

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. CITY	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CJS	LAKE	23	1
CONTRACT NO. 62H28				

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED WITHIN THE VILLAGES OF LIBERTYVILLE AND GREEN OAKS

PROPOSED HIGHWAY PLANS

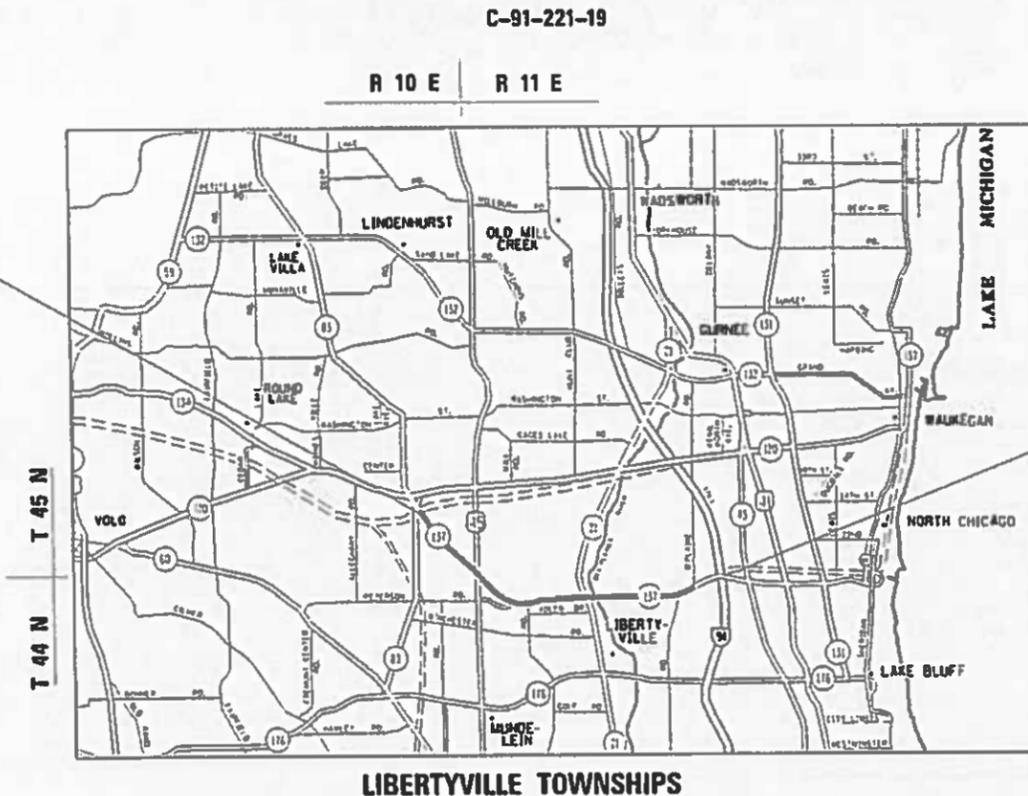
F.A.P. ROUTE 352: IL 137 (BUCKLEY RD.)
IL RTE. 83 TO O'PLAINE RD.
SECTION: 2018-088-CJS
PROJECT: STP-P4SF(431)
CRACK AND JOINT SEALING
LAKE COUNTY



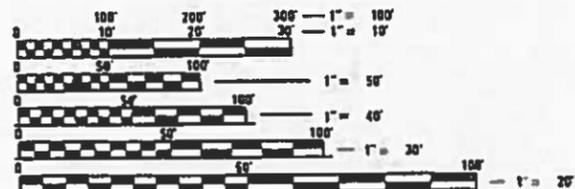
TRAFFIC DATA

2017 ADT = 13400 TO 30100
POSTED SPEED LIMIT = 40 TO 55 MPH

PROJECT BEGINS STA. 21+00



PROJECT ENDS STA. 377+70.8



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: JEAN A. MIDY (847) 221-3056
PROJECT MANAGER: FAWAD F. AQUEEL (847) 705-4247

GROSS LENGTH = 35670.8 FT. = 6.76 MILE

CONTRACT NO. 62H28

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED January 25, 2019

Anthony J. Pennington REGIONAL ENGINEER
Margaret A. Etkin ENGINEER OF DESIGN AND ENVIRONMENT
David P. [Signature] DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS

<u>SHEET NO.</u>	<u>DESCRIPTION</u>
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4-16	PROPOSED PLANS AND PAVEMENT MARKING PLANS
17	CRACK & JOINT SEALING DETAIL
18	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
19	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
20	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
21	ARTERIAL ROAD INFORMATION SIGNING (TC-22)
22-23	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKINGS TREATMENT FOR RAILROAD CROSSING (TC-23 a, b)

STANDARDS

701006-05	OFF-RD OPERATIONS, 2L, 2W, 15'(4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5m) TO 24" (600mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-04	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS ≥ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS-DAY ONLY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS > 45 MPH
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS < 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701502-09	URBAN LANE CLOSURE, 2L, 2W BI-DIRECTIONAL LEFT TURN LANE
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NON TRAVERSABLE MEDIAN
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN SINGLE LANE CLOSURE, MULTILANE INTERSECTION
701901-08	TRAFFIC CONTROL DEVICES

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E". AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES OF THE VILLAGES OF ITASCA AND BENSENVILLE AND THE CITY OF WOOD DALE.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT kalpana.kannan -hosadurga@illinois.gov A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY AREA TRAFFIC FIELD ENGINEER VIA EMAIL AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

ALL CRACK ROUTING & CRACK SEALING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER. CRACK ROUTING SHALL BE OMITTED WHERE VEHICLE DETECTOR LOOPS & RAISED REFLECTIVE PAVEMENT MARKERS ARE PRESENT IN THE PAVEMENT.

MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN THE EXISTING PAVEMENT MAY BE PLACED, SHAPED AND COMPACTED TO THE SATISFACTION OF THE ENGINEER ALONG EXISTING AGGREGATE SHOULDERS ADJACENT TO THE PAVEMENT. ALL MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN PAVEMENTS WITHOUT AGGREGATE SHOULDERS AND SURPLUS MATERIALS RESULTING FROM THE ROUTING OF CRACKS IN PAVEMENTS WITH AGGREGATE SHOULDERS, WHERE ALL MATERIALS ARE NOT PLACED ALONG EXISTING AGGREGATE SHOULDERS, SHALL BE DISPOSED OF AS SPECIFIED IN ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS. OLD SEALANTS REMOVED PRIOR TO ROUTING SHALL BE DISPOSED OF AS SPECIFIED IN ARTICLE 202.03. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT THE COST SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR THE CONSTRUCTION ITEMS INVOLVED, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

REV. - MS

MODEL: Default
FILE NAME: p:\11\0848\BID\NTEC\Illinois.gov\PWD\DOT\Documents\DOT Office\District 1\Projects\102719\CADD\Drawings\102219-akt-plan.dgn

USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
IL 137 (BUCKLEY RD) (IL 83 TO O'PLAINE RD)**

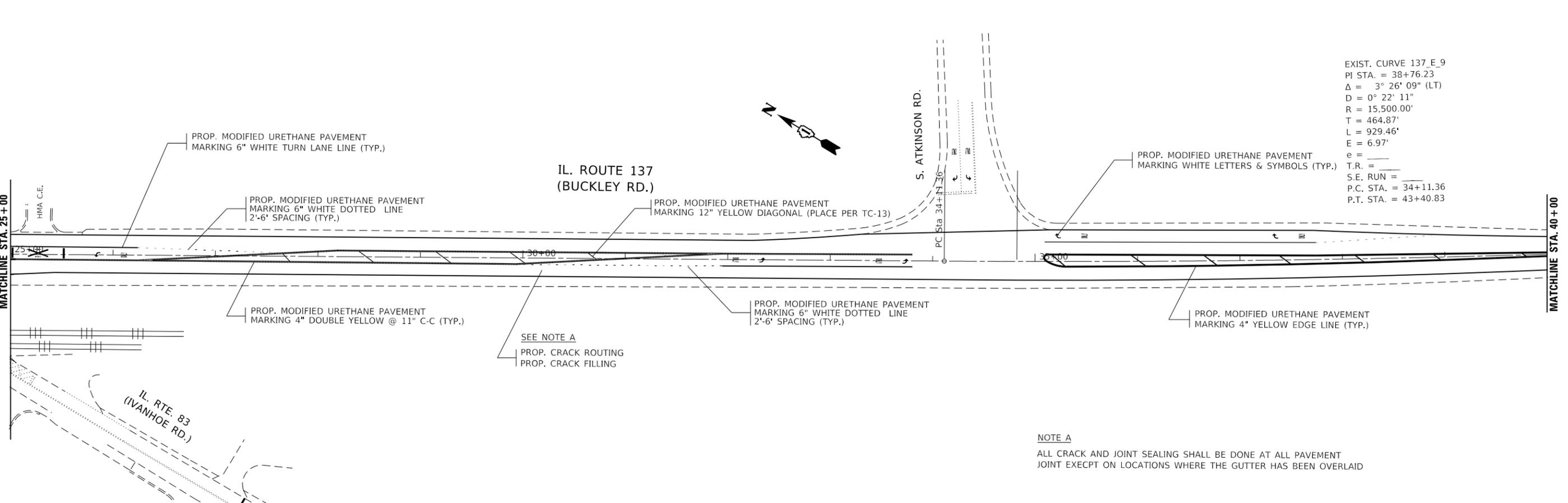
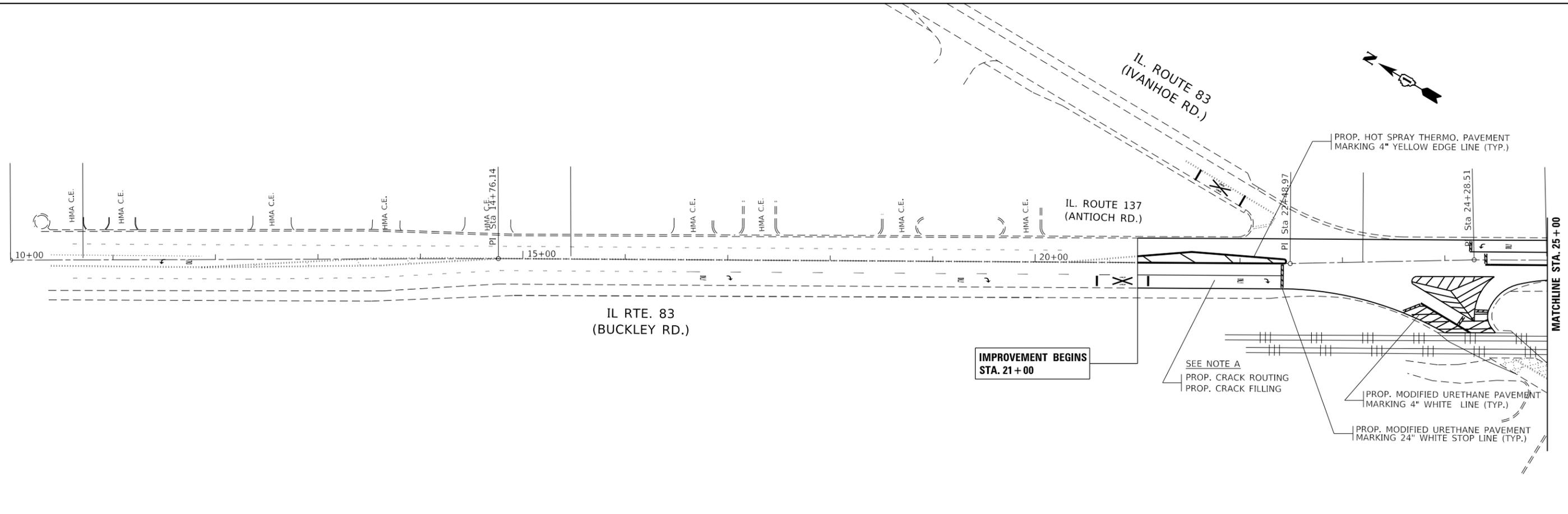
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-C15	LAKE	23	2
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE 0005					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FEDERAL 20% STATE 0005					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	80% FEDERAL 20% STATE 0005				
45100100	CRACK ROUTING (PAVEMENT)	FOOT	118069	118069					* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	165644	165644				
45100200	CRACK FILLING	POUND	70842	70842					* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	11050	11050				
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3					* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	2566	2566				
67100100	MOBILIZATION	L SUM	1	1					* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	5624	5624				
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	2	2					* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	598	598				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1					Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	102.8	102.8				
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1													
70102622	TRAFFIC CONTROL AND PROTECTION, STANDARD 701502	L SUM	1	1													
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1													
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1													
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1													
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1													
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	2198	2198													
	* SPECIALTY ITEM																

REV. - MS

MODEL: Default
 FILE: \\blm\c:\p\ul\B&E\ID\ITC\Illinois\gov\p\ul\DOT\Documents\1\DOT\Office\ID\ITC\1\Project\102219\CADD\Drawings\102219-ehp-plan.dgn



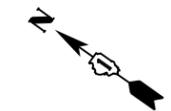
NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLAN	
IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)	
SCALE: 1" = 50'	SHEET OF SHEETS STA. TO STA.

