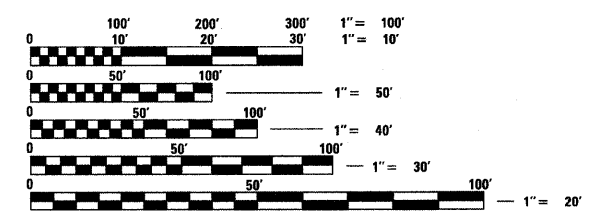


INDEX OF SHEETS
FOR INDEX OF SHEETS, SEE SHEET NO. 2
PROJECT LOCATED IN VILLAGE OF WILMETTE

DESIGN DESIGNATION

	ADT (2008)	POSTED SPEED MPH	DESIGN SPEED MPH
GREEN BAY ROAD (MINOR ARTERIAL)	18,000	30	35
WILMETTE AVENUE (URBAN COLLECTOR)	8,500	30	30
CENTRAL AVENUE (URBAN LOCAL ST.)	7,100	30	30
LAKE AVENUE (MINOR ARTERIAL)	17,600	30	30

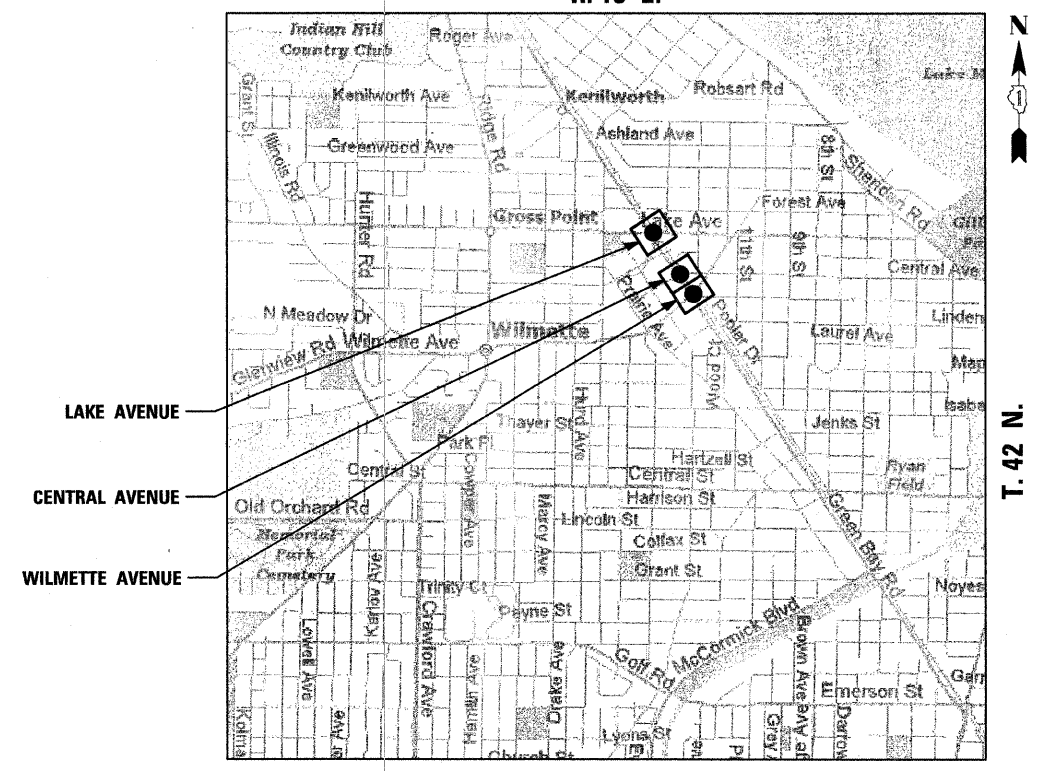


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 83999

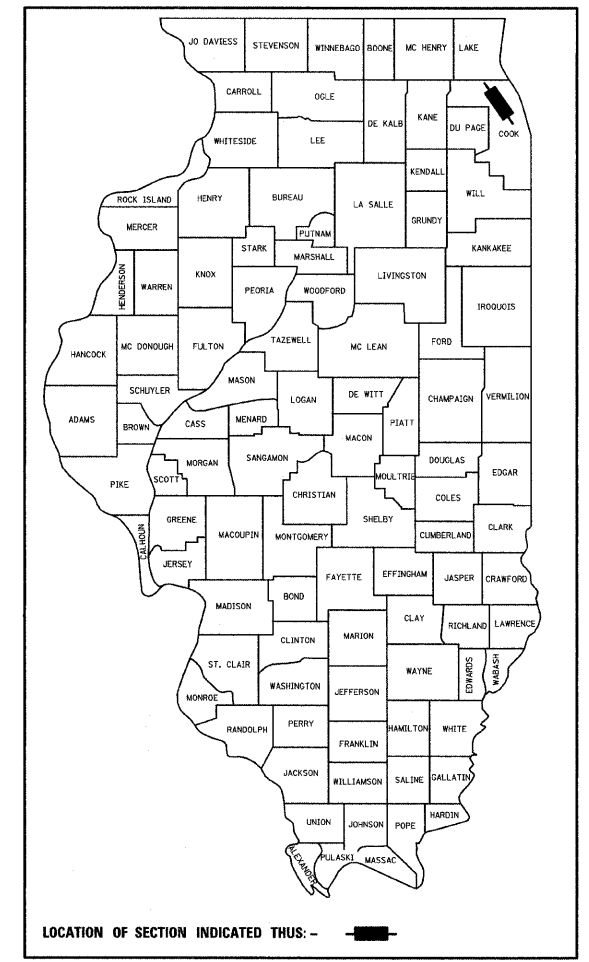
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
LOCAL AGENCY IMPROVEMENT
FAU ROUTE 2744 (GREEN BAY ROAD)
INTERSECTIONS WITH WILMETTE, CENTRAL AND LAKE AVENUES
VILLAGE OF WILMETTE
SECTION 06-00183-00-CH
PROJECT NO.: CMM-8003 (778)
WIDENING, RESURFACING AND TRAFFIC SIGNAL IMPROVEMENTS
COOK COUNTY
JOB NO: C-91-179-07
R. 13 E.



NOT TO SCALE
TOWNSHIP
GROSS LENGTH OF PROJECT = 1900 FT. = 0.36 MI.
NET LENGTH OF PROJECT = 1681 FT. = 0.32 MI.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	1
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83999

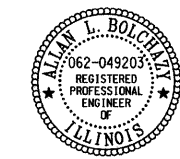


VILLAGE OF WILMETTE

APPROVED Dec. 31 20 07
Brigitte Mayerhoff
CITY ENGINEER

PASSED JANUARY 2 20 08
CHRISTOPHER HOLT
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW January 3 20 08
Dino O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER



Signed Allan L. Bolchazy
Allan L. Bolchazy, P.E., H. L. No. 062-049203 For drawings
Expires 11-30-2009 1-68
Date 12/24/07

TYLIN INTERNATIONAL
PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

ASSOCIATE FIELD ENGINEER: KEVIN STALLWORTH (847) 705-4169
CONSULTANT SERVICES ENGINEER: ALLAN BOLCHAZY, P.E. (312) 777-2900

PLAN	DESIGNED	DATE
	NOTED	
	ALIGNMENT CHECKED	
	BY	
	NO.	

PROFILE	DESIGNED	DATE
	NOTED	
	GRADES CHECKED	
	BY	
	NO.	

INDEX OF STATE STANDARDS

STD. NO.	TITLE
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-04	TEMPORARY EROSION CONTROL SYSTEM
353001-04	BASE COURSE, PCC WITH HMA BINDER AND SURFACE COURSES
424001-05	CURB RAMPS FOR SIDEWALKS
442101-07	CLASS B PATCHES
442201-03	CLASS C AND D PATCHES
602011	CATCH BASIN,TYPE C
602301-01	INLET, TYPE A
602401-01	MANHOLE, TYPE A
602701-01	CAST IRON STEPS
604001-02	FRAME AND LIDS, TYPE 1
604051-02	FRAME AND GRATE, TYPE 11
606001-03	CONCRETE CURB TYPE B AND CONCRETE COMBINATION CURB & GUTTER
606301-03	PC CONCRETE ISLANDS AND MEDIANS
701311-02	LANE CLOSURE 2L, 2 WAY MOVING OPERATIONS - DAY ONLY
701501-04	URBAN LANE CLOSURE, 2 LANE, 2 WAY, UNDIVIDED
701601-05	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-05	URBAN LANE CLOSURE, MULTILANE, 2 WAY WITH MOUNTABLE MEDIAN
701701-05	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-03	LANE CLOSURE MULTILANE 1 WAY OR 2 WAY CROSSWALK OR SIDEWALK CLOSURE
701901	TRAFFIC CONTROL DEVICES
704001-04	TEMPORARY CONCRETE BARRIER
720001	SIGN PANEL MOUNTING DETAILS
720006-01	SIGN PANEL ERECTION DETAILS
720011	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
728001	TELESCOPING STEEL SIGN SUPPORT
729001	APPLICATION OF TYPE A AND TYPE B METAL POSTS
731001	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-01	TYPICAL PAVEMENT MARKINGS
814001-01	HANDHOLES
814006-01	DOUBLE HANDHOLES
857001	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCE
873001-01	TRAFFIC SIGNAL GROUNDING AND BONDING
857006	SUPERVISED RAILROAD INTERCONNECT CIRCUIT
876001-00	PEDESTRIAN PUSH BUTTON POST
877001-03	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
877006-02	STEEL MAST ARM ASSEMBLY AND POLE, DUAL MAST ARMS
878001-06	CONCRETE FOUNDATION DETAILS
880006	TRAFFIC SIGNAL MOUNTING DETAILS
886001	DETECTOR LOOP INSTALLATION
886006	TYPICAL LAYOUT FOR DETECTION LOOPS

INDEX OF SHEETS

SHEET NO.	SHEET TITLE
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2	INDEX OF STATE STANDARDS, INDEX OF SHEETS
3	GENERAL NOTES
4-7	SUMMARY OF QUANTITIES
8-9	EXISTING AND PROPOSED TYPICAL SECTIONS
10	ALIGNMENT AND TIES FOR CONTROL POINTS
11-13	REMOVAL PLAN AND PROPOSED PLAN
14	MAINTENANCE OF TRAFFIC NOTES
15-18	MAINTENANCE OF TRAFFIC PLANS - STAGES 1 & 2 (LAKE)
19	MAINTENANCE OF TRAFFIC PLAN (CENTRAL)
20	MAINTENANCE OF TRAFFIC PLAN (WILMETTE)
21-22	DETOUR SHEETS
23-25	DRAINAGE AND UTILITY PLANS
26	DRAINAGE SCHEDULE
27-29	PAVEMENT MARKING, SIGNING/RESTORATION PLANS
30	DETAILS SHEETS
31-36	TRAFFIC SIGNAL PLANS (LAKE)
37-40	TEMPORARY TRAFFIC SIGNAL PLANS (LAKE)
41-46	TRAFFIC SIGNAL PLANS (CENTRAL)
47-52	TRAFFIC SIGNAL PLANS (WILMETTE)
53-57	TRAFFIC SIGNAL PLANS (LINDEN)
58	INTERCONNECT PLAN, INTERCONNECT SCHEMATIC
59	BD-01, DRIVEWAY DETAILS
60	BD-07, DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
61	BD-08, DETAIL FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
62	BD-32, BUTT JOINT AND HMA TAPER DETAILS
63	BE-702, MISCELLANEOUS ELECTRICAL DETAILS SHEET
64	TC-10, DISTRICT ONE TRAFFIC CONTROL AND PROTECTION SIDE ROADS
65	TC-13, DISTRICT ONE TYPICAL PAVEMENT MARKINGS
66	TC-16, PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
67	TC-23, TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS
68	TC-26, DRIVEWAY ENTRANCE SIGNING
69-72	TS-05, STANDARD TRAFFIC SIGNAL DESIGN DETAILS
73	BDE PROCEDURE MEMORANDUM 45-05, TYPICAL TRAFFIC SIGNAL PRE-SIGNALS AT RAILROAD GRADE CROSSINGS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	00183 0101 01	COOK	73	2
STA.	TO STA.			
FED. ROAD	ST. NO.	MINOTS	FED. AID PROJECT	
CONTRACT NO. 83999				

REVISIONS	NAME	DATE	INDEX OF STATE STANDARDS, & INDEX OF SHEETS	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
				SHEET NO.	
				DRAWN	
				CHECKED	
				APPROVED	
				DATE: 02-14-2008	
CONSULTANT			TYLIN INTERNATIONAL	SCALE: NONE	
JOB NO.				PROJECT NO.	

PLAN	DESIGNED	DATE
	NOTED	BY
	ALIGNED	CHECKED
	NOTE BOOK	NO.

PROFILE	DESIGNED	DATE
	NOTED	BY
	ALIGNED	CHECKED
	NOTE BOOK	NO.

GENERAL NOTES:

1.

THE CONTRACTOR SHALL MAINTAIN ALL ROADWAYS OPEN TO TRAFFIC AS SHOWN PER THE MAINTENANCE OF TRAFFIC DETAILS.
2.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800) 892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, CABLE, AND GAS FACILITIES (48 HOURS NOTIFICATION IS REQUIRED).
3.

CONTACT VILLAGE ENGINEER, VILLAGE OF WILMETTE (847) 853-7660 FOR WATER, SEWER AND VILLAGE ELECTRIC LOCATIONS.
4.

THE CONTRACTOR SHALL NOT BE ALLOWED TO ERECT A YARD OR FIELD OFFICE ON VILLAGE OR RAILROAD RIGHT-OF-WAY OR PROPERTY WITHOUT WRITTEN PERMISSION FROM THE VILLAGE OF WILMETTE OR THE UNION PACIFIC RAILROAD.
5.

THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING THE PROPER CURING TIME FOR THE VARIOUS HMA LIFTS.
6.

ALL ELEVATIONS REFER TO U.S.G.S. MEAN SEA LEVEL DATUM 1929.
7.

NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS AND EROSION CONTROL MEASURES ARE MET.
8.

ALL UTILITIES, SCHOOL DISTRICTS, PARK DISTRICT, LOCAL POLICE, AND FIRE DEPARTMENTS SHALL BE NOTIFIED IN WRITING BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.
9.

UNLESS AUTHORIZED BY THE ENGINEER, ALL EXISTING ACCESS POINTS SHALL BE MAINTAINED AT ALL TIMES BY THE CONTRACTOR.
10.

TREES NOT MARKED FOR REMOVAL SHALL BE CONSIDERED AS DESIGNATED TO BE SAVED AND SHALL BE PROTECTED UNDER THE PROVISIONS OF ARTICLE 201.05 OF THE STANDARD SPECIFICATIONS.
11.

ALL LIMBS, BRANCHES, AND OTHER DEBRIS RESULTING FROM THIS WORK SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN EXPENSE OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY.
12.

ALL CLEARING, REMOVAL OF BUSHES, HEDGES AND TREES UNDER SIX (6) INCHES IN DIAMETER WILL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR EARTH EXCAVATION.
13.

DURING CONSTRUCTION, THE CONTRACTOR WILL BE REQUIRED, AT HIS EXPENSE, TO HAVE AVAILABLE A WATER TRUCK OR SIMILAR EQUIPMENT TO CONTROL DUST. IF NECESSARY, THE CONTRACTOR SHALL BE REQUIRED TO CONTROL DUST DURING NON-WORKING HOURS.
14.

THE CONTRACTOR SHALL MAKE PRIOR ARRANGEMENTS WITH THE VILLAGE OF WILMETTE WATER DEPARTMENT PRIOR TO THE USE OF ANY FIRE HYDRANT. HYDRANT METERS AND RPZ'S MUST BE USED. CONTACT THE ENGINEERING DEPARTMENT AT (847) 853-7660.
15.

ALL OPEN EXCAVATIONS SHALL BE ADEQUATELY COVERED (STEEL PLATED) OR FILLED IN AT THE END OF EACH DAY. NO OPEN EXCAVATIONS ARE ALLOWED OVERNIGHT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE ITEM REQUIRING EXCAVATION.
16.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL AND CLEAN UP OF MUD AND OTHER DEBRIS TRACKED ON ALL STREETS LEADING TO THE PROJECT SITE CAUSED BY THE CONSTRUCTION OPERATIONS. THE CLEAN UP IS AT THE DISCRETION OF AND SHALL BE DONE TO THE SATISFACTION OF THE VILLAGE ENGINEER.
17.

THE CONTRACTOR SHALL OBSERVE THE FOLLOWING WORKING HOURS:
WORK SHALL BE PERFORMED BETWEEN THE HOURS OF 7:00 AM AND 7:00 PM MONDAY THROUGH FRIDAY. WORK ON SATURDAYS SHALL BE LIMITED TO 8:00 AM TO 4:00 PM. NO WORK SHALL BE PERFORMED ON SUNDAYS OR THE FOLLOWING HOLIDAYS: NEW YEARS DAY, MEMORIAL DAY, 4th OF JULY, LABOR DAY, THANKSGIVING AND CHRISTMAS. ANY LANE CLOSURES ON GREEN BAY ROAD SHALL BE LIMITED TO THE HOURS OF 9:00 AM TO 3:30 PM. ANY ADJUSTMENTS SHALL BE APPROVED IN ADVANCE BY THE VILLAGE ENGINEER.
18.

ALL TRENCHES, HOLES OR OTHER EXCAVATIONS REQUIRED FOR UTILITY INSTALLATION SHALL BE BACK-FILLED AND STABILIZED AT THE END OF EACH WORKING DAY. NO EXCAVATION SHOULD BE OPENED MORE THAN WHAT CAN BE STABILIZED BY THE END OF THE SAME DAY.
19.

ALL WASTE GENERATED AS A RESULT OF CONSTRUCTION ACTIVITIES (CONCRETE TRUCK WASHOUT, CHEMICALS, LITTER, SANITARY WASTE OR ANY OTHER WASTE) SHALL BE PROPERLY DISPOSED OF AND BE PREVENTED FROM BEING CARRIED OFF THE SITE BY EITHER WIND, WATER, OR CONSTRUCTION VEHICLES.
20.

THE LOCATION AND ELEVATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE MUNICIPALITIES DO NOT GUARANTEE ITS ACCURACY. THE CONTRACTOR WILL BE REQUIRED TO ASCERTAIN THE EXACT LOCATIONS OF SUCH FACILITIES SO AS NOT TO DAMAGE THEM IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND ARTICLE 105.07 OF THE STANDARD SPECIFICATIONS. THEIR FACILITIES MAY BE REQUIRED TO BE ADJUSTED OR RELOCATED.
21.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE OR DESTRUCTION OF PUBLIC OR PRIVATE PROPERTY, AND SHALL RESTORE SUCH PROPERTY AT HIS OWN EXPENSE. COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT A PRE-CONSTRUCTION CONFERENCE. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTIVE MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION. IN PARTICULAR, THE CONTRACTOR WILL TAKE ADEQUATE MEASURES TO PREVENT THE UNDERMINING OF UTILITIES AND SEWERS WHICH ARE STILL IN SERVICE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

GENERAL NOTES (CONT.):

22.

WHENEVER THE CONTRACTOR ENCOUNTERS FACILITIES AND APPURTENANCES WITHIN THE LIMITS OF THE IMPROVEMENTS DURING TRENCHING OPERATIONS, HE WILL BE REQUIRED TO HAND TRENCH IN THAT AREA IN ORDER NOT TO DAMAGE THE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
23.

THE CONTRACTOR WILL BE REQUIRED TO DISPOSE OF ALL SIDEWALK, CURB AND GUTTER, PAVEMENT, AND ALL OTHER MATERIAL EXCAVATED OR REMOVED, DUE TO CONSTRUCTION OPERATIONS, AT HIS EXPENSE. ALL EXCESS EXCAVATED MATERIAL SHALL BE REMOVED FROM SITE ON THE DAY IT IS EXCAVATED. NO PAYMENT WILL BE MADE FOR HAULING OR TRUCKING MATERIAL TO LOCATIONS, PROVIDED BY THE CONTRACTOR, OUTSIDE THE LIMITS OF THE IMPROVEMENT.
24.

DURING CONSTRUCTION OPERATIONS, WHENEVER ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED BY THE CONTRACTOR AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM ALL DIRT AND DEBRIS. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
25.

ONLY PRECAST CONCRETE ADJUSTMENT RINGS WILL BE ALLOWED IN THE ADJUSTMENT OR RECONSTRUCTION OF CATCH BASIN, MANHOLE, INLET, AND VALVE VAULT STRUCTURES. COMMON BRICK WILL NOT BE ALLOWED.
26.

FRAMES ON ALL NEW STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE COST. ALL FRAMES, GRATES, LIDS AND BOXES REMOVED FROM EXISTING WATER SERVICE OR SEWER STRUCTURES WHICH ARE TO BE ABANDONED OR ADJUSTED WITH A NEW OR DIFFERENT FRAME AND LID SHALL BECOME THE PROPERTY OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR DISPOSAL. ANY OF THESE ITEMS WHICH ARE DAMAGED OR BROKEN DURING HANDLING SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.
27.

THE CONTRACTOR WILL BE REQUIRED TO RELOCATE OR REMOVE AND REPLACE SIGNS WHICH INTERFERE WITH CONSTRUCTION OPERATIONS, AND TO TEMPORARILY RESET ALL SUCH SIGNS DURING CONSTRUCTION OPERATIONS AT THE DIRECTION OF THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT. SIGNS SHALL NOT BE MOVED UNTIL PROGRESS OF WORK NECESSITATES. EVERY SIGN REMOVED MUST BE RE-ERECTED TO A TEMPORARY LOCATION IN AN APPROVED MANNER AND BE VISIBLE TO TRAFFIC FOR WHICH IT IS INTENDED. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE PROJECT. ALL SIGNS SHALL BE RE-ERECTED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. HORIZONTAL LOCATION FROM THE EDGE OF PAVEMENT SHALL BE AS DESIGNATED BY THE ENGINEER. ALL UNUSED SIGNS WILL BE RETURNED TO THE AGENCY HAVING JURISDICTION.
28.

AN EXPANSION JOINT SHALL BE PLACED A MINIMUM OF THREE (3) FEET OF EITHER SIDE OF ADJACENT TREES. THE CURB GAP SHALL BE HAND FORMED IN ORDER TO PROTECT THE TREE ROOT SYSTEM. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
29.

THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SAND BAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL. FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR (4) WEIGHTED SANDBAGS.

RAILROAD GENERAL NOTES:

1.

WORK WINDOWS FOR ANY WORK ON UNION PACIFIC RAILROAD (UPRR) PROPERTY ARE:
MON-FRI: 9:00AM TO 3:30PM
SAT: 7:00AM TO 5:00PM
• - SUBJECT TO APPROVAL OF UPRR AREA MANAGER
2.

CONTRACTORS WILL BE REQUIRED TO OBTAIN 'RIGHT OF ENTRY' PERMIT BEFORE ENTERING RAILROAD PROPERTY.
3.

FLAGGING PROTECTION REQUIRED WHEN WORKING WITHIN 25 FEET OF NEAREST TRACK.
4.

FOR RAILROAD UTILITIES NOT COVERED BY JULIE:
RAILROAD LOCATES: 312-496-4738
FIBER OPTIC LOCATES: 800-336-9193

MWRD GENERAL NOTES:

1.

THE MWRD LOCAL SEWER SYSTEMS SECTION FIELD OFFICE MUST BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF ANY WORK (CALL 708-588-4055).
2.

ELEVATION DATUM IS USGS.
3.

THERE ARE NO FLOOR DRAINS ON THIS PROJECT.
4.

THERE ARE NO FOOTING DRAINS OR DOWNSPOUTS ON THIS PROJECT.
5.

ALL SANITARY SEWER PIPE MATERIALS AND JOINTS (AND STORM SEWER PIPE MATERIALS AND JOINTS IN A COMBINED SEWER AREA) SHALL CONFORM TO:

PIPE MATERIAL SPEC. JOINT SPEC.

<u>VITRIFIED CLAY PIPE</u>	
VCP C-700	C-425
VCP (NO-BEL) C-700	
JOINT	C-425
COLLAR	D-1784
<u>CONCRETE PIPE C-14</u>	
RCP C-76	C-443
ACP C-428	D-1869
<u>ABS SEWER PIPE</u>	
SOLID WALL 6" DIA. SDR 23.5	
ABS D-2751	D-2751
<u>ABS COMPOSITE/TRUSS PIPE</u>	
8"-15" DIA.	
ABS D-2680	D-2680
<u>PVC GRAVITY SEWER PIPE</u>	
6"-15" DIA. SDR 26	
D-3034	D-3212 OR D-2855
18"-27" DIA. F/DY=46	
F-679	D-3212 OR D-2855
CISP A-74	C-564
DIP A-21.51	A-21.11

(NOTE: THE DISTRICT HAS APPROVED LESS COMMON PIPE MATERIALS ON A QUALIFIED BASIS IN ADDITION TO THOSE ABOVE. PLEASE CONTACT THE DISTRICT IF CONSIDERING USING PIPE NOT LISTED ABOVE.)

6.

ALL STORM SEWER CONSTRUCTION IN COMBINED SEWER AREAS REQUIRES STONE BEDDING WITH STONE 1/4" TO 1" IN SIZE, WITH MINIMUM BEDDING THICKNESS EQUAL TO 1/4 THE OUTSIDE DIAMETER OF THE SEWER PIPE, BUT NOT LESS THAN FOUR (4) INCHES NOR MORE THAN EIGHT (8) INCHES. MATERIAL SHALL BE CA-11 OR CA-13 AND SHALL BE EXTENDED AT LEAST 12" ABOVE THE TOP OF THE PIPE WHEN USING PVC.
7.

"BAND SEAL" OR SIMILAR FLEXIBLE-TYPE COUPLINGS SHALL BE USED IN THE CONNECTION OF SEWER PIPES OF DISSIMILAR MATERIALS.
8.

WHEN CONNECTING TO AN EXISTING SEWER MAIN BY MEANS OTHER THAN AN EXISTING WYE, TEE, OR AN EXISTING MANHOLE, ONE OF THE FOLLOWING METHODS SHALL BE USED:
1. CIRCULAR SAW-CUT OF SEWER MAIN BY PROPER TOOLS ("SEWER-TAP" MACHINE OR SIMILAR) AND PROPER INSTALLATION OF HUBWYE SADDLE OR HUB-TEE SADDLE.
2. REMOVE AN ENTIRE SECTION OF PIPE (BREAKING ONLY THE TOP OF ONE BELL) AND REPLACE WITH A WYE OR TEE BRANCH SECTION.
3. WITH PIPE CUTTER, NEATLY AND ACCURATELY CUT OUT DESIRED LENGTH OF PIPE FOR INSERTION OF PROPER FITTING, USING "BAND SEAL" OR SIMILAR COUPLINGS TO HOLD IT FIRMLY IN PLACE.
9.

WHENEVER A SANITARY/COMBINED SEWER CROSSES UNDER A WATERMAIN, THE MINIMUM VERTICAL DISTANCE FROM THE TOP OF THE SEWER TO THE BOTTOM OF THE WATERMAIN SHALL BE 18 INCHES. FURTHERMORE, A MINIMUM HORIZONTAL DISTANCE OF 10 FEET BETWEEN SANITARY/COMBINED SEWERS AND WATERMANS SHALL BE MAINTAINED UNLESS; THE SEWER IS LAID IN A SEPARATE TRENCH, KEEPING A MINIMUM 18" VERTICAL SEPARATION; OR THE SEWER IS LAID IN THE SAME TRENCH WITH THE WATERMAIN LOCATED AT THE OPPOSITE SIDE
10.

ON A BENCH OF UNDISTURBED EARTH, KEEPING A MINIMUM 18" VERTICAL SEPARATION. IF EITHER THE VERTICAL OR HORIZONTAL DISTANCES DESCRIBED ABOVE CAN NOT BE MAINTAINED, OR THE SEWER CROSSES ABOVE THE WATERMAIN, THE SEWER SHALL BE CONSTRUCTED TO WATERMAIN STANDARDS.
11.

ALL SANITARY MANHOLES, (AND STORM MANHOLES IN COMBINED SEWER AREAS), SHALL HAVE A MINIMUM INSIDE DIAMETER OF 48 INCHES, AND SHALL BE CAST IN PLACE OR PRE-CAST REINFORCED CONCRETE.
12.

RESILIENT CONNECTORS, CONFORMING TO ASTM C-923, SHALL BE USED BETWEEN DRAINAGE AND SANITARY STRUCTURES AND PIPES.
13.

MAINTAIN A MINIMUM OF 2 FEET VERTICAL SEPARATION FROM THE OUTSIDE OF MWRD SEWERS AND A HORIZONTAL SEPARATION OF 10 FEET FROM MWRD STRUCTURES TO THE OUTSIDE OF PROPOSED SEWERS/FACILITIES.
14.

EXCAVATION WITHIN 2 FEET OF MWRD FACILITIES SHALL BE EITHER BY HAND OR VACUUM EXCAVATORS.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	3
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 83999	

REVISIONS		GENERAL NOTES	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS
NAME	DATE		
CONSULTANT		TYLIN INTERNATIONAL	SHEET NO. PROJECT NO.
			DRAWN:
			CHECKED:
			APPROVED:
			DATE: 02-14-2008
			SCALE: NONE
			JOB NO.:

PLAN

DATE

BY

DESIGNED

NOTED

CHECKED

DATE

FILE NAME

PROFILE

DATE

BY

DESIGNED

NOTED

CHECKED

DATE

FILE NAME

SUMMARY OF QUANTITIES

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	4
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				

IDOT CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	CONSTRUCTION TYPE CODES								VILLAGE OF WILMETTE
				I000-1A ROADWAY 80% IDOT 20% WILMETTE	Y030-1E LIGHTING 80% IDOT 20% WILMETTE	Y031-1F						
						TRAFFIC SIGNALS						
						LAKE AVE 80% IDOT 20% WILMETTE	CENTRAL AVE 80% IDOT 20% WILMETTE	WILMETTE AVE 80% IDOT 20% WILMETTE	LINDEN AVE 80% IDOT 20% WILMETTE	INTERCONNECT 80% IDOT 20% WILMETTE		
* 20100110	TREE REMOVAL (6-15 UNIT DIAMETER)	UNIT	42	42								
* 20101000	TEMPORARY FENCE	FOOT	120	120								
* 20200100	EARTH EXCAVATION	CU YD	749	749								
* 20800150	TRENCH BACKFILL	CU YD	9	9								
* 21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	198	198								
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	3	3								
* 25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	3	3								
* 25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	3	3								
* 25200110	SODDING, SALT TOLERANT	SQ YD	198	198								
* 25200200	SUPPLEMENTAL WATERING	UNIT	2.0	2.0								
* 28000510	INLET FILTERS	EACH	18	18								
* 31101200	SUB-BASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	1288	1288								
* 35301400	PORTLAND CEMENT CONCRETE BASE COURSE (VARIABLE DEPTH)	SQ YD	810	810								
* 40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	4.4	4.4								
* 40600300	AGGREGATE (PRIMECOAT)	TON	22	22								
* 40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	234	234								
* 40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	464	464								
* 42001300	PROTECTIVE COAT	SQ YD	1140	1140								
* 42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	21	21								
* 42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	174	174								
* 42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5"	SQ FT	2830	2830								
* 42400800	DETECTABLE WARNINGS	SQ FT	112	112								
* 44000100	PAVEMENT REMOVAL	SQ YD	425	425								
* 44000158	HOT-MIX ASPHALT SURFACE REMOVAL 2 1/4"	SQ YD	4852	4852								
* 44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	366	366								
* 44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1624	1624								
* 44000600	SIDEWALK REMOVAL	SQ FT	4109	4109								
* 44003100	MEDIAN REMOVAL	SQ FT	796	796								
* 44201474	CLASS C PATCHES, TYPE I	SQ YD	13	13								
* 44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	1450	1450								
* 550A2520	STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 2, 12"	FOOT	35	35								
* 55100500	STORM SEWER REMOVAL 12"	FOOT	20	20								
* 56400400	FIRE HYDRANTS TO BE RELOCATED	EACH	1	1								
* 60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH	1	1								
* 60234200	INLETS, TYPE A, TYPE 1 FRAME OPEN LID	EACH	2	2								
* 60250200	CATCH BASINS TO BE ADJUSTED	EACH	11	11								
* 60255500	MANHOLES TO BE ADJUSTED	EACH	20	20								
* 60257900	MANHOLES TO BE RECONSTRUCTED	EACH	6	6								
* 60260100	INLETS TO BE ADJUSTED	EACH	3	3								
* 60500060	REMOVING INLETS	EACH	2	2								
* 60600605	CONCRETE CURB, TYPE B	FOOT	215	215								

Δ SPECIALTY ITEMS

REVISIONS		SUMMARY OF QUANTITIES	SHEET 1 OF 4	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			DRAWN: CBS	
				CHECKED:	
				APPROVED:	
				DATE: 02-14-2008	
				SCALE: NONE	
				JOB NO.:	
				PROJECT NO.:	
				CONSULTANT	
				TYLIN INTERNATIONAL	

PLAN	DATE	BY	REVIEWED	CHECKED
			PLOTTED	ALIGNMENT
				NO. _____
				NO. _____

PROFILE	DATE	BY	REVIEWED	CHECKED
			PLOTTED	GRADES
				STRUCTURE
				NO. _____

SUMMARY OF QUANTITIES

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	5
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				

IDOT CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	CONSTRUCTION TYPE CODES								VILLAGE OF WILMETTE
				I000-1A ROADWAY 80% IDOT 20% WILMETTE	Y030-1E LIGHTING 80% IDOT 20% WILMETTE	Y031-1F						
						TRAFFIC SIGNALS						
						LAKE AVE 80% IDOT 20%WILMETTE	CENTRAL AVE 80% IDOT 20% WILMETTE	WILMETTE AVE 80% IDOT 20% WILMETTE	LINDEN AVE 80% IDOT 20% WILMETTE	INTERCONNECT 80% IDOT 20% WILMETTE		
* 60601105	CONCRETE CURB, TYPE M	FOOT	315	315								
* 60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1414	1414								
* 60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	2105	2105								
* 60623711	CONCRETE MEDIAN	SQ FT	289	289								
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6								
67100100	MOBILIZATION	L SUM	1	1								
* 70101800	TRAFFIC CONTROL AND PROTECTION (SPECIAL)	L SUM	1	1								
* 70102550	TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR	EACH	-2	2								
* 70106800	CHANGEABLE MESSAGE SIGN	CAL MO	3	3								
* 70300100	SHORT TERM PAVEMENT MARKING	FOOT	240	240								
* 70300510	PAVEMENT MARKING TAPE, TYPE III - LETTERS & SYMBOLS	SQ FT	109	109								
* 70300520	PAVEMENT MARKING TAPE, TYPE III - LINE 4"	FOOT	4502	4502								
* 70300560	PAVEMENT MARKING TAPE, TYPE III - LINE 12"	FOOT	51	51								
* 70300570	PAVEMENT MARKING TAPE, TYPE III - LINE 24"	FOOT	51	51								
* 70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	1763	1763								
* 72000100	SIGN PANEL - TYPE 1	SQ FT	275	275								
* 72400710	RELOCATE SIGN PANEL, TYPE 1	SQ FT	138	138								
* 72800100	TELESCOPING STEEL SIGN SUPPORT	FOOT	105	105								
* 72900100	METAL POST, TYPE A	FOOT	41	41								
* 72900200	METAL POST, TYPE B	FOOT	46	46								
* 73100100	BASE FOR TELESCOPING STEEL SIGN SUPPORT	EACH	8	8								
Δ * 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	874	874								
Δ * 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3775	3775								
Δ * 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	5637	5637								
Δ * 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	900	900								
Δ * 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	462	462								
* 78300100	PAVEMENT MARKING REMOVAL	SQ FT	3860	3860								
Δ XX006937	GROUND ROD, 5/8" DIAMETER x 10 FT.	EACH	6		6							
Δ 81000600	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL	FOOT	115			99	8	8				
Δ 81000700	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL	FOOT	54			50	4					
Δ 81000800	CONDUIT IN TRENCH, 3" DIA., GALVANIZED STEEL	FOOT	123		123							
Δ 81001100	CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL	FOOT	38			38						
Δ 81018500	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL	FOOT	70			35		35				
Δ 81018900	CONDUIT PUSHED, 4" DIA., GALVANIZED STEEL	FOOT	62				62					
Δ 81019100	CONDUIT PUSHED, 6" DIA., GALVANIZED STEEL	FOOT	60			60						
Δ 81400100	HANDHOLE	EACH	5			2	2	1				
Δ 81400200	HEAVY-DUTY HANDHOLE	EACH	5			4		1				
Δ 81400300	DOUBLE HANDHOLE	EACH	1			1						
Δ 81603090	UNIT DUCT, 600V, 3-1/C NO. 4, 1/C NO. 6 GROUND (XLP TYPE USE), 1-1/4 INCH POLYETHYLENE	FOOT	200		200							
Δ 81900200	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	692		487	187	10	8				

Δ SPECIALTY ITEMS

REVISIONS		SUMMARY OF QUANTITIES SHEET 2 OF 4	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			
CONSULTANT		TYLIN INTERNATIONAL	DRAWN: CBS	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.	PROJECT NO.

PLAN	DATE	BY
	REVISED	
	NOTED	
	NO.	

PROFILE	DATE	BY
	REVISED	
	NOTED	
	NO.	

