

COMMITMENTS

THERE ARE NO COMMITMENTS FOR THIS PROJECT.

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-08	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
542401-02	METAL END SECTION FOR PIPE CULVERTS
602011-02	CATCH BASIN TYPE C
602301-04	INLET - TYPE A
602401-03	MANHOLE TYPE A
602601-04	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-04	FRAME AND LIDS TYPE 1
604036-03	GRATE TYPE 8
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-05	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS

INDEX OF SHEETS

SHEET NO.	TITLE
1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, AND COMMITMENTS
3	GENERAL NOTES
4 - 5	SUMMARY OF QUANTITIES
6	TYPICAL SECTIONS AND HOT-MIX ASPHALT MIXTURE REQUIREMENTS
7	ALIGNMENT & TIES, BENCHMARKS
8 - 9	GEOMETRIC PLAN: SPRINGDALE AVENUE
10	MISCELLANEOUS DETAILS
11	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
12	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)

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DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS, AND COMMITMENTS	
SCALE: NONE	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	12-00181-00-SW	COOK	12	2
SRTS-4009(350)		CONTRACT NO. 61C65		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

GENERAL NOTES

- ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
- THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE ENGINEER DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES, INCLUDING SPRINKLER SYSTEMS, EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, FIBER OPTICS, GAS, WATER, SEWER AND CABLE TELEVISION FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR MAY OBTAIN MUNICIPAL WATER IN BULK, AT NO CHARGE, AS LONG AS THERE IS NOT A "WATERING BAN" IN EFFECT. THE INDISCRIMINATE USE OF FIRE HYDRANTS IS STRICTLY PROHIBITED. WATER FOR CONSTRUCTION SHALL BE METERED OR OTHERWISE ACCOUNTED FOR AND A DAILY LOG MAINTAINED. THE CONTRACTOR SHALL PROVIDE THE WATER TRUCK AND DRIVER REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE VILLAGE RESERVES THE RIGHT TO RESTRICT OR REFUSE THE USE OF VILLAGE WATER IF DEEMED NECESSARY.
- ACCESS TO DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING CRUSHED STONE OR CRUSHED GRAVEL AS TEMPORARY ACCESS.
- EXISTING PAVEMENT, DRIVEWAY PAVEMENT, CURB AND GUTTER AND SIDEWALK TO REMAIN IN PLACE SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT VERTICAL FACE BETWEEN THE PROPOSED AND EXISTING AND SHALL BE INCLUDED IN THE PRICE OF THE APPROPRIATE REMOVAL PAY ITEM.
- CURB AND GUTTER SHALL BE DEPRESSED AT DRIVEWAYS AND SIDEWALK RAMPS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS. SIDEWALK RAMPS FOR ACCESS FOR THE DISABLED SHALL BE PROVIDED AT THE PROPOSED CROSSWALKS IN ACCORDANCE WITH THE IDOT HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER. SIDE CURBS SHOWN IN IDOT HIGHWAY STANDARDS SHALL NOT BE INSTALLED ON THIS PROJECT.
- A 1/2-INCH THICK EXPANSION JOINT SHALL BE PROVIDED AT THE JUNCTION OF THE DRIVEWAY APRON AND CURB, AND AT THE JUNCTION OF THE DRIVEWAY APRON AND THE SIDEWALK. THIS WORK WILL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT.
- PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE THICKENED TO 6-INCHES AT LOCATIONS WHERE THE SIDEWALK CROSSES RESIDENTIAL DRIVEWAYS. TRANSVERSE EXPANSION JOINTS 1/2" SHALL BE PLACED EVERY 50 FEET OR AS DETERMINED BY THE ENGINEER.
- DETECTABLE WARNINGS SHALL BE CONSTRUCTED WITH THE INSTALLATION OF A REPLACEABLE CAST-IN-PLACE 24"x48" NOMINAL PANEL WIDTH. THE PANEL SHALL COMPLY WITH ADA REQUIREMENTS. THE PANEL COLOR SHALL BE BRICK RED UNLESS DETERMINED OTHERWISE BY THE VILLAGE. INSTALLATION SHALL OCCUR IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATIONS AND THE APPLICABLE HIGHWAY STANDARD.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY AND PRIOR TO ACCEPTANCE OF THE IMPROVEMENT. ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL BE INCLUDED IN THE COST OF MOBILIZATION.
- IT SHALL BE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD AT THE JOB SITE PRIOR TO BIDDING ON THE PROJECT AND PRIOR TO ORDERING MATERIALS AND COMMENCEMENT OF CONSTRUCTION. IF THERE ARE ANY DISCREPANCIES FROM WHAT IS SHOWN ON THE CONSTRUCTION PLANS, THE CONTRACTOR MUST IMMEDIATELY REPORT SAME TO THE ENGINEER BEFORE DOING ANY WORK, OTHERWISE THE CONTRACTOR ASSUMES FULL RESPONSIBILITY. IN THE EVENT OF DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, SPECIALS PROVISIONS, STANDARD SPECIFICATIONS AND/OR DETAILS, THE CONTRACTOR SHALL SECURE WRITTEN INSTRUCTIONS FROM THE ENGINEER PRIOR TO PROCEEDING WITH ANY PART OF WORK AFFECTED BY OMISSION OR DISCREPANCIES. FAILING TO SECURE SUCH INSTRUCTIONS, THE CONTRACTOR WILL BE CONSIDERED TO HAVE PROCEEDED AT HIS OWN RISK AND EXPENSE. IN THE EVENT OF ANY DOUBT OR QUESTION ARISING WITH RESPECT TO THE MEANING OF THE CONSTRUCTION PLANS AND/OR SPECIFICATIONS, THE DECISION OF THE ENGINEER SHALL BE FINAL AND CONCLUSIVE.
- THE CURB SHALL BE TAPERED TO THE GUTTER IN A FIVE (5) FOOT LENGTH WHEREVER THE CURB AND GUTTER TERMINATES, WITH AN EXPANSION JOINT PLACED AT THE START OF THE TAPER.
- ALL POSTS, RAILROAD TIES, AND DECORATIVE TIMBER IN CONFLICT WITH THE PROPOSED IMPROVEMENTS SHALL BE REMOVED AND RELOCATED AS DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF EARTH EXCAVATION. EVERY EFFORT SHALL BE MADE BY THE CONTRACTOR WHEN REMOVING THESE ITEMS TO PRESERVE THEM FROM HARM. ITEMS NOT RELOCATED SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL ABOVE AND BELOW GROUND UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR UTILITY OWNER. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTIFY AND COORDINATE WITH IMPACTED UTILITY OWNERS AND HIS CONSTRUCTION SCHEDULE.
- ALL REQUIRED CONSTRUCTION STAKING (BENCHMARK ESTABLISHMENT, ROW, SIDEWALK, UTILITY LOCATION AND ELEVATIONS) SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY FOR DIMENSIONS AND ELEVATIONS MEASURED FROM CONTRACTOR'S STAKES. THE CONTRACTOR SHALL EXERCISE PROPER CARE IN THE PRESERVATION OF STAKES SET UNTIL THE ENGINEER'S APPROVAL FOR STAKE REMOVAL IS OBTAINED.
- INITIAL HORIZONTAL LAYOUT MAY BE REQUESTED BY THE ENGINEER TO VERIFY FIELD LOCATIONS. HORIZONTAL LAYOUT MAY BE ADJUSTED BY THE ENGINEER DURING CONSTRUCTION AND SHALL BE INCLUDED IN THE COST OF CONSTRUCTION LAYOUT.
- ALL TREE PROTECTION (TEMPORARY FENCE INSTALLATION), TREE REMOVAL, TREE PRUNING AND ROOT PRUNING SHALL BE COMPLETED BEFORE WORK OPERATIONS COMMENCE IN ANY AREA. AT NO TIME SHALL THE CONTRACTOR PRUNE OR REMOVE ANY TREES UNLESS SPECIFICALLY DETERMINED BY THE ENGINEER.
- TREE REMOVAL NOTICES WILL BE POSTED BY THE VILLAGE ON THE TREES TO BE REMOVED. TREE REMOVAL WORK WILL BE ALLOWED 10 DAYS AFTER POSTING OF THE TREE REMOVAL NOTICE.

