

72694

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172		ADAMS	108	1

• (1-5)RS-1; (1-6)RS  
★ 108+3=111

**INDEX OF SHEETS**

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90	LONGITUDINAL JOINT REPAIR DETAILS
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98-100B	BRIDGE REPAIR DETAILS, SN 001-0049, CH 3 (ELLINGTON ROAD) OVER FAI 72
101-108	BRIDGE REPAIR DETAILS, SN 001-0050 & 0051, FAI 72 OVER BNRR
108A	TEMPORARY BARRIER WALL DETAIL

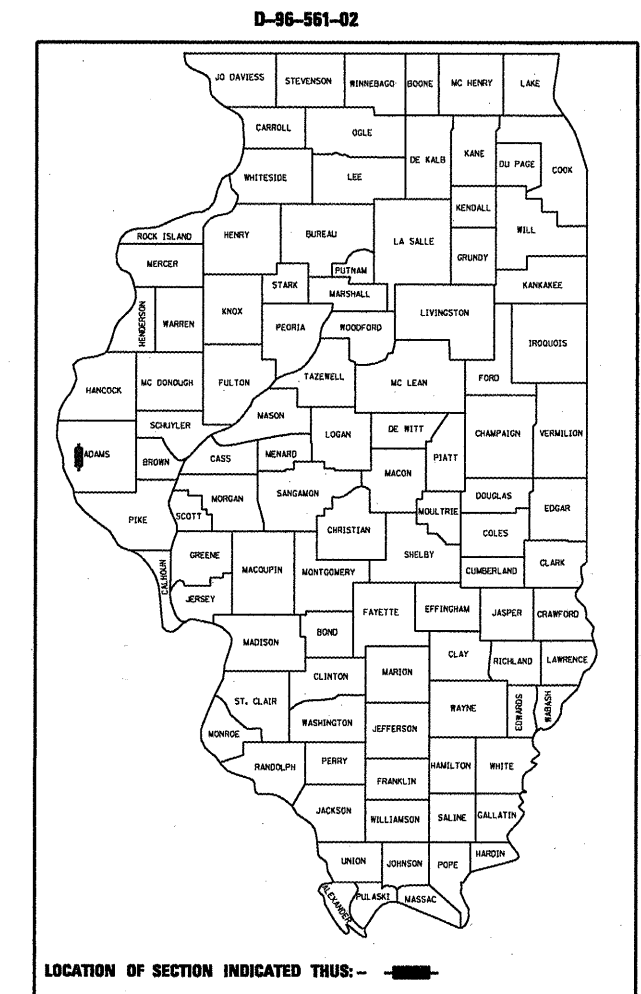
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**  
**DIVISION OF HIGHWAYS**

**PROPOSED**  
**HIGHWAY PLANS**

**F.A. ROUTE 172 (I-172)**  
**SECTION (1-5)RS-1; (1-6)RS**  
**C-96-049-09**

**ADAMS COUNTY**

PROJECT: ESP-172-1(002)012



**F.A.I. ROUTE 172**  
FUNCTIONAL CLASS = INTERSTATE  
A.D.T. (2003) 6600 PV 86.0%  
A.D.T. (2006) 6800 SU 6.0%  
A.D.T. (2026) 8200 MU 7.0%

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

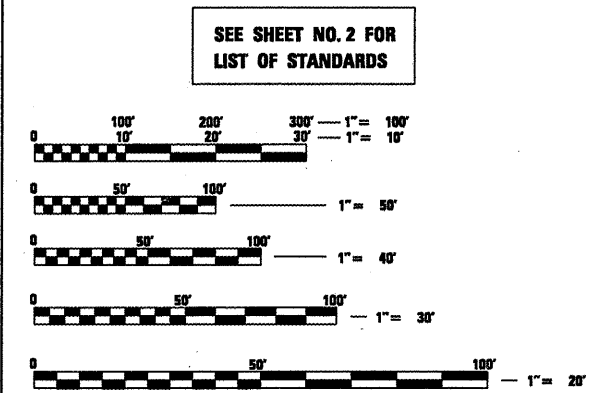
SUBMITTED February 11, 20 09  
By: [Signature] DISTRICT ENGINEER

March 13, 20 09  
Charles G. Ingervoll [Signature]  
ENGINEER OF DESIGN AND ENVIRONMENT

March 13, 20 09  
Christine M. Reed [Signature]  
DIRECTOR, DIVISION OF HIGHWAYS

- END PROJECT STA. 247+56 S.B. 247+31.66 N.B. 1-6RS
- CH 3 (ELLINGTON RD) OVER FAI-172 SN 001-0049
- FAI-172 OVER B.N. RAILROAD S.N. 001-0050 (SB) & S.N. 001-0051 (NB)
- FAS 604/TR 349 (COLUMBUS RD) OVER FAI-172 SN 001-0048
- STATION EQUATION STA. 288+81.55 (BK)= STA. 0+00 (AH)
- FAP 765 (IL 104) OVER FAI-172 SN 001-0047
- FAS 591/CH5 (STATE ST) OVER FAI-172 SN 001-0046
- TR 423 (ST. ANTHONY RD) OVER FAI-172 SN 001-0045
- BEGIN PROJECT STA. 75+24.00 S.B. STA. 78+25.00 N.B.

PROJECT INCLUDES BRIDGE REPAIRS TO SN 001-0045-0048; 0049; 0050; AND 0051

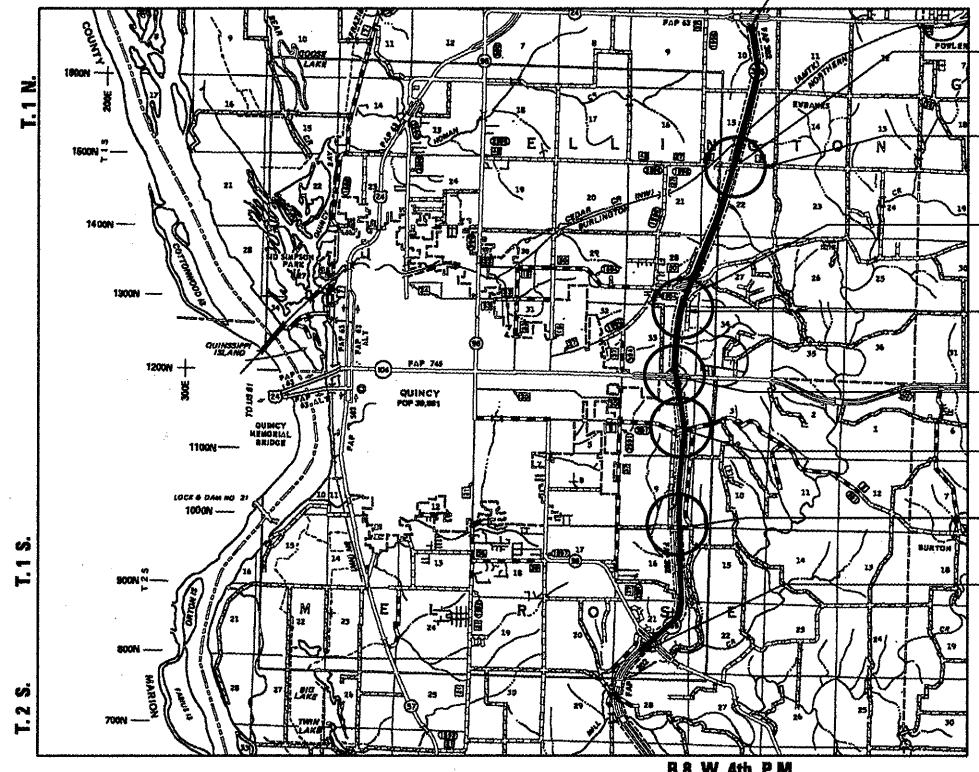


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

PROJECT ENGINEER: JOHN NEGANGARD (217) 782-6990  
SQUAD LEADER: VICTOR YOUNG (217) 557-7897

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123

**CONTRACT NO. 72694**



APPROXIMATE SCALE 1"=1 MILE

NET LENGTH OF THE PROJECT- 46,780.800 FEET = 8.860 MILES  
NET LENGTH OF T.R. 349 (COLUMBUS ROAD) - 0.550 MILES  
TOTAL LENGTH OF PROJECT = 8.860 MILES

RATES OF APPLICATION TABLE	
AGGREGATE (SURFACE, BASE, SUBBASE, OR BACKFILL)	2.05 TON / CU YD
STONE DUMPED RIPRAP	1.75 TON / CU YD
BITUMINOUS MATERIALS (PRIME COAT)	0.00038 TON / SQ YD
AGGREGATE (PRIME COAT)	0.002 TON / SQ YD
BITUMINOUS SURFACE / BINDER (112 LBS)	0.056 TON / SQ YD • IN
SEEDING AREAS:	
NITROGEN FERTILIZER NUTRIENT	90 LBS / ACRE
PHOSPHORUS FERTILIZER NUTRIENT	90 LBS / ACRE
POTASSIUM FERTILIZER NUTRIENT	90 LBS / ACRE
AGGRICULTURAL GROUND LIMESTONE	2 TON / ACRE
MULCH	2 TON / ACRE

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

MIXTURE USE(S)	POLYMER HMA SURFACE	POLYMER HMA BINDER	HMA LEVEL BINDER
AC/PG	SBS PG 70-22	SBS PG 70-22	PG 64-22
RAP % (MAX)			
DESIGN AIR VOIDS	4.0% @ N DESIGN 70	4.0% @ N DESIGN 70	4.0% @ N DESIGN 70
MIXTURE COMPOSITION (GRADATION MIXTURE)	IL 9.5 OR IL 12.5	IL 19.0	IL 9.5
FRICTION AGGREGATE	MIX D	N/A	N/A

MIXTURE USE(S)	HMA SHOULDER LOWER LIFT	HMA SHOULDER UPPER LIFT	HMA SURFACE (OVERHEAD BRIDGES & APPROACHES)
AC/PG	PG 58-22	PG 64-22	PG 64-22
RAP % (MAX)			
DESIGN AIR VOIDS	2.0% @ N DESIGN 30	4.0% @ N DESIGN 50	4.0% @ N 50
MIXTURE COMPOSITION (GRADATION MIXTURE)	BAM (OTHER)	IL 9.5	IL 9.5 OR 12.5
FRICTION AGGREGATE	N/A	MIX C	MIX C

COMMITMENTS

- THE FIELD/RESIDENT ENGINEER SHALL CONTACT STUDIES & PLANS CONCERNING ANY MAJOR PLAN CHANGES TO MAKE SURE NO PREVIOUS COMMITMENTS (NOT LISTED) WERE MADE AFFECTING THE DESIGN, AND TO ALLOW IMPROVEMENT IN THE DESIGN FOR FUTURE PROJECTS.
- SWPPP (NPDES PERMIT)
- LETTER OF UNDERSTANDING WITH ADAMS COUNTY & TOWNSHIPS

GENERAL NOTES:

- WHERE SECTION OR SUB-SECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER AND AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.
- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION, AS INDICATED BY THE SUB-NUMBER LISTED IN THE INDEX OF SHEETS, OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.(COPY OF STANDARD IN PLANS WILL GOVERN).
- EXISTING RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE REMOVED PRIOR TO RESURFACING.
- PRIOR TO RESURFACING ACTIVITIES, THE CONTRACTOR AND THE ENGINEER SHALL VERIFY AND DOCUMENT THE EXISTING MINIMUM VERTICAL CLEARANCE AT ALL OVERHEAD STRUCTURES. IF NECESSARY, THE ENGINEER WILL MAKE ADJUSTMENTS TO THE RESURFACING THICKNESS SHOWN IN THE PLANS. UPON COMPLETION OF THE RESURFACING, THE ENGINEER WILL MEASURE AND DOCUMENT THE MINIMUM VERTICAL CLEARANCE AT ALL OVERHEAD STRUCTURES. IF THE MINIMUM VERTICAL CLEARANCE IS LESS THAN 16'-0", THE CONTRACTOR SHALL REMOVE AND REPLACE THE BITUMINOUS CONCRETE SURFACE COURSE AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK OR ANY INCONVENIENCE CAUSED BY COMPLYING WITH THIS REQUIREMENT WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE RESURFACING PAY ITEMS IN THIS CONTRACT.
- A POLYMER MODIFIED EMULSIFIED ASPHALT PRIME SHALL BE REQUIRED FOR THE PRIMING OF I-172 MAINLINE AND RAMP PAVEMENT AND TO TR349 PAVEMENT. THE AREA PRIMED SHALL BE LIMITED TO THAT WHICH WILL BE COVERED WITH BITUMINOUS CONCRETE THAT SAME DAY.
- THE CONTRACTOR SHALL MAINTAIN ACCESS CONTROL AT ALL TIMES DURING CONSTRUCTION.
- ELEVATIONS ALONG THE RADII WHERE THE RAMPS ARE TO BE WIDENED SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- ALL SIGNS AND DELINEATORS CONFLICTING WITH TRAFFIC CONTROL OR CONSTRUCTION SHALL BE REMOVED AND REPLACED BY THE CONTRACTORS. THIS WORK WILL BE INCLUDED WITH THE COST FOR TRAFFIC CONTROL ON THE PROJECT. IN ADDITION, THE COST TO RELOCATE SIGNS DESIGNATED ON THE PLAN SHEETS WILL ALSO BE INCIDENTAL TO THE TRAFFIC CONTROL FOR THOSE AREAS.

LIST OF STANDARDS

000001-05	631031-07	701402-07
001001-02	635001-01	701406-05
001006	635006-03	701411-05
280001-04	635011-02	701426-03
420001-07	642001.01	701451
442001-04	665001-02	701456
442101-07	701001-02	701901-01
542301-02	701006-03	704001-05
601001-03	701011-02	720001-01
601101-01	701101-02	720006-02
604001-03	701106-02	720011-01
604101-01	701201-03	728001-01
606001-04	701301-03	729001-01
606301-04	701306-02	731001-01
606401-01	701311-03	780001-02
630001-08	701326-03	781001-03
630301-05	701400-03	
631011-05	701401-05	

DISTRICT SIX	
EXAMINED <u>Feb 10</u> 20 <u>09</u>	
<i>Ron J. Hoar</i>	
OPERATIONS ENGINEER	
EXAMINED <u>Feb 10</u> 20 <u>09</u>	
<i>Ron J. Hoar</i>	
PROGRAM IMPLEMENTATION ENGINEER	
EXAMINED <u>Feb 11</u> 20 <u>09</u>	
<i>R. J. Hoar</i>	
PROGRAM DEVELOPMENT ENGINEER	

REVISIONS	
NAME	DATE

100% FED

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	3
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS	FED. AID PROJECT	
• (1-51RS-1) (1-61RS)				
<b>CONTRACT NO. 72694</b>				

SUMMARY OF QUANTITIES							
CODE NO.	ITEM	UNIT	TOTAL	RURAL ROADWAY 1000-2A	URBAN ROADWAY 1000-2A	SN 001-0050 SN 001-0051 X120-2A	SN 001-0045 SN 001-0048 SN 001-0049 X220-2A
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	159	159			
20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	81	81			
20200100	EARTH EXCAVATION	CU YD	219	219			
20200200	ROCK EXCAVATION	CU YD	25	25			
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	14	14			
20201400	SUB-BASE GRANULAR MATERIAL, TYPE A	TON	449	449			
21400100	GRADING AND SHAPING DITCHES	FOOT	13,945	13945			
25101005	HEAVY DUTY EXCELSIOR BLANKET	SQ YD	635	635			
28000500	INLET AND PIPE PROTECTION	EACH	9	9			
28001000	AGGREGATE (EROSION CONTROL)	TON	434	434			
28100807	STONE DUMPED RIPRAP, CLASS A4	TON	7,052	7052			
XX005519	STONE DUMPED RIPRAP, CLASS A4 (SPECIAL)	TON	2,009	2009			
28200200	FILTER FABRIC	SQ YD	12,088	12088			
35100700	AGGREGATE BASE COURSE, TYPE A 8"	SQ YD	211	211			
35300300	PORTLAND CEMENT CONCRETE BASE COURSE 8"	SQ YD	387	387			
35300700	PORTLAND CEMENT CONCRETE BASE COURSE 12"	SQ YD	34	34			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	362.5	309.2	53.3		
40600300	AGGREGATE (PRIME COAT)	TON	1,908	1700	208		
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	774	774			
40600895	CONSTRUCTING TEST STRIP	EACH	2	2			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	6,182	6115	67		
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SQ YD	4,289	4181	108		
40600990	TEMPORARY RAMP	SQ YD	726	668	58		
40603235	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	34,616	29324	5292		

URBAN ROADWAY STATION 236+00.00 TO STATION 288+81.55

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SUMMARY OF QUANTITIES**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	4
STA.		TO STA.		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				
* (1-51RS-1) (1-61RS)				
<b>CONTRACT NO. 72694</b>				

100% FED

SUMMARY OF QUANTITIES							
CODE NO.	ITEM	UNIT	TOTAL	RURAL ROADWAY 1000-2A	URBAN ROADWAY 1000-2A	SN 001-0050 SN 001-0051 X120-2A	SN 001-0045 SN 001-0048 SN 001-0049 X220-2A
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	168	97			71
40603540	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	24,516	20973	3543		
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	894	894			
44000915	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	846				846
44001700	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	53	53			
44002020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	2,336	2336			
44004250	PAVED SHOULDER REMOVAL	SQ YD	2,602	2602			
44200529	CLASS A PATCHES, TYPE II, 8 INCH	SQ YD	1716	1409	307		
44200533	CLASS A PATCHES, TYPE III, 8 INCH	SQ YD	530	349	181		
44200535	CLASS A PATCHES, TYPE IV, 8 INCH	SQ YD	4612	4284	328		
44200934	CLASS B PATCHES, TYPE II, 8 INCH	SQ YD	1380	1197	183		
44200942	CLASS B PATCHES, TYPE III, 8 INCH	SQ YD	229	229			
44200944	CLASS B PATCHES, TYPE IV, 8 INCH	SQ YD	930	930			
44201294	CLASS B PATCH - EXPANSION JOINT	FOOT	16.5		16.5		
44201296	DEFORMED BARS - EXPANSION JOINT	EACH	14		14		
44213000	PATCHING REINFORCEMENT	SQ YD	6857	6041	816		
44213100	PAVEMENT FABRIC	SQ YD	1158	1158			
44213200	SAW CUTS	FOOT	34,183	29435	4748		
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	21,388	18429	2959		
48101200	AGGREGATE SHOULDERS, TYPE B	TON	12,997	11330	1667		
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	3,186	3186			
48203100	HOT-MIX ASPHALT SHOULDERS	TON	37,448	32406	5042		
50102400	CONCRETE REMOVAL	CU YD	70			70	
50105200	REMOVE EXISTING CULVERTS	EACH	40	40			

URBAN ROADWAY STATION 236+00.00 TO STATION 288+81.55

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SUMMARY OF QUANTITIES**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_

F.A.T. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	988	5
STA.		TO STA.		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				
* (1-51RS-1); (1-61RS)				
<b>CONTRACT NO. 72694</b>				

100% FED

SUMMARY OF QUANTITIES							
CODE NO.	ITEM	UNIT	TOTAL	RURAL ROADWAY 1000-2A	URBAN ROADWAY 1000-2A	SN 001-0050 SN 001-0051 X120-2A	SN 001-0045 SN 001-0048 SN 001-0049 X220-2A
50157305	PROTECTIVE SHIELD	L SUM	1				1
50300255	CONCRETE SUPERSTRUCTURE	CU YD	70			70	
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	12,520			12520	
50800515	BAR SPLICERS	EACH	56			56	
52000110	PREFORMED JOINT STRIP SEAL	FOOT	301			301	
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTION 12"	EACH	1	1			
54213669	PRECAST REINFORCED CONCRETE FLARED END SECTION 24"	EACH	1	1			
54213675	PRECAST REINFORCED CONCRETE FLARED END SECTION 30"	EACH	1	1			
54213681	PRECAST REINFORCED CONCRETE FLARED END SECTION 36"	EACH	1	1			
54213687	PRECAST REINFORCED CONCRETE FLARED END SECTION 42"	EACH	1	1			
58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD	846				846
60108100	PIPE UNDERDRAINS 4" (SPECIAL)	FOOT	255	255			
60260300	INLETS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1			
60262700	INLETS TO BE RECONSTRUCTED	EACH	6	6			
60265505	MEDIAN INLET (604101) TO BE RECONSTRUCTED	EACH	11	11			
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	15	15			
60608600	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06	FOOT	44	44			
60610900	COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.24 (VARIABLE WIDTH GUTTER FLAG)	FOOT	76	76			
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	868	868			
60622400	CONCRETE MEDIAN, TYPE SM-6.06	SQ FT	1,407	1407			
63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	5,775	5225	550		
63000003	STEEL PLATE BEAM GUARDRAIL, TYPE A, 9 FOOT POSTS	FOOT	450	450			
63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	10	8	2		
63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	16	16			

• **SPECIALTY ITEM**  
URBAN ROADWAY STATION 236+00.00 TO STATION 288+81.55

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION	
NAME	DATE	<b>SUMMARY OF QUANTITIES</b>	

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	6
STA.		TO STA.		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				
• (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				

100% FED

SUMMARY OF QUANTITIES							
CODE NO.	ITEM	UNIT	TOTAL	RURAL ROADWAY 1000 - 2A	URBAN ROADWAY 1000 - 2A	SN 001-0050 SN 001-0051 X120-2A	SN 001-0045 SN 001-0048 SN 001-0049 X220-2A
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1(SPECIAL) TANGENT	EACH	30	28	2		
63200310	GUARDRAIL REMOVAL	FOOT	4,375	4375			
64200105	SHOULDER RUMBLE STRIP	FOOT	174,150	174150			
66502405	WOVEN WIRE FENCE REMOVAL AND REPLACEMENT	FOOT	1,065	1065			
67000500	ENGINEER'S FIELD OFFICE, TYPE B	CAL MO	15	15			
67100100	MOBILIZATION	L SUM	1	1			
70100207	TRAFFIC CONTROL AND PROTECTION, STANDARD 701402	EACH	2	2			
70100405	TRAFFIC CONTROL AND PROTECTION, STANDARD 701321	EACH	1	1			
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	10	10			
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1			
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1			
70100500	TRAFFIC CONTROL AND PROTECTION, STANDARD 701326	L SUM	1	1			
70100700	TRAFFIC CONTROL AND PROTECTION, STANDARD 701406	L SUM	1	1			
70100800	TRAFFIC CONTROL AND PROTECTION, STANDARD 701401	L SUM	1	1			
70100820	TRAFFIC CONTROL AND PROTECTION, STANDARD 701451	L SUM	1	1			
70100825	TRAFFIC CONTROL AND PROTECTION, STANDARD 701456	L SUM	1	1			
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	45	45			
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1	1			
7010 6800	CHANGEABLE MESSAGE SIGN	CAL MO	18	18			
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	42,500	42500			
70300210	TEMPORARY PAVEMENT MARKING - LETTERS <sup>AND</sup> SYMBOLS	SQ FT	291	245	46		
70300230	TEMPORARY PAVEMENT MARKING - LINE 5"	FOOT	252,986	218461	34525		
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	364	364			
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	343	343			

• SPECIALTY ITEMS

URBAN ROADWAY STATION 236+00.00 TO STATION 288+81.55

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SUMMARY OF QUANTITIES**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

100% FED

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	7
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS FED. AID PROJECT		
* (1-5)RS-1; (1-6)RS				
CONTRACT NO. 72694				

SUMMARY OF QUANTITIES							
CODE NO.	ITEM	UNIT	TOTAL	RURAL ROADWAY 1000-2A	URBAN ROADWAY 1000-2A	SN 001-0050 SN 001-0051 X120-2A	SN 001-0045 SN 001-0048 SN 001-0049 X220-2A
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	299	299			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	120	120			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	7,100	7100			
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,525	1525			
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1,525	1525			
78004200	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LETTERS <sup>AND</sup> SYMBOLS	SQ FT	291	245	46		
78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LINE 5"	FOOT	22,980	20340	2640		
78004240	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LINE 8"	FOOT	343	343			
7804250	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LINE 12"	FOOT	299	299			
78004280	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B-INLAID - LINE 24"	FOOT	120	120			
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1,623	1623			
78200410	GUARDRAIL MARKERS, TYPE A	EACH	67	67			
78201000	TERMINAL MARKERS - DIRECT APPLIED	EACH	30	28	2		
78300105	PAVEMENT MARKING REMOVAL	FOOT	15,000	15000			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1,600	1600			
X0301424	SILICONE JOINT SEALER	FOOT	299				299
X0301508	REMOVE AND REINSTALL CONCRETE HEADWALL	EACH	12	12			
X0322729	MATERIAL TRANSFER DEVICE	TON	50,643	50643			
X0322879	GRADING AND SHAPING FORESLOPES	SQ YD	445	445			
X0326350	POSTS FOR WOVEN WIRE FENCE	EACH	20	20			
X0326351	CONCRETE APRON FOR MEDIAN INLET	EACH	13	13			
X0323665	RIPRAP SLURRY	SQ YD	3444	3444			
X0350800	BOLLARDS	EACH	135	135			

• SPECIALTY ITEMS

URBAN ROADWAY STATION 236+00.00 TO STATION 288+81.55

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SUMMARY OF QUANTITIES**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	8
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS	FED. AID PROJECT	
* (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				

