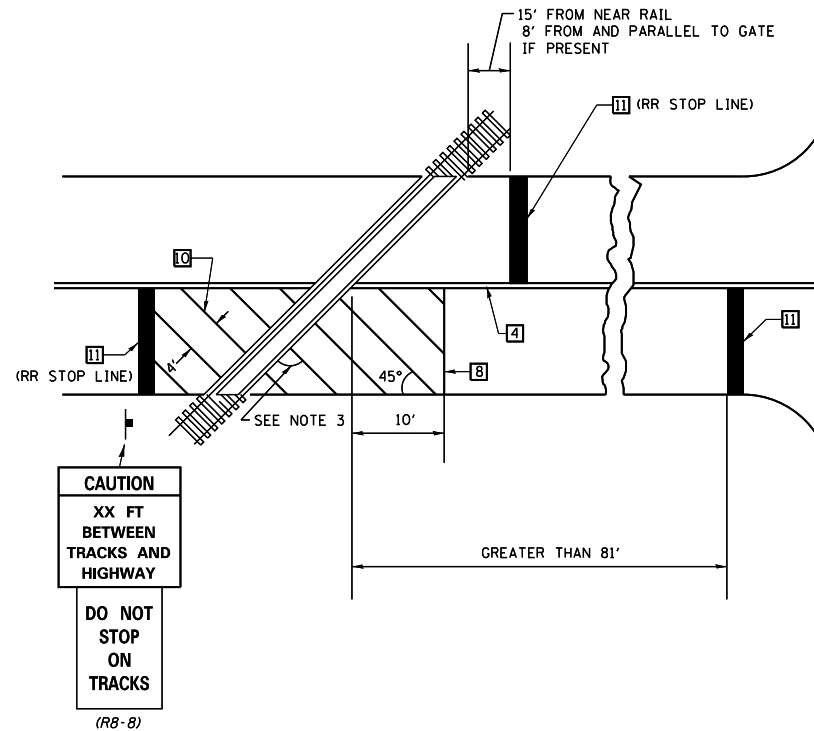
PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

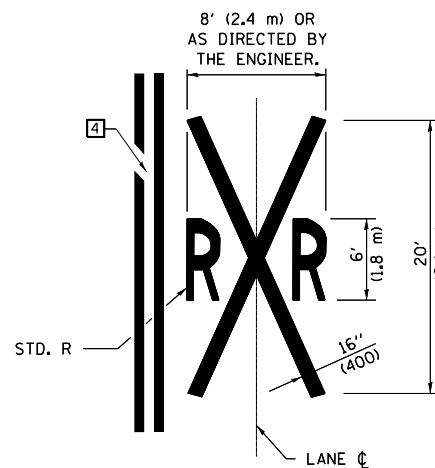
WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.



SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
- WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 5 DETAIL NO. 7800AAAA

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND MARKERS  
(RURAL & URBAN APPLICATIONS)

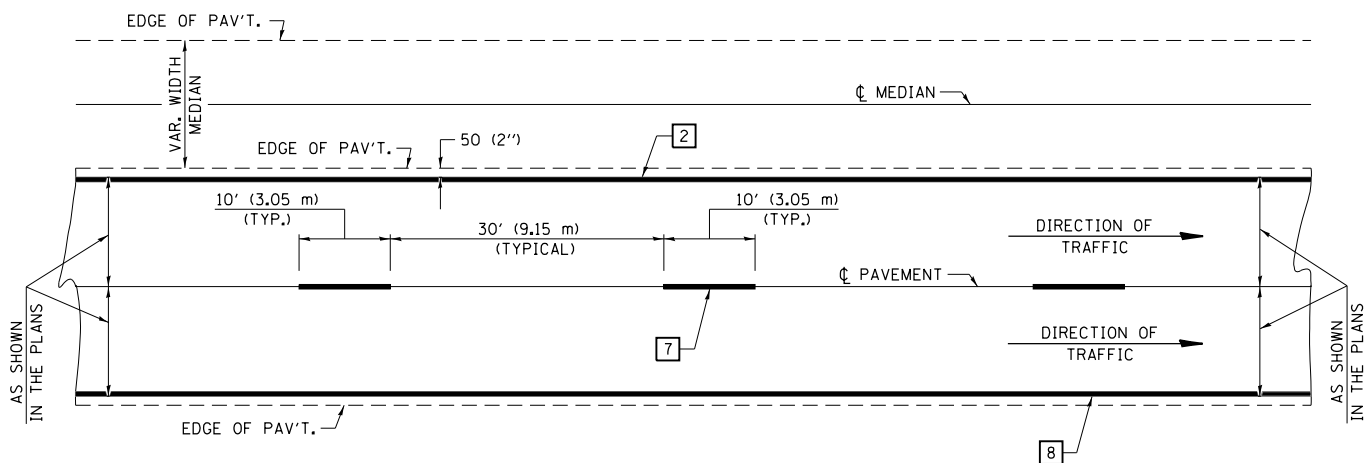
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 102       |
| CONTRACT NO. 70790 |         |        |              |           |

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT CONTRACT NO. 70790 \*\* CHAMPAIGN & VERMILION

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:59 PM | DATE -     | REVISED - |

**CENTERLINE INTERSTATE OR MULTI-LANE TWO WAY DIVIDED HIGHWAY**

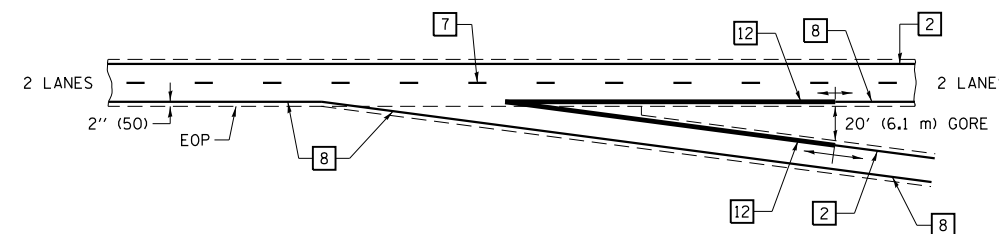


NOTE: PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.

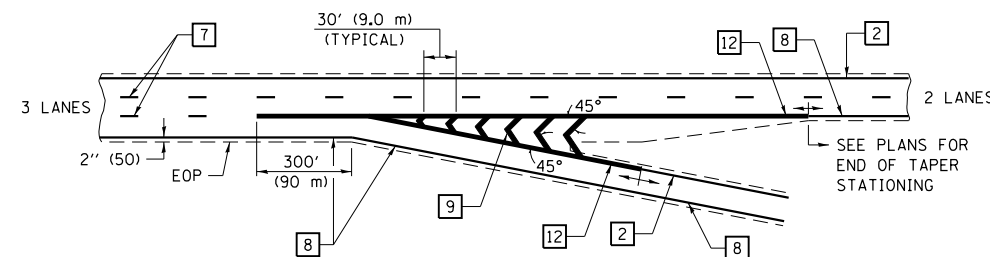
NOTE: SEE ARTICLES 780.04 & 781.03 FOR LOCATION OF STRIPES AND MARKERS RELATIVE TO EDGES OR JOINTS.

FOR RAISED REFLECTIVE PAVEMENT MARKERS, REFER TO STANDARD 781001.

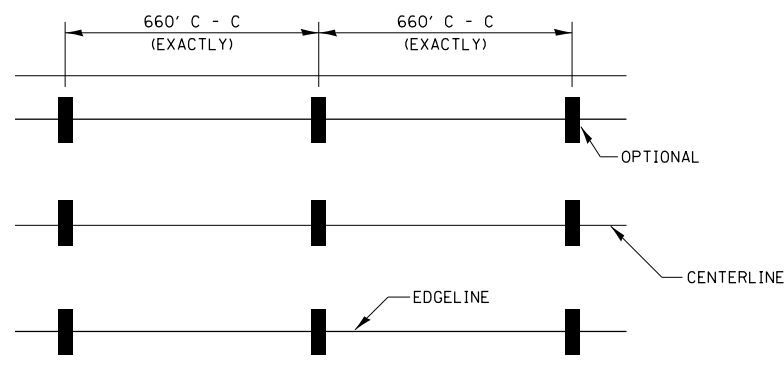
**TYPICAL EXIT RAMP TERMINAL**



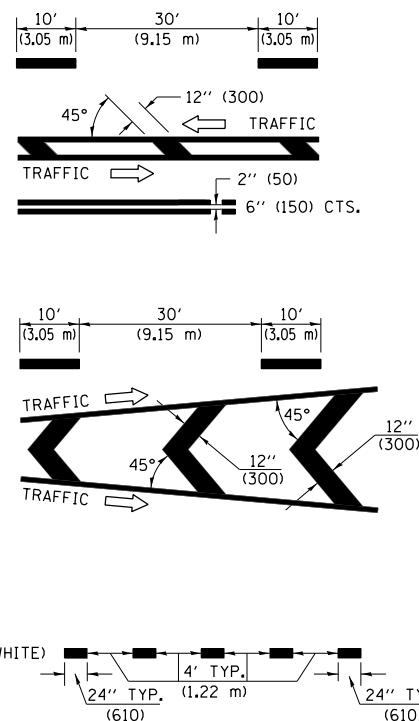
**EXIT RAMP TERMINAL with EXCLUSIVE (auxiliary) LANE**



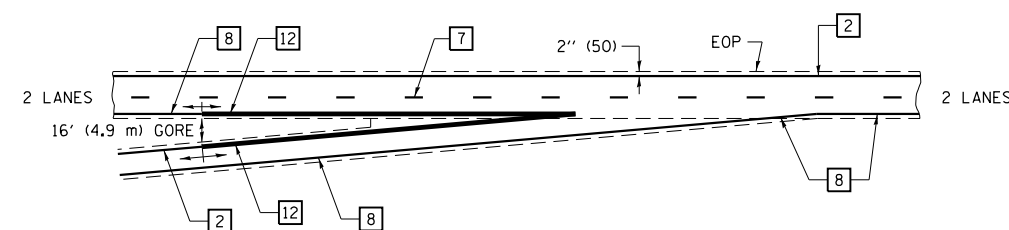
**TYPICAL PAVEMENT MARKING LEGEND**



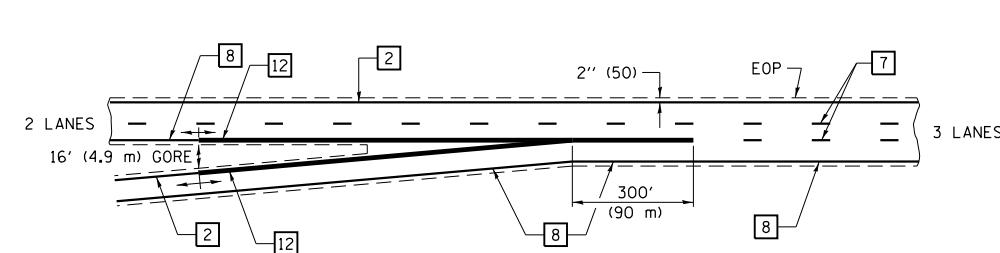
- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 4" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) LANE LINE EXTENSIONS (WHITE)



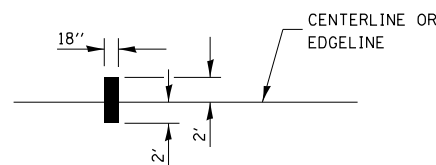
**TYPICAL ENTRANCE RAMP TERMINAL**



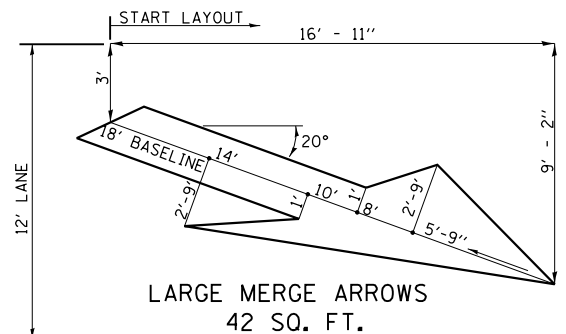
**ENTRANCE RAMP TERMINAL with EXCLUSIVE LANE**



IT WILL BE NECESSARY TO HAVE A REPRESENTATIVE OF THE STATE POLICE PRESENT SO THAT THE ACCURACY OF MEASUREMENT CAN BE ATTESTED TO IN COURT.



**AERIAL SPEED CHECK ZONES**



**LARGE MERGE ARROWS  
42 SQ. FT.**

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 780BBBB**

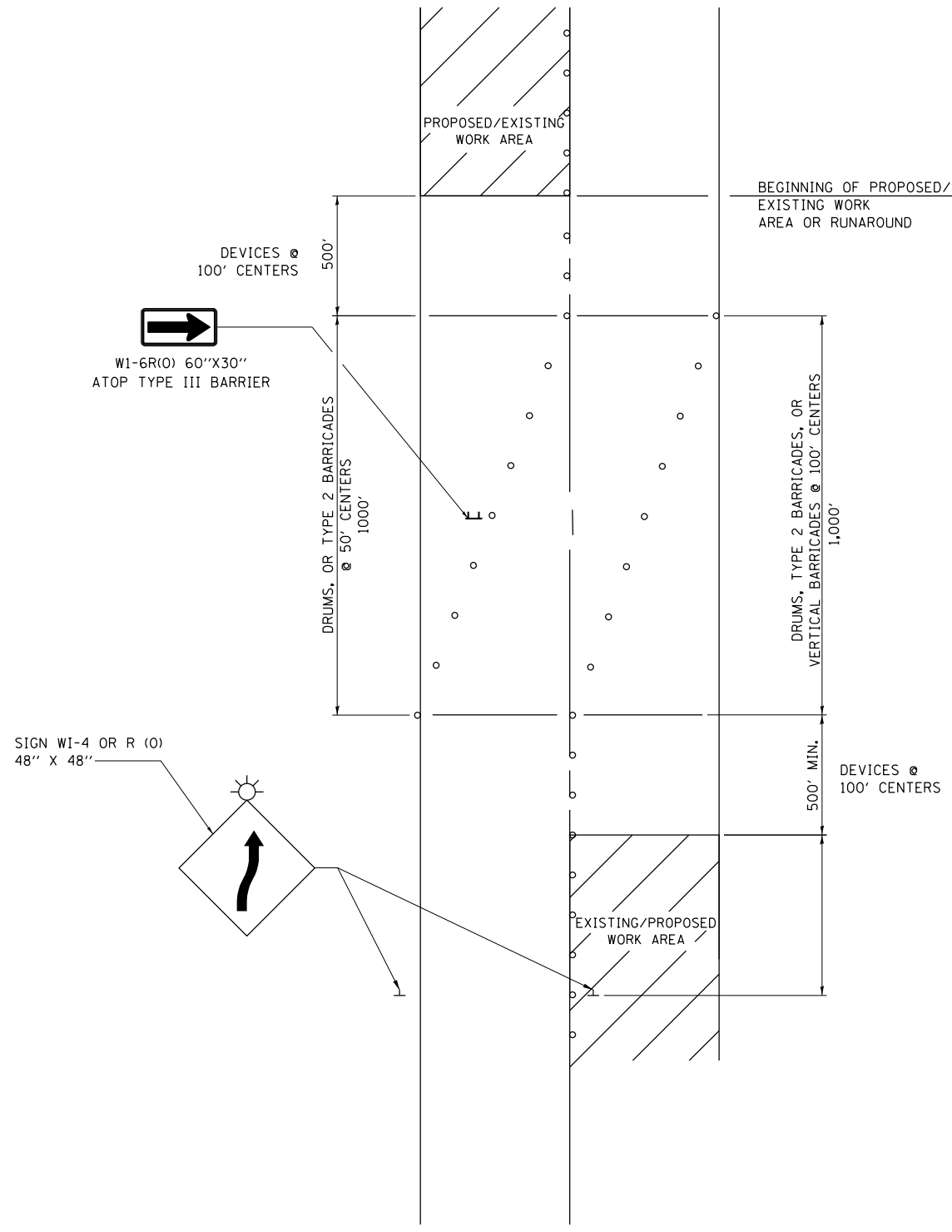
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40.0000 "/in.         | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:05:59 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING (INTERSTATE & MULTI-LANE APPLICATIONS)**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 103       |
| CONTRACT NO. 70790 |         |        |              |           |



- NOTES:**
1. THIS DETAIL ALSO APPLIES WHEN WORK IS BEING PREFORMED IN THE LEFT LANE PRIOR TO THE RIGHT LANE. UNDER THESE CONDITIONS, THE SETUP WOULD BE A MIRROR IMAGE TO WHAT IS SHOWN.
  2. ALL DEVICES SHALL CONFORM TO STD. 701901
  3. ALL CONFLICTING OR UNNECESSARY SIGNS SHALL BE REMOVED OR COVERED.
  4. ALL DEVICES REQUIRE STEADY BURN MONODIRECTIONAL LIGHTS IF WORK INVOLVES A NIGHT-TIME CLOSURE.
  5. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS.

MODEL: Default  
 FILE: Model5\_100570790-shc-detaile.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:00 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

|                   |       |      |        |
|-------------------|-------|------|--------|
| <b>SWITCHBACK</b> |       |      |        |
| SCALE: N.T.S.     | SHEET | OF   | SHEETS |
| STA.              | TO    | STA. |        |

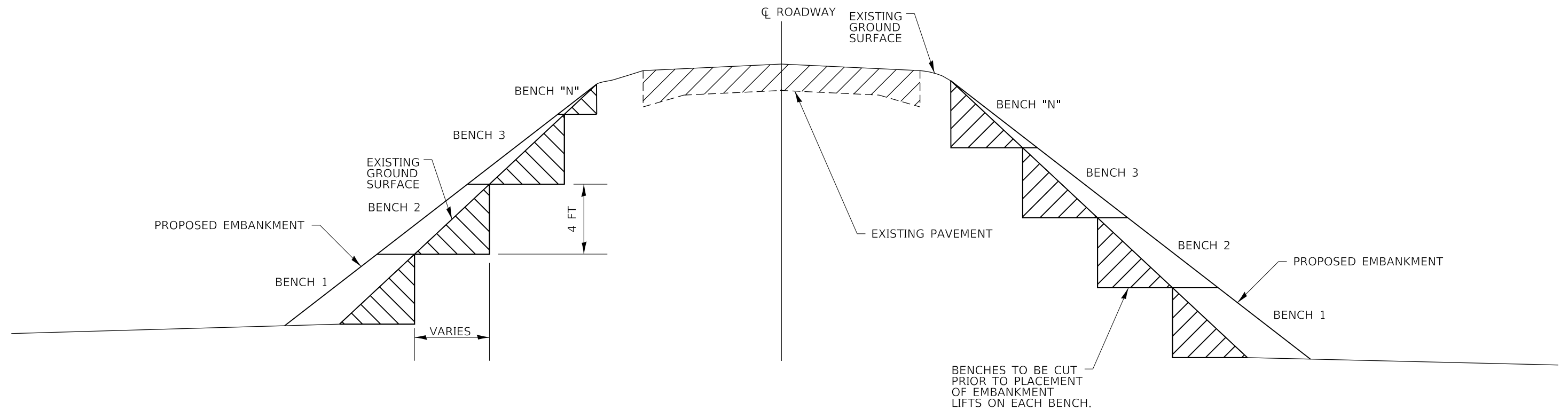
| <b>DISTRICT 5 DETAIL NO. 70100100</b> |         |        |              |           |
|---------------------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.                           | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                                  | *       | **     | 142          | 104       |
| CONTRACT NO. 70790                    |         |        |              |           |

NOTES:  
COST OF SOIL BENCHING INCIDENTAL TO CONTRACT.

EMBANKMENT SHALL BE BROUGHT UP BY STAGE ALONG WITH THE BENCHING AND COMPACTED PARALLEL TO THE BENCHES.

THE TOP 12" ± OF THE OGDEN INTERCHANGE INFIELDS ARE ORGANIC/TOPSOIL AND IT IS RECOMMENDED TO BE SET ASIDE AND PLACED ON TOP OF THE SLOPES RATHER THAN INCORPORATED INTO THE INTERIOR OF THE SLOPES.

THE MOISTURE CONTENT OF THE MATERIAL IS HIGHER THAN OPTIMUM AND WILL NEED DRIED BY SPREADING AND DISKING PRIOR TO USE.



**EXISTING SLOPE SOIL BENCHING DETAIL**

MODEL: Default  
FILE: Model - 10/27/2019 4:06:00 PM

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:00 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SOIL BENCHING**

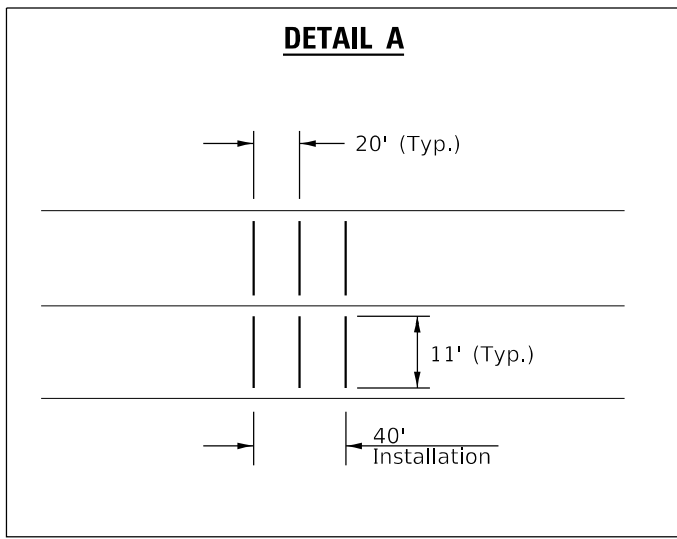
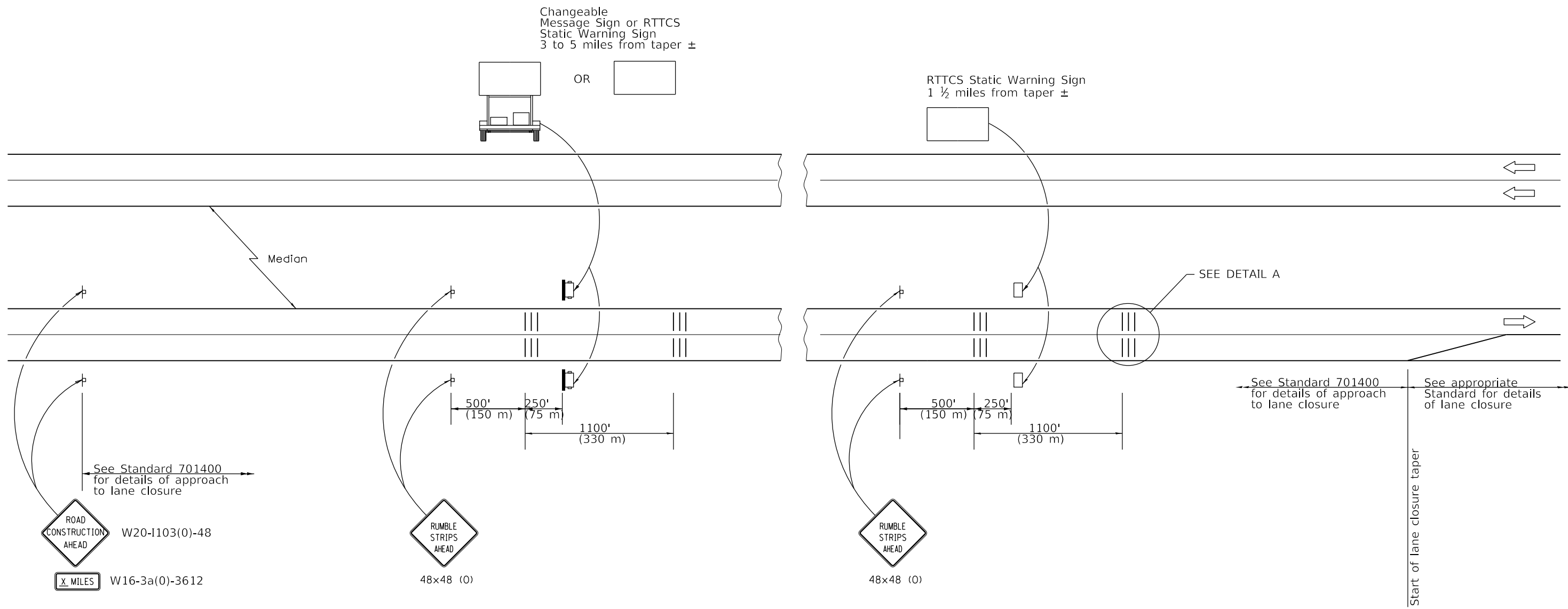
SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|                  |           |           |                    |               |
|------------------|-----------|-----------|--------------------|---------------|
| F.A.I. RTE. I-74 | SECTION * | COUNTY ** | TOTAL SHEETS 142   | SHEET NO. 105 |
|                  |           |           | CONTRACT NO. 70790 |               |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

