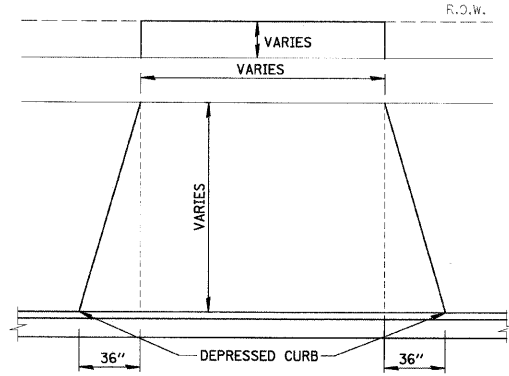


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1678	07-00064-00-RS	COOK	17	13
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

CONTRACT NO. 63009

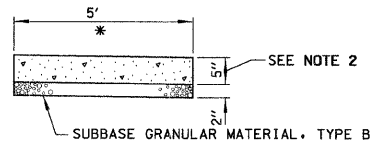


DETAIL OF DRIVEWAY

THIS TYPICAL DRIVE LAYOUT IS FOR BOTH CONCRETE AND ASPHALT DRIVES

- P.C.C. DRIVE - 7" P.C.C. DRIVEWAY PAVEMENT
- 2" SUB-BASE GRANULAR MATERIAL, TYPE B
- ASPHALT DRIVE - 2" HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (QC/QA)
- 6" AGGREGATE BASE COURSE, TYPE B

NOTE: PROPOSED SIDEWALK THROUGH DRIVEWAY SHALL BE 7 INCHES OF PORTLAND CEMENT CONCRETE ON 2 INCH SUB-BASE GRANULAR MATERIAL, TYPE B.

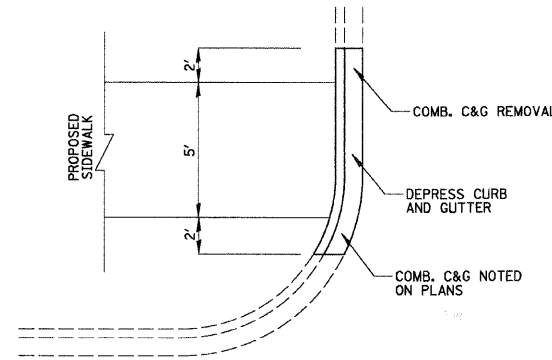


* CROSS SLOPE 2%

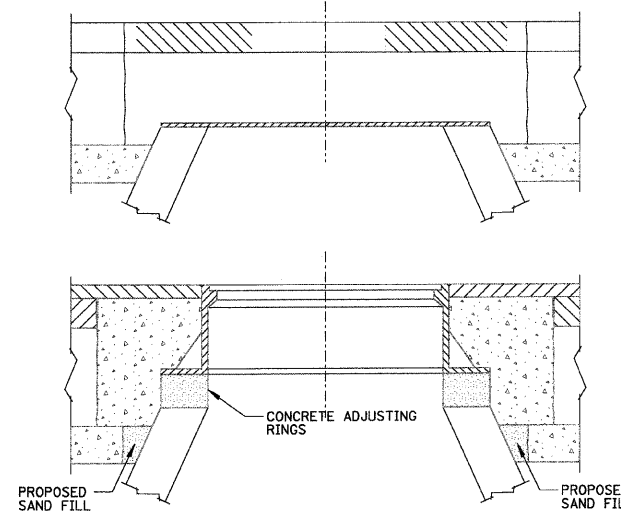
NOTES:

- ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCLUDED IN THE COST FOR P.C.C. SIDEWALK.
- WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.
- ALL LANDSCAPE RESTORATION (TOPSOIL, SEEDING CLASS 1 AND SUPPLEMENTAL WATERING) SHALL BE INCLUDED IN THE COST FOR P.C.C. SIDEWALK.

P.C.C. SIDEWALK DETAIL



**PROPOSED DEPRESSED CURB & GUTTER
EXISTING CURB & GUTTER NOT DEPRESSED
NOT TO SCALE**



CONSTRUCTION PROCEDURES

STAGE I (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2" THICK BITUMINOUS MATERIAL APPROVED BY THE ENGINEER.