100% FED

SUMMARY OF QUANTITIES							
CODE NO.	ITEM	UNIT	TOTAL	RURAL ROADWAY 1000-2A	URBAN ROADWAY 1000-2A	SN 001-0050 SN 001-0051 X120-2A	SN 001-0045 SN 001-0048 SN 001-0049 X220-2A
X0350810	BOLLARD REMOVAL	EACH	135	135			
X2503000	MAINTENANCE MOWING	ACRE	225	225			
X7200201	WIDTH RESTRICTION SIGNING	L SUM	1	1			
X7800620	URETHANE PAVEMENT MARKING - LINE 5"	FOOT	230012	198127	31885		
X7800630	URETHANE PAVEMENT MARKING - LINE 6"	FOOT	364	364			
XX003437	REMOVE AND REINSTALL EXISTING PRECAST REINFORCED CONCRETE FLARED END SECTIONS	EACH	5	5			
XX003503	FLARED END SECTION REMOVAL	EACH	5	5			
Z0005300	BOX CULVERTS TO BE CLEANED	EACH	4	4			
Z0005600	BREAKING PAVED DITCH	FOOT	536	536			
Z0014700	CULVERT TO BE CLEANED	EACH	21	21			
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD	9.5				9.5
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	36				36
Z0017202	DOWEL BARS 1 1/2"	EACH	4,130	3668	462		
Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	4	4			
Z0030330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	4	4			
Z0033600	LONGITUDINAL JOINT REPAIR	FOOT	33,919	28355	5564		
Z0075300	TIE BARS	EACH	1,705	1589	116		
X0326352	REMOVE AND RELOCATE CROSSOVER	EACH	1	1			
X0326353	REATTACH WOVEN WIRE FENCE BRACE	EACH	123	123			
X0326354	WOVEN WIRE FENCE REPAIR (SPECIAL)	FOOT	1,645	1645			
X0322905	PRECAST PRESTRESSED CONCRETE I-BEAM REPAIRS	L SUM	1			1	
44212900	PAVEMENT PATCHING (PARTIAL DEPTH)	SQ YD	800	800			

Δ NON PARTICIPATING (100% STATE)  
 • SPECIALTY ITEM  
 URBAN ROADWAY STATION 236+00.00 TO STATION 288+81.55

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SUMMARY OF QUANTITIES**

SCALE: VERT.  
HORIZ.  
DATE

DRAWN BY  
CHECKED BY



GRADING & SHAPING					
STATION	OFFSET	LOCATION	GRADING & SHAPING DITCHES	GRADING & SHAPING FORESLOPES	HEAVY DUTY EXCELISIOR BLANKET
	(FOOT)		(FOOT)	(SQ YD)	(SQ YD)
<u>MAINLINE</u>					
102+00 TO 102+50	95'	LT.	50		
107+20 TO 110+35.67	150'	RT.	316		
110+35.67 TO 110+85.6	132'	RT.	50		
110+95 TO 111+15	132'	RT.	20		
110+95 TO 112+90	200'	RT.	195		
112+80 TO 113+00	132'	RT.	20		
113+00 TO 113+20	132'	RT.	20		
117+00 TO 123+00	0	MEDIAN	600		
151+50 TO 155+90	84'	RT.	440		
151+50 TO 152+25	106'	LT.	75		
155+16 TO 156+16	108'	LT.	100		
171+78.63 TO 187+13.91	97'	RT.	1535		
176+76 TO 177+76	102'	LT.	100		
181+88 TO 182+08	190'	RT.	20		
182+10 TO 182+30	190'	RT.	20		
187+13.91 TO 198+00	98'	RT.			
193+10 TO 195+00	0	MEDIAN	190		
195+86 TO 196+06	190'	RT.	20		
196+08 TO 196+28	190'	RT.	20		
203+90 TO 204+40	98'	RT.	50		
206+49.88 TO 209+27.31	98'	LT.	277		
215+00 TO 217+84.42	98'	LT.	284		
217+84.42 TO 220+92.99	98'	LT.	309		
224+39.33 TO 228+14.18	98'	LT.	375		
241+51.69 TO 244+04.05	108'	RT.	288		
249+00 TO 250+00	149'	LT.	100		
251+75 TO 254+00	110'	RT.	225		
15+79.65 TO 17+02.67	98'	RT.	123		
24+40 TO 26+50	98'	RT.	210		467
45+45.12 TO 50+47.42	100'	LT.	502		
82+50 TO 83+00	160'	RT.		111	57
87+77 TO 87+27	100'	LT.		167	111
86+82 TO 87+32	90'	RT.		167	
110+50 TO 111+00	0	MEDIAN	50		
122+00 TO 128+73.56	110'	RT.	674		
128+73.56 TO 138+78	110'	RT.	1004		
121+00 TO 128+73.56	110'	LT.	780		
128+73.56 TO 138+20	110'	LT.	946		
139+75 TO 143+95	110'	LT.	420		
140+25 TO 143+25	110'	RT.	300		
SUBTOTAL 1=			10708	445	635

GRADING & SHAPING					
STATION	OFFSET	LOCATION	GRADING & SHAPING DITCHES	GRADING & SHAPING FORESLOPES	HEAVY DUTY EXCELISIOR BLANKET
	(FOOT)		(FOOT)	(SQ YD)	(SQ YD)
179+50 TO 180+00	0	MEDIAN	50		
188+50 TO 190+13.56	110'	RT.	164		
190+13.56 TO 190+50	110'	RT.	36		
195+75 TO 196+75	115'	RT.	100		
196+95 TO 197+95	115'	RT.	100		
198+30 TO 200+20	100'	LT.	190		
208+00 TO 211+25	160'	RT.	325		
216+90 TO 217+90	115'	RT.	100		
217+50 TO 218+75	105'	LT.	125		
224+95 TO 225+95	118'	LT.	100		
226+05 TO 227+05	118'	LT.	100		
225+75 TO 223+50	98'	RT.	75		
<u>IL 104 RAMP B</u>					
03+90 TO 04+50	40'	RT.	60		
12+50 TO 13+00	29'	RT.	50		
<u>TR 349 RAMP A</u>					
07+52.79 TO 8+00	40'	RT.	47.21		
<u>TR 349 RAMP B</u>					
10+00 TO 10+50	35'	RT.	50		
<u>US 24 RAMP A</u>					
4+50 TO 8+00	50'	RT.	350		
8+00 TO 14+15	70'	RT.	615		
<u>US 24 RAMP D</u>					
11+00 TO 17+20	50'	LT.	600		
SUBTOTAL 2=			3237	0	0
SUBTOTAL 1=			10708	445	635
GRAND TOTAL			13945	445	635

REVISIONS	
NAME	DATE

RIPRAP SCHEDULE					
LOCATION	LT /RT	STONE DUMPED RIPRAP A4 (TON)	FILTER FABRIC (SQ YD)	STONE DUMPED RIPRAP, A4 (SPECIAL) (TON)	RIPRAP SLURRY (SQ YD)
85+00 MEDIAN		17.5	30		
91+21 TO 92+20	LT	87.5	150		
92+60 TO 93+00	LT	32.1	55		
105+20	LT	43.8	75		
110+35.67 TO 111+00	RT	29.2	50		
113+00	RT	29.2	50		
119+42	LT	134.2	230		
116+50	RT	11.1	19		
117+42	RT			128.3	220.0
117+70 TO 123+00	RT			347.4	595.6
142+00	LT	55.8	95.6		
151+10	LT	20.4	35		
151+10	RT	23.3	40		
151+58	LT	20.4	35		
151+58	RT	23.3	40		
155+81	RT	58.3	100		
165+00 TO 167+78	LT	113.4	194.4		
177+00	LT	11.7	20		
206+30 TO 204+05	LT	72.9	125		
209+00	LT	64.2	110		
217+50	LT	78.8	135		
217+50	RT	20.4	35		
219+00	RT	46.7	80		
219+80	LT	37.9	65		
221+00	LT	105	180		
223+10	LT	35	60		
223+10	RT	37.9	65		
223+50	LT	35	60		
223+70	RT	14.6	25		
226+00	LT	37.9	65		
227+00	LT	37.9	65		
231+00	RT			296.2	507.8
249+00	LT			37.0	63.4
250+00	LT			50.9	87.2
261+70	RT	17.5	30		
261+91	LT	17.5	30		
262+61	RT	17.5	30		
262+90	LT	17.5	30		
265+20	LT	8.8	15		
267+50	LT	32.1	55		
<b>SUBTOTAL 1 =</b>		<b>1446.3</b>	<b>2479</b>	<b>859.8</b>	<b>1474.0</b>

RIPRAP SCHEDULE					
LOCATION	LT /RT	STONE DUMPED RIPRAP A4 (TON)	FILTER FABRIC (SQ YD)	STONE DUMPED RIPRAP, A4 (SPECIAL) (TON)	RIPRAP SLURRY (SQ YD)
28+00	RT	11.7	20		
30+30	RT	11.7	20		
31+00	LT	46.7	80		
42+00	RT	96.3	165		
44+00	LT	81.7	140		
45+00	LT	81.7	140		
46+00	RT	81.7	140		
47+00	RT	81.7	140		
50+45	RT	8.8	15		
50+45	LT	8.8	15		
72+50	RT	35	60		
76+12 TO 76+80	LT	58.3	100		
78+92	LT	35	60		
81+40	RT			291.7	500.0
83+00 TO 88+10	RT	361.7	620		
85+00	LT	20.4	35		
85+50 TO 89+60	LT	300.4	515		
91+00	LT	37.9	65		
92+50	LT	37.9	65		
94+00	LT	37.9	65		
97+00	RT	37.9	65		
99+46	LT	37.9	65		
101+31	RT	37.9	65		
102+32	LT	64.2	110		
104+12 TO 105+40	RT	96.3	165		
106+00	LT	37.9	65		
107+21	LT	8.8	15		
109+00	LT	26.3	45		
110+00	RT			81.7	140.0
110+00 TO 112+50	RT			245	420.0
113+38.56 TO 122+00	RT	583.3	1000		
120+02 TO 124+90	LT	96.3	165		
130+00	LT	29.2	50		
131+00	LT	29.2	50		
134+45	RT	11.7	20		
138+85	LT	46.7	80		
139+28	RT	40.8	70		
139+40	LT	40.8	70		
139+80	RT	40.8	70		
151+00	RT	35	60		
172+15	RT	46.7	80		
181+00	RT	58.3	100		
190+65	RT	43.8	75		
196+80	RT	37.9	65		
217+35	LT	70	120		
225+00	LT	11.7	20		
225+00	RT	46.7	80		
228+55	RT	64.2	110		
229+10	LT	70	120		
231+40 TO 232+31	RT	70	120		
<b>SUBTOTAL 2 =</b>		<b>3255.6</b>	<b>5580</b>	<b>618.4</b>	<b>1060.0</b>

RIPRAP SCHEDULE					
LOCATION	LT /RT	STONE DUMPED RIPRAP A4 (TON)	FILTER FABRIC (SQ YD)	STONE DUMPED RIPRAP, A4 (SPECIAL) (TON)	RIPRAP SLURRY (SQ YD)
COLUMBUS ROAD					
RAMP A					
3+21	RT	26.3	45		
RAMP B					
6+60	RT	37.9	65		
11+00	RT	14.6	25		
14+28 TO 15+25	RT	87.5	150		
RAMP C					
7+65	LT	40.8	70		
RAMP D					
8+00	RT	37.9	65		
11+50	RT			46.7	80.0
IL 104					
RAMP A					
1+79 TO 17+36	RT	1087.3	1864		
RAMP B					
8+50 TO 15+80	RT	469.6	805		
RAMP C					
7+75 TO 10+00	LT			175	300.0
RAMP D					
8+10	LT	29.2	50		
8+10	RT	145.8	250		
US 24					
RAMP A					
4+29	RT	70	120		
16+93.34	LT	291.7	500		
16+93.34	RT			233.3	400.0
RAMP D					
2+22	LT	11.7	20		
11+00	LT			75.8	130.0
<b>SUBTOTAL 3 =</b>		<b>2350.3</b>	<b>4029</b>	<b>530.8</b>	<b>910</b>
<b>SUBTOTAL 1 =</b>		<b>1446.3</b>	<b>2479</b>	<b>859.8</b>	<b>1474.0</b>
<b>SUBTOTAL 2 =</b>		<b>3255.6</b>	<b>5580</b>	<b>618.4</b>	<b>1060.0</b>
<b>GRAND TOTAL =</b>		<b>7052.2</b>	<b>12088</b>	<b>2009.0</b>	<b>3444.0</b>

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY  
 HORIZ.              CHECKED BY  
 DATE

BUTT-JOINT SCHEDULE										
LOCATION	STATION TO STATION		WIDTH (FOOT)	LENGTH (FOOT)	THICKNESS (INCH)	PCC SURFACE REMOVAL - BUTT JOINT (SQ YD)	HMA SURFACE REMOVAL - BUTT JOINT (SQ YD)	TEMPORARY RAMP (SQ YD)		
NB MAINLINE	78+25.	TO	78+55.	40	30.00	1.5	80.0	53.3	22.2	
	160+12.	TO	160+68.82	40	56.82	1.5	151.5	101.0	22.2	
	163+57.36	TO	164+36.	40	78.64	1.5	209.7	139.8	22.2	
	247+01.66	TO	247+31.66	40	30.00	1.5	80.0	53.3	22.2	
SB MAINLINE	75+24.	TO	75+54.	40	30.00	1.5	80.0	53.3	22.2	
	149+90.	TO	152+70.	40	280.00	1.5	746.7	497.8	22.2	
	158+21.	TO	159+02.43	40	81.43	1.5	217.1	144.8	22.2	
	161+91.32	TO	162+88.	40	96.68	1.5	257.8	171.9	22.2	
	247+26.46	TO	247+56.46	40	30	1.5	80.0	53.3	22.2	
IL 104										
RAMP A	19+39.06	TO	19+54.06	26	15	1.5	26.7	16.7	14.4	
RAMP B	1+26.78	TO	1+41.78	26	15	1.5	26.7	16.7	14.4	
RAMP C	18+40.09	TO	18+55.09	26	15	1.5	26.7	16.7	14.4	
RAMP D	1+29.	TO	1+44.	26	15	1.5	26.7	16.7	14.4	
TR 349										
MAINLINE	35+48.85	TO	35+78.85	40	30	1.5	66.7	53.3	22.2	
MAINLINE	38+20.00	TO	39+69.00				65.6			
RT	47+54.	TO	BRIDGE	22	57	1.5		115.5	7.8	
LT	48+03.5	TO	BRIDGE	22	36	1.5		86.7	7.8	
RT	BRIDGE	TO	51+93.	22	57	1.5		86.0	7.8	
LT	BRIDGE	TO	52+43.	22	36	1.5		115.5	7.8	
MAINLINE	60+84.00	TO	62+65.00				80.1			
MAINLINE	64+45.	TO	64+75.	40	30	1.5	66.7	53.3	22.2	
ENTRANCES	LT 37+00			18	12	1.5		31.1	10.0	
	RT 62+45			23	30	1.5		84.3	12.8	
US 24										
RAMP A	19+25.	TO	20+50.	VAR	125	2.5		785.0	147.8	
RAMP D	+24.	TO	1+45.	VAR	121	2.5		585.0	221.7	
TOTALS =							2288.6	3331.0	725.3	

PIPE UNDERDRAIN SCHEDULE					
STATION	LOCATION	PIPE UNDERDRAINS 4" (SPECIAL) REPLACE EXISTING (FOOT)	REMOVE AND REINSTALL CONCRETE HEADWALL (EACH)	HMA SHOULDERS 8" (SQ YD)	PAVED SHOULDER REMOVAL (SQ YD)
N. B. 135+00	OUTSIDE SHOULDER	20	1	4.4	4.4
N. B. 135+00	INSIDE SHOULDER	20	1	2.7	2.7
N. B. 140+00	OUTSIDE SHOULDER	20	1	4.4	4.4
N. B. 153+00	OUTSIDE SHOULDER	25	1	4.4	4.4
N. B. 158+00	OUTSIDE SHOULDER	20	1	4.4	4.4
N. B. 164+00	OUTSIDE SHOULDER	20	1	4.4	4.4
N. B. 172+00	OUTSIDE SHOULDER	20	1	4.4	4.4
N. B. 286+00	OUTSIDE SHOULDER	25	1	4.4	4.4
S. B. 140+00	OUTSIDE SHOULDER	20	1	4.4	4.4
S. B. 202+50	OUTSIDE SHOULDER	20	1	4.4	4.4
S. B. 206+30	OUTSIDE SHOULDER	25	1	4.4	4.4
TOTAL =		255	12	51.1	51.1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY JJS  
 HORIZ.              CHECKED BY LLO  
 DATE 08/04/2004

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	12

STA. TO STA.  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

PAVING SCHEDULE

LOCATION	STATION TO STATION		LENGTH (FT)	BITUMINOUS MATERIALS (PRIME COST)	AGGREGATE (PRIME COAT)	HMA SURFACE COURSE, MIX D, N70	HMA BINDER COURSE, IL-9.5, N70	LEVELING BINDER (MACHINE METHOD) N70	HMA SHOULDERS	AGGREGATE SHOULDERS, TYPE B	SHOULDER RUMBLE STRIP	MATERIAL TRANSFER DEVICE	
				(TON)	(TON)	(TON)	(TON)	(TON)	(TON)	(TON)	(TON)	(FOOT)	(TON)
NB MAINLINE	78+25.00	TO	288+81.55	21056.55	71.1	374.3	4716.7	7057.4		7464.4	2295.9	40413	11774.0
	0+00.00	TO	160+68.82	16068.82	54.3	285.7	3599.4	5229.7	72.8	5602.4	1739.2	30438	8829.1
	163+57.36	TO	247+31.66	8374.30	28.3	148.9	1875.8	2762.1		2998.1	917.1	16199	4638.0
SB MAINLINE	75+24.00	TO	288+81.55	21357.55	72.1	379.7	4784.1	7003.9	30.2	7576.8	2329.5	41015	11788.0
	0+00.00	TO	159+02.43	15902.43	53.7	282.7	3562.1	5308.0		5540.2	1720.7	30105	8870.1
	161+91.32	TO	247+56.46	8565.14	28.9	152.3	1918.6	2825.2		2929.3	919.3	15980	4743.8
CROSSOVER	95+90.00				0.1	0.7	14.8	22.2					
	221+00.00				0.2	0.8	17.7	26.5					
	99+46.00				0.2	0.9	19.0	28.6					
	196+90.00				0.1	0.7	14.8	22.2					
IL 104	0+00.00	TO	5+48.00	548.00	1.0	5.2	61.8	92.6		118.1	43.7		
	5+48.00	TO	19+54.06	1406.06	3.1	16.2	210.0	309.4		328.1	224.2		
RAMP B	1+26.78	TO	15+41.00	1414.22	3.1	16.3	211.2	311.2		330.0	225.5		
	15+41.00	TO	26+74.17	1133.17	1.8	9.7	125.4	188.1		197.4	90.3		
RAMP C	0+00.00	TO	5+49.00	549.00	1.0	5.3	64.1	96.1		117.3	43.8		
	5+49.00	TO	18+55.09	1306.09	2.9	15.1	195.0	287.0		304.8	208.2		
RAMP D	1+29.00	TO	14+11.00	1282.00	2.8	14.8	191.4	281.6		299.1	204.4		
	14+11.00	TO	25+60.59	1149.59	1.8	9.4	117.7	176.5		197.3	91.6		
TR 349	0+00.00	TO	5+52.50	552.50	1.0	5.3	65.6	98.5		116.4	44.0		
	5+52.50	TO	22+75.56	1723.06	3.8	19.9	257.3	386.0		402.0	274.7		
	22+75.56	TO	24+19.67	144.11	0.5	2.6	54.9		38.2	31.2	23.0		
RAMP B	0+23.19	TO	1+57.26	134.07	0.4	2.2	45.8		32.1	33.3	21.4		
	1+57.26	TO	9+36.00	778.74	1.7	9.0	116.3	174.4		181.7	124.2		
	9+36.00	TO	21+00.00	1164.00	1.9	9.8	124.9	187.3		204.5	92.8		
RAMP C	0+00.00	TO	5+49.00	549.00	1.0	5.3	62.1	93.1		121.2	43.8		
	5+49.00	TO	21+98.93	1649.93	3.6	19.1	246.4	369.6		385.0	263.1		
	21+98.93	TO	23+34.94	136.01	0.4	2.3	47.5		33.2	28.1	21.7		
RAMP D	0+23.35	TO	1+52.00	128.65	0.3	1.8	37.5		26.6	26.3	20.5		
	1+52.00	TO	9+59.00	807.00	1.8	9.3	120.5	180.8		188.3	128.7		
	9+59.00	TO	21+14.08	1155.08	1.9	10.0	128.0	192.1		202.7	92.1		
MAINLINE	35+48.85	TO	48+25.28	1276.43	3.5	18.2	382.5		238.2	270.8	69.4		
	51+71.36	TO	64+75.00	1303.64	3.6	18.7	393.7		245.7	260.9	66.8		
ENTRANCES	LT 37+00.00				0.1	0.3	11.3						
	RT 62+45.00				0.2	0.9	38.0						
US 24	0+00.00	TO	5+48.00	548.00	1.0	5.3	63.6	95.4		117.5	43.7		
	5+48.00	TO	19+36.12	1388.12	3.2	16.8	223.7	335.5		323.9	221.3		
	19+36.12	TO	20+50.00	113.88	0.4	2.2	45.2		32.9	25.9	18.2		
RAMP D	0+24.00	TO	1+43.76	119.76	0.3	1.6	34.5		24.5	24.4	19.1		
	1+43.76	TO	14+40.00	1296.24	2.8	15.0	193.6	290.4		302.5	206.7		
	14+40.00	TO	25+80.50	1140.50	1.8	9.6	123.1	184.6		197.7	90.9		
TOTALS =				361.7	1903.9	24515.6	34616.0	774.4	37447.6	12939.5	174150.0	50643.0	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT. 1"=40'  
 HORIZ. 1"=40'  
 DATE 08/04/2004

DRAWN BY JJS  
 CHECKED BY LLQ



GUARDRAIL SCHEDULE											
LOCATION					GUARDRAIL REMOVAL	SPBGR TYPE A	TBT, T-6	TBT, T-1 (SPL) TANG.	TBT, T-2	GUARDRAIL MARKERS, T-A	TERMINAL MARKERS
STATION	TO	STATION	OFFSET	MED/OUT	FOOT	FOOT	EACH	EACH	EACH	EACH	EACH
FAI 172											
106+50.00	TO	121+00.00	78'	RT OUTSIDE	1350	1350	-	2	-	17	2
115+00.00	TO	122+75.00	78'	LT OUTSIDE	675	675	-	2	-	10	2
148+65.00	TO	151+40.00	21'	RT MEDIAN	-	212.5	-	1	1	-	1
151+25.00	TO	154+00.00	21'	LT MEDIAN	-	212.5	-	1	1	-	1
220+75.00	TO	223+50.00	21'	RT MEDIAN	-	212.5	-	1	1	-	1
223+35.00	TO	226+10.00	21'	LT MEDIAN	-	212.5	-	1	1	-	1
259+30.00	TO	262+67.5	21'	RT MEDIAN	-	275	-	1	1	-	1
261+92.50	TO	265+30.00	21'	LT MEDIAN	-	275	-	1	1	-	1
42+90.00	TO	46+15.00	21'	RT MEDIAN	-	262.5	-	1	1	-	1
45+20.00	TO	48+45.00	21'	LT MEDIAN	-	262.5	-	1	1	-	1
136+65.00	TO	139+40.00	21'	RT MEDIAN	-	212.5	-	1	1	-	1
139+25.00	TO	142+00.00	21'	LT MEDIAN	-	212.5	-	1	1	-	1
155+62.05	TO	160+33.30	38'	RT MEDIAN	200	375	1	1	-	6	1
157+62.50	TO	160+83.75	78'	RT OUTSIDE	150	225	1	1	-	4	1
161+75.96	TO	164+97.21	78'	LT OUTSIDE	150	225	1	1	-	4	1
162+26.41	TO	167+02.66	38'	LT MEDIAN	200	375	1	1	-	6	1
FAS 1604 / TR 349 (COLUMBUS ROAD)											
45+97.14	TO	47+93.39	31'	RT OUTSIDE	100	100	1	1	-	2	1
47+50.62	TO	48+46.87	31'	LT OUTSIDE	100	-	1	1	-	1	1
51+48.98	TO	52+45.23	31'	RT OUTSIDE	100	-	1	1	-	1	1
52+03.26	TO	53+99.51	31'	LT OUTSIDE	100	100	1	1	-	2	1
TOTAL =					3125	5775	8	22	10	53	22

CURB & GUTTER SCHEDULE										
LOCATION		STATION TO STATION		COMB. CURB & GUTTER REMOVAL	CONCRETE MEDIAN SURFACE REMOVAL	COMBINATION CC&G TYPE M-6.06	COMBINATION CC&G TYPE M-6.24 (VGF)	CONCRETE MEDIAN, TYPE SM-6.06	CONCRETE MEDIAN SURFACE 4"	COMB. C & G REMOVE AND REPLACE (FOOT)
				(FOOT)	(SQ FT)	(FOOT)	(FOOT)	(SQ FT)	(SQ FT)	(FOOT)
TR 349										
MAINLINE	MED	39+09.50	TO 39+25.40	32.6	41.8			76.3		
MAINLINE	WB	41+52.00	TO 41+64.00							12.0
MAINLINE	MED	43+32.10	TO 43+51.70	41.4	136.5			136.5		
MAINLINE	MED	44+05.70	TO 44+87.60	161.0	271.6			248.0		
MAINLINE	MED	48+25.00	TO 48+46.00		217.0				217.0	21.0
MAINLINE	MED	51+80.00	TO 52+04.00		240.0				240.0	8.0
MAINLINE	MED	55+10.10	TO 55+88.40	157.5	266.0			248.0		
MAINLINE	MED	56+43.50	TO 56+66.70	48.5	181.0			208.7		
MAINLINE	MED	59+78.00	TO 59+90.00							12.0
MAINLINE	MED	61+47.50	TO 61+56.70	17.7	16.0			45.6		
RAMP A	ISLAND			56.5	175.0	18.6	32.6		154.0	
RAMP C	ISLAND			73.6	278.5	25.3	43.1		257.3	
US 24										
MAINLINE	MED	2165+78.50	TO 2166+54.80	154.0	264.7			223.0		
MAINLINE	MED	2176+84.80	TO 2177+59.80	151.5	248.0			220.5		
TOTALS =				894.3	2336.1	43.9	75.7	1406.6	868.3	53.0