# DETAIL FOR TEMPORARY RUMBLE STRIPS (SPECIAL)



- SYMBOLS
- † Sign
  - ||| Temporary Rumble Strips (Special)
  - ☐ Trailer Mounted Sign
  - ☐ RTTCS Static Warning Sign

## GENERAL NOTES

Remove the Temporary Rumble Strips (Special) prior to the removal of the advanced warning signs.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
FILE: Model\_0305170790-shc-detailed.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:00 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

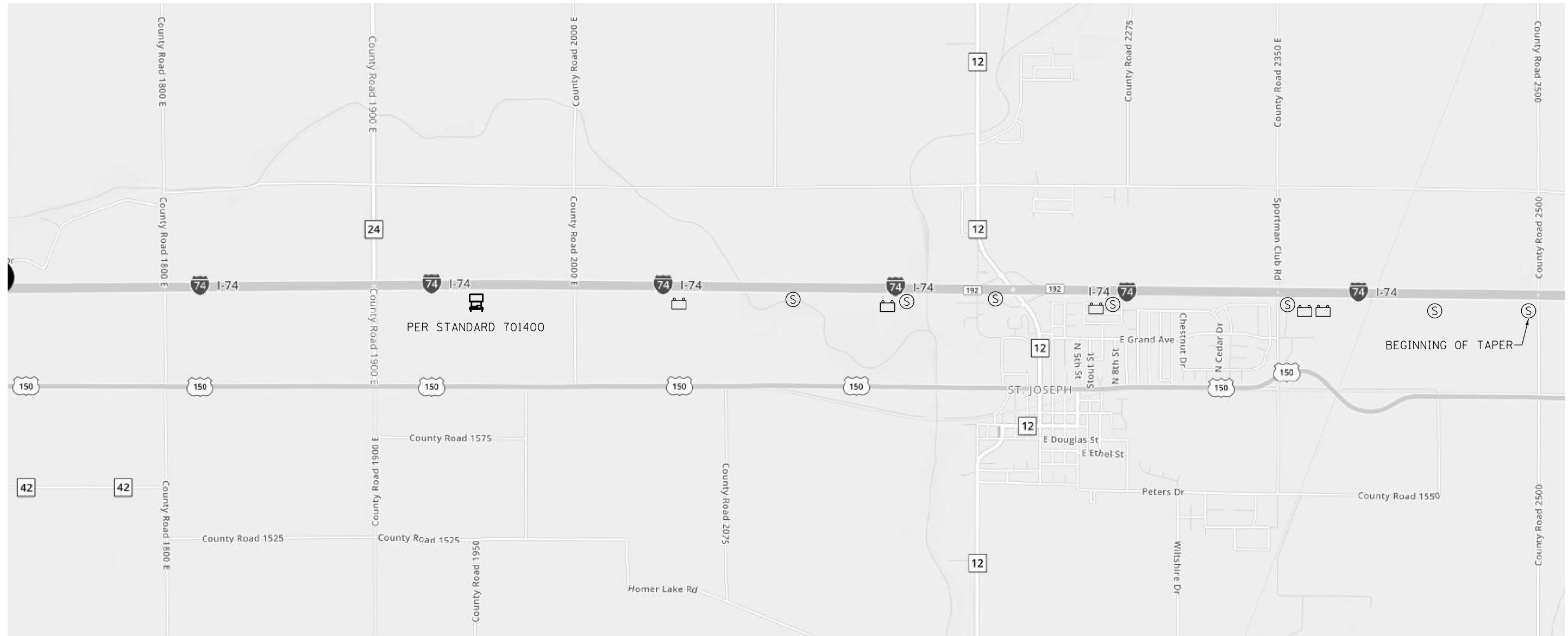
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION


|  |         |    |        |
|--|---------|----|--------|
| DETAIL FOR TEMPORARY RUMBLE STRIPS (SPECIAL) |         |    |        |
| SCALE: N.T.S.                                | SHEET   | OF | SHEETS |
| STA.   | TO STA. |    |        |

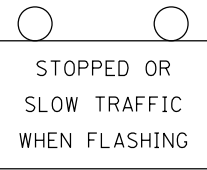
|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 106       |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |


\* (10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION


# REAL-TIME TRAFFIC CONTROL SYSTEM



 RTTCS STATIC WARNING SIGN  
 •Paid for as Real-Time Traffic Control Sensor Unit  
 •48" x 96" (1200mm x 2400mm) Warning Sign that says "STOPPED OR SLOW TRAFFIC WHEN FLASHING"



 Single Portable Changeable Message Sign placed 1/2 mile in advance of 4th RTTCS board. It shall read "RIGHT/LEFT LANE CLOSED/5 MILES AHEAD"  
 •This Portable Changeable Message Sign is included in Standard 701400

 Speed Sensor

Note:

The number of RTTCS Static Warning Signs may be changed as directed by the Engineer. Relocation of RTTCS Static Warning Signs is included in the RTTCS pay items.

MODEL: Default  
FILE: Model\_0305201905-shc-detailed.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:00 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|  |       |           |              |
|--|-------|-----------|--------------|
| <b>SMART TRAFFIC MONITORING SYSTEM</b> |       |           |              |
| SCALE: N.T.S.                          | SHEET | OF SHEETS | STA. TO STA. |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 107       |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3

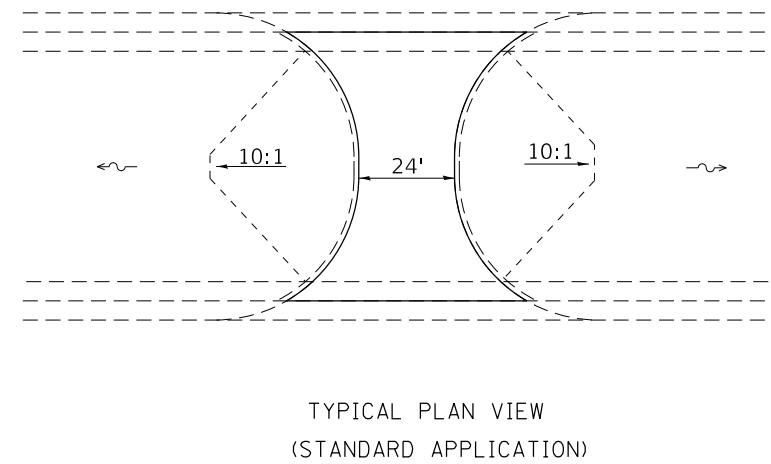
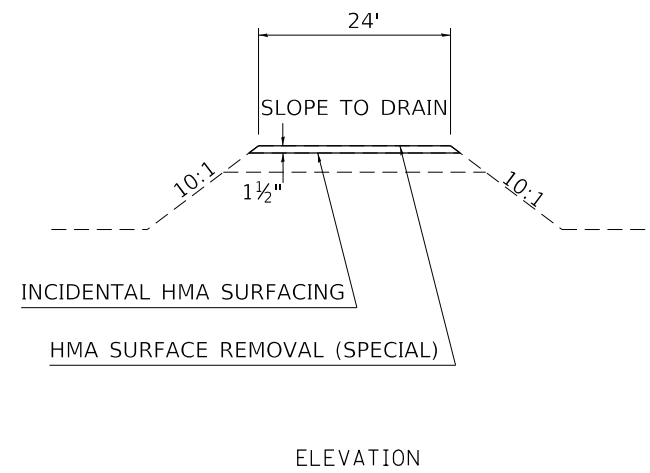
\*\* CHAMPAIGN & VERMILION





## GENERAL NOTES

OMIT SHOULDER RUMBLE STRIPS THROUGHOUT MEDIAN CROSSOVER.



## MEDIAN CROSSOVER

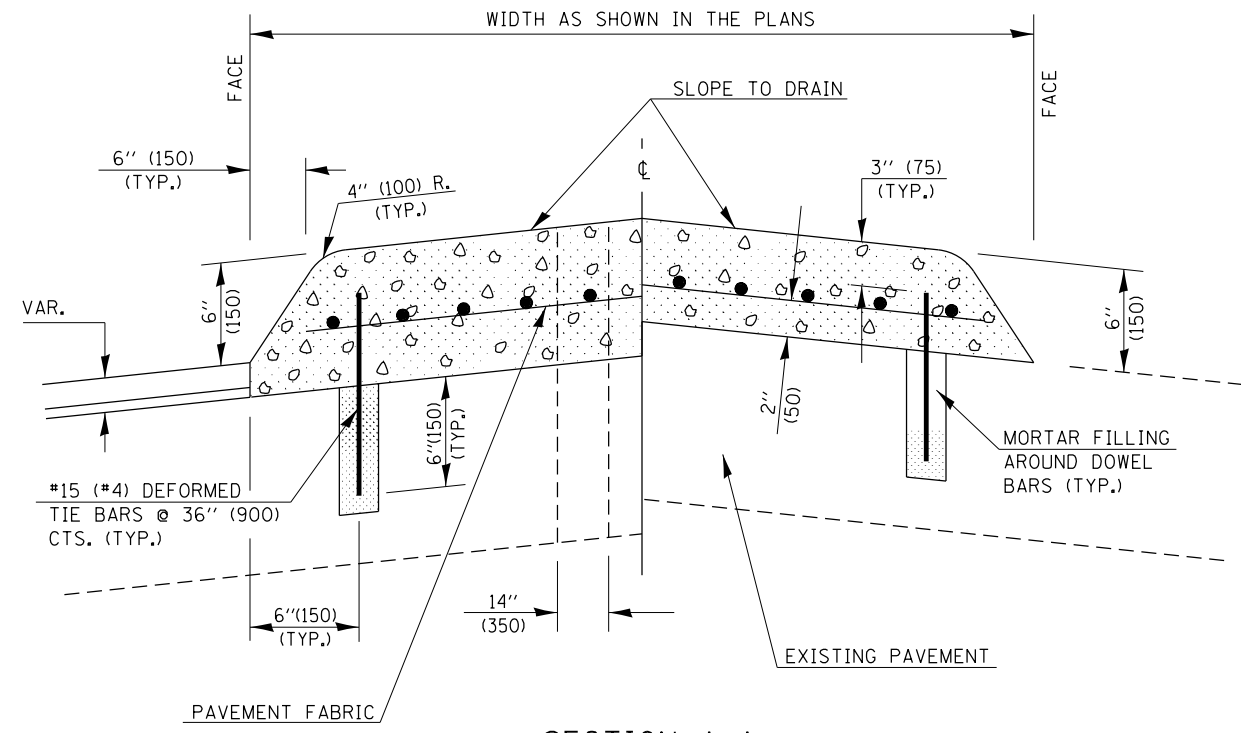
STA. 888+12.03 TO STA. 888+58.55

STA. 1052+92.88 TO STA. 1053+36.68

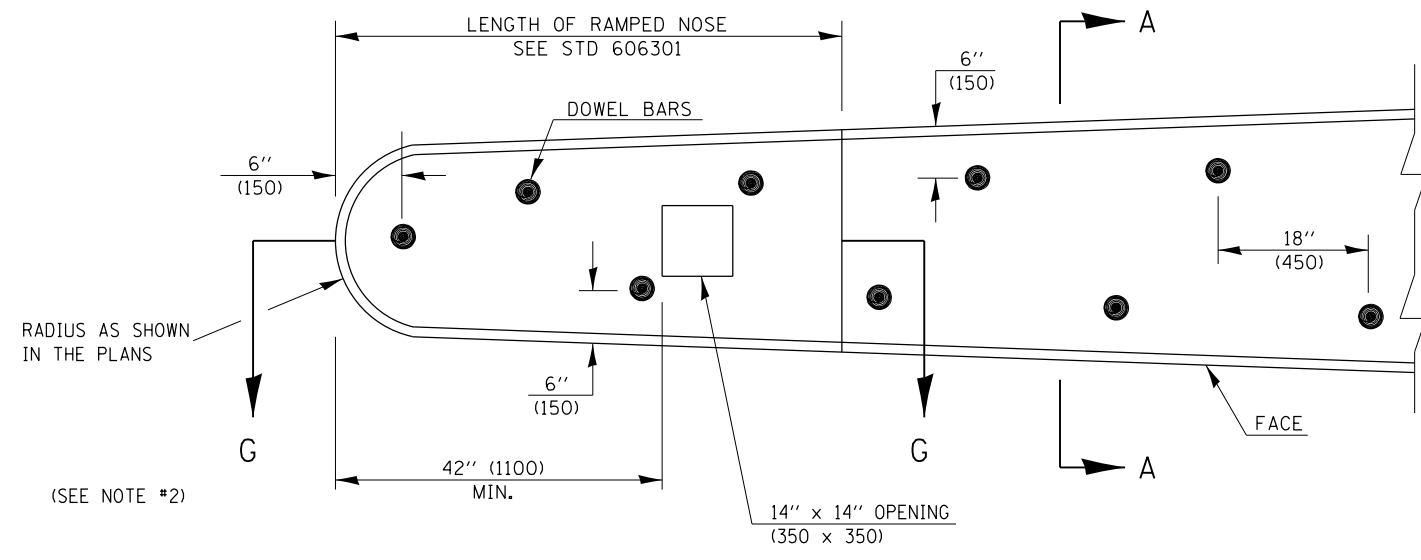
MODEL: Default  
FILE: Model - ...030570790-shc-drt-ell.dgn

|  |                                    |            |           |   |                           |       |    |                        |              |              |                        |                          |
|--|------------------------------------|------------|-----------|---|---------------------------|-------|----|------------------------|--------------|--------------|------------------------|--------------------------|
|  | USER NAME = bemery                 | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>MEDIAN CROSSOVER</b>   |       |    | F.A.I.<br>RTE.<br>I-74 | SECTION<br>* | COUNTY<br>** | TOTAL<br>SHEETS<br>142 | SHEET<br>NO.<br>109      |
|  | PLOT SCALE = 40.0000' / in.        | CHECKED -  | REVISED - |   | SCALE: N.T.S.             | SHEET | OF | SHEETS                 | STA.         | TO STA.      | CONTRACT NO. 70790     |                          |
|  | PLOT DATE = 1/24/2019 - 4:06:09 PM | DATE -     | REVISED - |   | ILLINOIS FED. AID PROJECT |       |    |                        |              |              |                        | ** CHAMPAIGN & VERMILION |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3



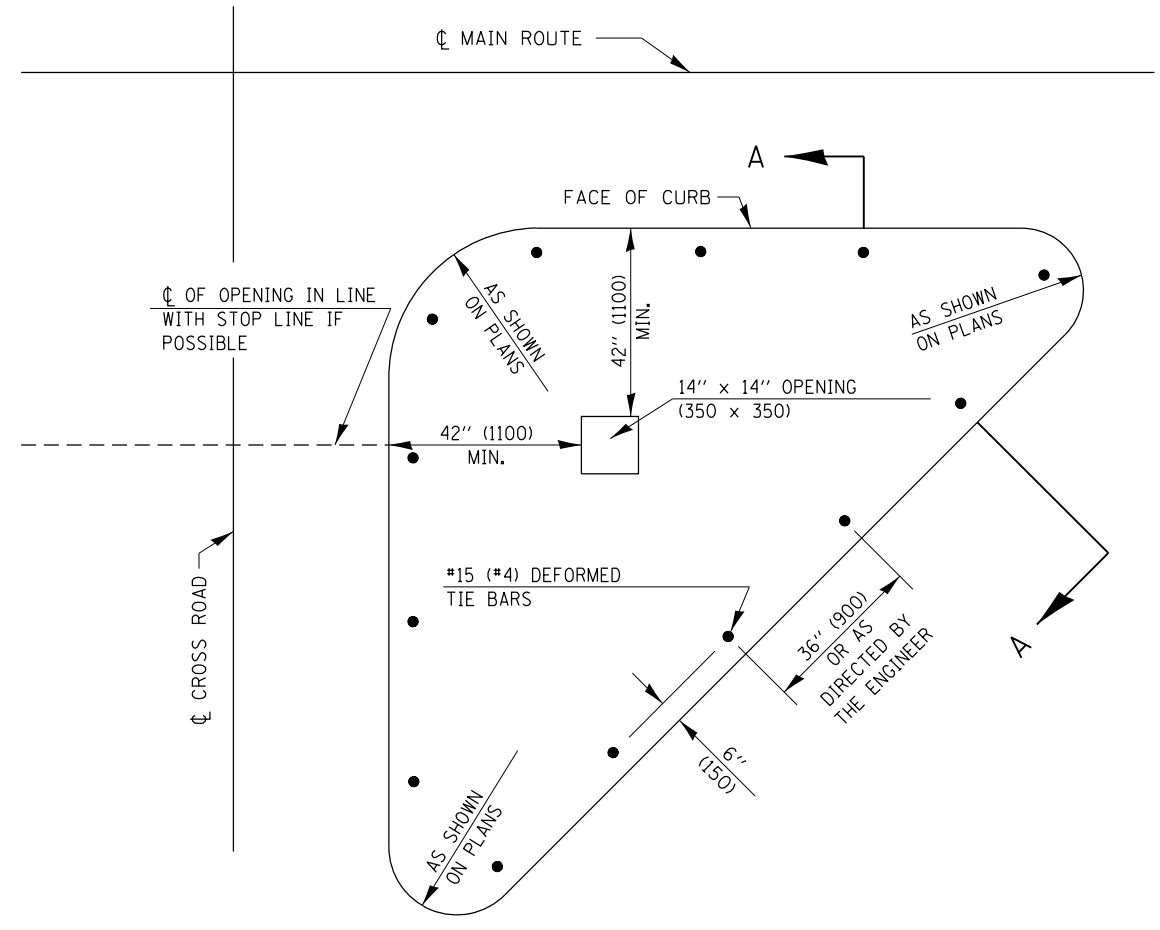
SECTION A-A



PLAN OF MEDIAN

GENERAL NOTES

1. THE GENERAL NOTES FOR STANDARD 606301 SHALL APPLY.
2. DOWEL BARS @ 36" (900) CTS. OR AS DIRECTED BY THE ENGINEER.
3. NOSES ARE TO BE RAMPED PER HIGHWAY STANDARD 606301.
4. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SO FOOT (m<sup>2</sup>) FOR CONCRETE MEDIAN, TYPE SM (DOWELLED), INCLUDING THE COST OF FURNISHING AND INSTALLING THE DOWEL BARS, MORTAR FILLING, PAVEMENT FABRIC AND THE REMOVAL AND DISPOSAL OF THE EXISTING PAVEMENT FOR THE 14" x 14" (350 x 350) OPENING, IF REQUIRED, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



PLAN OF CORNER ISLAND

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 5 DETAIL NO. 60622354

MODEL: Default  
FILE: Model5\_03021905-shc-detailed.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bebery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:09 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

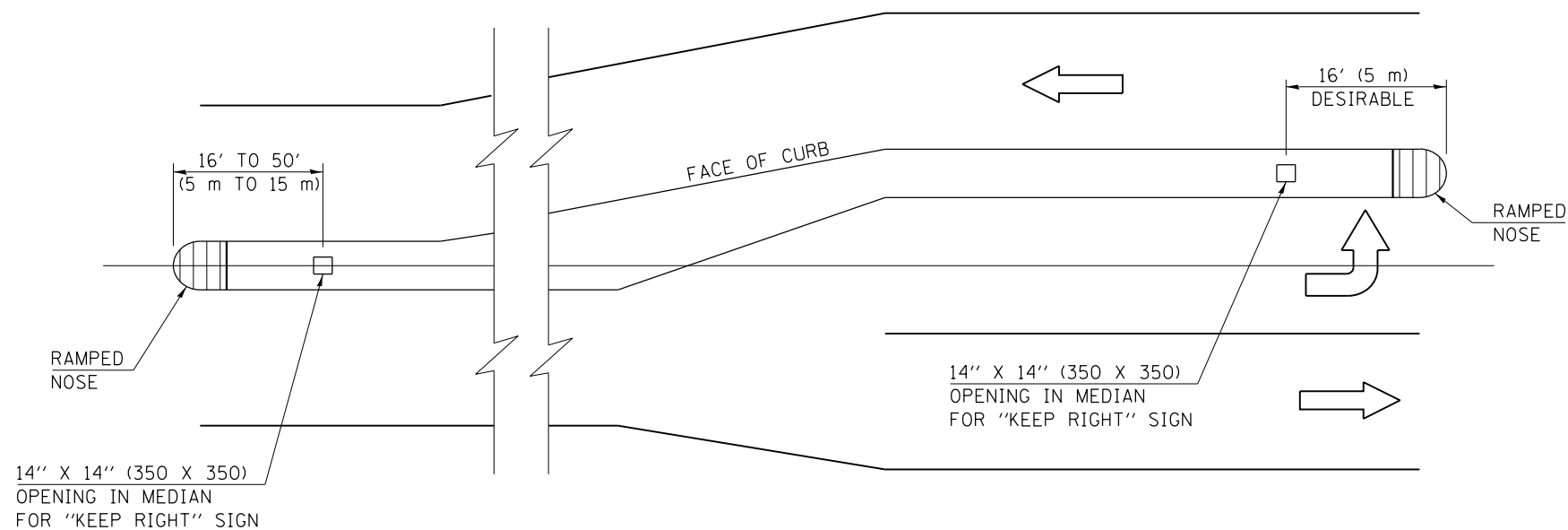
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CONCRETE MEDIAN, TYPE SM (DOWELLED)

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 110       |
| CONTRACT NO. 70790 |         |        |              |           |

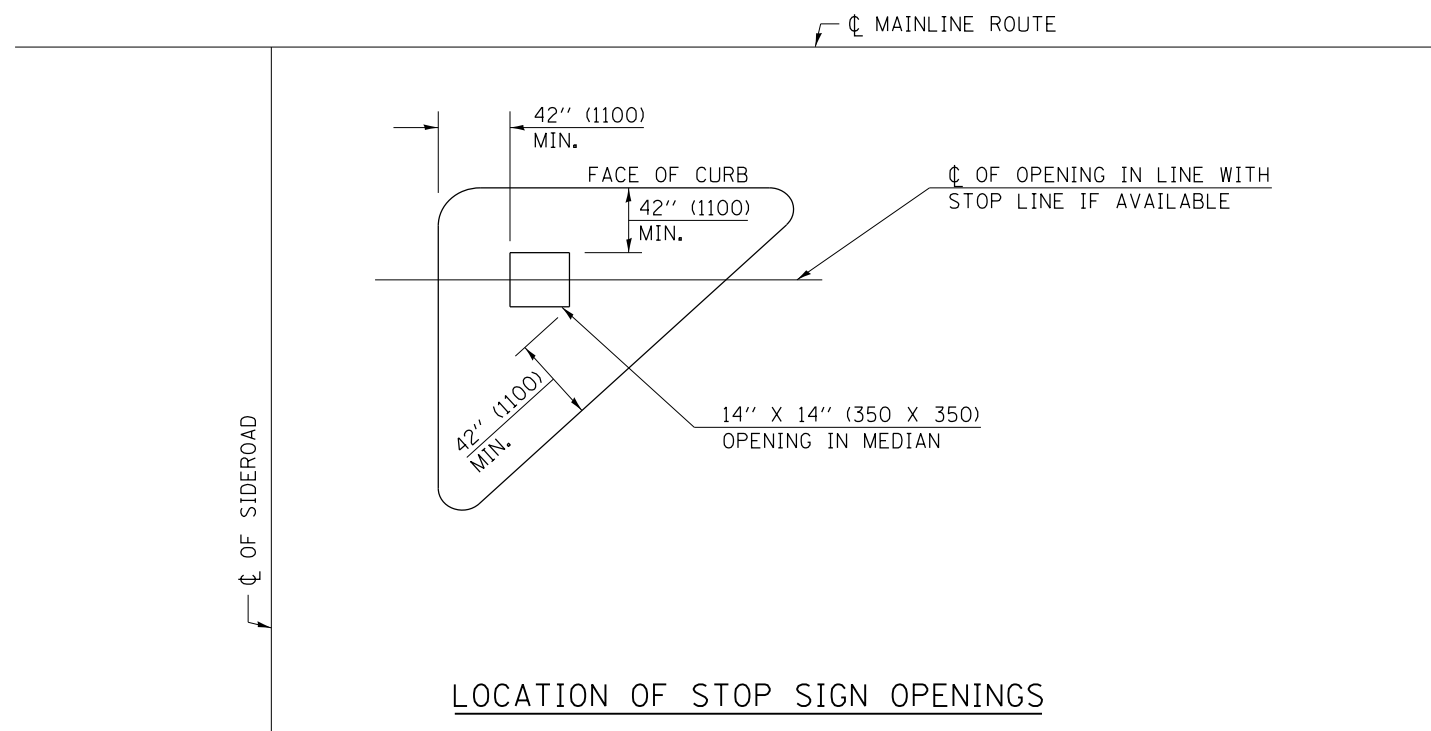
(10-7.8;92-8.9)RS-2(92-10)RS-3 ILLINOIS FED. AID PROJECT \*\* CHAMPAIGN & VERMILION



