

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PROPOSED
HIGHWAY PLANS

F.A.P.	SECTION	COUNTY	TOTAL SHEET NO.
317/630	20RS-2; 19RS-4; 18RS-5	FULTON	30
	ITE. US 24/L 100	ILLINOIS	CONTRACT NO. 68953

D-94-044-10



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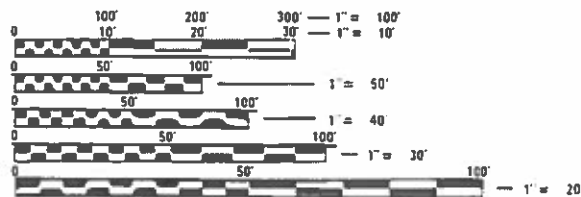
HIGHWAY STANDARDS

406201-01	667101-02
424001-11	701306-04
701311-03	701901-08
701501-06	780001-05
781001-04	701801-06

CADD STANDARDS

406101-D4
440001-D4
780001-04

ADT 5650/2017
 MU 9.43%
 SU 11.32%

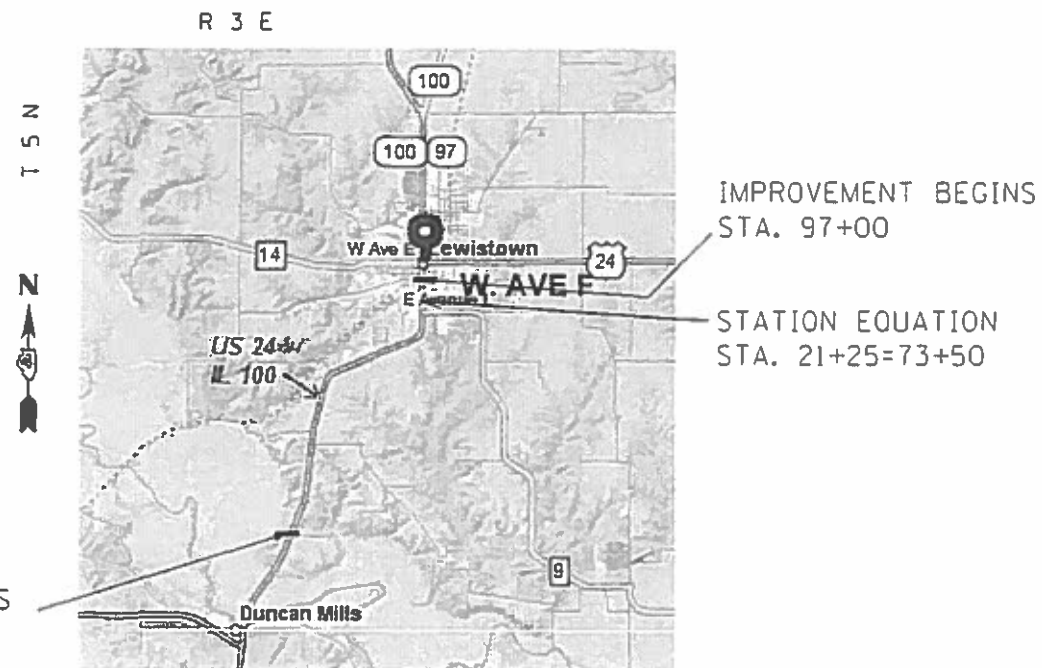


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT ENGINEER: GEORGE ELIAS, 309-671-3467
 PROJECT MANAGER: MIKE LEWIS, 309-671-3454
 CONTRACT NO. 68953
 CATALOG NO. 034375-00D

FAP 317, ROUTE US 24/L 100
SECTION 20RS-2; 19RS-4; 18RS-5
PROJECT: NHPP-RF3H(402)
TYPE of IMPROVEMENT: RESURFACING
FULTON COUNTY
C-94-044-10



JOB DESCRIPTION:
 COLD-IN-PLACE, MILLING & RESURFACING OF US 24 FROM AVE F IN LEWISTOWN, TO NORTH OF RUT ROAD

GROSS LENGTH = 15'218 FT. = 2.88 MILE
 NET LENGTH = 15'218 FT. = 2.88 MILE

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

SUBMITTED Jan 31 20 20
Kensel A. Bennett ESQ
 DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 20 20
[Signature]
 ENGINEER OF DESIGN AND ENVIRONMENT

March 20 20
[Signature]
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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 OF THE STATE OF ILLINOIS**

COMMITMENTS

Commitments are not to be altered without the written approval of all parties to which the commitment was made. No Commitment were made.

CONSECUTIVE SIDE STREET (ROAD) CLOSURE – PROHIBITED

1. No two consecutive side streets (roads) may be closed at the same time during construction. The Contractor must alternate streets (roads).
2. Adjacent side roads will not be closed simultaneously. BLR Standard 21 shall be used for all local road closures without any entrances within the closed area. BLR Standard 22 can be used where it is necessary to allow local traffic access.

TEMPORARY MATERIAL REQUIREMENTS – UTILITY AND DRIVEWAY CROSSINGS

Incidental hot-mix asphalt surface shall be used for all temporary side road crossings. Aggregate surface course may be used for all driveway crossings except during winter shutdown in accordance with Article 107.09.

PAVING SURFACE COURSE

Continuous paving operations on the main roadway shall be maintained at all times during the construction of the hot-mix asphalt surface. No interruptions for side roads, entrances, turn lanes, etc. will be allowed.

TAPER REMOVAL FRAME & GRATES ADJUSTED BY OTHERS

At locations where frames and grates have previously been adjusted by others and they are surrounded by hot-mix asphalt tapers, the Contractor for this contract shall remove and dispose of the hot-mix asphalt taper material prior to the placement of the hot-mix asphalt surface course. This work will not be paid for separately, but will be considered as included in the cost of the HOT-MIX ASPHALT SURFACE COURSE pay item.

ENVIRONMENTAL REVIEWS

Prior to the use of any proposed borrow areas, use areas (temporary access roads, detours, run-arounds, etc.) and/or waste areas, the Contractor shall file the required environmental resource request surveys according to Section 107.22 of the Standard Specifications. These surveys are required in order for the Department to conduct cultural and biological resource surveys for the proposed site.

Prior to any waste materials being removed from the construction site the required environmental resource surveys will need to be obtained and filed by the Contractor. Excess waste products removed from the construction site shall be disposed of as required in Section 202.03 of the Standard Specifications.

Any protruding metal bars shall be removed prior to the disposal of broken concrete at approved disposal sites.

The required environmental resource documentation shall include the following:

- * BDE Form 2289 (Environmental Survey Request)
- * A location map showing the size limits and location of the use area
- * Signed property owner agreement form-D4 PI0100
- * Color photographs depicting the use area
- * Borrow Area Entry Agreement form-D4 PI0101

Please note that a minimum of two weeks shall be allowed for the District to obtain the required environmental clearances.

NO PASSING ZONE VERIFICATION

The resident shall contact Operations to verify the location of no passing zones prior to placement of centerline striping.

AGGREGATE (DESCRIPTION), TYPE B

Aggregate (Description), Type B shall be required for all granular construction of side roads, entrances, and mailbox turnouts, whether or not portions of the surfaces thus constructed are to be covered with a bituminous surface, except where noted differently on the plans.

FILE NAME =	USER NAME = hudelsonme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES			F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\planroom.dot\illinois.gov\PIDOT\Documents\IDOT Offices\District 4\Projects\04.68953\Drawings\0468953-sht-plen.dgn	DRAWN	CHECKED -	REVISED -					317/630	20RS-4;19RS-4;18RS-5	FULTON	30	2
Default	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -		SCALE:	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 68953				
	PLOT DATE = 1/31/2020	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

PAVEMENT STATIONING NUMBERS & PLACEMENT

The Contractor shall provide labor and materials required to imprint pavement station numbers in the finished surface of the pavement and/or overlay. The numbers shall be approximately 3/4 inch (20mm) wide, 5 inches (125 mm) high and 5/8 inch (15 mm) deep.

The pavement station numbers shall be installed as specified herein:

Interval – 200 feet (English stationing) or 100 meters (metric stationing)

Bottom of Numbers – 6 inches (150 mm) from the inside edge of the pavement marking

Location:

- * 2,3, & 5 Lane Pavements – right edge of pavement in direction of increasing stations
- * Multi-Lane Divided Roadways – outside edge of pavement in both directions
- * Ramps – along baseline edge of pavement

Position – stations shall be placed so they can be read from the adjacent shoulder

Format – English (Metric) pavement stations shall use this format "XXX (XX + X00)" where X represents the pavement station

This work will not be paid for separately, but will be considered included in the cost of the associated pavement and/or overlay pay items.

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

Mixture Use(s):	Surface Course 1.5"	Polymer Binder Course 1"	Binder Course 2.25"	Incidental Surface
AC/PG:	PG 64-22	SBS or SBR 76-22	PG 64-22	PG 64-22
Design Air Voids:	4.0% @ N=50	4.0% @ N=50	4.0% @ N=50	4.0% @ N=50
Mixture Composition: (Mixture Gradation)	IL 9.5	IL 4.75	IL 9.5	IL 9.5
Friction Aggregate:	Mix D	N.A.	N.A.	Mix C
Quality Management Program:	QCP	QCQA	QCQA	QCQA

Notes: Use MTD for all 1" & 2.25" Binder, & all Surfaces. We will use NTEA for Tack on all lifts at 0.08lbs/sf residual.

Notes: Individual lift thickness of each mix type will be no less than 3 times nominal maximum aggregate size and no more than 6 times nominal aggregate size.

BUTT JOINT CUTTING TIME RESTRICTION

Butt joints shall not be milled more than three (3) days prior to placement of the HMA surface course.

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				0006	0006
				FULTON CO.	FULTON CO.
				80% FED 20% STATE	100% STATE
21400100	GRADING AND SHAPING DITCHES	FOOT	115	115	
25000750	MOWING	ACRE	8.7		8.7
25100630	EROSION CONTROL BLANKET	SQ YD	250	250	
28100825	STONE DUMPED RIPRAP, CLASS B3	TON	31	31	
40600982	HOT - MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	5623	5623	
40600990	TEMPORARY RAMP	SQ YD	4617	4617	
40602978	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N50	TON	1338	1338	
40603200	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-4.75, N50	TON	2143	2143	
40604060	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N50	TON	4107	4107	
40800050	INCIDENTAL HOT - MIX ASPHALT SURFACING	TON	945	945	
42000560	PORTLAND CEMENT CONCRETE PAVEMENT 13"	SQ YD	13.5	13.5	
42400100	PORTLAND CEMENT CONCRETE SIDEWALK 4 INCH	SQ FT	409	409	
* 42400800	DETECTABLE WARNINGS	SQ FT	47	47	
44000100	PAVEMENT REMOVAL	SQ YD	13.5	13.5	

*= SPECIALTY ITEM

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		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/	20RS-419RS-418RS-5	FULTON	30	4
630	RTE. US 24/IL 100		CONTRACT NO. 68953	
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				0006	0006
				FULTON CO. 80% FED 20% STATE	FULTON CO. 100% STATE
44000164	HOT - MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SQ YD	10314	10314	
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1013	1013	
44000600	SIDEWALK REMOVAL	SQ FT	373	373	
48101200	AGGREGATE SHOULDERS, TYPE B	TON	2288	2288	
54001001	BOX CULVERT END SECTIONS, CULVERT NO. 1	EACH	1	1	
60262700	INLETS TO BE RECONSTRUCTED	EACH	2	2	
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	1013	1013	
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4	4	
67100100	MOBILIZATION	L SUM	1	1	
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1	
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1	
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	42	42	

*= SPECIALTY ITEM

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/	20RS-419S-418RS-5	FULTON	30	5
630	RT. US 24/IL 100	FULTON	CONTRACT NO.	68953
ILLINOIS FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				0006	0006
				FULTON CO. 80% FED 20% STATE	FULTON CO. 100% STATE
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	6862	6862	
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2285	2285	
* 78009000	MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	130	130	
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	38262	38262	
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	36	36	
* 78009024	MODIFIED URETHANE PAVEMENT MARKING - LINE 24"	FOOT	52	52	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	220	220	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	220	220	
X0300003	TRACKLESS TACK	POUND	8475	8475	
X4200001	CIR-FDR EMULSIFIED ASPHALT	GALLON	55761	55761	
X4200002	COLD IN-PLACE RECYCLING, 4.0"	SQ YD	37889	37889	
X4401198	HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	37889	37889	
* X7830060	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	130	130	
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	38262	38262	
* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	36	36	

*= SPECIALTY ITEM

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		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-419RS-418RS-5	FULTON	30	6
RTE. US 24/IL 100		CONTRACT NO. 68953		

ILLINOIS FED. AID PROJECT

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTRUCTION CODE	
				0006	0006
				FULTON CO. 80% FED 20% STATE	FULTON CO. 100% STATE
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	52	52	
XZ013798	CONSTRUCTION STATION LAYOUT	L SUM	1	1	
Z0034105	MATERIAL TRANSFER DEVICE	TON	7588	7588	
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1	