REVISIONS	
NAME	DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	15

STA. TO STA.  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

(1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

PATCHING SCHEDULE											
*	STATION	NOTES	LENGTH	WIDTH	AREA	PATCHING CL A, TY2, 8"	PATCHING CL A, TY3, 8"	PATCHING CL A, TY4, 8"	SAW CUTS	PATCH REINFORCEMENT	TIE BARS
			FT	FT	SQYD	SQYD	SQYD	SQYD	FT	SQYD	EACH
<i>SOUTHBOUND I-172</i>											
1	237+05	DL	6	12	8.0	8.0			60.0	8.0	
2	237+05	PL	6	12	8.0	8.0			60.0	8.0	
3	235+95	DL	6	12	8.0	8.0			60.0	8.0	
4	235+95	PL	6	12	8.0	8.0			60.0	8.0	
5	233+90	DL	35	12	46.7			46.7	118.0	46.7	17
6	226+45	PL	12	12	16.0		16.0		72.0	16.0	
7	210+89	PL	5	12	6.7	6.7			58.0	6.7	
8	202+46	PL	5	12	6.7	6.7			58.0	6.7	
9	186+77	DL	5	12	6.7	6.7			58.0	6.7	
10	179+67	PL	8	12	10.7	10.7			64.0	10.7	
11	178+29	PL	5	12	6.7	6.7			58.0	6.7	
12	177+89	PL	5	12	6.7	6.7			58.0	6.7	
13	177+60	PL	5	12	6.7	6.7			58.0	6.7	
14	172+90	PL	5	12	6.7	6.7			58.0	6.7	
15	172+00	PL	5	12	6.7	6.7			58.0	6.7	
16	170+36	PL	5	12	6.7	6.7			58.0	6.7	
17	166+35	PL	5	12	6.7	6.7			58.0	6.7	
18	155+25	PL	5	12	6.7	6.7			58.0	6.7	
19	140+85	DL	5	12	6.7	6.7			58.0	6.7	
20	139+80	PL	5	12	6.7	6.7			58.0	6.7	
21	139+53	PL	5	12	6.7	6.7			58.0	6.7	
22	135+00	PL	5	12	6.7	6.7			58.0	6.7	
23	135+00	DL	5	12	6.7	6.7			58.0	6.7	
24	128+32	PL	9	12	12.0	12.0			66.0	12.0	
25	128+12	DL	5	12	6.7	6.7			58.0	6.7	
26	127+76	PL	6	12	8.0	8.0			60.0	8.0	
27	115+35	DL	8.7	12	11.6	11.6			65.4	11.6	
28	110+20	PL	5	12	6.7	6.7			58.0	6.7	
29	109+53	DL	5	12	6.7	6.7			58.0	6.7	
30	109+36	PL	5	12	6.7	6.7			58.0	6.7	
31	94+31	DL	5	12	6.7	6.7			58.0	6.7	
32	79+32	PL	5	12	6.7	6.7			58.0	6.7	
33	76+00	PL	6	12	8.0	8.0			60.0	8.0	
34	75+10	DL	5	12	6.7	6.7			58.0	6.7	
35	75+10	PL	5	12	6.7	6.7			58.0	6.7	
36	74+89	DL	5	12	6.7	6.7			58.0	6.7	
37	274+10	DL	24	12	32.0			32.0	96.0	32.0	11
38	267+05	PL	14	12	18.7		18.7		76.0	18.7	
39	266+20	PL	9	12	12.0	12.0			66.0	12.0	
40	266+00	DL	17.8	12	23.7		23.7		83.6	23.7	
41	264+20	DL	43	12	57.3			57.3	134.0	57.3	21
42	263+50	DL	16	12	21.3		21.3		80.0	21.3	
43	262+04	DL	5	12	6.7	6.7			58.0	6.7	
44	261+50	DL	5	12	6.7	6.7			58.0	6.7	
45	261+50	PL	5	12	6.7	6.7			58.0	6.7	
46	249+57	DL	10	12	13.3	13.3			68.0	13.3	
47	246+30	PL	7.4	12	9.9	9.9			62.8	9.9	
48	243+35	DL	14	12	18.7		18.7		76.0	18.7	
49	242+12	DL	5	12	6.7	6.7			58.0	6.7	
50	241+38	DL	14	12	18.7		18.7		76.0	18.7	
51	240+70	DL	8.5	12	11.3	11.3			65.0	11.3	
52	240+10	DL	6	12	8.0	8.0			60.0	8.0	
53	239+78	DL	8	12	10.7	10.7			64.0	10.7	
54	239+44	DL	6	12	8.0	8.0			60.0	8.0	
55	238+80	DL	5	12	6.7	6.7			58.0	6.7	
56	205+17	DL	5	12	6.7	6.7			58.0	6.7	
57	198+43	DL	7	12	9.3	9.3			62.0	9.3	
58	179+17	DL	5	12	6.7	6.7			58.0	6.7	
59	163+86	DL	5	12	6.7	6.7			58.0	6.7	
60	94+57	DL	6.5	12	8.7	8.7			61.0	8.7	
61	91+82	DL	5	12	6.7	6.7			58.0	6.7	
62	71+74	DL	5	12	6.7	6.7			58.0	6.7	
63	65+90	DL	26	12	34.7			34.7	100.0	34.7	12
64	65+90	PL	14	12	18.7		18.7		76.0	18.7	
TOTALS					714.5	408.0	135.8	170.7	4141.8	714.5	61

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY JJS  
 HORIZ.              CHECKED BY LLQ  
 DATE 08/04/2004

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	16

STA. TO STA.  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

PATCHING SCHEDULE											
*	STATION	NOTES	LENGTH	WIDTH	AREA	PATCHING CL A, TY2, 8"	PATCHING CL A, TY3, 8"	PATCHING CL A, TY4, 8"	SAW CUTS	PATCH REINFORCEMENT	TIE BARS
			FT	FT	SQYD	SQYD	SQYD	SQYD	FT	SQYD	EACH
<i>NOUTHBOUND I-172</i>											
1	92+00	DL	7.7	12	10.3	10.3			63.4	10.3	
2	94+26	DL	10	12	13.3	13.3			68.0	13.3	
3	139+16	DL	5	12	6.7	6.7			58.0	6.7	
4	141+04	DL	9	12	12.0	12.0			66.0	12.0	
5	145+20	DL	5	12	6.7	6.7			58.0	6.7	
6	145+20	PL	5	12	6.7	6.7			58.0	6.7	
7	151+63	DL	5.7	12	7.6	7.6			59.4	7.6	
8	154+58	DL	6.5	12	8.7	8.7			61.0	8.7	
9	161+19	DL	5	12	6.7	6.7			58.0	6.7	
10	161+74	DL	5.9	12	7.9	7.9			59.8	7.9	
11	162+00	DL	5	12	6.7	6.7			58.0	6.7	
12	162+42	DL	5	12	6.7	6.7			58.0	6.7	
13	175+42	PL	6	12	8.0	8.0			60.0	8.0	
14	177+54	DL	5	12	6.7	6.7			58.0	6.7	
15	177+54	DL	5	12	6.7	6.7			58.0	6.7	
16	178+40	DL	6.2	12	8.3	8.3			60.4	8.3	
17	178+64	DL	5	12	6.7	6.7			58.0	6.7	
18	179+85	DL	22	12	29.3			29.3	92.0	29.3	10
19	180+80	DL	5	12	6.7	6.7			58.0	6.7	
20	180+80	PL	5	12	6.7	6.7			58.0	6.7	
21	181+17	DL	5	12	6.7	6.7			58.0	6.7	
22	181+17	PL	5	12	6.7	6.7			58.0	6.7	
23	181+37	PL	5	12	6.7	6.7			58.0	6.7	
24	182+20	DL	5	12	6.7	6.7			58.0	6.7	
25	182+20	PL	5	12	6.7	6.7			58.0	6.7	
26	182+73	DL	5	12	6.7	6.7			58.0	6.7	
27	182+73	PL	5	12	6.7	6.7			58.0	6.7	
28	182+94	DL	5	12	6.7	6.7			58.0	6.7	
29	182+94	PL	5	12	6.7	6.7			58.0	6.7	
30	196+24	PL	5	12	6.7	6.7			58.0	6.7	
31	197+26	DL	14.4	12	19.2		19.2		76.8	19.2	
32	201+21	DL	8.2	12	10.9	10.9			64.4	10.9	
33	202+08	DL	7.2	12	9.6	9.6			62.4	9.6	
34	202+90	DL	5	12	6.7	6.7			58.0	6.7	
35	205+50	DL	5	12	6.7	6.7			58.0	6.7	
36	205+50	PL	5	12	6.7	6.7			58.0	6.7	
37	208+12	DL	7.5	12	10.0	10.0			63.0	10.0	
38	209+62	DL	5	12	6.7	6.7			58.0	6.7	
39	209+91	DL	5	12	6.7	6.7			58.0	6.7	
40	209+91	PL	5	12	6.7	6.7			58.0	6.7	
41	213+92	DL	5	12	6.7	6.7			58.0	6.7	
42	217+70	DL	5	12	6.7	6.7			58.0	6.7	
43	217+95	PL	7	12	9.3	9.3			62.0	9.3	
44	221+09	DL	5	12	6.7	6.7			58.0	6.7	
45	222+24	DL	5	12	6.7	6.7			58.0	6.7	
46	224+33	DL	5	12	6.7	6.7			58.0	6.7	
47	224+23	PL	5	12	6.7	6.7			58.0	6.7	
48	226+67	DL	5	12	6.7	6.7			58.0	6.7	
49	228+22	DL	5	12	6.7	6.7			58.0	6.7	
50	230+88	DL	5	12	6.7	6.7			58.0	6.7	
51	232+80	PL	5	12	6.7	6.7			58.0	6.7	
52	235+34	DL	5	12	6.7	6.7			58.0	6.7	
53	243+38	DL	6.9	12	9.2	9.2			61.8	9.2	
54	243+66	DL	8.5	12	11.3	11.3			65.0	11.3	
55	244+06	DL	6	12	8.0	8.0			60.0	8.0	
56	244+46	DL	35	12	46.7			46.7	118.0	46.7	17
57	244+96	DL	6.5	12	8.7	8.7			61.0	8.7	
58	251+10	DL	8.5	12	11.3	11.3			65.0	11.3	
59	256+76	DL	18.7	12	24.9		24.9		85.4	24.9	
60	256+90	PL	5	12	6.7	6.7			58.0	6.7	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY JJS  
 HORIZ.              CHECKED BY LLO  
 DATE 08/04/2004



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	17

STA. TO STA.  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

PATCHING SCHEDULE											
#	STATION	NOTES	LENGTH	WIDTH	AREA	PATCHING CL A, TY2, 8"	PATCHING CL A, TY3, 8"	PATCHING CL A, TY4, 8"	SAW CUTS	PATCH REINFORCEMENT	TIE BARS
			FT	FT	SQYD	SQYD	SQYD	SQYD	FT	SQYD	EACH
<i>NOUTHBOUND I-172</i>											
61	258+39	DL	38.4	12	51.2			51.2	124.8	51.2	18
62	259+10	DL	6.8	12	9.1	9.1			61.6	9.1	
63	260+15	DL	5	12	6.7	6.7			58.0	6.7	
64	261+93	DL	29.4	12	39.2			39.2	106.8	39.2	14
65	262+24	DL	5	12	6.7	6.7			58.0	6.7	
66	262+50	DL	9	12	12.0	12.0			66.0	12.0	
67	263+00	PL	12	12	16.0		16.0		72.0	16.0	
68	263+52	DL	14.2	12	18.9		18.9		76.4	18.9	
69	265+00	DL	6.9	12	9.2	9.2			61.8	9.2	
70	265+00	PL	6.9	12	9.2	9.2			61.8	9.2	
71		PL	21.9	12	29.2			29.2	91.8	29.2	10
72	276+86	DL	8	12	10.7	10.7			64.0	10.7	
73	278+36	DL	28.5	12	38.0			38.0	105.0	38.0	13
74	279+16	DL	5	12	6.7	6.7			58.0	6.7	
75	279+54	DL	6.8	12	9.1	9.1			61.6	9.1	
76	280+00	DL	25.9	12	34.5			34.5	99.8	34.5	12
77	280+84	DL	5	12	6.7	6.7			58.0	6.7	
78	281+61	DL	6.6	12	8.8	8.8			61.2	8.8	
79	281+73	DL	6.4	12	8.5	8.5			60.8	8.5	
80	282+17	DL	6	12	8.0	8.0			60.0	8.0	
81	284+85	DL	5	12	6.7	6.7			58.0	6.7	
82	285+31	DL	5	12	6.7	6.7			58.0	6.7	
83	285+78	DL	15.1	12	20.1		20.1		78.2	20.1	
84	286+04	DL	5	12	6.7	6.7			58.0	6.7	
85	288+78	PL	9.9	12	13.2	13.2			67.8	13.2	
86	2+29	PL	9.5	12	12.7	12.7			67.0	12.7	
87	2+95	PL	15	12	20.0		20.0		78.0	20.0	
88	3+57	PL	16.3	12	21.7		21.7		80.6	21.7	
89	4+28	PL	14.9	12	19.9		19.9		77.8	19.9	
90	4+62	PL	5	12	6.7	6.7			58.0	6.7	
91	7+84	DL	10.2	12	13.6	13.6			68.4	13.6	
92	7+97	DL	5	12	6.7	6.7			58.0	6.7	
93	12+64	PL	5	12	6.7	6.7			58.0	6.7	
94	13+17	PL	5	12	6.7	6.7			58.0	6.7	
95	18+54	DL	9.2	12	12.3	12.3			66.4	12.3	
96	22+42	DL	6.9	12	9.2	9.2			61.8	9.2	
97	22+42	PL	5	12	6.7	6.7			58.0	6.7	
98	23+57	PL	9	12	12.0	12.0			66.0	12.0	
99	24+90	PL	19.5	12	26.0			26.0	87.0	26.0	
100	28+31	DL	20	12	26.7			26.7	88.0	26.7	
101	30+20	PL	38.9	12	51.9			51.9	125.8	51.9	18
102	34+85	DL	6.2	12	8.3	8.3			60.4	8.3	
103	35+20	DL	5	12	6.7	6.7			58.0	6.7	
104	43+90	PL	23.5	12	31.3			31.3	95.0	31.3	11
105	44+27	PL	5	12	6.7	6.7			58.0	6.7	
106	44+78	PL	5	12	6.7	6.7			58.0	6.7	
107	46+19	PL	21.7	12	28.9			28.9	91.4	28.9	10
108	46+60	PL	48.8	12	65.1			65.1	145.6	65.1	23
109	47+02	DL	26	12	34.7			34.7	100.0	34.7	12
110	47+62	DL	13.3	12	17.7		17.7		74.6	17.7	
111	53+16	PL	22.5	12	30.0			30.0	93.0	30.0	10
112	56+96	PL	5	12	6.7	6.7			58.0	6.7	
113	57+34	PL	51.6	12	68.8			68.8	151.2	68.8	25
114	62+66	PL	5	12	6.7	6.7			58.0	6.7	
115	63+11	PL	10.6	12	14.1	14.1			69.2	14.1	
116	64+00	PL	5	12	6.7	6.7			58.0	6.7	
117	64+00	DL	5	12	6.7	6.7			58.0	6.7	
118	64+18	PL	5	12	6.7	6.7			58.0	6.7	
119	65+46	PL	23.6	12	31.5			31.5	95.2	31.5	11
120	65+77	DL	7	12	9.3	9.3			62.0	9.3	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY JJS  
 HORIZ.              CHECKED BY LLQ  
 DATE 08/04/2004

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	18

STA. TO STA.  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

PATCHING SCHEDULE											
*	STATION	NOTES	LENGTH	WIDTH	AREA	PATCHING CL A, TY2, 8"	PATCHING CL A, TY3, 8"	PATCHING CL A, TY4, 8"	SAW CUTS	PATCH REINFORCEMENT	TIE BARS
			FT	FT	SQYD	SQYD	SQYD	SQYD	FT	SQYD	EACH
<i>NOUTHBOUND I-172</i>											
121	65+96	PL	11.4	12	15.2		15.2		70.8	15.2	
122	65+96	DL	5	12	6.7	6.7			58.0	6.7	
123	66+78	DL	6.2	12	8.3	8.3			60.4	8.3	
124	67+45	DL	5.5	12	7.3	7.3			59.0	7.3	
125	67+94	DL	5	12	6.7	6.7			58.0	6.7	
126	68+26	DL	26.5	12	35.3			35.3	101.0	35.3	12
127	68+67	DL	5	12	6.7	6.7			58.0	6.7	
128	69+72	DL	6.9	12	9.2	9.2			61.8	9.2	
129	70+12	DL	17.4	12	23.2		23.2		82.8	23.2	
130	71+40	DL	5	12	6.7	6.7			58.0	6.7	
131	72+75	DL	5	12	6.7	6.7			58.0	6.7	
132	73+47	DL	5	12	6.7	6.7			58.0	6.7	
133	73+47	PL	5	12	6.7	6.7			58.0	6.7	
134	74+57	DL	5	12	6.7	6.7			58.0	6.7	
135	80+62	PL	5	12	6.7	6.7			58.0	6.7	
136	80+99	DL	5	12	6.7	6.7			58.0	6.7	
137	80+99	PL	5	12	6.7	6.7			58.0	6.7	
138	81+58	PL	131.5	12	175.3			175.3	311.0	175.3	65
139	85+58	PL	5	12	6.7	6.7			58.0	6.7	
140	85+98	PL	5	12	6.7	6.7			58.0	6.7	
141	86+19	PL	7.3	12	9.7	9.7			62.6	9.7	
142	88+00	PL	5	12	6.7	6.7			58.0	6.7	
143	90+46	PL	5	12	6.7	6.7			58.0	6.7	
144	91+78	PL	16.3	12	21.7		21.7		80.6	21.7	
145	92+10	PL	31.1	12	41.5			41.5	110.2	41.5	15
146	96+84	DL	5	12	6.7	6.7			58.0	6.7	
147	97+10	PL	8	12	10.7	10.7			64.0	10.7	
148	98+69	PL	5	12	6.7	6.7			58.0	6.7	
149	99+16	PL	9	12	12.0	12.0			66.0	12.0	
150	99+38	PL	5	12	6.7	6.7			58.0	6.7	
151	99+59	PL	6.5	12	8.7	8.7			61.0	8.7	
152	99+88	PL	5	12	6.7	6.7			58.0	6.7	
153	100+10	PL	5	12	6.7	6.7			58.0	6.7	
154	100+70	DL	5	12	6.7	6.7			58.0	6.7	
155	110+71	PL	5	12	6.7	6.7			58.0	6.7	
156	111+67	PL	8.8	12	11.7	11.7			65.6	11.7	
157	112+50	PL	19.8	12	26.4			26.4	87.6	26.4	
158	113+15	PL	9.4	12	12.5	12.5			66.8	12.5	
159	117+06	PL	8.5	12	11.3	11.3			65.0	11.3	
160	119+03	PL	7.7	12	10.3	10.3			63.4	10.3	
161	120+50	PL	14.2	12	18.9		18.9		76.4	18.9	
162	124+40	PL	33	12	44.0			44.0	114.0	44.0	16
163	125+19	PL	7.4	12	9.9	9.9			62.8	9.9	
164	133+66	PL	6.4	12	8.5	8.5			60.8	8.5	
165	134+26	PL	5	12	6.7	6.7			58.0	6.7	
166	134+49	PL	5	12	6.7	6.7			58.0	6.7	
167	134+87	PL	7	12	9.3	9.3			62.0	9.3	
168	135+24	PL	24	12	32.0			32.0	96.0	32.0	11
169	137+11	DL	5	12	6.7	6.7			58.0	6.7	
170	137+11	PL	5	12	6.7	6.7			58.0	6.7	
171	138+56	PL	5	12	6.7	6.7			58.0	6.7	
172	138+94	PL	5	12	6.7	6.7			58.0	6.7	
173	139+28	DL	6.4	12	8.5	8.5			60.8	8.5	
174	139+28	PL	6.4	12	8.5	8.5			60.8	8.5	
175	139+62	PL	21	12	28.0			28.0	90.0	28.0	10
176	140+07	PL	5	12	6.7	6.7			58.0	6.7	
177	146+35	PL	13.1	12	17.5		17.5		74.2	17.5	
178	147+28	PL	5	12	6.7	6.7			58.0	6.7	
179	147+50	PL	6	12	8.0	8.0			60.0	8.0	
180	148+86	DL	5	12	6.7	6.7			58.0	6.7	

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY JJS  
 HORIZ.              CHECKED BY LLO  
 DATE 08/04/2004

PATCHING SCHEDULE											
*	STATION	NOTES	LENGTH	WIDTH	AREA	PATCHING CL A, TY2, 8"	PATCHING CL A, TY3, 8"	PATCHING CL A, TY4, 8"	SAW CUTS	PATCH REINFORCEMENT	TIE BARS
			FT	FT	SQYD	SQYD	SQYD	SQYD	FT	SQYD	EACH
<i>NOUTHBOUND I-172</i>											
181	149+00	PL	6	12	8.0	8.0			60.0	8.0	
182	153+55	PL	6.8	12	9.1	9.1			61.6	9.1	
183	154+00	PL	5	12	6.7	6.7			58.0	6.7	
184	156+70	DL	7	12	9.3	9.3			62.0	9.3	
185	156+70	PL	5	12	6.7	6.7			58.0	6.7	
186	157+00	PL	5	12	6.7	6.7			58.0	6.7	
187	157+27	PL	21	12	28.0			28.0	90.0	28.0	10
188	166+18	PL	7.9	12	10.5	10.5			63.8	10.5	
189	168+30	PL	5	12	6.7	6.7			58.0	6.7	
190	168+57	PL	5	12	6.7	6.7			58.0	6.7	
191	169+54	PL	24.2	12	32.3			32.3	96.4	32.3	11
192	173+25	PL	6.2	12	8.3	8.3			60.4	8.3	
193	173+69	PL	7.7	12	10.3	10.3			63.4	10.3	
194	174+80	PL	18.7	12	24.9		24.9		85.4	24.9	
195	179+49	PL	5	12	6.7	6.7			58.0	6.7	
196	179+75	PL	9.9	12	13.2	13.2			67.8	13.2	
197	183+61	PL	5	12	6.7	6.7			58.0	6.7	
198	184+00	PL	5	12	6.7	6.7			58.0	6.7	
199	184+96	PL	16.6	12	22.1		22.1		81.2	22.1	
200	185+53	PL	7.5	12	10.0	10.0			63.0	10.0	
201	188+35	PL	12	12	16.0		16.0		72.0	16.0	
202	189+44	PL	33	12	44.0			44.0	114.0	44.0	16
203	190+58	PL	15	12	20.0		20.0		78.0	20.0	
204	190+99	PL	9.8	12	13.1	13.1			67.6	13.1	
205	194+34	PL	5	12	6.7	6.7			58.0	6.7	
206	195+11	PL	5	12	6.7	6.7			58.0	6.7	
207	195+90	PL	5	12	6.7	6.7			58.0	6.7	
208	197+85	PL	15	12	20.0		20.0		78.0	20.0	
209	198+60	PL	40	12	53.3		53.3		128.0	53.3	19
210	215+15	DL	5	12	6.7	6.7			58.0	6.7	
211	217+35	PL	10.5	12	14.0	14.0			69.0	14.0	
212	224+84	PL	20	12	26.7			26.7	88.0	26.7	
213	226+94	PL	80	12	106.7			106.7	208.0	106.7	39
214	229+09	PL	52	12	69.3			69.3	152.0	69.3	25
215	237+50	PL	12	12	16.0		16.0		72.0	16.0	
216	243+19	PL	20	12	26.7			26.7	88.0	26.7	
TOTALS					3,133.7	1,307.3	393.9	1,432.5	15,063.6	3,133.7	463

CLASS A PATCHING SUMMARY						
LOCATION	PATCHING CL A, TY2, 8"	PATCHING CL A, TY3, 8"	PATCHING CL A, TY4, 8"	SAW CUTS	PATCH REINFORCEMENT	TIE BARS
	SQYD	SQYD	SQYD	FT	SQYD	EACH
SB I-172	408.0	135.8	170.7	4141.8	714.5	61
NB I-172	1307.3	393.9	1432.5	15063.6	3133.7	463
RAMPS	0.0	0.0	3008.5	4165.8	3008.5	831
TOTAL =	1715.3	529.7	4611.7	23371.2	6856.7	1355

PATCHING SCHEDULE											
*	STATION	NOTES	LENGTH	AVE WIDTH	AREA	PATCHING CL A, TY2, 8"	PATCHING CL A, TY3, 8"	PATCHING CL A, TY4, 8"	SAW CUTS	PATCH REINFORCEMENT	TIE BARS
			FT	FT	SQYD	SQYD	SQYD	SQYD	FT	SQYD	EACH
<i>RAMPS</i>											
<i>Columbus Rd.</i>											
1	1+84	Ramp A	368	11.2	458.0			458.0	780.8	458.0	183
2	4+72	Ramp A	117	26.1	339.3			339.3	338.4	339.3	58
3	14+21	Ramp B	84	14.7	137.2			137.2	226.8	137.2	41
4	4+19	Ramp C	44	23.2	113.4			113.4	180.8	113.4	21
5	4+77	Ramp C	18	26.3	52.6			52.6	141.2	52.6	0
6	13+26	Ramp D	280	16.7	519.6			519.6	626.8	519.6	139
7	15+72	Ramp D	59	11.8	77.4			77.4	165.2	77.4	29
8	17+90	Ramp D	59	7.45	48.8			48.8	147.8	48.8	29
					0.0						0
					0.0						0
<i>US 24</i>											
9	2+60	Ramp A	104	14.9	172.2			172.2	267.6	172.2	51
10	4+70	Ramp A	120	26	346.7			346.7	344.0	346.7	59
11	18+87	Ramp D	443	15.1	743.3			743.3	946.4	743.3	221
TOTALS					3008.5	0.0	0.0	3008.5	4165.8	3008.5	831