EROSION CONTROL NOTES

- ALL AREAS LOCATED DOWNSTREAM FROM DISTURBED AREAS OF CONSTRUCTION SHALL BE PROTECTED FROM POTENTIAL INCREASE OF EROSION AND SEDIMENTATION RESULTING FROM UPSTREAM ACTIVITIES.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF CONSTRUCTION SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SODDING IS PERFORMED.
- SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED AND FUNCTIONAL PRIOR TO THE START OF DISTURBANCE.
- STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICAL IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED; BUT IN NO CASE SHALL THESE MEASURES BE INSTALLED MORE THAN 7 DAYS AFTER THE CONSTRUCTION IN THIS AREA TEMPORARILY OR PERMANENTLY CEASES.
- ALL STORM SEWER STRUCTURES THAT RECEIVE RUNOFF DURING CONSTRUCTION SHALL INCLUDE INLET PROTECTION FILTERS TO PREVENT DEBRIS AND EXCESSIVE SEDIMENT FROM ENTERING THE STORM SEWER SYSTEM. THESE PROTECTIVE MEASURES SHALL BE PROPERLY INSTALLED, MAINTAINED, AND REMOVED IN THEIR ENTIRETY AFTER THE AREA TRIBUTARY TO THE STORM STRUCTURE IS STABILIZED.
- ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE PROPERLY STABILIZED OR DISPOSED.
- REPAIR, REPLACE OR MAINTAIN EROSION AND SEDIMENT CONTROL STRUCTURES AFTER A RAINFALL EVENT OF 1/2 INCH OR MORE OVER A 24-HOUR PERIOD AND ON A BI-WEEKLY BASIS AS A MINIMUM.
- MAKE ADJUSTMENTS TO THE SEDIMENTATION AND EROSION CONTROL PLAN AND METHODS, AS NEEDED, TO ACCOMPLISH THE INTENDED PURPOSE.
- ALL ADJACENT ROADWAYS MUST BE KEPT CLEAR OF DEBRIS, INSPECTED DAILY, AND CLEANED WHEN NECESSARY OR AS DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL AND/OR DEBRIS UPON OR INTO ANY CHANNEL, DITCH, SWALE OR ANY OTHER TEMPORARY OR PERMANENT LOCATIONS MEANT TO CONVEY SITE DRAINAGE.
- IF THE CONTRACTOR IS NOTIFIED BY THE ENGINEER OF AN EROSION AND SEDIMENT CONTROL DEFICIENCY, THE DEFICIENCY MUST BE CORRECTED WITHIN 24-HOURS OF BEING NOTIFIED.

