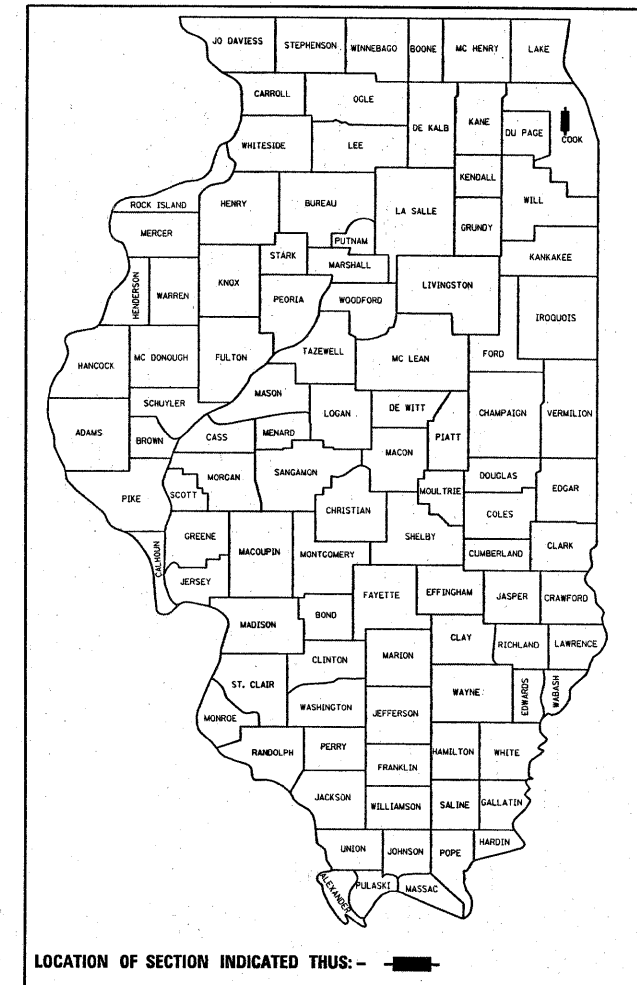


F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464Y-RS-2	COOK	24	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 60F07		

D -91-043-04



LOCATION OF SECTION INDICATED THUS: - -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PROPOSED
HIGHWAY PLANS**

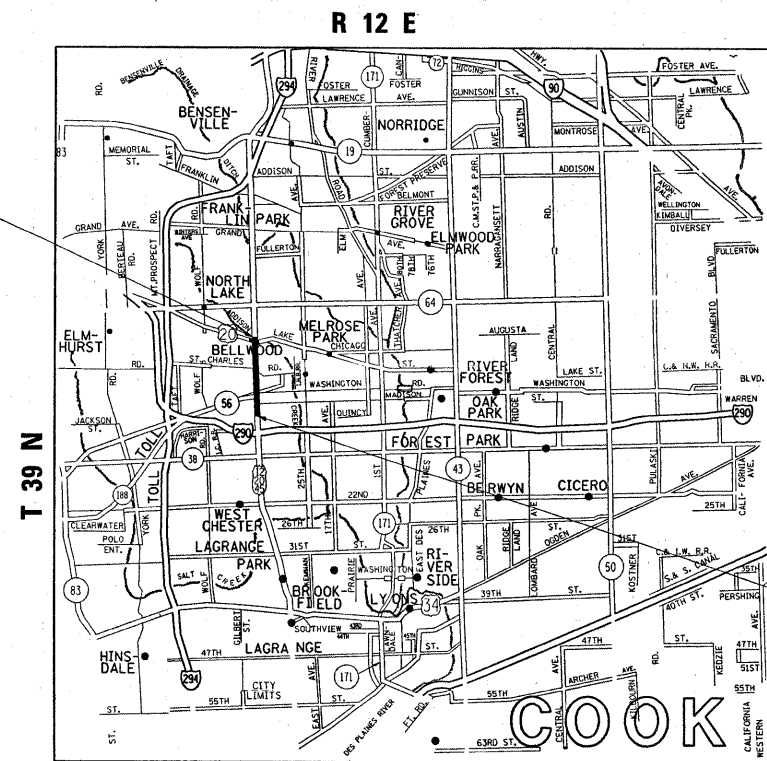
FAP 330 US 12/20/45 (MANNHEIM RD.)
LAKE ST. TO I-290
SECTION: 464Y-RS-2
RESURFACING (MAINTENANCE)
PROJECT: *ESP-0330(053)*
COOK COUNTY
C-91-028-09

FOR INDEX OF SHEETS, SEE SHEET NO. 2

THE PROJECT IS LOCATED IN THE VILLAGES OF BELLWOOD, HILLSIDE, MELROSE PARK AND STONE PARK

IMPROVEMENT ENDS
STA. 117+48

OMISSION
STA. 88+50 - STA. 102+58

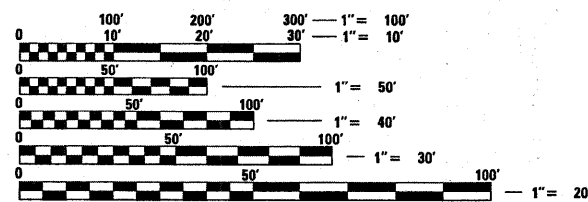


PROVISO TOWNSHIP

TRAFFIC DATA

2006 ADT = 41,100
POSTED SPEED LIMIT = 30-35 MPH

IMPROVEMENT BEGINS
STA. 29+67



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER FRITZ GUILLAUME
PROJECT MANAGER KEN ENG (847) 705-4247

CONTRACT NO. 60F07

GROSS LENGTH OF IMPROVEMENT = 8,781 LINEAL FEET = 1.66 MILE
NET LENGTH OF IMPROVEMENT = 7373 LINEAL FEET = 1.39 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED FEBRUARY 4, 20 09
Diane M. O'Keefe DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 27, 20 09
Charles G. Ingersoll ENGINEER OF DESIGN AND ENVIRONMENT
March 27, 20 09
Christie M. Reed DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, AND GENERAL NOTES
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS PLAN
5-8	ROADWAY & PAVEMENT MARKINGS PLANS
9-13	DETECTOR LOOP REPLACEMENT PLANS
14	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
15	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
16	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
17	BUTT JOINT AND HMA TAPER DETAILS
18	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
19	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)
20	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
21	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
22	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
23	ARTERIAL ROAD INFORMATION SIGN
24	DISTRICT ONE DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

LIST OF STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-03	FRAME AND LIDS, TYPE 1
604086-02	FRAME AND GRATES, TYPE 23
606001-04	COMBINATION CONCRETE CURB AND GUTTER
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701602-04	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701701-06	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-01	TRAFFIC CONTROL DEVICES
886001-01	DETECTOR LOOP INSTALLATION
886006-01	TYPICAL LAYOUT FOR DETECTOR LOOPS

GENERAL NOTES

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF BELLWOOD, HILLSIDE, MELROSE PARK AND STONE PARK.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (45 KM/H) OR LESS AND 1 INCH WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH (45 KM/H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

THE RESIDENT ENGINEER SHALL CONTACT MS PATRICE HARRIS AREA TRAFFIC FIELD ENGINEER AT (708) 597-9800 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKING.

THE RESIDENT ENGINEER SHALL VERIFY ALL EXISTING PAVEMENT MARKINGS BEFORE MILLING

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKING ON ALL FINAL SURFACES. THE COST OF THE PAVEMENT MARKING TAPE, TYPE III AND ITS REMOVAL SHALL BE INCLUDED IN THE COST OF SHORT TERM PAVEMENT MARKING.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847)705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

FILE NAME =	USER NAME = guilloumefp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, LIST OF STATE STANDARDS & GENERAL NOTES U.S. 12/2045 (I-290 - LAKE ST.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pwork\PIWID01\GUILLOUMEFP\dms88737	sh_rdygdgn	DRAWN -	REVISED -			330	464Y-RS-2	COOK	24	2	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60F07					
	PLOT DATE = 2/4/2009	DATE -	REVISED -			SCALE: 1"=50'	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

CONTRACT NO. 60F07

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464Y-RS-2	COOK	24	3
FED. ROAD DIST. NO. 1		ILLINOIS	HIGHWAY PROJECT	

SUMMARY OF QUANTITIES			URBAN 100% FED.		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	56	56					
40600300	AGGREGATE (PRIME COAT)	TON	280	280					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	22	22					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	507	507					
40603245	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N105	TON	1710	1710					
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	5450	5450					
40603600	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N105	TON	1330	1330					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	55550	55550					
44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SO YD	13500	13500					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	200	200					
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SO YD	150	150					
44201831	CLASS D PATCHES, TYPE III, 15 INCH	SO YD	200	200					
44201833	CLASS D PATCHES, TYPE IV, 15 INCH	SO YD	400	400					
NP 55039700	STORM SEWERS TO BE CLEANED	FOOT	600	600					
60250200	CATCH BASINS TO BE ADJUSTED	EACH	15	15					
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	98	98					
60404940	FRAMES AND GRATES, TYPE 23	EACH	25	25					
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	15	15					
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	30	30					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
67100100	MOBILIZATION	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					

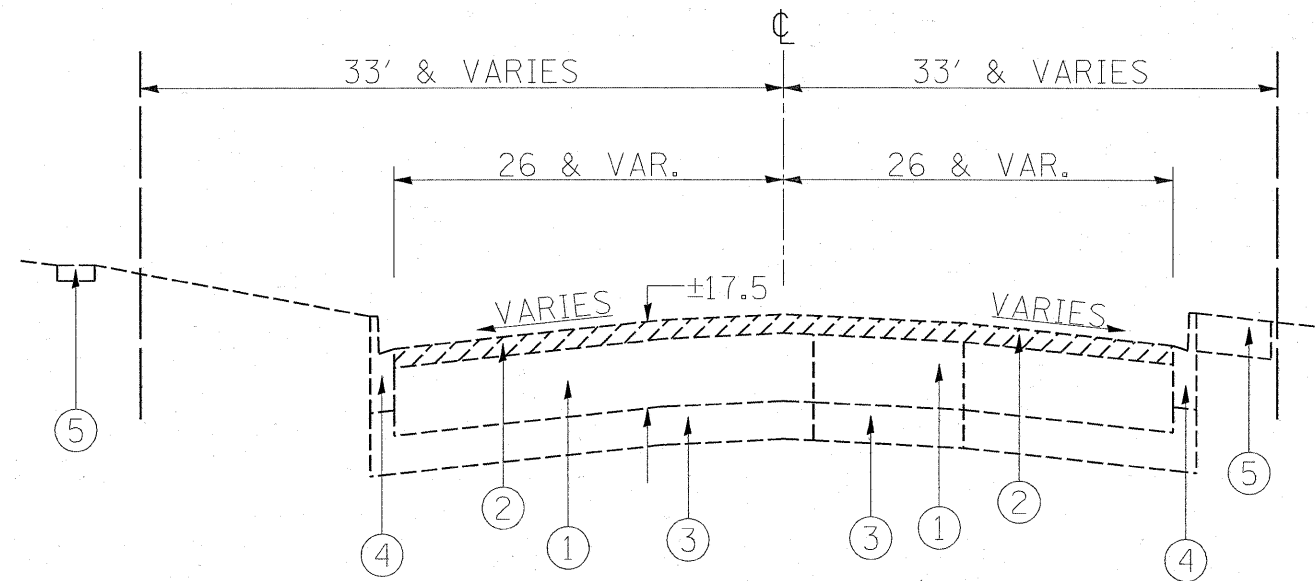
SUMMARY OF QUANTITIES			URBAN 100% FED.		CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	1000					
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	9720	9720					
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1236	1236					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	18140	18140					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4740	4740					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	420	420					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2600	2600					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	920	920					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	18140	18140					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4740	4740					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	420	420					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2600	2600					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	920	920					
* 78000900	THERMOPLASTIC PAVEMENT MARKING - ALKYD - LETTERS AND SYMBOLS	SO FT	1236	1236					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	920	920					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	920	920					
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	3549	3549					
X0322256	TEMPORARY INFORMATION SIGNING	SO FT	330	330					
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	2910	2910					
0 20076600	TRAINEES	HOUR	500	500					

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* Specialty Items
 NP= Non-participating
 © Y080

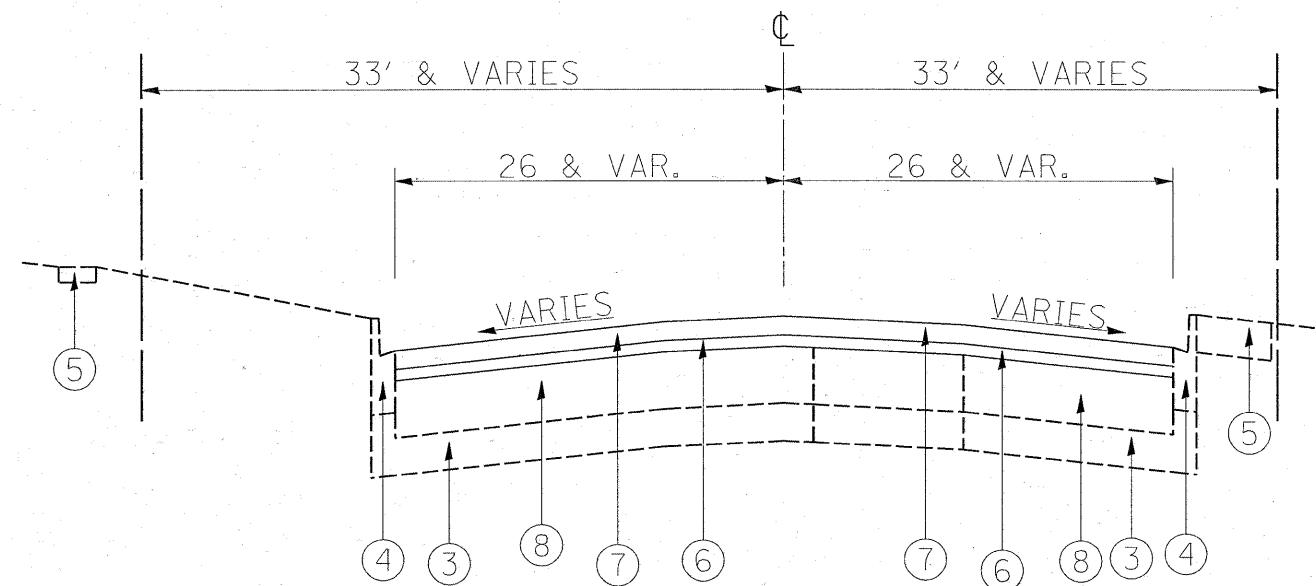
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 SUMMARY OF QUANTITIES



EXISTING TYPICAL CROSS SECTION
U.S. RTE. 12/20/45 (MANNHEIM RD.)