LOCATION OF OPENINGS FOR "KEEP RIGHT" SIGNS

GENERAL NOTES

1. ALL SMALL ISLANDS SHALL BE CONSTRUCTED WITH THE STOP SIGN ISLANDS AS SHOWN, UNLESS OTHERWISE SPECIFIED.
2. OPENINGS FOR SIGNS IN MEDIANS SHALL BE AS SHOWN OR AS DIRECTED BY THE ENGINEER.
3. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE TYPE OF MEDIAN SPECIFIED IN THE PLANS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



LOCATION OF STOP SIGN OPENINGS

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 5 DETAIL NO. 60623711

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

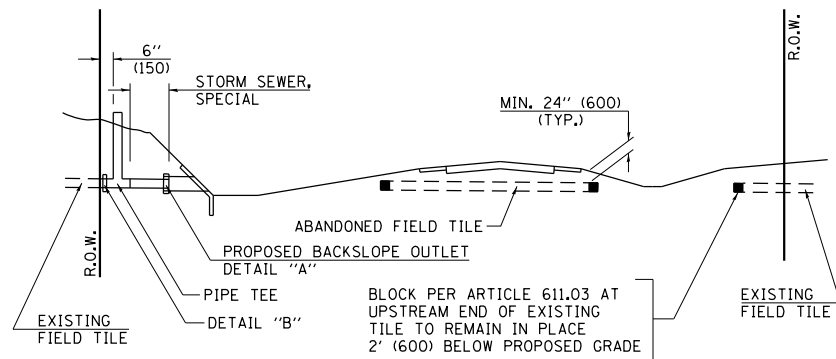
SIGN POST BLOCKOUTS

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 111       |
| CONTRACT NO. 70790 |         |        |              |           |

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

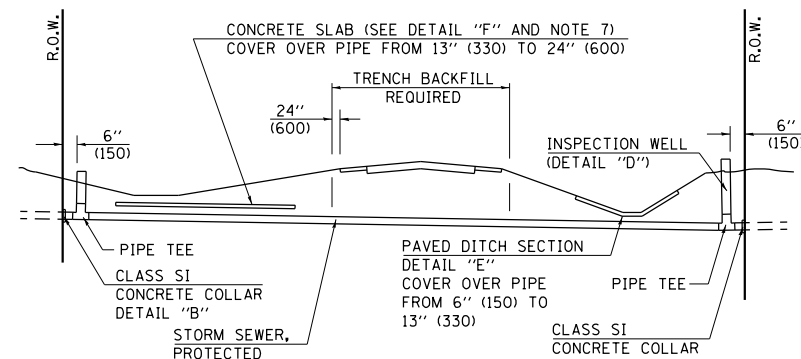
\* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:09 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |



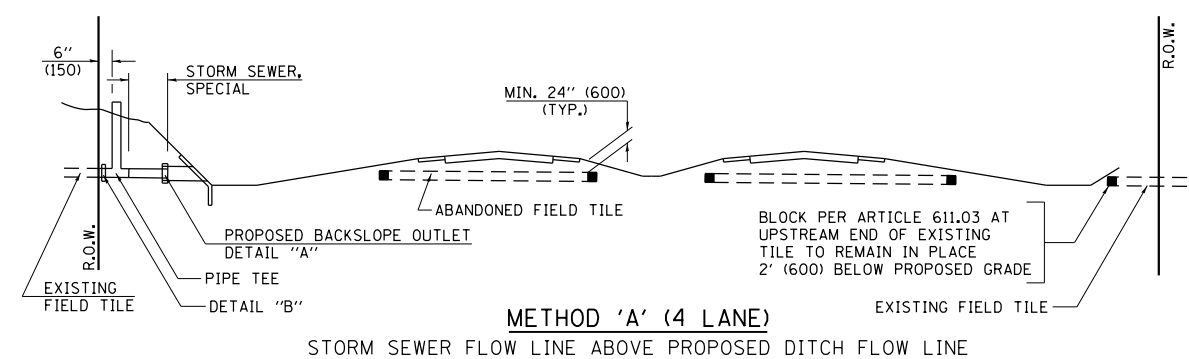
**METHOD 'A' (2 LANE)**

STORM SEWER FLOW LINE ABOVE PROPOSED DITCH FLOW LINE



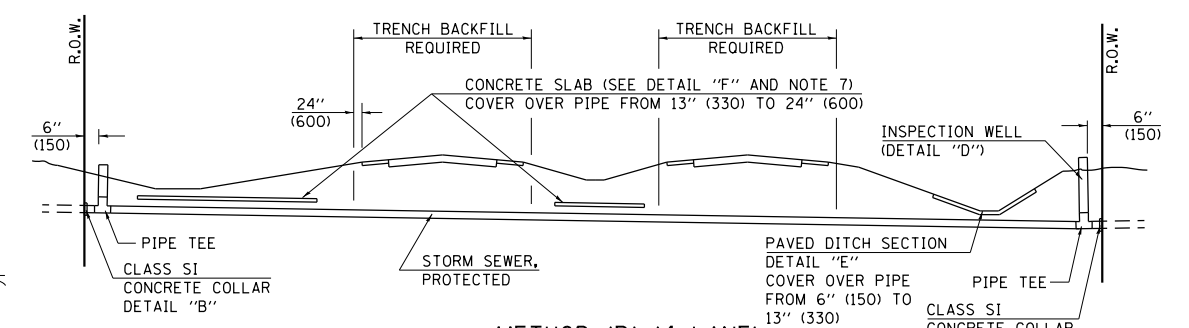
**METHOD 'B' (2 LANE)**

STORM SEWER LESS THAN 2' (600 mm) BELOW DITCH FLOW LINE AND STORM SEWERS CROSSING UNDER PAVEMENT AND PAVED DITCH



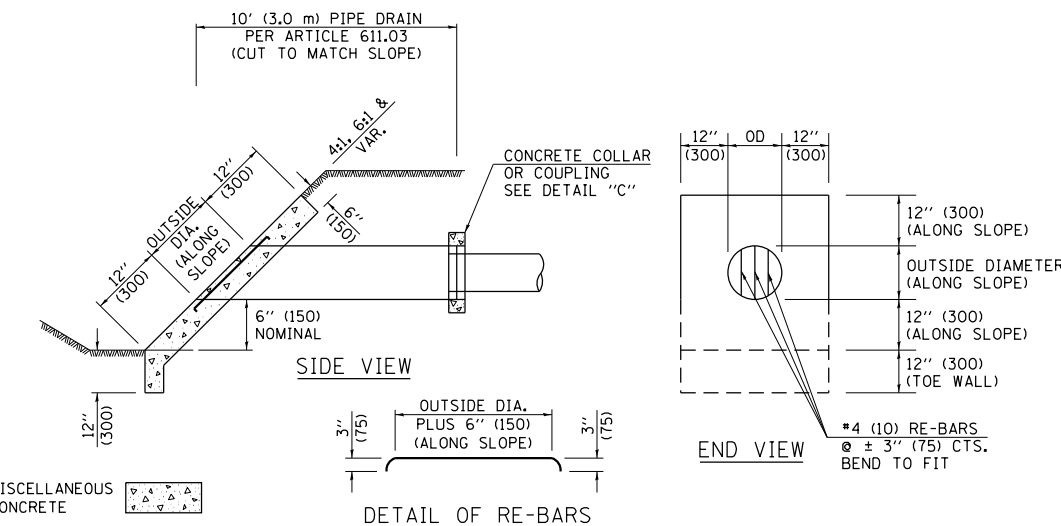
**METHOD 'A' (4 LANE)**

STORM SEWER FLOW LINE ABOVE PROPOSED DITCH FLOW LINE

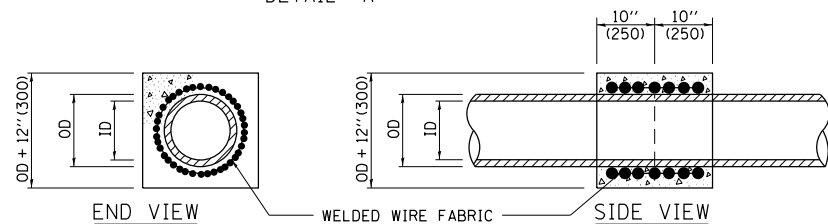


**METHOD 'B' (4 LANE)**

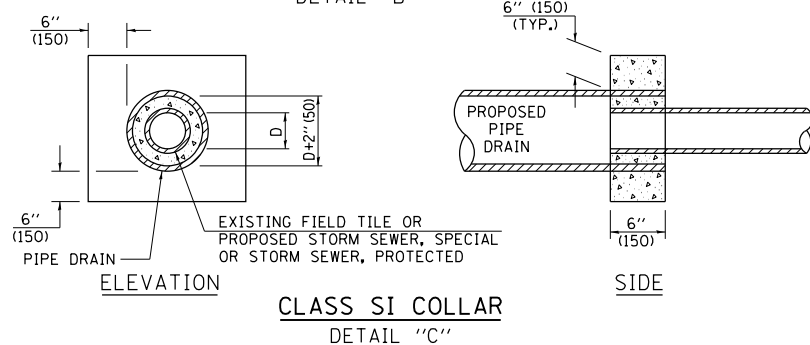
STORM SEWER LESS THAN 2' (600 mm) BELOW DITCH FLOW LINE AND STORM SEWERS CROSSING UNDER PAVEMENTS AND PAVED DITCHES



**HEADWALL FOR BACKSLOPE OUTLET**  
DETAIL "A"



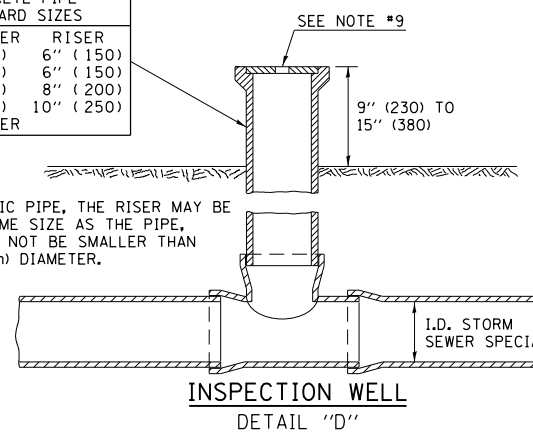
**CONCRETE COLLAR**  
DETAIL "B"



**CLASS SI COLLAR**  
DETAIL "C"

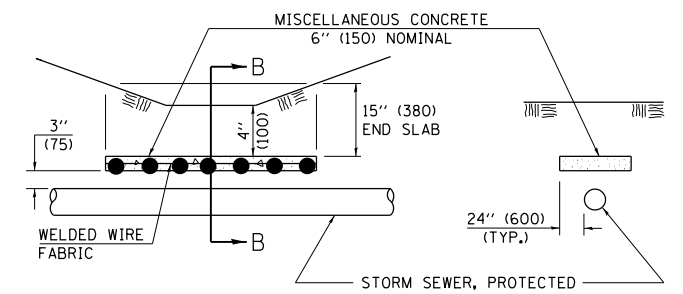
| CONCRETE PIPE STANDARD SIZES |           |
|------------------------------|-----------|
| STORM SEWER RISER            |           |
| 6" (150)                     | 6" (150)  |
| 8" (200)                     | 6" (150)  |
| 10" (250)                    | 8" (200)  |
| 12" (300)                    | 10" (250) |
| OR GREATER                   |           |

FOR PLASTIC PIPE, THE RISER MAY BE OF THE SAME SIZE AS THE PIPE, BUT SHALL NOT BE SMALLER THAN 4" (100 mm) DIAMETER.



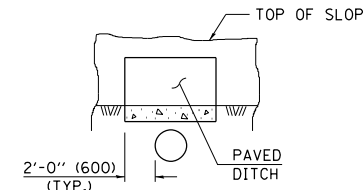
**GENERAL NOTES**

- EXISTING FIELD TILE ENCOUNTERED BY EXPLORATION TRENCH SHALL BE INSPECTED BY THE ENGINEER FOR UNOBSTRUCTED FLOW WITHIN THE LIMITS OF THE RIGHT-OF-WAY.
- ONLY FIELD TILE THAT DOES NOT HAVE SATISFACTORY FLOW AND OR HAS VISIBLE SIGNS OF DETERIORATION (SINK HOLES, ETC.) SHALL BE REPLACED WITHIN THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH METHOD "B".
- INSPECTION WELLS SHALL BE CONSTRUCTED APPROXIMATELY 6" (150 mm) INSIDE OF BOTH RIGHT-OF-WAY LINES AT ALL FIELD TILE LOCATIONS.
- EXISTING FIELD TILE ABANDONED UNDER EXISTING PAVEMENTS OR PAVED SHOULDERS SHALL BE FILLED WITH FLOWABLE GROUT AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID FOR ACCORDING TO ARTICLE 109.04.
- NON-CIRCULAR FIELD TILE SHALL BE REPLACED WITH STORM SEWER, SPECIAL OF AT LEAST THE SAME CROSS SECTIONAL AREA. ALL EXISTING FIELD TILE SHALL BE REPLACED WITH STORM SEWER OF THE TYPE REQUIRED FOR THE MINIMUM DEPTH OF COVER.
- THE 6" (150 mm) CONCRETE SLAB OR DITCH LINING SHALL BE POURED THE LENGTH OF THE TRENCH AT ALL DITCH FLOW LINE LOCATIONS WITHIN THE RIGHT-OF-WAY WITH LESS THAN 2' (600 mm) OF EARTH COVER. MISCELLANEOUS CONCRETE SHALL BE USED ACCORDING TO SECTION 611.
- ALL MISCELLANEOUS SLABS, APRONS AND DITCH LININGS SHALL BE REINFORCED WITH WELDED WIRE FABRIC AS SHOWN FOR PAVED DITCH IN STANDARD 606401.
- HEADWALL FOR BACKSLOPE OUTLET MAY BE USED FOR PIPE DRAIN DIAMETERS UP TO 10" (250 mm). SPECIAL DESIGNS WILL BE REQUIRED FOR LARGER SIZES.
- THE INSPECTION WELL LID FOR P.C.C. PIPE SHALL BE CONSTRUCTED OF 3/8" (10 mm) CAST IRON AND PROVIDED WITH A 1" (25 mm) DIAMETER HOLE IN CENTER. THE LID FOR THE OTHER PIPE MATERIALS SHALL BE A GRATE ASSEMBLY PREFABRICATED FOR AND COMPATIBLE WITH THE PIPE SYSTEM.



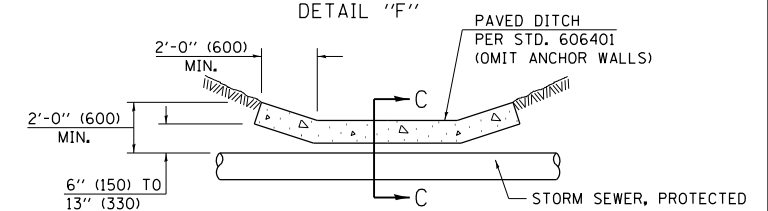
**SLAB ELEVATION**

**CONCRETE SLAB**  
DETAIL "F"



**SECTION C-C**

**PAVED DITCH**  
DETAIL "E"



**PAVED DITCH ELEVATION**

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 61101011A**

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
| PLOT SCALE = 40.0000' / in.        | DRAWN -    | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:09 PM | CHECKED -  | REVISED - |
|                                    | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

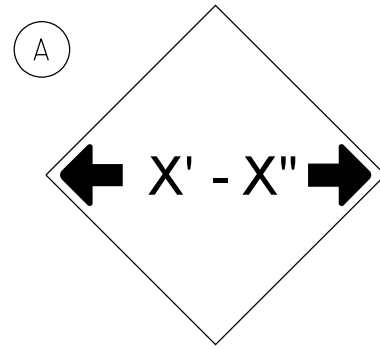
**FIELD TILE SYSTEMS (TREATMENT OF EXISTING)**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO.          |
|-------------|---------|--------|--------------|--------------------|
| I-74        | *       | **     | 142          | 112                |
|             |         |        |              | CONTRACT NO. 70790 |

DESIGNER NOTE: PROVIDE MAP WITH SIGN LOCATIONS (A, B, ETC.) AND COORDINATE WITH TRAFFIC OPERATIONS ENGINEER.

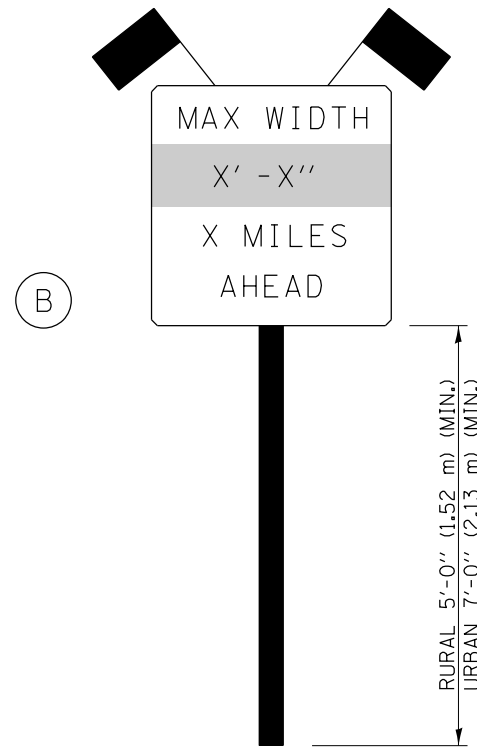
INCLUDE DISTRICT SPECIAL PROVISION - "WIDTH RESTRICTION SIGNING"



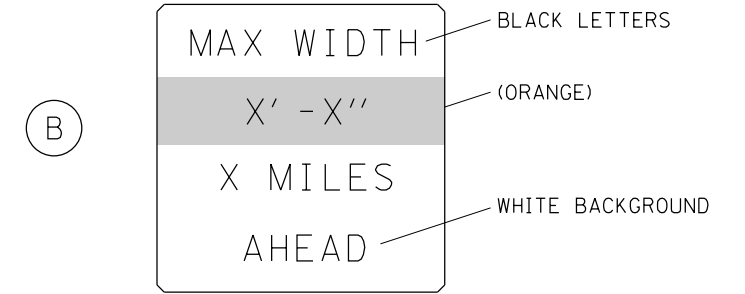
W12-2(0) - 48"x48" (1200x1200)

SIGN (A) 2 SIGNS - W12-2(0) - 48"x48" (1200x1200) ARE TO BE PLACED AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

SIGN (B) 2 SIGNS - (SIGN PANEL, TYPE II) AS SHOWN ARE TO BE PLACED AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.



SIGN PANEL, TYPE II



W12-I103(0) - 48"x48" (1200x1200)  
"D" LETTERS/NUMBERS

GENERAL NOTES

1. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED AND MAINTAINED BY THE CONTRACTOR.
2. ALL (B) SIGNS SHALL HAVE FLAGS INSTALLED UNLESS OTHERWISE DIRECTED.
3. LOCATIONS OF TRAFFIC CONTROL DEVICES MAY BE ADJUSTED BY THE ENGINEER.
4. ALL TRAFFIC CONTROL SHOWN ON THIS SHEET SHALL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR WIDTH RESTRICTION SIGNING.
5. ALL SIGNS SHALL BE POST MOUNTED UNLESS OTHERWISE DIRECTED.
6. ALL SIGNS SHOWN ORANGE (O) SHALL BE FLUORESCENT ORANGE.
7. ALL SIGNS SHOWN SHALL CONSIST OF THE CURRENT RETROREFLECTIVE SHEETING REQUIREMENTS AS OUTLINED IN SECTION 1106.01 OF THE STANDARD SPECIFICATIONS BOOK.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

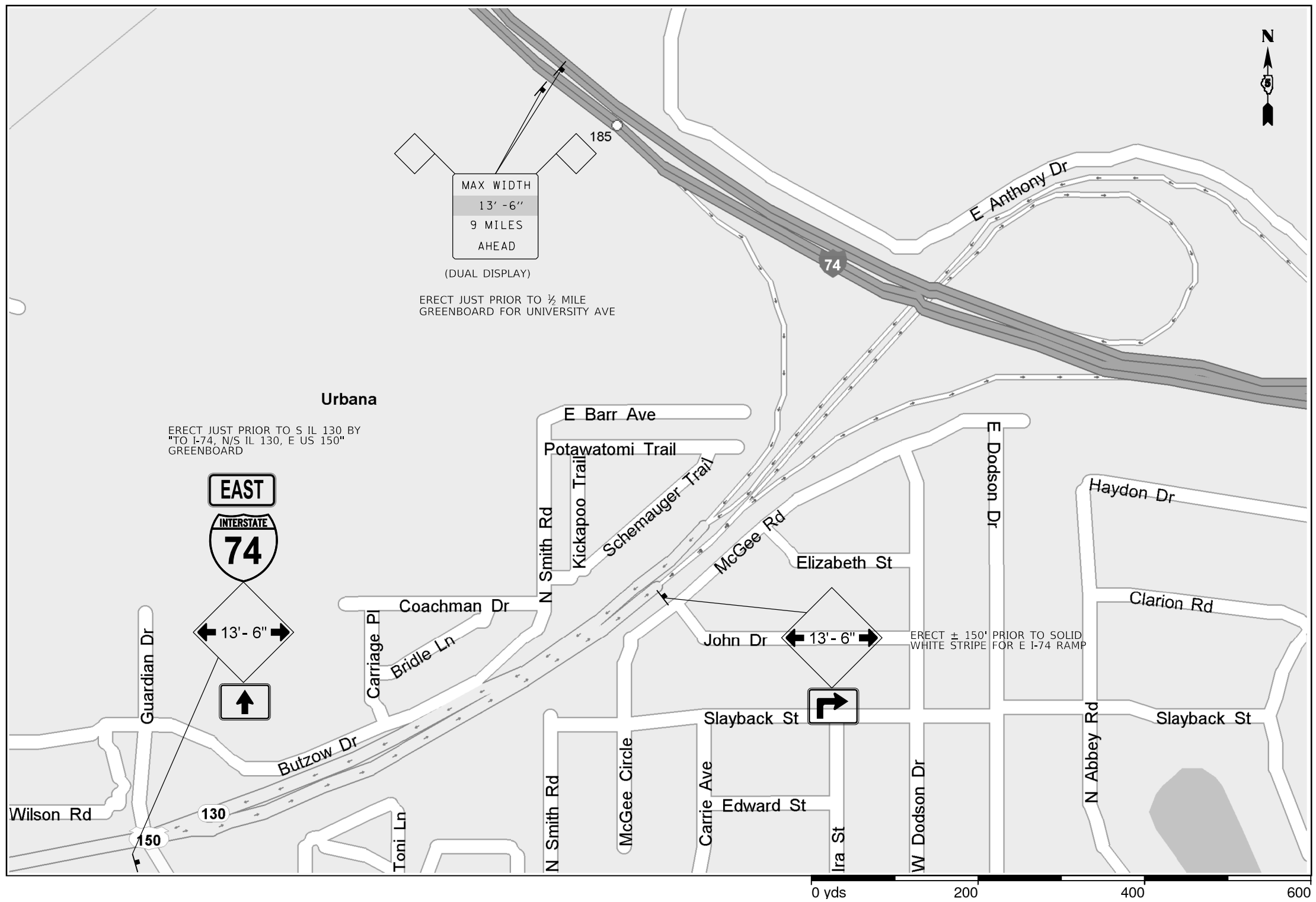
MODEL: Default  
FILE: Model - 03/27/2019 5:05:46:46:46.dgn