MAINTENANCE OF TRAFFIC NOTES

- EMERGENCY VEHICLE ACCESS SHALL BE MAINTAINED AT ALL TIMES. ANY SIGNIFICANT CHANGE IN TRAFFIC CONTROL SHALL HAVE PRIOR APPROVAL BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF THREE (3) DAYS NOTICE IN ADVANCE OF CONSTRUCTION ACTIVITIES WHICH IMPACT EMERGENCY SERVICES.
- ALL LANES SHALL BE OPEN TO TRAFFIC AT THE END OF EACH DAY, MAINTAINING A MINIMUM 11' WIDTH PER LANE.
- DAILY LANE CLOSURES SHALL BE IN ACCORDANCE WITH THE APPLICABLE HIGHWAY STANDARDS.
- THE CONTRACTOR SHALL PROVIDE TEMPORARY ACCESS TO DRIVEWAYS AND SIDE ROADS DURING CONSTRUCTION, UTILIZING PAY ITEMS TEMPORARY ACCESS (PRIVATE ENTRANCE).
- ALL DRUMS AND VERTICAL PANELS SHALL BE EQUIPPED WITH STEADY BURN BI-DIRECTIONAL LIGHTS. SPACING SHALL BE AT 50 FOOT INTERVALS ALONG WORK AREAS AND 25 FEET WHERE DIRECTION CHANGES AND TAPERS OCCUR, OR AS DIRECTED BY THE ENGINEER.
- CONSTRUCTION VEHICLES AND/OR EQUIPMENT SHALL NOT BE PARKED OVERNIGHT WITHIN VILLAGE RIGHT-OF-WAY. THE ENGINEER WILL ASSIST THE CONTRACTOR IN IDENTIFYING CONSTRUCTION STAGING LOCATIONS AT THE START OF CONSTRUCTION.

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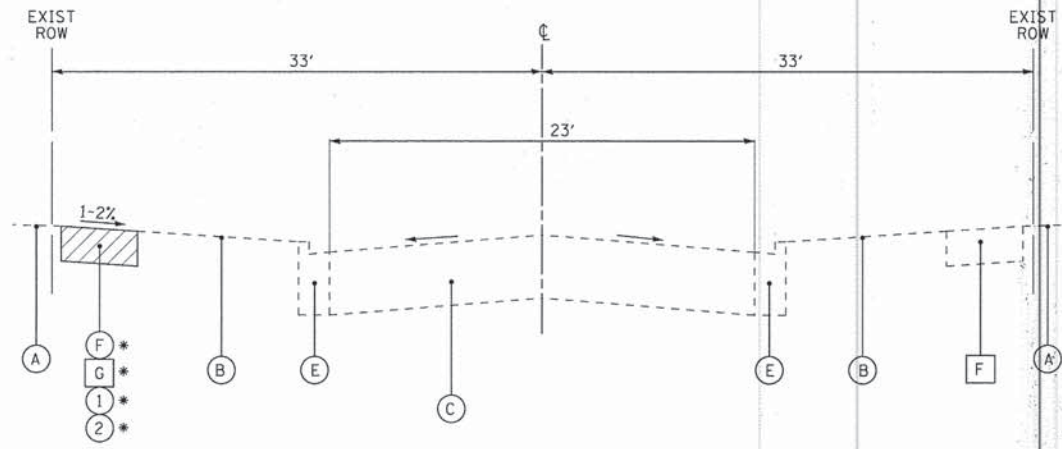
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GENERAL NOTES

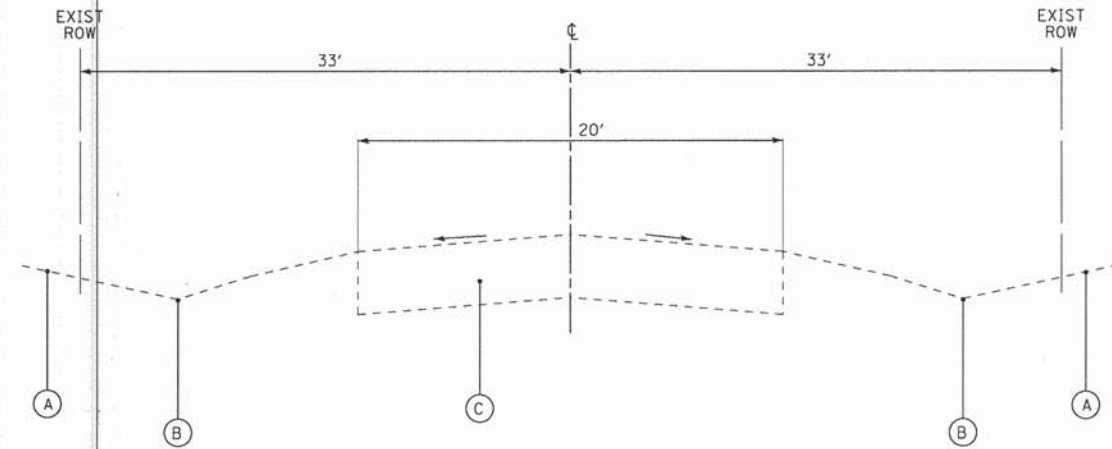
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	12-00181-00-SW	COOK	12	3
	SRTS-4009(350)	CONTRACT NO. 61C65		
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			



* AS DETERMINED BY THE ENGINEER

**EXISTING & PROPOSED TYPICAL SECTION
SPRINGDALE AVENUE
STA 48+20 TO STA 50+00**



**EXISTING TYPICAL SECTION
SPRINGDALE AVENUE
STA 50+00 TO STA 57+39**

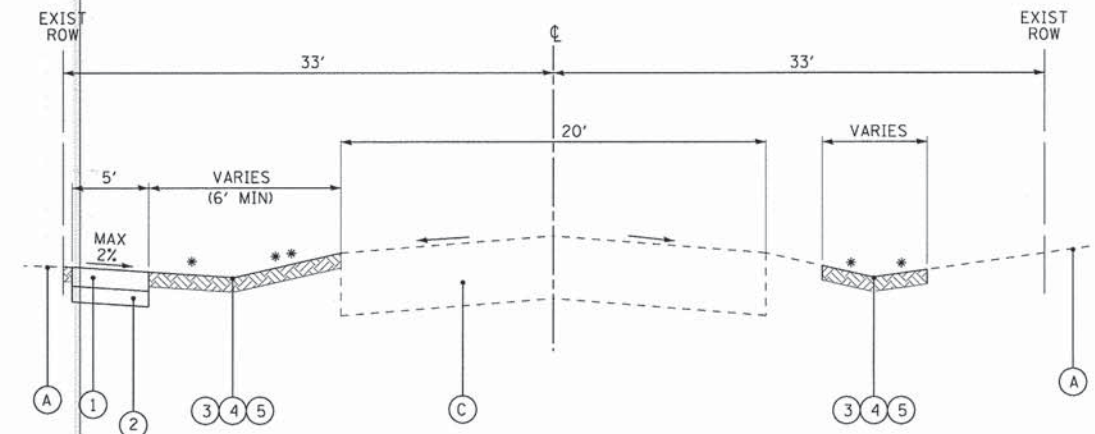
EARTHWORK				
		21101615	*	20201200
		TOPSOIL FURNISH AND PLACE, 4"	EMBANKMENT	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
STA	TO STA	SQ YD	CU YD	CU YD
50+30	51+13	154	10	24
51+34	52+38	216	12	30
52+76	53+89	458	13	46
54+09	54+90	460	9	40
55+09	55+80	287	8	30
56+00	57+03	325	12	38
TOTAL		1900	64	208

* INCLUDED IN THE COST OF REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
DRIVEWAY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MX "D", N50 (IL 9.5 mm) - 3"	4% @ 50 Gyr.

THE UNIT WEIGHT USED TO CALCULATE ALL HMA MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY THE DISTRICT ONE SPECIAL PROVISIONS.
FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.



**PROPOSED TYPICAL SECTION
SPRINGDALE AVENUE
STA 50+00 TO STA 57+39**

* 1:4 (MAX)
** 1:6 (MAX)

EXISTING LEGEND

- (A) EXISTING GROUND SURFACE
- (B) EXISTING SWALE
- (C) HOT-MIX ASPHALT SURFACE
- (D) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
- (E) ITEM TO BE REMOVED
- (F) EXISTING CURB & GUTTER
- (G) EXISTING SIDEWALK
- (H) SIDEWALK REMOVAL (SPOT REPAIR)

PROPOSED LEGEND

- (1) PCC SIDEWALK, 5" (THICKEN TO 6" THROUGH DRIVEWAYS)
- (2) AGGREGATE BASE COURSE, TYPE B, 4"
- (3) TOPSOIL, FURNISH AND PLACE 4"
- (4) SODDING
- (5) GRADING AND SHAPING DITCHES

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**TYPICAL SECTIONS AND
HOT-MIX ASPHALT MIXTURE REQUIREMENTS**

SCALE: NONE STA. 50+00 TO STA. 58+50

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	12-00181-00-SW	COOK	12	6
SRTS-4009(350)		CONTRACT NO. 61C65		
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

CURVE DATA

PROP. CURVE X-SPRING-1
 PI STA. = 54+70.63
 $\Delta = 7^{\circ} 21' 37''$ (LT)
 D = 4° 19' 27"
 R = 1,325.00'
 T = 85.22'
 L = 170.21'
 E = 2.74'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA = 53+85.40
 P.T. STA = 55+55.62

PROP. CURVE X-SPRING-2
 PI STA. = 56+72.10
 $\Delta = 5^{\circ} 39' 53''$ (LT)
 D = 11° 27' 33"
 R = 500.00'
 T = 24.74'
 L = 49.43'
 E = 0.61'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA = 56+47.36
 P.T. STA = 56+96.80

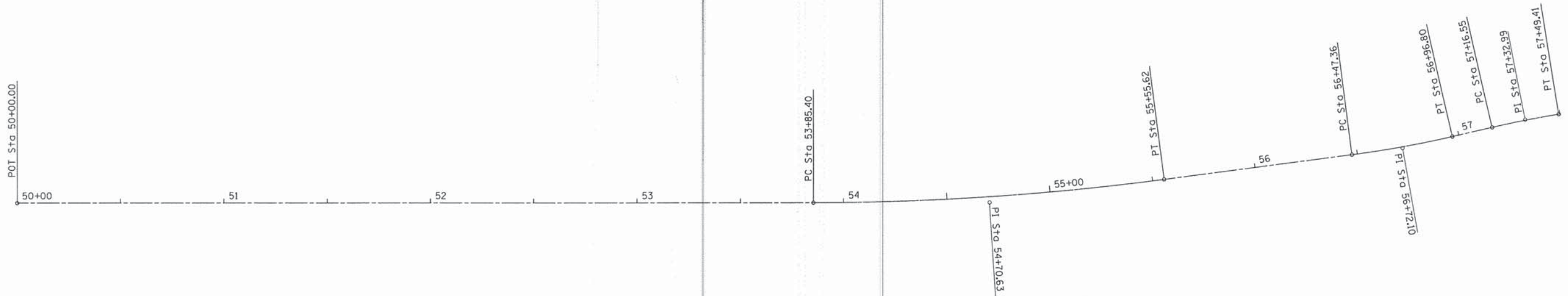
PROP. CURVE X-SPRING-3
 PI STA. = 57+32.99
 $\Delta = 3^{\circ} 45' 57''$ (RT)
 D = 11° 27' 33"
 R = 500.00'
 T = 16.44'
 L = 32.86'
 E = 0.27'
 e = -----
 T.R. = -----
 S.E. RUN = -----
 P.C. STA = 57+16.55
 P.T. STA = 57+49.41

CONTROL POINTS

CP 1, N 1971079.446749, E 1115038.610821, ELEV. 682.83, CP 3853 SPRINGDALE AVENUE,
 CP 2, N 1971079.405322, E 1115387.128381, ELEV. 680.74, CP 3828 SPRINGDALE AVENUE,
 CP 3, N 1970218.595936, E 1115222.310752, ELEV. 683.35, CP KNOLLCASTLE,

BENCHMARKS

- NOTE:
 CONTRACTOR SHALL ESTABLISH ADDITIONAL BENCHMARKS
 PRIOR TO REMOVAL OF BENCHMARKS PROVIDED BELOW.
- BM #1 - WEST ARROW BONNET BOLT ON FIRE HYDRANT
 @ 3853 SPRINGDALE LANE
 ELEV = 684.95
 - BM #2 - WEST BONNET BOLT ON FIRE HYDRANT
 @ 3823 SPRINGDALE LANE
 ELEV = 682.59
 - BM #3 - OPERATING NUT ON FIRE HYDRANT
 @ WSW CORNER OF SPRINGDALE AVE AND PFINGSTEN RD
 ELEV = 681.02
 - BM #4 - WEST ARROW BONNET BOLT ON FIRE HYDRANT
 @ 3733 SPRINGDALE LANE
 ELEV = 681.47



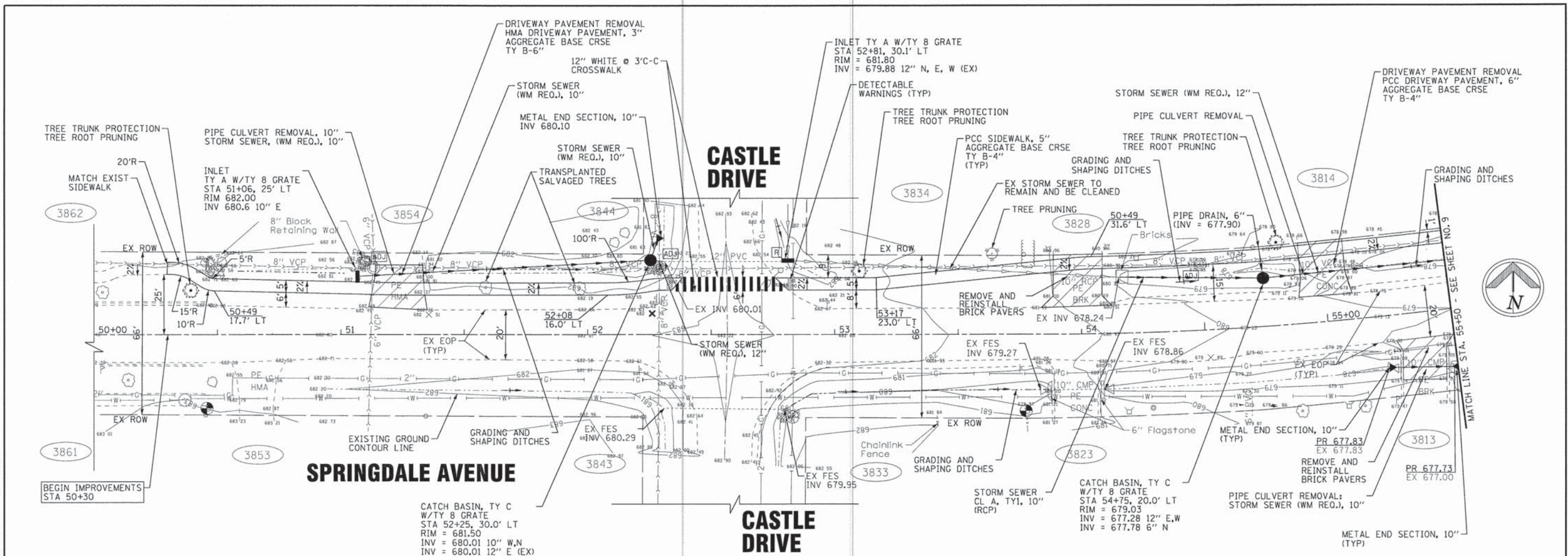
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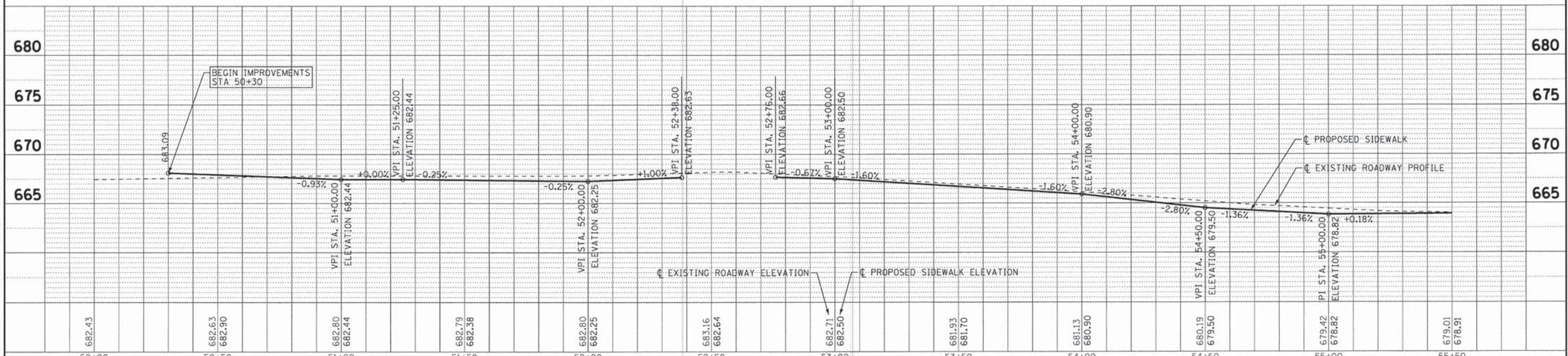
ALIGNMENT AND TIES SPRINGDALE AVENUE	
SCALE:	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	12-00181-00-SW	COOK	12	7
SRTS-4009(350)		CONTRACT NO. 61C65		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



NOTES

1. SIDEWALK R&R LOCATIONS ON SPRINGDALE AVENUE WITHIN THE PROJECT LIMITS TO BE VERIFIED AND DETERMINED BY THE ENGINEER PRIOR TO CONSTRUCTION



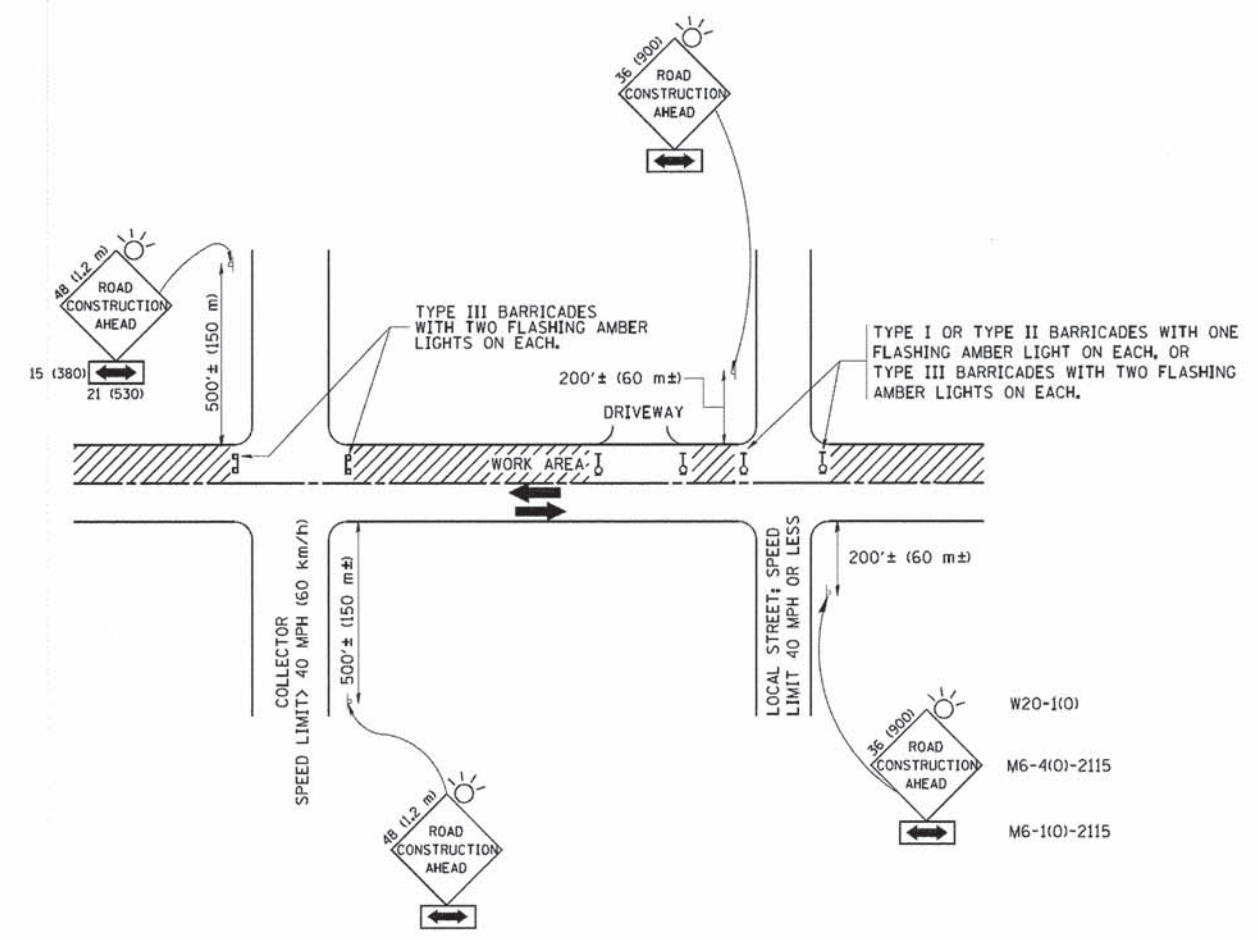
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DEPARTMENT OF TRANSPORTATION

PLAN AND PROFILE
SPRINGDALE AVENUE

SCALE: H: 1"=20' V: 1"=5'	STA. 50+30 TO STA. 55+50	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			12-00181-00-SW	COOK	12	8
			SRTS-4009(350)		CONTRACT NO. 61C65	
			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1,2 m x 1,2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

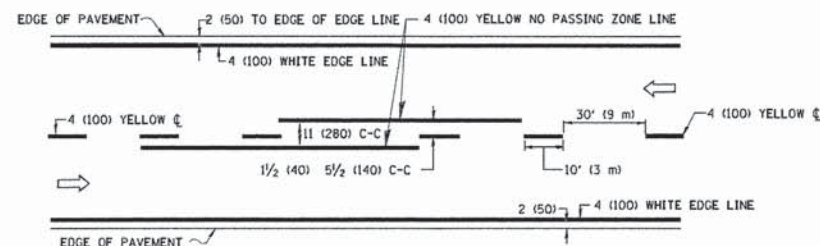
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME = M:\d\stata\22x34\c18.dgn	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

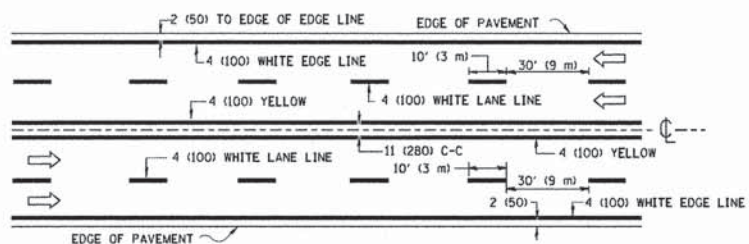
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

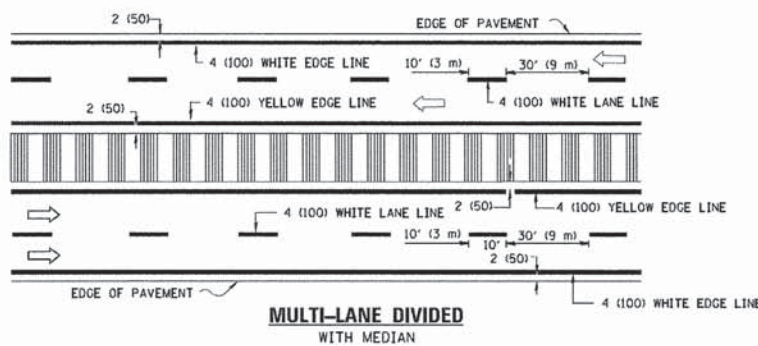
F.A.P. RTE.	SECTION 12-00181-00-SW	COUNTY COOK	TOTAL SHEETS 12	SHEET NO. 11
TC-10			CONTRACT NO. 61C65	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

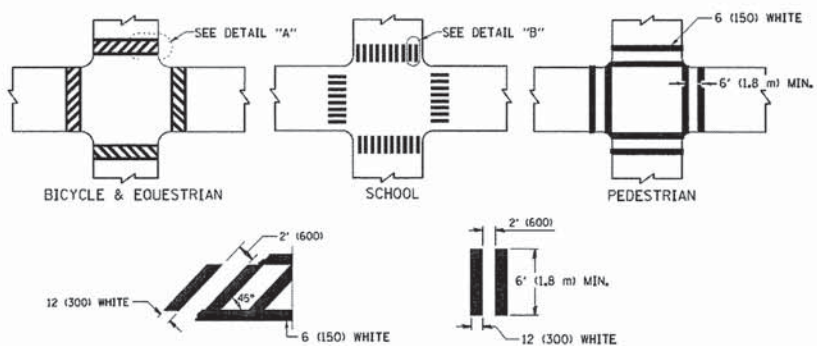


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

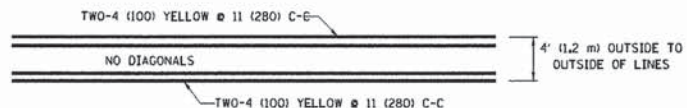


DETAIL "A"

DETAIL "B"

TYPICAL CROSSWALK MARKING

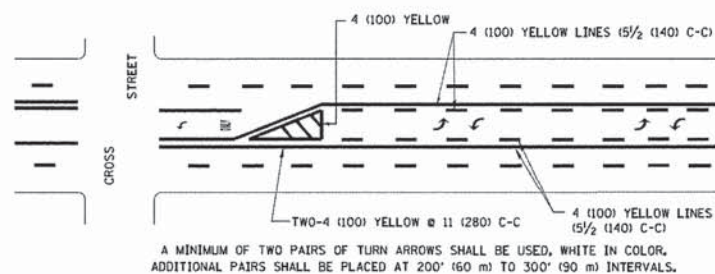
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



4' (1.2 m) WIDE MEDIANS ONLY

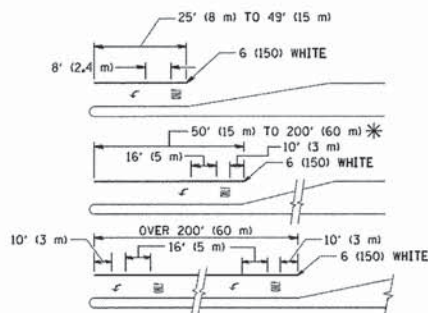


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

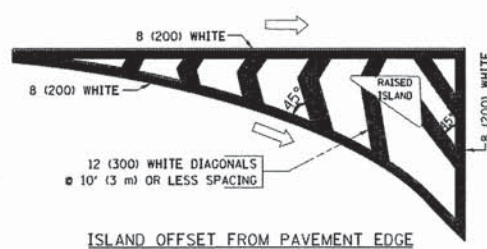
TYPICAL PAINTED MEDIAN MARKING



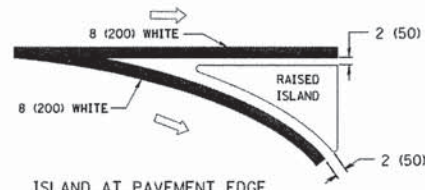
TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

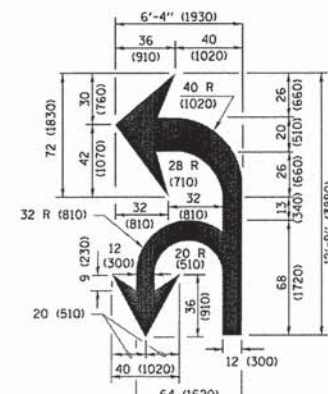


ISLAND OFFSET FROM PAVEMENT EDGE

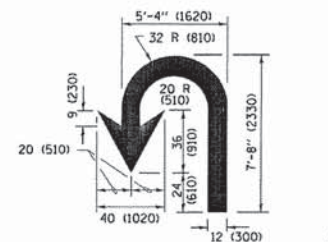


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = lszokrf	DESIGNED - EVERS	REVISED - T. RAMMACH 10-27-94
pw:\NL884EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\Dist		DRAWN: CAD\Deco\CAD\sheds\td13.dgn	REVISED - C. JUCIUS 09-09-09
Default	PLOT SCALE = 50,000' / in	CHECKED -	REVISED - C. JUCIUS 07-01-13
	PLOT DATE = 12/21/2015	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	12-00181-00-SW	COOK	12	12
TC-13		CONTRACT NO. 61C65		
[ILLINOIS] FED. AID PROJECT				