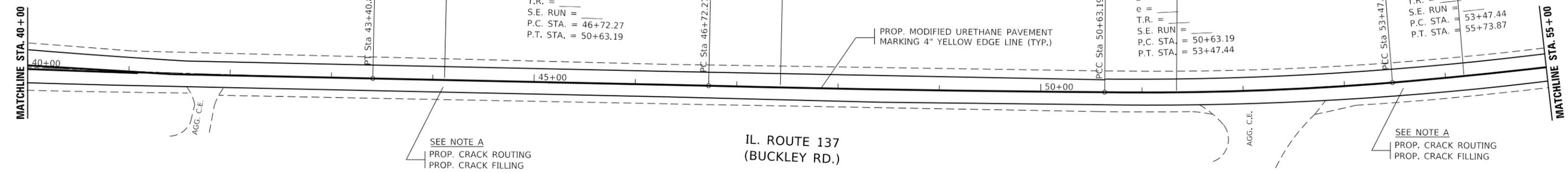
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	4
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



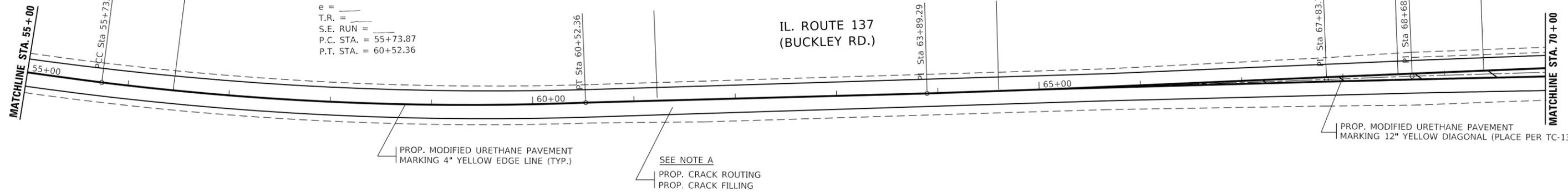
EXIST. CURVE 137_E_12
 PI STA. = 48+67.73
 $\Delta = 1^\circ 03' 22''$ (LT)
 $D = 0^\circ 16' 13''$
 $R = 21,208.69'$
 $T = 195.47'$
 $L = 390.92'$
 $E = 0.90'$
 $e =$
 T.R. =
 S.E. RUN =
 P.C. STA. = 46+72.27
 P.T. STA. = 50+63.19

EXIST. CURVE 137_E_13
 PI STA. = 52+05.42
 $\Delta = 5^\circ 23' 26''$ (LT)
 $D = 1^\circ 53' 47''$
 $R = 3,021.34'$
 $T = 142.23'$
 $L = 284.25'$
 $E = 3.35'$
 $e =$
 T.R. =
 S.E. RUN =
 P.C. STA. = 50+63.19
 P.T. STA. = 53+47.44

EXIST. CURVE 137_E_14
 PI STA. = 54+60.68
 $\Delta = 2^\circ 52' 04''$ (LT)
 $D = 1^\circ 16' 00''$
 $R = 4,523.66'$
 $T = 113.24'$
 $L = 226.43'$
 $E = 1.42'$
 $e =$
 T.R. =
 S.E. RUN = 53+47.44
 P.C. STA. = 55+73.87
 P.T. STA. = 55+73.87



EXIST. CURVE 137_E_15
 PI STA. = 58+13.64
 $\Delta = 9^\circ 14' 38''$ (LT)
 $D = 1^\circ 55' 55''$
 $R = 2,965.85'$
 $T = 239.77'$
 $L = 478.49'$
 $E = 9.68'$
 $e =$
 T.R. =
 S.E. RUN =
 P.C. STA. = 55+73.87
 P.T. STA. = 60+52.36



NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

MODEL: Default
 FILE: \\nas0101\proj\102219\CADD\Drawings\102219-ehp-plan.dgn

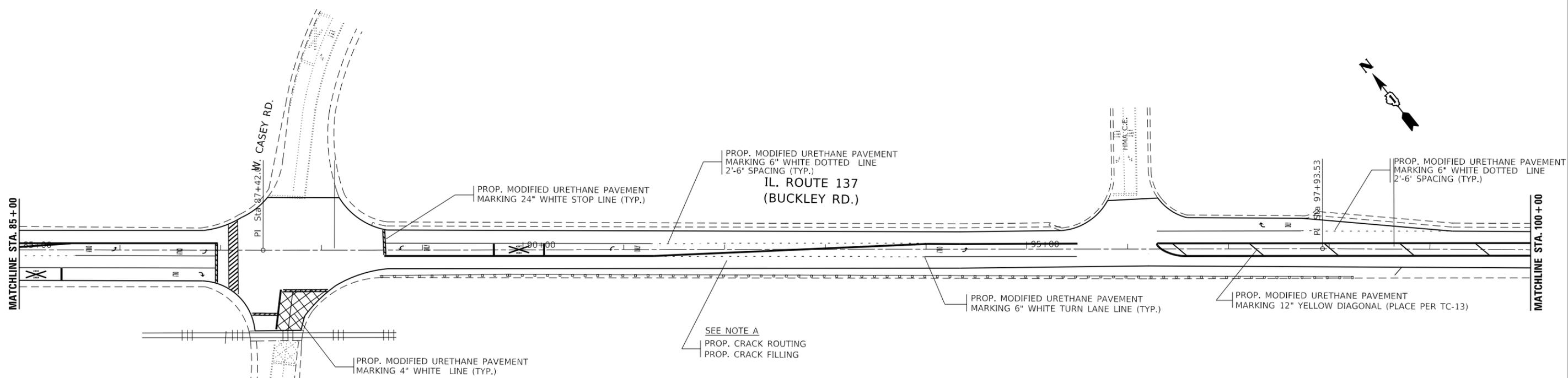
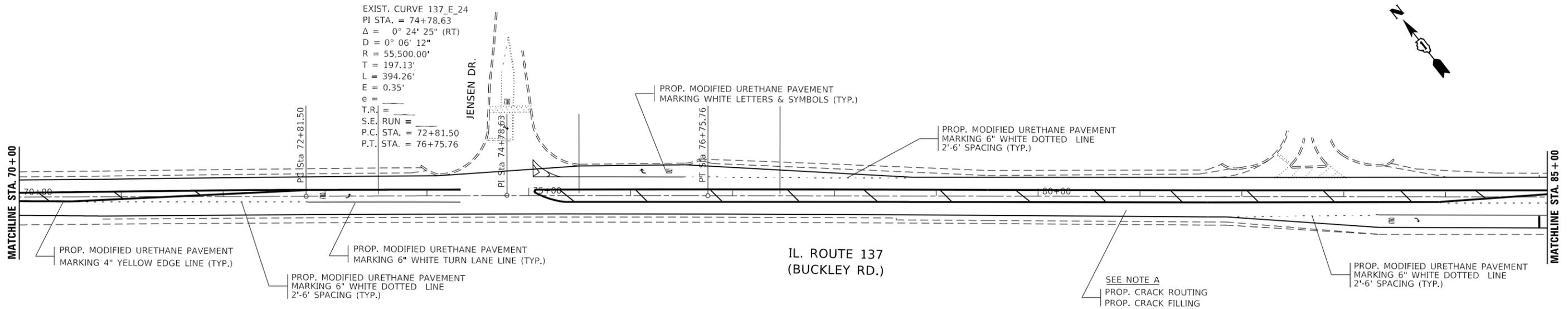
USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/1/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)
 SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 352	SECTION 2018-088-CIS	COUNTY LAKE	TOTAL SHEETS 23	SHEET NO. 5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62H28	

EXIST. CURVE 137_E_24
 PI STA. = 74+78.63
 $\Delta = 0^\circ 24' 25''$ (RT)
 $D = 0^\circ 06' 12''$
 $R = 55,500.00'$
 $T = 197.13'$
 $L = 394.26'$
 $E = 0.35'$
 $e =$
 $T.R.I. =$
 $S.E. RUN =$
 P.C. STA. = 72+81.50
 P.T. STA. = 76+75.76



NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

MODEL: Default
 FILE: \\bls01\B\BID\NTEC\Illinois\gov\PIWDOT\Documents\102219\CADD\Drawings\102219-dhr-plan.dgn

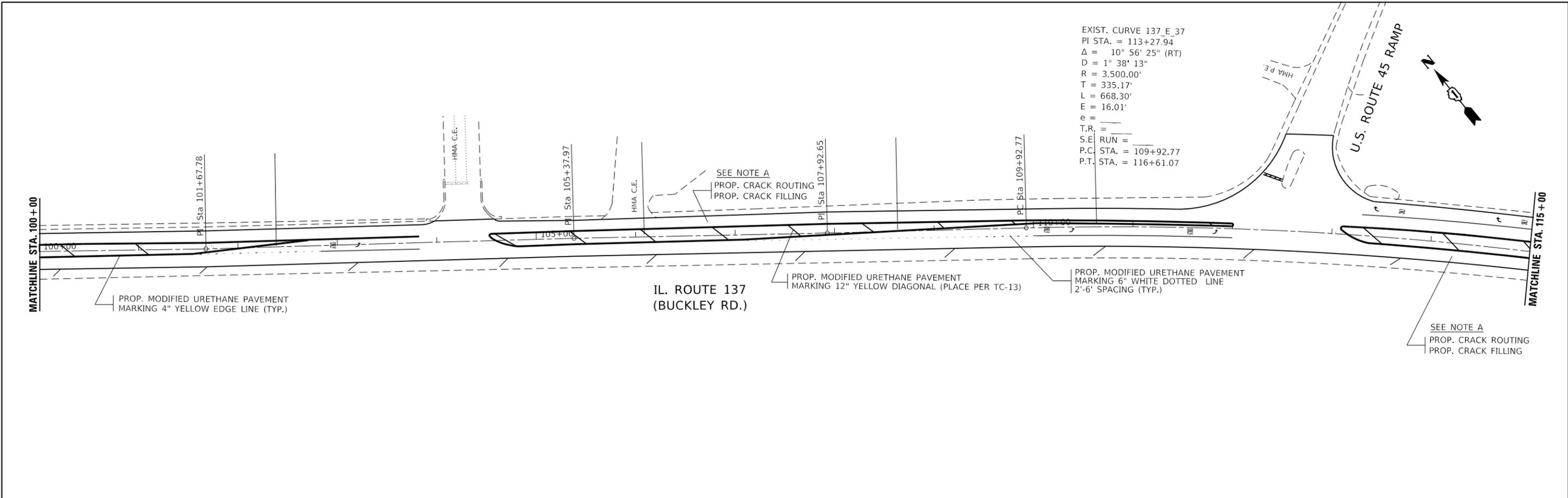
USER NAME = abebawa	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 2/22/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

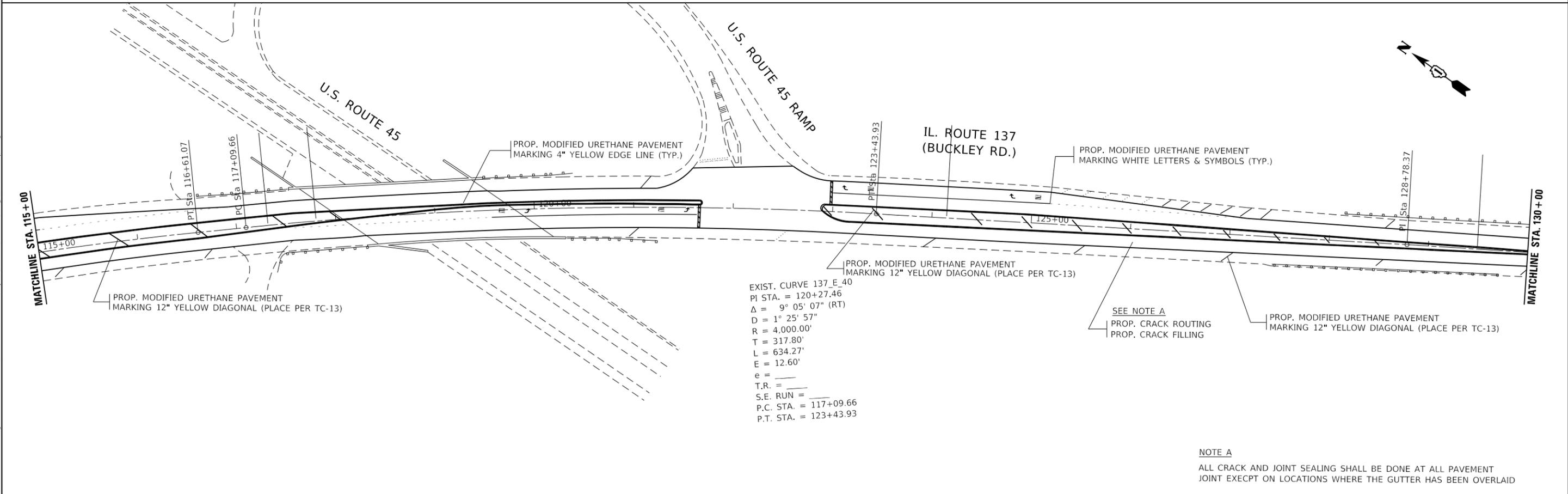
**EXISTING AND PROPOSED ROADWAY PLAN
 IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	6
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



EXIST. CURVE 137_E_37
 PI STA. = 113+27.94
 $\Delta = 10^\circ 56' 25''$ (RT)
 $D = 1^\circ 38' 13''$
 $R = 3,500.00'$
 $T = 335.17'$
 $L = 668.30'$
 $E = 16.01'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 109+92.77$
 $P.T. STA. = 116+61.07$



EXIST. CURVE 137_E_40
 PI STA. = 120+27.46
 $\Delta = 9^\circ 05' 07''$ (RT)
 $D = 1^\circ 25' 57''$
 $R = 4,000.00'$
 $T = 317.80'$
 $L = 634.27'$
 $E = 12.60'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 117+09.66$
 $P.T. STA. = 123+43.93$

NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

MODEL: Default
 FILE: \\nas01.cba.eidnet.ec.llnwd.net\proj\p102219\CADD\Drawings\DOT Documents\102219\CADD\Drawings\102219-ah-rl-plan.dgn

USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

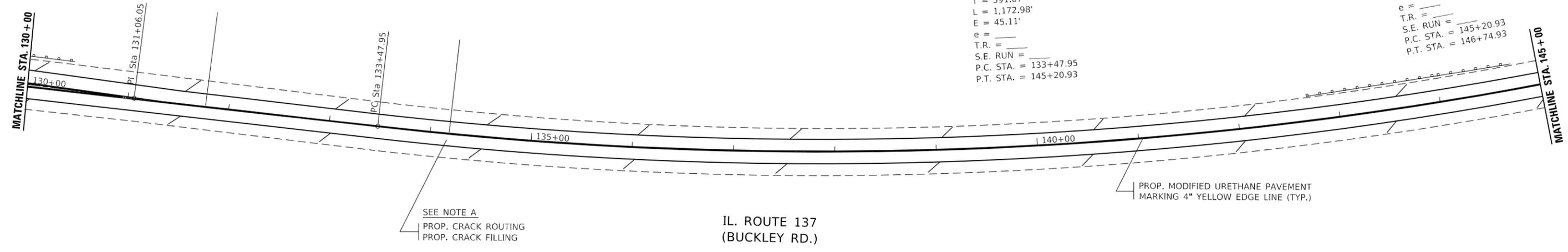
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLAN			
IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)			
SCALE: 1"= 50'	SHEET	OF SHEETS	STA. TO STA.