- LEGEND**
- ① EXISTING HMA SURFACE COURSE, 17 1/2"(±)
 - ② PROPOSED HMA SURFACE REMOVAL, 2 1/2"
 - ③ EXISTING SUB-BASE GRANULAR MATERIAL, TYPE A, 6"
 - ④ EXISTING CURB & GUTTER, B-6.24(STA. 29+67 TO 43+60) AND B-6.12 STA. 43+60 TO 117+48
 - ⑤ EXISTING P.C.C. SIDEWALK, 5"
 - ⑥ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
 - ⑦ PROPOSED POLYMERIZED HMA SURFACE COURSE, MIX "F", N90, 1 3/4"
 - ⑧ EXISTING HMA SURFACE OVERLAY, ±15"



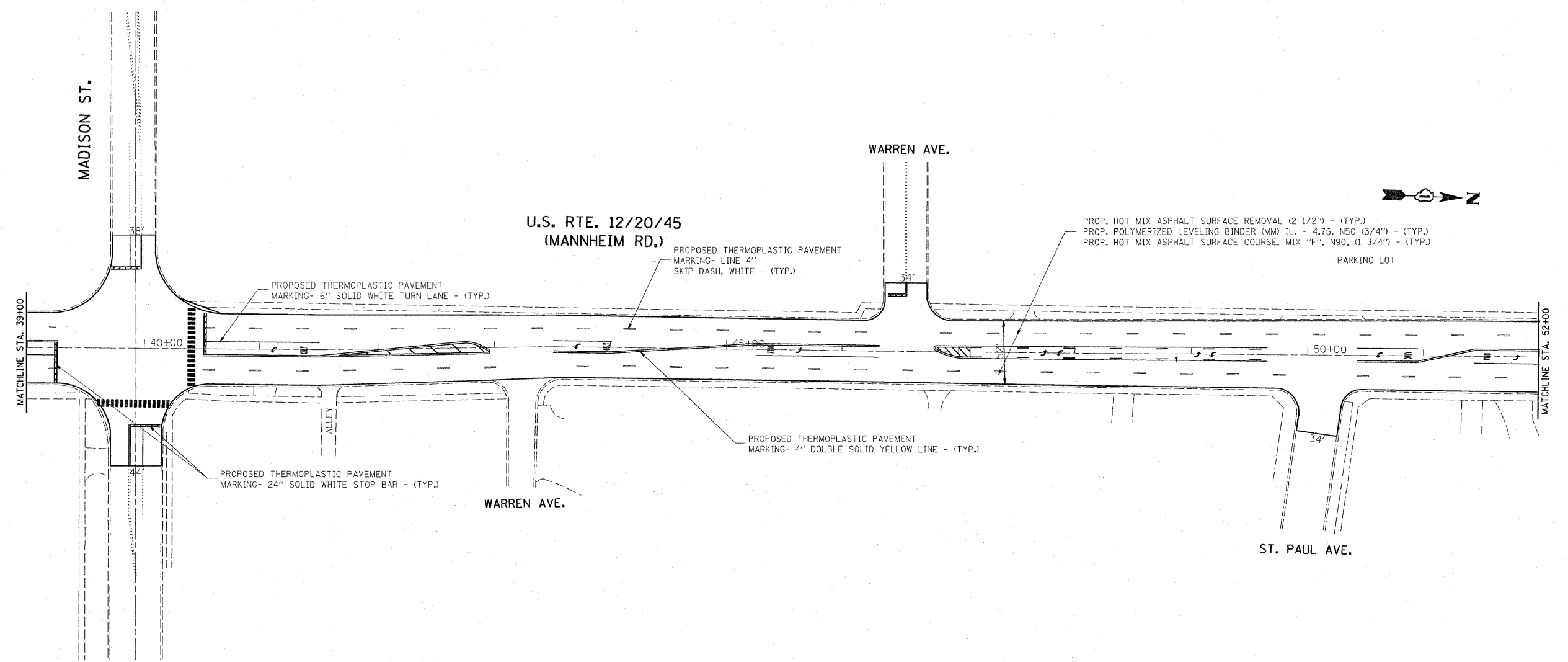
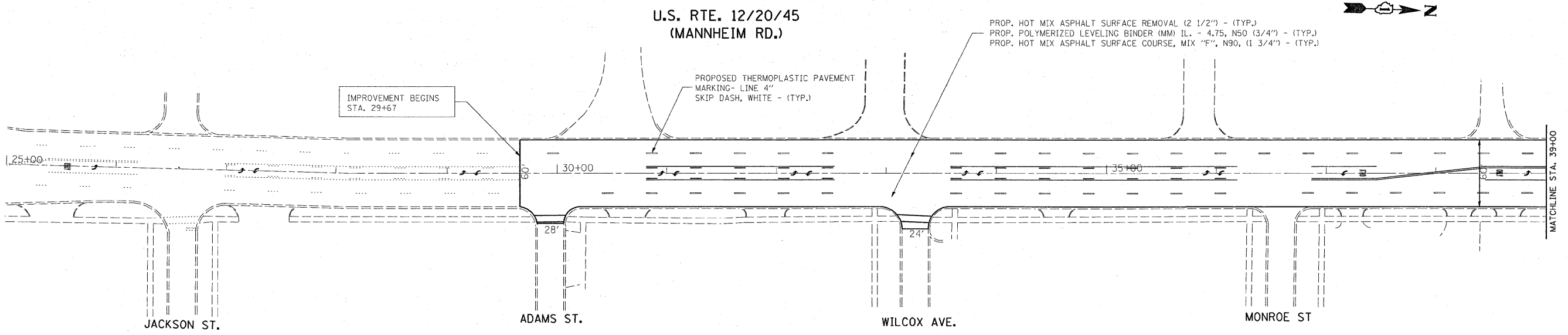
PROPOSED TYPICAL CROSS SECTION
U.S. RTE. 12/20/45 (MANNHEIM RD.)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AC TYPE	AIR VOIDS (%)
POLYMERIZED HMA SURFACE COURSE, MIX "F", N90 (IL 9.5 mm)	SBS/SBR PG 70-22	4% AT 90 GYR.
POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50	SBS/SBR PG 76-28/-22	4% AT 50 GYR.
CLASS D PATCHES, (HMA BINDER IL 19 mm)	PG 64-22*	4% AT 70 GYR.
POLYMERIZED HMA SURFACE COURSE, MIX "F", N105 (IL 9.5 mm)	SBS/SBR PG 76-22	4% AT 105 GYR.
POLYMERIZED HMA BINDER COURSE, MIX "F", N105 (IL 9.5 mm)	SBS/SBR PG 76-22	4% AT 105 GYR.

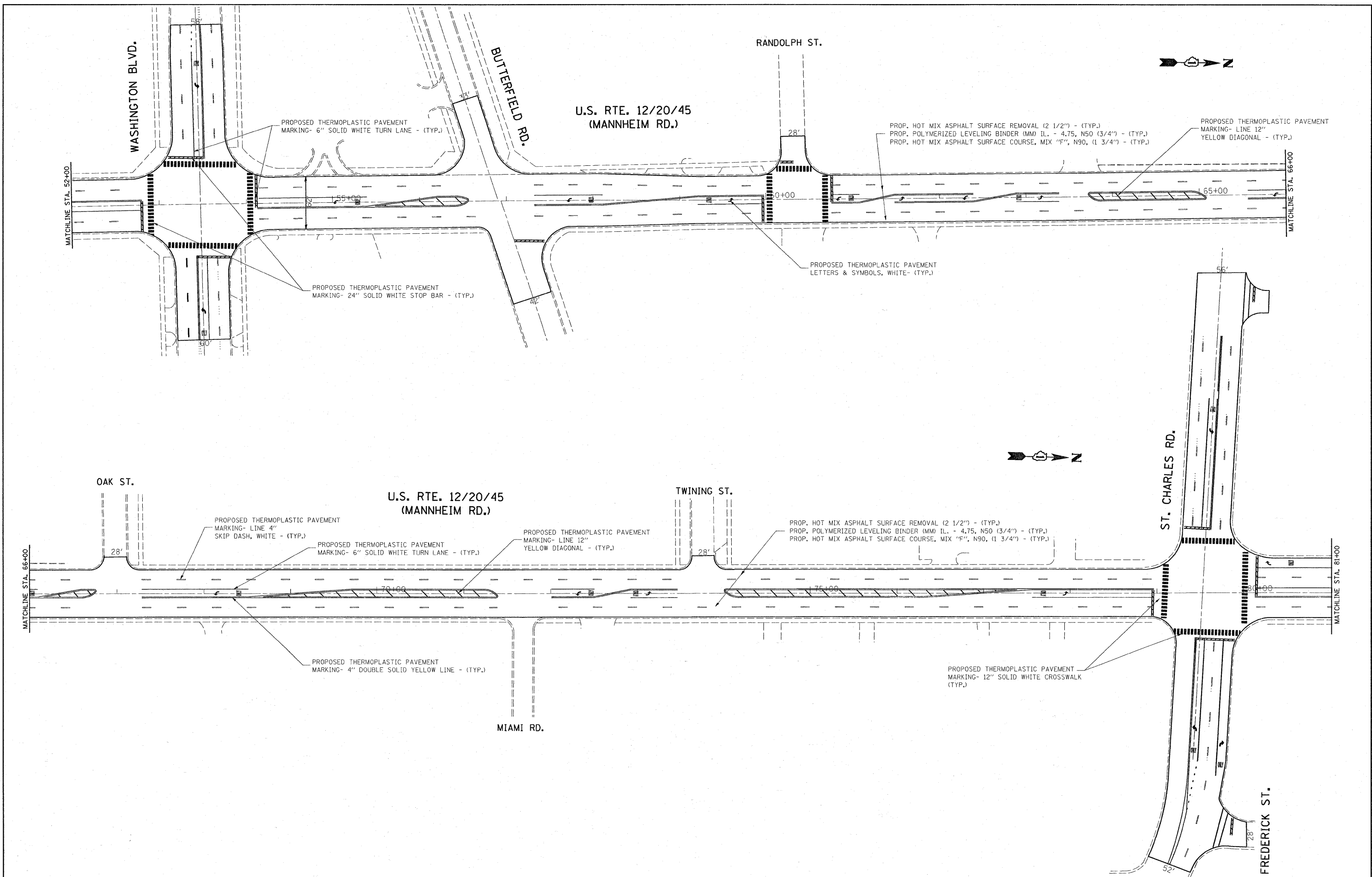
NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE COURSE MIXTURES IS 112 LBS/SQ YD/IN
*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

NOTE:
THE CONTRACTOR SHALL MILL FIRST (BEFORE CLASS D PATCHING)



FILE NAME = c:\pw_work\PW1001\GULLLAUMEFP\dms88737	USER NAME = guillaumefp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN U.S. 12/20/45 (I-290 - LAKE ST.)		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	sh_rdwj.dgn	DRAWN -	REVISED -				330	464 Y-RS-2	COOK	24	5
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -				CONTRACT NO. 60F07				
	PLOT DATE = 2/4/2009	DATE -	REVISED -				SCALE: 1"=50'		SHEET NO. 1 OF 4 SHEETS		STA. 25+00 TO STA. 52+00



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USER NAME = guillaumefp
 sh_rdwj.dgn
 PLOT SCALE = 50.0000 / IN.
 PLOT DATE = 2/4/2009

