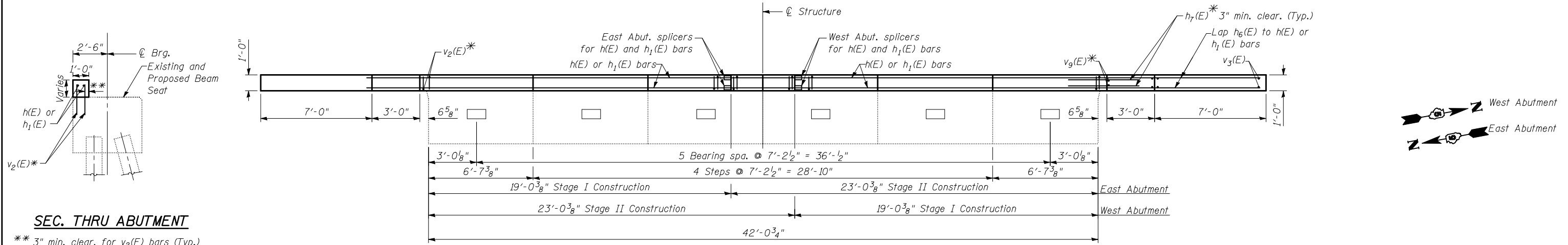
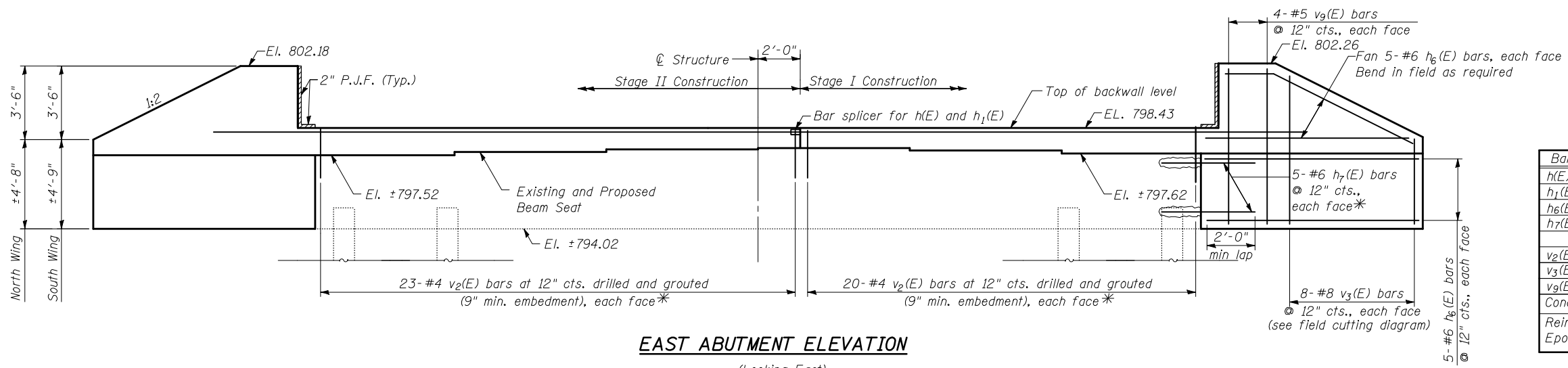


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



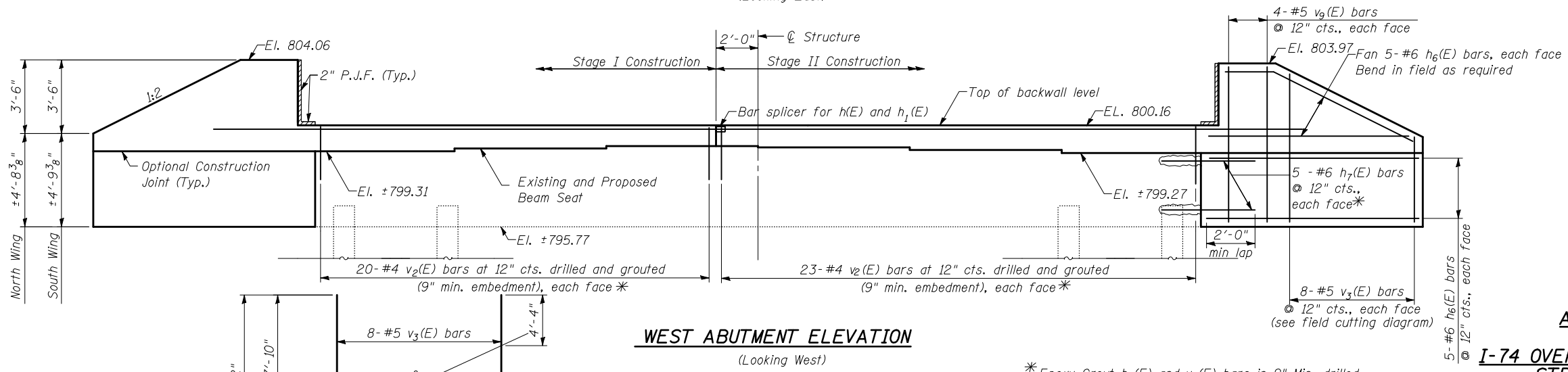
SEC. THRU ABUTMENT

** 3" min. clear. for v₂(E) bars (Typ.)



EAST ABUTMENT ELEVATION

(Looking East)

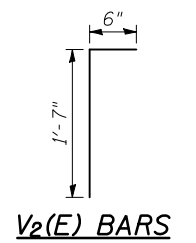


WEST ABUTMENT ELEVATION

(Looking West)

BILL OF MATERIALS

Bar	No.	Size	Length	Shape
h(E)	4	#7	23'-1"	—
h ₁ (E)	4	#7	27'-1"	—
h ₆ (E)	80	#6	10'-2"	—
h ₇ (E)	40	#6	3'-0"	—
v ₂ (E)	172	#4	2'-1"	—
v ₃ (E)	32	#5	12'-2"	—
v ₉ (E)	32	#5	7'-10"	—
Concrete Structures		CU YD	14	
Reinforcement Bars, Epoxy Coated		LB	2,720	



v₂(E) BARS

ABUTMENT DETAILS WESTBOUND

I-74 OVER LITTLE KICKAPOO CREEK STRUCTURE NO. 057-0121

STRAND ASSOCIATES, INC.

Notes:
Where new concrete is to be poured against existing, roughen existing concrete to 1/4" minimum amplitude and apply Bonding Agent. Cost shall be included with Concrete Structures.

*Epoxy Grout h₇(E) and v₂(E) bars in 9" Min. drilled holes according to Article 584 of the Standard Specifications

DESIGNED	RRD
CHECKED	AJS
DRAWN	KAS
CHECKED	JAR

FIELD CUTTING DIAGRAM

Order v₃(E) full length. Cut as shown and use remainder of bars in opposite face.

SHEET NO. 21 27 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	74	(57-21B)BR	MCLEAN	61	33
	ABUT. CAP AND WINGWALLS (2 OF 2)		CONTRACT NO. 70641		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			