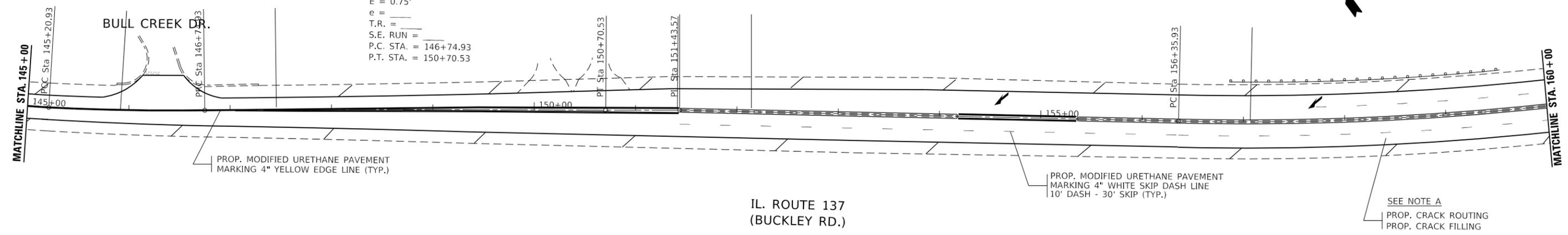
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	7
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				

EXIST. CURVE 137_E_47
 PI STA. = 139+39.02
 $\Delta = 17^\circ 27' 23''$ (LT)
 $D = 1^\circ 29' 18''$
 $R = 3,850.00'$
 $T = 591.07'$
 $L = 1,172.98'$
 $E = 45.11'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 133+47.95
 P.T. STA. = 145+20.93

EXIST. CURVE 137_E_48
 PI STA. = 145+97.95
 $\Delta = 3^\circ 37' 40''$ (LT)
 $D = 2^\circ 21' 20''$
 $R = 2,432.38'$
 $T = 77.03'$
 $L = 154.01'$
 $E = 1.22'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 145+20.93
 P.T. STA. = 146+74.93



EXIST. CURVE 137_E_49
 PI STA. = 148+72.74
 $\Delta = 0^\circ 52' 11''$ (RT)
 $D = 0^\circ 13' 11''$
 $R = 26,060.89'$
 $T = 197.80'$
 $L = 395.60'$
 $E = 0.75'$
 $e = \text{---}$
 T.R. = ---
 S.E. RUN = ---
 P.C. STA. = 146+74.93
 P.T. STA. = 150+70.53



NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

MODEL: Default
 FILE: \\nas0101\B&E\BID\NTEC\Illinois\gov\PI\DOT\Documents\BID\DOT_Offices\BID\ref\1\Project\102219\CADD\B&E\Drawings\102219-shr-pln-bw.dgn

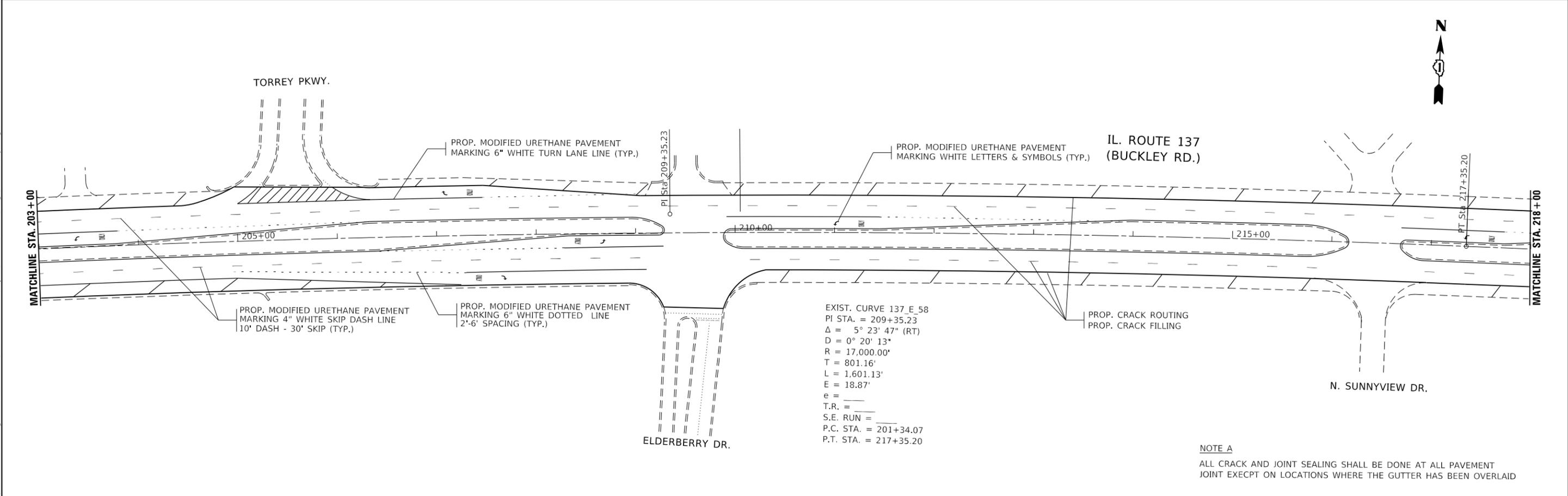
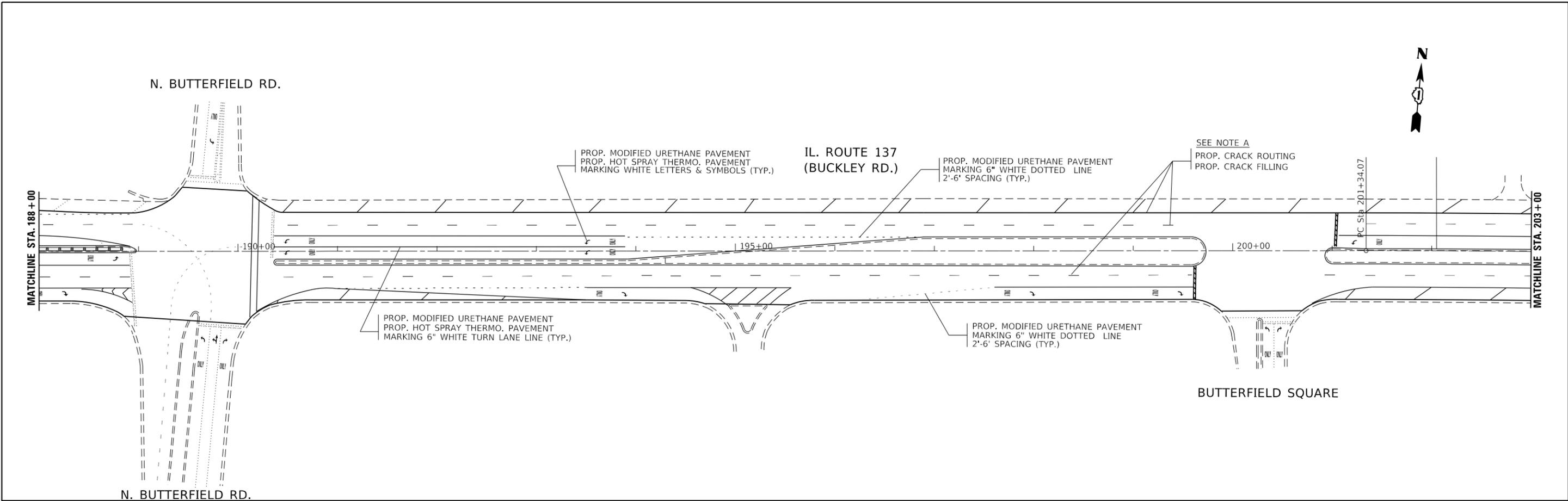
USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN
 IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	8
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



EXIST. CURVE 137_E_58
 PI STA. = 209+35.23
 $\Delta = 5^\circ 23' 47''$ (RT)
 $D = 0^\circ 20' 13''$
 $R = 17,000.00'$
 $T = 801.16'$
 $L = 1,601.13'$
 $E = 18.87'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 201+34.07$
 $P.T. STA. = 217+35.20$

NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

MODEL: Default
 FILE: \\nas01.cba.us.ibm.com\BIBID\NTEC\Illinois\gov\pww\DOT\Documents\DOT_Offices\BIBID\1\Projects\102219\CADD\Drawings\102219_sht-plan.dgn

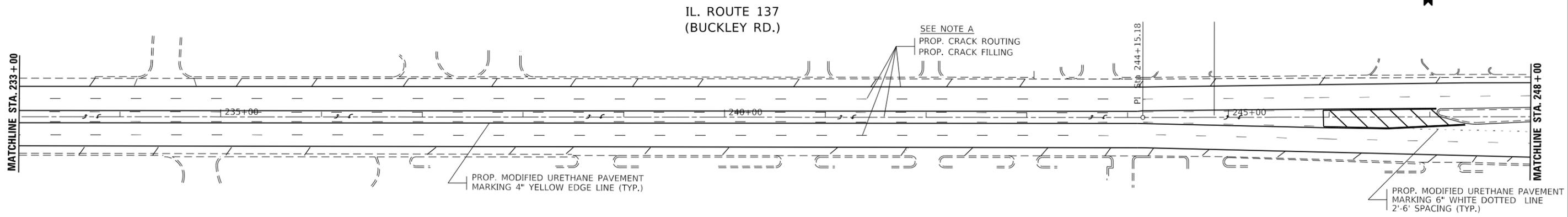
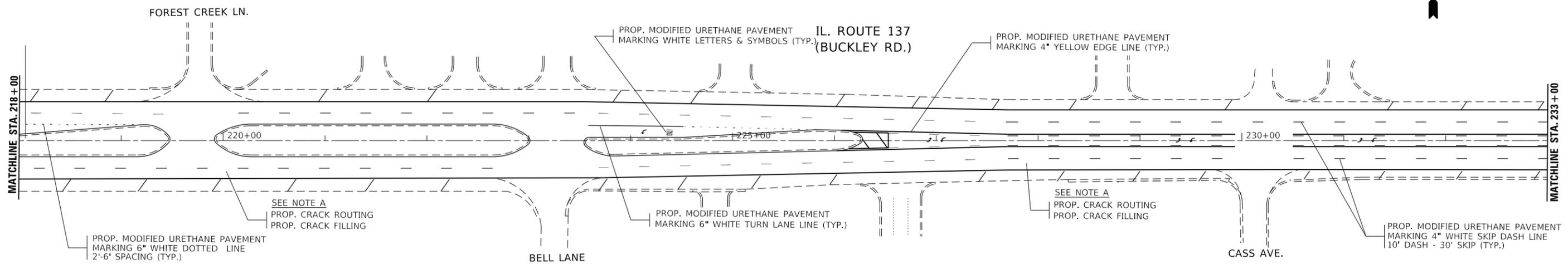
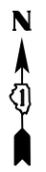
USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN
 IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)**

SCALE: 1" = 50'
 SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	10
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

MODEL: Default
 FILE: \\nas01.pva.us:public\BID\NTEC\Illinois\gov\PIWDOT\Documents\1\DOT Office\1\102219\CADD\Buckley\102219-19-eh-r-plan.dgn