*= SPECIALTY ITEM

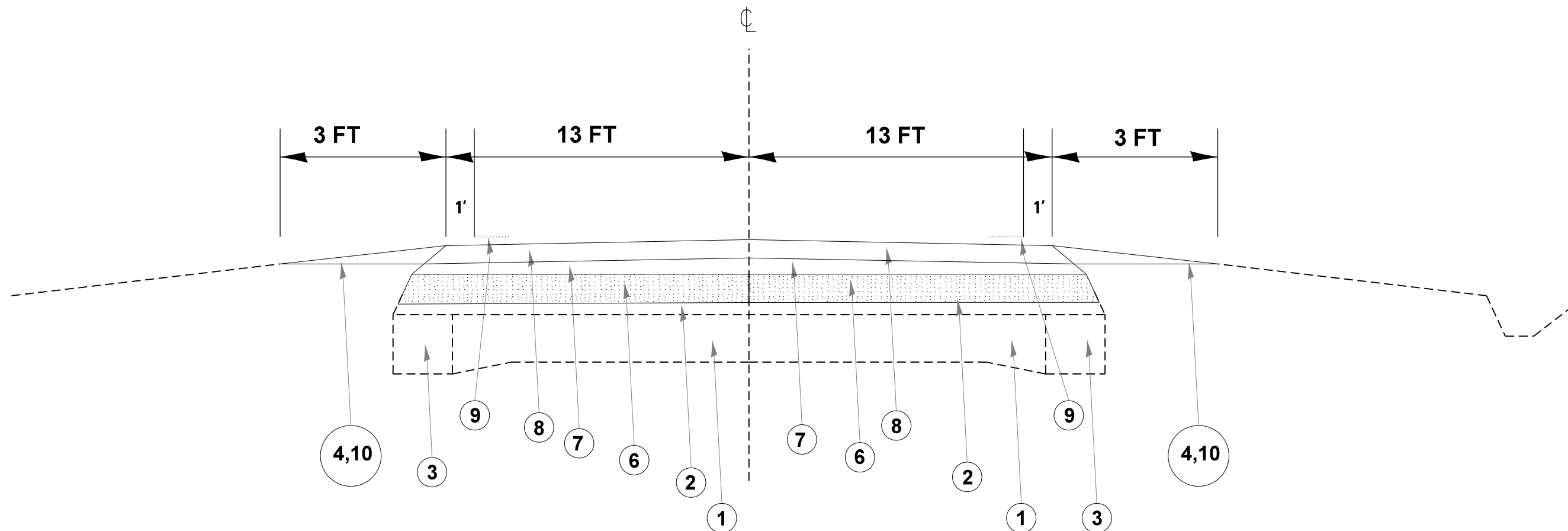
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	PLOT DATE = 1/31/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

E.A.P	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/	2ORS-4+19RS-4+18RS-5	FULTON	30	7
630	RTE. US 24/IL 100	CONTRACT NO. 68953		

ILLINOIS FED. AID PROJECT



Typical Section # 1
 US 24 / IL 100
 STA. 21+25 TO 149+93
 (OUT OF TOWN)

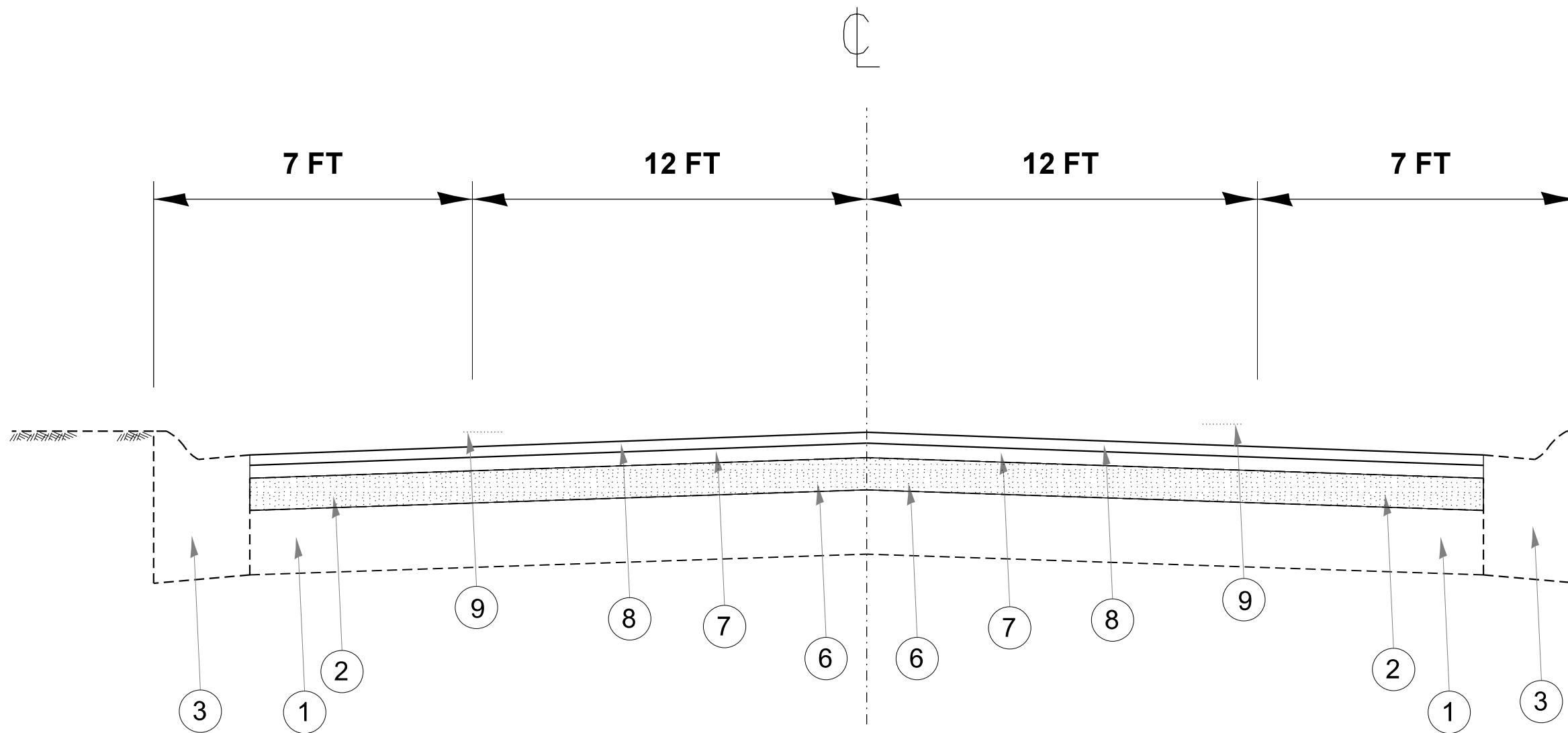
EXISTING

- 1. PCC PAVEMENT
- 2. HMA OVERLAY
- 3. WIDENING
- 4. AGGREGATE SHOULDER

PROPOSED

- 6. 4" COLD-IN-PLACE RECYCLING
- 7. HMA POLY-LEVEL BINDER COURSE 1" N50, IL 4.75,
- 8. HMA SURFACE COURSE 1 1/2" N50, IL 9.5, MIX D
- 9. MODIFIED URETHANE PAVEMENT MARKING
- 10. AGGREGATE SHOULDER TYPE B

FILE NAME =	USER NAME = hudsonme	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL SECTION # 1			F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -		RTE. US 24/IL 100			CONTRACT NO. 68953				
	PLOT DATE = 1/31/2020	DATE -	REVISED -		SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				



Typical Section # 2
US 24/IL 100
STA. 73+50 TO 97+00
(IN TOWN)

EXISTING

- 1. PCC Pavement
- 2. HMA Overlay
- 3. Concrete Curb & Gutter

PROPOSED

- 6. HMA SURFACE REMOVAL 3 3/4"
- 7. HMA BINDER COURSE 2.25" N50, IL 9.5
- 8. HMA SURFACE COURSE 1.5" N50, IL 9.5, MIX D
- 9. MODIFIED URETHANE PAVEMENT MARKING

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	PLOT DATE = 1/31/2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION # 2			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-2;19RS-4;18RS-5	FULTON	30	9
	RTE. US 24/IL 100		CONTRACT NO. 68953	
ILLINOIS FED. AID PROJECT				

STA. TO STA.			LENGTH	WIDTH	AREA	BUTT JOINT	TEMP RAMP	4" COLD-IN-PLACE RECYCLING	* CIR-FDR EMULSIFIED	HMA SURFACE REMOVAL VARIABLE DEPTH	HMA SURFACE REMOVAL (3.75")	POLY BIN. CSE. (MM) IL-4.75, N50 (1")	HMA SURF CSE, IL 9.5 MIX D, N50 (1.5")	BINDER COURSE (MM) IL 9.5, N50 (2.25")	TRACK/ LESS COAT	MAT. TRANSF DEVICE (MTD)	AGG SHLD TY. B	INCIDENTAL HMA SURF N=50, IL 9.5 MIX C
			FT	FT	SQ YD	SQ. YD.	SQ. YD.	SQ. YD	GALLON	SQ YD	SQ YD	TON	TON	TON	POUND	TON	TON	TON
US 24 & IL 100 (OUT-OF-TOWN LEWISTOWN)																		
21+25	TO	149+93	12,868	26	37,174			37,174	55,761			2,082	3,123		5948	5,204	2,282	
149+93	TO	150+23	35	26	101	101	14					6	8		16	14	6	
4" CIP, 1" Binder Taper & Length to Curb & Gutter Section			75	34	283	567	38					16	24		46	40		
Rail Road Crossing			35	39	152	303	43						13	19	24	32		
97+00	TO	96+70	35	39	152	152	22						13	19	24	32		
PROFILE CORRECTION																		
21+25	TO	149+93	12,868	26	37,174					37,174								
MILLING & PAVING 6" WIDER MAINLINE																		
21+25	TO	149+93	12,868	0.5	715			715				40	60		57	100		
*STATION EQUATION																		
AVE L TO AVE F (IN-TOWN)																		
73+50	TO	97+00	2,350	39	10,183					10,183		855	1,283	1630	2,139			
MILLING & PAVING 6" WIDER MAINLINE																		
73+50	TO	97+00	2,350	0.5	131					131		11	16	10	27			
SIDE ROADS RADIUS RETURN, ENTRANCES & MAILBOX TURNOUT					4,500	4,500	4,500								720			945
SUB-TOTAL						5,623	4,617	37,889	55,761	37,174	10,314	2,143	4,107	1,338	8475	7,588	2,288	945
GRAND TOTAL						5,623	4,617	37,889	55,761	37,174	10,314	2,143	4,107	1,338	8,475	7,588	2,288	945

* The quantity for the CIR-FDR Emulsified was calculated assuming it would be 4% by weight of the processed materials

Station Equation @ US 24 & IL 100 (AVE. L) 21+25=73+50

RAISED REFLECTIVE PAVEMENT MARKERS PLACEMENT				
RAISED REFLECTIVE PAVEMENT MARKERS REMOVAL				
US 24 & IL 100				
STA	TO	STA	EACH	EACH
21+25	TO	149+93	161	161
73+50	TO	97+00	59	59
TOTAL			220	220

LOCATION / LEWISTOWN US 24	CC&G Removal	CC&G Type B-6.24	Grading & Shaping Ditches	Inlet to be Reconstructed	LOCATION (LEWISTOWN)	Detectable Warning	Sidewalk Removal	PCC Sidewalk 4"	CC&G Removal	CC&G Type B-6.24	Pavement Removal	PCC PAVEMENT 13"
	FT	FT	FT	EACH		SQ.FT	SQ.FT	SQ.FT	FT	FT	SQ.YD	SQ.YD
LEFT					SE Corner Ave F	9	52.8	55	32	32	3.8	3.8
AVE F - J												
Across from Sycamore	7	7										
House next to Casey's	34	34										
Liquir store next to Casey's				1								
Country Side Restaurant	134	134										
House next to Country Side Restaurant	50	50										
AVE J - K					NW Corner Ave J	11	110.2	129	10	10	1.5	1.5
Wash Zone												
AVE K - L					SE Corner Ave J	9	33.8	40.1	10	10	1.5	1.5
Fulton County Fair Ground												
RIGHT												
AVE L					SE Corner Ave K	9	39.8	47.3	6	6	1.3	1.3
S.E Corner Ave L												
AVE L - K					NW Corner Ave K	9	35	42.7	7	7	1.2	1.2
VFW Post 5001												
D.Q + Pekin Dempster					NE Corner Ave L	0	100.9	94.6	35	35	4.2	4.2
AVE J - F												
House # 999												
Entrance across County Side Rest.												
Entrance across Liquir Store												
Sycamore Rd												
Total					Total	47	373	409	100	100	13.5	13.5
Grand Total					Grand Total	47	373	409	100	100	13.5	13.5

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 DATE = 1/31/2020 DATE - REVISIONS -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITY

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-2;19RS-4;18RS-5	FULTON	30	11
	RTE. US 24/IL 100		CONTRACT NO. 68953	

ILLINOIS FED. AID PROJECT

MODIFIED URETHANE PAVEMENT MARKING								
LOCATION	LENTGH	LINE-4"	SKIP DASH 4"	CENTER LINE-4"	LINE-12" DIAGNOL	LINE 24"	LETTERS, SYMBOLS, SHARROW, & ARROWS	
		WHITE	YELLOW	YELLOW	YELLOW	WHITE	WHITE	
		FT	FT	FT	FT	FT	SQ. FT	
US 24 & IL 100 / LEWISTOWN								
21+25	TO	149+93	12,868	25,865				
21+25	TO	25+90	465		116	465		
25+90	TO	39+55	1,365		0	2,730		
39+55	TO	46+70	715		179	715		
46+70	TO	74+15	2,745		686	0		
74+15	TO	81+30	715		179	715		
81+30	TO	91+65	1,035		0	2,070		
91+65	TO	99+00	735		184	735		
99+00	TO	142+95	4,395		1,099	0		
142+95	TO	149+93	698		175	698		
STATION EQUATION BETWEEN STA. 24+00 & 75+00								
73+50	TO	97+00	2,350					
73+50	TO	75+70	220		55	220		
75+70	TO	90+70	1,500		375	0		
90+70	TO	93+80	310		78	310		
93+80	TO	94+40	60		0	240		
94+40	TO	95+50	110		0	0	36	
95+50	TO	97+00	150		75	300		
LETTERS AND SYMBLOLS								
90+70	TO	97+00					52	
90+70	TO	97+00						114
SHARED LANE SYMBOL BETWEEN AVE F & RAILROAD CROSSING (NB, & SB LANES)								
								16
SUB-TOTAL			25,865	3,200	9,198	36	52	130
GRAND TOTAL			38,262			36	52	130

STATION TO STATION			LENGTH	SHORT-TERM PVT MARK	SHORT-TERM PVT MARK REM
			FT	FT	FT
21+25	TO	150+23	12,898	5,804	5,804
STATION EQUATION @ 24+00 & 75+00					
73+50	TO	97+00	2,350	1,058	1,058
TOTAL				6,862	6,862

If tape does not adhere to the CIPR surface, Paint shall be used.

GROOVING FOR RECESSED PAVEMENT MARKING										
LOCATION (LEWISTOWN)	LENTGH	5"			13"	25"	LETTERS, SYMBOLS, SHARROWS, & ARROWS			
		FT	FT	FT	FT	FT	SQ. FT			
US 24 & IL 100 / LEWISTOWN										
21+25	TO	149+93	12,868	25,865						
21+25	TO	25+90	465		116	465				
25+90	TO	39+55	1,365		0	2,730				
39+55	TO	46+70	715		179	715				
46+70	TO	74+15	2,745		686	0				
74+15	TO	81+30	715		179	715				
81+30	TO	91+65	1,035		0	2,070				
91+65	TO	99+00	735		184	735				
99+00	TO	142+95	4,395		1,099	0				
142+95	TO	149+93	698		175	698				
STATION EQUATION BETWEEN STA. 24+00 & 75+00										
73+50	TO	97+00	2,350							
73+50	TO	75+70	220		55	220				
75+70	TO	90+70	1,500		375	0				
90+70	TO	93+80	310		78	310				
93+80	TO	94+40	60		0	240				
94+40	TO	95+50	110		0	0	36			
95+50	TO	97+00	150		75	300				
LETTERS AND SYMBLOLS										
90+70	TO	97+00							52	
90+70	TO	97+00								114
SHARED LANE SYMBOL BETWEEN AVE F & RAILROAD CROSSING (NB, & SB LANES)										
										16
SUB-TOTAL			25,865	3,200	9,198	36	52	130		
GRAND TOTAL			38,262			36	52	130		

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	PLOT DATE = 1/31/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITY

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-2;19RS-4;18RS-5	FULTON	30	12
	RTE. US 24/IL 100	CONTRACT NO.	68953	
ILLINOIS FED. AID PROJECT				

CONSTRUCTING TEST STRIP	EACH	Box Culvert End Sect. CULVERT NO. 1	
SURFACE COURSE	1	LOCATION	EACH
TOTAL	1	STA. 30+00	1.0
		TOTAL	1.0
ENGINEER'S FIELD OFFICE TYPE A	CAL MO	STONE DUMP RIPRAP, CLASS B3 (28100825)	
JOB SITE	4	LOCATION	TON
TOTAL	4	STA. 30+00 (Around headwall section)	16.0
		STA. 36+00 (around inlet)	15.0
MOBILIZATION	L SUM	TOTAL	31.0
JOB SITE	1	EROSION CONTROL BLANKET (25100630)	
TOTAL	1	LOCATION	SQ.YD.
		JOB SITE	250
TRAF CONT & PROT STD 701306	L. SUM	TOTAL	250
JOB SITE	1	GRADING & SHAPING DITCHES (SOUTH BOUND) (21400100)	
TOTAL	1	LOCATION	FT
		STA. 30+00 (Around headwall section)	50
CONSTRUCTION STATION LAYOUT	L. SUM	STA. 36+00 (around inlet)	65
JOB SITE	1	TOTAL	115
TOTAL	1	MOWING (25000750)	
		LOCATION	ACRE
TRAF CONT & PROT STD 701201	L. SUM	JOB SITE	8.70
JOB SITE	1	TOTAL	8.70
TOTAL	1	CHANGEABLE MESSAGE SIGN	
		CAL. DAY	
3 SIGNS 14 CALENDER DAYS EACH	42		
2 Signs @ North End of Project, (Ave E & US 24) 1 Sign @ South End of Project			
TOTAL	42		
		PERMANENT SURVEY MARKERS TYPE I	
	EACH		
Sta. 149+98 & 150+09	2		
TOTAL	2		
		TRAF CONT & PROT STD 701801	
	L. SUM		
JOB SITE	1		
TOTAL	1		
		CONSTRUCTION LAYOUT	
	L. SUM		
JOB SITE	1		
TOTAL	1		

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Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 1/31/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITY

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-2;19RS-4;18RS-5	FULTON	30	13
	RTE. US 24/IL 100	CONTRACT NO. 68953		
ILLINOIS FED. AID PROJECT				

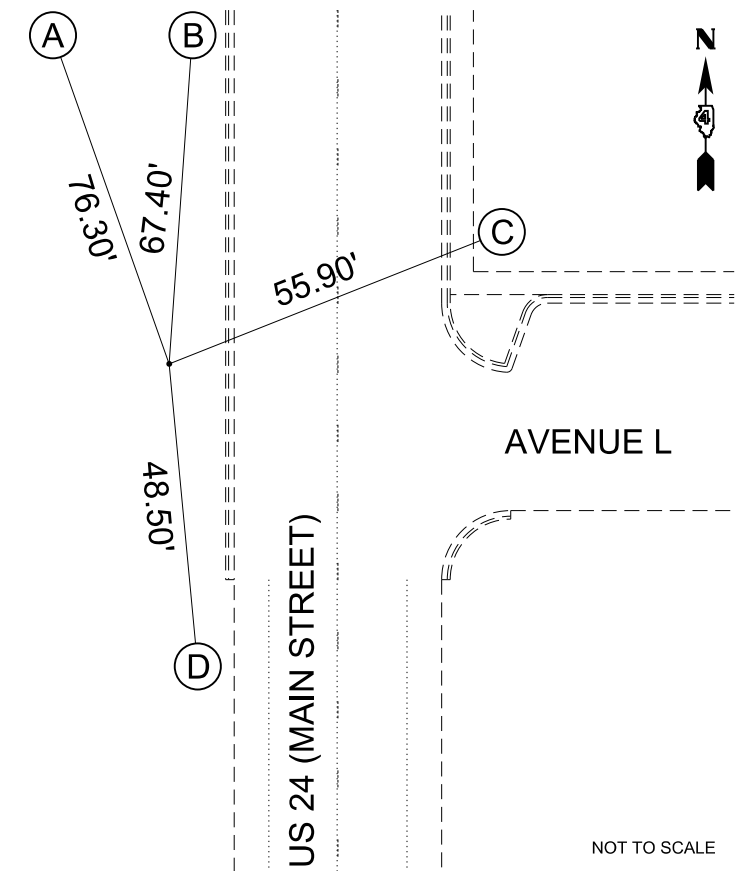
SURVEY BOOK # 3012

COMB FACT 0.999922

US 24 (MAIN STREET) AND AVENUE L

TIES TO POINT 4613
SPIRE IN VAULT
N 1354616.587
E 2299942.046
ELEV. 584.240

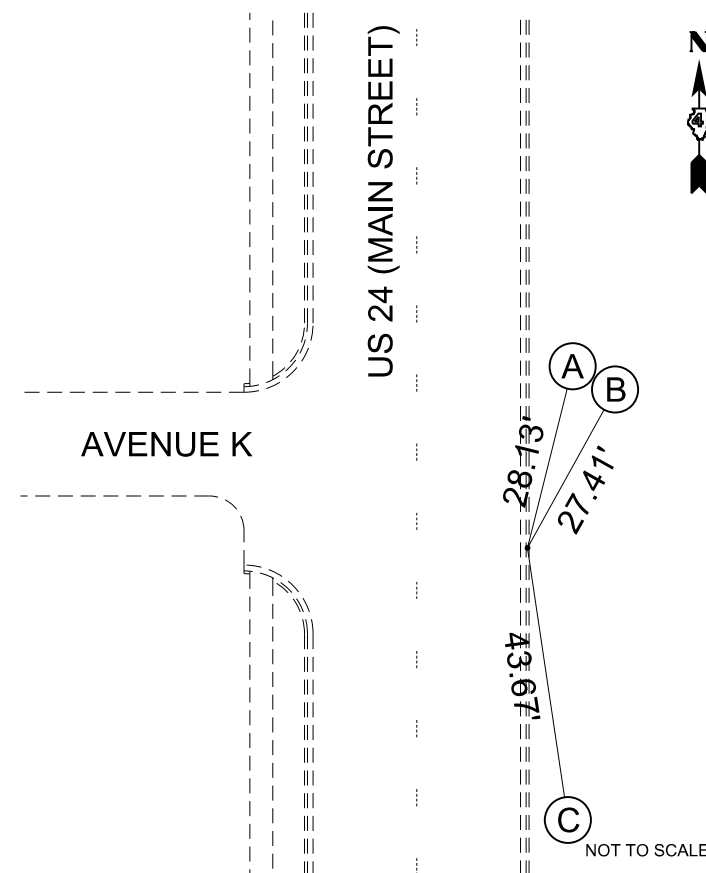
POINT A
FACE OF LIGHT POLE FOUNDATION
POINT B
NAIL IN POWER POLE
POINT C
NAIL IN POWER POLE
POINT D
NAIL IN POWER POLE



US 24 (MAIN STREET) AND AVENUE K

TIES TO POINT 11
CHISELED "X" ON TOP OF CURB
N 1355010.380
E 2299807.529
ELEV. 584.535

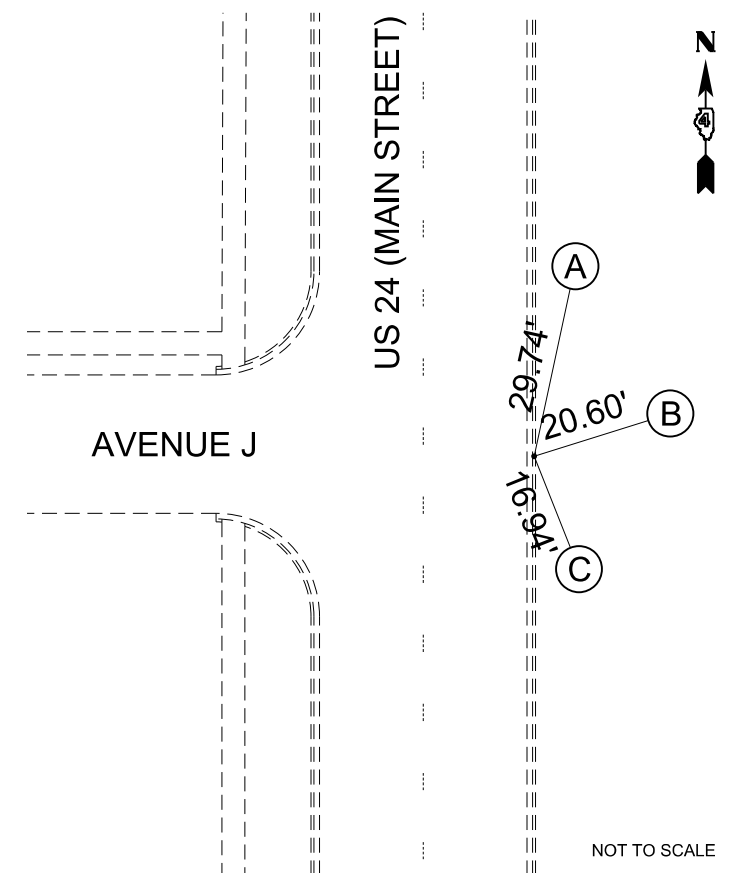
POINT A
CENTER OF POWER POLE
POINT B
SOUTHWEST CORNER OF SIGN
FOUNDATION
POINT C
CENTER OF POWER POLE



US 24 (MAIN STREET) AND AVENUE J

TIES TO POINT 12
CHISELED "X" ON TOP OF CURB
N 1355310.967
E 2299806.014
ELEV. 584.283

POINT A
NAIL IN POWER POLE
POINT B
CENTER OF 12 INCH MAPLE TREE
POINT C
NORTHWEST CORNER OF 2 FOOT
SIDEWALK



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BENCHMARKS & TIES

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PLOT DATE = 1/31/2020	DATE -	REVISED -

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

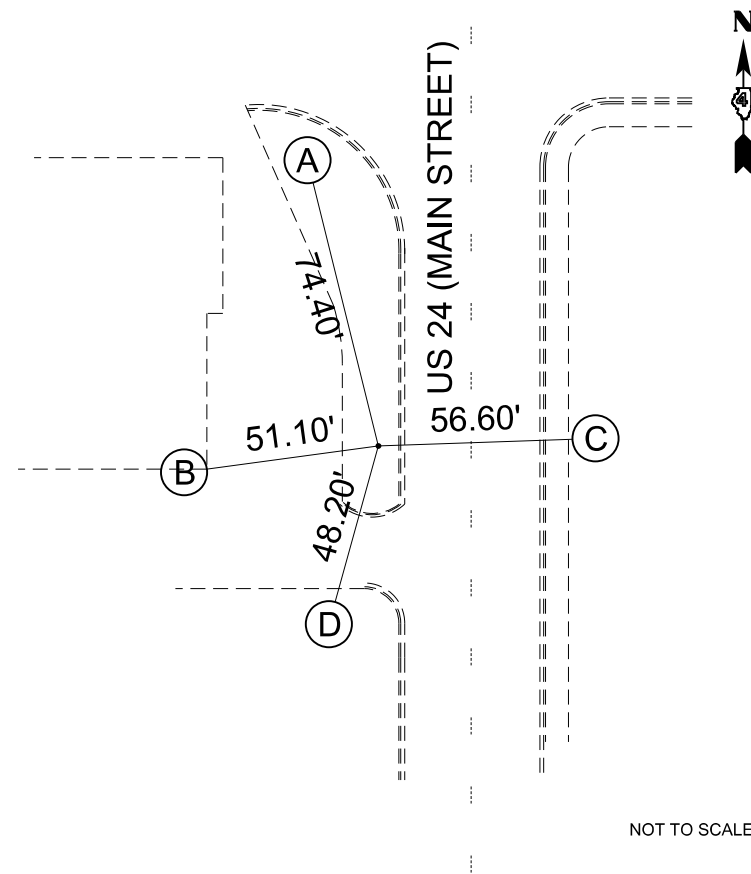
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317	20RS-2;19RS-4;18RS-5	FULTON	30	14
CONTRACT NO. 68953				
ILLINOIS FED. AID PROJECT				

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US 24 (MAIN STREET) AND AVENUE E - USE FOR AVE. F

TIES TO POINT 19
 CHISELED "+" IN PCC CURB
 N 1357050.527 (GROUND)
 E 2299935.210 (GROUND)
 ELEV. 588.908

POINT A
 FACE OF SIGNAL FOUNDATION
 POINT B
 SOUTHEAST CORNER OF BRICK
 FOUNDATION
 POINT C
 FACE OF PCC LIGHT FOUNDATION



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PLOT DATE = 1/31/2020	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BENCHMARKS & TIES

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	20R5-2;19R5-4;18R5-5	FULTON	30	15
CONTRACT NO. 68953			ILLINOIS FED. AID PROJECT	

OUT OF TOWN
4" CIP

IN TOWN
CURB & GUTTER SECTION
3.75" HMA MILLING

1:240 TAPER
AT 75 LENGTH

HMA SURF. CSE IL 9.5
MIX D, N50 (1.5")

POLY. BINDER CSE (1")
IL 4.75, N50

PROPOSED
4" CIP

EX. HMA
OVERLAY

EX. PCC
PAVEMENT

HMA SURF. CSE IL 9.5
MIX D, N50 (1.5")

HMA BINDER CSE
IL 9.5, N50 (2.25")

EX. HMA
OVERLAY

EX. PCC
PAVEMENT

STATION EQUATION
STA. 21 + 25 = STA. 73 + 50
AT AVE. L

NOT TO SCALE

USER NAME = hudelsonme

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PLOT SCALE = 100.0000' / in.

DATE -

REVISED -

PLOT DATE = 1/31/2020

DATE -

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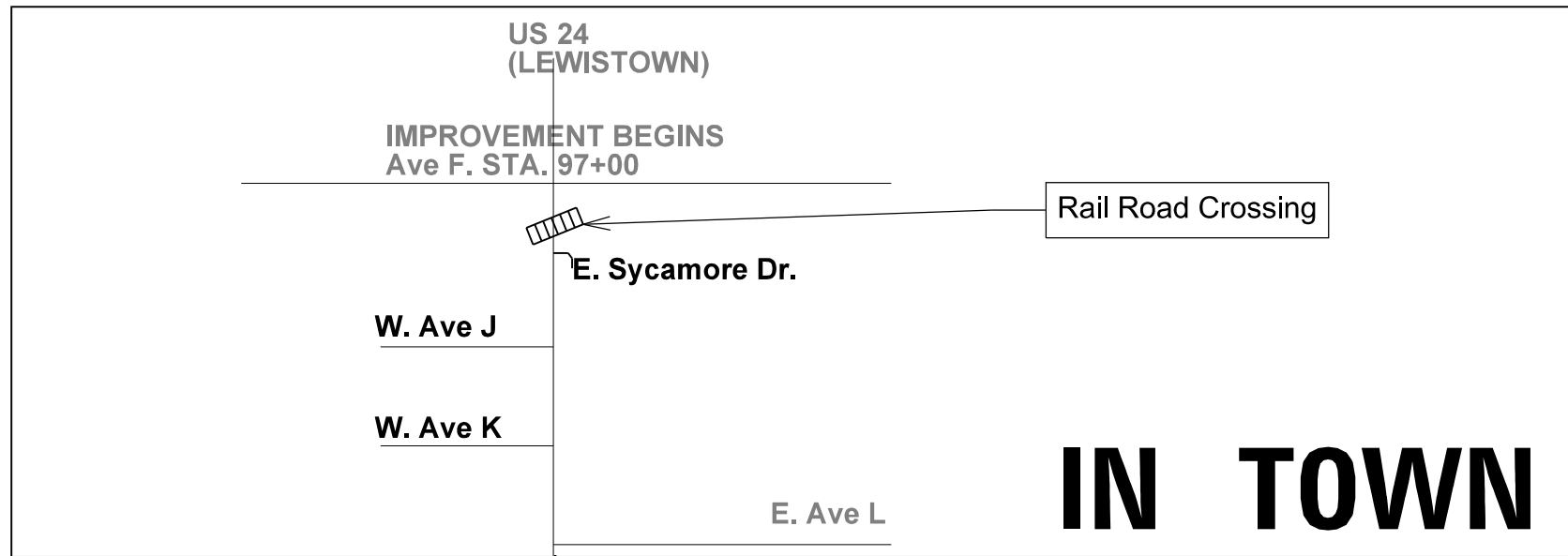
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CIP & MILLING DETAIL

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-2;19RS-4;18RS-5	FULTON	30	16
RTE. US 24/IL 100		CONTRACT NO. 68953		

ILLINOIS FED. AID PROJECT



CULVERT @ STATION 30+00 RT
 (WEST SIDE OF ROAD)
 GRADING AND SHAPING DITCH = 50 FT
 STONE DUMPED RIP RAP = 16.0 TON
 EROSION CONTROL BLANKET

STA. EQUATION @ SOUTH SIDE OF AVE L.
 73+50 = 21+25 (SEE NOTE 1)

CULVERT @ STATION 36+00 RT
 (WEST SIDE OF ROAD)
 GRADING AND SHAPING DITCH = 65 FT
 STONE DUMPED RIP RAP = 15.0 TON
 EROSION CONTROL BLANKET

US 24
 IL 100

IMPROVEMENT ENDS
 STA. 149+93
 (N. of Rut Rd.)

NOTE 1: STATIONING INCREASES IN BOTH DIRECTIONS FROM THE STATION EQUATION:
 STATIONING INCREASES TO THE NORTH FROM THE STATION EQUATION (73+50) TO START OF PROJECT (97+00).
 STATIONING INCREASES TO THE SOUTH FROM THE STATION EQUATION (21+25) TO THE END OF THE PROJECT (149+93).

NOT TO SCALE

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

LINE DIAGRAM			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-2;19RS-4;18RS-5	FULTON	30	17
	RTE. US 24/IL 100		CONTRACT NO. 68953	
ILLINOIS FED. AID PROJECT				

NOTES FOR ADA RAMP CONSTRUCTION (REMOVAL)

1. CURB AND GUTTER REMOVAL SHALL REQUIRE FULL DEPTH SAWCUTS. THE SAWCUTS SHALL BE MADE 1 FT BEYOND THE EDGE OF THE CURB AND GUTTER TO ALLOW FOR PLACEMENT OF FORMS.
2. PAVEMENT REMOVAL SHALL REQUIRE FULL DEPTH SAWCUTS. WHERE PAVEMENT REMOVAL IS REQUIRED FOR INSTALLATION OF CURB AND/OR GUTTER THE SAWCUTS SHALL BE MADE 1 FT BEYOND THE EDGE OF THE NEW CURB AND/OR GUTTER TO ALLOW FOR PLACEMENT OF FORMS.
3. SAWCUTTING THE SIDEWALK IS INCLUDED IN THE COST OF SIDEWALK REMOVAL.
4. RADIAL DIMENSIONS SHOWN ARE MEASURED ALONG FACE OF CURB FOR CURB & GUTTER REMOVAL.

NOTES FOR ADA RAMP CONSTRUCTION (PROPOSED)

1. SIDEWALK CROSS SLOPE = 1.5% (2.0% MAX.)
2. UPPER LANDINGS AND LOWER LANDINGS SHALL BE 1.5% MAXIMUM IN ANY DIRECTION; INCLUDING DIAGONALLY
3. MAXIMUM RAMP LONGITUDINAL SLOPE = 8.3% (1:12)
4. MAXIMUM SLOPE AROUND RADIUS = 2%
5. FLARES ARE 1.5' UNLESS OTHERWISE NOTED.
6. CONTRACTOR TO UTILIZE RADIAL DETECTABLE WARNING PANELS AROUND RADIUS. CONTRACTOR IS RESPONSIBLE FOR MEASURING THE RADIUS PRIOR TO ORDERING PANELS.
7. CONTRACTOR SHALL VERIFY REMOVAL LIMITS AND FIELD ELEVATIONS TO ENSURE THAT ADA STANDARDS ARE MET.
8. PCC BASE COURSE, 13" TO BE USED FOR 1 FT WIDE STRIP BEYOND THE EDGE OF THE NEW CURB AND GUTTER (SEE SCHEDULE FOR QUANTITY AND LOCATION).
9. SAWCUTTING THE SIDEWALK IS INCLUDED IN THE COST OF SIDEWALK REMOVAL.
10. ANY EXCAVATION NECESSARY FOR THE PROPOSED SIDEWALK IS INCLUDED IN THE COST OF PCC SIDEWALK, 4".

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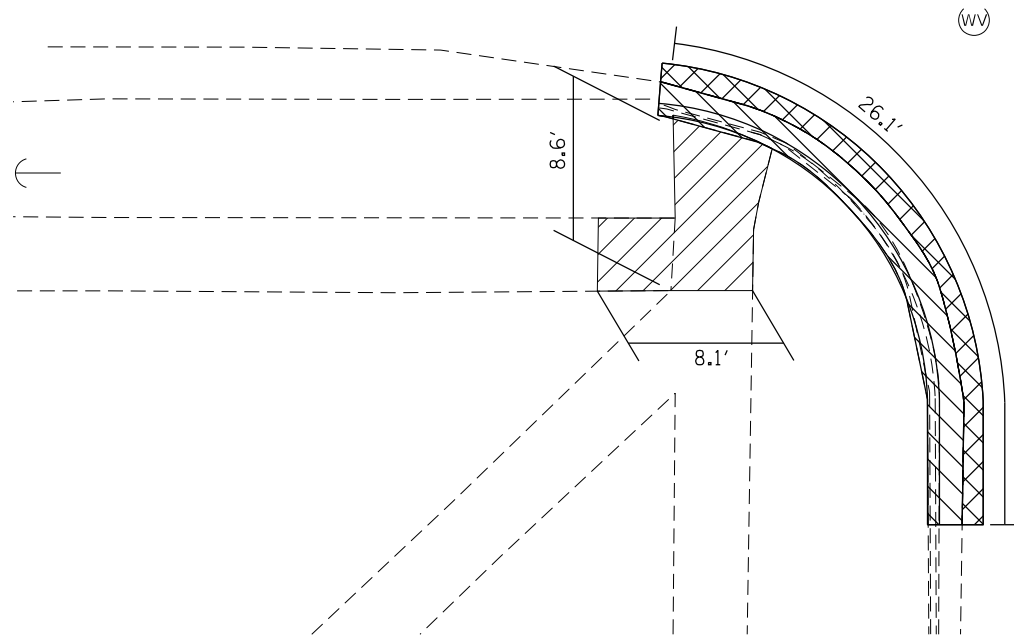
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ADA RAMP CONSTRUCTION NOTES

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

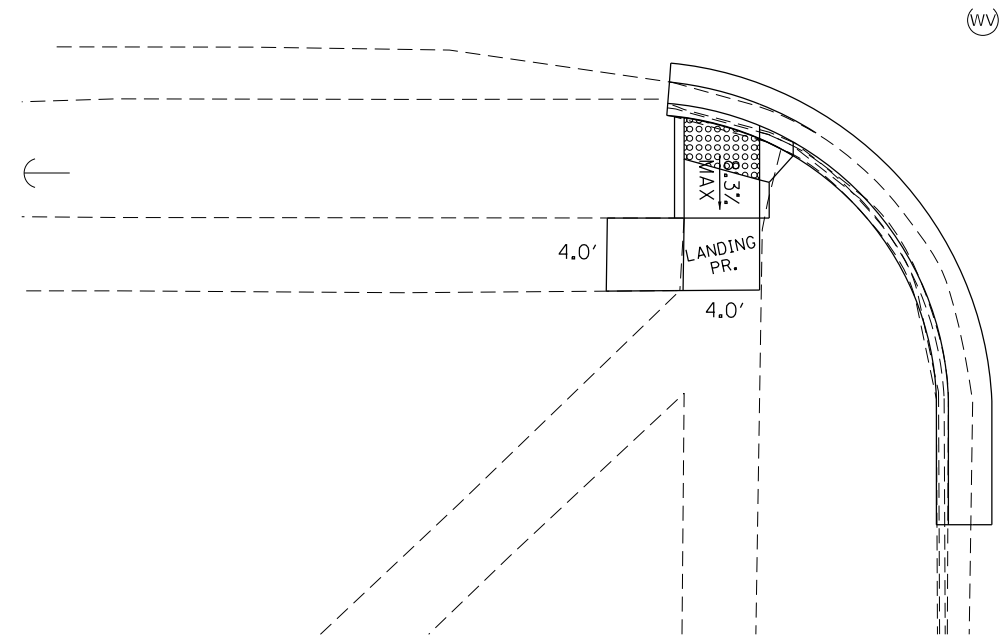
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317	20RS-2;19RS-4;18RS-5	FULTON	30	18
			CONTRACT NO. 68953	
		ILLINOIS	FED. AID PROJECT	

AVE F



US 24 (MAIN ST)

AVE F



US 24 (MAIN ST)

REMOVAL LEGEND



CURB & GUTTER REMOVAL



SIDEWALK /SIDE CURB REMOVAL



PAVEMENT REMOVAL

PROPOSED LEGEND



DETECTABLE WARNING

LANDING PR. = 1.5% MAX SLOPE EACH DIRECTION

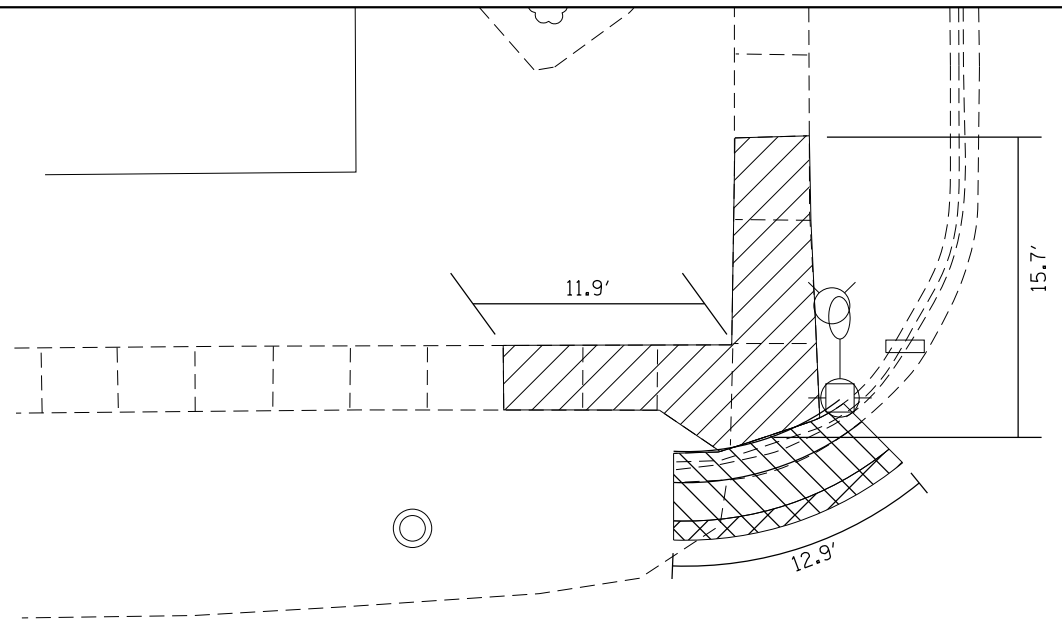
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP CONSTRUCTION
US 24 /AVE. F**

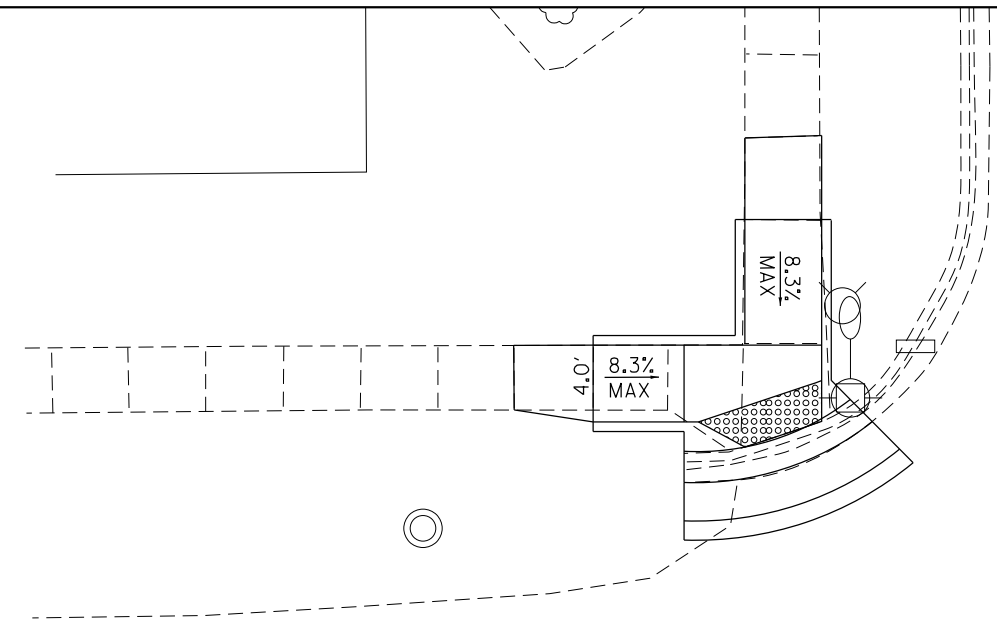
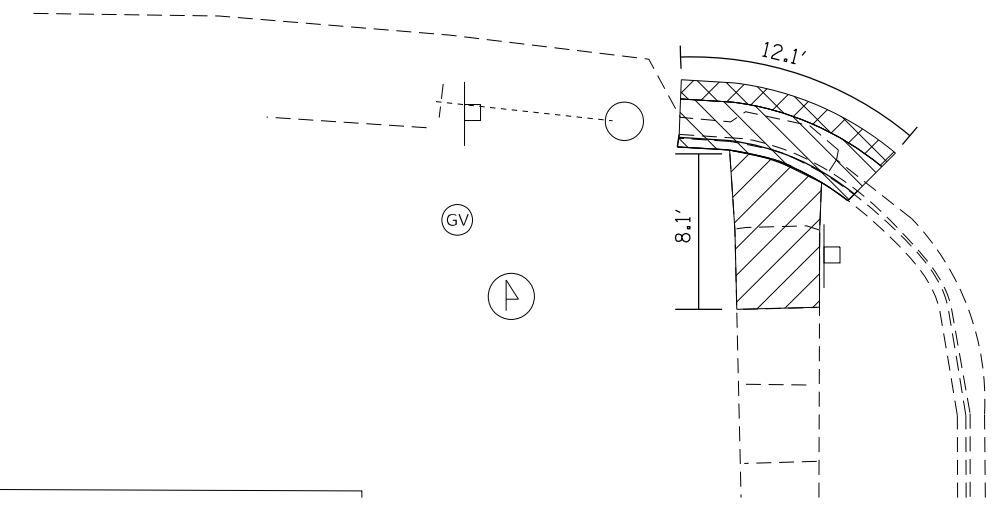
SCALE: SHEET OF SHEETS STA. TO STA.

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317/630	20RS-2;19RS-4;18RS-5	FULTON	30	19
	RTE. US 24/IL 100		CONTRACT NO. 68953	
ILLINOIS FED. AID PROJECT				



AVE J

US 24 (MAIN ST)



AVE J

US 24 (MAIN ST)



LEGEND



CURB & GUTTER REMOVAL



SIDEWALK /SIDE CURB REMOVAL



PAVEMENT REMOVAL

PROPOSED LEGEND



DETECTABLE WARNING

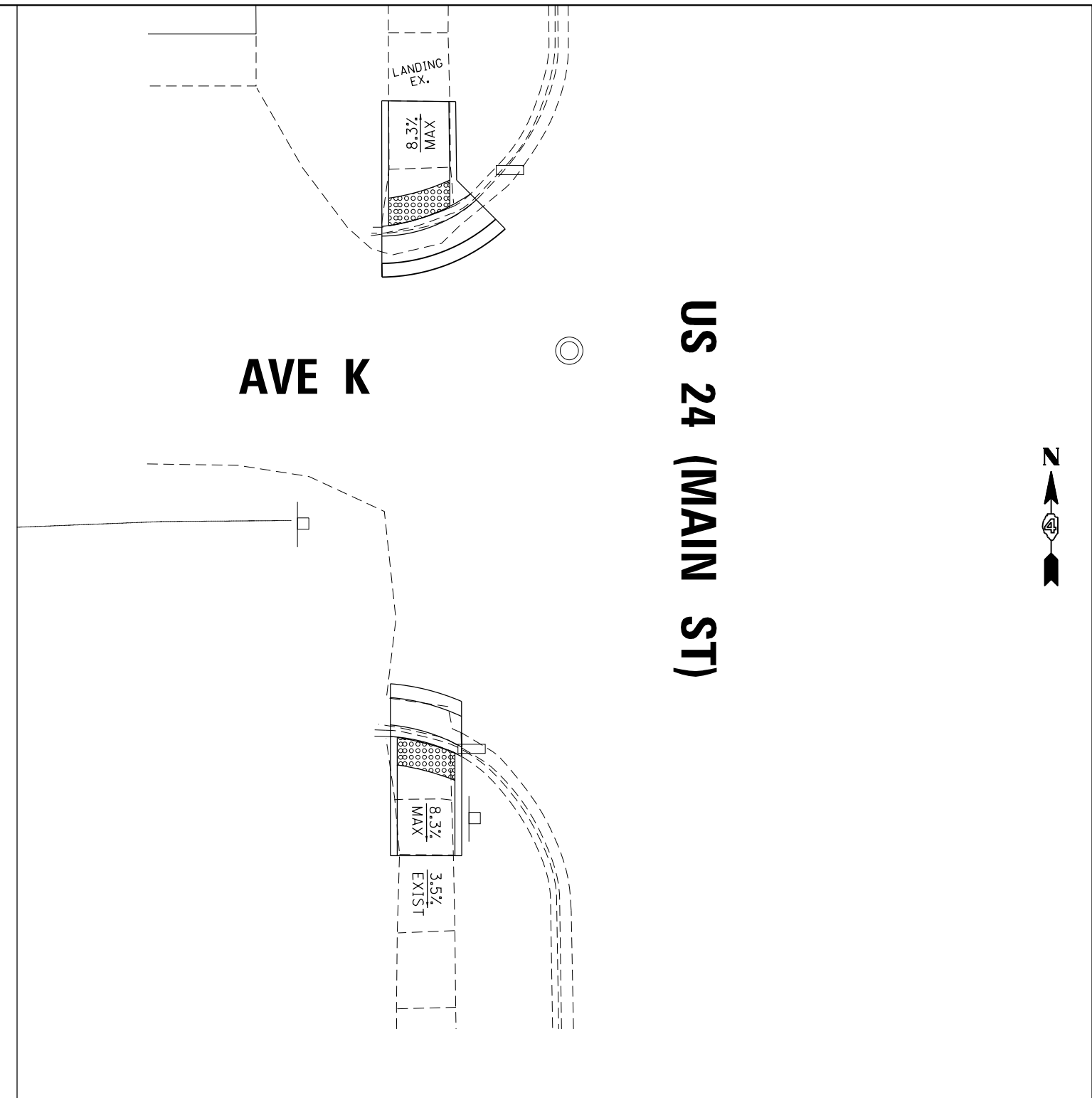
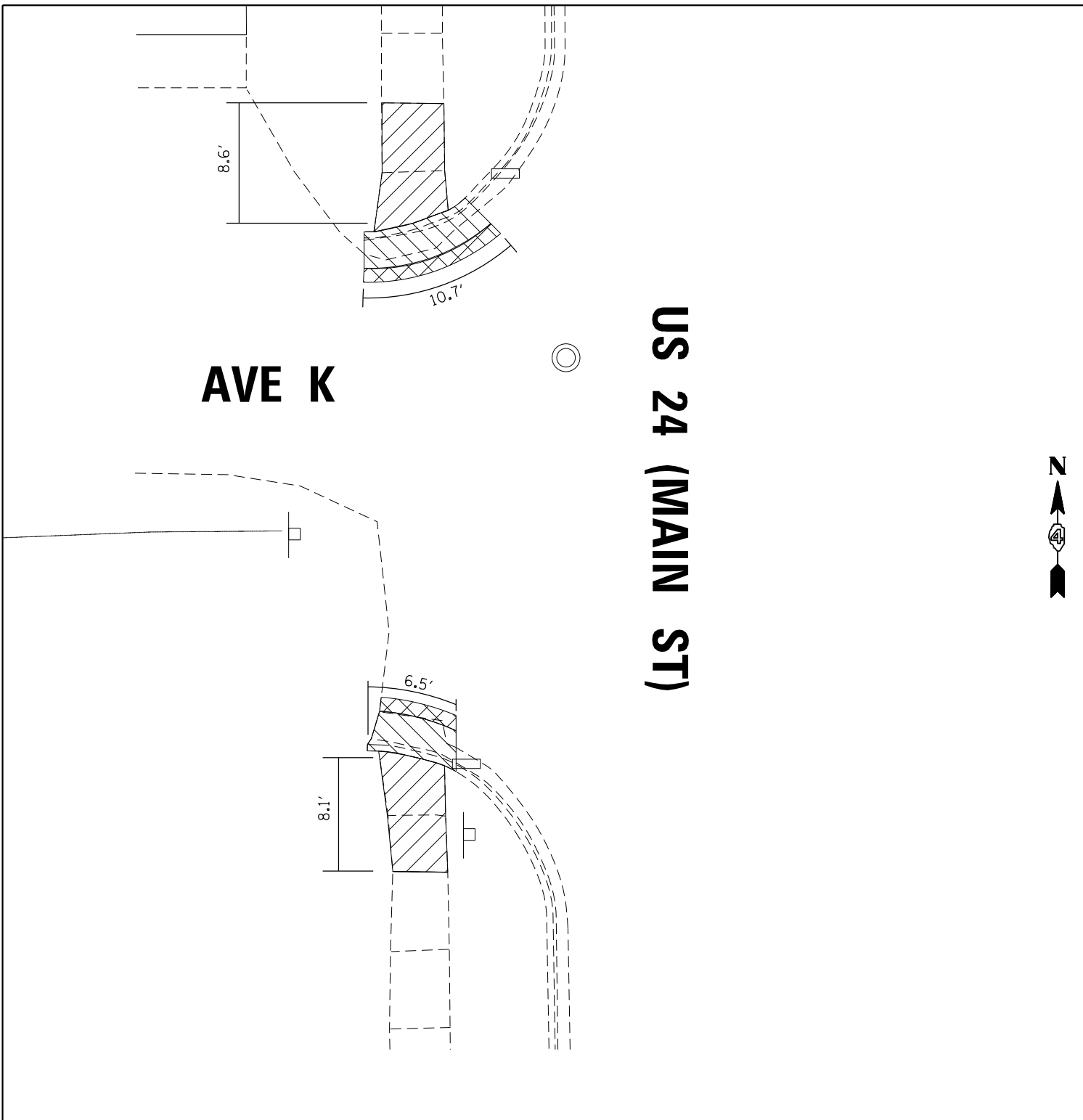
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP CONSTRUCTION
US 24 /AVE. J**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-2;19RS-4;18RS-5	FULTON	30	20
	RTE. US 24/IL 100		CONTRACT NO. 68953	
ILLINOIS FED. AID PROJECT				



LEGEND



CURB & GUTTER REMOVAL



SIDEWALK /SIDE CURB REMOVAL



PAVEMENT REMOVAL

PROPOSED LEGEND



DETECTABLE WARNING

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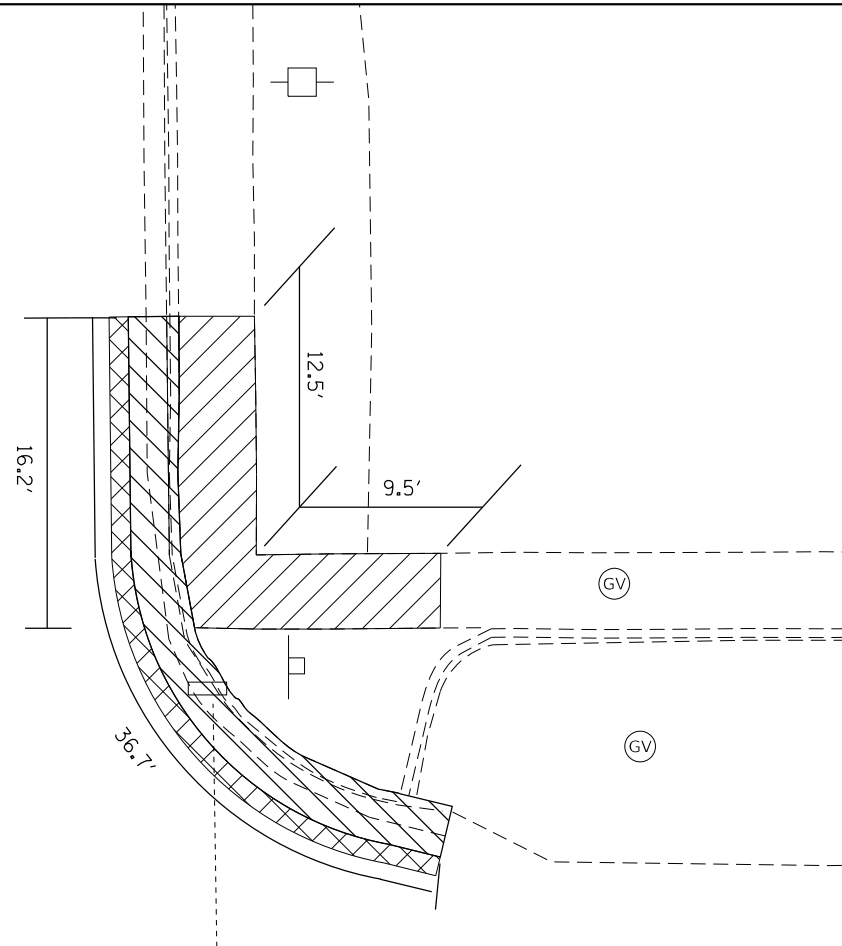
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP CONSTRUCTION
US 24 / AVE. K**

SCALE: SHEET OF SHEETS STA. TO STA.

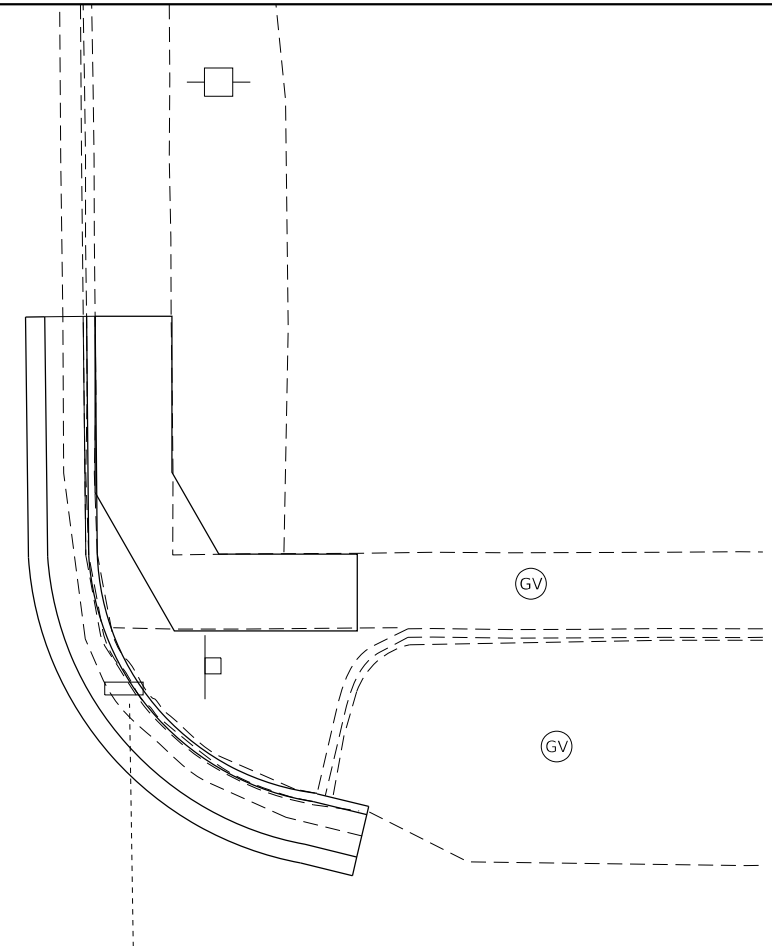
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	RTE. US 24/IL 100		CONTRACT NO. 68953	
ILLINOIS FED. AID PROJECT				

US 24 (MAIN ST)



AVE L

US 24 (MAIN ST)



AVE L

LEGEND



CURB & GUTTER REMOVAL



SIDEWALK /SIDE CURB REMOVAL



PAVEMENT REMOVAL

PROPOSED LEGEND



DETECTABLE WARNING

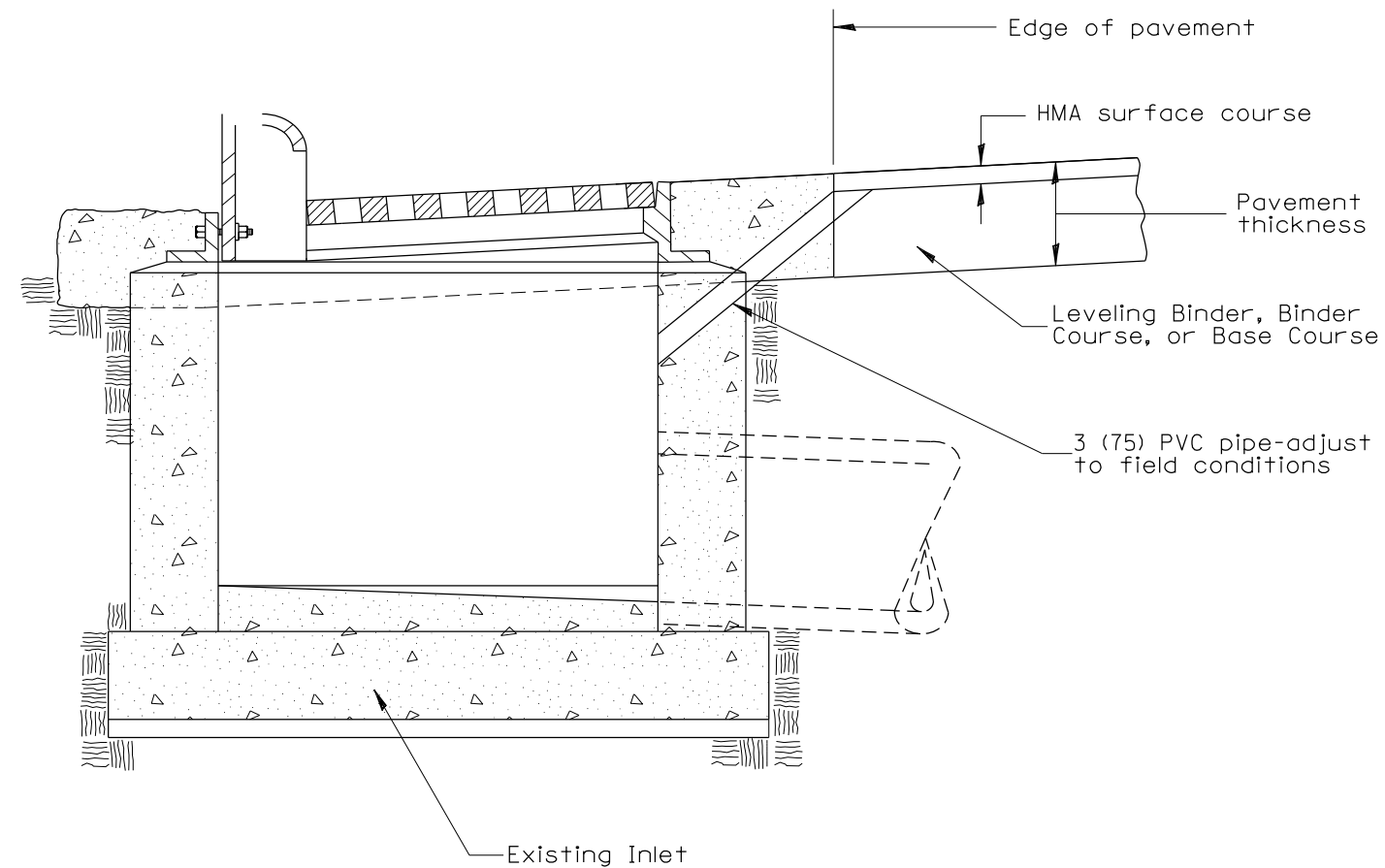
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP CONSTRUCTION
US 24 / AVE. L**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317/630	20RS-2;19RS-4;18RS-5	FULTON	30	22
	RTE. US 24/IL 100		CONTRACT NO. 68953	
ILLINOIS FED. AID PROJECT				



GENERAL NOTES:

1. Temporary Drainage - This work consists of furnishing and installing a 3 (75) PVC pipe in the existing inlets to facilitate drainage off the pavement prior to the placement of HMA surface course.
2. Adequate pavement drainage shall be maintained during all phases of construction on milled and leveling binder surfaces.
3. The PVC pipe is to be plugged at both ends with non-shrink hydraulic cement prior to surface course placement.
4. This detail applies to the referenced locations. However, the Engineer reserves the right to move or add locations at his or her discretion. Additional locations treated in excess of the quantity shown will be paid for in accordance with Article 109.04.

LOCATIONS:

- | | |
|-----------------------|-----------------------------|
| 1. NB N. OF RR TRACKS | 10. SB N. SIDE OF RR TRACKS |
| 2. NB N. OF RR TRACKS | 11. SB N. SIDE OF RR TRACKS |
| 3. NB S. OF RR TRACKS | 12. SB S. SIDE OF RR TRACKS |
| 4. NB S. OF RR TRACKS | 13. SB S. SIDE OF RR TRACKS |
| 5. NB ACROSS FROM #14 | 14. SB S. OF CASEYS |
| 6. NB ACROSS FROM #15 | 15. SB S. OF RESTAURANT |
| 7. NB ACROSS FROM #16 | 16. SB N. SIDE OF AVE. "J" |
| 8. NB ACROSS FROM #17 | 17. SB N. SIDE OF AVE. "K" |
| 9. NB N SIDE AVE. "L" | 18. SB S. SIDE OF AVE. "K" |

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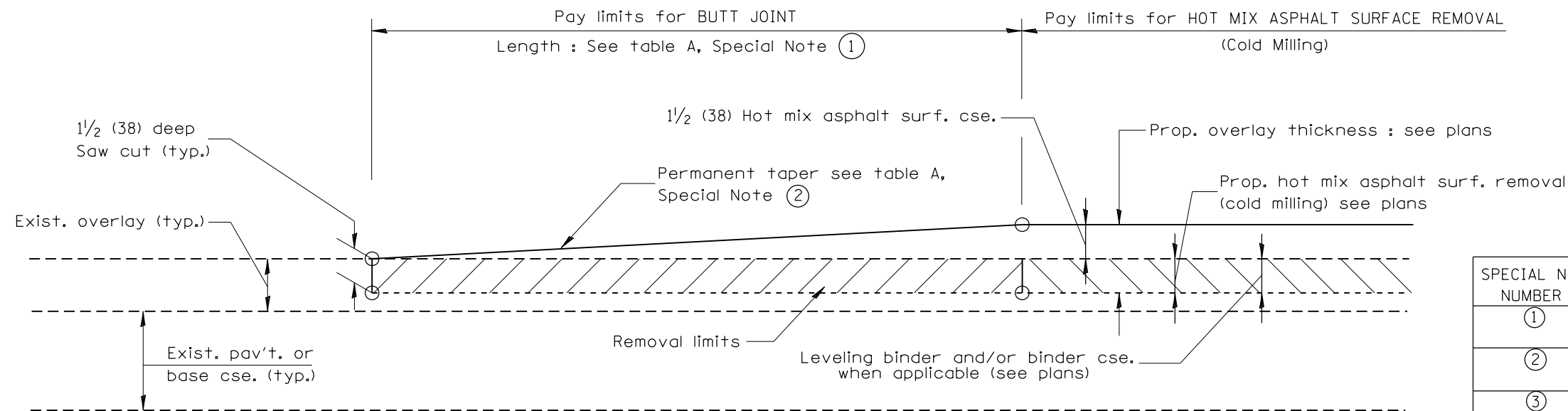
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/31/2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TEMPORARY INLET DRAINAGE TREATMENT

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	20RS-2;19RS-4;18RS-5	FULTON	30	24
CONTRACT NO. 68953				
ILLINOIS FED. AID PROJECT				



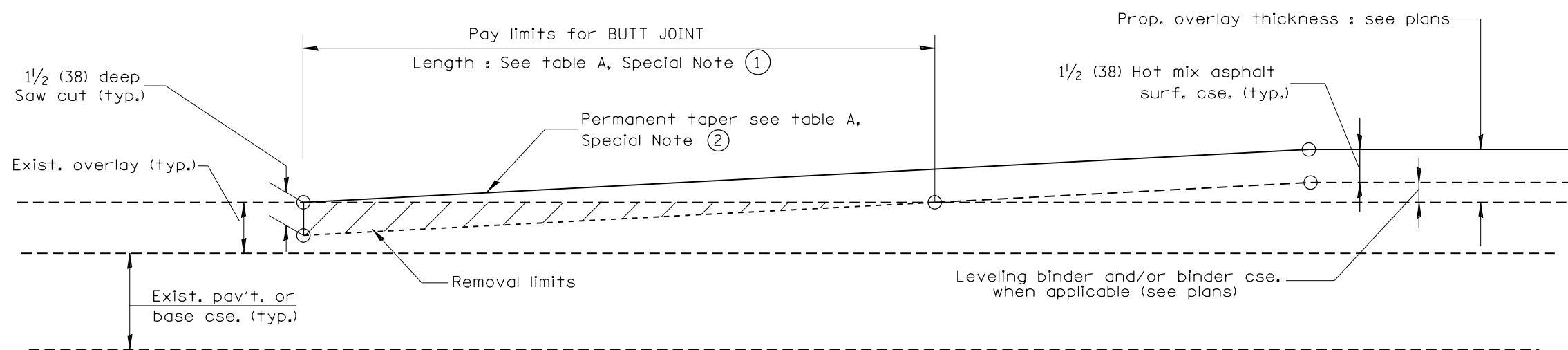
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

TABLE A
(LENGTHS AND TAPER RATES)

SPECIAL NOTE NUMBER	ELEMENT	MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS	ALL OTHERS
①	LENGTH OF BUTT JOINT	60'(18.0 m)	30'(9.0 m)
②	PERMANENT TAPER RATE	1:480	1:240
③	TEMPORARY RAMP TAPER RATE	1:80	1:40
④	TEMPORARY RAMP LENGTH	10'(3.0 m)	5'(1.5 m)
⑤	LENGTH OF BUTT JOINT	10'(3.0 m)	10'(3.0 m)

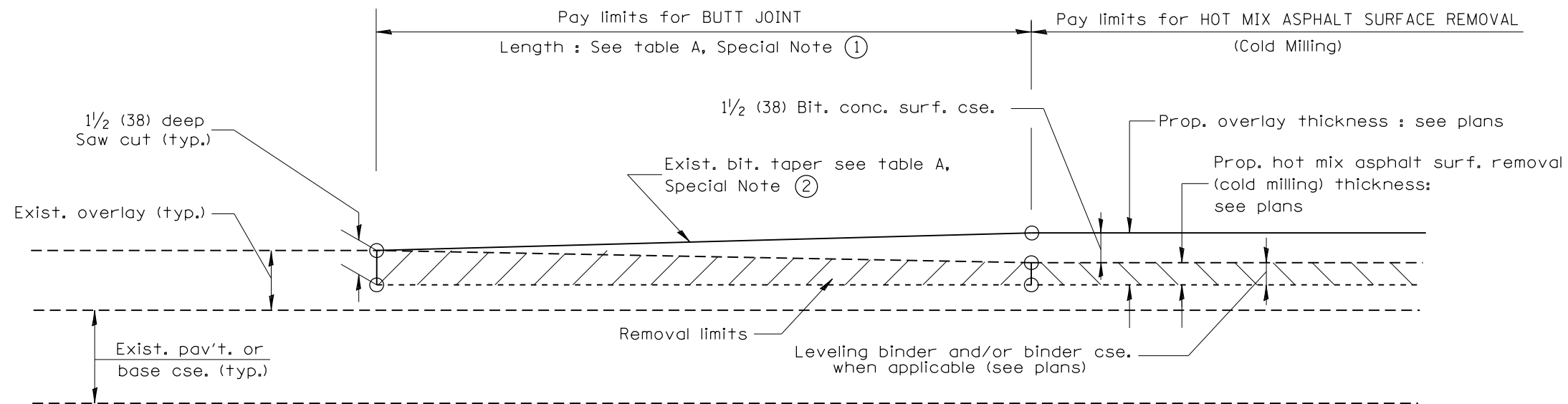
GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.

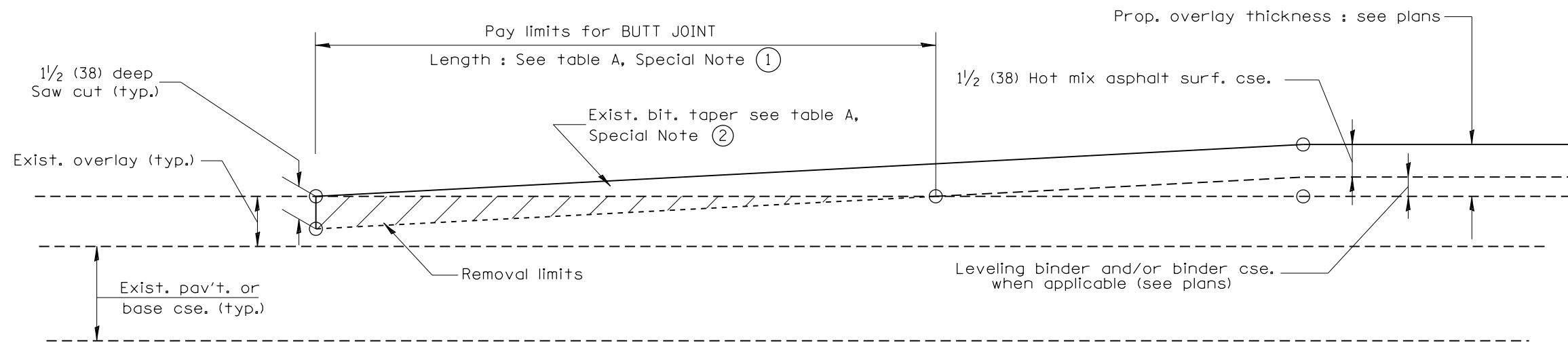


CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

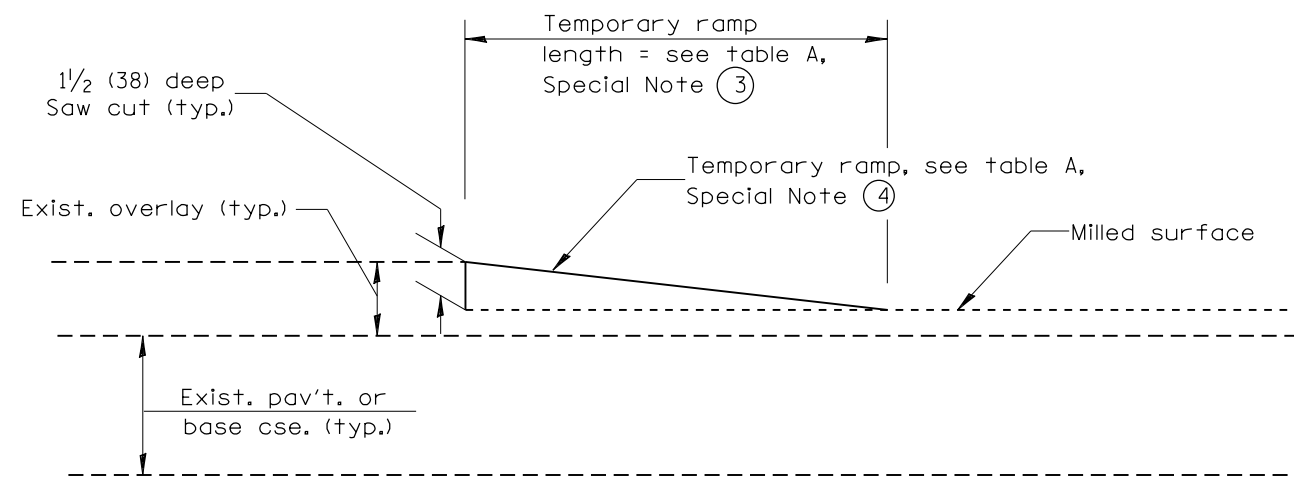
All dimensions are in inches (millimeters) unless otherwise noted.



**CASE 3 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER**



**CASE 4 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER**



DETAIL TEMPORARY RAMP

All dimensions are in inches (millimeters) unless otherwise noted.

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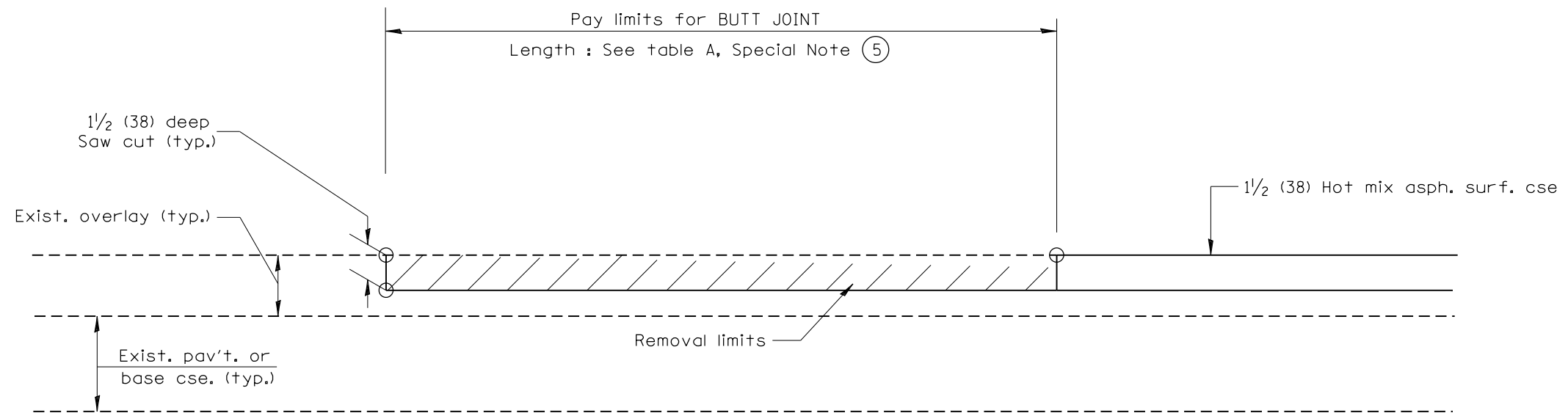
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINTS

NOT TO SCALE

SHT. 2 OF 3
CADD STD. 406101-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	20RS-2+19RS-4+18RS-5	FULTON	30	26
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68953	



CASE 5 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in inches (millimeters) unless otherwise noted.

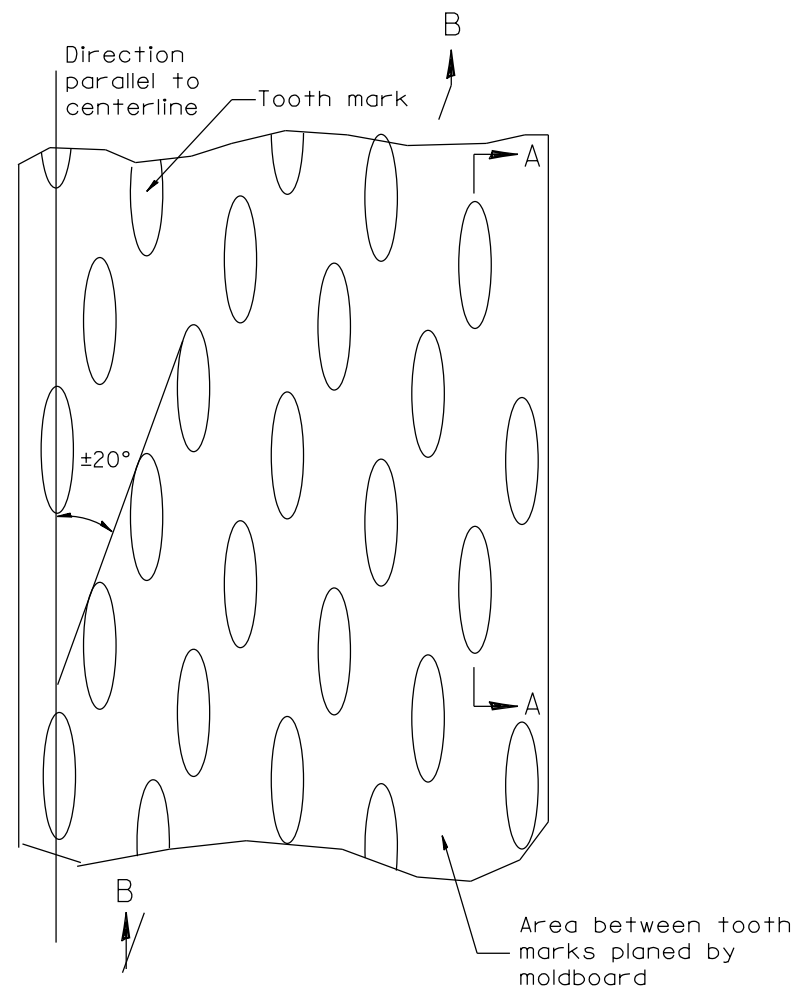
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

BUTT JOINTS

SHT. 3 OF 3
CADD STD. 406101-D4

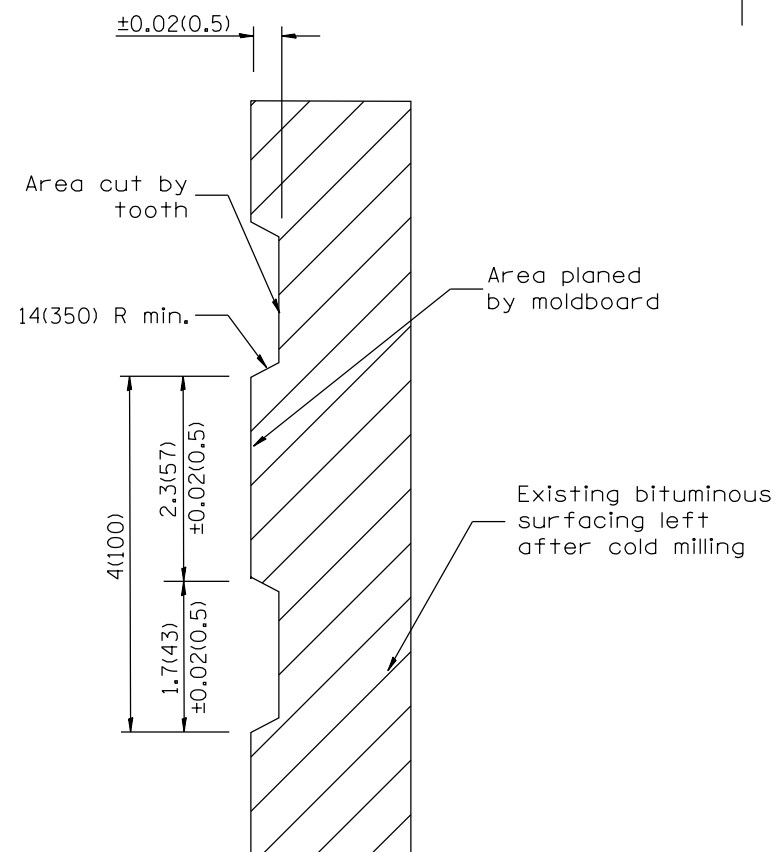
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	20RS-2+19RS-4+18RS-5	FULTON	30	27
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68953	



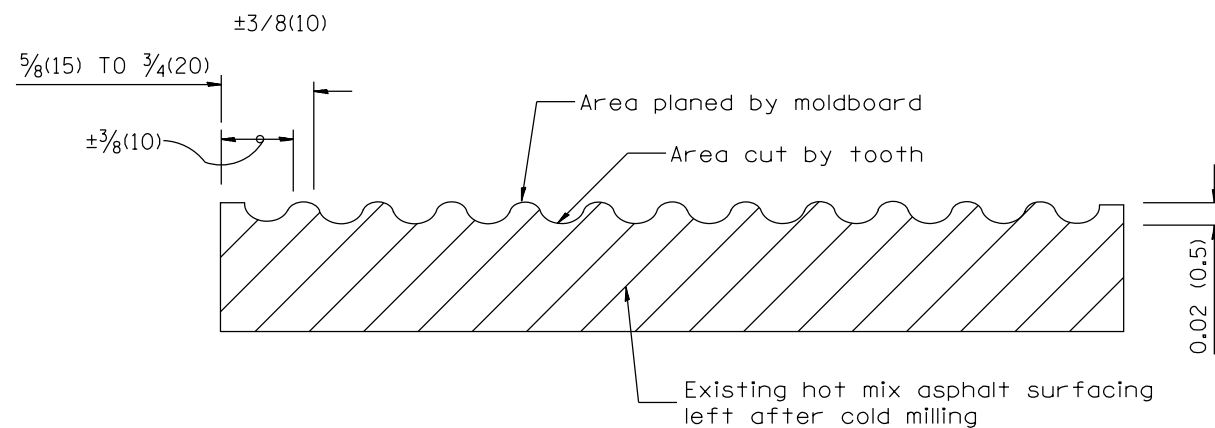
PLAN

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



SECTION A-A



SECTION B-B PROJECTED
PERPENDICULAR TO CENTERLINE

DESIGNER NOTES:
1. INCLUDE DISTRICT SPECIAL PROVISION, IF APPLICABLE.

All dimensions are in inches (millimeters) unless otherwise noted.

01-01-97	RENUM. C-104.01, NEW REVISION BOX	T.P.
04-20-98	REMOVED MILLING DETAIL FROM STANDARD	J.A.
09-08-98	CORRECT NOTE LEADER PLACEMENT	R.W.
10-16-06	REVISED TO 2007 SPEC.	M.A.

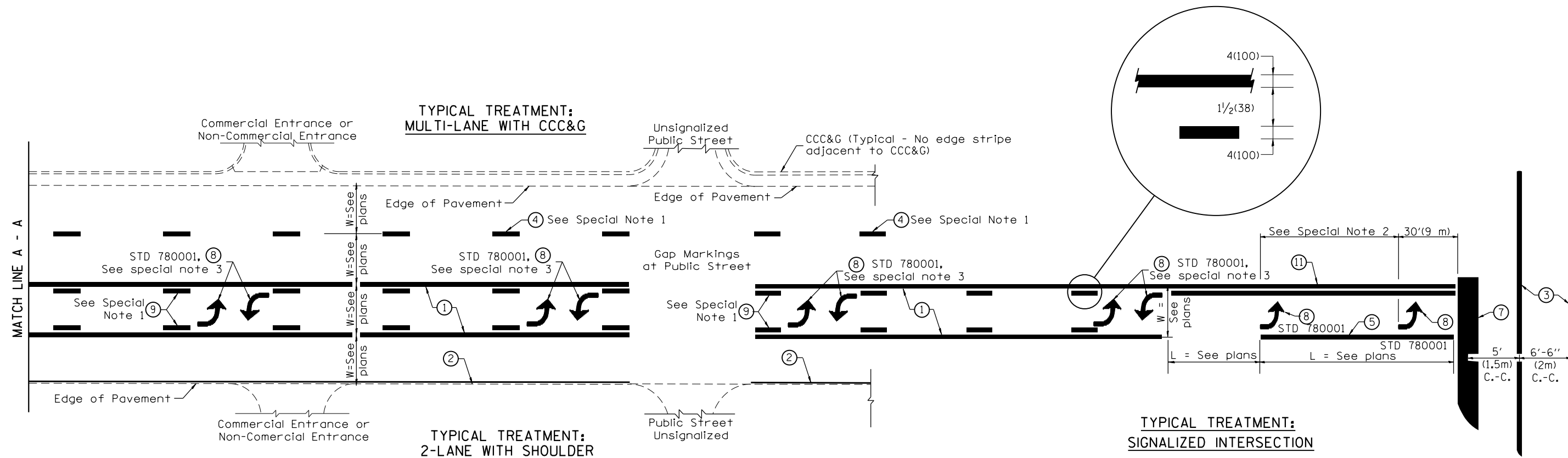
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

NOT TO SCALE

CADD STD. 440001-D4

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	20RS-2;19RS-4;18RS-5	FULTON	30	28
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68953	



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) ⑩
- ⑪ 4(100) Double Solid (Yellow) ⑪

SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.
3. Refer to Article 780.13 for letter, number and symbol areas (sq. ft.)
4. Areas are grooved 1" beyond each edge for the following symbols:
Through Arrow= 14.8 sq. ft.
Large Left or Right Arrow= 21.9 sq. ft.
2 Arrow Combination Left (or Right) and Through= 34.9 sq. ft.
Wrong Way Arrow= 29.5 sq. ft.
Railroad Crossing Symbol= 69.8 sq. ft.
(For further information, refer to BDE Special Provision: Grooving for Recessed Pavement Markings)

01-01-97	RENUM. F-8.03, NEW REVISION BOX	T.P.	10-16-06	REVISED TO 2007 SPEC.	
02-07-97	ADD BI DIRECTIONAL DIMENSION	J.A.	2/29/16	ADDED GROOVING AREAS	R.D.
10-97	CORRECT BI DIRECTIONAL DIMENSION	J.A.	07-16-19	SPELLING CORRECTIONS	R.D.
08-02	ADD CROSSWALK DMNS. WITH T.S.	M.A.			

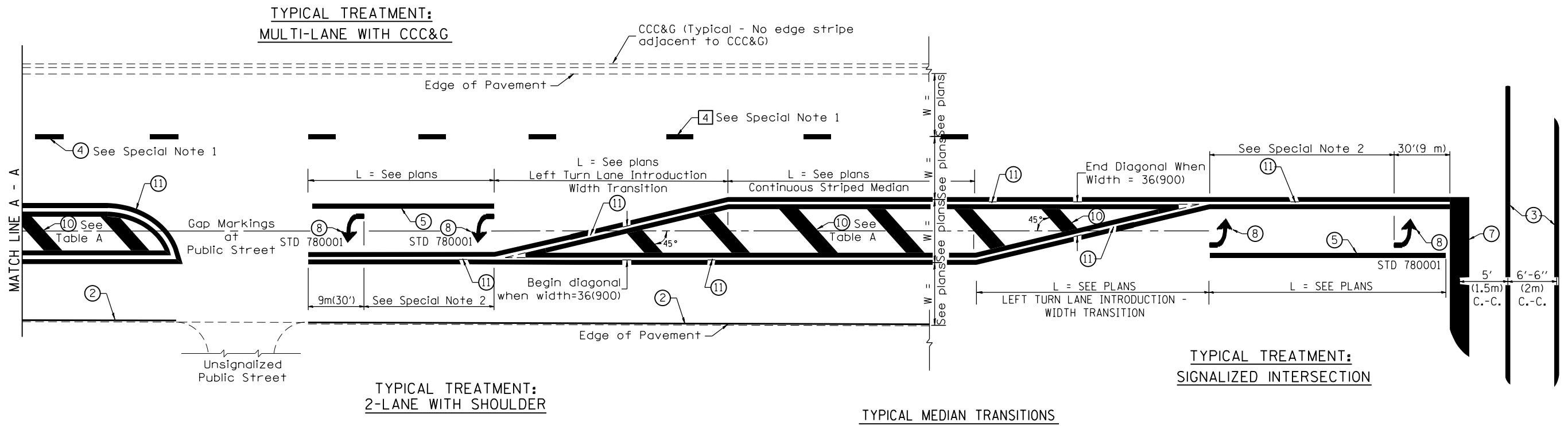
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

NOT TO SCALE

TYPICAL PAVEMENT MARKINGS

SHT. 1 OF 2
CADD STD. 780001-D4

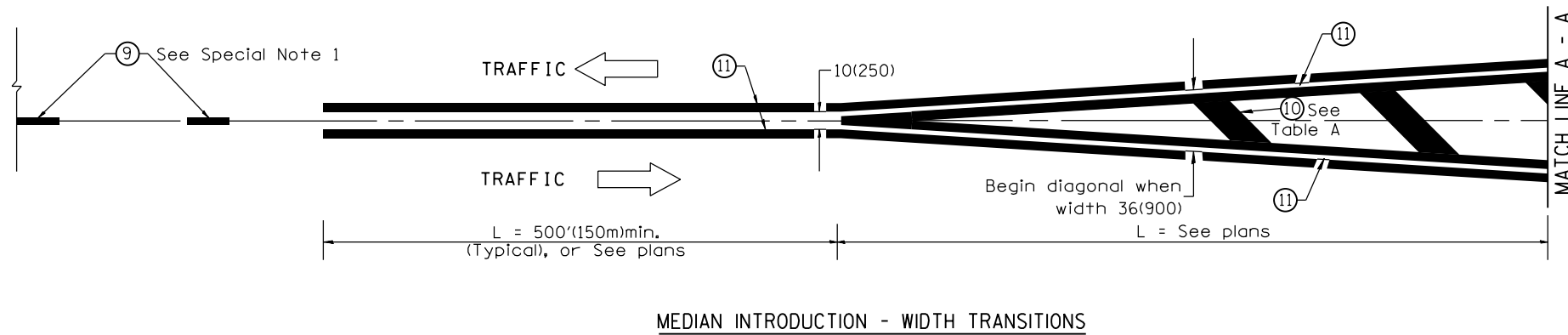
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
317	20RS-2+19RS-4+20RS-5	FULTON	30	29
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 68953	



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

SPEED LIMIT RANGE	INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions)	
	CONTINUOUS	
Less Than 30 mph (50 km/h)	50' (15m)	15' (5m)
30 - 45 mph (50 - 70 km/h)	75' (23m)	20' (6m)
Over 45 mph (70 km/h)	150' (46m)	30' (9m)



All dimensions are in inches (millimeters) unless otherwise noted.