ROUTE	SECTION	COUNTY	TOTAL SHEETS	
VARIOUS	2009-024 PP	COOK	33	
		······································		

# STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS DISTRICT ONE PROPOSED HIGHWAY PLANS

FOR INDEX OF SHEETS SEE SHEET 2

VARIOUS ROUTES
SECTION: 2009-024 PP
VARIOUS LOCATIONS IN NORTH COOK COUNTY
INTERMITTENT PAVEMENT RESURFACING
PROJECT: ESP-000\$ (GSI)
COOK COUNTY
C-91-354-09

CONTRACT NO. 60G23

D-91-354-09

McHenry Lake

DuPage

Cook

SHEET NUMBER

**CONTRACT NO. 60G23** 

LOCATION OF IMPROVEMENT INDICATED THUS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED

FEBRUARY 5, 2009

DEPLITY DIRECTOR OF H

OF HIGHWAYS, REGION ENGINEE

Charles J. Ingus

Christine M. Reed D

DIRECTOR DIVISION OF HIGHWAY

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

J.U.L.I.E.: JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION (312) 744-7000

DISTRICT ONE – DESIGN – PLAN PREPARATION ENGINEER: KEN ENG / (847) 705-4247

#### INDEX OF SHEETS

SHEET NO.	<u>DESCRIPTION</u>
1	TITLE SHEET
2	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	GENERAL LOCATION MAP
5	SUMMARY OF PATCHING SCHEDULE
6-25	PATCHING SCHEDULE
26	BUTT JOINT AND HMA TAPER DETAILS
27	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS
28	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
29	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
30	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
31	ARTERIAL ROAD INFORMATION SIGN
32	STANDARD TRAFFIC SIGNAL DESIGN DETAILS
33	DETECTOR LOOP INSTALLATION DETAIL FOR ROADWAY RESURFACING
	STATE STANDARDS
STANDARD NO.	DESCRIPTION
000001 - 05	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS

701306-02 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS - DAY ONLY

701336-05 LANE CLOSURE, 2L, 2W, WORK AREAS IN SERIES

701501-05 URBAN LANE CLOSURE, 2L. 2W. UNDIVIDED

701601-03 URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN

701606 W URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN

701701-06 URBAN LANE CLOSURE, MULTILANE INTERSECTION

701901-4\ TRAFFIC CONTROL DEVICES

#### GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE (OR TOLLWAY) PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT (OR ISTHA)

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS. THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE ENGINEER SHALL CONTACT MR. WALLY CZARNY. AREA TRAFFIC FIELD ENGINEER AT (773) 685-8386 MINIMUM OF TWO (2) WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE EXISTING ROADWAY TYPICAL SECTION IS ASSUMED TO HAVE A 3 INCH HOT-MIX ASPHALT OVERLAY ON TOP OF A TEN INCH CONCRETE BASE.

ALL PAVEMENT PATCHES SHOWN IN THE PLANS ARE TWO (2) INCH MILL AND RESURFACE ONLY. THE MINIMUM WIDTH FOR MILLING AND PATCHING SHALL BE TWO (2) FEET.

THE COST OF TRAFFIC CONTROL AND PROTECTION FOR THE PROJECT SHALL BE INCLUDED IN THE COST OF THE ASSOCIATED ROAD WORK.

THE COST OF ANY PARTIAL OR FULL DEPTH PATCHING REQUIRED AFTER THE REMOVAL OF THE EXISTING 2 INCH HOT-MIX ASPHALT SURFACE SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

#### HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AC TYPE	AIR VOIDS (%)
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N7O (IL 9.5MM), 2"	PG 64-22	4% <b>©</b> 70 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -			F.A. SECTION COUNTY SHEETS NO
c:\pw_work\PWIDDT\SMITHKL\dØ125Ø91\Des	dgn DRAWN - REVISED -		STATE OF ILLINOIS	INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	VAR. 2009-024 PP COOK 33 2	
	PLOT SCALE = 100.0000 / IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	Z MACH (AM)	CONTRACT NO. 60G23
	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT

VAR. 2009-024 PP COOK 33 3 FED. ROAD DIST. NO. 1 ILLINOIS HIGHWAY PROJECT	F.A RT	:	SECTION	١		COUNT	Y	TOTAL SHEETS	SHEET NO.
FED. ROAD DIST. NO. 1 ILLINOIS HIGHWAY PROJECT	VAF		2009-024	PP		COOK		33	3
	FE	D. ROAD	DIST. NO.	. 1	ILL	INOIS	HIG	HWAY PRO	JECT

CONTRACT NO. 60G23

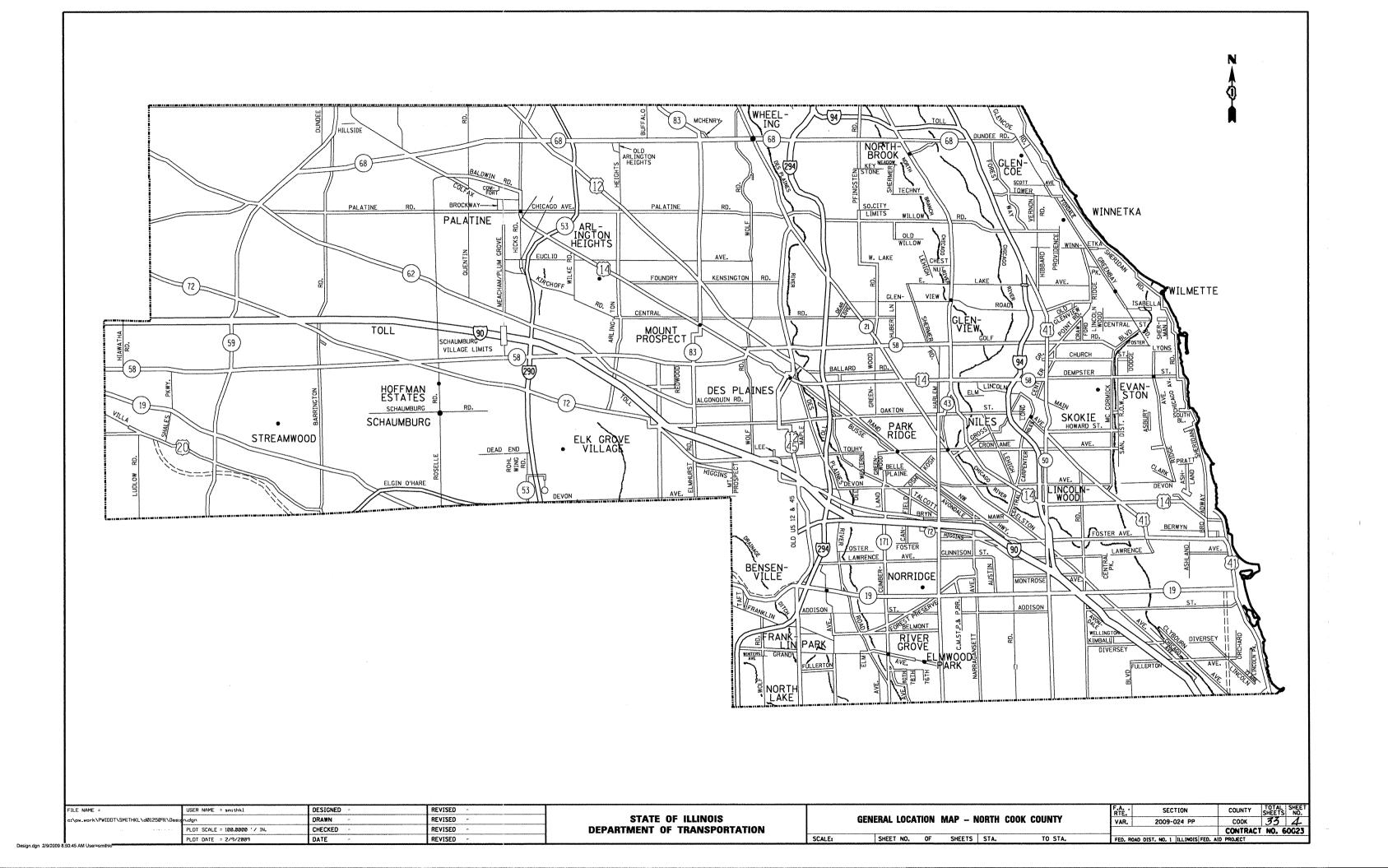
	SUMMARY OF QUANTITIES			ļ	CONST	RUCTION	TYPE CODE	<del> </del>			SUMMA	ARY OF QUANTITIE	S		1	CONSTRUCT	ON TYPE CO	ODE
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	URBAN 100 % FED 1000-2A						CODE NO		ITEM	UNIT	TOTAL QUANTITIES				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	19	19														
40600300	AGGREGATE (PRIME COAT)	TON	91	91														
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	136	136														
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	1807	1807														
	HOT-MIX ASPHALT SURFACE COURSE.	TON	5058	5058														
44000157	HOT-MIX ASPHALT SURFACE REMOVAL. 2"	SO YD	45156	45156														
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6														
67100100	MOBILIZATION	L SUM	1	1														
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	10091	10091														
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	3364	3364														
€ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	140	140														
÷ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	55500	55500														
€ <b>78000400</b>	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	800	800														
€ <b>78000600</b>	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	150	150														
€ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	150	150														
¢ 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	850	850														
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	850	850														
e 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1300	1300														
x0322256	TEMPORARY INFORMATION SIGNING	SO FT	1131	1131														
30076680	TRAINEES	HOUR	1500	1500														
																	.	
							1		l									

· 4080

ILLINOIS	REVISIONS							
	DATE	HAVE						
SUMM								
		,						

MARY OF QUANTITIES

PLOT DATE: 2/9/2009



SUMMARY - NORTH COOK COUNTY	HMA 2" MILL & RESURFACE (SY)
CALDWELL AVE (DEVON AVE. TO LINCOLN AVE.)	12578
DEMPSTER ST. (I-294 TO GREENWOOD RD.)	3333
GOLF RD. (I-294 TC) WAUKEGAN RD.)	2112
GREENWOOD RD. (WESTLAKE AVE. TO GOLF RD.)	2444
HIBBARD RD. (ILLINOIS AVE. TO SKOKIE BLVD.)	1071
IL 53 EAST FRONTAGE RD. (PALATINE RD. TO RAND RD.)	2145
IL 53 EAST FRONTAGE RD. (RAND RD. TO IL 68)	641
IL 53 WEST FRONTAGE RD. (PALATINE RD. TO RAND RD.)	1442
IL 53 WEST FRONTAGE RD. (RAND RD. TO IL 68)	1015
IL 68 (BARRINGTON RD. TO GLENCLEST DR.)	127
IL 68 (STERLING AVE. TO KNOLLWOOD DR.)	228
IL 68 (1000 FT. W/O SMITH ST. TO ST. MARKS PL.)	150
IL 72 EB (0.1 MILE E/O MEACHAM RD. TO 0.2 MILE E/O MEACHAM RD.)	35
OAKTON ST. (ELMHURST RD. TO 0.1 MILE W/O ELMHURST RD.)	3854
TOUHY AVE. (MCCORMICK BLVD. TO KEDZIE AVE.)	382
WESTLAKE AVE. (GREENWOOD RD. TO PFINGSTEN RD.)	732
WILLOW RD. (WAUKEGAN RD. TO LANDWEHRE RD.)	2228
WOLF RD. (MILWAUKEE AVE. TO PALATINE RD.)	916
CHURCH ST. (GRCSS POINT RD. TO MCCORMICK BLVD.)	2023
LINCOLN AVE. (DEVON AVE. TO TOUHY AVE.)	2400
MILWAUKEE AVE. (GOLF RD. TO PALATINE RD.)	4200
RAND RD. (WOLF RD. TO ELK BLVD)	1100
SUMMARY TOTALS:	45156 SY

FILE NAME :	USER NAME ≈ smithkl	DESIGNED -	REVISED -		PATCHING SCHEDULE		F.A. SECTION	COUNTY TOTAL SHEET NO.
c:\pw_work\PWIDOT\SMITHKL\dØ125091\Desi	gn,dgn	DRAWN -	REVISED -	STATE OF ILLINOIS		VAR. 2009-024 PP	соок 33 5	
The analysis of the	PLOT SCALE = 100.0000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	SUMMARY OF PATCHING SCHEDU			CONTRACT NO. 60G23
]	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE: SHEET NO. OF SHEETS STA.	TO STA.	FED. ROAD DIST. NO.   ILLINOIS FED. A	ND PROJECT