**DISTRICT 5 DETAIL NO. X7200201**

|                                    |            |           |   |                                  |       |    |        |             |         |          |                    |           |
|------------------------------------|------------|-----------|---|----------------------------------|-------|----|--------|-------------|---------|----------|--------------------|-----------|
| USER NAME = bebery                 | DESIGNED - | REVISED - | <b>STATE OF ILLINOIS<br/>DEPARTMENT OF TRANSPORTATION</b> | <b>WIDTH RESTRICTION SIGNING</b> |       |    |        | F.A.I. RTE. | SECTION | COUNTY   | TOTAL SHEETS       | SHEET NO. |
|                                    | DRAWN -    | REVISED - |   | SCALE: N.T.S.                    | SHEET | OF | SHEETS | STA.        | TO STA. | ILLINOIS | FED. AID PROJECT   |           |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |   |                                  |       |    |        |             |         | **       | 142                | 113       |
| PLOT DATE = 1/24/2019 - 4:06:10 PM | DATE -     | REVISED - |   |                                  |       |    |        |             |         |          | CONTRACT NO. 70790 |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



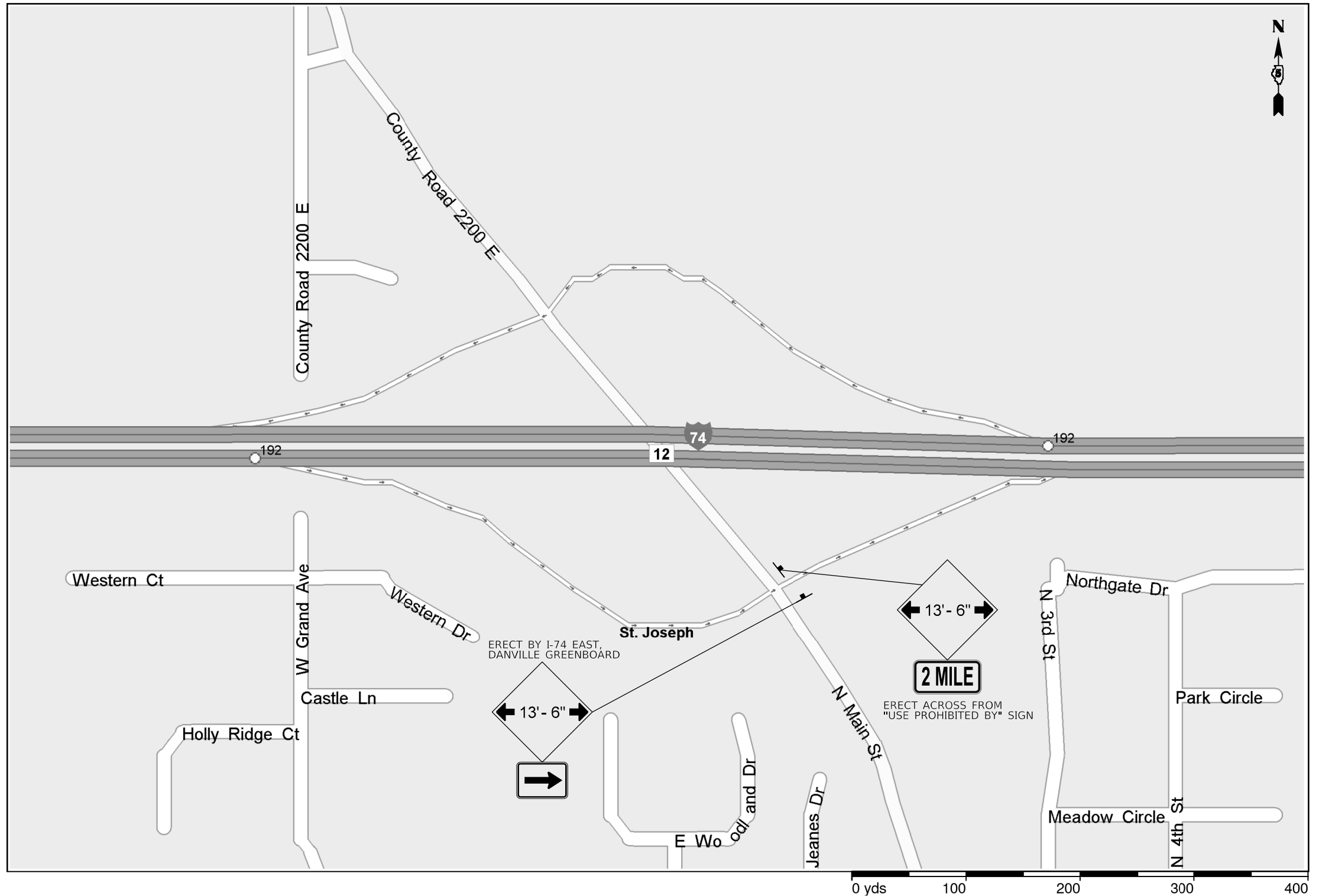
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 FILE: N:\m\70790\chc-deta\114.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:10 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

| WIDTH RESTRICTION DETAIL |       |           |              |
|--------------------------|-------|-----------|--------------|
| SCALE: N.T.S.            | SHEET | OF SHEETS | STA. TO STA. |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 114       |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |



MODEL: Default  
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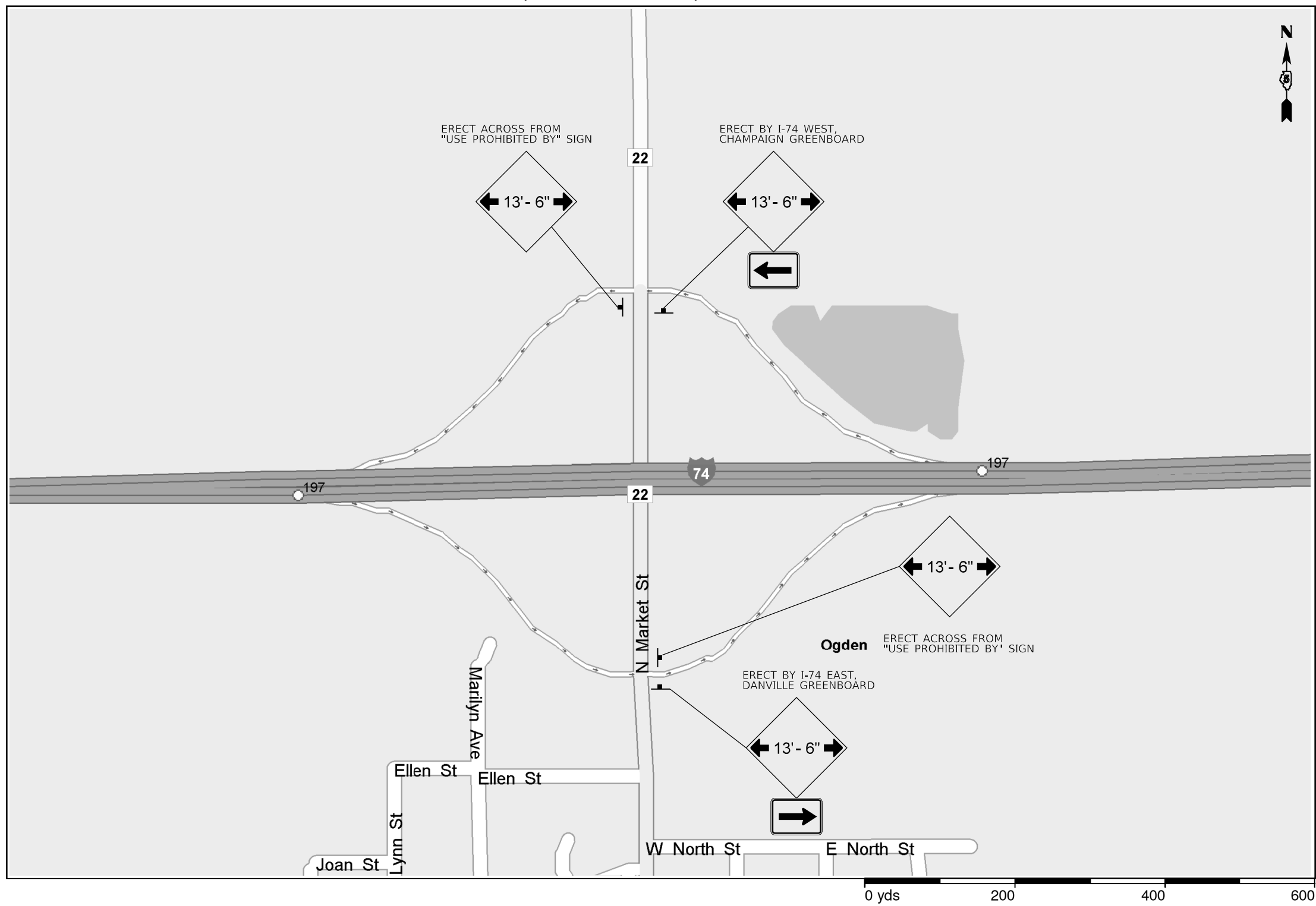
|                                    |            |           |
|------------------------------------|------------|-----------|
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|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:16 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WIDTH RESTRICTION DETAIL**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 115       |
| CONTRACT NO. 70790 |         |        |              |           |



MODEL: Default  
 FILE: N:\I-74\70790\chc-detailed.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:23 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

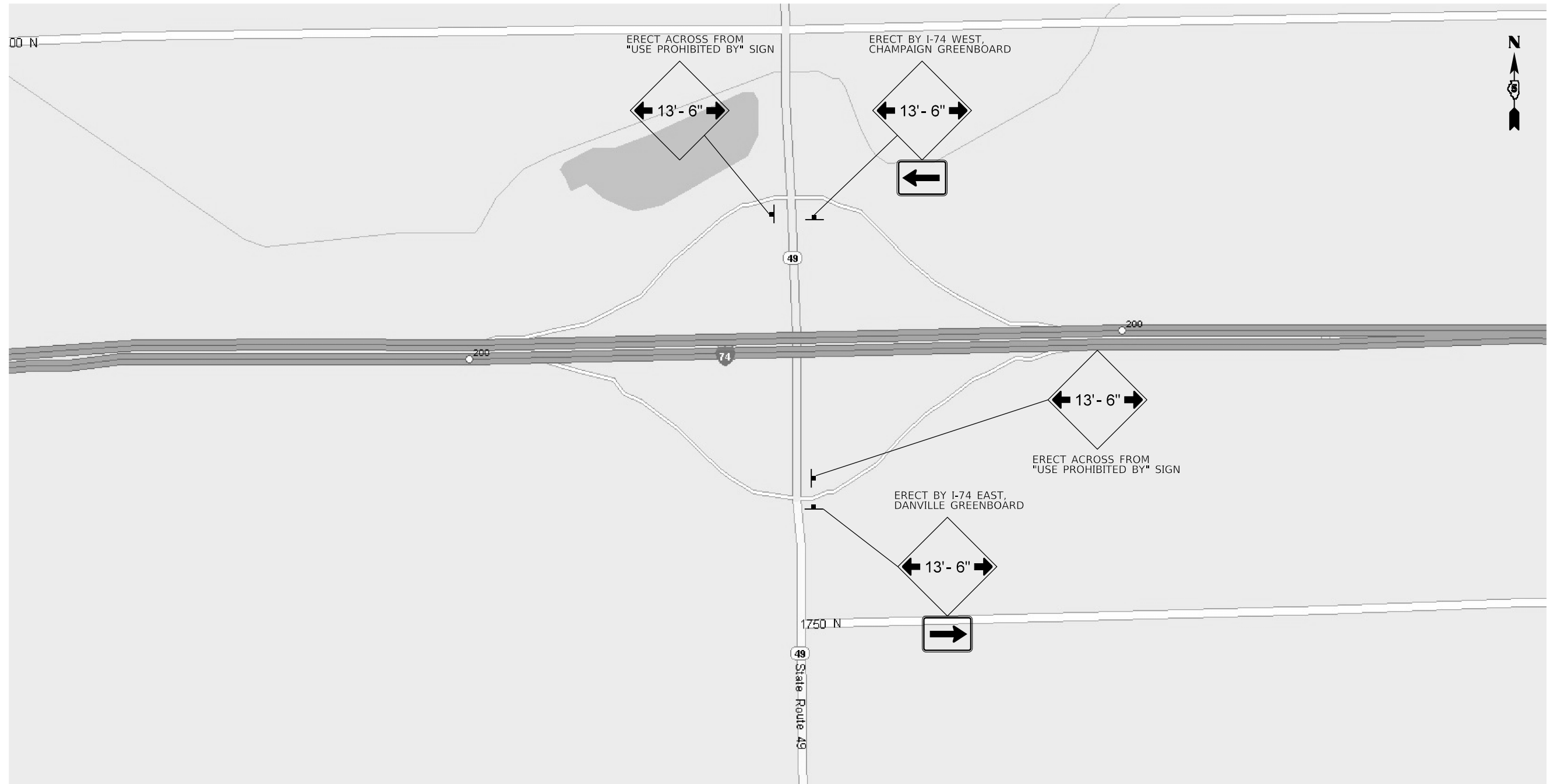
**WIDTH RESTRICTION DETAIL**

SCALE: N.T.S.    SHEET    OF    SHEETS    STA.    TO STA.

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 116       |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3    \*\* CHAMPAIGN & VERMILION





MODEL: Default  
 FILE: Model\_0305170790-shc-detailed.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:29 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

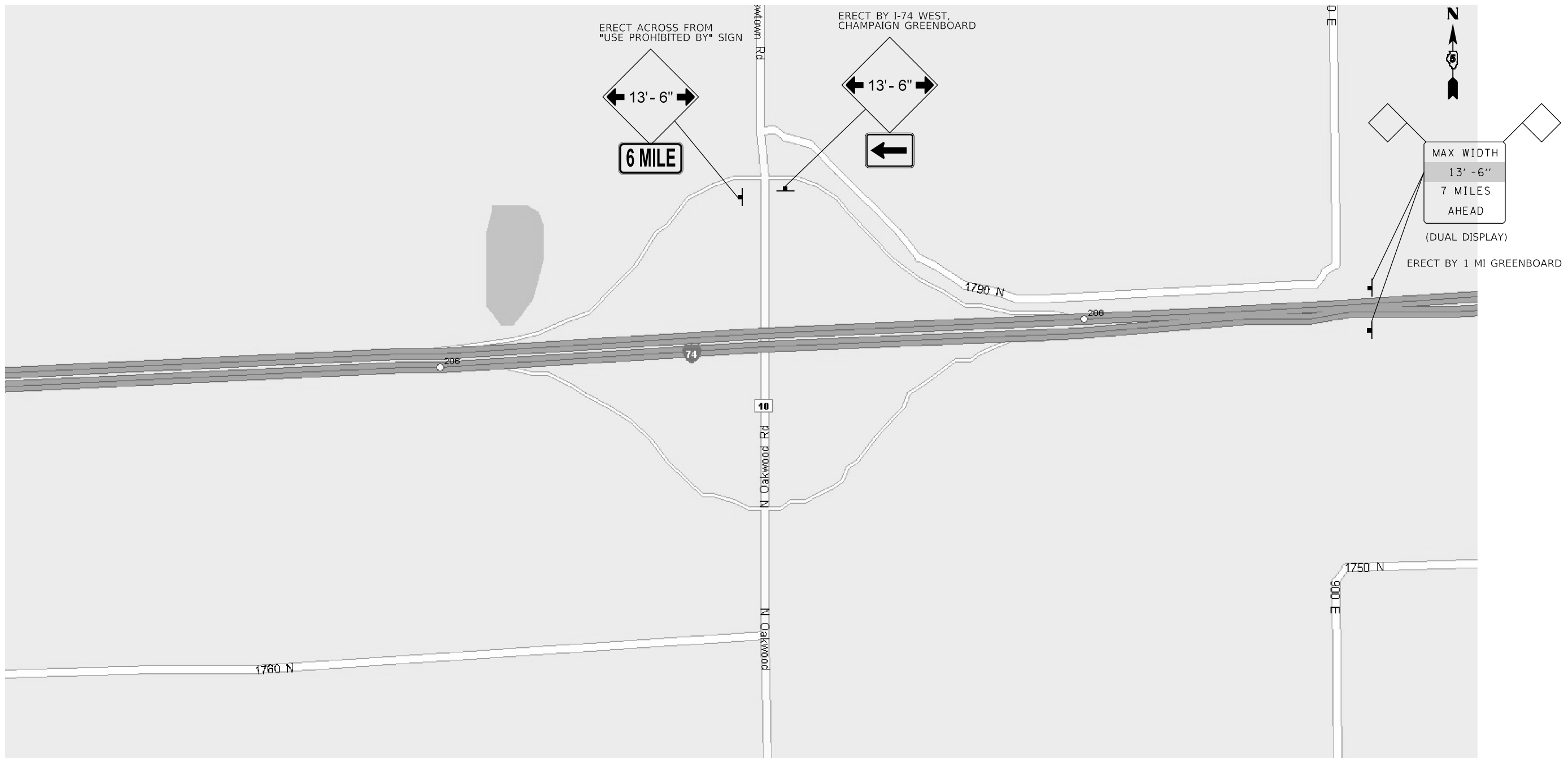
**WIDTH RESTRICTION DETAIL**

SCALE: N.T.S.    SHEET    OF    SHEETS    STA.    TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 117       |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



MODEL: Default  
 FILE: Model\_0305170790-shc-detailed.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:34 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**WIDTH RESTRICTION DETAIL**

SCALE: N.T.S.    SHEET    OF    SHEETS    STA.    TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 118       |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7,8;92-8,9)RS-2(92-10)RS-3    \*\* CHAMPAIGN & VERMILION

**NOTES**

PRIOR TO INSTALLING POST MOUNTED SIGNS, THE CONTRACTOR SHALL CONTACT J.U.L.I.E.

ALL SIGN MESSAGES AND SIZES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.

THE DISTRICT SHALL FURNISH THE SIGN PANELS. THE CONTRACTOR SHALL FURNISH AND ERECT THE SIGNS AT THE LOCATIONS DIRECTED BY THE ENGINEER. ALL THE SIGNS SHALL BE POST MOUNTED.

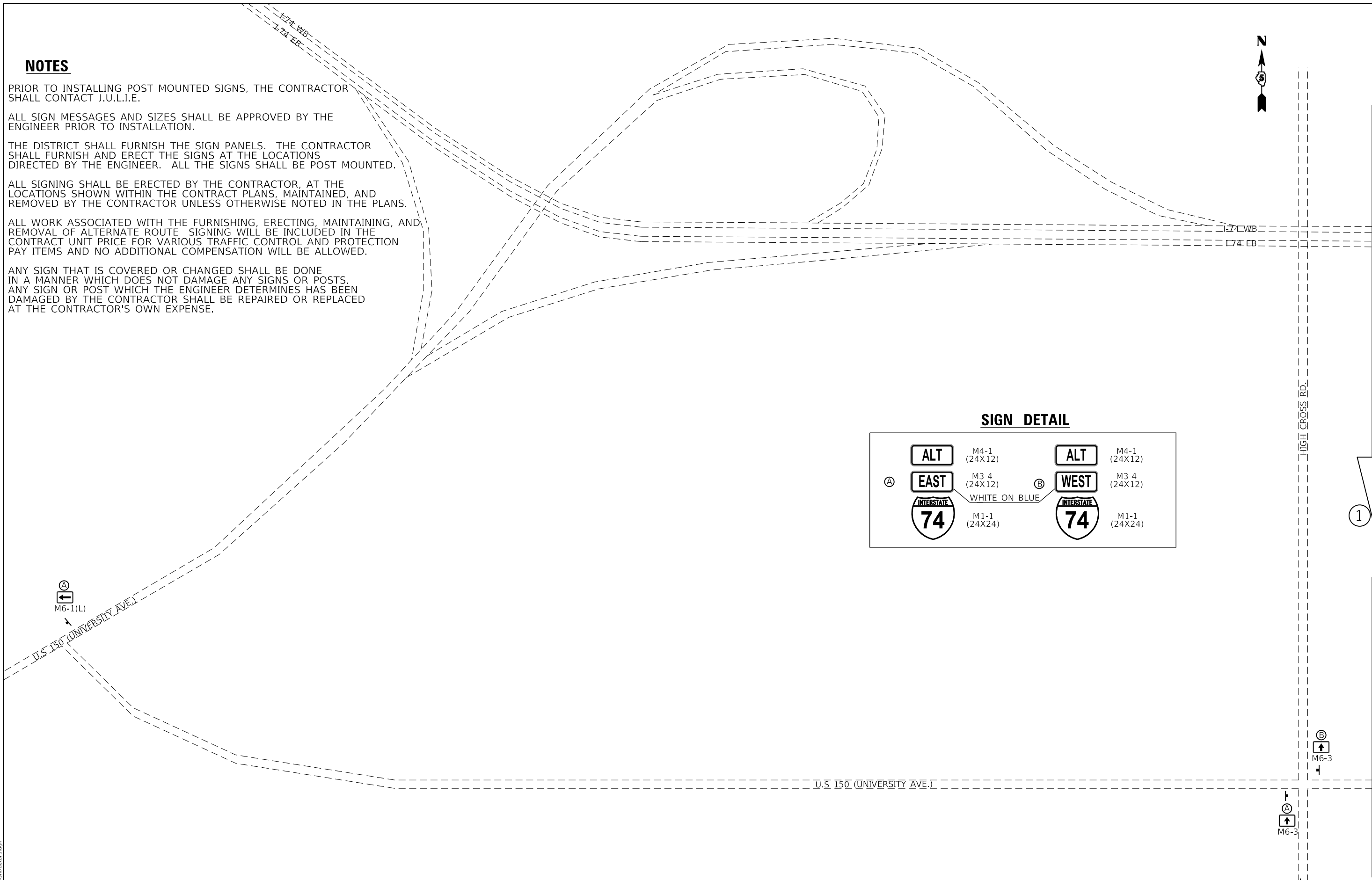
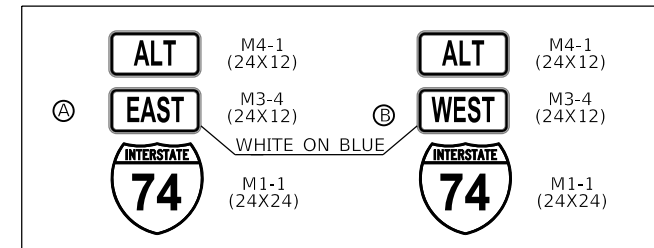
ALL SIGNING SHALL BE ERECTED BY THE CONTRACTOR, AT THE LOCATIONS SHOWN WITHIN THE CONTRACT PLANS, MAINTAINED, AND REMOVED BY THE CONTRACTOR UNLESS OTHERWISE NOTED IN THE PLANS.

ALL WORK ASSOCIATED WITH THE FURNISHING, ERECTING, MAINTAINING, AND REMOVAL OF ALTERNATE ROUTE SIGNING WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR VARIOUS TRAFFIC CONTROL AND PROTECTION PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

ANY SIGN THAT IS COVERED OR CHANGED SHALL BE DONE IN A MANNER WHICH DOES NOT DAMAGE ANY SIGNS OR POSTS. ANY SIGN OR POST WHICH THE ENGINEER DETERMINES HAS BEEN DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S OWN EXPENSE.



