

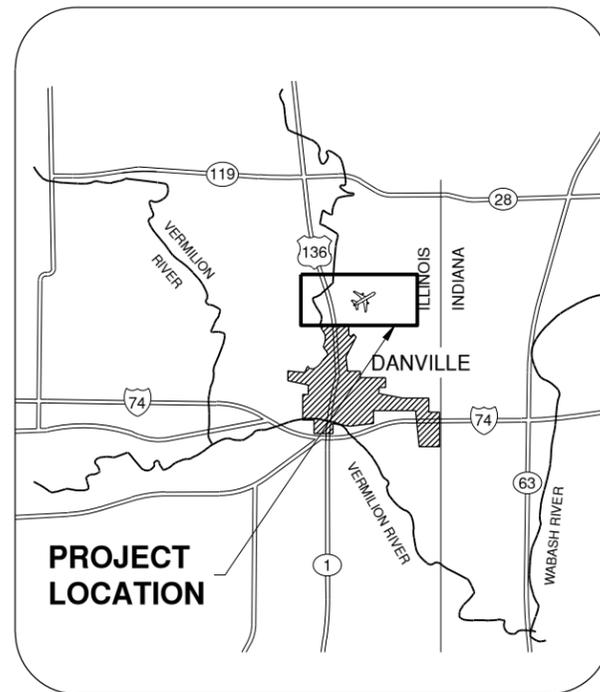
CONSTRUCTION PLANS FOR VERMILION REGIONAL AIRPORT AUTHORITY

VERMILION REGIONAL AIRPORT DANVILLE, IL

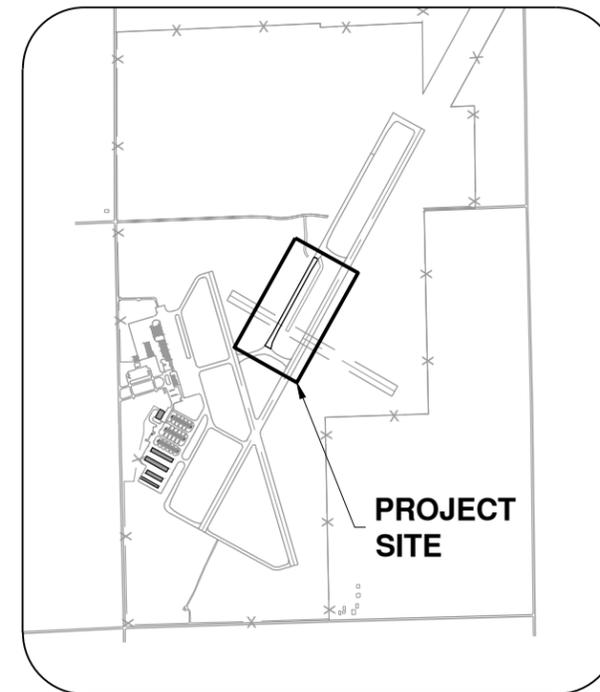
IL. PROJ. NO: DNV-5169 FED. PROJ. NO. 3-17-SBGP-TBD

REALIGN TAXIWAY A, PHASE 3

SUBMITTAL DATE: FEBRUARY 28, 2025



LOCATION MAP



SITE PLAN

811 Know what's below. Call before you dig. COMMON GROUND ALLIANCE
www.call811.com or Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123
TOWNSHIP: 20 NORTH
RANGE: 11 WEST
SECTION: 15
COUNTY: VERMILION
CIVIL TOWNSHIP: NEWELL

COMMON TRAFFIC ADVISORY FREQUENCY 122.7

APPROXIMATE MAXIMUM HEIGHT OF EQUIPMENT ABOVE GROUND IS 25 FT.

APPROVED: 
DATE: 2-26-2025



Eric Hills
EXP. 11/30/25



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SUBMITTED BY: 
DATE: 2/25/2025

CMT JOB NUMBER: 190042-02-30



License No. 184-00613

CONSULTANTS

100% SUBMITTAL
APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

MARK | DATE | DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD

IL. PROJ. NO. DNV-5169

CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-G1002.DWG

DESIGNED BY: HCH

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: EMH

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SHEET TITLE

INDEX TO SHEETS &
SUMMARY OF
QUANTITIES

G1002
SHEET 2 OF 63

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2	GI002	INDEX TO SHEETS & SUMMARY OF QUANTITIES
3	GI003	WORK LIMITS
4	GH01	AIRPORT SITE PLAN & CONSTRUCTION ACCESS
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9	GC102	CONSTRUCTION ACTIVITY PLAN WORK AREA A
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12	CD101	DEMOLITION PLAN 1
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15	CD501	PAVEMENT DEMOLITION AND SUBGRADE DETAIL
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21	CS101	STAKING PLAN
22	CU101	GRADING & DRAINAGE PLAN 1
23	CU102	GRADING & DRAINAGE PLAN 2
24	CU103	GRADING & DRAINAGE PLAN 3
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34	CU508	DRAINAGE STRUCTURE DETAIL 1
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46	CL101	EROSION CONTROL & TURFING 1
47	CL102	EROSION CONTROL & TURFING 2
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63	GC634	PROPOSED STOCKPILE DETAIL

HOT MIX ASPHALT (HMA) MIXTURE REQUIREMENTS TABLE					
ITEM	UNDER 60K LBS. RUNWAY/TAXIWAY NDES	PG BINDER	AGGREGATE QUALITY	MAX RAP	DENSITY ACCEPTANCE
401: SURFACE (2")	N30@3.0%	SBS PG70-28	A	0%	METHOD I MAINLINE: NUCLEAR DENSITY JOINTS: CORES
403: BINDER (5")	N30@3.0%	TOP LIFT: SBS PG70-28	B	20%	METHOD I (METHOD II IF AA #1 IS AWARDED)
MIN. LIFT THICKNESS - 2.25"		LOWER LIFT: PG 64-22			METHOD I: MAINLINE - NUCLEAR GAUGE JOINTS - CORES METHOD II: PWL CORES

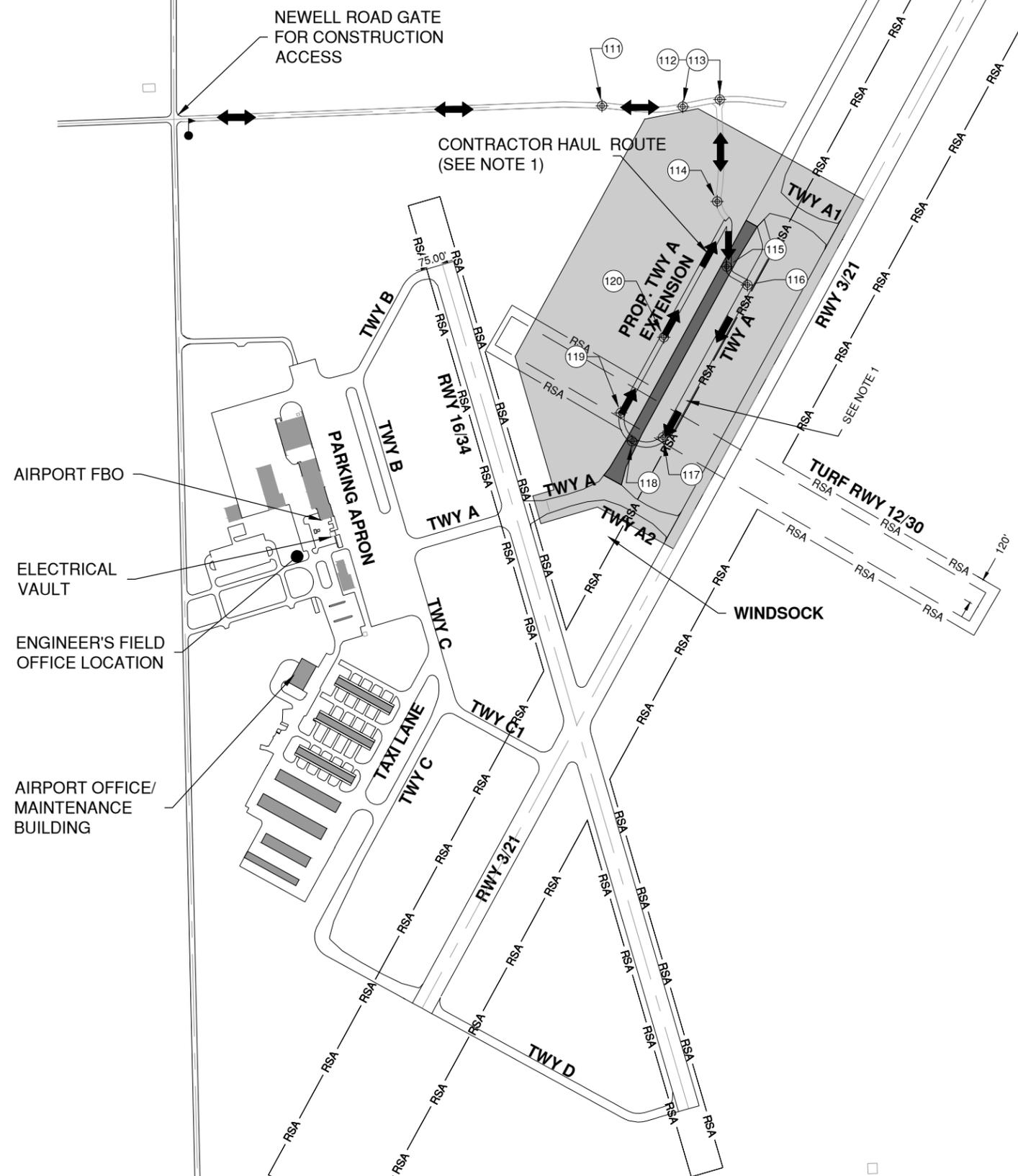
BASE BID SUMMARY OF QUANTITIES			
PAY ITEM	ITEM DESCRIPTION	UNIT	QTY
AR108158	1/C #8 5 KV UG CABLE IN UD	FOOT	3,100.0
AR108258	2/C #8 5 KV UG CABLE IN UD	FOOT	1,850.0
AR108706	1/C #6 COUNTERPOISE	FOOT	3,100.0
AR108960	REMOVE CABLE	FOOT	3,400.0
AR125410	MITL - STAKE MOUNTED	EACH	16.0
AR125415	MITL - BASE MOUNTED	EACH	7.0
AR125444	TAXI GUIDANCE SIGN, 4 CHARACTER	EACH	2.0
AR125445	TAXI GUIDANCE SIGN, 5 CHARACTER	EACH	1.0
AR125446	TAXI GUIDANCE SIGN, 6 CHARACTER	EACH	2.0
AR125565	SPLICE CAN	EACH	1.0
AR125904	REMOVE TAXI GUIDANCE SIGN	EACH	1.0
AR125906	REMOVE SPLICE CAN	EACH	2.0
AR125913	REMOVE EDGE LIGHT	EACH	18.0
AR150510	ENGINEER'S FIELD OFFICE	L SUM	1.0
AR150520	MOBILIZATION	L SUM	1.0
AR150550	CONSTRUCTION ACCESS	L SUM	1.0
AR150552	RESTORE CONSTRUCTION ACCESS	L SUM	1.0
AR152410	UNCLASSIFIED EXCAVATION	CU YD	16,950.0
AR152490	CRUSHED AGGREGATE BACKFILL	CU YD	650.0
AR152515	SUBGRADE UNDERCUT	CU YD	650.0
AR156510	SILT FENCE	FOOT	350.0
AR156513	SEPARATION FABRIC	SQ YD	1,300.0
AR156515	STRAW WATTLE	FOOT	350.0
AR156520	INLET PROTECTION	EACH	4.0
AR209607	CRUSHED AGG. BASE COURSE - 7"	SQ YD	6,500.0
AR401010	CONTRACTOR QUALITY CONTROL PROGRAM	L SUM	1.0
AR401610	BITUMINOUS SURFACE COURSE	TON	720.0
AR401921	REMOVE PAVEMENT	SQ YD	4,900.0
AR403610	BITUMINOUS BASE COURSE	TON	1,830.0
AR602510	BITUMINOUS PRIME COAT	GALLON	2,000.0
AR603510	BITUMINOUS TACK COAT	GALLON	650.0
AR620520	PAVEMENT MARKING - WATERBORNE	SQ FT	2,600.0
AR620525	PAVEMENT MARKING - BLACK BORDER	SQ FT	2,800.0
AR620900	PAVEMENT MARKING REMOVAL	SQ FT	500.0
AR701518	18" RCP, CLASS IV	FOOT	470.0
AR701530	30" RCP, CLASS IV	FOOT	180.0
AR701900	REMOVE PIPE	FOOT	650.0
AR705524	4" PERFORATED UNDERDRAIN W/ SOCK	FOOT	2,700.0
AR705544	4" NON PERFORATED UNDERDRAIN	FOOT	60.0
AR705635	UNDERDRAIN COLLECTION STRUCTURE	EACH	1.0
AR705640	UNDERDRAIN CLEANOUT	EACH	7.0
AR705645	UNDERDRAIN CONNECTION	EACH	1.0
AR705900	REMOVE UNDERDRAIN	FOOT	600.0
AR705904	REMOVE UNDERDRAIN CLEANOUT	EACH	3.0
AR751411	INLET - TYPE A	EACH	2.0
AR751550	MANHOLE 5'	EACH	2.0
AR751900	REMOVE INLET	EACH	2.0
AR800194	REMOVE ELEVATED RETROREFLECTIVE MARKER	EACH	4.0
AR801456	UNDERDRAIN DIRECT CONNECT	EACH	2.0
AR801512	JUMPER CABLE	FOOT	300.0
AR901510	SEEDING	ACRE	23.5
AR904510	SODDING	SQ YD	1,520.0
AR908514	LIGHT-DUTY HYDRAULIC MULCH	ACRE	23.5

ADDITIVE ALTERNATE #1 SUMMARY OF QUANTITIES			
PAY ITEM	ITEM DESCRIPTION	UNIT	QTY
AS152490	CRUSHED AGGREGATE BACKFILL	CU YD	180.0
AS152515	SUBGRADE UNDERCUT	CU YD	180.0
AS156513	SEPARATION FABRIC	SQ YD	400.0
AS209607	CRUSHED AGG. BASE COURSE - 7"	SQ YD	1,800.0
AS401610	BITUMINOUS SURFACE COURSE	TON	200.0
AS403610	BITUMINOUS BASE COURSE	TON	500.0
AS403630	BITUMINOUS BASE TEST SECTION	EACH	1.0
AS602510	BITUMINOUS PRIME COAT	GALLON	550.0
AS603510	BITUMINOUS TACK COAT	GALLON	200.0
AS620520	PAVEMENT MARKING - WATERBORNE	SQ FT	75.0
AS620525	PAVEMENT MARKING - BLACK BORDER	SQ FT	150.0

ADDITIVE ALTERNATE #2 SUMMARY OF QUANTITIES			
PAY ITEM	ITEM DESCRIPTION	UNIT	QTY
AT152410	UNCLASSIFIED EXCAVATION	CU YD	100.0
AT156520	INLET PROTECTION	EACH	1.0
AT701518	18" RCP, CLASS IV	FOOT	300.0
AT701900	REMOVE PIPE	FOOT	530.0
AT751411	INLET - TYPE A	EACH	1.0
AT901510	SEEDING	ACRE	2.0
AT908514	LIGHT-DUTY HYDRAULIC MULCH	ACRE	2.0

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NOTES

1. CONTRACTOR TO CONSTRUCT HAUL ROUTE AS SHOWN IN PLANS. CONSTRUCTION OF HAUL ROUTE WILL BE PAID FOR UNDER CONSTRUCTION ACCESS, AND REMOVAL WILL BE PAID FOR UNDER RESTORE CONSTRUCTION ACCESS.

LEGEND

-  PROPOSED PROJECT AREA
-  CONTRACTOR ACCESS
-  GATE GUARD (PROVIDED BY CONTRACTOR)
-  RSA RUNWAY SAFETY AREA (RSA)

CTAF FREQUENCY - 122.70

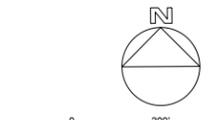
FAA POINT OF INTEREST TABLE

POINT	LATITUDE	LONGITUDE	EXIST. ELEV.	ELEVATION +25'	DESCRIPTION
111	N40° 12' 18.08"	W87° 35' 47.62"	694'	719'	ACCESS ROAD
112	N40° 12' 18.02"	W87° 35' 42.88"	691'	716'	ACCESS ROAD
113	N40° 12' 18.32"	W87° 35' 40.72"	687'	712'	ACCESS ROAD
114	N40° 12' 13.74"	W87° 35' 40.91"	689'	714'	ACCESS ROAD
115	N40° 12' 10.81"	W87° 35' 40.38"	685'	710'	ACCESS ROAD
116	N40° 12' 09.98"	W87° 35' 39.18"	686'	711'	ACCESS ROAD
117	N40° 12' 03.17"	W87° 35' 44.21"	677'	702'	ACCESS ROAD
118	N40° 12' 03.01"	W87° 35' 46.03"	676'	701'	ACCESS ROAD
119	N40° 12' 04.28"	W87° 35' 46.68"	679'	704'	ACCESS ROAD
120	N40° 12' 07.66"	W87° 35' 44.11"	681'	706'	ACCESS ROAD



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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

100% SUBMITTAL
 APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL
 AIRPORT AUTHORITY
 DANVILLE, IL

MARK | DATE | DESCRIPTION

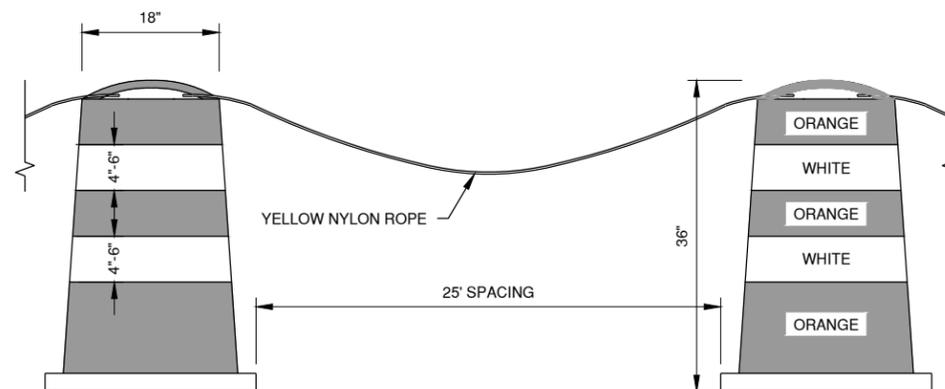
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 IL PROJ. NO. DNV-5169
 CMT PROJECT NO: 190042-02-30
 CAD DWG FILE: 19004202-PH3-GI101.DWG
 DESIGNED BY: HCH
 DRAWN BY: DPA
 CHECKED BY: MJD
 APPROVED BY: EMH
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**AIRPORT SITE PLAN &
 CONSTRUCTION
 ACCESS**

20. AIRPORT SECURITY REQUIEMENTS

(NOTES CONTINUED FROM SHEET GC001)

1. MAINTAINING THE SECURITY REQUIREMENTS OF THE AIRPORT SHALL BE A PRIMARY CONCERN FOR THE CONTRACTOR.
2. A LIST OF PERSONNEL AUTHORIZED TO WORK ON THE AIRFIELD SHALL BE PROVIDED TO THE RESIDENT ENGINEER BY THE CONTRACTOR. THE SUPERINTENDENT AND FOREMAN SHALL BE DIRECTLY RESPONSIBLE FOR THE IDENTITY AND LOCATION OF THOSE THEY ARE SUPERVISING WHILE ON THE AIRFIELD.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING AIRPORT SECURITY BY SUPERVISING OPENINGS OR MAINTAINING THE AIRPORT PERIMETER FENCE LINE AT ALL TIMES.
4. THE CONTRACTOR SHALL PROVIDE A SIGN AT ALL ACCESS GATES STATING " AUTHORIZED PERSONNEL ONLY." ALL COSTS RELATING TO CONTRACTOR'S ACCESS, GATE GUARDS, FLAGMEN, AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
5. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
6. THE AIRPORT WILL BE HOSTING EVENTS THROUGHOUT THE YEAR AND RESERVES THE RIGHT TO SUSPEND CONSTRUCTION ACTIVITY. REFER TO THE CONSTRUCTION ACTIVITY PLANS FOR NOTES REGARDING THIS PLANNED SUSPENSION.



1 IDOT DRUM BARRICADES
N.T.S.

DRUM BARRICADE NOTES	
1.	NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
2.	PLACE AND MAINTAIN UNTIL REMOVAL ACCORDING TO CSPP.
3.	BARRELS SHALL BE WEIGHTED SUFFICIENTLY TO PREVENT DISPLACEMENT.



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VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION

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CMT PROJECT NO: 190042-02-30

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CONSTRUCTION
ACTIVITY NOTES &
DETAILS 2

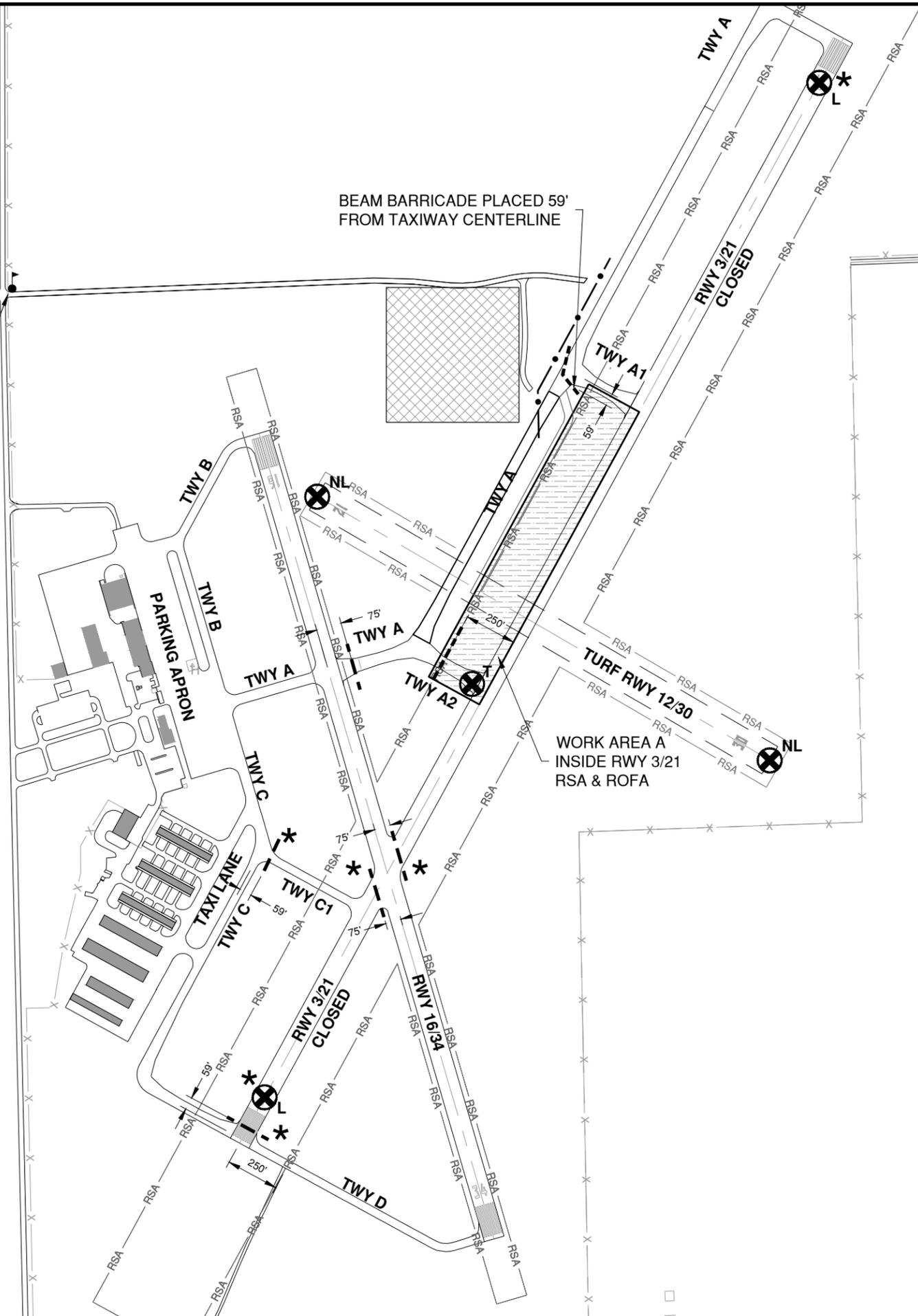
GC002
SHEET 6 OF 63

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NEWELL ROAD
GATE FOR
CONTRACTOR
ACCESS

BEAM BARRICADE PLACED 59'
FROM TAXIWAY CENTERLINE

WORK AREA A
INSIDE RWY 3/21
RSA & ROFA



WORK AREA A NOTES

1. WORK AREA A SHALL BE PERFORMED BETWEEN 8:00AM AND 8:00PM. RUNWAY 3/21 SHALL BE OPEN TO AIR TRAFFIC FROM 8:00PM TO 8:00AM DAILY, WITH APPROPRIATE NOTAMS COORDINATED WITH THE AIRPORT. THE CONTRACTOR SHALL PLACE AND REMOVE BARRICADES AND CLOSURE MARKERS AS SHOWN DAILY.
2. WORK TO BE COMPLETED WITHIN THIS WORK AREA SHALL, AT A MINIMUM, INCLUDE THE FOLLOWING MAJOR WORK ITEMS:
 - A. INSTALL JUMPER CABLE
 - B. REMOVE AND DISPOSE OF EXISTING LIGHT BASES.
 - C. DEMOLISH EXISTING PAVEMENT
 - D. GRADING AND DRAINAGE IMPROVEMENTS
 - E. TURFING
3. TAXIWAY A CIRCUIT SHALL BE ACTIVE THROUGHOUT PROJECT. SEE SHEET CE503 FOR COVERING OF LIGHTS AND SIGNS PRIOR TO PAVEMENT CLOSURES.

LEGEND

- BEAM BARRICADES
- IDOT DRUM BARRICADES
- ⊗^T TAXIWAY CLOSURE MARKER (TEMPORARY)
- ⊗^{NL} GATE GUARD/FLAGMAN
- ⊗^{NL} RUNWAY CLOSURE MARKER (NON-LIGHTED)
- * CLOSURE MARKER/BARRICADE TO BE REMOVED DAILY (NOTE 1)
- [Hatched Box] WORK AREA A
- [Cross-hatched Box] STAGING & STORAGE

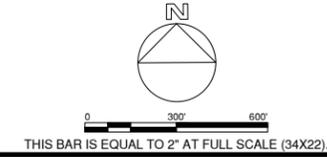
PAVEMENT STATUS

WORK AREA A		
PAVEMENT	STATUS	
* RUNWAY 3/21	OPEN BETWEEN 8:00PM & 8:00AM. CLOSED BETWEEN 8:00AM & 8:00PM.	
RUNWAY 16/34	OPEN	
RUNWAY 12/30	CLOSED	
TAXIWAY A	CLOSED BETWEEN RWY 16/34 AND TWY A1	
* TAXIWAY A1	OPEN BETWEEN 8:00PM & 8:00AM. CLOSED BETWEEN 8:00AM & 8:00PM.	
TAXIWAY A2	CLOSED	
TAXIWAY B	OPEN	
* TAXIWAY C1	OPEN BETWEEN 8:00PM & 8:00AM. CLOSED BETWEEN 8:00AM & 8:00PM.	
TAXIWAY D	OPEN	



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APRIL 25, 2025

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OWNER



VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION

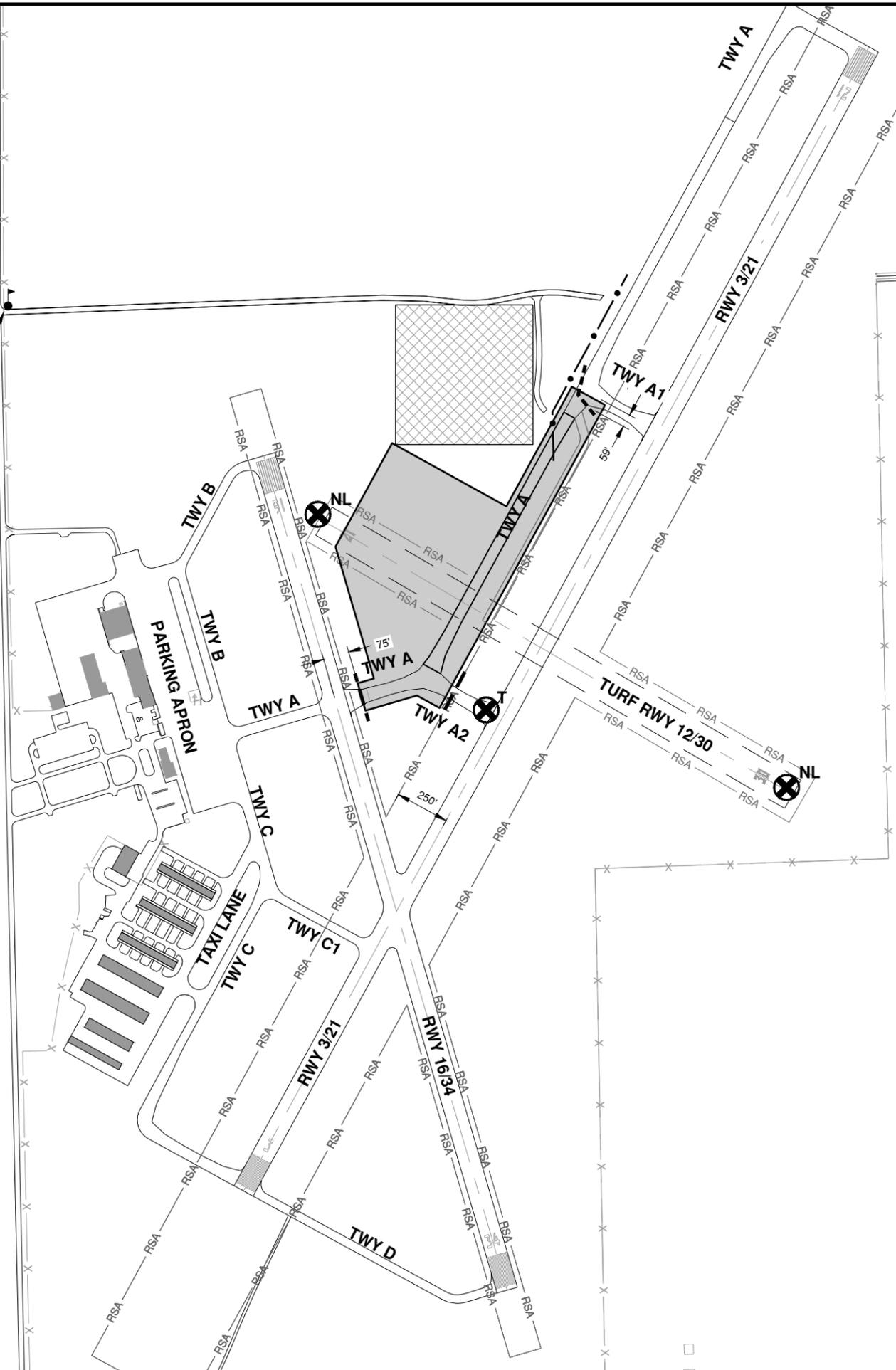
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**CONSTRUCTION
ACTIVITY PLAN
WORK AREA A**

GC102
SHEET 9 OF 63

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NEWELL ROAD
 GATE FOR
 CONTRACTOR
 ACCESS



WORK AREA B NOTES

1. WORK TO BE COMPLETED WITHIN WORK AREA B SHALL, AS A MINIMUM, INCLUDE THE FOLLOWING MAJOR WORK ITEMS:
 - A. REMOVE AND DISPOSE OF EXISTING LIGHT BASES
 - B. REMOVE EXISTING PAVEMENT
 - C. GRADING AND DRAINAGE IMPROVEMENTS
 - D. CONSTRUCT NEW PAVEMENT
 - E. CONSTRUCT NEW LIGHT BASES AND INSTALL NEW LIGHTING CIRCUITS
 - H. CONSTRUCT NEW SIGN BASES AND SIGNS
 - I. PAVEMENT MARKING
 - J. GRADING & TURFING
2. IN LIEU OF BEAM BARRICADES, CONTRACTOR MAY USE LIGHTED CONES WHERE NEEDED TO DESIGNATE A RUNWAY CLOSURE. SEE DETAIL 5, GC501
3. TAXIWAY A CIRCUIT SHALL BE ACTIVE THROUGHOUT PROJECT. SEE SHEET CE503 FOR COVERING OF LIGHTS AND SIGNS PRIOR TO PAVEMENT CLOSURES.