# ROUTE: Caldwell between Devon and Lincoln

*************************************	SS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
DEVON	LEHIGH	EB	2	0	0	0	0
LEHIGH	CENTRAL	EB	2	12	25	300	33
CENTRAL	LENOX	EB	2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
			2	12	25	300	33
			2	12	50	600	67
			2	12	100	1200	133
			2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
***************************************			2	12	200	2400	267
LENOX	1-94	EB	2	12	100	1200	133
h bull 471			2	12	200	2400	267
			2	12	300	3600	400
I-94	CICERO	EB	2	0	0	0	0
CICERO	GLENN FOREST	EB	<del></del>	12			
GICERO	GLENN FOREST	ED	2		50	600	67
<del></del>			2	12	200	2400	267
			2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
			2	12	200	2400	267
			2	12	200	2400	267
GLENN FOREST	KOSTNER	EB	2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
KOSTNER	ROGERS	EB	2	12	250	3000	333
			2	12	50	600	67
			2	12	100	1200	133
ROGERS	PULASKI	EB	2	12	25	300	33
***************************************			2	12	25	300	33
PULASKI	CENTRAL PARK	EB	2	12	25	300	33
· · · · · · · · · · · · · · · · · · ·			2	12	25	300	33
	· ·		2	12	50	600	67
			2	12	25	300	33
			2	12	100	1200	133
			2	12	. 25	300	33
			2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
CENTRAL PARK	KIMBAL	EB	2	0	0	0	0
KIMBAL	KEDZIE	EB		<del> </del>			
KEDZIE			2	0	0	0	0
VENTIC	LINCOLN	EB	2	12	50	600	67
LINCOLN	IVED 31E		<del> </del>	<del>                                     </del>		0	0
LINCOLN	KEDZIE	WB	2	12	25	300	33
KEDZIE	KIMBAL	WB	2	12	25	300	33
	021/221/		2	2	100	200	22
KIMBAL	CENTRAL PARK	WB	2	12	25	300	33
CENTRAL PARK	PULASKI	WB	2	12	25	300	33
	·		2	12	25	300	33
			2	12	25	300	33
			2	12	25	300	33
			2	12	25	300	33
			2	12	50	600	67
			2	12	25	300	33
PULASKI	ROGERS	WB	2	12	25	300	33
			2	12	250	3000	333
			2	12	25	300	33
ROGERS	KOSTNER	WB	2	12	200	2400	267
			2	2	200	400	44
KOSTNER	CICERO	WB	2	12	200	2400	267
			2	12	50	600	67
			2	12	50	600	67
	<del></del>		<del></del>				
			2	2 12	500 200	1000 2400	111 267
				1 77	. 200	77(1)()	1 7h/