**SIGN DETAIL**



MODEL: Default  
FILE: Model\_0305201905-shc-draft.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:38 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

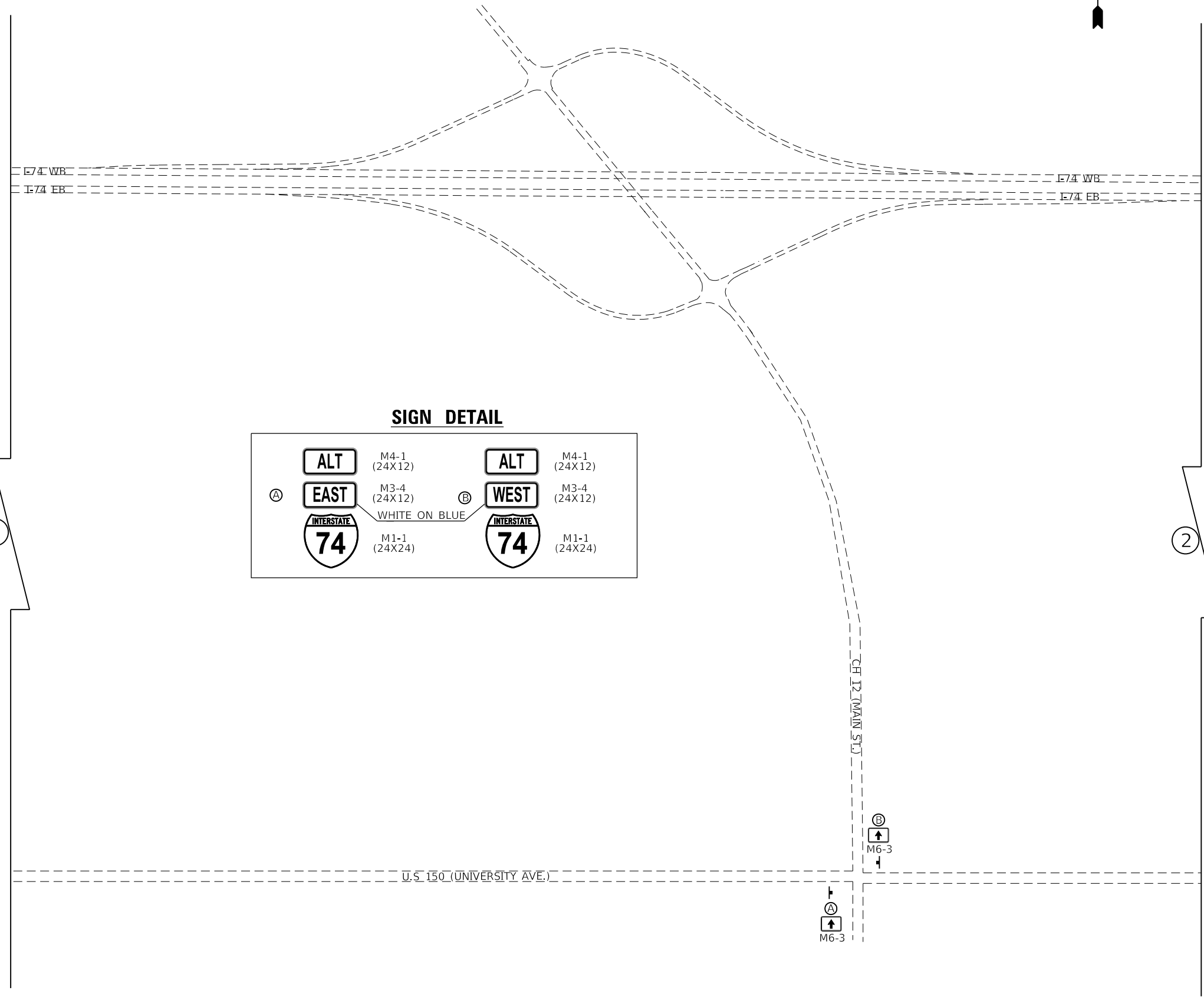
**ALTERNATE ROUTE SIGNING**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

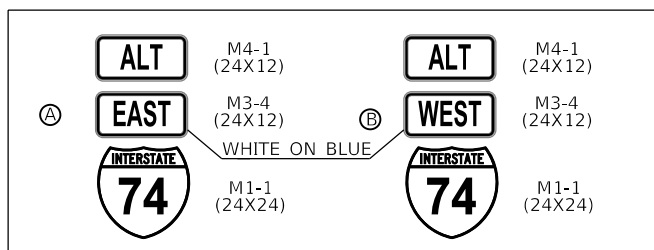
|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 119       |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



**SIGN DETAIL**



MODEL: Default  
 FILE: ModelE\_...0327090-shc-dtbl.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:38 PM | DATE -     | REVISED - |

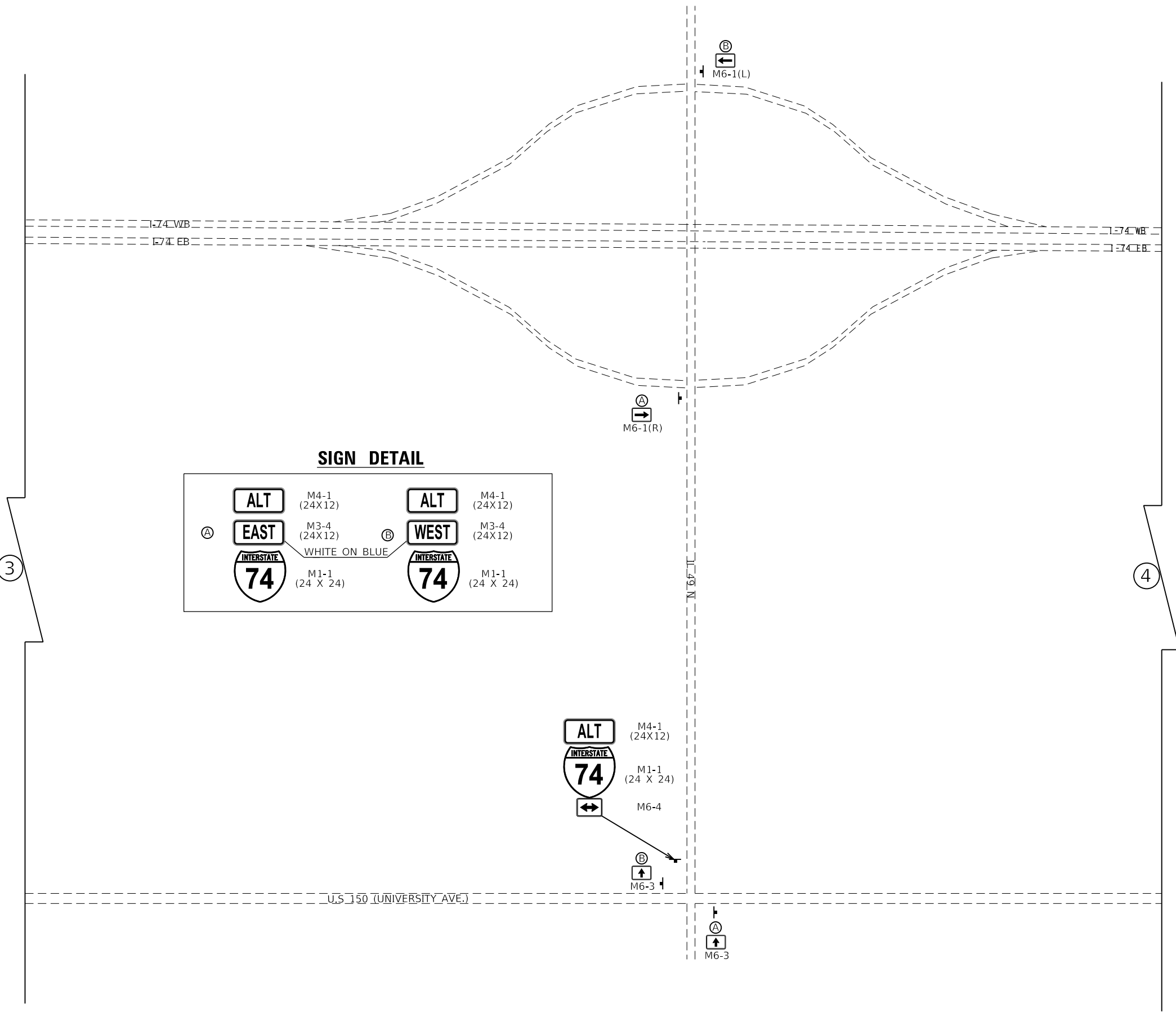
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**ALTERNATE ROUTE SIGNING**







SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.




|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 120       |
| CONTRACT NO. 70790 |         |        |              |           |





**SIGN DETAIL**

|   |  |
|---|--|
| <br>M4-1<br>(24X12)     | <br>M4-1<br>(24X12)     |
| <br>M3-4<br>(24X12)    | <br>M3-4<br>(24X12)    |
| WHITE ON BLUE   |  |
| <br>M1-1<br>(24 X 24) | <br>M1-1<br>(24 X 24) |

  
 M4-1  
 (24X12)  
  
 M1-1  
 (24 X 24)  
  
 M6-4

MODEL: Default  
 FILE: Model\_030519090-shc-draw.dgn

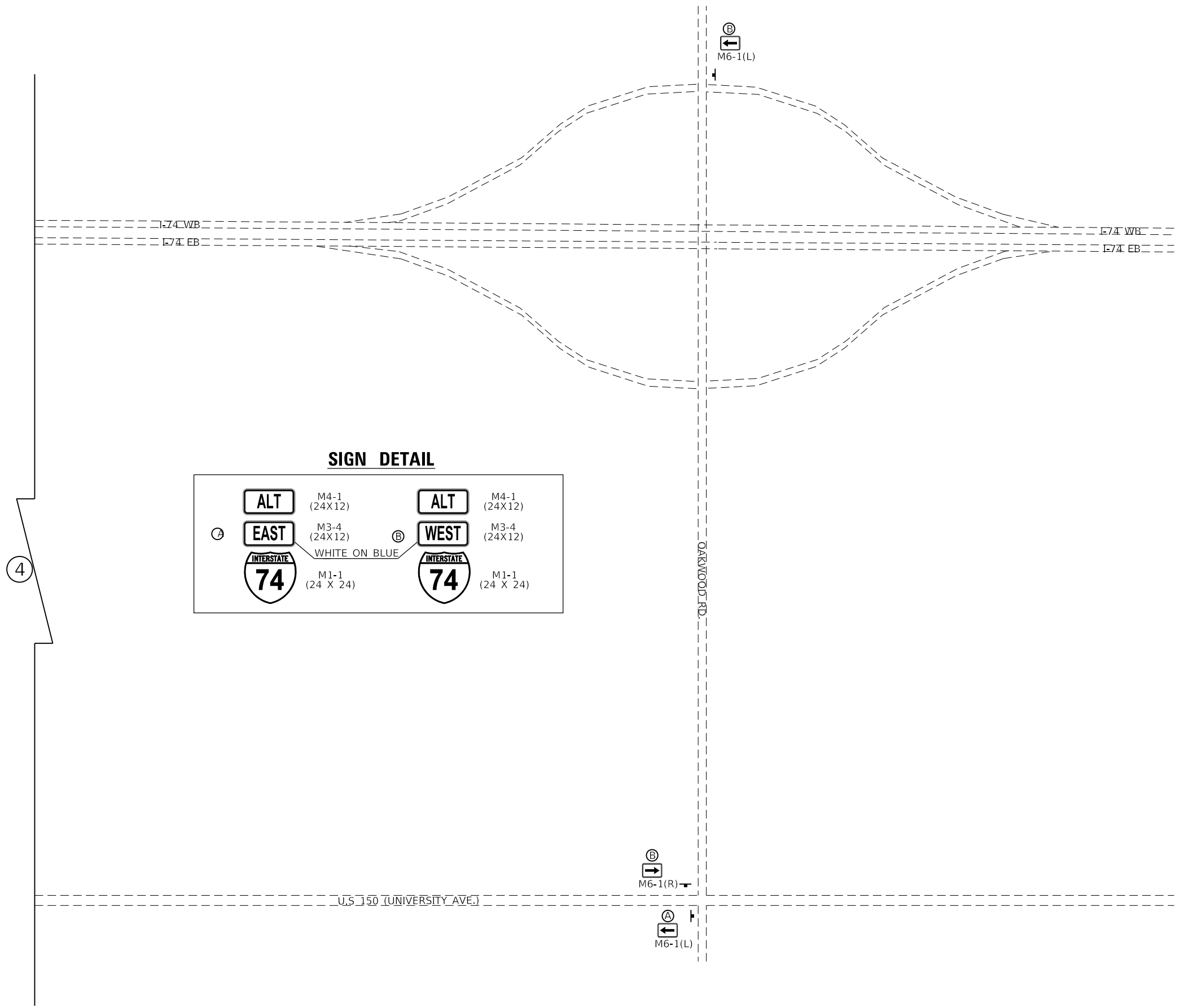
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:39 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

| ALTERNATE ROUTE SIGNING |                 |              |  |
|-------------------------|-----------------|--------------|--|
| SCALE: N.T.S.           | SHEET OF SHEETS | STA. TO STA. |  |

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 122       |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION



**SIGN DETAIL**

|                           |                           |
|---------------------------|---------------------------|
| <p>M4-1<br/>(24X12)</p>   | <p>M4-1<br/>(24X12)</p>   |
| <p>M3-4<br/>(24X12)</p>   | <p>M3-4<br/>(24X12)</p>   |
| <p>M1-1<br/>(24 X 24)</p> | <p>M1-1<br/>(24 X 24)</p> |

WHITE ON BLUE

MODEL: Default  
 FILE: N:\GIS\70790-shc-detailed.dgn

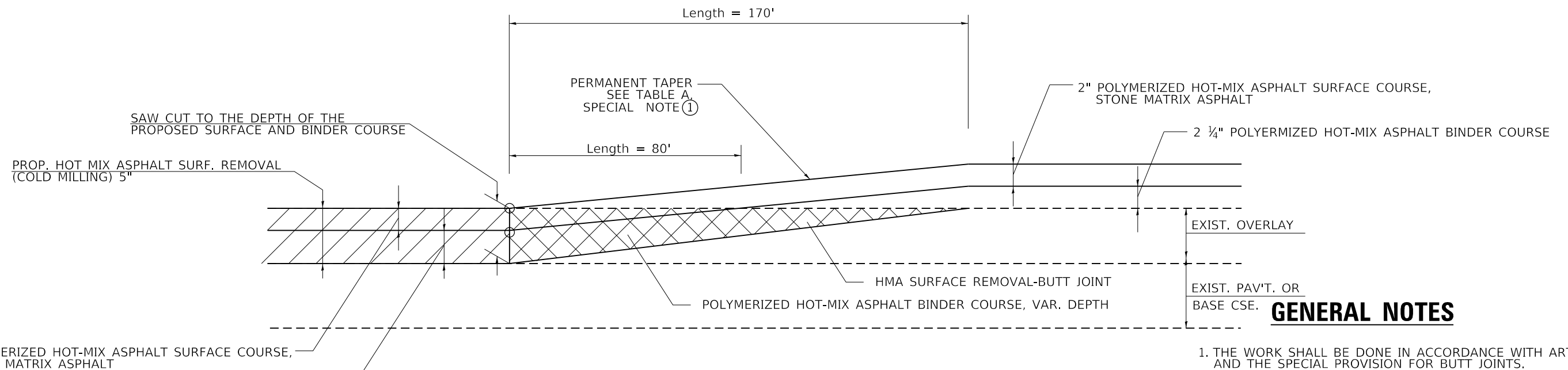
|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:39 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

| ALTERNATE ROUTE SIGNING |                 |              |  |
|-------------------------|-----------------|--------------|--|
| SCALE: N.T.S.           | SHEET OF SHEETS | STA. TO STA. |  |

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 123       |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3      \*\* CHAMPAIGN & VERMILION



### PASSING LANE

TO BE USED:

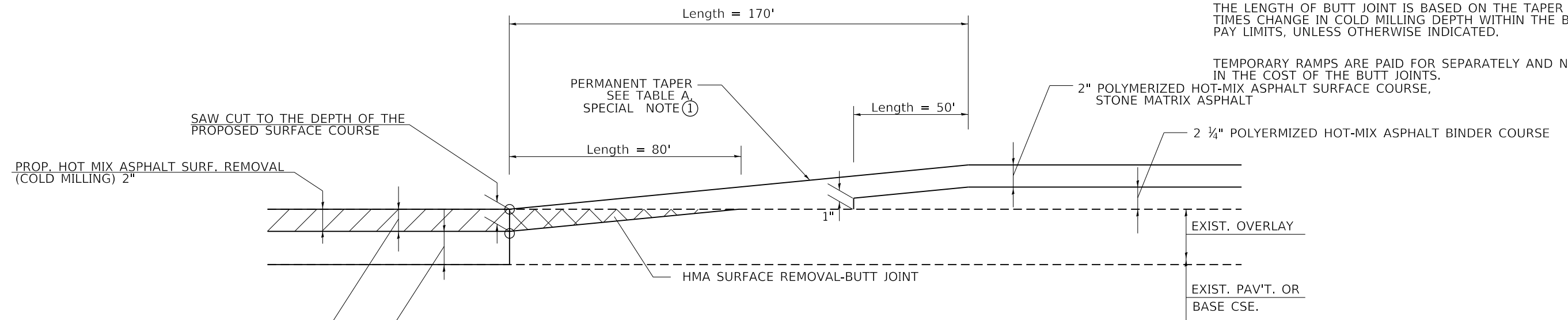
- I-74 WB STA 978+53.97 TO STA 980+23.97
- I-74 EB STA 978+53.97 TO STA 980+23.97
- I-74 WB STA 1023+45.08 TO STA 1025+15.08
- I-74 EB STA 1023+45.08 TO STA 1025+15.08

### GENERAL NOTES

1. THE WORK SHALL BE DONE IN ACCORDANCE WITH ARTICLE 406.08 AND THE SPECIAL PROVISION FOR BUTT JOINTS.
2. THE PAVEMENT SURFACE TO BE REMOVED MAY BE EITHER BITUMINOUS OR P.C. CONCRETE. THE WORK SHALL BE PERFORMED IN ACCORDANCE WITH ARTICLE 440.04 AND THE SPECIAL PROVISIONS FOR BUTT JOINTS.
3. THE SAW CUT JOINTS SHALL BE PRIMED JUST PRIOR TO THE PLACING OF BITUMINOUS MATERIAL. THE WORK WILL BE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF ARTICLE 406.05.

THE LENGTH OF BUTT JOINT IS BASED ON THE TAPER RATE TIMES CHANGE IN COLD MILLING DEPTH WITHIN THE BUTT JOINT PAY LIMITS, UNLESS OTHERWISE INDICATED.

TEMPORARY RAMPS ARE PAID FOR SEPARATELY AND NOT INCLUDED IN THE COST OF THE BUTT JOINTS.



### DRIVING LANE

TO BE USED:

- I-74 WB STA 978+53.97 TO STA 980+23.97
- I-74 EB STA 978+53.97 TO STA 980+23.97
- I-74 WB STA 1023+45.08 TO STA 1025+15.08
- I-74 EB STA 1023+45.08 TO STA 1025+15.08

TABLE A  
TAPER RATES

| SPECIAL NOTE NUMBER | ELEMENT                   | TAPER RATE |
|---------------------|---------------------------|------------|
| ①                   | TAPER RATE                | 1:480      |
| ②                   | TEMPORARY RAMP TAPER RATE | 1:40       |

All dimensions are in inches (millimeters) unless otherwise noted.

MODEL: Default  
FILE: Model.dwg  
DATE: 1/24/2019 4:06:39 PM

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:39 PM | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

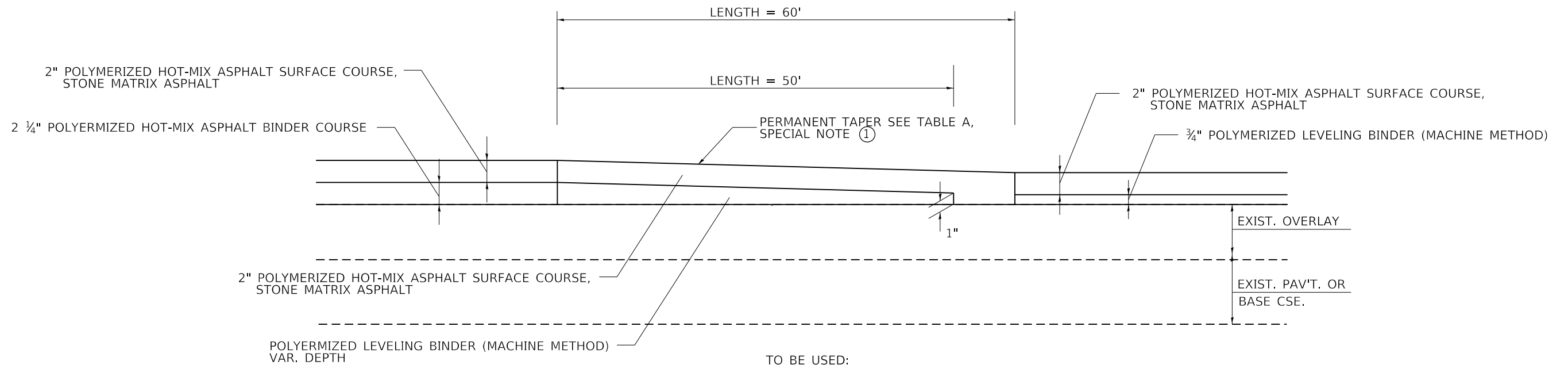
|                         |       |           |              |
|-------------------------|-------|-----------|--------------|
| BUTT JOINT DETAILS I-74 |       |           |              |
| SCALE: N.T.S.           | SHEET | OF SHEETS | STA. TO STA. |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 124       |
| CONTRACT NO. 70790 |         |        |              |           |

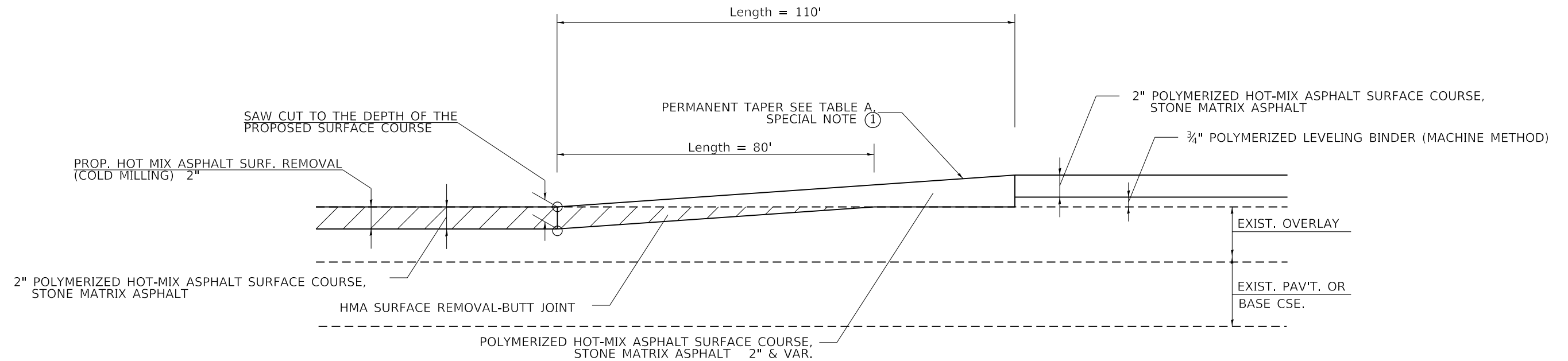
(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION





TO BE USED:  
 CH 22 RAMP A STA 389+16.72 TO STA 389+76.72  
 CH 22 RAMP B STA 312+00.65 TO STA 312+60.65  
 CH 22 RAMP C STA 488+98.29 TO STA 489+58.29  
 CH 22 RAMP D STA 111+98.99 TO STA 112+58.99



TO BE USED:  
 CH 22 RAMP A STA 400+99.56 TO STA 402+09.56  
 CH 22 RAMP B STA 299+58.16 TO STA 300+68.16  
 CH 22 RAMP C STA 501+01.15 TO STA 502+11.15  
 CH 22 RAMP D STA 99+35.96 TO STA 100+45.96

All dimensions are in inches (millimeters) unless otherwise noted.