DESIGNED -
 DRAWN -
 CHECKED -
 DATE -

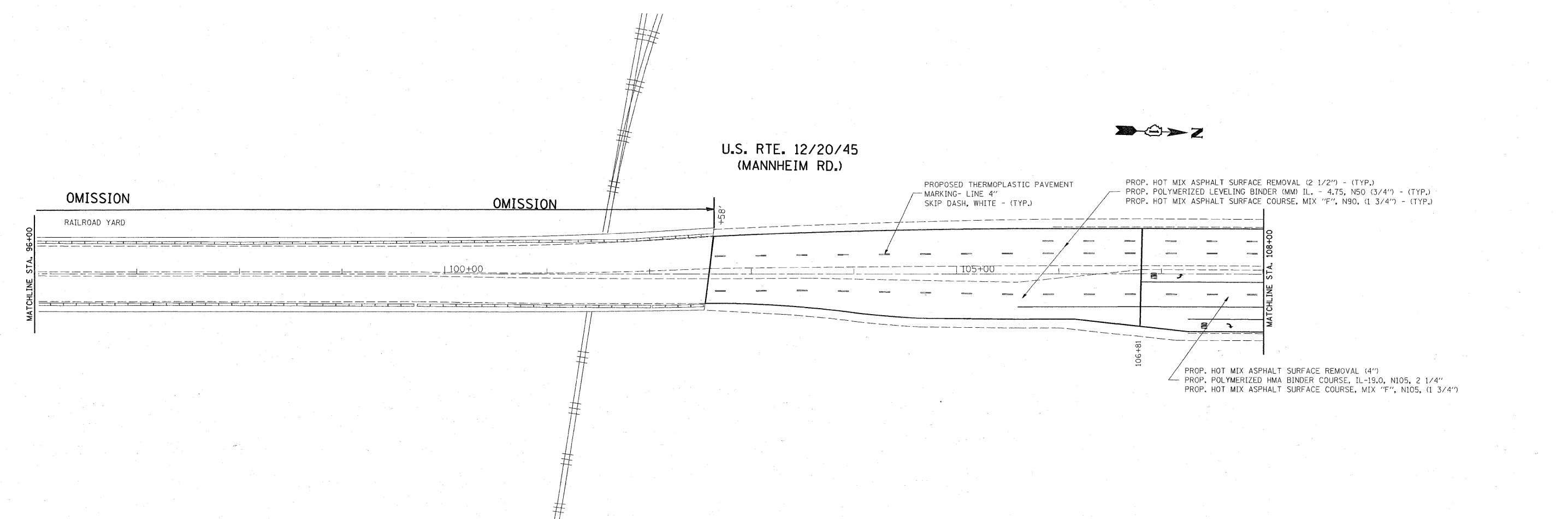
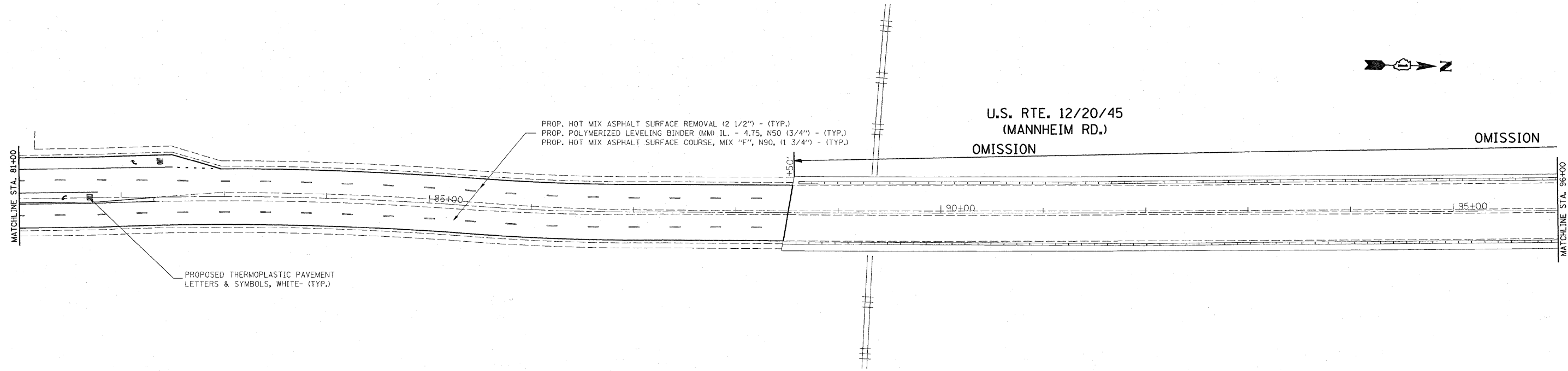
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

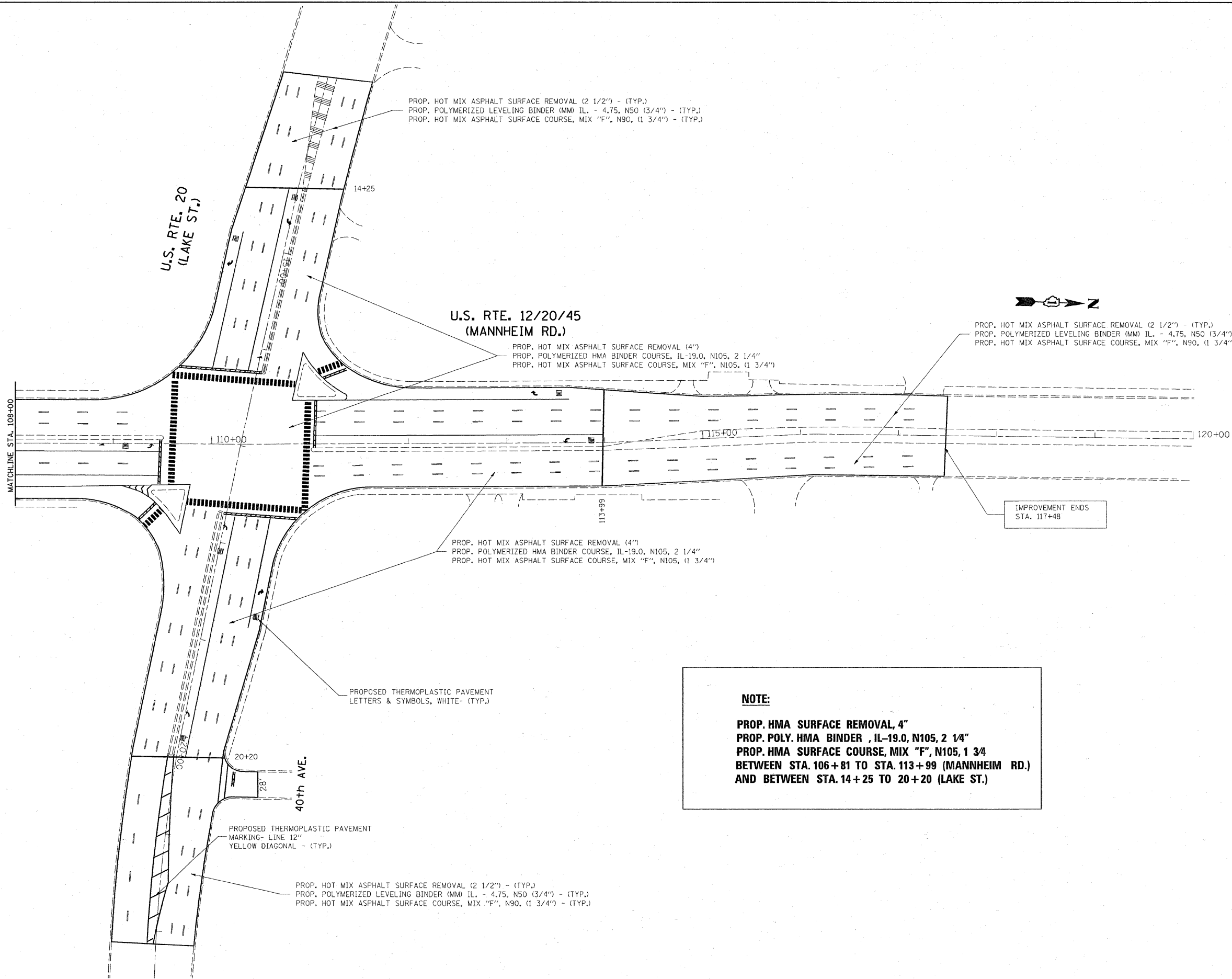
**ROADWAY & PAVEMENT MARKING PLAN
 U.S. 12/20/45 (I-290 - LAKE ST.)**

SCALE: 1"=50' SHEET NO. 2 OF 4 SHEETS STA. 52+00 TO STA. 82+00

F.A.P. RTE. 330	SECTION 464 Y-RS-2	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 6
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60F07	



FILE NAME =	USER NAME = guillaumejp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN U.S. 12/20/45 (I-290 - LAKE ST.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\PWIDOT\GUILLAUMEFP\dms88737	sh_rdw.dgn	DRAWN -	REVISED -			330	464 Y-RS-2	COOK	24	7	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60F07					
	PLOT DATE = 2/4/2009	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
					SCALE: 1"=50'	SHEET NO. 3 OF 4 SHEETS		STA. 81+00 TO STA. 108+00			



NOTE:

**PROP. HMA SURFACE REMOVAL, 4"
 PROP. POLY. HMA BINDER, IL-19.0, N105, 2 1/4"
 PROP. HMA SURFACE COURSE, MIX "F", N105, 1 3/4"
 BETWEEN STA. 106+81 TO STA. 113+99 (MANNHEIM RD.)
 AND BETWEEN STA. 14+25 TO 20+20 (LAKE ST.)**

FILE NAME =	USER NAME = gulloumefp	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN U.S. 12/2045 (I-290 - LAKE ST.)	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pw_work\PWIDOT\GULLLAUMEFP\dms88737	sh_rdwj.dgn	DRAWN -	REVISED -			330	464 Y-RS-2	COOK	24	8	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60F07					
	PLOT DATE = 2/5/2009	DATE -	REVISED -			SCALE: 1"=50' SHEET NO. 4 OF 4 SHEETS STA. 108+00 TO STA. 120+00 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					

REPLACE DETECTOR LOOPS WHERE INDICATED

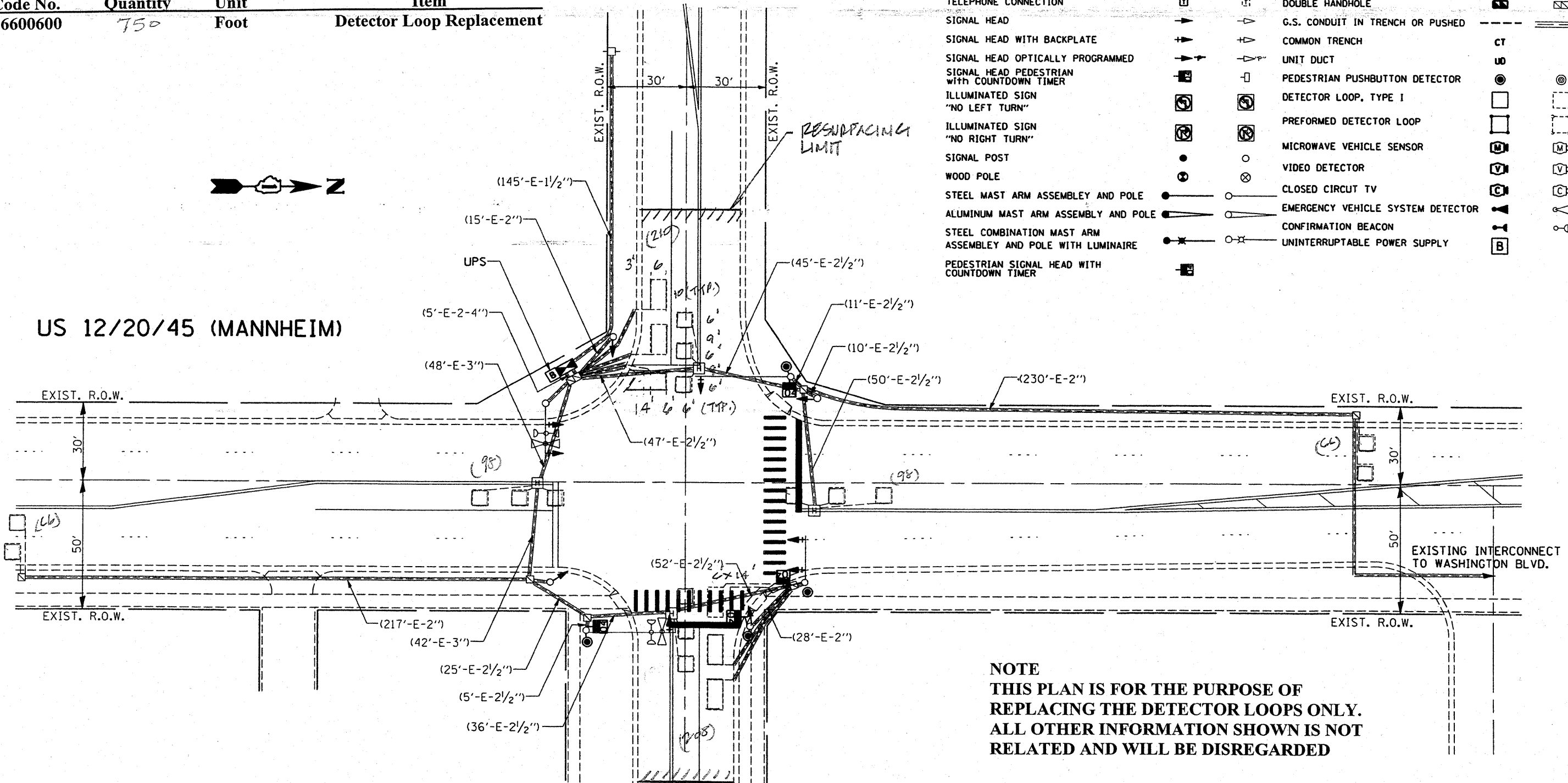
(WITHIN THE RESURFACING LIMITS)

Code No.	Quantity	Unit	Item
86600600	750	Foot	Detector Loop Replacement

MADISON STREET

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING		PROPOSED	EXISTING
CONTROLLER CABINET			JUNCTION BOX		
RAILROAD CONTROL CABINET			HANDHOLE		
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT			HEAVY DUTY HANDHOLE		
TELEPHONE CONNECTION			DOUBLE HANDHOLE		
SIGNAL HEAD			G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE			COMMON TRENCH	CT	
SIGNAL HEAD OPTICALLY PROGRAMMED			UNIT DUCT	UD	
SIGNAL HEAD PEDESTRIAN WITH COUNTDOWN TIMER			PEDESTRIAN PUSHBUTTON DETECTOR		
ILLUMINATED SIGN "NO LEFT TURN"			DETECTOR LOOP, TYPE I		
ILLUMINATED SIGN "NO RIGHT TURN"			PREFORMED DETECTOR LOOP		
SIGNAL POST			MICROWAVE VEHICLE SENSOR		
WOOD POLE			VIDEO DETECTOR		
STEEL MAST ARM ASSEMBLY AND POLE			CLOSED CIRCUIT TV		
ALUMINUM MAST ARM ASSEMBLY AND POLE			EMERGENCY VEHICLE SYSTEM DETECTOR		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			CONFIRMATION BEACON		
PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER			UNINTERRUPTABLE POWER SUPPLY		



NOTE
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE ADJACENT SYSTEM.

RESTORATION OF WORK AREA, RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOO, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

FILE NAME =	USER NAME = kanthaphixybc	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TRAFFIC SIGNAL PLAN US 12/20/45 (MANNHEIM) @ MADISON ST.	F.A.P. RTE. 330	SECTION *	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 9
PROJECT = traffic\070027\us12_20_45.dgn	PLOT SCALE = 48.0000' / IN.	CHECKED - DAD	REVISED -			CONTRACT NO. 60907				
PLOT DATE = 10/10/2008	DATE -	REVISED -	REVISED -			FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			
						SCALE: 1"=20'	SHEET NO. OF SHEETS STA. TO STA.			

*464Y-R5-2

WASHINGTON BLVD.