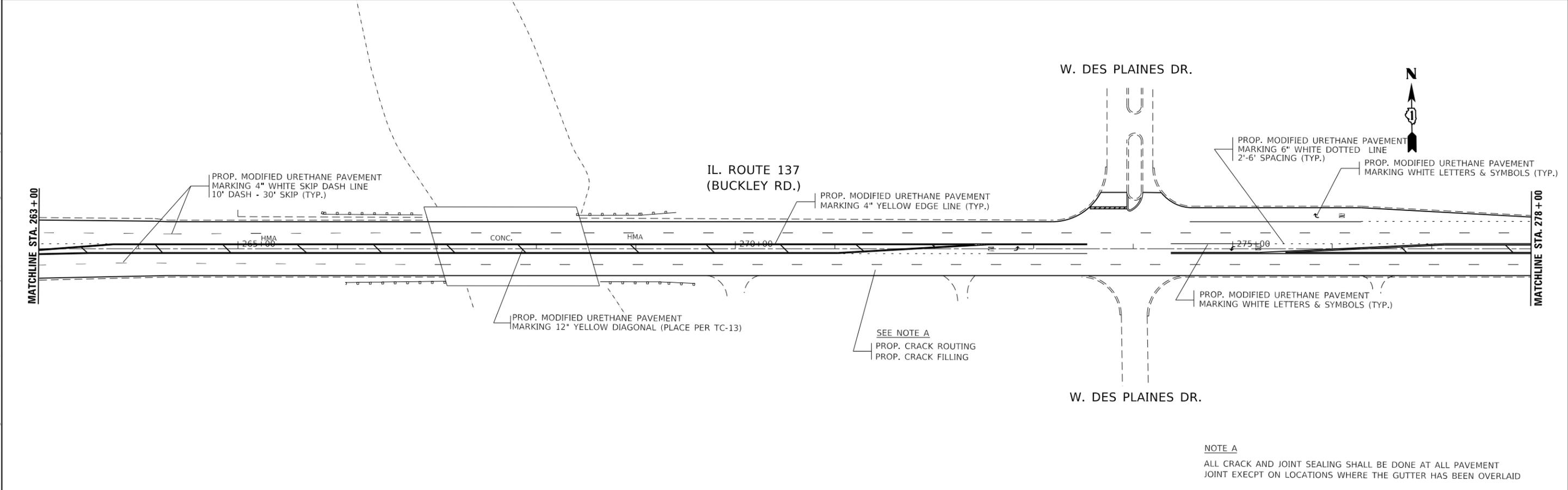
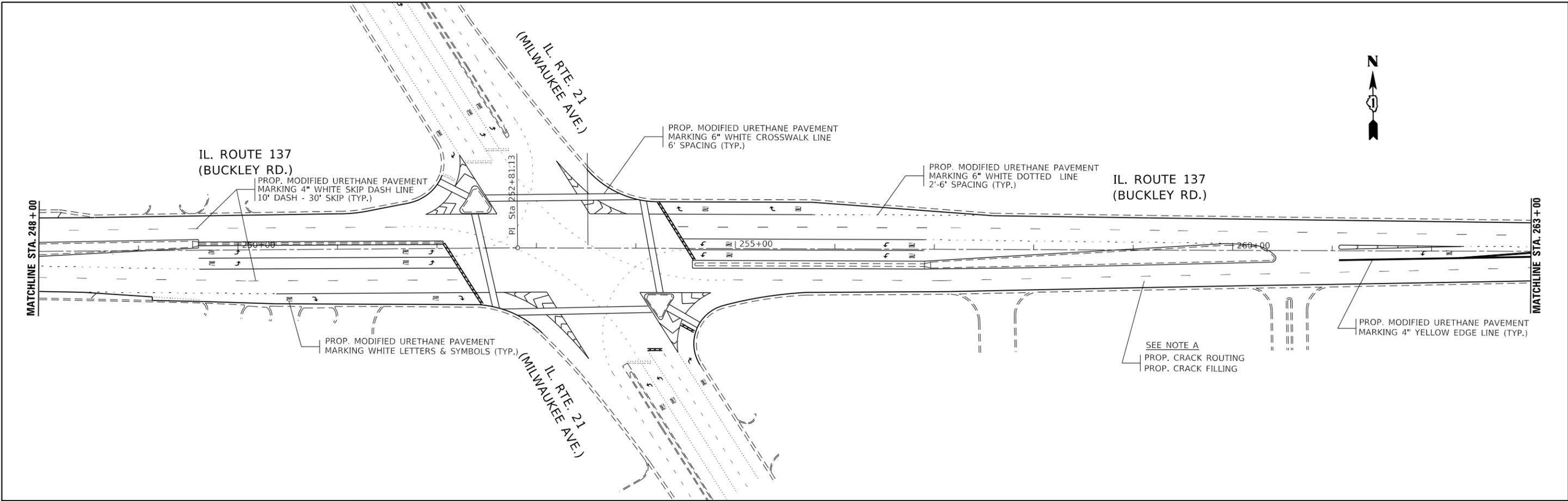
USER NAME = abebawa	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 2/22/2019	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN
 IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	11
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



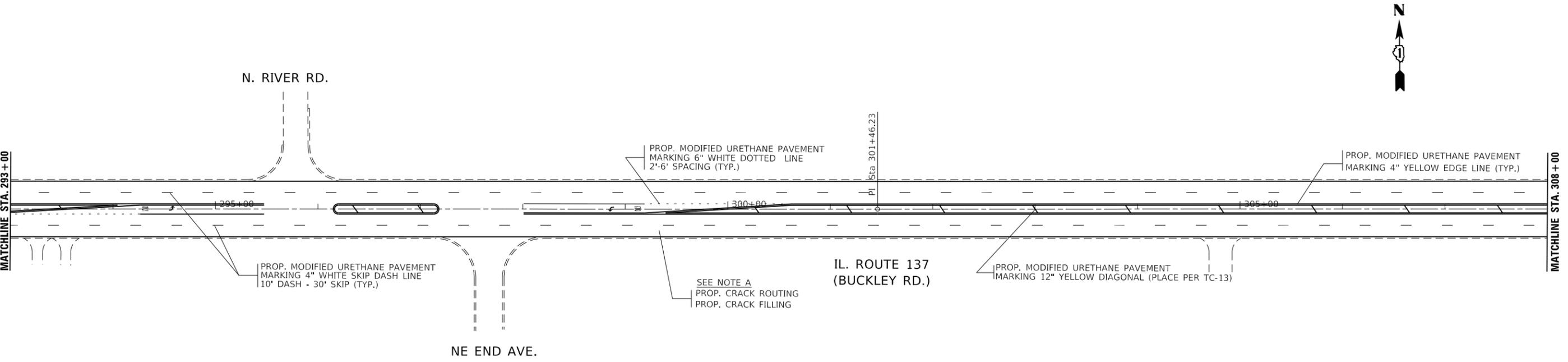
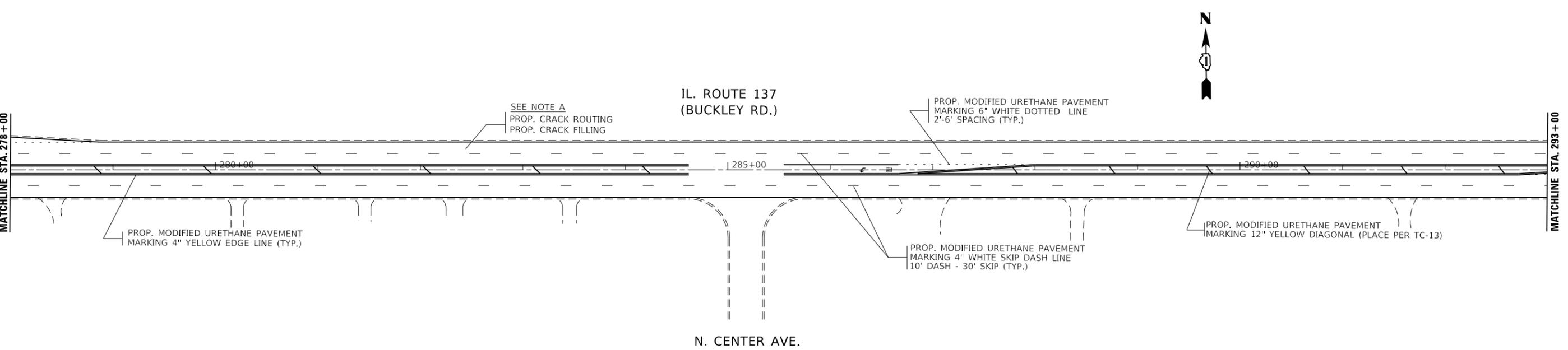
MODEL: Default
 FILE: \\nas0101\pub\B&E\BID\ITEC\Illinois\gov\PIW\DOT\Documents\1\DOT\Office\1\2022\19-ahr-plans.dgn

USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLAN			
IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)			
SCALE: 1"= 50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	12
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

MODEL: Default
 FILE: \\nas0101\B84E81D01\TEC\Illinois\gov\PIWDOT\Documents\DOT_Offices\DI\2022\19\CADD\B84E81D01\2022\19-shr-plan.dgn

USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

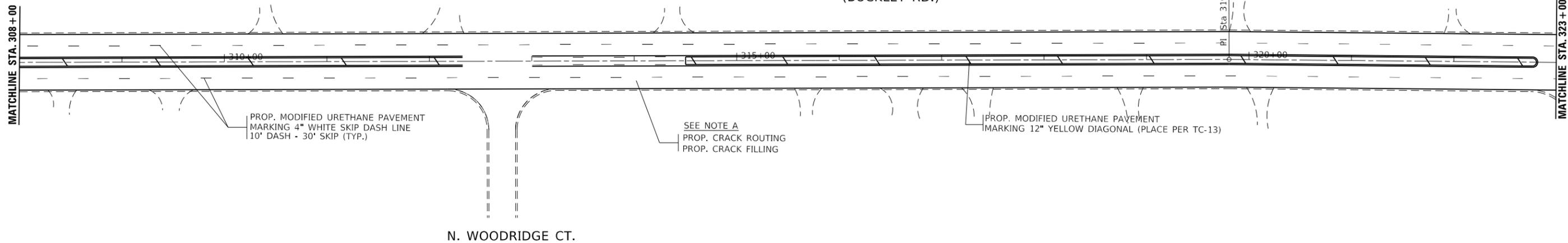
EXISTING AND PROPOSED ROADWAY PLAN
IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	13
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



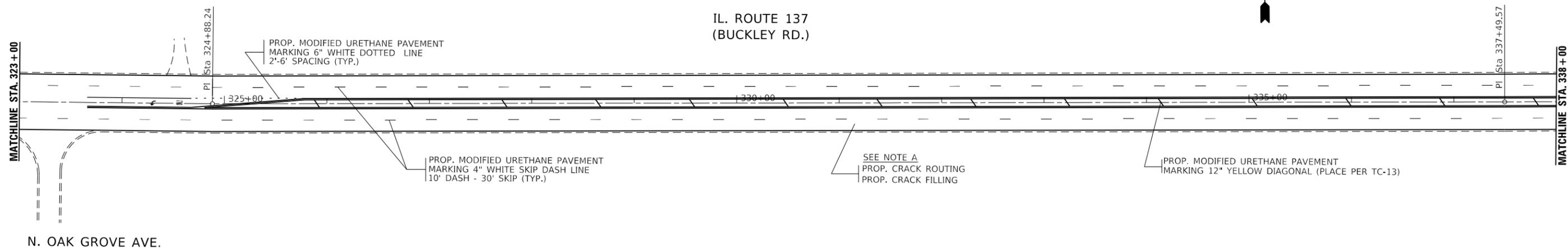
IL. ROUTE 137
(BUCKLEY RD.)



N. WOODRIDGE CT.



IL. ROUTE 137
(BUCKLEY RD.)



N. OAK GROVE AVE.

NOTE A
ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT
JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

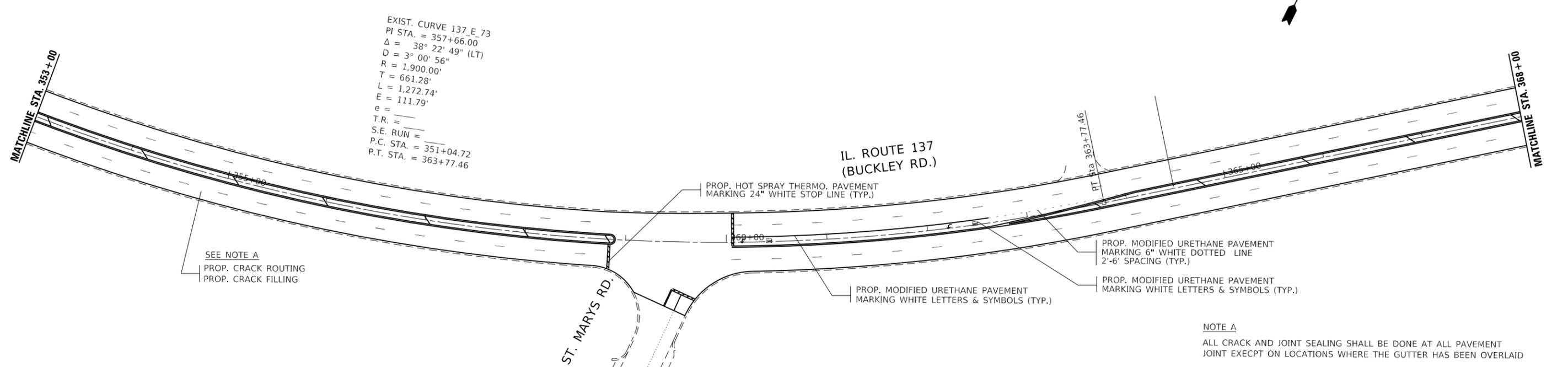
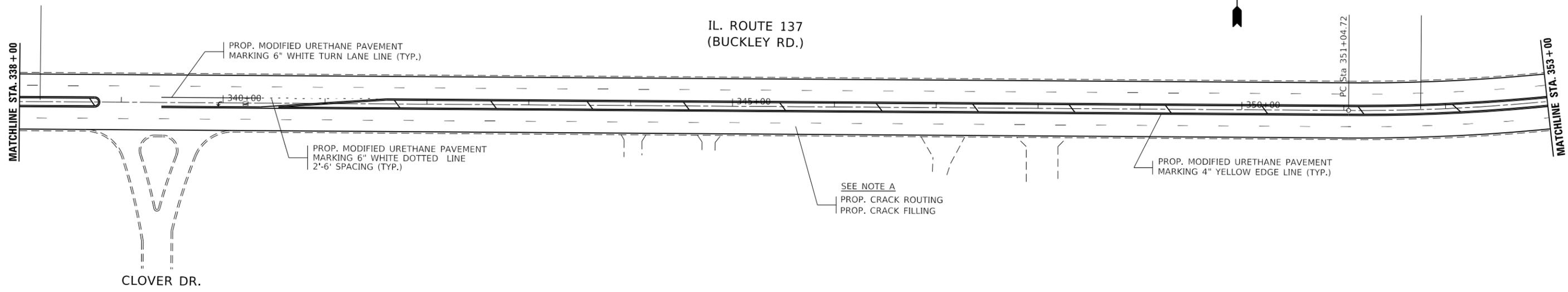
MODEL: Default
 FILE NAME: p:\11\08\BID\NTEC\Illinois.gov\FWIDOT\Documents\102219\CADD\Buckley\102219-eh-plan.dgn

USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN			
IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)			
SCALE: 1" = 50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	14
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



EXIST. CURVE 137_E_73
 PI STA. = 357+66.00
 $\Delta = 38^\circ 22' 49''$ (LT)
 $D = 3^\circ 00' 56''$
 $R = 1,900.00'$
 $T = 661.28'$
 $L = 1,272.74'$
 $E = 111.79'$
 $e =$
 $T.R. =$
 $S.E. RUN =$
 $P.C. STA. = 351+04.72$
 $P.T. STA. = 363+77.46$

NOTE A
 ALL CRACK AND JOINT SEALING SHALL BE DONE AT ALL PAVEMENT JOINT EXCEPT ON LOCATIONS WHERE THE GUTTER HAS BEEN OVERLAID

MODEL: Default
 FILE: \\nas01.psu.edu\BEBID\NTEC\Illinois\gov\PWIDOT\Documents\DOT_Offices\DI\02219\CADD\Buckley\02219-shr-plan.dgn

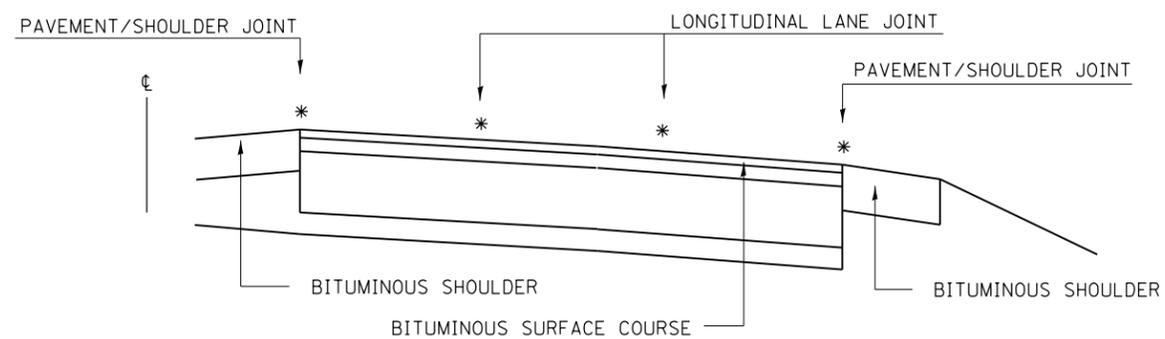
USER NAME = abebawa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/22/2019	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

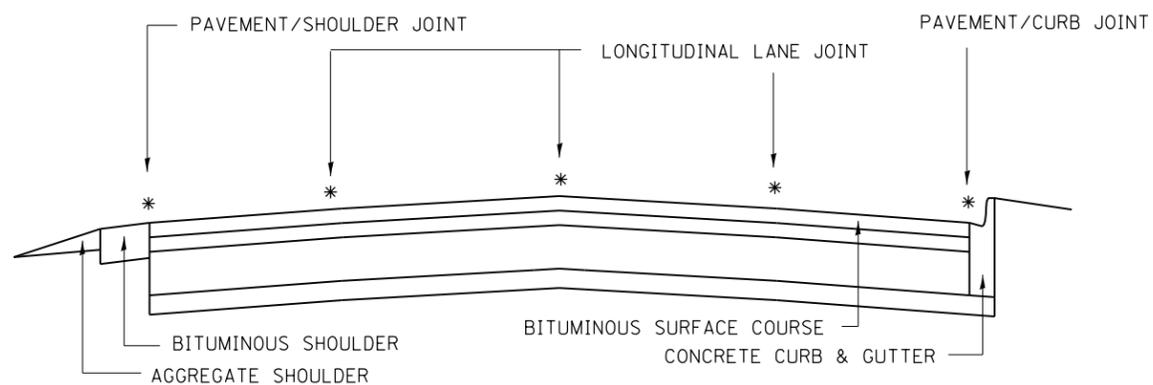
**EXISTING AND PROPOSED ROADWAY PLAN
 IL. RTE. 137 (BUCKLEY RD.) (IL. 83 TO O'PLAINE RD.)**

SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

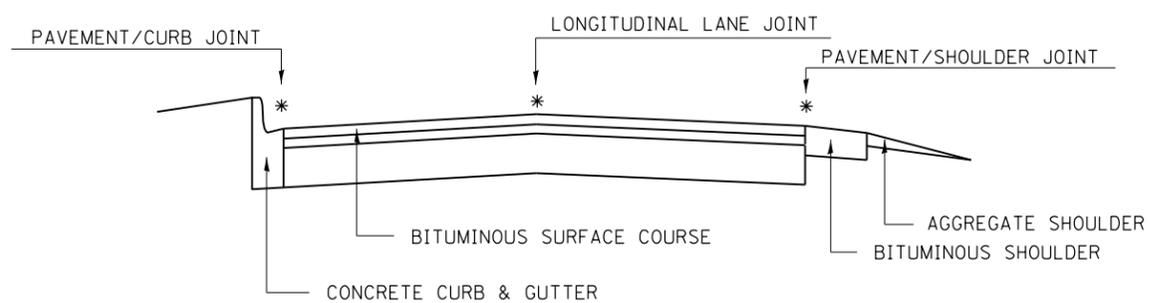
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CIS	LAKE	23	15
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



MULTI-LANE DIVIDED PAVEMENT



MULTI-LANE UNDIVIDED PAVEMENT

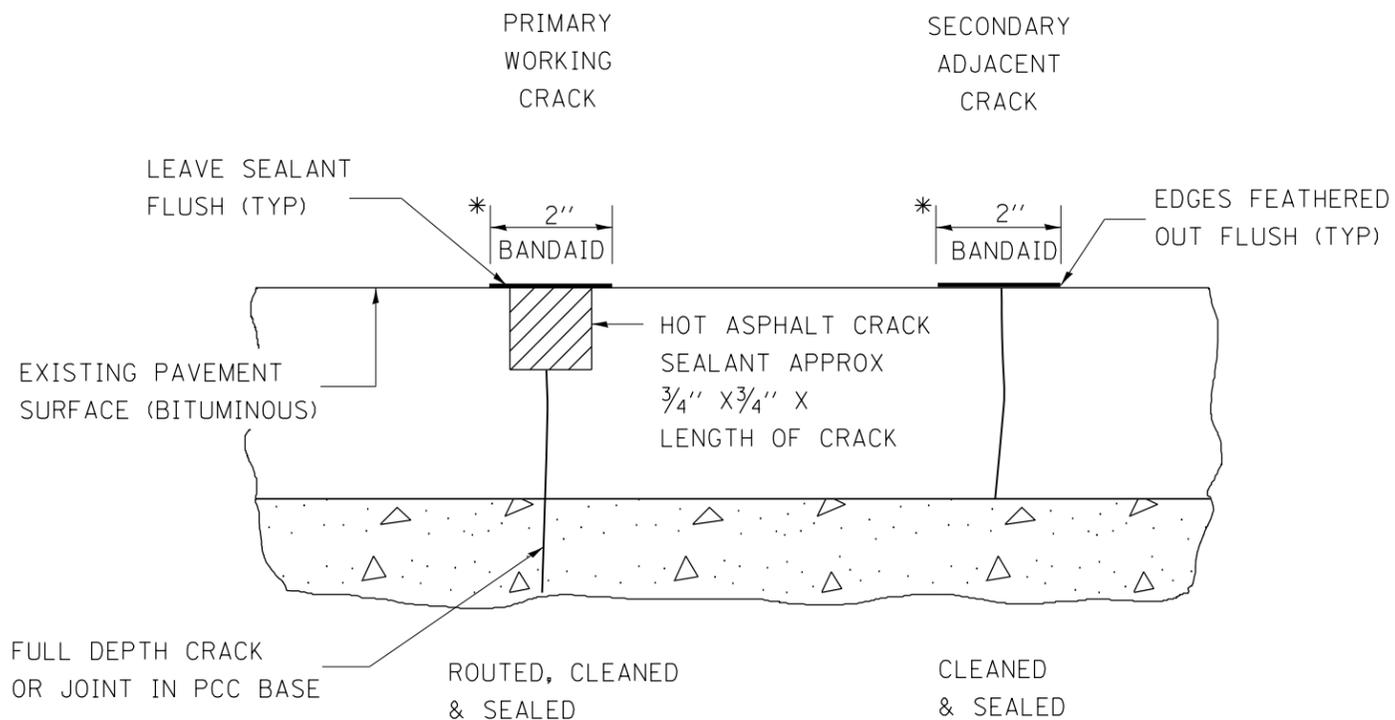


TWO-LANE PAVEMENT

PROPOSED TRANSVERSE CRACK ROUTING (PAVEMENT) & CRACK FILLING LOCATIONS NOT SHOWN

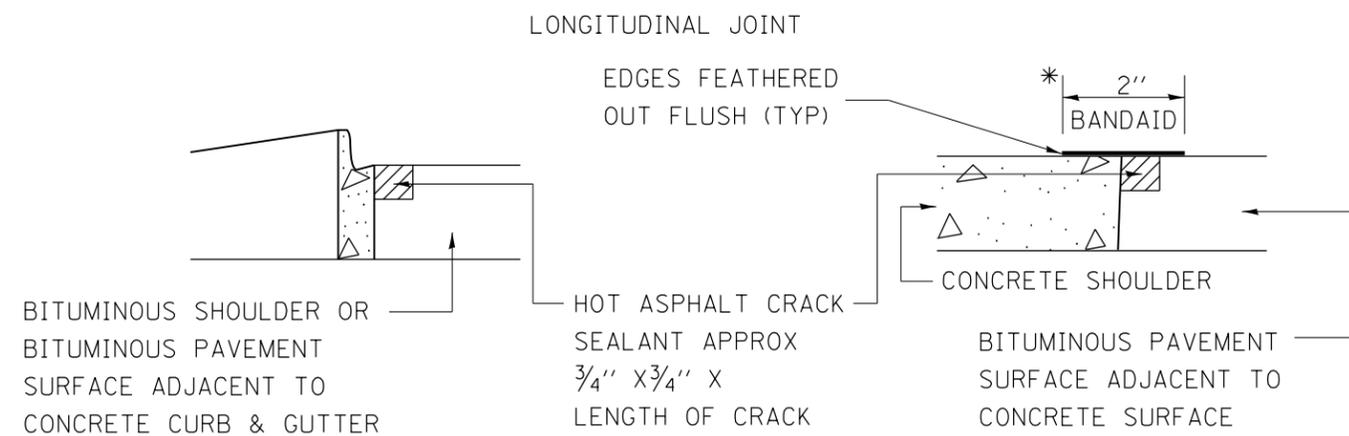
* PROPOSED LONGITUDINAL CRACK ROUTING (PAVEMENT) & CRACK FILLING LOCATIONS

GENERAL EXISTING TYPICAL SECTIONS



*

IN ALL LOCATIONS WHERE THE LONGITUDINAL CRACK CONFLICTS WITH EXISTING PAVEMENT MARKINGS (EITHER EDGE LINE OR LANE LINE) THE CRACK SHALL BE ROUTED & THE RESULTING RESERVOIR FILLED WITH SEALANT. ALL DAMAGES TO EXISTING RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REPLACED AT THE EXPENSE OF THE CONTRACTOR. ALL EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS SHALL BE RESTORED WITH HOT MODIFIED URETHANE PAVEMENT MARKINGS.



CRACK & JOINT SEALING DETAIL

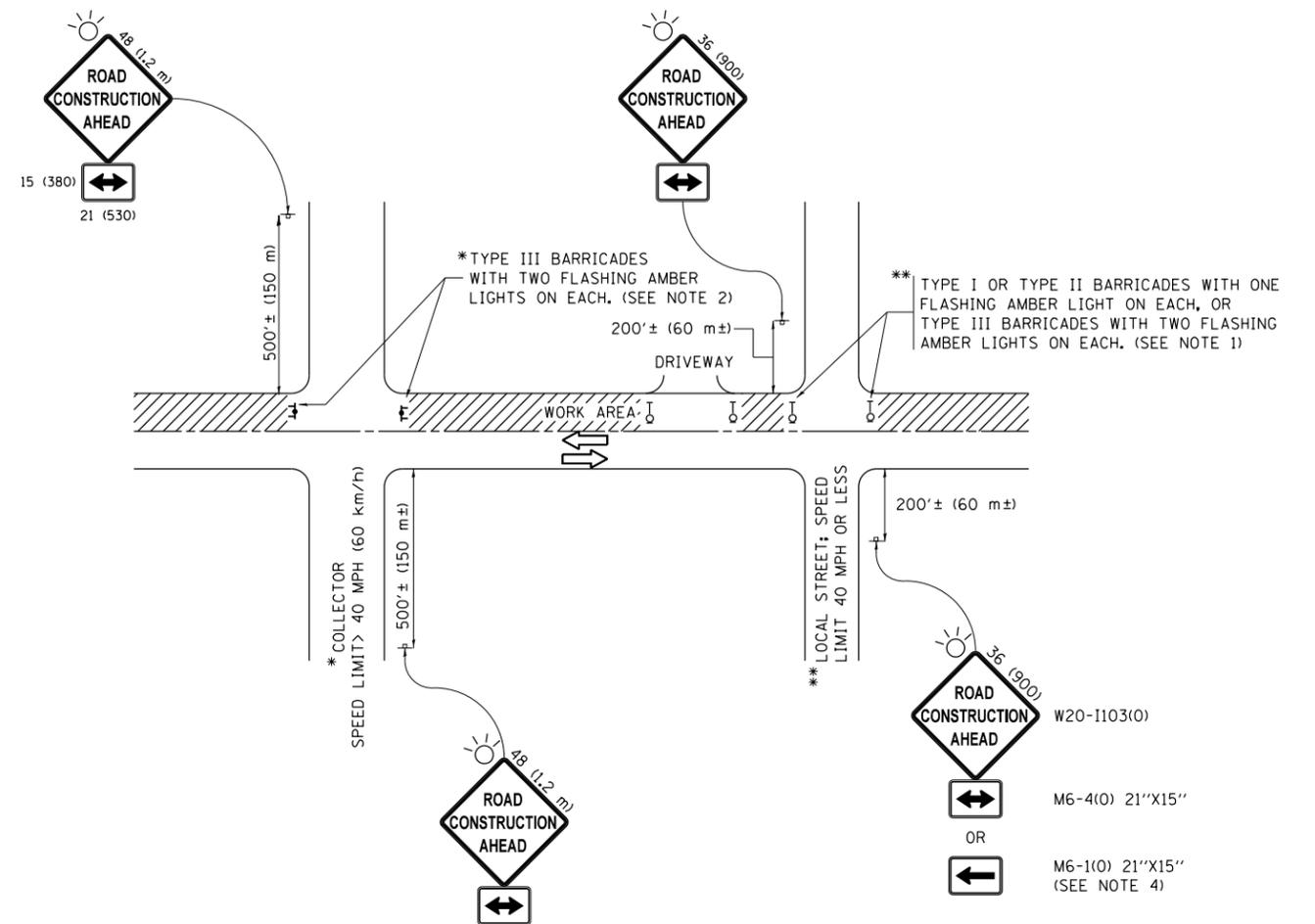
FILE NAME = D:\02219-sht-plen.dgn	USER NAME = abebawa	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -
	PLOT DATE = 3/15/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL 137 (BUCKLEY RD) (IL 83 TO O'PLAINE RD)
CRACK & JOINT SEALING DETAIL

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE. 352	SECTION 2018-088-CJS	COUNTY LAKE	TOTAL SHEETS 23	SHEET NO. 17
CONTRACT NO. 62H28				
ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

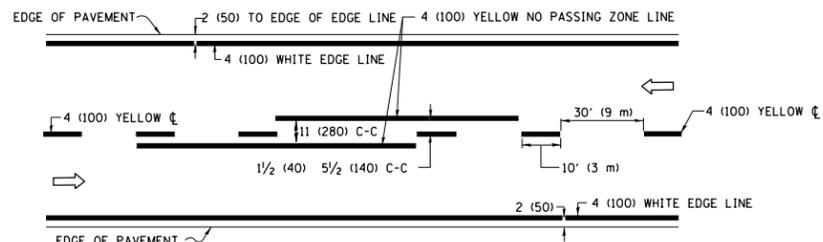
FILE NAME =	USER NAME = abebawa	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\11084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\1022\Drawings\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 2/1/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

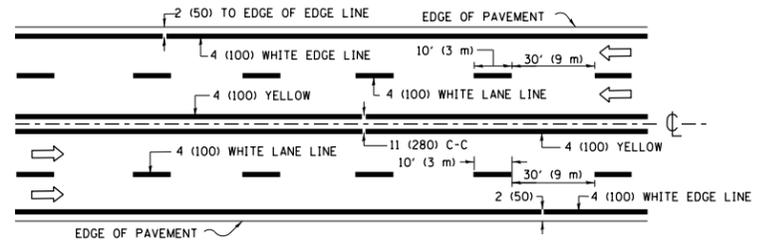
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

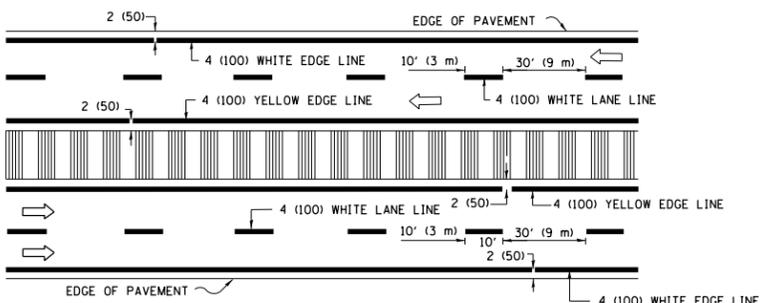
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CJS	LAKE	23	18
TC-10			CONTRACT NO. 62H28	
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

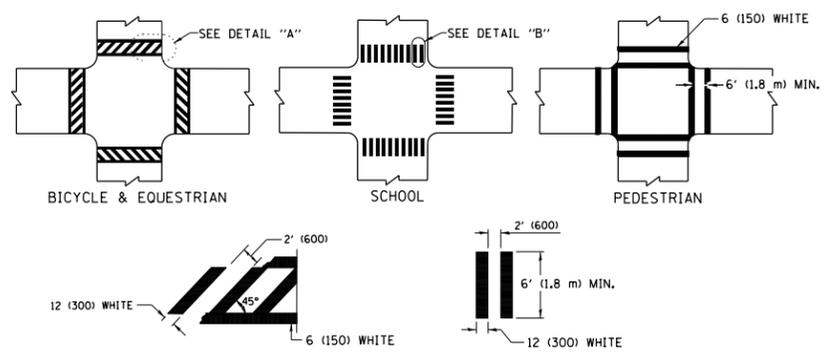


MULTI-LANE UNDIVIDED



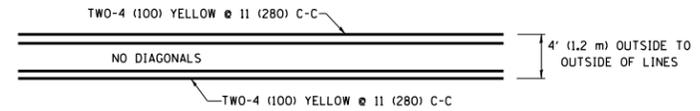
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

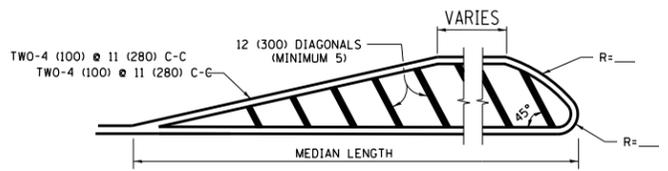


TYPICAL CROSSWALK MARKING

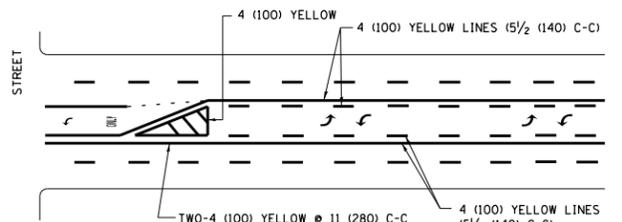
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



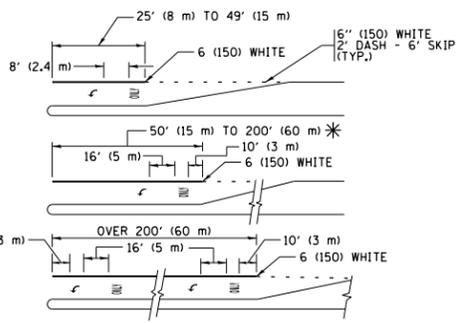
4' (1.2 m) WIDE MEDIANS ONLY



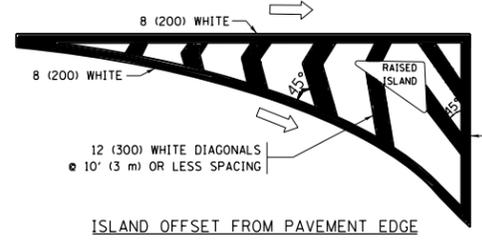
MEDIANS OVER 4' (1.2 m) WIDE



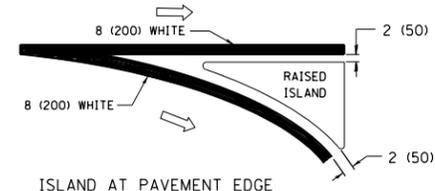
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



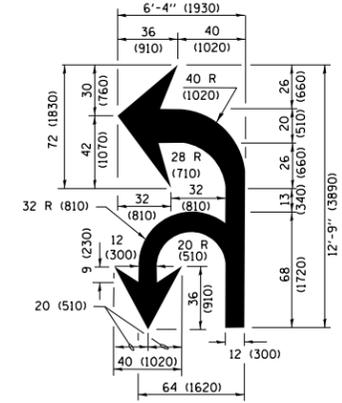
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING



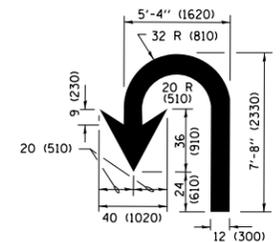
ISLAND OFFSET FROM PAVEMENT EDGE



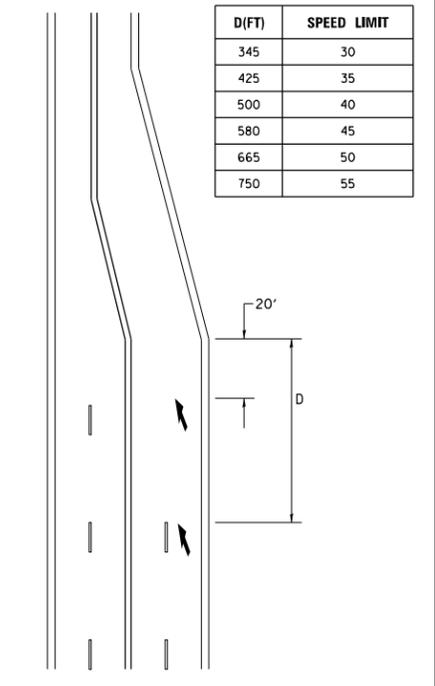
ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN



LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (22.5 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = abebawa	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG\illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\01022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 2/1/2019		REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CJS	LAKE	23	19
TC-13		CONTRACT NO. 62H28		
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

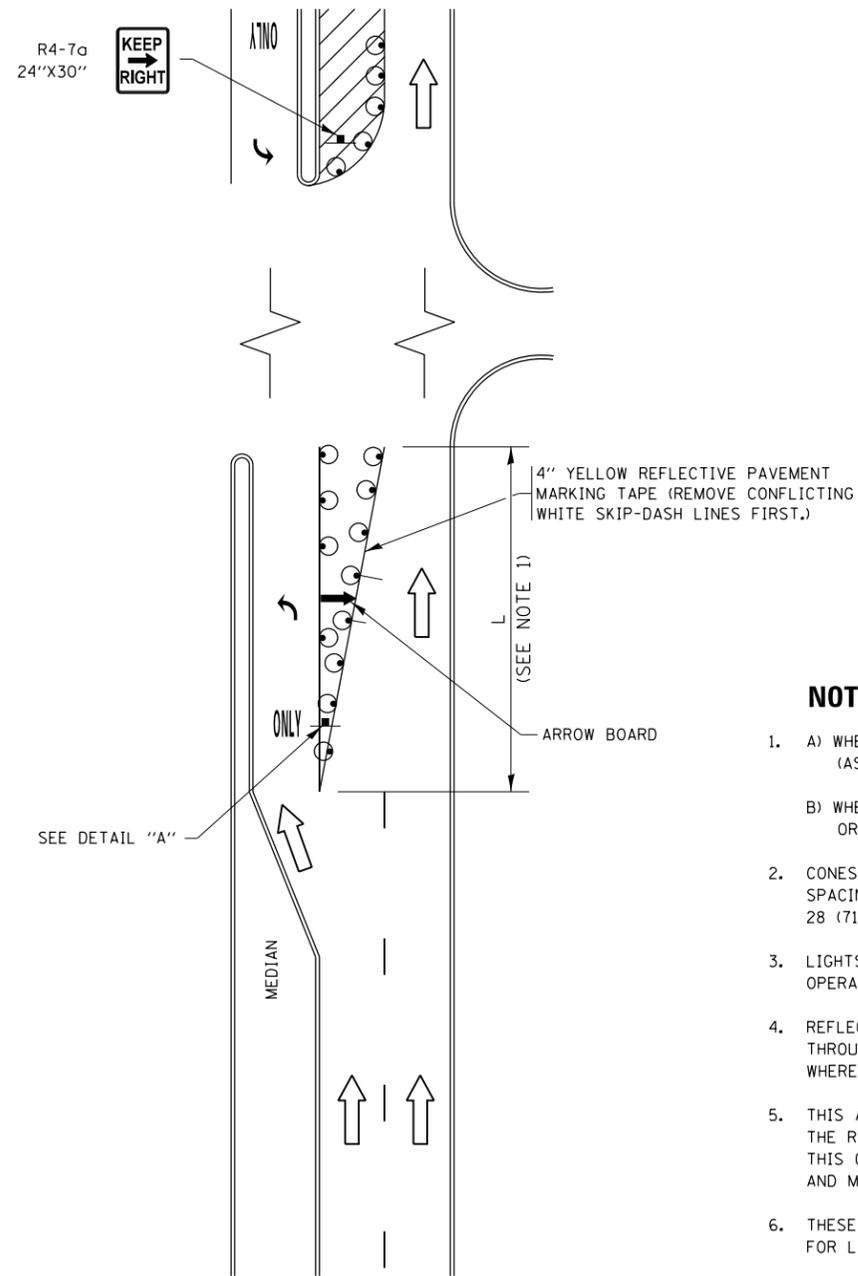


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

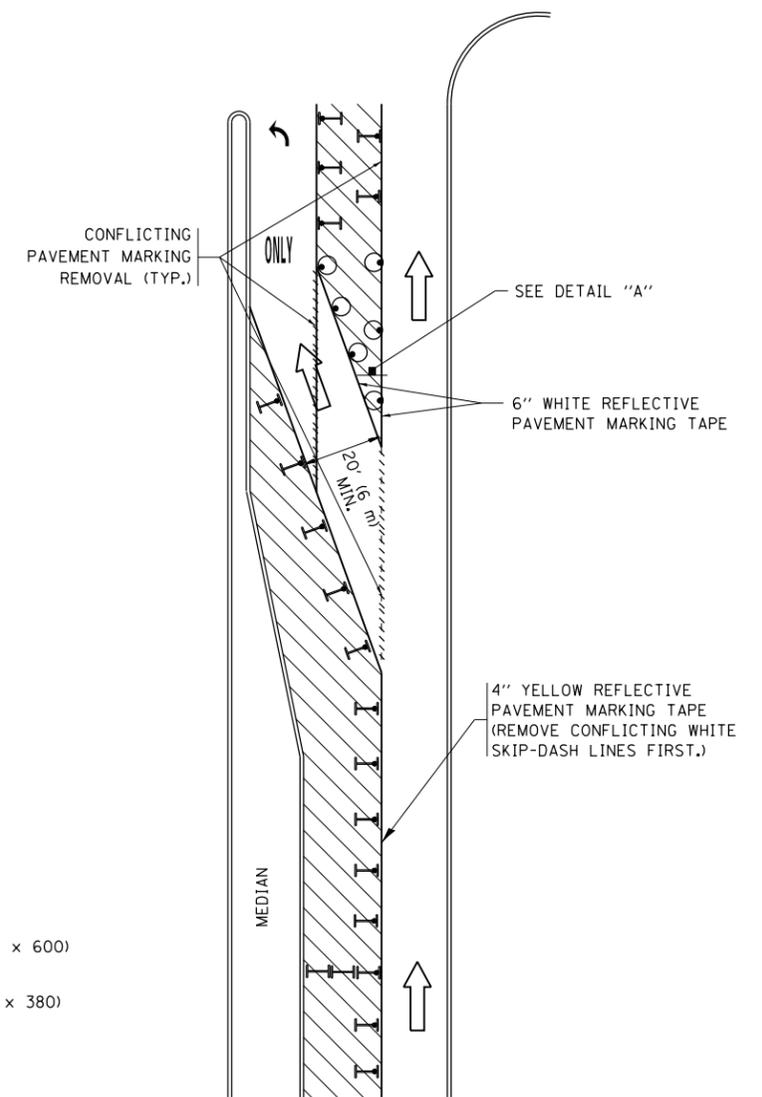
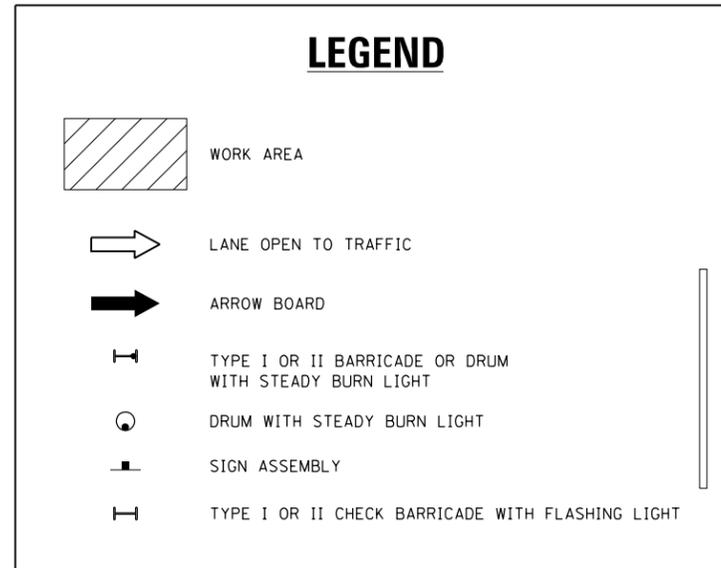
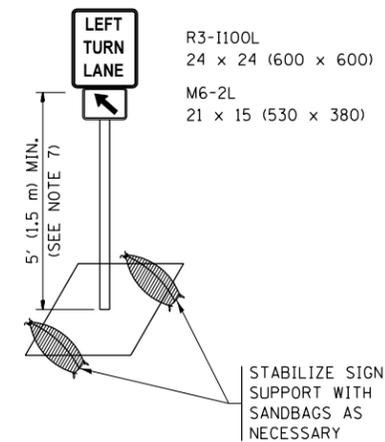


FIGURE 2



NOTES:

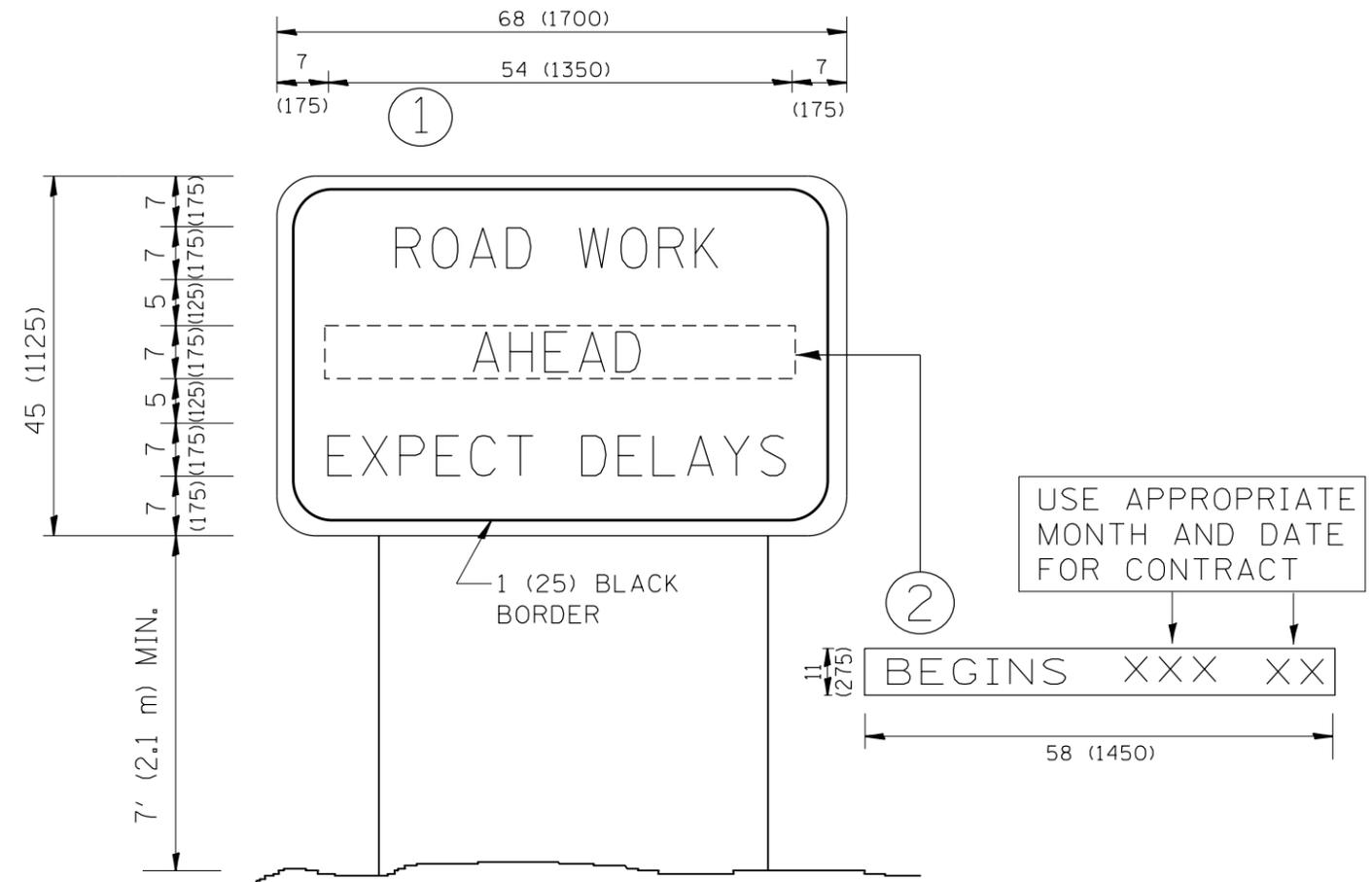
1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = abebawa	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)			F.A.P. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
Default		REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13					352	2018-088-CJS	LAKE	23	20
		REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		TC-14			CONTRACT NO. 62H28				
		REVISED - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

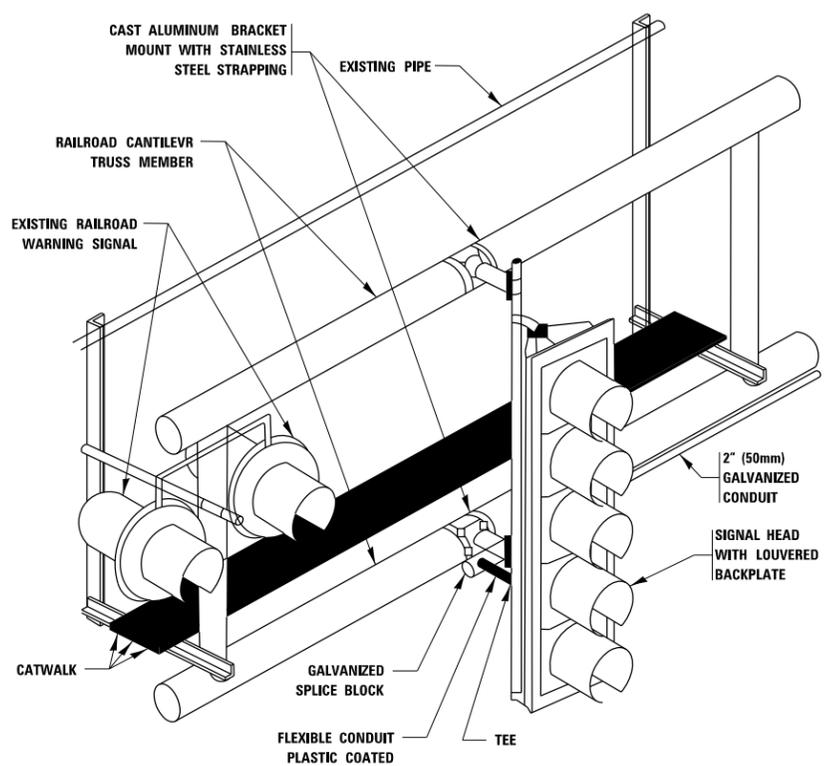
FILE NAME =	USER NAME = abebawa	DESIGNED -	REVISED - R. MIRS 09-15-97
p:\11\084EBIDINTEG\illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\DI022019\11\084EBIDINTEG\DistStd.dgn		CHECKED -	REVISED - R. MIRS 12-11-97
		PLOT SCALE = 100.0000' / 1in.	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CJS	LAKE	23	21
TC-22			CONTRACT NO. 62H28	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



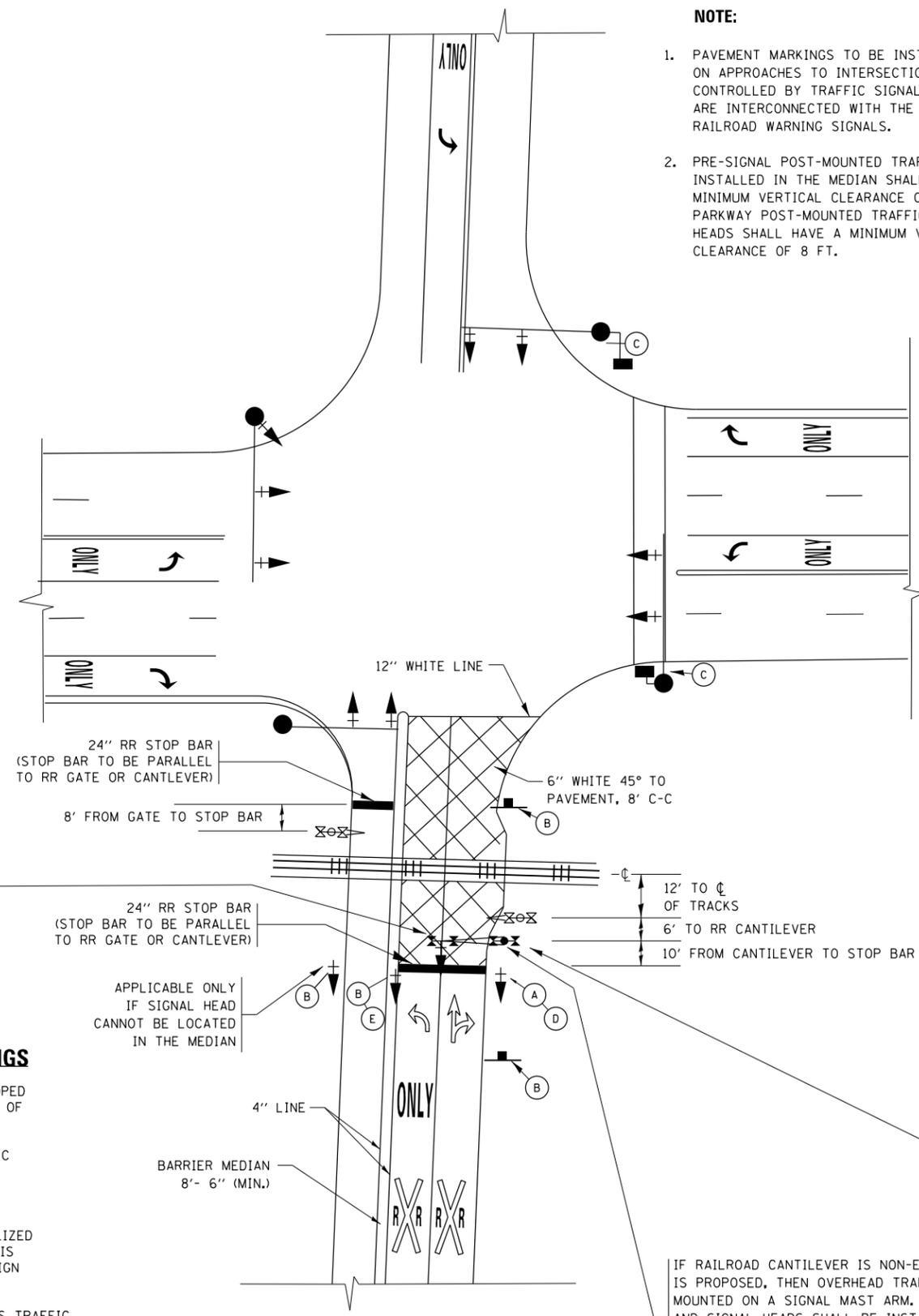
RAILROAD CANTILEVER SIGNAL HEAD MOUNTING
 USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION
 N.T.S.

SIGNING AND PAVEMENT MARKING AT RAILROAD CROSSINGS

SIGNING AND PAVEMENT MARKING TRAFFIC CONTROL STANDARD (TC-23) HAS BEEN DEVELOPED IN CONSULTATION WITH THE ILLINOIS COMMERCE COMMISSION AND THE U.S. DEPARTMENT OF TRANSPORTATION'S GRADE CROSSING SAFETY TASK FORCE. THIS STANDARD PROVIDES INFORMATION ON UPDATES TO THE PAVEMENT MARKING AND SIGNING DETAILS IN ORDER TO INCORPORATE CHANGES ADOPTED IN THE 2009 NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICE (MUTCD). THESE NEW DETAILS HAVE BEEN STUDIED AND TESTED BY THE DEPARTMENT AND ACCEPTED BY THE ILLINOIS COMMERCE COMMISSION.

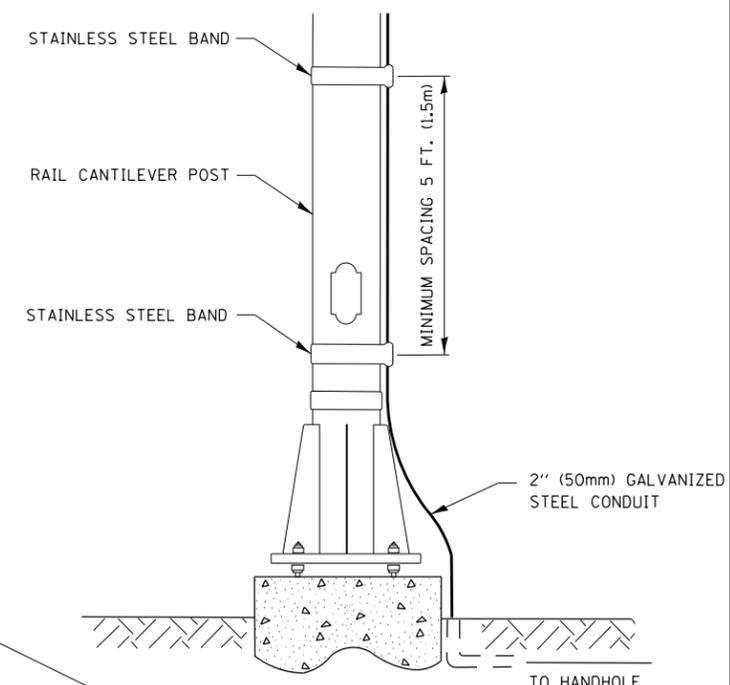
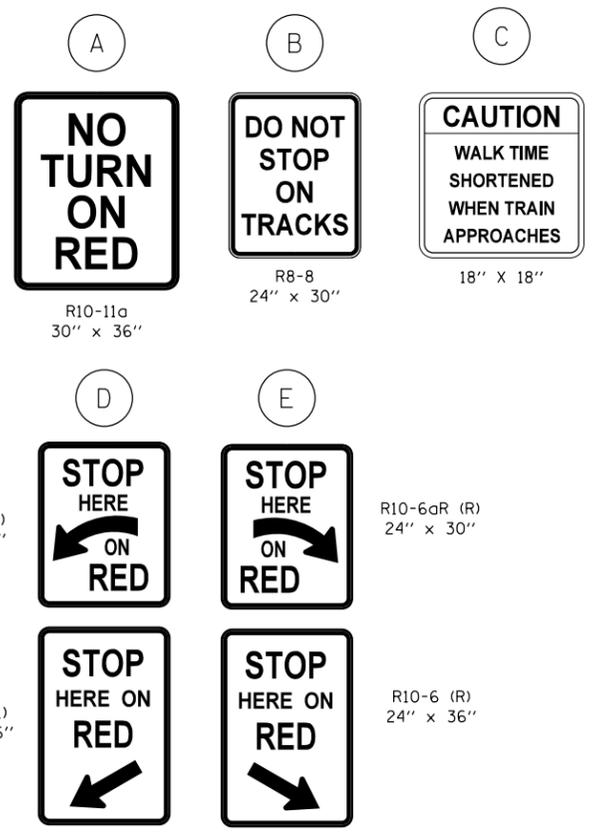
THIS APPLIES TO PROJECTS WHICH INCLUDE RAILROAD INTERCONNECTED TRAFFIC SIGNALS, WITH OR WITHOUT PRE-SIGNALS. THIS STANDARD ALSO APPLIES TO NON-SIGNALIZED INTERSECTIONS THAT ARE WITHIN 81 FEET OF A RAILROAD GRADE CROSSING. THE ILLINOIS SUPPLEMENT TO THE MUTCD SHOULD BE CONSULTED FOR ADDITIONAL INFORMATION ON SIGN REQUIREMENTS AT NON-SIGNALIZED INTERSECTIONS NEAR RAILROAD GRADE CROSSINGS.

THESE DETAILS WILL BE INCLUDED IN A FUTURE UPDATE TO THE BUREAU OF OPERATIONS TRAFFIC POLICIES AND PROCEDURES MANUAL.



SIGNALIZED INTERSECTION WITH NEAR-SIDE TRAFFIC SIGNAL

- NOTE:**
- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
 - PRE-SIGNAL POST-MOUNTED TRAFFIC SIGNAL HEADS INSTALLED IN THE MEDIAN SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 4.5 FT. PARKWAY POST-MOUNTED TRAFFIC SIGNAL HEADS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 8 FT.



SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL

USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

IF RAILROAD CANTILEVER IS NON-EXISTANT AND NONE IS PROPOSED, THEN OVERHEAD TRAFFIC SIGNAL TO BE MOUNTED ON A SIGNAL MAST ARM. SIGNAL MAST ARM AND SIGNAL HEADS SHALL BE INSTALLED AS CLOSE AS PRACTICABLE TO THE RAILROAD TRACKS WITHOUT OBSTRUCTING ANY RAILROAD WARNING DEVICES. SIGNAL MAST ARM SHALL BE AT LEAST 12 FT. FROM NEAREST RAIL.

FILE NAME =	USER NAME = abebawa	DESIGNED -	REVISED - 02-25-11
pw\1\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - 04-26-12
Default	PLOT SCALE = 100.0000' / 1in.	DATE -	REVISED - A.R. 07-11-16
	PLOT DATE = 2/1/2019		REVISED -

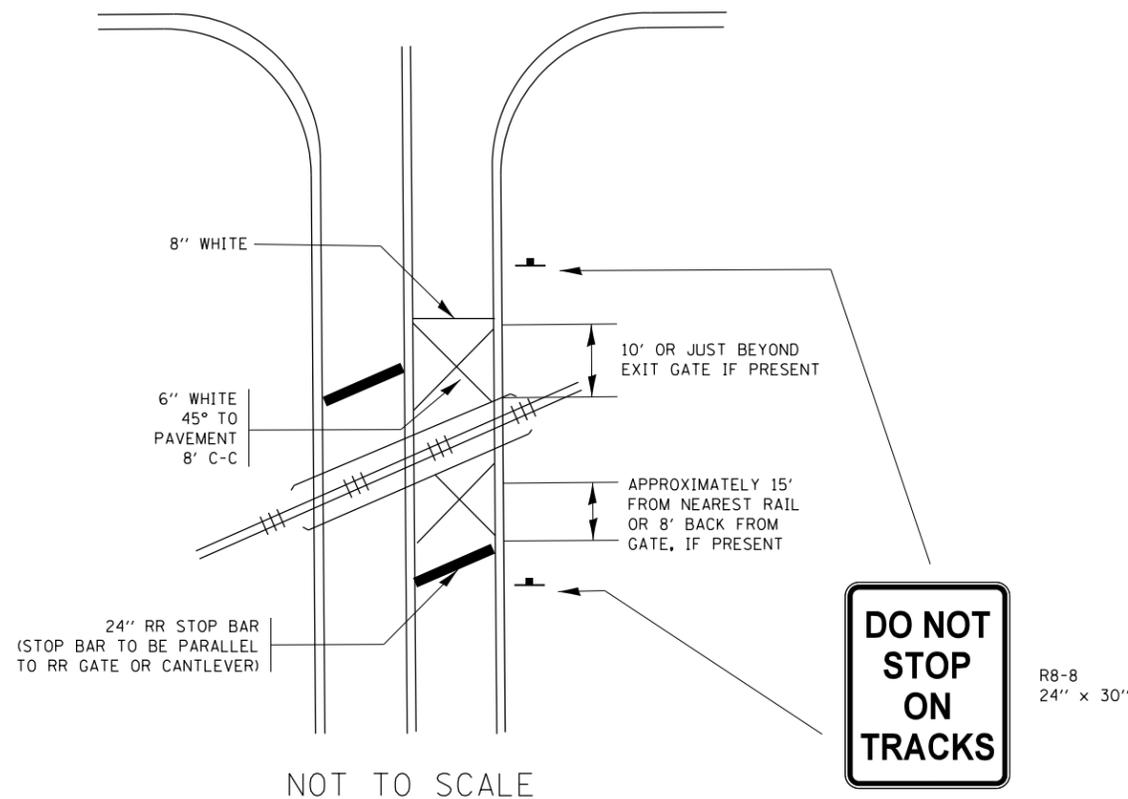
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
 TREATMENT FOR RAILROAD CROSSINGS**
 SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
352	2018-088-CJS	LAKE	23	22
TC-23		CONTRACT NO. 62H28		
ILLINOIS FED. AID PROJECT				

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

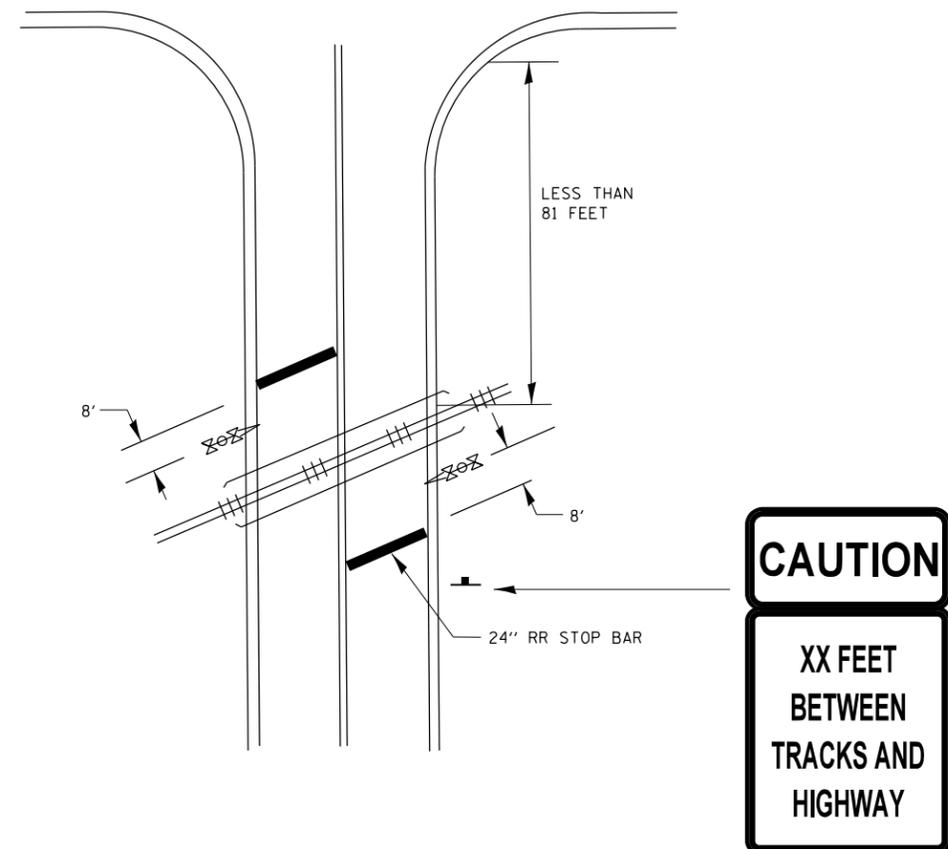
WITH SIGNALIZED INTERSECTION



NOTE:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON-SIGNALIZED INTERSECTION 81' OR LESS TO CLOSEST RAIL



NOTE:

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = abebawa	DESIGNED -	REVISED - 02-25-11	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\11022\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - 04-26-12			352	2018-088-CJS	LAKE	23	23
Default		PLOT SCALE = 100.0000' / 1in.	REVISED - A.R. 07-11-16			TC-23		CONTRACT NO. 62H28		
		PLOT DATE = 2/1/2019	DATE -			SCALE: NONE	SHEET 2 OF 2 SHEETS	STA.	TO STA.	