SUMMARY OF QUANTITIES

IDOT CODE NUMBER	ITEM DESCRIPTION	UNIT	TOTAL	CONSTRUCTION TYPE CODES								VILLAGE OF WILMETTE 100% WILMETTE
				1000-1A ROADWAY 80% IDOT 20% WILMETTE	Y030-1E LIGHTING 80% IDOT 20% WILMETTE	Y031-1F TRAFFIC SIGNALS						
						LAKE AVE 80% IDOT 20% WILMETTE	CENTRAL AVE 80% IDOT 20% WILMETTE	WILMETTE AVE 80% IDOT 20% WILMETTE	LINDEN AVE 80% IDOT 20% WILMETTE	INTERCONNECT 80% IDOT 20% WILMETTE		
△ 83600200	LIGHT UNIT FOUNDATION, 24" DIAMETER	FOOT	44		44							
△ 84200700	LIGHTING FOUNDATION REMOVAL	EACH	6		6							
△ 85000200	MAINTAIN EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	3				1	1	1			
△ 85700205	FULL ACTUATED CONTROLLER IN TYPE IV CABINET, SPECIAL	EACH	3				1	1	1			
△ 85700305	FULL ACTUATED CONTROLLER IN TYPE V CABINET, SPECIAL	EACH	1			1						
△ 86200200	UNINTERRUPTIBLE POWER SUPPLY, STANDARD	EACH	4			1	1	1	1			
△ 86400100	TRANSCEIVER - FIBER OPTIC	EACH	4			1	1	1	1			
△ 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2/C	FOOT	875			744		131				
△ 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3/C	FOOT	3572			1443					2129	
△ 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5/C	FOOT	13262			4947	4053	4262				
△ 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7/C	FOOT	1090			364	362	364				
△ 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	5592			2455	1510	1627				
△ 87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	8			3	2	3				
△ 87602000	PEDESTRIAN PUSH BUTTON POST	EACH	1			1						
△ 87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1				1					
△ 87700180	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.	EACH	1			1						
△ 87700210	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.	EACH	1			1						
△ 87700220	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.	EACH	2			1		1				
△ 87702280	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS, 22 FEET AND 40 FEET	EACH	1			1						
△ 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	28			8	8	12				
△ 87800200	CONCRETE FOUNDATION, TYPE D	FOOT	4			4						
△ 87800400	CONCRETE FOUNDATION, TYPE E, 30-INCH DIAMETER	FOOT	27.0			13		14				
△ 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	13.0			13.0						
△ 87900200	DRILL EXISTING HANDHOLE	EACH	18			2	6	10				
△ 88040070	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	19			2	8	5	4			
△ 88040090	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3 SECTION, MAST ARM MOUNTED	EACH	46			15	12	12	7			
△ 88040110	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED	EACH	2						2			
△ 88040120	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED	EACH	2						2			
△ 88040160	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED	EACH	3			1	1	1				
△ 88040230	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED	EACH	6			2	2	2				
△ 88040260	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 1-3-SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	3			1	1	1				
△ 88102810	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, BRACKET MOUNTED	EACH	14			4	2	2	6			
△ 88102830	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, BRACKET MOUNTED	EACH	5			1	2	2				
△ 88200100	TRAFFIC SIGNAL BACK PLATE	EACH	57			16	13	13	15			
△ 88500100	INDUCTIVE LOOP DETECTOR	EACH	26			9	8	9				
△ 88600100	DETECTOR LOOP, TYPE I	FOOT	2277			811	663	803				
△ 88700200	LIGHT DETECTOR	EACH	12								12	
△ 88700300	LIGHT DETECTOR AMPLIFIER	EACH	3								3	
△ 88800100	PEDESTRIAN PUSH BUTTON	EACH	14			6	4	4				
△ 89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1			1						

△ SPECIALTY ITEMS

F.A.I.D. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	6
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 83999	

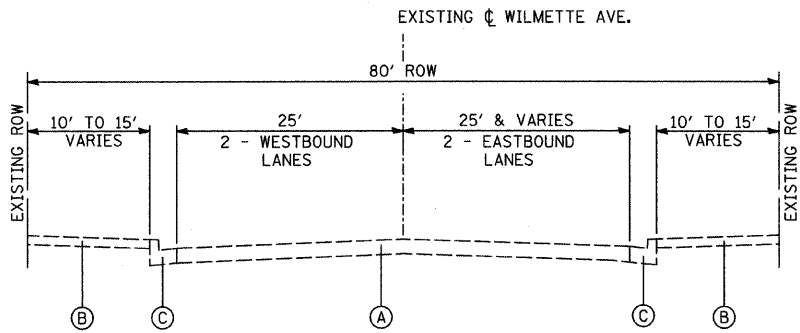
REVISIONS NAME DATE	SUMMARY OF QUANTITIES SHEET 3 OF 4	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
		DRAWN: CBS	SHEET NO.
		CHECKED:	
		APPROVED:	
		DATE: 02-14-2008	
		SCALE: NONE	
		JOB NO.:	PROJECT NO.:
		CONSULTANT TYLIN INTERNATIONAL	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	7
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				

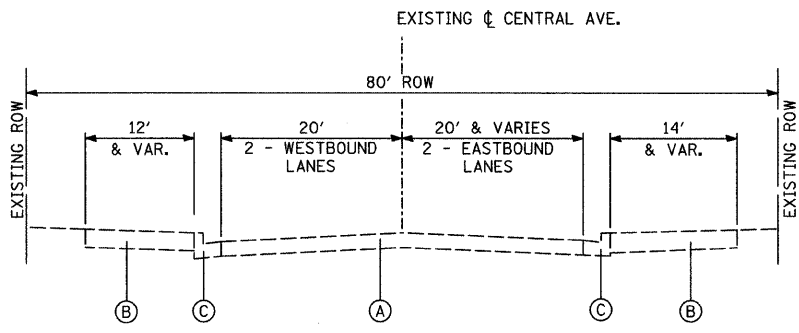
+ Y080
Δ SPECIALTY ITEMS

REVISIONS		SUMMARY OF QUANTITIES	VILLAGE OF WILMETTE	
NAME	DATE		GREEN BAY ROAD	
			INTERSECTION IMPROVEMENTS	
CONSULTANT		SHEET 4 OF 4	DRAWN: CBS	
TYLIN INTERNATIONAL			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
		JOB NO.:	SHEET NO.:	
			PROJECT NO.:	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	8
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				



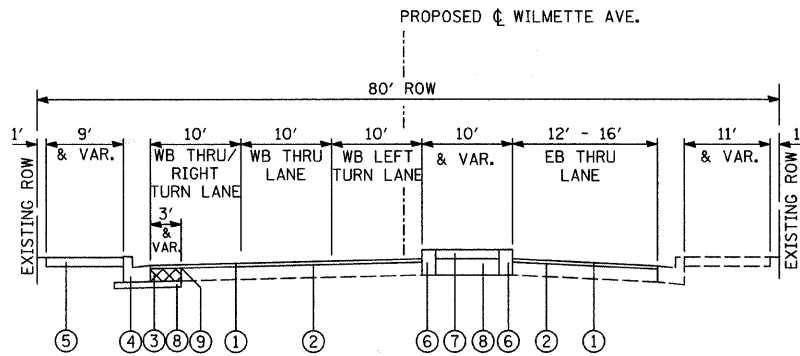
EXISTING TYPICAL SECTION
WILMETTE AVENUE
(EAST OF GREEN BAY ROAD)



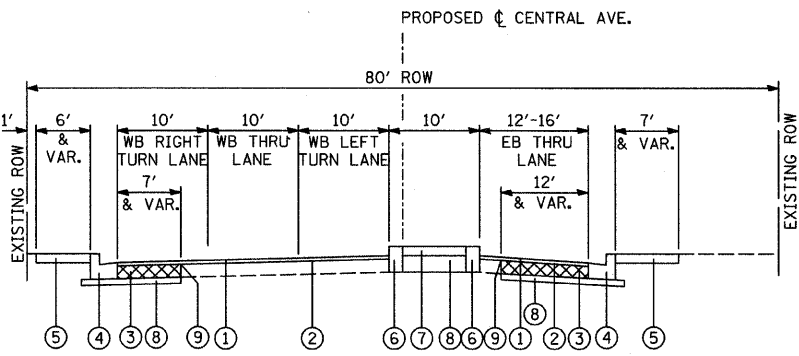
EXISTING TYPICAL SECTION
CENTRAL AVENUE
(EAST OF GREEN BAY ROAD)

EXISTING LEGEND:

- (A) EXISTING PAVEMENT - (3"± HMA OVER 8-12" PCC BASE COURSE)
- (B) EXISTING PCC SIDEWALK OR CONCRETE ACCENT PAVERS
- (C) EXISTING CURB AND GUTTER, TYPE B-6.12
- (D) EXISTING PCC RAISED MEDIAN
- (E) EXISTING PARKWAY



PROPOSED TYPICAL SECTION
WILMETTE AVENUE
(EAST OF GREEN BAY ROAD)



PROPOSED TYPICAL SECTION
CENTRAL AVENUE
(EAST OF GREEN BAY ROAD)

PROPOSED LEGEND:

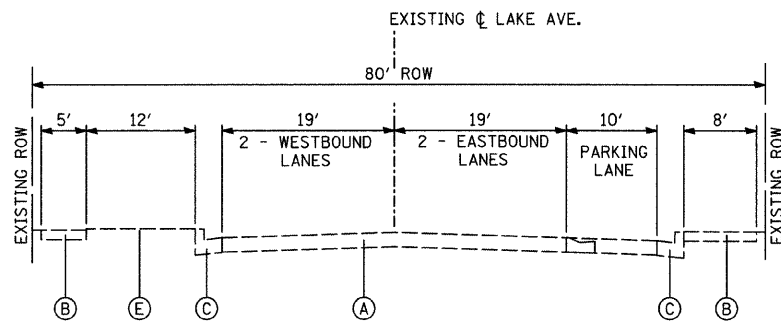
- (1) HOT MIX ASPHALT SURFACE COURSE, MIX D, N70 (IL-9.5mm), 1 1/2"
- (2) LEVEL BINDER (MACHINE METHOD), N70, 3/4"
- (3) PCC BASE COURSE WIDENING (VARIABLE DEPTH) - 8" TO 12" (MATCH EXISTING)
- (4) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- (5) PCC SIDEWALK 5" OR CONCRETE ACCENT PAVER REINSTALLATION (SEE PLANS)
- (6) CONCRETE CURB, TYPE B OR TYPE M (SEE PLANS)
- (7) CONCRETE MEDIAN SURFACE - 4"
- (8) SUBBASE GRANULAR MATERIAL, TYPE B - 4"
- (9) STRIP REFLECTIVE CRACK CONTROL TREATMENT

MIXTURE TYPE	AC TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) 1 1/2"	PG 64-22	4% @ 70 Gyr.
LEVELING BINDER (MACHINE METHOD), N70 3/4" (IL-9.5mm)	PG 64-22/58-22	4% @ 70 Gyr.

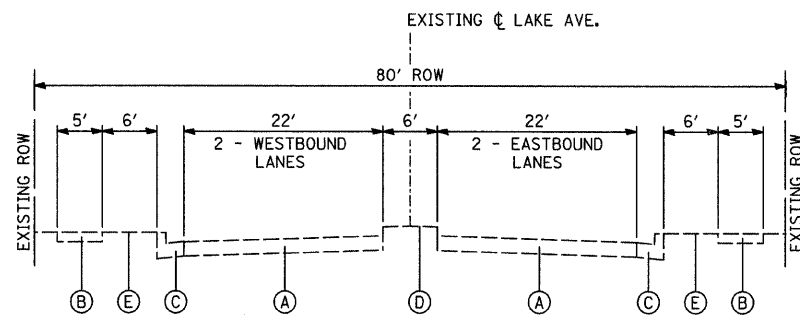
1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.
3. THE MAXIMUM LIFT OF HMA MATERIALS IS 4".

REVISIONS		EXISTING AND PROPOSED TYPICAL PAVEMENT SECTIONS WILMETTE AVENUE CENTRAL AVENUE	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.:	PROJECT NO.:
		CONSULTANT TYLIN INTERNATIONAL		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	9
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				



EXISTING TYPICAL SECTION
LAKE AVENUE
(WEST OF GREEN BAY ROAD)



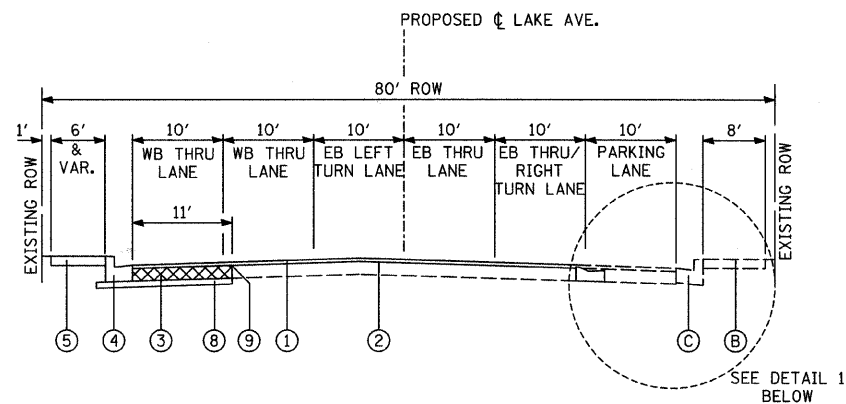
EXISTING TYPICAL SECTION
LAKE AVENUE
(EAST OF GREEN BAY ROAD)

EXISTING LEGEND:

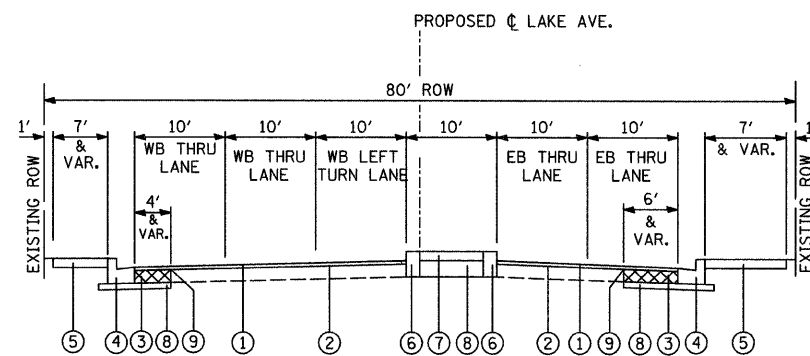
- (A) EXISTING PAVEMENT - (3"± HMA OVER 8-12" PCC BASE COURSE)
- (B) EXISTING PCC SIDEWALK OR CONCRETE ACCENT PAVERS
- (C) EXISTING CURB AND GUTTER, TYPE B-6.12
- (D) EXISTING PCC RAISED MEDIAN
- (E) EXISTING PARKWAY

MIXTURE TYPE	AC TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm) 1 1/2"	PG 64-22	4% @ 70 Gyr.
LEVELING BINDER (MACHINE METHOD), N70 3/4" (IL-9.5mm)	PG 64-22/58-22	4% @ 70 Gyr.

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.
3. THE MAXIMUM LIFT OF HMA MATERIALS IS 4".



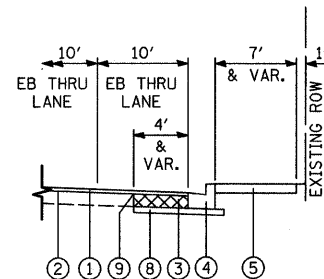
PROPOSED TYPICAL SECTION
LAKE AVENUE
(WEST OF GREEN BAY ROAD)



PROPOSED TYPICAL SECTION
LAKE AVENUE
(EAST OF GREEN BAY ROAD)

PROPOSED LEGEND:

- 1 HOT MIX ASPHALT SURFACE COURSE, MIX D, N70 (IL-9.5mm), 1 1/2"
- 2 LEVEL BINDERING (MACHINE METHOD), N70, 3/4"
- 3 PCC BASE COURSE WIDENING (VARIABLE DEPTH) - 8" TO 12" (MATCH EXISTING)
- 4 COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- 5 PCC SIDEWALK 5" OR CONCRETE ACCENT PAVER REINSTALLATION (SEE PLANS)
- 6 CONCRETE CURB, TYPE B OR TYPE M (SEE PLANS)
- 7 CONCRETE MEDIAN SURFACE - 4"
- 8 SUBBASE GRANULAR MATERIAL, TYPE B - 4"
- 9 STRIP REFLECTIVE CRACK CONTROL TREATMENT

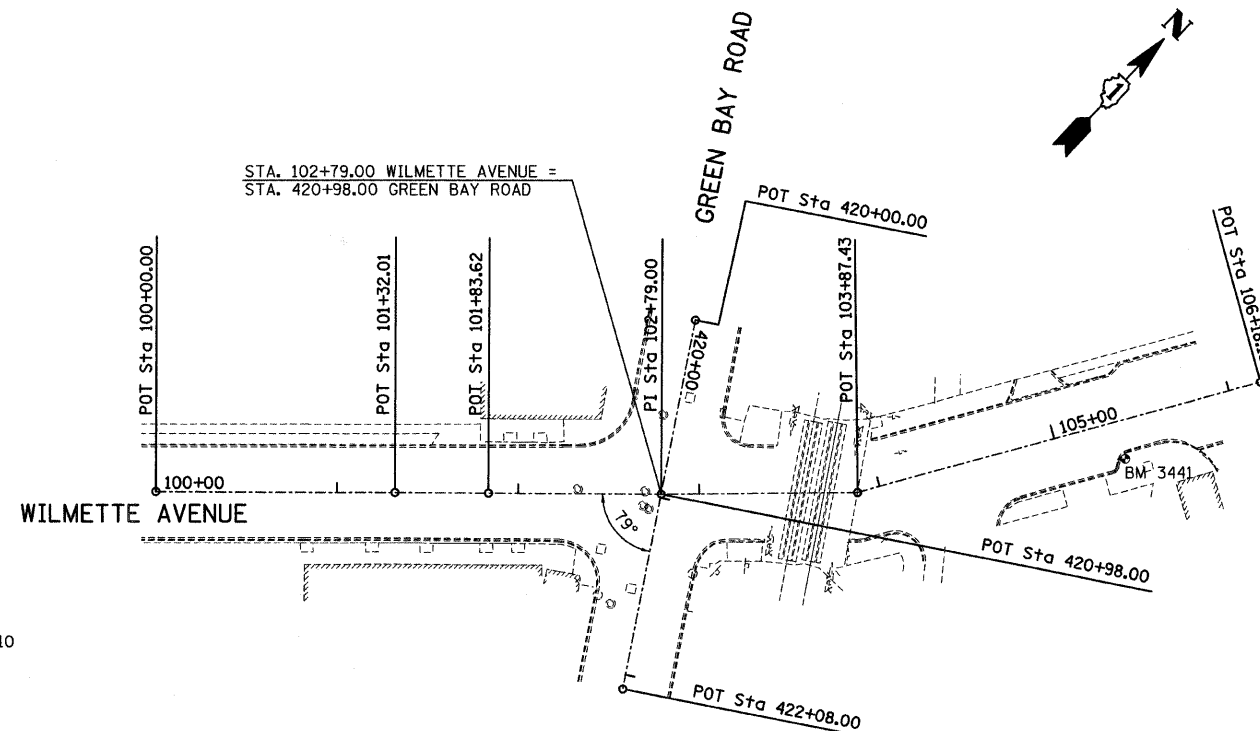


DETAIL 1
STA. 302+31 TO 303+23

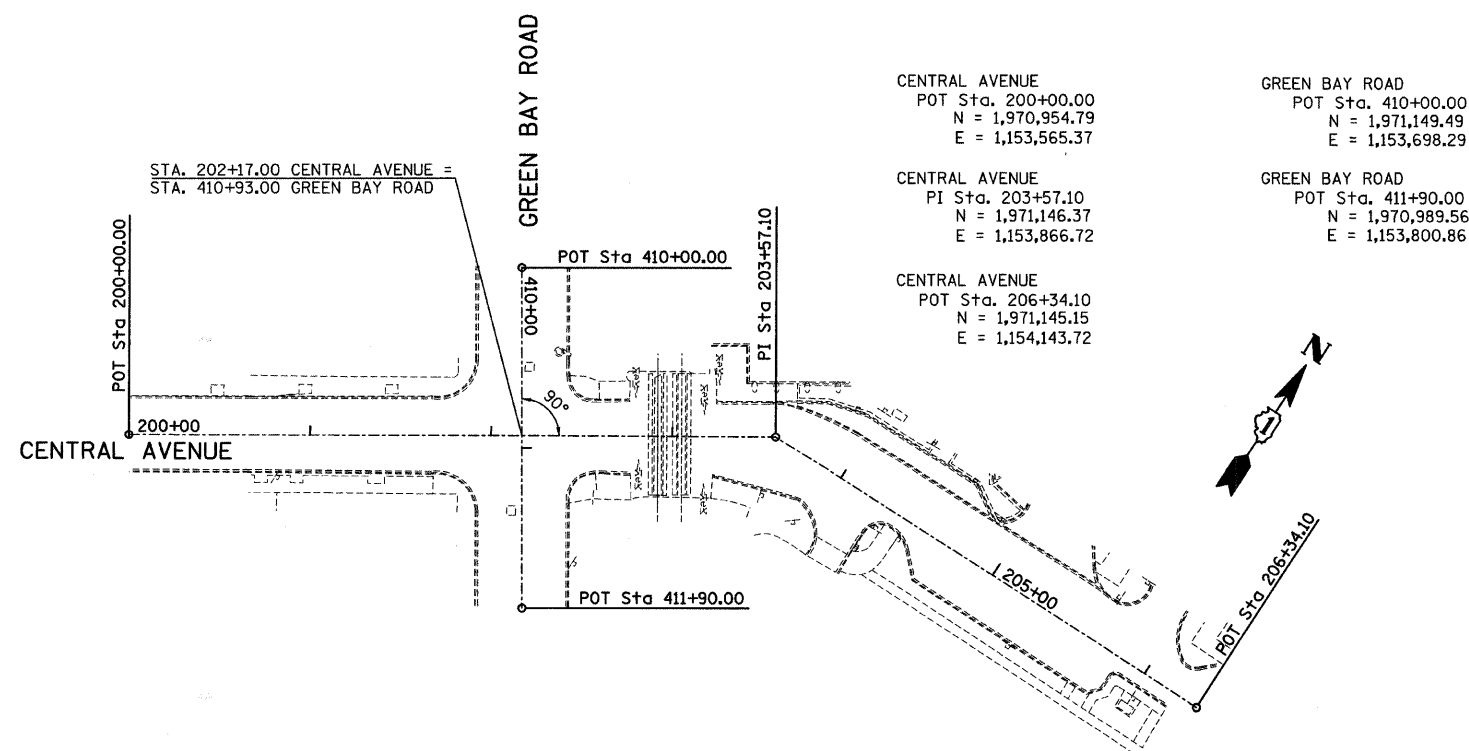
REVISIONS		EXISTING AND PROPOSED TYPICAL PAVEMENT SECTIONS LAKE AVENUE (WEST OF GREEN BAY ROAD) LAKE AVENUE (EAST OF GREEN BAY ROAD)		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE				
		CONSULTANT TYLIN INTERNATIONAL		DRAWN:	SHEET NO.
				CHECKED:	
				APPROVED:	
				DATE: 02-14-2008	
				SCALE: NONE	
				JOB NO.:	PROJECT NO.:

PLAN	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	ALIGNMENT CHECKED		
	RT. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED _____	BY _____	DATE _____
NOTE BOOK	PLOTTED _____		
	GRADES CHECKED _____		
	B.M. NOTED _____		
NO. _____	STRUCTURE NOTATIONS CHK'D _____		

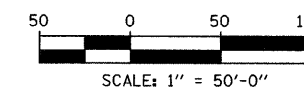


BM 3441
N=1,970,852.84 E=1,154,180.94 ELEV.=610.56
NAIL IN CONCRETE SIDEWALK,
WILMETTE AVENUE STA. 105+34.89, 21.15 RT



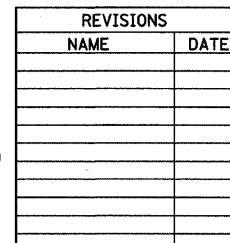
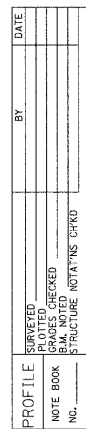
WILMETTE AVENUE
PI Sta. 103+87.43
N = 1,970,737.88
E = 1,154,086.20

GREEN BAY ROAD
POT Sta. 422+08.00
N = 1,970,569.55
E = 1,154,066.55



REVISIONS		ALIGNMENT AND TIES LAKE AVENUE, CENTRAL AVENUE, AND WILMETTE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS		
NAME	DATE				
				DRAWN:	SHEET NO.
				CHECKED:	
				APPROVED:	
				DATE: 02-14-2008	
				SCALE: 1"=50'-0"	
		CONSULTANT	JOB NO.:	PROJECT NO.:	
		TYLIN INTERNATIONAL			

PLAN	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	ALIGNMENT	CHECKED	
	RT. OF WAY	CHECKED	
	CADD FILE NAME		

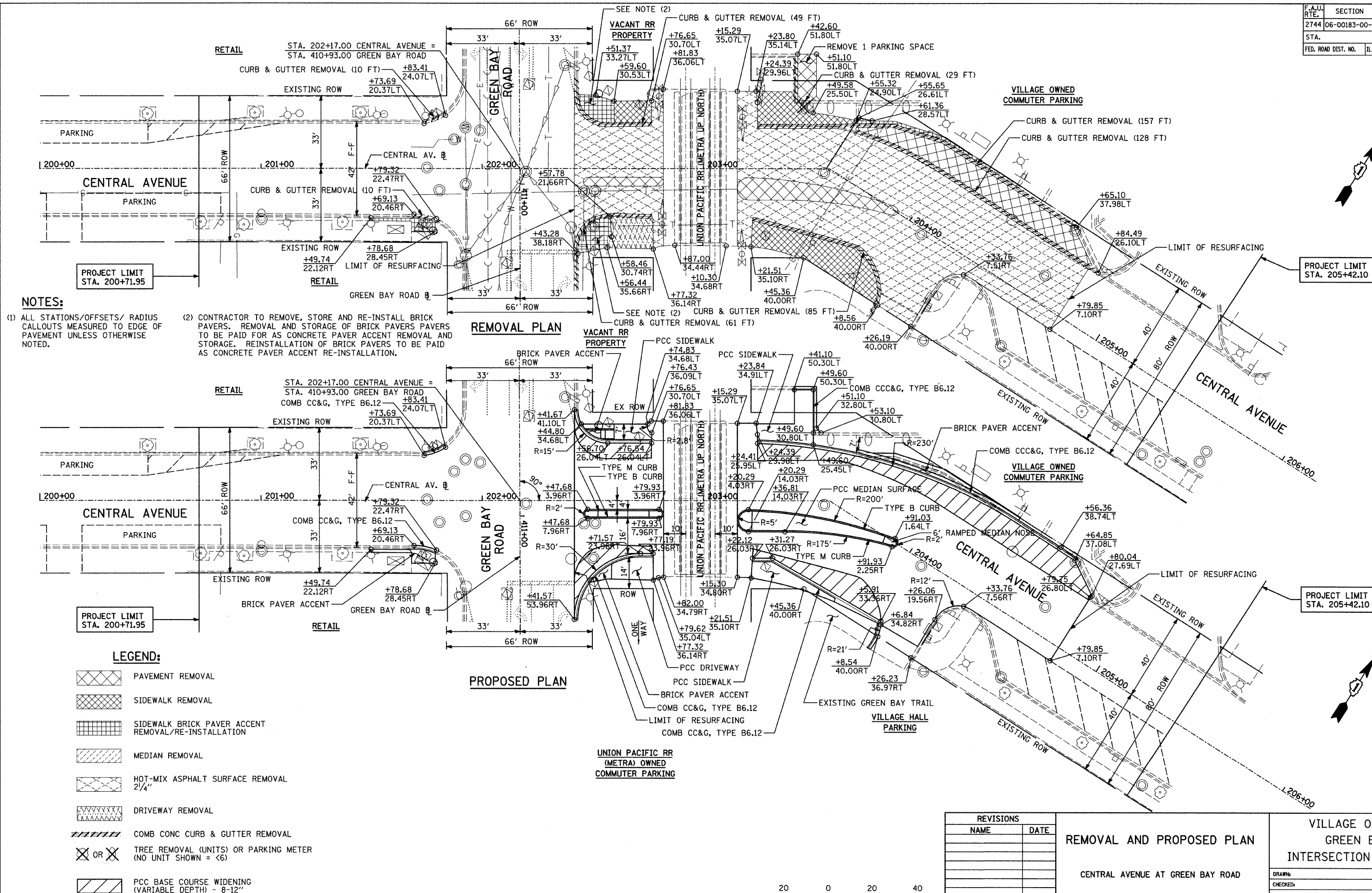


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DRAWN:		SHEET NO.
CHECKED:		
APPROVED:		
DATE:	02-14-2008	
SCALE:	1"=20'-0"	
JOB NO.		PROJECT NO.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	12
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 83999	

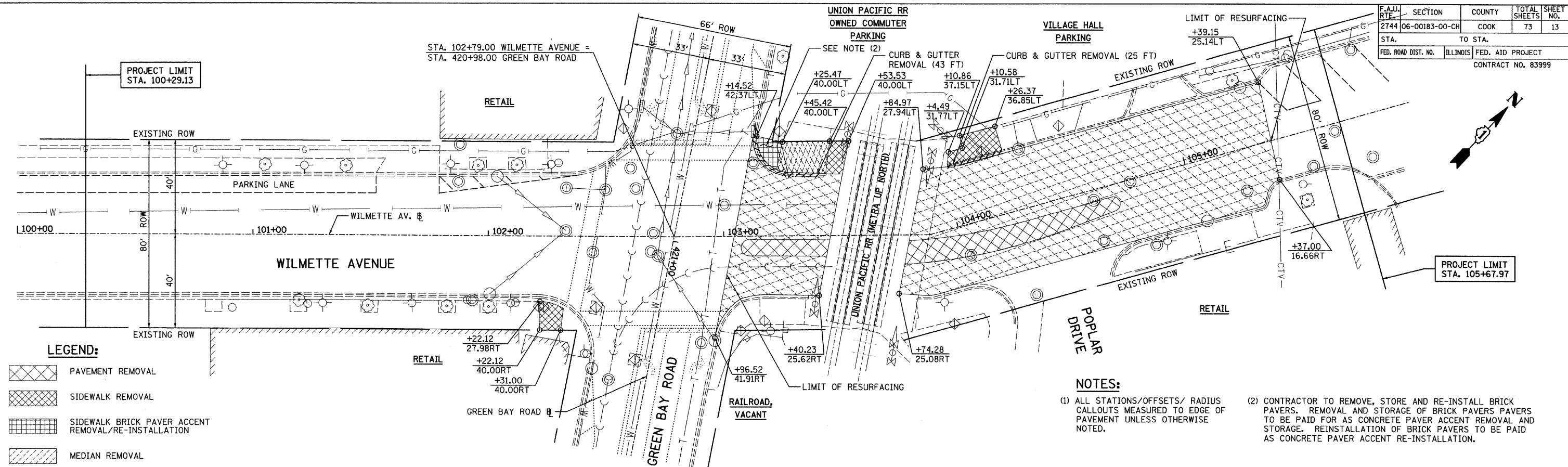
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DESIGNED				
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PROFILE	DATE	BY	CHECKED	NO.
DESIGNED				
NOTED				
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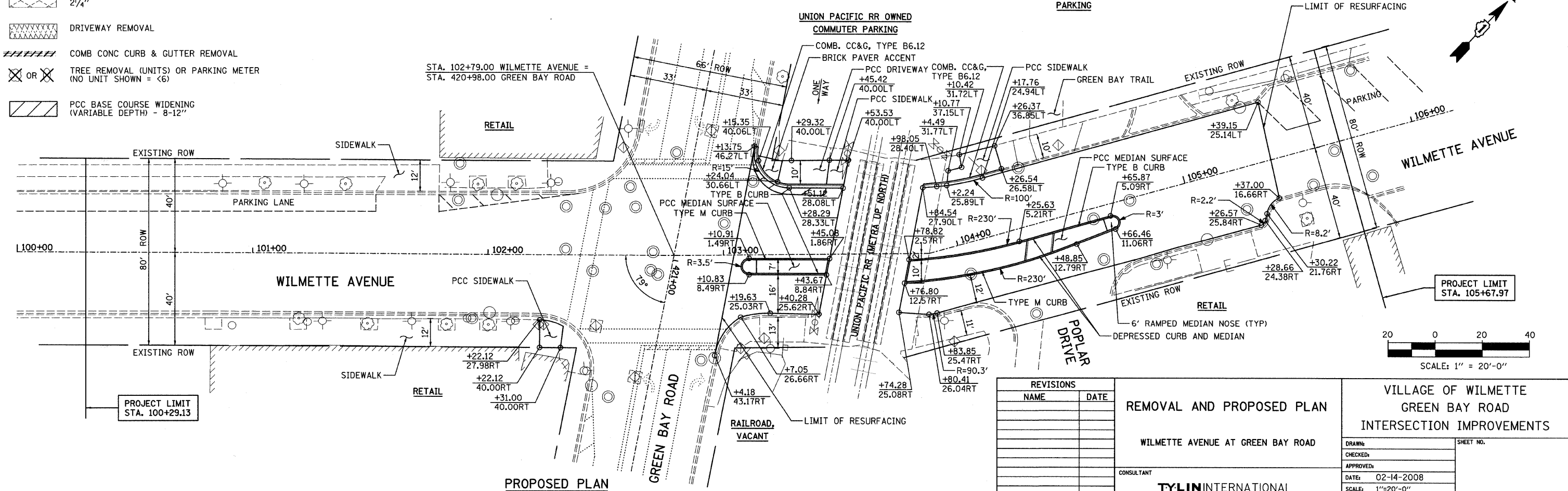


PLAN	DATE	BY
DESIGNED		
CHECKED		
NOTED		
NO.		

PROFILE	DATE	BY
DESIGNED		
CHECKED		
NOTED		
NO.		



REMOVAL PLAN



PROPOSED PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	13
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
		CONTRACT NO. 83999		

NOTES:

- (1) ALL STATIONS/OFFSETS/ RADIUS CALLOUTS MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- (2) CONTRACTOR TO REMOVE, STORE AND RE-INSTALL BRICK PAVERS. REMOVAL AND STORAGE OF BRICK PAVERS TO BE PAID FOR AS CONCRETE PAVEMENT ACCENT REMOVAL AND STORAGE. REINSTALLATION OF BRICK PAVERS TO BE PAID AS CONCRETE PAVEMENT ACCENT RE-INSTALLATION.

REVISIONS	NAME	DATE

REMOVAL AND PROPOSED PLAN
WILMETTE AVENUE AT GREEN BAY ROAD
CONSULTANT
TYLIN INTERNATIONAL

VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
DRAWN:	SHEET NO.
CHECKED:	
APPROVED:	
DATE: 02-14-2008	
SCALE: 1"=20'-0"	
JOB NO.:	PROJECT NO.:

PLAN	NO.	NOTE BOOK	SURVEYED	BY	DATE	PROPOSED	REVISIONS
						ALIGNED	CHECKED
						PAVED	PAVED
						PAID	FILE NAME

MOT GENERAL NOTES:

- SEE SPECIAL PROVISION TITLED TRAFFIC CONTROL AND PROTECTION (SPECIAL).
- A WORK ZONE SPEED LIMIT OF 25 M.P.H. MUST BE MAINTAINED AT ALL TIMES.
- ALL TEMPORARY PAVEMENT MARKINGS PLACED ON EXISTING HMA PAVEMENT SHALL BE PAID FOR AS "THERMOPLASTIC PAVEMENT MARKING" OF THE LINE WIDTH SPECIFIED. REMOVAL OF THESE MARKINGS WILL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL".
- ALL TEMPORARY PAVEMENT MARKINGS PLACED ON NEW HMA PAVEMENT SHALL BE PAID FOR AS "PAVEMENT MARKING TAPE, TYPE III" OF THE LINE WIDTH SPECIFIED. REMOVAL OF THESE MARKINGS WILL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL".
- CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS DURING CONSTRUCTION. THE COST OF THE MAINTAINING ACCESS IS INCLUDED AS PART OF "TEMPORARY ACCESS (PRIVATE ENTRANCE)", "TEMPORARY ACCESS (COMMERCIAL ENTRANCE)", AND NO ADDITIONAL COST WILL BE ALLOWED. TEMPORARY ACCESS CLOSURES WILL BE ALLOWED ONLY AT THE DIRECTION OF THE RESIDENT ENGINEER.
- CONTRACTOR SHALL MAINTAIN ACCESS TO THE FIRE STATION ON LAKE AVE., ALL COMMUTER PARKING LOTS, AND VILLAGE HALL PARKING LOT ENTRANCES AT WILMETTE AND CENTRAL AVENUES FOR THE DURATION OF CONSTRUCTION.
- CONTRACTOR SHALL PROVIDE AT LEAST 48 HOURS NOTICE PRIOR TO CONSTRUCTION TO ALL DRIVEWAY OWNERS.
- CONTRACTOR SHALL PLACE "DRIVEWAY ENTRANCE" SIGNS FACING BOTH DIRECTIONS OF TRAFFIC AT EACH DRIVEWAY IN THE CONTRACT WORK AREA. THE SIGNS SHALL REMAIN IN PLACE FOR THE DURATION OF CONSTRUCTION.
- THE CONTRACTOR IS REPSONSIBLE FOR COORDINATING HIS CONSTRUCTION OPERATIONS WITH THE UNION PACIFIC RAILROAD AND CONSTRUCTION AND MAINTENANCE OPERATIONS, INCLUDING UTILITY RELOCATIONS AND ADJUSTMENTS, OF OTHER CONTRACTORS WORKING ADJACENT TO/AND OR WITHIN THE LIMITS OF THE PROJECT.
- TRAFFIC LANES MUST NOT BE REOPENED TO TRAFFIC UNTIL ALL RAILROAD SIGNALS (LIGHTS AND GATES) AT THE LOCATION ARE INSTALLED AND FINALLY OPERATIONAL.
- NO ADDITIONAL COMPENSATION WILL BE PROVIDED IF DRIVEWAYS ARE CONSTRUCTED IN STAGES.

LAKE AVE STAGING NOTES:

STAGE 1 NOTES:

- INSTALL TEMPORARY TRAFFIC SIGNALS, SEE TEMPORARY TRAFFIC SIGNAL PLANS.
- WIDEN NORTH SIDE OF LAKE AVE FROM 300+08 TO 303+08 AND FROM 303+59 TO 306+41.
- WIDEN SOUTH SIDE OF LAKE AVE FROM 302+31 TO 303+53 AND FROM 304+18 TO 306+15.
- CONSTRUCT TRAFFIC SIGNALS/LIGHTING RELOCATIONS.
- INSTALL TEMPORARY PAVEMENT MARKING AS REQUIRED FOR STAGE 1A.

STAGE 1A NOTES:

- CONSTRUCT MEDIANS AS SHOWN ON PLANS.
- COMPLETE PERMANENT TRAFFIC SIGNALS, SEE TRAFFIC CONTROL SIGNAL PLANS.

CENTRAL AVE STAGING NOTES:

STAGE 2 NOTES:

- STAGE 2 WORK MUST NOT COMMENCE UNTIL THE COMPLETION OF WORK IN STAGE 1A OF THIS CONTRACT.
- INSTALL DETOUR SIGNING AND CLOSE CENTRAL AVE. FROM 202+31 TO 204+34.
- CONSTRUCT PAVEMENT WIDENING ON CENTRAL AVE.
- CONSTRUCT MEDIANS AS SHOWN ON PLANS.
- CONSTRUCT TRAFFIC SIGNALS.