MATCH LINE A

TRAFFIC SIGNAL LEGEND

		PROPOSED	EXISTING		
CONTROLLER CABINET				PROPOSED	EXISTING
RAILROAD CONTROL CABINET					
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT					
TELEPHONE CONNECTION					
SIGNAL HEAD					
SIGNAL HEAD WITH BACKPLATE					
SIGNAL HEAD OPTICALLY PROGRAMMED					
SIGNAL HEAD PEDESTRIAN WITH COUNTDOWN TIMER					
ILLUMINATED SIGN "NO LEFT TURN"					
ILLUMINATED SIGN "NO RIGHT TURN"					
SIGNAL POST					
WOOD POLE					
STEEL MAST ARM ASSEMBLY AND POLE					
ALUMINUM MAST ARM ASSEMBLY AND POLE					
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE					
JUNCTION BOX					
HANDHOLE					
HEAVY DUTY HANDHOLE					
DOUBLE HANDHOLE					
G.S. CONDUIT IN TRENCH OR PUSHED					
COMMON TRENCH	CT				
UNIT DUCT	UD				
PEDESTRIAN PUSHBUTTON DETECTOR					
DETECTOR LOOP, TYPE I					
PERFORMED DETECTOR LOOP					
MICROWAVE VEHICLE SENSOR					
VIDEO DETECTOR					
CLOSED CIRCUIT TV					
EMERGENCY VEHICLE SYSTEM DETECTOR					
CONFIRMATION BEACON					
UNINTERRUPTIBLE POWER SUPPLY					

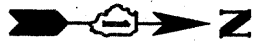
PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DET.

MATCH LINE A

NOTE
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED

US 12/20/45 (MANNHEIM)

LIMIT OF RESURFACING



EXISTING INTERCONNECT TO RANDOLPH ST.

250' TO STOPBAR

250' TO STOPBAR

EXISTING INTERCONNECT TO MADISON ST.

NOTE: THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE ADJACENT SYSTEM.

REPLACE DETECTOR LOOPS WHERE INDICATED

(WITHIN THE RESURFACING LIMITS)

Code No.	Quantity	Unit	Item
86600600	426	Foot	Detector Loop Replacement

MATCH LINE B

MATCH LINE B

PROPOSED INTERSECTION AND SAMPLING(SYSTEM) DETECTORS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TRAFFIC SIGNAL PLAN
US 12/20/45 (MANNHEIM) @ WASHINGTON BLVD.

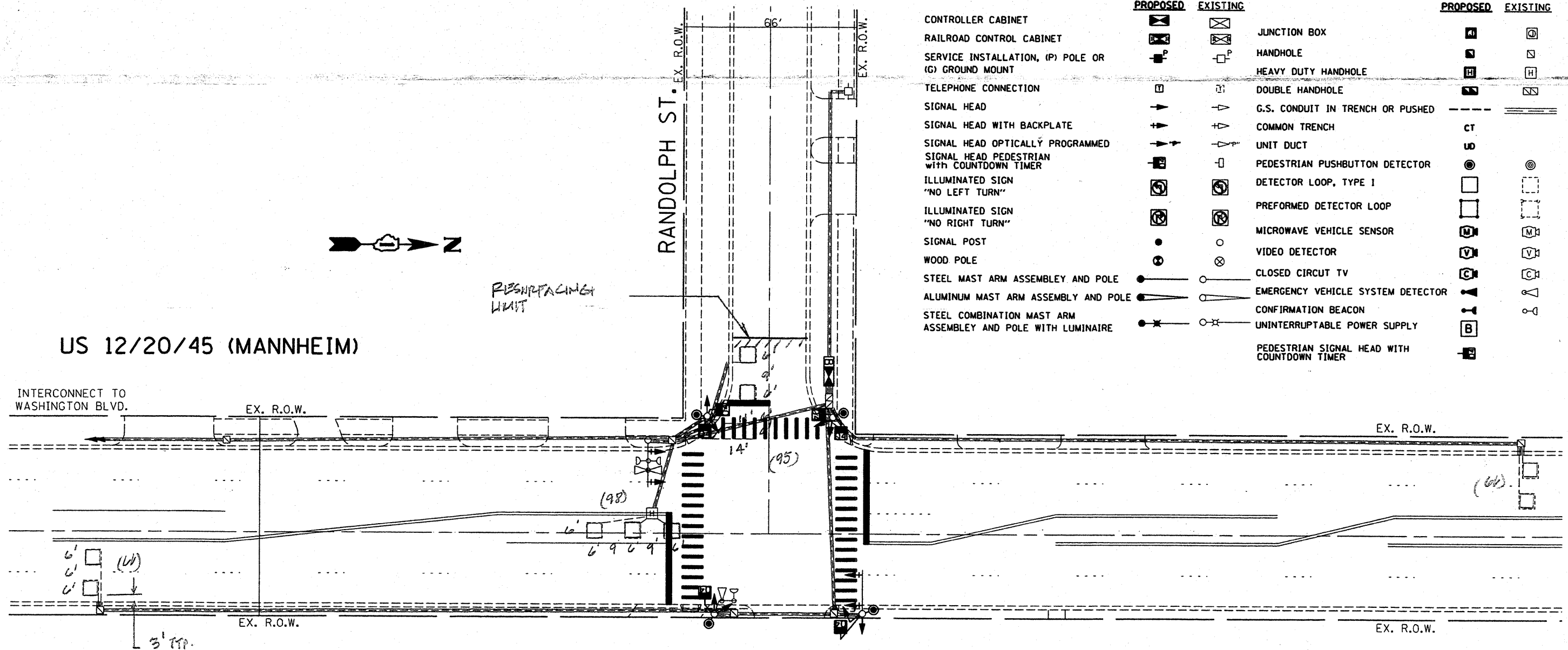
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330		COOK	24	10
FED. ROAD DIST. NO. (ILLINOIS) FED. AID PROJECT			CONTRACT NO. 60E31	

SCALE: 1"=20' SHEET NO. OF SHEETS STA. TO STA.

*464Y-RS-2

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING		PROPOSED	EXISTING
CONTROLLER CABINET			JUNCTION BOX		
RAILROAD CONTROL CABINET			HANDHOLE		
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT			HEAVY DUTY HANDHOLE		
TELEPHONE CONNECTION			DOUBLE HANDHOLE		
SIGNAL HEAD			G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE			COMMON TRENCH	CT	
SIGNAL HEAD OPTICALLY PROGRAMMED			UNIT DUCT	UD	
SIGNAL HEAD PEDESTRIAN with COUNTDOWN TIMER			PEDESTRIAN PUSHBUTTON DETECTOR		
ILLUMINATED SIGN "NO LEFT TURN"			DETECTOR LOOP, TYPE I		
ILLUMINATED SIGN "NO RIGHT TURN"			PERFORMED DETECTOR LOOP		
SIGNAL POST			MICROWAVE VEHICLE SENSOR		
WOOD POLE			VIDEO DETECTOR		
STEEL MAST ARM ASSEMBLY AND POLE			CLOSED CIRCUIT TV		
ALUMINUM MAST ARM ASSEMBLY AND POLE			EMERGENCY VEHICLE SYSTEM DETECTOR		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			CONFIRMATION BEACON		
			UNINTERRUPTIBLE POWER SUPPLY		
			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		



US 12/20/45 (MANNHEIM)

REPLACE DETECTOR LOOPS WHERE INDICATED

(WITHIN THE RESURFACING LIMITS)

NOTE
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

Code No.	Quantity	Unit	Item
86600600	325	Foot	Detector Loop Replacement

NOTE:
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE 'ECONOLITE' TO MATCH THE ADJACENT SYSTEM.

FILE NAME =	USER NAME = kanthaphaybc	DESIGNED - BCK	REVISIONS	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TRAFFIC SIGNAL PLAN US 12/20/45 (MANNHEIM) @ RANDOLPH ST.	F.A.P. RTE. 330	SECTION	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 11		
	PLOT SCALE = 40.0000' / IN.	CHECKED - DAD	REVISIONS			SCALE: 1"=20'	SHEET NO. OF SHEETS	STA. TO STA.	CONTRACT NO. 6031			
	PLOT DATE = 10/10/2008	DATE	REVISIONS			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT						
*464Y-RS-2												

60907

TRAFFIC SIGNAL LEGEND

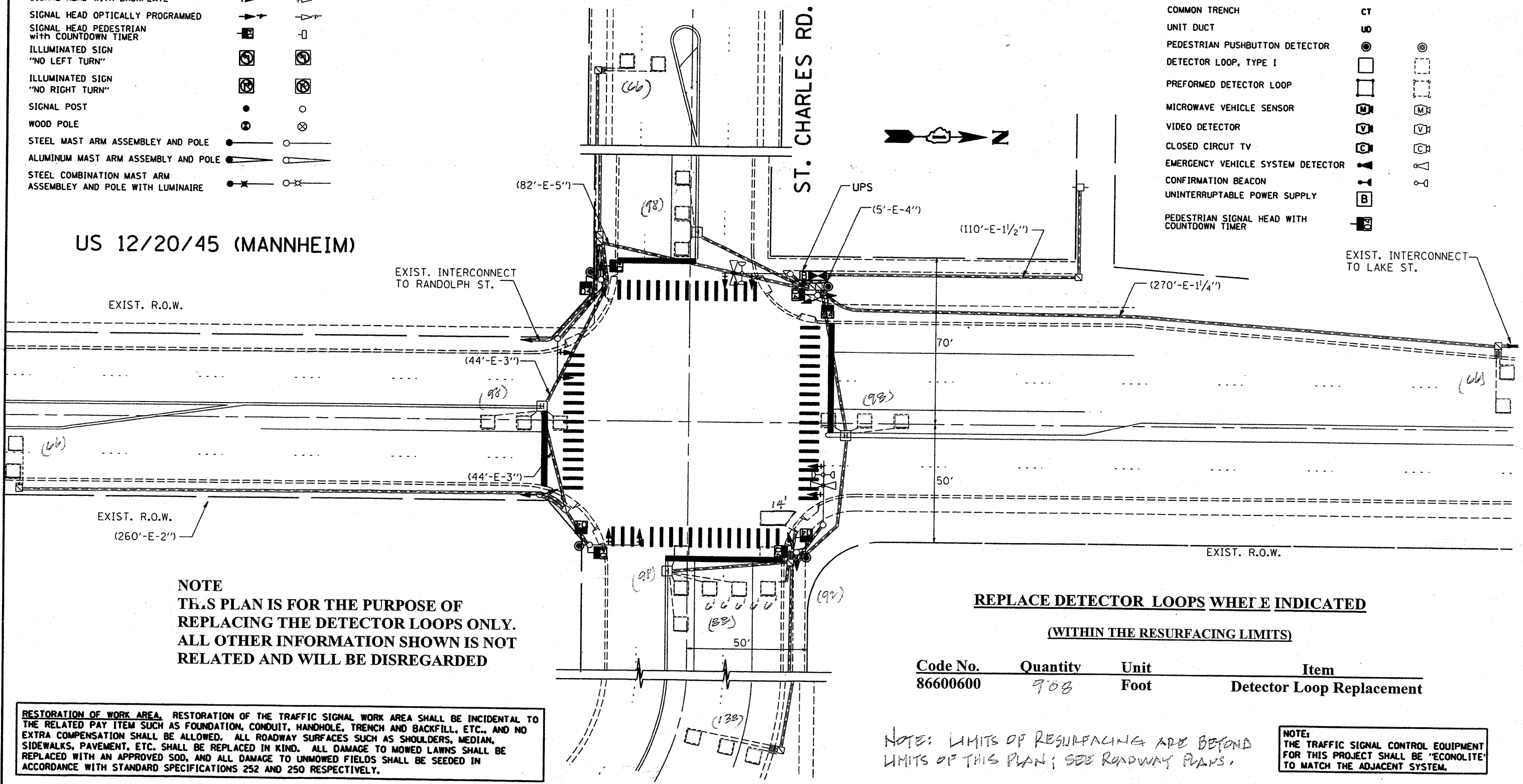
	PROPOSED	EXISTING
CONTROLLER CABINET		
RAILROAD CONTROL CABINET		
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT		
TELEPHONE CONNECTION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD OPTICALLY PROGRAMMED		
SIGNAL HEAD PEDESTRIAN WITH COUNTDOWN TIMER		
ILLUMINATED SIGN "NO LEFT TURN"		
ILLUMINATED SIGN "NO RIGHT TURN"		
SIGNAL POST		
WOOD POLE		
STEEL MAST ARM ASSEMBLY AND POLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE		

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
JUNCTION BOX		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN TRENCH OR PUSHED		
COMMON TRENCH	CT	
UNIT DUCT	UD	
PEDESTRIAN PUSHBUTTON DETECTOR		
DETECTOR LOOP, TYPE I		
PREFORMED DETECTOR LOOP		
MICROWAVE VEHICLE SENSOR		
VIDEO DETECTOR		
CLOSED CIRCUIT TV		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
UNINTERRUPTIBLE POWER SUPPLY	B	
PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		

US 12/20/45 (MANNHEIM)

ST. CHARLES RD.



NOTE
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED

REPLACE DETECTOR LOOPS WHERE INDICATED
(WITHIN THE RESURFACING LIMITS)

Code No.	Quantity	Unit	Item
86600600	908	Foot	Detector Loop Replacement

NOTE: LIMITS OF RESURFACING ARE BEYOND LIMITS OF THIS PLAN; SEE ROADWAY PLANS.

NOTE: THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE ADJACENT SYSTEM.

RESTORATION OF WORK AREA. RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TRAFFIC SIGNAL PLAN
US 12/20/45 (MANNHEIM) @ ST. CHARLES RD.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330		COOK	24	12

FILE NAME =	USER NAME = kenthphraybc	DESIGNED - BCK	REVISED -
ct:\projects\traffic\12045\us12_20_45.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE -	REVISED -

SCALE: 1"=20' SHEET NO. OF SHEETS STA. TO STA.

*464Y-RS-2

60F07

RESTORATION OF WORK AREA RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIAN, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD, AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

CONSTRUCTION NOTES:

- ① RE-STRIP EXISTING THERMOPLASTIC PAVEMENT MARKING STOP LINE WITH HIGH VISIBILITY THERMOPLASTIC PAVEMENT MARKING STOP LINE 24" (TYP.)

NOTE
THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED

NOTE:
RELOCATION OF THE EXISTING EMERGENCY VEHICLE PRIORITY SYSTEM, PHASING UNIT, IS INCIDENTAL TO THE COST OF THE NEW CONTROLLER.