STAGE II (AFTER PAVEMENT MILLING)

- REMOVE THE BITUMINOUS MATERIAL AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

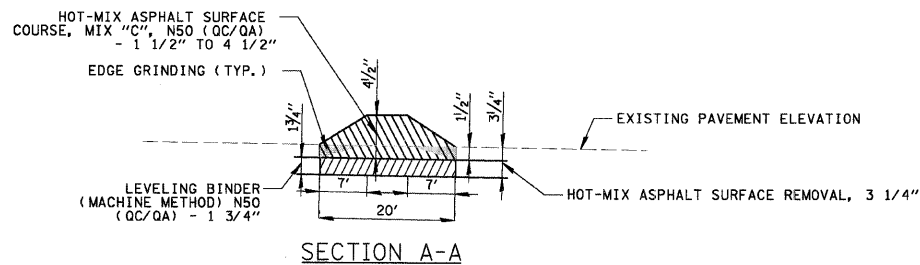
BASIS OF PAYMENT

DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL).

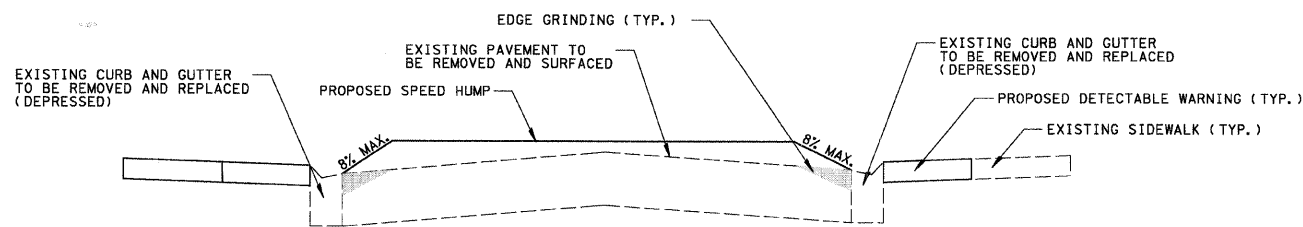
NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

DRAINAGE STRUCTURE ADJUSTMENT (SPECIAL)



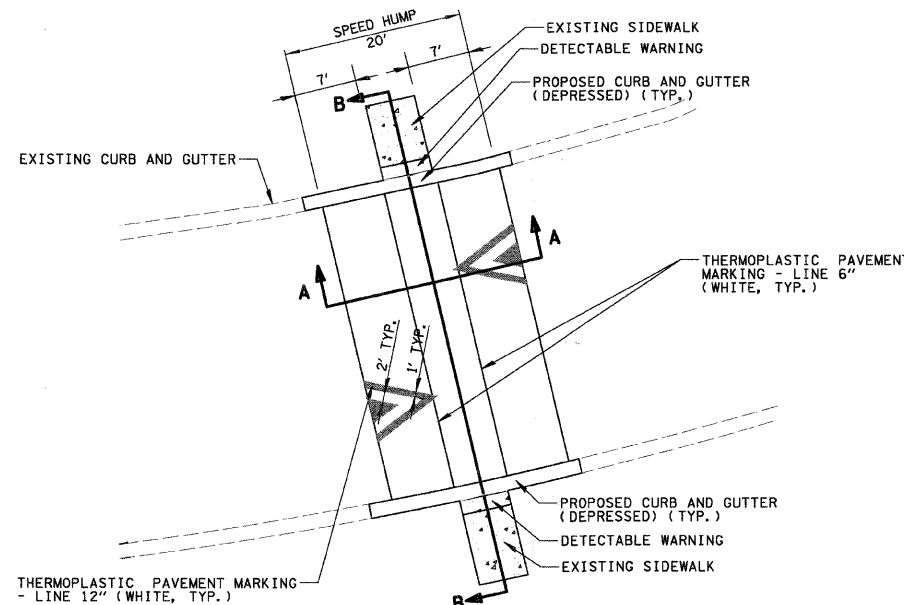
SECTION A-A



SECTION B-B

SPEED HUMP - PROFILES

NOT TO SCALE



SPEED HUMP - PLAN

NOT TO SCALE

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION CONSTRUCTION DETAILS CITY OF NORTHLAKE LEMOYNE SREET
NAME	DATE	
		SCALE N.T.S. DATE 12/27/2007
		DRAWN BY EDT CHECKED BY MW

DATE	BY
DATE	BY
DATE	BY

CHRISTOPHER B. BURKE
ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

DATE	BY
DATE	BY
DATE	BY