MODEL: Default  
 FILE: Model - 10/27/2019 4:06:39 PM

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:39 PM | DATE -     | REVISED - |

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

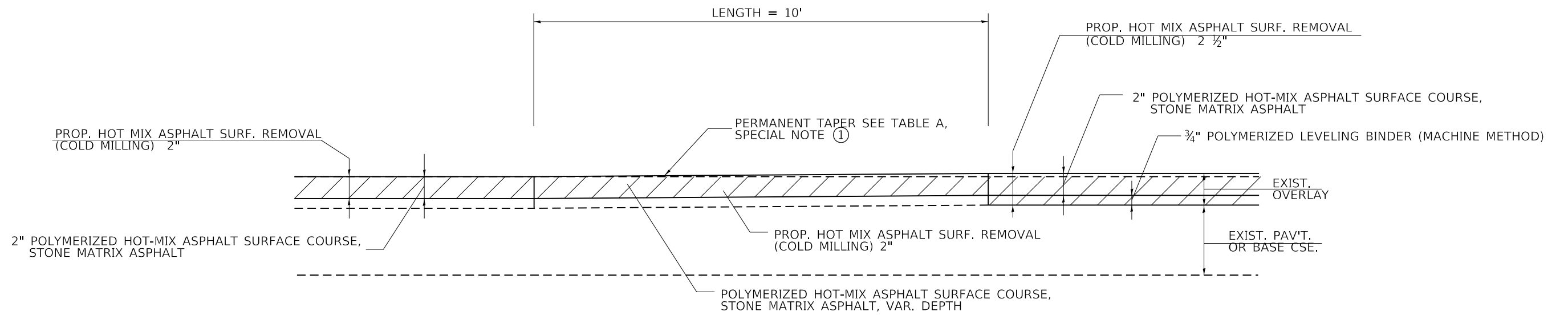
RESURFACING TRANSITION DETAILS CH 22

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 125       |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION



TO BE USED:

|              |                              |              |                              |
|--------------|------------------------------|--------------|------------------------------|
| IL 49 RAMP A | STA 48+90.76 TO STA 49+00.76 | IL 49 RAMP A | STA 61+03.65 TO STA 61+13.65 |
| IL 49 RAMP B | STA 74+99.75 TO STA 75+09.75 | IL 49 RAMP B | STA 63+25.52 TO STA 63+35.52 |
| IL 49 RAMP C | STA 48+82.74 TO STA 48+92.74 | IL 49 RAMP C | STA 60+78.08 TO STA 60+88.08 |
| IL 49 RAMP D | STA 71+25.66 TO STA 71+35.66 | IL 49 RAMP D | STA 0+89.54 TO STA 0+99.54   |

All dimensions are in inches (millimeters) unless otherwise noted.

MODEL: Default  
FILE: Model - 10/27/2019 4:06:40 PM

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemory                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:40 PM | DATE -     | REVISED - |

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

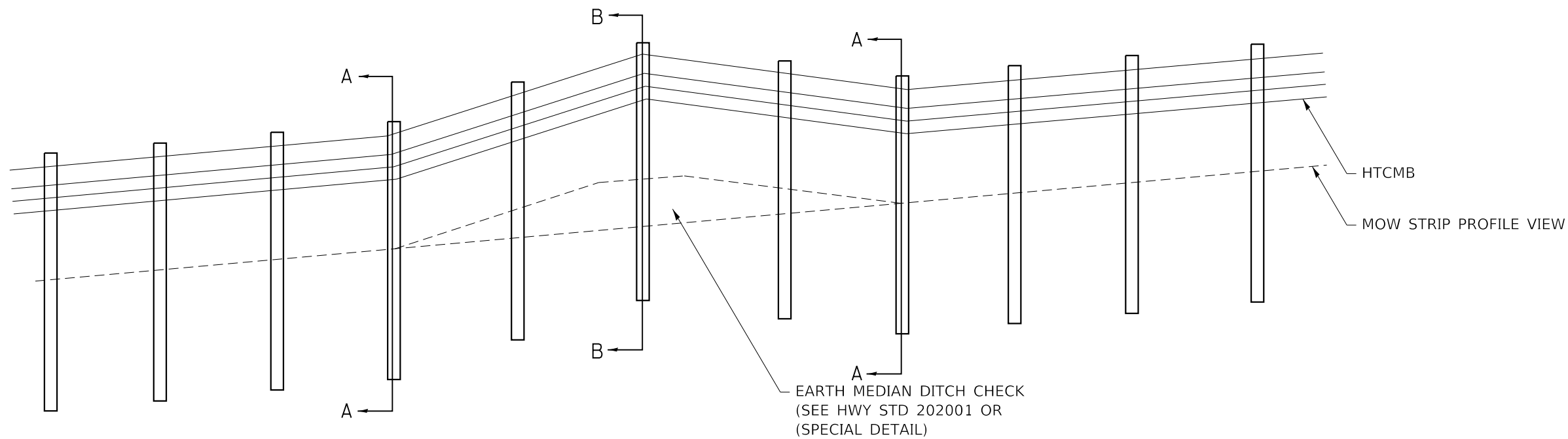
RESURFACING TRANSITION DETAILS IL 49

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

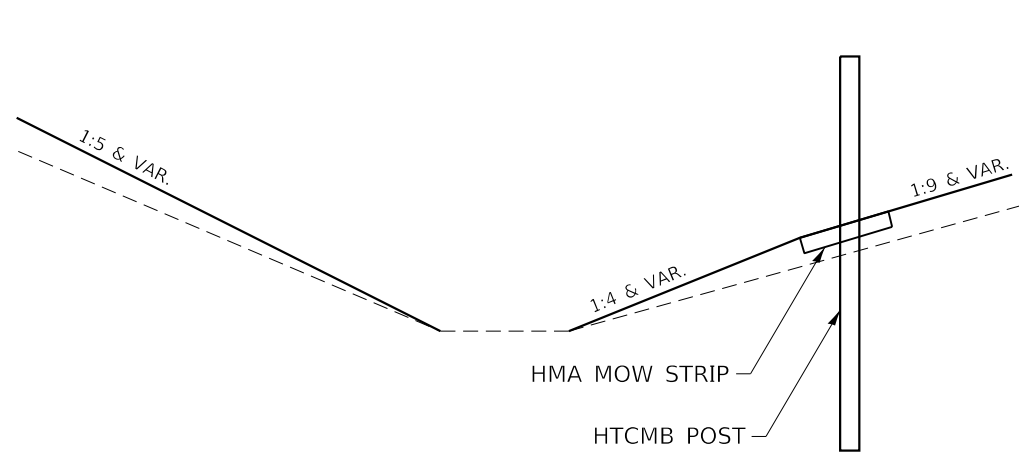
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 126       |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3

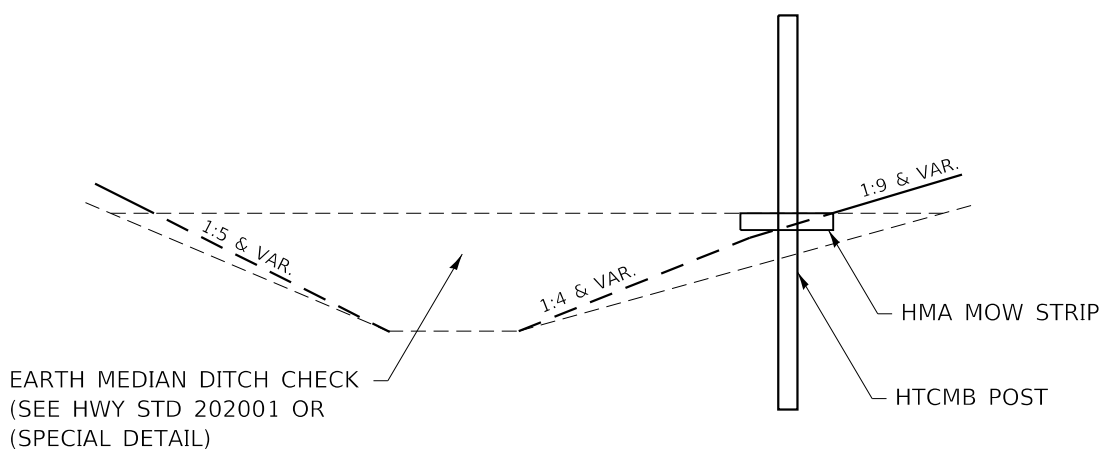
\*\* CHAMPAIGN & VERMILION



MOW STRIP PROFILE VIEW



SECTION A-A



SECTION B-B

**NOTES**

WHEN MOW STRIPS INTERSECT EARTH MEDIAN DITCH CHECKS, THE MOW STRIP PROFILE AND CROSS SLOPE SHALL FOLLOW THE LONGITUDINAL AND TRANSVERSE SLOPE OF THE DITCH CHECK AS SHOWN IN THIS DETAIL.

POSTS SHALL BE SET AT THE LOCATIONS OR SECTION "A" AND SECTIONS "B". POSTS MAY BE REQUIRED TO BE PLACED BETWEEN THSES SECTIONS IN ORDER TO MEET MAXIMUM POST SPACING REQUIREMENTS. THE POSTS AND CABLE SHALL BE SECURED PER MANUFACTURER SPECIFICATIONS TO PREVENT THEM FROMN UPLIFTING.

MODEL: Default  
FILE: hma\_mf\_0305201905-hc-ditch.dgn

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40.0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:40 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

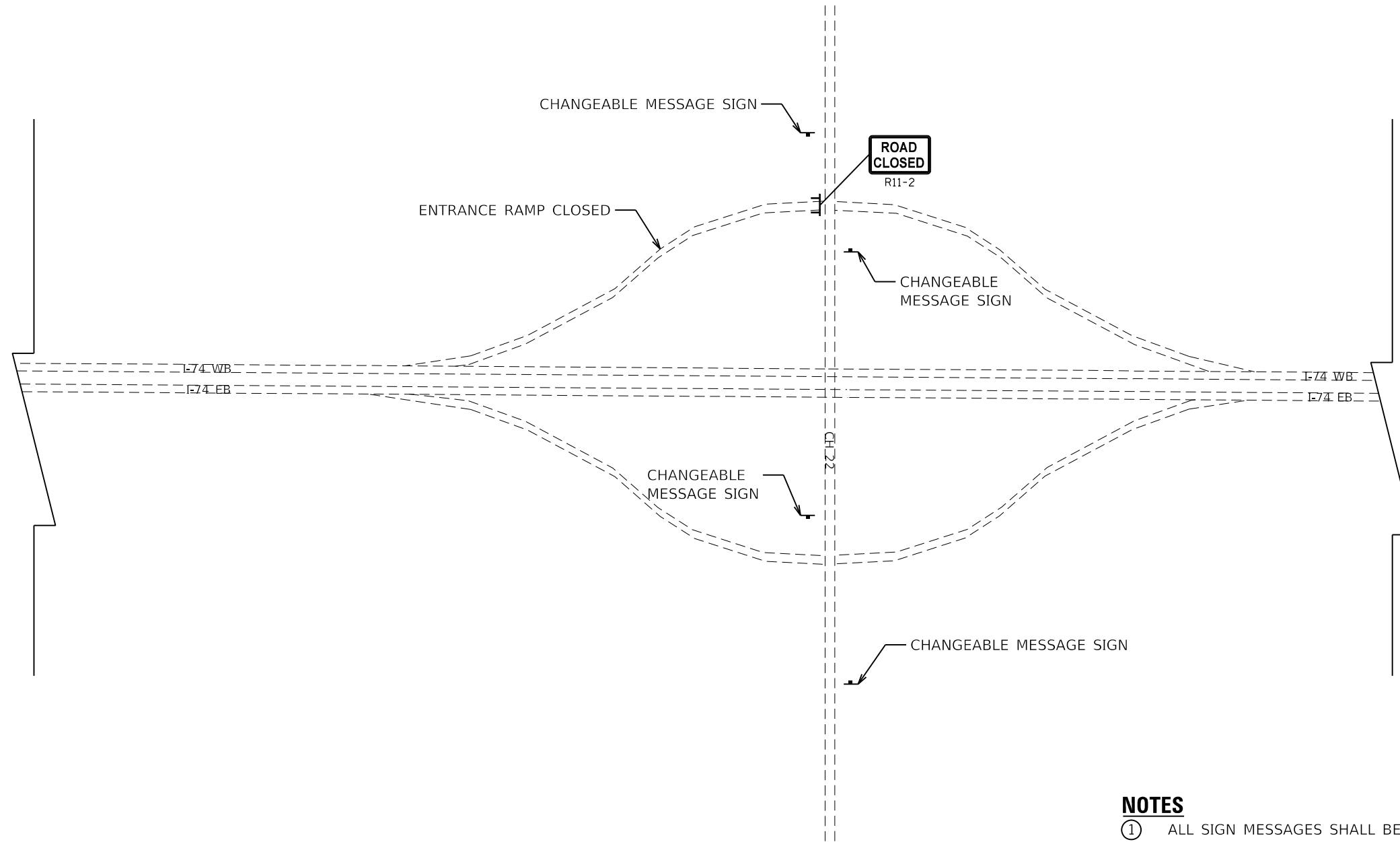
**HMA MOW STRIP  
PROFILE DETAIL**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 127       |
| CONTRACT NO. 70790 |         |        |              |           |







**NOTES**

- ① ALL SIGN MESSAGES SHALL BE APPROVED BY THE ENGINEER.
- ② THE CHANGEABLE MESSAGE SIGNS SHALL STATE THE DAY THE ROAD CLOSURE WILL BEGIN AND SHALL BE LOCATED AS DIRECTED BY THE ENGINEER. ONCE CONSTRUCTION BEGINS THE SIGNS SHALL STATE RAMP CLOSED USE I-74/IL149 INTERCHANGE. THE EXACT MESSAGES SHALL BE APPROVED BY THE ENGINEER. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE OF CAL DA FOR CHANGEABLE MESSAGE SIGN.
- ③ CONTRACTOR SHALL NOTIFY IDOT DISTRICT 5 OPERATIONS 21 DAYS IN ADVANCE OF ROAD CLOSURE.
- ④ RAMP SHALL ONLY BE CLOSED 1 CALENDAR DAY.
- ⑤ TYPE III BARRICADE AND ROAD CLOSED SIGN ARE INCLUDED IN THE COST OF THE CHANGEABLE MESSAGE SIGNS.

MODEL: Default  
FILE: Model1 - 10/27/2019 4:06:50 PM

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemory                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 40,0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:06:50 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**RAMP C DETOUR PLAN**

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
|---------------------------|---------|--------|--------------------|-----------|
| I-74                      | *       | **     | 142                | 130       |
| ILLINOIS FED. AID PROJECT |         |        | CONTRACT NO. 70790 |           |

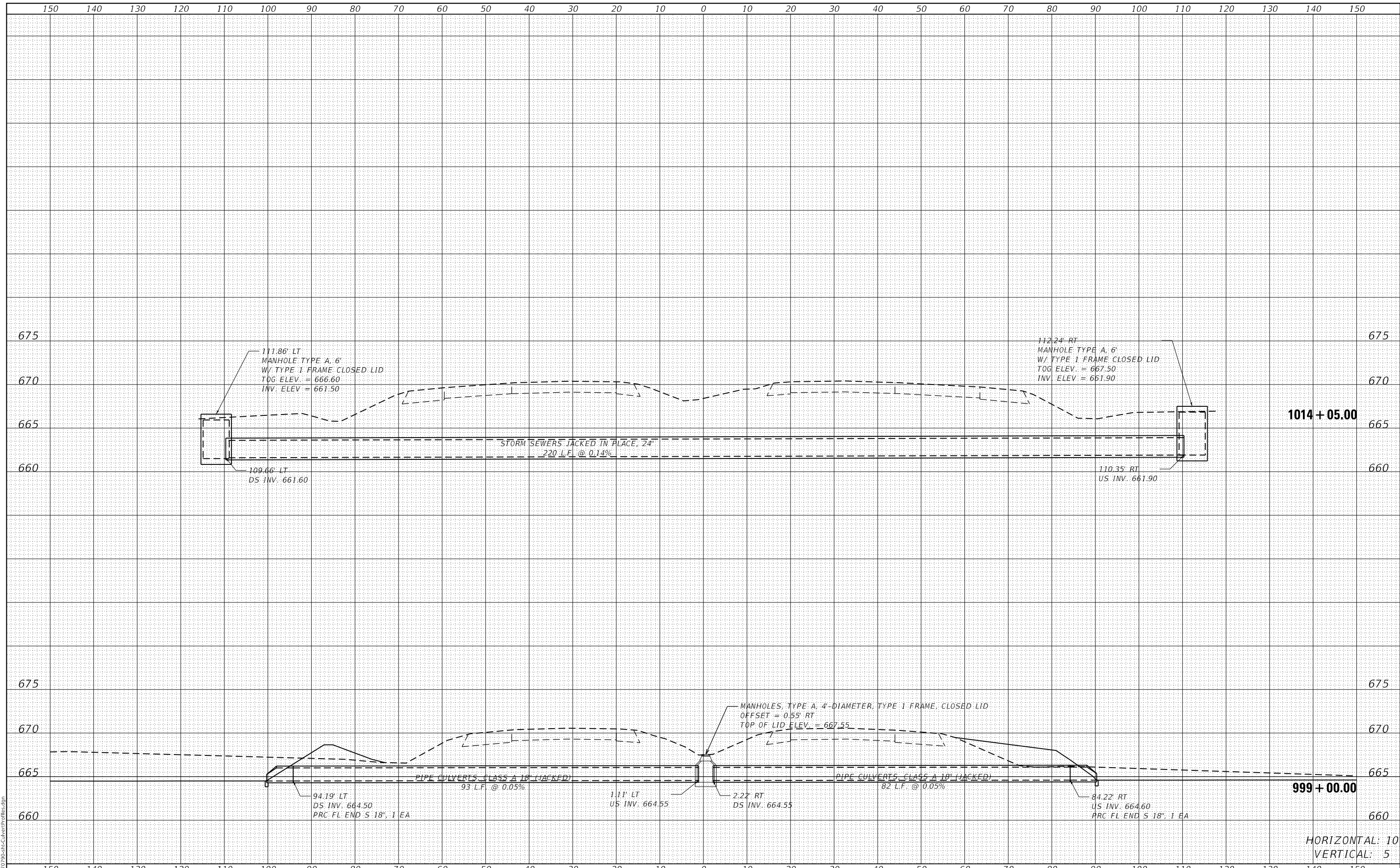
(10-7.8;92-8.9)RS-2(92-10)RS-3

\*\* CHAMPAIGN & VERMILION

|               |  |
|---------------|--|
| DATE          |  |
| BY            |  |
| SURVEYED      |  |
| PLOTTED       |  |
| TEMPLATE      |  |
| AREAS CHECKED |  |
| FINAL SURVEY  |  |
| NOTE BOOK     |  |
| NO.           |  |

|                 |  |
|-----------------|--|
| DATE            |  |
| BY              |  |
| SURVEYED        |  |
| PLOTTED         |  |
| TEMPLATE        |  |
| AREAS CHECKED   |  |
| ORIGINAL SURVEY |  |
| NOTE BOOK       |  |
| NO.             |  |

MODEL: Default  
FILE NAME: ...03/19/19-08-31-CulvertProfiles.dgn



1014 + 05.00

999 + 00.00

HORIZONTAL: 10  
VERTICAL: 5

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 20.0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:07:20 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CULVERT PROFILES  
I-74**

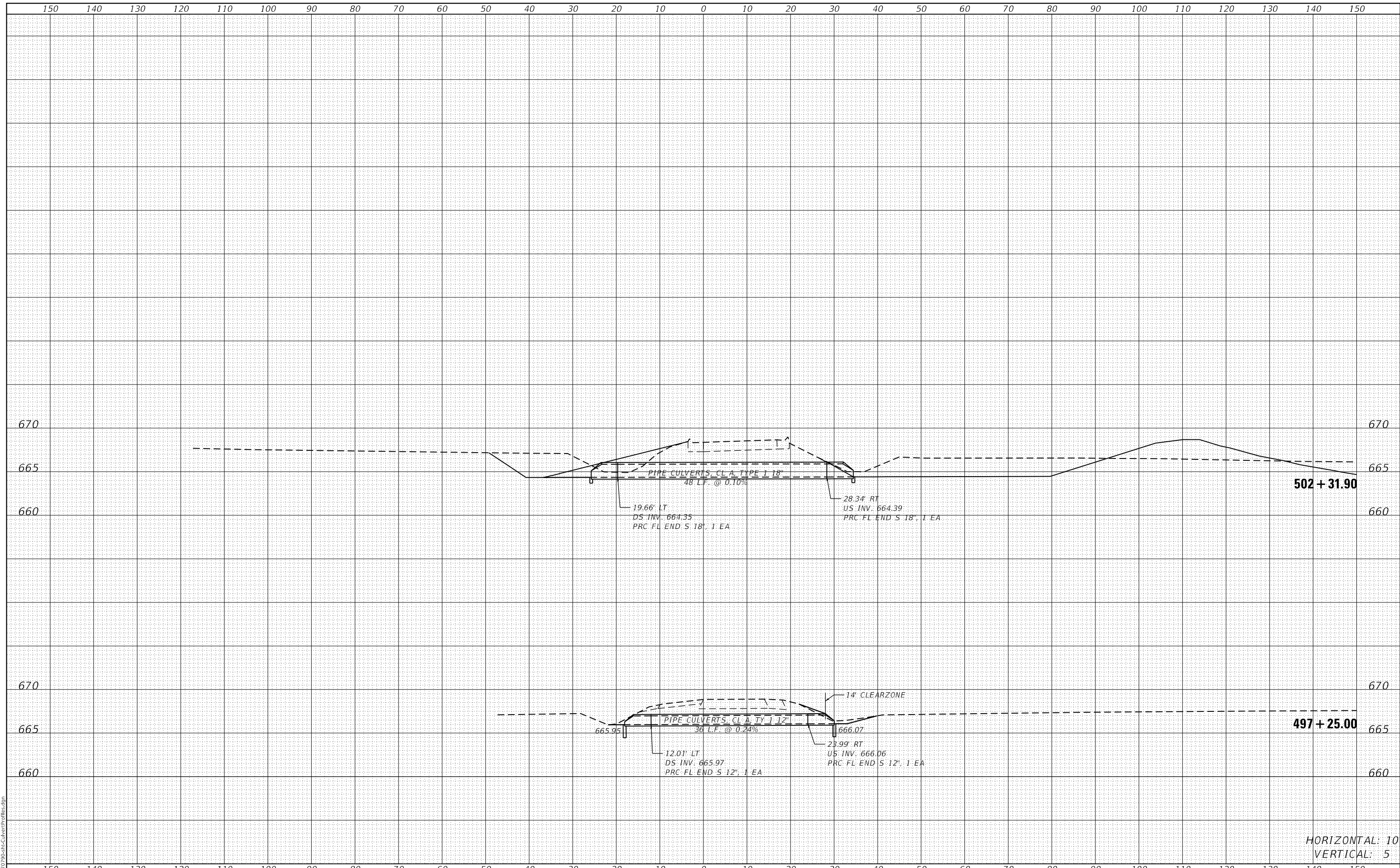
SCALE: 1:10H:1.5V SHEET OF SHEETS STA. 999+00.00 TO STA. 1014+05.00