LEGEND

- BEAM BARRICADES
- IDOT DRUM BARRICADES
- ⊗ T TAXIWAY CLOSURE MARKER (TEMPORARY)
- ⊗ NL GATE GUARD/FLAGMAN
- ⊗ NL RUNWAY CLOSURE MARKER (NON-LIGHTED)
- * CLOSURE MARKER/BARRICADE TO BE REMOVED DAILY (NOTE 1)
- WORK AREA B
- ▨ STAGING & STORAGE

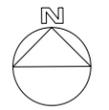
PAVEMENT STATUS

WORK AREA B	
PAVEMENT	STATUS
RUNWAY 3/21	OPEN
RUNWAY 16/34	OPEN
RUNWAY 12/30	CLOSED
TAXIWAY A	CLOSED BETWEEN RWY 16/34 & TWY A1
TAXIWAY A1	OPEN
TAXIWAY A2	CLOSED
TAXIWAY B	OPEN
TAXIWAY C1	OPEN
TAXIWAY D	OPEN



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OWNER



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 DANVILLE, IL

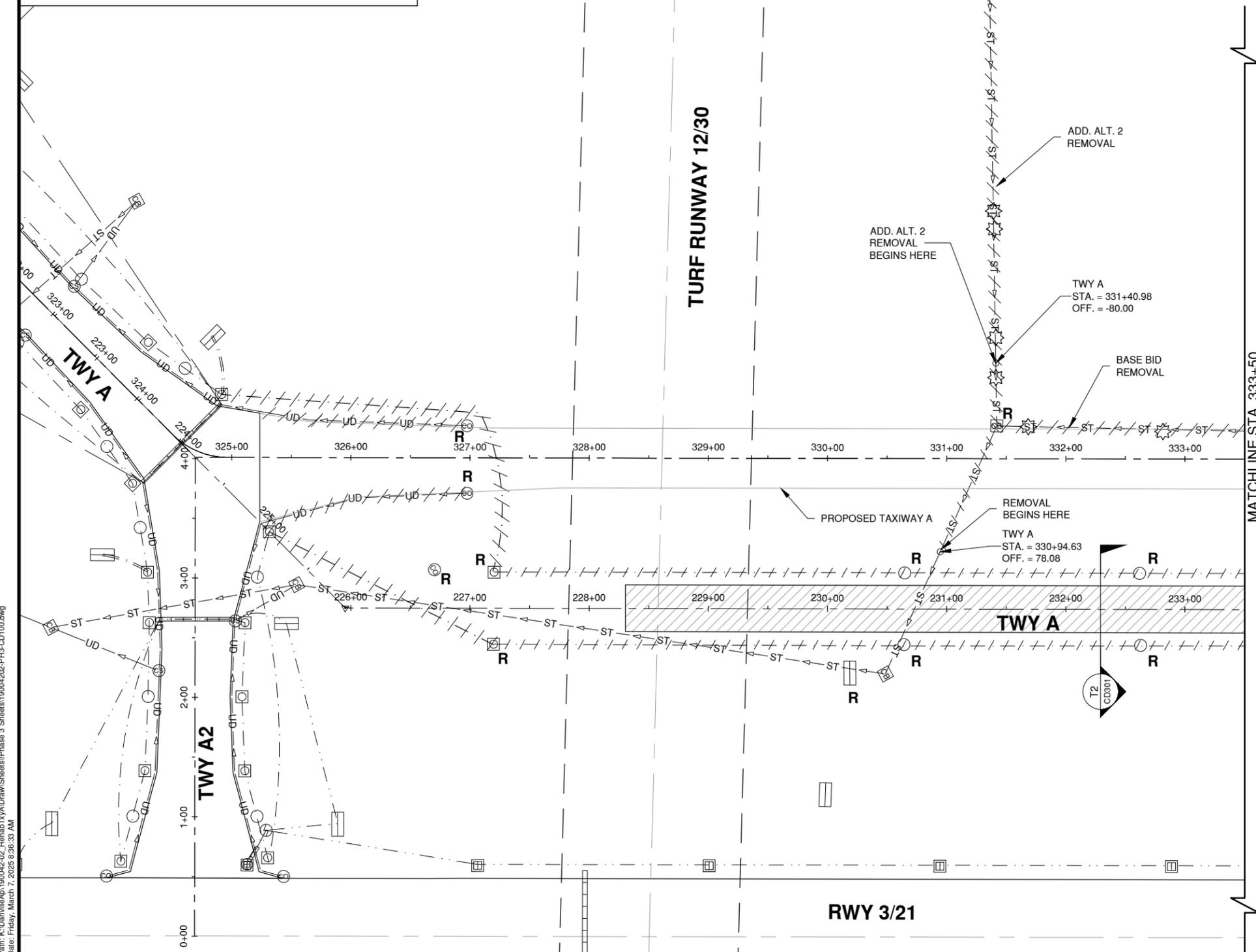
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CAD DWG FILE: 19004202-PH3-GC100.DWG		
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**CONSTRUCTION
 ACTIVITY PLAN
 WORK AREA B**

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY AND FAA OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.



NOTES

- EXISTING PAVEMENT STRUCTURES SHOWN ARE FROM AVAILABLE RECORD DRAWINGS AND ARE CONSIDERED GENERALLY REPRESENTATIVE OF THE AS-CONSTRUCTED PAVEMENT SECTION. SOME VARIABILITY IN ACTUAL PAVEMENT THICKNESS IS TO BE EXPECTED.
- CONTRACTOR SHALL PROTECT PAVEMENTS THAT ARE ADJACENT TO THE REMOVAL LIMITS. CONTRACTOR SHALL REPAIR ANY DAMAGE TO PAVEMENT TO REMAIN AT HIS EXPENSE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY JUMPER CABLES AS NEEDED TO KEEP ALL CIRCUITS ACTIVE ON PAVEMENTS OPEN TO AIR TRAFFIC BETWEEN DEMOLITION AND CONSTRUCTION OF NEW PAVEMENTS. SEE SHEET CE103 FOR PROPOSED JUMPER CABLE PATH.
- THE CONTRACTOR SHALL INSTALL PIPING AS NEEDED TO ENSURE POSITIVE DRAINAGE OF THE SITE WHERE OBSTRUCTED BY THE TEMPORARY HAUL ROUTE. INCIDENTAL TO MOBILIZATION.

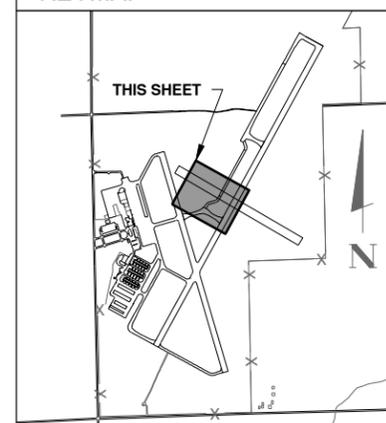
PAVEMENT LEGEND

REMOVE PAVEMENT: AR401921

LEGEND

- EXISTING STAKE MOUNTED TAXIWAY LIGHT
- EXISTING BASE MOUNTED TAXIWAY LIGHT
- EXISTING RUNWAY LIGHT
- EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING SPLICE CAN
- EXISTING WINDCONE
- EXISTING DUCT MARKER
- EXISTING CATCH BASIN
- EXISTING UNDERDRAIN CLEANOUT
- EXISTING COLLECTION STRUCTURE
- R** REMOVALS
- EXISTING DUCT BANK
- EXISTING TAXIWAY A CIRCUIT
- EXISTING CIRCUIT/CABLE TO BE REMOVED
- EXISTING RUNWAY 3/21 CIRCUIT
- EXISTING RUNWAY 16/34 CIRCUIT
- EXISTING STORM SEWER
- EXISTING STORM SEWER TO BE REMOVED
- EXISTING UNDERDRAIN
- EXISTING UNDERDRAIN TO BE REMOVED
- ELEVATED RETROREFLECTIVE MARKER
- EXISTING SINKHOLE LOCATION

KEYMAP



License No. 184-00613
CONSULTANTS



0 50' 100'
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

100% SUBMITTAL
APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD
IL. PROJ. NO. DNV-5169
CMT PROJECT NO: 190042-02-30
CAD DWG FILE: 19004202-PH3-CD100.DWG
DESIGNED BY: HCH
DRAWN BY: DPA
CHECKED BY: MJD
APPROVED BY: EMH
COPYRIGHT:

SHEET TITLE
DEMOLITION PLAN 1

CD101
SHEET 12 OF 63

UTILITY DISCLAIMER

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER, NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

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NOTES

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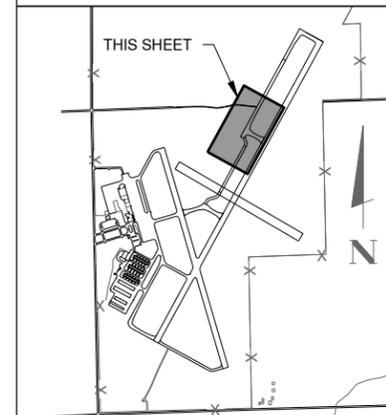
PAVEMENT LEGEND

 REMOVE PAVEMENT: AR401921

LEGEND

-  EXISTING STAKE MOUNTED TAXIWAY LIGHT
-  EXISTING BASE MOUNTED TAXIWAY LIGHT
-  EXISTING RUNWAY LIGHT
-  EXISTING TAXIWAY GUIDANCE SIGN
-  EXISTING SPLICE CAN
-  EXISTING WINDCONE
-  EXISTING DUCT MARKER
-  EXISTING CATCH BASIN
-  EXISTING UNDERDRAIN CLEANOUT
-  EXISTING COLLECTION STRUCTURE
- R** REMOVALS
-  EXISTING DUCT BANK
-  EXISTING TAXIWAY A CIRCUIT
-  EXISTING CIRCUIT/CABLE TO BE REMOVED
-  EXISTING RUNWAY 3/21 CIRCUIT
-  EXISTING RUNWAY 16/34 CIRCUIT
-  JUMPER CABLE
-  EXISTING STORM SEWER
-  EXISTING STORM SEWER TO BE REMOVED
-  EXISTING UNDERDRAIN
-  EXISTING UNDERDRAIN TO BE REMOVED
-  ELEVATED RETROREFLECTIVE MARKER
-  EXISTING SINKHOLE

KEYMAP



License No. 184-00613

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APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

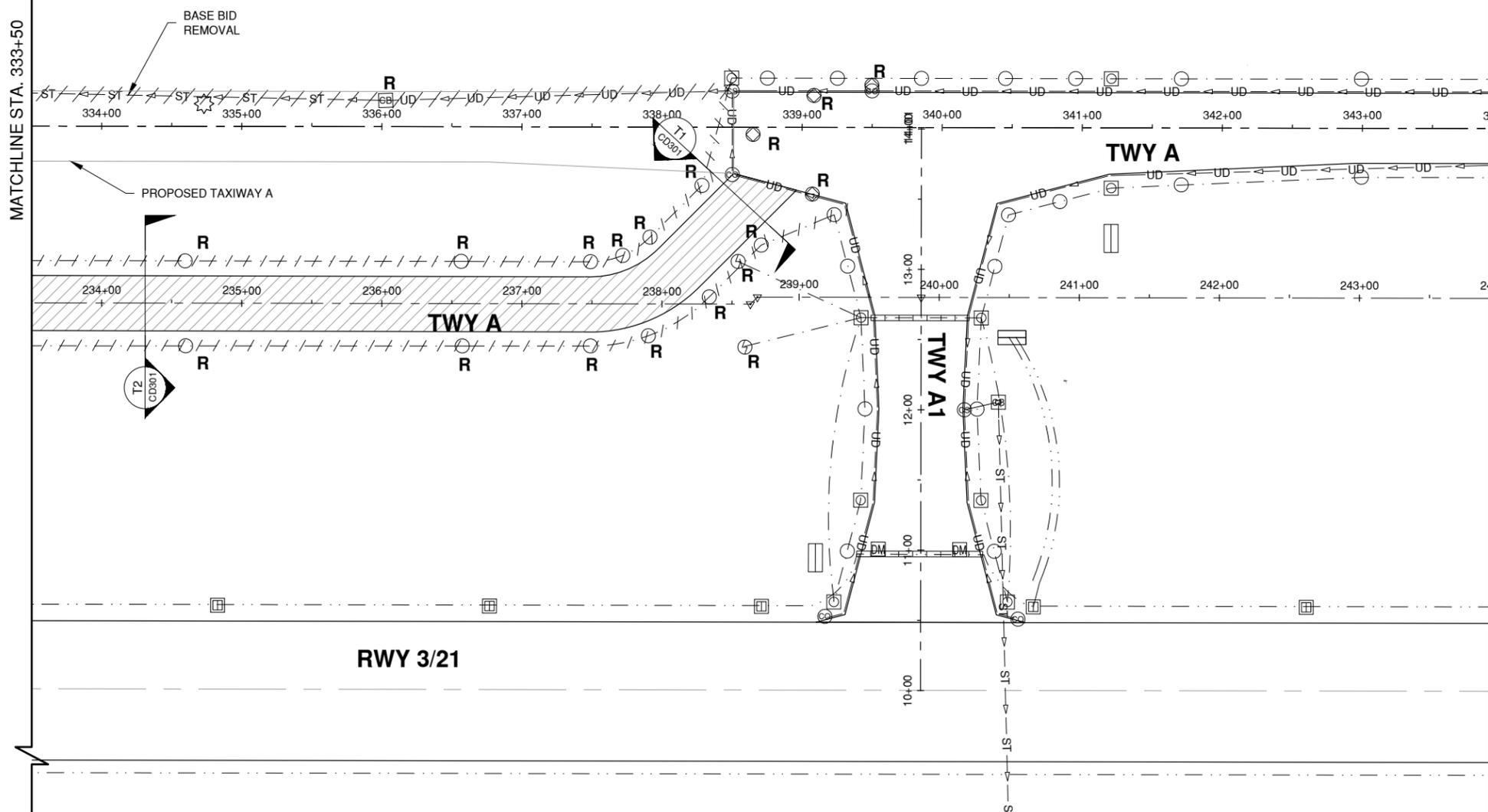
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IL PROJ. NO: DNV-5169
CMT PROJECT NO: 190042-02-30
CAD DWG FILE: 19004202-PH3-CD100.DWG
DESIGNED BY: HCH
DRAWN BY: DPA
CHECKED BY: MJD
APPROVED BY: EMH
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SHEET TITLE
DEMOLITION PLAN 2

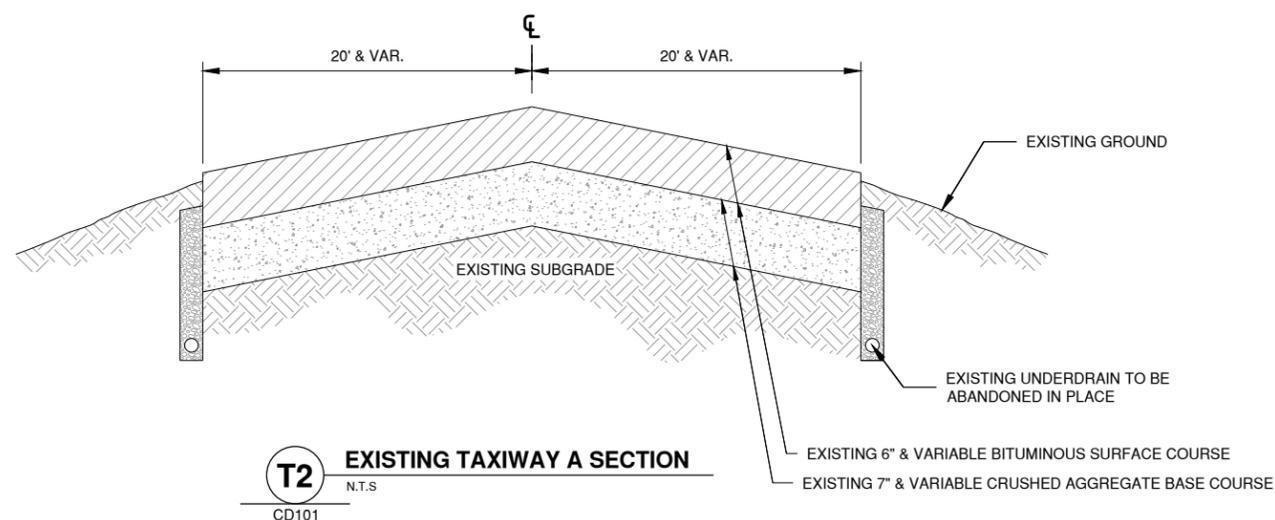
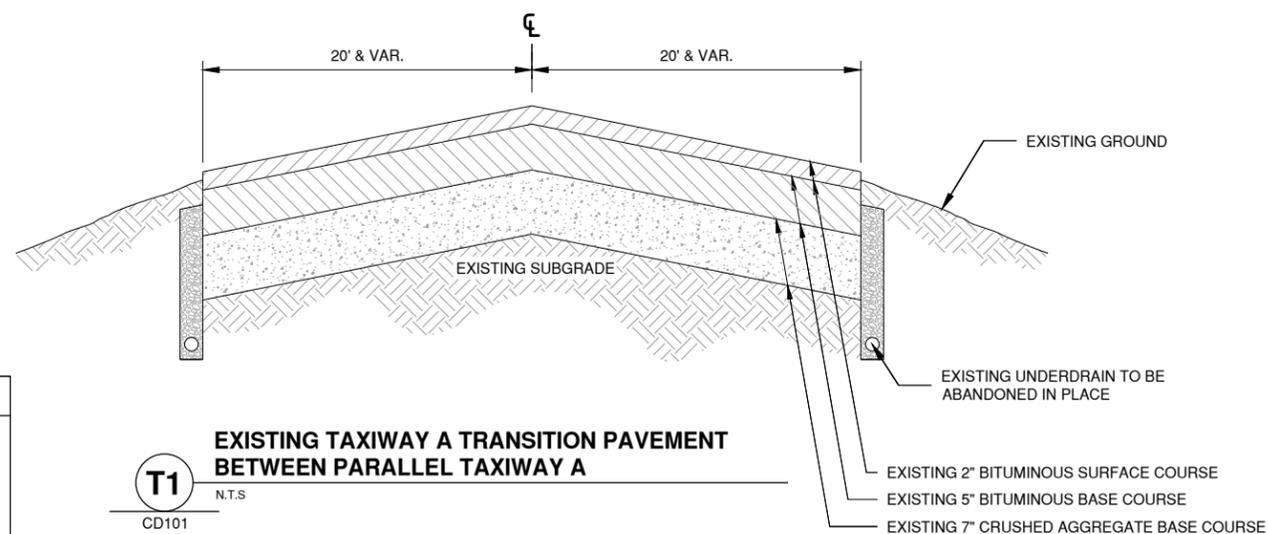
CD102
SHEET 13 OF 63

MATCHLINE STA. 333+50



NOTES

1. EXISTING PAVEMENT STRUCTURES SHOWN ARE FROM AVAILABLE RECORD DRAWINGS AND ARE CONSIDERED GENERALLY REPRESENTATIVE OF THE AS-CONSTRUCTED PAVEMENT SECTION. SOME VARIABILITY IN ACTUAL PAVEMENT THICKNESS IS TO BE EXPECTED.



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APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD

IL PROJ. NO: DNV-5169

CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-CD301.DWG

DESIGNED BY: HCH

DRAWN BY: DPA

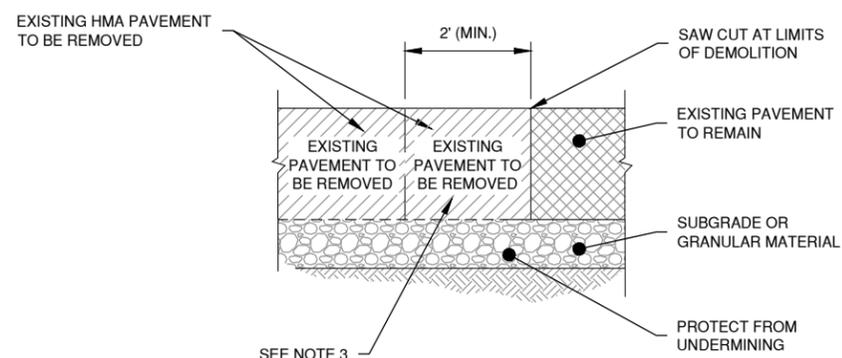
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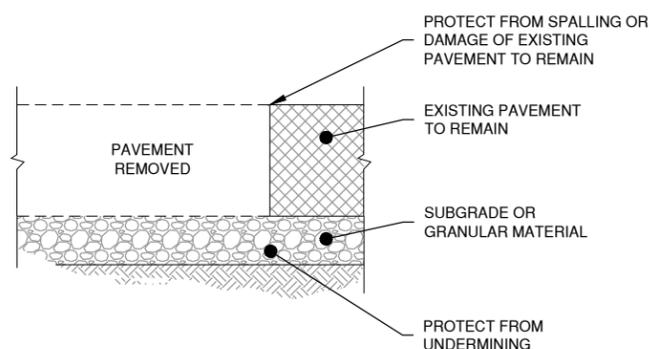
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SHEET TITLE

**EXISTING PAVEMENT
SECTIONS**



STEP 1

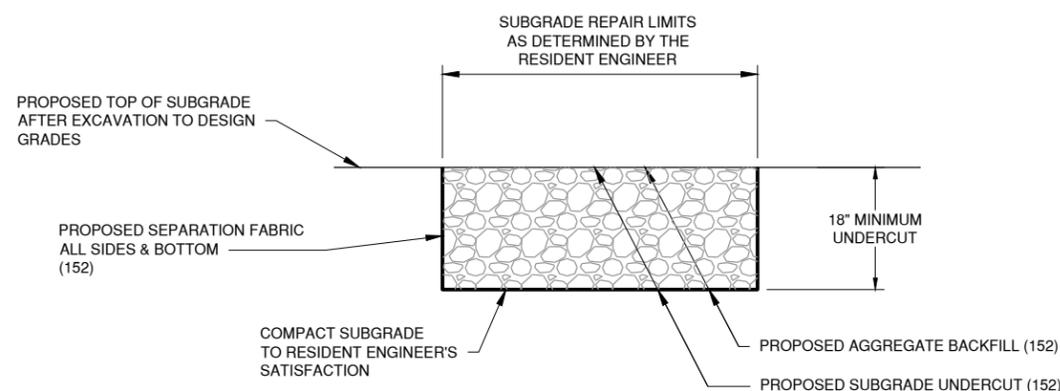


STEP 2

1
N.T.S
PROPOSED SAWCUT ADJACENT TO PAVEMENT TO REMAIN

SAW CUT AND OTHER DEMOLITION NOTES

1. PROTECT PAVEMENT TO REMAIN IN PLACE FROM UNDERMINING.
2. SAW CUTS SHALL BE FULL DEPTH ON PAVEMENT PRIOR TO REMOVAL OF PAVEMENT SECTION.
3. DO NOT REMOVE 2' OF HMA PAVEMENT AGAINST EXISTING HMA PAVEMENT TO REMAIN UNTIL AREA IS READY TO RECEIVE MODIFIED SUBGRADE OR AGGREGATE BASE COURSE. WORK ON EXPOSED SUBGRADE PRIOR TO RECEIVING MODIFIED SUBGRADE OR AGGREGATE BASE COURSE SHALL BE COMPLETED WITHIN 48 HOURS AFTER THE REMOVAL OF HMA PAVEMENT



2
N.T.S
PROPOSED SUBGRADE UNDERCUT & AGGREGATE BACKFILL

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APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



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AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD
IL PROJ. NO: DNV-5169
CMT PROJECT NO: 190042-02-30
CAD DWG FILE: 19004202-PH3-CD501.DWG
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APPROVED BY: EMH
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SHEET TITLE
**PAVEMENT
DEMOLITION AND
SUBGRADE DETAIL**