REPLACE DETECTOR LOOPS WHERE INDICATED
(WITHIN THE RESURFACING LIMITS)

Code No.	Quantity	Unit	Item
86600600	1140	Foot	Detector Loop Replacement

60907

INTERCONNECT TO HIRSCH (SOFFEL) AVE.
(SEE INTERCONNECT PLAN)

EXISTING INTERCONNECT TO 44 TH AV.

EXISTING R.O.W.

LAKE STREET

(MANNHEIM ROAD)

EXISTING R.O.W.

(6'-E-(31")

TRAFFIC SIGNAL LEGEND

- | | |
|--|--|
| PROPOSED | EXISTING |
| CONTROLLER | CONTROLLER |
| RAILROAD CONTROL CABINET | RAILROAD CONTROL CABINET |
| SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNTED | SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNTED |
| SIGNAL HEAD | SIGNAL HEAD |
| SIGNAL HEAD WITH BACKPLATE | SIGNAL HEAD WITH BACKPLATE |
| SIGNAL HEAD, PEDESTRIAN | SIGNAL HEAD, PEDESTRIAN |
| SIGNAL POST | SIGNAL POST |
| MAST ARM ASSEMBLY AND POLE, STEEL | MAST ARM ASSEMBLY AND POLE, STEEL |
| MAST ARM ASSEMBLY AND POLE, ALUMINIUM | MAST ARM ASSEMBLY AND POLE, ALUMINIUM |
| COMMON TRENCH | COMMON TRENCH |
| UNIT DUCT | UNIT DUCT |
| HANDHOLE | HANDHOLE |
| HEAVY DUTY HANDHOLE | HEAVY DUTY HANDHOLE |
| DOUBLE HANDHOLE | DOUBLE HANDHOLE |
| G.S.CONDUIT IN TRENCH OR PUSHED | G.S.CONDUIT IN TRENCH OR PUSHED |
| CAST IRON JUNCTION BOX | CAST IRON JUNCTION BOX |
| SIGNAL HEAD OPTICALLY PROGRAMMED | SIGNAL HEAD OPTICALLY PROGRAMMED |
| CONDUIT SPLICE | CONDUIT SPLICE |
| WOOD POLE | WOOD POLE |
| RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II | RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II |
| VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE | VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE |
| ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN" | ILLUMINATED SIGN, FIBER OPTIC "NO LEFT TURN" |
| ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN" | ILLUMINATED SIGN, FIBER OPTIC "NO RIGHT TURN" |
| TELEPHONE CONNECTION | TELEPHONE CONNECTION |
| PEDESTRIAN PUSHBUTTON DETECTOR | PEDESTRIAN PUSHBUTTON DETECTOR |
| DETECTOR LOOP, TYPE I | DETECTOR LOOP, TYPE I |
| PREFORMED DETECTOR LOOP | PREFORMED DETECTOR LOOP |
| CLOSED CIRCUIT TV | CLOSED CIRCUIT TV |
| EMERGENCY VEHICLE SYSTEM DETECTOR | EMERGENCY VEHICLE SYSTEM DETECTOR |
| CONFIRMATION BEACON | CONFIRMATION BEACON |
| UNINTERRUPTIBLE POWER SUPPLY | UNINTERRUPTIBLE POWER SUPPLY |
| PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER | PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER |

NOTE:
RESURFACING LIMITS ARE BEYOND LIMITS OF THIS PLAN; SEE ROADWAY PLANS.

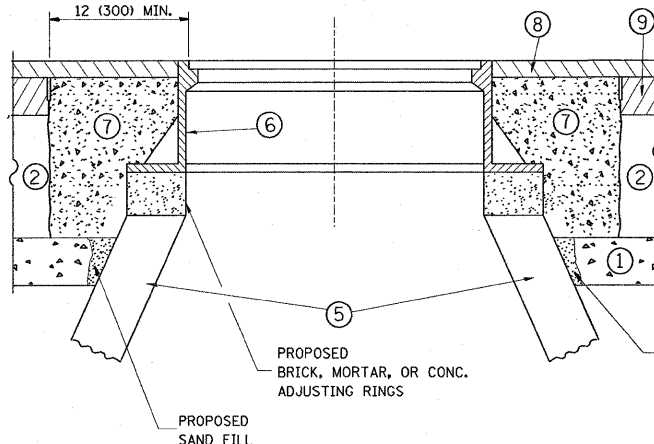
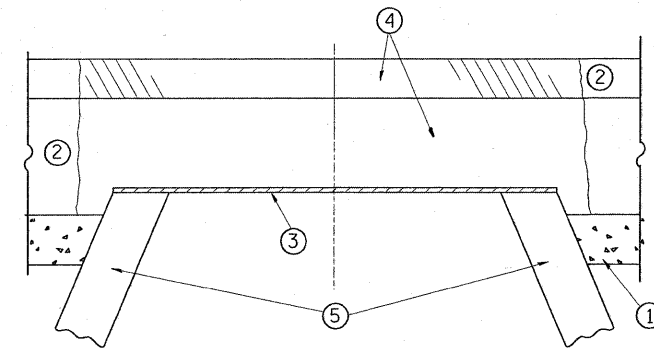
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TRAFFIC SIGNAL PLAN
US 12/2045 (MANNHEIM ROAD) AT LAKE AVENUE

F.A.P. RTE. 330	SECTION	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 113
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		CONTRACT NO. 60E31

SCALE: 1"=20' SHEET NO. OF SHEETS STA. TO STA.

*464Y-RS-2



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

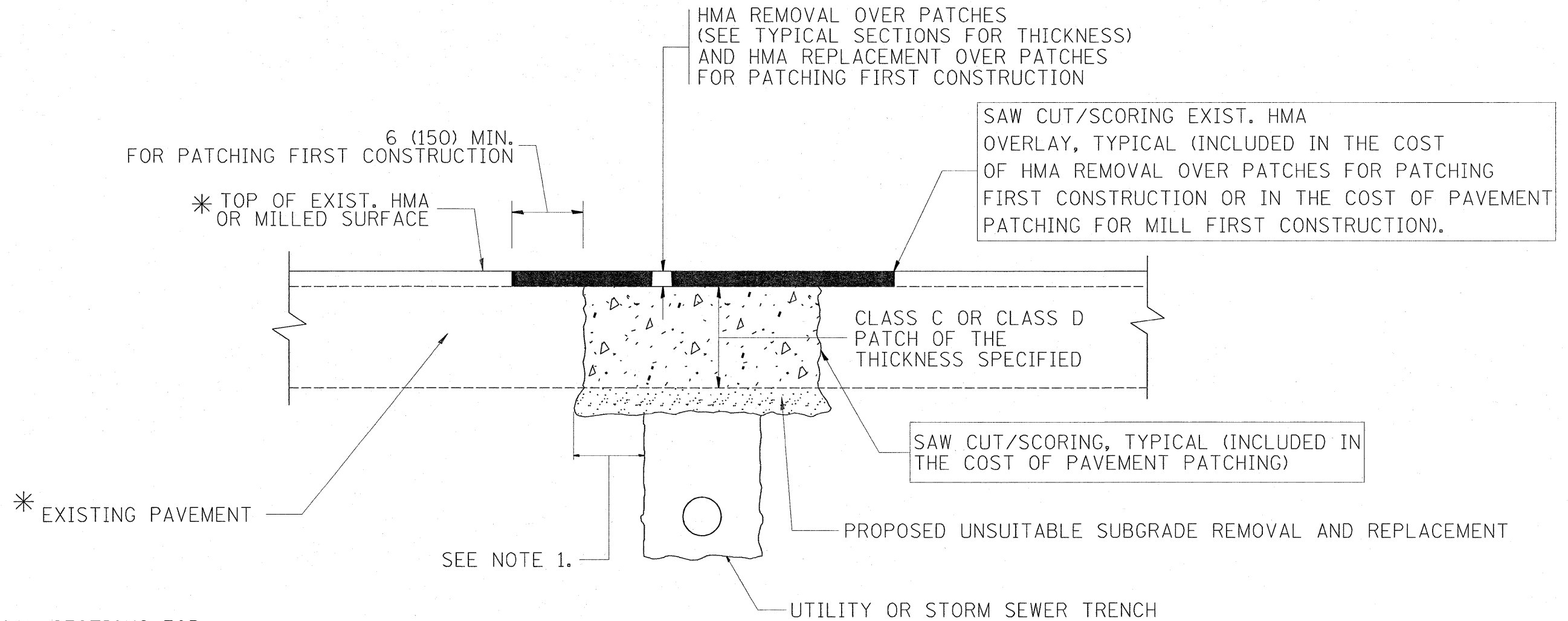
THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = guillaumejp	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.P. RTE. 330	SECTION 464Y-RS-2	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 14	
c:\pwwork\PWIDOT\GUILLAUMEFP\dms88737	sh_rdw.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	BD600-03 (BD-8)		CONTRACT NO. 60F07		
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04									
	PLOT DATE = 2/4/2009	DATE - 10-25-94	REVISED - R. BORO 01-01-07									



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulloumefp	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.P. RTE. 330	SECTION 464Y-RS-2	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 15		
oi:\pwork\PWIDOT\GUILLAUMEFP\dms88737	sh_rdw.dgn	DRAWN -	REVISED - R. BORO 01-01-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. _____ TO STA. _____	BD400-04 (BD-22)			
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07			CONTRACT NO. 60F07						
	PLOT DATE = 2/4/2009	DATE - 10-25-94	REVISED - K. ENG 10-27-08			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SALT TOLERANT SOD AND TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

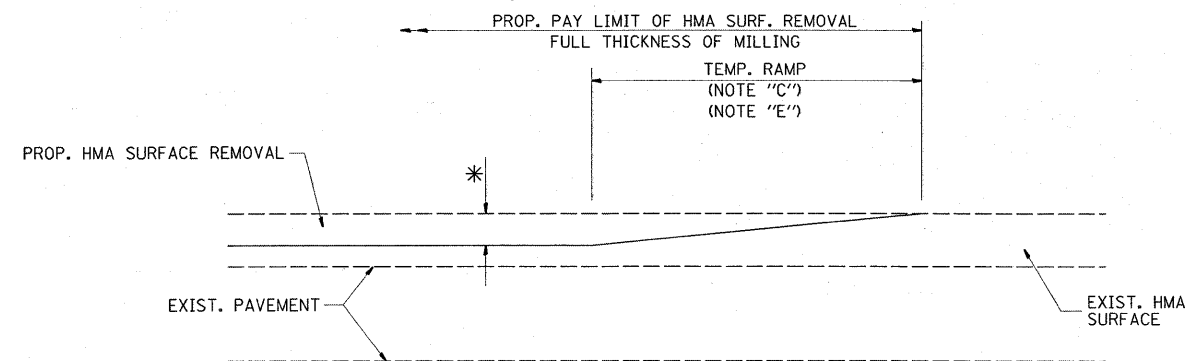
⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

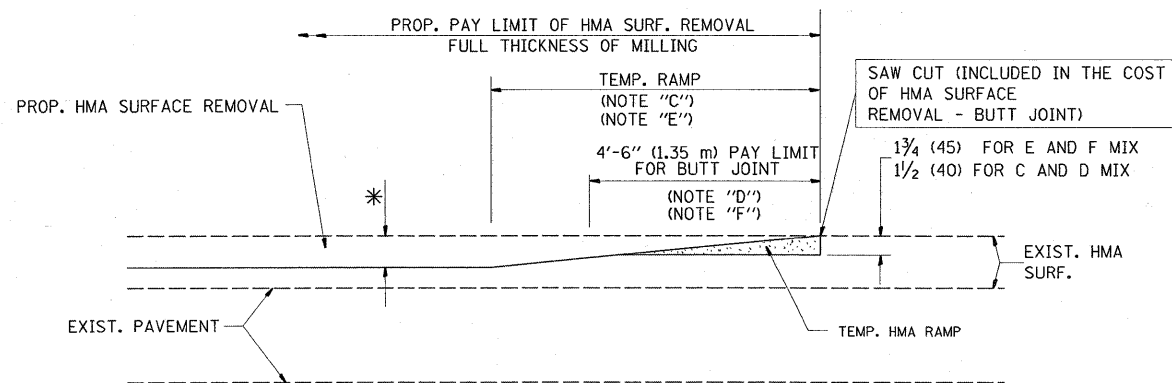
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pwork\pwork\GUILLEUMEP\dms88737	USER NAME = guilleumep sh_rdwg.dgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE. 330	SECTION 464Y-RS-2	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 16
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD600-06 (BD-24) CONTRACT NO. 60F07			
	PLOT DATE = 2/4/2009	DATE - 03-11-94	REVISED - M. GOMEZ 01-22-01		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
			REVISED - R. BORO 01-01-07									



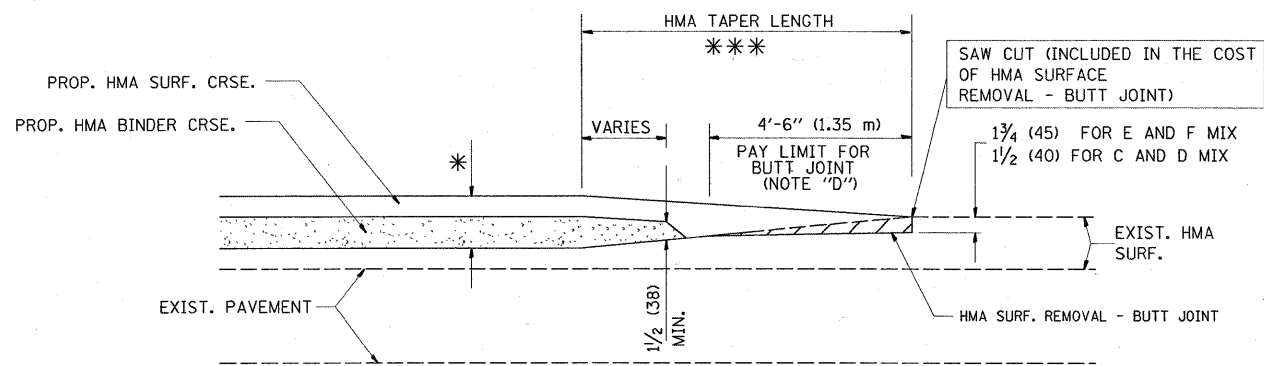
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1