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 131       |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

|              |  |
|--------------|--|
| DATE         |  |
| BY           |  |
| SURVEYED     |  |
| PLOTTED      |  |
| TEMPLATE     |  |
| AREAS        |  |
| CHECKED      |  |
| FINAL SURVEY |  |
| NOTE BOOK    |  |
| NO.          |  |

|                 |  |
|-----------------|--|
| DATE            |  |
| BY              |  |
| SURVEYED        |  |
| PLOTTED         |  |
| TEMPLATE        |  |
| AREAS           |  |
| CHECKED         |  |
| ORIGINAL SURVEY |  |
| NOTE BOOK       |  |
| NO.             |  |



HORIZONTAL: 10  
 VERTICAL: 5

MODEL: Defaul  
 FILE NAME: ...0519190-31-CulvertProfiles.dgn

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|------------------------------------|------------|-----------|
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|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 20.0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:07:20 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

| CULVERT PROFILES                       |       |                   |        |
|--|-------|-------------------|--------|
| I-74 /CH 22 RAMP C (OGDEN INTERCHANGE) |       |                   |        |
| SCALE: 1:10H:1.5V                      | SHEET | OF                | SHEETS |
| STA. 497+25.00                         |       | TO STA. 502+31.90 |        |

| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 132       |
| CONTRACT NO. 70790 |         |        |              |           |

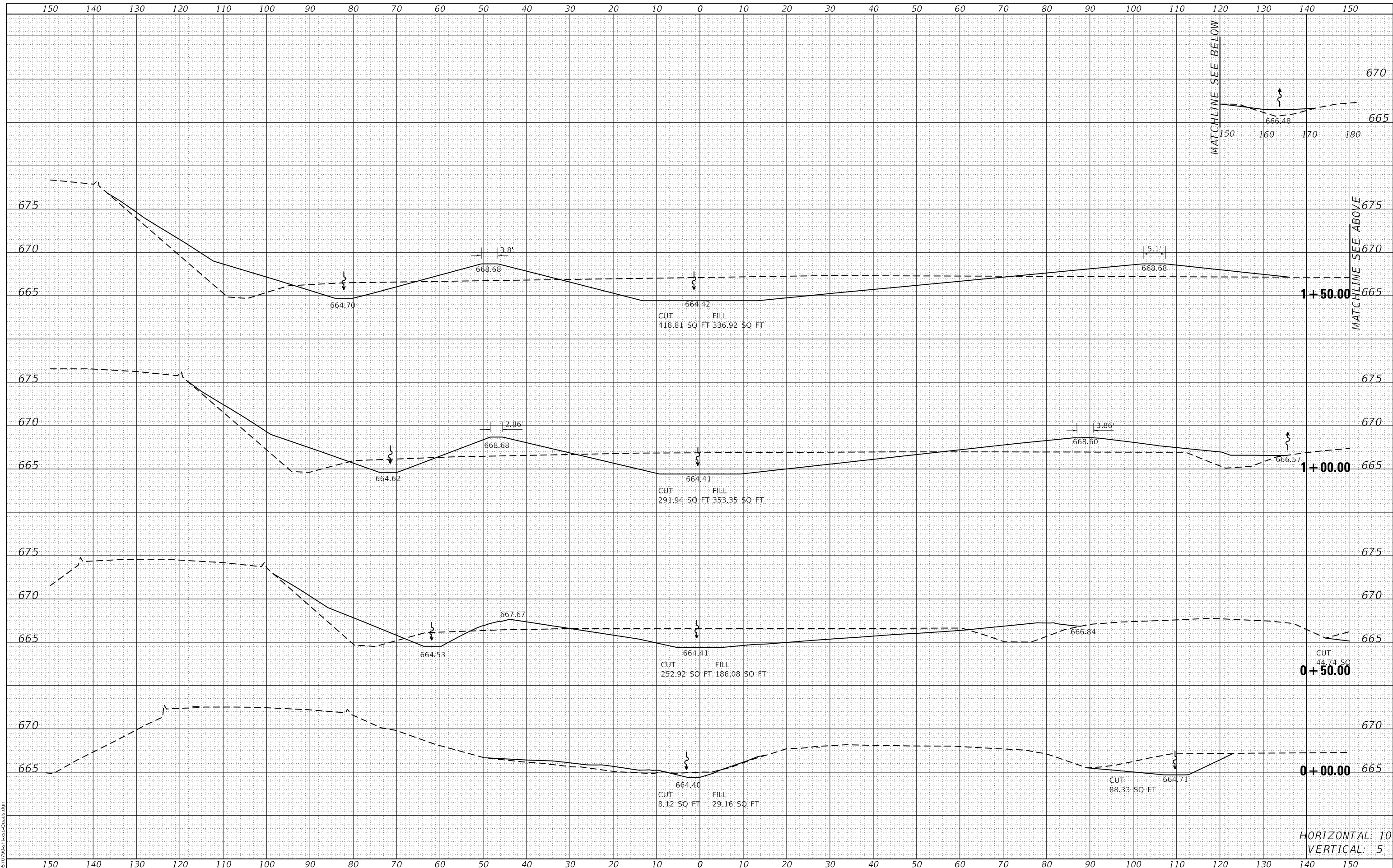
(10-7.8;92-8.9)RS-2(92-10)RS-3  
 \*\* CHAMPAIGN & VERMILION



|              |          |
|--------------|----------|
| DATE         |          |
| BY           |          |
| FINAL SURVEY | SURVEYED |
| NOTE BOOK    | PLOTTED  |
| NO.          | TEMPLATE |
|              | AREAS    |
|              | CHECKED  |

|                 |          |
|-----------------|----------|
| DATE            |          |
| BY              |          |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK       | PLOTTED  |
| NO.             | TEMPLATE |
|                 | AREAS    |
|                 | CHECKED  |

MODEL: Default  
FILE NAME: ...03/19/19-08-Hesse-Quad.dgn



MATCHLINE SEE BELOW

MATCHLINE SEE ABOVE

HORIZONTAL: 10  
VERTICAL: 5

|              |                        |
|--------------|------------------------|
| USER NAME =  | bemery                 |
| DESIGNED -   | REVISD -               |
| DRAWN -      | REVISD -               |
| PLOT SCALE = | 20.0000' / in.         |
| CHECKED -    | REVISD -               |
| DATE -       | REVISD -               |
| PLOT DATE =  | 1/24/2019 - 4:08:28 PM |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
ODGEN INTERCHANGE / N.W. QUAD**

SCALE: 1:10H:1:5V SHEET OF SHEETS STA. +00.00 TO STA. 1+50.00

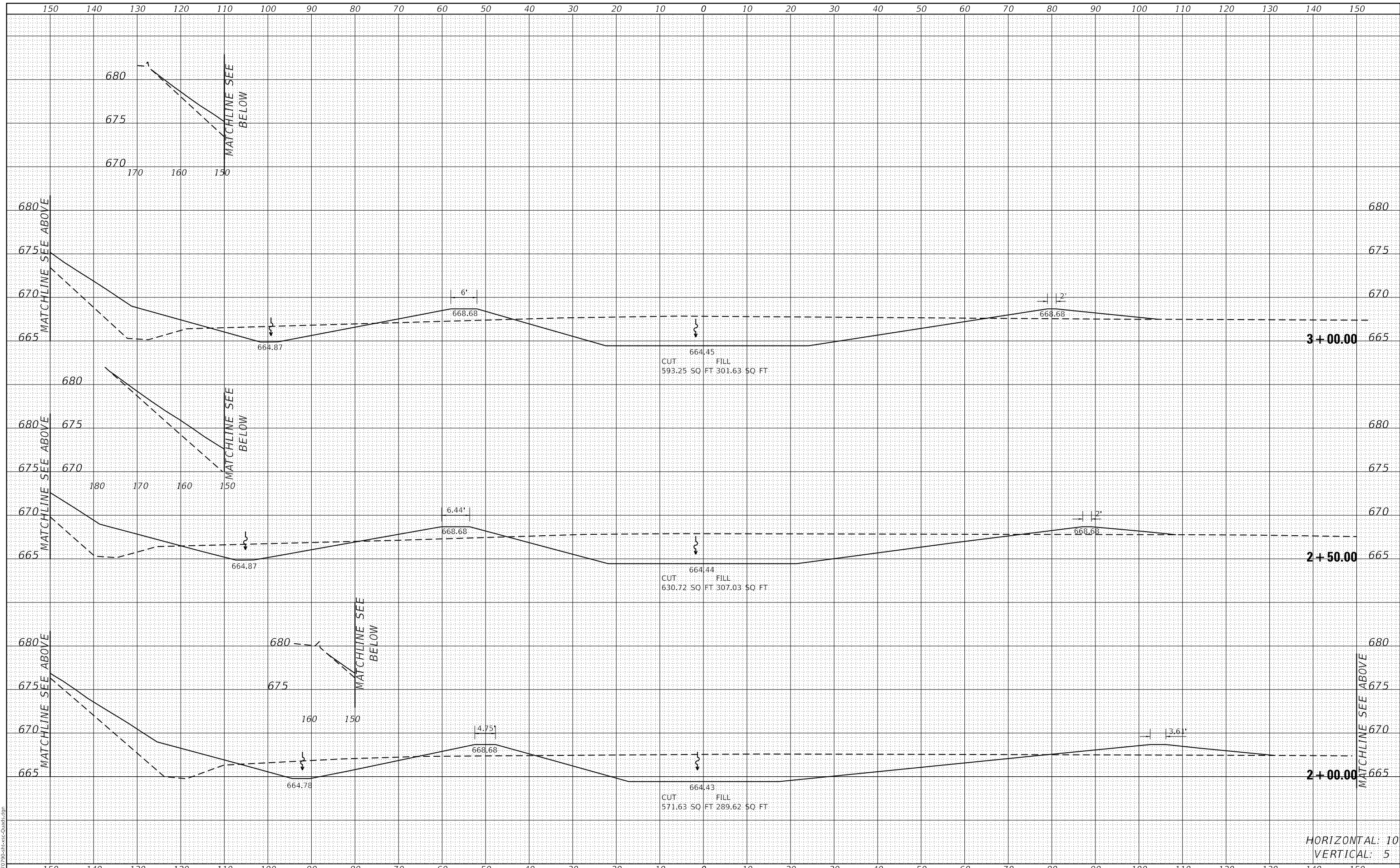
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|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 133       |
| CONTRACT NO. 70790 |         |        |              |           |

\* (10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

|              |          |
|--------------|----------|
| DATE         |          |
| BY           |          |
| FINAL SURVEY | SURVEYED |
| NOTE BOOK    | PLOTTED  |
| NO.          | TEMPLATE |
|              | AREAS    |
|              | CHECKED  |

|                 |          |
|-----------------|----------|
| DATE            |          |
| BY              |          |
| ORIGINAL SURVEY | SURVEYED |
| NOTE BOOK       | PLOTTED  |
| NO.             | TEMPLATE |
|                 | AREAS    |
|                 | CHECKED  |

MODEL: Definit  
FILE NAME: ...03/19/19-08-Hesse-Quad.dgn



HORIZONTAL: 10  
VERTICAL: 5

|              |                        |            |          |
|--------------|------------------------|------------|----------|
| USER NAME =  | bemery                 | DESIGNED - | REVISD - |
|              |                        | DRAWN -    | REVISD - |
| PLOT SCALE = | 20.0000' / in.         | CHECKED -  | REVISD - |
| PLOT DATE =  | 1/24/2019 - 4:08:28 PM | DATE -     | REVISD - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

| CROSS SECTIONS                |       |    |                 |
|-------------------------------|-------|----|-----------------|
| OGDEN INTERCHANGE / N.W. QUAD |       |    |                 |
| SCALE: 1:10H:1.5V             | SHEET | OF | SHEETS          |
| STA. 2+00.00                  |       |    | TO STA. 3+00.00 |

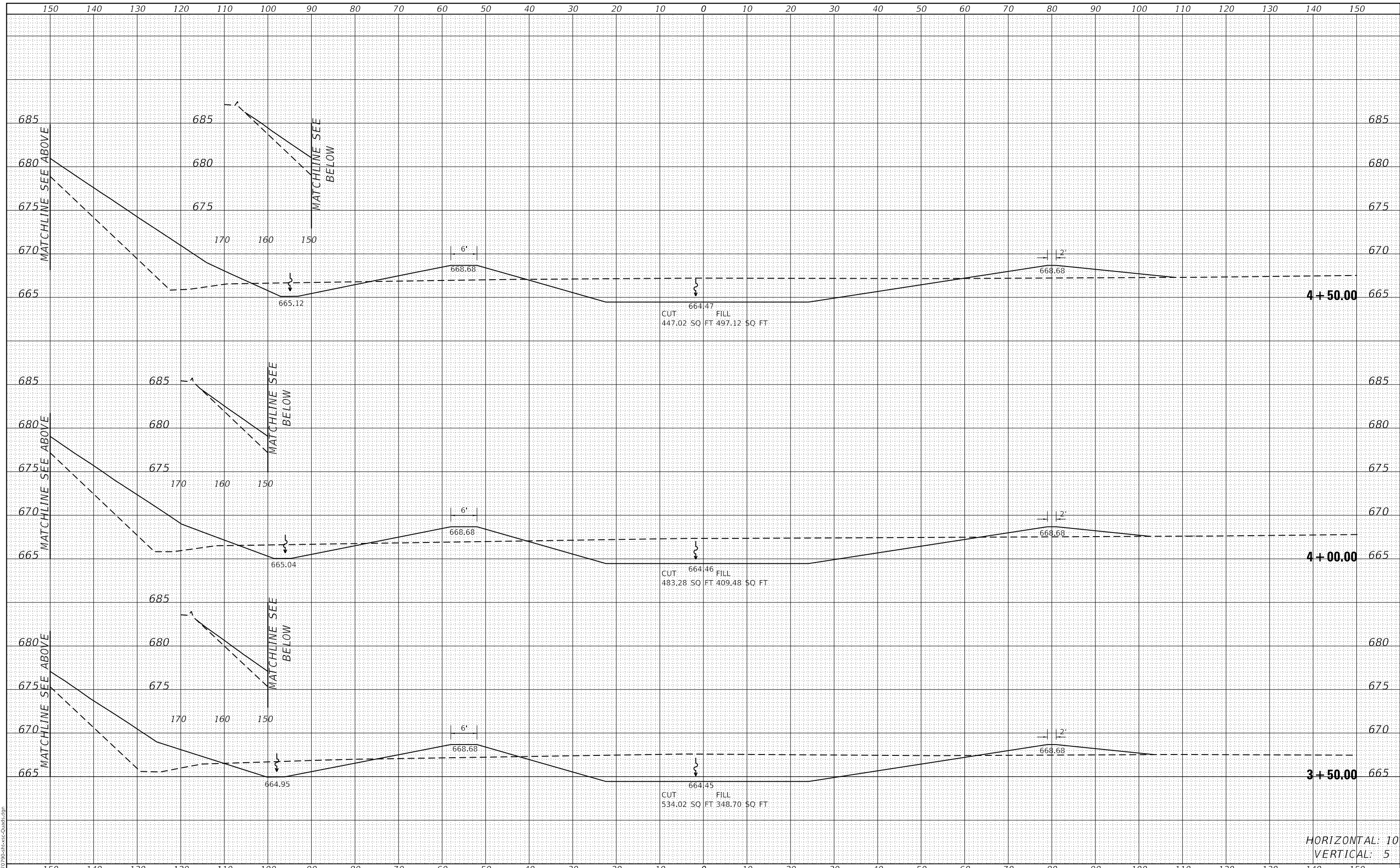
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|--------|--------------|-----------|
| I-74               | *       | **     | 142          | 134       |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

|               |  |
|---------------|--|
| DATE          |  |
| BY            |  |
| SURVEYED      |  |
| PLOTTED       |  |
| TEMPLATE      |  |
| NOTE BOOK     |  |
| AREAS CHECKED |  |
| NO.           |  |

|               |  |
|---------------|--|
| DATE          |  |
| BY            |  |
| SURVEYED      |  |
| PLOTTED       |  |
| TEMPLATE      |  |
| NOTE BOOK     |  |
| AREAS CHECKED |  |
| NO.           |  |

MODEL: Definit  
FILE NAME: ...03/19/19-08-Hesse-Quads.dgn



HORIZONTAL: 10  
VERTICAL: 5

|                                  |            |           |
|----------------------------------|------------|-----------|
| USER NAME = bemory               | DESIGNED - | REVISED - |
|                                  | DRAWN -    | REVISED - |
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| PLOT DATE = 1/24/2019 4:08:29 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

| CROSS SECTIONS                |       |    |                 |
|-------------------------------|-------|----|-----------------|
| ODGEN INTERCHANGE / N.W. QUAD |       |    |                 |
| SCALE: 1:10H:1.5V             | SHEET | OF | SHEETS          |
| STA. 3+50.00                  |       |    | TO STA. 4+50.00 |

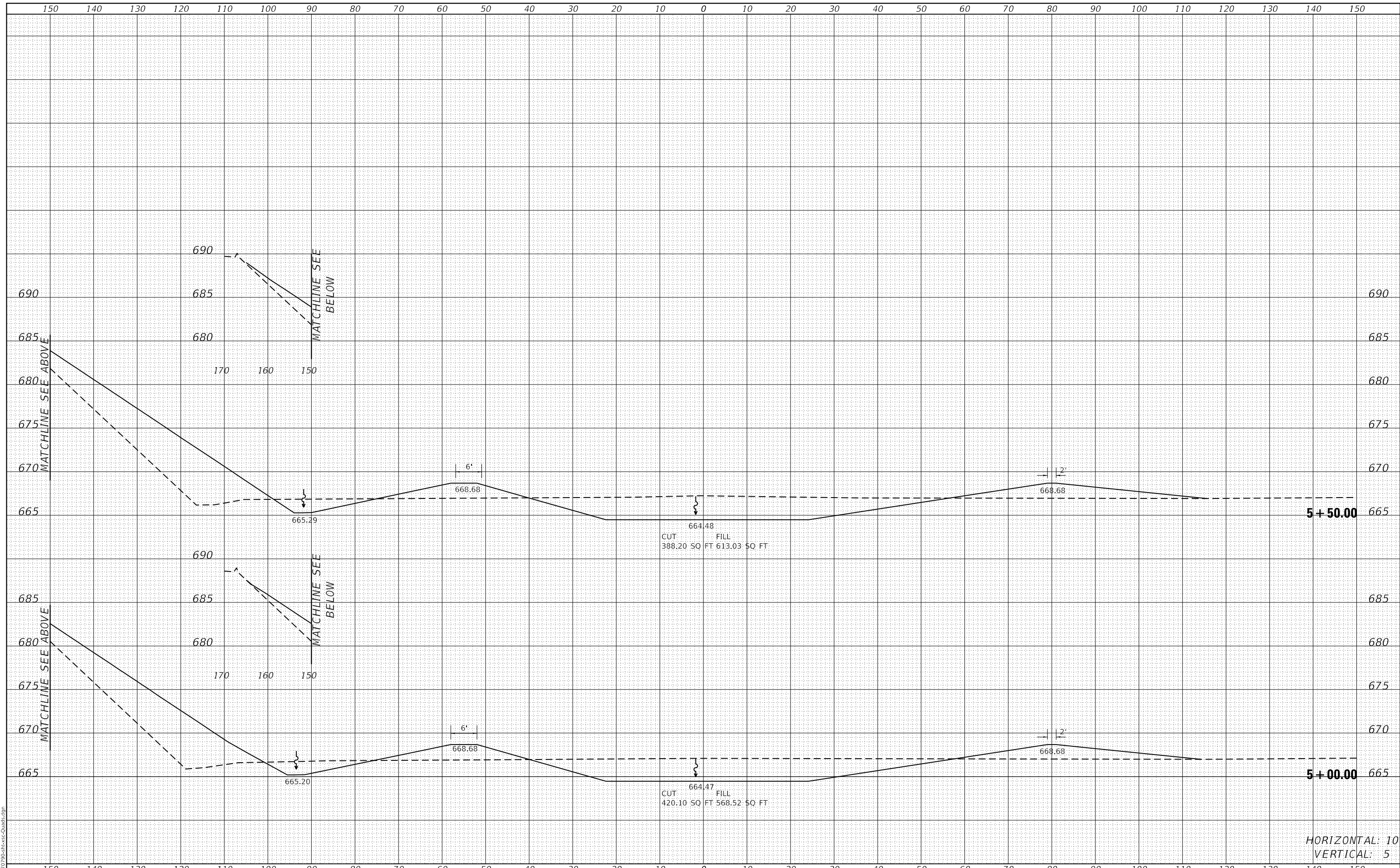
| F.A.I. RTE.        | SECTION | COUNTY           | TOTAL SHEETS | SHEET NO. |
|--------------------|---------|------------------|--------------|-----------|
| I-74               | *       | **               | 142          | 135       |
| CONTRACT NO. 70790 |         |                  |              |           |
| ILLINOIS           |         | FED. AID PROJECT |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

|              |               |
|--------------|---------------|
| DATE         |               |
| BY           |               |
| FINAL SURVEY | SURVEYED      |
| NOTE BOOK    | PLOTTED       |
| NO.          | TEMPLATE      |
|              | AREAS CHECKED |

|                 |               |
|-----------------|---------------|
| DATE            |               |
| BY              |               |
| ORIGINAL SURVEY | SURVEYED      |
| NOTE BOOK       | PLOTTED       |
| NO.             | TEMPLATE      |
|                 | AREAS CHECKED |

MODEL: Definit  
FILE NAME: ...03/19/19-08-Hesse-Quad.dgn



HORIZONTAL: 10  
VERTICAL: 5

|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemory                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 20.0000' / in.        | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:08:29 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
OGDEN INTERCHANGE / N.W. QUAD**

SCALE: 1:10H:1:5V SHEET OF SHEETS STA. 5+00.00 TO STA. 5+50.00

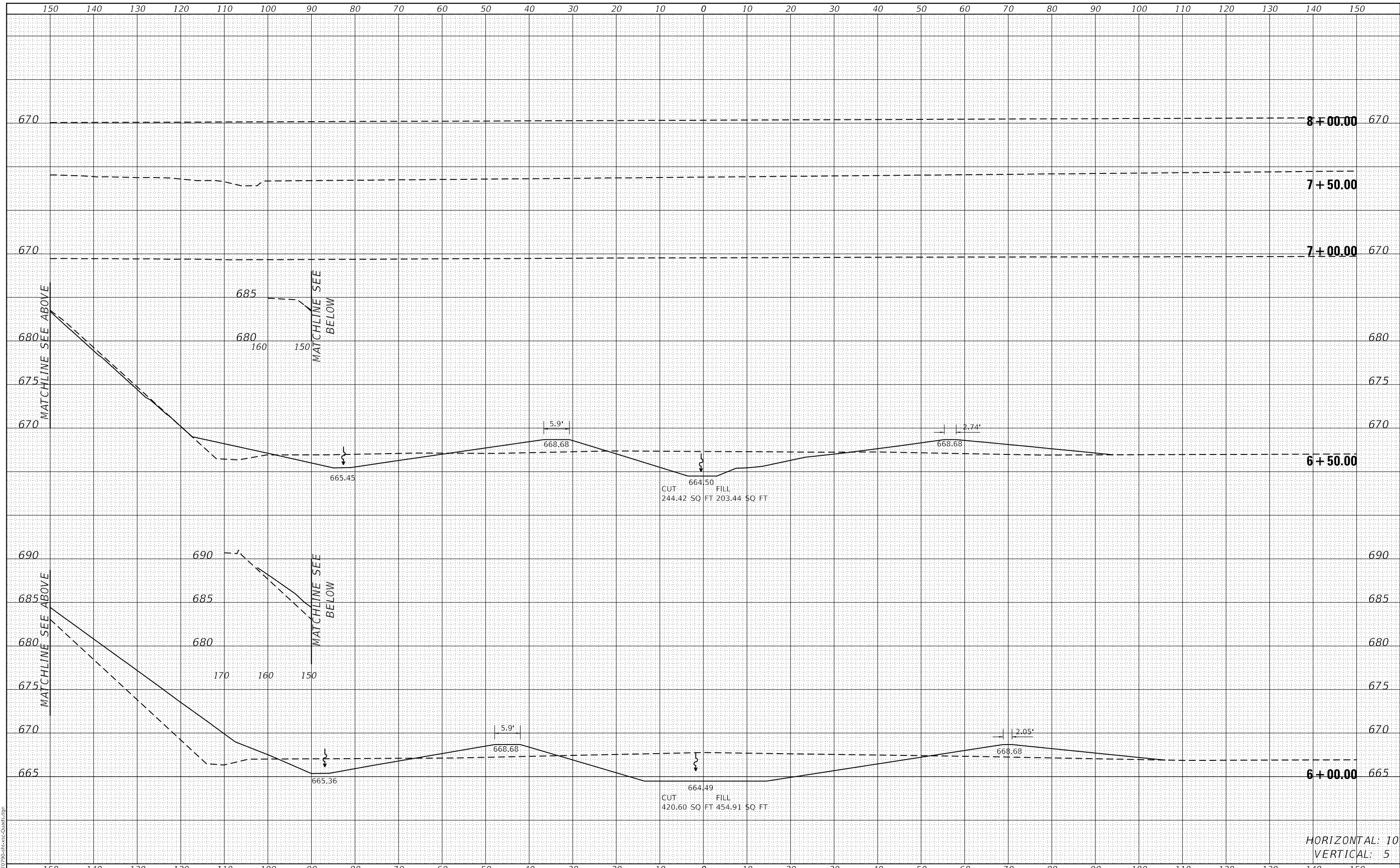
|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 136       |
| CONTRACT NO. 70790 |         |        |              |           |