CD501
SHEET 15 OF 63

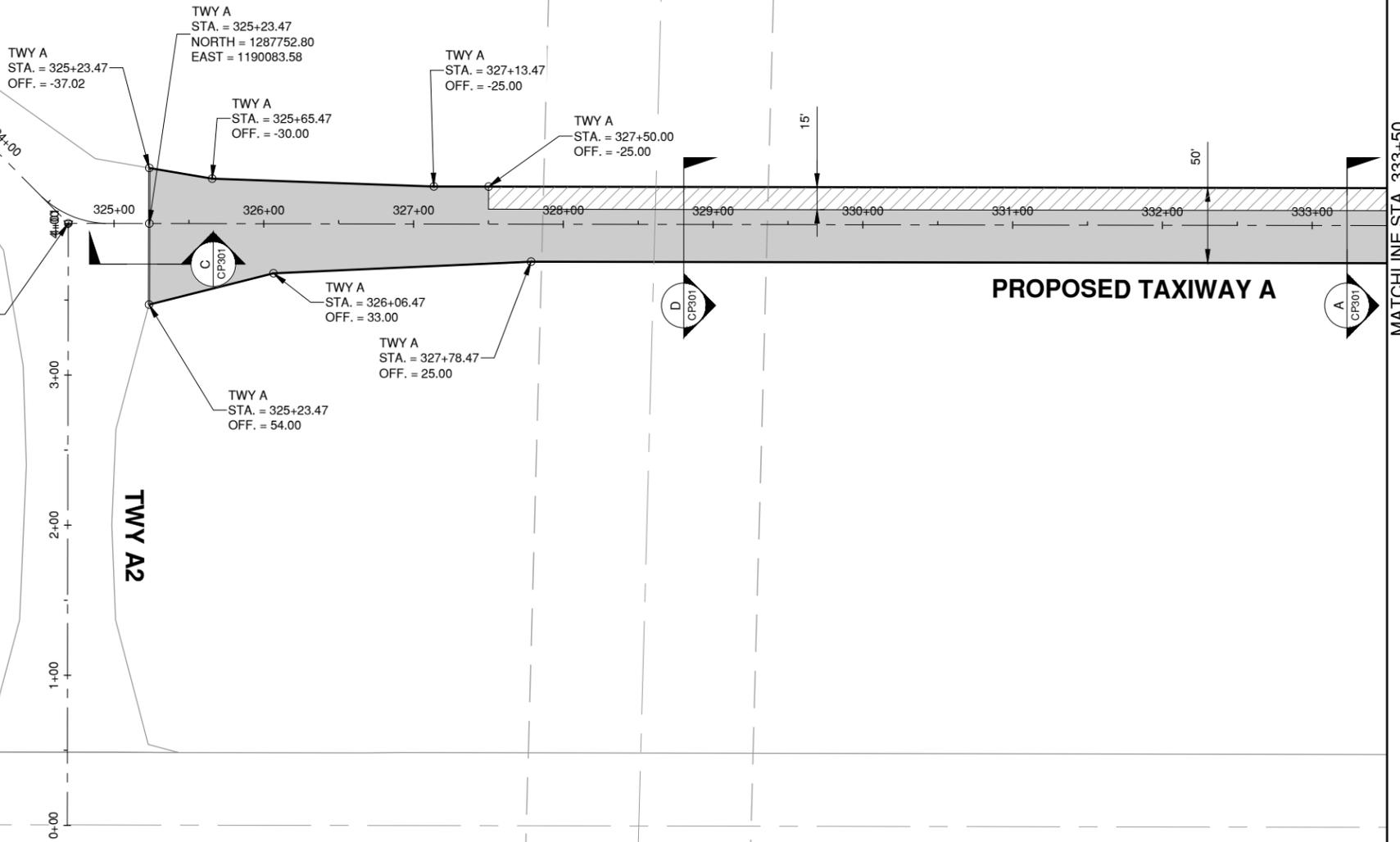
RWY 16/34

TURF RUNWAY 12/30

RUNWAY 3/21

MATCHLINE STA. 333+50

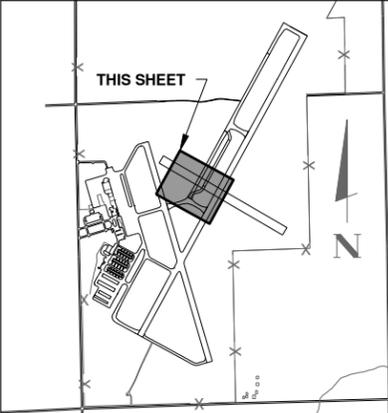
PROPOSED TAXIWAY A



LEGEND

-  TYPICAL SECTION
-  PROPOSED PAVEMENT - FULL DEPTH (BASE BID)
-  PROPOSED PAVEMENT - FULL DEPTH (ADD. ALT. 1)

KEYMAP



License No. 184-00613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

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APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION

FED. PROJ. NO.	3-17-SBGP-TBD
IL PROJ. NO.	DNV-5169
CMT PROJECT NO.	190042-02-30
CAD DWG FILE:	19004202-PH3-CP100.DWG
DESIGNED BY:	HCH
DRAWN BY:	DPA
CHECKED BY:	MJD
APPROVED BY:	EMH
COPYRIGHT:	

SHEET TITLE
**PROPOSED
GEOMETRY PLAN 1**

CP101
SHEET 16 OF 63

Path: K:\Danville\A\190042-02_RehabTaxiway\Draw\Sheets\Phase 3 Sheets\19004202-PH3-CP100.dwg
Date: Thursday, March 6, 2025 3:13:36 PM



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APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



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AIRPORT AUTHORITY
DANVILLE, IL

LEGEND



TYPICAL SECTION

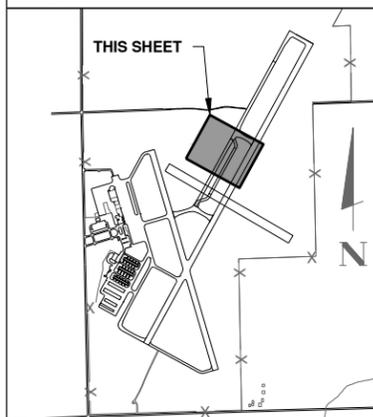


PROPOSED PAVEMENT - FULL DEPTH
(BASE BID)



PROPOSED PAVEMENT - FULL DEPTH
(ADD. ALT. 1)

KEYMAP

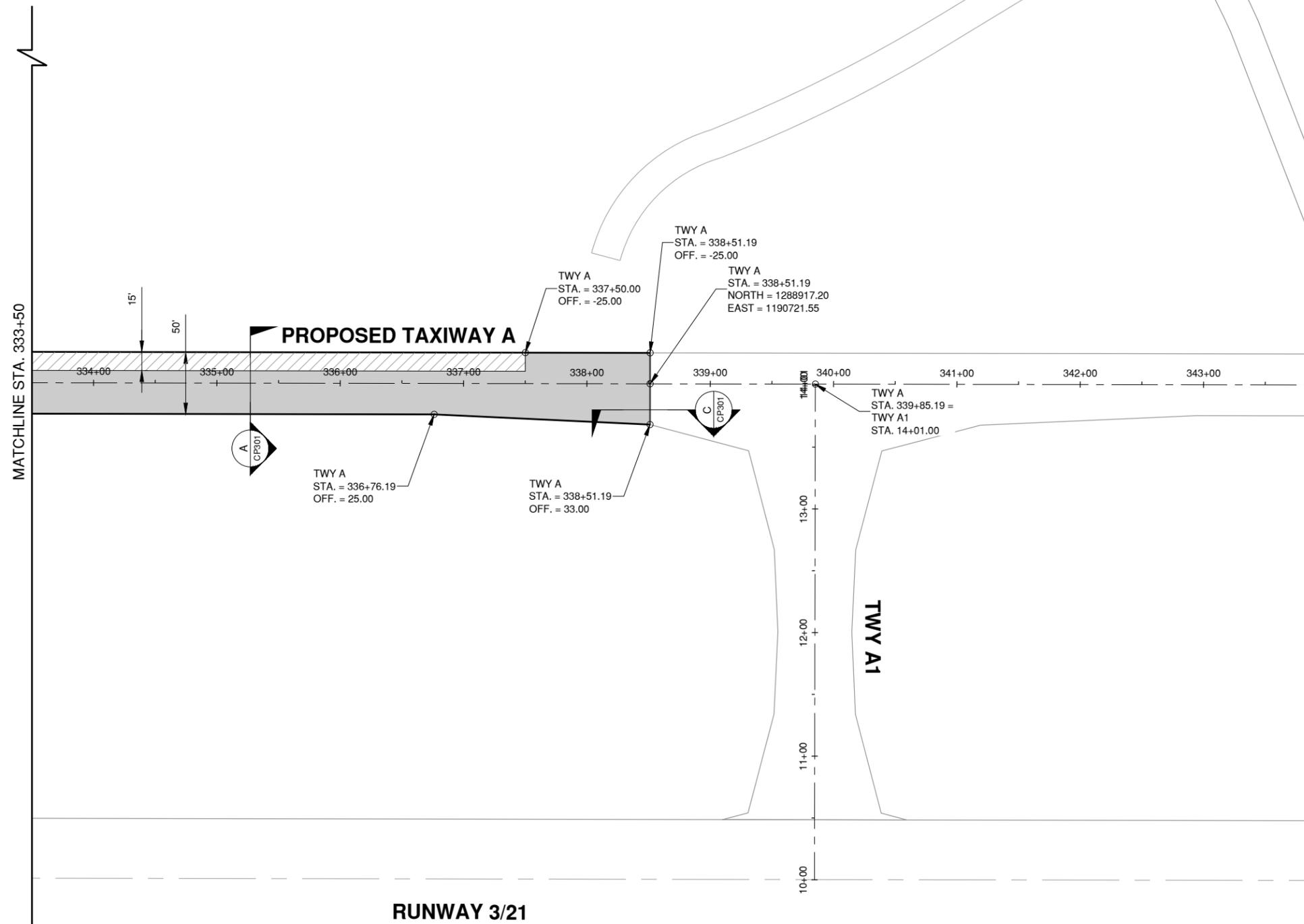


MARK	DATE	DESCRIPTION

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IL PROJ. NO. DNV-5169
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DESIGNED BY: HCH
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APPROVED BY: EMH
COPYRIGHT:

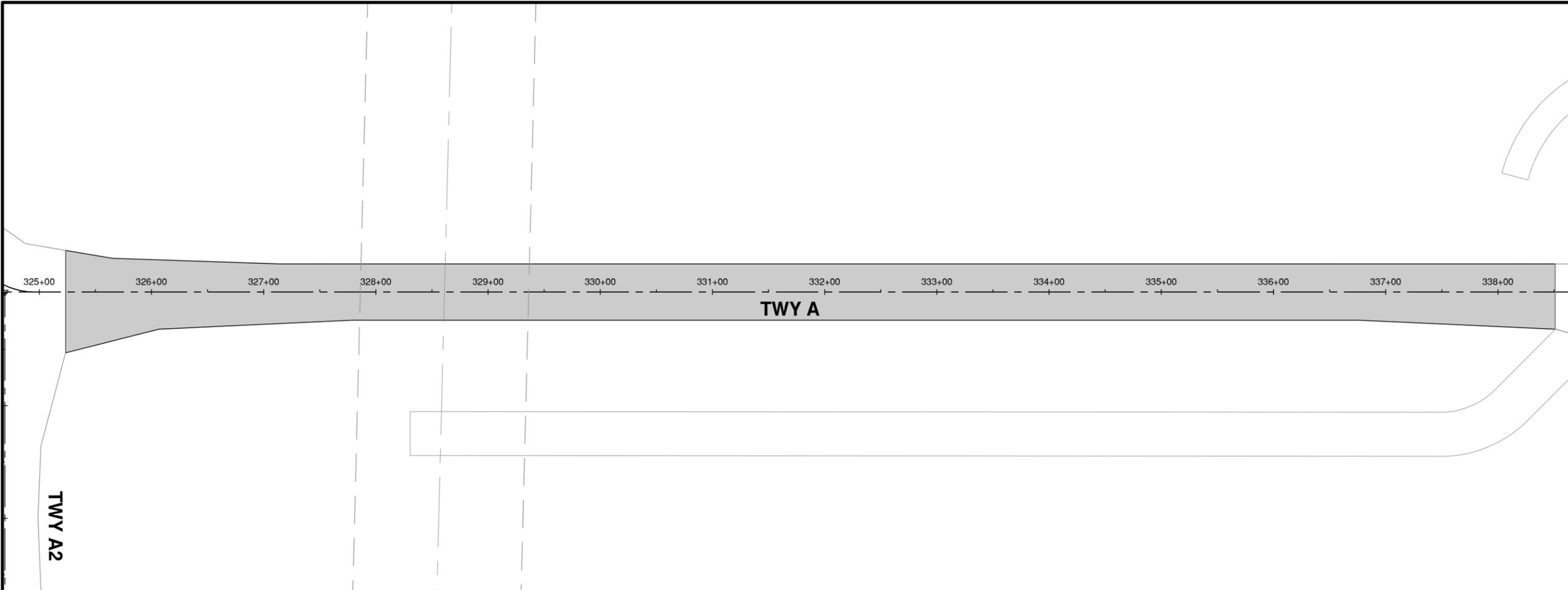
**PROPOSED
GEOMETRY PLAN 2**

CP102
SHEET 17 OF 63





THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



100% SUBMITTAL
APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

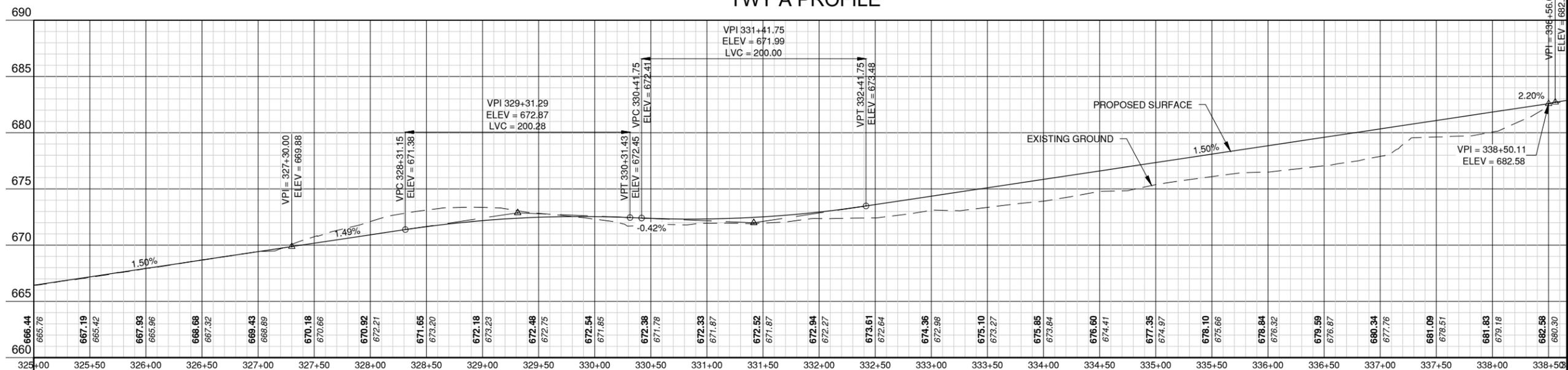
OWNER



VERMILION REGIONAL
AIRPORT

VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

TWY A PROFILE



MARK	DATE	DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD
 IL. PROJ. NO: DNV-5169
 CMT PROJECT NO: 190042-02-30
 CAD DWG FILE: 19004202-PH3-CP201.DWG
 DESIGNED BY: HCH
 DRAWN BY: DPA
 CHECKED BY: MJD
 APPROVED BY: EMH
 COPYRIGHT:

SHEET TITLE
PLAN & PROFILE TWY A



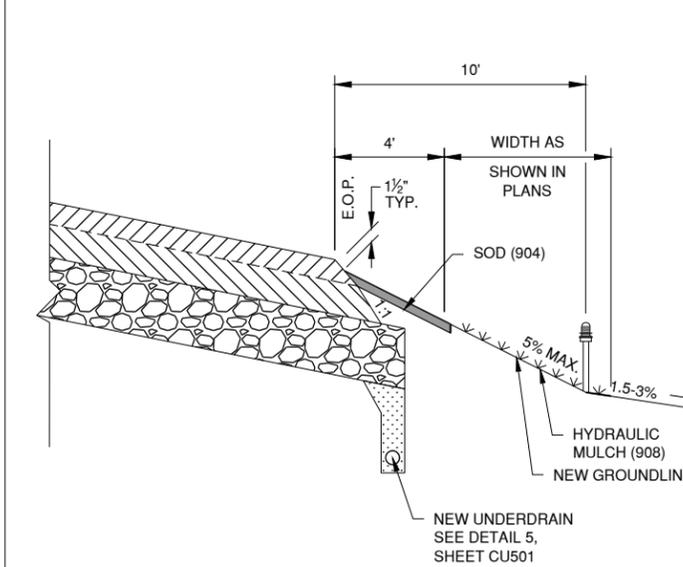
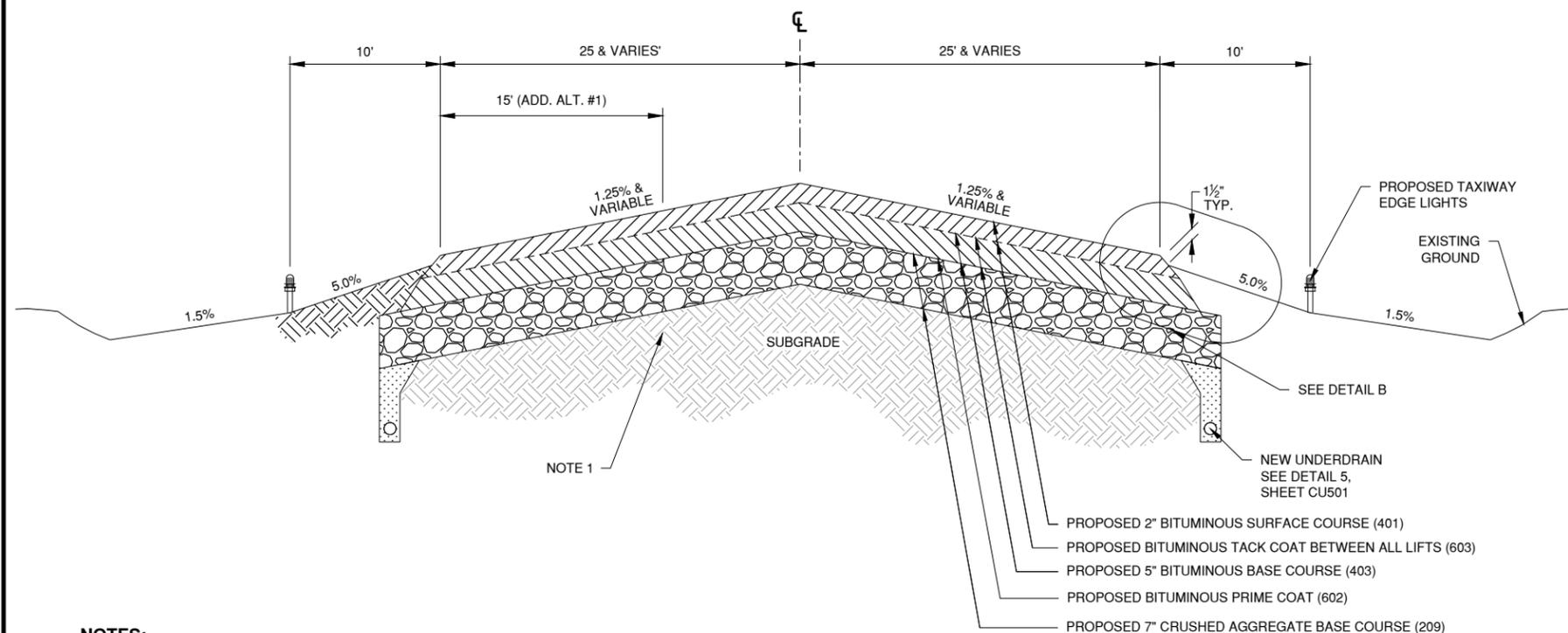
VERMILION REGIONAL
AIRPORT

VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION

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IL PROJ. NO: DNV-5169
CMT PROJECT NO: 190042-02-30
CAD DWG FILE: 19004202-PH3-CP301.DWG
DESIGNED BY: HCH
DRAWN BY: DPA
CHECKED BY: MJD
APPROVED BY: EMH
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SHEET TITLE
**NEW PAVEMENT
SECTIONS**

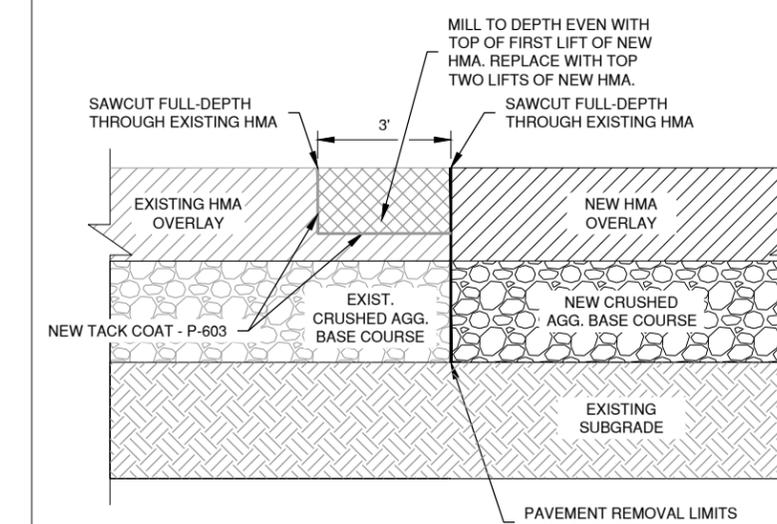
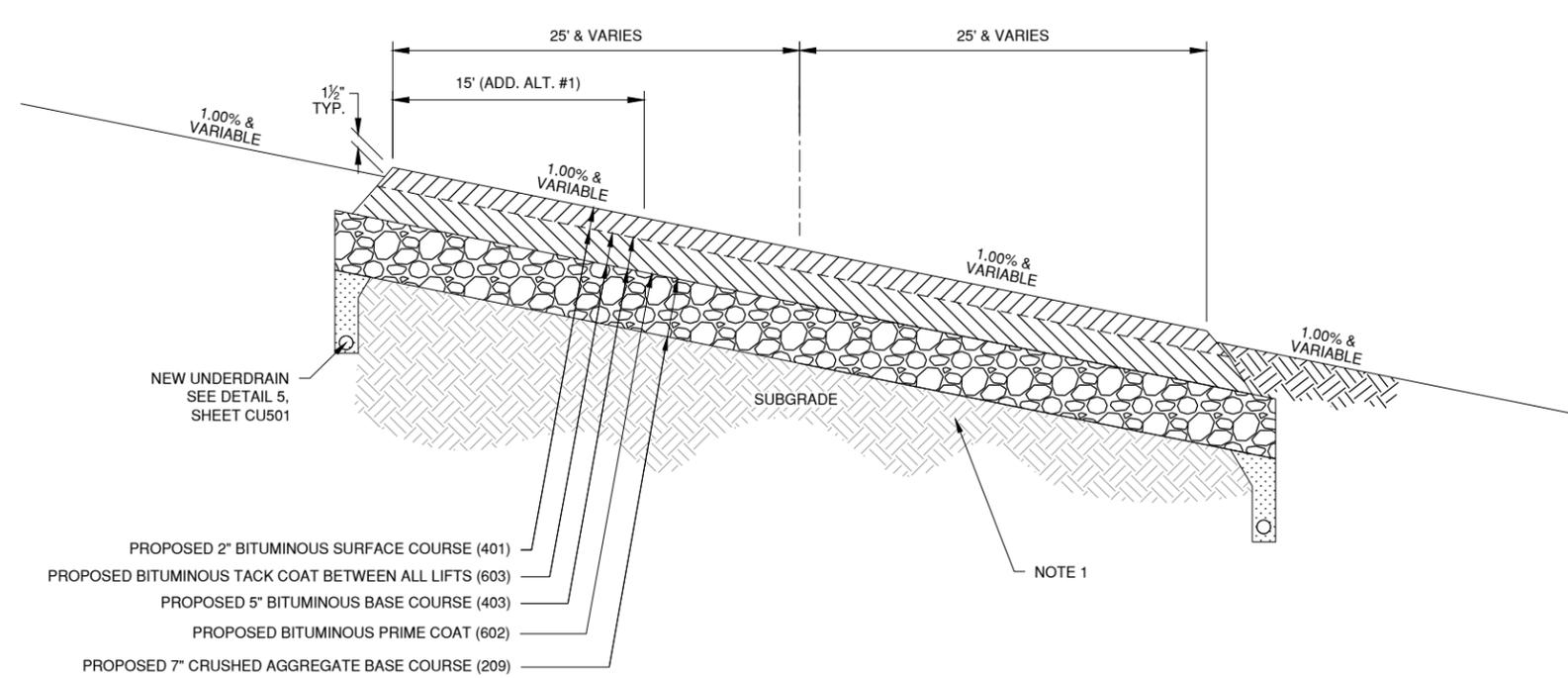


NOTES:

- SEE SUBGRADE UNDERCUT AND AGGREGATE BACKFILL DETAIL SHEET CD501 FOR SUBGRADE REPAIR.

A NEW TAXIWAY SECTION - FULL WIDTH
N.T.S.
CP101

B NEW SHOULDER DETAIL B
N.T.S.
CP101



NOTES:

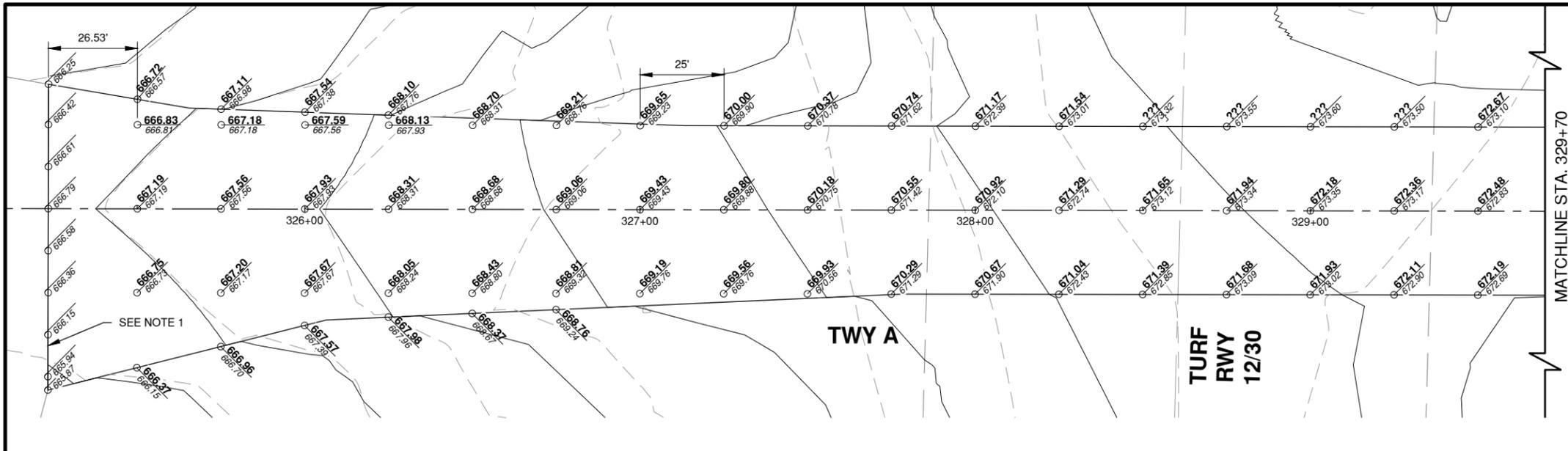
- SEE SUBGRADE UNDERCUT AND AGGREGATE BACKFILL DETAIL SHEET CD501 FOR SUBGRADE REPAIR.

D NEW TAXIWAY SECTION - AT TURF RUNWAY INTERSECTION
N.T.S.
CP101

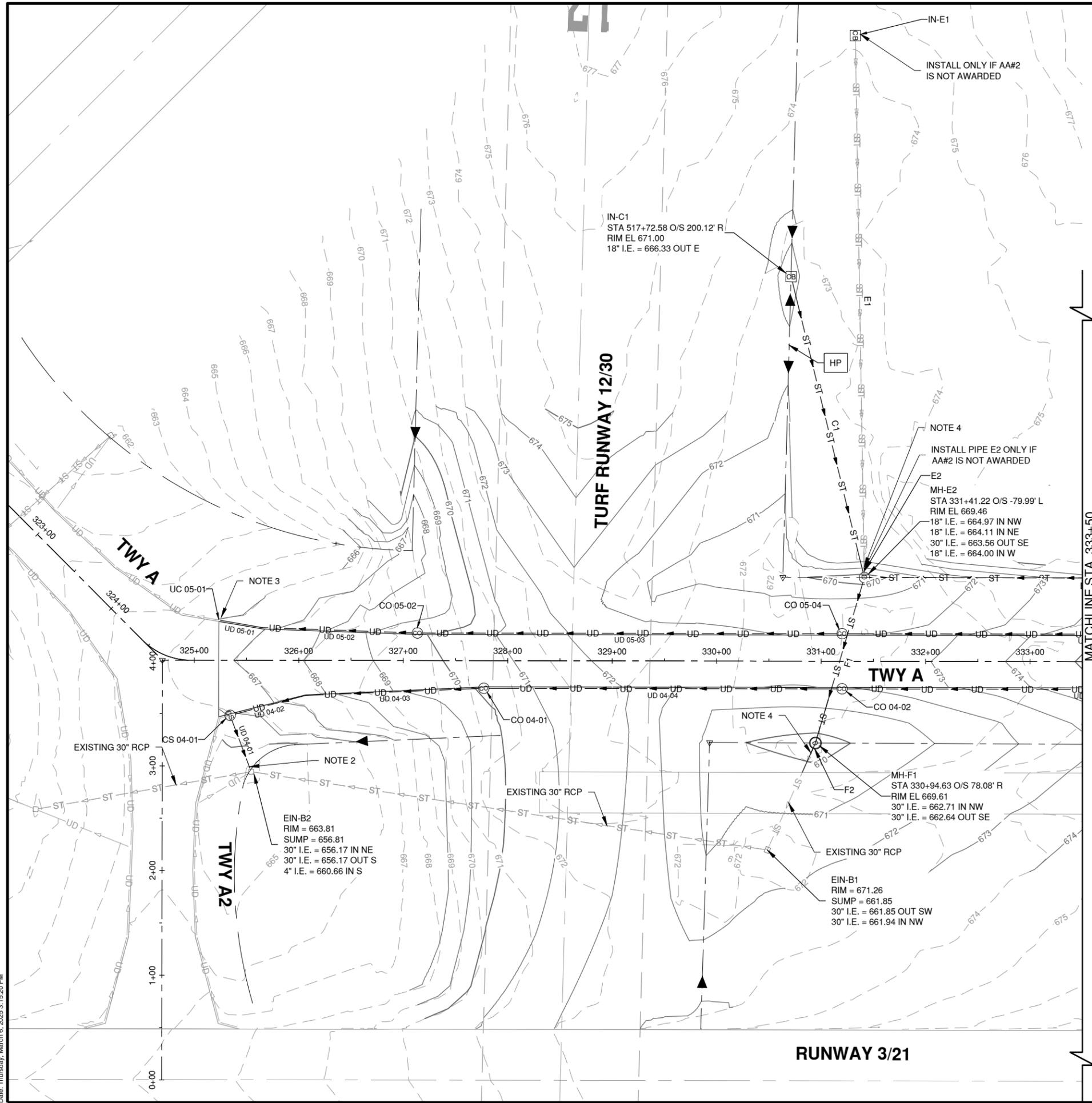
C HMA ABUTT REMOVAL AT EXISTING FULL DEPTH HMA & REPLACEMENT DETAIL
N.T.S.
CP101

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Date: Thursday, March 6, 2025 3:14:23 PM

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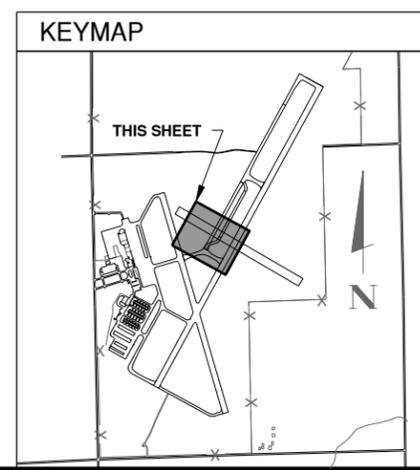


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Date: Thursday, March 6, 2025 3:15:20 PM



- ### NOTES
- CONTRACTOR SHALL FIELD VERIFY DEPTHS OF ALL EXISTING DRAINAGE PIPES AND STRUCTURES AT POINTS OF CONNECTION PRIOR TO PRE-CASTING STRUCTURES OR INSTALLING PIPE.
 - UNDERDRAIN DIRECT CONNECT TO EXISTING STRUCTURE.
 - UNDERDRAIN CONNECTION TO EXISTING UNDERDRAIN PIPE.
 - CONNECT NEW AND EXISTING STORM SEWER PIPE WITH P-610 CONCRETE COLLAR.
 - PROPOSED GRADING SHALL NOT EXCEED MAXIMUM SLOPE OF 6:1.

- ### LEGEND
- NEW PAVEMENT EDGE
 - PROPOSED CONTOUR
 - EXISTING CONTOUR
 - PROPOSED UNDERDRAIN LINE
 - PROPOSED STORM SEWER LINE
 - EXISTING UNDERDRAIN LINE
 - EXISTING STORM SEWER LINE
 - PROPOSED DITCH FLOWLINE
 - PROPOSED UNDERDRAIN CLEANOUT
 - PROPOSED STORM SEWER INLET
 - PROPOSED MANHOLE
 - PROPOSED COLLECTION STRUCTURE
 - HIGH POINT (DITCH GRADING)



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CONSULTANTS

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100% SUBMITTAL
APRIL 25, 2025

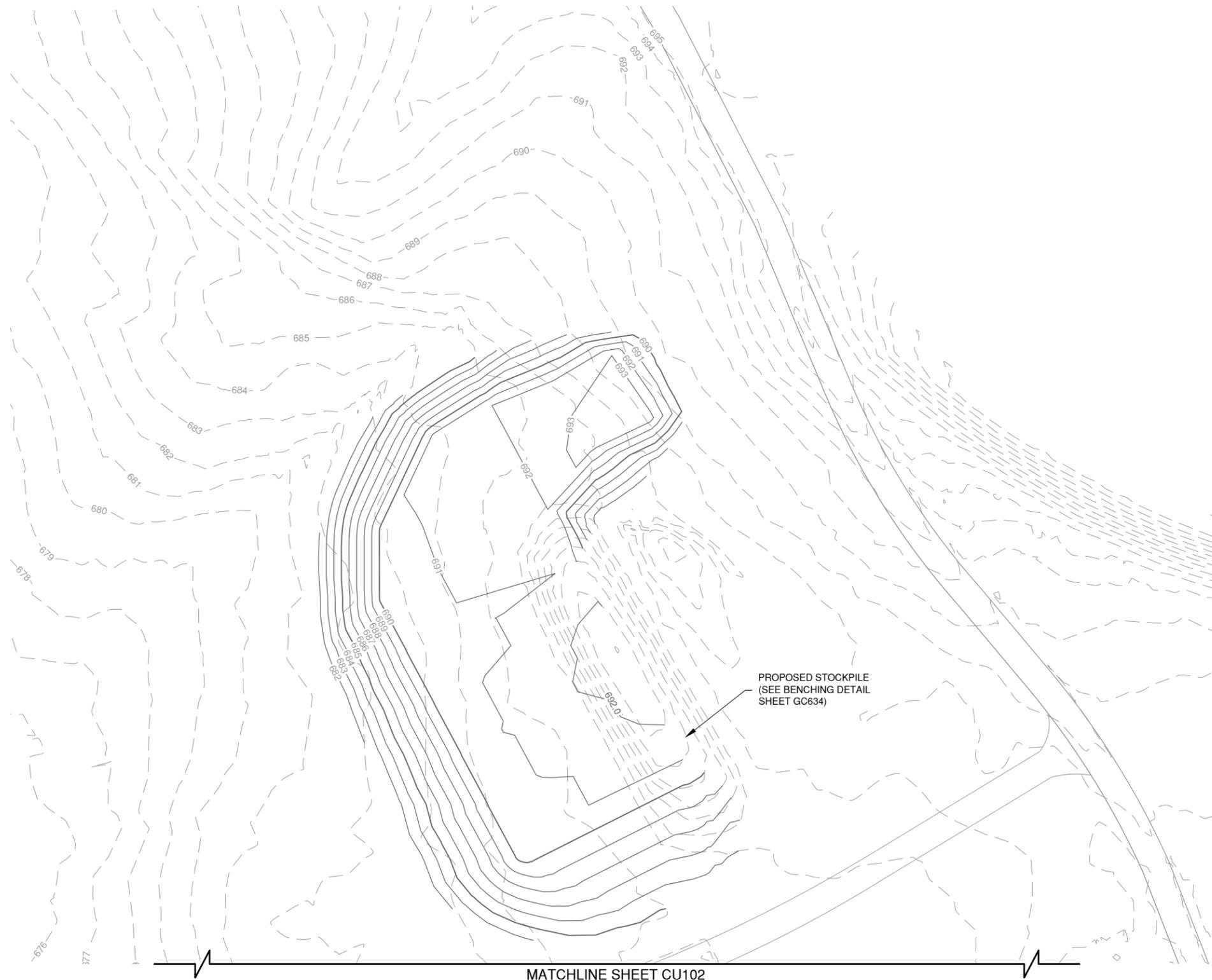
REALIGN TAXIWAY A, PHASE 3

OWNER

VERMILION REGIONAL AIRPORT
VERMILION REGIONAL AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION
		FED. PROJ. NO. 3-17-SBGP-TBD
		IL PROJ. NO: DNV-5169
		CMT PROJECT NO: 190042-02-30
		CAD DWG FILE: 19004202-PH3-CU100.DWG
		DESIGNED BY: HCH
		DRAWN BY: DPA
		CHECKED BY: MJD
		APPROVED BY: EMH
		COPYRIGHT:
SHEET TITLE		
GRADING & DRAINAGE PLAN 1		
CU101		
SHEET	22	OF 63

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MATCHLINE SHEET CU102

PROPOSED STOCKPILE
 (SEE BENCHING DETAIL
 SHEET GC634)

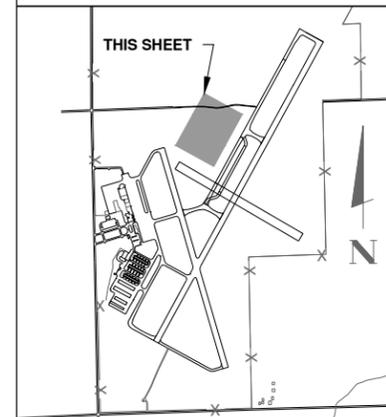
NOTES

1. CONTRACTOR SHALL FIELD VERIFY DEPTHS OF ALL EXISTING DRAINAGE PIPES AND STRUCTURES AT POINTS OF CONNECTION PRIOR TO PRE-CASTING STRUCTURES OR INSTALLING PIPE.
2. UNDERDRAIN DIRECT CONNECT TO EXISTING STRUCTURE.
3. UNDERDRAIN CONNECTION TO EXISTING UNDERDRAIN PIPE
4. PROPOSED GRADING SHALL NOT EXCEED MAXIMUM SLOPE OF 6:1.

LEGEND

- NEW PAVEMENT EDGE
- 589 — PROPOSED CONTOUR
- - - 589 - - - EXISTING CONTOUR
- > UD —— PROPOSED UNDERDRAIN LINE
- > ST —— PROPOSED STORM SEWER LINE
- - -> UD - - - EXISTING UNDERDRAIN LINE
- - -> ST - - - EXISTING STORM SEWER LINE
- ← - - - - -> PROPOSED DITCH FLOWLINE
- ⊙ PROPOSED UNDERDRAIN CLEANOUT
- ⊠ PROPOSED STORM SEWER INLET
- ⊙ PROPOSED MANHOLE
- ⊙ PROPOSED COLLECTION STRUCTURE
- ⊠ HP PROPOSED HIGH POINT (DITCH GRADING)

KEYMAP



License No. 184-00613

CONSULTANTS



0 50' 100'
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100% SUBMITTAL
 APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



VERMILION REGIONAL
 AIRPORT

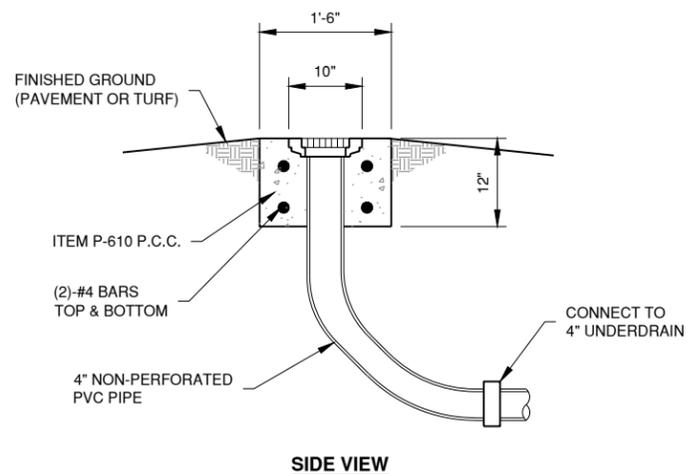
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 AIRPORT AUTHORITY
 DANVILLE, IL

MARK	DATE	DESCRIPTION

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IL PROJ. NO:	DNV-5169
CMT PROJECT NO:	190042-02-30
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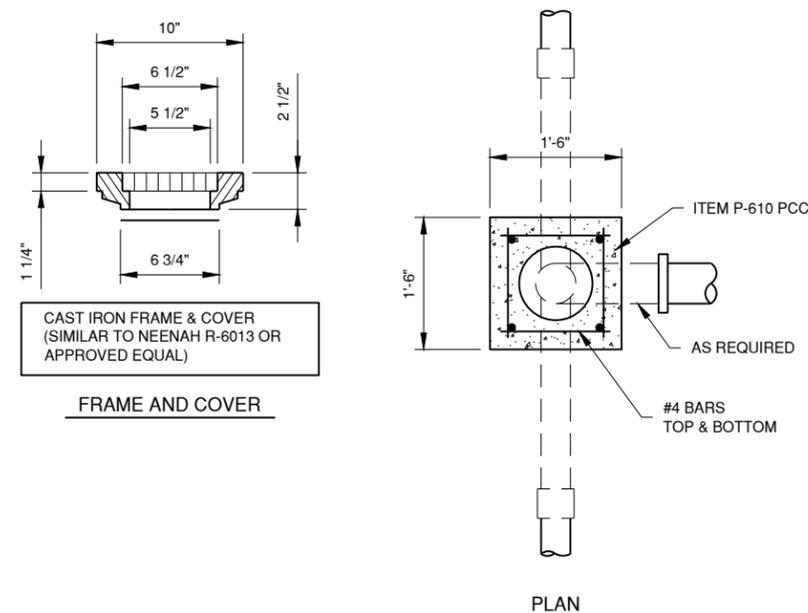
SHEET TITLE
**GRADING &
 DRAINAGE PLAN 3**

CU103
 SHEET 24 OF 63



SIDE VIEW

1 UNDERDRAIN CLEAN-OUT - TYPE 1
N.T.S.

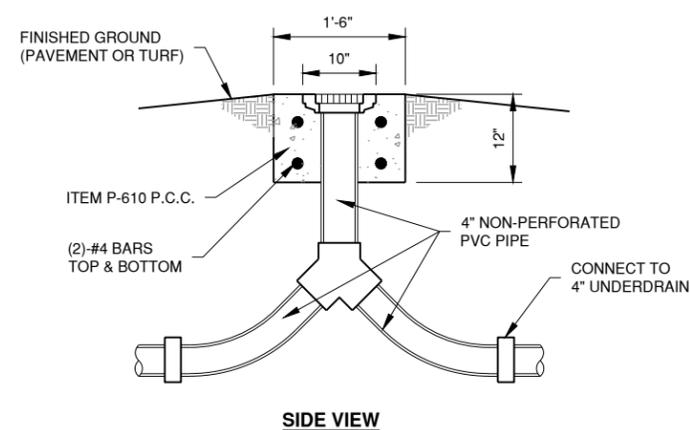


CAST IRON FRAME & COVER
(SIMILAR TO NEENAH R-6013 OR
APPROVED EQUAL)

FRAME AND COVER

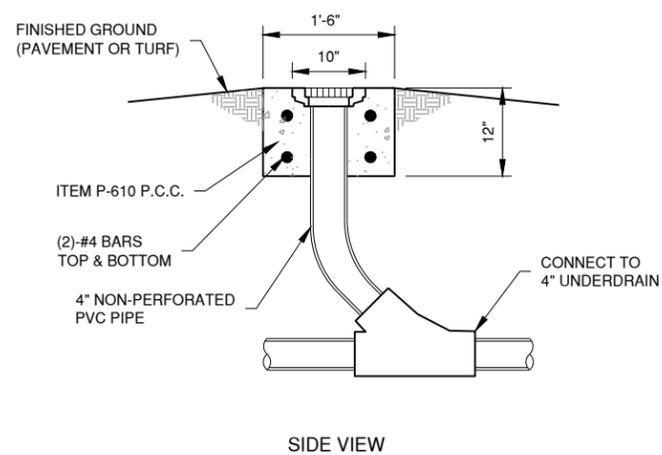
PLAN

4 UNDERDRAIN CLEAN-OUT
N.T.S.



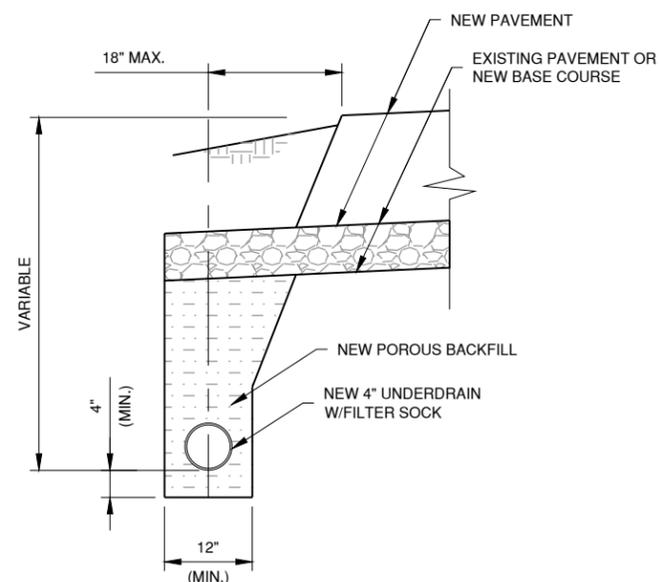
SIDE VIEW

2 UNDERDRAIN CLEAN-OUT - TYPE 2
N.T.S.



SIDE VIEW

3 UNDERDRAIN CLEAN-OUT - TYPE 3
N.T.S.



5 UNDERDRAIN ALONG PAVEMENT EDGE
N.T.S.

DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL WHICH WILL AFFECT DRAINAGE.

100% SUBMITTAL
APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



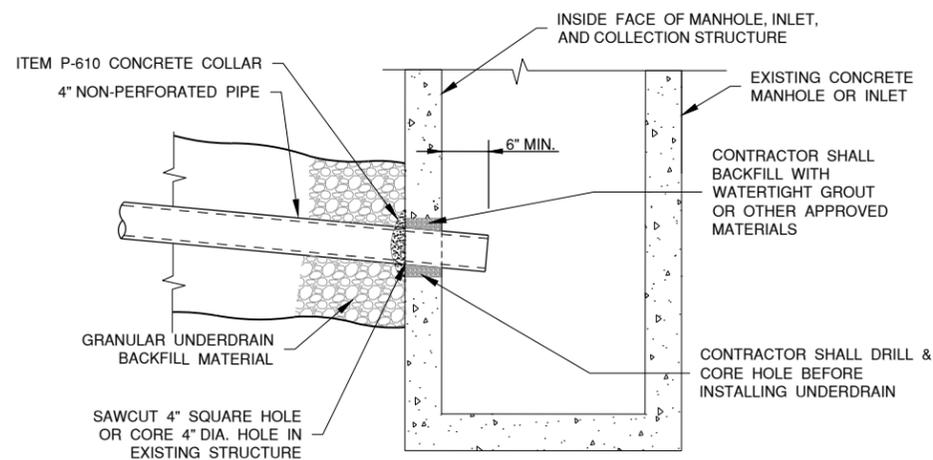
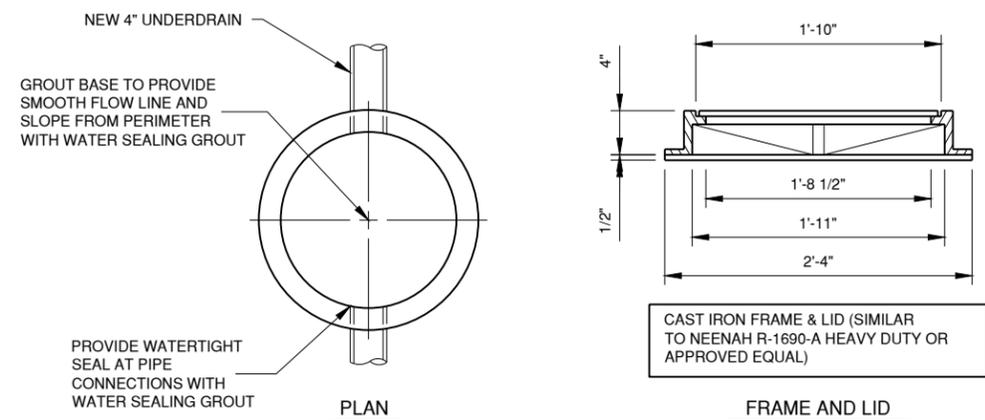
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AIRPORT AUTHORITY
DANVILLE, IL

MARK	DATE	DESCRIPTION

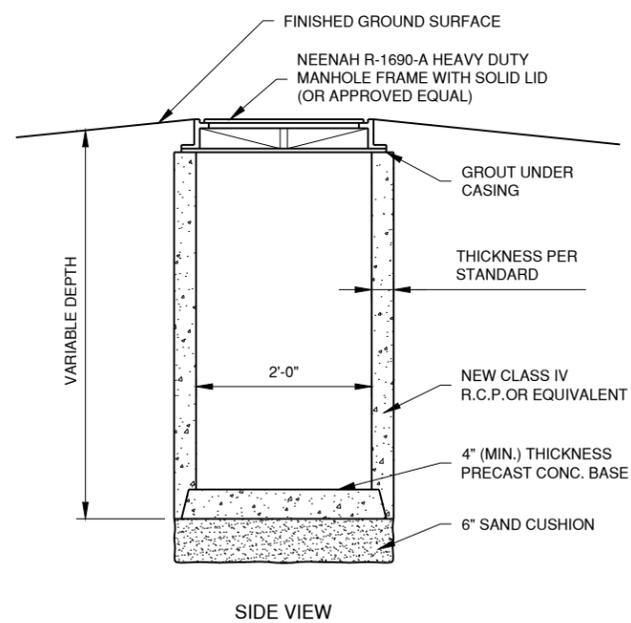
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IL PROJ. NO. DNV-5169
CMT PROJECT NO: 190042-02-30
CAD DWG FILE: 19004202-PH3-CU500.DWG
DESIGNED BY: HCH
DRAWN BY: DPA
CHECKED BY: MJD
APPROVED BY: EMH
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SHEET TITLE
**UNDERDRAIN
DETAILS 1**

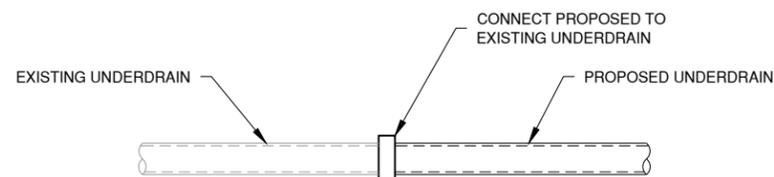
CU501
SHEET 27 OF 63



2 UNDERDRAIN DIRECT CONNECTION TO STRUCTURE
N.T.S.



1 UNDERDRAIN COLLECTION STRUCTURE
N.T.S.



3 UNDERDRAIN CONNECTION TO EXISTING UNDERDRAIN
N.T.S.

100% SUBMITTAL
APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER

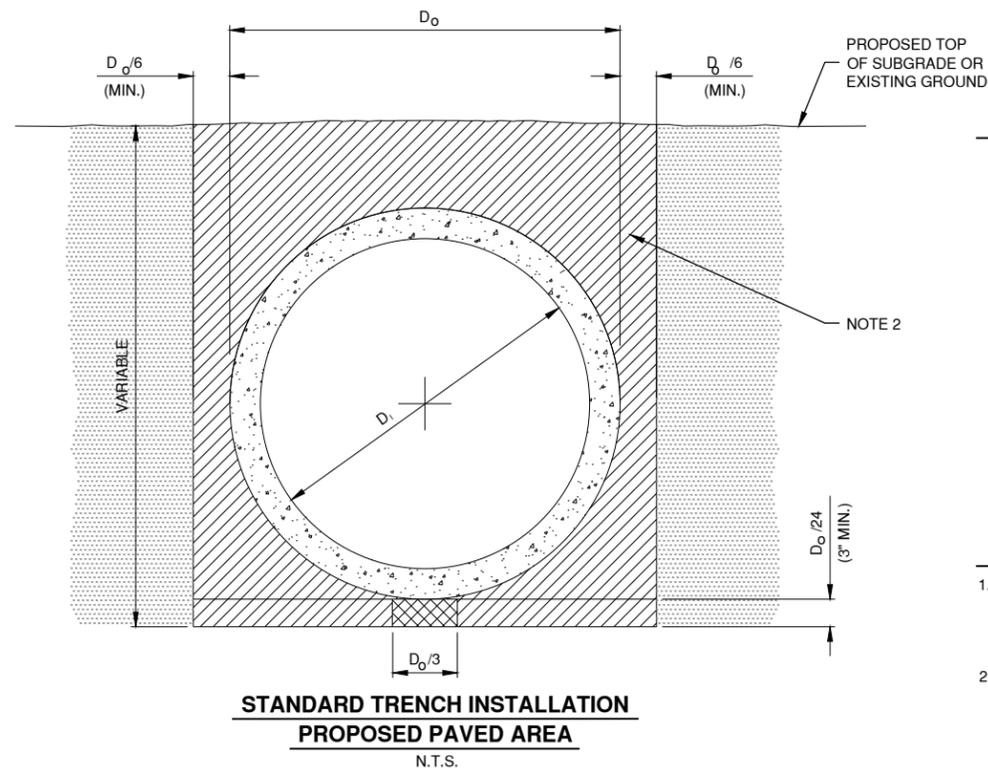
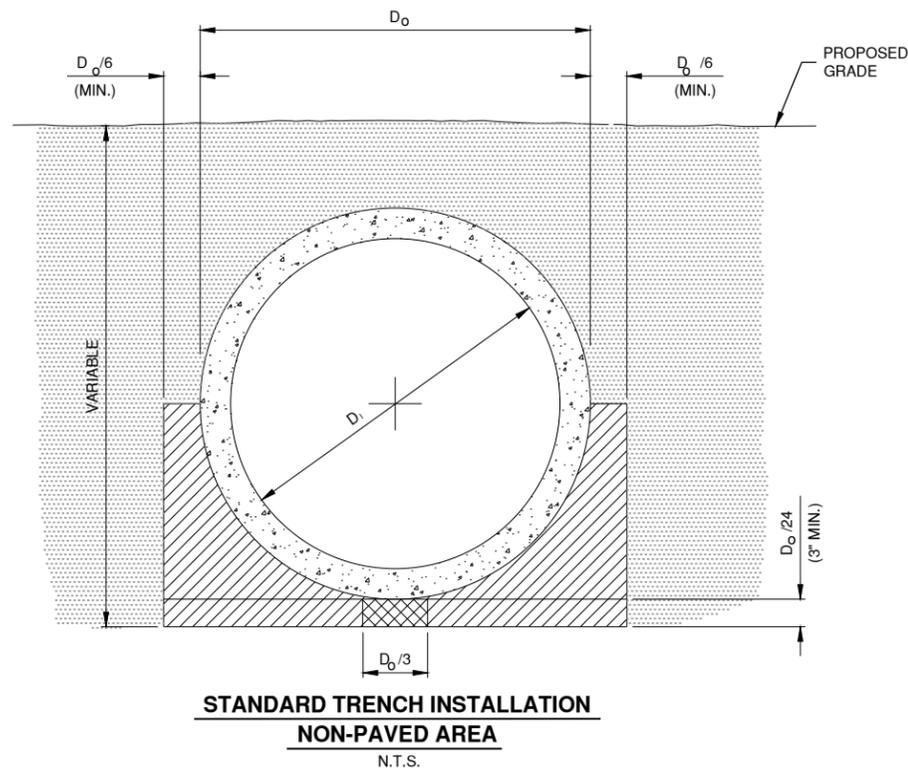


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AIRPORT AUTHORITY
DANVILLE, IL

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SHEET TITLE
**UNDERDRAIN
DETAILS 2**



LEGEND

	DRAINAGE CONDUIT MATERIAL-CONCRETE
	MIDDLE BEDDING LOOSELY PLACED UNCOMPACTED BEDDING
	HAUNCH AND OUTER BEDDING COMPACTION- 95% STANDARD PROCTOR
	LOWER SIDE AND OVERFILL COMPACTION- SAME AS EMBANKMENT REQUIREMENTS
D_o	PIPE OUTSIDE DIAMETER
D_i	PIPE INSIDE DIAMETER

- NOTES**
- BEDDING SHOWN IS IN ACCORDANCE WITH "STANDARD EMBANKMENT INSTALLATIONS", STANDARD INSTALLATION & BEDDING FACTORS FOR THE INDIRECT DESIGN METHOD (DESIGN DATA 40), AMERICAN CONCRETE PIPE ASSOCIATION.
 - BACKFILL TO EXTEND 3' BEYOND EDGES OF PROPOSED PAVEMENT.

100% SUBMITTAL
APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

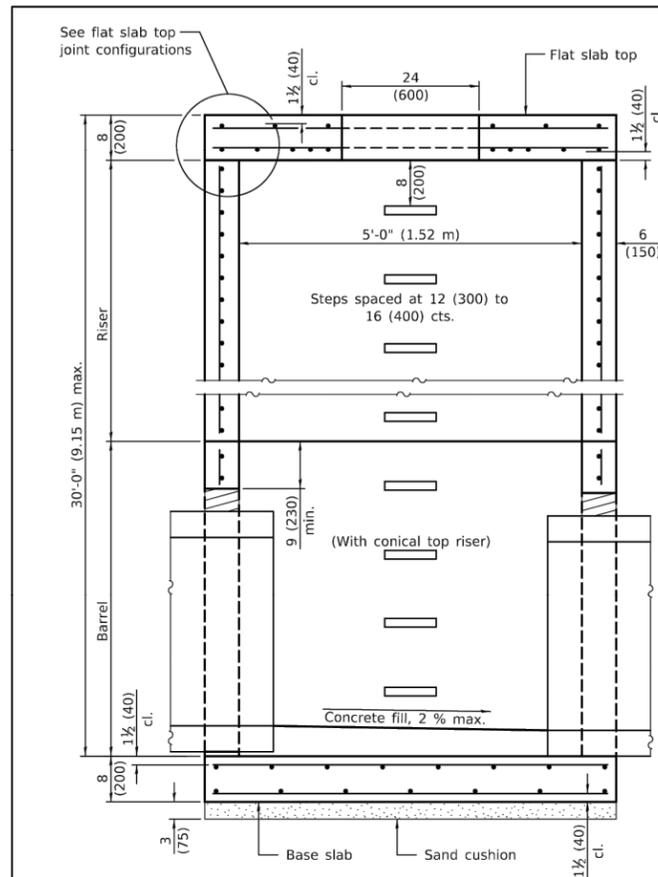


VERMILION REGIONAL
AIRPORT AUTHORITY
DANVILLE, IL

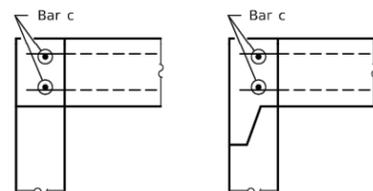
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IL. PROJ. NO. DNV-5169
CMT PROJECT NO: 190042-02-30
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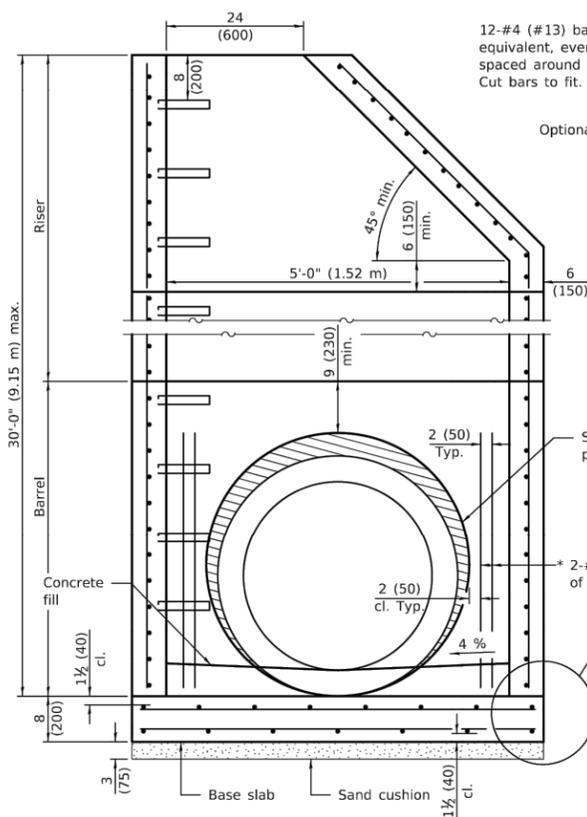
SHEET TITLE
**DRAINAGE TRENCH
DETAILS**



SECTION PARALLEL TO PIPE
(Without conical top riser)



FLAT SLAB TOP JOINT CONFIGURATIONS
(Shown at access hole)

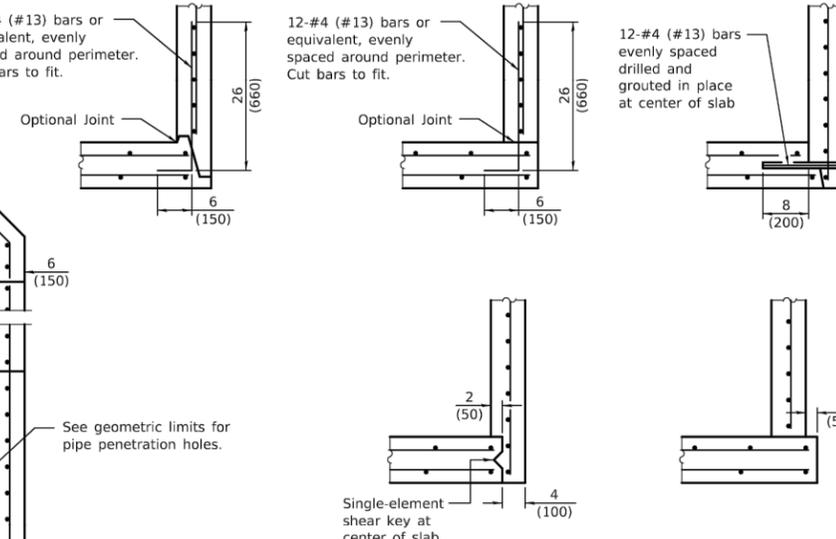


SECTION PERPENDICULAR TO PIPE
(With conical top riser)

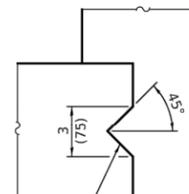
* As an alternate, the barrel wall reinforcement may be reduced to riser wall reinforcement with #3 (#10) bars placed around the pipe penetration holes as shown. This option may be utilized when the pipe penetration holes are formed as opposed to cored.

GEOMETRIC LIMITS FOR PIPE PENETRATION HOLES

1. A minimum of 9 (230) of monolithic reinforced concrete shall be maintained above pipe penetration holes > 32 (810).
2. A minimum 12 (300) inside arc length of reinforced concrete shall be maintained between pipe penetration holes > 15 (380).
3. A maximum of 60 percent of the inside perimeter of the reinforced concrete manhole walls may be removed.
4. Horizontal joints that intersect pipe penetration holes > 15 (380) shall have one joint splice for every location around the perimeter of the joint where the inside arc length between pipe penetration holes is < 24 (600). See joint splice detail.
5. The recommended pipe penetration hole is equal to the O.D. of the pipe plus 4 (100).
6. Only pipe penetration holes ≤ 15 (380) are allowed in riser sections.



BASE SLAB JOINT CONFIGURATIONS



Single-element shear key at center of slab

SHEAR KEY GEOMETRY
(Reinforcement not shown for clarity)

GENERAL NOTES

The manufacturer shall ensure that all precast manhole sections are additionally reinforced where required to resist damage from handling, shipping and installation stresses.

Lifting holes shall be located in the sections as per the manufacturer's recommendations, except as noted.

See Standard 602701 for details of manhole steps.

All dimensions are in inches (millimeters) unless otherwise noted.

DATE	REVISIONS
3-1-19	Moved wall reinforcement from inside face to middle.
1-1-19	Expanded / refined reinforcement options. Increased manhole depths.

PRECAST MANHOLE TYPE A
5' (1.52 m) DIAMETER

(Sheet 1 of 2)

IDOT STANDARD 602402-02

Illinois Department of Transportation

PASSED March 1, 2019
ENGINEER OF POLICY AND PROCEDURES

APPROVED March 1, 2019
ENGINEER OF DESIGN AND ENVIRONMENT

81-1-1 GENISE

100% SUBMITTAL
APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



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DANVILLE, IL

MARK | DATE | DESCRIPTION

FED. PROJ. NO. 3-17-SBGP-TBD

IL PROJ. NO: DNV-5169

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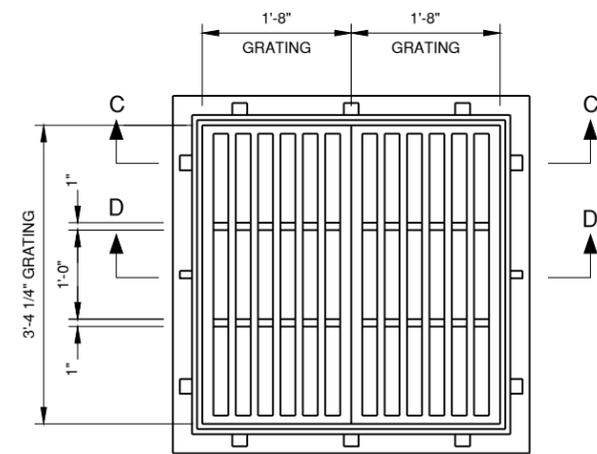
SHEET TITLE
**DRAINAGE MANHOLE
DETAIL 2**



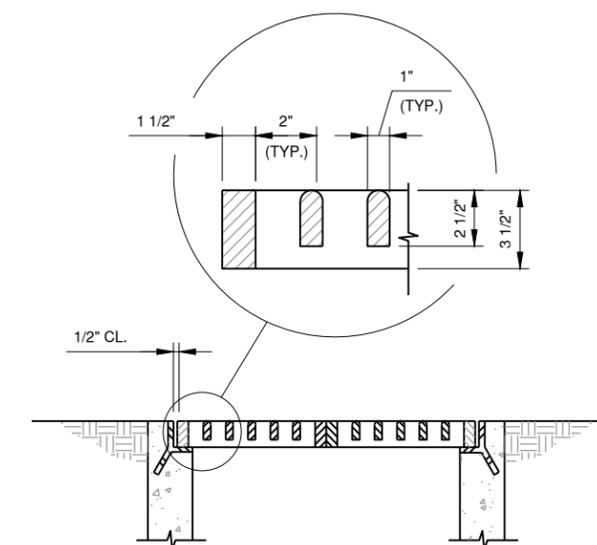
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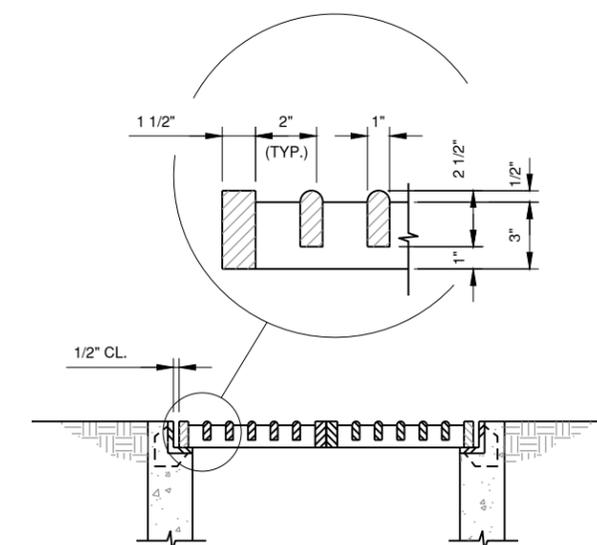
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FRAMES & LIDS



PLAN
N.T.S.

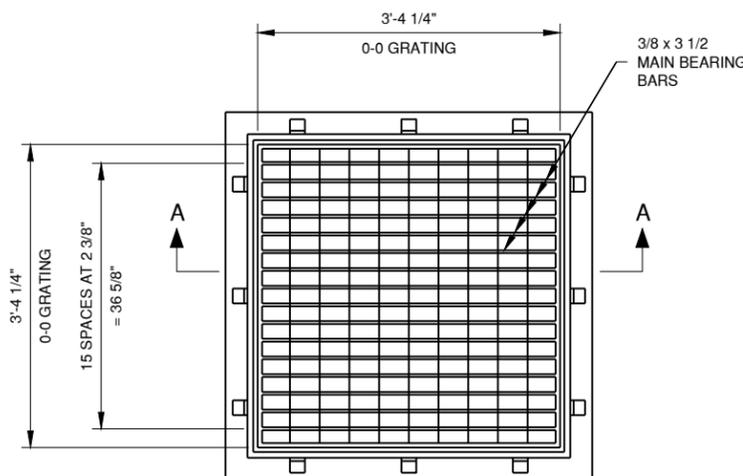


SECTION C-C
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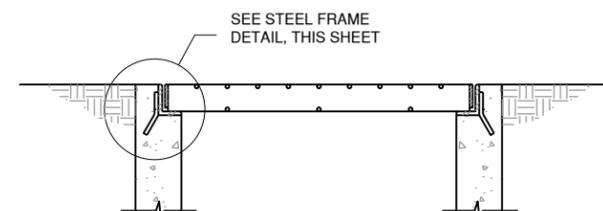


SECTION D-D

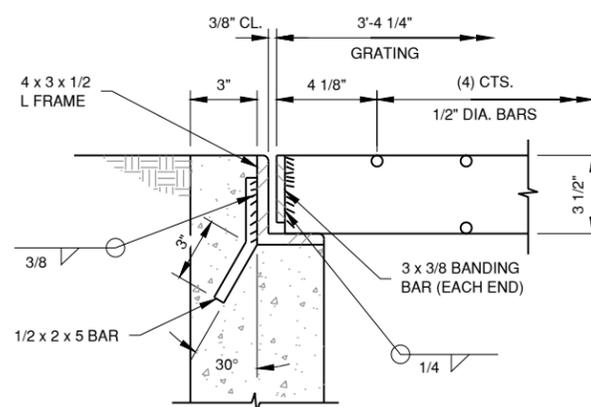
4 CAST GRATE DETAILS
N.T.S.



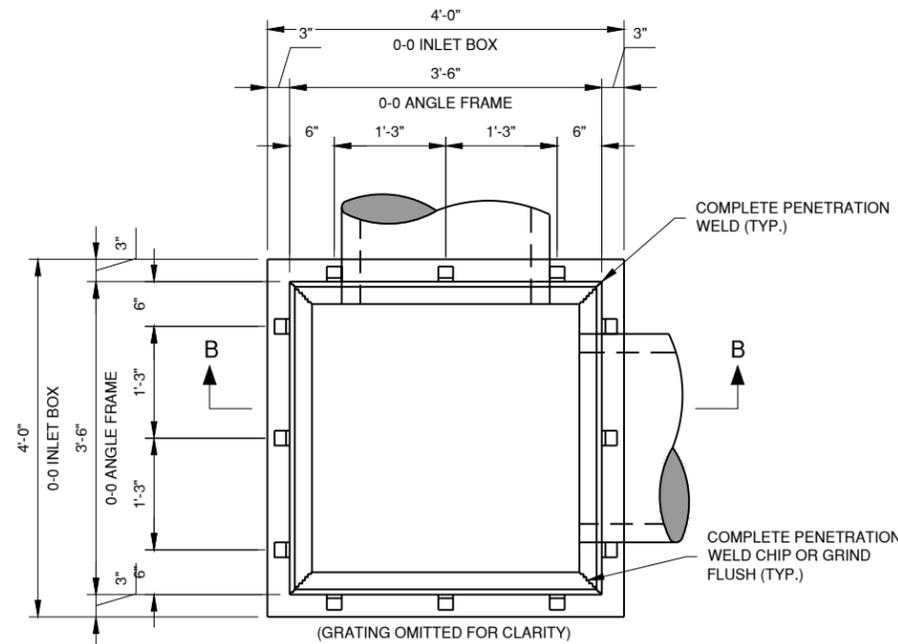
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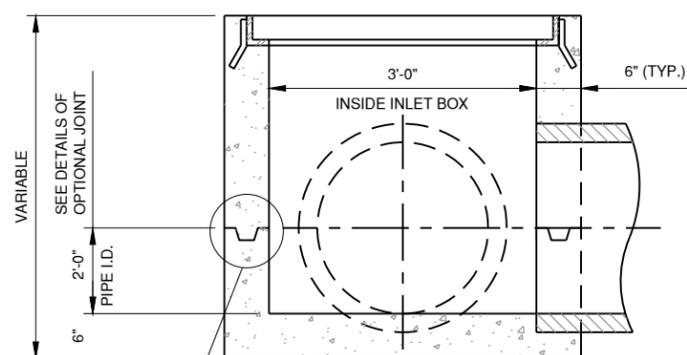
SECTION A-A
N.T.S.



3 STEEL FRAME DETAIL
N.T.S.



PLAN
N.T.S.

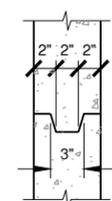


SECTION B-B
N.T.S.

36" INLET NOTE

INLET IS AN IDOT STD. 542546, FLUSH INLET BOX FOR MEDIAN. FRAME & GRATES ARE NEENAH R-3807 OR APPROVED EQUAL.

1 36" INLET DETAIL
N.T.S.



2 OPTIMAL JOINT DETAIL
N.T.S.

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APRIL 25, 2025

REALIGN TAXIWAY A, PHASE 3

OWNER



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AIRPORT

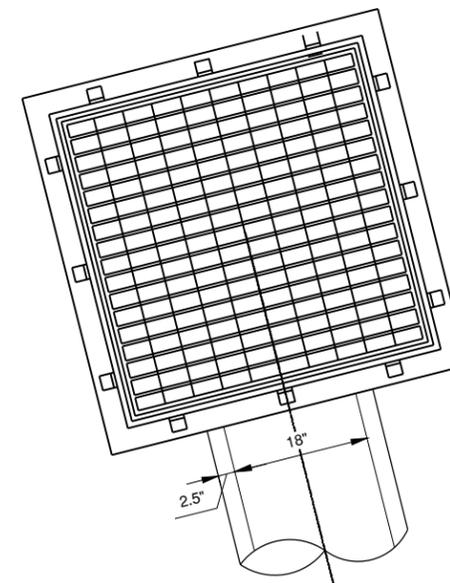
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AIRPORT AUTHORITY
DANVILLE, IL

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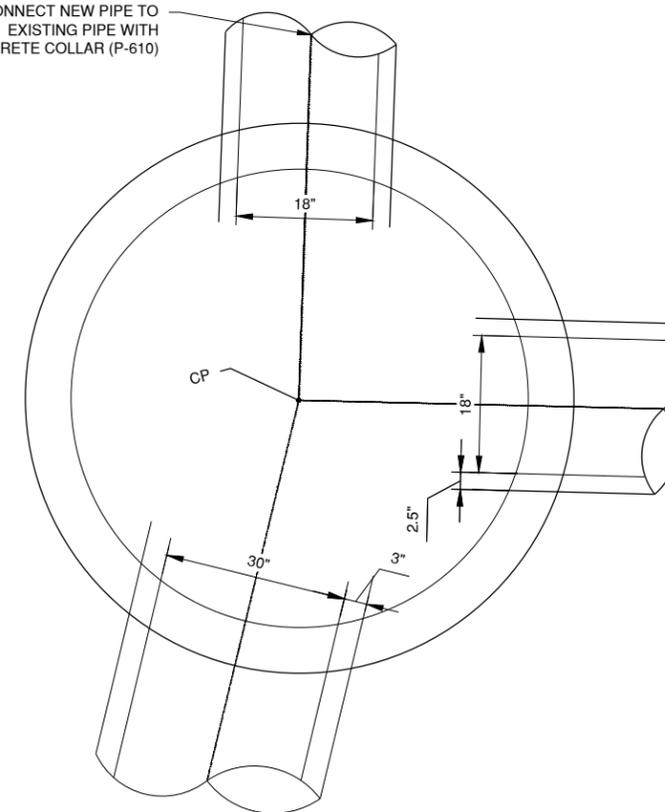
SHEET TITLE
**DRAINAGE
STRUCTURE DETAIL 1**

CU508
SHEET 34 OF 63

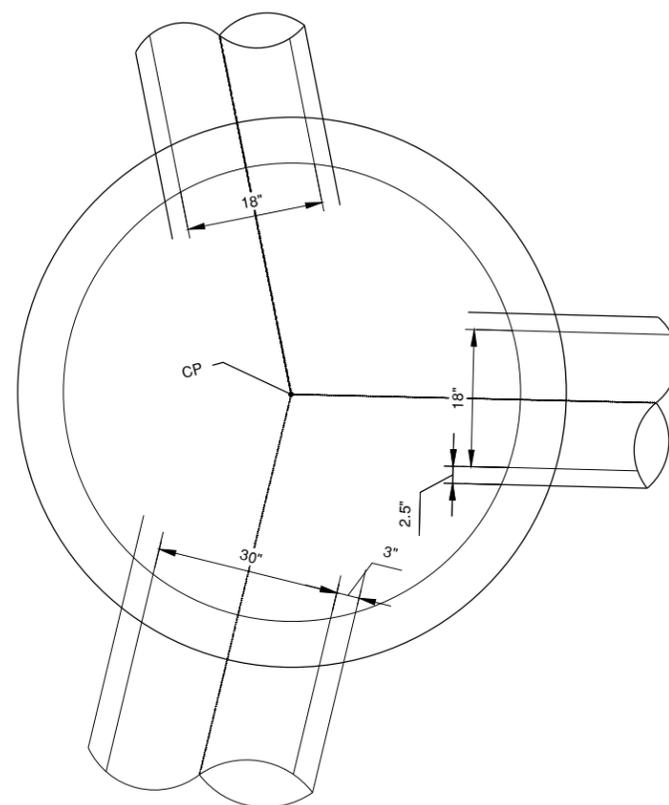


2 INLET IN-C1 - TYPE A
N.T.S
CU101

CONNECT NEW PIPE TO
EXISTING PIPE WITH
CONCRETE COLLAR (P-610)



3 MANHOLE MH-E2 - BB
N.T.S
CU101



1 MANHOLE MH-E2 - AA#2
N.T.S
CU101

SIGN SCHEDULE

SIGN #	# OF CHAR.	POWER CKT	SIGN STYLE	SIGN CLASS	CIRCUIT INTENSITY	LEGEND HEIGHT (SIZE 2)	PANEL HEIGHT (SIZE 2)	TYPE
GS-1	4	NEW TW - A	2	2	MEDIUM	15"	24"	L-858 (LED)
GS-2	6	NEW TW - A	2	2	MEDIUM	15"	24"	L-858 (LED)
GS-3	6	NEW TW - A	2	2	MEDIUM	15"	24"	L-858 (LED)
GS-4	4	NEW TW - A	2	2	MEDIUM	15"	24"	L-858 (LED)
GS-5	5	NEW TW - A	2	2	MEDIUM	15"	24"	L-858 (LED)

* SEE SIGN DETAIL ON SHEET CE504

LEGEND

EXISTING	PROPOSED	
		SPLICE CAN
		TAXIWAY GUIDANCE SIGN
		TAXIWAY EDGE LIGHT - STAKE MOUNTED
		TAXIWAY EDGE LIGHT - BASE MOUNTED
		RUNWAY BASE MOUNTED EDGE LIGHT
		4-WAY DUCT BANK
		RUNWAY EDGE LIGHT - FLUSH MOUNTED
		ELEVATED RETROREFLECTIVE MARKER
		DUCT MARKER
		NEW TAXIWAY A CABLE; #8 5KV IN UD (BASE BID)
		EXISTING TAXIWAY A CABLE; #8 5KV IN UD TO REMAIN
		EXISTING RUNWAY 3-21 CABLE; #8 5KV TO REMAIN
		EXISTING RUNWAY 16-34 CABLE; #8 5KV TO REMAIN
		CIRCUIT NAME; # OF CABLES

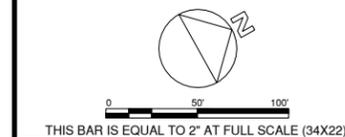
NOTES

- FOR PROPOSED JUMPER LOCATION, SEE SHEET CE103.
- CONNECT NEW CIRCUIT TO EXISTING CIRCUIT AT EXISTING LIGHT CAN.
- IT IS THE CONTRACTORS RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES, ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTORS EXPENSE. THE CONTRACTOR SHALL VERIFY EXISTING CIRCUIT ROUTING PRIOR TO MAKING ANY FIELD MODIFICATIONS.
- PRIOR TO INSTALLATION OF NEW CONDUIT AND CABLE. THE CONTRACTOR SHALL VERIFY THE ROUTING OF THE EXISTING AND PROPOSED CIRCUIT TO ENSURE A CONTINUOUS LOOP WILL BE ACHIEVED. COSTS INCIDENTAL TO THE CONDUIT AND CABLE INSTALLATION.
- REMOVE EXISTING TAXIWAY EDGE LIGHTS ALONG ABANDONED TAXIWAY SECTION. DISCONNECT EXISTING SIGNS FROM CIRCUIT AT SPLICE CAN.
- THE AIRPORT SHALL HAVE RIGHT OF FIRST REFUSAL ON ALL REMOVED ELECTRICAL EQUIPMENT.
- INSTALL COUNTERPOISE AS DETAILED ON SHEET CE501.
- LIGHTS SHALL BE NO MORE THAN 10', BUT NO LESS THAN 2' FROM THE PAVEMENT EDGE AND IN A STRAIGHT LINE FROM PC TO PT OR TAPER POINT.
- IF ADD. ALT. #1 IS NOT AWARDED, LIGHT OFFSET FROM CENTERLINE SHALL BE REDUCED BY 15' TO MEET THE PAVEMENT EDGE OFFSET REQUIRED IN NOTE 8.



License No. 184-00613

CONSULTANTS



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APRIL 25, 2025

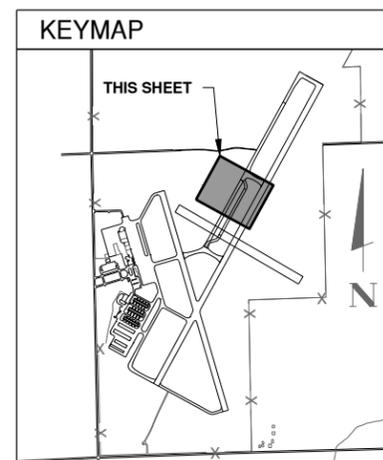
REALIGN TAXIWAY A, PHASE 3

OWNER



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AIRPORT AUTHORITY
DANVILLE, IL

NOTE: THIS PAGE MAY BE
PRINTED IN COLOR TO
IMPROVE CLARITY OF SIGNS

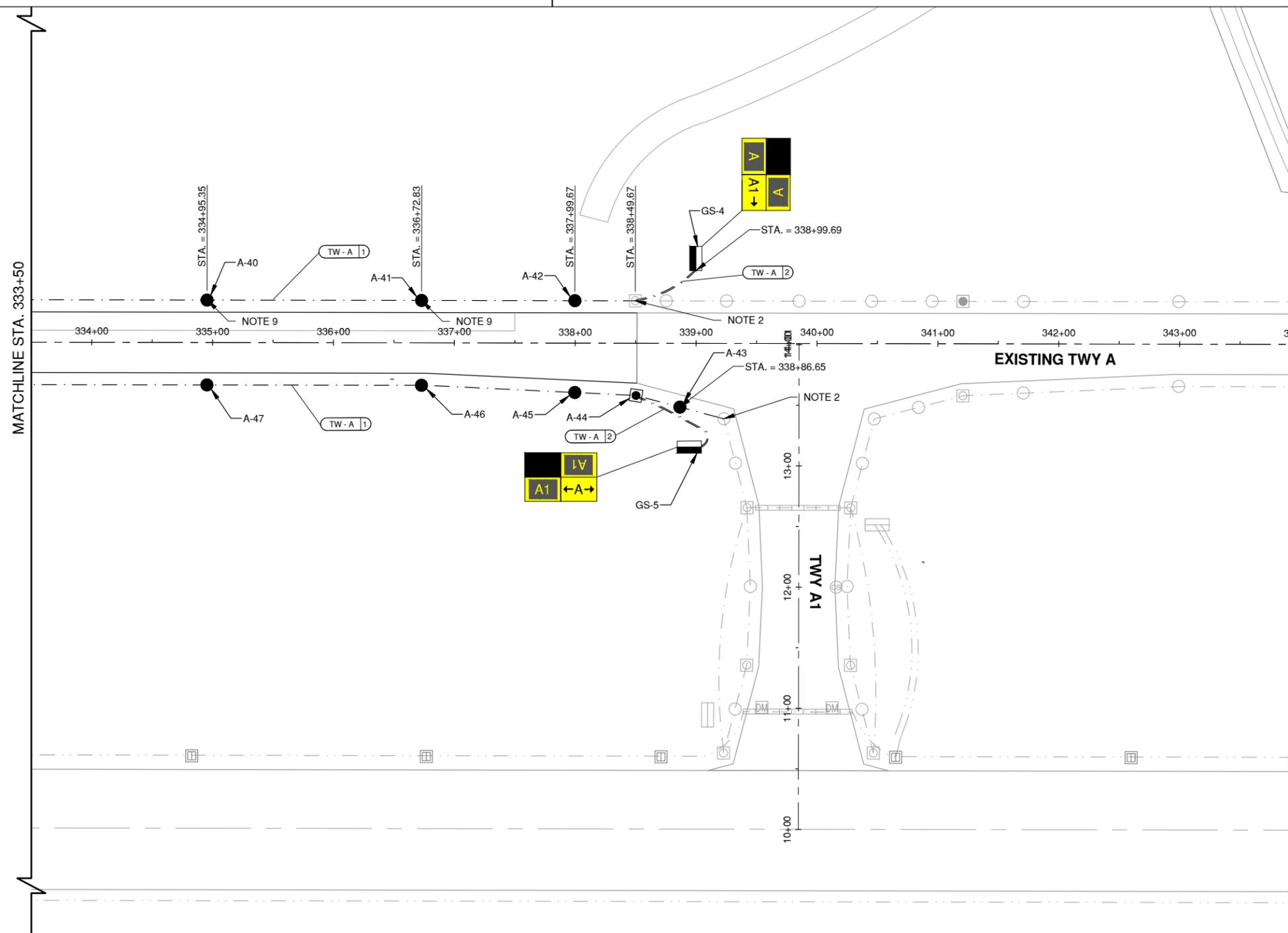


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SHEET TITLE
**ELECTRICAL & SIGN
LAYOUT 2**

CE102
SHEET 37 OF 63





MARK	DATE	DESCRIPTION

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CMT PROJECT NO: 190042-02-30

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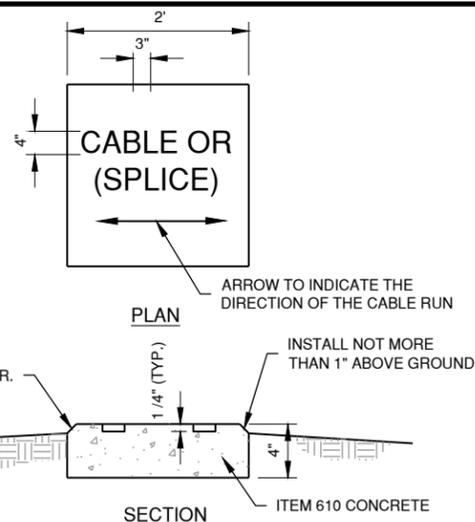
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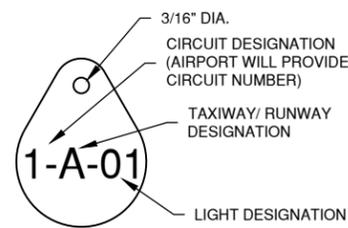
ELECTRICAL DETAILS

1



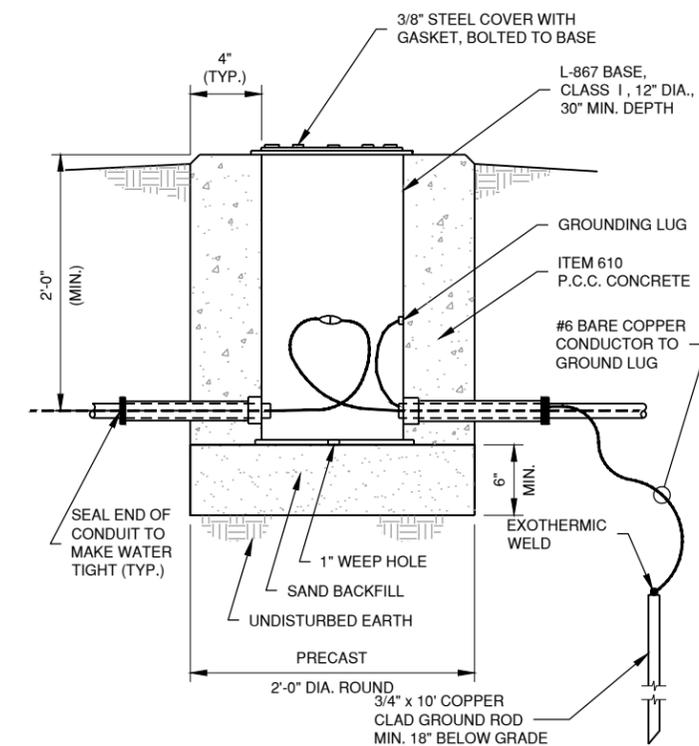
1 TURF CABLE / SPLICE MARKER
N.T.S.

- NOTES:
1. DUCT MARKERS SHALL BE INSTALLED AT BOTH EDGES OF PAVEMENT WHERE PROPOSED ELECTRICAL DUCTS CROSS BOTH NEW AND EXISTING PAVEMENTS.
 2. CABLE MARKERS SHALL BE INSTALLED AT ALL BENDS AND EVERY 200' ALONG THE HOMERUN.
 3. ITEM 610 CONCRETE SHALL BE USED.
 4. ALL EXPOSED EDGES SHALL BE EDGED WITH A 1/4" RADIUS TOOL.
 5. THE COST OF FURNISHING AND INSTALLING NEW MARKERS SHALL BE INCIDENTAL TO THE ASSOCIATED ITEMS.
 6. 0.049 CU. YD. CONCRETE PER MARKER.
 7. A MARKER CONFORMING TO THIS DETAIL MARKED "SPLICE" SHALL BE INSTALLED AT ALL SPLICE LOCATIONS NOT IN LIGHT CANS OR MANHOLES.

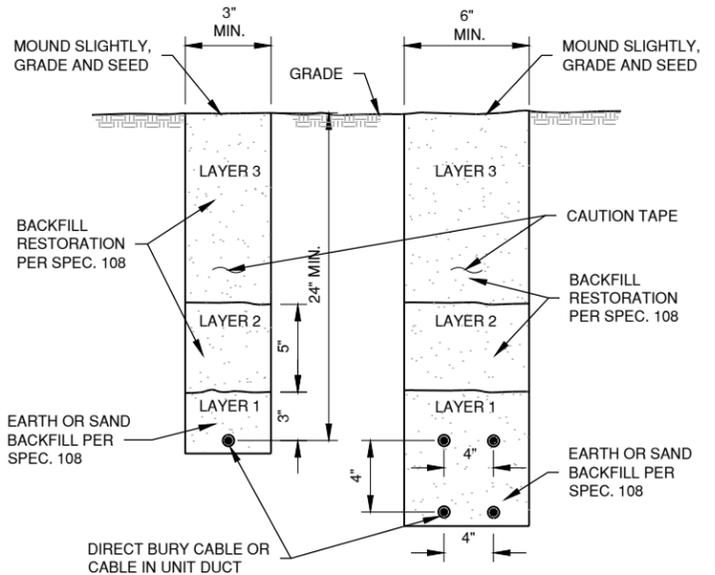


2 LIGHT IDENTIFICATION TAG
N.T.S.

- NOTES:
1. INSTALL A NON-CORROSIVE DISC OF 2" MINIMUM DIAMETER WITH THE NUMBER PERMANENTLY STAMPED, CUT OUT, OR ENGRAVED UNDER THE HEAD OF THE BASE PLATE BOLT OR ATTACHED TO LIGHT FLANGE WITH SET SCREW.
 2. LEGENDS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. CONTRACTOR TO COORDINATE LEGEND WITH AIRPORT.
 3. THE CONTRACTOR SHALL NUMBER THE EXISTING/ PROPOSED LIGHTS AND SIGNS IN EACH CIRCUIT STARTING AT THE HOMERUN CONTINUING AROUND THE ENTIRE CIRCUIT BACK TO THE HOMERUN.
 4. AIRFIELD SIGNS SHALL BE TAGGED & NUMBERED.

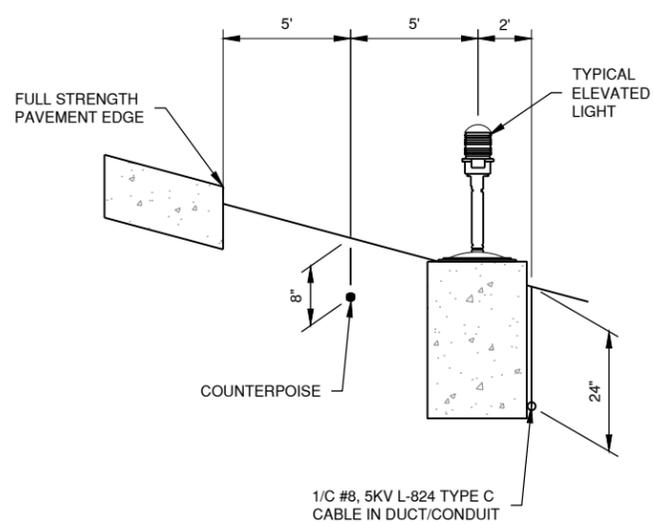


3 SPLICE CAN
N.T.S.



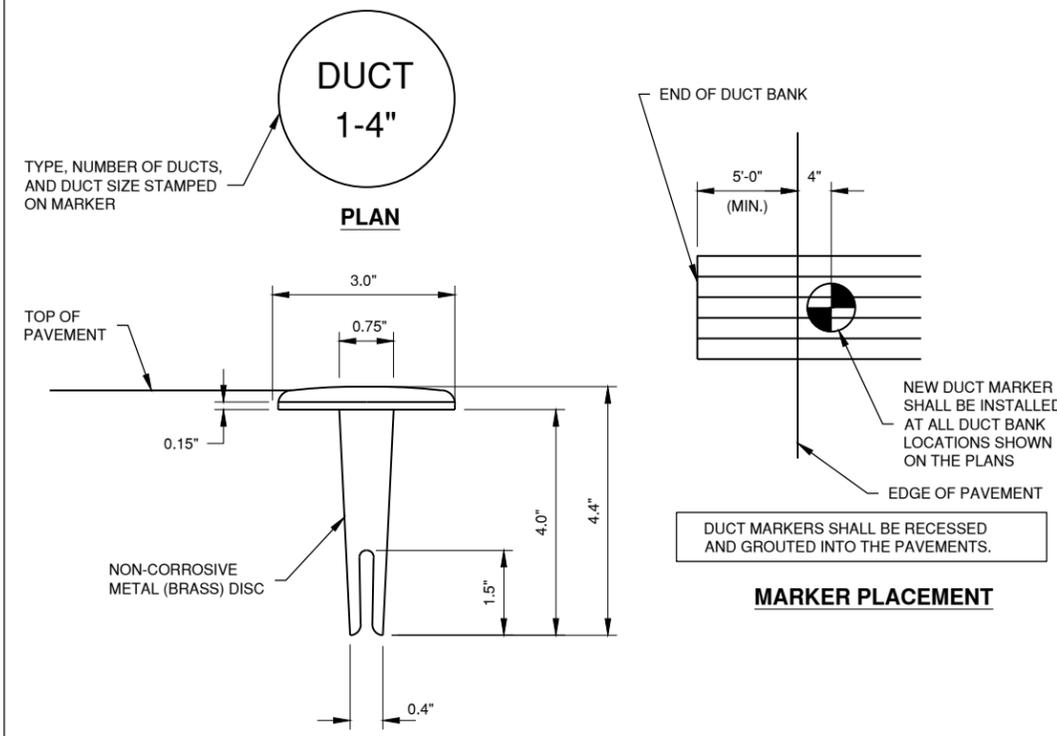
4 CABLE TRENCH
N.T.S.

- NOTES:
1. CABLES SHALL NOT BE PLACED LESS THAN 24" DEEP IN ANY ONE TRENCH UNLESS PERMITTED BY ENGINEER.
 2. WHERE PERMITTED, CONTRACTOR MAY INSTALL CABLE IN UNIT DUCT BY PLOWING METHOD.



5 COUNTERPOISE LOCATION
N.T.S.

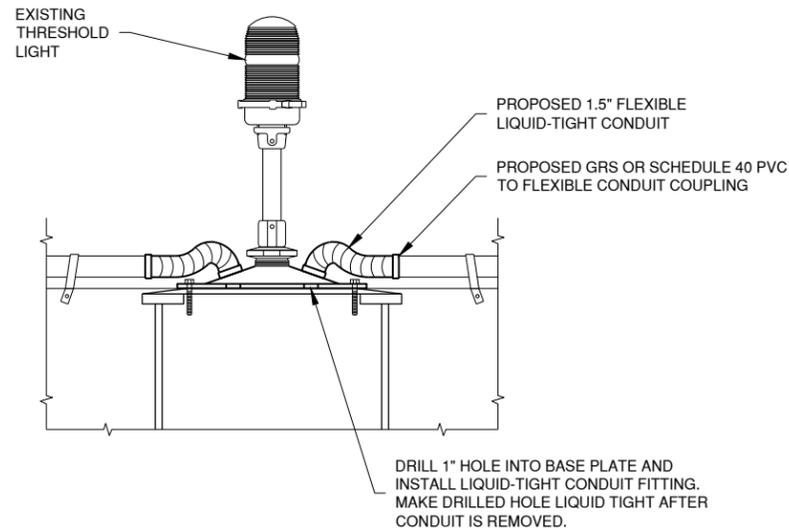
- NOTES:
- #6 BARE COUNTERPOISE WITH 3/4" x 10' GROUND ROD INSTALLED AT MAX. 500' SPACING. ALSO USE GROUND ROD TO TERMINATE THE COUNTERPOISE AT BOTH ENDS OF DUCT. GROUND RODS SHALL BE CONSIDERED INCIDENTAL TO OTHER PAY ITEMS.



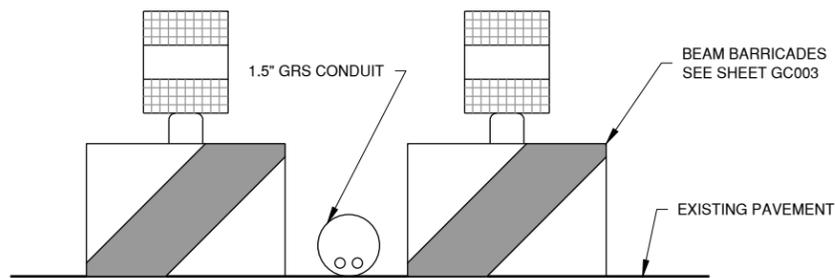
5 DUCT MARKER IN-PAVEMENT
N.T.S.

- TYPE, NUMBER OF DUCTS, AND DUCT SIZE STAMPED ON MARKER
- END OF DUCT BANK
- 5'-0" (MIN.)
- 4"
- NEW DUCT MARKER SHALL BE INSTALLED AT ALL DUCT BANK LOCATIONS SHOWN ON THE PLANS
- EDGE OF PAVEMENT
- DUCT MARKERS SHALL BE RECESSED AND GROUTED INTO THE PAVEMENTS.
- MARKER PLACEMENT
- TOP OF PAVEMENT
- 0.15"
- 3.0"
- 0.75"
- NON-CORROSIVE METAL (BRASS) DISC
- 1.5"
- 4.0"
- 4.4"
- 0.4"

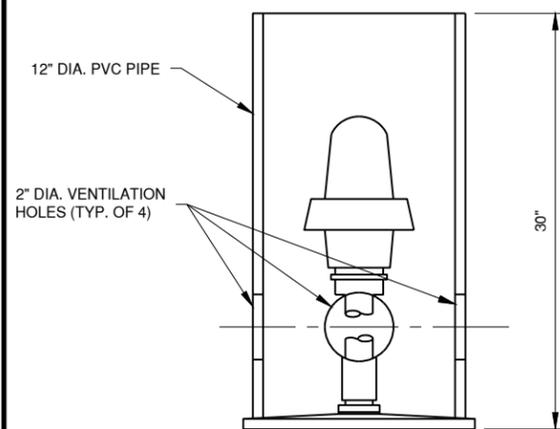
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Date: Thursday, March 6, 2025 3:18:03 PM



1 TEMPORARY JUMPER LIGHT DETAIL
N.T.S.



2 TEMPORARY JUMPER CABLE CONDUIT PROTECTION DETAIL
N.T.S.



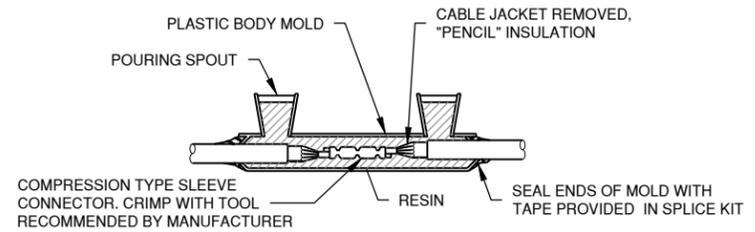
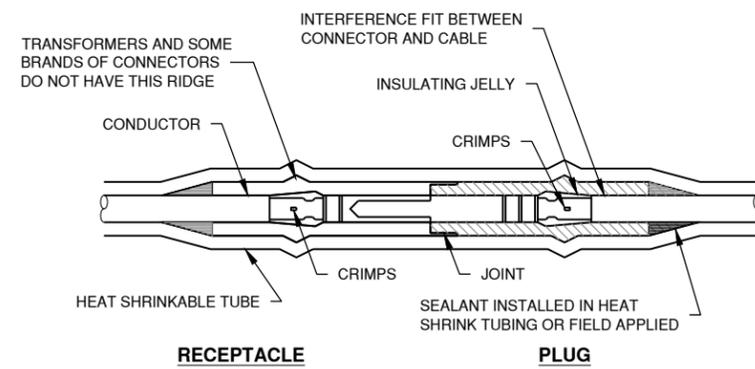
3 TAXIWAY EDGE LIGHT COVER DETAIL
N.T.S.

NOTES

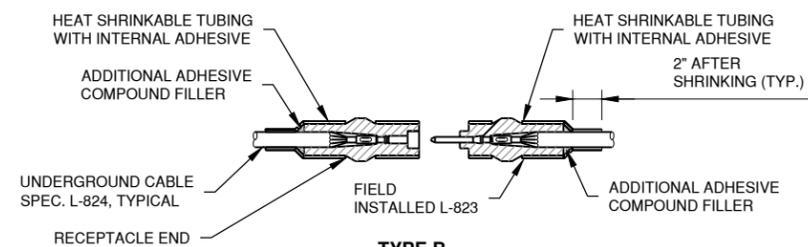
3\"/>

INSTALLATION INSTRUCTIONS TO SUPPLEMENT THE MANUFACTURER'S INSTRUCTIONS

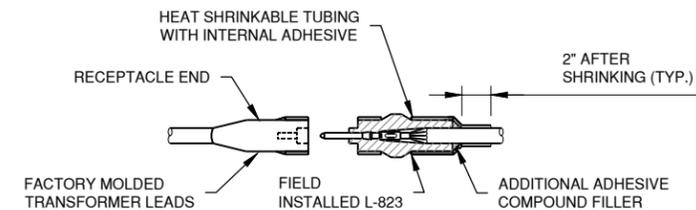
- CLEAN THE CABLE THOROUGHLY 9\"/>
- REMOVE INSULATION PER MANUFACTURER'S INSTRUCTIONS. DO NOT NICK THE CONDUCTOR. DO NOT PENCIL INSULATION ON L-824 TYPE C CABLE.
- INSTALL PIN AND/OR RECEPTICAL WITH CRIMPING TOOL WHICH MUST BE COMPLETELY CLOSED BEFORE THE TOOL MAY BE REMOVED.
- BE SURE CABLE AND CONDUCTOR FITTINGS ARE CLEAN. COAT THE CABLE INSULATION WITH INSULATION JELLY FROM THE CONNECTOR.
- CAREFULLY INSERT CABLE INTO CONNECTOR TO THE PROPER DEPTH.
- SLIP 14 INCH LENGTH OF HEAT SHRINK TUBING ON TRANSFORMER LEAD RAYCHEM TCS-13-14-U OR APPROVED EQUAL.
- COMPLETE CONNECTION BY MATING THE PLUG AND RECEPTICAL. ****CAUTION**** BE SURE THE CABLE DOES NOT SLIP WHEN THE CONNECTION IS MADE.
- APPLY RUBBER TAPE AND PLASTIC TAPE, ONE HALF LAPPED 1-1/2\"/>
- ANY CONNECTOR WHICH IS CONTAMINATED BY DIRT OR OTHER DELETERIOUS MATERIAL SHALL BE REMOVED NOT REINSTALLED.
- CLEAN CONNECTOR AND CABLE INSULATION WITH WAX OR GREASE SOLVENT TO REMOVE SURFACE SILICONE JELLY.
- WRAP SEALANT SECURELY AROUND THE CABLE. INSULATION TO EXTEND 1-1/2\"/>
- CENTER HEAT SHRINK OVER THE CONNECTOR. APPLY HEAT EVENLY BEGINNING AT THE CENTER AND WORKING AROUND CABLE TO ENDS. THERMOCHROMIC PAINT SHALL SHOW PROPER HEAT HAS BEEN USED. ***** DO NOT OVER HEAT *****.
- THE HEAT SOURCE SHALL BE AN ELECTRIC HEAT GUN OR A PROPANE WITH FLAME SPREADER.
- CONTRACTOR MAY ELECT TO INSTALL "COMPLETE KIT" IN LIEU OF L-823 CONNECTOR WITH HEAT SHRINK.



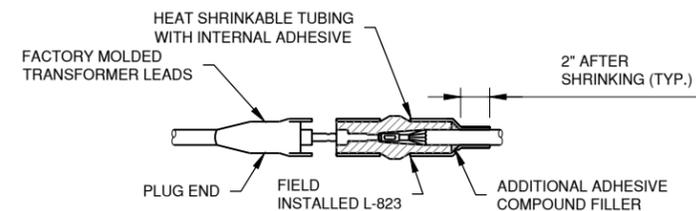
TYPE A
FOR SPLICES IN HOMERUNS AND FOR EXTENSIONS TO EXISTING CABLES ONLY



TYPE B
NOT TO BE USED UNLESS DIRECTED BY ENGINEER



TYPE C
FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS



TYPE D
FOR SPLICES AT RUNWAY LIGHTS, TAXIWAY LIGHT AND SIGNS

4 CABLE SPLICES
N.T.S.

NOTES

- MATCH THE OUTSIDE DIAMETER OF CABLE INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY
- WRAP WITH AT LEAST ONE LAYER OF RUBBER OR TAPE, ONE-HALF LAPPED, EXTENDING AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT.

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REALIGN TAXIWAY A, PHASE 3

OWNER



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ELECTRICAL DETAILS

3

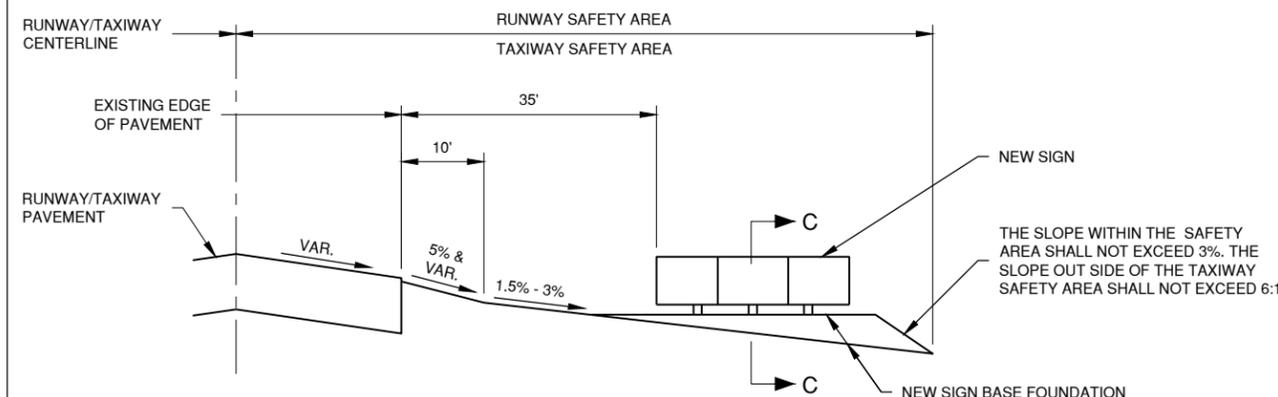


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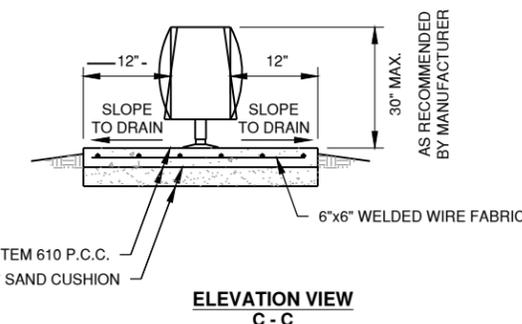
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SHEET TITLE
ELECTRICAL DETAILS

4



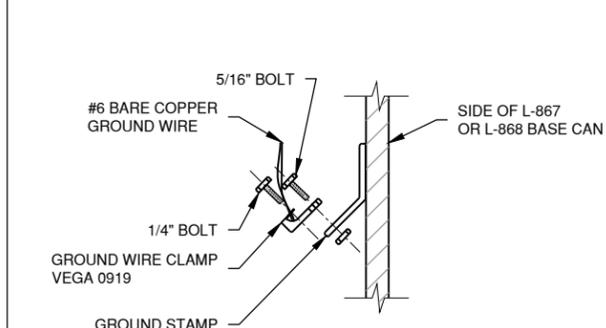
ELEVATION



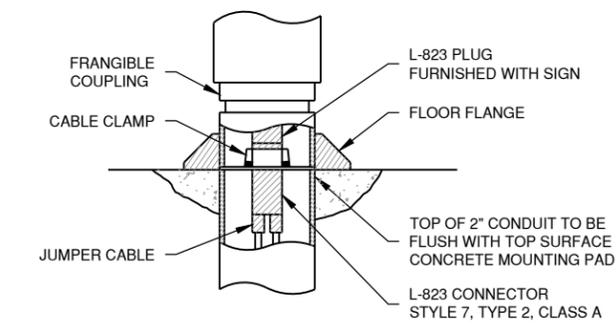
ELEVATION VIEW
C - C

2 AIRFIELD SIGN INSTALLATION
N.T.S.

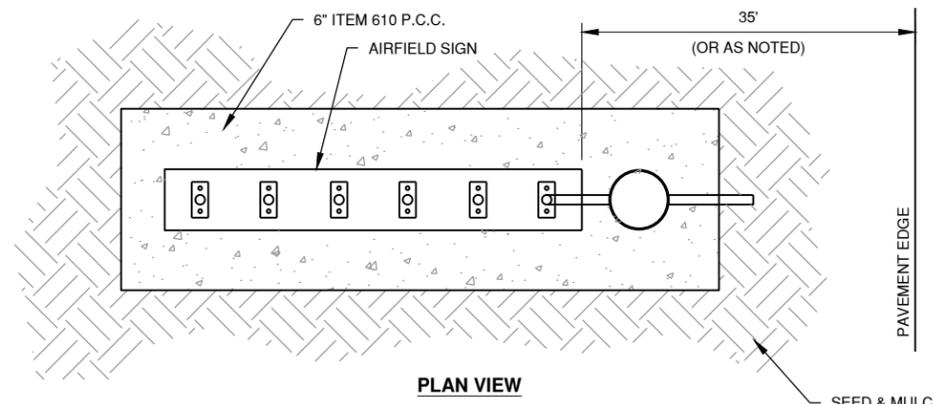
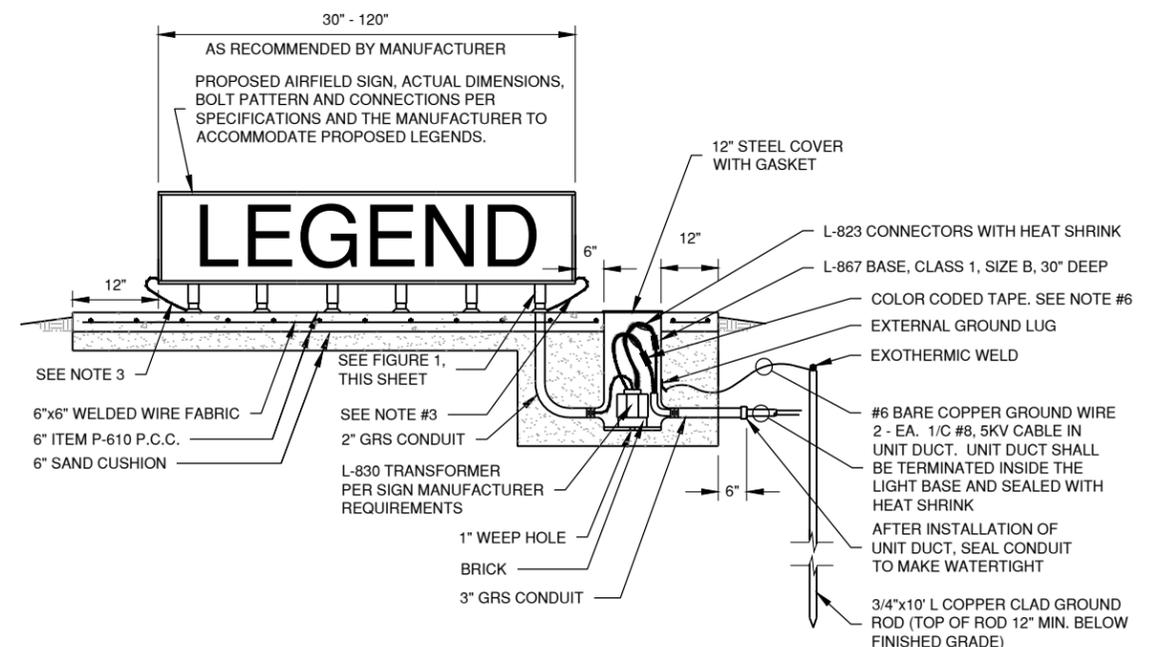
- SIGN NOTES**
- SLOPES SHOWN ARE FROM FAA STANDARDS AND MAY NOT REFLECT THE ACTUAL GRADES IN THE FIELD
 - ESTIMATED 1 C.Y. OF EMBANKMENT MAY BE REQUIRED TO CONSTRUCT SIGN BASE FOUNDATION. COSTS TO CONSTRUCT SHALL BE INCIDENTAL TO SIGN PAY ITEM.
 - ACTUAL LOCATION OF THE SIGN WITHIN THE TAXIWAY SAFETY AREA WILL VARY DUE TO PAVEMENT WIDTHS AND VARIANCES IN SIGN FOUNDATION LENGTHS.
 - 4" OF KNITTED STRAW MAT SHALL BE PLACED AROUND THE PROTECTION APRON. COST FOR MAT SHALL BE INCIDENTAL TO SIGN PAY ITEM.



3 FACTORY GROUND LUG
N.T.S.



4 ELECTRICAL CONNECTION
N.T.S.



1 L-858 AIRFIELD GUIDANCE SIGN - LED
N.T.S.

- AIRFIELD GUIDANCE SIGN NOTES**
- TRANSFORMER WATTAGE SHALL BE AS REQUIRED BY LED SIGN MANUFACTURER. SIGNS ON RUNWAY CIRCUITS SHALL BE STYLE 2 OR 3 DEPENDING ON REGULATOR.
 - SIGN LEGEND SHALL BE AS SHOWN IN THE PLANS. SIGN SCHEDULE IS SUBJECT TO FAA APPROVAL OF THE SIGNAGE PLAN. CHANGES TO NEW LEGENDS MAY OCCUR DURING CONSTRUCTION.
 - SIGN ANCHOR TETHERS AND GROUND WIRES ARE REQUIRED. SEE SPECIFICATIONS.
 - SIGNS SHALL BE SIZE 2, STYLE 2 OR 3, CLASS 2, AND MODE 2. SEE SIGN SCHEDULE FOR DETAILS
 - LIGHT I.D. TAG FOR SIGN SHALL INCLUDE SIGN DESIGNATOR SHOWN IN THE PLAN TABLES.
 - DIRECTION OF PRIMARY CABLES MUST BE IDENTIFIED BY COLOR CODING AS FOLLOWS: WHEN FACING SIGN IN BACK FACING THE RELATED RUNWAY OR TAXIWAY PAVEMENT, THE CABLE FOR THE CIRCUIT TO THE LEFT IS CODED RED AND CABLE FOR THE CIRCUIT TO THE RIGHT IS CODED BLUE.

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TWY A
STA. = 337+50.00
OFF. = -77.71

NEW TAXIWAY CENTERLINE

1
CM501

REMARK EXISTING
TAXIWAY CENTERLINE

TWY A
STA. = 339+25.15
OFF. = 0.00

TWY A
STA. = 340+45.18
OFF. = 0.00

334+00 335+00 336+00 337+00 338+00 339+00 340+00 341+00 342+00 343+00

PROPOSED TAXIWAY A

TWY A
STA. = 338+00.00
OFF. = 85.21

TWY A
STA. = 339+25.15
OFF. = 60.00

TWY A1
STA. = 13+41.02
OFF. = 0.00

EXISTING TAXIWAY A

REMARK EXISTING
TAXIWAY CENTERLINE

NOTE 1

**BASE BID
MARKING PLAN**

12+00
11+00
TWY A1

RUNWAY 3/21

MATCHLINE STA. 333+50

NOTES

1. TERMINATE NEW CENTERLINE PAVEMENT MARKING 12" SHORT OF RUNWAY HOLD POSITION MARKING AS SHOWN IN DETAIL 2.
2. LOCATIONS REQUIRING REMARKING OVER EXISTING MARKINGS SHALL PRECISELY FOLLOW THE PATH OF EXISTING MARKINGS UNLESS THE NEW MARKING LOCATIONS SHOWN IN THE PLANS DEVIATE SIGNIFICANTLY FROM THE ORIGINAL MARKINGS.

LEGEND

- EXIST. PAVEMENT MARKING
- PROP. TXY CENTERLINE MARKING (YELLOW W/ BLACK BORDER)
- PROP. HOLD POSITION MARKING (YELLOW W/ BLACK BORDER)
- PROP. RWY EDGE MARKING (WHITE W/ BLACK BORDER)
- EXISTING PAVEMENT MARKING TO BE REMOVED
- EXIST. TAXIWAY EDGE MARKING (YELLOW W/ BLACK BORDER)



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REALIGN TAXIWAY A, PHASE 3

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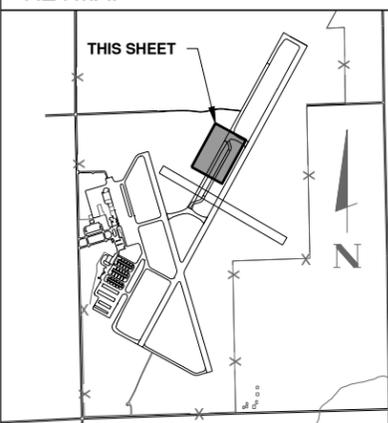
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SHEET TITLE
**PAVEMENT MARKING
2**

CM102
SHEET 44 OF 63

KEYMAP

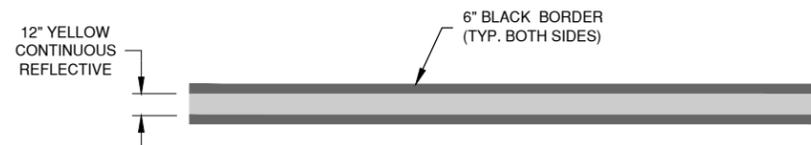


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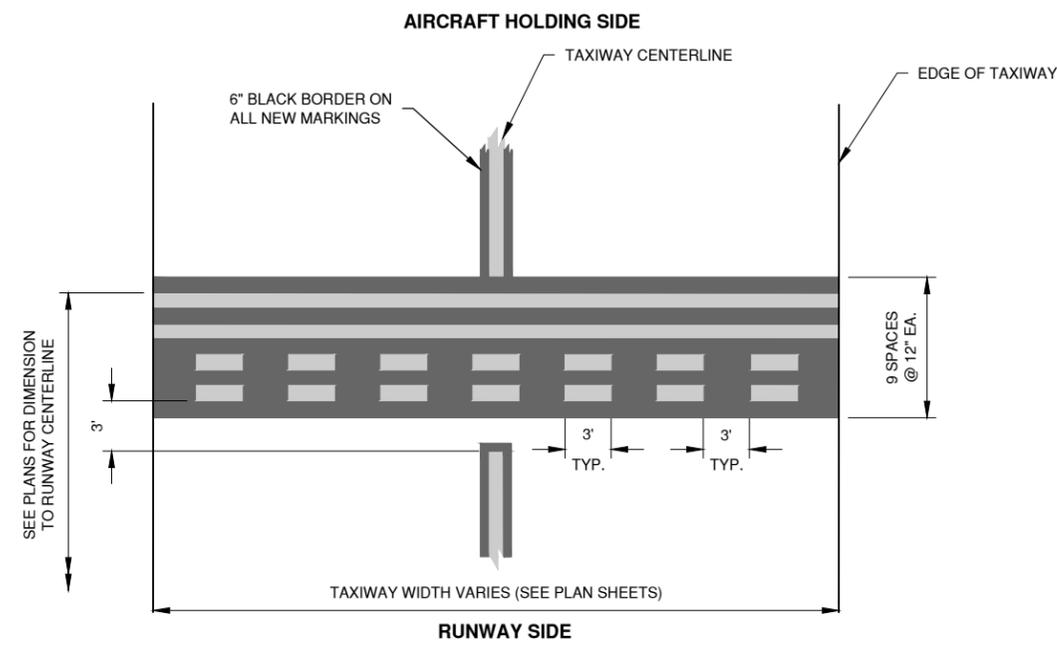
**BASE BID & ADD. ALT.
MARKING PLAN**

RUNWAY 3/21

MATCHLINE STA. 333+50



1 TAXIWAY CENTERLINE DETAIL
N.T.S.



2 RUNWAY HOLDING POSITION
N.T.S.

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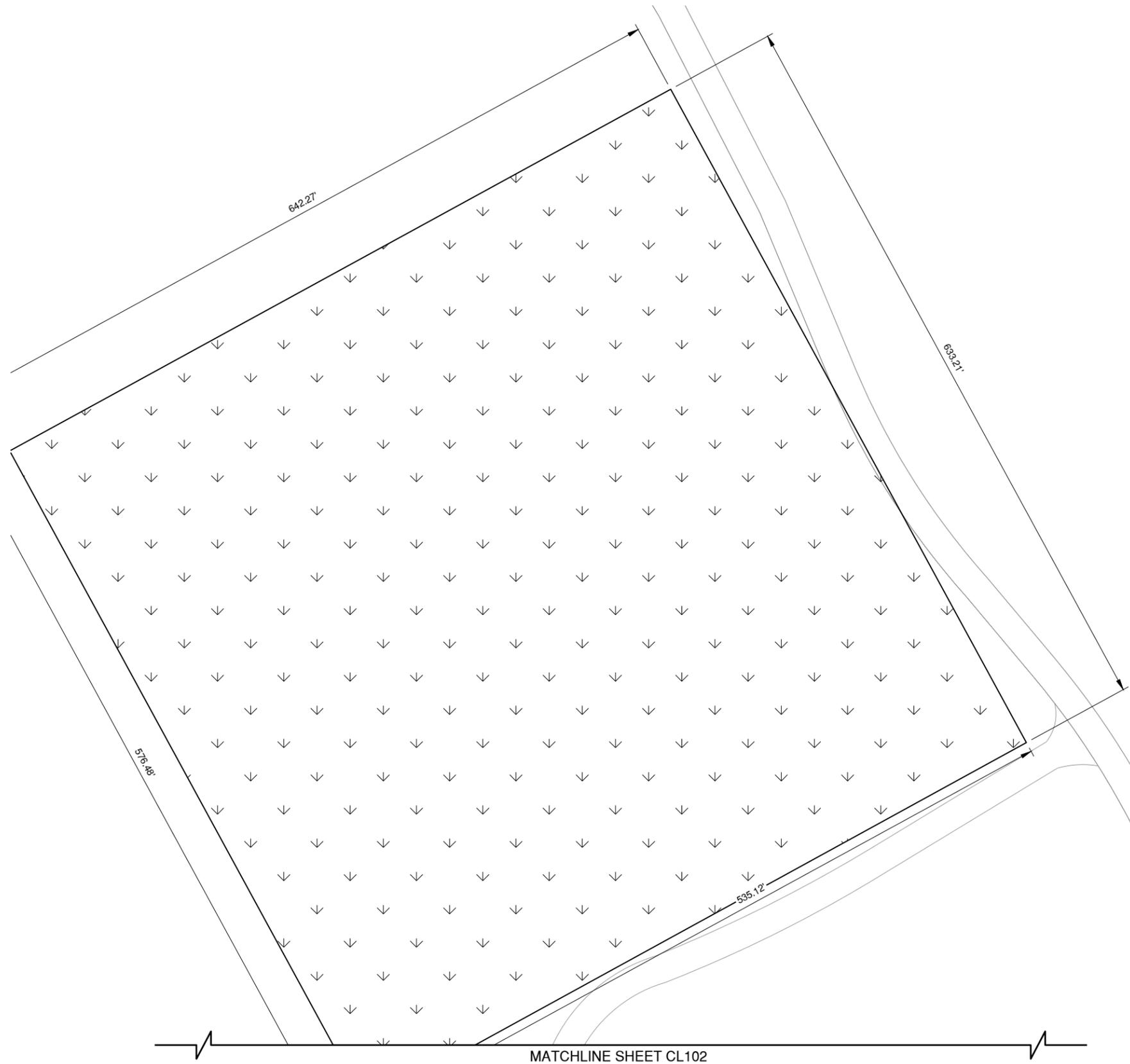
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**PAVEMENT MARKING
DETAILS**

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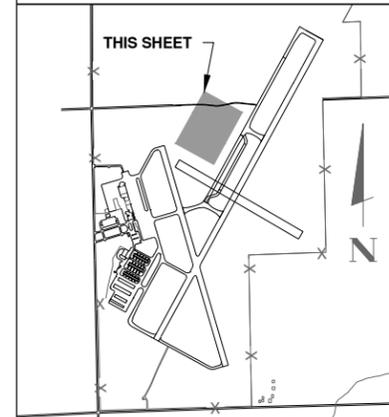


NOTES

LEGEND

-  SEEDING & MULCHING
-  SOD (4' TYPICAL WIDTH)
-  INLET PROTECTION
-  STRAW WATTLES
-  SILT FENCE

KEYMAP



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**EROSION CONTROL &
 TURFING 3**

CL103
 SHEET 48 OF 63



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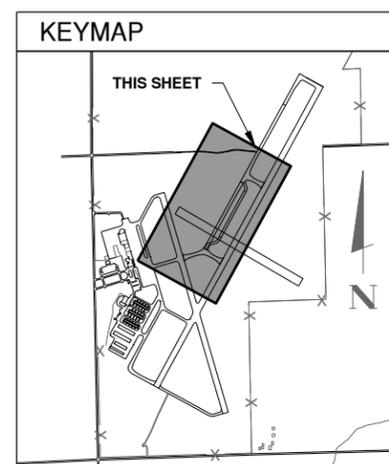
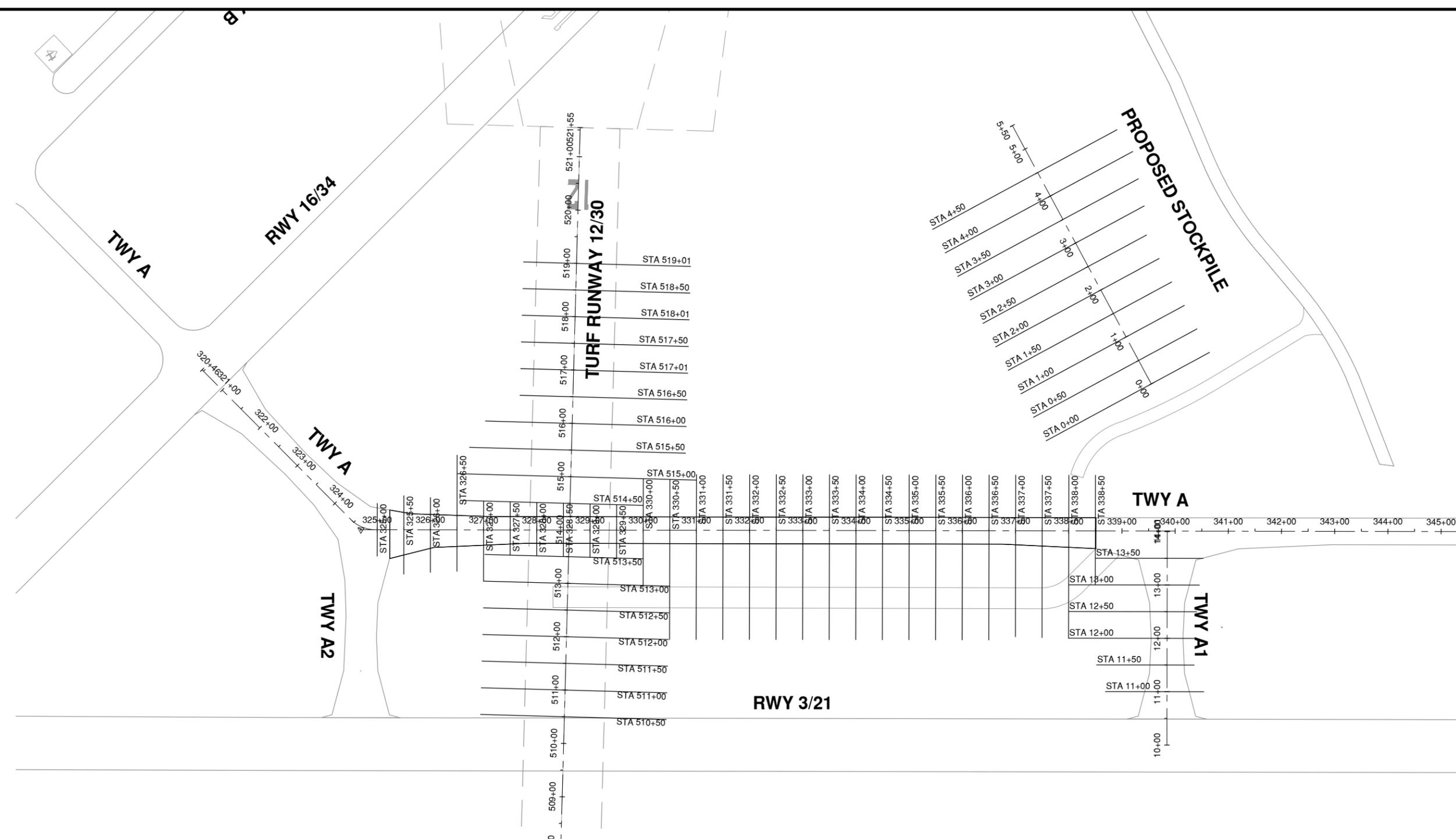
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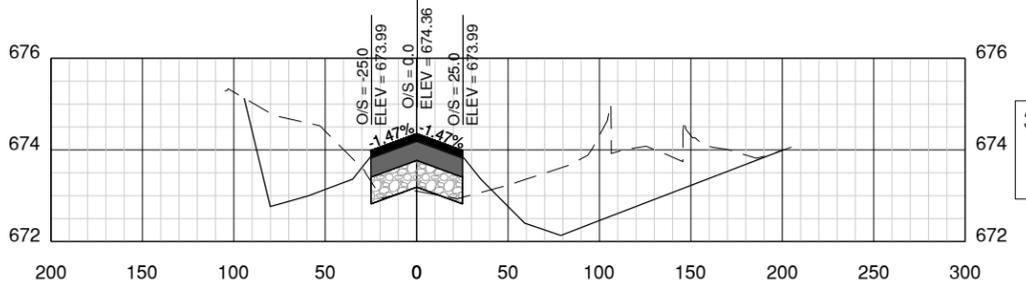
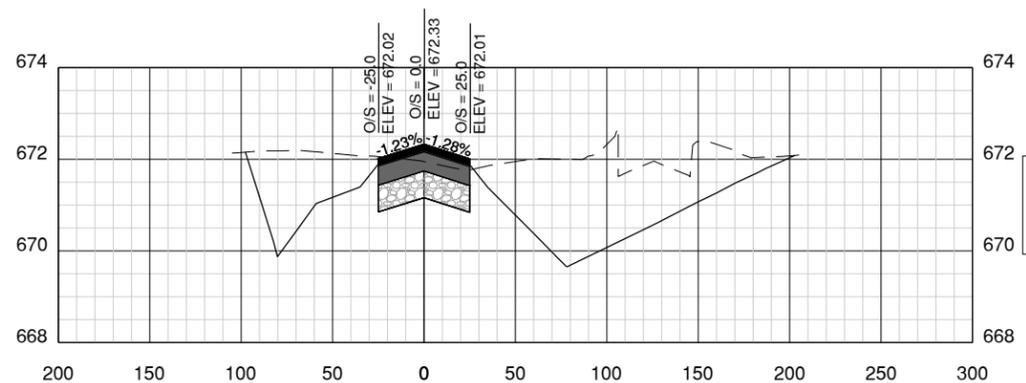
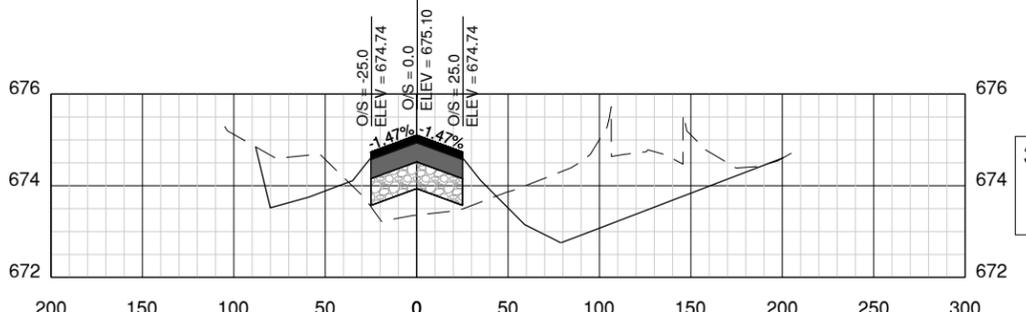
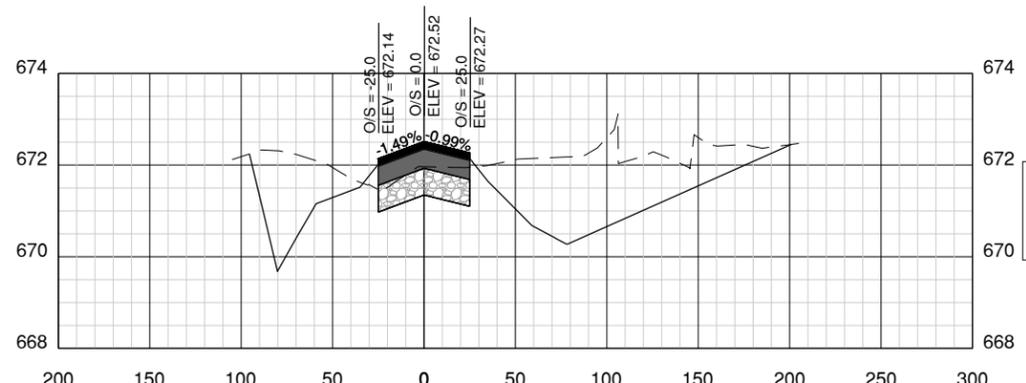
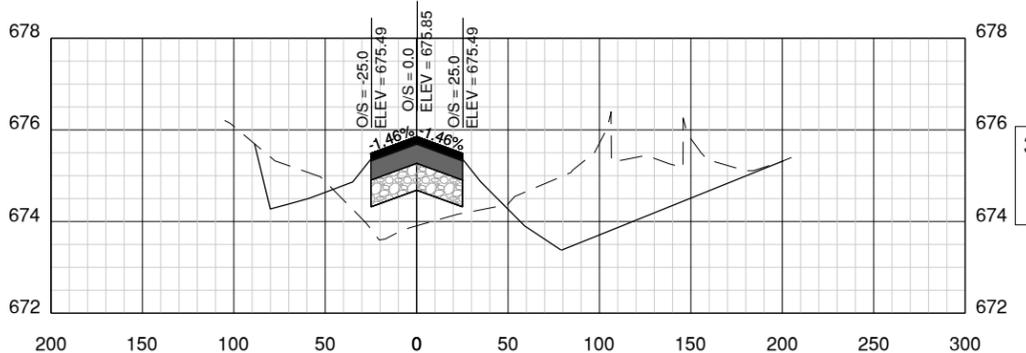
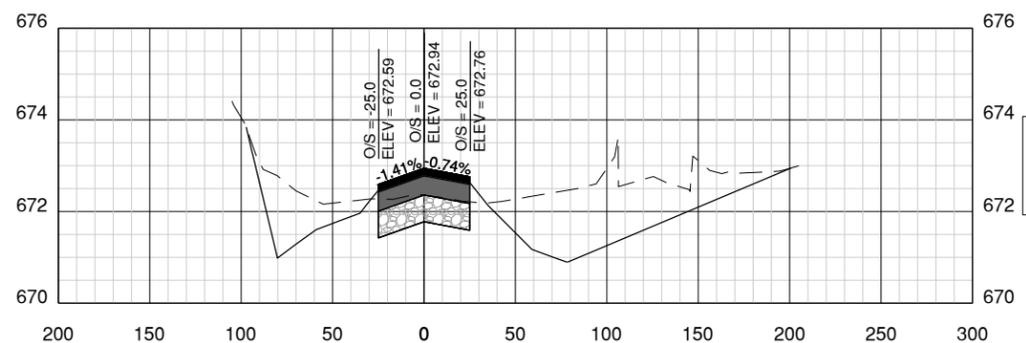
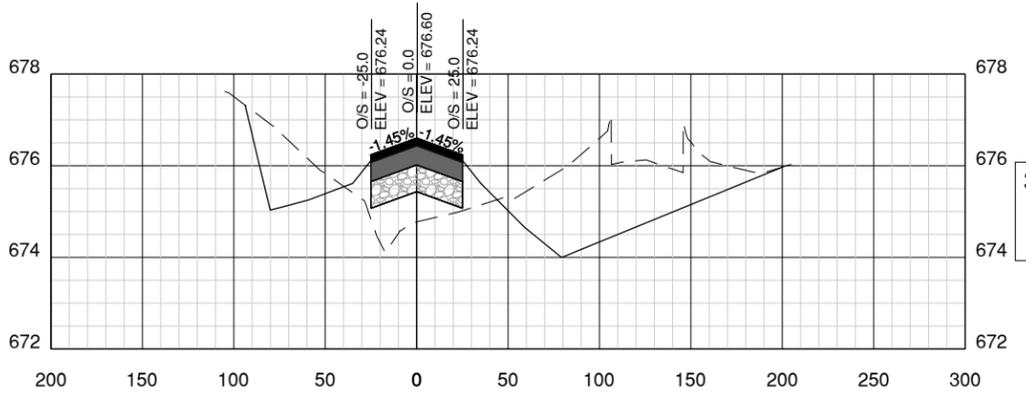
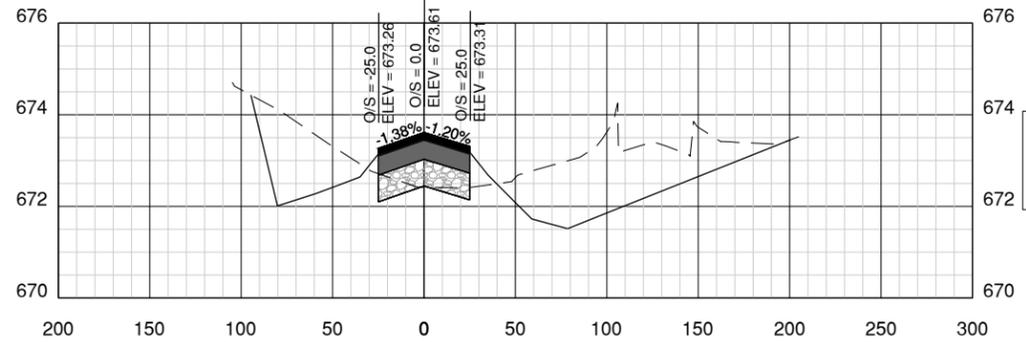
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SHEET TITLE
**CROSS SECTION
INDEX**

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SHEET 50 OF 63





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**PROPOSED TAXIWAY
A CROSS SECTION 2**

CG602
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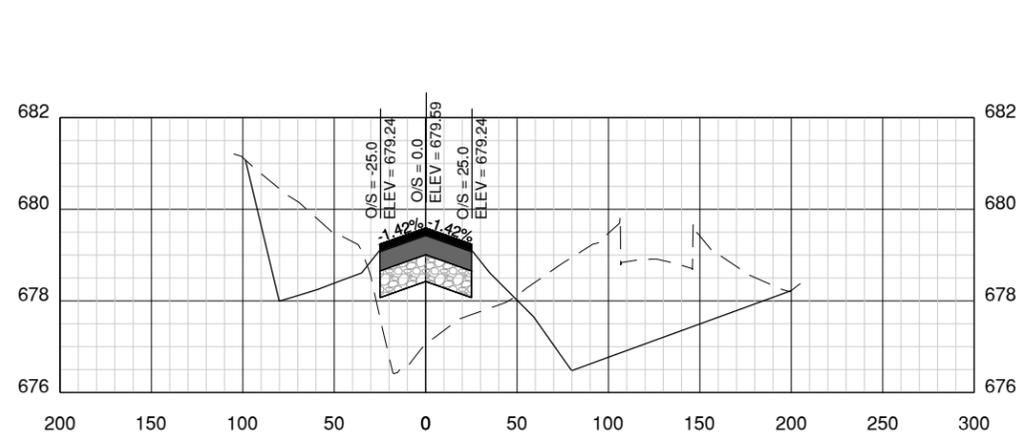
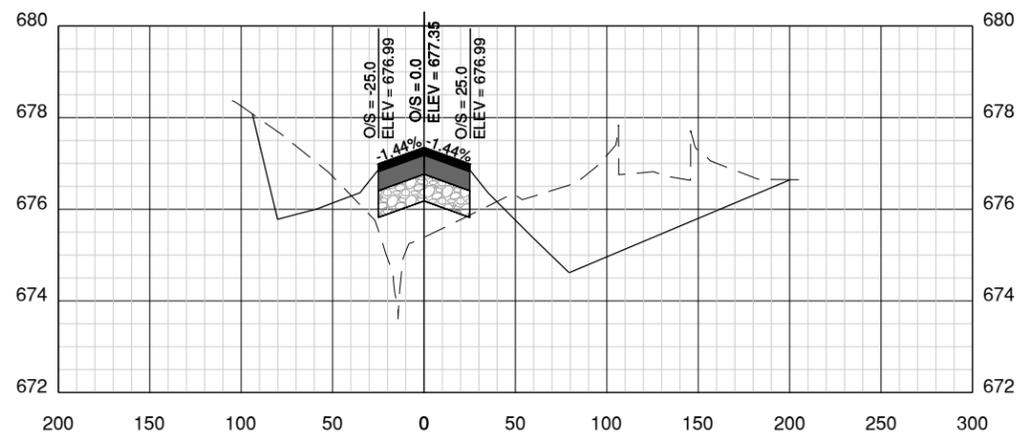
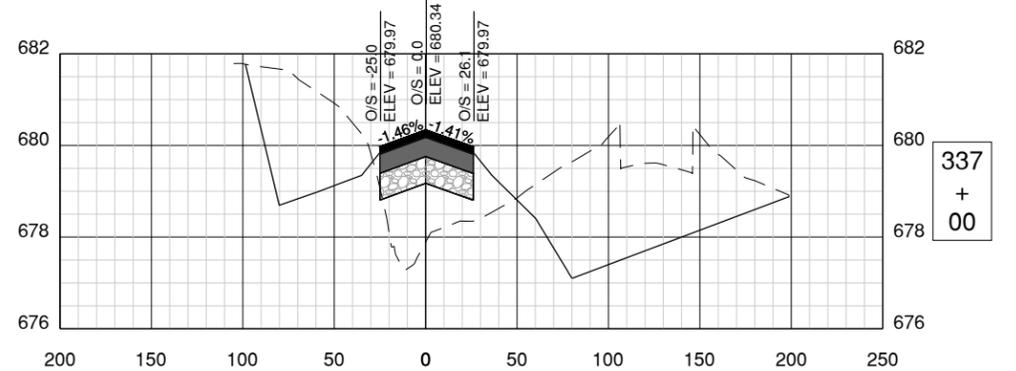
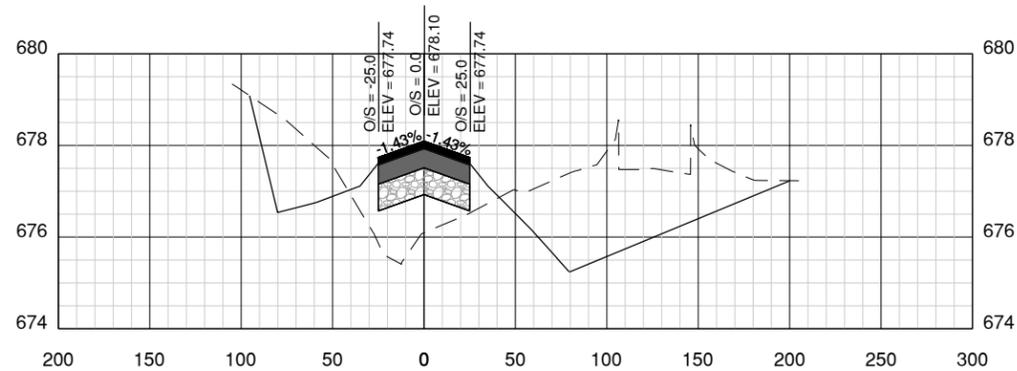
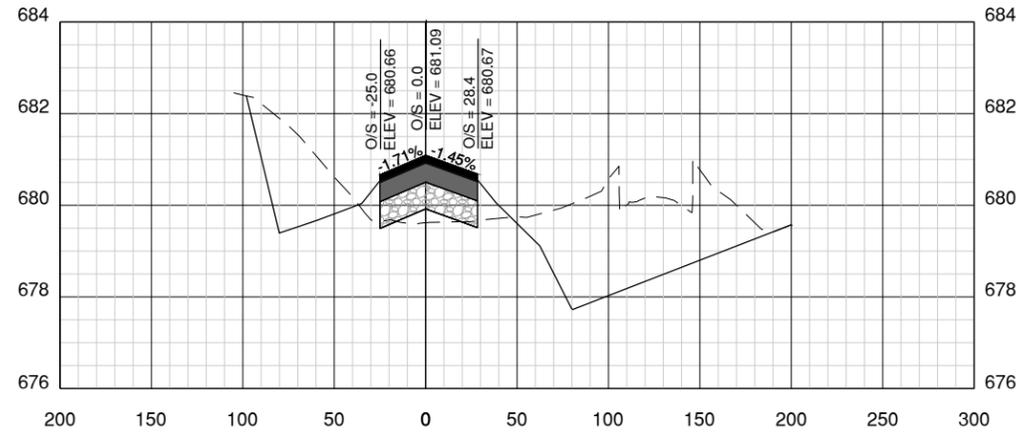
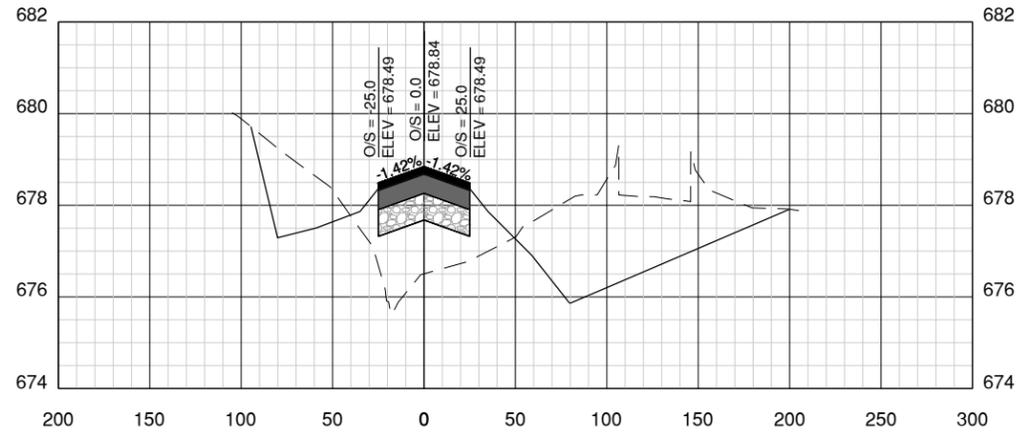
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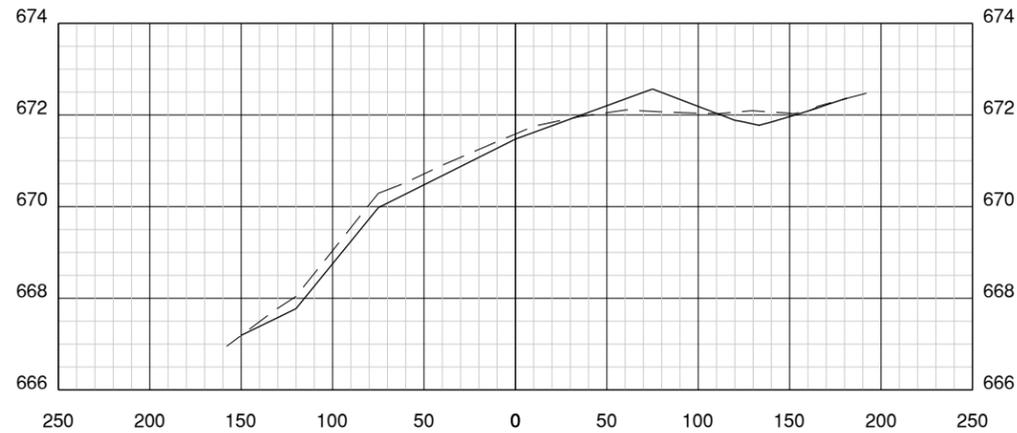
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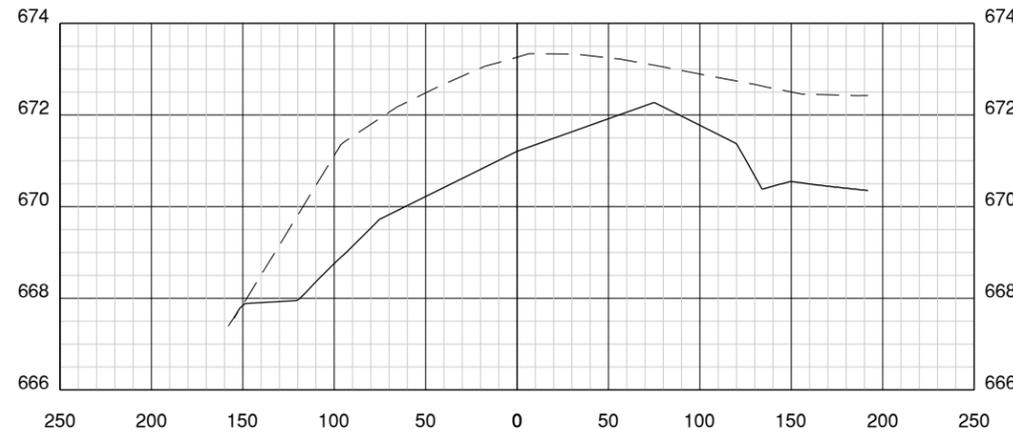
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A CROSS SECTION 3**

CG603
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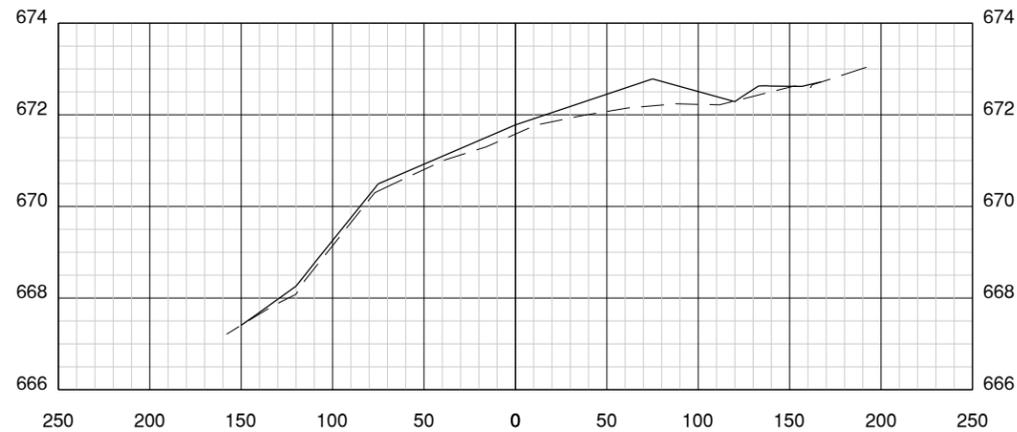




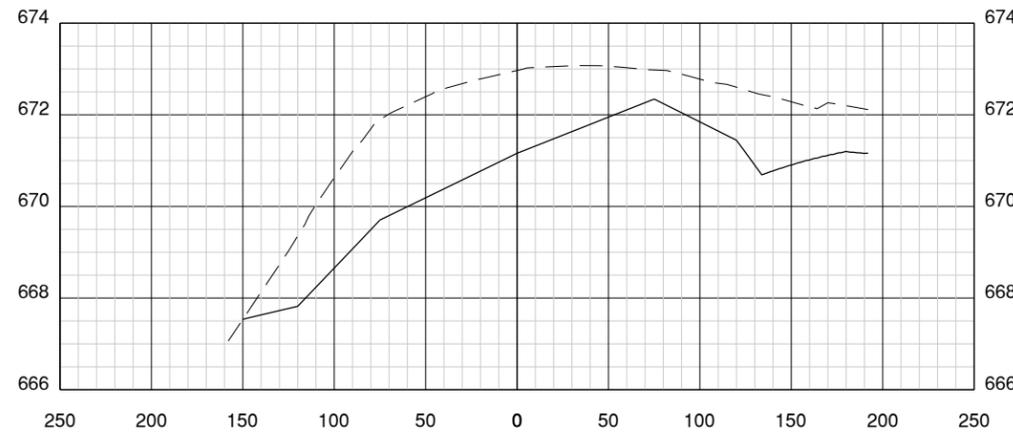
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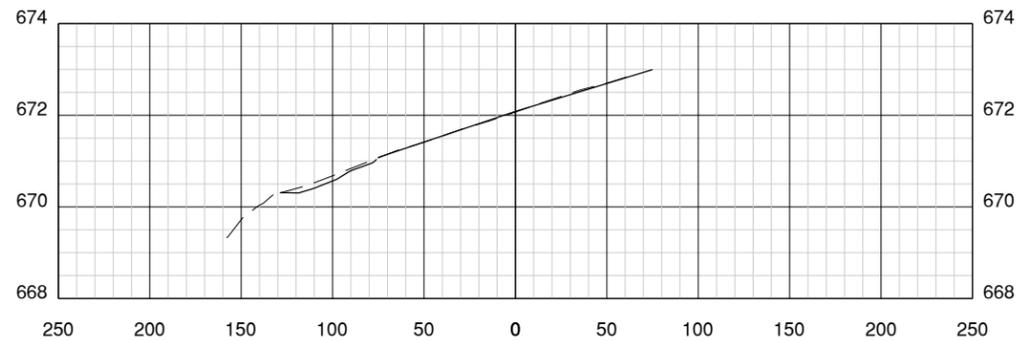
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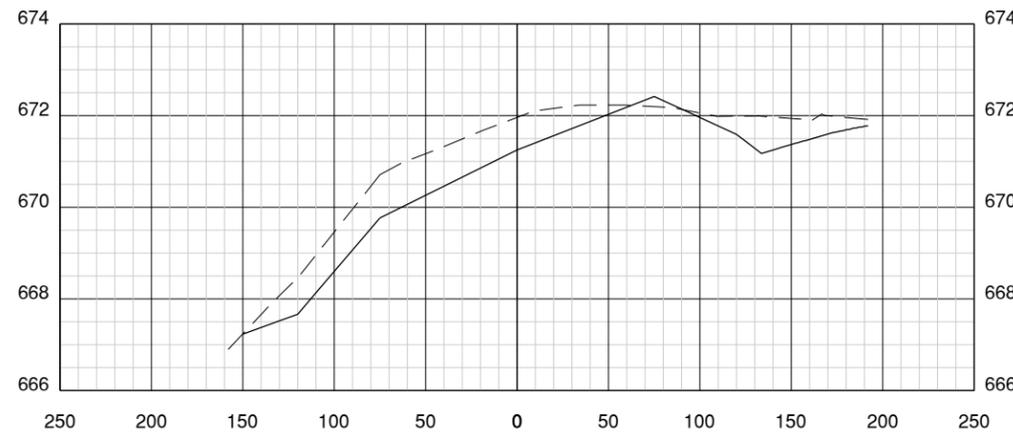
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SHEET TITLE
**PROPOSED TURF
RUNWAY SECTION 1**

CG611
SHEET **55** OF **63**



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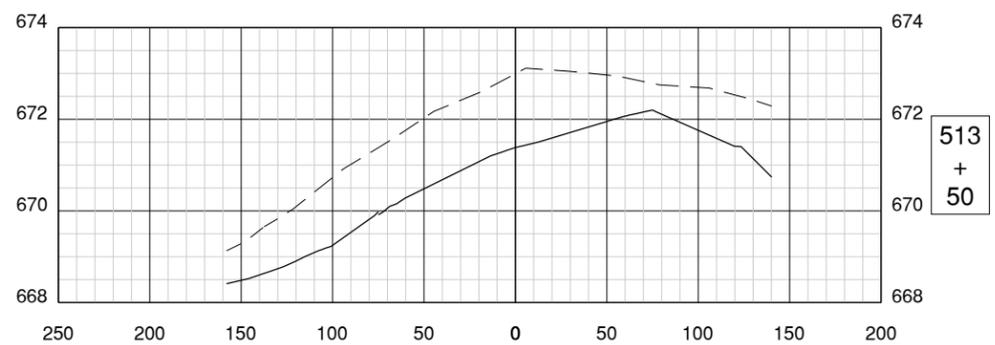
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PROPOSED TURF
RUNWAY SECTION 2

CG612

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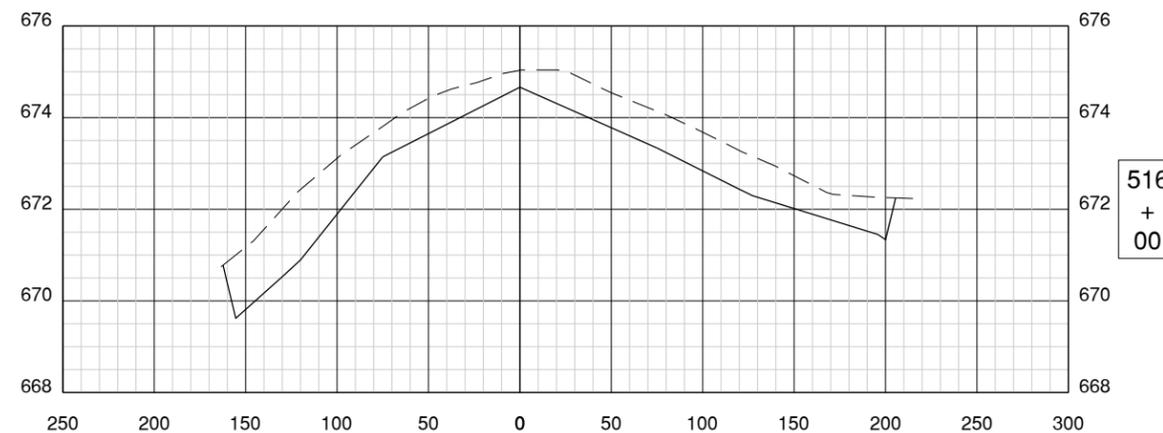
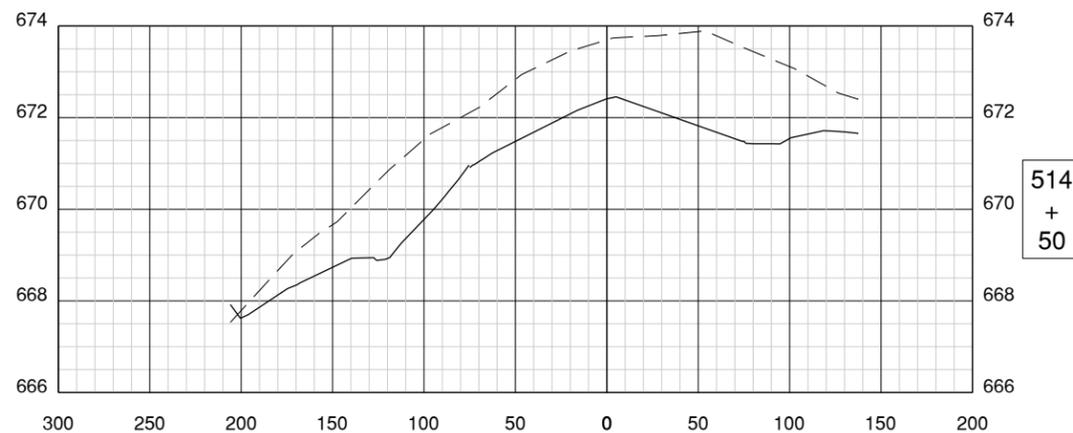
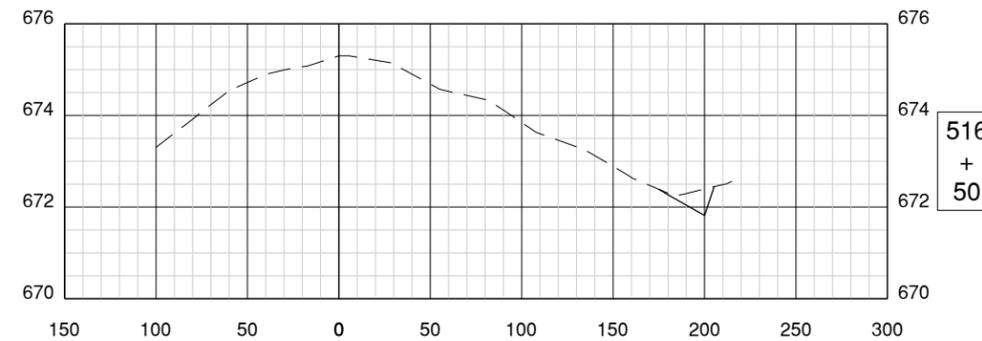
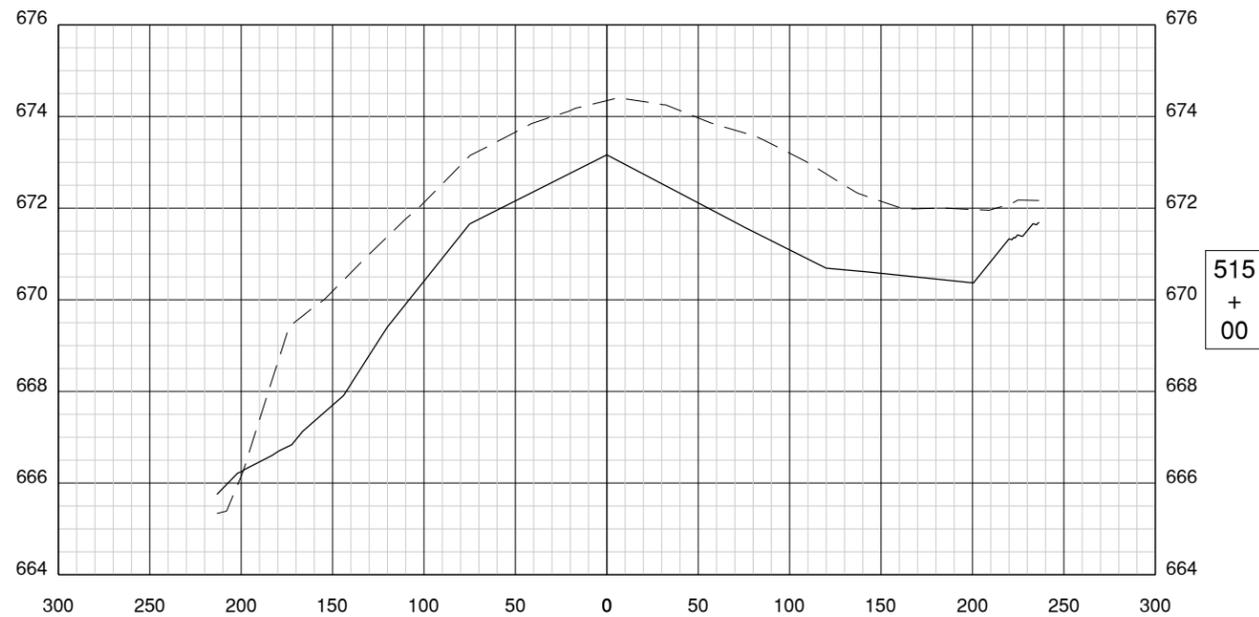
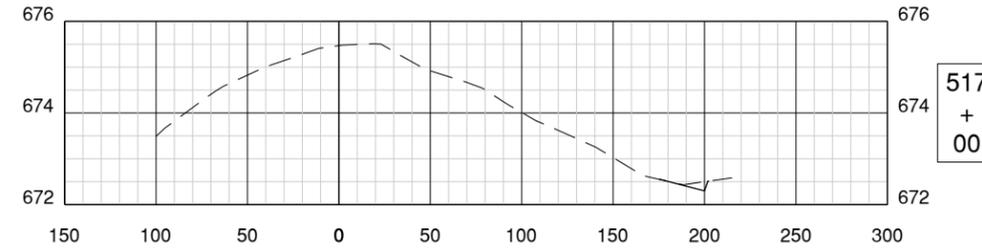
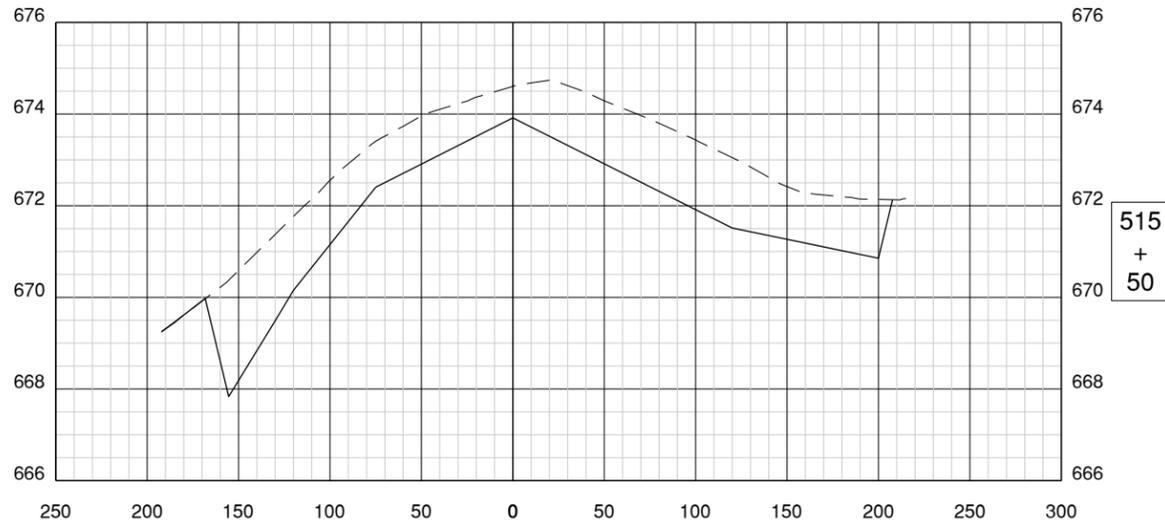
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PROPOSED TURF
RUNWAY SECTION 3

CG613

SHEET 57 OF 63



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IL. PROJ. NO: DNV-5169

CMT PROJECT NO: 190042-02-30

CAD DWG FILE: 19004202-PH3-GC300.DWG

DESIGNED BY: HCH

DRAWN BY: DPA

CHECKED BY: MJD

APPROVED BY: EMH

COPYRIGHT:

SHEET TITLE

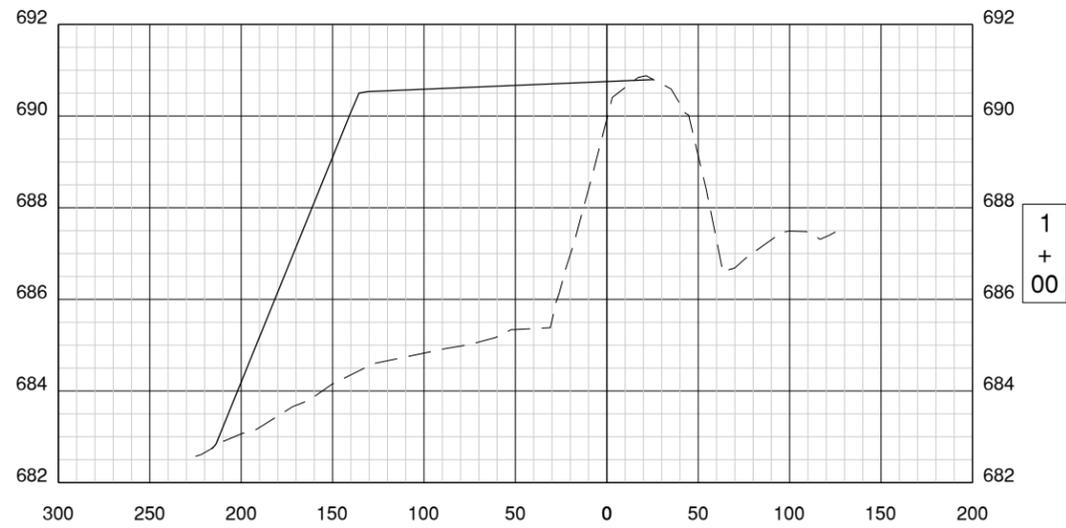
**PROPOSED
STOCKPILE SECTION 1**

CG631

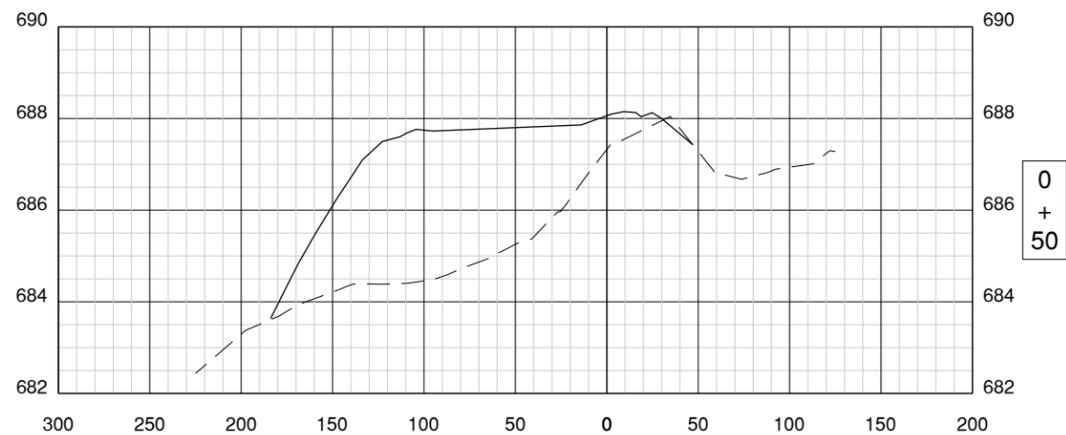
SHEET 60 OF 63

NOTES

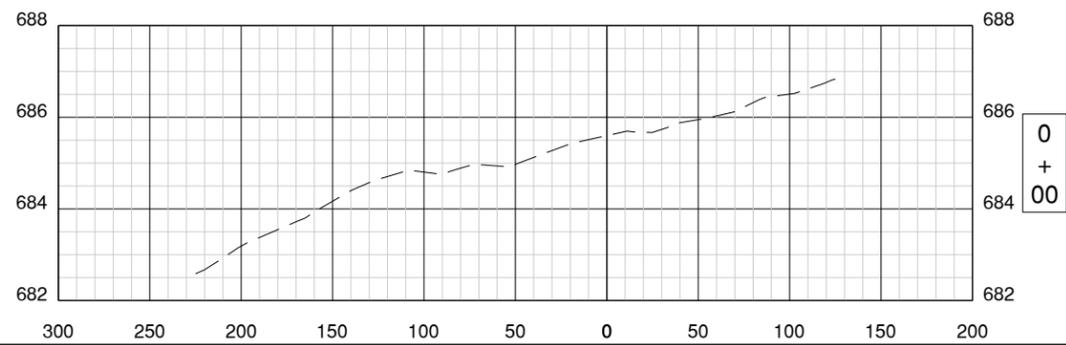
1. THE CROSS SECTIONS PROVIDED FOR STOCKPILE GRADING ARE FOR REFERENCE ONLY. THE CONTRACTOR SHALL FOLLOW THE BENCHING METHOD DETAILED ON SHEET CG633 FOR MATERIAL PLACEMENT.
2. THE CONSTRUCTION OF THE STOCKPILE IS INCIDENTAL TO THE UNCLASSIFIED EXCAVATION PAY ITEM AND WILL NOT BE MEASURED SEPARATELY FOR PAYMENT.



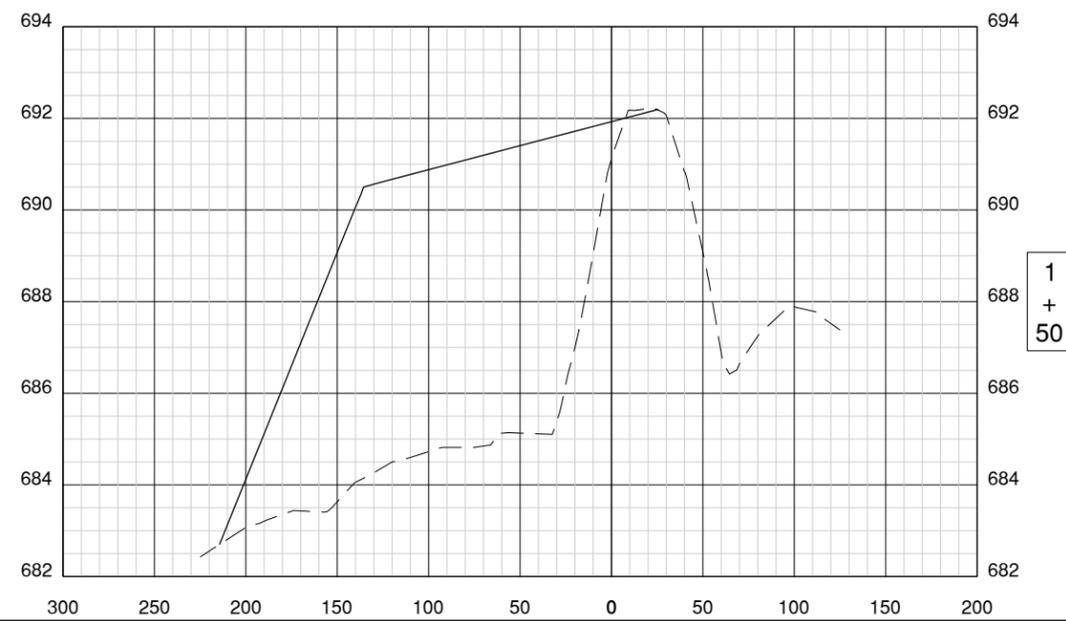
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