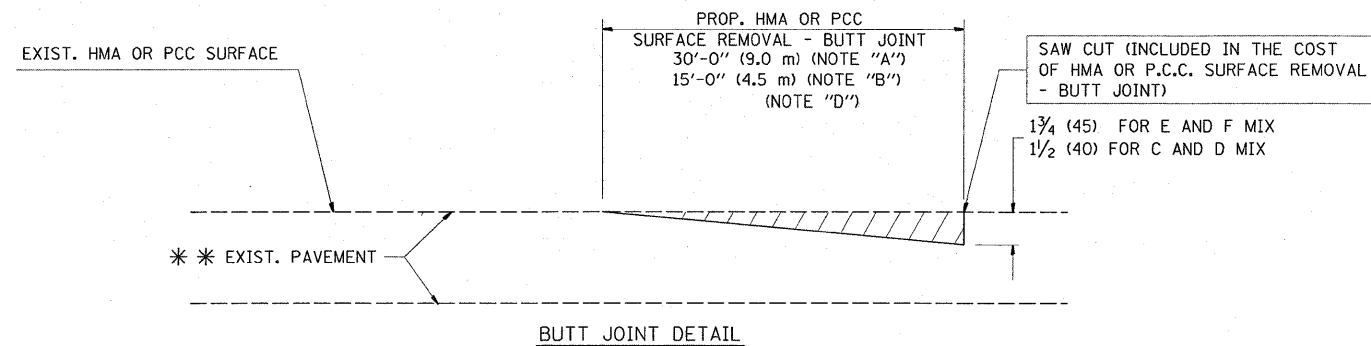
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2
TYPICAL TEMPORARY RAMP

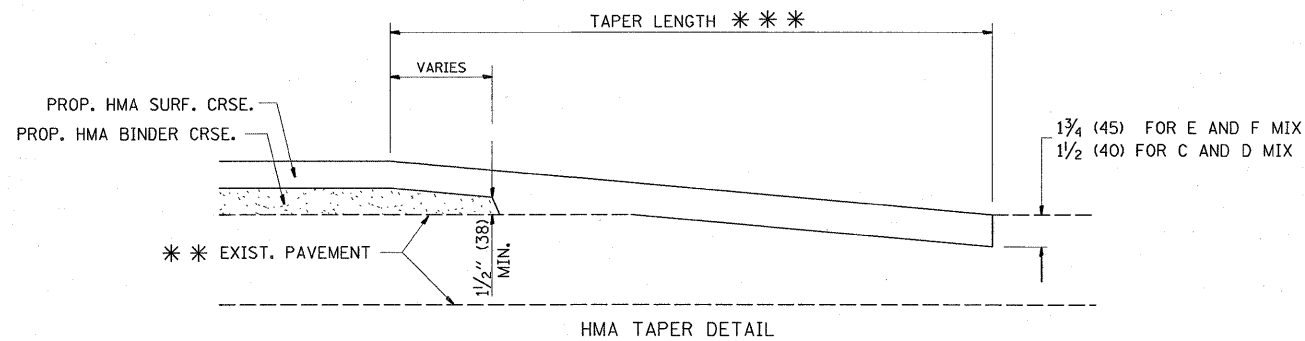


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

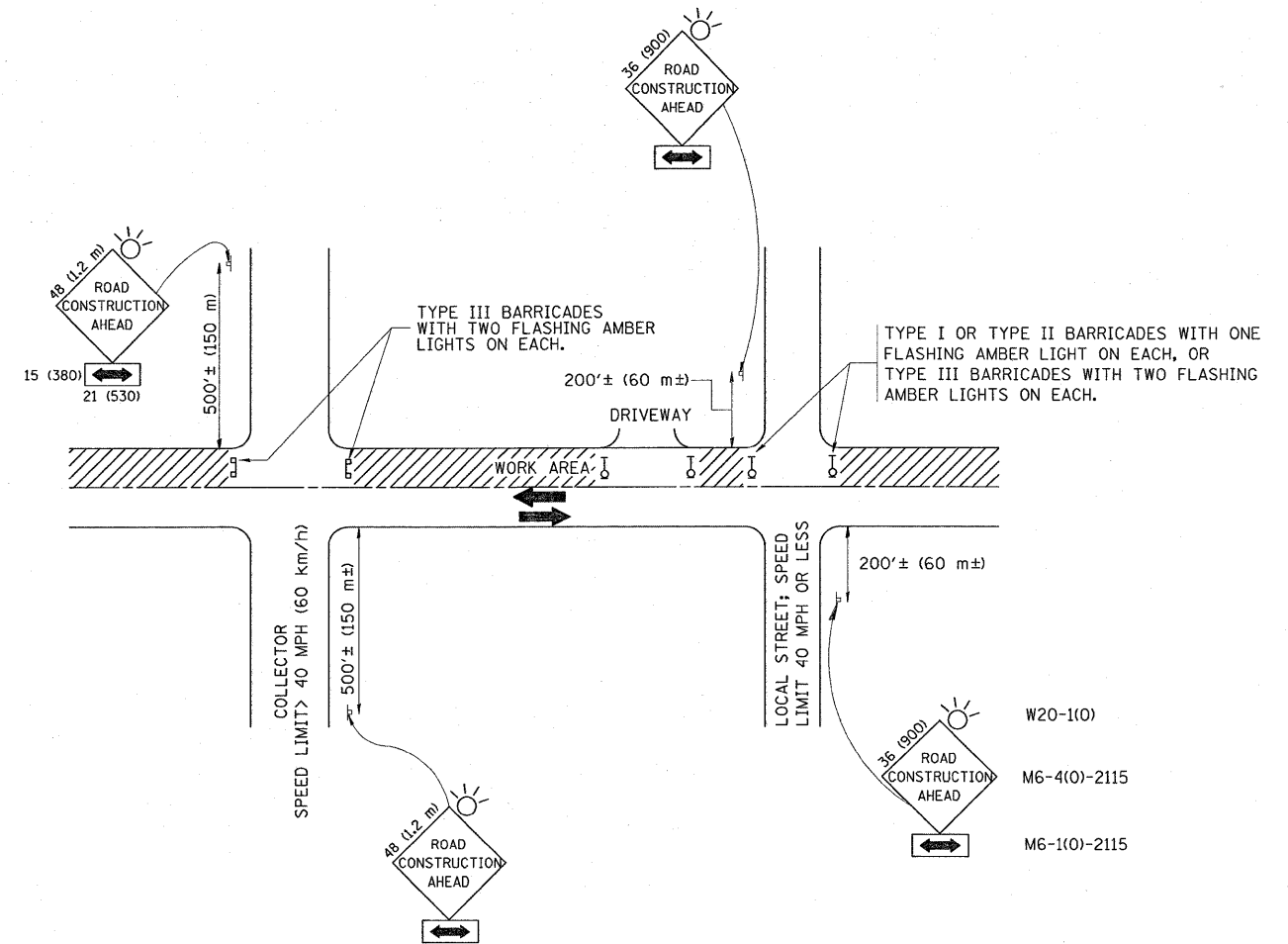
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	PLOT DATE = 2/4/2009	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
330	464Y-RS-2	COOK	24	17
BD400-05 BD32			CONTRACT NO. 60F07	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

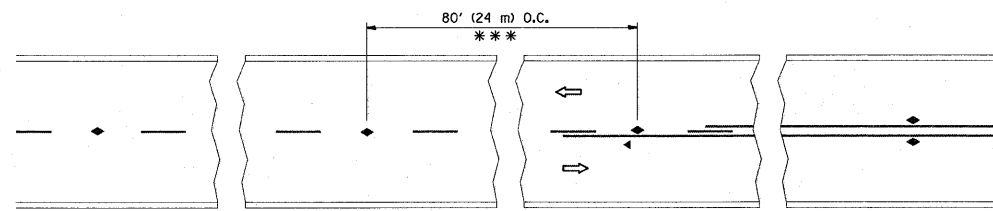
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	PLOT DATE = 2/4/2009	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

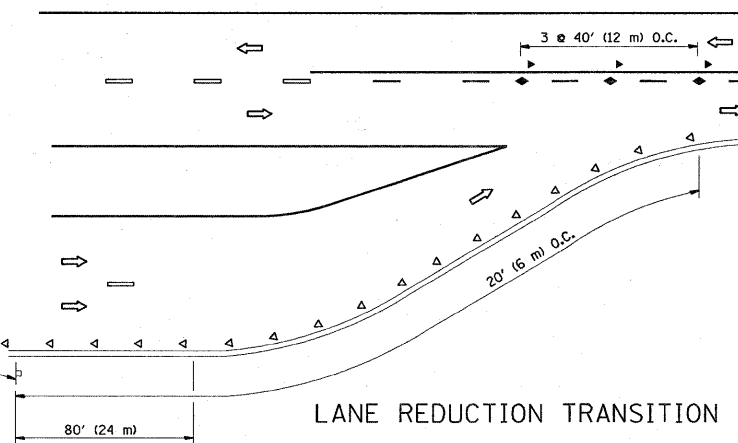
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TC-10		CONTRACT NO. 60F07		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

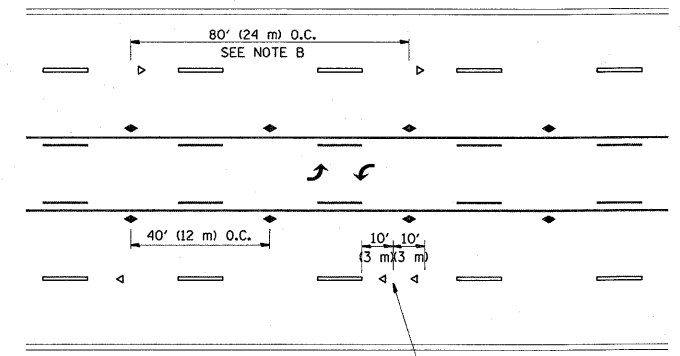


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

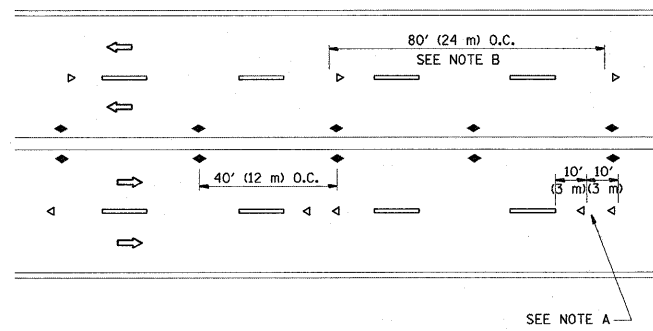
TWO-LANE/TWO-WAY



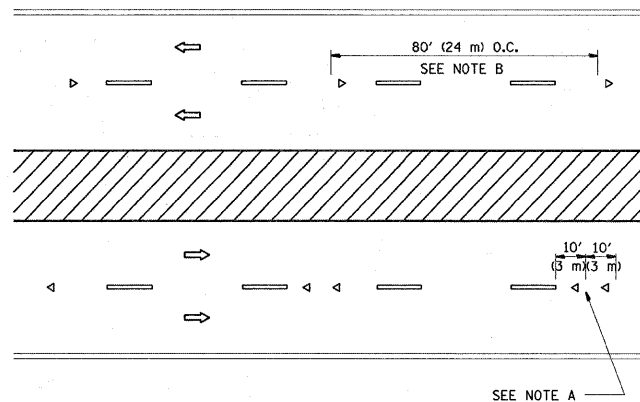
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

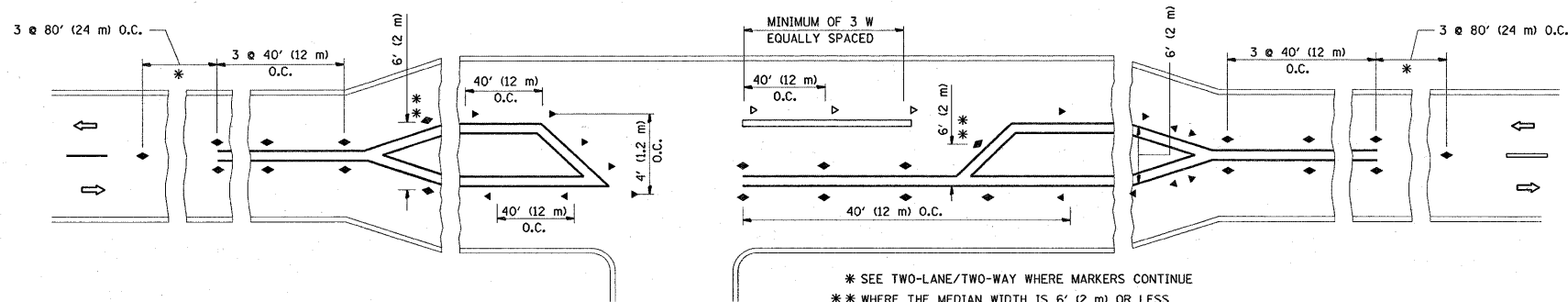
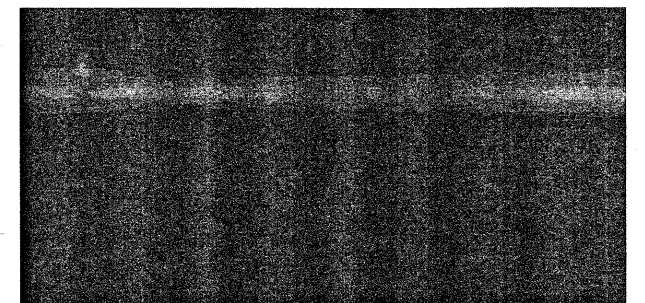
1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◄ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