WILMETTE AVE STAGING NOTES:

STAGE 3 NOTES:

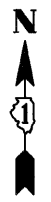
- STAGE 3 WORK MUST NOT COMMENCE UNTIL THE COMPLETION OF STAGE 2 WORK OF THIS CONTRACT.
- INSTALL DETOUR AND CLOSE WILMETTE AVE. FROM 103+00 TO 105+39.
- CONSTRUCT WIDENING ON NORTH SIDE OF WILMETTE AVE. FROM 103+13 TO 103+50.
- CONSTRUCT MEDIANS AS SHOWN ON PLANS.
- CONSTRUCT TRAFFIC SIGNALS.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	14
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				

PROFILE	NO.	NOTE BOOK	SURVEYED	BY	DATE	PROPOSED	REVISIONS
						ALIGNED	CHECKED
						PAVED	PAVED
						PAID	FILE NAME

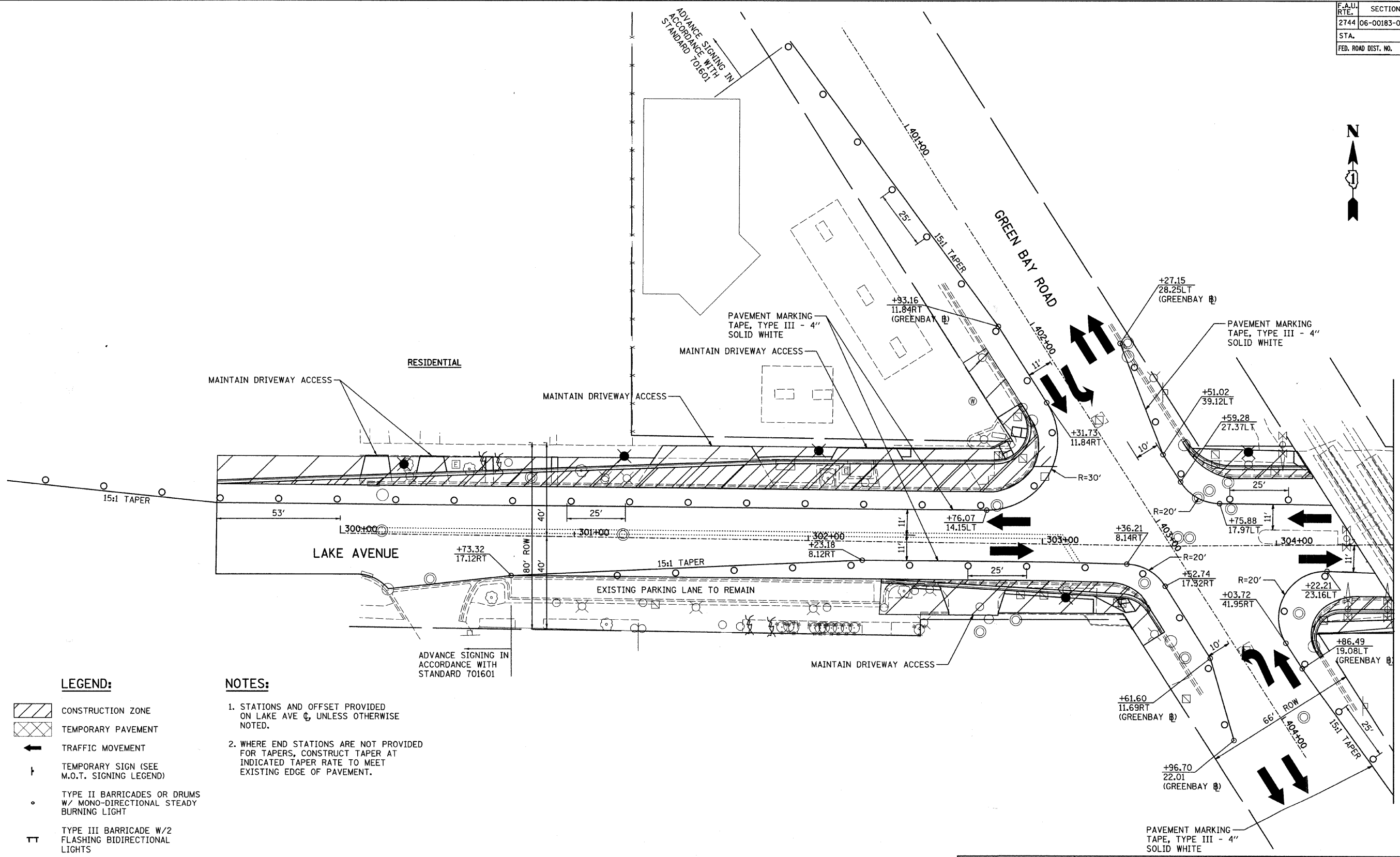
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				SHEET NO.	
				DRAWN:	
				CHECKED:	
				APPROVED:	
				DATE: 02-14-2008	
CONSULTANT			TYLIN INTERNATIONAL	SCALE: NONE	
JOB NO.:				PROJECT NO.:	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	15
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				



PLAN	DATE	BY
DESIGNED		
CHECKED		
NOTED		
FILE NAME		

PROFILE	DATE	BY
DESIGNED		
CHECKED		
NOTED		
FILE NAME		

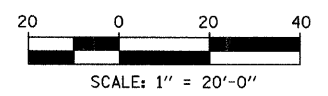


LEGEND:

- CONSTRUCTION ZONE
- TEMPORARY PAVEMENT
- TRAFFIC MOVEMENT
- TEMPORARY SIGN (SEE M.O.T. SIGNING LEGEND)
- TYPE II BARRICADES OR DRUMS W/ MONO-DIRECTIONAL STEADY BURNING LIGHT
- TYPE III BARRICADE W/2 FLASHING BIDIRECTIONAL LIGHTS
- TEMPORARY CONCRETE BARRIER
- ARROW BOARD
- M.O.T. APPURTENANCES REMAINING FROM PREVIOUS STAGES

NOTES:

- STATIONS AND OFFSET PROVIDED ON LAKE AVE, UNLESS OTHERWISE NOTED.
- WHERE END STATIONS ARE NOT PROVIDED FOR TAPERS, CONSTRUCT TAPER AT INDICATED TAPER RATE TO MEET EXISTING EDGE OF PAVEMENT.



REVISIONS	
NAME	DATE

MAINTENANCE OF TRAFFIC
STAGE 1
SHEET 1 OF 2
LAKE AVENUE AT GREEN BAY ROAD

CONSULTANT
TYLIN INTERNATIONAL

VILLAGE OF WILMETTE
GREEN BAY ROAD
INTERSECTION IMPROVEMENTS

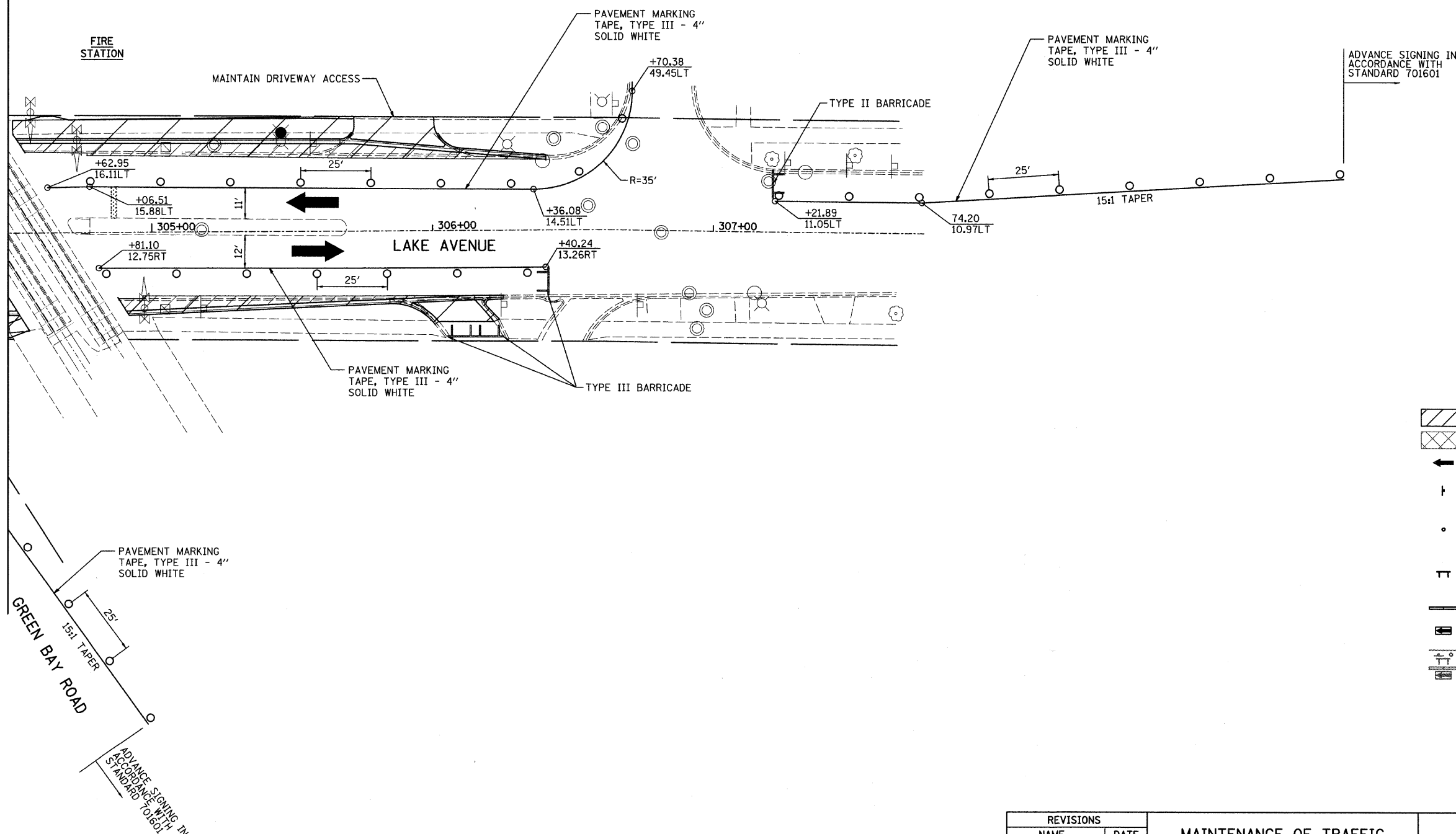
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APPROVED:	
DATE: 02-14-2008	
SCALE: 1"=20'-0"	
JOB NO.:	PROJECT NO.:

MATCHLINE STA. 301+50.00 - SHEET 2 OF 2

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	16
STA. TO STA.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				

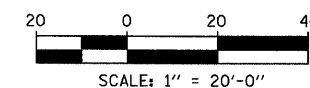


MATCHLINE STA. 304+50.00 - SHEET 1 OF 2



LEGEND:

- CONSTRUCTION ZONE
- TEMPORARY PAVEMENT
- TRAFFIC MOVEMENT
- TEMPORARY SIGN (SEE M.O.T. SIGNING LEGEND)
- TYPE II BARRICADES OR DRUMS W/ MONO-DIRECTIONAL STEADY BURNING LIGHT
- TYPE III BARRICADE W/2 FLASHING BIDIRECTIONAL LIGHTS
- TEMPORARY CONCRETE BARRIER
- ARROW BOARD
- M.O.T. APPURTENANCES REMAINING FROM PREVIOUS STAGES



REVISIONS		MAINTENANCE OF TRAFFIC STAGE 1 SHEET 2 OF 2 LAKE AVENUE AT GREEN BAY ROAD		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			DRAWN:	SHEET NO.
		CONSULTANT TYLIN INTERNATIONAL		CHECKED:	
				APPROVED:	
				DATE: 02-14-2008	
				SCALE: 1"=20'-0"	
				JOB NO.:	PROJECT NO.:

PLAN	DATE	BY
DESIGNED		
CHECKED		
NOTE BOOK		
NO.		
PAID FILE NAME		

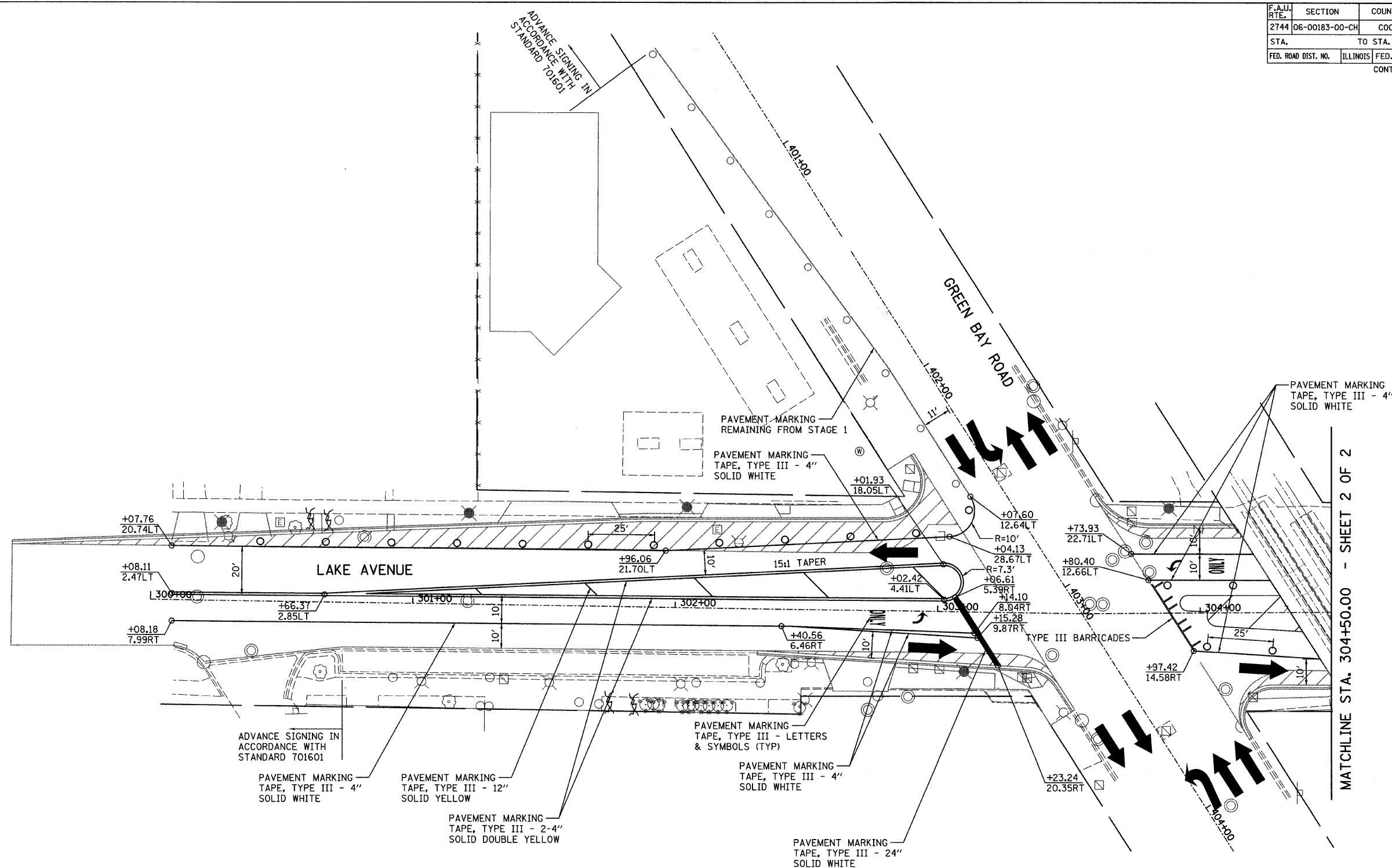
PROFILE	DATE	BY
DESIGNED		
CHECKED		
NOTE BOOK		
NO.		
STRUCTURE NOTATIONS CHKD		

LEGEND:

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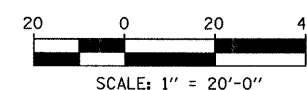
PLAN	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	RT. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
FLOTTED			
GRADES CHECKED			
B.M. NOTED			
STRUCTURE NOTATIONS CHK'D			
NO. _____			

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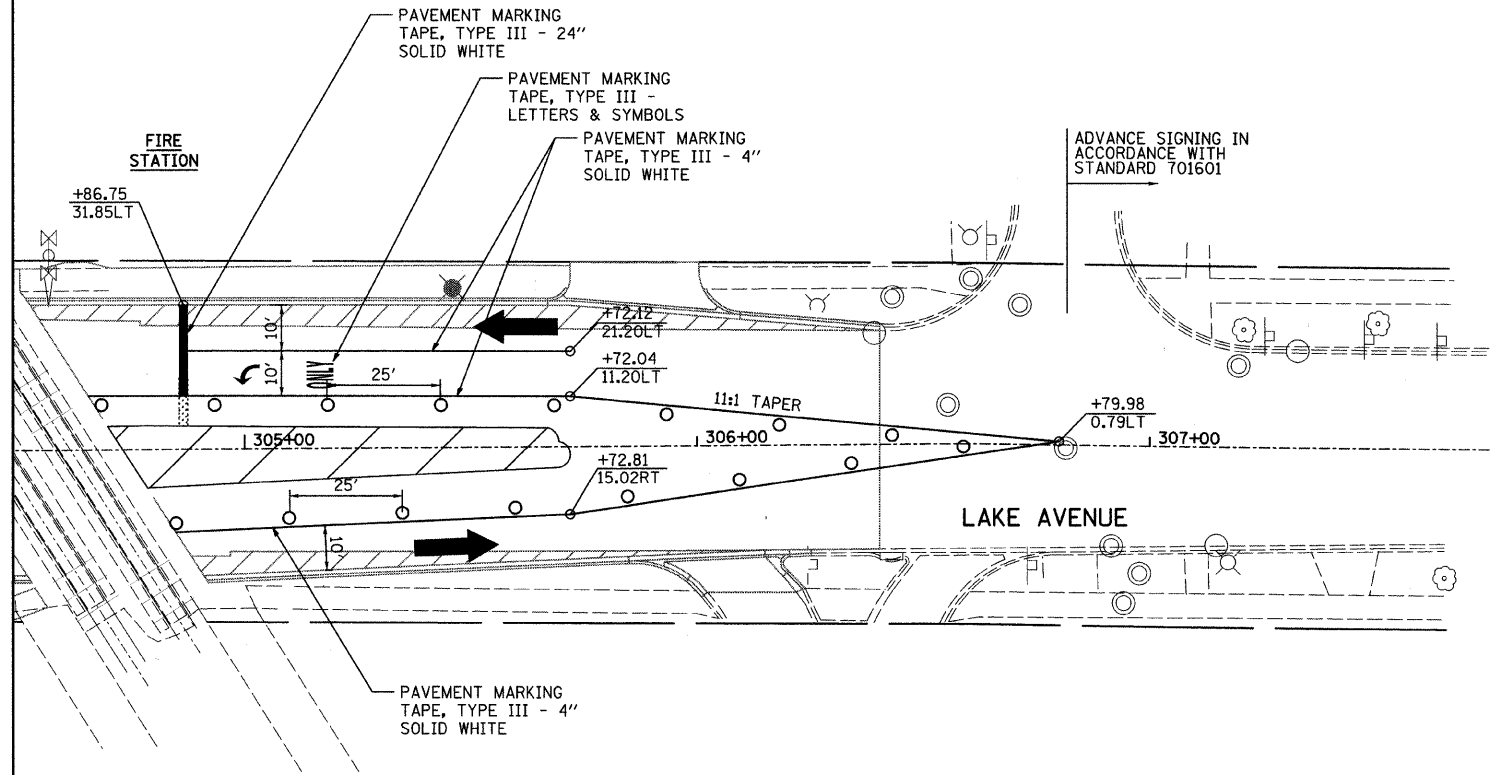
CONSULTANT	TYLIN INTERNATIONAL
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DATE: 02-14-2008	
SCALE: 1"=20'-0"	
JOB NO.:	PROJECT



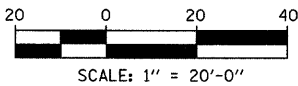
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2744	06-00183-00-CH	COOK	73	18
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				

MATCHLINE STA. 304+50.00 - SHEET 1 OF 2



LEGEND:

- CONSTRUCTION ZONE
- TEMPORARY PAVEMENT
- TRAFFIC MOVEMENT
- TEMPORARY SIGN (SEE M.O.T. SIGNING LEGEND)
- TYPE II BARRICADES OR DRUMS W/ MONO-DIRECTIONAL STEADY BURNING LIGHT
- TYPE III BARRICADE W/2 FLASHING BIDIRECTIONAL LIGHTS
- TEMPORARY CONCRETE BARRIER
- ARROW BOARD
- M.O.T. APPURTENANCES REMAINING FROM PREVIOUS STAGES

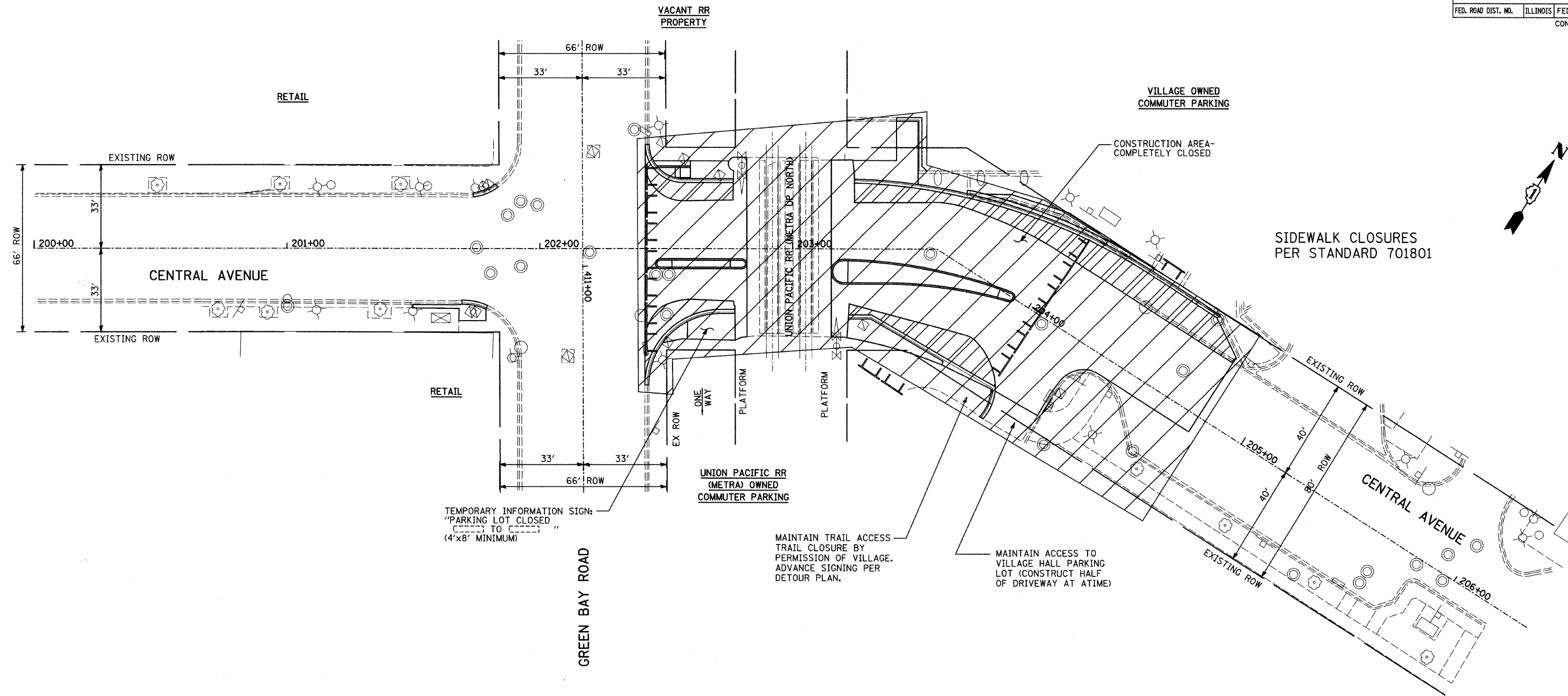


REVISIONS		MAINTENANCE OF TRAFFIC STAGE 2 SHEET 2 OF 2 LAKE AVENUE AT GREEN BAY ROAD		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			DRAWN:	SHEET NO.
		CONSULTANT TYLIN INTERNATIONAL		CHECKED:	
				APPROVED:	
				DATE: 02-14-2008	
				SCALE: 1"=20'-0"	
				JOB NO.:	PROJECT NO.:

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	19
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				

PLAN	DATE
DESIGNED BY	
CHECKED BY	
NOTED BY	
NO.	

PROFILE	DATE
DESIGNED BY	
CHECKED BY	
NOTED BY	
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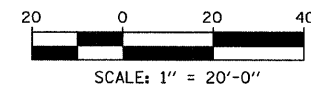


LEGEND:

- CONSTRUCTION WORK ZONE
- TYPE III BARRICADE

NOTE:

SEE DETOUR PLAN FOR ADDITIONAL SIGNING



REVISIONS	
NAME	DATE

**MAINTENANCE OF TRAFFIC
ROAD CLOSURE PLAN**

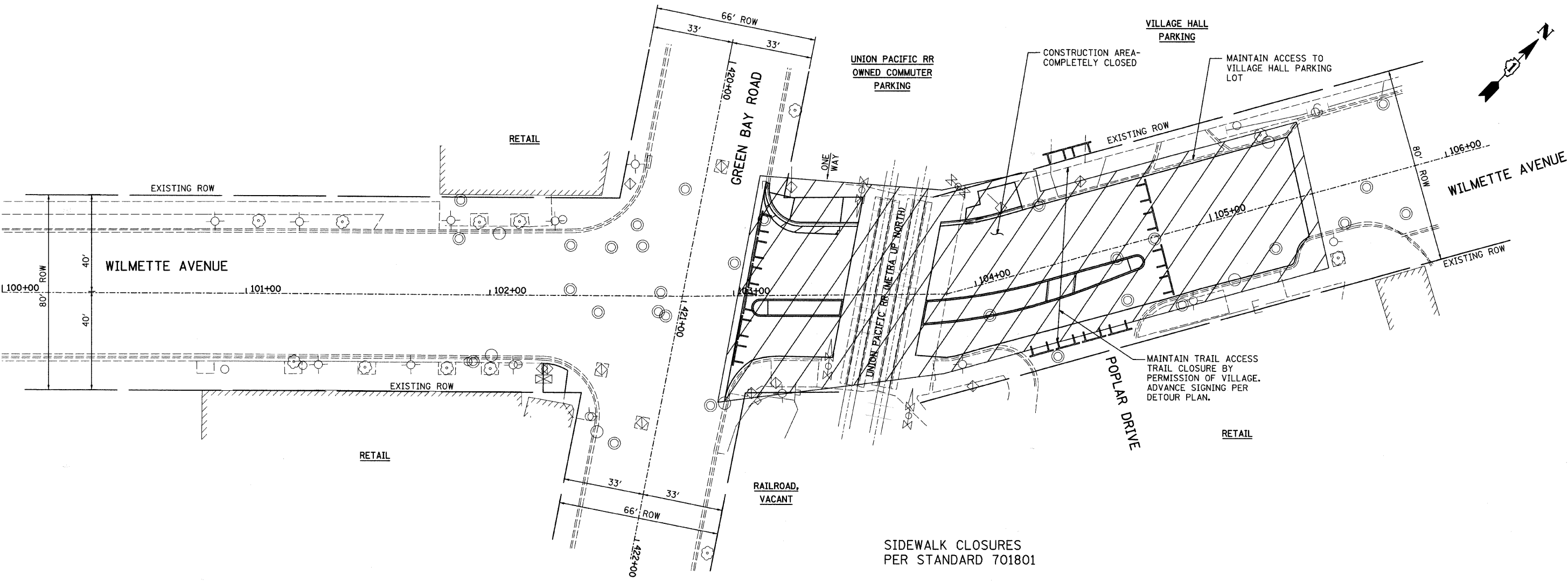
CENTRAL AVENUE AT GREEN BAY ROAD

CONSULTANT
TYLIN INTERNATIONAL

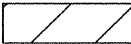

**VILLAGE OF WILMETTE
GREEN BAY ROAD
INTERSECTION IMPROVEMENTS**

DRAWN:	SHEET NO.
CHECKED:	
APPROVED:	
DATE: 02-14-2008	
SCALE: 1"=20'-0"	
JOB NO.:	PROJECT NO.:

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	20
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
		CONTRACT NO. 83999		

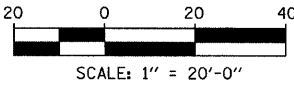


LEGEND:

-  CONSTRUCTION WORK ZONE
-  TYPE III BARRICADE

NOTE:

SEE DETOUR PLAN FOR
ADDITIONAL SIGNING

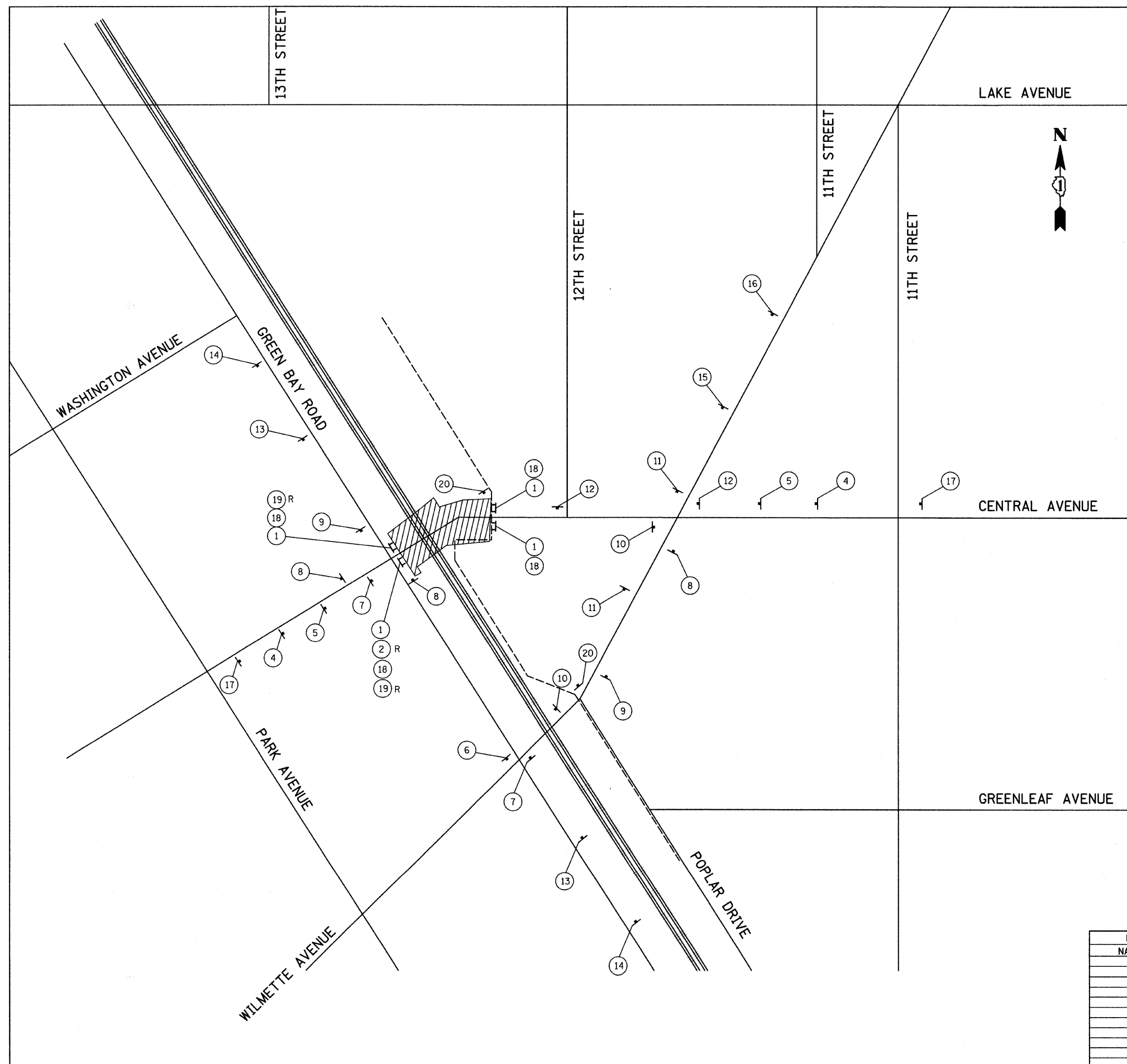


REVISIONS		MAINTENANCE OF TRAFFIC ROAD CLOSURE PLAN WILMETTE AVENUE AT GREEN BAY ROAD		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			DRAMA:	SHEET NO.
		CONSULTANT TYLIN INTERNATIONAL		CHECKED:	
				APPROVED:	
				DATE: 02-14-2008	
				SCALE: 1"=20'-0"	
				JOB NO.:	PROJECT NO.:

PLAN	DATE
DESIGNED	
CHECKED	
NO.	
FILE NAME	

PROFILE	DATE
DESIGNED	
CHECKED	
NO.	
FILE NAME	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEE NO.
2744	06-00183-00-CH	COOK	73	21
STA. TO STA.				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				





 R11-2 1	 M4-10 2	<p>NOT USED</p>	 CENTRAL AVENUE 4	 CENTRAL AVENUE 5
 6	 7	 CENTRAL AVENUE M9-8a 8	 9	 10
 11	 12	 EAST CENTRAL AVENUE 13	 EAST CENTRAL AVENUE 14	 WEST CENTRAL AVENUE 15
 WEST CENTRAL AVENUE 16	 CENTRAL AVENUE 17	 R9-9 18	 M4-9b 19	 20

NOTES:

1. ALL DETOUR SIGNS SHALL BE IN ORANGE COLOR BACKGROUND WITH BLACK LETTERS.
2. FOLLOW LATEST MUTCD MANUAL, BUREAU OF LOCAL ROADS, AND IDOT STANDARDS FOR DETOUR SIGNS AND MAINTENANCE OF TRAFFIC SIGNS.
3. SIGN 1 ROAD SHALL BE PLACED ON TYPE III BARRICADES.

LEGEND:

 CONSTRUCTION WORK ZONE
 TYPE III BARRICADE

REVISIONS		MAINTENANCE OF TRAFFIC DETOUR PLAN CENTRAL AVENUE	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE:	
			JOB NO.:	
				PROJECT NO.:
		CONSULTANT		
		TYLIN INTERNATIONAL		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	22
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				

GREEN BAY ROAD

12TH STREET

CENTRAL AVENUE

GREENLEAF AVENUE

LINDEN AVENUE

11TH STREET

WILMETTE AVENUE

PARK AVENUE

POPLAR DRIVE



ROAD CLOSED R11-2 1	DETOUR → M4-10 2	NOT USED 3	ROAD CLOSED AHEAD WILMETTE AVENUE 4	DETOUR AHEAD WILMETTE AVENUE 5
NORTH WILMETTE AVENUE DETOUR ← 6	NORTH WILMETTE AVENUE DETOUR → 7	END DETOUR WILMETTE AVENUE M9-8a 8	NORTH WILMETTE AVENUE DETOUR ↑ 9	SOUTH WILMETTE AVENUE DETOUR → 10
SOUTH WILMETTE AVENUE DETOUR ↑ 11	SOUTH WILMETTE AVENUE DETOUR ← 12	ROAD CLOSED AHEAD NORTH WILMETTE AVENUE 13	DETOUR AHEAD NORTH WILMETTE AVENUE 14	ROAD CLOSED AHEAD SOUTH WILMETTE AVENUE 15
DETOUR AHEAD SOUTH WILMETTE AVENUE 16	ROAD WORK AHEAD WILMETTE AVENUE 17	SIDEWALK CLOSED R9-9 18	DETOUR M4-9b 19	DETOUR AHEAD SOUTH POPLAR DRIVE 20
SOUTH POPLAR DRIVE DETOUR ← 21	SOUTH POPLAR DRIVE DETOUR → 22	END DETOUR POPLAR DRIVE 23	ROAD CLOSED AHEAD 24	TRAIL CLOSED AHEAD 25

NOTES:

- ALL DETOUR SIGNS SHALL BE IN ORANGE COLOR BACKGROUND WITH BLACK LETTERS.
- FOLLOW LATEST MUTCD MANUAL, BUREAU OF LOCAL ROADS, AND IDOT STANDARDS FOR DETOUR SIGNS AND MAINTENANCE OF TRAFFIC SIGNS.
- SIGN 1 ROAD CLOSED SHALL BE PLACED ON TYPE III BARRICADES.

LEGEND:

- CONSTRUCTION WORK ZONE
- TYPE III BARRICADE

REVISIONS NAME DATE	MAINTENANCE OF TRAFFIC DETOUR PLAN WILMETTE AVENUE, POPLAR DRIVE CONSULTANT TYLIN INTERNATIONAL	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS		
		DRAWN:	SHEET NO.	
		CHECKED:		
		APPROVED:		
		DATE: 02-14-2008		
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		JOB NO.:	PROJECT NO.:	

PLAN	DATE	BY
REVIEWED		
ALIGNED		
CHECKED		
NO.:		

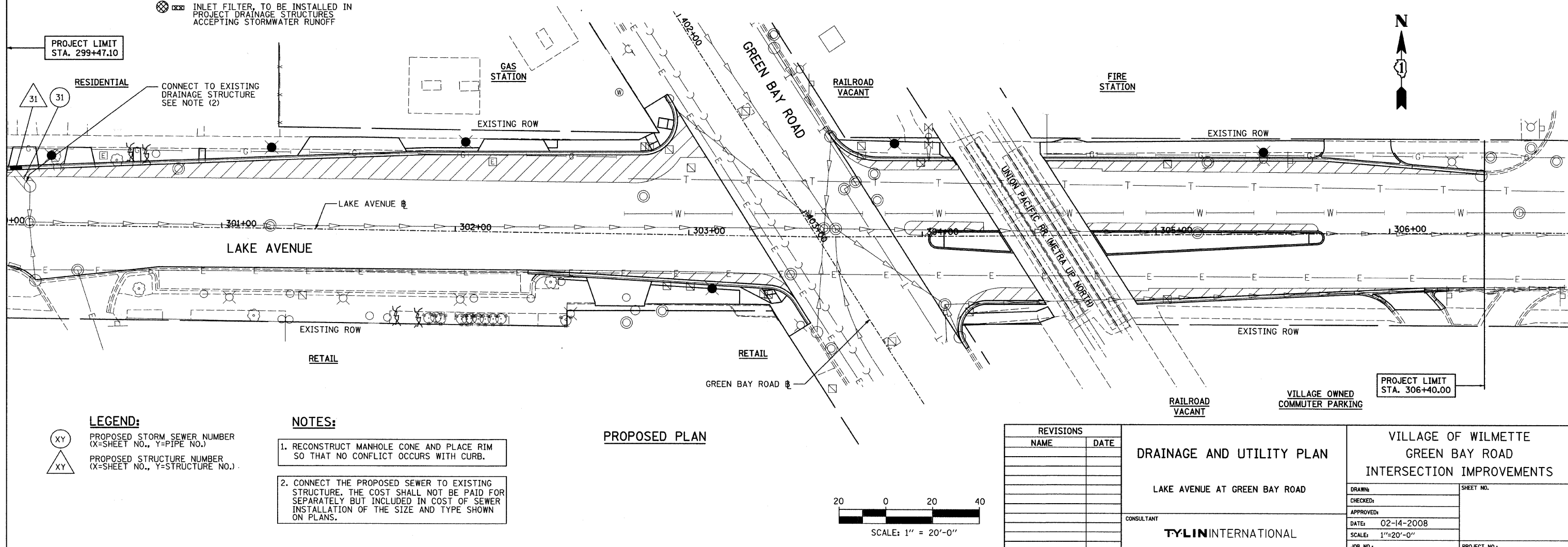
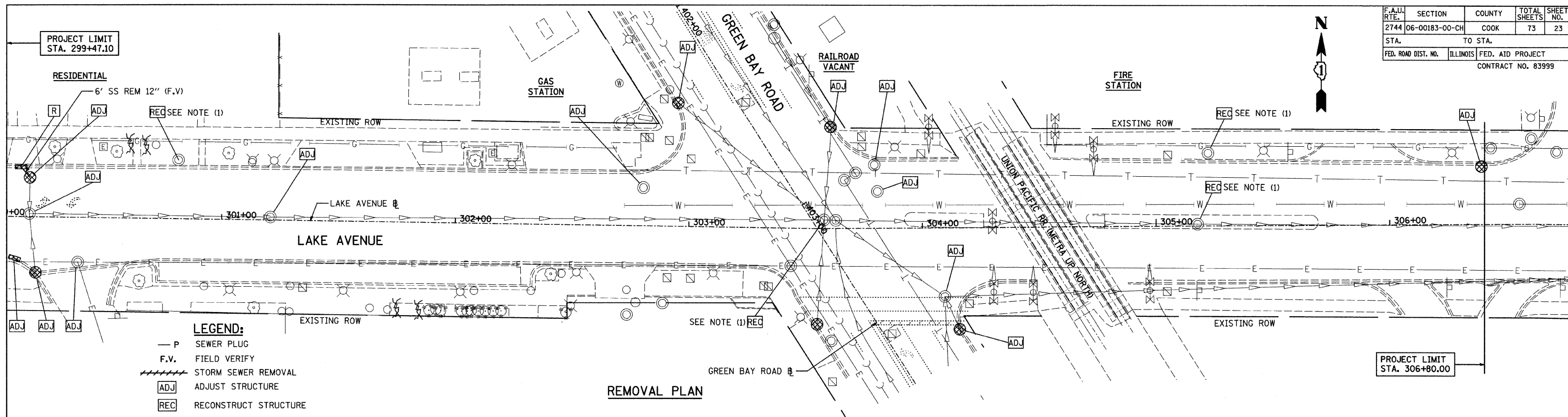
PROFILE	DATE	BY
REVIEWED		
ALIGNED		
CHECKED		
NO.:		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	23
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 83999	



PLAN	DATE	BY	REVISIONS
NO.			
NO.			
NO.			

PROFILE	DATE	BY	REVISIONS
NO.			
NO.			
NO.			

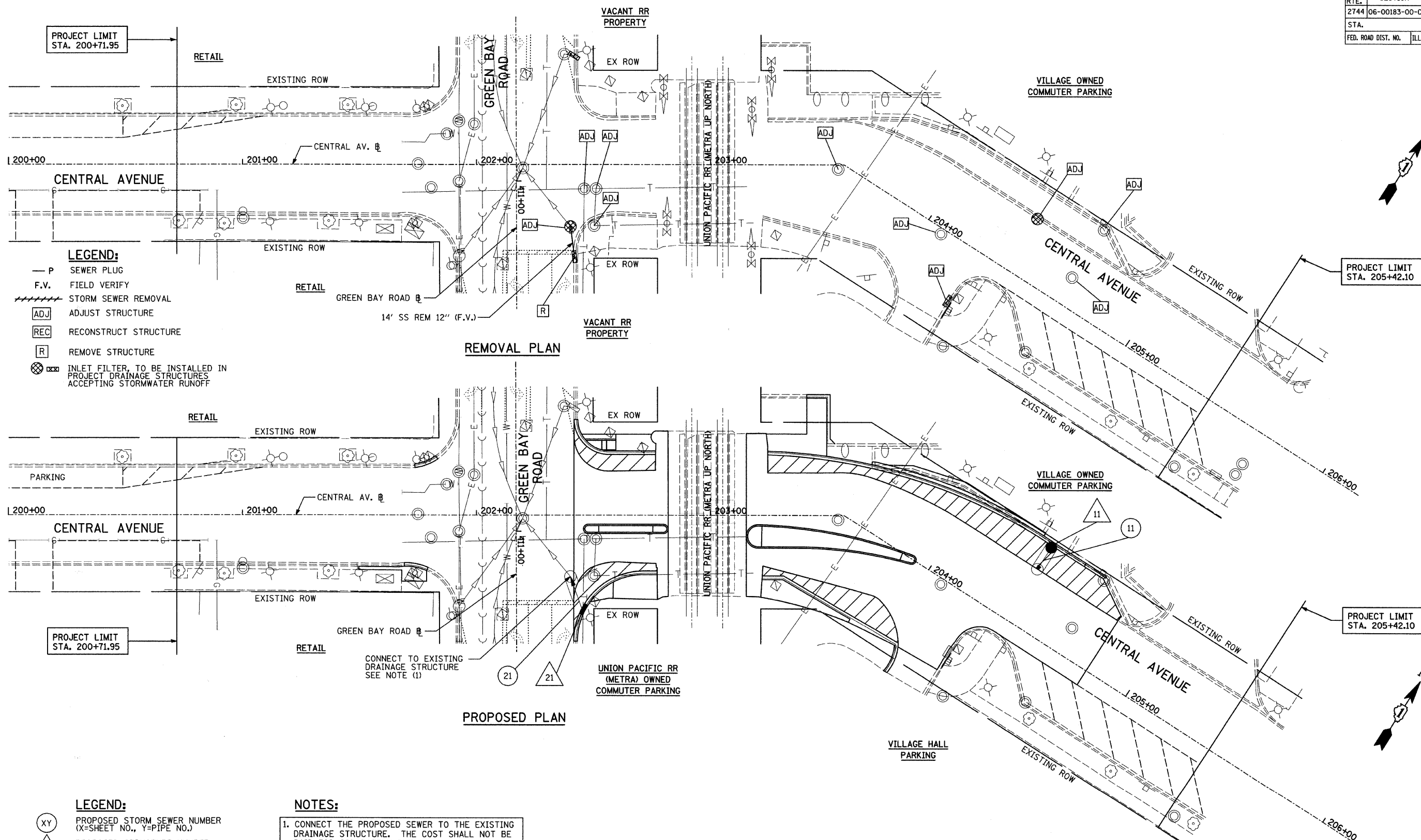


REVISIONS		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		
		DRAWN: CHECKED: APPROVED: DATE: 02-14-2008 SCALE: 1"=20'-0" JOB NO.:	
DRAINAGE AND UTILITY PLAN LAKE AVENUE AT GREEN BAY ROAD CONSULTANT TYLIN INTERNATIONAL		SHEET NO. PROJECT NO.:	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	24
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				

PLAN	DATE	BY
DESIGNED		
NOTED		
NOTED		
NO.		

PROFILE	DATE	BY
DESIGNED		
NOTED		
NOTED		
NO.		

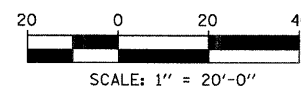


LEGEND:

- (XY) PROPOSED STORM SEWER NUMBER (X=SHEET NO., Y=PIPE NO.)
- (XY) PROPOSED STRUCTURE NUMBER (X=SHEET NO., Y=STRUCTURE NO.)

NOTES:

1. CONNECT THE PROPOSED SEWER TO THE EXISTING DRAINAGE STRUCTURE. THE COST SHALL NOT BE PAID FOR SEPARATELY BUT INCLUDED IN COST OF SEWER INSTALLATION OF THE SIZE AND TYPE SHOWN ON PLANS.

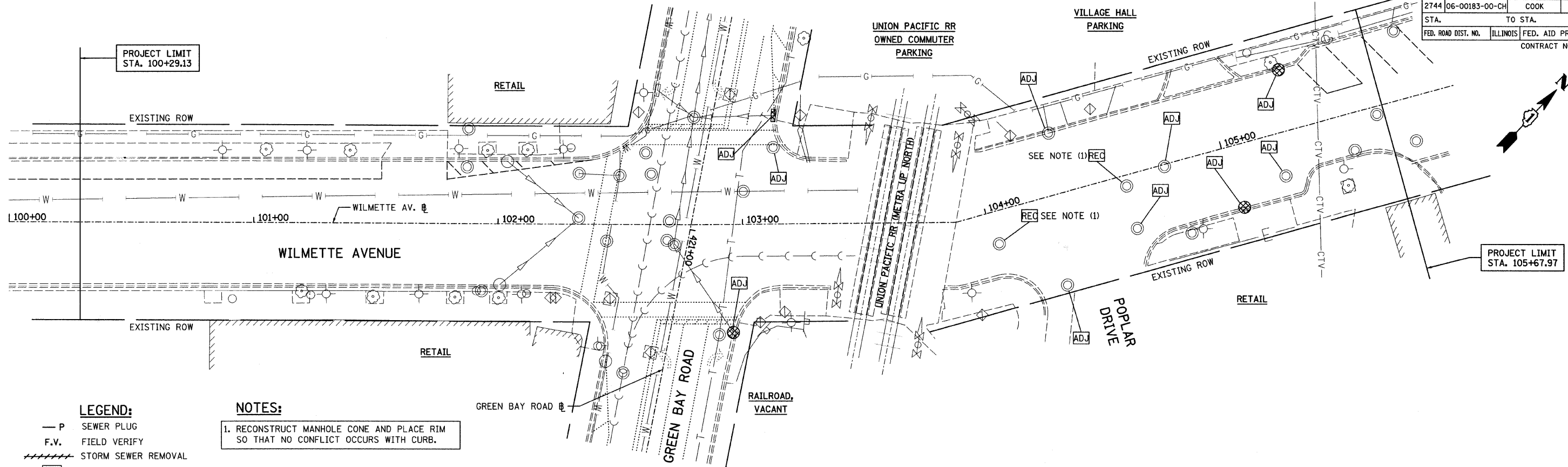


REVISIONS		DRAINAGE AND UTILITY PLAN		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE				
		CENTRAL AVENUE AT GREEN BAY ROAD		DRAWN: _____	
		CONSULTANT TYLIN INTERNATIONAL		CHECKED: _____	
				APPROVED: _____	
				DATE: 02-14-2008	
				SCALE: 1"=20'-0"	
				JOB NO.: _____	
				PROJECT NO.: _____	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	25
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 83999	

PLAN	DATE	BY	DATE	BY
SURVEYED				
ALIGNED				
CHECKED				
NO.				

PROFILE	DATE	BY	DATE	BY
SURVEYED				
GRADES				
CHECKED				
NO.				



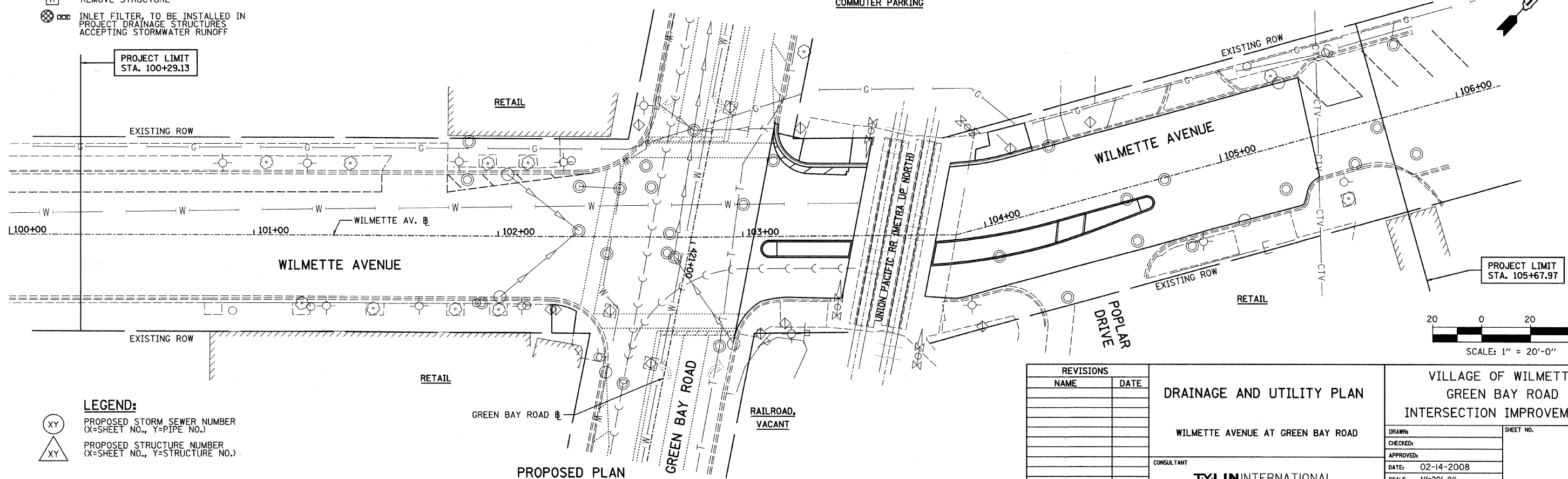
LEGEND:

- P SEWER PLUG
- F.V. FIELD VERIFY
- ++++ STORM SEWER REMOVAL
- ADJ ADJUST STRUCTURE
- REC RECONSTRUCT STRUCTURE
- R REMOVE STRUCTURE
- ⊗ INLET FILTER, TO BE INSTALLED IN PROJECT DRAINAGE STRUCTURES ACCEPTING STORMWATER RUNOFF

NOTES:

1. RECONSTRUCT MANHOLE CONE AND PLACE RIM SO THAT NO CONFLICT OCCURS WITH CURB.

REMOVAL PLAN



LEGEND:

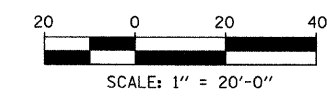
- XY PROPOSED STORM SEWER NUMBER (X=SHEET NO., Y=PIPE NO.)
- XY PROPOSED STRUCTURE NUMBER (X=SHEET NO., Y=STRUCTURE NO.)

PROPOSED PLAN

REVISIONS	
NAME	DATE

DRAINAGE AND UTILITY PLAN	
WILMETTE AVENUE AT GREEN BAY ROAD	
CONSULTANT	
TYLIN INTERNATIONAL	

VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
DRAWN:	SHEET NO.
CHECKED:	
APPROVED:	
DATE: 02-14-2008	
SCALE: 1"=20'-0"	
JOB NO.:	PROJECT NO.:



PLAN	DATE	BY
	DESIGNED	
	NOTED	
	CHECKED	
NOTE BOOK NO.		
CADD FILE NAME		

PROFILE	DATE	BY
	DESIGNED	
	NOTED	
	CHECKED	
NOTE BOOK NO.		
CADD FILE NAME		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	26
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
CONTRACT NO. 83999				

DRAINAGE STRUCTURE SCHEDULE

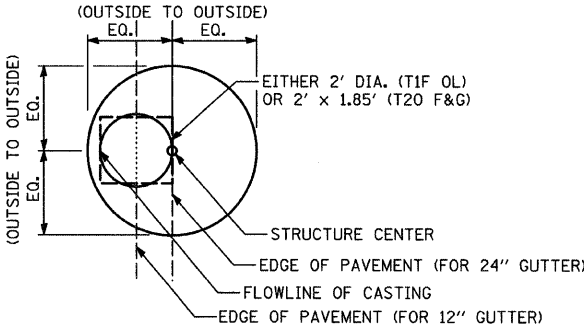
STRUCTURE NUMBER	STATION	OFFSET	STRUCTURE TYPE		DIA. (FT)	FRAME & LID	TOP OF FRAME	N INV.	E INV.	S INV.	W INV.
			MH	CB							
11	204+38.87	37.17 LT		C	2	T1F OL	609.75			607.25	
21	202+45.00	39.92 RT		A(1)	2	T1F OL	609.23				606.73
31	300+11.72	23.69 LT		A(1)	2	T1F OL	609.03			606.53	

STORM SEWER AND CULVERT SCHEDULE

PIPE NUMBER	UPSTREAM STATION	DOWNSTREAM STATION	TYPE	DIA. (IN)	LENGTH (FT)	SLOPE (%)	T.B. (CU.YD)
11	204+38.87	204+38.92	2	12	10	1.00	2.4
21	202+45.00	202+40.15	2	12	15	1.00	3.5
31	300+11.72	300+17.80	2	12	10	1.00	2.4

NOTES:

- (1) INDICATES INLET TYPE A, 2' DIAMETER, TYPE 20 FRAME & GRATE.
- (2) SEE THE DRAINAGE & UTILITY PLANS FOR LOCATION OF ALL STRUCTURES.
- (3) CATCH BASIN STATIONS AND OFFSETS ARE MEASURED TO EDGE OF PAVEMENT. (SEE BELOW)
- (4) THE STATION/OFFSET/ELEVATION NOTED FOR ALL DRAINAGE STRUCTURES LOCATED IN THE CURB LINE AND/OR SHOULDER REFER TO THE POSITION OF THE ADJACENT PROPOSED EDGE OF PAVEMENT OR EDGE OF SHOULDER AS APPLICABLE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE OFFSET NECESSARY FOR THE STRUCTURES TO SET THE FRAME AND GRATE IN THE PROPOSED LOCATION. THE STATION/OFFSET FOR ALL OTHER DRAINAGE STRUCTURES ARE DIMENSIONED TO THE CENTER OF STRUCTURE AND ELEVATION TO TOP OF GRATE.

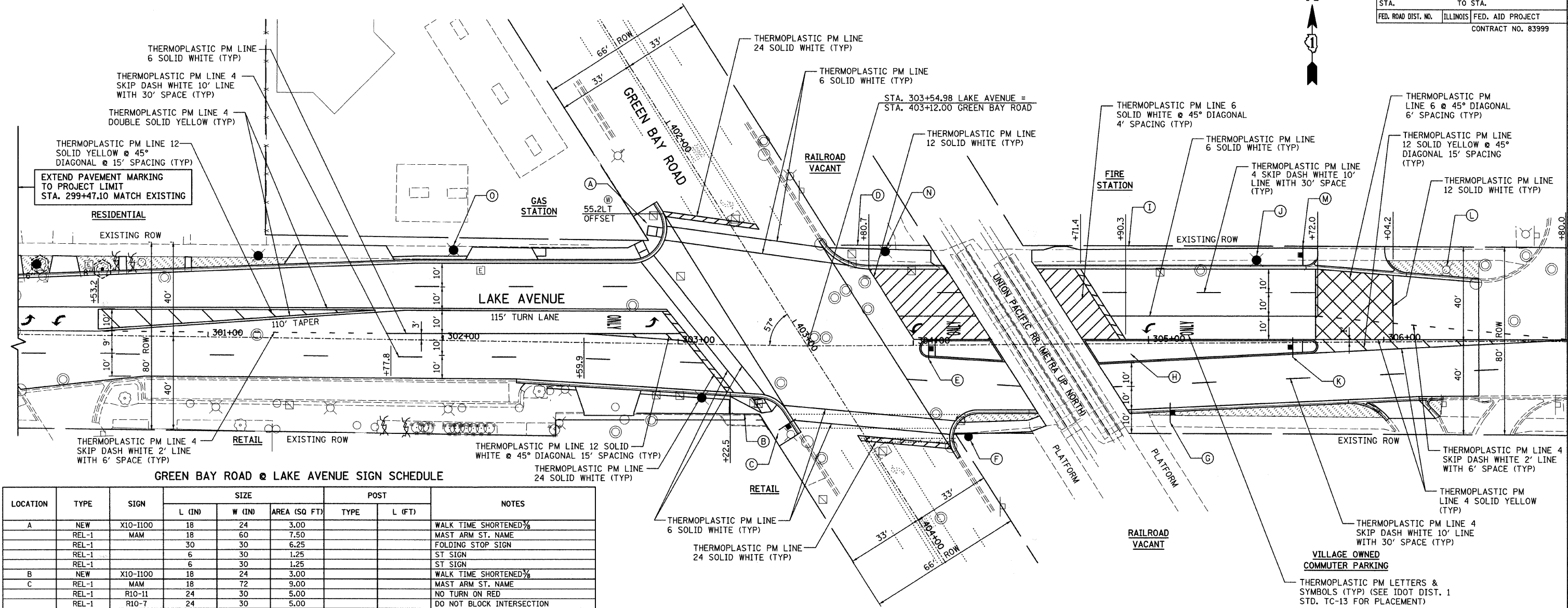


CATCH BASIN

(PRECAST REINFORCED CONCRETE SECTION)

REVISIONS		DRAINAGE SCHEDULE	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			
		CONSULTANT		
		TYLIN INTERNATIONAL		
			DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.:	PROJECT NO.:

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	27
STA. TO STA.		FED. AID PROJECT		
FED. ROAD DIST. NO.		CONTRACT NO. 83999		



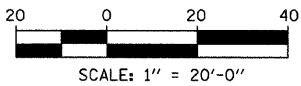
GREEN BAY ROAD @ LAKE AVENUE SIGN SCHEDULE

LOCATION	TYPE	SIGN	SIZE			POST		NOTES
			L (IN)	W (IN)	AREA (SQ FT)	TYPE	L (FT)	
A	NEW	X10-1100	18	24	3.00			WALK TIME SHORTENED 3/8
	REL-1	MAM	18	60	7.50			MAST ARM ST. NAME
	REL-1		30	30	6.25			FOLDING STOP SIGN
	REL-1		6	30	1.25			ST SIGN
	REL-1		6	30	1.25			ST SIGN
B	NEW	X10-1100	18	24	3.00			WALK TIME SHORTENED 3/8
	REL-1	MAM	18	72	9.00			MAST ARM ST. NAME
	REL-1	R10-11	24	30	5.00			NO TURN ON RED
	REL-1	R10-7	24	30	5.00			DO NOT BLOCK INTERSECTION
D	NEW	X10-1100	18	24	3.00			WALK TIME SHORTENED 3/8
	NEW	R10-7	24	30	5.00			
	NEW	R4-7	18	24	3.00	T	13	
	NEW	X10-1100	18	24	3.00			WALK TIME SHORTENED 3/8
F	REL-1	MAM	18	66	8.25			MAST ARM ST. NAME
	REL-1	R8-8	24	30	5.00			DO NOT STOP ON TRACKS
	REL-1	R10-11	24	30	5.00			NO TURN ON RED
	REL-1	R10-7	24	30	5.00			DO NOT BLOCK INTERSECTION
	REL-1		30	30	6.25			FOLDING STOP SIGN
G	REL-1	W11-1	36	36	9.00	A	14	BIKE CROSSING
	REL-1		12	24	2.00			AHEAD PLATE
H	NEW	R10-6(R)	24	30	5.00			
	NEW	R8-8	18	30	3.75			
I	NEW	R10-6(L)	24	30	5.00			
	NEW	R10-11	18	30	3.75			
J	NEW	R8-8	18	30	3.75			
	REL-1		12	18	1.50			NO PARKING SIGN
K	NEW	R4-7	18	24	3.00	T	14	
	NEW	R10-7	24	30	5.00			
	NEW	PLATE	24	18	3.00			\$100 FINE
L	NEW	W11-8	36	36	9.00			
	NEW	W11-8P	24	18	3.00			FIRE STATION
M	NEW	R10-7	24	30	5.00	B	14	
	NEW	PLATE	24	18	3.00			\$100 FINE
N	NEW	R10-11	18	30	3.75			
	REL-1	R8-8	30	30	6.25			FOLDING STOP SIGN
	REL-1		6	30	1.25			ST SIGN
	REL-1		6	30	1.25			DESTINATION SIGN
O	NEW	W9-1R	36	36	9.00			RIGHT LANE ENDS
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY

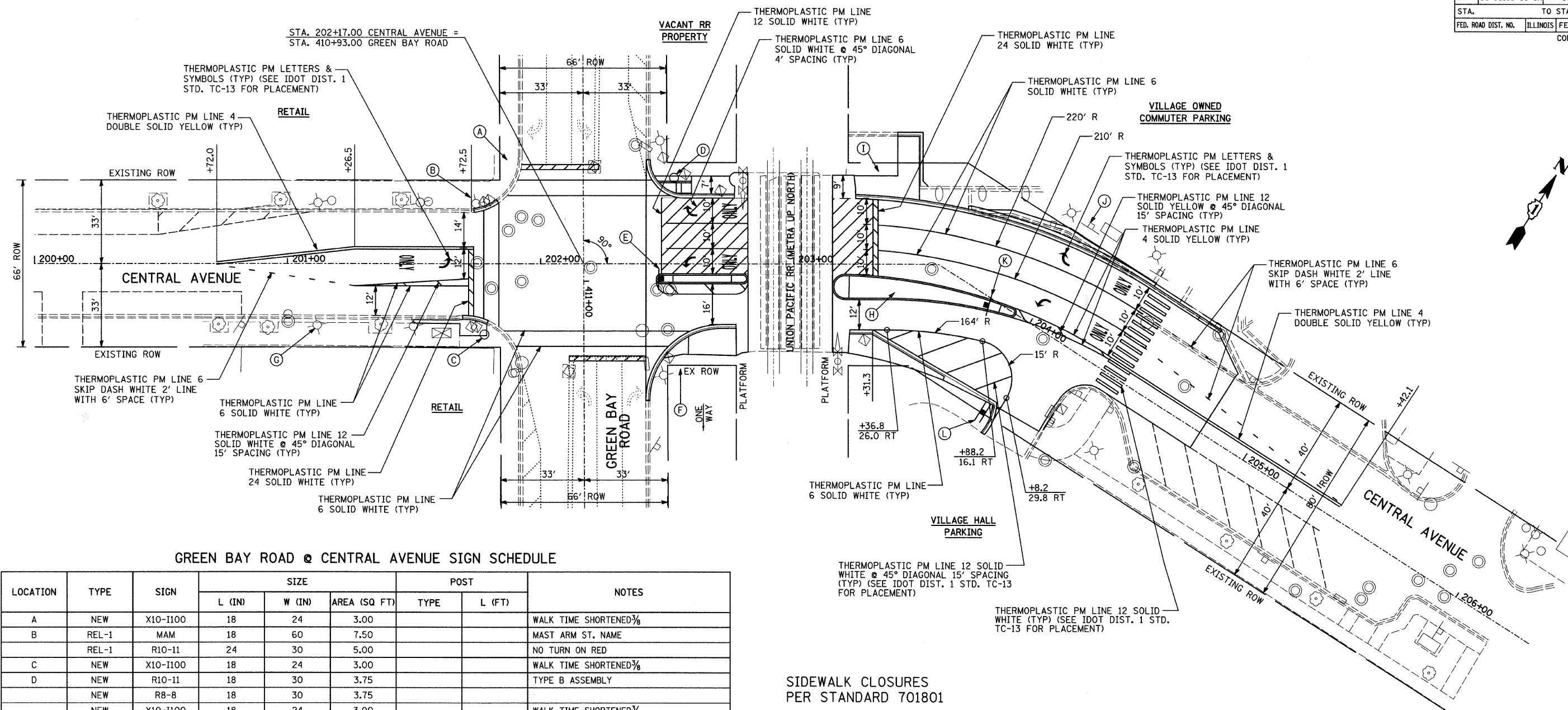
* - SEE TRAFFIC SIGNAL PLANS FOR SIGN LOCATIONS

LEGEND:

- TREE TRUNK PROTECTION
- SODDING, SALT TOLERANT



REVISIONS		PAVEMENT MARKING PLAN SIGNING PLAN RESTORATION PLAN		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE				
		LAKE AVENUE AT GREEN BAY ROAD		DRAWN: _____ CHECKED: _____ APPROVED: _____ DATE: 02-14-2008 SCALE: 1"=20'-0" JOB NO.: _____	
CONSULTANT		TYLIN INTERNATIONAL		PROJECT NO.: _____	

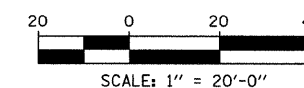


GREEN BAY ROAD @ CENTRAL AVENUE SIGN SCHEDULE

LOCATION	TYPE	SIGN	SIZE			POST		NOTES
			L (IN)	W (IN)	AREA (SQ FT)	TYPE	L (FT)	
A	NEW	X10-I100	18	24	3.00			WALK TIME SHORTENED $\frac{3}{8}$
B	REL-1	MAM	18	60	7.50			MAST ARM ST. NAME
	REL-1	R10-11	24	30	5.00			NO TURN ON RED
C	NEW	X10-I100	18	24	3.00			WALK TIME SHORTENED $\frac{3}{8}$
D	NEW	R10-11	18	30	3.75			TYPE B ASSEMBLY
	NEW	R8-8	18	30	3.75			
	NEW	X10-I100	18	24	3.00			WALK TIME SHORTENED $\frac{3}{8}$
E	NEW	R4-7	18	24	3.00	T	13	
F	NEW	X10-I100	18	24	3.00			WALK TIME SHORTENED $\frac{3}{8}$
	REL-1	MAM	18	72	9.00			MAST ARM ST. NAME
	REL-1		6	30	1.25			DESTINATION
	REL-1	R6-2L	18	24	3.00			ONE WAY
G	NEW	R3-8	30	30	6.25			ARROW LEFT/ONLY, ARROW THROUGH/RIGHT
H	NEW	R10-6(R)	24	30	5.00			
	NEW	R8-8	18	30	3.75			
I	NEW	R10-6(L)	24	30	5.00			
	NEW	R10-11	18	30	3.75			
J	NEW	R8-8	18	30	3.75	T	13	
	REL-1		12	18	1.50			NO PARKING SIGN
K	NEW	R4-7	18	24	3.00	T	13	
L	REL-1		12	12	1.00	T	13	BIKEWAY NARROWS
	REL-1		12	18	1.50			PACE BUS STOP
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY

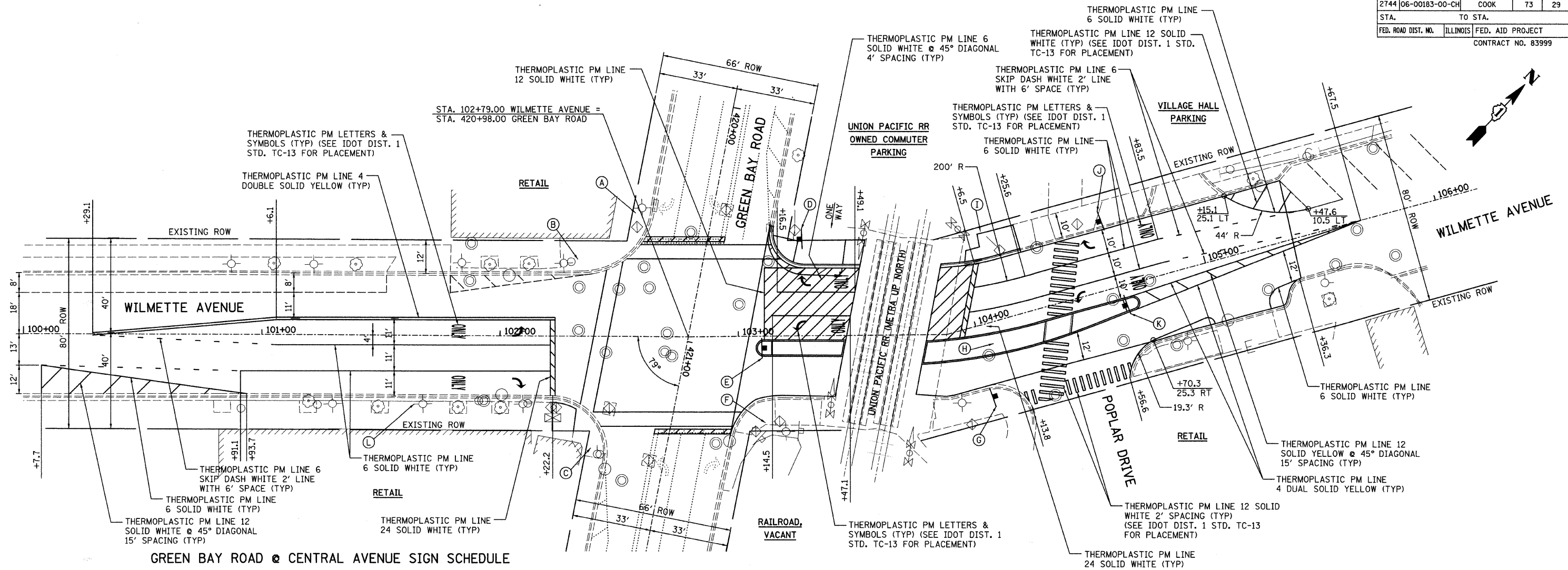
* - SEE TRAFFIC SIGNAL PLANS FOR SIGN LOCATIONS

SIDEWALK CLOSURES
PER STANDARD 701801



REVISIONS		PAVEMENT MARKING PLAN SIGNING PLAN RESTORATION PLAN CENTRAL AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			
			DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: 1"=20'-0"	
			JOB NO.:	PROJECT NO.:
			CONSULTANT	
			TYLIN INTERNATIONAL	

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	29
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				

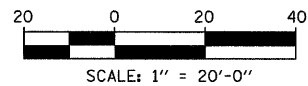


GREEN BAY ROAD @ CENTRAL AVENUE SIGN SCHEDULE

LOCATION	TYPE	SIGN	SIZE			POST		NOTES
			L (IN)	W (IN)	AREA (SQ FT)	TYPE	L (FT)	
A	NEW	X10-I100	18	24	3.00			WALK TIME SHORTENED $\frac{3}{8}$
B	REL-1	MAM	18	72	9.00			MAST ARM ST. NAME
C	NEW	X10-I100	18	24	3.00			WALK TIME SHORTENED $\frac{3}{8}$
D	NEW	R10-11	18	30	3.75	2-B	32	TYPE B ASSEMBLY
	NEW	R8-8	18	30	3.75			WALK TIME SHORTENED $\frac{3}{8}$
	NEW	X10-I100	18	24	3.00			WALK TIME SHORTENED $\frac{3}{8}$
E	NEW	R4-7	18	24	3.00	T	13	
F	NEW	X10-I100	18	24	3.00			WALK TIME SHORTENED $\frac{3}{8}$
	REL-1	MAM	18	72	9.00			MAST ARM ST. NAME
	REL-1		6	30	1.25			ST NAME
	REL-1		6	30	1.25			ST NAME
	REL-1		6	30	1.25			DESTINATION
G	NEW	W11-1	36	36	9.00	A	14	FLOURESCENT YELLOW GREEN
H	NEW	R10-6(R)	24	30	5.00			
	NEW	R8-8	18	30	3.75			
I	NEW	R10-6(L)	24	30	5.00			
	NEW	R10-11	18	30	3.75			
J	NEW	R8-8	18	30	3.75	A	13	
	NEW	W11-1	36	36	9.00			FLOURESCENT YELLOW GREEN
K	NEW	R4-7	18	24	3.00	T	13	
L	NEW	R3-8B	30	30	6.25			
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY
SIGNALS*	NEW	R10-5	24	30	5.00			LEFT TURN ON ARROW ONLY

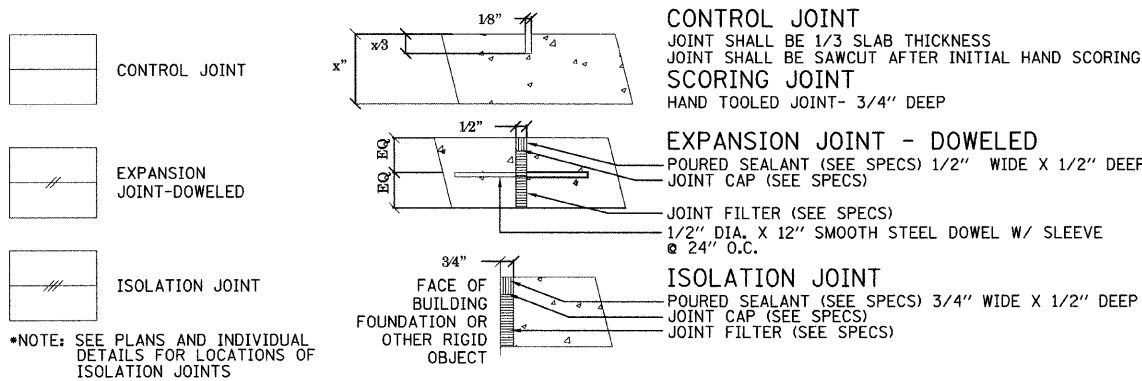
* - SEE TRAFFIC SIGNAL PLANS FOR SIGN LOCATIONS

NOTE:
ADDITIONAL PAVEMENT MARKING
ON GREEN BAY ROAD MAY BE DONE
AT TIME OF CONSTRUCTION

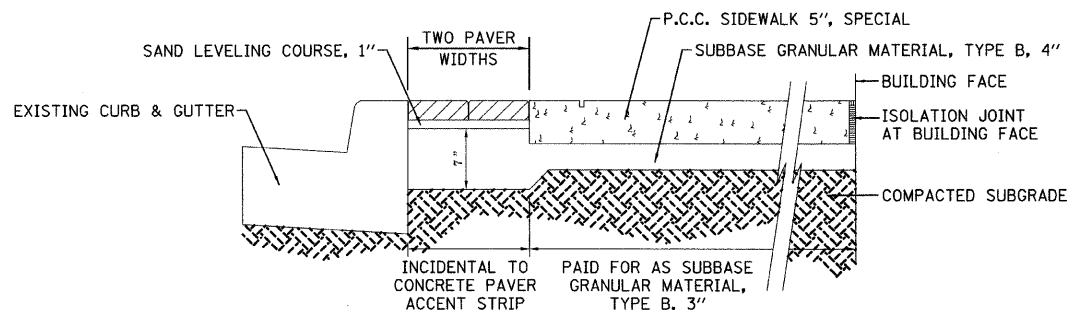


REVISIONS		PAVEMENT MARKING PLAN SIGNING PLAN RESTORATION PLAN WILMETTE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS		
NAME	DATE		DRAWN:	SHEET NO.	
			CHECKED:		
			APPROVED:		
			DATE: 02-14-2008		
			SCALE: 1"=20'-0"		
			JOB NO. 1	PROJECT NO. 1	
			CONSULTANT		
			TYLIN INTERNATIONAL		

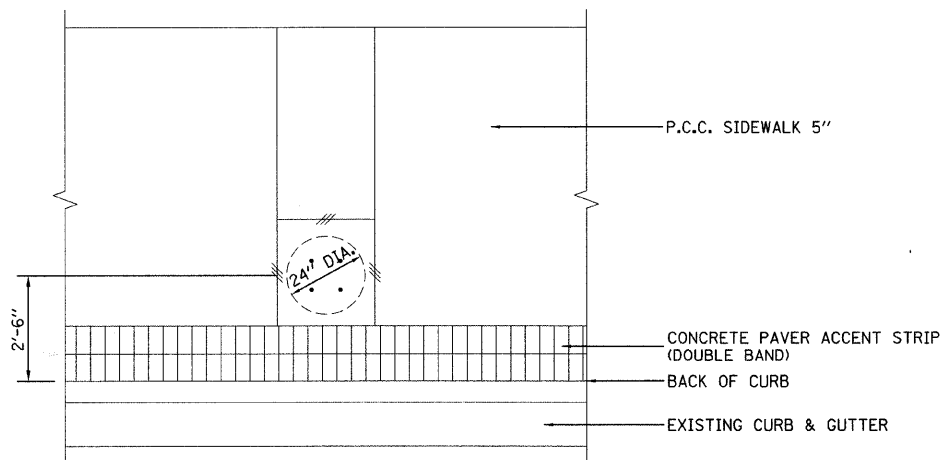
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	30
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				