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

|              |               |
|--------------|---------------|
| DATE         |               |
| BY           |               |
| FINAL SURVEY | SURVEYED      |
| NOTE BOOK    | PLOTTED       |
| NO.          | TEMPLATE      |
|              | AREAS CHECKED |

|                 |               |
|-----------------|---------------|
| DATE            |               |
| BY              |               |
| ORIGINAL SURVEY | SURVEYED      |
| NOTE BOOK       | PLOTTED       |
| NO.             | TEMPLATE      |
|                 | AREAS CHECKED |

MODEL: Definit  
FILE NAME: ...0519190-8-ss-0-Quads.dgn



HORIZONTAL: 10  
VERTICAL: 5

|                                    |            |          |
|------------------------------------|------------|----------|
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|                                    | DRAWN -    | REVISD - |
| PLOT SCALE = 20.0000 ' / in.       | CHECKED -  | REVISD - |
| PLOT DATE = 1/24/2019 - 4:08:29 PM | DATE -     | REVISD - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
ODGEN INTERCHANGE / S.W. QUAD**

SCALE: 1:10H:1:5V SHEET OF SHEETS STA. 6+00.00 TO STA. 8+00.00

|             |         |        |                    |           |
|-------------|---------|--------|--------------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS       | SHEET NO. |
| I-74        | *       | **     | 142                | 137       |
|             |         |        | CONTRACT NO. 70790 |           |

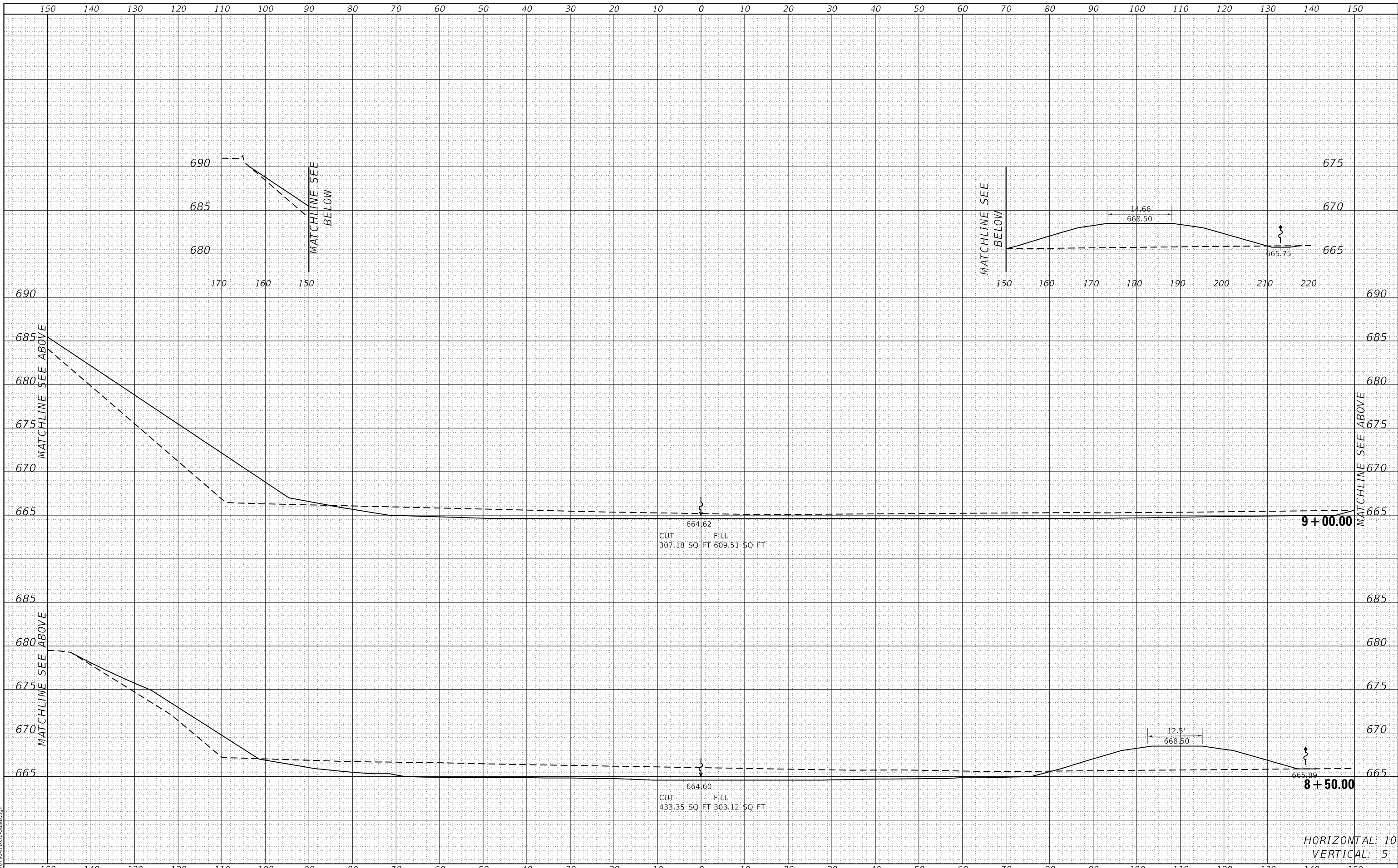
(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION



|               |  |
|---------------|--|
| DATE          |  |
| BY            |  |
| SURVEYED      |  |
| PLOTTED       |  |
| TEMPLATE      |  |
| AREAS CHECKED |  |
| FINAL SURVEY  |  |
| NOTE BOOK     |  |
| NO.           |  |

|                 |  |
|-----------------|--|
| DATE            |  |
| BY              |  |
| SURVEYED        |  |
| PLOTTED         |  |
| TEMPLATE        |  |
| AREAS CHECKED   |  |
| ORIGINAL SURVEY |  |
| NOTE BOOK       |  |
| NO.             |  |

MODEL: Definit  
FILE NAME: ...03/19/19-08-Hesse-Quad.dgn



|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 20.0000" / 1'         | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:08:30 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                                      |       |           |                              |
|--------------------------------------|-------|-----------|------------------------------|
| <b>CROSS SECTIONS</b>                |       |           |                              |
| <b>OGDEN INTERCHANGE / S.W. QUAD</b> |       |           |                              |
| SCALE: 1:10H:1.5V                    | SHEET | OF SHEETS | STA. 8+50.00 TO STA. 9+00.00 |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 138       |
| CONTRACT NO. 70790 |         |        |              |           |

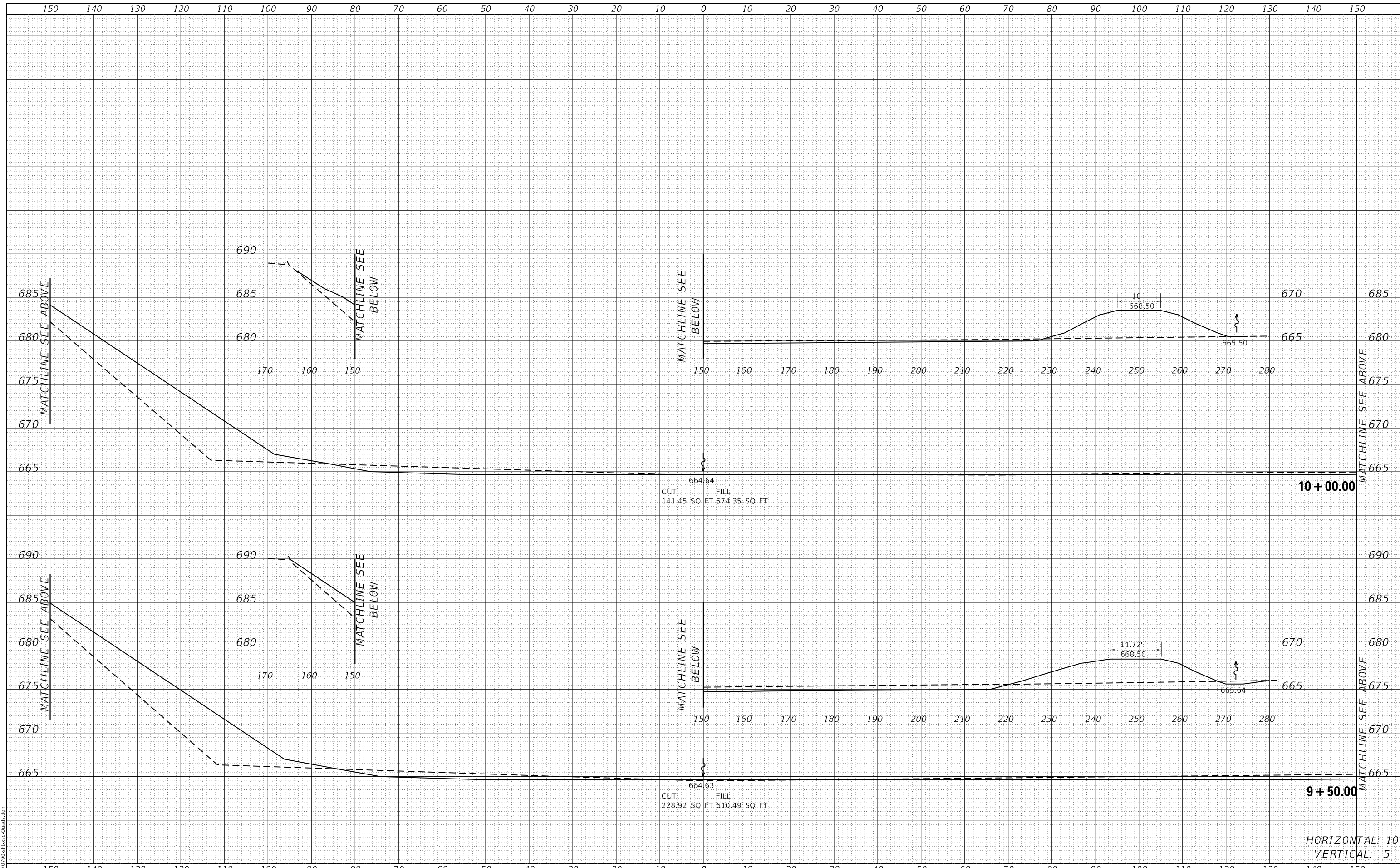
HORIZONTAL: 10  
VERTICAL: 5

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

|              |               |      |
|--------------|---------------|------|
| FINAL SURVEY | SURVEYED      | DATE |
| NOTE BOOK    | PLOTTED       | BY   |
| NO.          | TEMPLATE      |      |
|              | AREAS CHECKED |      |

|                 |               |      |
|-----------------|---------------|------|
| ORIGINAL SURVEY | SURVEYED      | DATE |
| NOTE BOOK       | PLOTTED       | BY   |
| NO.             | TEMPLATE      |      |
|                 | AREAS CHECKED |      |

MODEL: Definit  
FILE NAME: ...10-79-8-9-RS-2(92-10)RS-3.dwg



|              |                        |
|--------------|------------------------|
| USER NAME =  | bemery                 |
| DESIGNED -   |                        |
| DRAWN -      |                        |
| PLOT SCALE = | 20.0000 "/td>          |
| PLOT DATE =  | 1/24/2019 - 4:08:30 PM |

|            |           |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN -    | REVISED - |
| CHECKED -  | REVISED - |
| DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                                      |                  |
|--------------------------------------|------------------|
| <b>CROSS SECTIONS</b>                |                  |
| <b>OGDEN INTERCHANGE / S.W. QUAD</b> |                  |
| SCALE: 1:10H:1:5V                    | SHEET OF SHEETS  |
| STA. 9+50.00                         | TO STA. 10+00.00 |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 139       |
| CONTRACT NO. 70790 |         |        |              |           |

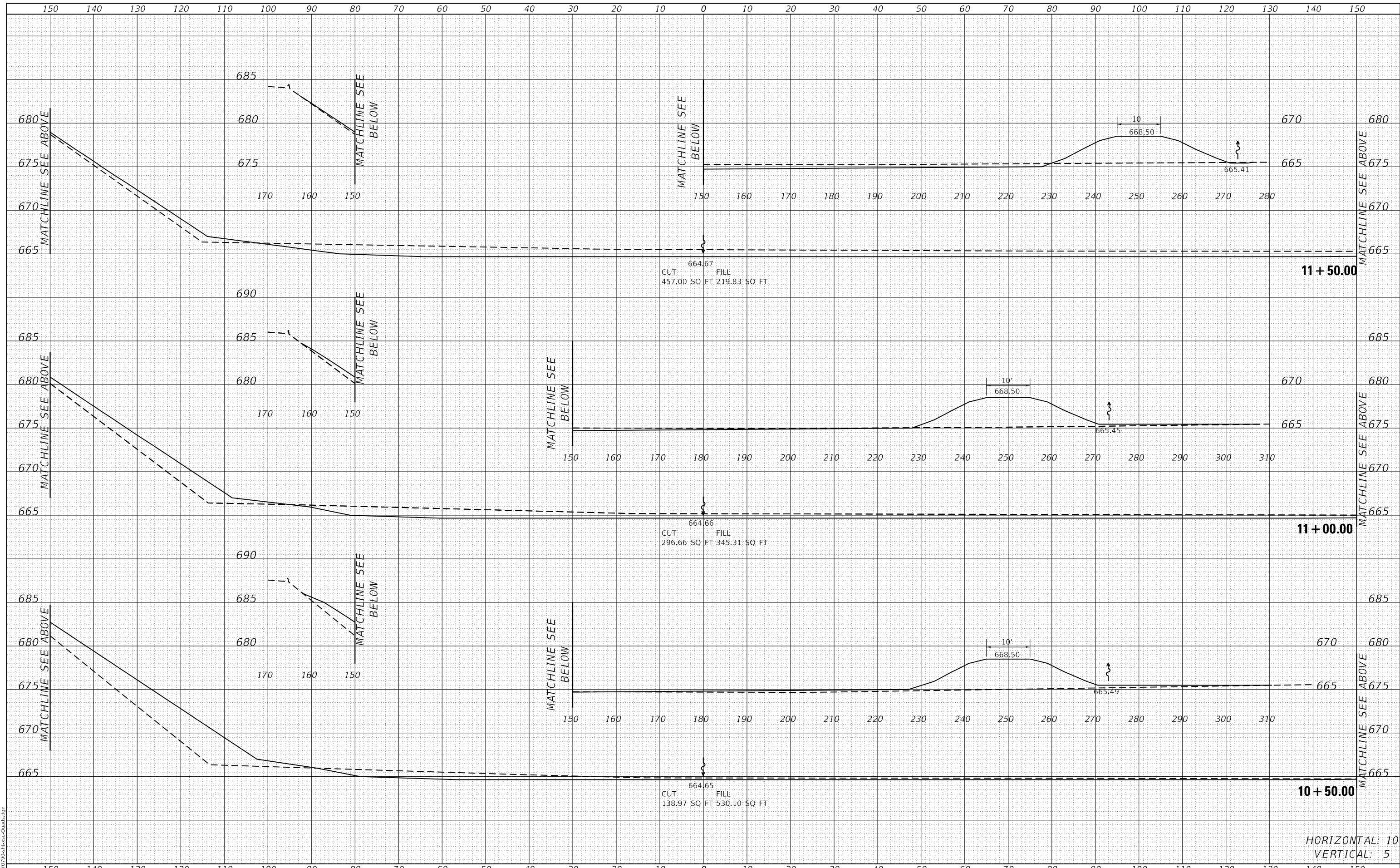
HORIZONTAL: 10  
VERTICAL: 5

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

|              |  |
|--------------|--|
| DATE         |  |
| BY           |  |
| SURVEYED     |  |
| PLOTTED      |  |
| TEMPLATE     |  |
| AREAS        |  |
| CHECKED      |  |
| FINAL SURVEY |  |
| NOTE BOOK    |  |
| NO.          |  |

|                 |  |
|-----------------|--|
| DATE            |  |
| BY              |  |
| SURVEYED        |  |
| PLOTTED         |  |
| TEMPLATE        |  |
| AREAS           |  |
| CHECKED         |  |
| ORIGINAL SURVEY |  |
| NOTE BOOK       |  |
| NO.             |  |

MODEL: Definit  
FILE NAME: ...10799-08-Hesse-Quad.dgn



|              |                        |
|--------------|------------------------|
| USER NAME =  | bemery                 |
| DESIGNED -   | REVISIED -             |
| DRAWN -      | REVISIED -             |
| PLOT SCALE = | 20.0000 ' / in.        |
| CHECKED -    | REVISIED -             |
| PLOT DATE =  | 1/24/2019 - 4:08:30 PM |
| DATE -       | REVISIED -             |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                                      |                  |
|--------------------------------------|------------------|
| <b>CROSS SECTIONS</b>                |                  |
| <b>OGDEN INTERCHANGE / S.W. QUAD</b> |                  |
| SCALE: 1:10H:1.5V                    | SHEET OF SHEETS  |
| STA. 10+50.00                        | TO STA. 11+50.00 |

|                           |         |        |              |           |
|---------------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.               | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74                      | *       | **     | 142          | 140       |
| CONTRACT NO. 70790        |         |        |              |           |
| ILLINOIS FED. AID PROJECT |         |        |              |           |

HORIZONTAL: 10  
VERTICAL: 5

(10-7.8;92-8.9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION

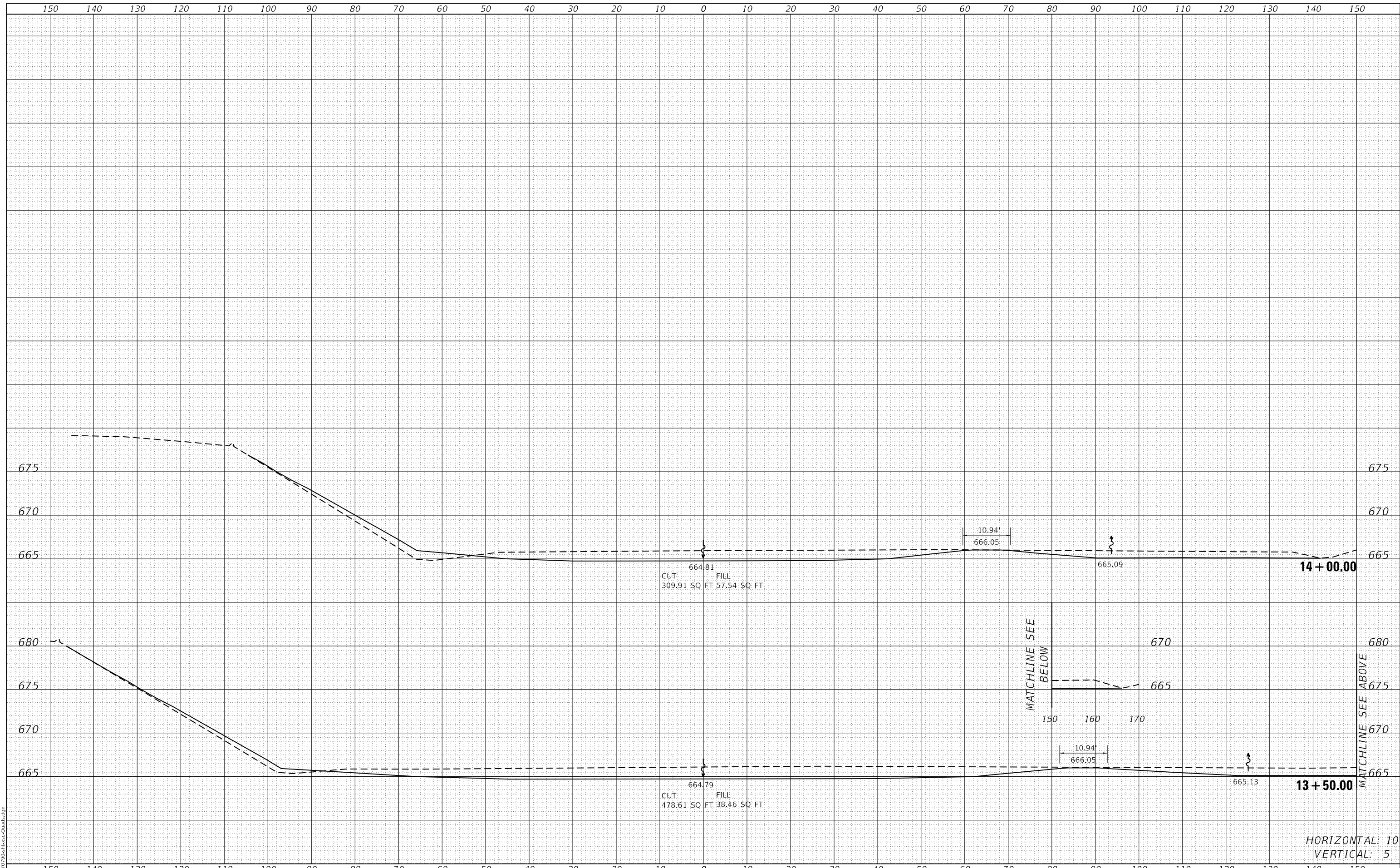




|                  |               |      |
|------------------|---------------|------|
| FINAL SURVEY NO. | SURVEYED      | DATE |
| NOTE BOOK        | PLOTTED       |      |
|                  | TEMPLATE      |      |
|                  | AREAS CHECKED |      |

|                     |               |      |
|---------------------|---------------|------|
| ORIGINAL SURVEY NO. | SURVEYED      | DATE |
| NOTE BOOK           | PLOTTED       |      |
|                     | TEMPLATE      |      |
|                     | AREAS CHECKED |      |

MODEL: Definit  
FILE NAME: ...10-7-8;92-8,9)RS-2(92-10)RS-3



|                                    |            |           |
|------------------------------------|------------|-----------|
| USER NAME = bemery                 | DESIGNED - | REVISED - |
|                                    | DRAWN -    | REVISED - |
| PLOT SCALE = 20.0000 ' / in.       | CHECKED -  | REVISED - |
| PLOT DATE = 1/24/2019 - 4:08:31 PM | DATE -     | REVISED - |

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

|                                      |                  |
|--------------------------------------|------------------|
| <b>CROSS SECTIONS</b>                |                  |
| <b>OGDEN INTERCHANGE / S.W. QUAD</b> |                  |
| SCALE: 1:10H:1:5V                    | SHEET OF SHEETS  |
| STA. 13+50.00                        | TO STA. 14+00.00 |

|                    |         |        |              |           |
|--------------------|---------|--------|--------------|-----------|
| F.A.I. RTE.        | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| I-74               | *       | **     | 142          | 142       |
| CONTRACT NO. 70790 |         |        |              |           |

HORIZONTAL: 10  
VERTICAL: 5

(10-7,8;92-8,9)RS-2(92-10)RS-3 \*\* CHAMPAIGN & VERMILION