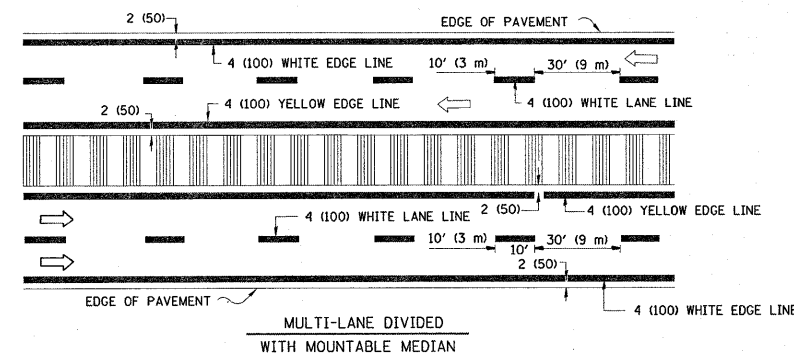
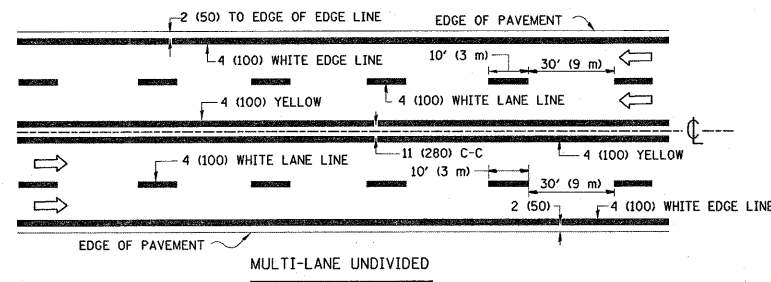
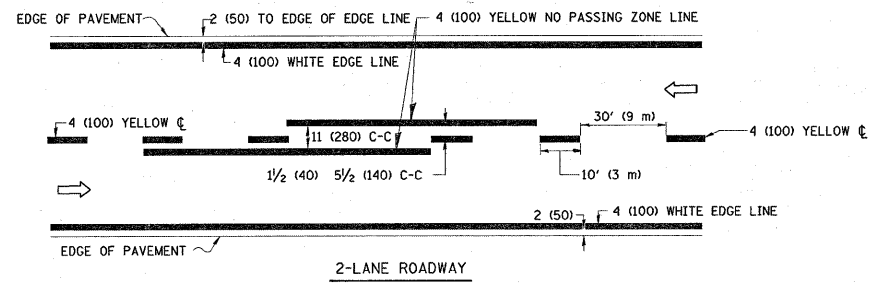


LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

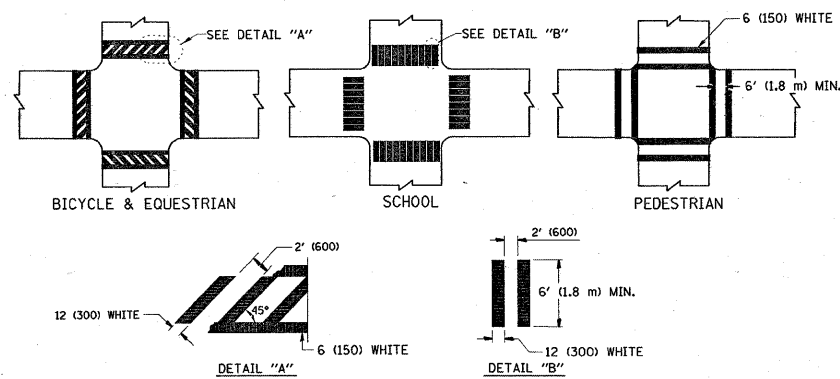
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 50,0000'' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	TC-11				
	PLOT DATE = 2/4/2009	DATE -	REVISED -					CONTRACT NO. 60F07				

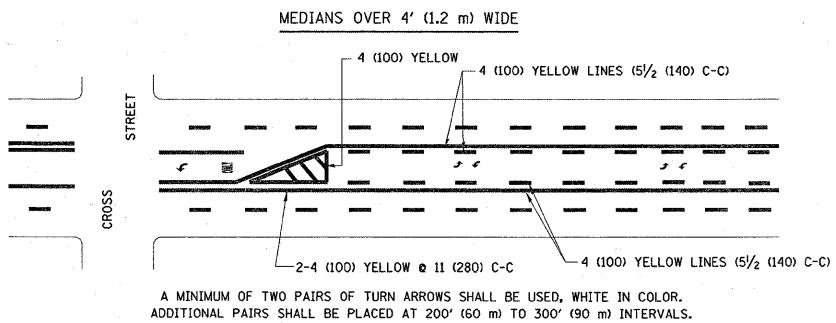
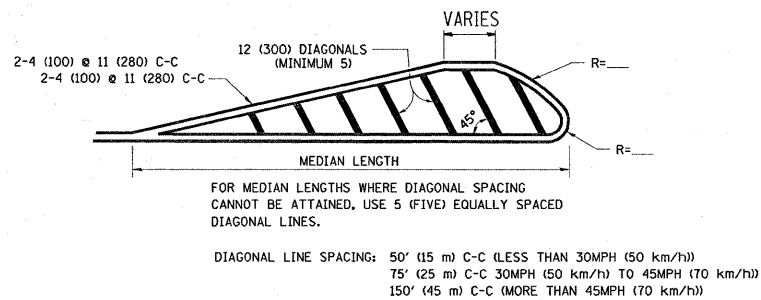
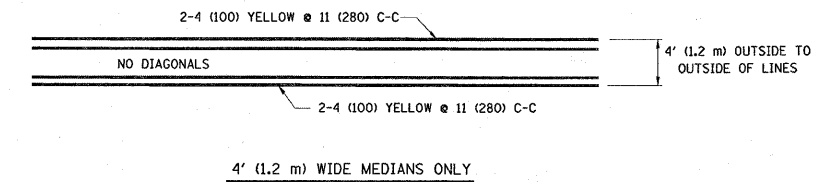


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

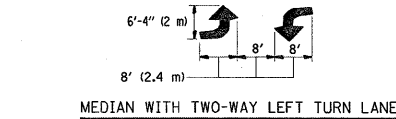
TYPICAL LANE AND EDGE LINE MARKING



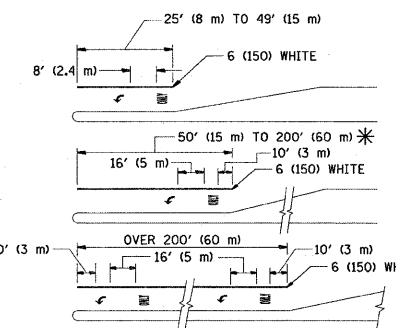
TYPICAL CROSSWALK MARKING



TYPICAL PAINTED MEDIAN MARKING



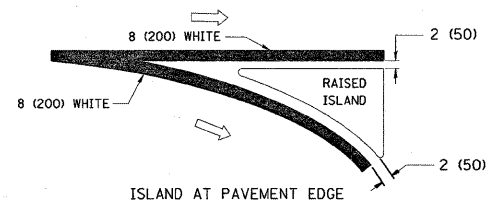
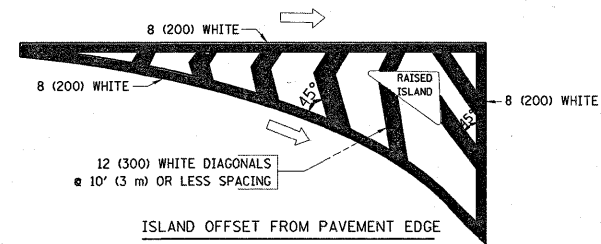
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



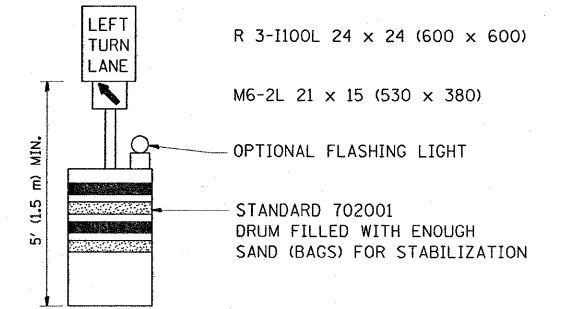
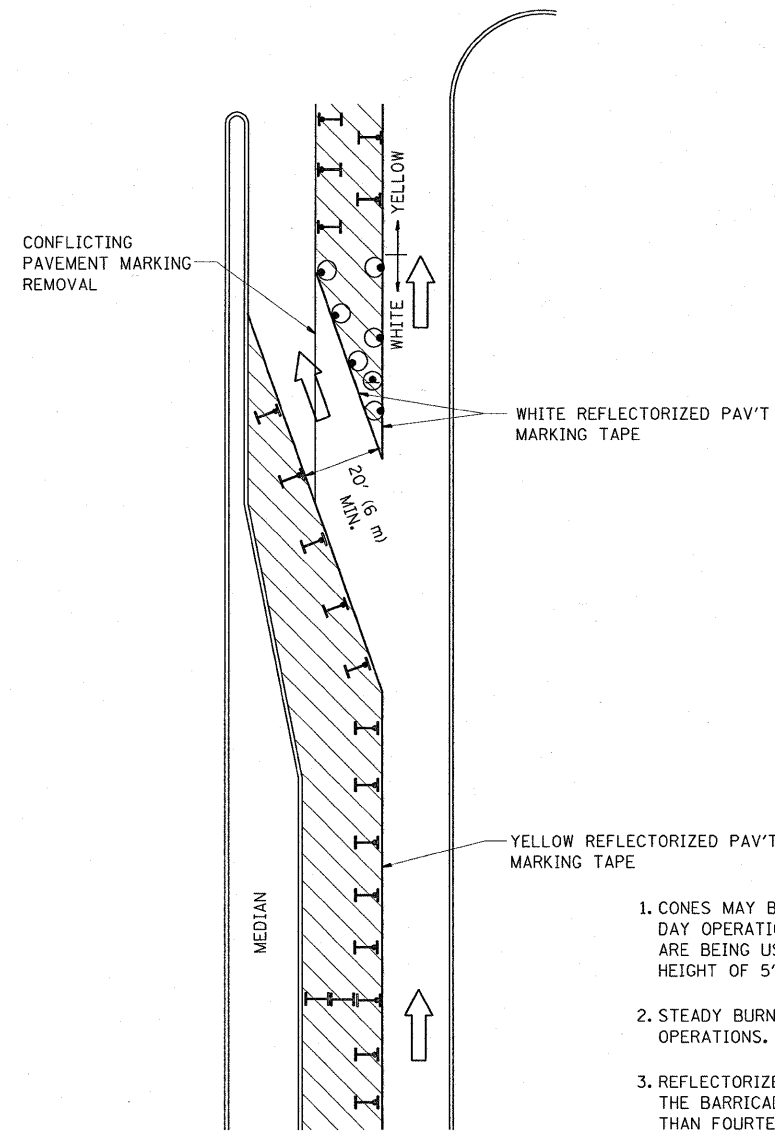
TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

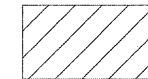





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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-17-96		TC-13				CONTRACT NO. 60F07			
	PLOT DATE = 2/4/2009	DATE - 03-19-90	REVISED - T. RAMMACHER 01-06-00		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

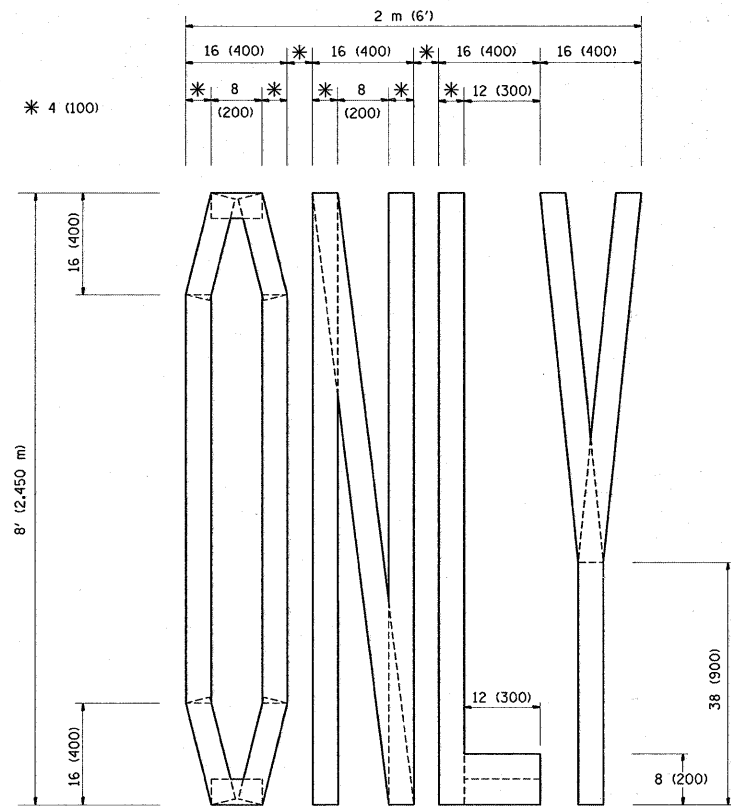
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

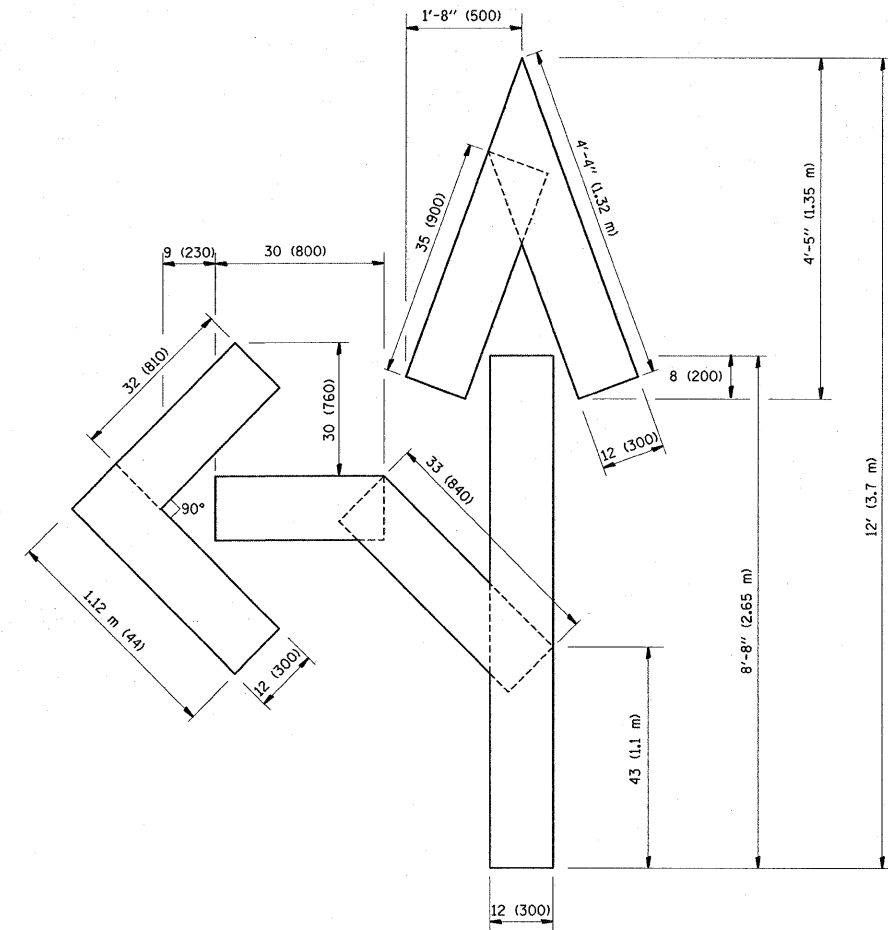
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