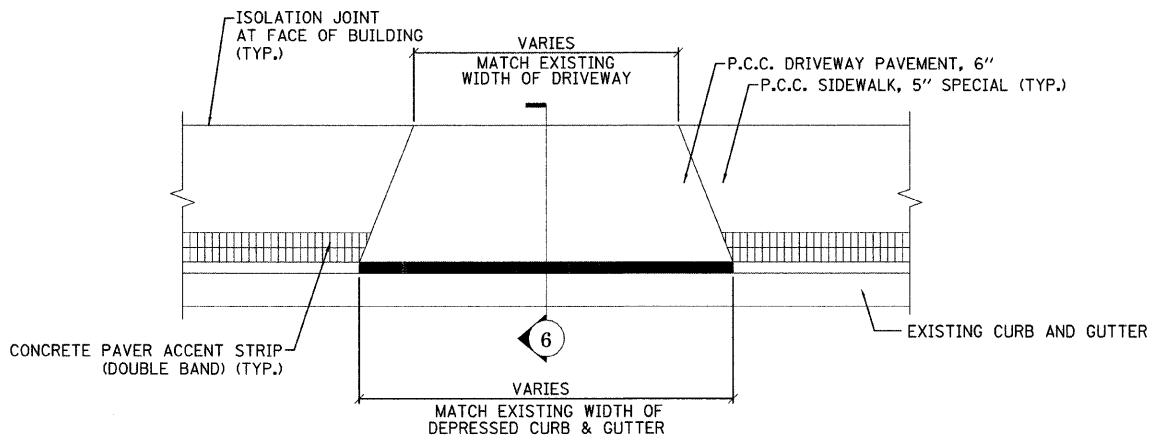
1 **CONC. PAVEMENT / WALK JOINT LEGEND**
NO SCALE



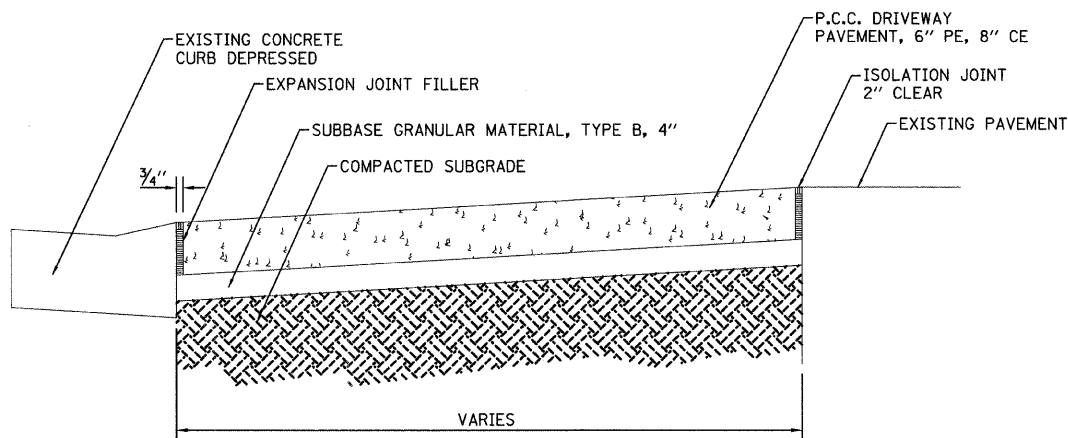
2 **P.C.C. FLUSH BAND W/ CONC. UNIT PAVERS - CENTRAL AVENUE AND LAKE AVENUE**
NO SCALE



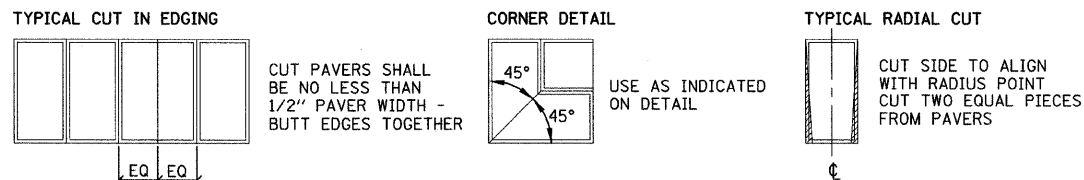
3 **LIGHT FOUNDATION PAVING - CENTRAL AVENUE AND LAKE AVENUE**
NO SCALE



4 **DRIVE LAYOUT (TYP.)**
NO SCALE



5 **P.C.C. DRIVEWAY - 6" PRIVATE ENTRANCE, 8" COMMERCIAL ENTRANCE**
NO SCALE

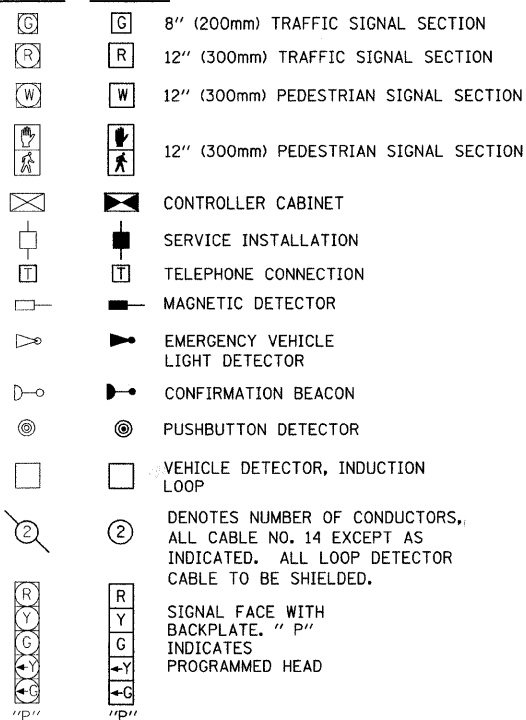


6 **TYPICAL PAVER CUTS**
NO SCALE

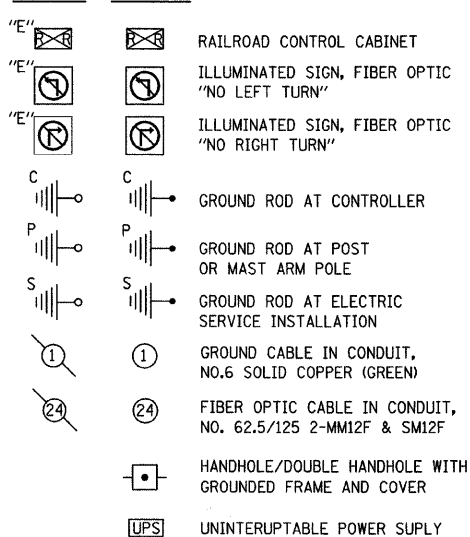
REVISIONS		SIDWALK PAVEMENT DETAILS		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE				
CONSULTANT		TYLIN INTERNATIONAL		SHEET NO.	
				DRAWN:	
				CHECKED:	
				APPROVED:	
				DATE: 02-14-2008	
				SCALE: NONE	
				JOB NO.:	
				PROJECT NO.:	

CABLE PLAN LEGEND

EXISTING PROPOSED



EXISTING PROPOSED



SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
3	EACH	TRAFFIC SIGNAL POST, 14 FOOT
6	EACH	PEDESTRIAN PUSH BUTTON
1	EACH	PEDESTRIAN PUSH BUTTON POST
1	EACH	DOUBLE HANDHOLE
4	EACH	HEAVY DUTY HANDHOLE
99	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
50	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
38	FOOT	CONDUIT IN TRENCH, 5" DIA., GALVANIZED STEEL
35	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
60	FOOT	CONDUIT PUSHED, 6" DIA., GALVANIZED STEEL
2	EACH	HANDHOLE
322	FOOT	ELECTRIC CABLE IN CONDUIT, GROUNDING, NO. 6 1/C
744	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C
2209	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
4947	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
364	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
2455	EACH	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 28 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 34 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE WITH DUAL MAST ARMS 22FT AND 40FT.
15	EACH	SIGNAL HEAD, POLYCARBONATE, LED 1-FACE, 3 SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED 1-FACE, 5 SECTION, MAST ARM MOUNTED
2	EACH	SIGNAL HEAD, POLYCARBONATE, LED 1-FACE, 3 SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, POLYCARBONATE, LED 2-FACE, 2-3 SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED 2-FACE, 1-3 SECTION, 1-5 SECTION BRACKET MOUNTED
4	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED 1-FACE, BRACKET MOUNTED
1	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED 2-FACE, BRACKET MOUNTED
16	EACH	TRAFFIC SIGNAL BACKPLATE
9	EACH	INDUCTIVE LOOP DETECTOR
811	FOOT	DETECTOR LOOP, TYPE I
4	EACH	LIGHT DETECTOR
1	FOOT	LIGHT DETECTOR AMPLIFIER
187	EACH	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
5	FOOT	REMOVE EXISTING CONCRETE FOUNDATION
766	EACH	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED
1	EACH	TEMPORARY TRAFFIC SIGNAL INSTALLATION
2	EACH	DRILL EXISTING HANDHOLE
2	EACH	ILLUMINATED SIGN, LED
8	FOOT	CONCRETE FOUNDATION, TYPE A
4	FOOT	CONCRETE FOUNDATION, TYPE D
13.5	FOOT	CONCRETE FOUNDATION, TYPE E, 30 INCH DIA.
13	FOOT	CONCRETE FOUNDATION, TYPE E, 36 INCH DIA.
3	EACH	REBUILD EXISTING HANDHOLE
1	EACH	REBUILD EXISTING HANDHOLE, SPECIAL
6	EACH	REMOVE EXISTING HANDHOLE
9861	FOOT	REMOVE EXISTING CABLE FROM CONDUIT
232	FOOT	ELECTRIC CABLE IN CONDUIT, RAILROAD, NO. 14-3C
1	EACH	FULL - ACTUATED CONTROLLER, IN TYPE V CABINET
1	EACH	UNINTERRUPTABLE POWER SUPPLY, STANDARD
1	EACH	TRANSCEIVER, FIBER OPTIC
93	FOOT	REMOVE AND RE-INSTALL ELECTRIC CABLE IN CONDUIT
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING

FOUNDATION	DEPTH	DEPTH (FT.)	CABLE SLACK	(FT.)	VERTICAL	(FT.)
TYPE A - POST	4		HANDHOLE	6.5	ALL FOUNDATIONS	3.5
D - CONTROLLER	4		DOUBLE HANDHOLE	13	MAST ARM (L) POLE	=20' HL-2
E - MAST ARM POLE			SIGNAL POST	2	BRACKET MOUNTED	13
24"	10		CONTROLLER CAB.	1	PED. PUSHBUTTON	4
30"	15		FIBER OPTIC	13	ELECTRICAL SERVICE	13.5
			ELECTRICAL SERVICE	1	SERVICE TO GROUND	13.5
			GROUND CABLE	1	POSTMOUNTED	6

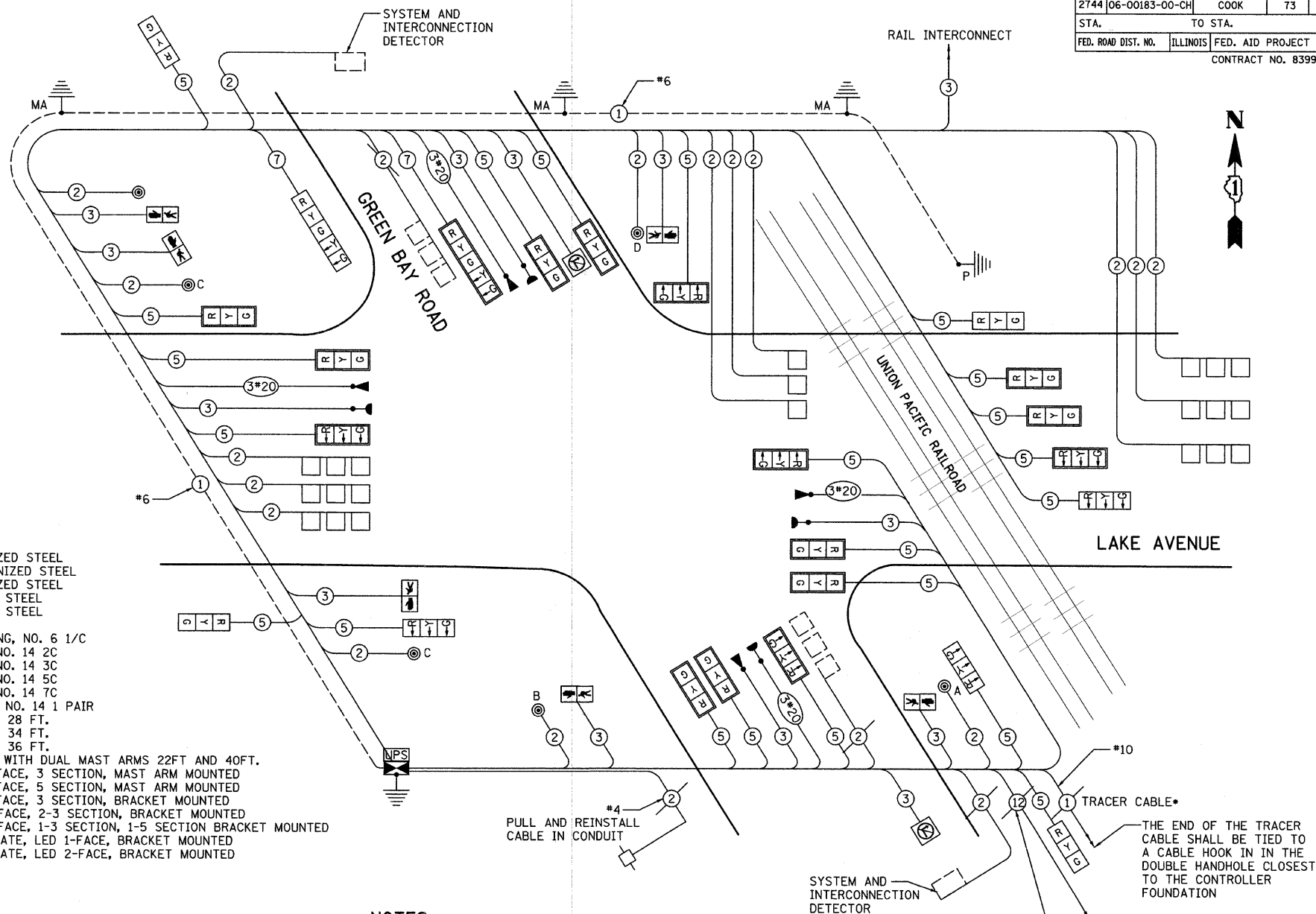
NOTES:

PEDESTRIAN PUSH BUTTON "A" TO CALL PHASE 3.
PEDESTRIAN PUSH BUTTON "B" TO CALL PHASE 3 AND 6.
PEDESTRIAN PUSH BUTTON "C" TO CALL PHASE 4 AND 6.
PEDESTRIAN PUSH BUTTON "D" TO CALL PHASE 4.

CABLE PLAN
NOT TO SCALE

* REMOVE AND REINSTALL FIBER OPTIC IN CONDUIT (APPROX. 55 FT.)

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	32
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				



REVISIONS	NAME	DATE

CABLE PLAN, SCHEDULE OF
QUANTITIES AND PHASE
DESIGNATION DIAGRAM

LAKE AVENUE AT GREEN BAY ROAD

CONSULTANT

TYLIN INTERNATIONAL

VILLAGE OF WILMETTE
GREEN BAY ROAD
INTERSECTION IMPROVEMENTS

DRAWN: BK	SHEET NO.
CHECKED: DAJ	
APPROVED: DAJ	
DATE: 02-14-2008	
SCALE: NONE	
JOB NO.:	PROJECT NO.:

PROPOSED NORMAL SEQUENCE OF OPERATIONS

MOVEMENT																																													FLASH																																																																																																																																																																																																																					
PHASE	1+5								1+6				2+5				2+6				3+7								3+8				4+7				4+8																																																																																																																																																																																																																													
INTERVAL	1	2A	2B	3A	3B	4A	4B	5	6	7A	7B	8A	8B	9	10A	10B	11A	11B	12	13	14A	14B	15	16A	16B	17A	17B	17C	17D	18A	18B	18C	18D	19	20	21A	21B	21C	21D	22	23	24A	24B	25		26	27A	27B	27C	27D	28A	28B	29A	29B	29C	29D																																																																																																																																																																																																										
CHANGE TO		1+6		2+5		2+6 3+7 3+8 4+7 4+8				1+5 2+5 3+7 3+8 4+7 4+8		2+6			3+7 3+8 4+7 4+8		2+6				1+6 3+7 3+8 4+7 4+8			3+8		4+7				4+8						4+8						4+8					1+5 1+6 2+5 2+6 3+7		3+8		4+7																																																																																																																																																																																																															
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R																																																																																																																																																																																																						
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B			R				R	R	R	R	R	R	R		Y	R		G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R																																																																																																																																																																																																		
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	R	R	R	R	R	G	G	Y	R	G	G	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R																																																																																																																																																																																																					
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B																																																																																																																																																																																																																																																																		

P = ILLUMINATED PERSON = WALK

FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK

H = ILLUMINATED SOLID HAND = DON'T WALK

* TO APPEAR ONLY UPON PUSHBUTTON ACTUATION

** FLASHING IS TO TERMINATE AT THE COMPLETION OF THE PEDESTRIAN INTERVAL CLEARANCE.

AND FLASHING TIMINGS TO BE SET ONLY ON PHASES WHERE

AND FLASHING ARE INDICATED IN THE SEQUENCE OF OPERATION.

= THIS OR FLASHING INTERVAL MAY FINISH TIMING IN THE BI-DIRECTIONAL STRAIGHT THROUGH MOVEMENT IF THE LEFT ARROW TIME IS NOT SUFFICIENT TO COMPLETE OR FLASHING INTERVALS.

PHASE 2 AND 6 SHALL BE PLACED ON RECALL FOR COORDINATED SYSTEM

REVISIONS		SEQUENCE OF OPERATIONS LAKE AVENUE AT GREEN BAY ROAD CONSULTANT TYLIN INTERNATIONAL	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN:	
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.:	
			SHEET NO.	
			PROJECT NO.:	

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

																				PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6	CLEAR TO NORMAL SEQUENCE
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1		1		1		5			5	9	9		12			12							
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	2	3	4	5	
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	3	1F	4 OR 5	1H	1J	2, 4 OR 5	3	2	1N	3, 4 OR 5	1Q	1R	2	1T	1U	3, 4 OR 5					
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	G	R	R	R	
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R ←G	R ←G	R ←Y		R ←Y	R	R	R	R	R	G ←G	Y	R	G	G	G	G	Y	R	G ←G	R	R	R	
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	R	R	R	R	G	Y	R	G	R	R	R	G	Y	R	G	Y	R	R	G	R	R	
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	←Y	←R	←G	←G	←Y	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	←R	
LAKE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	
LAKE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	
LAKE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	
LAKE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	
LAKE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G		
LAKE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G		
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF LAKE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF LAKE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING LAKE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	FH	H	H	FH	H	H	H	FH	H	H	FH	H	H	H	H	H	H	

P = ILLUMINATED PERSON = WALK

FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK

H = ILLUMINATED SOLID HAND = DON'T WALK

◆= EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

REVISIONS		EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 1 OF 2 LAKE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.:	PROJECT NO.:
CONSULTANT				
TYLIN INTERNATIONAL				

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

																														PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6					
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	15				15		15				19				19	22				22	25				25			25									CLEAR TO NORMAL SEQUENCE	
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD	1EE	1FF	1GG	1HH	1JJ	1KK	1LL	1MM	1NN	1PP	1QQ	1RR	1SS	1TT	1UU	1VV	1WW	1XX	1YY	1ZZ	1AAA	1BBB	1CCC	1DDD	2	3	4		5
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1W	1X	1Y	2 OR 3	1AA	4	1CC	1DD	1EE	5	1GG	1HH	1JJ	1KK	2, 3 OR 5	4	1NN	1PP	2, 3 OR 4	5	1SS	1TT	1UU	1VV	2 OR 3	1XX	1YY	4	1AAA	1BBB	1CCC	1DDD	5					◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	R	◆
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	R	◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	◆
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	←R	◆	
LAKE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	G	R	R	R	R	G	G	G	Y	R	G	G	G	G	G	G	Y	R	R	R	G	R	◆
LAKE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	←G	←G	←Y	←R	←G	←G	←G	←G	←Y	←R	←G	←G	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆	
LAKE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	←Y	←R	←R	←R	←G	←G	←Y	←R	←R	←R	←G	←Y	←R	←R	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆	
LAKE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	Y	R	R	R	G	R	R	R	R	G	Y	R	R	R	G	G	G	G	Y	R	R	R	R	R	R	◆	
LAKE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	Y	R	G	Y	R	G	G	G	G	G	R	R	R	G	◆	
LAKE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	←G	←G	←Y	←R	←Y	←R	←G	←G	←G	←G	←R	←R	←R	←R	←R	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆		
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF LAKE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	FH	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	◆	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF LAKE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	FH	H	H	H	H	FH	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	◆	
PEDESTRIAN SIGNALS CROSSING LAKE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆	

P = ILLUMINATED PERSON = WALK
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
H = ILLUMINATED SOLID HAND = DON'T WALK
◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

REVISIONS		EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 2 OF 2 LAKE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS				
NAME	DATE						
CONSULTANT		TYLIN INTERNATIONAL	SHEET NO.				
		DATE: 02-14-2008	PROJECT NO.				
		SCALE: NONE					

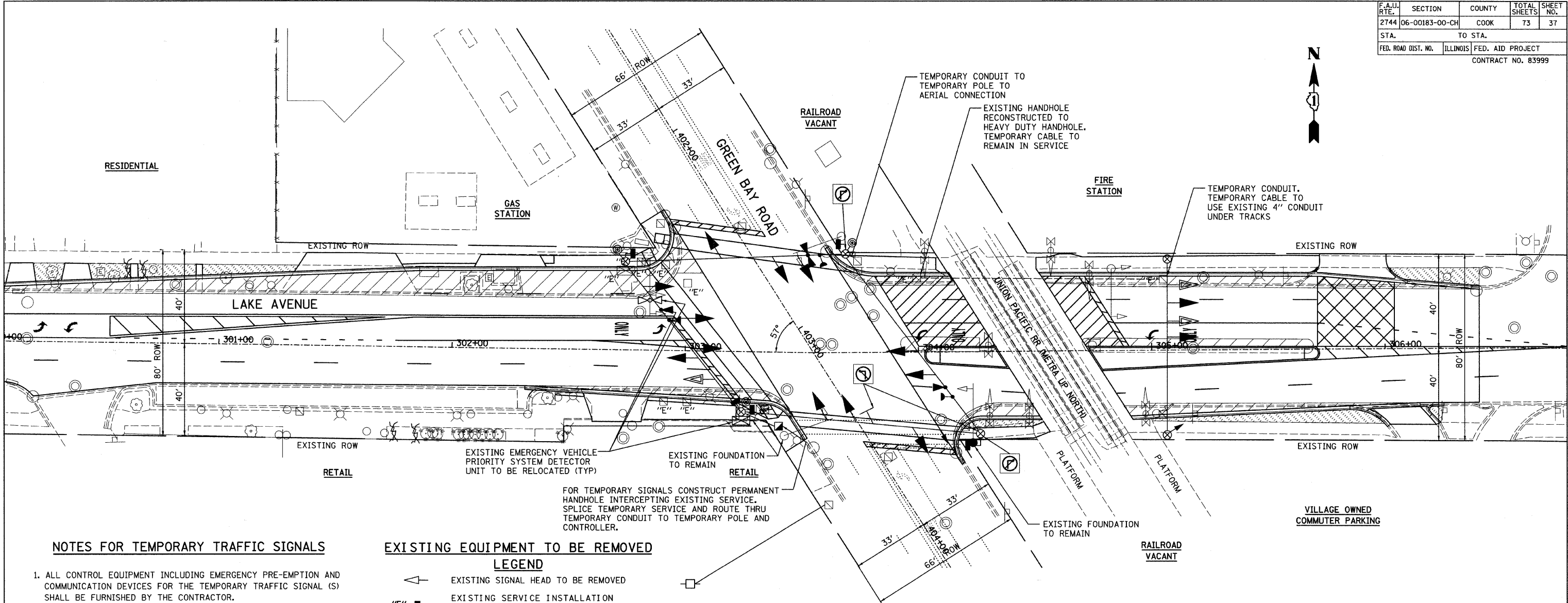
PROPOSED RAILROAD PREEMPTION SEQUENCE OF OPERATIONS

																PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4		PREEMPTOR NUMBER 4		PREEMPTOR NUMBER 5		PREEMPTOR NUMBER 2											
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1		5		9		12		15		19		22		25																		CLEAR TO NORMAL SEQUENCE	
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																2		3		4		5												
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	2	3	4	5						
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	1H	2	2	2	1M	2	1P	2	1R	2	1T	2	1V	2	1X	2	1Z	2	3	4	5		▲					
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲					
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R Y	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲					
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	G	▲					
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	Y	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	▲					
LAKE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	G	R	R	G	G	R	R	R	G	G	G	R	R	G	Y	R	R	▲					
LAKE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	R	R	R	R	R	R	R	R	G	G	G	G	R	R	R	R	R	R	R	G	G	R	R	G	Y	R	R	▲						
LAKE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	▲						
LAKE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	Y	R	R	R	R	R	Y	R	R	R	R	R	R	R	▲					
LAKE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	▲					
LAKE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	R	R	R	R	R	R	R	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	▲						
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF LAKE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	▲					
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF LAKE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲					
PEDESTRIAN SIGNALS CROSSING LAKE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	FH	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲					
INTERNALLY ILLUMINATED NRT SIGNS - GREEN BAY ROAD	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	▲					
																											HOLD							

RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL 2, 3 OR 4 (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

P = ILLUMINATED PERSON = WALK
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
H = ILLUMINATED SOLID HAND = DON'T WALK

REVISIONS		RAILROAD PREEMPTION SEQUENCE OF OPERATIONS LAKE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.	PROJECT NO.
CONSULTANT		TYLIN INTERNATIONAL		



NOTES FOR TEMPORARY TRAFFIC SIGNALS

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL (S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS1 OR TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE 12" (300mm). HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SIGNAL SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.

EXISTING EQUIPMENT TO BE REMOVED

LEGEND

- EXISTING SIGNAL HEAD TO BE REMOVED
- "E" ■ EXISTING SERVICE INSTALLATION TO BE REMOVED
- "E" ⊗ EXISTING CONTROLLER AND FOUNDATION TO BE REMOVED
- "E" □ EXISTING HANDHOLE TO BE REMOVED
- "E" ⊞ EXISTING HEAVY DUTY HANDHOLE TO BE REMOVED
- EXISTING EMERGENCY VEHICLE SYSTEM DETECTOR TO BE REMOVED
- EXISTING EMERGENCY VEHICLE SYSTEM BEACON TO BE REMOVED
- EXISTING SIGNAL POST AND FOUNDATION TO BE REMOVED
- EXISTING PEDESTRIAN SIGNAL HEAD TO BE REMOVED
- EXISTING PEDESTRIAN PUSHBUTTON TO BE REMOVED
- EXISTING STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED
- EXISTING ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE STATE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE STATE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

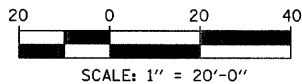
EACH

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- | | | |
|---|------|--------------------------------|
| 7 | EACH | SIGNAL HEAD, 1-FACE, 3-SECTION |
| 4 | EACH | SIGNAL HEAD, 1-FACE, 4-SECTION |
| 4 | EACH | SIGNAL HEAD, 1-FACE, 5-SECTION |
| 9 | EACH | TRAFFIC SIGNAL BACKPLATE |
| 4 | EACH | STEEL MAST ARM AND POLE |
| 2 | EACH | SIGNAL POST |
| 4 | EACH | PEDESTRIAN SIGNAL HEAD, 1-FACE |
| 1 | EACH | PEDESTRIAN SIGNAL HEAD, 2-FACE |

TEMPORARY TRAFFIC SIGNAL LEGEND

- TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED ORIGINAL LOCATION
- TEMPORARY TRAFFIC SIGNAL HEAD SPAN WIRE MOUNTED SECONDARY LOCATION
- TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM
- TEMPORARY CONTROLLER CABINET
- TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE
- TEMPORARY SERVICE INSTALLATION
- TEMPORARY PEDESTRIAN SIGNAL HEAD, BRACKET MOUNTED
- MICROWAVE VEHICLE SENSOR
- PEDESTRIAN PUSHBUTTON DETECTOR
- EMERGENCY VEHICLE LIGHT DETECTOR
- CONFIRMATION BEACON
- VEHICLE DETECTOR, INDUCTION LOOP
- CT COMMON TRENCH
- UD UNIT DUCT
- G.S. CONDUIT IN TRENCH OR PUSHED
- HANDHOLE
- HEAVY DUTY HANDHOLE
- ILLUMINATED SIGN "NO LEFT TURN" OR "NO RIGHT TURN"

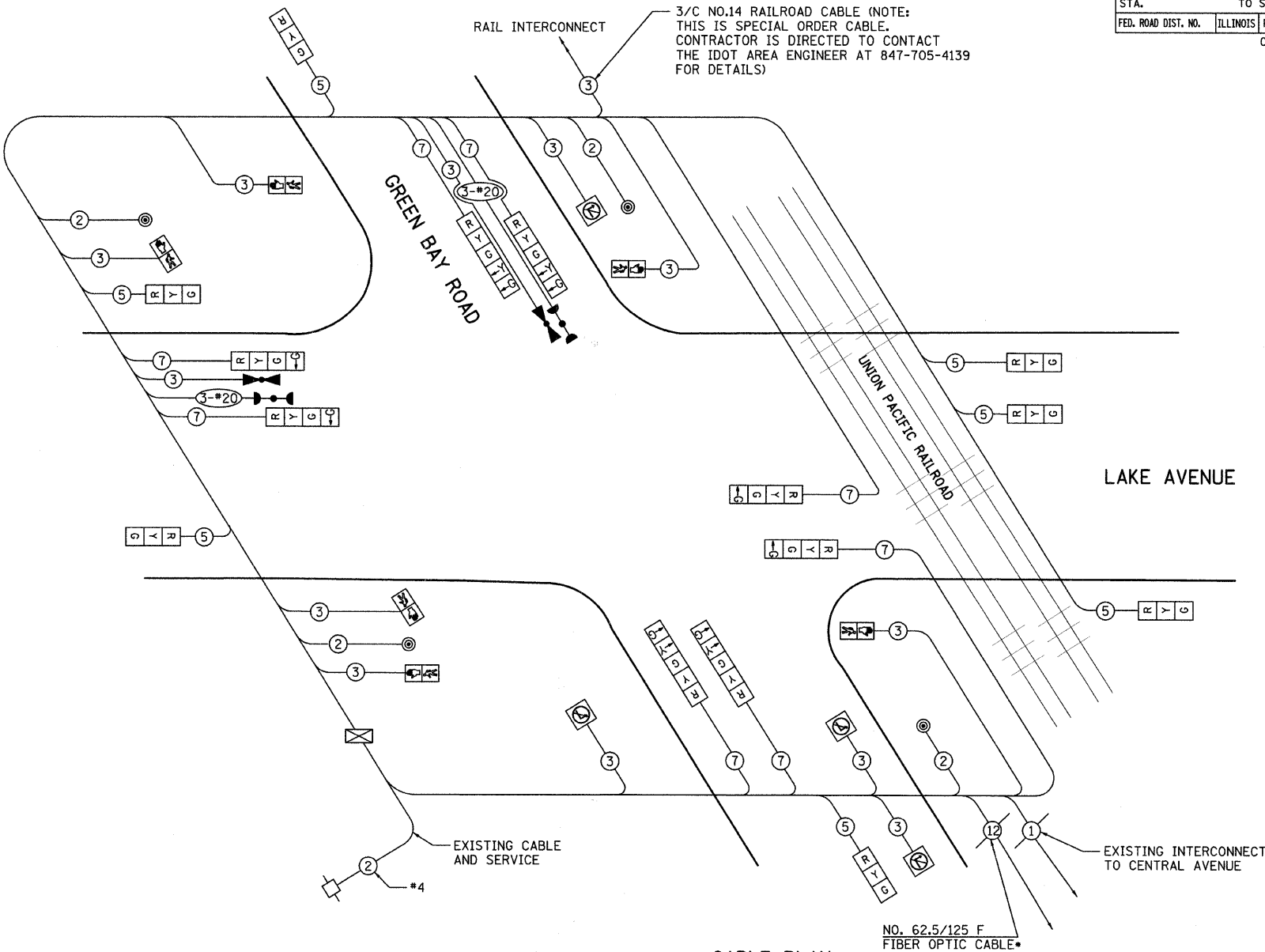
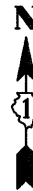


REVISIONS	
NAME	DATE

TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN	
LAKE AVENUE AT GREEN BAY ROAD	
CONSULTANT	
TYLIN INTERNATIONAL	

VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
DRAWN: BK	SHEET NO.
CHECKED: DAJ	
APPROVED: DAJ	
DATE: 02-14-2008	
SCALE: 1"=20'-0"	
JOB NO.	PROJECT NO.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	38
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 83999				



CABLE PLAN
NOT TO SCALE

* PULL AND REINSTALL EXISTING FIBER OPTIC CABLE TO MAINTAIN INTERCONNECT





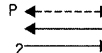































TEMPORARY CABLE DIAGRAM LEGEND

- [R] TEMPORARY TRAFFIC SIGNAL SECTION OR PEDESTRIAN SIGNAL SECTION 12" (300mm)
- [X] TEMPORARY CONTROLLER CABINET
- [] TEMPORARY SERVICE INSTALLATION
- (5) INDICATES NUMBER OF CONDUCTORS. IN CABLE. ALL CONDUCTORS TO BE NUMBER 14 AWG WIRE UNLESS OTHERWISE NOTED.
- [] VEHICLE DETECTOR, INDUCTION LOOP
- [] TEMPORARY PEDESTRIAN PUSHBUTTON DETECTOR
- [] 12" (300mm) PEDESTRIAN SIGNAL SECTION
- [] TEMPORARY INTERNALLY ILLUMINATED SIGNS
- [] EMERGENCY VEHICLE LIGHT DETECTOR
- [] CONFIRMATION BEACON

I. D. O. T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO. LAMPS	WATTAGE	% OPERATIONS	TOTAL WATTAGE
SI GNAL (RED)	12	17	0.50	102
(YELLOW)	12	25	0.25	75
(GREEN)	12	15	0.25	45
ARROW	12	12	0.10	14.4
PED. SIGNAL	8	25	1.00	200
CONTROLLER	1	100	1.00	100
		25	0.05	0
				0
				0
				0
FLASHER LED			0.05	0
TOTAL =				536.4
ENERGY COSTS- BILLED TO: VILLAGE OF WILMETTE				
(ADDRESS) WILMETTE, IL				
ENERGY SUPPLY - CONTACT				
PHONE				
COMPANY COMED				

REVISIONS		TEMPORARY CABLE PLAN, PHASE DESIGNATION DIAGRAM		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			DRWN: BK	SHEET NO.
		LAKE AVENUE AT GREEN BAY ROAD		CHECKED: DAJ	
				APPROVED: DAJ	
				DATE: 02-14-2008	
				SCALE: NONE	
				JOB NO.:	PROJECT NO.:
		CONSULTANT TYLIN INTERNATIONAL			

TEMPORARY RAILROAD PREEMPTION SEQUENCE OF OPERATIONS

MOVEMENT																															F L A S H
PHASE	1+5				1+6			2+5		2+6			3					4													
INTERVAL	1	2	3	4	5	6	7	8	9	10	11	12A	12B	13	14	15A	15B	15C	15D	16	17	18A	18B								
CHANGE TO		2+5	1+6	2+6			2+6		2+6			3				4						1+5 1+6 2+5 2+6									
GREEN BAY ROAD SIGNALS NEAR SIDE	N/B	R	R	R	R	R	R	G	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R							
GREEN BAY ROAD SIGNALS FAR SIDE	N/B					R	R	R			G	G	Y	R	R	R	R	R	R	R	R	R	R	R							
GREEN BAY ROAD SIGNALS NEAR SIDE	S/B	R	R	R	R	G	G	G	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R							
GREEN BAY ROAD SIGNALS FAR SIDE	S/B							G	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R							
LAKE AVENUE SIGNALS SIGNALS (EAST OF TRACKS)	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R							
LAKE AVENUE SIGNALS FAR RIGHT SIDE	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	G	G	Y	R	R	R	R	R							
LAKE AVENUE SIGNALS FAR LEFT SIDE	W/B	R	R	R	R	R	R	R	R	R	R	R	R	R					Y	R	R	R	R								
LAKE AVENUE SIGNALS NEAR SIDE	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R							
LAKE AVENUE SIGNALS FAR SIDE	E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R			Y	R							
PEDESTRIAN SIGNALS - CROSSING LAKE AVENUE ON WEST SIDE	H	H	H	H	P	FH	H	H	H	P*	FH	H	H	H	H	H	H	H	H	H	H	H	H	H							
PEDESTRIAN SIGNALS - CROSSING GREEN BAY ROAD ON SOUTH SIDE	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	P*	FH	H	H	H							
PEDESTRIAN SIGNALS - CROSSING GREEN BAY ROAD ON NORTH SIDE	H	H	H	H	H	H	H	H	H	H	H	H	H	P*	FH*	FH	H	H	H	H	H	H	H	H							

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5	8	10	13	16											
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																	
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1I	1K	1L	1M	2	3	4	5	CLEAR TO NORMAL SEQUENCE
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	1H	2	1K	2	1M	2	3	4	5		
GREEN BAY ROAD SIGNALS NEAR SIDE N/B	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	G	△
GREEN BAY ROAD SIGNALS FAR SIDE N/B	R ←	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	G	△
GREEN BAY ROAD SIGNALS NEAR SIDE S/B	R	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	G	△
GREEN BAY ROAD SIGNALS FAR SIDE S/B	R ←	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	G	△
LAKE AVENUE SIGNALS SIGNALS (EAST OF TRACKS) W/B	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	△
LAKE AVENUE SIGNALS FAR RIGHT SIDE W/B	R	R	R	R	R	R	R	R	G	G	R	R	G	Y	R	R	△
LAKE AVENUE SIGNALS FAR LEFT SIDE W/B	R	R	R	R	R	R	R	R	G	G	R	R	G	Y	R	R	△
LAKE AVENUE SIGNALS NEAR SIDE E/B	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	△
LAKE AVENUE SIGNALS FAR SIDE E/B	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	△
INTERNALLY ILLUMINATED NO RIGHT TURN SIGNS	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	△
INTERNALLY ILLUMINATED NO LEFT TURN SIGNS	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	△
PEDESTRIAN SIGNALS - CROSSING LAKE AVENUE ON WEST SIDE OF GREEN BAY ROAD	H	H	FH	H	H	H	FH	H	H	H	H	H	H	H	H	H	△
PEDESTRIAN SIGNALS - CROSSING GREEN BAY ROAD ON SOUTH SIDE OF LAKE AVENUE	H	H	H	H	H	H	H	H	FH	H	H	H	H	H	H	H	△
PEDESTRIAN SIGNALS - CROSSING GREEN BAY ROAD ON NORTH SIDE OF LAKE AVENUE	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	H	△

P = ILLUMINATED PERSON = WALK
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
H = ILLUMINATED SOLID HAND = DON'T WALK

NRT = "NO RIGHT TURN" OR 

NLT = "NO LEFT TURN" OR

△ RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

REVISIONS		<p style="text-align: center;">TEMPORARY SEQUENCE OF OPERATIONS</p> <p style="text-align: center;">LAKE AVENUE AT GREEN BAY ROAD</p>	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN: BK	SHEET NO.
			CHECKED: DAJ	
			APPROVED: DAJ	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.	PROJECT NO.
		CONSULTANT		
		TYLIN INTERNATIONAL		

PLAN	SERVED	BY	DATE
NOTE BOOK	ALIGNED CHECKED		
NO.	RT. OF WAY CHECKED		
	CAD FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK	LOTTED		
	GRADES CHECKED		
	BLM. NOTED		
NO. _____	STRUCTURE	NOTATION	CHKD

4 OF 4

REVISIONS		TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS LAKE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN: MPG	SHEET NO.
			CHECKED: DAJ	
			APPROVED: DAJ	
			DATE: 02-14-2008	
			SCALE: NONE	
		CONSULTANT	JOB NO:	PROJECT NO:
		TYLIN INTERNATIONAL		

CABLE PLAN LEGEND

EXISTING	PROPOSED		EXISTING	PROPOSED	
		8" (200mm) TRAFFIC SIGNAL SECTION			RAILROAD CONTROL CABINET
		12" (300mm) TRAFFIC SIGNAL SECTION			ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
		12" (300mm) PEDESTRIAN SIGNAL SECTION			ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
		12" (300mm) PEDESTRIAN SIGNAL SECTION			GROUND ROD AT CONTROLLER
		CONTROLLER CABINET			GROUND ROD AT POST OR MAST ARM POLE
		SERVICE INSTALLATION			GROUND ROD AT ELECTRIC SERVICE INSTALLATION
		TELEPHONE CONNECTION			GROUND CABLE IN CONDUIT, NO.6 SOLID COPPER (GREEN)
		MAGNETIC DETECTOR			FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 2-MM12F & SM12F
		EMERGENCY VEHICLE LIGHT DETECTOR			HANDHOLE/DOUBLE HANDHOLE WITH GROUNDED FRAME AND COVER
		CONFIRMATION BEACON			UNINTERRUPTIBLE POWER SUPPLY
		PUSHBUTTON DETECTOR			
		VEHICLE DETECTOR, INDUCTION LOOP			
		DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.			
		SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD			

CENTRAL AVENUE

GREEN BAY ROAD

UNION PACIFIC RAILROAD

SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
4	EACH	PEDESTRIAN PUSH BUTTON
6	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
4	FOOT	CONDUIT IN TRENCH, 2 1/2" DIA., GALVANIZED STEEL
62	FOOT	CONDUIT, PUSH-4" DIA., GALVANIZED STEEL
2	EACH	HANDHOLE
4	EACH	REBUILD EXISTING HANDHOLE
6	EACH	DRILL EXISTING HANDHOLE
645	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
4053	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
362	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1510	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.
8	FOOT	CONCRETE FOUNDATION, TYPE A
12	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
8	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 1-3-SECTION, 1-5 SECTION BRACKET MOUNTED
2	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, BRACKET MOUNTED
13	EACH	TRAFFIC SIGNAL BACKPLATE
8	EACH	INDUCTIVE LOOP DETECTOR
663	FOOT	DETECTOR LOOP, TYPE I
10	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
1	EACH	REMOVE EXISTING HANDHOLE
2	EACH	REMOVE EXISTING CONCRETE FOUNDATION
5484	FOOT	REMOVE EXISTING CABLE FROM CONDUIT
645	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED
2	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 14 FT.
4	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR AMPLIFIER
1	EACH	FULL - ACTUATED CONTROLLER, IN TYPE IV CABINET
1	EACH	UNINTERRUPTIBLE POWER SUPPLY, STANDARD
1	EACH	MAINTAIN EXISTING TRAFFIC SIGNAL INSTALLATION
1	EACH	TRANSCEIVER, FIBER OPTIC
2	EACH	ILLUMINATED SIGN, LED
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING

CABLE PLAN
NOT TO SCALE

NOTES:

PEDESTRIAN PUSH BUTTON "A" TO CALL PHASE 3.
PEDESTRIAN PUSH BUTTON "B" TO CALL PHASE 3 AND 6.
PEDESTRIAN PUSH BUTTON "C" TO CALL PHASE 4 AND 6.
PEDESTRIAN PUSH BUTTON "D" TO CALL PHASE 4.

2 OF 6

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO. LAMPS	WATTAGE LED	% OPERATIONS	TOTAL WATTAGE
SIGNAL (RED)	24	17	0.50	204
(YELLOW)	24	25	0.25	150
(GREEN)	24	15	0.25	90
ARROW	4	12	0.10	4.8
PED. SIGNAL	6	25	1.00	150
CONTROLLER	1	100	1.00	100
IL SIGN	2	25	0.05	2.5
FLASHER LED			0.05	0
TOTAL =				701.3

ENERGY COSTS-
ENERGY SUPPLY -
BILLED TO: VILLAGE OF WILMETTE
(ADDRESS) 1200 WILMETTE AVENUE
WILMETTE, IL
CONTACT
PHONE
COMPANY COMED

FOUNDATION	DEPTH	DEPTH (FT.)	CABLE SLACK	(FT.)	VERTICAL	(FT.)
TYPE A - POST	4		HANDHOLE	6.5	ALL FOUNDATIONS	3.5
D - CONTROLLER	4		DOUBLE HANDHOLE	13	MAST ARM (L) POLE	=20' HL-2
E - MAST ARM POLE			SIGNAL POST	13	BRACKET MOUNTED	
24"	10		CONTROLLER CAB.	1	PED. PUSHBUTTON	4
30"	15		FIBER OPTIC	13	ELECTRICAL SERVICE	13.5
			ELECTRICAL SERVICE	1	SERVICE TO GROUND	13.5
			GROUND CABLE	1	POSTMOUNTED	6

REVISIONS	
NAME	DATE

CABLE PLAN, SCHEDULE OF
QUANTITIES AND PHASE
DESIGNATION DIAGRAM
CENTRAL AVENUE AT GREEN BAY ROAD
CONSULTANT
TYLIN INTERNATIONAL

VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
DRAWN: BK CHECKED: DAJ APPROVED: DAJ DATE: 02-14-2008 SCALE: NONE JOB NO.:	SHEET NO. PROJECT NO.:

PROPOSED NORMAL SEQUENCE OF OPERATIONS

[illegible]

P = ILLUMINATED PERSON = WALK

FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK

H = ILLUMINATED SOLID HAND = DON'T WALK

* TO APPEAR ONLY UPON PUSHBUTTON ACTUATION

**FLASHING IS TO TERMINATE AT THE COMPLETION

OF THE PEDESTRIAN INTERVAL CLEARANCE.

AND FLASHING TIMINGS TO BE SET ONLY ON PHASES WHERE

AND FLASHING ARE INDICATED IN THE SEQUENCE OF OPERATION.

PHASE 2 AND 6 SHALL BE
PLACED ON RECALL FOR
COORDINATED SYSTEM

REVISIONS NAME DATE		SEQUENCE OF OPERATIONS CENTRAL AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
			DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.	PROJECT NO.

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

																			PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6			
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1		1		1		5			5	9	9		12			12								CLEAR TO NORMAL SEQUENCE
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	2	3	4	5		
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	3	1F	4 OR 5	1H	1J	2, 4 OR 5	3	2	1N	3, 4 OR 5	1Q	1R	2	1T	1U	3, 4 OR 5						◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	G	R	R	R	◆	
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R ←G	R ←G	R ←Y	R	R ←Y	R	R	R	R	R	G ←G	Y	R	G	G	G	G	Y	R	G ←G	R	R	R	◆	
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	R	R	R	R	G	Y	R	G	R	R	R	G	Y	R	G	Y	R	R	G	R	R	◆	
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	←Y	←R	←G	←G	←Y	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	←R	◆	
CENTRAL AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◆	
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	◆	
CENTRAL AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	◆	
CENTRAL AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◆	
CENTRAL AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	◆	
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	◆		
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF CENTRAL AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF CENTRAL AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆	
PEDESTRIAN SIGNALS CROSSING CENTRAL AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	FH	H	H	FH	H	H	H	FH	H	H	FH	H	H	H	H	H	H	◆	

P = ILLUMINATED PERSON = WALK
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
H = ILLUMINATED SOLID HAND = DON'T WALK
◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

REVISIONS NAME DATE	EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 1 OF 2 CENTRAL AVENUE AT GREEN BAY ROAD		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
	CONSULTANT TYLIN INTERNATIONAL		SHEET NO.	
			DRAWN	
			CHECKED	
			APPROVED	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.	
			PROJECT NO.	

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

																														PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6					
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	15				15		15				19				19	22				22	25				25			25									CLEAR TO NORMAL SEQUENCE	
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD	1EE	1FF	1GG	1HH	1JJ	1KK	1LL	1MM	1NN	1PP	1QQ	1RR	1SS	1TT	1UU	1VV	1WW	1XX	1YY	1ZZ	1AAA	1BBB	1CCC	1DDD	2	3	4		5
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1W	1X	1Y	2 OR 3	1AA	4	1CC	1DD	1EE	5	1GG	1HH	1JJ	1KK	2, 3 OR 5	4	1NN	1PP	2, 3 OR 4	5	1SS	1TT	1UU	1VV	2 OR 3	1XX	1YY	4	1AAA	1BBB	1CCC	1DDD	5					◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	R	◆
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	R	◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	◆
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	←R	◆	
CENTRAL AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	G	R	R	R	R	G	G	G	Y	R	G	G	G	G	G	G	Y	R	R	G	R	◆	
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	←G	←G	←Y	←R	←G	←G	←G	←G	←Y	←R	←G	←G	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆		
CENTRAL AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	←Y	←R	←R	←R	←G	←G	←Y	←R	←R	←R	←G	←Y	←R	←R	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆		
CENTRAL AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	Y	R	R	R	G	R	R	R	R	G	Y	R	R	R	G	G	G	G	Y	R	R	R	R	R	◆		
CENTRAL AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	G	Y	R	G	G	G	G	G	R	R	R	◆	
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	←G	←G	←Y	←R	←Y	←R	←G	←G	←G	←G	←R	←R	←R	←R	←R	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆		
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF CENTRAL AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	FH	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	◆		
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF CENTRAL AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	FH	H	H	H	H	FH	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	◆		
PEDESTRIAN SIGNALS CROSSING CENTRAL AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆		

P = ILLUMINATED PERSON = WALK
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◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

REVISIONS		EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 2 OF 2 CENTRAL AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE			
CONSULTANT		TYLIN INTERNATIONAL	SHEET NO.	
			DRAWING	
			CHECKED	
			APPROVED	
			DATE: 02-14-2008	
		SCALE: NONE		PROJECT NO.
		JOB NO.		

PROPOSED RAILROAD PREEMPTION SEQUENCE OF OPERATIONS

																PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4		PREEMPTOR NUMBER 4		PREEMPTOR NUMBER 5		PREEMPTOR NUMBER 2										
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1		5		9		12		15		19		22		25																		CLEAR TO NORMAL SEQUENCE
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																2		3		4		5											
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	2	3	4	5					
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	1H	2	2	2	1M	2	1P	2	1R	2	1T	2	1V	2	1X	2	1Z	2	3	4	5		▲				
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲				
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R ←Y	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲				
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	Y	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	G	▲				
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	←Y	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←R	←R	▲					
CENTRAL AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	G	R	R	G	G	R	R	R	G	G	G	R	R	G	Y	R	R	▲				
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←G	←G	←G	←G	←R	←R	←R	←R	←R	←R	←R	←G	←G	←R	←R	←G	←Y	←R	←R	▲					
CENTRAL AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←R	▲					
CENTRAL AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	Y	R	R	R	R	Y	R	R	R	R	R	R	R	R	▲				
CENTRAL AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	▲					
CENTRAL AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←R	←Y	←R	←R	←R	←R	←R	←R	←R	←R	←Y	←R	←R	←R	←R	←R	←R	▲					
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF CENTRAL AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	FH	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲				
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF CENTRAL AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲				
PEDESTRIAN SIGNALS CROSSING CENTRAL AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	FH	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲				
INTERNALLY ILLUMINATED NRT SIGNS - GREEN BAY ROAD	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	▲				
																												HOLD					

RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL 2, 3 OR 4 (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

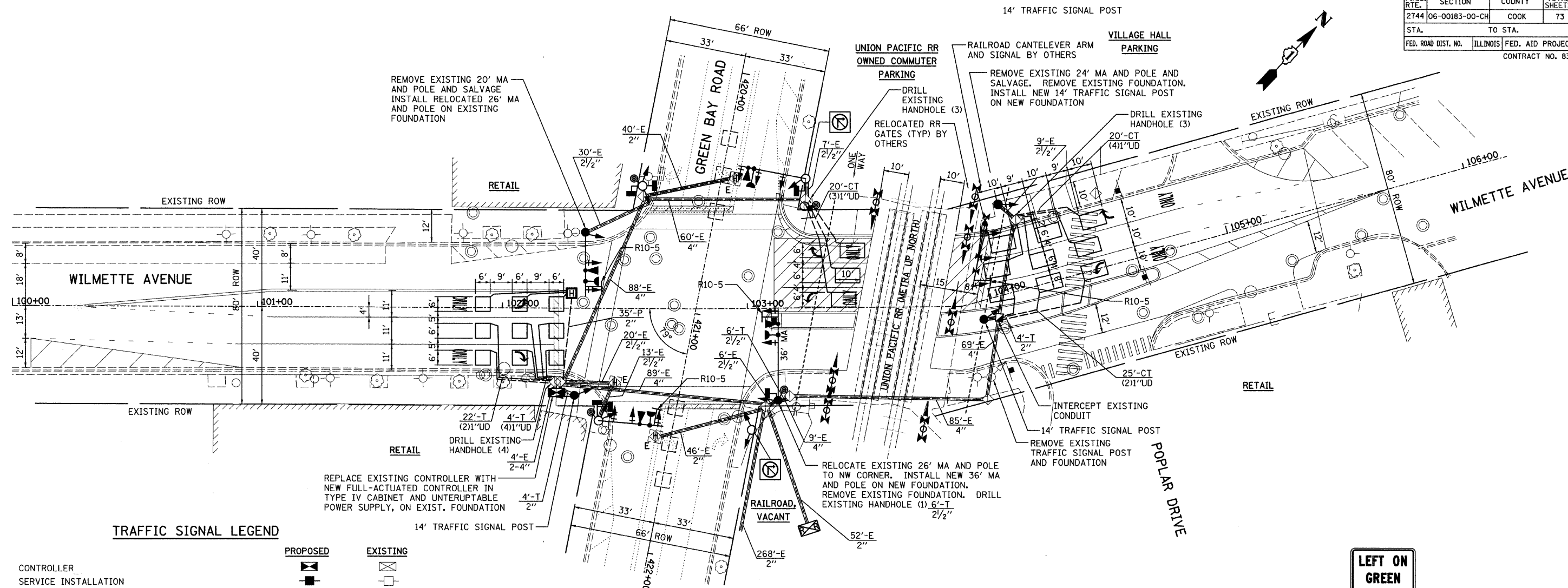
P = ILLUMINATED PERSON = WALK
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
H = ILLUMINATED SOLID HAND = DON'T WALK

REVISIONS		RAILROAD PREEMPTION SEQUENCE OF OPERATIONS CENTRAL AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.:	
CONSULTANT			PROJECT NO.:	
TYLIN INTERNATIONAL				

PLAN	DATE	BY
DESIGNED		
CHECKED		
NOTED		
NO.		

PROFILE	DATE	BY
DESIGNED		
CHECKED		
NOTED		
NO.		

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2744	06-00183-00-CH	COOK	73	47
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	CONTRACT NO. 83999	



TRAFFIC SIGNAL LEGEND

- CONTROLLER
SERVICE INSTALLATION
SIGNAL HEAD
SIGNAL HEAD WITH BACKPLATE
SIGNAL HEAD, PEDESTRIAN
SIGNAL POST
MAST ARM ASSEMBLY AND POLE, STEEL
MAST ARM ASSEMBLY AND POLE, ALUMINUM
COMMON TRENCH
UNIT DUCT
HANDHOLE
HEAVY DUTY HANDHOLE
DOUBLE HANDHOLE
G.S. CONDUIT IN TRENCH OR PUSHED
PEDESTRIAN PUSHBUTTON DETECTOR
DETECTOR LOOP
CAST IRON JUNCTION BOX
EMERGENCY VEHICLE SYSTEM DETECTOR
CONFIRMATION BEACON
SIGNAL HEAD OPTICALLY PROGRAMMED
CONDUIT SPLICE
WOOD POLE
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE
RAILROAD CONTROL CABINET
ILLUMINATED SIGN, FIBER OPTIC
"NO LEFT TURN"
ILLUMINATED SIGN, FIBER OPTIC
"NO RIGHT TURN"
TELEPHONE

PROPOSED	EXISTING

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR, SHALL REMAIN THE PROPERTY OF THE VILLAGE OF WILMETTE AND SHALL BE DELIVERED BY THE CONTRACTOR TO THE VILLAGE'S TRAFFIC SIGNAL MAINTENANCE CONTRACTOR'S MAIN FACILITY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

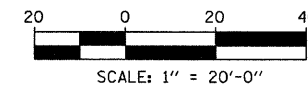
- 1 EACH STEEL MAST ARM ASSEMBLY POLE 24'
- 1 EACH STEEL MAST ARM ASSEMBLY POLE 20'

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH PEDESTRIAN PUSH BUTTONS
- 1 EACH SIGNAL POST
- 7 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 4 EACH SIGNAL HEAD, 1-FACE, 4-SECTION
- 4 EACH SIGNAL HEAD, 1-FACE, 5-SECTION
- 2 EACH PEDESTRIAN HEAD, 1-FACE, 2-SECTION
- 2 EACH PEDESTRIAN HEAD, 2-FACE, 2-SECTION
- 6485 FOOT EXISTING CABLE FROM CONDUIT



R10-5
24"X30"
SIGN DETAIL
N.T.S
4 REQUIRED



REVISIONS		VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		
		<div> <div>TRAFFIC SIGNAL PLAN</div> <div>WILMETTE AVENUE AT GREEN BAY ROAD</div> <div>CONSULTANT TYLIN INTERNATIONAL</div> </div>	
		DRAWN: BK CHECKED: DAJ APPROVED: DAJ DATE: 02-14-2008 SCALE: 1"=20'-0"	SHEET NO. PROJECT NO.

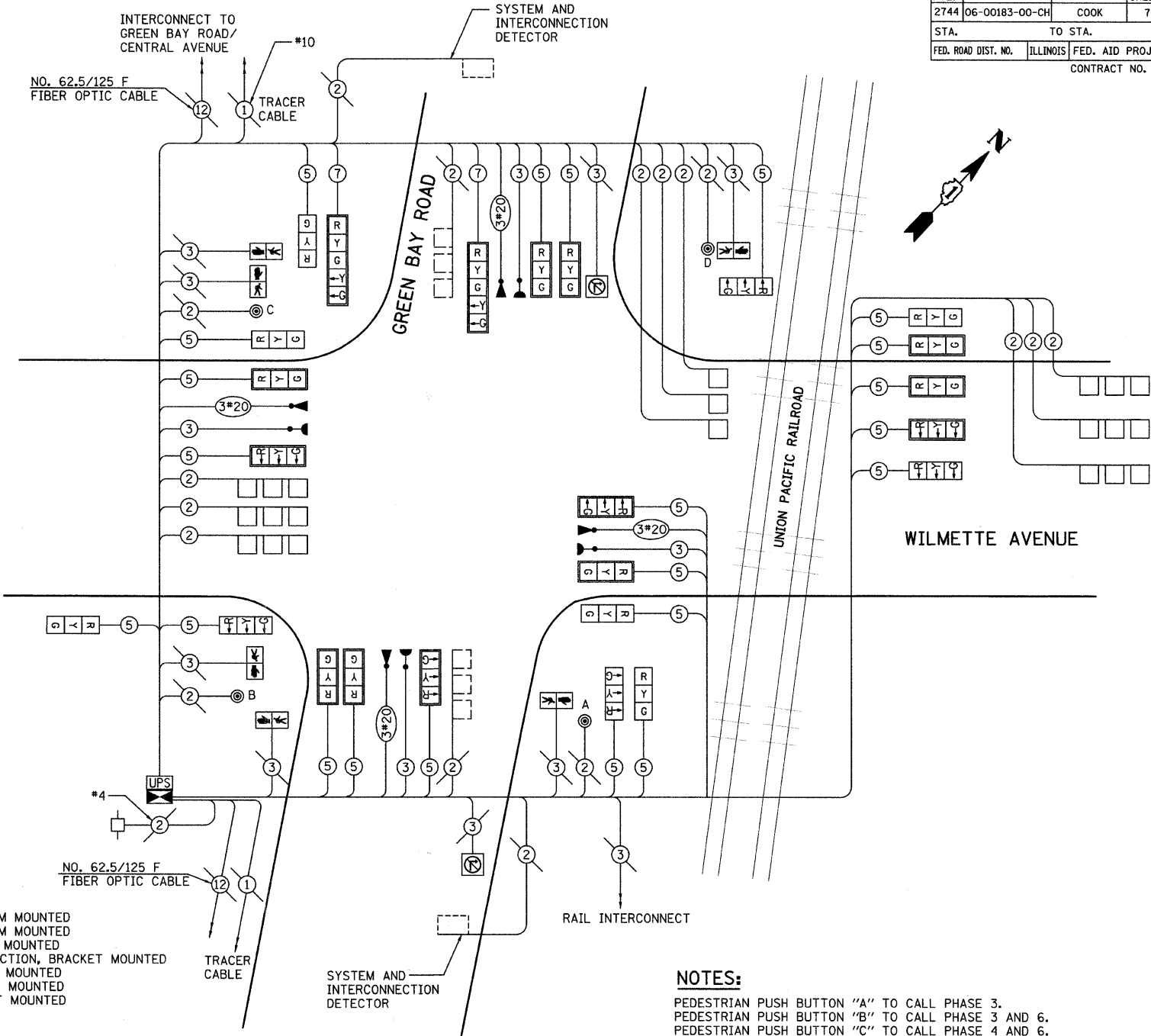
CABLE PLAN LEGEND

EXISTING	PROPOSED	
		8" (200mm) TRAFFIC SIGNAL SECTION
		12" (300mm) TRAFFIC SIGNAL SECTION
		12" (300mm) PEDESTRIAN SIGNAL SECTION
		12" (300mm) PEDESTRIAN SIGNAL SECTION
		CONTROLLER CABINET
		SERVICE INSTALLATION
		TELEPHONE CONNECTION
		MAGNETIC DETECTOR
		EMERGENCY VEHICLE LIGHT DETECTOR
		CONFIRMATION BEACON
		PUSHBUTTON DETECTOR
		VEHICLE DETECTOR, INDUCTION LOOP
		DENOTES NUMBER OF CONDUCTORS, ALL CABLE NO. 14 EXCEPT AS INDICATED. ALL LOOP DETECTOR CABLE TO BE SHIELDED.
		SIGNAL FACE WITH BACKPLATE. "P" INDICATES PROGRAMMED HEAD

EXISTING	PROPOSED	
		RAILROAD CONTROL CABINET
		ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN"
		ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN"
		GROUND ROD AT CONTROLLER
		GROUND ROD AT POST OR MAST ARM POLE
		GROUND ROD AT ELECTRIC SERVICE INSTALLATION
		GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)
		FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125 2-MM12F & SM12F
		HANDHOLE/DOUBLE HANDHOLE WITH GROUNDED FRAME AND COVER
		UNINTERRUPTABLE POWER SUPPLY

SCHEDULE OF QUANTITIES

QTY	UNIT	ITEM DESCRIPTION
4	EACH	PEDESTRIAN PUSH BUTTON
12	FOOT	CONCRETE FOUNDATION, TYPE A
13.5	FOOT	CONCRETE FOUNDATION, TYPE E, 30 INCH DIAMETER
8	FOOT	CONDUIT IN TRENCH, 2" DIA., GALVANIZED STEEL
35	FOOT	CONDUIT PUSHED, 2" DIA., GALVANIZED STEEL
1	EACH	HANDHOLE
10	EACH	HEAVY DUTY HANDHOLE
1	EACH	DRILL EXISTING HANDHOLE
718	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C
4262	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C
378	FOOT	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C
1627	FOOT	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR
1	EACH	STEEL MAST ARM ASSEMBLY AND POLE, 36 FT.
1	EACH	RELOCATE EXISTING MAST ARM ASSEMBLY AND POLE
12	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 5-SECTION, MAST ARM MOUNTED
5	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED
1	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED
2	EACH	SIGNAL HEAD, POLYCARBONATE, LED, 2-FACE, 3-SECTION, BRACKET MOUNTED
2	EACH	PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, BRACKET MOUNTED
13	EACH	TRAFFIC SIGNAL BACKPLATE
9	EACH	INDUCTIVE LOOP DETECTOR
803	FOOT	DETECTOR LOOP, TYPE I
4	EACH	LIGHT DETECTOR
1	EACH	LIGHT DETECTOR, AMPLIFIER
8	FOOT	TRENCH AND BACKFILL FOR ELECTRICAL WORK
1	EACH	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT
718	FOOT	ELECTRIC CABLE IN CONDUIT, NO. 20 3/C, TWISTED, SHIELDED
3	EACH	TRAFFIC SIGNAL POST, GALVANIZED STEEL, 14 FT.
6485	FOOT	REMOVE EXISTING CABLE FROM CONDUIT
3	EACH	REMOVE EXISTING CONCRETE FOUNDATION
1	EACH	FULL - ACTUATED CONTROLLER, IN TYPE IV CABINET
1	EACH	UNINTERRUPTABLE POWER SUPPLY, STANDARD
1	EACH	TRANSCEIVER, FIBER OPTIC
2	EACH	ILLUMINATED SIGN, LED
1	EACH	TEMPORARY TRAFFIC SIGNAL TIMING
1	EACH	MAINTAIN EXISTING TRAFFIC SIGNAL INSTALLATION



CABLE PLAN
NOT TO SCALE

NOTES:
PEDESTRIAN PUSH BUTTON "A" TO CALL PHASE 3.
PEDESTRIAN PUSH BUTTON "B" TO CALL PHASE 3 AND 6.
PEDESTRIAN PUSH BUTTON "C" TO CALL PHASE 4 AND 6.
PEDESTRIAN PUSH BUTTON "D" TO CALL PHASE 4.

I.D.O.T. TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS				
TYPE	NO. LAMPS	WATTAGE LED	% OPERATIONS	TOTAL WATTAGE
SIGNAL (RED)	24	17	0.50	204
(YELLOW)	24	25	0.25	150
(GREEN)	24	15	0.25	90
ARROW	4	12	0.10	4.8
PED. SIGNAL	6	25	1.00	150
CONTROLLER	1	100	1.00	100
IL SIGN	2	25	0.05	2.5
FLASHER LED			0.05	0
TOTAL =				701.3

ENERGY COSTS-
ENERGY SUPPLY -
BILLED TO: VILLAGE OF WILMETTE
(ADDRESS) 1200 WILMETTE AVENUE
WILMETTE, IL
CONTACT
PHONE
COMPANY COMED

FOUNDATION	DEPTH	DEPTH (FT.)	CABLE SLACK	(FT.)	VERTICAL	(FT.)
TYPE A - POST		4	HANDHOLE	6.5	ALL FOUNDATIONS	3.5
D - CONTROLLER		4	DOUBLE HANDHOLE	13	MAST ARM (L) POLE	=20' HL-2
E - MAST ARM POLE			SIGNAL POST	2	BRACKET MOUNTED	13
24"		10	CONTROLLER CAB.	1	PED. PUSHBUTTON	4
30"		15	FIBER OPTIC	13	ELECTRICAL SERVICE	13.5
			ELECTRICAL SERVICE	1	SERVICE TO GROUND	13.5
			GROUND CABLE	1	POSTMOUNTED	6

REVISIONS		CABLE PLAN, SCHEDULE OF QUANTITIES AND PHASE DESIGNATION DIAGRAM	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS
NAME	DATE		
		WILMETTE AVENUE AT GREEN BAY ROAD CONSULTANT TYLIN INTERNATIONAL	DRAWN: BK
			CHECKED: DAJ
			APPROVED: DAJ
			DATE: 02-14-2008
			SCALE: NONE
			JOB NO.:
			PROJECT NO.:

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

																			PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6		
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1		1		1		5			5	9	9		12			12							CLEAR TO NORMAL SEQUENCE
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	2	3	4	5	
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	3	1F	4 OR 5	1H	1J	2, 4 OR 5	3	2	1N	3, 4 OR 5	1Q	1R	2	1T	1U	3, 4 OR 5					◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	G	R	R	R	◆
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R ←G	R ←G	R ←Y	R	R ←Y	R	R	R	R	R	G ←G	Y	R	G	G	G	G	Y	R	G ←G	R	R	R	◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	R	R	R	R	G	Y	R	G	R	R	R	G	Y	R	G	Y	R	R	G	R	R	◆
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	←Y	←R	←G	←G	←Y	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	←R	◆
WILMETTE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◆
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	◆
WILMETTE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	◆
WILMETTE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◆
WILMETTE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	◆
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	◆	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF WILMETTE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF WILMETTE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆
PEDESTRIAN SIGNALS CROSSING WILMETTE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	FH	H	H	FH	H	H	H	FH	H	H	FH	H	H	H	H	H	H	◆

P = ILLUMINATED PERSON = WALK
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
H = ILLUMINATED SOLID HAND = DON'T WALK
◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE
THE PROPER CLEARANCE INTERVAL TO RESUME
THE NORMAL OPERATION OR PROPER CLEARANCE
INTERVAL TO DISPLAY A DIFFERENT EMERGENCY
VEHICLE INTERVAL AFTER EMERGENCY VEHICLE
INTERVAL 2 OR 3 IS TERMINATED.

REVISIONS		EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 1 OF 2 WILMETTE AVENUE AT GREEN BAY ROAD CONSULTANT TYLIN INTERNATIONAL	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		SHEET NO.	
			DRAWN:	
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.:	
			PROJECT NO.:	

PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

																														PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 6					
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	15				15		15				19				19	22				22	25				25			25									CLEAR TO NORMAL SEQUENCE	
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD	1EE	1FF	1GG	1HH	1JJ	1KK	1LL	1MM	1NN	1PP	1QQ	1RR	1SS	1TT	1UU	1VV	1WW	1XX	1YY	1ZZ	1AAA	1BBB	1CCC	1DDD	2	3	4		5
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1W	1X	1Y	2 OR 3	1AA	4	1CC	1DD	1EE	5	1GG	1HH	1JJ	1KK	2, 3 OR 5	4	1NN	1PP	2, 3 OR 4	5	1SS	1TT	1UU	1VV	2 OR 3	1XX	1YY	4	1AAA	1BBB	1CCC	1DDD	5					◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	R	◆
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G ←G	R	R	R	◆
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	R	◆
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←G	←R	←R	◆	
WILMETTE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	G	G	Y	R	G	R	R	R	R	G	G	G	Y	R	G	G	G	G	G	G	Y	R	R	R	G	R	◆
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	←G	←G	←Y	←R	←G	←G	←G	←G	←Y	←R	←G	←G	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆	
WILMETTE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	←Y	←R	←R	←R	←G	←G	←Y	←R	←R	←R	←G	←Y	←R	←R	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆	
WILMETTE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	Y	R	R	R	G	R	R	R	R	G	Y	R	R	R	G	G	G	G	Y	R	R	R	R	R	R	◆	
WILMETTE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	Y	R	G	Y	R	G	G	G	G	G	G	R	R	R	G	◆
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	←G	←G	←Y	←R	←Y	←R	←G	←G	←G	←G	←R	←R	←R	←R	←R	←R	←G	←Y	←R	←G	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	←R	◆		
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF WILMETTE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	FH	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	◆	
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF WILMETTE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	FH	H	H	H	H	FH	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	◆	
PEDESTRIAN SIGNALS CROSSING WILMETTE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◆	

P = ILLUMINATED PERSON = WALK
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
H = ILLUMINATED SOLID HAND = DON'T WALK
◆ = EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY VEHICLE INTERVAL AFTER EMERGENCY VEHICLE INTERVAL 2 OR 3 IS TERMINATED.

REVISIONS	NAME	DATE	EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS SHEET 2 OF 2 WILMETTE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS				
CONSULTANT			TYLIN INTERNATIONAL	SHEET NO. PROJECT NO.				
			DATE: 02-14-2008 SCALE: NONE JOB NO.					

PROPOSED RAILROAD PREEMPTION SEQUENCE OF OPERATIONS

																PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 4	PREEMPTOR NUMBER 5	PREEMPTOR NUMBER 2													
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1		5		9		12		15		19		22		25																		CLEAR TO NORMAL SEQUENCE
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER																2		3		4		5											
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	2	3	4	5					
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1B	2	1D	2	1F	2	1H	2	2	2	1M	2	1P	2	1R	2	1T	2	1V	2	1X	2	1Z	2	3	4	5		▲				
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS N/B	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲				
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS N/B	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	R	R	G	▲				
GREEN BAY ROAD NEAR RIGHT AND CENTER AND FAR RIGHT MAST ARM SIGNALS S/B	R	R	Y	R	R	Y	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	R	G	▲				
GREEN BAY ROAD LEFT MAST ARM AND FAR LEFT SIGNALS S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	▲				
WILMETTE AVENUE FAR RIGHT AND CENTER MAST ARM SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	G	G	R	R	G	G	R	R	R	G	G	G	R	R	G	Y	R	R	▲				
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS W/B	R	R	R	R	R	R	R	R	G	G	G	G	R	R	R	R	R	R	R	G	G	R	R	G	Y	R	R	▲					
WILMETTE AVENUE (EAST OF TRACKS) LEFT CANTILEVER AND FAR LEFT MEDIAN SIGNALS W/B	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	R	R	▲				
WILMETTE AVENUE (EAST OF TRACKS) FAR RIGHT AND CENTER CANTILEVER SIGNALS W/B	R	R	R	R	R	R	R	R	R	R	Y	R	R	R	Y	R	R	R	R	R	Y	R	R	R	R	R	R	R	▲				
WILMETTE AVENUE NEAR RIGHT CENTER AND FAR RIGHT MAST ARM SIGNALS E/B	R	R	R	R	R	R	R	R	R	R	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R	R	▲				
WILMETTE AVENUE LEFT MAST ARM AND FAR LEFT SIGNALS E/B	R	R	R	R	R	R	R	R	Y	R	R	Y	R	R	R	R	R	R	R	R	R	Y	R	R	R	R	R	▲					
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON SOUTH SIDE OF WILMETTE AVENUE (S. LEG)	H	H	H	H	H	H	H	H	H	H	H	FH	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲				
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD ON NORTH SIDE OF WILMETTE AVENUE (N. LEG)	H	H	H	H	H	H	H	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲				
PEDESTRIAN SIGNALS CROSSING WILMETTE AVENUE ON WEST SIDE OF GREEN BAY ROAD (W. LEG)	H	H	FH	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	▲				
INTERNALLY ILLUMINATED NRT SIGNS - GREEN BAY ROAD	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	▲				
																												HOLD					

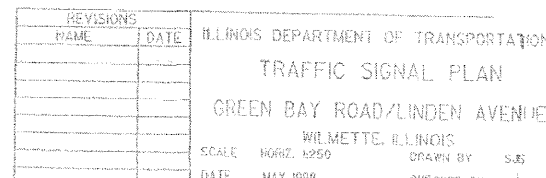
RAILROAD PREEMPTION SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY AN EMERGENCY VEHICLE INTERVAL 2, 3 OR 4 (IF APPLICABLE) AFTER RAILROAD PREEMPTION INTERVAL 5 IS TERMINATED.

P = ILLUMINATED PERSON = WALK
FH = ILLUMINATED FLASHING HAND = FLASHING DON'T WALK
H = ILLUMINATED SOLID HAND = DON'T WALK

REVISIONS		RAILROAD PREEMPTION SEQUENCE OF OPERATIONS WILMETTE AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN:	SHEET NO.
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.:	PROJECT NO.:
		CONSULTANT		
		TYLIN INTERNATIONAL		

FILE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA.	TO STA.			
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

PROFILE	SURVEYED	BY	DATE
	PLOTTED		
NOTE BOOK	GRADES CHECKED		
	B.M. NOTED		
NO.	STRUCTURE NOTATIONS CHK'D		

[illegible]

TRAFFIC SIGNAL PLAN

LINDEN AVENUE AT GREEN BAY ROAD

CONSULTANT

TYLIN INTERNATIONAL

<p>VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS</p>		
DRAWN: BK		SHEET NO.
CHECKED: DAJ		
APPROVED: DAJ		
DATE: 02-14-2008		
SCALE: NTS		
APP. NO.		PROJECT NO.

LINDEN AVENUE

NORMAL SEQUENCE OF OPERATIONS

MOVEMENT															FLASH
PHASE	2 + 6				3						4				
INTERVAL	1	2	3A	3B	4	5	6A	6B	6C	6D	7	8	9	9B	
CHANGE TO			3 OR 4				2+ 6 OR 4						2+6 OR 3		
GREEN BAY ROAD MAST ARM AND FAR LEFT N/B	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R
GREEN BAY ROAD MAST ARM AND FAR LEFT S/B	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R
LINDEN AV. (EAST OF TRACKS) FAR LEFT, MAST ARM AND FAR RIGHT W/B	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R
LINDEN AV. RIGHT MAST ARM W/B	R	R	R	R	G	G	G	G	Y	R	R	R	R	R	R
LINDEN AV., LEFT MAST ARM AND FAR LEFT W/B	R	R	R	R	G	G	G	G	Y	R	R	R	R	R	R
LINDEN AV. RIGHT MAST ARM E/B	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	R
LINDEN AV., LEFT MAST ARM AND FAR LEFT E/B	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	R
PEDESTRIAN SIGNALS CROSSING GREEN BAY RD. SOUTH LEG	H	H	H	H	H	H	H	H	H	H	P	FL H	H	H	DARK
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD, NORTH LEG	H	H	H	H	P	FL H	H	H	H	H	H	H	H	H	DARK
PEDESTRIAN SIGNALS CROSSING LINDEN AV., WEST LEG	P	FL H	H	H	H	H	H	H	H	H	H	H	H	H	DARK

APPEARS ONLY UPON ACTUATION OF PEDESTRIAN PUSHBUTTONS

REVISIONS		SEQUENCE OF OPERATIONS LINDEN AVENUE AT GREEN BAY ROAD CONSULTANT TYLIN INTERNATIONAL	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS	
NAME	DATE		DRAWN:	
			CHECKED:	
			APPROVED:	
			DATE: 02-14-2008	
			SCALE: NONE	
			JOB NO.:	
			PROJECT NO.:	
			SHEET NO.:	

LINDEN AVENUE

EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS

															PREEPTOR NUMBER 3	PREEPTOR NUMBER 4	PREEPTOR NUMBER 5	
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	1			4	4					7	7						CLEAR TO NORMAL SEQUENCE
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1O	2	3	4	
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	2	1C	1D	3 OR 4	3	1G	1H	1J	1K	2 OR 4	4	1N	1O	2 OR 3				
GREEN BAY ROAD MAST ARM AND FAR LEFT N/B	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	G	R	R	◊
GREEN BAY ROAD MAST ARM AND FAR LEFT S/B	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	G	R	R	◊
LINDEN AV. (EAST OF TRACKS) FAR LEFT, MAST ARM AND FAR RIGHT W/B	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	G	R	◊
LINDEN AV. RIGHT MAST ARM W/B	R	◀ R	R	R	G	G	G	G	Y	R	R	R	R	R	R	G	R	◊
LINDEN AV., LEFT MAST ARM AND FAR LEFT W/B	R	R	R	R	◀ G G	◀ G G	◀ G G	◀ G G	Y	R	R	R	R	R	R	◀ G G	R	◊
LINDEN AV. RIGHT MAST ARM E/B	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	◀ R	R	G	◊
LINDEN AV., LEFT MAST ARM AND FAR LEFT E/B	R	R	R	R	R	R	R	R	R	R	◀ G G	◀ G G	Y	R	◀ R	R	◀ G G	◊
PEDESTRIAN SIGNALS CROSSING GREEN BAY RD. SOUTH LEG	H	H	H	H	H	H	H	H	H	H	FL H	FL H	H	H	◀ H	H	H	◊
PEDESTRIAN SIGNALS CROSSING GREEN BAY ROAD, NORTH LEG	H	H	H	H	FL H	FL H	H	H	H	H	H	H	H	H	H	H	H	◊
PEDESTRIAN SIGNALS CROSSING LINDEN AV., WEST LEG	FL H	FL H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◊

REVISIONS NAME DATE	EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATIONS LINDEN AVENUE AT GREEN BAY ROAD	VILLAGE OF WILMETTE GREEN BAY ROAD INTERSECTION IMPROVEMENTS
CONSULTANT TYLIN INTERNATIONAL		DRAWN CHECKED APPROVED DATE: 02-14-2008 SCALE: NONE JOB NO.: PROJECT NO.:

PROPOSED RAILROAD PREEMPTION SEQUENCE OF OPERATION

CHANGE FROM NORMAL SEQUENCE
OF OPERATION INTERVAL NUMBERCHANGE FROM EMERGENCY
VEHICLE PREEMPTION SEQUENCE
OF OPERATION INTERVAL NUMBER

OF OPERATION INTERVAL NUMBER	
RAILROAD PREEMPTION SEQUENCE	
OF OPERATION INTERVAL NUMBER	

CHANGE TO RAILROAD PREEMPTION
SEQUENCE OF OPERATION
INTERVAL NUMBER

GREEN BAY ROAD MAST ARM AND FAR LEFT	N/B
---	-----

GREEN BAY ROAD MAST ARM AND
EAR LEFT S/B

LINDEN AV. (EAST OF TRACKS) FAR
LEFT, MAST ARM AND FAR RIGHT
W/R

LINDEN AV. RIGHT MAST ARM
W/B

LINDEN AV., LEFT MAST ARM AND
EAR LEFT W/R

LINDEN AV. RIGHT MAST ARM
E/B

LINDEN AV., LEFT MAST ARM AND
EAP LEFT E/R

PEDESTRIAN SIGNALS CROSSING
GREEN BAY RD. SOUTH LEG

PEDESTRIAN SIGNALS CROSSING
GREEN BAY ROAD, NORTH LEG

PEDESTRIAN SIGNALS CROSSING
LINDEN AV., WEST LEG

INTERNALLY ILLUMINATED NO RIGHT
TURN SIGNS (NRT) - N/B

INTERNALLY ILLUMINATED NO LEFT
TURN SIGNS (NLT) - S/B

RAILROAD PREEMPTION
SEQUENCE OF OPERATIONS
LINDEN AVENUE AT GREEN BAY ROAD

CONSULTANT	
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TY·LIN INTERNATIONAL

VILLAGE OF WILMETTE
GREEN BAY ROAD
INTERSECTION IMPROVEMENTS

DRAWN:	SHEET NO.
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DRAWN:	SHEET NO.
--------	-----------

DRAWING
CHECKED:

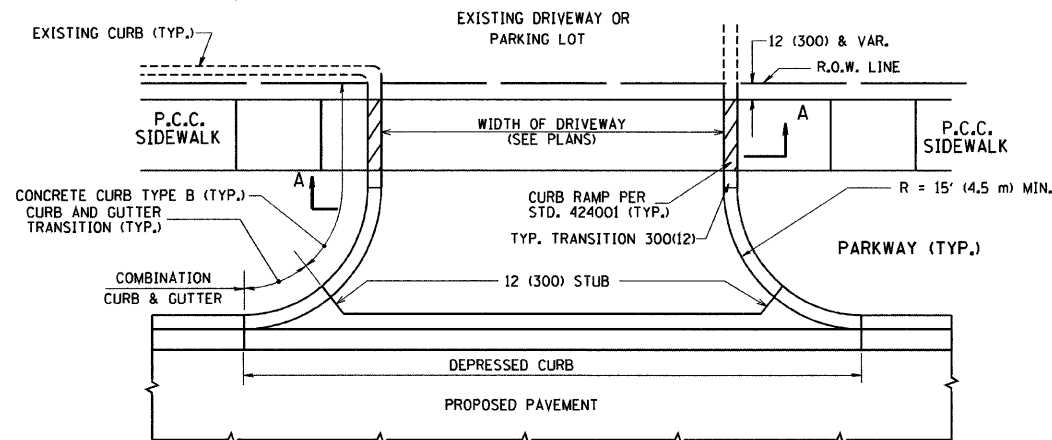
CHECKED:
LOCATION:

APPROVED: _____

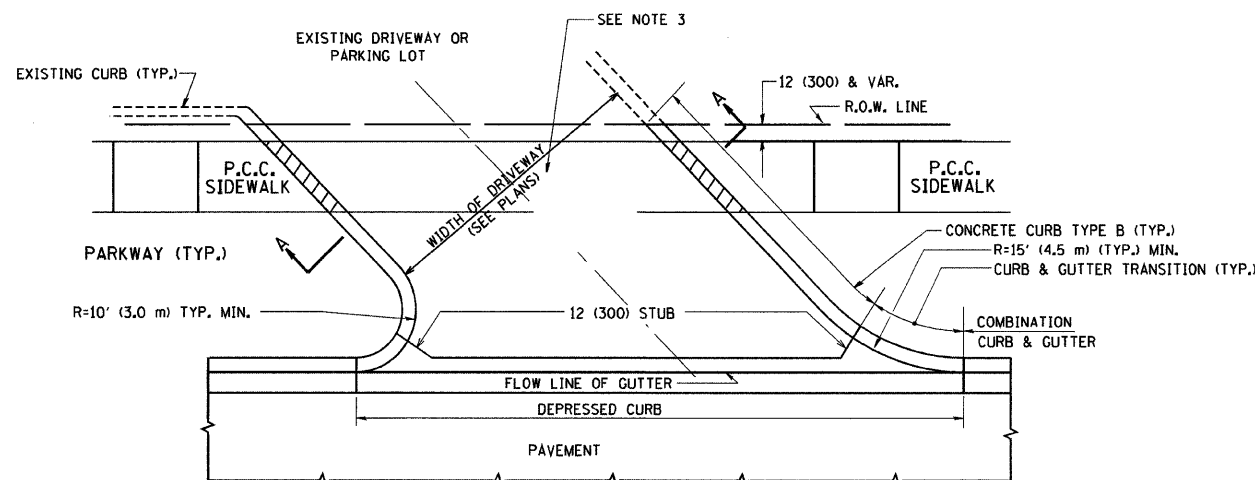
DATE: 02-14-2008

SCALE: NONE	
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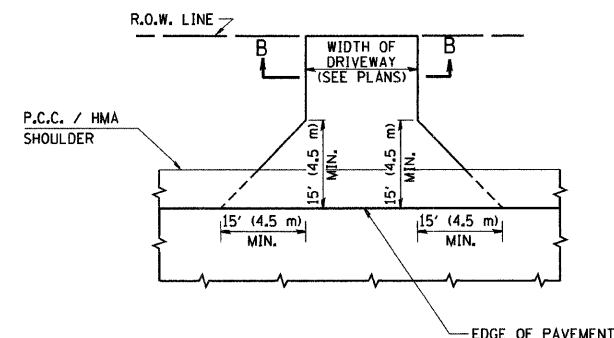
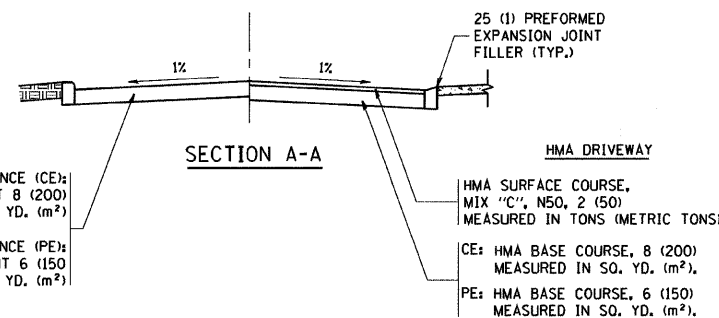
JOB NO.	PROJECT
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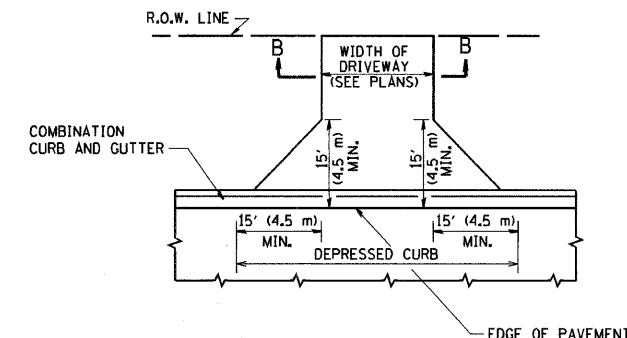
WITH CONCRETE CURB, TYPE B



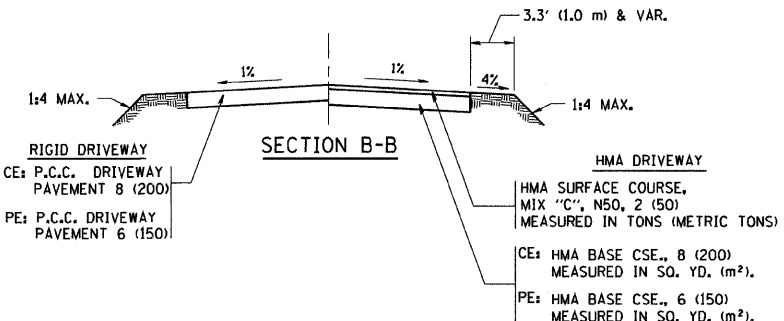
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

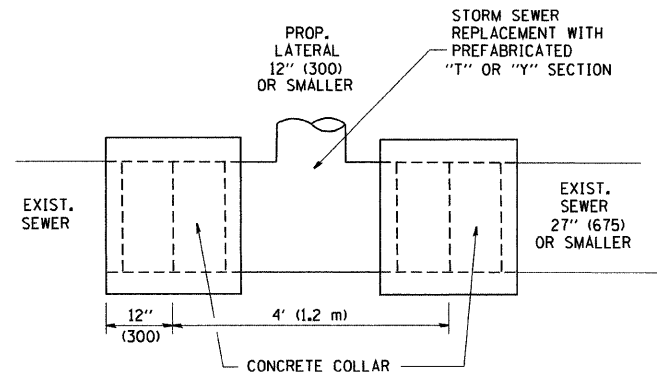
RURAL FIELD ENTRANCE (FE)
HMA SURFACE COURSE,
MIX "C", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)
AGGREGATE BASE CSE., TYPE A 8 (200)
MEASURED IN SQ. YD. (m²).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED

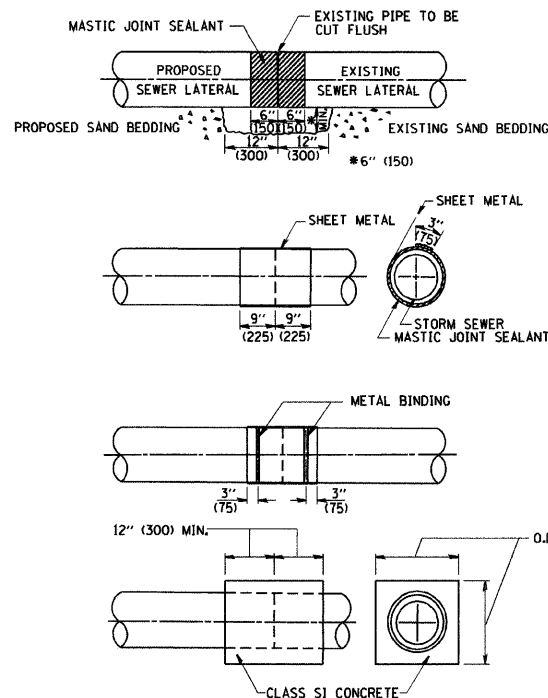
REVISIONS	
NAME	DATE
R. SHAH	11-04-95
J. POLLASTRINI	08-12-96
J. POLLASTRINI	12-14-96
A. ABBAS	03-21-97
T. HOLTZ	04-08-97
M. GOMEZ	04-06-01
P. LOFLEUR	04-15-03
R. BORO	01-01-07

ILLINOIS DEPARTMENT OF TRANSPORTATION
DRIVEWAY DETAILS
DISTANCE BETWEEN R.O.W. AND
FACE OF CURB & EDGE OF
SHOULDER >= 15' (4.5 m)
SCALE: VERT. NONE
HORIZ.
DRAWN BY
CHECKED BY

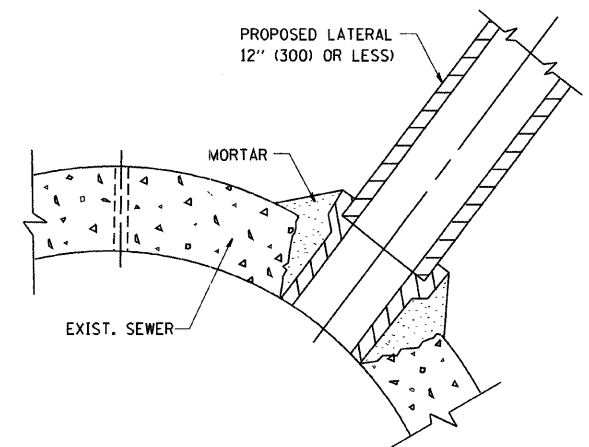
CONTRACT NO. 83999				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	60
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



DETAIL "A"
LATERAL CONNECTION TO EXISTING SEWER
OF 27" (675) OR SMALLER



DETAIL "B"
CLASS SI CONCRETE COLLAR



DETAIL "C"
PROPOSED LATERAL
CONNECTION TO EXISTING SEWER
OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS. THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

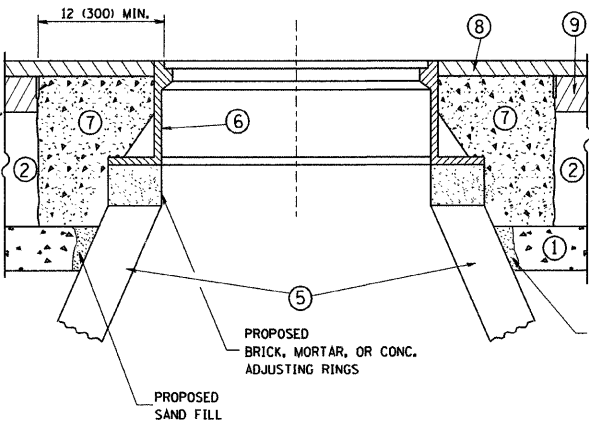
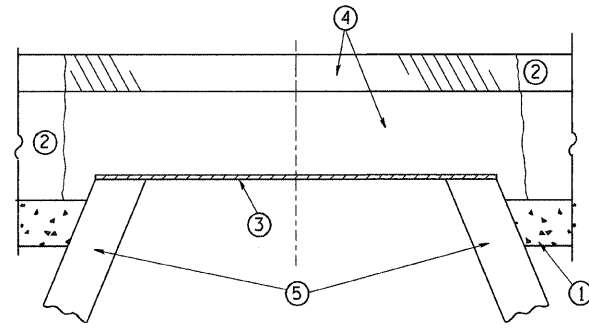
REVISIONS	
NAME	DATE
M. DE YONG	07/25/90
M. DE YONG	02/05/92
M. DE YONG	05/08/92
R. SHAH	09/09/94
R. SHAH	10/25/94
R. SHAH	06/12/96

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAIL OF STORM SEWER
CONNECTION TO EXISTING SEWER

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	61
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- 1 SUB-BASE GRANULAR MATERIAL
- 2 EXISTING PAVEMENT
- 3 36 (900) DIAMETER METAL PLATE
- 4 PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- 5 EXISTING STRUCTURE
- 6 FRAME AND LID (SEE NOTES)
- 7 CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- 8 PROPOSED HMA SURFACE COURSE
- 9 PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

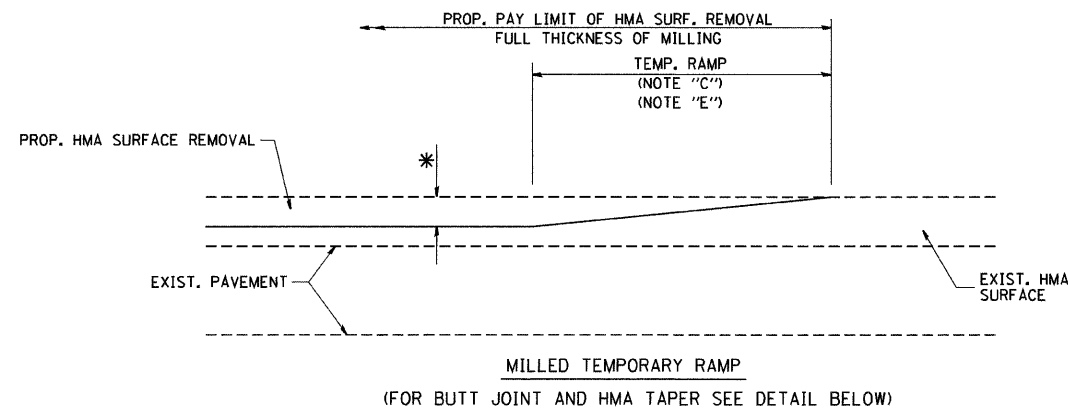
REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR
FRAMES AND LIDS ADJUSTMENT
WITH MILLING

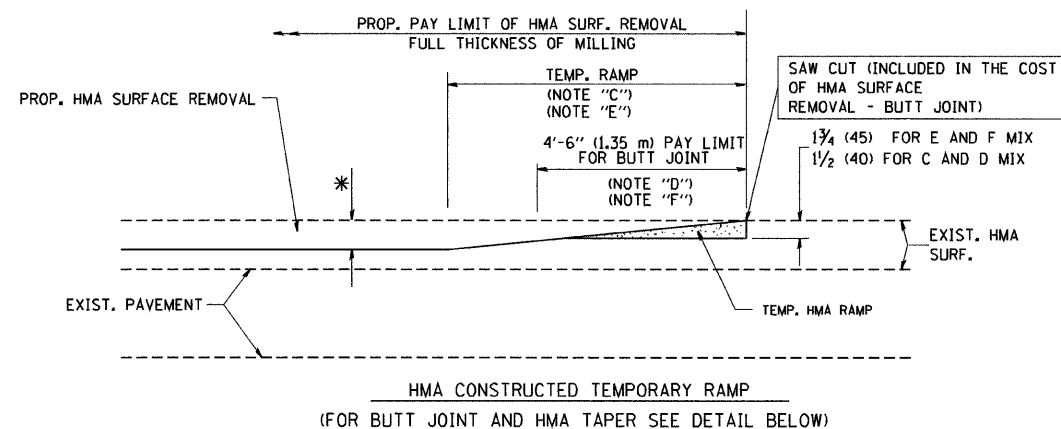
SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	62
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		

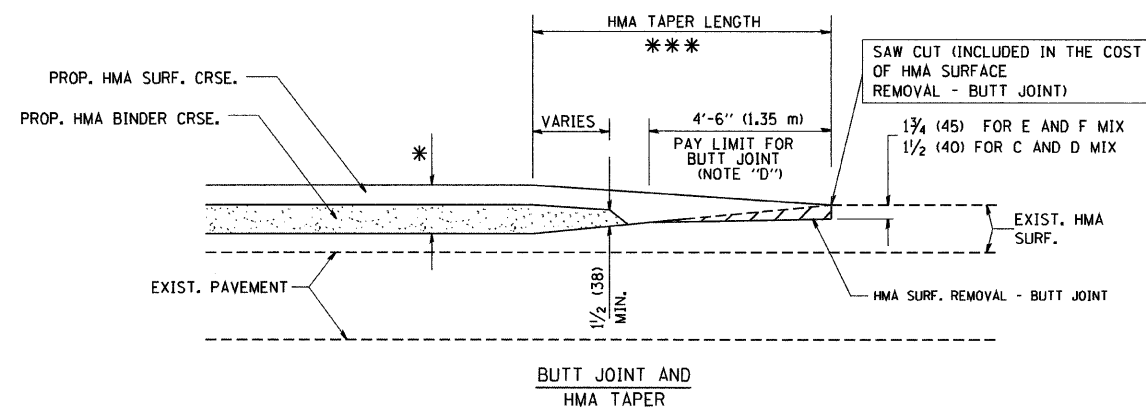
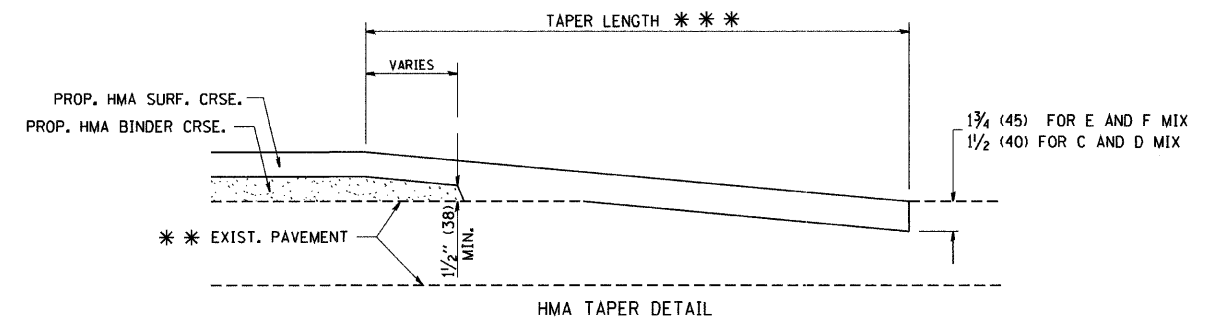
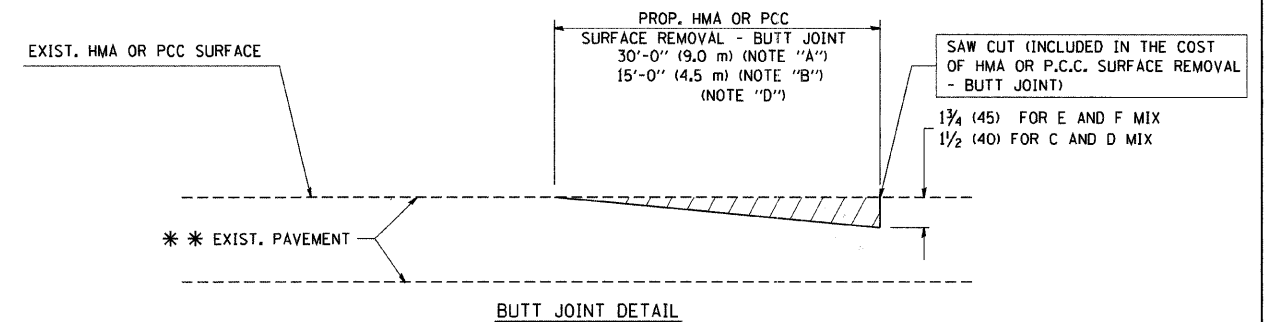


OPTION 1



OPTION 2

TYPICAL TEMPORARY RAMP

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACINGTYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- ** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

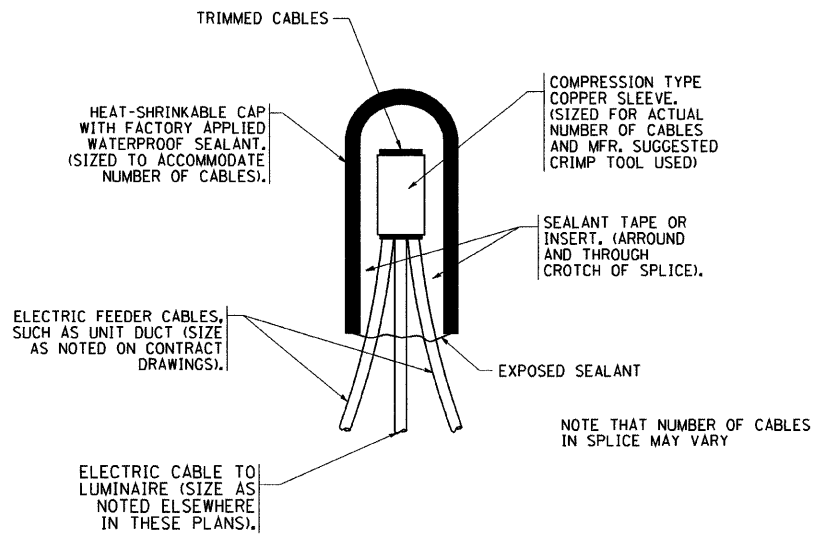
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

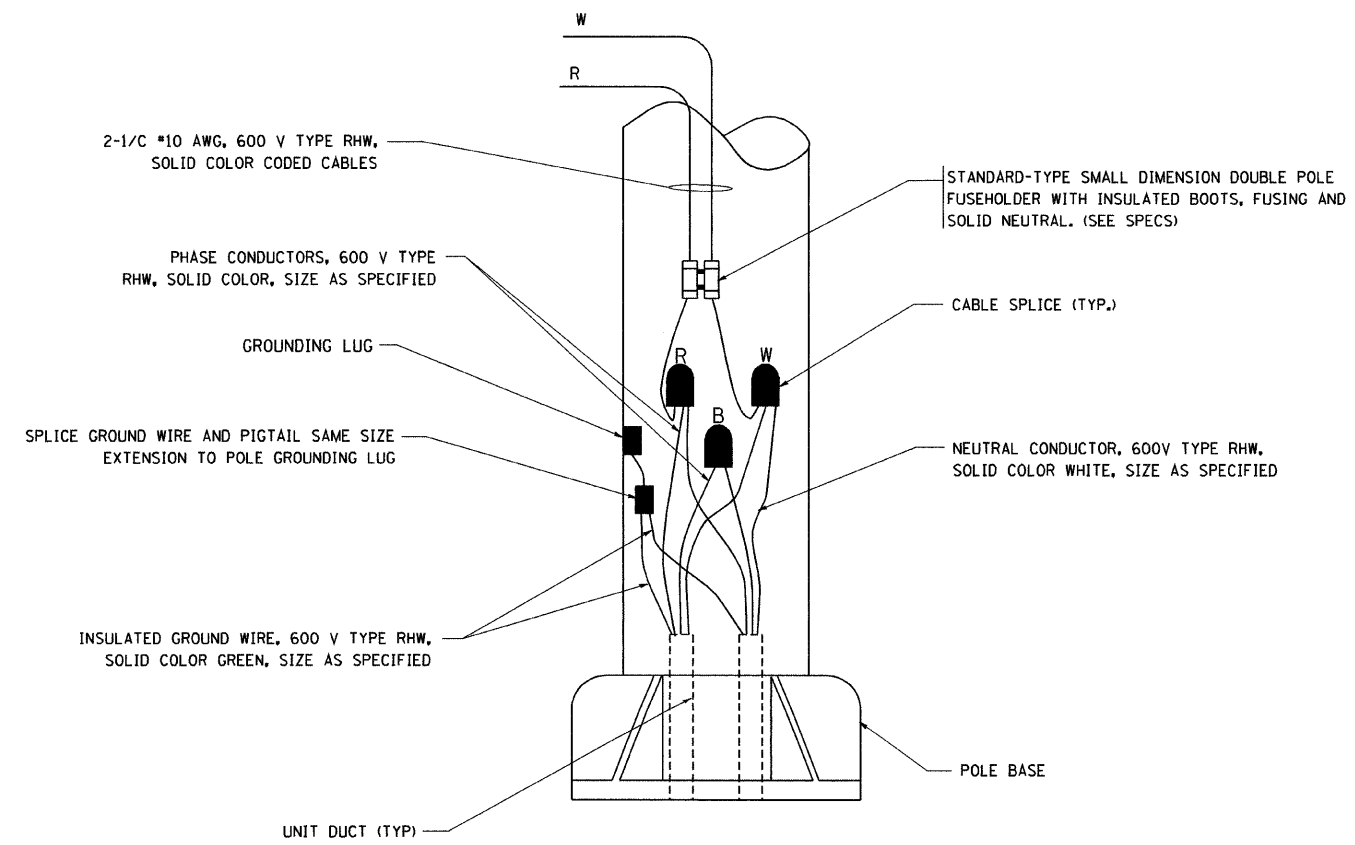
ILLINOIS DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER
DETAILSSCALE: VERT. NONE
HORIZ.DRAWN BY
CHECKED BY

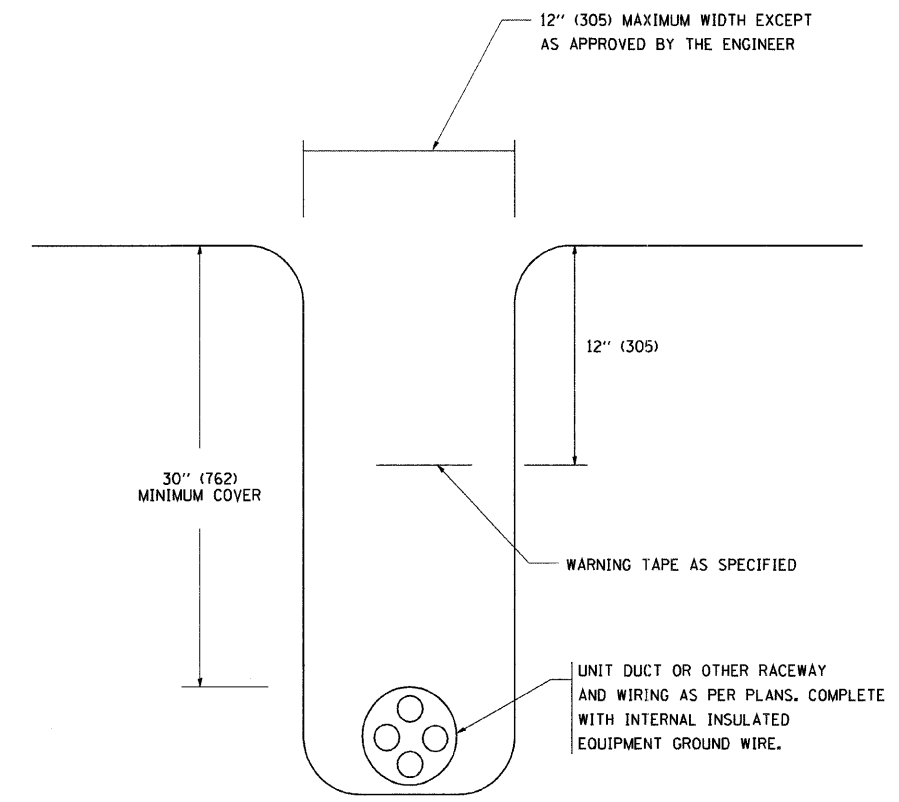
CONTRACT NO. 8999				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	63
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



TYPICAL SPLICE DETAIL
N.T.S.



POLE WIRING DETAIL
N.T.S.

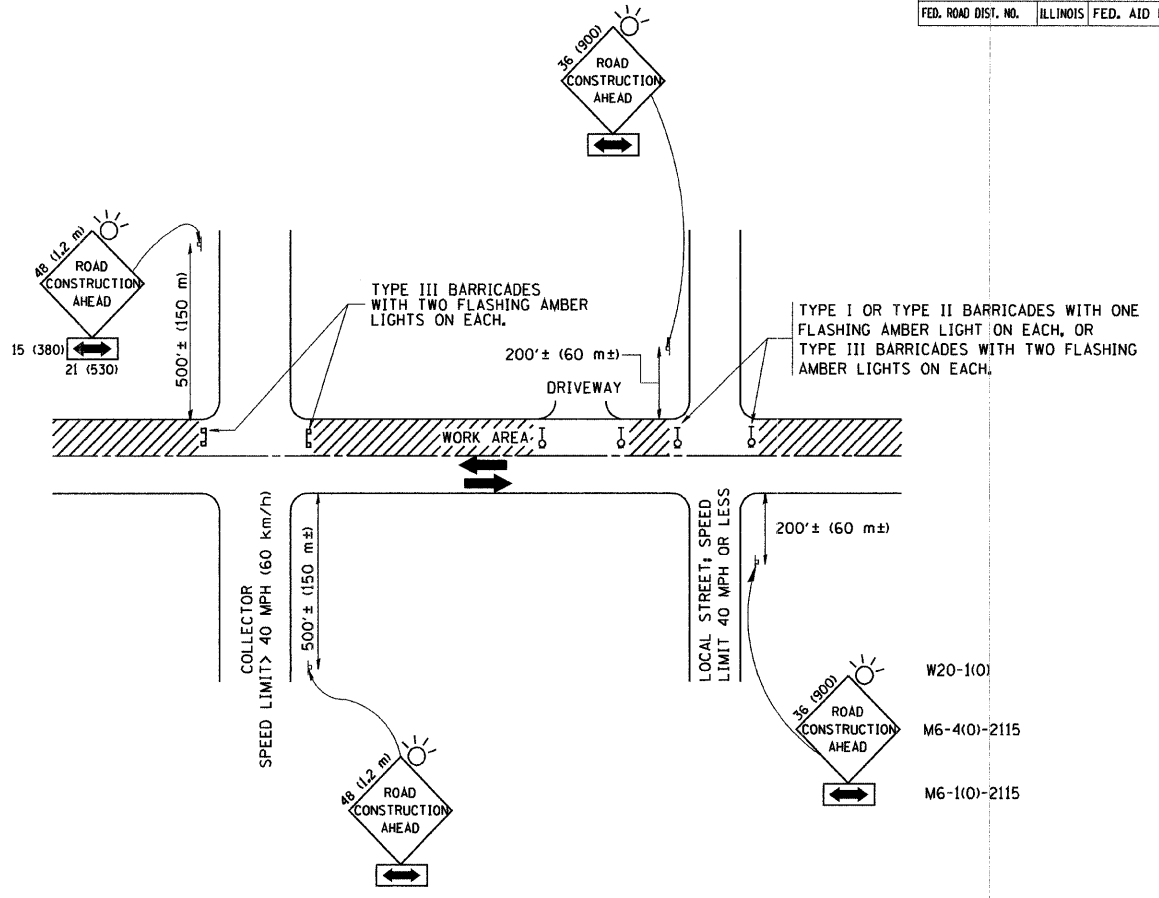


TYPICAL WIRING IN TRENCH DETAIL
N.T.S.

PLOT DATE = 4/18/2007
FILE NAME = K:\data\std\bae702.dgn
PLOT SCALE = 0.0000' / IN.
USER NAME = baeard

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE		
	08/08/03	MISC. ELECTRICAL DETAILS SHEET A	
SCALE: VERT. NONE HORIZ.		DRAWN BY CHECKED BY	

CONTRACT NO. 83999				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	64
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (12 m x 12 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

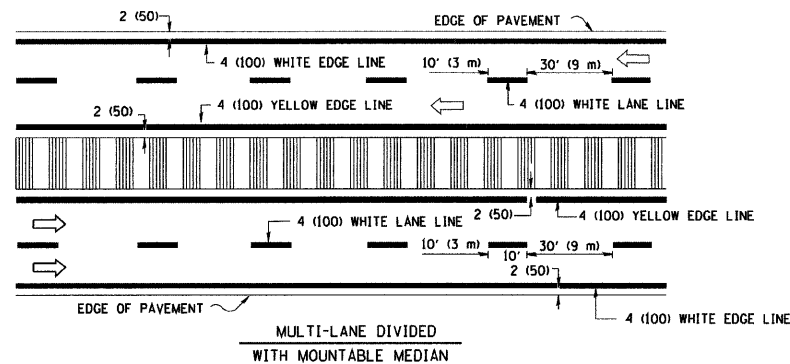
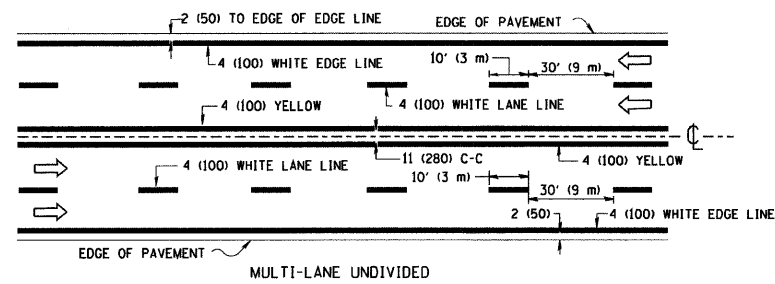
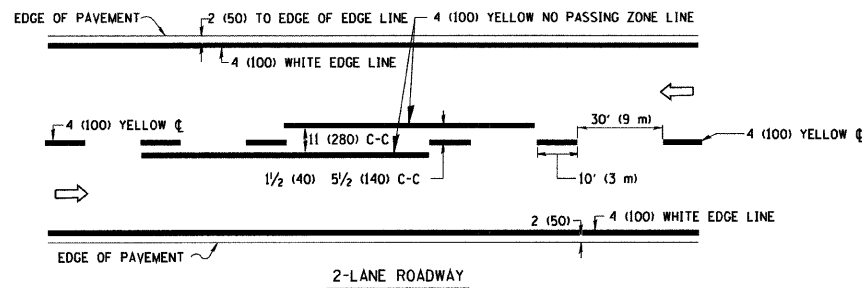
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL AND PROTECTION
FOR
SIDE ROADS, INTERSECTIONS, AND
DRIVEWAYS

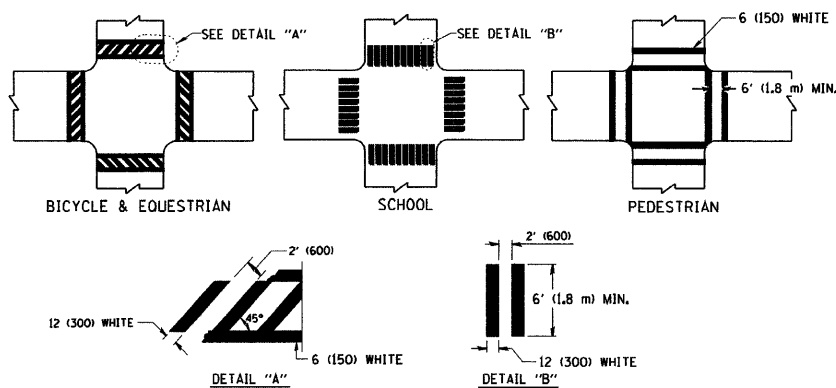
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CHECKED BY

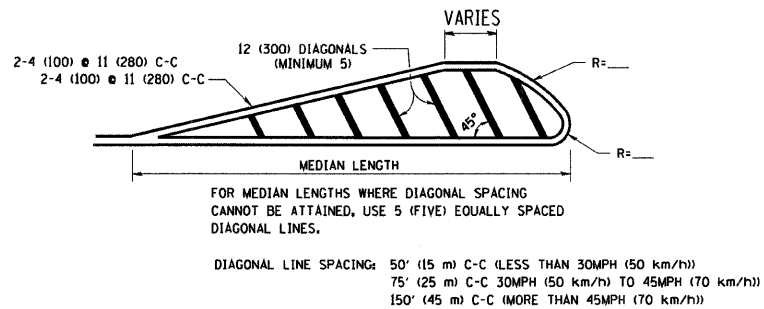
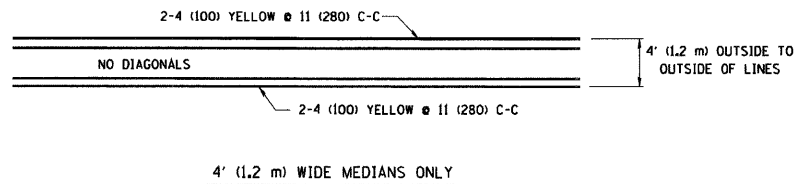
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	63
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



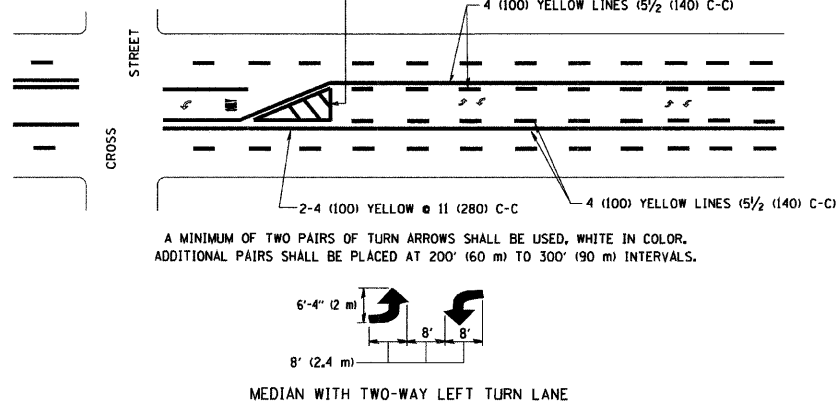
TYPICAL LANE AND EDGE LINE MARKING



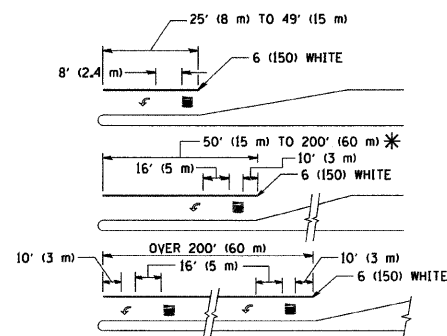
TYPICAL CROSSWALK MARKING



MEDIANS OVER 4' (1.2 m) WIDE



TYPICAL PAINTED MEDIAN MARKING

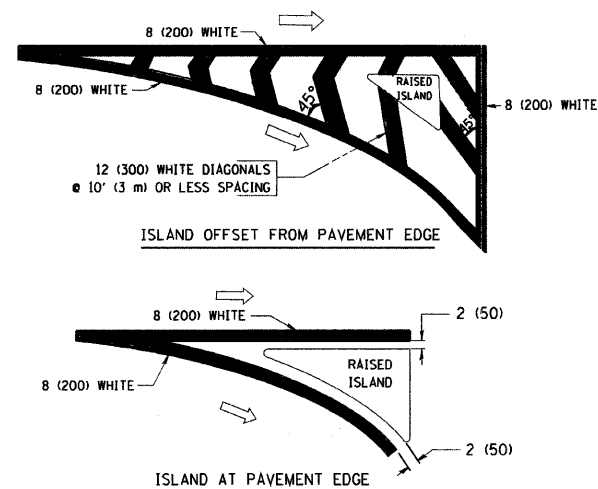


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 2' (600) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACH	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACH	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

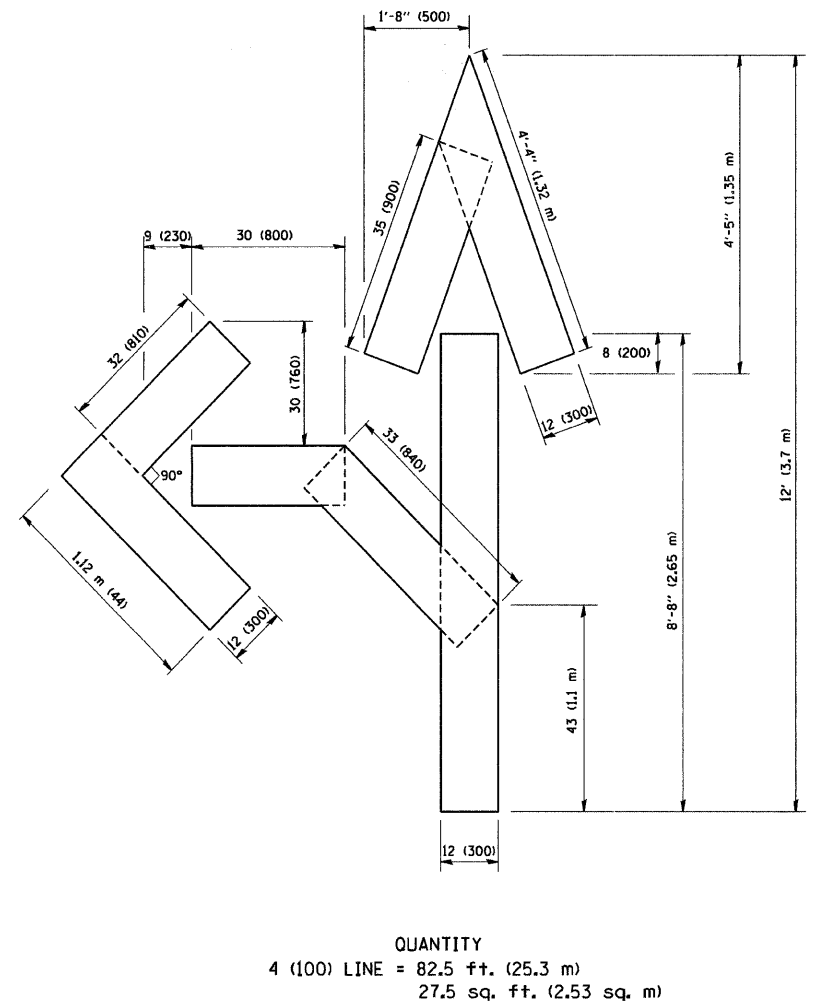
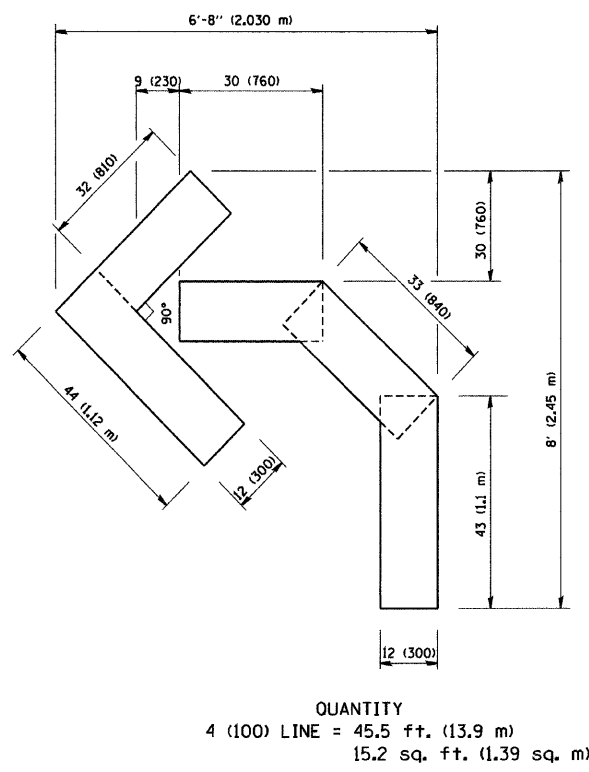
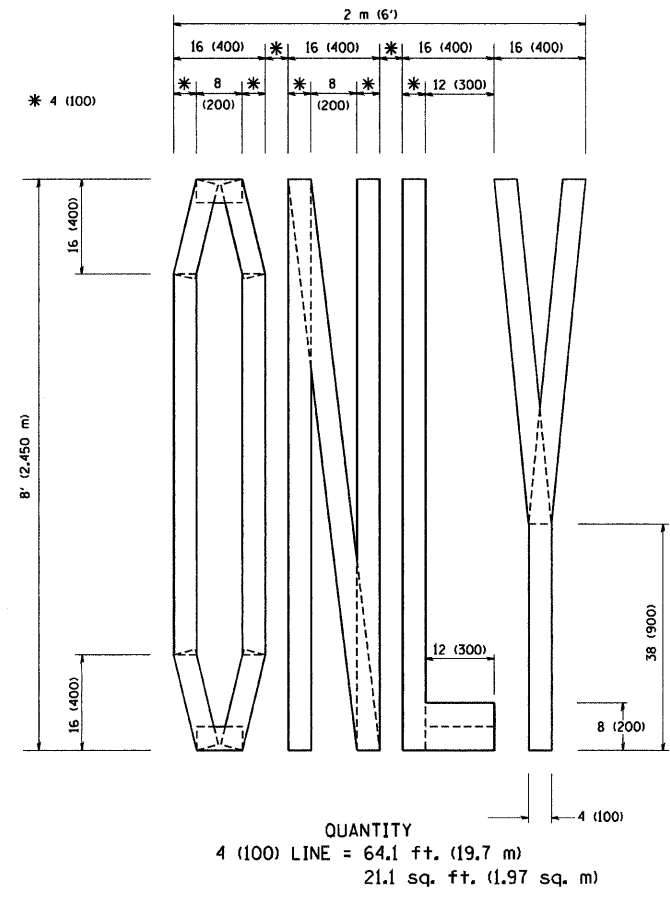
DISTRICT ONE
TYPICAL PAVEMENT
MARKINGS

SCALE: NONE

DRAWN BY CADD

CHECKED BY

CONTRACT NO. 83999				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	66
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

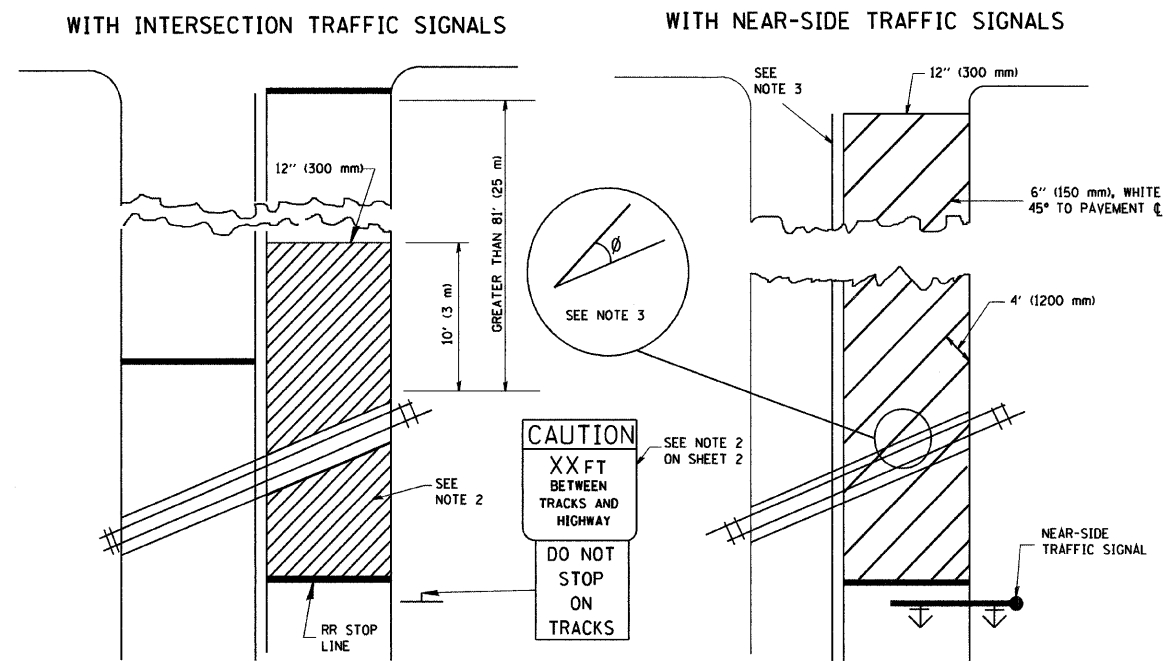
ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
LETTERS AND SYMBOLS
FOR TRAFFIC STAGING

SCALE: NONE

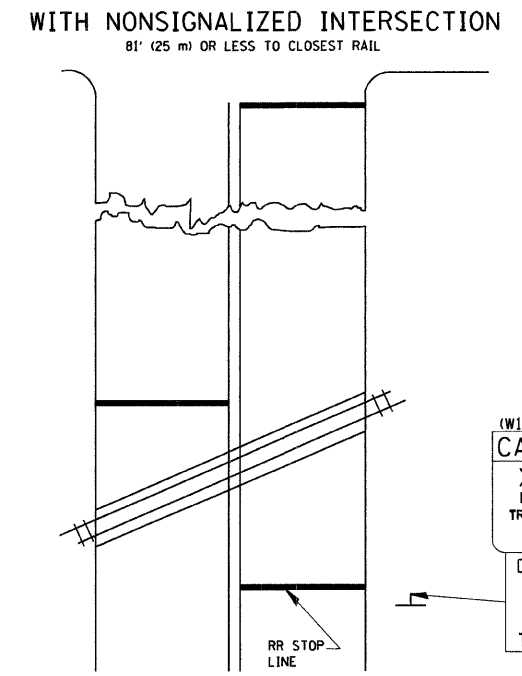
DRAWN BY CADD
CHECKED BY

PLOT DATE = 3/7/2007
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PLOT SCALE = 50.0000' / 1" IN.
USER NAME = bward



NOTES:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- WHERE THE ANGLE BETWEEN THE DIAGONAL STRIPES AND THE TRACK (θ) WOULD BE LESS THAN APPROXIMATELY 20°, THE STRIPES SHOULD BE SLOPED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.



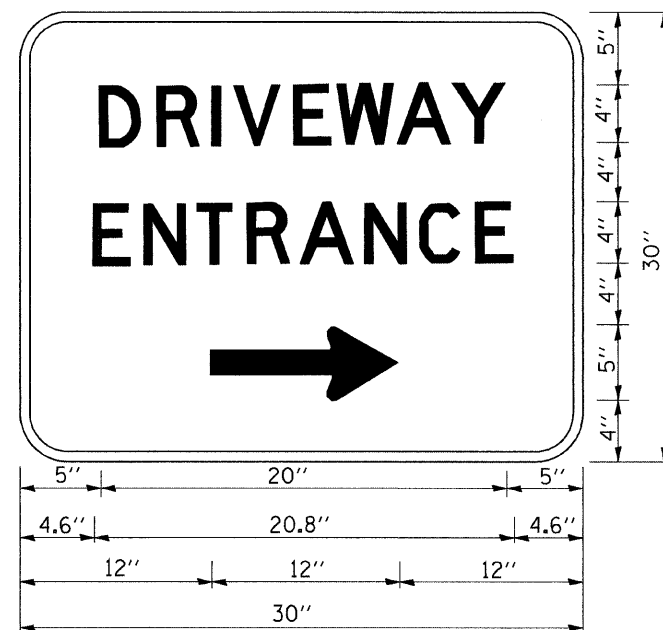
NOTE:

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	TYPICAL SUPPLEMENTAL	
	01/01/07	SIGNING AND PAVEMENT MARKING	
		TREATMENT FOR RAILROAD CROSSINGS	
		SCALE: NONE	
		DRAWN BY	
		CHECKED BY	

CONTRACT NO. 83999				
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	68
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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USER NAME = bawrdi

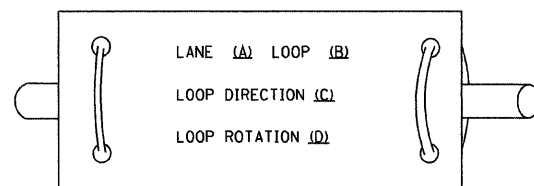
REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	DRIVEWAY ENTRANCE SIGNING	
C. JUCIUS	02/15/07		
SCALE: NONE		DRAWN BY R.H.	
DATE		CHECKED BY	

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	69
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

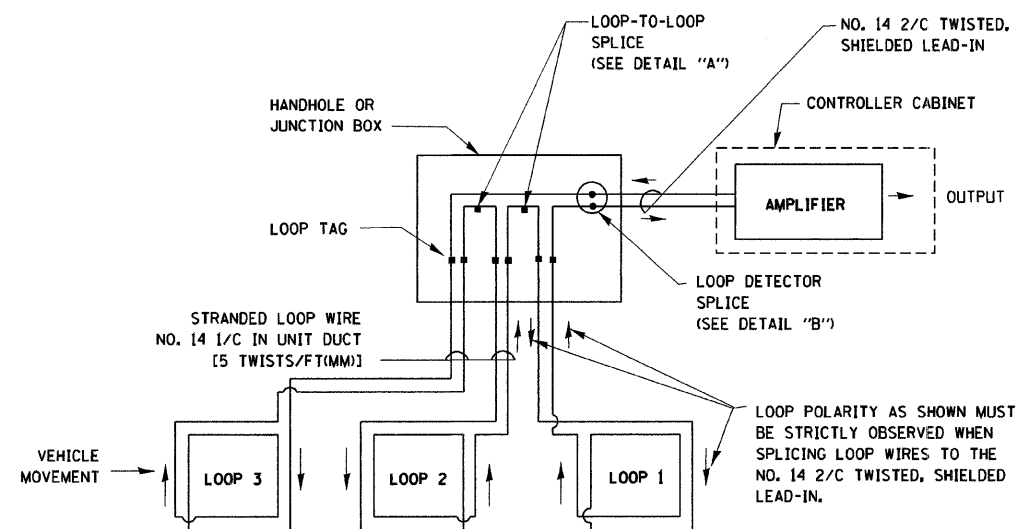
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

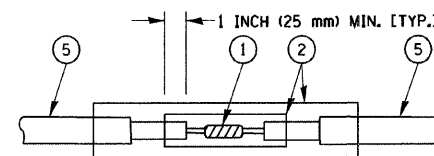
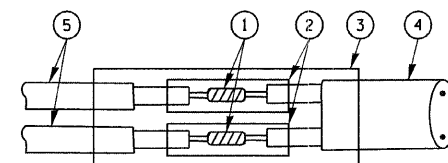


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

DETAIL "A"
LOOP-TO-LOOP SPLICEDETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

REVISIONS	
NAME	DATE
CADD	5/30/00
ADD NOTE NO. 8	11/12/01
BUREAU OF TRAFFIC	1-01-02

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

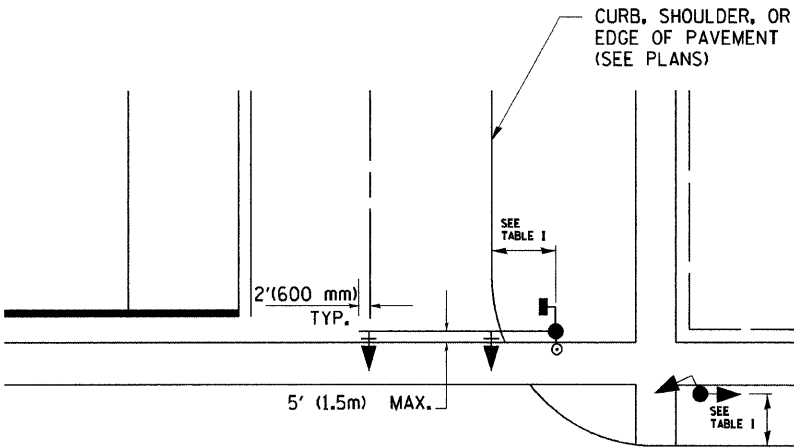
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DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 1 OF 4

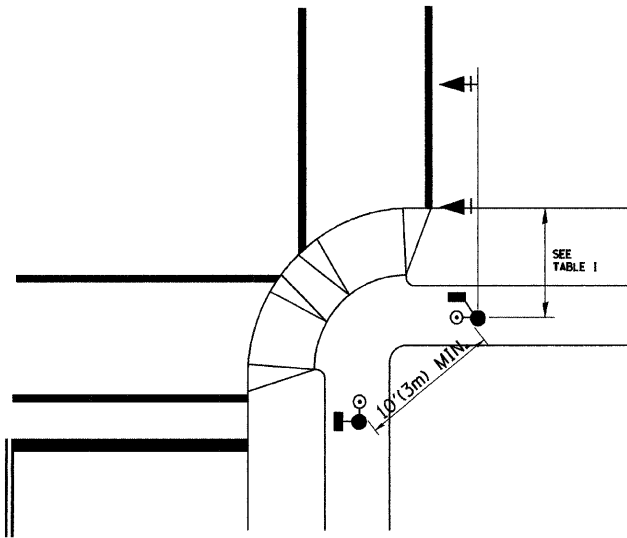
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			73	70
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

- AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.

AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.

PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:

A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL-WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.

B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.

C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).

E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK
- PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.
- THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

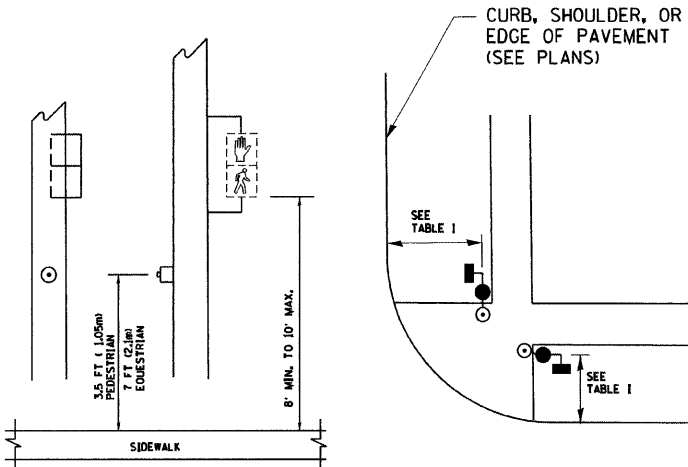


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

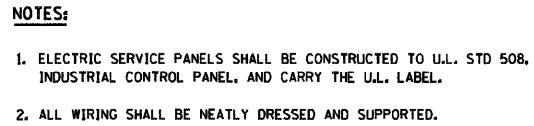
REVISIONS	
NAME	DATE
BUREAU OF TRAFFIC	1/01/02

ILLINOIS DEPARTMENT OF TRANSPORTATION

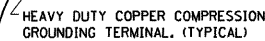
DISTRICT 1
STANDARD TRAFFIC SIGNAL
DESIGN DETAILS

SCALE: NONE

DRAWN BY: RWP
DESIGNED BY: DAZ
CHECKED BY: DAZ
SHEET 2 OF 4



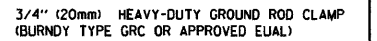
(NOT TO SCALE)



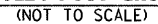
(NOT TO SCALE)

GROUNDING SYSTEM

- HEAVY-DUTY COMPRESSION TERMINAL
(BURNDY TYPE YGHA OR APPROVED EQUAL)

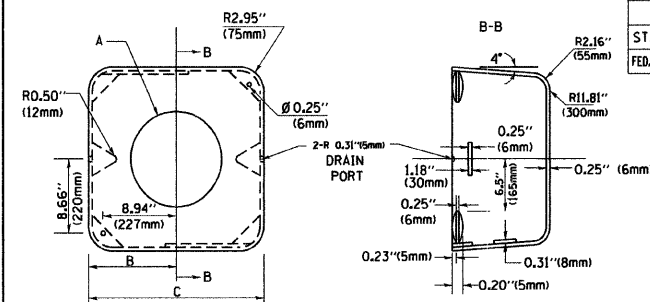
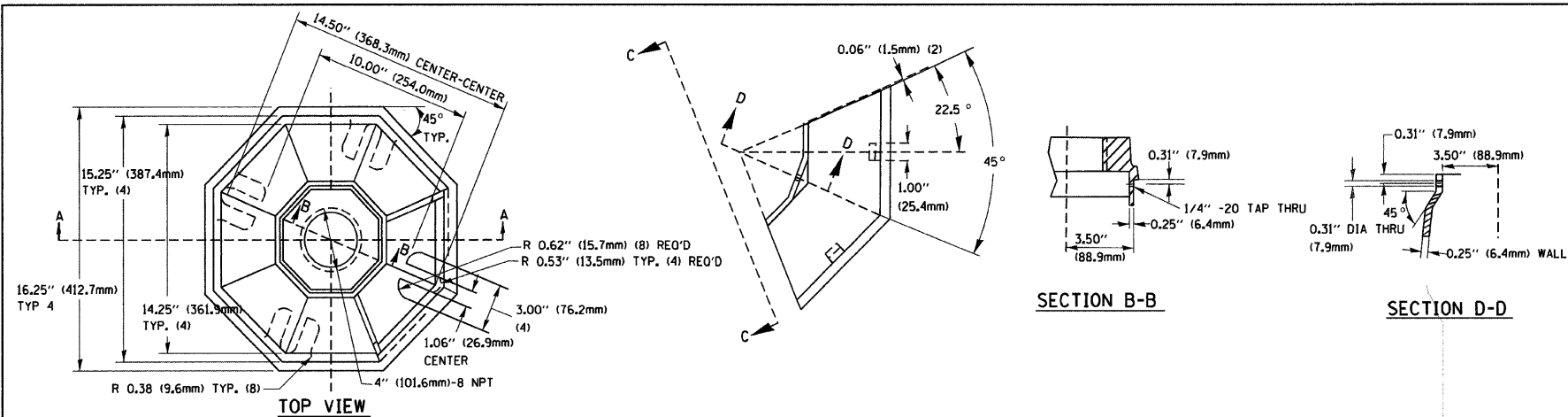


- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.

ILLINOIS DEPARTMENT OF TRANSPORTATION

SCALE: NONE

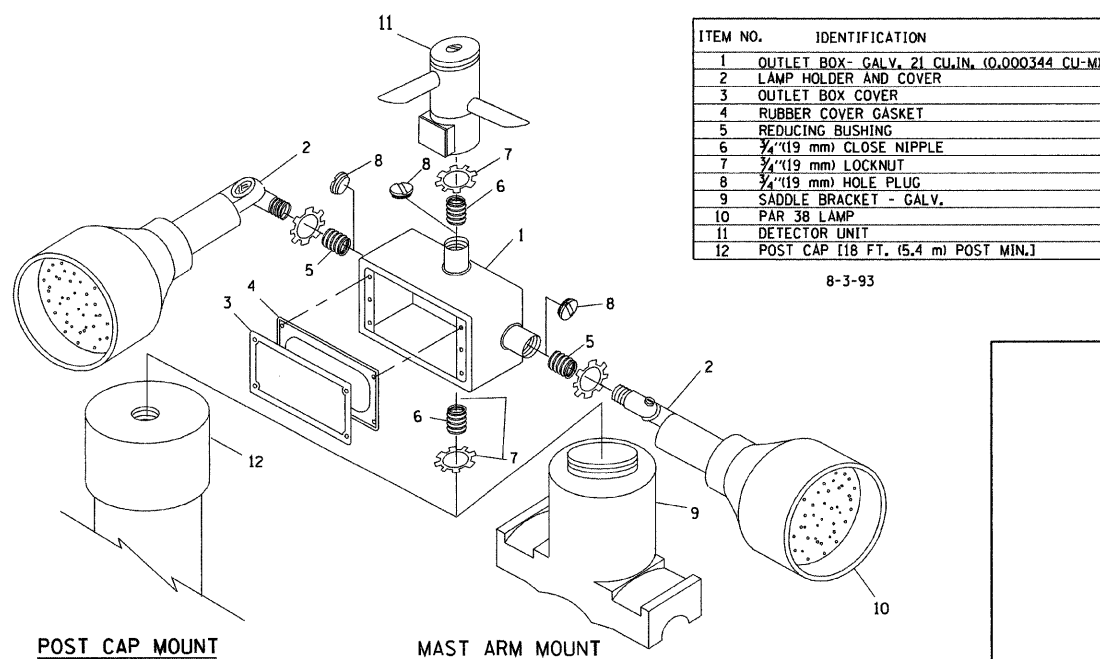
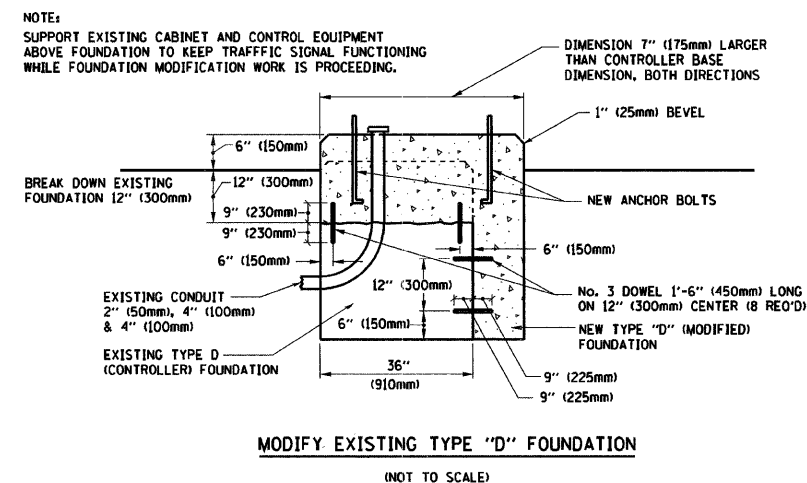
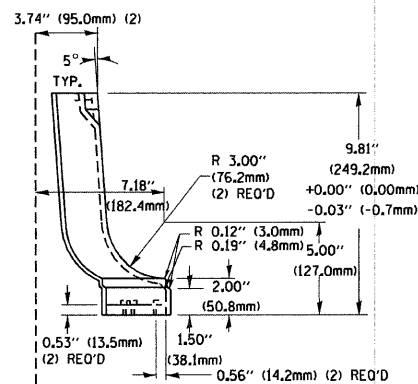
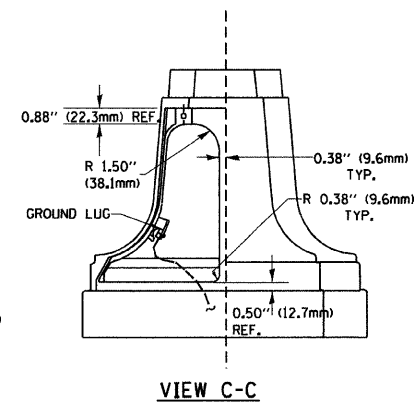
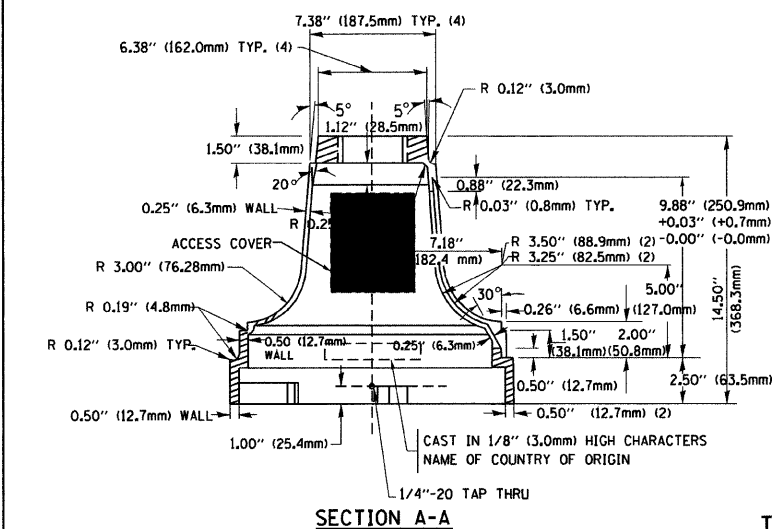
DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 3 OF 4



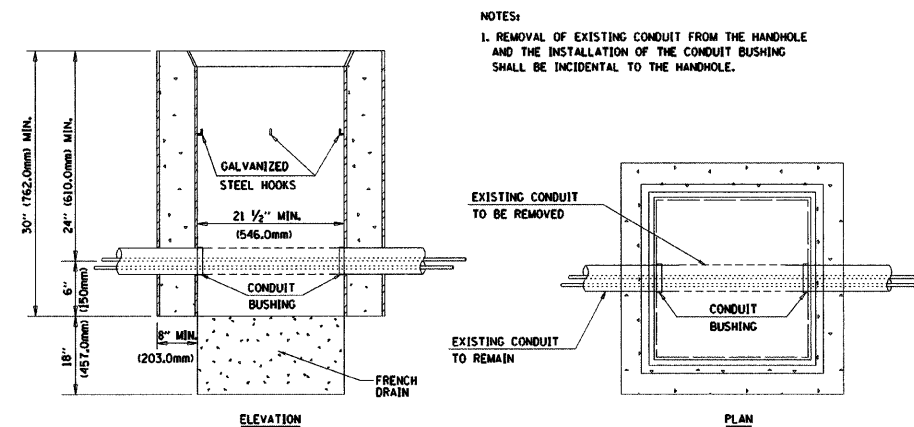
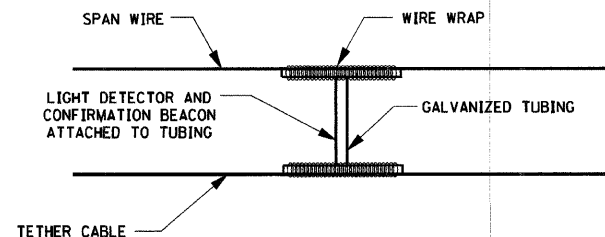
TYPE	A	B	C	HEIGHT	WEIGHT
I	∅ 10.125"(257mm)	9.5"(241mm)	19"(483mm)	12" (300mm)	24kg
II	∅ 11.125"(283mm)	10.75"(273mm)	21.5"(546mm)	12" (300mm)	26kg

MATERIAL:

- ASTM A48 CLASS 30 GREY IRON
- ASTM A123 HOT DIPPED GALVANIZED



- NOTES:**
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



REVISIONS	
NAME	DATE
BUREAU OF TRAFFIC	5/30/01
BUREAU OF TRAFFIC	3/15/01
BUREAU OF TRAFFIC	11/12/00
BUREAU OF TRAFFIC	1-01-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

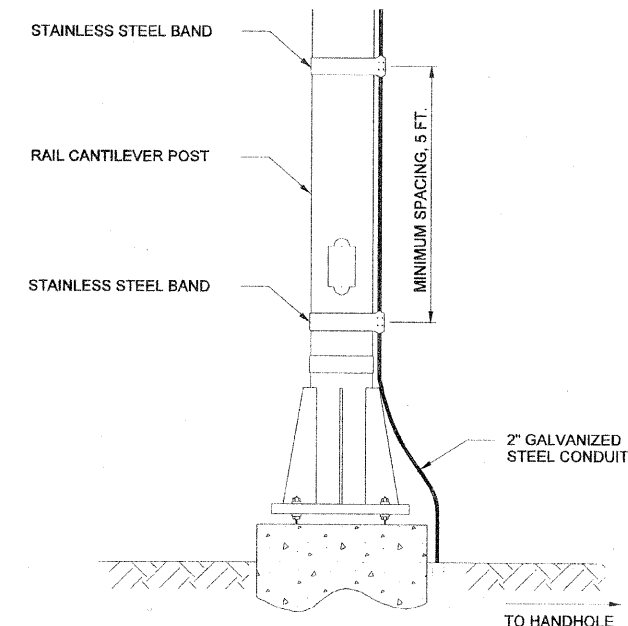
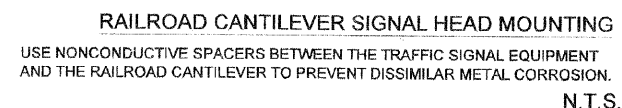
DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE

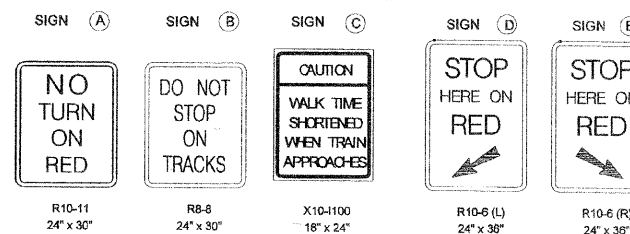
DRAWN BY: RWP
DESIGNED BY: DAD
CHECKED BY: DAZ
SHEET 4 OF 4

PLAN	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	ALIGNMENT CHECKED		
	RT. OF WAY CHECKED		
	CADD FILE NAME		

PROFILE	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	GRADES CHECKED		
	B.M. NOTED		
	STRUCTURE NOTATIONS CHK'D		



USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION.



TYPICAL TRAFFIC PRE-SIGNALS AT RAILROAD GRADE CROSSING

BDE PROCEDURE MEMORANDUM 44-05
ATTACHMENT

06/01/05

[illegible]

BDE PROCEDURE
MEMORANDUM 45-05

TYPICAL TRAFFIC PRE-SIGNALS
AT RAILROAD GRADE CROSSING

CONSULTANT	
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TY·LIN INTERNATIONAL

VILLAGE OF WILMETTE
GREEN BAY ROAD
INTERSECTION IMPROVEMENTS

DRAWN:	SHEET NO.
CHECKED:	
APPROVED:	
DATE: 02-14-2008	
SCALE: NONE	
JOB NO.:	PROJECT N