# ROUTE: Caldwell between Devon and Lincoln

	SS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
CICERO	1-94	WB	2	12	50	600	67
-94	LENOX	WB	2	12	200	2400	267
			2	12	50	600	67
ENOX	CENTRAL	WB	2	12	50	600	67
~~~~			2	12	50	600	67
			2	12	200	2400	267
			2	12	200	2400	267
			2	12	50	600	67
			2	12	50	600	67
			2	12	50	600	67
,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,			2	12	150	1800	200
			2	12	200	2400	267
			2	12	200	2400	267
			2	12	200	2400	267
CENTRAL	LEHIGH	WB	2	0	0	0	0
LEHIGH	DEVON	WB	2	12	50	600	67
		<u> </u>	<del>                                     </del>	<del> </del>		0	0
DEVON	LEHIGH	EB	1	0	0	0	0
LEHIGH	CENTRAL	EB	1	0	0	0	0
CENTRAL	LENOX	EB	1	0	0	0	0
LENOX	I-94	EB	<del></del>	12			67
I-94	CICERO		1		50	600	
		EB EB	1	0	0	0	0
CICERO	KOSTNER	EB	1	2	100	200	22
			1	12	25	300	33
			1	12	100	1200	133
			11	2	100	200	22
KOSTNER	ROGERS	EB	1	2	100	200	22
ROGERS	PULASKI	EB	1	2	200	400	44
PULASKI	CENTRAL PARK	EB	11	12	25	300	33
			1	12	25	300	33
			1	2	100	200	22
			1	2	100	200	22
			1	2	100	200	22
CENTRAL PARK	KIMBAL	EB	1	2	100	200	22
			1	12	50	600	67
KIMBAL	KEDZIE	EB	1	2	100	200	22
			1	12	50	600	67
KEDZIE	LINCOLN	EB	1 1	0	0	0	0
			<del> </del>				
LINCOLN	KEDZIE	WB	1 1	2	50	100	11
		<del>                                     </del>	<del>                                     </del>	12	50	600	67
KEDZIE	KIMBAL	WB	1 1	2	50	100	11
		<u> </u>	1 1	12	25	300	33
KIMBAL	CENTRAL PARK	WB	<del> </del>	2	50	100	11
	JERTITULE FAIRE	770	<del>   </del>	12	25	300	33
CENTRAL PARK	PULASKI	WB	1 1	2	50	100	11
OF MINUTE LAUVE	FULAGIN	- VVD	1 1	12	200	2400	267
PULASKI	ROGERS	WB	1	12	25	300	33
LOLAUN	NOGLING	VVD	<del>                                     </del>		50	600	67
ROGERS	KOSTNER	WB	1	12			
NOGLING	NOSTNEK	- VVD	<del></del>	12	200	2400	267
KOCTNED	FOREST OLENN	14/6	1 1	2	100	200	22
KOSTNER	FOREST GLENN	WB	1 1	2	200	400	44
CICEBO	LENOY		1 1	12	300	3600	400
CICERO	LENOX	WB	1 1	2	100	200	22
I THOU	- LOENTE A		1 1	12	25	300	33
LENOX	CENTRAL	WB	11	2	100	200	22
			11	12	50	600	67
CENTRAL	LEHIGH	WB	11	0	0	0	0
LEHIGH	DEVON	WB	1	2	100	200	22
		1	1	12	50	600	67

TOTALS: 11600 12578 FT SY

FILE NAME :	USER NAME = smithkl	DESIGNED -	REVISED -		PATCHING SCHEDULE	F.A. SECTION COUNTY	TOTAL SHEET
c:\pw_work\PWIDOT\SMITHKL\dØ125Ø91\Desig	n,dgn	DRAWN -	REVISED -	STATE OF ILLINOIS		VAR. 2009-024 PP COOK	33 (c)
	PLOT SCALE = 103.7051 ' / IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	CALDWELL AVE.		T NO. 60G23
L	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO.   ILLINOIS FED. AID PROJECT	110. 00023

# ROUTE: DEMPSTER ST BETWEEN I-294 AND GREENWOOD

CRO	OSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
GREENWOOD	WESTERN	WB	2	12	50	600	67
WESTERN	LUTHER	WB	2	12	400	4800	533
LUTHER	DEE	WB	2	12	50	600	67
			2	2	50	100	11
DEE	POTTER	WB	2	12	50	600	67
			2	12	25	300	33
			2	2	50	100	11
POTTER	1-294	WB	2	12	50	600	67
			2	2	250	500	56
						0	0
1-294	POTTER	EB	2	12	100	1200	133
			2	2	100	200	22
			2	12	25	300	33
			2	12	25	300	33
	••••••••••••••••••••••••••••••		2	12	25	300	33
		······································	2	12	25	300	33
			2	12	25	300	33
POTTER	LUTHER	EB	2	12	50	600	67
			2	2	50	100	11
LUTHER	WESTERN	EB	2	12	200	2400	267
			2	2	50	100	11
WESTERN	GREENWOOD	EB	2	12	50	600	67
			2	12	50	600	67
			2	12	250	3000	333
			2	2	200	400	44
						0	0
GREENWOOD	LUTHER	WB	1	12	25	300	33
			1	12	25	300	33
LUTHER	POTTER	WB	1	12	50	600	67
			1	2	200	400	44
POTTER	I-294	WB	1	12	100	1200	133
						0	0
I-294	POTTER	EB	1	12	200	2400	267
			1	2	200	400	44
POTTER	LUTHER	EB	1	12	25	300	33
			1	2	200	400	44
LUTHER	WESTERN	EB	1	12	25	300	33
	· · · · · · · · · · · · · · · · · · ·		1	12	25	300	33
			1	2	300	600	67
WESTERN	GREENWOOD	EB	1	12	100	1200	133
			1 1	12	200	2400	267

TOTAL

3875 FT

	FILE NAME :	USER NAME = smithkl	DESIGNED ~	REVISED -	
1	c:\pw.work\PWIDDT\SMITHKL\dØ125Ø91\Desig	n.dgn	DRAWN -	REVISED -	STATE OF
1		PLOT SCALE = 103.7051 ' / IN.	CHECKED -	REVISED -	DEPARTMENT OF
		PLOT DATE = 2/9/2009	DATE -	REVISED -	

STATE	OF	ILLINOIS
DEPARTMENT	OF	TRANSPORTATION

	PA'	TCHING	SCH	EDULE			F.A. RTE.	
		DEMPS	TER :	ST.			VAR.	
SHEET	NO. O	F SHE	ETS	STA.	TO	STA.	FED. R	OAD DIST.

	SECTION	COUNTY	SHEETS	NO.
_	2009-024 PP	COOK	33	7
_		CONTRACT	NO. 6	<b>0</b> G23
=				

# ROUTE: GOLF RD. (I-294 to WAUKEGAN RD.)

CROSS	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
I-294 (EB)	REDING ST.	EB	1	12	12	144.0	16.0
" "	11	EB	2	12	6	72.0	8.0
" "	11	EB	1,2	24	6	144.0	16.0
		EB EB	2	12 24	4	48.0 96.0	5.3 10.7
"	11	EB	1,2 1	12	4	48.0	5.3
ш	11	EB	1	12	4	48.0	5.3
H	11	EB	CL.	30	2	60.0	6.7
H H	11	EB	1	12	4	48.0	5.3
19	11	EB	1,2	24	4	96.0	10.7
н <sub>.</sub>	"	EB	1,2	12	4	48.0	5.3
	tr.	EB	1	12	4	48.0	5.3
"	11	EB	1	12	4	48.0	5.3
	"	EB	1	12	4	48.0	5.3
11	41	EB	1	12	12	144.0	16.0
······································	#	EB	1	12	4	48.0	5.3
11	11	EB	CL	25	2	50.0	5.6
11	11	EB	1	50	12	600.0	66.7
I-294 (EB)	REDING ST.	EB	2	12	4	48.0	5.3
LINÇOLN ST. (EB)	HARLEM AVE	EB	1,2,3	36	4	144.0	16.0
"	11	EB	1,2,3	36	4	144.0	16.0
II.	II .	EB	1,2	24	4	96.0	10.7
"	11	EB	1	12	4	48.0	5.3
11	11	ĒB	2,3	24	4	96.0	10.7
"	1)	EB	2,3	24	4	96.0	10.7
II	II .	EB	2,3	24	4	96.0	10.7
It	II .	EB	3	12	4	48.0	5.3
" .	ll .	EB	2,3	24	4	96.0	10.7
II .	н	EB	1	12	4	48.0	5.3
10	и	EB	2,3	24	4	96.0	10.7
"	u	EB	1	12	4	48.0	5.3
U	q	EB	LT,1	24	4	96.0	10.7
II .	и	EB	2,3	24	10	240.0	26.7
LINCOLN ST. (EB)	HARLEM AVE	EB	1,2,3	36	4	144.0	16.0
HARLEM AVE (EB)	ALLSTATE ENTRANCE	EB	2,3	24	4	96.0	10.7
11	н	EB	1	12	4	48.0	5.3
11	и	EB	2,3	24	4	96.0	10.7
1t	li .	EB	1	12	4	48.0	5.3
II .	ts	EB	2,3	24	4	96.0	10.7
11	"	EB	1,2,3	36	4	144.0	16.0
н	"	EB	1	8	4	32.0	3.6
н	r ·	EB	1	12	4	48.0	5.3
11	"	EB	1,2,3	36	4	144.0	16.0
ii	"	EB	2,3	24	4	96.0	10.7
11	11	EB	1,2,3	36	4	144.0	16.0
fl .	. #	EB	1,2,3	65	4	260.0	28.9
ff .	11,	EB	2,3	24	4	96.0	10.7
11	11	EB	1	12	4	48.0	5.3
it	*	EB	2,3	24	4	96.0	10.7
"	"	EB	LT,1	24	4	96.0	10.7
11	11	EB	2,3	24	4	96.0	10.7
и	н	EB	2,3	24	4	96.0	10.7
11	"	EB .	1	12	4	48.0	5.3
H	11	EB	2,3	24	4	96.0	10.7
11	н	EB	2,3	24	4	96.0	10.7
li .	11	EB	1,2,3	36	4	144.0	16.0
HARLEM AVE (EB)	ALLSTATE ENTRANCE	EB	2,3	24	4	96.0	10.7
LLSTATE ENTRANCE	WAUKEGAN RD.	EB	1	12	4	48.0	5.3
tt		EB	1	12	8	96.0	10.7
11	"	EB	2	12	8	96.0	10.7
!!	и .	EB	2,3	24	4	96.0	10.7
11	II .	EB	1	12	6	72.0	8.0
11	11	EB	1,2,3	36	4	144.0	16.0
11	11	EB	1,2,3	36	4	144.0	16.0
"	II .	EB	1,2,3	36	4	144.0	16.0
u	II.	EB	1	12	4	48.0	5.3
	"	EB	2,3	24	4	96.0	10.7
11	H	EB	1,2,3	36	4	144.0	16.0
II				1 40	4	48.0	5.3
If	11	EB	3	12			
tr tr	. "	EB	2	12	8	96.0	10.7
# # # # # # # # # # # # # # # # # # #	0 0	EB EB	2 2,3	12 24	8 4	96.0 96.0	10.7 10.7
tr tr	. "	EB	2	12	8	96.0	10.7

# ROUTE: GOLF RD. (I-294 to WAUKEGAN RD.)

FROM	STREETS TO	DIRECTION (EB/WB)	LANE NO.	PAVEMENT PATCH	PAVEMENT PATCH	REPAIR AREA	REPAIR AREA
i NOW	10	(EB/WB) (NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
HARIEM AVE (ED)	ALLSTATE ENTRANCE	<del></del>		<del></del>			
HARLEM AVE (EB) ALLSTATE ENTRANCE	~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~	EB	1 2	12	4	48.0	5.3
TLLOIA I E EN IKANUE	WAUKEGAN RD.	EB	2	12	4	48.0	5.3
		EB	3	12	4	48.0	5.3
"	"	EB	2	12	8	96.0	10.7
	11	EB	1	12	4	48.0	5.3
		EB	2,3	24	4	96.0	10.7
	"	EB	LT,1	24	4	96.0	10.7
		EB	1,2,3	36	4	144.0	16.0
ALLSTATE ENTRANCE	WAUKEGAN RD.	EB	LT	12	4	48.0	5.3
WAUKEGAN RD.	ALLSTATE ENTRANCE	WB	1,2,3	36	4	144.0	16.0
"	11	WB	1,2,3	36	4	144.0	16.0
" "	"	WB	2,3	24	4	96.0	10.7
"	"	WB	1,2,3	36	. 4	144.0	16.0
" "	"	WB	11	12	4	48.0	5.3
"	"	WB	3	12	4	48.0	5.3
"		WB	1	12	4	48.0	5.3
	11	WB	2,3	24	4	96.0	10.7
11	11	WB	1,2	24	4	96.0	10.7
11	11	WB	3	12	6	72.0	8.0
	1	WB	1,2	24	4	96.0	10.7
	11	WB	1	12	4	48.0	5.3
"	11	WB	2	12	4	48.0	5.3
"	11	WB	1	12	4	48.0	5.3
"	"	WB	2	12	4	48.0	5.3
11	11	WB	1,2,3	36	4	144.0	16.0
"	11	WB	1,2,3	36	4	144.0	16.0
"	п	WB	1	12	4	48.0	5.3
11	n	WB	2,3	24	4	96.0	10.7
11	н	WB	1	12	4	48.0	5.3
11	rt	WB	2,3	24	4	96.0	10.7
II .	n	WB	2,3	24	4	96.0	10.7
WAUKEGAN RD.	ALLSTATE ENTRANCE	WB	1	12	4	48.0	5.3
ALLSTATE ENTRANCE	HARLEM AVE	WB	2,3	24	4	96.0	10.7
It	11	WB	1	24	4	96.0	10.7
11	"	WB	2,3	12	4	48.0	5.3
и	11	WB	1	24	4	96.0	10.7
"	"	WB	2,3	12	4	48.0	5.3
li .	11	WB	1	24	4	96.0	10.7
íı	"	WB	2,3	12	4	48.0	5.3
II .	II.	WB	2,3	24	4	96.0	10.7
ıı .	11	WB	1	24	4	96.0	10.7
"	II II	WB	2,3	36	4	144.0	16.0
11	11	WB	1	24	6	144.0	16.0
11	n	WB	2,3	12	5	60.0	6.7
11	li li	WB	1,2,3	24	4	96.0	10.7
ıı .	11	WB	2,3	12	4	48.0	5.3
li'	11	WB	2,3	24	4	96.0	10.7
11	"	WB	LT,1	36	6	216.0	24.0
	11	WB	1	8	4	32.0	3.6
II .	1)	WB	2,3	12	4	48.0	5.3
11		WB	2,3	36	4	144.0	16.0
"	It	WB	1,2,3	24	4	96.0	10.7
"	"	WB	1 1	12	4	48.0	5.3
ALLSTATE ENTRANCE	HARLEM AVE	WB	2,3	24	4	96.0	10.7
HARLEM AVE	WASHINGTON ST.	WB	1,2	2	24	48.0	5.3
I IVITE IN VAC	WASTON HARD LONG ST.	WB	1,2,3	36	4	144.0	16.0
11		WB	1,2,3	36	4	144.0	16.0
	# The state of the	WB		36	4	144.0	16.0
11	"	WB	1,2,3	36	4	144.0	16.0
WASHINGTON ST.	LINCOLN ST.	WB	1,2,3	24	4	96.0	10.7
WASHINGTON ST.	LINCOLN ST.	WB	2,3	12	4		5.3
			<del></del>	·	<del> </del>	48.0	
	"	WB	2,3	24	4	96.0	10.7
"	11	WB	1,2	24	4	96.0	10.7
		WB	2	12	4	48.0	5.3
WASHINGTON ST.	LINCOLN ST.	WB	1,2,3	36	5	180.0	20.0
REDING ST.	l-294	WB	1,2	24	8	192.0	21.3
"	"	WB	1,2	24	200	4800.0	533.3
	11	WB	1,2	24	4	96.0	10.7
	11	WB	1,2	24	4	96.0	10.7
	11	WB	1,2	24	4	96.0	10.7
11		<del></del>			<del></del>		
" REDING ST.	" I-294	WB WB	1,2 1,2	2 2	200 100	400.0 200.0	44.4 22.2

TOTALS 1152 2112 FT SY

FILE NAME = DESIGNED -REVISED PATCHING SCHEDULE c:\pw\_work\PWIDOT\SMITHKL\dØ125091\Design.dgn DRAWN -STATE OF ILLINOIS REVISED GOLF RD. PLOT SCALE = 103.7051 '/ IN. CHECKED -REVISED DEPARTMENT OF TRANSPORTATION PLOT DATE = 2/9/2009 REVISED SCALE: SHEET NO. OF SHEETS STA. TO STA.

# ROUTE: Greenwood Rd. (Westlake Ave. to Golf Rd.)

CRO	OSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
GOLF	GLENVIEW	NB	1	2	200	400	44
			1	12	25	300	33
			1	12	25	300	33
GLENVIEW	EASTLAKE	NB	1	12	25	300	33
			1	12	150	1800	200
			1	12	25	300	33
			1	2	400	800	89
EAST LAKE	WEST LAKE	NB	1	12	200	2400	267
			1	2	200	400	44
WEST LAKE	EAST LAKE	SB	11	12	50	600	67
			1	12	50	600	67
			11	12	100	1200	133
			11	12	100	1200	133
			1	2	200	400	44
GLENVIEW	CENTRAL	SB	1	12	50	600	67
			1	12	25	300	33
			11	2	200	400	44
			1	12	25	300	33
			1	12	25	300	33
			1	12	25	300	33
CENTRAL	MILWAUKEE	SB	1	12	50	600	67
			1	12	50	600	67
			1	12	50	600	67
		·	1	12	50	600	67
			1	2	200	400	44
MILWAUKEE	GOLF	SB	1	12	50	600	67
			1	12	100	1200	133
			1	2	100	200	22
GOLF	MILWAUKEE	NB	2	12	25	300	33
			2	12	25	300	33
MILWAUKEE	CENTRAL	NB	2	2	100	200	22
GLENVIEW	EAST LAKE	NB	2	12	50	600	67
			2	12	50	600	67
			2	2	100	200	22
EAST LAKE	WEST LAKE	NB	2	0	0	0	0
WEST LAKE	EAST LAKE	SB	2	0	0	0	. 0
EAST LAKE	GLENVIEW	SB	2	0	0	0	0
GLENVIEW	CENTRAL	SB	2	0	0	0	0
CENTRAL	MII_WAUKEE	SB	2	12	25	300	33
			2	12	50	600	67
			2	2	100	200	22
MILWAUKEE	GOLF	SB	2	2	200	400	44
·····			2	12	25	300	33

TOTALS:

3500 FT

FILE NAME =	USER NAME = smithkl	DESIGNED ~	REVISED -			PATCHING SCHEDULE	F.A. SECTION COUNTY TOTAL SHEET
c:\pw_work\PWIDOT\SMITHKL\dØ125091\Desig	n.dgn	DRAWN ~	REVISED -	STATE OF ILLINOIS			VAR. 2009-024 PP COOK 33 9
· ·	PLOT SCALE = 103.7051 ' / IN.	CHECKED -	REVISED ~	DEPARTMENT OF TRANSPORTATION		GREENWOOD RD.	CONTRACT NO. 60G23
	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE:	SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO.   ILLINOIS FED. AID PROJECT

# ROUTE: Hibbard Road - Skokie Road to Illinois Avenue

CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Skokie Rd	Lake Avenue	SB	1	12	30	360	40
		NB	1	12	74	888	99
		NB	1	12	17	204	23
		C/L	-	2	174	348	39
		SB	1	12	6	72	8
		NB	1	12	6	72	8
		NB	1	12	12	144	16
		NB	1	12	58	696	77
		SB	1	12	22	264	29
		C/L	-	2	176	352	39
		NB	1	12	21	252	28
		SB	1	12	35	420	47
		NB	1.	12	55	660	73
		SB	1	12	17	204	23
		NB	1	12	17	204	23
		C/L	-	2	100	212	22
		NB	1	12	7	84	9
		NB	1	12	2	24	3
		SB	1	12	2	24	3
		NB	1	12	19	228	25
		NB	1	12	2	24	3
		SB	1	12	2	24	3
·		C/L	-	2	300	600	67
		NB	1&2	2	53	106	12
		SB	1&2	2	53	106	12
		NB	1	12	10	120	13
		NB	1	12	10	120	13
		SB	1	12	10	120	13
Lake Avenue	Walden St.	C/L	-	2	590	1180	131
		NB	1	12	6	72	8
		NB	1	12	6	72	8
		SB	1	12	6	72	8
,		SB	2	12	6	72	8
		NB	1	12	2	24	3
		NB	1	12	4	48	5
*****		SB	1	12	4	48	5
		NB	1	12	4	48	5
Walden St.	Sprucewood	C/L	-	2	350	700	78
		NB	1	12	10	120	13
		NB	1	12	4	48	5
		SB	1	12	4	48	5
Sprucewood	Illinois Ave.	C/L	-	2	84	168	19

TOTALS:

2370 FT

	FILE NAME :	USER NAME = amrthkl	DESIGNED ~	REVISED -			1	DATCHI	ING SC	ueniii e		F.A.	SECT	ION	COUNTY	TOTA	SHI
	c:\pw_work\PW[DOT\SMITHKL\d0125091\Desig	n.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS		•					VAR.	2009-0	24 PP	COOK	33	17
ı		PLOT SCALE = 103.7051 '/ IN.	CHECKED ~	REVISED -	DEPARTMENT OF TRANSPORTATION			HIE	BBARD	KU.					CONTRAC		
		PLOT DATE = 2/9/2009	DATE ~	REVISED -		SCALE:	SHEET NO.	<b>OF</b>	SHEETS	STA.	TO STA.	FED. ROAD	DIST. NO.	LLINOIS FED. AL			

# ROUTE: IL 53 East Fromtage Rd: Palatine Rd. to Rand Rd.

CROSS	S STREETS	DIREC	TION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM.	ТО	(EΒ/\	(EB/WB)		PATCH	PATCH	AREA	AREA
		(NB/	SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Raleigh	Woods	NB	В	1	12	10	120	13
		SB	В	1	12	10	120	13
		NB	В	1	12	100	1200	133
		SB	В	1	12	100	1200	133
·		SB	В	1	12	300	3600	400
		NB	В	1	12	30	360	40
		NB	В	1	12	20	240	27
		SB	В	1	12	100	1200	133
		NB	В	1	12	220	2640	293
		SB	В	1	12	320	3840	427
Woods	Rand	NB	В	1	12	150	1800	200
		SB	В	1	12	250	3000	333

TOTALS:

1610 FT

2145 SY

# ROUTE: IL 53 East Frontage: Rand to IL Rte 68

CRO	OSS STREETS	DIREC	TION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/\	NB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/	SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Rand Intersection	Cellular	NB	В	1	12	200	2400	267
		SB	В	1 .	12	200	2400	267
Shure	IL Rte 68	NB	В	1	12	6	72	8
		NB	В	1	12	6	72	8
		NB	В	1	12	12	144	16
		NB	В	1	12	10	120	13
		NB	В	1	12	6	72	8
		SB	В	1	12	15	180	20
		SB	В	1	12	6	72	8
		SB	В	1	12	14	168	19
		SB	В	1	12	6	72	8

TOTALS:

481 FT

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -				DATCH	ING SCH	ENIII E		F.A. PTF	SECTION	COUNTY	TOTAL	SHEET
c:\pw_work\PWIDOT\SMITHKL\dØ125091\Desig	n.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS							VAR. 20	009-024 PP	соок	33	11
	PLOT SCALE = 103.7051 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		ı	IL 53 E.	FRONTA	GE RD.		1		CONTRACT	T NO. 6	OG23
	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD DIST. N	NO. ILLINOIS FED.	ND PROJECT		

# ROUTE: IL Rte 53 West Frontage from IL 68 to Rand Rd

CRC	CROSS STREETS				PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/V	VB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/S	SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
IL Rte 68	Carol	NB	В	1	12	80	960	107
		SB	В	1	12	80	960	107
Rand Intersection	Rand	SB	В	1	12	200	2400	267
		NB	В	1	12	200	2400	267
		SB	В	2	12	200	2400	267

TOTALS:

760 FT 1015 SY

ROUTE: IL 53 West Frontage: Rand Rd to Palatine Rd.

CRO	OSS STREETS	DIREC	TION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/	VB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/	SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Rand Rd	Limit of Conc Pavement	SB	В	1	12	12	144	16
		SB	В	1	12	20	240	27
		NB	В	1	12	100	1200	133
		SB	В	1	12	15	180	20
		NB	В	1	12	100	1200	133
		NB	В	1	12	80	960	107
		SB	В	1	12	60	720	80
		SB	В	1	12	60	720	80
		NB	В	1	12	110	1320	147
		NB	В	1	12	80	960	107
		SB	В	1	12	120	1440	160
		NB	В	1	12	100	1200	133
	ì	NB	В	1	12	200	2400	267
		SB	В	1	12	12	144	16
		SB	В	1	12	6	72	8
		NB	В	1	12	6	72	8

TOTALS:

1081 FT

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -			n	DATCUIR	NO CON	ENUIE		F.A.	SECTION	COUNTY	TOTAL SHE
c:\pw_work\PWIDOT\SMITHKL\dØ125091\Desi	n.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS	PATCHING SCHEDULE IL 53 W. FRONTAGE RD.			VAR.	2009-024 PP	COOK	33 /2			
	PLOT SCALE = 103.7051 '/ IN.	CHECKED -	REVISED ~	DEPARTMENT OF TRANSPORTATION				·····	2002 02		T NO. 6002			
	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD D	IST. NO. ILLINOIS FEE	. AID PROJECT	

# ROUTE: Route Name IL 68 ( 1/4 mile east of Barrington Rd to Glencrest)

			·	<del></del>	<del></del>		
CRC	SS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
	· · ·	(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
1/4 mile East of		EB	1	12	6	72	8.0
Barrington Rd	·		1	12	6	72	8.0
			1	12	5	60	6.7
			1	12	6	72	8.0
			1	12	8	96	10.7
			1	12	8	96	10.7
			1	12	6	72	8.0
			1	- 12	6	72	8.0
-			1	12	6	72	8.0
			1	12	, 6	72	8.0
			1	12	10	120	13.3
			1	12	8	96	10.7
			1	12	6	72	8.0
	Glencrest		1	12	8	96	10.7

TOTAL 95 126.7 SF SY

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -				DATCH	INC CO	CHEDULE	Ε
c:\pw_work\PWIDOT\SMITHKL\dØ12509I\Desi	n.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS		•	raion	MG SC	MEDULE	ű.
	PLOT SCALE = 103.7051 ' / IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION				IL 68		
	PLOT DATE = 2/9/2009	DATE ~	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	S STA.	

PATCHING SCHEDULE			F.A. RTE.	SEC.	TION		COUNTY	SHEETS	SHE	٤		
		IL 68			VAR.	2009-0	)24 PP		COOK	33	13	۶
									CONTRACT	NO.	60G2	Ī
0.	OF	SHEETS	STA.	TO STA.	FED. RO	DAD DIST. NO.	ILLINOIS	FED. AIC	PROJECT			_

# ROUTE: Route Name IL 68 ( Sterling to Knollwood )

CROSS	STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Sterling		EB	1.	12	6	72	8.0
		EB	1	12	5	60	6.7
		EB	1	12	6	72	8.0
		EB	1	12	6	72	8.0
		EB	1	15	8	120	13.3
		EB	1	15 <sup>-</sup>	8	120	13.3
		EB	1	20	8	160	17.8
		EB	1	20	8	160	17.8
		EB	1	15	8	120	13.3
		EB	1	20	8	160	17.8
		EB	1	12	8	96	10.7
		EB	1	12	12	144	16.0
		EB	1	6	30	180	20.0
		EB	1	5	25	125	13.9
		EB	1	4	30	120	13.3
		EB	1	12	6	72	8.0
		EB	1	4	30	120	13.3
	Knollwood	EB	1	12	6	72	8.0

TOTAL 218 227.2 FT SY

1	FILE NAME =	USER NAME ≈ smithkl	DESIGNED -	REVISED -	
	c:\pw.work\PWIDOT\SMITHKL\dØ125Ø91\Desig	n.dgn	DRAWN -	REVISED -	ı
		PLOT SCALE = 103.7051 '/ IN.	CHECKED ~	REVISED -	ı
- 1		PLOT DATE = 2/9/2009	DATE -	REVISED -	i

STATE	: OF	ILLINOIS	
DEPARTMENT	OF	TRANSPORTATION	

	PATCHING SCHEDULE							COUNTY	TOTAL	SHEET NO.	
	IL 68						)24 PP	COOK	33	14	
	·							CONTRACT	r NO. (	50G23	
SCALE:	SHEET NO.	OF	SHEETS STA.	TO STA.	FED. R	OAD DIST, NO.	ILLINOIS FED. A	D PROJECT			

# ROUTE: IL 68 ( 1000 ft West of Smith Rd at St Marks )

CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
		EB	1	12	7	84	9.3
			1.	8	8	64	7.1
4			1	12	7	84	9.3
			1	12	6	72	8.0
			1	12	6	72	8.0
			1	12	8	96	10.7
			1	12	8	96	10.7
			1	12	. 8	96	10.7
			1	12	8	96	10.7
		·				0	0.0
		WB	1	12	8	96	10.7
			1	12	10	120	13.3
			1	12	6	72	8.0
			1	12	8	96	10.7
			1	12	7	84	9.3
	<u> </u>		1	12	10	120	13.3

TOTAL

115 FT

	FILE NAME =	USER NAME = smithkl	DESIGNED ~	REVISED -	
	c:\pw_work\PWIDDT\SMITHKL\dØ125091\Desig	n.dgn	DRAWN ~	REVISED ~	STATE OF ILLINOIS
.		PLOT SCALE = 103.7051 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION
		PLOT DATE = 2/9/2009	DATE -	REVISED -	
,				······································	

PATCHING SCHEDULE		F.A. RTE.	SECTION	COUNTY	TOTAL	SHEE NO.
IL 68		VAR.	2009-024 PP	COOK	33	15
IL VO				CONTRACT	NO. 6	50G23
SHEET NO. OF SHEETS STA.	TO STA.	FED. RO		D PROJECT		

# ROUTE: IL 72 EB (0.1 mile E/O Meacham to 0.2 mile E/O Meacham)

CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
·		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
0.1 mile E/O Meacham	0.2 Mile E/O Meacham	EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	2	12	2	24	3
		EB	2	12	2	24	3
		EB	2	12	2	24	3
		EB	2	12	2	24	3
		EB	3	12	2	. 24	3
		EB	3	12	2	24	3
		EB	3	12	2	24	3
		EB	3	12	2	24	3
		EB	3	12	2	24	3

TOTALS:

26

35

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -
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	PLOT SCALE = 103.7051 '/ IN.	CHECKED ~	REVISED -
	PLOT DATE = 2/9/2009	DATE ~	REVISED -

STATE	: OF	ILLINOIS
DEPARTMENT	0F	TRANSPORTATION

PATCHING	SCHEDULE
IL	72
	***************************************

# ROUTE: Oakton St. EB & WB (Elmhurst Rd. to 0.1 mile W/O Elmhurst Rd.)

CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Elmhurst Rd.	0.1 mile W of Elmhurst Rd.	WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	2	24	3
		WB	1	12	600	7200	800
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	2	24	3
		WB	2	12	600	7200	800
		EB	1	12	600	7200	800
		EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	1	12	2	24	3
		EB	2	12	600	7200	800
		EB	2	12	2	24	3
		EB	Lt Turn	8	400	3200	356
		EB	Lt Turn	12	200	2400	267

TOTALS:

3024 FT

FILE NAME =	USER NAMÉ = smithkl	DESIGNED ~	REVISED -
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	PLOT SCALE = 103.7051 '/ IN.	CHECKED ~	REVISED -
	PLOT DATE = 2/9/2009	DATE -	REVISED -

STATE	OF	ILLINOIS
DEPARTMENT	OF	TRANSPORTATION

PATCHING SCHEDULE							F.A. RTE.	
	OAKTON ST.							
	SHEET	NO.	0F	SHEETS	STA.	TO STA.	FED. 1	

SECTION	COUNTY	TOTAL	SHEET NO.
2009-024 PP	COOK	33	17
	CONTRACT	NO. 6	OG23

# ROUTE: Touhy Avenue (McCormick Blvd to Kedzie Ave)

CRO	CROSS STREETS		LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	FROM TO		NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
McCormick Blvd	Kedzie Ave.	EB	1	12	4	48	5
		WB	1	12	24	288	32
		WB	2	12	24	288	32
		EB	1	12	24	288	32
		EB	2	12	24	288	32
		EB	1	12	7	84	9
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	17	204	23
		EB	2	12	17	204	23
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	11	12	4	48	5
		EB	2	12	4	48	5
		WB	1	12	4	48	5
		WB	2	12	4	48	5
***************************************		WB	2	12	30	360	40
		WB	11	12	12	144	16
		WB	2	12	32	384	43
	· ·	EB	2	12	32	384	43

TOTAL

287 FT

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -
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	PLOT SCALE = 103.7051 '/ IN.	CHECKED -	REVISED -
	PLOT DATE = 2/9/2009	DATE -	REVISED -

STATE	OF	ILLINOIS
DEPARTMENT	OF	TRANSPORTATION

			PATCH	IING SCH	EDULE	,
			T	VA YHUO	E.	
ALE:	SHEET	NO.	OF	SHEETS	STA.	TO STA.

A. TÉ.	SECTION	COUNTY	TOTAL	SHEET NO.
AR.	2009-024 PP	COOK	33	18
		CONTRACT	NO. E	50G23

# ROUTE: West Lake Ave. (Pfingsten Rd. to Greenwood)

CROSS STREETS		DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Pfingsten	Greenwood	EB	1	2	10	20	2
		EB	1	12	10	120	13
		EB	1	2	30	60	7
		EB	1	2	15	30	3
		EB	1	12	15	180	20
		EB	1	12	10	120	13
		EB	1	2	10	20	2
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	30	60	7
		EB	1	12	15	180	20
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	50	100	11
		EB	1	12	10	120	13
	·	EB	1	12	40	480	53
		EB	1	2	12	24	3
		EB	1	12	100	1200	133
		EB	1	12	100	1200	133
	·						
		WB	1	12	15	180	20
		WB	1	12	10	120	13
		WB	1	2	20	40	4
		WB	11	2	40	80	9
		WB	11	2	10	20	2
		WB	1	12	20	240	27
		WB	1	12	100	1200	133
		WB	1	12	50	600	67
		WB	1	2	20	40	4
		WB	1	2	15	30	3

TOTALS:

817 FT

732

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -
c:\pw_work\PWIDDT\SMITHKL\dØ125Ø91\Desig	n.dgn	DRAWN -	REVISED -
	PLOT SCALE = 103.7051 '/ IN.	CHECKED -	REVISED -
	PLOT DATE = 2/9/2009	DATE -	REVISED -

:	STATE O	F ILLINOIS	
DEPARTN	MENT OF	TRANSPO	RTATION

,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,			PATCHI	NG SCH	IEDULE	
			WEST	LAKE	AVE.	
1	SHEET	NO.	OF	SHEETS	STA.	

F.A. RTÉ.	SECTION	COUNT
VAR.	2009-024 PP	COOK
		CONTR

#### ROUTE: Willow Road (Landwehre Rd. to Waukegan Rd.)

	S STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	то	(EB/WB)	NO.	PATCH WIDTH	PATCH LENGTH	AREA (SQ FT)	AREA (SQ YD)
andwehre Rd.	Pfingsten	(NB/SB) EB	(1, 2, 3)	2	12	24	3
andwerne No.	Filigaten	EB	2	2	12	24	3
	_	EB	2	2	12	24	3
	<b>†</b>	EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	35	70	8
<sup>2</sup> fingsten	Greenwood	EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
	<b>_</b>	EB	2	2	12	24	3
	<b></b>	EB	2	22	12	24	3
	<b></b>	EB	2	22	12	24	3
	4	EB	2	2	12	24	3
		EB	2	2	12	24	3
		<u>EB</u>	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
	-	EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2 .	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
Greenwood	Shermer	EB	2	2	12	24	3
	<del></del>	EB	2	2	12	24 24	3
	<del></del>	EB	2	2	12	24	3
	<del></del>	EB EB	2 2	2 2	12 12	24	3
	~	EB	2	2	12	24	3
		EB	2	2	12	24	3
(4.44 <u>10.074</u>	<del></del>	EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
	-	EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
	+	EB	2	2	12	24	3
	+	EB	2	2	12	24	3
	-	EB	2	2	12	24	3
	<del></del>	EB	2	2	12	24	3
***************************************		EB	2	2	12	24	3
	-	EB	2	2	12	24	3
	<del></del>	EB	2	2	12	24	3
		EB	2	2	12	24	3
	<u></u>	EB	2	12	10	120	13
	-	EB	2	12	10	120	13
		EB	2	2	12	24	3
***************************************		EB	2	2	12	24	3 3
	-	EB	2	2	12	24	3
hermer	Ravine	EB	2	12	15	180	20
	1	EB	2	12	15	180	20
	1	EB	2	12	15	180	20
	1	EB	2	2	12	24	3
		EB	2	2	12	24	3
		EB	2	2	12	24	3
***********		EB	2	2	50	100	11
Davino	Waukegan	EB	2	12	12	144	16
Ravine		EB	2	12	12	144	16
Ravine	1	EB	2	12	12	144	16
Ravine	ł			•			
Ravine	<del></del>		2	2	12	24	3
Ravine		EB EB	2 2	2 2	12 12	24	3
Ravine		EB	2	2 2 2			3
Ravine		EB EB	2 2 2 2 2	2 2 2 2	12	24	3

### ROUTE: Willow Road (Landwehre Rd. to Waukegan Rd.)

FROM	SS STREETS TO	DIRECTION (EB/WB)	LANE NO.	PAVEMENT PATCH	PAVEMENT PATCH	REPAIR AREA	REPAIR AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Vaukegan	Ravine	WB	2	2	12	24	3
rauncyan		WB	2	2	12	24	3
·····		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	250	500	56
		WB	2	2	250	500	56
Pavino	Charmar	WB	2				
Ravine	Shermer			2	12	24	3
<u> </u>		WB	2	2	12	24	3
Shermer	Greenwood	WB	2	2	60	120	13
		WB	2	12	15	180	20
Greenwood	Pfingsten	WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
Ofingetor	Landwohro	WB	2	2	12	<del></del>	3
Pfingsten	Landwehre					24	
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
	<del></del>	WB	2	2	12	24	3
		WB	2	2	12	24	3
		T WB			12	24	
			2	2			3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
		WB	2	2	12	24	3
Landwehre	Pfingsten	EB	1	2	40	80	9
		EB	1	2	30	60	7
		EB	1	2	40	80	9
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1 1	2	12	24	3
		EB	<del>                                     </del>	2	12	<del></del>	3
						24	
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	150	300	33
Pfingsten	Greenwood	EB	<del>                                     </del>	2	12	24	3
90.011		EB	<del>                                     </del>	2	12	24	3
·····			1	2	<del></del>	24	3
~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~~		EB	1		12		
		EB EB		2		24	3
		EB EB	11	2	12	24	3
····		EB	1 1	2	12	24	3
		EB	11	2	12	24	3
		EB	1 1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
pagamanga dalamidipan pada pingan dalah bishte pindhili		EB	1	2	12	24	3
		EB	1	2	12	24	3
	<del></del>	EB	1 1	2	12	24	3
<del></del>		EB	<del>                                     </del>	2	12	24	3
		EB	<del>   </del>		12	24	3
			<del></del>	2			
		EB	11	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1	2	12	24	3
		EB	1 1	2	12	24	3
***************************************		EB	1	2	12	24	3

CONTINUED ON THE NEXT SHEET

FILE NAME *	USER NAME = smithkl	DESIGNED -	REVISED -		PATCHING SCHEDULE			F.A. RTE.	SECTION	COUNTY	TOTAL SHEET			
c:\pw_work\PWIDDT\SMITHKL\d0125091\Desig	nidgn	DRAWN ~	REVISED -	STATE OF ILLINOIS VAR.							VAR.	2009-024 PP	соок	33 20
	PLOT SCALE = 103.7051 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION	WILLOW RD.						CONTRAC	T NO. 60G23		
	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD	DIST. NO. ILLINOIS FED. A	AID PROJECT	

# ROUTE: Willow Road (Landwehre Rd. to Waukegan Rd.)

NE PAVEM PATC 2, 3) WIDT 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	CH PATCH IH LENGTH  200  12  12  12  12  12  12  12  12  1	AREA (SQ FT)  400 24 24 24 24 24 24 24 24 24 144 144 144	REPAIR AREA (SQ YD)  44  3  3  3  3  3  3  3  3  3  3  3  46  16  16  89  78  267  44  133  89  67  44  133  89  67  44  133  3  3  3  3  3  3  3  3  3  3  3
2 2 2 2 2 2 2 2 1 2 1 2 1 2 1 2 2 1 2	200 12 12 12 12 12 12 12 12 12 12 12 12 12	400 24 24 24 24 24 24 24 24 24 24 144 144 800 700 2400 400 1200 800 600 400 400 401 400 400 401 400 400 401 400 401 400 401 401	444 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 16 16 16 16 89 78 267 44 133 89 67 44 43 3 3 3 2222 9 56 16 3 3 3 3 3
2 2 2 2 12 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	24 24 24 24 24 24 24 24 24 24 144 144 800 700 2400 800 600 400 24 24 24 24 24 24 24 24 24 24 24 24 24	3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 4 16 16 89 78 267 44 133 89 67 44 43 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 5 6 7 8 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9
2 2 2 2 12 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	24 24 24 24 24 24 24 24 24 144 144 144 1	3 3 3 3 3 3 3 3 3 3 3 3 3 16 16 16 89 78 267 44 133 89 67 44 44 3 3 3 3 3 3 3 8 9 6 7 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9
2 2 2 2 1 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	24 24 24 24 24 24 24 144 144 144 1200 800 700 2400 400 400 220 800 600 400 24 24 24 24 24 24 24 24 24 24 24 24 24	3 3 3 3 3 3 3 3 3 16 16 16 16 16 16 267 44 44 133 89 67 44 44 3 3 3 3 2 222 9 9 16 16 16 16 16 16 16 16 16 16 16 16 16
2 2 2 1 12 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 2 1 2 2 2 2 2 2 2 1 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	24 24 24 24 24 24 144 144 144 800 700 2400 400 1200 800 600 400 24 24 24 24 24 24 24 24 24 24 24 24 24	3 3 3 3 3 3 3 16 16 16 16 16 16 89 78 267 44 133 89 67 44 3 3 3 3 3 3 3 3 89 67
2 2 2 1 2 12 12 12 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 12 12 1	24 24 24 24 24 144 144 1800 700 2400 400 1200 800 600 400 24 24 24 24 24 24 24 24 24 24 24 24 24	3 3 3 3 3 3 3 16 16 16 89 78 267 44 133 89 67 44 3 3 3 3 3 222 9 56 16 16 16 3 3 3 3 3 3 3 3 3 4 4 4 4 5 5 6 7 8 7 8 9 8 9 9 9 9 9 9 9 9 9 9 9 9 9 9
2 2 2 12 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 12 12 12 12 12 10 400 350 1200 600 400 300 200 12 12 12 12 1000 40 2550 12 12 12 12 12 12 12 12 12 12 12 12 12	24 24 24 24 144 144 1800 700 2400 400 1200 800 600 400 24 24 24 24 24 24 24 24 24 24 24 24 24	3 3 3 3 3 16 16 16 89 78 267 44 133 89 67 44 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3 3
2 2 12 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 12 12 12 12 400 3550 1200 200 600 400 300 200 12 12 12 12 1000 40 250 12	24 24 24 144 144 144 1200 800 700 2400 400 1200 800 600 400 24 24 24 24 24 200 500 144 24	3 3 3 16 16 16 16 89 78 267 44 133 89 67 44 3 3 3 3 222 9 56 16 3 3 3 3
2 2 2 1 12 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 2 1 2	12 12 12 12 12 12 12 10 400 350 1200 200 600 400 200 12 12 12 12 12 1000 40 2550 12 12 12	24 24 144 144 144 800 700 2400 400 1200 800 600 400 24 24 24 24 2000 80 500 144 24	3 3 16 16 16 16 89 78 267 44 133 89 67 44 3 3 3 3 222 9 56 16 16
2 12 12 12 12 12 12 12 12 12 12 12 12 12	12 12 12 12 400 350 1200 200 600 400 200 12 12 12 12 12 1000 40 250 12	24 144 144 144 800 700 2400 400 1200 800 600 400 24 24 24 24 2000 80 500 144 24	3 16 16 16 89 78 267 44 133 89 67 44 3 3 3 3 222 9 56 16 3 3 3
12 12 12 12 12 12 12 12 12 12 12 12 12 1	12 12 12 400 350 1200 600 400 300 2200 12 12 12 1000 40 250 12	144 144 144 800 700 2400 400 1200 800 600 400 24 24 24 24 22 2000 80 500 144 24	16 16 16 89 78 267 44 133 89 67 44 3 3 3 3 222 9 56 16 3 3 3
12 12 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	12 12 400 350 1200 200 600 400 300 200 12 12 12 1000 40 250 12 12	144 144 1800 700 2400 400 1200 800 600 400 24 24 24 22 200 500 144 24 24	16 16 89 78 267 44 133 89 67 44 3 3 3 222 9 56 16 3 3 3
12 2 2 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 2 1 2 2 2 1 2 2 2 2 1 2 2 2 2 1 2 2 2 2 2 1 2	12 400 350 1200 200 600 400 200 12 12 12 1000 40 250 12 12 12 12 12 12 12 12 12 12	144 800 700 2400 400 1200 800 600 400 24 24 24 24 2000 80 500 144 24	16 89 78 267 44 133 89 67 44 3 3 3 3 222 9 56 16 3 3 3
2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	400 350 1200 200 600 400 200 12 12 12 1000 40 250 12 12 12	800 700 2400 400 1200 800 600 400 24 24 24 2000 80 500 144 24	89 78 267 44 133 89 67 44 3 3 3 3 222 9 56 16 3 3 3
2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	350 1200 200 600 400 300 200 12 12 12 1000 40 250 12 12	700 2400 400 400 1200 800 600 400 24 24 24 24 2000 80 500 1444 24 24 24 24	78 267 44 133 89 67 44 3 3 3 3 222 9 56 16 3 3 3
2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 1 2 2 2 2 1 2	1200 200 600 400 300 200 12 12 12 1000 40 250 12 12	2400 400 1200 800 600 400 24 24 24 2000 80 500 144 24	267 44 133 89 67 44 3 3 3 222 9 56 16 3 3 3
2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 1 2 2 2 1 2 2 2 1 2 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2	200 600 400 300 200 12 12 12 1000 40 250 12 12 12 12	400 1200 800 600 400 24 24 24 2000 80 500 144 24 24	44 133 89 67 44 3 3 3 2222 9 56 16 3 3 3
2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	600 400 300 200 12 12 12 1000 40 2550 12 12	1200 800 600 400 24 24 24 2000 80 500 144 24 24 24	133 89 67 44 3 3 3 3 222 9 56 16 3 3 3
2 2 2 1 2 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 1 2 2 2 1 2	400 300 200 12 12 12 1000 40 250 12 12 12	800 600 400 24 24 24 2000 80 500 144 24 24	89 67 44 3 3 3 222 9 56 16 3 3 3
2 2 2 2 2 2 2 2 2 2 2 2 1 2 2 2 2 2 2 2	300 200 12 12 12 1000 40 250 12 12 12	600 400 24 24 2000 80 500 144 24 24 24	67 44 3 3 3 2222 9 56 16 3 3 3
2 2 2 2 2 2 2 2 2 1 2 2 1 2 2 2 2 2 2 2	200 12 12 12 1000 40 250 12 12 112	400 24 24 24 2000 80 500 144 24 24	44 3 3 3 222 9 56 16 3 3 3
2 2 2 2 2 2 2 1 2 1 2 2 2 2 2 2 2 2 2 2	12 12 12 1000 40 250 12 12 12	24 24 24 2000 80 500 144 24 24	3 3 3 222 9 56 16 3 3
2 2 2 2 2 2 12 12 2	12 12 1000 40 250 12 12	24 24 2000 80 500 144 24 24 24	3 3 2222 9 56 16 3 3
2 1 2 1 2 1 2 1 12 1 2	12 1000 40 250 12 12 12	24 2000 80 500 144 24 24 24	3 222 9 56 16 3 3
2 1 2 1 12 1 2	1000 40 250 12 12 12	2000 80 500 144 24 24 24	222 9 56 16 3 3
2 1 2 1 12 1 2	40 250 12 12 12	80 500 144 24 24 24 24	9 56 16 3 3 3
2 1 12 1 2	250 12 12 12	500 144 24 24 24 24	56 16 3 3 3
1 12 1 2 1 2	12 12 12	144 24 24 24 24	16 3 3 3
1 2	12 12	24 24 24	3 3 3
1 2	12	24 24	3 3
		24	3
	1 12		
1 2	12	24	3
1 2	12	24	3
2	12	24	3
1 2	12	24	3
1 2	12	24	3
1 2	100	200	22
2	400	800	89
1 2	12	24	3
. 2	12	24	3
1 2	12	24	3
1 2	12	24	3
1 2	12	24	3
2	12	24	3
			3
			3
	12		3 3
2	12		$+\frac{3}{3}$
1 - 5			+ 3
1 2		24	3
1 2	12	24	3
2	12	24	3
	12	24	3
1 2	12	24	3
1 2			3
1 2 1 2	1 40		3
1 2 1 2 1 2			3
1 2 1 2 1 2 1 2	12	1 24	3 3
1 2 1 2 1 2 1 2	12 12		
1 2 1 2 1 2 1 2 1 2	12	24	3
	1 2 1 2 1 2 1 2	1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12       1     2     12	1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24           1         2         12         24

# ROUTE: Willow Road (Landwehre Rd. to Waukegan Rd.)

CRO	SS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	TO	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Pfingsten	Landwehre	WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	12	24	3
		WB	1	2	150	300	33
		WB	1	2	15	30	3
		WB	1	2	50	100	11

TOTALS:

FT

	FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -				DATCH	ING SCH	EDIN E		F.A.	SECTION	COUNTY
, e)	c:\pw_work\PWIDOT\SMITHKL\d0125091\Desig	n.idgn	DRAWN -	REVISED -	STATE OF ILLINOIS	and the second	4 1			EDOLE		VAR.	2009-024 PP	COOK
		PLOT SCALE = 103.7051 ' / IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION			W	ILLOW R	ID.		<u> </u>		CONTRACT
		PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	FED. ROAD D	DIST. NO. ILLINOIS FED. AL	

ROUTE: Wolf Rd. (Milwaukee Ave. to Palatine Rd.)

	SS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	то	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
IL 21	Picardy Ln.	SB	11	12	12	144.0	16.0
	Longit. Jt.	SB	1-2	2	20	40.0	4.4
Diameter I -		SB	1	12	12	144.0	16.0
Picardy Ln.	Mayer Ave.	SB	1	12	12	144.0	16.0
Ma A	Widening Jt.	SB	2	2	150	300.0	33.3
Mayer Ave.	Strong St.	SB Edge	1	2	50	100.0	11.1
Ctrone Ct	Carri Tar	SB Edge	1	2	12	24.0	2.7
Strong St.	Capri Ter.	SB	1	12	6	72.0	8.0
Capri Ter.	Dundee Rd.	SB Edge		2	50	100.0	11.1
Dundee Rd.	Jeffrey Ave.	SB Edge		2	60	120.0	13.3
·····		SB	1	12	6	72.0	8.0
······		OD Edea	11	12	6	72.0	8.0
		SB Edge		6	15	90.0	10.0
		SB Edge		2 2	15 10	30.0 20.0	3.3
		SB Edge					2.2
		SB	1	12	6	72.0	8.0
		SB Edge	1	2 12	30	60.0	6.7
		SB			6	72.0	8.0
		SB Edge	ļ	2	10	20.0	2.2
		SB Edge		2	15	30.0	3.3
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
		SB	11	12	6	72.0	8.0
······		SB Edge		2	15	30.0	3.3
		SB Edge		2	15	30.0	3.3
		SB Edge	ļ	2	30	60.0	6.7
		SB Edge		2	15	30.0	3.3
····		SB Edge		2	25	50.0	5.6
Jeffrey Ave.	Manchester Dr.	SB Edge		2	50	100.0	11.1
			1	12	6	72.0	8.0
		SB Edge		2	25	50.0	5.6
		SB Edge		2	50	100.0	11.1
			1	12	12	144.0	16.0
		SB Edge		2	20	40.0	4.4
		SB Edge		2	15	30.0	3.3
Manchester Dr.	Crescent Dr.	NB	1	12	6	72.0	8.0
		NB	1	12	20	240.0	26.7
		NB	1	12	6	72.0	8.0
		NB Edge		2	20	40.0	4.4
		NB Edge		2	30	60.0	6.7
		NB	1	12	6	72.0	8.0
Crescent Dr.	Dundee Rd.	NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
		NB Edge		2	25	50.0	5.6
		NB	1	12	6	72.0	8.0
		NB	1	12	12	144.0	16.0
		NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
		NB Edge		2	25	50.0	5.6
		NB Edge		2	30	60.0	6.7
		NB Edge		2	25	50.0	5.6
		NB	1	12	12	144.0	16.0
		NB Edge	L	2	50	100.0	11.1
Dundee Rd.	Strong St.	NB Edge		2	25	50.0	5.6
		NB Edge		2	50	100.0	11.1
	Longit. Jt	CL		2	50	100.0	11.1
Strong St.	IL 21	NB Edge		2	30	60.0	6.7
		NB Edge		2	15	30.0	3.3
		NB Edge		2	15	30.0	3.3
		NB	1	12	6	72.0	8.0
		NB	2	12	6	72.0	8.0
		1 - 2		2	15	30.0	3.3
Palatine Rd.	Marquardt Dr.	NB	1	12	12	144.0	16.0
		NB	1	6	12	72.0	8.0
	Longit, Jt.	NB	1-2	2	20	40.0	4.4
		NB	1	12	18	216.0	24.0
		NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
	Longit. Jt.	NB	1-2	2	25	50.0	5.6
		NB	1	12	6	72.0	8.0
		NB	1	12	6	72.0	8.0
************************		NB	1	12	50	600.0	66.7
		NB NB	1 1	12	12	144.0	16.0
······································		NB	1 1	12	12	144.0	
	,						16.0

DOLLTE.	Mak Dd (Million des Aug to Deletion Dd)
ROUTE:	Wolf Rd. (Milwaukee Ave. to Palatine Rd.)

CROS	S STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	то	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
		(NB/SB)	(1, 2, 3)	WIDTH	LENGTH	(SQ FT)	(SQ YD)
Marquardt Dr.	Palatine Rd.	ŞB				0.0	0.0
	Longit. Jt. @ Messner	SB RHTB		6	80	480.0	53.3
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
		SB	1	12	15	180.0	20.0
		SB Edge	1	2	16	32.0	3.6
		SB	1	12	6	72.0	8.0
		SB	1	12	6	72.0	8.0
Widening area for Palati	ne Rd.	ŞB	1-2	2	200	400.0	44.4
		ŞB	1	12	6	72.0	8.0
		SB	2	12	12	144.0	16.0
		SB	1	12	6	72.0	8.0

TOTAL

1864 FT

916 SY

NOTE:

NO PATCHING IS TO BE DONE ON MARQUARDT DR. TO PALATINE RD. THESE LIMITS WILL BE RESURFACED AT A LATER DATE.

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -			DAT	CHING SC	UEDIH E		F.A.	SECTION	COUNTY	TOTAL SHEET
c:\pw_work\PWIDDT\SMITHKL\dØ125091\Desid	n.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS	WOLF. RD.			VAR.	VAR. 2009-024 PP		33 1Z		
	PLOT SCALE = 103.7051 '/ IN.	CHECKED -	REVISED ~	DEPARTMENT OF TRANSPORTATION						CONTRACT	NO. 60G23		
	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE:	SHEET NO. OF	SHEETS	STA.	TO STA.	FED. ROAD	DIST. NO.   ILLINOIS FED. A	ID PROJECT	

# ROUTE: Church St. (Gross Pointe Rd to McCormick Blvd.)

CRO	OSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
				WIDTH	LENGTH		
Grosse Pointe Rd	Skokie Rd	WB	Lt Turn	12	43	516	57
		WB	Lt Turn	2	100	200	22
		EB	1	12	4	48	5
		EB	1	12	4	48	5
		EB	2	12	4	48	5
······································		WB	1	12	4	48	5
		WB	2	12	4	48	5
Skokie Rd	Niles Center Rd	THIS SECTI		PAVEMENT			<u> </u>
Niles Center Rd	Kenton Av	C/L	-	2	60	120	13
Kenton Av	Kolmar Av	EB	1	12	2	24	3
		EB	2	12	2	24	3
		WB	1 1	12	2	24	3
******		WB	2	12	2	24	3
<del>, , , , , , , , , , , , , , , , , , , </del>		WB .	182	2	250	500	56
Kolmar Av	Kilbourn Av	C/L		2	40	80	9
TOTTICE 7 TV	TGIDOUITI 74	C/L	<u> </u>	2	100	200	22
		WB	1 1	12	6	72	8
		WB	2	12	6	72	8
	<del> </del>	C/L	<del> </del>	2	123	246	27
		WB	1&2	2	123	246	27
Kilbourn Av	Kenneth Av	WB	182	2	75	150	17
Nibouri Av	Refilledi AV	C/L	- 10.2	2	75	150	17
		WB	2	12	150	1800	200
		C/L	-	2	150	300	33
Kenneth Av	Kostner Av	WB	182	2	40	80	9
Kenneu Av	ROStrier AV	WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1 1	12	6	72	8
		EB	2	12	6	72	8
		EB	1&2	2	40	80	9
			182		40	80	9
		WB C/L	10.2	2 2	110	220	23
***************************************		WB	1&2	2	47	94	9
			<del></del>	<u> </u>			22
16 - 4 4		C/L	-	2	100	200	
Kostner Av	Lowell Av	C/L WB	182	2 2	388 100	76 200	8 22
		C/L	102	2	73	146	16
		EB	1	12	6	72	8
		C/L	<del> </del>	2	140	280	31
Lowell Av	Kildare Av	C/L	<del> </del> -	2	225	450	50
LOWEII AV	Niluale AV	EB		12	6	72	
		C/L	1	2	94	188	8 21
Vildoro Av	Tring Av	WB	182	2	87	174	19
Kildare Av	Tripp Av		<del></del>		195		
		C/L	100	2		390	43
Tring A.	Varior Ari	WB	1&2	2	110	220	24
Tripp Av	Keeler Av	EB	1 1	12	4	48	5
	· · · · · · · · · · · · · · · · · · ·	EB	2	12	4	48	5
		WB	1	12	4	48	5
		WB	2	12	4	48	5
<del> </del>		C/L		2	18	36	4
		WB	1&2	2	78	156	17
		EB	1&2	2	27	54	6
<u> </u>		C/L		2	67	134	15

# ROUTE: Church St. (Gross Pointe Rd to McCormick Blvd.)

CRO	SS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	то	(EB/WB)	NO.	PATCH WIDTH	PATCH LENGTH	AREA	AREA
Tripp Av	Keeler Av (CONT'D)	WB	1&2	2	42	84	9
		EB	2	12	6	72	8
····		C/L	-	2	10	20	2
Keeler Av	Kedvale Av	EB	1&2	2	25	50	6
		C/L	-	2	144	288	32
		C/L		2	24	48	5
Manda A.	I/CI A	C/L	-	2	77	154	17
Kedvale Av	Karlov Av	C/L	-	2	109	218	24
		WB WB	1&2 1&2	4	22	88 78	10 9
	····	C/L	1042	2	39 174		37
		WB	1&2	2 2	174 63	348 126	14
		WB	1	12	23	276	31
Karlov Av	Keystone Av	EB	1&2	2	34	68	7
TOTIO 7 (V	TKC/GROTIC / W	C/L	- 102	2	320	640	71
		WB	1&2	2	25	50	6
Keystone Av	Crawford Av	EB	1	12	6	72	8
		EB	2	12	6	72	8
		WB	1	12	6	72	8
**************************************		WB	2-Jan	12	6	72	8
		WB	1	2	25	50	6
		WB	1	4	12	48	5
		WB	2	4	12	48	5
		C/L	-	2	177	354	39
		WB	1	12	10	120	13
·		C/L	-	2	119	238	26
		WB	1&2	2	33	66	7
Crawford Av	Harding Av	WB	1	12	6	72	8
**************************************		WB	2	12	6	72	8
		EB	2	12	6	72	8
		EB	2	12	6	72	8
		EB EB	2	12 12	6	72 72	8 8
····		EB	2	12	6	72	8
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		C/L	-	2	50	100	11
Harding Av	Springfield Av	EB	1	12	6	72	8
		C/L	-	2	15	30	3
		EB	1	12	6	72	8
		EB	2	12	6	72	8
		WB	1&2	2	38	76	8
Springfield Av	Avers Av	WB	2	12	6	72	8
		WB	2	12	6	72	8
		CL	-	2	48	96	11
Avers Av	Samoset Av	EB	1	12	6	72	8
		EB	1&2	2	43	86	11
		C/L	-	2	35	70	8
		WB	1	12	10	120	13
***************************************		WB	2	12	10	120	13
		EB	1 1	12	6	72	8
C 1 A	I I a a clica A c	EB	2	12	6	72	8
Samoset Av	Hamlin Av	EB VA(B	2	12	6	72	8
Hamlin Av	Ridgeland Av. (SB) Ridgeland Av. (NB)	WB	182	2	60 6	120 72	13
Ridgeland Av. (SB)	[Ridgeland AV. (NB)	WB	1 1	12	6	12	8

CONTINUED ON THE NEXT SHEET

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -		PATCHING SCHEDULE		F.A.	SECTION	COUNTY	TOTAL SHEET
c:\pw_work\PWIDOT\SMITHKL\dØ125091\Desig	n.dgn	DRAWN -	REVISED -	STATE OF ILLINOIS			VAR.	2009-024 PP	соок	33 Z3
	PLOT SCALE = 103.7051 '/ IN.	CHECKED -	REVISED ~	DEPARTMENT OF TRANSPORTATION	CHURCH ST.					T NO. 60G23
•	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE: SHEET NO. OF SHEETS STA.	TO STA.	FED. ROAD D	IST. NO. ILLINOIS FED.	AID PROJECT	

# ROUTE: Church St. (Gross Pointe Rd to McCormick Blvd.)

	OSS STREETS	DIRECTION	LANE	PAVEMENT	PAVEMENT	REPAIR	REPAIR
FROM	ТО	(EB/WB)	NO.	PATCH	PATCH	AREA	AREA
				WIDTH	LENGTH		
Ridgleland Av. (NB)	Lawndale Av.	WB	2	12	6	72	8
		WB	11	12	6	72	8
MANAGEMENT		WB	2	12	6	72	8
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	1&2	2	27	54	.6
_awndale Av.	Central Park Av.	WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	2	12	6	. 72	8
		WB	1	12	4	48	5
		WB	2	12	4	48	5
		EB	1	12	4	48	5
		EB	2	12	4	48	5
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		WB	2	12	6	72	8
		WB	1-Jan	12	6	72	8
		C/L	-	2	54	108	12
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	2	12	6	72	8
Central Park Av.	Drake Av.	WB	2	2	200	400	44
Drake Av.	Lincolnwood Av.	WB	1	12	6	72	8
***************************************		WB	2	12	6	72	8
		WB	1	12	8	96	11
**************************************		WB	2	12	8	96	11
		WB	1	12	6	72	8
		WB	2	12	6	72	8
		EB	1	12	6	72	8
		EB	2	12	6	72	8
		WB	2	12	10	120	13
Lincolnwood Av.	Ewing Av.	EB	1	12	6	72	8
		EB	2	12	6	72	8
***************************************		EB	1	12	6	72	8
		EB	1	12	6	72	8
		EB	1	12	6	72	8
	······	EB	2	12	6	72	8
Ewing Av.	McCormick Blvd	EB	2	12	6	72	8
<u>J </u>		EB	1	12	6	72	8
		EB	2	12	6	72	8

TOTALS:

5853 FT

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I	FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED -
	c:\pw_work\PWIDOT\SMITHKL\dØ125091\Desig	n.dgn	DRAWN -	REVISED -
1		PLOT SCALE = 103.7051 ' / IN.	CHECKED -	REVISED -
1		PLOT DATE = 2/9/2009	DATE -	REVISED -

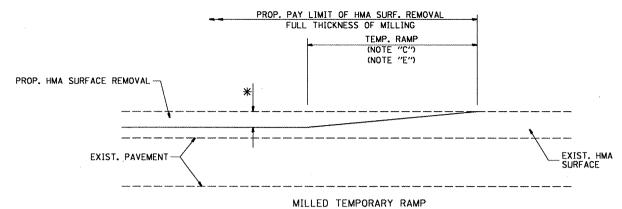
STATE	OF	ILLINOIS
DEPARTMENT	OF	TRANSPORTATION

	PATCHING SCHEDULE						F.A. RTE.	SECTION	COU
1	CHIDOL CT					VAR.	2009-024 PP	CO	
									CON
	SCALE:	SHEET	NO.	OF	SHEETS STA.	TO STA.		OAD DIST. NO.   ILLINOIS FED. A	D PROJEC

# ROUTE: North Cook County - Various Routes and Limits

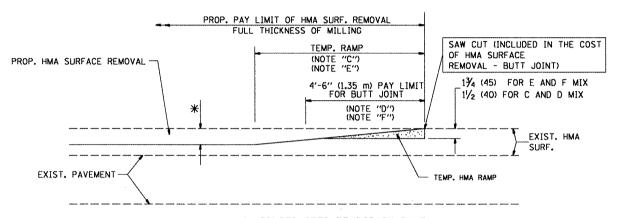
	CR	OSS STREETS	DIRECTION	REPAIR
ROUTE	FROM	ТО	(EB/WB)	AREA
			(NB/SB)	(SQ YD)
LINCOLN AVE.	DEVON AVE.	TOUHY AVE.	NB/SB	2400
MILWAUKEE AVE.	GOLF RD.	PALATINE RD.	NB/SB	4200
RAND RD.	WOLF RD.	ELK BLVD.	EB/WB	1100

FILE NAME *	USER NAME = smithkl	DESIGNED ~	REVISED - 7/17/08 - DPW			PATCH	IING SCHEDULE		F.A. RTE.	SECTION	COUNTY TOTAL SHEET
c:\pw_work\PWIDOT\SMITHKL\dØ125Ø91\Desig	n.dgn	DRAWN ~	REVISED -	STATE OF ILLINOIS	]				VAR.	2009-024 PP	COOK 33 25
	PLOT SCALE = 100.0000 '/ IN.	CHECKED -	REVISED -	DEPARTMENT OF TRANSPORTATION		VAK	IOUS ROUTES		_		CONTRACT NO. 60G23
	PLOT DATE = 2/9/2009	DATE -	REVISED -		SCALE: SHEET NO.	QF	SHEETS STA.	TO STA.	FED. ROAD D	IST. NO. ILLINOIS FED.	AID PROJECT



(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

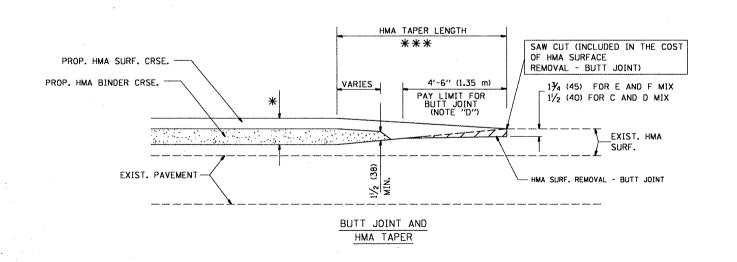
### OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP (FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

#### OPTION 2

# TYPICAL TEMPORARY RAMP



# TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

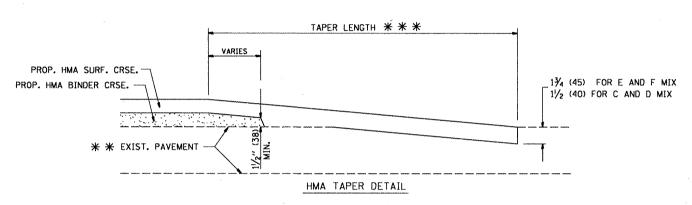
DESIGNED - M. DE YONG :\ow\_work\PWIDDT\SMITHKL\dØ125Ø91\Dis DRAWN REVISED - A. ABBAS 03-21-97 PLOT SCALE = 100.0000 '/ IN. CHECKED REVISED M. GOMEZ 04~06~01 PLOT DATE = 2/3/2009 DATE REVISED -

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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION** 

SECTION COUNTY **BUTT JOINT AND** COUNTY SHEETS NO. 2009-024 PP VAR. HMA TAPER DETAILS CONTRACT NO. 60G23 BD400-05 BD32 SHEET NO. 1 OF 1 SHEETS STA. TO STA.

PROP. HMA OR PCC SURFACE REMOVAL - BUTT JOINT SAW CUT (INCLUDED IN THE COST EXIST. HMA OR PCC SURFACE 30'-0" (9.0 m) (NOTE "A") OF HMA OR P.C.C. SURFACE REMOVAL 15'-0" (4.5 m) (NOTE "B") - BUTT JOINT) (NOTE "D") 13/4 (45) FOR E AND F MIX 11/2 (40) FOR C AND D MIX \* \* EXIST. PAVEMENT BUTT JOINT DETAIL



# TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-O" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\* \*\* \*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

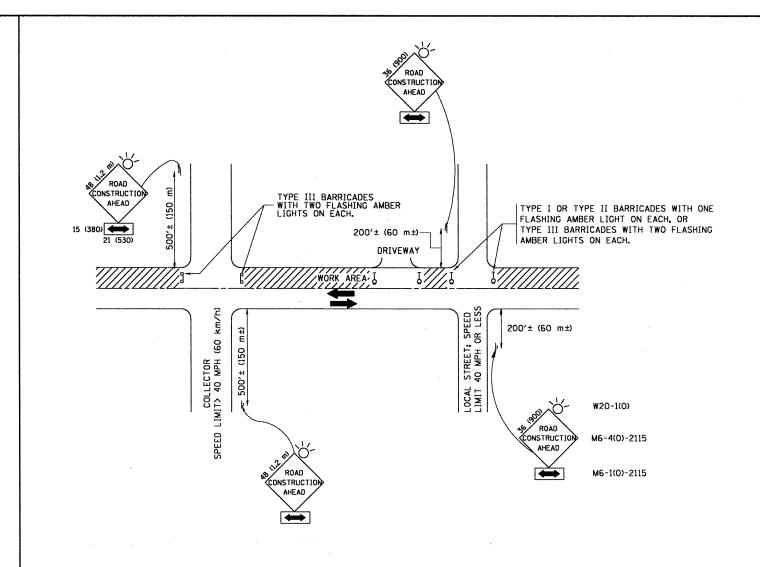
#### BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER)
FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

SCALE: NONE

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

R. BORO 01-01-07



# TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

#### NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- O) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h)
  AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- O) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

SCALE: NONE

#### B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

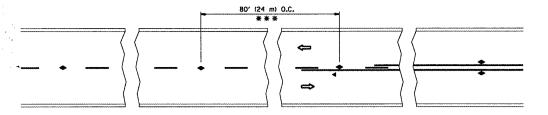
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

	FILE NAME =	USER NAME = smithkl	DESIGNED ~	LHA	REVISED	- J. OBERLE 10-18-95
	c:\pw.work\PWIDDT\SMITHKL\dØ125Ø91\DistS	td.dgn	DRAWN -		REVISED	~ A. HOUSEH 03-06-96
San		PLOT SCALE = 100.0000 '/ IN.	CHECKED -		REVISED	- A. HOUSEH 10-15-96
		PLOT DATE ≈ 2/3/2009	DATE -	06-89	REVISED	-T. RAMMACHER 01-06-00
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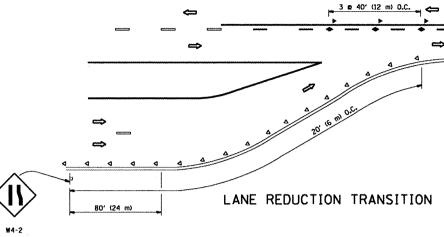
STATE	OF	ILLINOIS
DEPARTMENT	OF	TRANSPORTATION

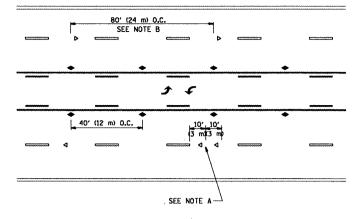
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS		SECTION	COUNTY	TOTAL SHEET SHEETS NO.	
		2009-024 PP	COOK	33 27	
		TC-10	CONTRACT	NO. 60G23	
SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. R	DAD DIST. NO. 1   ILLINOIS FED. AL	D PROJECT		



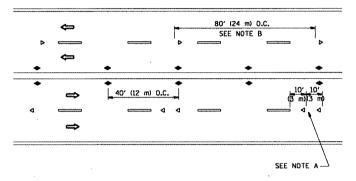
\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

#### TWO-LANE/TWO-WAY

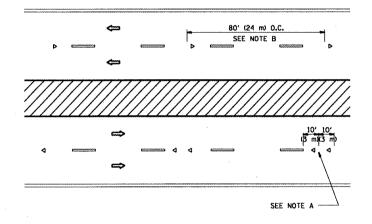




TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

# GENERAL NOTES

- 1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

#### LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

# SYMBOLS

YELLOW STRIPE

WHITE STRIPE

- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

#### DESIGN NOTES

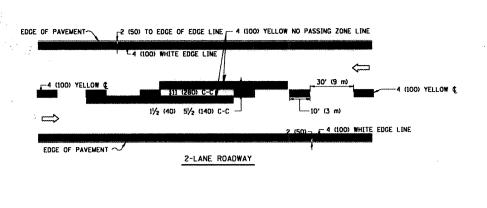
- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
- 3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

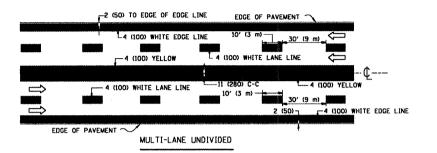
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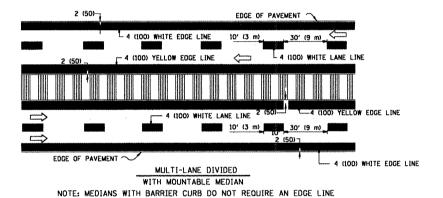
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

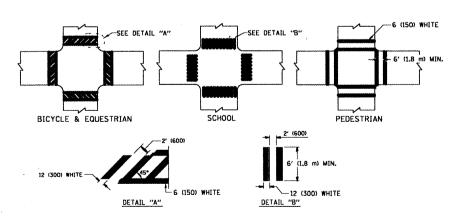
	COUNTY SHEETS NO.
TYPICAL APPLICATIONS  TYPICAL APPLICATIONS	COOK 33 28
PLOT SCALE = 100.00000 // IN. CHECKED - REVISED -T. RAMMACHER 01-06-00 DEPARTMENT OF TRANSPORTATION RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT) TC-11	CONTRACT NO. 60G23
PLOT DATE = 2/3/2009 DATE - REVISED - SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA. FED. ROAD DIST. NO. 1 ILLINOIS	FED. AID PROJECT



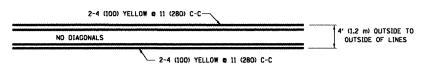




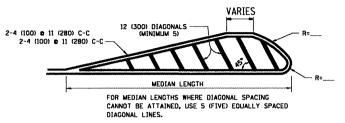
#### TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

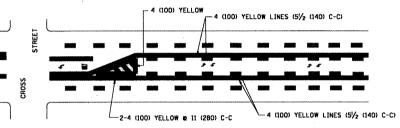


#### 4' (1.2 m) WIDE MEDIANS ONLY

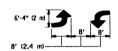


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

#### MEDIANS OVER 4' (1.2 m) WIDE

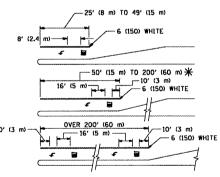


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR.
ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

# TYPICAL PAINTED MEDIAN MARKING

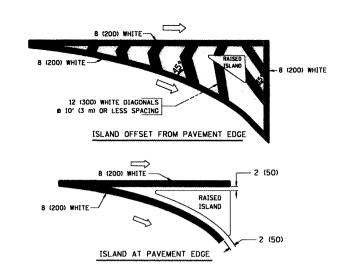


FULL SIZE LETTERS B' (2.4 m) AND ARROWS SHALL BE USED.  $\P$  AREA = 15.6 SO. FT. (1.5 m<sup>2</sup> )  $\P$  AREA = 20.8 SO. FT. (1.9 m<sup>2</sup>)

\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY",

TYPICAL LEFT (OR RIGHT) TURN LANE

#### TYPICAL TURN LANE MARKING



#### TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVEDED PAVEMENT	2 g 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 <b>g</b> 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 1280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1,8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 0 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 % 6 (150) 12 (300) % 45° 12 (300) % 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.B m) APART 2' (500) APART 2' (500) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLIO	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 b 4 (100) WITH 12 (300) DIAGONALS b 45° NO DIAGONALS USED FOR 4' (1,2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS & 45°	SOLID	WHITE	DIACONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"*3.6 SO. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) <b>e</b> 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) T0 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

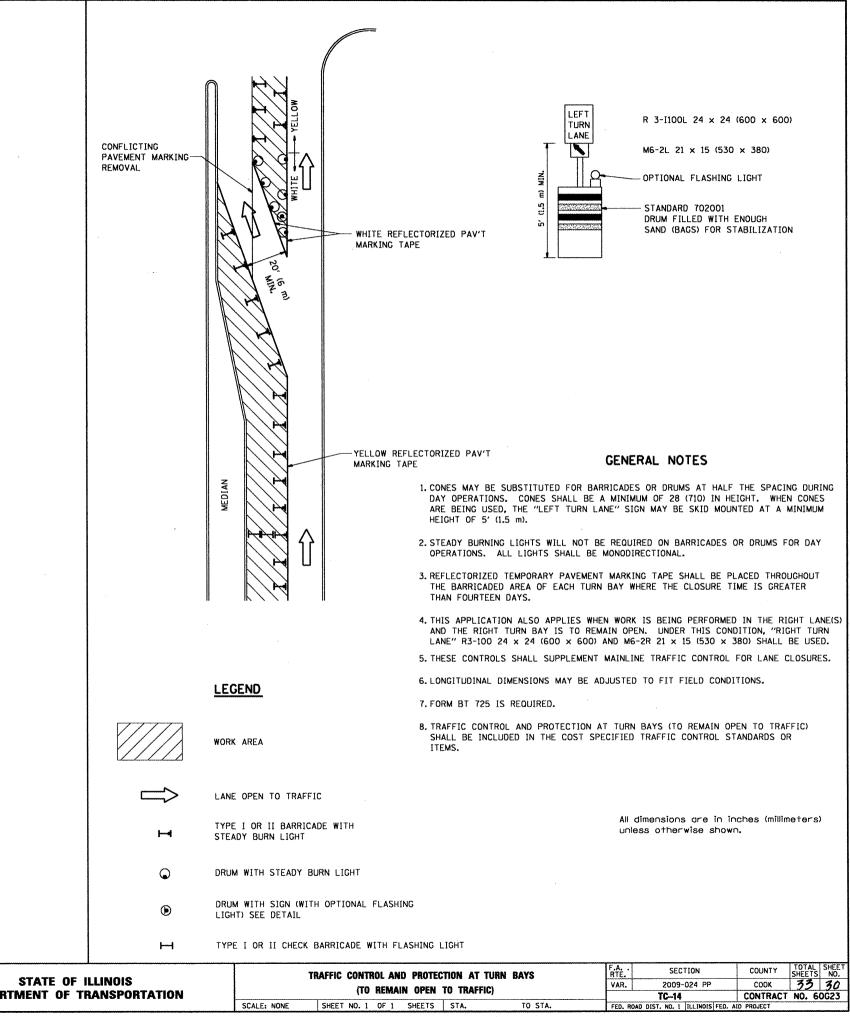
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

	FILE NAME =	USER NAME = smithkl	DESIGNED	-	EVERS	REVISED	-T.	RAMMACHER 10-27-94
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, San Jana		PLOT SCALE = 100.0000 '/ IN.	CHECKED	-		REVISED	- A.	HOUSEH 10-17-96
		PLOT DATE = 2/3/2009	DATE	-	03-19-90	REVISED	- T.	RAMMACHER 01-06-00
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STATE	OF	ILLINOIS
<b>DEPARTMENT</b>	OF 1	TRANSPORTATION

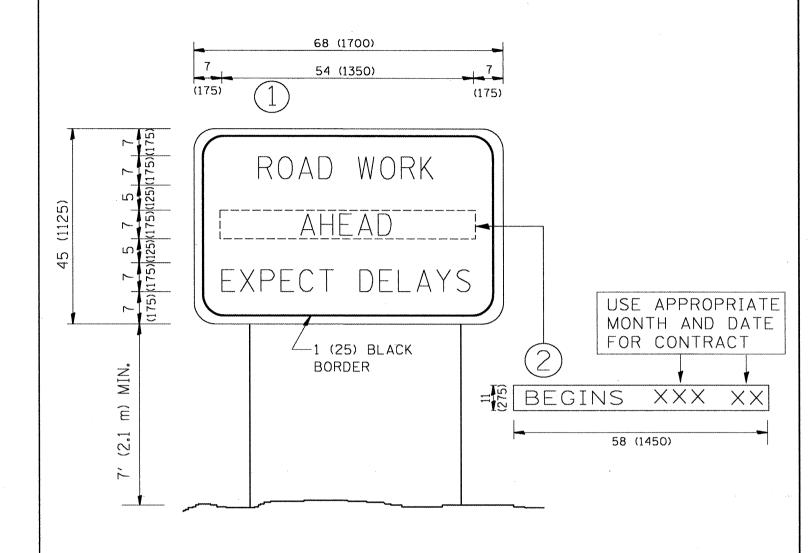
				ISTRICT O	 VE		F.A RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEE NO.
		TVDI	CAL E	AVENENT	MARKINGS		VAR. 2009-024 PP COOK 33				29
		HIF	UML F	WACINICIAI	WANKINGS	v		TC-13	CONTRACT	NO. 6	0G23
SCALE: NONE	SHEET N	0. 1	OF 1	SHEETS	STA.	TO STA.	FED. RO	DAD DIST. NO. 1   ILLINOIS FED. A	D PROJECT		



REVISED -T. RAMMACHER 09-08-9-FILE NAME = DESIGNED USER NAME = smithkl DRAWN REVISED - A. HOUSEH 11-07-95 CHECKED PLOT SCALE = 100.0000 '/ IN. REVISED - A. HOUSEH 10-12-96 DATE REVISED -T. RAMMACHER 01-06-0 PLOT DATE = 2/3/2009

**DEPARTMENT OF TRANSPORTATION** 

DistStd.dgn 2/3/2009 2:15:10 PM User≈smit



# NOTES:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN 1 WITH INSTALLED PANEL 2 ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL 2 SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

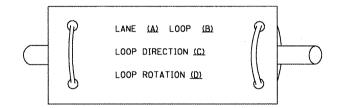
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED - R. MIRS 09-15-97		ARTERIAL ROAD	F.A. SECTION	ION COUNTY TOTAL SHEET
c:\pwwwork\PWIDOT\SM[THKL\dØ125Ø91\[	Dist <sup>9</sup> td.dgn	DRAWN -	REVISED - R. MIRS 12-11-97	STATE OF ILLINOIS		VAR. 2009-02	24 PP COOK 33 31
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	PLOT DATE = 2/3/2009	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1 IL	LLINOIS FED. AID PROJECT

### LOOP DETECTOR NOTES

- 1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- 2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- 3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- 4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- 5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- 7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

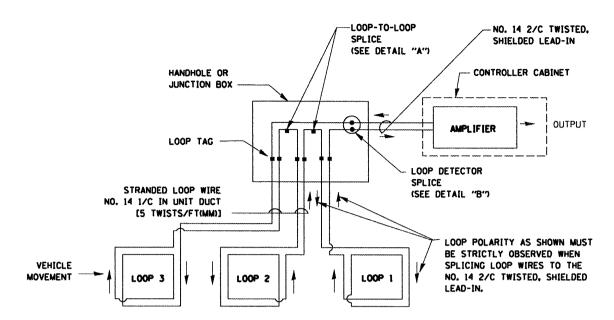
#### LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP "1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

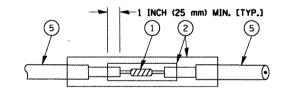
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D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

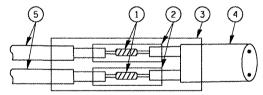


#### DETECTOR LOOP WIRING SCHEMATIC

- " LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- " LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.







DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

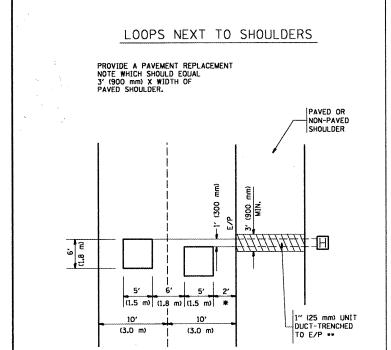
# LOOP DETECTOR SPLICE

- 1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- (2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER CRADE.
- (3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.
- (4) NO. 14 2/C TWISTED, SHIELDED CABLE.
- (5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

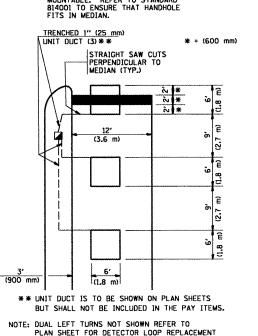
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1		VAR.	2009-024 PP	COOK	33	32
1			TS-05	CONTRACT	NO. 60	0G23
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# LEFT TURN LANES WITH MEDIANS VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

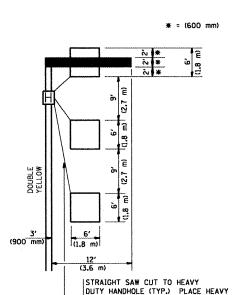
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



# VOLUME DENSITY ("FAR OUT" DETECTION) ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



DUTY HANDHOLE BETWEEN FIRST AND SECOND LOOP AS SHOWN.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

SCALE: NONE

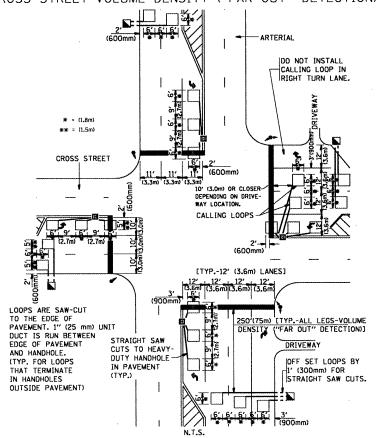
\* \* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

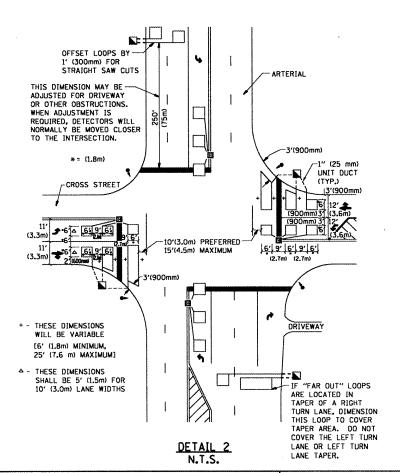
\* = (600 mm)

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)

CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)

CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)





#### NOTES:

#### VEHICLES LOOP DETECTORS

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF <u>ALL</u> DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

#### PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON  $\underline{\mathsf{ALL}}$  SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

#### NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

SHEET NO. 1 OF 1 SHEETS STA. TO STA. FED. ROAD