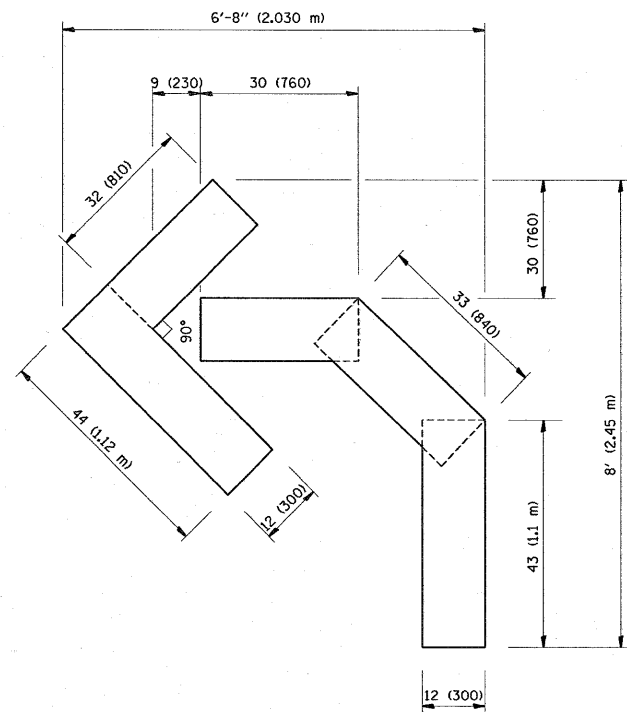
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330	464Y-RS-2	COOK	24	21
TC-14			CONTRACT NO. 60F07	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in Inches (millimeters) unless otherwise shown.

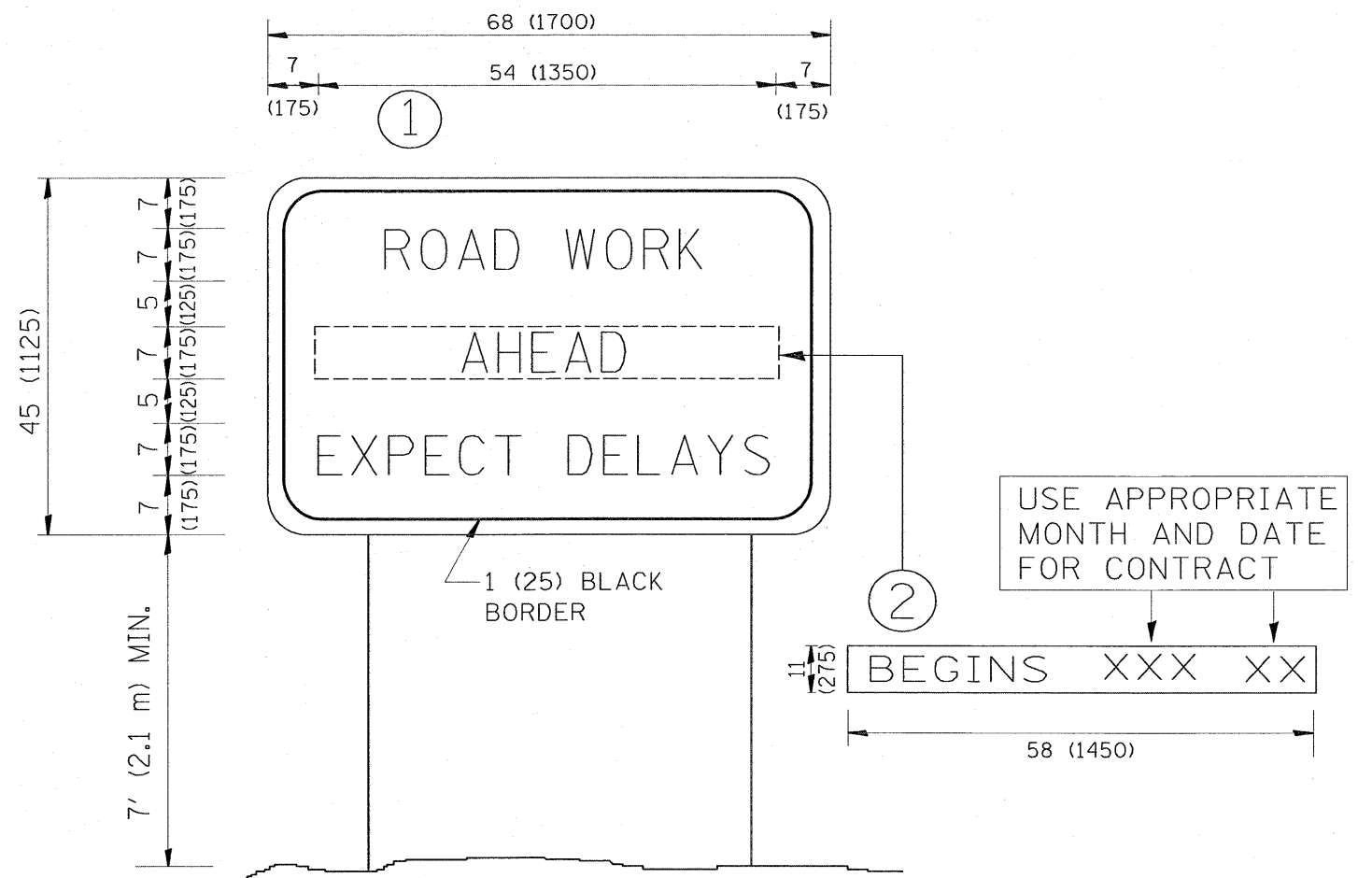
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PLOT SCALE = 50.0000 / IN.	CHECKED -	REVISED - T. RAMMACHER 03-02-98	
PLOT DATE = 2/4/2009	DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-16			CONTRACT NO. 60F07	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

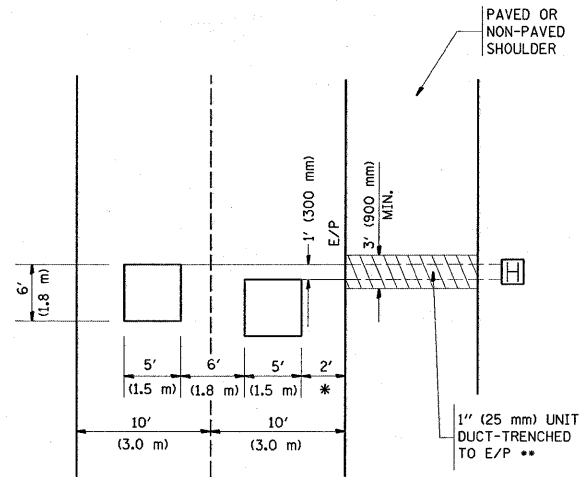
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulllaumeffp	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ct:\pw_work\PWIDOT\GULLLAUMEFF\dms88737	sh_rdrp.dgn	DRAWN -	REVISED - R. MIRS 12-11-97			330	464Y-RS-2	COOK	24	23	
	PLOT SCALE = 58.0000" / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99			TC-22		CONTRACT NO. 60F07			
	PLOT DATE = 2/4/2009	DATE -	REVISED - C. JUCIUS 01-31-07			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

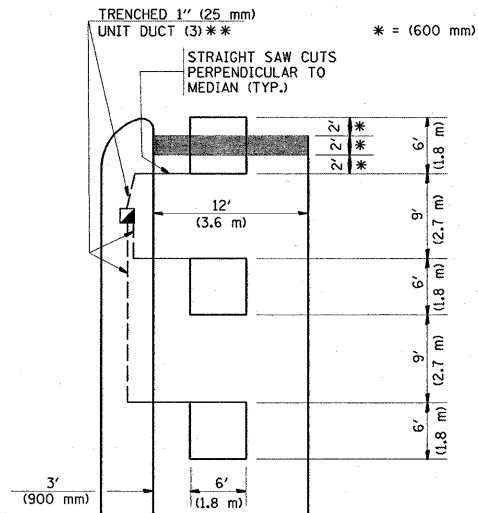


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

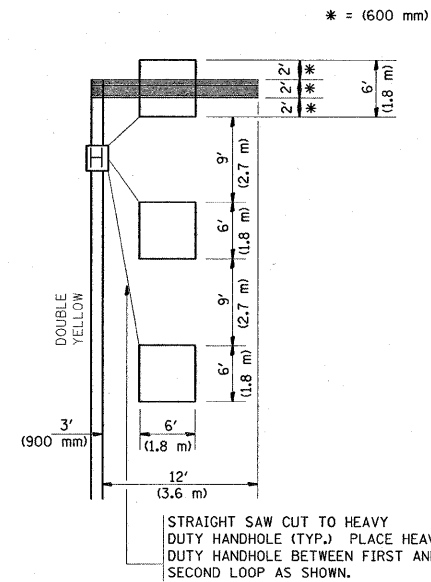
HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

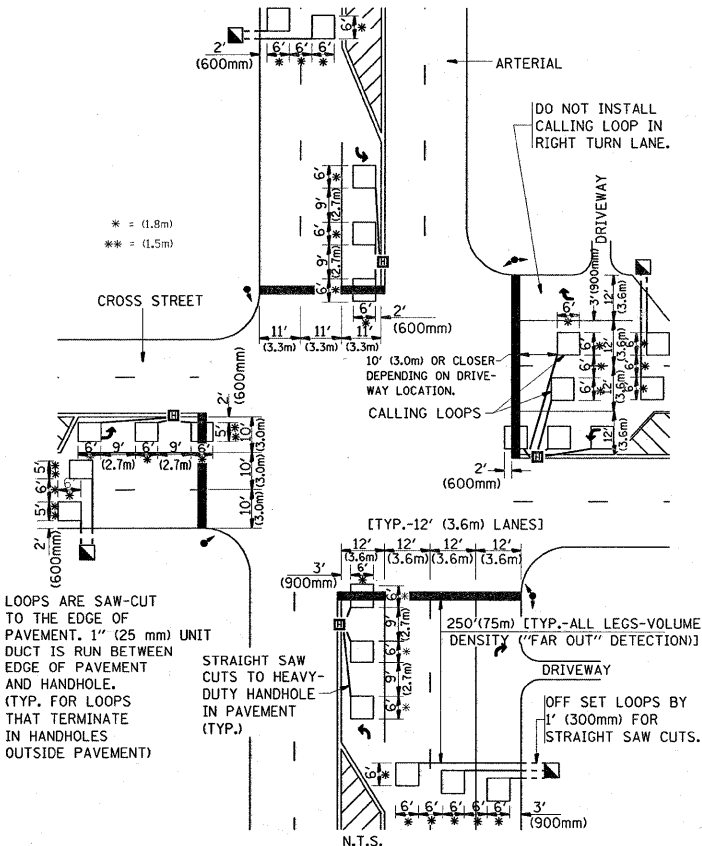
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



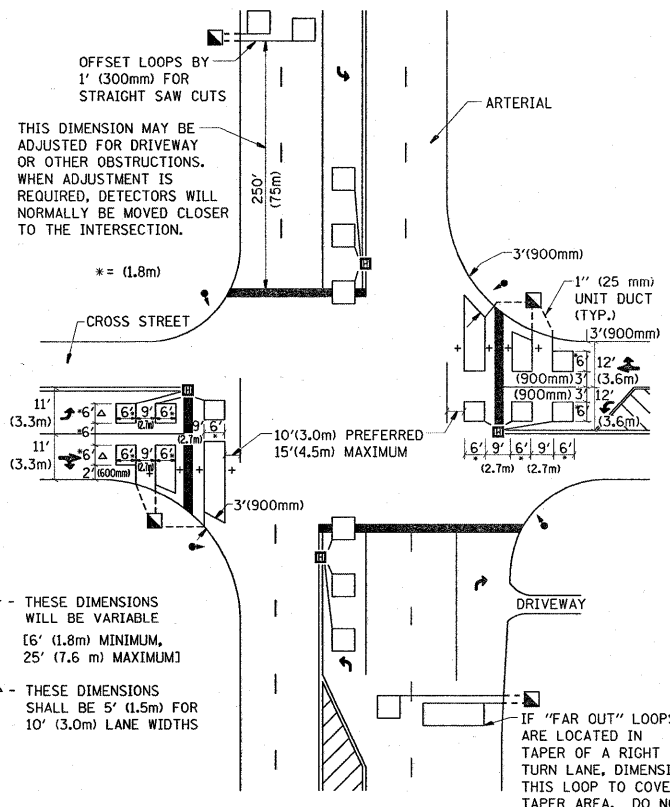
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = c:\pw_work\PW\IDOT\GUILLAUMEFP\dms88737	USER NAME = guillaumefp sh_rdwj.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.P. RTE. 330	SECTION 464Y-RS-2	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 24	
PLOT SCALE = 50.0000' / IN.		CHECKED - R.K.F.	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TS-07 CONTRACT NO. 60FO7	
PLOT DATE = 2/4/2009		DATE -	REVISED -					FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT			