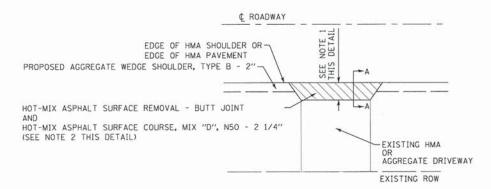
MCHENRY 28 14-00025-00-RS STATE OF ILLINOIS ILLINOIS CONTRACT NO.61B36 M-4003(482) FOR INDEX OF SHEETS, SEE SHEET NO. 2 3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD) DEPARTMENT OF TRANSPORTATION **DIVISION OF HIGHWAYS** FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2 PLANS FOR PROPOSED FEDERAL AID HIGHWAY PROJECT LOCATED IN THE VILLAGE OF FOX RIVER GROVE AND THE ALGONQUIN TOWNSHIP ROAD DISTRICT FAU 3878 (ALGONQUIN ROAD) PLUM TREE ROAD TO US ROUTE 14 TRAFFIC DATA MERCER ALGONQUIN ROAD ADT = 6,363 (2014)SPEED LIMIT = 30 MPH FAU 3879 (PLUM TREE ROAD) ALGONQUIN ROAD TO VILLAGE LIMITS PLUM TREE ROAD ADT = 1,800 (2014)SPEED LIMIT = 25 MPH FAU 0014 (DOYLE ROAD) US ROUTE 14 TO IL ROUTE 22 DOYLE ROAD ADT = 1.508 (2014)SPEED LIMIT = 25 MPH RESURFACING **SECTION: 14-00025-00-RS DESIGN DESIGNATION** PROJECT: M-4003(482) MAJOR COLLECTOR ALGONQUIN ROAD VILLAGE OF FOX RIVER GROVE PLUM TREE ROAD DOYLE ROAD McHENRY COUNTY ALGONQUIN ROAD **END OMISSION** JOB NO: C-91-208-15 STA 56+54 ALGONQUIN ROAD **END RESURFACING / BEGIN OMISSION** LOCATION MAP STA 55+59 ALGONQUIN ROAD **BEGIN RESURFACING /END OMISSION** LOCATION OF SECTION INDICATED THUS: - -STA 51+06 DOYLE ROAD **END RESURFACING** ALGONQUIN ROAD STA 13+33 END RESURFACING /BEGIN OMISSION STA 19+41 ALGONQUIN ROAD STATE OF ILLINOIS BEGIN RESURFACING DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS STA 4+04 DOYLE ROAD RIVER GROVE, VILLAGE ADMINISTRATOR **BEGIN RESURFACING** ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT PASSED FEBRUARY 6, 2015 CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS STA 3+09 ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED. J.U.L.I.E. DESIGN STAGE REQUEST PLUM TREE ROAD DIG. No. A1620624 BEGIN RESURFACING DEPUTY DIRECTOR OF Call STA 20+55 62-056311 SECTIONS 19 & 20, T43N. R9E. OF THE THIRD PRINCIPAL MERIDIAN ALGONQUIN TOWNSHIP
GROSS LENGTH = 7.586 FT = 1.437 MILE CONTACT JULIE AT 811 OR 800-892-0123 PROFESSIONAL NET LENGTH = 4,326 FT = 0.819 MILE ENGINEER OF PLUM TREE ROAD COUNTY = MCHENRY LENGTH OF OMISSION = 3,260 FT = 0.618 MILE **END RESURFACING** CITY-TWNSHP. = FOX RIVER GROVE-ALGONOUIN PRINTED BY THE AUTHORITY SEC. & 1/4 SEC. NO. = 20NW, NE; 19SW, SE, NW, NE; T43N R9E MILINOIS SEC. & 1/4 SEC. NO. = ZUNN, NE. .SS., 48 HOURS (2 working days) BEFORE YOU DIG STA 33+67 OF THE STATE OF ILLINOIS WOODMAN

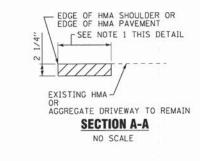
"LICENSE EXPIRES 11-30-2015"

B&W PROJECT NO.: 140536

DATE: 1-29-15

CONTRACT NO. 61B36





NOTES: 1. 3' MIN WHEN MATCHING EXISTING HMA DRIVEWAY UNLESS OTHERWISE DIMENSIONED 3' MIN WHEN MATCHING EXISTING AGGREGATE DRIVEWAY UNLESS OTHERWISE DIMENSIONED

2. FOR COMMERCIAL DRIVEWAYS ON DOYLE ROAD SEE PLAN SHEETS

## **HMA DRIVEWAY DETAIL**

NO SCALE

# INDEX OF SHEETS

| SHEET NO. | TITL |  |
|-----------|------|--|
|           |      |  |
|           |      |  |

- 1 COVER SHEET
- INDEX OF SHEETS, HIGHWAY STANDARDS, BENCHMARKS AND DETAILS 2
- 3 GENERAL NOTES
- 4 6 SUMMARY OF QUANTITIES
- TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS 7 - 9
- ROADWAY PLAN INTERSECTION ALCONQUIN ROAD, PLUM TREE ROAD 10 AND OLD HUNT ROAD
- ROADWAY PLAN ALGONOUIN ROAD 11
- ROADWAY PLAN ALGONQUIN ROAD 12
- ROADWAY PLAN PLUM TREE ROAD 13
- 14 15 ROADWAY PLAN AND PROFILE - DOYLE ROAD
- DISTRICT 1 DETAIL BD-01 DRIVEWAY DETAILS DISTANCE BETWEEN R.O.W. 16 AND FACE OF CURB AND EDGE OF SHOULDER >= 15' (4.5m)
- DISTRICT 1 DETAIL BD-08 FRAMES AND LIDS ADJUSTMENT WITH MILLING; AND FRAMES AND LIDS ADJUSTMENT WITHOUT FILLING
- 18 DISTRICT 1 DETAIL - BD-22 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
- DISTRICT 1 DETAIL BD-24 CURB OR CURB AND GUTTER 19 REMOVAL AND REPLACEMENT
- DISTRICT 1 DETAIL BD-32 BUTT JOINTS AND HMA TAPER 20
- DISTRICT 1 DETAIL TC-10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
- DISTRICT 1 DETAIL TC-13 TYPICAL PAVEMENT MARKINGS 22
- 23 DISTRICT 1 DETAIL - TC-16 PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
- 24 DISTRICT 1 DETAIL - TC-22 ARTERIAL ROAD INFORMATION SIGN
- DISTRICT 1 DETAIL TC-23 TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT 25 MARKING TREATMENT FOR RAILROAD CROSSINGS
- DISTRICT 1 DETAIL TS-05 STANDARD TRAFFIC SIGNAL DESIGN DETAILS
- DISTRICT 1 DETAIL TS-07 DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING
- CROSS SECTIONS DOYLE ROAD 28

## **BENCHMARKS**

NORTHEAST TAG BOLT OF FIRE HYDRANT LOCATED APPROX 107 FEET SOUTHEAST OF THE SOUTHWEST CORNER OF IL ROUTE 22 (HALF DAY ROAD) AND DOYLE ROAD (EAST SIDE OF DOYLE ROAD) EL = 763.28

MINI RAILROAD SPIKE IN WEST FACE OF UTILITY POLE LOCATED APPROX 107 FEET SOUTHEAST OF THE SOUTHWEST CORNER OF IL ROUTE 22 (HALF DAY ROAD) AND DOYLE ROAD. (EAST SIDE OF DOYLE ROAD)

EL = 764.17

NOTE: DATUM USED FOR THESE BENCHMARKS IS NAVD 88

3878 (ALGONQUIN ROAD)

| INDEX | OF  | SHEETS, | HI | GHWA | Y | STANDARDS, |
|-------|-----|---------|----|------|---|------------|
|       | BEN | ICHMARK | S  | AND  | D | ETAILS     |

|                               | C              | 014 (DOYLE ROAD) |         |         |              |
|-------------------------------|----------------|------------------|---------|---------|--------------|
| OF CHEETS HIGHWAY STANDARDS   | F.A.U.<br>RTE. | SECTION          | COUNTY  | TOTAL   | SHEET<br>NO. |
| OF SHEETS, HIGHWAY STANDARDS, |                | 14-00025-00-RS   | MCHENRY | 28      | 2            |
| BENCHMARKS AND DETAILS        |                |                  | CONTRAC | T NO CO | 1076         |

BAXTER WOODMAN

REVISED DESIGNED - JDW DRAWN - KAR REVISED MWP REVISED CHECKED FILE - 140536SHT-Index.dgn DATE 1-29-15

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** 

SCALE: NONE

STA.

CONTRACT NO. 61B36

- 1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
- 2. UTILITY LOCATIONS HAVE NOT BEEN SHOWN IN THESE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY TO VERIFY THAT GRADE CONFLICTS WILL NOT OCCUR WITH ANY PROPOSED UTILITIES PRIOR TO CONSTRUCTION AND ORDERING ANY MATERIALS. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED Y THE CONTRACTOR. THE COST OF THIS EXPLORATION SHALL BE INCLUDED IN THE EXPLORATION TRENCE 48" DEPTH PAY
- 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISIFACTION OF THE ENGINEER
- 4. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
- 5. THE CONTRACTOR SHALL NOTIFY THE VILLAGE ASSISTANT SUPERINTENDENT OF PUBLIC WORKS, JOHN REESE (847) 639-6165 AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN VILLAGE UTILITY LOCATIONS AND SHALL NATE ALL CONSTRUCTION OPERATIONS WITH THE VILLAGE ASSISTANT SUPERINTENDENT OF PUBLIC WORKS AND
- 6. THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
- 7 ACCESS TO PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING CRUSHED STONE OR CRUSHED GRAVEL AS TEMPORARY ACCESS.
- 8. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE ENGINEER WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CURB AND GUTTER AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE VILLAGE TO RESIDENTS AT LEAST 24 HOURS PRIOR TO PLANNED CLOSURE. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES INCLUDING KNOCKING ON DOORS WHEN DRIVEWAYS ARE ABOUT TO BE CLOSED. THE COST OF THIS WORK IS INCLUDED IN THE APPLICABLE PAY ITEM CALISING THE CLOSURE
- 9. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 8-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES COMMERICAL DRIVEWAYS. TRANSVERSE EXPANSION JOINTS % SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER. TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED EVERY 5-FEET.
- 10. A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT 8 INCH
- 11. THE CONTRACTOR SHALL CONTACT THE LOCAL AGENCY MATERIAL INSPECTOR (SOIL AND MATERIAL CONSULTANTS -THOMAS JOHNSON - 847-870-0544) AT LEAST 48 HOURS PRIOR TO ANY CONCRETE OR HOT-MIX ASPHALT MATERIAL
- 12. ALL FRAME AND LID CASTINGS LOCATED WITHIN THE PAVEMENT WHICH REQUIRE RESETTING TO FINISH GRADE SHALL BE BACKFILLED WITH CLASS PP-1 CONCRETE AND ALLOWED TO CURE FOR 72 HOURS PRIOR TO PLACEMENT OF SURFACE COURSE. HMA MATERIALS WILL NOT BE ALLOWED AS BACKFILL AROUND AN ADJUSTED CASTING. THIS WORK SHALL APPLY TO ALL CASTINGS ADJUSTED OR RECONSTRUCTED AS PART OF THIS CONTRACT. WHETHER PAID FOR SEPARATELY OR INCLUDED IN OTHER CONTRACT WORK, SEE SPECIAL PROVISION
- 13. THE DAYS PAVING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. ANY COLD LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY PAVING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE PAVING OF THE ADJACENT LANE IN THE SAME DAY.
- 14. IN AREAS WHERE THE EXISTING DRIVEWAY OR CURB AND GUTTER IS TO BE REMOVED AND REPLACED. THE REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL REQUIRED TO ESTABLISH THE PROPOSED DRIVEWAY, SIDEWALK, OR CURB AND GUTTER SUBGRADE ELEVATION SHALL BE INCLUDED IN THE REMOVAL PAY ITEMS.
- 15. THE CONTRACTOR WILL BE REQUIRED TO USE A STEEL PLATE OR PLATES TO CLOSE ANY GAPS OCCURRING WHEN A FRAME IS OFFSET FROM THE STRUCTURE. THE STEEL PLATE SHALL BE 'S-INCH THICK AND APPROXIMATELY S-INCH WIDE BY 24-INCH LONG. SOME ADJUSTMENT IN SIZE MAY BE NECESSARY TO PREVENT THE STEEL PLATE FROM OVERHANGING THE OUTSIDE OF THE STRUCTURE WALL. THE STEEL PLATE SHALL BE BEDDED IN AND COVERED WITH MORTAR. THIS WORK SHALL BE INCLUDED IN THE COST OF STRUCTURE ADJUSTMENTS OR STRUCTURE RECONSTRUCTION
- 16. THE CURB SHALL BE TAPERED TO THE GUTTER IN A FIVE (5) FOOT LENGTH WHEREVER THE CURB AND GUTTER TERMINATES. WITH AN EXPANSION JOINT PLACED AT THE START OF THE TAPER
- 17. ALL POSTS, RAILROAD TIES, AND DECORATIVE TIMBER IN CONFLICT WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED AND RELOCATED AS DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION. EVERY EFFORT SHALL BE MADE BY THE CONTRACTOR WHEN REMOVING THESE ITEMS RESERVE THEM FROM HARM. ITEMS NOT RELOCATED SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR
- 18. A PORTABLE BATHROOM(S) SHALL BE PLACED ON THE JOB SITE(S) AND RELICCATED WHEN NECESSARY SO IT IS ACCESSIBLE TO WORKERS. IF WORK IS OCCURRING AT SEVERAL LOCATIONS, ONE PORTABLE BATHROOM SHALL BE PLACED AT EACH LOCATION WITHIN A REASONABLE DISTANCE FROM THE WORK AS DETERMINED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE PAY ITEM FOR MOBILIZATION IN ACCORDANCE WITH ARTICLE 107.08.
- 19. ALL CRACKS AND JOINTS SHALL BE CLEANED PRIOR TO FILLING THEM. THIS WORK SHALL BE INCLUDED IN THE ITEM "MIXTURE FOR CRACKS, JOINTS AND FLANGEWAYS."
- 20. ALL AGGREGATE USED ON THIS PROJECT SHALL BE CRUSHED MATERIAL

DESIGNED - JDW

DRAWN - KAR

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MWP

- THE CONTRACTOR SHALL UTILIZE A MECHANICAL SWEEPER TO CLEAN STREETS AFFECTED BY CONTRACTORS OPERATIONS, INCLUDING HAUL ROUTES, AT LEAST TWICE PER WEEK AND ADDITIONALLY AS DETERMINED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST OF MOBILIZATION.
- 22. THE CONTRACTOR WILL ONLY BE ALLOWED TO REMOVE AND REPLACE CURB AND GUTTER ON ONE SIDE OF THE ROAD AT A ME TO MINIMIZE CONGESTION, REPLACEMENT MUST BE COMPLETE ON ONE SIDE OF THE ROAD BEFORE THE CONTRACTOR IS ALLOWED TO BEGIN REMOVING CURB AND GUTTER ON THE OTHER SIDE OF THE ROAD.

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- 23. THE FINISHED HOT-MIX ASHPALT SURFACE SHALL BE CONSTRUCTED 0.25 INCHES ABOVE THE GUTTER FLAG.
- 24. NO STREET CLOSURES WILL BE ALLOWED

BAXTER WOODMAN

- 25 AGGREGATE WEDGE SHOULDER TYPE B SHALL MEET THE REQUIREMENTS OF ARTICLE 1004 4 OF THE STANDARD SPECIFICATIONS. RECLAIMED ASPHALT PAVEMENT (RAP) SHALL NOT BE ALLOWED AS AN ACCEPTABLE MATERIAL FOR AGGREGATE WEDGE SHOULDER, TYPE B.
- 26. THE DETAIL FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT SHOWN IN THE PLANS SHALL BE MODIFIED TO INCLUDE THE FOLLOWING. THE WORK SHALL INCLUDE SAW-CUTTING AND REMOVING THE EXISTING PAVEMENT A MINIMUM OF 6-INCHES MEASURED FROM THE EXISTING EDGE OF PAVEMENT, AND FILLING THE 6" CAP WITH CLASS SI CONCRETE TO AN ELEVATION ½" BELOW THE PROPOSED BINDER COURSE. IF THE CONCRETE IS PLACED HIGHER THAN ½" BELOW THE BINDER COURSE FOR STREETS TO BE RESURFACED. THE CONTRACTOR WILL BE REQUIRED TO GRIND ADDITIONAL CONCRETE TO THE REQUIRED DEPTH. THIS WORK SHALL BE INCLUDED IN THE COST OF THE APPLICABLE CURB
- 27. ON STREETS TO BE FULL WIDTH MILLED (2" OR MORE), THE EXISTING STRUCTURES IN THE PAVEMENT SHALL BE ADJUSTED. IN ACCORDANCE WITH THE IDOT DETAIL "DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING". THIS WORK SHALL BE IN ADDITION TO THE REQUIREMENTS FOR MANHOLES TO BE ADJUSTED AND SHALL BE PAID FOR ONCE AT THE CONTRACT UNIT PRICE FOR MANHOLES TO BE ADJUSTED.
- 28. WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 ½ INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND SHALL NOT EXCEED 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. A MAXIMUM GRADE DIFFERENCE OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H), AS DETERMINED BY THE ENGINEER
- 29. THE DAYS MILLING OPERATION SHOULD RESULT IN A SINGLE TRANSVERSE JOINT. ANY LONGITUDINAL JOINTS WILL NOT BE ACCEPTED. PROVIDING A SINGLE TRANSVERSE JOINT SHALL BE ACCOMPLISHED BY MILLING ONE LANE OF SUFFICIENT LENGTH THAT WILL ALLOW FOR THE MILLING OF THE ADJACENT LANE IN THE SAME DAY. A TEMPORARY RAMP SHALL BE INSTALLED AT THE SINGLE TRANSVERSE JOINT AT THE END OF EACH DAY. THE COST OF THIS WORK IS INCLUDED IN THE APPLICABLE HOT-MIX ASPHALT SURFACE REMOVAL PAY ITEM.
- 30. EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
- 31 CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER
- 32. DURING CONSTRUCTION STAGING OPERATIONS, THE FOX RIVER GROVE VILLAGE POLICE AT 847-639-2411 AND FIRE DEPARTMENTS AT 847-639-9232 SHALL BE NOTIFIED IN WRITING 24 HOURS PRIOR TO LANE CLOSURES. EMERGENCY ACCESS SHALL BE MAINTAINED AND ALLOWED AT ALL TIMES. NO OVERNIGHT LANE CLOSURES WILL BE ALLOWED
- 33. DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A CAST-IN-PLACE 24"X60" NOMINAL PANEL WIDTH. THE PANEL SHALL PARALLEL THE PAVEMENT CROSS WALK WITH THE CLOSEST EDGE LOCATED AT THE BACK OF CURB. THE GENERAL COLOR SHALL BE SELECTED BY THE VILLAGE. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS.
- 34, ALL SIGNS OR MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH ARTICLE 107.20. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES.
- 35. INLET FILTERS SHALL BE CLEANED OF ALL SEDIMENT AND DEBIS OR REPLACED AFTER EVERY 1/2" OR GREATER RAINFALL OR AS REQUIRED BY THE ENGINEER. COST INCLUDED IN PAY ITEM INLET FILTERS.
- 36. DURING CONSTRUCTION, THE CONTRACTOR WILL BE PERMITTED TO LIMIT ON-STREET PARKING IN ORDER TO COMPLETE CONSTRUCTION OPERATIONS. THE CONTRACTOR WILL BE REQUIRED TO COORDINATE WITH THE MUNICIPALITY A MINIMUM OF 48 HOURS IN ADVANCE. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PLACE ADVANCE SIGNS TO ALERT RESIDENTS AND COMMUTERS OF THE CONTRUCTION WORK. THE PLACEMENT OF THESE SIGNS SHALL TAKE PLACE 48 HOURS IN ADVANCE IN ORDER TO ALLOW SUFFICIENT TIME FOR RESIDENTS AND GENERAL PUBLIC TO REVISE THEIR PARKING PATTERNS
- 37. TEMPORARY EROSION CONTROL MEASURES SHALL BE INSTALLED AS REQUIRED. WHERE PARKWAYS ARE DISTURBED, TEMPORARY.EROSION CONTROL MEASURES SHALL BE ESTABLISHED WITHIN 7 DAYS OF COMPLETION OF DISTURBANCE AND MAINTAINED TO THE SATISFACTION OF THE ENGINEER. THIS WORK, INCLUDING MAINTENANCE, SHALL BE INCLUDED IN
- 38. WORK SHALL BE LIMITED TO ONE SIDE OF THE ROADWAY AT A TIME TO MINIMIZE CONGESTION WHERE PRACTICAL
- 39. STRAW BALES SHALL NOT BE USED FOR EROSION CONTROL.
- 40. ADD AN ANIONIC OR NONIONIC POLYACRYLAMIDE (PAM) TO A TURBID DISCHARGE (E.G., DEWATERING, STORMWATER RUN-OFF) OR AS DETERMINED BY THE ENGINEER OR REGULATORY AGENCY. THIS SHALL BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND ILLINOIS URBAN MANUAL. THIS SHALL BE INCLUDED IN THE APPLICABLE PAY
- 41. A CONCRETE TRUCK WASHOUT SHALL BE LOCATED ON THE WEST SIDE OF DOYLE ROAD. THE EXACT LOCATION SHALL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION. THE CONCRETE TRUCK WASHOUT SHALL COMPLY WITH IDOT REQUIREMENTS AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION. THE CONCRETE TRUCK WASHOUT SHALL BE FURNISHED, INSTALLED, MAINTAINED AND PROPERLY DISPOSED. ALL ITEMS RELATED TO THE CONCRETE TRUCK WASHOUT SHALL BE INCLUDED IN THE APPLICABLE CONCRETE PAY ITEM REQUIRING THE CONCRETE TRUCK WASHOUT
- 42. TRENCH BACKFILL FOR THIS PROJECT SHALL CONSIST OF CRUSHED CA-6 AND SHALL BE COMPACTED BY METHOD 1 ONLY.
- 43. CONNECTION OF PROPOSED PIPE CULVERTS / STORM SEWER INTO EXISTING STORM SEWER OR EXISTING STORM SEWER STRUCTURES WHERE THE DIAMETER DOES NOT CHANGE SHALL BE INCLUDED IN THE COST OF PIPE CULVERT / STORM SEWERS.
- 44. PIPE CULVERT REMOVAL SHALL APPLY TO ALL EXISTING PIPES TO BE REMOVED, REGARDLESS OF SIZE, TYPE OR MATERIAL.
- 45. THE LOCATIONS OF CLASS D PATCHES SHALL BE DETERMINED BY THE ENGINEER DURNING CONSTRUCTION
- 46, WORK AND MATERIALS REQUIRED TO INSTALL 1-INCH UNIT DUCT SHALL BE INCLUDED IN THE PAY ITEM DETECTOR LOOP REPLACEMENT
- 47. THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF AN TEMPORARY TRAFFIC CONTROL DEVICES.

SCALE: NONE

- 48. ANY PROPOSED ACTIVITY IN THE VICINITY OF A HIGHWAY-RAIL GRADE CROSSING MUST ADHERE TO THE GUIDELINES SET FORTH IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) UNDER SECTION 6G.18: WORK IN THE VICINITY A GRADE CROSSING WHICH STATES: "WHEN GRADE CROSSINGS EXIST FITHER WITHIN OR IN THE VICINITY OF A TTC ZONE LANE RESTRICTIONS, FLAGGING, OR OTHER OPERATIONS SHALL NOT CREATE CONDITIONS WHERE VEHICLES CAN BE QUEUED ACROSS THE TRACKS. IF THE QUEUING OF VEHICLES ACROSS THE TRACKS CANNOT BE AVOIDED, A UNIFORMED LAW ENFORCEMENT OFFICER OR FLAGGER SHALL BE PROVIDED AT THE CROSSING TO PREVENT VEHICLES FROM STOPPING ON THE TRACKS, EVEN IF AUTOMATIC WARNING DEVICES ARE IN PLACE."
- 49. IT IS THE CONTRACTOR'S SOLE RESPONISBILITY TO COORDINATE WITH THE UNION PACIFIC RAILROAD WHENEVER CONSTRUCTION ACTIVITY IS WITHIN 25 FEET OF THE RAILROAD ROW. THE CONTRACTOR SHALL RETAIN FLAGMEN EMPLOYED AND DESIGNATED BY THE UNION PACIFIC RAILROAD TO MONITOR ON-COMING TRAIN TRAFFIC, AND ADVISE CONTRACTOR PERSONNEL WHEN ACTIVITY ON OR NEAR THE RAILROAD RIGHT-OF-WAY MAY PROCEED. THIS ITEM WILL BE PAID FOR ACCORDING TO ARTICLE 107.12 AND WILL BE REIMBURSED ACCORDING TO ARTICLE 109.05.
- 50. WHEN THE PLANS OR SPECIAL PROVISIONS INCLUDE INFORMATION PERTAINING TO THE LOCATION OF UNDERGROUND UTILITY FACILITIES, SUCH INFORMATION REPRESENTS ONLY THE OPINION OF FOX RIVER GROVE AS TO THE LOCATION OF SUCH UTILITIES AND IS ONLY INCLUDED FOR THE CONVENIENCE OF THE BIDDER.

3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD)

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

SECTION COUNTY **GENERAL NOTES** McHENRY 28 3 14-00025-00-RS CONTRACT NO. 61B36 STA. TO STA.

| CODE     |   | LINUT | TOTAL    | CONSTRUCTION<br>TYPE CODE<br>RESURFACING |
|----------|---|-------|----------|--|
| NUMBER   | ITEM  | UNIT  | QUANTITY | 0005<br>STU                              |
| 20200100 | EARTH EXCAVATION                            | CUYD  | 40       | 40                                       |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CUYD  | 162      | 162                                      |
| 20400800 | FURNISHED EXCAVATION                        | CUYD  | 152      | 15:                                      |
| 20800150 | TRENCH BACKFILL                             | CUYD  | 97       | 9  |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4"               | SQYD  | 415      | 415                                      |
| 21301048 | EXPLORATION TRENCH 48" DEPTH                | FOOT  | 10       | 10                                       |
| 21400100 | GRADING AND SHAPING DITCHES                 | FOOT  | 310      | 31                                       |
| 25000400 | NITROGEN FERTILIZER NUTRIENT                | POUND | 8        |  |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT              | POUND | 8        |  |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT               | POUND | 8        |  |
| 25200110 | SODDING, SALT TOLERANT                      | SQYD  | 415      | 41                                       |
| 25200200 | SUPPLEMENTAL WATERING                       | UNIT  | 7        |  |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING           | POUND | 9        | )  |
| 28000305 | TEMPORARY DITCH CHECKS                      | FOOT  | 110      | ) 11                                     |
|          |   |       |          |  |

# SUMMARY OF QUANTITIES

| CODE<br>NUMBER | ITEM   | UNIT  | TOTAL<br>QUANTITY | CONSTRUCTION<br>TYPE CODE<br>RESURFACING<br>0005<br>STU |
|----------------|--|-------|-------------------|---|
| 30300001       | AGGREGATE SUBGRADE IMPROVEMENT                             | CUYD  | 77                | 77  |
| 35101400       | AGGREGATE BASE COURSE, TYPE B                              | TON   | 333               | 333   |
| 35101600       | AGGREGATE BASE COURSE, TYPE B 4"                           | SQ YD | 459               | 459   |
| 35101800       | AGGREGATE BASE COURSE, TYPE B 6"                           | SQ YD | 69                | 69  |
| 35102400       | AGGREGATE BASE COURSE, TYPE B 12"                          | SQ YD | 369               | 369   |
| 35800100       | PREPARATION OF BASE  | SQ YD | 4,100             | 4,100   |
| 4060027        | BITUMINOUS MATERIALS (PRIME COAT)                          | POUND | 18,884            | 18,88   |
| 4060040        | ) MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS               | TON   | 8                 |   |
| 4060082        | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON   | 746               | 74  |
| 4060098        | 2 HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT             | SQ YD | 738               | 73  |
| 4060308        | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50                | TON   | 738               | 73  |
| 4060333        | 5 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50             | TON   | 1,711             | 1,71  |
| 4200130        | D PROTECTIVE COAT  | SQYD  | 599               | 59  |
| 4230040        | D PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH       | SQYD  | 210               | 21  |
| 4240020        | D PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH                 | SQ FT | 1,685             | 1,68  |

<sup>#</sup> INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

BAXTER WOODMAN

| DESIGNED |   | JDW     | REVISED -                |
|----------|---|---------|--------------------------|
| DRAWN    | - | KAR     | REVISED -                |
| CHECKED  | - | MWP     | REVISED -                |
| DATE     |   | 1-29-15 | FILE - 140536SHT-S00.dgn |

|             |                | F.A.U.<br>RTE. | SECTION   | COUNTY                  | SHEETS           | NO.      |     |
|-------------|----------------|----------------|-----------|-------------------------|------------------|----------|-----|
| SUM         | MARY OF QUANTI | TIES           |           | 14-00025-00-RS          | MCHENRY          | 28       | 4   |
|             |                |                |           |                         | CONTRACT         | NO. 61   | B36 |
| SCALE: NONE | STA.           | TO STA.        | FED. ROAD | DIST. NO. ILLINOIS FED. | AID PROJECT M-40 | 003(482) |     |

<sup>\*</sup> SPECIALTY ITEM # INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

<sup>\* 3878 (</sup>ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD)

| CODE<br>NUMBER | ITEM                                     | UNIT | TOTAL<br>QUANTITY | CONSTRUCTION<br>TYPE CODE<br>RESURFACING<br>0005<br>STU |
|----------------|--|------|-------------------|---|
| 42400410       | PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH | SQFT | 1,104             | To take the second                                      |
| 42400800       | DETECTABLE WARNINGS                      | SQFT | 92                | 92  |
| 44000156       | HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"  | SQYD | 3,250             | 3,250   |
| 44000158       | HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"  | SQYD | 9,690             | 9,690   |
| 44000200       | DRIVEWAY PAVEMENT REMOVAL                | SQYD | 395               | 395   |
| 44000600       | SIDEWALK REMOVAL                         | SQFT | 435               | 435   |
| 44201705       | CLASS D PATCHES, TYPE II, 5 INCH         | SQYD | 36                | 36  |
| 44201709       | CLASS D PATCHES, TYPE III, 5 INCH        | SQYD | 108               | 108   |
| 44201711       | CLASS D PATCHES, TYPE IV, 5 INCH         | SQYD | 60                | 60  |
| 44201765       | CLASS D PATCHES, TYPE II, 10 INCH        | SQYD | 72                | 72  |
| 44201769       | CLASS D PATCHES, TYPE III, 10 INCH       | SQYD | 216               | 216   |
| 44201771       | CLASS D PATCHES, TYPE IV, 10 INCH        | SQYD | 150               | 150   |
| 44201796       | CLASS D PATCHES, TYPE IV, 12 INCH        | SQYD | 35                | 35  |
| 48102100       | AGGREGATE WEDGE SHOULDER, TYPE B         | TON  | 108               | 108   |
|                |  |      |                   |   |

<sup>\*</sup> SPECIALTY ITEM

# SUMMARY OF QUANTITIES

| CODE<br>NUMBER | ITEM   | UNIT  | TOTAL<br>QUANTITY | CONSTRUCTION<br>TYPE CODE<br>RESURFACING<br>0005<br>STU |
|----------------|--|-------|-------------------|---|
| 542D1057       | PIPE CULVERTS, CLASS D, TYPE 2 12"                       | FOOT  | 30                | 30  |
| 542D8203       | PIPE CULVERTS, CLASS D, TYPE 2 EQUIVALENT ROUND-SIZE 18" | FOOT  | 82                | 82  |
| 54215547       | METAL END SECTIONS 12"                                   | EACH  | 1                 |   |
| 54215763       | METAL END SECTIONS, EQUIVALENT ROUND-SIZE 18"            | EACH  | 2                 |   |
| 550A2320       | STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 12"         | FOOT  | 12                | 12  |
| 550A2340       | STORM SEWERS, RUBBER GASKET, CLASS A, TYPE 1 18"         | FOOT  | 307               | 307   |
| 60240395       | INLETS, TYPE B   | EACH  | 3                 |   |
| 60255500       | MANHOLES TO BE ADJUSTED                                  | EACH  | 2                 |   |
| 60402210       | GRATES, TYPE 8   | EACH  | 3                 |   |
| 67100100       | MOBILIZATION   | L SUM | 1                 |   |
| 70102620       | TRAFFIC CONTROL AND PROTECTION, STANDARD 701501          | L SUM | 1                 |   |
| 70102635       | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701          | L SUM | 1                 |   |
| 70102640       | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801          | L SUM | 1                 |   |
| 70300100       | SHORT TERM PAVEMENT MARKING                              | FOOT  | 7,180             | 7,18  |
| 70301000       | WORK ZONE PAVEMENT MARKING REMOVAL                       | SQ FT | 404               | 40  |

BAXTER WOODMAN

| 1 | DESIGNED - | JDW     | REVISED -                |
|---|------------|---------|--------------------------|
|   | DRAWN -    | KAR     | REVISED -                |
|   | CHECKED -  | MWP     | REVISED -                |
|   | DATE -     | 1-29-15 | FILE - 140536SHT-S00.dgn |

|             |           |          |         |          | SE                                 | CTION         | COUNTY          | SHEETS   | NO. |
|-------------|-----------|----------|---------|----------|------------------------------------|---------------|-----------------|----------|-----|
|             | SUMMARY O | F QUANTI | TIES    |          | <ul> <li>14-00025-00-RS</li> </ul> |               | MCHENRY         | 28       | 5   |
|             |           | owner:   |         |          |                                    | CONTRAC       | T NO. 61        | B36      |     |
| SCALE: NONE |           | STA.     | TO STA. | FED. ROA | AD DIST. NO.                       | ILLINOIS FED. | AID PROJECT M-4 | 003(482) |     |

<sup>#</sup> INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

<sup>#</sup> INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

# **SUMMARY OF QUANTITIES**

| 8000100                                 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS  | SQ FT   | 124  | 124  |
|---|---|---|--|--|
|   | THE DAYON ACTIO DAYEMENT MADIZING, LINE 4"  |   |  |  |
| 2000400                                 | THERMOPLASTIC PAVEMENT MARKING - LINE 4"  | FOOT  | 18,095   | 18,095   |
| 8000400                                 | THERMOPLASTIC PAVEMENT MARKING - LINE 6"  | FOOT  | 890  | 890  |
| 8000500                                 | THERMOPLASTIC PAVEMENT MARKING - LINE 8"  | FOOT  | 140  | 140  |
| 8000600                                 | THERMOPLASTIC PAVEMENT MARKING - LINE 12"   | FOOT  | 369  | 369  |
| 8000650                                 | THERMOPLASTIC PAVEMENT MARKING - LINE 24"   | FOOT  | 252  | 252  |
| 8600600                                 | DETECTOR LOOP REPLACEMENT   | FOOT  | 366  | 366  |
| 0003300                                 | BASE COURSE REMOVAL (SPECIAL)   | SQYD  | 369  | 369  |
| 0004562                                 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT                                    | FOOT  | 378  | 378  |
| 0013798                                 | CONSTRUCTION LAYOUT   | L SUM   | 1  | 1  |
| 0019600                                 | DUST CONTROL WATERING   | UNIT  | 50   | 50   |
| 0030850                                 | TEMPORARY INFORMATION SIGNING   | SQ FT   | 390  | 390  |
| 0048665                                 | RAILROAD PROTECTIVE LIABILITY INSURANCE   | L SUM   | 1  | 1  |
| 0322936                                 | REMOVE EXISTING FLARED END SECTION  | EACH  | 2  | 2  |
| 2020110                                 | GRADING AND SHAPING SHOULDERS   | UNIT  | 48   | 48   |
| 8 8 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 | 3000650<br>3600600<br>0003300<br>0004562<br>0013798<br>0019600<br>0030850<br>0048665<br>0322936 | THERMOPLASTIC PAVEMENT MARKING - LINE 24*  DO003000 DETECTOR LOOP REPLACEMENT  DO003300 BASE COURSE REMOVAL (SPECIAL)  COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT  DO13798 CONSTRUCTION LAYOUT  DO19600 DUST CONTROL WATERING  DO30850 TEMPORARY INFORMATION SIGNING  DO48665 RAILROAD PROTECTIVE LIABILITY INSURANCE | 3000650 THERMOPLASTIC PAVEMENT MARKING - LINE 24"  FOOT  3600600 DETECTOR LOOP REPLACEMENT  FOOT  3003300 BASE COURSE REMOVAL (SPECIAL)  3004562 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT  3013798 CONSTRUCTION LAYOUT  L SUM  3019600 DUST CONTROL WATERING  UNIT  3030850 TEMPORARY INFORMATION SIGNING  3048665 RAILROAD PROTECTIVE LIABILITY INSURANCE  L SUM  30322936 REMOVE EXISTING FLARED END SECTION  EACH | 1000650   THERMOPLASTIC PAVEMENT MARKING - LINE 24"   FOOT   252 |

\* SPECIALTY ITEM

# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

# SUMMARY OF QUANTITIES

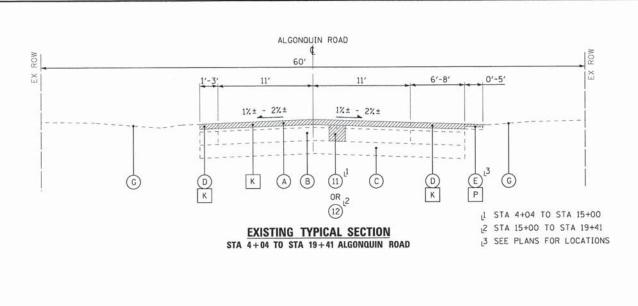
| CODE<br>NUMBER | ITEM   | UNIT  | TOTAL<br>QUANTITY  | CONSTRUCTION<br>TYPE CODE<br>RESURFACING<br>0005<br>STU   |
|----------------|--|---|--|---|
| X4021000       | TEMPORARY ACCESS (PRIVATE ENTRANCE)                                  | EACH  | 1  |   |
| X4022000       | TEMPORARY ACCESS (COMMERCIAL ENTRANCE)                               | EACH  | 8  | 8   |
| X4023000       | TEMPORARY ACCESS (ROAD)  | EACH  | 4  | 4   |
| X4400196       | HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL                             | SQYD  | 4,100  | 4,100   |
| X6026624       | VALVE BOXES TO BE ADJUSTED (SPECIAL)                                 | EACH  | 6  | 6   |
| X6030310       | FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)                             | EACH  | 19   | 15  |
| XX003424       | CONNECTION TO EXISTING STRUCTURE                                     | EACH  | 1  |   |
| XX008824       | CATCH BASINS, SPECIAL  | EACH  | 1  |   |
|                |  |   |  |   |
|                |  |   |  |   |
|                |  |   |  |   |
|                |  |   |  |   |
|                |  |   |  |   |
|                |  |   |  |   |
|                |  |   |  | 7   |
|                | X4021000  X4022000  X4023000  X4400196  X6026624  X6030310  XX003424 | NUMBER  X4021000 TEMPORARY ACCESS (PRIVATE ENTRANCE)  X4022000 TEMPORARY ACCESS (COMMERCIAL ENTRANCE)  X4023000 TEMPORARY ACCESS (ROAD)  X4400196 HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL  X6026624 VALVE BOXES TO BE ADJUSTED (SPECIAL)  X6030310 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)  XX003424 CONNECTION TO EXISTING STRUCTURE | X4021000 TEMPORARY ACCESS (PRIVATE ENTRANCE)  EACH  X4022000 TEMPORARY ACCESS (COMMERCIAL ENTRANCE)  EACH  X4023000 TEMPORARY ACCESS (ROAD)  EACH  X400196 HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL  X6026624 VALVE BOXES TO BE ADJUSTED (SPECIAL)  EACH  X6030310 FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)  EACH  XX003424 CONNECTION TO EXISTING STRUCTURE  EACH | CODE NUMBER         ITEM         UNIT         TOTAL QUANTITY           X4021000         TEMPORARY ACCESS (PRIVATE ENTRANCE)         EACH         1           X4022000         TEMPORARY ACCESS (COMMERCIAL ENTRANCE)         EACH         8           X4023000         TEMPORARY ACCESS (ROAD)         EACH         4           X4400196         HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL         SQ YD         4,100           X6026624         VALVE BOXES TO BE ADJUSTED (SPECIAL)         EACH         6           X6030310         FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)         EACH         19           XX003424         CONNECTION TO EXISTING STRUCTURE         EACH         1 |

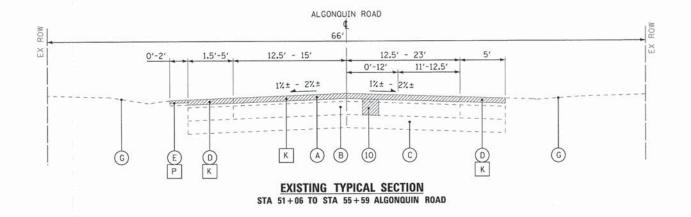
# INDICATES SPECIAL PROVISION AND/OR GENERAL NOTE AND/OR DETAIL

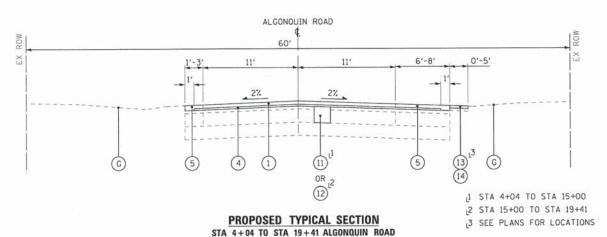
BAXTER WOODMAN

DESIGNED - JDW REVISED DRAWN - KAR REVISED CHECKED - MWP REVISED FILE - 140536SHT-S00.dgn 1-29-15

| EM<br>ECIAL PROVISION AND/OR GENER/ | AL NOTE AND/ | OR DETAIL | •              | 3878 (ALGONQUIN ROAD)<br>3879 (PLUM TREE ROAD)<br>0014 (DOYLE ROAD) |                  |          |              |
|-------------------------------------|--------------|-----------|----------------|---|------------------|----------|--------------|
|                                     |              |           |                |   | COUNTY           | TOTAL    | SHEET<br>NO. |
| SUMMARY                             | ITIES        |           | 14-00025-00-RS | MCHENRY   | 28               | 6        |              |
|                                     |              |           |                | CONTRACT  | NO. 61           | B36      |              |
| CALE: NONE                          | STA.         | TO STA.   | FED. RO        | AD DIST. NO. ILLINOIS FED.  | AID PROJECT M-40 | 003(482) |              |







ALGONQUIN ROAD 12.5' - 23' 12.5' - 15' 1.5'-5' 0'-12' 11'-12.5' 13 14 (4) (5) (G)

## PROPOSED TYPICAL SECTION STA 51+06 TO STA 55+59 ALGONQUIN ROAD

## **EXISTING LEGEND**

#### EXISTING HOT-MIX ASPHALT PAVEMENT B EXISTING AGGREGATE BASE COURSE 0 EXISTING SUB-GRADE 0 EXISTING HOT-MIX ASPHALT SHOULDER E EXISTING AGGREGATE SHOULDER F EXISTING CURB AND GUTTER (0) EXISTING GROUND SURFACE Н HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL J HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4" HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4" BASE COURSE REMOVAL (SPECIAL) M REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (UNDERCUT).. REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)

EXACT THICKNESS VARIES
(MATCH HOT-MIX ASPHALT SURFACE REMOVAL UNLESS OTHERWISE NOTED)

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

(NOT MEASURED FOR PAYMENT AND NOT PAID FOR SEPARATELY)

AGGREGATE SHOULDER REMOVAL, 2" MIN

## PROPOSED LEGEND

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2" 2 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2" 345678 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 3" POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4" HOT-MIX ASPHALT SHOULDERS (PAID FOR AS (1) AND (4) OR (2) AND (3)) PREPARATION OF BASE AGGREGATE BASE COURSE, TYPE B AGGREGATE BASE COURSE, TYPE B 12" 9 0 11 2 13 14 15 6 AGGREGATE SUB-GRADE IMPROVEMENT .. CLASS D PATCHES, 5-INCH CLASS D PATCHES, 10-INCH CLASS D PATCHES, 12-INCH AGGREGATE WEDGE SHOULDER, TYPE B GRADING AND SHAPING SHOULDERS TOPSOIL FURNISH AND PLACE, 4" SODDING, SALT TOLERANT

\*\* AGGREGATE SUBGRADE IMPROVEMENT (ASI) HAS BEEN PROVIDED FOR USE AT THE LOCATIONS INDICATED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER, ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH A STATIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.03 AND THE UNDERCUT GUIDELINES IN THE IDOT SUBGRADE STABILITY MANUAL. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH IN COMMERCIAL DRIVEWAYS)

(18) AGGREGATE BASE COURSE, TYPE B 4" (AGGREGATE BASE COURSE, TYPE B 6" IN COMMERCIAL DRIVEWAYS)

(19) FURNISHED EXCAVATION

(17)

# HOT-MIX ASPHALT MIXTURE REQUIREMENTS

CONTRACTOR WILL MILL FIRST

| MIXTURE TYPE  | AIR VOIDS<br>@ Ndes |  |  |  |
|---|---------------------|--|--|--|
| RESURFACING - ALGONQUIN ROAD AND PLUM TREE ROAD  HOT-MIX ASPHALT SURFACE COURSE, MIX"D", N50, (IL 9.5mm); 1-1/2"  4% @ 50 Gyr. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"  3.5% @ 50 Gyr. RESURFACING - DOYLE ROAD  HOT-MIX ASPHALT SURFACE COURSE, MIX"D", N50; (IL 9.5mm); 2"  4% @ 50 Gyr.  HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 3"  4% @ 50 Gyr.  DRIVEWAYS (COMMERCIAL) DOYLE ROAD  HOT-MIX ASPHALT SURFACE COURSE, MIX"D", N50; (IL 9.5mm); 2"  4% @ 50 Gyr.  HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 3"  4% @ 50 Gyr.  DRIVEWAYS (RESURFACING)  HOT-MIX ASPHALT SURFACE COURSE, MIX"D", N50, (IL 9.5mm); 1-1/2"  4% @ 50 Gyr. |                     |  |  |  |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm); 1-1/2"  | 4% @ 50 Gyr.        |  |  |  |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"  | 3.5% @ 50 Gyr       |  |  |  |
| RESURFACING - DOYLE ROAD  |                     |  |  |  |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50; (IL 9.5mm); 2"  | 4% @ 50 Gyr.        |  |  |  |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 3"   | 4% @ 50 Gyr.        |  |  |  |
| DRIVEWAYS (COMMERCIAL) DOYLE ROAD   |                     |  |  |  |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50; (IL 9.5mm); 2"  | 4% @ 50 Gyr.        |  |  |  |
| HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50; 3"   | 4% @ 50 Gyr.        |  |  |  |
| DRIVEWAYS (RESURFACING)   | Al-                 |  |  |  |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, (IL 9.5mm); 1-1/2"  | 4% @ 50 Gyr.        |  |  |  |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50; 3/4"  | 3.5% @ 50 Gyr       |  |  |  |
| PATCHING  |                     |  |  |  |
| CLASS D PATCHES (HMA BINDER, IL-19 mm, N50); 5", 10", 12"   | 4% @ 70 Gyr.        |  |  |  |
| THE INVENTIGE TO CALCULATE ALL DAMA CLIDEACE MIXTURE OF   | ANTITIES IS 112     |  |  |  |

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS

FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS. FOR "PERCENT OF RAP" SEE SPECIAL PROVISIONS.

3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD)

BAXTER WOODMAN

REVISED DESIGNED - JDW DRAWN KAR REVISED REVISED CHECKED FILE - 140536SHT-TypSec.dgn 1-29-15

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** 

TYPICAL SECTIONS AND HOT-MIX **ASPHALT MIXTURE REQUIREMENTS** 

COUNTY SECTION MCHENRY 28 7 CONTRACT NO. 61B36

EXISTING SIDEWALK

EARTH EXCAVATION

ITEM TO BE REMOVED

## **EXISTING LEGEND**

(A) EXISTING HOT-MIX ASPHALT PAVEMENT B

EXISTING HOT-MIX ASPHALT SHOULDER

EXISTING AGGREGATE BASE COURSE

0 EXISTING SUB-GRADE

(G)

М

0 E EXISTING AGGREGATE SHOULDER

F

EXISTING CURB AND GUTTER

EXISTING GROUND SURFACE

HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL

HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"

HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"

BASE COURSE REMOVAL (SPECIAL)

REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (UNDERCUT)..

REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (TOPSOIL)

AGGREGATE SHOULDER REMOVAL, 2" MIN

EXACT THICKNESS VARIES
(MATCH HOT-MIX ASPHALT SURFACE REMOVAL UNLESS OTHERWISE NOTED) (NOT MEASURED FOR PAYMENT AND NOT PAID FOR SEPARATELY)

COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

EXISTING SIDEWALK

EARTH EXCAVATION

ITEM TO BE REMOVED

# PROPOSED LEGEND

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2" 2 HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 2"

3 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50 - 3"

(4) (5) POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 - 3/4"

HOT-MIX ASPHALT SHOULDERS (PAID FOR AS 1) AND 4 OR 2 AND 3)

PREPARATION OF BASE

AGGREGATE BASE COURSE, TYPE B

6 7 8 AGGREGATE BASE COURSE, TYPE B 12"

AGGREGATE SUB-GRADE IMPROVEMENT ..

9 10 CLASS D PATCHES, 5-INCH

(11) CLASS D PATCHES, 10-INCH

(12) CLASS D PATCHES, 12-INCH

(13) AGGREGATE WEDGE SHOULDER, TYPE B

(14) GRADING AND SHAPING SHOULDERS

TOPSOIL FURNISH AND PLACE, 4"

(15) (16) SODDING, SALT TOLERANT

PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH IN COMMERCIAL DRIVEWAYS)

(18) AGGREGATE BASE COURSE, TYPE B 4" (AGGREGATE BASE COURSE, TYPE B 6" IN COMMERCIAL DRIVEWAYS)

(19) FURNISHED EXCAVATION

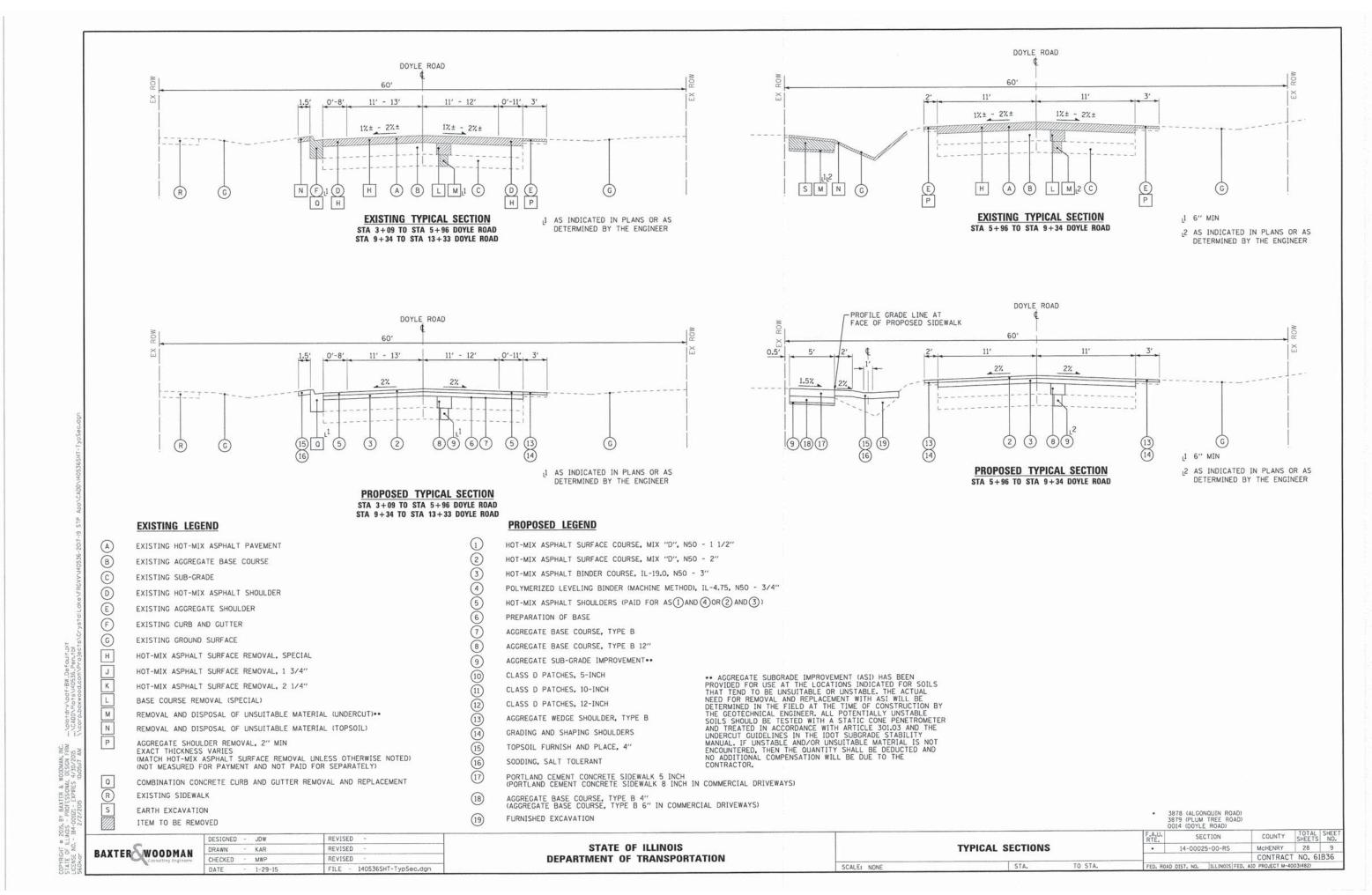
17

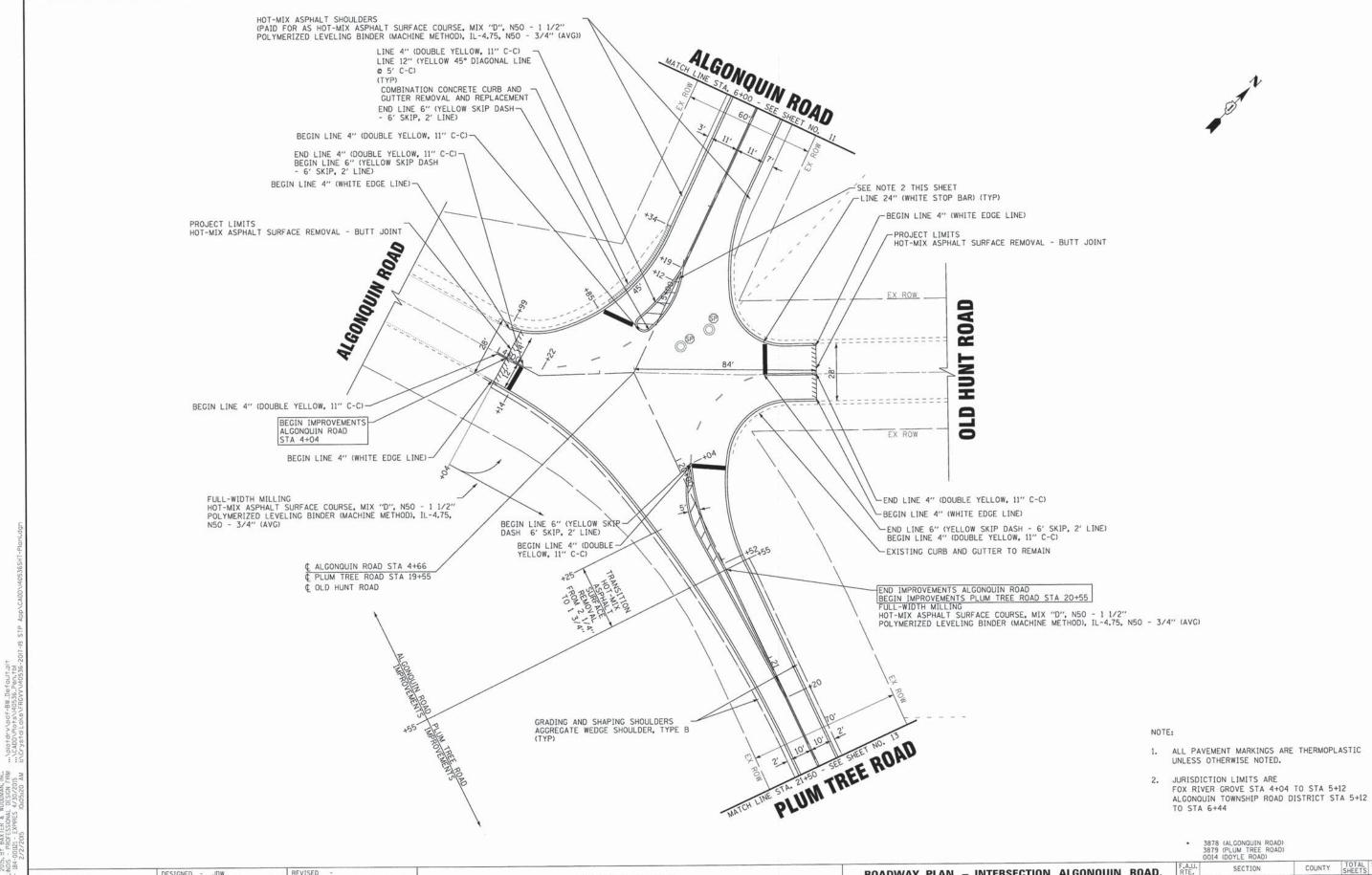
STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** 

3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD) COUNTY TOTAL SHEE NO. SECTION TYPICAL SECTIONS MCHENRY 28 8 14-00025-00-RS CONTRACT NO. 61B36 STA. TO STA. SCALE: NONE

BAXTER WOODMAN

DESIGNED - JDW REVISED REVISED DRAWN - KAR CHECKED MWP REVISED FILE - 140536SHT-TypSec.dgn

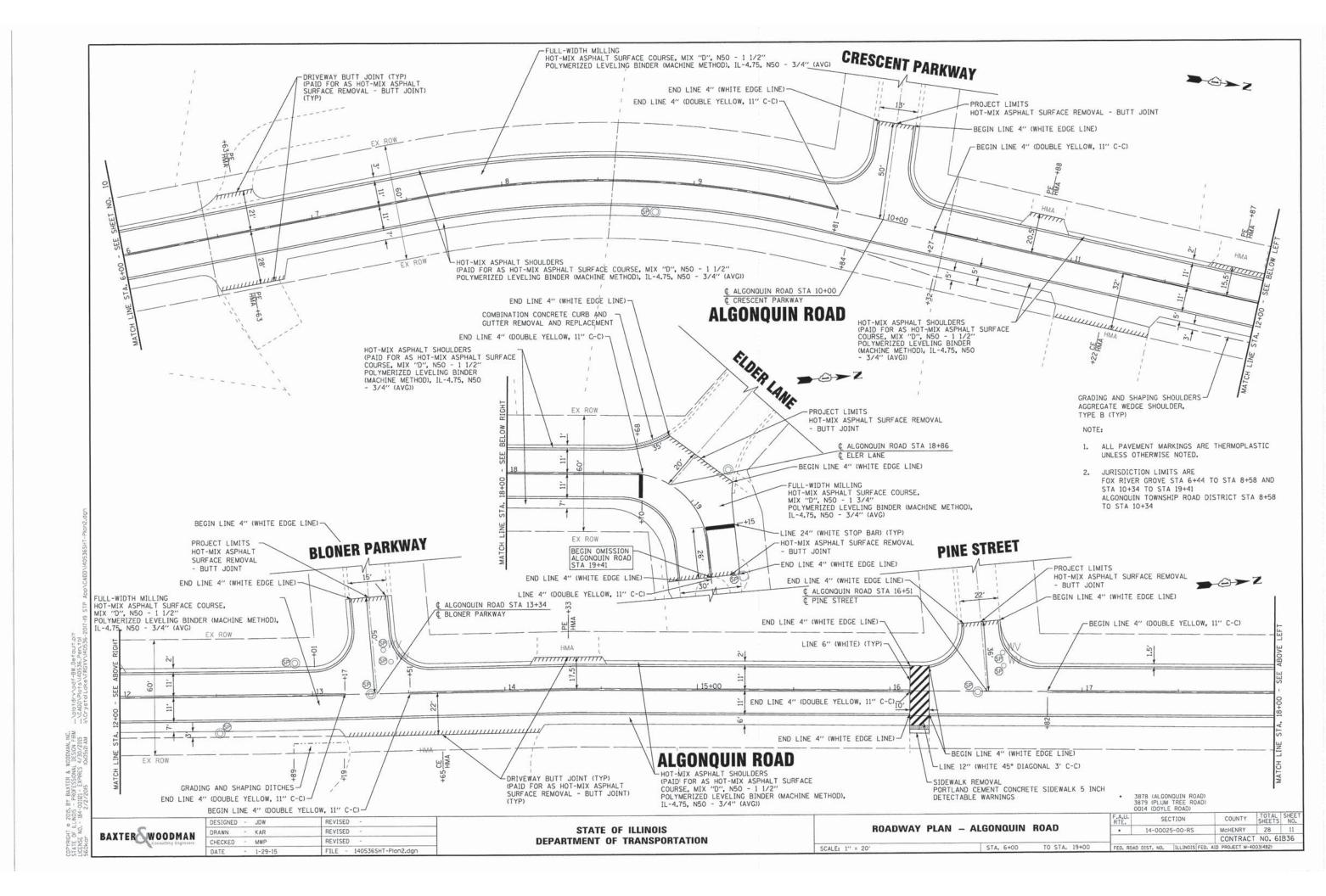


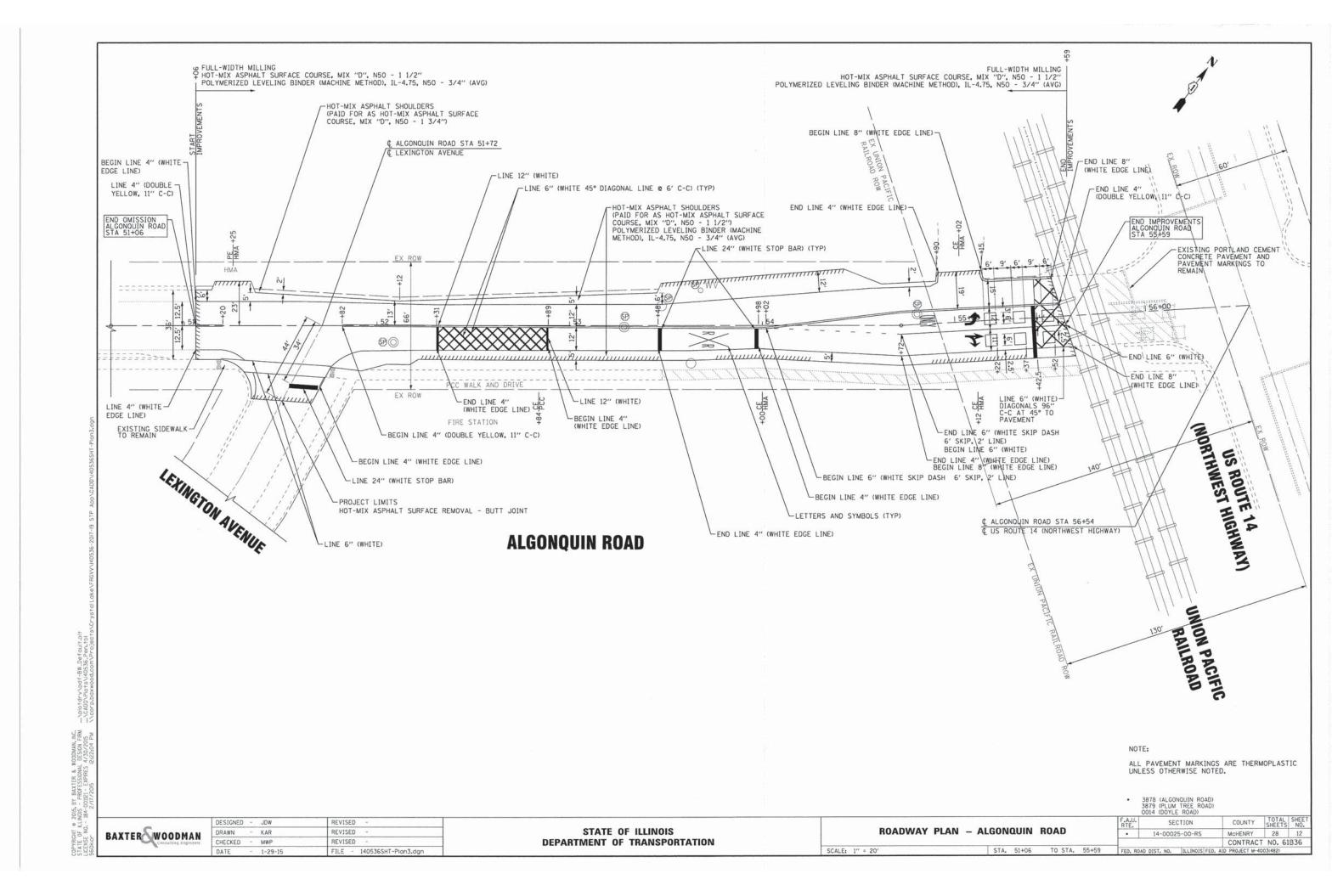


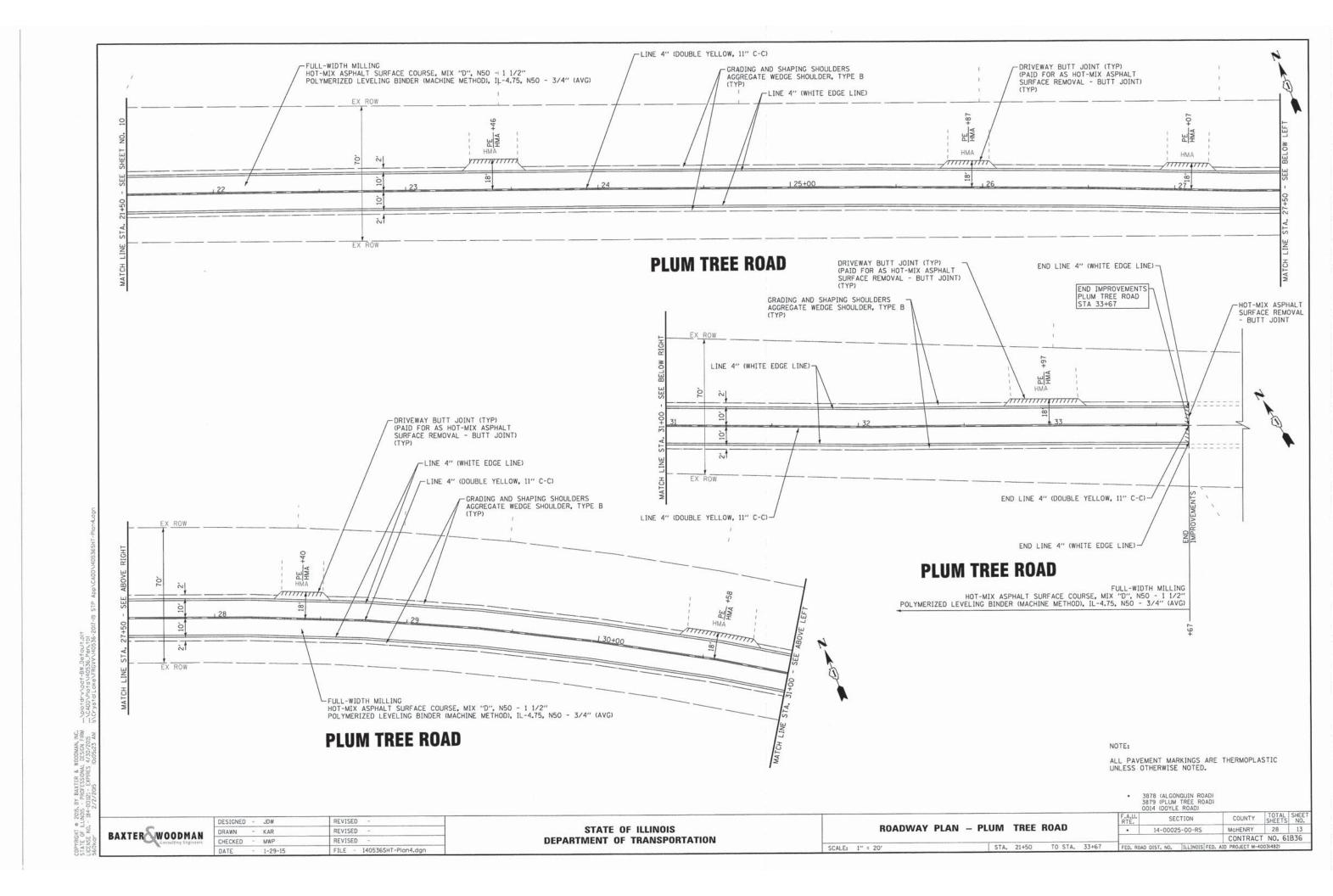
REVISED DESIGNED - JDW REVISED DRAWN - KAR BAXTER WOODMAN REVISED CHECKED FILE - 140536SHT-Plan1.dgn 1-29-15

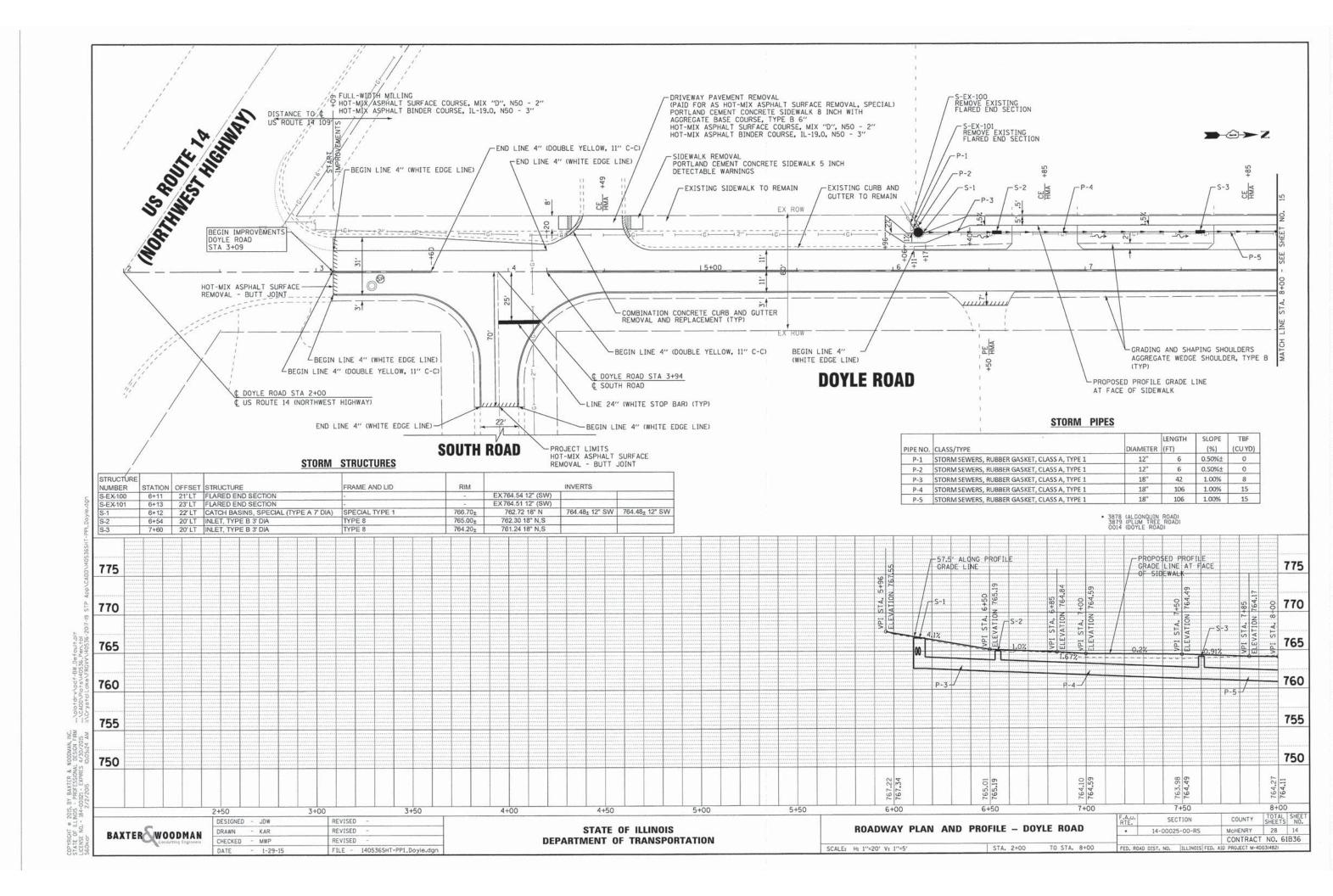
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION ROADWAY PLAN - INTERSECTION ALGONOUIN ROAD, PLUM TREE ROAD AND OLD HUNT ROAD

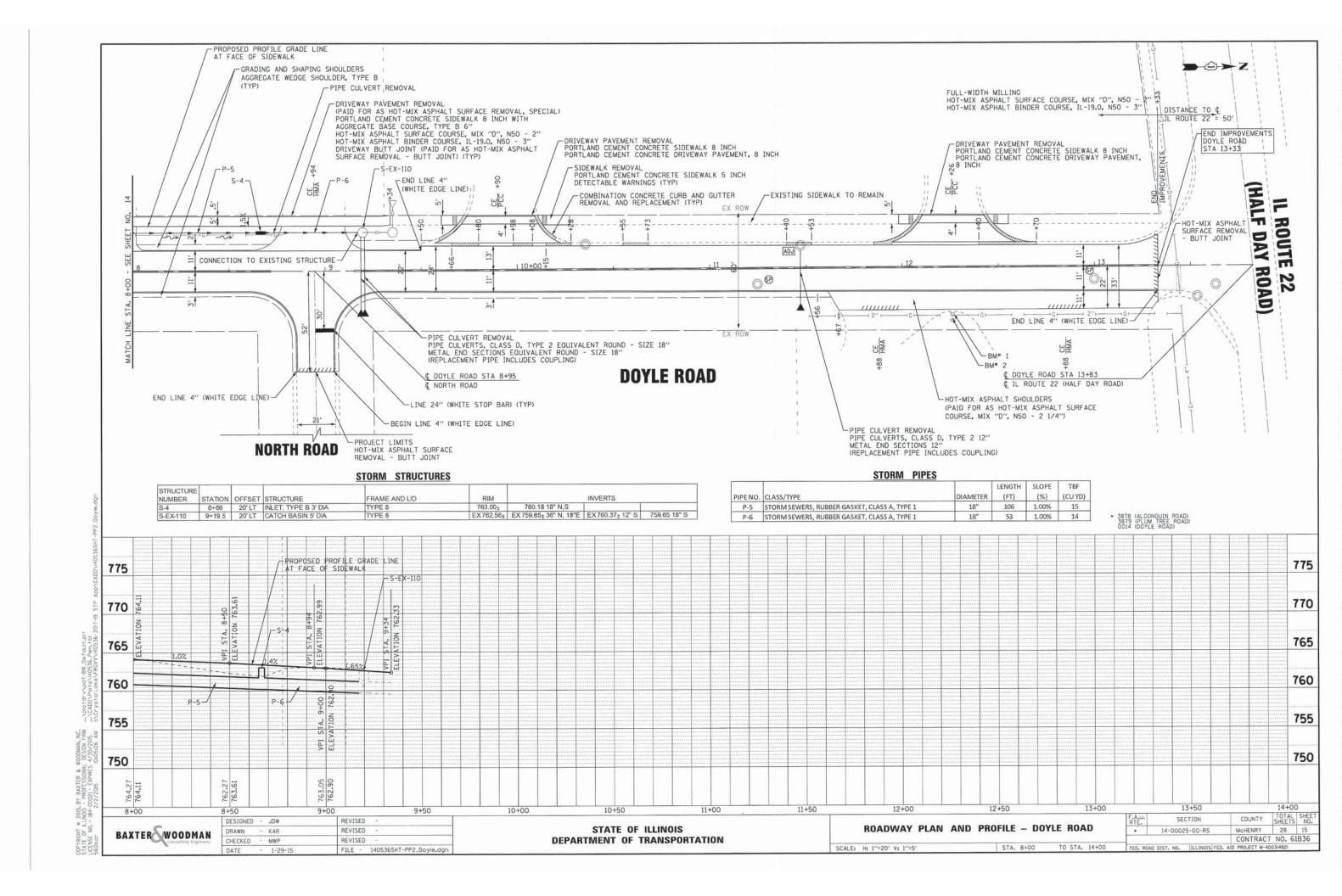
MCHENRY 28 10 14-00025-00-RS CONTRACT NO. 61B36



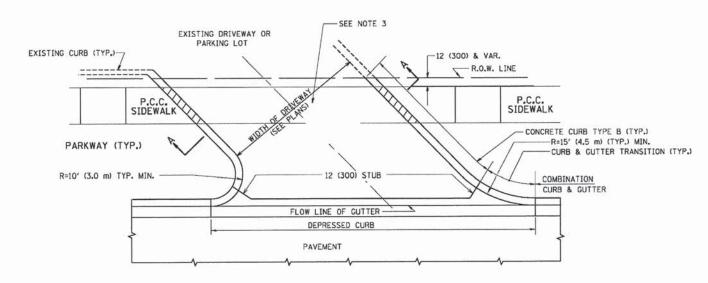




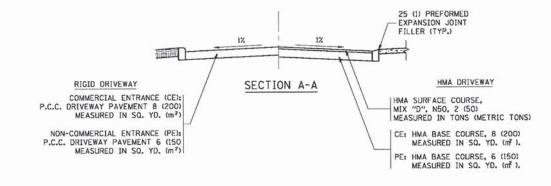


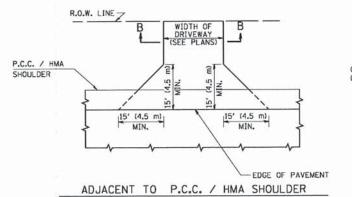


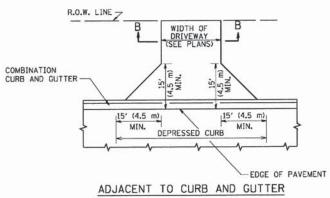
## WITH CONCRETE CURB, TYPE B

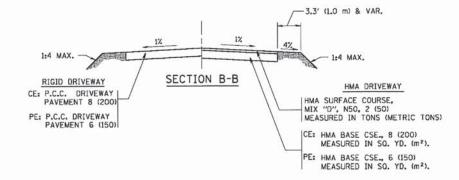


## WITH CONCRETE CURB, TYPE B









RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE, MIX "D", N50, 2 (50) MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200) MEASURED IN SQ. YD. (m²).

## GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

SCALE: NONE

3878 (ALGONQUIN ROAD)
 3879 (PLUM TREE ROAD)
 O014 (DOYLE ROAD)

| DI  | RIVEWAY | DETA  | ILS - | - DISTAN | CE BETWEE | N R.O | .W.         |
|-----|---------|-------|-------|----------|-----------|-------|-------------|
| AND | FACE OF | CUR   | 8 8   | EDGE OF  | SHOULDER  | >=    | 15' (4.5 m) |
|     | SHEET N | 10. 1 | OF 1  | SHEETS   | STA.      |       | TO STA.     |

| F.A.U.<br>RTE. | SECTION                         | COUNTY           | TOTAL<br>SHEETS | SH |  |
|----------------|---------------------------------|------------------|-----------------|----|--|
|                | 14-00025-00-RS                  | MCHENRY          | 28              | 1  |  |
|                | BD0156-07 (BD-01)               | CONTRACT         | NO. 61          | ВЗ |  |
| FED. R         | OAD DIST. NO. 1   ILLINOIS FED. | AID PROJECT M-40 | 03(482)         |    |  |

4 2 3 12 (300) MIN. 9 -6 \*\* PROPOSED BRICK, MORTAR, OR CONC. ADJUSTING RINGS 7

## NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

## CONSTRUCTION PROCEDURES

## STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

## STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\*
  CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- \* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

### LEGEND

- 1 SUB-BASE GRANULAR MATERIAL
- (6) FRAME AND LID (SEE NOTES)
- 2 EXISTING PAVEMENT
- (7) CLASS PP-1\* CONCRETE
- (3) 36 (900) DIAMETER METAL PLATE
- (8) PROPOSED HMA SURFACE COURSE
- PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- (5) EXISTING STRUCTURE
- (9) PROPOSED HMA BINDER COURSE

## LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

## BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

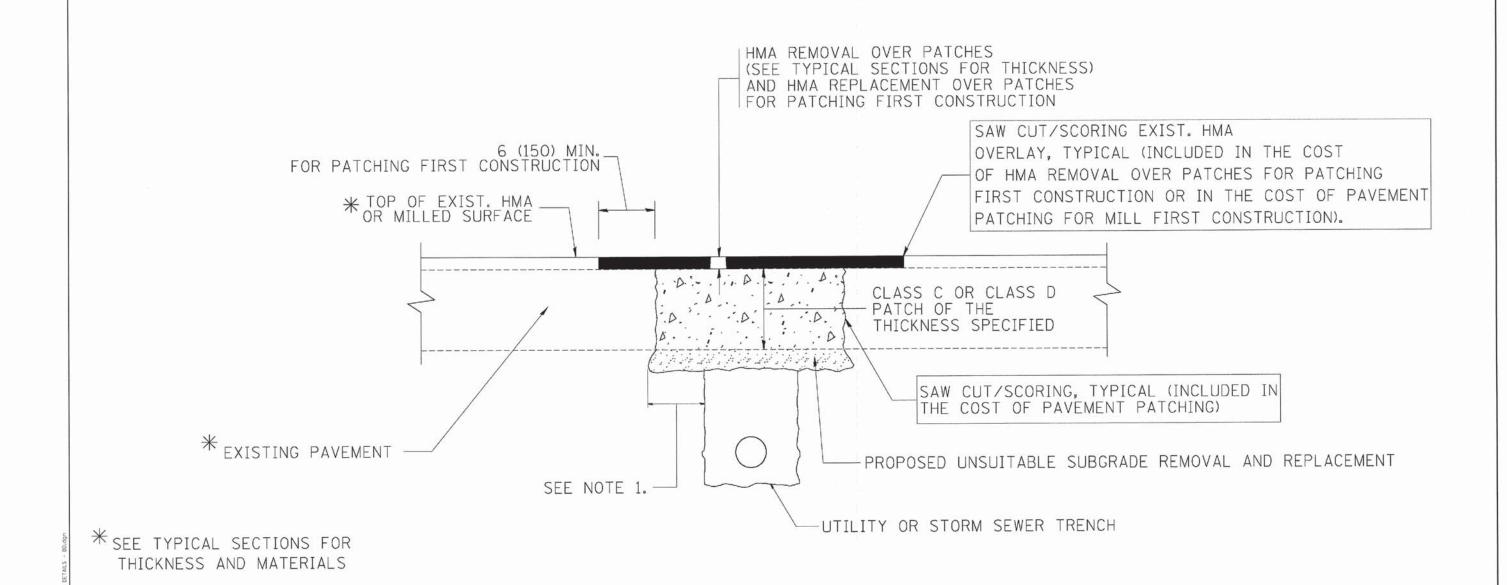
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

# DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

• 3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

COUNTY TOTAL SHEET NO. DESIGNED - R. SHAH REVISED - R. WIEDEMAN 05-14-04 SECTION USER NAME = beverd! FILE NAME = DETAILS FOR STATE OF ILLINOIS REVISED - R. BORO 01-01-07 MCHENRY DRAWN Nov. vork\nwidot\bauerd1\d2128315\bd28. FRAMES AND LIDS ADJUSTMENT WITH MILLING **DEPARTMENT OF TRANSPORTATION** BD600-03 (BD-8) CONTRACT NO. 61B36 REVISED - R. BORO 03-09-11 PLOT SCALE = 1968.5000 '/ m CHECKED SHEET NO. 1 OF 1 SHEETS STA. TO STA. ATD PROJECT M-4003(482) REVISED - R. BORO 12-06-11 PLOT DATE = 12/6/2011 DATE 10-25-94



# NOTES:

- 1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
- 2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

# SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

- 1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
- 2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
- 3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

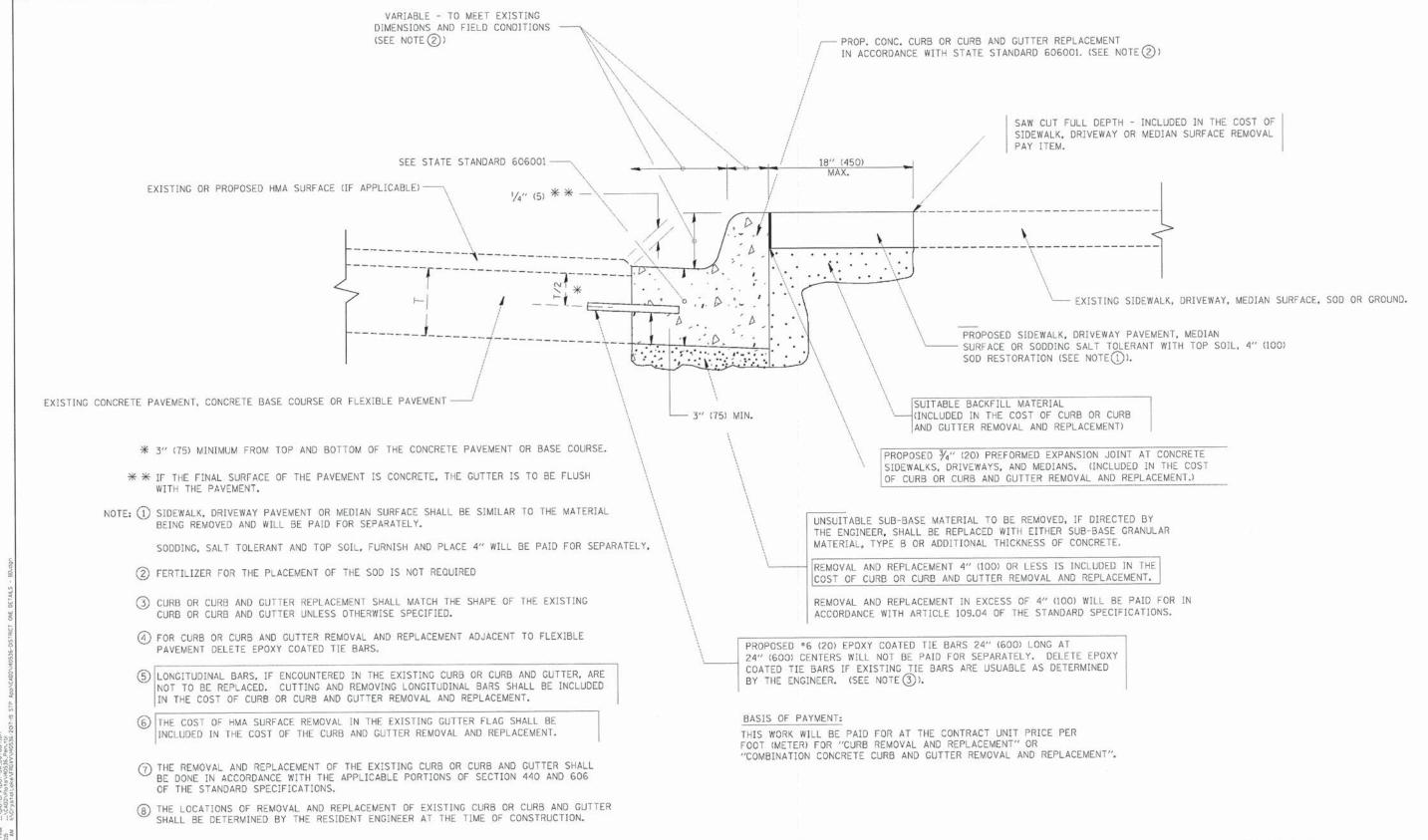
# SEQUENCE OF CONSTRUCTION (MILLING FIRST)

- 1. MILL HMA FIRST IF THERE IS AT LEAST 41/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
- 2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| 558                                   |                             |                    |                             |                              |  | F.A.U. SECTION                        | TOTAL SHEET             |
|---------------------------------------|-----------------------------|--------------------|-----------------------------|------------------------------|--|---------------------------------------|-------------------------|
| FILE NAME =                           | USER NAME = bewerdl         | DESIGNED - R. SHAH | REVISED - A. ABBAS 04-27-98 |                              | PAVEMENT PATCHING FOR                            | RTE. SECTION                          | COUNTY SHEETS NO.       |
| ci\pro.leots\diststd22x34\bd2         | 22.dgn                      | DRAWN -            | REVISED - R. BORO 01-01-07  | STATE OF ILLINOIS            | HMA SURFACED PAVEMENT                            | • 14-00025-00-RS                      | MCHENRY 28 18           |
| or .                                  | PLDT SCALE = 50.000 ' / IN. | CHECKED -          | REVISED - R. BORO 09-04-07  | DEPARTMENT OF TRANSPORTATION |  | BD400-04 (BD-22)                      | CONTRACT NO. 61B36      |
| P P P P P P P P P P P P P P P P P P P | PLOT DATE = 10/27/2008      | DATE - 10-25-94    | REVISED - K. ENG 10-27-08   |                              | SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. | FED. ROAD DIST. NO. 1   ILLINOIS FED. | AID PROJECT M-4003(482) |

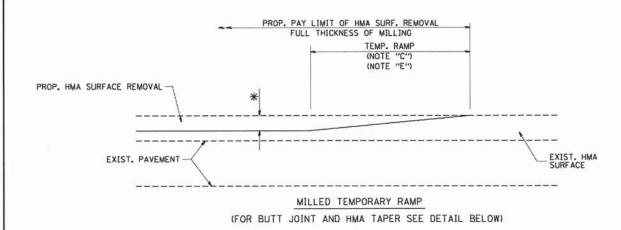


# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

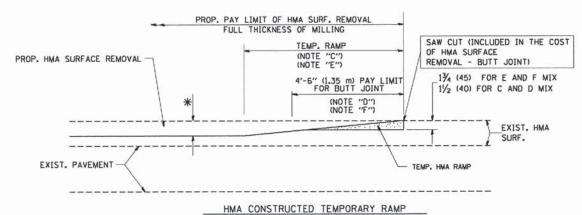
3878 (ALGONQUIN ROAD)
 3879 (PLUM TREE ROAD)
 O014 (DOYLE ROAD)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

| 84-00<br>84-00 | III KOW -                                 | 173 I- ROM I H-syskeson       | DESIGNED A. HOUSEH | REVISED R. SHAH 10-03-96    |  | CURB OR CURB AND GUTTER                          | F.A.U. SECTION                        | COUNTY TOTAL SHEET NO.  |
|----------------|---|-------------------------------|--------------------|-----------------------------|--|--|---------------------------------------|-------------------------|
| 01             | o:\px_work\pwidot\drivakosgn\d3188315\bc2 | 2/.dgn                        | DRAWN -            | REVISED - A. ABBAS 03-21-97 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | REMOVAL AND REPLACEMENT                          | • 14-00025-00-RS                      | MCHENRY 28 19           |
| P 2 2          |   | 14.1.1.1309 = 938.300 1.7.101 | CHECKED            | REVISED M. GOMEZ 01-22-01   |  |  | BD600-06 (BD-24)                      | CONTRACT NO. 61B36      |
| 955            |   | BLCT CATE = 12/15/2009        | DATE - 03-11-94    | REVISED - R. BORO 12-15-09  |  | SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. | FED. ROAD DIST. NO. 1  ILLINOIS  FED. | ALD PROJECT M-4003/4822 |



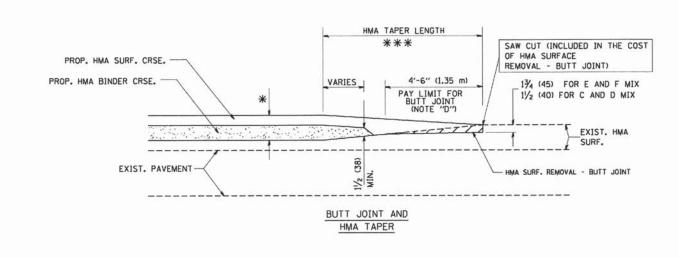
# OPTION 1



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

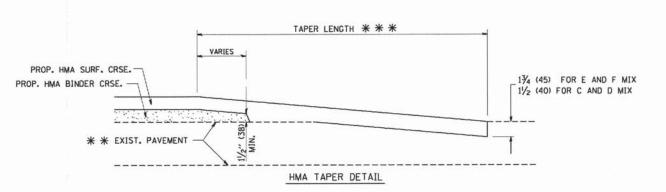
# OPTION 2

# TYPICAL TEMPORARY RAMP



# TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

PROP. HMA OR PCC SURFACE REMOVAL - BUTT JOINT SAW CUT (INCLUDED IN THE COST EXIST. HMA OR PCC SURFACE 30'-0" (9.0 m) (NOTE "A") OF HMA OR P.C.C. SURFACE REMOVAL 15'-0" (4.5 m) (NOTE "B") - BUTT JOINT) (NOTE "D") 13/4 (45) FOR E AND F MIX 11/2 (40) FOR C AND D MIX \* \* EXIST. PAVEMENT BUTT JOINT DETAIL



# TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

## NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\* \*\* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

## BASIS OF PAYMENT:

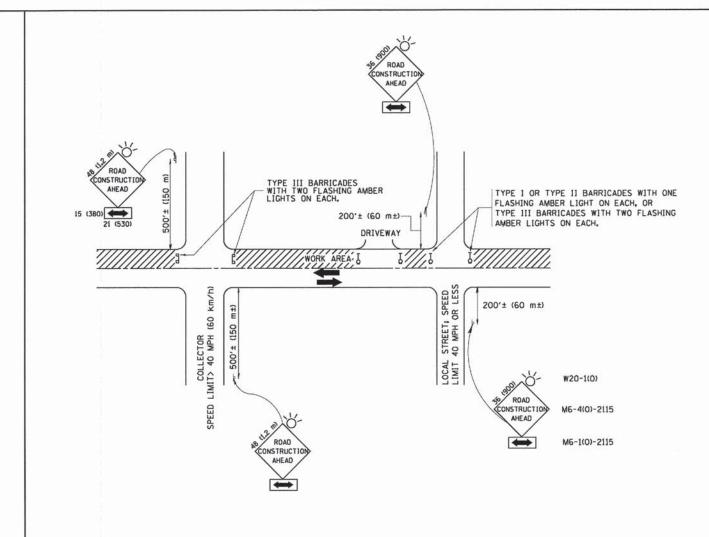
THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

- R. SHAH 10-25-94 DESIGNED - M. DE YONG FILE NAME = USER NAME = gaglianobt REVISED - A. ABBAS 03-21-97 DRAWN Wr\distate(\22v34\hel32.don PLOT SCALE = 58,0000 '/ IN. CHECKED REVISED M. GOMEZ 04-06-01 PLOT DATE = 1/4/2008 DATE 06-13-90 REVISED - R. BORO 01-01-07

|             | RUTT J              | DINT AND  |         | F.A.U.<br>RTE. | SECTION                      | COUNTY           | TOTAL   | SHEET<br>NO. |
|-------------|---------------------|-----------|---------|----------------|------------------------------|------------------|---------|--------------|
|             |                     |           |         | 14-00025-00-RS | MCHENRY                      | 28               | 20      |              |
|             | HMA TAP             |           |         | BD400-05 BD32  | CONTRACT                     | NO. 6            | IB36    |              |
| SCALE: NONE | SHEET NO. 1 OF 1 SH | EETS STA. | TO STA. | FED. RO        | AD DIST. NO. 1 ILLINOIS FED. | AID PROJECT M-40 | 03(482) |              |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

## NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- O) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE 1, TYPE 11 OR TYPE 111 BARRICADES, 1/3 OF THE CORES PROTION
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- c) ONE ROAD CONSTRUCTION AHEAD SIGN 48  $\times$  48 (1.2 m  $\times$  1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (MG-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (MG-4).

# B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

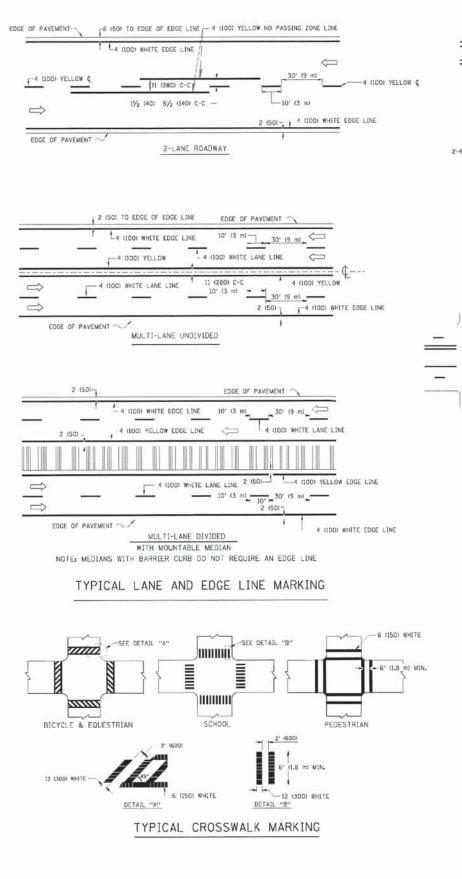
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ISTD. 701501. STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

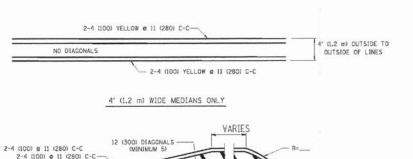
3878 (ALGONQUIN ROAD)
 3879 (PLUM TREE ROAD)
 0014 (DOYLE ROAD)

All dimensions are in millimeters (inches

DESIGNED - LHA FILE NAME = USER NAME = gaglianobt REVISED - J. OBERLE 10-18-95 TRAFFIC CONTROL AND PROTECTION FOR STATE OF ILLINOIS MCHENRY 28 //\diststd\22x34\to10.dgn DRAWN REVISED - A. HOUSEH 03-06-96 14-00025-00-RS SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS CONTRACT NO. 61B36 CHECKED REVISED A. HOUSEH 10-15-96 DEPARTMENT OF TRANSPORTATION TC-10 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT M-4003(482 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. PLOT DATE = 1/4/2008 DATE 06-89 REVISED -T. RAMMACHER 01-06-00

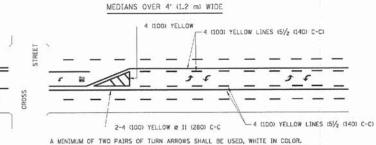
יים שני מיים במונים לא במונים לא במונים במונים לא במונים לא מונים במונים לא מיים מונים לא מיים מונים לא מיים מ מיים לא מיים ל





FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

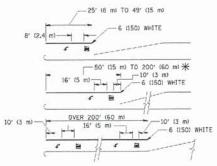


ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

## TYPICAL PAINTED MEDIAN MARKING

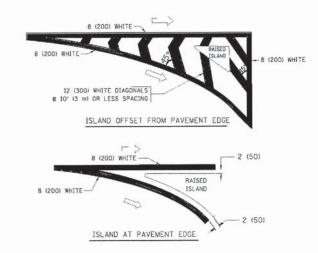


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m2 ) (MLY AREA = 20.8 SQ. FT. (1.9 m2)

\*\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

# TYPICAL TURN LANE MARKING



# TYPICAL ISLAND MARKING

| TYPE OF MARKING   | WIDTH OF LINE   | PATTERN                 | COLOR   | SPACING / REMARKS  |
|---|---|-------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT   | 4 (100)   | SKIP-DASH               | YELLOW  | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT   | 2 8 4 (100)   | SOLID                   | YELLOW  | 11 (280) C-C   |
| NO PASSING ZONE LINES:<br>FOR ONE DIRECTION<br>FOR BOTH DIRECTIONS                                | 4 (100)<br>2 a 4 (100)  | SOLID<br>SOLID          | YELLOW<br>YELLOW  | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE<br>11 (280) C-C<br>OMIT SKIP-DASH CENTERLINE BETWEEN   |
| LANE LINES  | 4 (100)<br>5 (125) ON FREEWAYS  | SKIP-DASH<br>SKIP-DASH  | WHITE<br>WHITE  | 10' (3 m) LINE WITH 30' (9 m) SPACE  |
| DOTTED LINES<br>EXTENSIONS OF CENTER, LANE OR<br>TURN LANE MARKINGS)                              | SAME AS LINE BEING EXTENDED   | SKIP-DASH               | SAME AS LINE BEING EXTENDED                             | 2' (600) LINE WITH 6' (1.8 m) SPACE  |
| EDGE LINES  | 4 (100)   | SOL10                   | YELLOW-LEFT<br>WHITE-RIGHT                              | OUTLINE MOUNTABLE MEDIANS IN<br>YELLOW: EDGE LINES ARE NOT<br>USED NEXT TO BARRIER CURB  |
| TURN LANE MARKINGS  | 6 (150) LINE; FULL<br>SIZE LETTERS &<br>SYMBOLS (8' (2.4m))   | SOL10                   | WHITE   | SEE TYPICAL TURN LANE MARKING DETAIL   |
| TWO WAY LEFT TURN MARKING   | 2 B 4 (100)<br>EACH DIRECTION   | SKIP-DASH<br>AND SOLID  | YELLOW  | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE  |
|   | 8' (2.4m) LEFT ARROW  | IN PAIRS                | WHITE   | SEE TYPICAL TWO-WAY LEFT TURN<br>MARKING DETAIL  |
| CROSSWALK LINES (PEDESTRIAN)<br>A. DIAGONALS (BIKE & EQUESTRIAN)<br>B. LONGITUDINAL BARS (SCHOOL) | 2 % 6 (150)<br>12 (300) % 45°<br>12 (300) % 90°   | SOLID<br>SOLID<br>SOLID | WHITE<br>WHITE<br>WHITE                                 | NOT LESS THAN 6' (1.8 m) APART<br>2' (500) APART<br>2' (500) APART<br>SEE TYPICAL CROSSWALK MARKING DETAILS.   |
| STOP LINES  | 24 (600)  | SOLID                   | WHITE   | PLACE 4' (1/2 m) IN ADVANCE OF AND PARALLEL TO (ROSSMALK, IF PRESENT. DIMERMISC, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS   | 2 m 4 (100) WITH<br>12 (300) DIAGONALS<br>m 45°<br>NO DIAGONALS USED FOR<br>4' (1,2 m) WIDE MEDIANS | SOLID                   | YELLOW:<br>TWO WAY TRAFFIC<br>WHITE:<br>ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE<br>SEE TYPICAL PAINTED MEDIAN MARKING.  |
| GORE MARKING AND<br>CHANNELIZING LINES  | 8 (200) WITH 12 (300)<br>DIAGONALS @ 45°  | SOLID                   | WHITE   | DIAGONALS:<br>15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h))<br>20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))<br>30' (9 m) C-C (OVER 45MPH (70 km/h))             |
| RAILROAD CROSSING   | 24 (GOO) TRANSVERSE<br>LINES: "RR" IS 6' (1.8 m)<br>LETTERS: 16 (400)<br>LINE FOR "X"               | S0L10                   | WHITE   | SEE STATE STANDARD 780001<br>AREA OF:<br>"R"3.6 SQ. FT. (0.33 m²) EACH<br>"X"54.0 SQ. FT. (5.0 m²)   |
| SHOULDER DIAGONALS  | 12 (300) © 45°  | SOL1D                   | WHITE - RIGHT<br>YELLOW - LEFT                          | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))<br>75' (25 m) C-C (30 MPH (50 km/h) 10 45MPH (70 km/h)<br>150' (45 m) C-C (0VER 45MPH (70 km/h))                        |

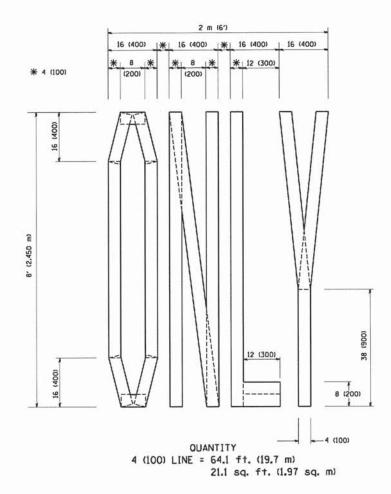
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

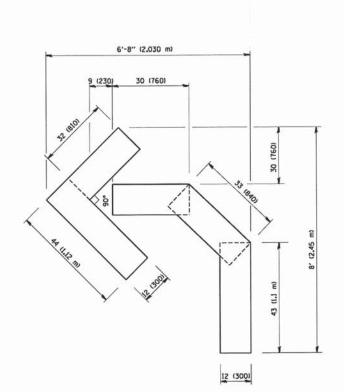
All dimensions are in inches (millimeters)

• 3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD)

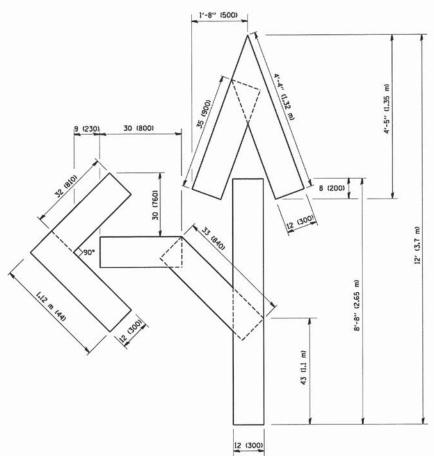
REVISED T. RAMMACHER 10-27-94 DESIGNED EVERS BE NAME = REVISED -C. JUCIUS 09-09-09 DRAWN :\px\_work\pwidct\drivakozon\d3lE8315\to|3.dgn CHECKED REVISED DATE 03-19-90 REVISED

|             | DISTRICT               | ONE              |         | F.A.U.<br>RTE. | SECTION                     | COUNTY          | TOTAL    | SHEET<br>NO. |
|-------------|------------------------|------------------|---------|----------------|-----------------------------|-----------------|----------|--------------|
|             |                        |                  |         | 14-00025-00-RS | MCHENRY                     | 28              | 22       |              |
|             | TYPICAL PAVEMEN        | TC-13 CONTRACT N |         |                | NO. 6                       | io. 61B36       |          |              |
| SCALE: NONE | SHEET NO. 1 OF 1 SHEET | STA.             | TO STA. | FED. ROA       | D DIST. NO. 1 ILLINOIS FED. | AID PROJECT M-4 | 003(482) |              |





OUANTITY 4 (100) LINE = 45.5 ft. (13.9 m) 15.2 sq. ft. (1.39 sq. m)



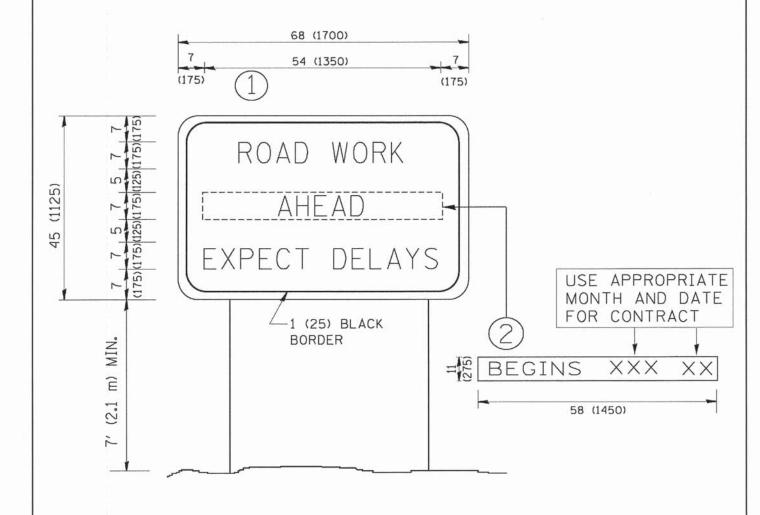
QUANTITY 4 (100) LINE = 82.5 ft. (25.3 m) 27.5 sq. ft. (2.53 sq. m)

3878 (ALGONQUIN ROAD)
 3879 (PLUM TREE ROAD)
 0014 (DOYLE ROAD)

All dimensions are in inches (millimeters) unless otherwise shown.

| FILE NAME =               | USER NAME = gaglianobt       | DESIGNED -      | REVISED -T. RAMMACHER 06-05-96 |
|---------------------------|------------------------------|-----------------|--------------------------------|
| Wi\diststd\22×34\to16.dgn |                              | DRAWN -         | REVISED -T. RAMMACHER 11-04-97 |
|                           | PLOT SCALE = 58.0800 ' / IN. | CHECKED -       | REVISED -T. RAMMACHER 03-02-98 |
|                           | PLOT DATE = 1/4/2009         | DATE - 09-18-94 | REVISED -E. GOMEZ 08-28-00     |

|  | PAVEMENT I | MARKIN | G LETTE | RS AND SY | MBOLS | F.A.U.<br>RTE. | SECTION                   | COUNTY             | TOTAL<br>SHEETS | SHEET<br>NO. |
|--|------------|--------|---------|-----------|-------|----------------|---------------------------|--------------------|-----------------|--------------|
|  |            |        |         |           |       |                | 14-00025-00-RS            | MCHENRY            | 28              | 23           |
| FOR TRAFFIC STAGING                              |            |        |         |           |       |                | TC-16                     | CONTRACT NO. 61B36 |                 |              |
| SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. |            |        |         |           |       | FED. ROAL      | DIST. NO. 1 ILLINOIS FED. | AID PROJECT M-40   | 03(482)         |              |



# NOTES:

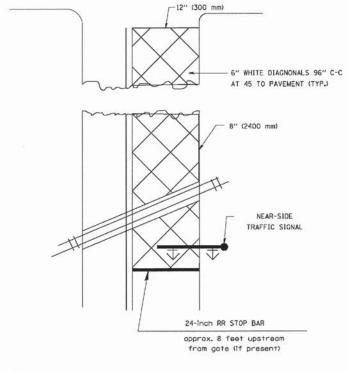
- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

- 3878 (ALGONQUIN ROAD)
3879 (PLUM TREE ROAD)
0014 (DOYLE ROAD)
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

| 2014<br>INOIS<br>184- | FILE NAME =  | USER NAME = gaglianobt   | DESIGNED -                      | REVISED - R, MIRS 09-15-97   |                   | ARTERIAL ROAD    |                              | F       | RTE.   | SECTION        | COUNTY             | TOTAL SHEETS | EET |
|-----------------------|--|--|---------------------------------|------------------------------|-------------------|------------------|------------------------------|---------|--|----------------|--------------------|--------------|-----|
| NO. E. e.             | Wi\diststd\22x34\to22.dgn  |  | DRAWN -                         | REVISED - R. MIRS 12-11-97   | STATE OF ILLINOIS |                  |                              |         |  | 14-00025-00-RS | MCHENRY            | 28           | 24  |
| SE OF                 | Security of the Control of the Contr | PLOT SCALE = 58.0000 '/ IN. CHECKED - REVISED -T. RAMMACHER 02-02-99 | REVISED - T. RAMMACHER 02-02-99 | DEPARTMENT OF TRANSPORTATION |                   | INFORMATION SIGN |                              |         | TC-22  |                | CONTRACT NO. 61B36 |              |     |
| STAT                  |  | PLOT DATE = 1/4/2008   | DATE -                          | REVISED - C. JUCIUS 01-31-07 |                   | SCALE: NONE      | SHEET NO. 1 OF 1 SHEETS STA. | TO STA. | FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT M. |                |                    | 1003(482)    |     |

# WITH NEAR-SIDE TRAFFIC SIGNALS

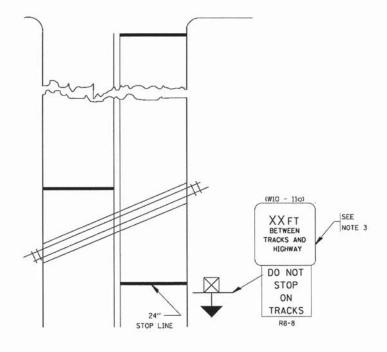
(SEE NOTE 1 & 2)



PLAN

# WITH NONSIGNALIZED INTERSECTION

81' (25 m) OR LESS TO CLOSEST RAIL



PLAN N. T. S

### NOTES:

- PAVEMENT MARKINGS TO BE INSTALLED ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- 2. WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- 3. DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK. WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF

THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6-FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

3878 (ALGONQUIN ROAD)
 3879 (PLUM TREE ROAD)
 OO14 (DOYLE ROAD)

MCHENRY 28 25

CONTRACT NO. 61B36

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

REVISED - 02-25-11 DESIGNED FILE NAME = USER NAME = drivakosan REVISED - 04-26-12 DRAWN CHECKED REVISED DATE PLOT DATE = 5/7/2012

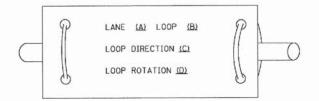
STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** 

SECTION TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING 14-00025-00-RS TREATMENT FOR RAILROAD CROSSINGS TC-23 SHEET NO. 1 OF 1 SHEETS STA. SCALE: NONE

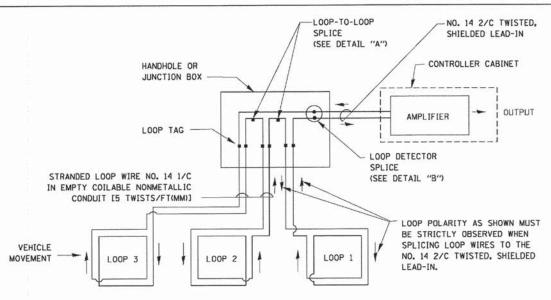
OF 1, LINDIS F NO. - 184-0

- 1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- 2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- 6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

## LOOP LEAD-IN CABLE TAG

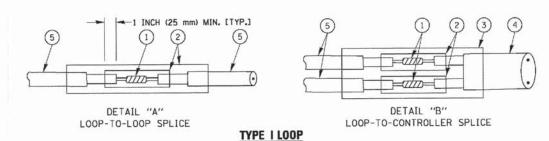


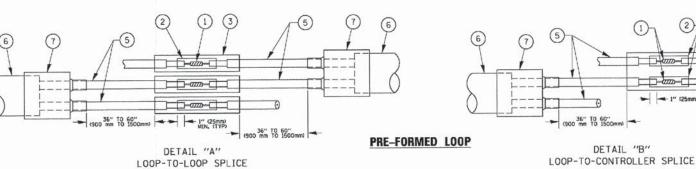
- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP "1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

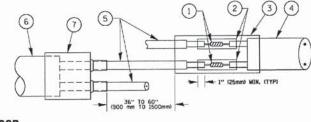


## **DETECTOR LOOP WIRING SCHEMATIC**

- . LOOPS SHALL BE SPLICED IN SERIES.
- \* SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- " SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.







DETAIL "B"

LOOP DETECTOR SPLICE

- 1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- (2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- (3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.

- (5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- (6) PRE-FORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

3878 (ALGONQUIN ROAD) 3879 (PLUM TREE ROAD) 0014 (DOYLE ROAD)

| ILE NAME =                              | USER NAME = factorj         | DESIGNED | +   | DAD      | REVISED - DAG 1-1-14 |   |
|---|-----------------------------|----------|-----|----------|----------------------|---|
| ::\pw_wark\pwidot\footomj\d0108315\ts05 | i.dgn                       | DRAWN    |     | ВСК      | REVISED -            |   |
|   | PLUT SCALE = 50.0000 '/ in- | CHECKED  | 100 | DAD      | REVISED -            |   |
|   | PLOT DATE = 1/13/2014       | DATE     | +   | 10-28-09 | REVISED -            | 1 |

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** 

|             | nis              | TRICT ON | IF.  | F.A.U.<br>RTE.     | SECTION  | COUNTY  | TOTAL   | SHEET<br>NO. |    |  |
|-------------|------------------|----------|------|--------------------|----------|---|---------|--------------|----|--|
|             |                  |          |      |                    |          | 14-00025-00-RS  | MCHENRY | 28           | 26 |  |
|             | STANDARD TRAFFIC | SIGNAL   |      | CONTRACT NO. 61B36 |          |   |         |              |    |  |
| SCALE: NONE | SHEET NO. 2 OF 7 | SHEETS   | STA. | TO STA.            | FED. ROA | FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT M-4003(482) |         |              |    |  |

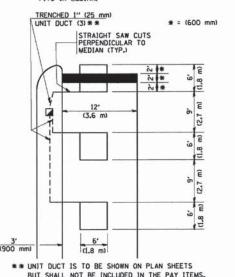
OF ILLINOIS

# PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER. PAYED OR NON-PAVED SHOULDER PAYED OR NON-PAVED SHOULDER \*\* = (600 mm) \*\* \*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

# LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

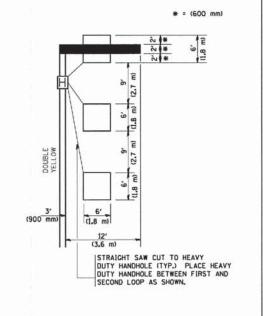
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO

# LEFT TURN LANES WITHOUT MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

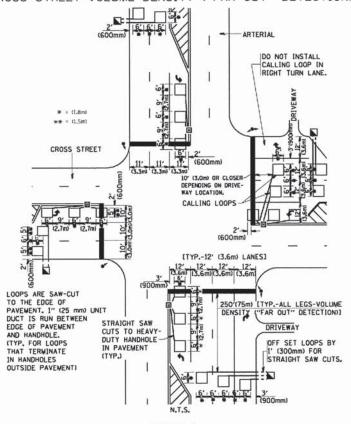


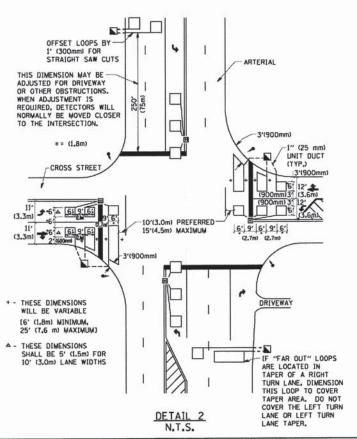
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

SCALE: NONE

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)





NOTES:

VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX, EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (I.e. 1-1/2, 1-3/4, 2).
- \*\* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. <u>EACH</u> ONE OF THESE TYPE OF LOOPS REQUIRES A <u>SEPARATE</u> TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A <u>SEPARATE</u> INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN, WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

## PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON  $\underline{\mathsf{ALL}}$  SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

### NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE 1.D.O.T. DISTRICT 1
TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

3878 (ALGONQUIN ROAD 3879 (PLUM TREE ROAD

COUNTY

CONTRACT NO. 61B36

MCHENRY

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

10:07:08 AM \\corp.baxwood.com\Projects\Cr

MS - PROFESSIONAL DESIGN FRM ... Npiotdrylpc 4-001121 - EXPRES 4/30/2015 ... NCADD/Plots 2/2/2015 10:07:08 AM NYCOFD. DAXWG

HT & 2014, BY BAXTER & W OF RLINGS - PROFESSIONAL NO. - 184-001121 - EXPIRES