REVISIONS	
NAME	DATE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	20

STA. TO STA.  
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

PAVEMENT PATCHING CLASS B													
PATCH NUMBER	LOCATION	LENGTH	WIDTH	AREA	CLASS B PATCH TY II - 8"	CLASS B PATCH TY III - 8"	CLASS B PATCH TY IV - 8"	CLASS B PATCH - EXPANSION JOINT	DEFORMED BARS EXPANSION JOINT	DOWEL BARS 1 1/2"	SAW CUTS	PAVEMENT FABRIC	
STATION	DESCRIPTION	(FT)	(FT)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(FT)	(EACH)	(EACH)	(FT)	(SQ YD)	
IL 104 (BROADWAY STREET) INTERCHANGE													
RAMP A													
B1	6+67		6.0	16.3	10.9	10.9		16.3	14	14	60.9		
B2	11+19		6.0	16.3	10.9	10.9				28	60.9		
B3	11+19		6.0	16.3	10.9	10.9				28	60.9		
RAMP B													
B4	2+27		6.0	16.1	10.7	10.7				28	60.3		
B5	3+67		6.0	16.1	10.7	10.7				28	60.3		
B6	8+22		6.0	16.1	10.7	10.7				28	60.3		
B7	9+68		6.0	16.1	10.7	10.7				28	60.3		
B8	10+24		6.0	16.1	10.7	10.7				28	60.3		
B9	13+19		6.0	16.1	10.7	10.7				28	60.3		
B10	13+71		6.0	16.1	10.7	10.7				28	60.3		
B11	16+17		6.0	16.1	10.7	10.7				28	60.3		
RAMP C													
B12	11+18		6.0	16.2	10.8	10.8				28	60.6		
B13	12+17		6.0	16.2	10.8	10.8				28	60.6		
B14	13+22		6.0	16.2	10.8	10.8				28	60.6		
RAMP D													
B15	6+67		6.0	16.2	10.8	10.8				28	60.6		
B16	10+32		6.0	16.2	10.8	10.8				28	60.6		
B17	13+20		6.0	16.2	10.8	10.8				28	60.6		
TR 349 (COLUMBUS ROAD) INTERCHANGE													
RAMP A													
B18	7+42		6.0	16.3	10.9	10.9				28	60.9		
B19	13+75		6.0	16.3	10.9	10.9				28	60.9		
B20	14+00		6.0	16.3	10.9	10.9				28	60.9		
B21	15+00		6.0	16.3	10.9	10.9				28	60.9		
B22	21+77		6.0	16.3	10.9	10.9				28	60.9		
B23	22+45		6.0	16.3	10.9	10.9				28	60.9		
B24	23+25		16.0	18.0	32.0			32.0		30	86.0	32.0	
RAMP B													
B25	0+85	LT	51.0	16.2	91.8			91.8		28	150.6	91.8	
B26	0+85	RT	51.0	8.0	45.3			45.3		26	126.0	45.3	
B27	1+58		6.0	16.3	10.9	10.9				2	60.9		
B28	9+17		6.0	16.0	10.7	10.7				28	60.0		
B29	10+16		6.0	14.5	9.7	9.7				24	55.5		
B30	11+14		6.0	14.0	9.3	9.3				24	54.0		
RAMP C													
B31	6+83		6.0	16.0	10.7	10.7				28	60.0		
B32	10+75		6.0	16.0	10.7	10.7				28	60.0		
B33	11+50		6.0	16.0	10.7	10.7				28	60.0		
B34	13+00		6.0	16.0	10.7	10.7				28	60.0		
B35	15+00		6.0	16.0	10.7	10.7				28	60.0		
B36	22+94	WEST OF ISLAND	6.0	11.0	7.3	7.3				22	45.0		
B37	22+98	EAST OF ISLAND	6.0	15.0	10.0	10.0				28	57.0		
B38	23+10	EAST OF ISLAND	37.0	6.0	24.7		24.7			12	92.0	24.7	
B39	23+11	WEST OF ISLAND	54.0	10.0	60.0			60.0		20	138.0	60.0	
B40	23+16	WEST OF ISLAND	6.0	11.0	7.3	7.3				22	45.0		
B41	23+24	EAST OF ISLAND	11.0	16.0	19.6		19.6			28	70.0	19.6	
B42	23+24	STA 43+12 COLUMBUS RD	6.0	10.0	6.7	6.7				20	42.0		
RAMP D													
B43	0+44	LT	16.0	19.0	33.8			33.8		38	89.0	33.8	
B44	0+44	RT	13.0	13.5	19.5		19.5			24	66.5	19.5	
B45	0+63	LT	6.5	15.5	11.2	11.2				28	59.5		
B46	0+63	RT	6.5	14.0	10.1	10.1				24	55.0		
B47	1+09		6.0	18.0	12.0	12.0				32	66.0		
B48	2+54		6.0	16.0	10.7	10.7				28	60.0		
B49	3+80		6.0	16.0	10.7	10.7				28	60.0		
B50	4+61		6.0	16.0	10.7	10.7				28	60.0		
B51	9+23		6.0	16.0	10.7	10.7				28	60.0		
B52	9+45		6.0	16.0	10.7	10.7				28	60.0		
B53	10+51		6.0	14.0	9.3	9.3				24	54.0		

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 08/10/04

DRAWN BY JJS  
CHECKED BY LLQ

PAVEMENT PATCHING CLASS B												
PATCH NUMBER	LOCATION	LENGTH	WIDTH	AREA	CLASS B PATCH TY II - 8"	CLASS B PATCH TY III - 8"	CLASS B PATCH TY IV - 8"	CLASS B PATCH - EXPANSION JOINT	DEFORMED BARS EXPANSION JOINT	DOWEL BARS 1 1/2"	SAW CUTS	PAVEMENT FABRIC
	STATION	DESCRIPTION	(FT)	(FT)	(SQ YD)	(SQ YD)	(SQ YD)	(FT)	(EACH)	(EACH)	(FT)	(SQ YD)
TR 349 (COLUMBUS ROAD)												
B54	35+51	EB	6.0	12.0	8.0	8.0				20	48.0	
B55	36+47	EB	6.0	12.0	8.0	8.0				20	48.0	
B56	37+03	WB	6.0	14.0	9.3	9.3				24	54.0	
B57	38+02	WB	6.0	17.3	11.5	11.5				28	63.9	
B58	38+17	EB	6.0	13.0	8.7	8.7				20	51.0	
B59	38+39	WB	6.0	13.0	8.7	8.7				20	51.0	
B60	38+65	WB	6.0	13.0	8.7	8.7				20	51.0	
B61	38+84	WB	6.0	13.0	8.7	8.7				20	51.0	
B62	39+22	WB	15.0	13.0	21.7		21.7			20	69.0	21.7
B63	39+46	WB	6.0	14.0	9.3	9.3				24	54.0	
B64	39+63	WB	6.0	14.0	9.3	9.3				24	54.0	
B65	39+70	EB	6.0	15.0	10.0	10.0				24	57.0	
B66	39+76	WB	6.0	14.0	9.3	9.3				24	54.0	
B67	40+00	WB	6.0	14.0	9.3	9.3				24	54.0	
B68	40+50	EB	6.0	15.0	10.0	10.0				24	57.0	
B69	40+57	WB	25.0	14.0	38.9			38.9		24	92.0	38.9
B70	40+89	EB	6.0	15.0	10.0	10.0				24	57.0	
B71	40+95	WB	6.0	13.0	8.7	8.7				24	51.0	
B72	41+05	EB	6.0	15.0	10.0	10.0				24	57.0	
B73	41+11	WB	6.0	13.0	8.7	8.7				24	51.0	
B74	41+36	WB	18.0	13.0	26.0			26.0		24	75.0	26.0
B75	41+52	WB	6.0	13.0	8.7	8.7				24	51.0	
B76	41+62	EB	6.0	14.0	9.3	9.3				24	54.0	
B77	41+97	EB	6.0	14.0	9.3	9.3				24	54.0	
B78	42+00	WB	6.0	14.0	9.3	9.3				24	54.0	
B79	42+33	WB RadIus	20.0	6.0	13.3	13.3				10	58.0	
B80	42+39	EB	12.0	15.0	20.0		20.0			24	69.0	20.0
B81	42+55	WB	7.0	14.0	10.9	10.9				24	56.0	
B82	42+60	EB	9.0	17.5	17.5		17.5			28	70.5	17.5
B83	42+80	EB	6.0	9.0	6.0	6.0				18	39.0	
B84	43+20	WB	6.0	14.0	9.3	9.3				24	54.0	
B85	43+35	WB	6.0	14.0	9.3	9.3				24	54.0	
B86	43+47	EB	34.0	14.5	54.8			54.8		24	111.5	54.8
B87	43+53	WB	6.0	14.0	9.3	9.3				24	54.0	
B88	43+69	WB	6.0	14.0	9.3	9.3				24	54.0	
B89	44+05	EB	6.0	14.0	9.3	9.3				24	54.0	
B90	44+09	WB	6.0	14.0	9.3	9.3				24	54.0	
B91	44+09	WB TurnIane	6.0	12.0	8.0	8.0				20	48.0	
B92	44+42	WB	6.0	14.0	9.3	9.3				24	54.0	
B93	44+42	WB TurnIane	6.0	12.0	8.0	8.0				20	48.0	
B94	44+46	EB	6.0	14.0	9.3	9.3				24	54.0	
B95	44+57	EB	6.0	14.0	9.3	9.3				24	54.0	
B96	44+75	EB	6.0	14.0	9.3	9.3				24	54.0	
B97	44+89	WB	6.0	14.0	9.3	9.3				24	54.0	
B98	44+89	WB TurnIane	6.0	12.0	8.0	8.0				20	48.0	
B99	44+89	EB	6.0	14.0	9.3	9.3				24	54.0	
B100	45+04	EB	6.0	14.0	9.3	9.3				24	54.0	
B101	45+50	WB	6.0	14.0	9.3	9.3				24	54.0	
B102	45+50	WB TurnIane	6.0	12.0	8.0	8.0				20	48.0	
B103	45+50	EB	6.0	14.0	9.3	9.3				24	54.0	
B104	46+01	EB	6.0	14.0	9.3	9.3				24	54.0	
B105	46+11	WB	6.0	14.0	9.3	9.3				24	54.0	
B106	46+11	WB TurnIane	6.0	12.0	8.0	8.0				20	48.0	
B107	46+23	WB	6.0	14.0	9.3	9.3				24	54.0	
B108	46+50	EB	6.0	14.0	9.3	9.3				24	54.0	
B109	47+11	EB	6.0	14.0	9.3	9.3				24	54.0	
B110	47+25	WB	17.0	14.0	26.4			26.4		24	76.0	26.4
B111	52+57	EB	6.0	18.0	12.0	12.0				32	66.0	
B112	52+73	EB	6.0	19.5	13.0	13.0				34	70.5	
B113	53+05	EB TurnIane	6.0	8.5	5.7	5.7				16	37.5	
B114	53+05	EB	6.0	13.5	9.0	9.0				24	52.5	
B115	53+15	WB	6.0	14.0	9.3	9.3				24	54.0	
B116	53+24	EB TurnIane	6.0	10.0	6.7	6.7				16	42.0	
B117	53+24	EB	6.0	14.0	9.3	9.3				24	54.0	
B118	53+28	WB	6.0	14.0	9.3	9.3				24	54.0	

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**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY JJS  
 HORIZ.              CHECKED BY LLQ  
 DATE 08/10/04

PAVEMENT PATCHING CLASS B													
PATCH NUMBER	LOCATION		LENGTH	WIDTH	AREA	CLASS B PATCH TY II - 8"	CLASS B PATCH TY III - 8"	CLASS B PATCH TY IV - 8"	CLASS B PATCH - EXPANSION JOINT	DEFORMED BARS - EXPANSION JOINT	DOWEL BARS 1 1/2"	SAW CUTS	PAVEMENT FABRIC
	STATION	DESCRIPTION	(FT)	(FT)	(SQ YD)	(SQ YD)	(SQ YD)	(SQ YD)	(FT)	(EACH)	(EACH)	(FT)	(SQ YD)
B119	53+46	EB Turnlane	6.0	11.0	7.3	7.3					20	45.0	
B120	53+46	EB	6.0	14.0	9.3	9.3					24	54.0	
B121	53+68	WB	13.0	14.0	20.2		20.2				24	68.0	20.2
B122	53+81	EB Turnlane	28.0	12.0	37.3			37.3			20	92.0	37.3
B123	53+81	EB	28.0	14.0	43.6			43.6			24	98.0	43.6
B124	54+19	EB Turnlane	6.0	12.0	8.0	8.0					20	48.0	
B125	54+19	EB	6.0	14.0	9.3	9.3					24	54.0	
B126	54+95	EB Turnlane	6.0	12.0	8.0	8.0					20	48.0	
B127	54+95	EB	6.0	14.0	9.3	9.3					24	54.0	
B128	55+10	WB	6.0	14.0	9.3	9.3					24	54.0	
B129	55+14	EB Turnlane	9.0	12.0	12.0	12.0					20	54.0	
B130	55+14	EB	9.0	14.0	14.0	14.0					24	60.0	
B131	55+36	WB	6.0	14.0	9.3	9.3					24	54.0	
B132	55+41	EB Turnlane	18.0	12.0	24.0		24.0				20	72.0	24.0
B133	55+41	EB	18.0	14.0	28.0			28.0			24	78.0	28.0
B134	55+63	EB Turnlane	6.0	12.0	8.0	8.0					20	48.0	
B135	55+65	WB	6.0	14.0	9.3	9.3					24	54.0	
B136	56+00	EB Turnlane/Int.	45.0	9.0	45.0			45.0			18	117.0	45.0
B137	56+00	EB Turnlane	45.0	9.0	45.0			45.0			18	117.0	45.0
B138	56+00	EB	45.0	14.0	70.0			70.0			24	132.0	70.0
B139	56+50	WB	6.0	14.0	9.3	9.3					24	54.0	
B140	56+57	EB	6.0	18.0	12.0	12.0					32	66.0	
B141	56+69	EB	6.0	15.0	10.0	10.0					24	57.0	
B142	57+05	WB	6.0	14.0	9.3	9.3					24	54.0	
B143	57+17	EB	6.0	14.0	9.3	9.3					24	54.0	
B144	57+60	EB	49.0	14.0	76.2			76.2			24	140.0	76.2
B145	57+60	EB Radlus	30.0	2.5	8.3	8.3					6	67.5	
B146	57+70	WB	6.0	14.0	9.3	9.3					24	54.0	
B147	57+90	WB	6.0	14.0	9.3	9.3					24	54.0	
B148	58+05	WB	6.0	14.0	9.3	9.3					24	54.0	
B149	58+32	EB	13.0	14.0	20.2		20.2				24	68.0	20.2
B150	58+47	WB	6.0	14.0	9.3	9.3					24	54.0	
B151	58+93	EB	6.0	14.0	9.3	9.3					24	54.0	
B152	59+05	EB	12.0	14.0	18.7		18.7				24	66.0	18.7
B153	60+80	WB	6.0	15.0	10.0	10.0					26	57.0	
B154	60+92	EB	14.5	14.0	22.6		22.6				24	71.0	22.6
B155	61+50	EB	6.0	14.0	9.3	9.3					24	54.0	
US 24 INTERCHANGE													
RAMP A													
B156	6+85		6.0	16.0	10.7	10.7					28	60.0	
B157	10+00		6.0	16.0	10.7	10.7					28	60.0	
B158	13+01		6.0	16.0	10.7	10.7					28	60.0	
B159	15+66		6.0	16.0	10.7	10.7					28	60.0	
B160	17+15		7.0	16.0	12.4	12.4					28	62.0	
B161	17+91	RT	152.0	7.0	118.2			118.2			14	325.0	118.2
B162	18+89	RT	43.0	12.0	57.3			57.3			20	122.0	57.3
B163	19+00	LT	6.0	16.0	10.7	10.7					28	60.0	
RAMP D													
B164	1+04		6.0	16.0	10.7	10.7					28	60.0	
B165	2+41		6.0	16.0	10.7	10.7					28	60.0	
B166	9+53		6.0	16.0	10.7	10.7					28	60.0	
B167	10+50		6.0	16.0	10.7	10.7					28	60.0	
B168	11+64		6.0	16.0	10.7	10.7					28	60.0	
B169	14+48		6.0	16.0	10.7	10.7					28	60.0	
B170	15+50		6.0	14.0	9.3	9.3					24	54.0	
CLASS B TOTAL:					2,538.1	1,379.8	228.7	929.6	16.3	14	4130	10,812.0	1,158.3

REVISIONS	
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
407	*	ADAMS	108	23

STA. TO STA.  
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

(1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

DRAINAGE STRUCTURE REPAIR SCHEDULE								
STATION	LOCATION	MEDIAN INLET (604101) TO BE RECONSTRUCTED	CONCRETE APRON FOR MEDIAN INLET	INLETS TO BE RECONSTRUCTED	FRAMES & GRATES TO BE ADJUSTED	BREAKING PAVED DITCH	INLET TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	INLET & PIPE PROTECTION
		( EACH )	( EACH )	( EACH )	( EACH )	( FOOT )	( EACH )	( EACH )
75+81	MAINLINE MEDIAN	1						
85+00	MAINLINE MEDIAN		1					
107+20	NB OUTSIDE		1					1
110+95	NB OUTSIDE		1					1
113+00	NB OUTSIDE		1					1
117+50	MAINLINE MEDIAN							1
117+64 to 123+00	NB OUTSIDE					536		
142+00	MAINLINE MEDIAN	1						
155+64	MAINLINE MEDIAN	1						
182+14	NB OUTSIDE		1					1
195+00	MAINLINE MEDIAN	1						1
196+12	NB OUTSIDE		1					1
230+42	MAINLINE MEDIAN		1					
243+50	MAINLINE MEDIAN		1					
250+00	SB OUTSIDE				1			
278+00	MAINLINE MEDIAN			1				
15+90	MAINLINE MEDIAN	1						
15+90	NB OUTSIDE	1						1
31+60	MAINLINE MEDIAN			1				
42+00	MAINLINE MEDIAN		1					
57+10	SB OUTSIDE				1			
60+00	MAINLINE MEDIAN			1				
60+50	NB OUTSIDE				1			
83+50	MAINLINE MEDIAN	1						
113+50	MAINLINE MEDIAN		1					
151+00	MAINLINE MEDIAN	1						
159+00	SB APPROACH				2			
162+00	SB APPROACH				2			
163+00	NB APPROACH				2			
171+00	MAINLINE MEDIAN		1					
180+00	MAINLINE MEDIAN	1						1
190+10	MAINLINE MEDIAN			1				
196+75	MAINLINE MEDIAN			1				
209+50	MAINLINE MEDIAN			1				
226+00	MAINLINE MEDIAN		1					
228+80	MAINLINE MEDIAN	1						
231+50	NB OUTSIDE				1			
236+10	SB OUTSIDE				1			
237+00	MAINLINE MEDIAN		1					
246+20	NB OUTSIDE				1			
8+50	RAMP A TR 349	1						
39+17 TR 349	MEDIAN						1	
51+55 TR 349	RT OUTSIDE				1			
52+09 TR 349	LT OUTSIDE				1			
61+47 TR 349	MEDIAN				1			
TOTAL =		11	13	6	15	536	1	9

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY  
 HORIZ.              CHECKED BY  
 DATE

CULVERT SCHEDULE											
STATION	LOCATION	PRC FLARED END SECTION 12 "	PRC FLARED END SECTION 24 "	PRC FLARED END SECTION 30 "	PRC FLARED END SECTION 36 "	PRC FLARED END SECTION 42 "	REMOVE & REINSTALL EXISTING PRC FLARED END SECTIONS	FLARED END SECTION REMOVAL	CULVERT TO BE CLEANED	BOX CULVERT TO BE CLEANED	REMOVE EXISTING CULVERTS
		(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)
91+50	SB OUTSIDE										1
92+00	SB OUTSIDE										1
93+00	SB OUTSIDE										1
98+00	SB OUTSIDE		1					1			
113+00	NB OUTSIDE				1			1	1		
119+50	SB OUTSIDE										1
142+00	SB OUTSIDE										1
167+00	MAINLINE MEDIAN							1			
195+00	SB OUTSIDE							1			
202+00	SB OUTSIDE					1		1			
206+50	SB OUTSIDE										1
206+95	SB OUTSIDE										1
209+00	SB OUTSIDE										1
217+50	SB OUTSIDE							1			3
219+00	NB OUTSIDE										1
219+75	NB OUTSIDE										1
221+00	SB OUTSIDE										3
226+00	SB OUTSIDE										1
227+00	SB OUTSIDE										1
7+80								1			
28+00								1			
30+75	SB OUTSIDE										1
31+60								1			
34+00	SB OUTSIDE										1
42+00	NB OUTSIDE						1				
50+50	SB & NB OUTSIDE						2				
57+10	SB OUTSIDE							1			
60+00	SB OUTSIDE							1			
61+00	NB OUTSIDE	1						1			
63+50	NB OUTSIDE										1
63+90	NB OUTSIDE										1
72+50								1			
76+50	SB OUTSIDE										1
80+20									1		
87+00	SB OUTSIDE						1				
87+00	NB OUTSIDE										1
88+00	NB OUTSIDE										1
89+50	SB OUTSIDE										1
92+50	SB OUTSIDE										1
94+00	SB OUTSIDE										1
97+00	NB OUTSIDE										1
99+50	SB OUTSIDE										1
101+30	NB OUTSIDE										1
102+40	SB OUTSIDE										1
102+60	SB OUTSIDE										1
104+20	NB OUTSIDE										1
104+80	NB OUTSIDE										1
106+10	SB OUTSIDE										1
121+00	SB OUTSIDE										1
134+50								1			
151+00								1			
162+45								1			
171+00								1			
180+00								1			
196+75								1			
209+50									2		
217+33								2			
226+00	SB OUTSIDE							1			
226+00	NB OUTSIDE										1
228+80									1		
231+50	NB OUTSIDE							1			
236+10	SB OUTSIDE						1				
237+00								1			
7+75	TR 349 RAMP C										1
6+60	TR 349 RAMP B										1
2+20 LT	RAMP D US 24			1				1			
TOTAL =		1	1	1	1	1	5	5	21	4	40

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**SCHEDULE OF QUANTITIES**

SCALE: VERT.      DRAWN BY  
 HORIZ.              CHECKED BY  
 DATE





LOCATION		LONGITUDINAL JOINT REPAIR (FOOT)	AREA RELECTIVE CRACK CONTROL (SQ YD)
STATION TO STATION	DESCRIPTION	(FOOT)	(SQ YD)
<i>SOUTHBOUND I-172 (PROPOSED LONGITUDINAL REPAIR)</i>			
75+24 - 76+01	CENTERLINE	77	34.2
76+83 - 77+50	CENTERLINE	67	29.8
78+26 - 79+35	CENTERLINE	109	48.4
79+41 - 80+73	CENTERLINE	132	58.7
81+04 - 81+06	CENTERLINE	2	0.9
82+84 - 83+49	CENTERLINE	65	28.9
84+18 - 84+20	CENTERLINE	2	0.9
84+37 - 85+50	CENTERLINE	113	50.2
86+26 - 87+65	CENTERLINE	139	61.8
94+82 - 95+54	CENTERLINE	72	32.0
102+48 - 102+50	CENTERLINE	2	0.9
103+05 - 103+07	CENTERLINE	2	0.9
105+07 - 106+17	CENTERLINE	110	48.9
115+47 - 116+10	CENTERLINE	63	28.0
117+38 - 117+67	CENTERLINE	29	12.9
117+81 - 117+83	CENTERLINE	2	0.9
117+91 - 117+98	CENTERLINE	7	3.1
118+57 - 118+75	CENTERLINE	18	8.0
120+56 - 120+59	CENTERLINE	3	1.3
120+71 - 121+08	CENTERLINE	37	16.4
121+29 - 121+33	CENTERLINE	4	1.8
121+41 - 121+45	CENTERLINE	4	1.8
123+88 - 123+99	CENTERLINE	11	4.9
124+12 - 124+52	CENTERLINE	40	17.8
124+74 - 125+93	CENTERLINE	119	52.9
126+17 - 126+75	CENTERLINE	58	25.8
126+88 - 126+97	CENTERLINE	9	4.0
127+22 - 127+45	CENTERLINE	23	10.2
127+61 - 127+63	CENTERLINE	2	0.9
127+67 - 127+69	CENTERLINE	2	0.9
127+76 - 128+12	CENTERLINE	36	16.0
130+19 - 130+21	CENTERLINE	2	0.9
130+56 - 130+74	CENTERLINE	18	8.0
130+88 - 131+21	CENTERLINE	33	14.7
131+21 - 131+40	CENTERLINE	19	8.4
132+44 - 133+38	CENTERLINE	94	41.8
133+76 - 134+28	CENTERLINE	52	23.1
149+77 - 149+86	CENTERLINE	9	4.0
151+07 - 151+09	CENTERLINE	2	0.9
151+16 - 151+18	CENTERLINE	2	0.9
151+22 - 151+25	CENTERLINE	3	1.3
151+44 - 152+96	CENTERLINE	152	67.6
153+27 - 153+32	CENTERLINE	5	2.2
157+33 - 157+35	CENTERLINE	2	0.9
165+05 - 165+46	CENTERLINE	41	18.2
169+30 - 169+54	CENTERLINE	24	10.7
179+95 - 180+70	CENTERLINE	75	33.3
183+33 - 184+33	CENTERLINE	100	44.4
185+36 - 186+57	CENTERLINE	121	53.8
200+46 - 201+54	CENTERLINE	108	48.0
204+50 - 204+93	CENTERLINE	43	19.1
205+25 - 205+85	CENTERLINE	60	26.7
214+57 - 216+09	CENTERLINE	152	67.6
216+72 - 222+25	CENTERLINE	553	245.8
223+13 - 223+29	CENTERLINE	16	7.1
224+33 - 225+51	CENTERLINE	118	52.4
227+13 - 228+19	CENTERLINE	106	47.1
228+65 - 229+75	CENTERLINE	110	48.9
230+30 - 230+37	CENTERLINE	7	3.1
241+02 - 248+22	RT EOP	720	320.0
248+59 - 249+29	CENTERLINE	70	31.1

LOCATION		LONGITUDINAL JOINT REPAIR (FOOT)	AREA RELECTIVE CRACK CONTROL (SQ YD)
STATION TO STATION	DESCRIPTION	(FOOT)	(SQ YD)
<i>SOUTHBOUND I-172 (PROPOSED LONGITUDINAL REPAIR)</i>			
248+18 - 249+57	RAMP D (IL104)	139	61.8
252+43 - 252+56	CENTERLINE	13	5.8
255+19 - 255+32	CENTERLINE	13	5.8
255+62 - 256+88	CENTERLINE	126	56.0
257+85 - 257+93	CENTERLINE	8	3.6
259+24 - 259+42	CENTERLINE	18	8.0
265+21 - 265+33	CENTERLINE	12	5.3
265+72 - 266+00	CENTERLINE	28	12.4
266+26 - 266+31	CENTERLINE	5	2.2
266+90 - 267+08	CENTERLINE	18	8.0
267+22 - 267+29	CENTERLINE	7	3.1
269+05 - 269+09	CENTERLINE	4	1.8
270+54 - 270+61	CENTERLINE	7	3.1
270+75 - 270+90	CENTERLINE	15	6.7
271+36 - 271+76	CENTERLINE	40	17.8
271+97 - 272+31	CENTERLINE	34	15.1
274+00 - 274+08	CENTERLINE	8	3.6
275+21 - 276+96	RAMP C (IL104)	175	77.8
276+42 - 277+49	RT EOP	107	47.6
277+81 - 277+84	RT EOP	3	1.3
278+31 - 278+36	RT EOP	5	2.2
282+40 - 282+44	CENTERLINE	4	1.8
287+51 - 287+60	CENTERLINE	9	4.0
288+60 - 288+78	CENTERLINE	18	8.0
STA. EQ. ± 288+81BK = 0+00AH			
1+95 - 2+02	CENTERLINE	7	3.1
2+58 - 3+09	CENTERLINE	51	22.7
3+66 - 3+68	CENTERLINE	2	0.9
3+82 - 3+84	CENTERLINE	2	0.9
3+95 - 3+97	CENTERLINE	2	0.9
4+61 - 4+63	CENTERLINE	2	0.9
5+51 - 5+56	CENTERLINE	5	2.2
5+85 - 6+23	CENTERLINE	38	16.9
11+80 - 12+05	CENTERLINE	25	11.1
12+36 - 12+39	CENTERLINE	3	1.3
12+70 - 12+72	CENTERLINE	2	0.9
26+51 - 26+54	CENTERLINE	3	1.3
28+67 - 30+68	RAMP D (TR 349)	201	89.3
21+21 - 30+15	RT EOP	894	397.3
26+00 - 26+50	CENTERLINE	50	22.2
37+93 - 38+25	CENTERLINE	32	14.2
38+41 - 38+77	CENTERLINE	36	16.0
54+75 - 55+91	CENTERLINE	116	51.6
57+02 - 58+20	RT EOP	118	52.4
57+45 - 59+19	RAMP C (TR 349)	174	77.3
58+58 - 59+19	CENTERLINE	61	27.1
59+18 - 61+13	CENTERLINE	195	86.7
61+24 - 61+28	CENTERLINE	4	1.8
61+35 - 61+49	CENTERLINE	14	6.2
62+02 - 62+41	CENTERLINE	39	17.3
60+69 - 62+41	RT EOP	172	76.4
61+00 - 61+78	CENTERLINE	78	34.7
66+30 - 66+33	CENTERLINE	3	1.3
71+43 - 71+58	CENTERLINE	15	6.7
75+00 - 76+75	CENTERLINE	175	77.8
77+21 - 77+26	CENTERLINE	5	2.2
77+42 - 77+47	CENTERLINE	5	2.2
89+19 - 89+21	CENTERLINE	2	0.9
89+56 - 89+58	CENTERLINE	2	0.9
89+92 - 89+94	CENTERLINE	2	0.9
90+71 - 90+73	CENTERLINE	2	0.9

LOCATION		LONGITUDINAL JOINT REPAIR (FOOT)	AREA RELECTIVE CRACK CONTROL (SQ YD)
STATION TO STATION	DESCRIPTION	(FOOT)	(SQ YD)
<i>SOUTHBOUND I-172 (PROPOSED LONGITUDINAL REPAIR)</i>			
91+18 - 91+20	CENTERLINE	2	0.9
92+99 - 93+03	CENTERLINE	4	1.8
93+16 - 93+39	CENTERLINE	23	10.2
94+19 - 94+94	CENTERLINE	75	33.3
95+49 - 95+77	CENTERLINE	28	12.4
98+87 - 100+30	CENTERLINE	143	63.6
111+03 - 111+43	CENTERLINE	40	17.8
112+04 - 112+22	CENTERLINE	18	8.0
112+53 - 112+62	CENTERLINE	9	4.0
112+91 - 113+97	CENTERLINE	106	47.1
114+22 - 114+51	CENTERLINE	29	12.9
114+88 - 114+97	CENTERLINE	9	4.0
115+33 - 115+37	CENTERLINE	4	1.8
116+44 - 116+63	CENTERLINE	19	8.4
116+75 - 116+82	CENTERLINE	7	3.1
117+19 - 117+60	CENTERLINE	41	18.2
127+02 - 127+80	CENTERLINE	78	34.7
129+25 - 129+30	CENTERLINE	5	2.2
129+38 - 129+44	CENTERLINE	6	2.7
132+03 - 132+79	CENTERLINE	76	33.8
139+47 - 139+80	CENTERLINE	33	14.7
148+81 - 148+92	CENTERLINE	11	4.9
157+24 - 157+32	CENTERLINE	8	3.6
174+16 - 174+23	CENTERLINE	7	3.1
178+29 - 178+31	CENTERLINE	2	0.9
183+23 - 183+37	CENTERLINE	14	6.2
189+13 - 189+15	CENTERLINE	2	0.9
189+30 - 189+39	CENTERLINE	9	4.0
191+83 - 192+79	CENTERLINE	96	42.7
192+91 - 192+97	CENTERLINE	6	2.7
193+04 - 193+08	CENTERLINE	4	1.8
195+31 - 195+41	CENTERLINE	10	4.4
195+99 - 196+70	CENTERLINE	71	31.6
198+45 - 198+85	CENTERLINE	40	17.8
199+05 - 200+09	CENTERLINE	104	46.2
201+13 - 201+16	CENTERLINE	3	1.3
201+31 - 201+33	CENTERLINE	2	0.9
201+72 - 203+53	CENTERLINE	181	80.4
214+40 - 214+60	CENTERLINE	20	8.9
221+41 - 222+05	RT EOP	64	28.4
222+13 - 222+24	CENTERLINE	11	4.9
222+35 - 222+38	CENTERLINE	3	1.3
223+62 - 223+65	CENTERLINE	3	1.3
223+72 - 223+74	CENTERLINE	2	0.9
226+51 - 235+95	RT EOP	944	419.6
227+19 - 227+28	CENTERLINE	9	4.0
227+97 - 228+48	CENTERLINE	51	22.7
232+27 - 232+59	CENTERLINE	32	14.2
233+08 - 234+14	CENTERLINE	106	47.1
241+93 - 242+20	CENTERLINE	27	12.0
247+02 - 247+15	CENTERLINE	13	5.8
SOUTHBOUND TOTAL:		10139	4506.4
NORTHBOUND TOTAL:		23780	10568.7
GRAND TOTAL:		33919	15075.1

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT. HORIZ.  
DATE \_\_\_\_\_ DRAWN BY \_\_\_\_\_ CHECKED BY \_\_\_\_\_

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	27

STA. TO STA.  
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

RAISED REFLECTIVE SCHEDULE	
LOCATION	RAISED REFLECTIVE PAVEMENT MARKER (EACH)
<b>I-172</b>	
NB MAINLINE	
78+25 TO 288+81.55	265
0+00 TO 247+31.66	310
SB MAINLINE	
75+24 TO 281+81.55	259
0+00 TO 247+31.66	310
<b>IL 104</b>	
RAMP A	
LT. 03+04 TO 06+64	9
RT. 00+00 TO 06+64	33
RAMP B	
LT. 15+16 TO 19+76	23
RT. 01+25 TO 26+74	21
RAMP C	
LT. 02+80 TO 06+40	9
RT 00+00 TO 06+40	32
RAMP D	
LT 13+40 TO 18+00	23
RT. 13+40 TO 25+60	31
<b>TR 349</b>	
RAMP A	
LT. 02+85 TO 06+45	9
RT. 00+00 TO 06+45	32
ISLAND	
RAMP B	
LT. 09+00 TO 13+60	12
RT. 09+00 TO 21+00	30
RAMP C	
LT. 02+92 TO 06+52	9
RT. 00+00 TO 06+52	33
ISLAND	
RAMP D	
LT. 09+10 TO 13+70	12
RT. 09+10 TO 21+15	30
<b>US 24</b>	
RAMP A	
LT. 02+81 TO 06+41	9
RT. 00+00 TO 06+41	32
ISLAND	
<b>US 24</b>	
RAMP D	
LT. 13+78 TO 18+38	12
RT. 13+78 TO 25+80	30
COLUMBUS ROAD ( TR 349)	
MAINLINE	48
<b>TOTAL</b>	<b>1623</b>

AREA REFLECTIVE CRACK CONTROL					
LOCATION		LENGTH	ESTIMATED WIDTH	AREA	
STATION TO	STATION	(FT)	(FT)	(SQ YD)	
<b>NORTHBOUND I-172 (EXISTING LONGITUDINAL REPAIR)</b>					
225+46	-	227+56	212	4	94.2
227+82	-	230+94	314	4	139.6
242+31	-	242+90	61	4	27.1
259+63	-	260+05	44	4	19.6
259+85	-	260+28	45	4	20
260+24	-	260+88	66	4	29.3
260+88	-	261+66	80	4	35.6
261+23	-	261+80	59	4	26.2
261+76	-	263+52	178	4	79.1
263+46	-	263+74	30	4	13.3
263+75	-	264+51	78	4	34.7
264+88	-	265+14	28	4	12.4
265+36	-	265+47	13	4	5.8
<b>STA. EQ. : 288+81BK = 0+00AH</b>					
10+85	-	18+72	789	4	350.7
18+67	-	22+73	408	4	181.3
22+76	-	25+46	272	4	120.9
29+42	-	35+19	579	4	257.3
41+53	-	42+12	61	4	27.1
42+76	-	43+81	107	4	47.6
43+98	-	46+96	317	4	140.9
46+79	-	48+77	200	4	88.9
50+90	-	51+65	77	4	34.2
52+03	-	52+38	37	4	16.4
53+62	-	55+07	147	4	65.3
55+42	-	57+45	205	4	91.1
86+10	-	86+53	45	4	20
105+25	-	106+00	77	4	34.2
107+02	-	107+75	75	4	33.3
108+59	-	108+87	30	4	13.3
111+50	-	113+28	180	4	80
113+78	-	114+38	62	4	27.6
114+31	-	114+64	35	4	15.6
122+03	-	123+78	177	4	78.7
146+87	-	149+86	301	4	133.8
221+32	-	221+69	39	4	17.3
<b>NORTHBOUND TOTAL:</b>			<b>5428</b>		<b>2412</b>

AREA REFLECTIVE CRACK CONTROL					
LOCATION		LENGTH	ESTIMATED WIDTH	AREA	
STATION TO	STATION	(FT)	(FT)	(SQ YD)	
<b>NORTHBOUND I-172 (EXISTING LONGITUDINAL REPAIR)</b>					
236+37	-	237+58	123	4	54.7
259+42	-	262+21	281	4	124.9
288+75	-	288+81	8	4	3.6
<b>SOUTHBOUND I-172 (EXISTING LONGITUDINAL REPAIR)</b>					
<b>STA. EQ. : 288+81BK = 0+00AH</b>					
0+00	-	2+41	243	4	108
5+03	-	5+41	40	4	17.8
7+34	-	8+00	68	4	30.2
8+88	-	10+99	213	4	94.7
14+29	-	26+45	1218	4	541.3
28+34	-	32+62	430	4	191.1
34+71	-	36+23	154	4	68.4
45+24	-	48+50	328	4	145.8
59+31	-	60+68	139	4	61.8
67+52	-	68+35	85	4	37.8
77+45	-	88+66	1123	4	499.1
90+27	-	90+57	32	4	14.2
97+03	-	97+85	84	4	37.3
100+30	-	111+03	1075	4	477.8
117+60	-	118+14	56	4	24.9
118+52	-	120+10	160	4	71.1
127+81	-	129+09	130	4	57.8
143+59	-	144+48	91	4	40.4
163+53	-	165+42	191	4	84.9
163+54	-	163+92	40	4	17.8
165+65	-	167+05	142	4	63.1
167+69	-	169+14	147	4	65.3
194+08	-	194+80	74	4	32.9
203+61	-	207+65	406	4	180.4
207+63	-	207+94	33	4	14.7
207+46	-	210+03	259	4	115.1
213+20	-	213+41	23	4	10.2
209+88	-	216+02	616	4	273.8
215+81	-	216+48	69	4	30.7
216+55	-	222+13	560	4	248.9
240+56	-	241+89	135	4	60
<b>SOUTHBOUND TOTAL:</b>			<b>8776</b>		<b>3900.5</b>
<b>NORTHBOUND TOTAL:</b>			<b>5428</b>		<b>2412.4</b>
<b>LONGITUDINAL JOINT REPIAR TOTAL:</b>					<b>15075.1</b>
<b>GRAND TOTAL:</b>					<b>21388.0</b>

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_

DATE \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
CHECKED BY \_\_\_\_\_

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	28

STA. TO STA.  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

PERFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID) LETTERS & SYMBOLS	
LOCATION	ARROWS (SQ FT)
COLUMBUS ROAD (TR 349)	
RAMP A	
LEFT-TURN LANE	15.6
THROUGH LANE	23.0
RAMP C	
LEFT-TURN LANE	15.6
THROUGH LANE	23.0
LEFT-TURN LANE WEST OF SN.001-0048	46.8
LEFT-TURN LANE EAST OF SN.001-0048	46.8
IL 104	
RAMP A	
THROUGH LANE	23.0
RAMP C	
THROUGH LANE	23.0
US 24	
RAMP A	
THROUGH LANE	11.5
LEFT-TURN LANE	31.2
RIGHT-TURN LANE	31.2
TOTAL =	290.7

URETHANE PAVEMENT MARKING - LINE 5"		
LOCATION	DOUBLE YELLOW (FOOT)	WHITE EDGE LINE (FOOT)
COLUMBUS ROAD (TR 349)		
35+48.85 TO 43+52.19 (MEDIAN)	1606.6	
44+37 TO 55+58.12 (MEDIAN)	2242.3	
56+40 TO 64+75 (MEDIAN)	1670.0	
LT 35+48.85 TO 36+54		105.2
LT 37+39 TO 42+00		461.0
LT 43+68.78 TO 56+21.34		1252.6
LT 57+69.65 TO 64+75.00		705.4
RT 35+48.85 TO 42+26.22		677.4
RT 43+74.52 TO 56+28.93		1254.4
RT 58+03.72 TO 61+62.31		352.6
RT 62+90.95 TO 64+75		184.1
TOTAL =	5518.9	4992.7

PERFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID) - 8"	
LOCATION	SOLID WHITE (FOOT)
COLUMBUS ROAD (TR 349)	
ISLAND @ RAMP A	163
ISLAND @ RAMP C	180
TOTAL =	343

URETHANE PAVEMENT MARKING - LINE 5"	
LOCATION	WHITE EDGE LINE (FOOT)
IL 104	
RAMP A	
RT 0+00 TO 19+72	1972
LT 06+64 TO 19+72	1308
RAMP B	
RT 01+25 TO 26+74	2549
LT 01+25 TO 15+16	1391
RAMP C	
RT 0+00 TO 18+50	1850
LT 0+60 TO 18+50	1210
RAMP D	
RT 0+11 TO 25+61	2550
LT 0+11 TO 13+40	1329
US 24	
RAMP A	
RT 0+ 00 TO 20+70	2070
LT 06+41 TO 20+50	1409
RAMP D	
RT 0+13 TO 25+80	2567
0+24 TO 13+78	1354
TOTAL =	21559

PERFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID) - LINE 24"	
LOCATION	SOLID WHITE STOP BARS (FOOT)
COLUMBUS ROAD (TR 349)	
RAMP A	
56+51.40	18
57+02.90	18
RAMP C	
42+76.26	18
43+46.50	18
HORSESHOE VALLEY DRIVE	
63+44.75	12
US 24	
RAMP A	
20+27.00	18
20+45.00	18
TOTAL =	120

URETHANE PAVEMENT MARKING, LINE 6"	
LOCATION	SOLID WHITE (FOOT)
COLUMBUS ROAD (TR 349)	
LEFT-TURN LANES	
53+75.78 TO 55+60.79	185.0
44+37.10 TO 46+15.85	178.8
TOTAL =	363.8

URETHANE PAVEMENT MARKING, LINE 5"	
LOCATION	WHITE EDGE LINE (FOOT)
COLUMBUS ROAD (TR 349)	
RAMP A	
RT 00+00 TO 24+42	2442
LT 06+45 TO 24+11	1766
RAMP B	
RT 00+40 TO 21+00	2060
LT 00+40 TO 09+00	860
RAMP C	
RT 00+00 TO 23+64	2364
LT 06+52 TO 23+20	1668
RAMP D	
RT 00+23 TO 21+14	2091
LT 00+23 TO 09+10	887
TOTAL =	14138

PERFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID) - LINE 12"		
LOCATION	SOLID WHITE DIAGONALS & CHEVRONS (FOOT)	SOLID YELLOW DIAGONALS & CHEVRONS (FOOT)
COLUMBUS ROAD (TR 349)		
ISLAND @ RAMP A	47	
ISLAND @ RAMP C	63	
MEDIAN WEST OF SN 001-0048		85
MEDIAN EAST OF SN 001-0048		104
TOTAL =	110	189

LOCATION	PERFORMED PLASTIC PAVEMENT MARKING TYPE B, LINE 5" SKIP-DASH (FOOT)	URETHANE PAVEMENT MARKING - LINE 5" EDGE LINE (FOOT)
I-172		
SB MAINLINE		
75+24 TO 288+81.55	5339.4	42715.1
0+00 TO 247+56	6189.0	49512.0
NB MAINLINE		
78+25 TO 288+81.55	5264.1	42113.1
0+00 TO 247+31.66	6182.9	49463.3
TOTAL =	22975.4	183803.5

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**SCHEDULE OF QUANTITIES**  
 SCALE: VERT. HORIZ. DATE: 08/04/2004  
 DRAWN BY: JJS  
 CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	29

STA. TO STA.  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

TEMPORARY PAVEMENT MARKING									
LOCATION	LETTERS & SYMBOLS ARROWS (WHITE)	LINE 24" STOP BARS (SOLID WHITE)	LINE 12" DIAGONALS & CHEVRONS (SOLID WHITE)	LINE 12" DIAGONALS & CHEVRONS (SOLID YELLOW)	LINE 8" (SOLID WHITE)	LINE 6" (SOLID WHITE)	LINE 5" EDGE LINE (WHITE)	LINE 5" DOUBLE LINE (YELLOW)	LINE 5" TYPE B SKIP-DASH (WHITE)
	(SQ FT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)
	COLUMBUS ROAD (TR 349)								
35+48.85 TO 43+52.19 (MEDIAN)								1607	
44+37 TO 55+58.12 (MEDIAN)								2242	
56+40 TO 64+75 (MEDIAN)								1670	
LT 35+48.85 TO 36+54							105		
LT 37+39 TO 42+00							461		
LT 43+68.78 TO 56+21.34							1253		
LT 57+69.65 TO 64+75.00							705		
RT 35+48.85 TO 42+26.22							677		
RT 43+74.52 TO 56+28.93							1254		
RT 58+03.72 TO 61+62.31							353		
RT 62+90.95 TO 64+75							184		
LEFT-TURN LANES									
- 53+75.78 TO 55+60.79							185		
- 44+37.10 TO 46+15.85							179		
ISLAND @ RAMP A			47						
ISLAND @ RAMP C			63						
MEDIAN WEST OF SN 001-0048				85					
MEDIAN EAST OF SN 001-0048				104					
LEFT-TURN LANE WEST OF SN.001-0048	46.8								
LEFT-TURN LANE EAST OF SN.001-0048	46.8								
ISLAND @ RAMP A						163			
ISLAND @ RAMP C						180			
RAMP A									
RT 00+00 TO 24+42							2442		
LT 06+45 TO 24+11							1766		
56+51.40		18.0							
57+02.90		18.0							
LEFT-TURN LANE	15.6								
THROUGH LANE	23.0								
RAMP B									
RT 00+40 TO 21+00							2060		
LT 00+40 TO 09+00							860.0		
RAMP C									
RT 00+00 TO 23+64							2364		
LT 06+52 TO 23+20							1668		
42+76.26		18.0							
43+46.50		18.0							
LEFT-TURN LANE	15.6								
THROUGH LANE	23.0								
RAMP D									
RT 00+23 TO 21+14							2091		
LT 00+23 TO 09+10							887		
HORSESHOE VALLEY DRIVE									
63+44.75		12.0							
SUBTOTAL 1=	170.8	84.0	110	189	343	364	19130	5519	0.0

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT. DATE: 08/04/2004  
 HORIZ. DATE: 08/04/2004

DRAWN BY: JJS  
 CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	30

STA. TO STA.  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

TEMPORARY PAVEMENT MARKING									
LOCATION	LETTERS & SYMBOLS ARROWS (WHITE)	LINE 24" STOP BARS (SOLID WHITE)	LINE 12" DIAGONALS & CHEVRONS (SOLID WHITE)	LINE 12" DIAGONALS & CHEVRONS (SOLID YELLOW)	LINE 8" (SOLID WHITE)	LINE 6" (SOLID WHITE)	LINE 5" EDGE LINE (WHITE)	LINE 5" DOUBLE LINE (YELLOW)	LINE 5" TYPE B SKIP-DASH (WHITE)
	(SQ FT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)	(FOOT)
IL 104									
RAMP A									
RT 0+00 TO 19+72							1972		
LT 06+64 TO 19+72							1308		
THROUGH LANE	23.0								
RAMP B									
RT 01+25 TO 26+74							2549		
LT 01+25 TO 15+16							1391		
RAMP C									
RT 0+00 TO 18+50							1850		
LT 0+60 TO 18+50							1210		
THROUGH LANE	23.0								
RAMP D									
RT 0+11 TO 25+61							2550		
LT 0+11 TO 13+40							1329		
US 24									
RAMP A									
RT 0+ 00 TO 20+70		18.0					2070		
LT 06+41 TO 20+50		18.0					1409		
THROUGH LANE	11.5								
LEFT-TURN LANE	31.2								
RIGHT-TURN LANE	31.2								
RAMP D									
RT 0+13 TO 25+80							2567		
0+24 TO 13+78							1354		
I-172									
SB MAINLINE									
75+24 TO 288+81.55							42715		5339
0+00 TO 247+56							49512		6189
NB MAINLINE									
78+25 TO 288+81.55							42113		5264
0+00 TO 247+31.66							49463		6183
SUBTOTAL 2=	119.9	36	0.0	0.0	0.0	0.0	205362	0.0	22975
SUBTOTAL 1=	170.8	84	110	189	343	364	19130	5519	0.0
TOTAL	290.7	120	110	189	343	364	224492	5519	22975

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT. HORIZ. DATE: 08/04/2004

DRAWN BY: JJS  
 CHECKED BY: LLO

ESTIMATED DITCH CHECKS				
STATION	OFF-SET (FT)	LT/ RT	EACH	AGGREGATE (EROSION CONTROL) (TON)
84+70		MEDIAN	1	3.5
117+30		MEDIAN	1	3.5
117+70		MEDIAN	1	3.5
116+70	170	RT	1	3.5
117+00	180	RT	1	3.5
117+10	220	RT	1	3.5
118+80	170	RT	1	3.5
119+65	150	RT	1	3.5
119+90	200	LT	1	3.5
120+20	210	LT	1	3.5
120+80	130	RT	1	3.5
121+80	110	RT	1	3.5
123+55	100	RT	1	3.5
119+86	190	LT	1	3.5
120+20	200	LT	1	3.5
129+80		MEDIAN	1	3.5
155+26	110	LT	1	3.5
155+70	130	RT	1	3.5
155+85	90	LT	1	3.5
156+10	180	RT	1	3.5
155+45		MEDIAN	1	3.5
166+09	110	LT	1	3.5
166+80		MEDIAN	1	3.5
166+80	100	RT	1	3.5
171+90	100	RT	1	3.5
175+00	100	RT	1	3.5
178+00	100	RT	1	3.5
181+20	100	RT	1	3.5
183+70	100	RT	1	3.5
198+00	100	RT	1	3.5
204+32	100	RT	1	3.5
209+30	100	LT	1	3.5
217+00	100	LT	1	3.5
217+70	100	LT	1	3.5
218+75	110	RT	1	3.5
230+60	230	RT	1	3.5
243+20		MEDIAN	1	3.5
249+10	140	LT	1	3.5
249+50	140	LT	1	3.5
255+80		MEDIAN	1	3.5
265+35		MEDIAN	1	3.5
267+70	180	LT	1	3.5
8+00		MEDIAN	1	3.5
16+00	100	RT	1	3.5
16+70	100	RT	1	3.5
25+00	100	RT	1	3.5
26+00	100	RT	1	3.5
26+60	100	RT	1	3.5

ESTIMATED DITCH CHECKS				
STATION	OFF-SET (FT)	LT/ RT	EACH	AGGREGATE (EROSION CONTROL) (TON)
31+30	130	RT	1	3.5
72+70	110	RT	1	3.5
81+50	250	RT	1	3.5
81+80	310	RT	1	3.5
82+67			1	3.5
83+20	150	RT	1	3.5
83+80	140	RT	1	3.5
84+51	130	RT	1	3.5
84+70	160	LT	1	3.5
85+50	120	RT	1	3.5
85+60	120	LT	1	3.5
86+60	110	RT	1	3.5
88+30	120	LT	1	3.5
84+80		MEDIAN	1	3.5
105+45	100	RT	1	3.5
109+85	310	RT	1	3.5
110+50	280	RT	1	3.5
111+20	275	RT	1	3.5
111+96	260	RT	1	3.5
113+80	120	RT	1	3.5
115+00	110	RT	1	3.5
120+11	110	LT	1	3.5
121+60		MEDIAN	1	3.5
122+70	110	LT	1	3.5
123+00	110	RT	1	3.5
125+00	110	RT	1	3.5
127+00	110	RT	1	3.5
128+00	110	RT	1	3.5
130+00	110	RT	1	3.5
132+00	110	RT	1	3.5
132+00	110	LT	1	3.5
134+00	110	LT	1	3.5
134+00	110	RT	1	3.5
134+25		MEDIAN	1	3.5
135+00	110	RT	1	3.5
136+00	110	LT	1	3.5
137+00	110	RT	1	3.5
138+00	110	LT	1	3.5
140+00	110	LT	1	3.5
140+40	110	RT	1	3.5
142+00	110	LT	1	3.5
150+75		MEDIAN	1	3.5
151+00	190	RT	1	3.5
171+40		MEDIAN	1	3.5
172+35	220	RT	1	3.5
180+00		MEDIAN	1	3.5
181+15	150	RT	1	3.5
190+82	140	RT	1	3.5

ESTIMATED DITCH CHECKS				
STATION	OFF-SET (FT)	LT/ RT	EACH	AGGREGATE (EROSION CONTROL) (TON)
196+40	110	RT	1	3.5
196+80	140	RT	1	3.5
197+40	140	RT	1	3.5
199+00	100	LT	1	3.5
200+00	100	LT	1	3.5
208+30	150	RT	1	3.5
209+70		MEDIAN	1	3.5
210+00	130	RT	1	3.5
211+20	130	RT	1	3.5
217+00	110	RT	1	3.5
217+20	150	LT	1	3.5
217+60	100	LT	1	3.5
217+70	110	RT	1	3.5
225+70	110	LT	1	3.5
225+80	110	RT	1	3.5
226+30	110	LT	1	3.5
226+50	110	RT	1	3.5
229+11	160	LT	1	3.5
235+00	140	LT	1	3.5
IL 104				
RAMP B				
10+00	50	RT	1	3.5
11+85	50	RT	1	3.5
14+30	35	RT	1	3.5
TR 349				
RAMP A				
7+80	40	RT	1	3.5
RAMP B				
10+15	35	RT	1	3.5
US 24				
RAMP A				
4+20	50	RT	1	3.5
6+00	50	RT	1	3.5
7+60	50	RT	1	3.5
RAMP D				
11+10	100	RT	1	3.5
TOTAL			124	434

BOLLARD SCHEDULE		
STATION	BOLLARDS REMOVED (EACH)	BOLLARDS REPLACED (EACH)
MAINLINE (1-5RS-1)		
STA. 75+71.05	5	5
STA. 85+00.00	5	5
STA. 130+00.00	5	5
STA. 142+00.00	5	5
STA. 155+60.00	5	5
STA. 167+00.00	5	5
STA. 177+00.00	5	5
STA. 256+00.00	5	5
STA. 265+50.00	5	5
STA. 278+00.00	5	5
MAINLINE (1-6RS)		
STA. 15+95.00	5	5
STA. 31+50.00	5	5
STA. 60+00.00	5	5
STA. 72+40.42	5	5
STA. 86+88.69	5	5
STA. 110+44.01	5	5
STA. 113+47.30	5	5
STA. 122+00.00	5	5
STA. 134+37.23	5	5
STA. 151+00.00	5	5
STA. 171+00.00	5	5
STA. 180+00.00	5	5
STA. 190+00.00	5	5
STA. 209+41.37	5	5
STA. 217+35.42	5	5
STA. 228+70.39	5	5
STA. 237+00.00	5	5
TOTAL	135	135

REVISIONS		ILLINOIS DEPARTMENT OF TRANSPORTATION
NAME	DATE	
SCALE: VERT. HORIZ.	DRAWN BY	CHECKED BY
DATE		

**SCHEDULE OF QUANTITIES**

ACCESS CONTROL IMPROVEMENT SCHEDULE					
STATION	LOCATION	WOVEN WIRE FENCE REMOVAL & REPLACEMENT (FOOT)	POST FOR WOVEN WIRE FENCE (EACH)	WOVEN WIRE FENCE REPAIR (SPECIAL) (FOOT)	REATTACH WOVEN WIRE FENCE BRACE (EACH)
1-5RS-1					
79+25.00	EAST			10	
109+00.00	EAST	25			
111+00.00	WEST	25			
113+40.00	EAST			15	
114+50.00	WEST			100	
115+38.00	EAST	15			
116+50.00	WEST	75			
118+00.00	EAST	20			
142+50.00	EAST			30	
150+30.00	WEST			30	
151+00.00	EAST			10	
156+00.00	EAST			10	
160+20.00	EAST			25	
164+50.00	EAST			15	
166+50.00	EAST		1	10	
172+50.00	EAST			30	
174+00.00	WEST				1
178+25.00	WEST			50	
222+20.00	WEST			15	
223+50.00	WEST			40	
224+00.00	WEST			10	
224+25.00	WEST			30	
224+50.00	WEST				1
225+75.00	EAST		1		1
226+50.00	WEST			50	
228+50.00	WEST				2
232+00.00	WEST				1
232+70.00	EAST			10	
233+00.00	WEST				2
234+50.00	WEST				2
237+00.00	WEST				1
239+45.00	EAST				1
239+75.00	WEST			75	
239+80.00	WEST			40	
243+90.00	EAST				1
244+10.00	EAST			10	
244+55.00	EAST			30	
245+00.00	WEST				2
247+50.00	WEST				1
249+00.00	EAST				2
259+00.00	EAST		1		
261+00.00	EAST			150	
267+00.00	WEST			50	
268+00.00	WEST	30			
1-6RS					
00+00.00	WEST				1
00+10.00	EAST				1
01+00.00	WEST				1
04+20.00	WEST				2
05+60.00	EAST		1		
06+00.00	EAST			10	
09+00.00	EAST				2
09+00.00	WEST				2
12+00.00	EAST				2
12+00.00	WEST				2
12+50.00	EAST		1		
13+50.00	EAST		1		
14+40.00	EAST			15	
15+00.00	WEST			20	
15+20.00	EAST				1
21+00.00	WEST				1
24+50.00	EAST				1
25+50.00	WEST	400			
29+75.00	EAST				1
30+00.00	WEST			10	

ACCESS CONTROL IMPROVEMENT SCHEDULE					
STATION	LOCATION	WOVEN WIRE FENCE REMOVAL & REPLACEMENT (FOOT)	POST FOR WOVEN WIRE FENCE (EACH)	WOVEN WIRE FENCE REPAIR (SPECIAL) (FOOT)	REATTACH WOVEN WIRE FENCE BRACE (EACH)
31+50.00	WEST				1
37+00.00	WEST				2
38+25.00	WEST			25	
39+50.00	EAST				1
42+00.00	EAST			10	
42+20.00	EAST		1		
42+70.00	EAST		1		
43+50.00	EAST		3		
44+35.00	EAST		5		
45+00.00	EAST		1		
47+50.00	EAST				1
48+50.00	EAST	60			
55+15.00	EAST				2
55+50.00	EAST	50			
55+85.00	EAST				1
59+35.00	EAST			20	
62+50.00	EAST		1		
63+50.00	EAST		1		
64+50.00	WEST				1
67+65.00	EAST			30	
69+00.00	WEST				1
70+00.00	WEST				1
72+50.00	EAST				1
74+00.00	WEST				1
74+25.00	EAST			20	
77+00.00	WEST				1
80+00.00	WEST				1
81+00.00	EAST			10	
84+00.00	WEST				1
85+00.00	WEST				1
86+00.00	WEST				1
87+60.00	WEST				1
90+00.00	EAST				1
92+00.00	WEST				2
92+30.00	EAST				2
95+30.00	EAST				1
97+00.00	WEST				2
97+80.00	WEST				2
99+85.00	EAST				2
100+00.00	WEST				1
101+00.00	WEST				1
103+00.00	WEST				1
104+00.00	WEST	25			
105+00.00	WEST				1
106+00.00	WEST				1
108+00.00	WEST				1
109+00.00	WEST	50			
109+50.00	WEST				1
110+10.00	EAST			15	
110+30.00	WEST				1
114+50.00	WEST	150			
116+70.00	WEST				1
117+20.00	WEST				1
118+00.00	WEST			10	
119+60.00	WEST				1
123+90.00	WEST				1
138+25.00	WEST			50	
138+50.00	WEST				1
138+75.00	EAST				1
139+30.00	EAST			10	
139+60.00	WEST				1
140+00.00	EAST			10	
140+50.00	EAST				1
140+90.00	EAST				1
144+00.00	WEST				2
145+20.00	EAST				1

ACCESS CONTROL IMPROVEMENT SCHEDULE					
STATION	LOCATION	WOVEN WIRE FENCE REMOVAL & REPLACEMENT (FOOT)	POST FOR WOVEN WIRE FENCE (EACH)	WOVEN WIRE FENCE REPAIR (SPECIAL) (FOOT)	REATTACH WOVEN WIRE FENCE BRACE (EACH)
149+10.00	WEST				1
153+00.00	WEST				2
156+00.00	EAST				1
158+05.00	WEST				2
158+60.00	WEST			60	
161+00.00	EAST				1
161+30.00	EAST				1
161+40.00	EAST				1
161+70.00	WEST			10	
162+00.00	WEST				1
163+00.00	EAST			10	
175+00.00	EAST			50	
181+00.00	EAST			10	
183+00.00	EAST			10	
185+10.00	WEST			100	
186+15.00	WEST				1
189+25.00	EAST				2
193+80.00	WEST				1
196+00.00	WEST				1
196+25.00	WEST			50	
199+00.00	EAST				1
199+75.00	EAST	50			
206+50.00	EAST			10	
206+80.00	EAST		1		
207+65.00	WEST				1
212+50.00	EAST			10	
212+70.00	EAST			10	
212+90.00	WEST				1
217+00.00	WEST			25	
217+75.00	EAST				2
223+00.00	WEST				2
227+40.00	EAST	50			
228+75.00	EAST			30	
229+00.00	EAST				2
234+00.00	EAST				2
235+15.00	WEST				2
236+75.00	EAST				2
239+10.00	WEST	40			
240+60.00	EAST			100	
242+25.00	WEST				2
243+90.00	WEST				1
245+10.00	EAST				2
247+00.00	WEST			50	
249+00.00	WEST				1
TOTAL		1065	20	1645	123

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT. HORIZ. DATE 08/04/2004

DRAWN BY JJS CHECKED BY LLO



F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	33

STA. TO STA.  
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

**BRIDGE SUMMARY SCHEDULE**

PAY ITEM	DESCRIPTION	UNITS	SN 001-0045	SN 001-0048	SN 001-0049	SN 001-0050 & SN 001-0051	BRIDGE TOTALS
X0301424	SILICONE JOINT SEALER	FOOT	60	173	66		299
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	9		5		14
48203029	HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	405		228		633
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	525		626		1151
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	0.2		0.6		0.8
40600300	AGGREGATE (PRIME COAT)	TON	1.1		3		4.1
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	44		124		168
48101200	AGGREGATE SHOULDERS, TYPE B	TON	37		20		57
63200310	GUARDRAIL REMOVAL	FOOT	850		400		1250
63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4		4		8
63000000	STEEL PLATE BEAM GUARD RAIL, TYPE A	FOOT	450				450
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4		4		8
78200410	GUARDRAIL MARKERS, TYPE A	EACH	10		4		14
78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4		4		8
44000915	HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD			846		846
50102400	CONCRETE REMOVAL	CU YD				70	70
50300255	CONCRETE SUPERSTRUCTURE	CU YD				70	70
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND				12,520	12520
50800515	BAR SPLICERS	EACH				56	56
	PPC I-BEAM REPAIRS	L SUM				1	1
52000110	PREFORMED JOINT STRIP SEAL	FOOT				301	301
Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH			2	2	4
Z0030330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE) TEST LEVEL 3	EACH			2	2	4
70400100	TEMPORARY CONCRETE BARRIER	FOOT			500	1025	1525
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT			500	1025	1525
Z0016002	DECK SLAB REPAIR (FULL DEPTH, TYPE II)	SQ YD			9.5		9.5
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD			36		36
70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH			1		1
58100200	WATERPROOFING MEMBRANE SYSTEM	SQ YD			846		846
50157305	PROTECTIVE SHIELD	L SUM			1		1

**ESTIMATED QUANTITIES**

PAY ITEM	DESCRIPTION	UNIT	QUANTITY	LOCATION/REASON
44004250	PAVED SHOULDER REMOVAL	SQ YD	1500	MAINLINE REPAIRS
48203029	HMA SHOULDER 8"	SQ YD	1500	MAINLINE REPAIRS
40600982	HMA SURFACE REMOVAL - BUTTJOINT	SQ YD	1700	BUTT JOINTS FOR BINDER (IF NEEDED)
40600985	PCC SURFACE REMOVAL- BUTT JOINT	SQ YD	2000	BUTT JOINTS FOR BINDER (IF NEEDED)
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	42500	TEMPORARY STRIPING ON BINDER/SURFACE
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	7100	REMOVE TEMP STRIPE
X2503000	MAINTENANCE MOWING	ACRE	225	JOBSITE WHEN NEEDED
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DAY	45	PATCHING & WIDENING
78300105	PAVEMENT MARKING REMOVAL	FOOT	15000	SN 001-0048, SN 001-0050, SN 001-0051
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1600	
701006800	CHANGEABLE MESSAGE BOARDS	CAL MO	18	MAINLINE & RAMPS
	PARTIAL DEPTH PAVEMENT PATCHING	SQ YD	800	
Z0075300	TIE BARS	EACH	350	TYPE B PATCHING; CURB & GUTTER

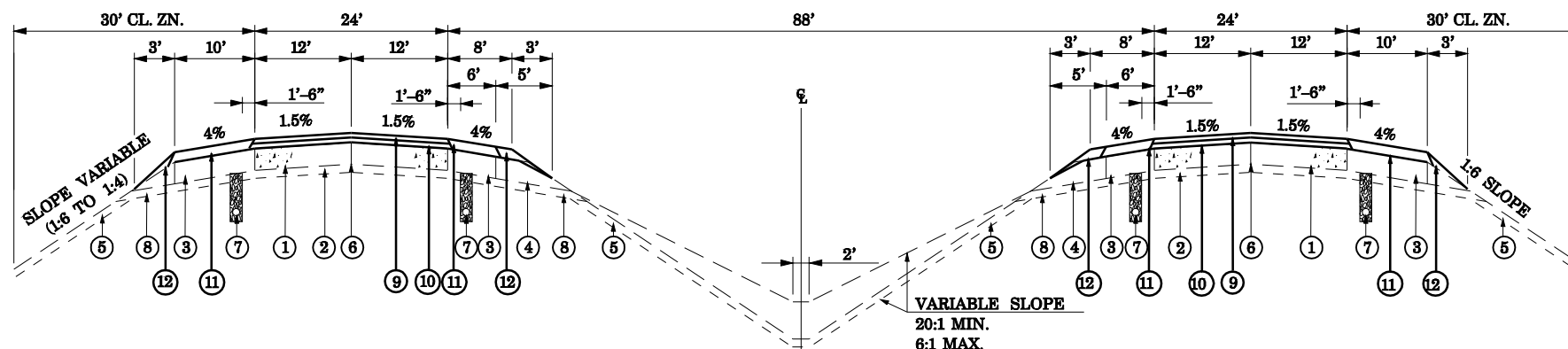
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**SCHEDULE OF QUANTITIES**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_

DRAWN BY \_\_\_\_\_  
 CHECKED BY \_\_\_\_\_



**FAI 172 - NORMAL SECTION**

SECTION (1-5)RS-1\*

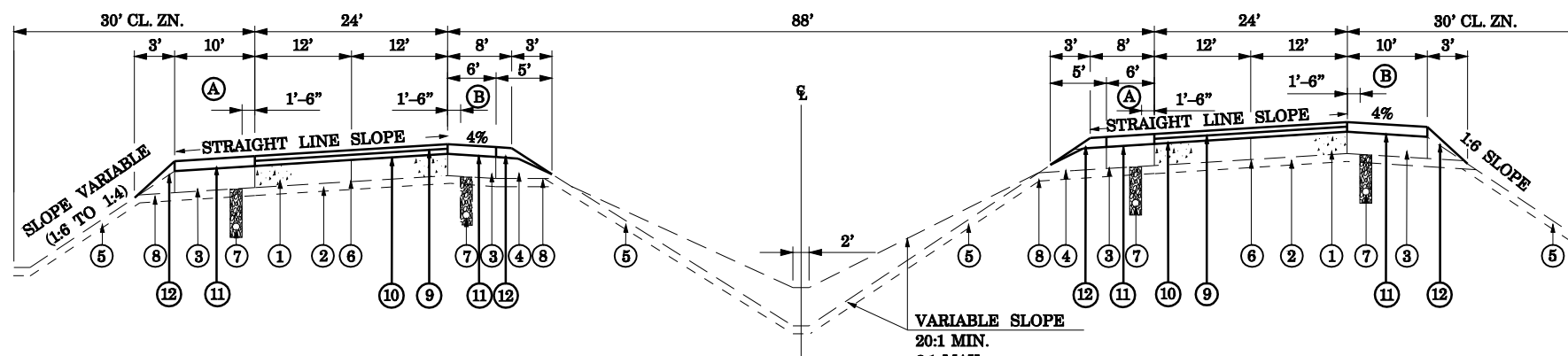
SECTION (1-6)RS\*\*

STA. 106+18.75 TO STA. 110+12.03  
 STA. 134+68.70 TO STA. 171+76.69  
 STA. 198+15.55 TO STA. 211+75.52

STA. 232+04.10 TO STA. 253+13.83  
 STA. 271+44.20 TO STA. 288+81.55 (BK)

STA. 0+00.00 (AH) TO STA. 7+06.89  
 STA. 26+97.79 TO STA. 94+15.53  
 STA. 113+81.52 TO STA. 202+19.11  
 STA. 223+16.77 TO STA. 246+56.66 (N.B.), STA. 246+81.46 (S.B.)

STA. EQTN: STA. 288+81.55(BK) = STA. 0+00.00(AH)



**FAI 172 - SUPERELEVATED SECTION**

SECTION (1-5)RS-1

SECTION (1-6)RS\*\*

STA. 75+24.00 (S.B.) TO STA. 106+18.75  
 STA. 78+25.00 (N.B.) TO STA. 106+18.75  
 STA. 110+12.03 TO STA. 134+68.70  
 STA. 171+76.69 TO STA. 198+15.55

STA. 211+75.52 TO STA. 232+04.10  
 STA. 253+13.83 TO STA. 271+44.20

NOTE: SEE CURVE DATA ON PLAN SHEETS FOR SE RATE; AND SE TRANSITIONS LOCATIONS. SHOWN ABOVE IS FOR CURVES GOING TO THE LEFT AND WILL HAVE TO BE ADJUSTED APPROPRIATELY FOR THE CURVES GOING TO THE RIGHT.

STA. 7+06.89 TO STA. 26+97.79  
 STA. 9+09.22 TO STA. 24+95.47  
 STA. 94+15.53 TO STA. 113+81.52  
 STA. 95+36.18 TO STA. 112+60.87  
 STA. 202+19.11 TO STA. 223+16.77  
 STA. 203+39.76 TO STA. 221+96.12

**LEGEND**

- ① EXISTING - 8" CONTINUOUSLY REINFORCED P.C. CONCRETE PAVEMENT/8" STD REINFORCED PCC PAVEMENT ON RAMPS (OUTSIDE OF GORE AREAS)
- ② EXISTING - 4" STABILIZED SUB-BASE
- ③ EXISTING - 8" STABILIZED SHOULDERS (BITUMINOUS AGGREGATE MIXTURE)
- ④ EXISTING - AGGREGATE SHOULDER TYPE "A"
- ⑤ EXISTING - 4" TOP SOIL
- ⑥ EXISTING - LONGITUDINAL JOINT
- ⑦ EXISTING - PIPE UNDERDRAINS
- ⑧ EXISTING - 4" SUB-BASE GRANULAR MATERIAL TYPE "C" FILL SECTION ONLY
- ⑨ PROPOSED - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE MIX D N70 - 1 1/2"
- ⑩ PROPOSED - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 19.0 N70 - 2 1/4"
- ⑪ PROPOSED - HOT-MIX ASPHALT SHOULDERS WRUMBLE STRIPS (STD 642001), 3 3/4"
- ⑫ PROPOSED - AGGREGATE SHOULDER TYPE "B"

**\*\* BRIDGE RESURFACING OMISSION**

SECTION (1-6)RS

STA. 159+02.43 TO STA. 161+91.32 (S.B.)  
 STA. 160+68.82 TO STA. 163+57.36 (N.B.)

**SHOULDER SLOPE DESCRIPTION**

- Ⓐ LOW SIDE OF SE: SLOPE SAME AS PAVEMENT SUPERELEVATION BUT NOT LESS THAN 4.0%.
- Ⓑ HIGH SIDE OF SE: WHEN THE SUPERELEVATION OF THE PAVEMENT EXCEEDS 4.0%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8.0%.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

PROPOSED  
 TYPICAL SECTIONS  
 I-172

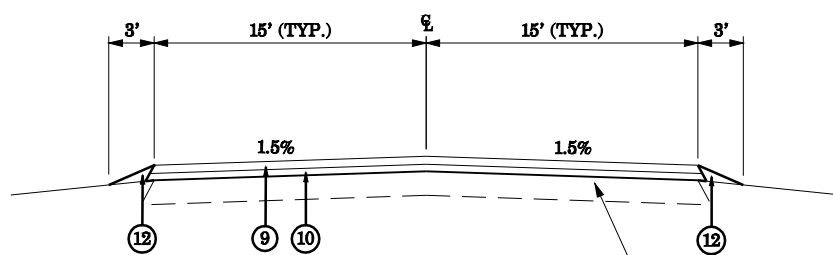
SCALE: VERT. NONE  
 HORIZ. NONE  
 DATE 04082004

DRAWN BY LLQ  
 CHECKED BY WJB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	35
STA.		TO STA.		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

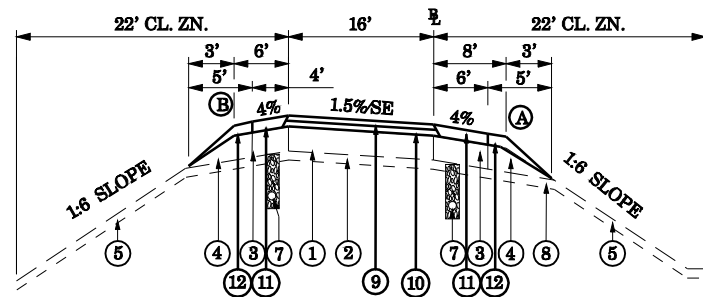
(1-5)RS-1; (1-6)RS

CONTRACT NO. 72694



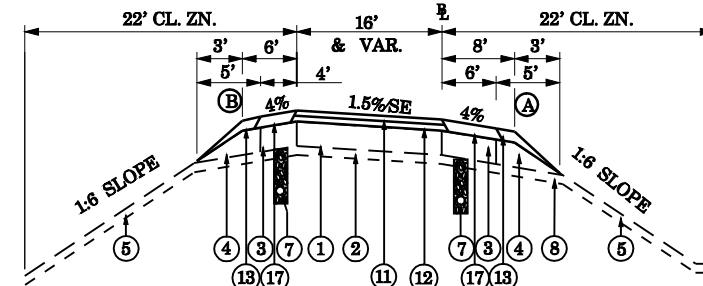
EXISTING AGGREGATE  
BASE COURSE, TYPE A, 8"  
OR BIT. CONC. OVERLAY

**PROPOSED MEDIAN CROSSOVER**



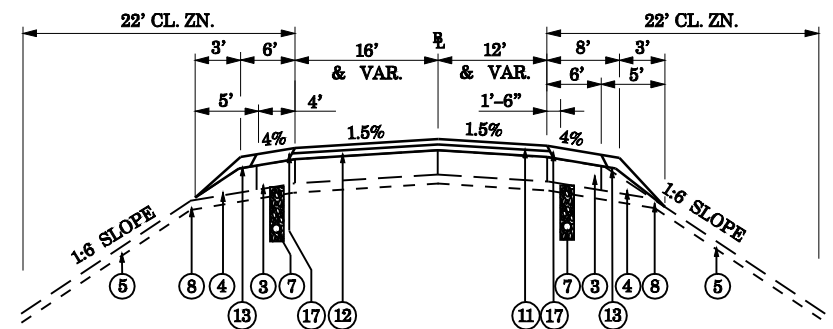
**RAMP TANGENT SECTION  
IL ROUTE 104**

RAMP A: STA. 0+00.00 TO STA. 19+29.06  
RAMP B: STA. 1+51.78 TO STA. 17+24.17  
RAMP C: STA. 0+00.00 TO STA. 18+30.09  
RAMP D: STA. 1+54.00 TO STA. 16+10.59



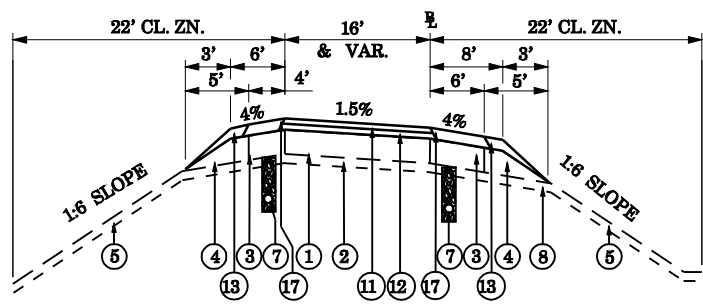
**TR 349 (COLUMBUS ROAD)  
RAMP**

RAMP A: STA. 0+00.00 TO STA. 24+12.19  
RAMP B: STA. 0+32.00 TO STA. 20+99.21  
RAMP C: STA. 0+00.00 TO STA. 23+27.75  
RAMP D: STA. 0+32.00 TO STA. 21+10.32



**US 24 RAMP  
TYPICAL SECTION**

RAMP A: STA. 17+00.00 TO STA. 20+50.00



**US 24 RAMP**

RAMP A: STA. 00+00.00 TO STA. 17+00.00  
RAMP D: STA. 00+24.00 TO STA. 21+13.93

NOTE: SEE CURVE DATA ON PLAN SHEETS FOR SE RATE; AND SE TRANSITIONS LOCATIONS SHOWN ABOVE IS FOR CURVES GOING TO THE RIGHT AND WILL HAVE TO BE ADJUSTED APPROPRIATELY FOR THE CURVES GOING TO THE LEFT.

NO PROPOSED RUMBLE STRIPS ON RAMP SHOULDERS AND COLUMBUS ROAD.

SEE HMA RAMP LOCATIONS AND DETAILS WHERE IT VARIES FROM TYPICAL SECTIONS

**LEGEND**

- ① EXISTING - 8" CONTINUOUSLY REINFORCED P.C. CONCRETE PAVEMENT/8" STD REINFORCED PCC PAVEMENT ON RAMP (OUTSIDE OF GORE AREAS)
- ② EXISTING - 4" STABILIZED SUB-BASE
- ③ EXISTING - 8" STABILIZED SHOULDERS (BITUMINOUS AGGREGATE MIXTURE)
- ④ EXISTING - AGGREGATE SHOULDER TYPE "A"
- ⑤ EXISTING - 4" TOP SOIL
- ⑥ EXISTING - LONGITUDINAL JOINT
- ⑦ EXISTING - PIPE UNDERDRAINS
- ⑧ EXISTING - 4" SUB-BASE GRANULAR MATERIAL TYPE "C" FILL SECTION ONLY
- ⑨ PROPOSED - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE MIX D N70 - 1 1/2"
- ⑩ PROPOSED - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL 19.0 N70 - 2 1/4"
- ⑪ PROPOSED - HOT-MIX ASPHALT SHOULDERS WRUMBLE STRIPS (STD 642001), 3 3/4"
- ⑫ PROPOSED - AGGREGATE SHOULDER TYPE "B"

**SHOULDER SLOPE DESCRIPTION**

- Ⓐ LOW SIDE OF SE: SLOPE SAME AS PAVEMENT SUPERELEVATION BUT NOT LESS THAN 4.0%.
- Ⓑ HIGH SIDE OF SE: WHEN THE SUPERELEVATION OF THE PAVEMENT EXCEEDS 4.0%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8.0%.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

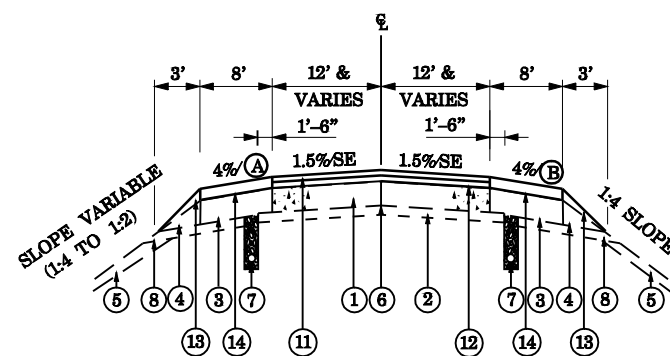
**PROPOSED  
TYPICAL SECTIONS  
I-172**

SCALE: VERT. NONE  
HORIZ. NONE  
DATE 04082004

DRAWN BY LLQ  
CHECKED BY WJS

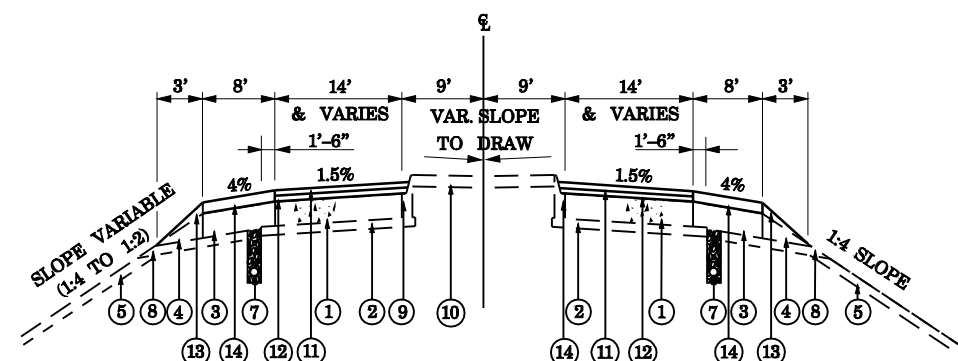
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	36
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS FED. AID PROJECT		

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



**FAS 1604 /TR 349 (COLUMBUS ROAD)**

STA. 35+48.85 TO STA. 39+11.08  
 STA. 61+54.48 TO STA. 64+75.00



**NORMAL SECTION**  
**FAS 1604 /TR 349 (COLUMBUS ROAD)**

STA. 39+11.08 TO STA. 47+25.28  
 STA. 52+71.36 TO STA. 61+54.48

NOTE: SEE CURVE DATA ON PLAN SHEETS FOR SE RATE; AND SE TRANSITIONS LOCATIONS. SHOWN ABOVE IS FOR CURVES GOING TO THE RIGHT AND WILL HAVE TO BE ADJUSTED APPROPRIATELY FOR THE CURVES GOING TO THE LEFT.

NO PROPOSED RUMBLE STRIPS ON RAMP SHOULDERS AND COLUMBUS ROAD.

SEE HMA RAMP LOCATIONS AND DETAILS WHERE IT VARIES FROM TYPICAL SECTIONS

**LEGEND**

- ① EXISTING - 8" STANDARD REINFORCED P.C. CONCRETE PAVEMENT
- ② EXISTING - 4" STABILIZED SUB-BASE
- ③ EXISTING - 8" STABILIZED SHOULDERS (BITUMINOUS AGGREGATE MIXTURE)
- ④ EXISTING - AGGREGATE SHOULDER TYPE "A"
- ⑤ EXISTING - 4" TOP SOIL
- ⑥ EXISTING - LONGITUDINAL JOINT
- ⑦ EXISTING - PIPE UNDERDRAINS
- ⑧ EXISTING - 4" SUB-BASE GRANULAR MATERIAL TYPE "C" FILL SECTION ONLY
- ⑨ EXISTING - CONCRETE CURB & GUTTER, TYPE M6.06, TO BE REMOVED BOTH SIDES OF MEDIAN
- ⑩ EXISTING - PCC MEDIAN SURFACE 4"
- ⑪ PROPOSED - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE MIX D N70 - 1 1/2"
- ⑫ PROPOSED - LEVELING BINDER (MACHINE METHOD), N70 - 1"
- ⑬ PROPOSED - AGGREGATE SHOULDER TYPE "B"
- ⑭ PROPOSED - HOT-MIX ASPHALT SHOULDERS, 2 1/2"

**SHOULDER SLOPE DESCRIPTION**

- Ⓐ LOW SIDE OF SE: SLOPE SAME AS PAVEMENT SUPERELEVATION BUT NOT LESS THAN 4.0%.
- Ⓑ HIGH SIDE OF SE: WHEN THE SUPERELEVATION OF THE PAVEMENT EXCEEDS 4.0%, THE SHOULDER SHALL BE SLOPED SO THAT THE ALGEBRAIC DIFFERENCE BETWEEN PAVEMENT AND SHOULDER WILL NOT BE GREATER THAN 8.0%.

**TR 349**  
**BRIDGE RESURFACING OMISSION**

STA. 48+25.28 TO STA. 51+71.36

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 TYPICAL SECTIONS  
 FAS 1C  
 TR 349 (COLUMBUS ROAD)

SCALE: VERT. NONE  
 HORIZ. NONE  
 DATE 04092004  
 DRAWN BY LLQ  
 CHECKED BY WJS

ROUTE: FAI 172  
SECTION: 1-5RS-1, 1-6RS; (1-6HB-1)I  
COUNTY: ADAMS COUNTY (DISTRICT 6)

JOB NO.: D-96-006-02  
CONTRACT NO.: 72694

STORM WATER POLLUTION PREVENTION PLAN

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	37
STA.		TO STA.		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

• (1-5RS-1; 1-6RS)  
**CONTRACT NO. 72694**

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISION OF THE NPDES PERMIT NUMBER ILR10 ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRE OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION SUBMITTED, IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE, AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

*Rog. Z. Smith*  
(SIGNATURE)

2-11-00  
(DATE)

*Deputy Director / Legal 484*  
(TITLE)

NOTE: THE ABOVE BOXED IN AREA WILL BE FILLED OUT BY IDOT - CONSTRUCTION AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT.

THE FOLLOWING PLAN WAS ESTABLISHED AND INCLUDED IN THESE PLANS TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE UNDER NPDES. THE CONTRACTOR SHALL ABIDE TO ALL REQUIREMENTS WITHIN THIS PLAN AS PART OF THE CONTRACT.

THE PURPOSE OF THIS PLAN IS TO PREVENT / MINIMIZE SILTATION WITHIN THE CONSTRUCTION ZONE AND TO ELIMINATE SEDIMENTS FROM ENTERING AND LEAVING THE CONSTRUCTION ZONE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE TIME.

CERTAIN ITEMS, AS SHOWN IN THIS PLAN AND REFERENCED BY THE LEGEND, SHALL BE PLACED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE PLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION RESULTING FROM THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF THE YEAR AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL PLACE PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A REASONABLE AMOUNT OF TIME; THEREFORE, REDUCING THE AMOUNT OF AREA BEING OPEN TO THE POSSIBILITY OF EROSION AND REDUCING THE AMOUNT OF TEMPORARY EROSION CONTROL SYSTEMS AND TEMPORARY SEEDING. THE RESIDENT ENGINEER WILL DETERMINE IF TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED, THE SIZE OF THE PROPOSED DITCH CHECKS, THE PROPER METHOD OF INSTALLATION, AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS SHALL BE ADDED WHICH ARE NOT INCLUDED IN THIS PLAN. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN IN SPECIAL DETAILS AND IN STANDARD 280001 OF THE PLANS.

THE SPECIAL PROVISIONS TEMPORARY SEEDING, TEMPORARY EROSION CONTROL SEEDING AND TEMPORARY EROSION CONTROL ADDITIONALLY SUPPLEMENT THIS PLAN.

SITE DESCRIPTION

DESCRIPTION OF CONSTRUCTION ACTIVITY:

1. THE PROPOSED PROJECT CONSISTS OF THE REHABILITATION OF I-172 FROM JUST NORTH OF IL RTE 96 TO THE US 24 OVERPASS. IT ALSO INCLUDES ALL OF THE RAMPS ALONG THE INTERSTATE AND SHORT SECTIONS OF TR 349 (COLUMBUS ROAD) AND US 24.
2. CONSTRUCTION CONSISTS OF DRAINAGE STRUCTURE IMPROVEMENTS, OPEN FLOW DRAINAGE IMPROVEMENTS, PCC PATCHING, HMA RESURFACING, GUARDRAIL REPLACEMENTS, AND OTHER MISCELLANEOUS WORK TO COMPLETE THE IMPROVEMENTS.

DESCRIPTION OF INTENDED SEQUENCE OF MAJOR CONSTRUCTION ACTIVITIES WHICH WILL DISTURB EARTH AND LEAD TO POSSIBLE EROSION FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE:

1. TREE REMOVAL WILL BE MINIMAL. TREES TO REMAIN WILL BE PROTECTED AGAINST DAMAGE. GRADING AND SHAPING DITCHES WILL BE ONLY IN AREAS DESIGNATED ON THE PLANS.
2. ADDITIONAL EXCAVATION IS ANTICIPATED FOR STONE DUMPED RIPRAP.
3. DRAINAGE STRUCTURES WILL BE INSTALLED BEFORE AND/OR DURING THE GRADING AND SHAPING OF DITCHES TO ALLOW PROPER DRAINAGE ACROSS OR ALONG THE INTERSTATE.
4. PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL, SUCH AS CHECKS, RIPRAP DITCH CHECKS, TEMPORARY SEEDING, ETC.
5. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS RIPRAP DITCH LINING, SEEDING, ETC.
6. FINAL PAVING AND OTHER MISCELLANEOUS ITEMS.

AREA OF CONSTRUCTION SITE:

THE TOTAL DRAINAGE AREA ENTERING AND INCLUDING THE CONSTRUCTION SITE IS ESTIMATED TO BE 285.00± ACRES IN WHICH 7.88 ACRES WILL BE DISTURBED BY GRADING OR OTHER ACTIVITIES.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THIS STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION ON THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM FIELD REVIEWS WHICH WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. EXISTING PLANS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES WERE CONTAINED IN THE PROJECT DESIGN REPORT. THE PROJECT PLAN DOCUMENTS WERE UTILIZED FOR PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES RECEIVING WATER FROM THIS CONSTRUCTION SITE:

1. MILL CREEK

CONTROLS - EROSION CONTROLS AND SEDIMENT CONTROLS

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

1. THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, MULCHING, PROTECTION OF TREES, PRESERVATION OF MATURE VEGETATION AND OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

(A) AREAS OF EXISTING VEGETATION (WOODS AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION SLOPE LIMITS SHALL BE IDENTIFIED FOR PRESERVING AND SHALL BE PROTECTED FROM MOWING, BRUSH CUTTING, TREE REMOVAL AND OTHER ACTIVITIES WHICH WOULD BE DETRIMENTAL TO THEIR MAINTENANCE AND DEVELOPMENT.

(B) DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER, ALONG WITH REQUIRED TREE REMOVAL.

(C) AS SOON AS REASONABLE ACCESS IS AVAILABLE (SUCH AS TREES CLEARED) TO ALL LOCATIONS WHERE WATER DRAINS AWAY FROM THE PROJECT, SEDIMENT BASINS, RIPRAP DITCH CHECKS, TEMPORARY DITCH CHECKS, INLET AND PIPE PROTECTION, AND/OR EROSION CONTROL FENCE SHALL BE INSTALLED AS CALLED OUT IN THIS PLAN AND DIRECTED BY THE ENGINEER.

(D) BARE AND SPARSELY VEGETATED GROUND IN HIGHLY ERODABLE AREAS, AS DETERMINED BY THE ENGINEER, SHALL BE TEMPORARILY SEEDED AT THE BEGINNING OF CONSTRUCTION WHERE NO CONSTRUCTION ACTIVITIES ARE IMMEDIATELY EXPECTED, AS STATED IN THE SPECIAL PROVISION "TEMPORARY EROSION CONTROL SEEDING".

(E) IMMEDIATELY AFTER TREE REMOVAL IS COMPLETED, AREAS WHICH ARE HIGHLY ERODABLE AS DETERMINED BY THE ENGINEER, SHALL BE TEMPORARILY SEEDED WHEN NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN DAYS.

(F) AT LOCATIONS WHERE A SIGNIFICANT AMOUNT OF WATER DRAINS INTO THE CONSTRUCTION ZONE FROM OUTSIDE AREAS (ADJACENT LANDOWNERS), EROSION CONTROL FENCE, TEMPORARY DITCH CHECKS, OR RIPRAP DITCH CHECKS WILL BE UTILIZED TO LOCALLY DIVERT WATER, REDUCE FLOW RATES, AND COLLECT OUTSIDE SILTATION INSIDE THE RIGHT-OF-WAY LINE. EROSION CONTROL ITEMS WILL NOT BE ALLOWED TO BE INSTALLED TO CAUSE FLOODING TO UPSTREAM PRIVATE PROPERTY WHICH COULD CAUSE CROP DAMAGES OR OTHER UNDESIRABLE CONDITIONS.

2. ESTABLISHMENT OF THESE TEMPORARY EROSION CONTROL MEASURES WILL HAVE ADDITIONAL BENEFITS TO THE PROJECT. DESIRABLE GRASS SEED WILL BECOME ESTABLISHED IN THESE AREAS AND WILL SPREAD SEEDS ONTO THE CONSTRUCTION SITE UNTIL PERMANENT SEEDING/MOWING AND OVERSEEDING CAN BE COMPLETE.
3. A THIRD BENEFIT OF THESE FILTER AREAS IS THAT THEY WILL BEGIN TO PROVIDE A SCREEN AND BUFFER. THEY WILL HELP PROTECT THE CONSTRUCTION SITE FROM WINDS AND EXCESS SUN AND MITIGATE CONSTRUCTION NOISE AND DUST.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**STORM WATER POLLUTION PREVENTION PLAN**  
SCALE: VERT.      DRAWN BY LLQ  
          HORIZ.      CHECKED BY  
DATE: 06/16/2004

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	1-5RS-1	ADAMS	108	38
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS	FED. AID PROJECT	

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**

**DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:**

1. DURING ROADWAY CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION SLOPE LIMITS, AS OUTLINED PREVIOUSLY HEREIN, SHALL BE PROTECTED FROM DAMAGING EFFECTS OF CONSTRUCTION. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESIGNATED ON THE PLANS OR DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.
  - (A) WITHIN THE CONSTRUCTION LIMITS ARE AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER. THESE AREAS SHALL REMAIN UNDISTURBED UNTIL FULL CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
  - (B) TOPSOIL AND EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
  - (C) AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
    - I. PLACE TEMPORARY EROSION CONTROL FACILITIES AS SHOWN ON THE PLANS.
    - II. TEMPORARILY SEED ERODABLE BARE EARTH ON A WEEKLY BASIS TO MINIMIZE THE AMOUNT OF ERODABLE SURFACE AREA WITHIN THE CONSTRUCTION LIMITS.
    - III. CONSTRUCT ROADSIDE DITCHES AND PROVIDE TEMPORARY EROSION CONTROL SYSTEMS.
    - IV. TEMPORARILY DIVERT WATER AROUND PROPOSED CULVERT LOCATIONS.
    - V. BUILD NECESSARY EMBANKMENT AT CULVERT LOCATIONS AND THEN EXCAVATE AND PLACE CULVERT.
    - VI. CONTINUE BUILDING UP THE EMBANKMENT TO THE PROPOSED GRADE WHILE AT THE SAME TIME PLACE PERMANENT EROSION CONTROL SUCH AS RIPRAP DITCH LINING AND CONDUCT FINAL SHAPING TO THE SLOPES.
  - (D) THE CONTRACTOR SHALL IMMEDIATELY FOLLOW MAJOR EARTH MOVING OPERATIONS WITH FINAL GRADING EQUIPMENT. AFTER THE MAJOR EARTH SPREAD OPERATION HAS MOVED TO A NEW LOCATION, FINAL GRADING SHALL BE COMPLETED WITHIN FOURTEEN DAYS. IF GRADING IS NOT COMPLETED WITHIN FOURTEEN DAYS, ALL MAJOR EARTH MOVING OPERATIONS WILL BE STOPPED, AS DIRECTED BY THE ENGINEER, UNTIL DISTURBED AREAS ARE FINAL GRADED AND SEEDED.
  - (E) EXCAVATED AREAS AND EMBANKMENTS SHALL BE PERMANENTLY SEEDED WHEN FINAL GRADED. IF NOT, THEY SHALL BE TEMPORARILY SEEDED IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR 7 DAYS.
  - (F) CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTION RUN-OFF IN COMPLIANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
  - (G) THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT DAILY DURING ACTIVITIES; AND WEEKLY OR AFTER LARGE RAINS DURING THE WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE CONSTRUCTION FIELD ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT EROSION CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.
  - (H) SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON THE SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE WILL BE INCLUDED IN THE UNIT BID PRICE FOR EARTH EXCAVATION.
  - (I) THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COSTS OF THIS REMOVAL SHALL BE INCLUDED IN THE UNIT BID PRICE FOR THE TEMPORARY EROSION CONTROL SYSTEM. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

**DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:**

1. TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED WITH A PROPER STAND.
2. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED, TEMPORARY RIPRAP DITCH CHECKS WILL BE ALLOWED TO REMAIN IN PLACE WHERE APPROVED BY THE ENGINEER.

**MAINTENANCE AFTER CONSTRUCTION:**

1. CONSTRUCTION IS COMPLETE AFTER ACCEPTANCE BY I.D.O.T. MAINTENANCE UP TO THIS DATE WILL BE BY THE CONTRACTOR.
2. AREAS WILL BE INSPECTED ON A REGULAR BASIS BY IDOT DISTRICT 6 BUREAU OF OPERATIONS.
3. MAINTENANCE CREWS WILL PERFORM REGULAR MOWINGS TO AID IN KEEPING WEEDS DOWN AND ESTABLISHING A GOOD ROADSIDE SEED STAND.
4. MAINTENANCE CREWS WILL ALSO AID IN ANY DITCH LINING MAINTENANCE OR IN ANY DRAINAGE PROBLEMS.
5. ALL MAINTENANCE WILL BE CONDUCTED AT TIMES WHEN WEATHER CONDITIONS WILL NOT CAUSE SITE DAMAGE.

**CONTRACTOR CERTIFICATION STATEMENT**

THIS CERTIFICATION STATEMENT IS PART OF THE STORM WATER POLLUTION PLAN FOR THE PROJECT DESCRIBED BELOW IN ACCORDANCE WITH NPDES PERMIT NO. ILR10 \_\_\_\_\_, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY ON \_\_\_\_\_.

ROUTE: FAI 172 MARKED: I-172  
 SECTION: 1-5RS-1, 1-6RS, (1-6HB-1)I JOB NO.: D-96-561-02  
 COUNTY: ADAMS COUNTY CONTRACT NO.: 72694

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND THE TERMS OF THE GENERAL NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT THAT AUTHORIZES THE STORM WATER DISCHARGES ASSOCIATED WITH INDUSTRIAL ACTIVITY FROM THE CONSTRUCTION SITE IDENTIFIED AS PART OF THIS CERTIFICATION.

SIGNATURE \_\_\_\_\_ DATE \_\_\_\_\_  
 TITLE \_\_\_\_\_  
 NAME OF FIRM \_\_\_\_\_ PHONE NO. \_\_\_\_\_  
 STREET ADDRESS \_\_\_\_\_  
 CITY, STATE, ZIP \_\_\_\_\_

NOTE: THE ABOVE BOXED IN AREA SHALL BE FILLED OUT BY THE CONTRACTOR AFTER THE AWARD OF THE CONTRACT TO OBTAIN THE REQUIRED NPDES PERMIT FROM IEPA.

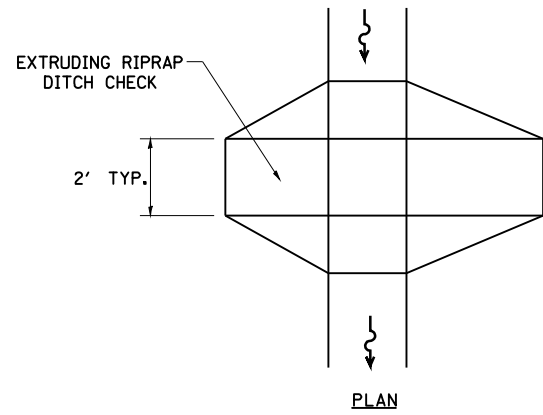
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

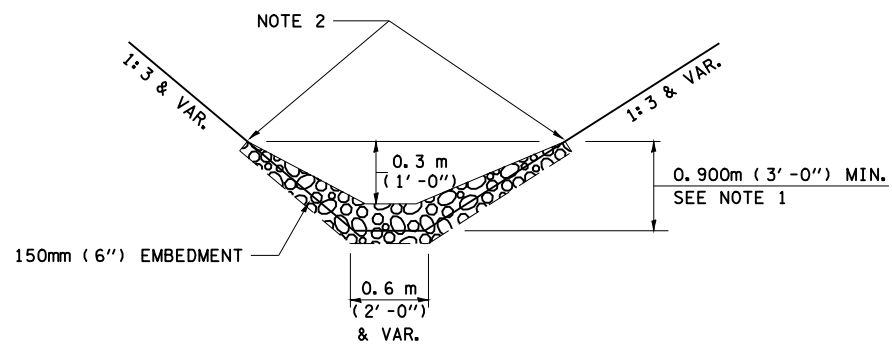
**STORM WATER POLLUTION PREVENTION PLAN**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 06/16/2004

DRAWN BY LLO  
 CHECKED BY \_\_\_\_\_



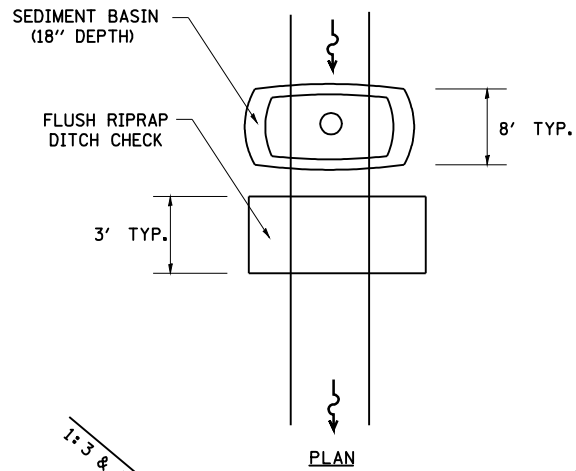
PLAN



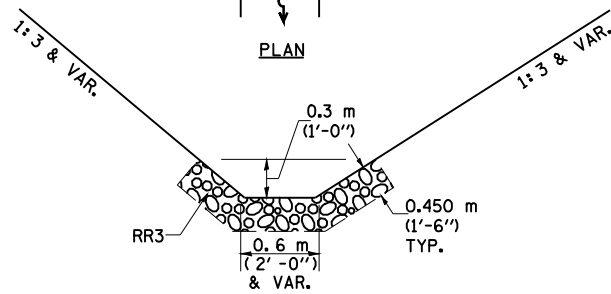
ELEVATION

OPTION 1

(EXTRUDING DITCH CHECK)  
RECOMMENDED FOR AREAS  
W/ RIPRAP DITCH LINING



PLAN



ELEVATION

OPTION 2

(FLUSH DITCH CHECK)  
RECOMMENDED FOR AREAS  
W/O RIPRAP DITCH LINING

**STONE DUMPED RIPRAP DITCH CHECK**  
(TYPICAL & OPTIONS 1 & 2  
AS DIRECTED BY THE ENGINEER)

NOTE 1: RIPRAP SHALL EXTEND FAR ENOUGH UP THE SLOPES TO ALLOW 0.3m (1') OVERTOPPING TO AVOID ERODING AROUND THE EDGES OF THE RIPRAP.

NOTE 2: ENDS SHALL BE TIED INTO SLOPES.

**LEGEND FOR STORM WATER POLLUTION PREVENTION PLAN**

ITEM	SYMBOL
AGGREGATE (EROSION CONTROL)	
ESTONE DUMPED RIPRAP DITCH CHECKS: Height = 0.6m (2') ]	
TEMPORARY DITCH CHECKS	
INLET PIPE PROTECTION (I&PP)	
EROSION CONTROL FENCE	
EARTH EXCAVATION FOR EROSION CONTROL (SEDIMENT BASINS)	
PRESERVE EXISTING TREES, WOODLANDS, AND UNDERSTORY (OUTSIDE CONSTRUCTION LIMITS)	
ITEM PLACED AT BEGINNING OF CONSTRUCTION (Requirement)	* ITEM *
ITEM PLACED AS DIRECTED BY ENGINEER (When required by situation)	ITEM
DIRECTION OF OVERLAND FLOW	

**GENERAL NOTES:**

All items shall be constructed as shown on this sheet, on Standard 280001, and as directed by the Engineer.

DITCH CHECKS UTILIZING STRAW BALES SHALL NOT BE USED ON THIS PROJECT. ALL DITCH CHECKS SHALL BE MADE USING AGGREGATE AND WILL BE PAID FOR AS AGGREGATE (EROSION CONTROL) BY THE TON. THE ENGINEER WILL DETERMINE THE LOCATION AND SIZE FOR ALL PROPOSED DITCH CHECKS.

DITCH CHECKS WILL BE REMOVED AT THE COMPLETION OF THE PROJECT AS DIRECTED BY THE ENGINEER.

FILE NAME = SWPPLAN.DGN	USER NAME = laughlinr1	DESIGNED -	REVISED - AUG 2007 (JCN)
et:\pwork\pwork\LAUGHLINRL\dms26563\39 SWPPP_IDOT_REV.dgn		DRAWN - CADD	REVISED -
		CHECKED - JCN	REVISED -
		DATE - APRIL 5, 1999	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

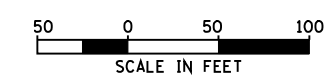
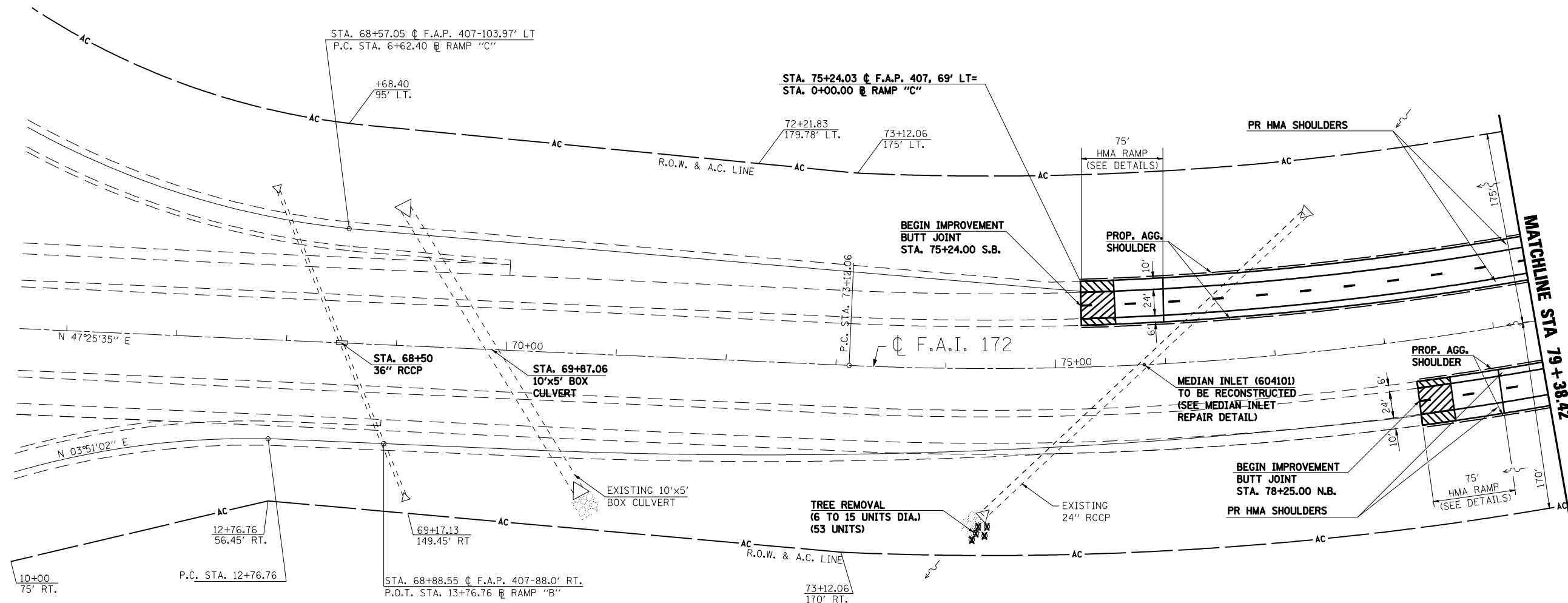
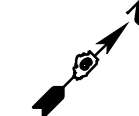
**STORM WATER POLLUTION  
PREVENTION PLAN**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	39
CONTRACT NO. 72694				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	40
STA. 75+00		TO STA. 79+38.42		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 75+00 TO STA. 79+38.42**  
F.A.I. 172  
ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE: 08/09/04  
DRAWN BY: JJS  
CHECKED BY: LLQ

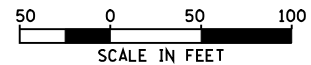
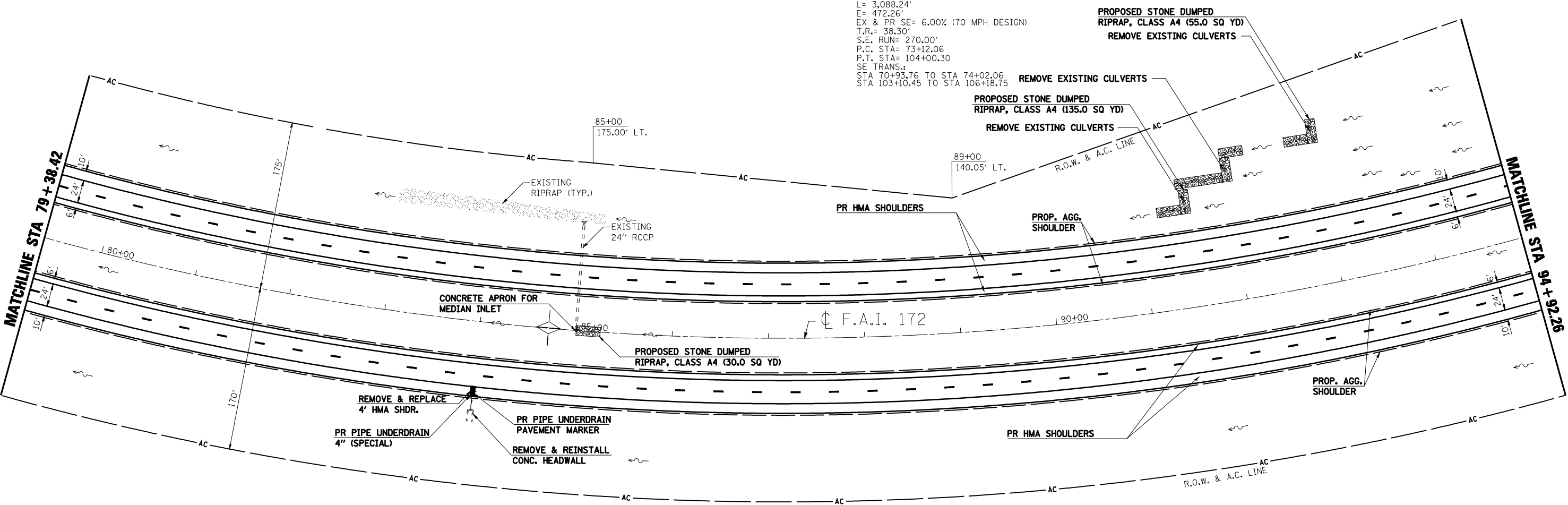


F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	41
STA. 79+38.42		TO STA. 94+92.26		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

**\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694**



CURVE  
P.I. STA= 90+24.84  
Δ= 61°39'37" (LT)  
D= 1°59'48"  
R= 2,869.79'  
T= 1,712.78'  
L= 3,088.24'  
E= 472.26'  
EX & PR SE= 6.00% (70 MPH DESIGN)  
T.R.= 38.30'  
S.E. RUN= 270.00'  
P.C. STA= 73+12.06  
P.T. STA= 104+00.30  
SE TRANS.:  
STA 70+93.76 TO STA 74+02.06  
STA 103+10.45 TO STA 106+18.75



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ✦ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 79+38.42 TO STA. 94+92.26**  
F.A.I. 172  
ADAMS COUNTY  
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 08/09/04  
DRAWN BY JJS  
CHECKED BY LLQ

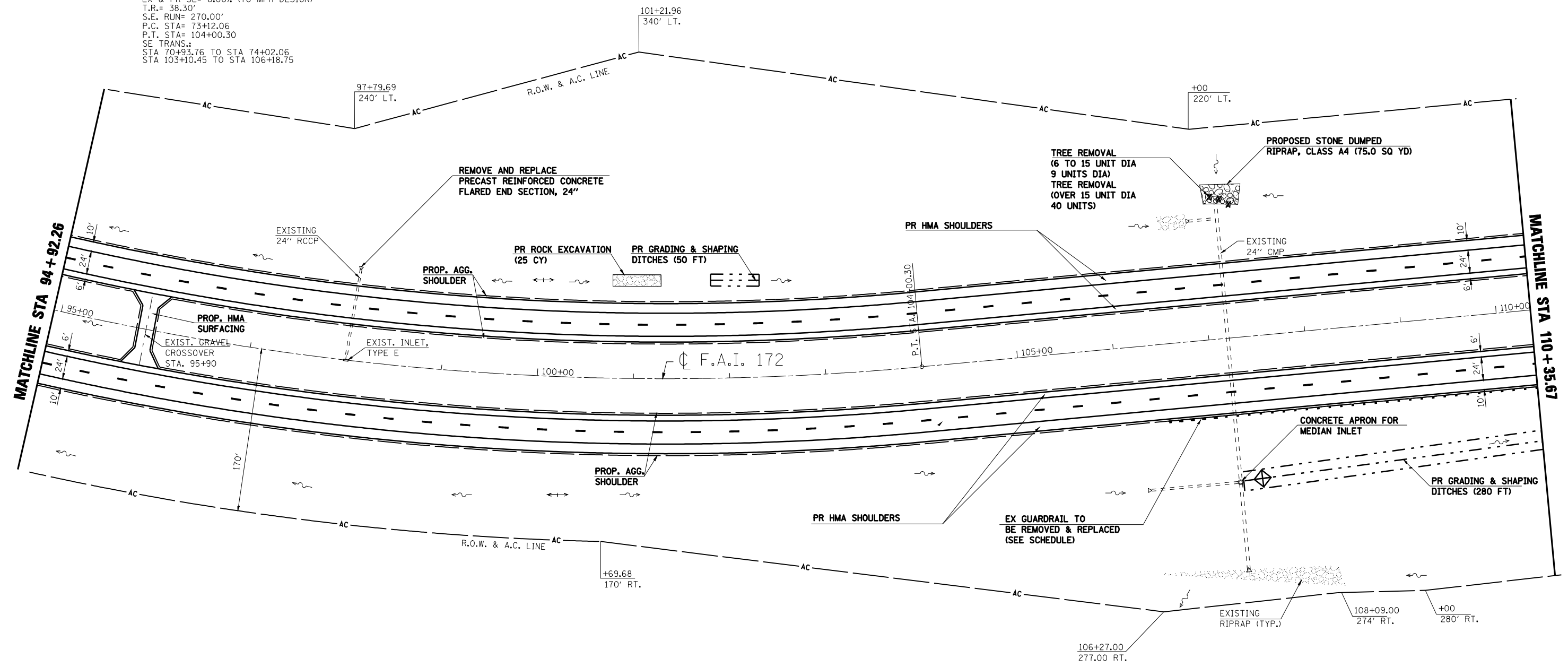
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	42

STA. 94+92.26 TO STA. 110+35.67  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

**\* (1-5)RS-1; (1-6)RS**  
**CONTRACT NO. 72694**

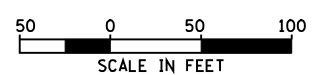


CURVE  
 P.I. STA= 90+24.84  
 $\Delta = 61^{\circ}39'37''$  (LT)  
 D= 1 $^{\circ}59'48''$   
 R= 2,869.79'  
 T= 1,712.78'  
 L= 3,088.24'  
 E= 472.26'  
 EX & PR SE= 6.00% (70 MPH DESIGN)  
 T.R.= 38.30'  
 S.E. RUN= 270.00'  
 P.C. STA= 73+12.06  
 P.T. STA= 104+00.30  
 SE TRANS.:  
 STA 70+93.76 TO STA 74+02.06  
 STA 103+10.45 TO STA 106+18.75



MATCHLINE STA 94+92.26

MATCHLINE STA 110+35.67



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 94+92.26 TO STA. 110+35.67**  
 F.A.I. 172  
 ADAMS COUNTY

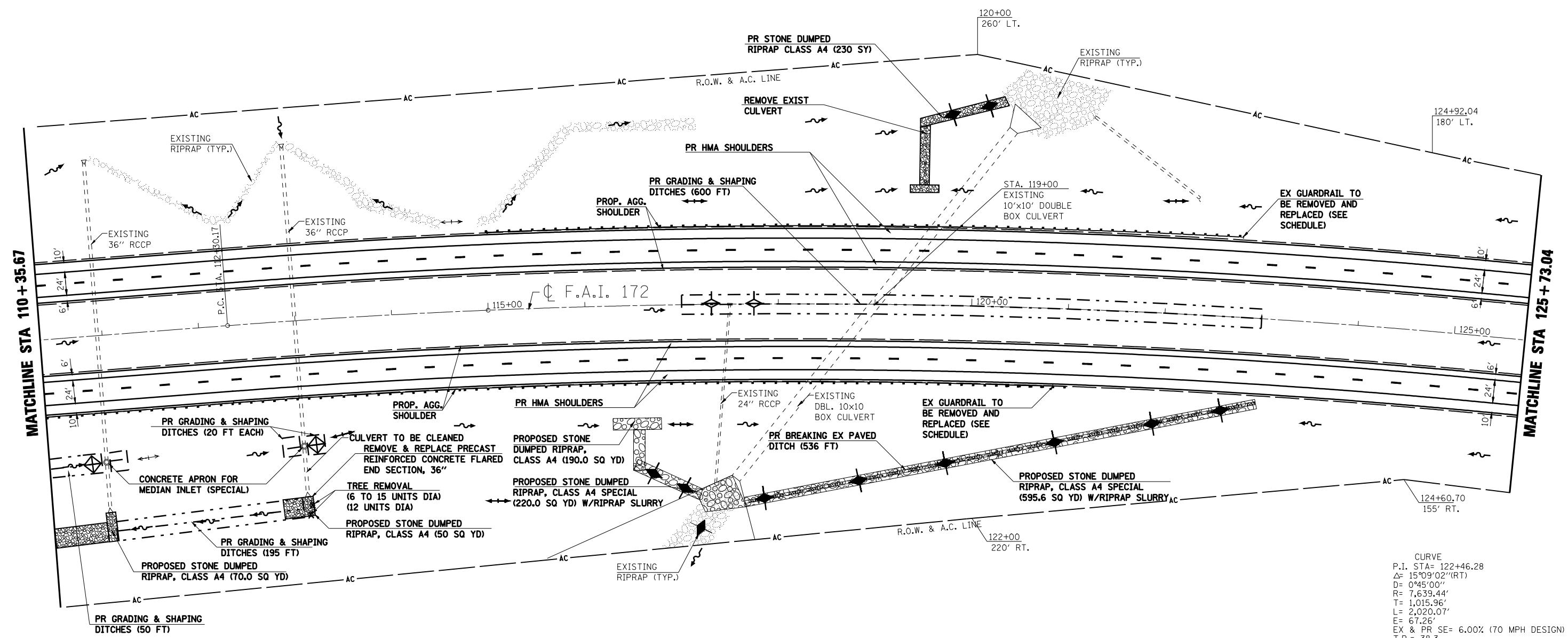
SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 08/09/04

DRAWN BY: JJS  
 CHECKED BY: LLQ

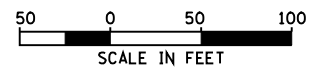
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	43

STA. 110+35.67 TO STA.125+73.04  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



CURVE  
 P.I. STA= 122+46.28  
 $\Delta$ = 15°09'02" (RT)  
 D= 0°45'00"  
 R= 7,639.44'  
 T= 1,015.96'  
 L= 2,020.07'  
 E= 67.26'  
 EX & PR SE= 6.00% (70 MPH DESIGN)  
 T.R.= 38.3  
 S.E. RUN= 270'  
 P.C. STA= 112+30.17  
 P.T. STA= 132+50.22  
 SE TRANS.:  
 STA 110+12.03 TO STA 113+20.33  
 STA 131+60.39 TO STA 134+68.695



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

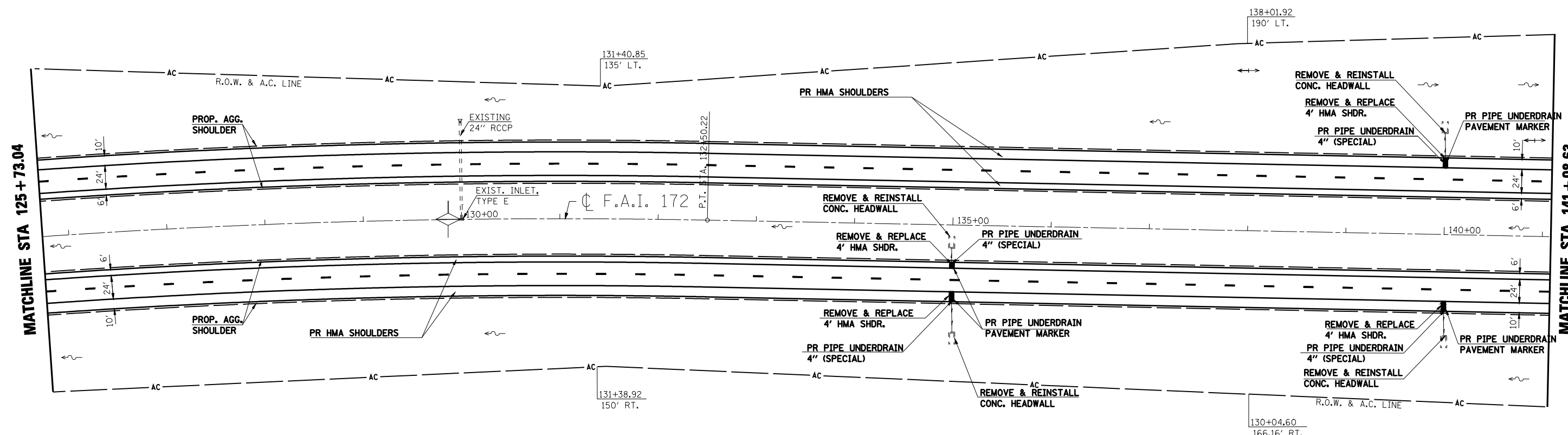
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 110 + 35.67 TO STA. 125 + 73.04**  
 F.A.I. 172  
 ADAMS COUNTY







SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 08/09/04

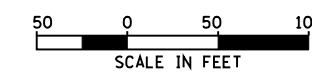
DRAWN BY: JJS  
 CHECKED BY: LLO

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	44
STA. 125+73.04		TO STA. 141+08.63		
FED. ROAD DIST. NO. 6		ILLINOIS FED. AID PROJECT		

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



-  INLET & PIPE PROTECTION
-  HMA SURFACE REMOVAL - BUTT JOINT
-  PCC SURFACE REMOVAL - BUTT JOINT
-  HEAVY DUTY EXCELSIOR BLANKET
-  TEMPORARY DITCH CHECK
-  DITCH FLOW LINE



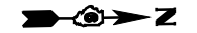
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 125+73.04 TO STA. 141+08.63**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT.      DRAWN BY JJS  
 HORIZ.              CHECKED BY LLQ  
 DATE 08/09/04

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	45
STA. 141+08.63		TO STA.156+43.63		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694



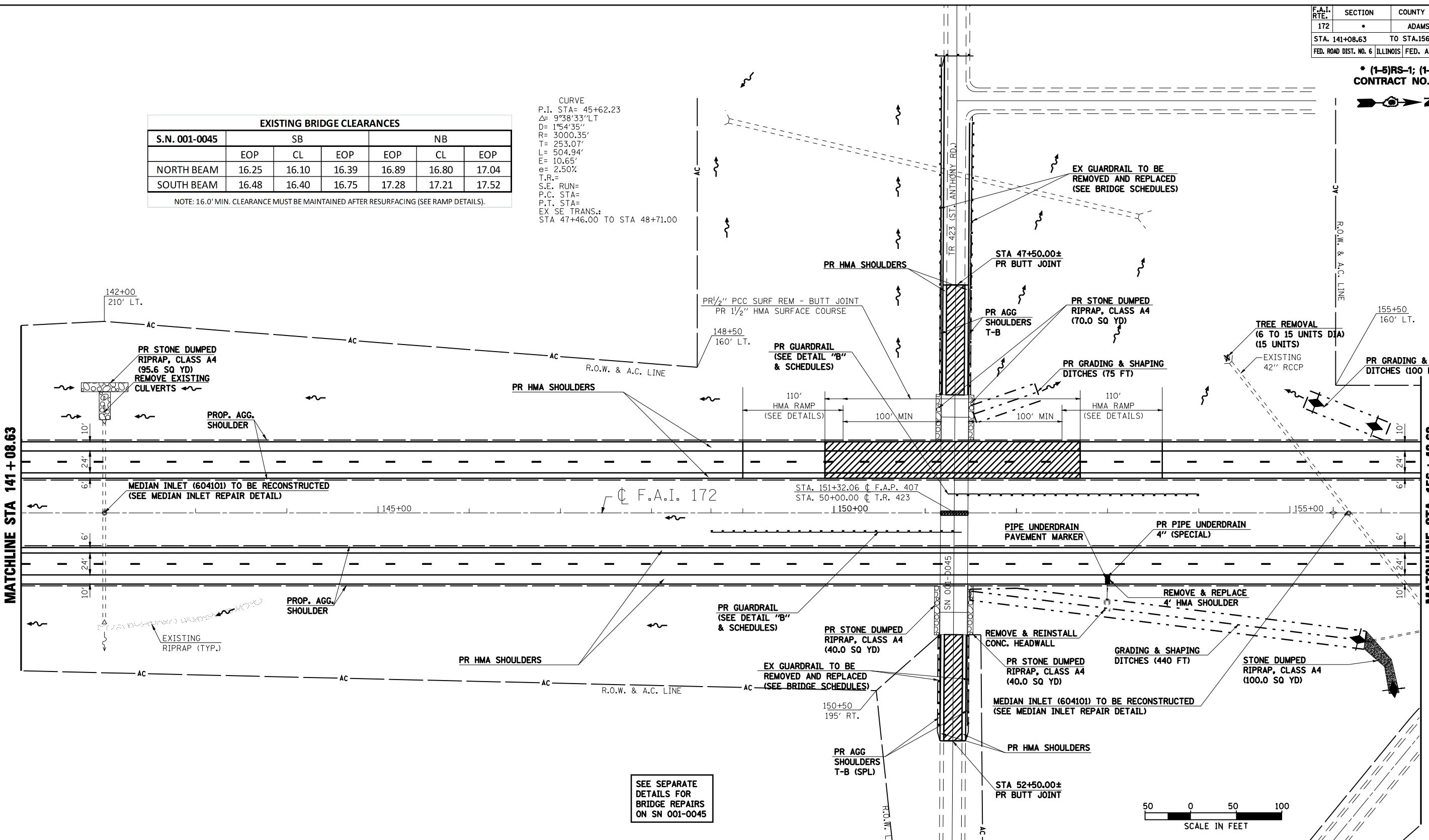
S.N. 001-0045	SB			NB		
	EOP	CL	EOP	EOP	CL	EOP
NORTH BEAM	16.25	16.10	16.39	16.89	16.80	17.04
SOUTH BEAM	16.48	16.40	16.75	17.28	17.21	17.52

NOTE: 16.0' MIN. CLEARANCE MUST BE MAINTAINED AFTER RESURFACING (SEE RAMP DETAILS).

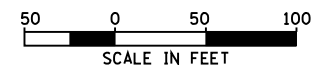
CURVE  
P.I. STA= 45+62.23  
Δ= 9°38'33"LT  
D= 1°54'35"  
R= 3000.35'  
T= 253.07'  
L= 504.94'  
E= 10.65'  
e= 2.50%  
T.R.=  
S.E. RUN=  
P.C. STA=  
P.T. STA=  
EX SE TRANS.:  
STA 47+46.00 TO STA 48+71.00

MATCHLINE STA 141 + 08.63

MATCHLINE STA 156 + 43.63



SEE SEPARATE  
DETAILS FOR  
BRIDGE REPAIRS  
ON SN 001-0045



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

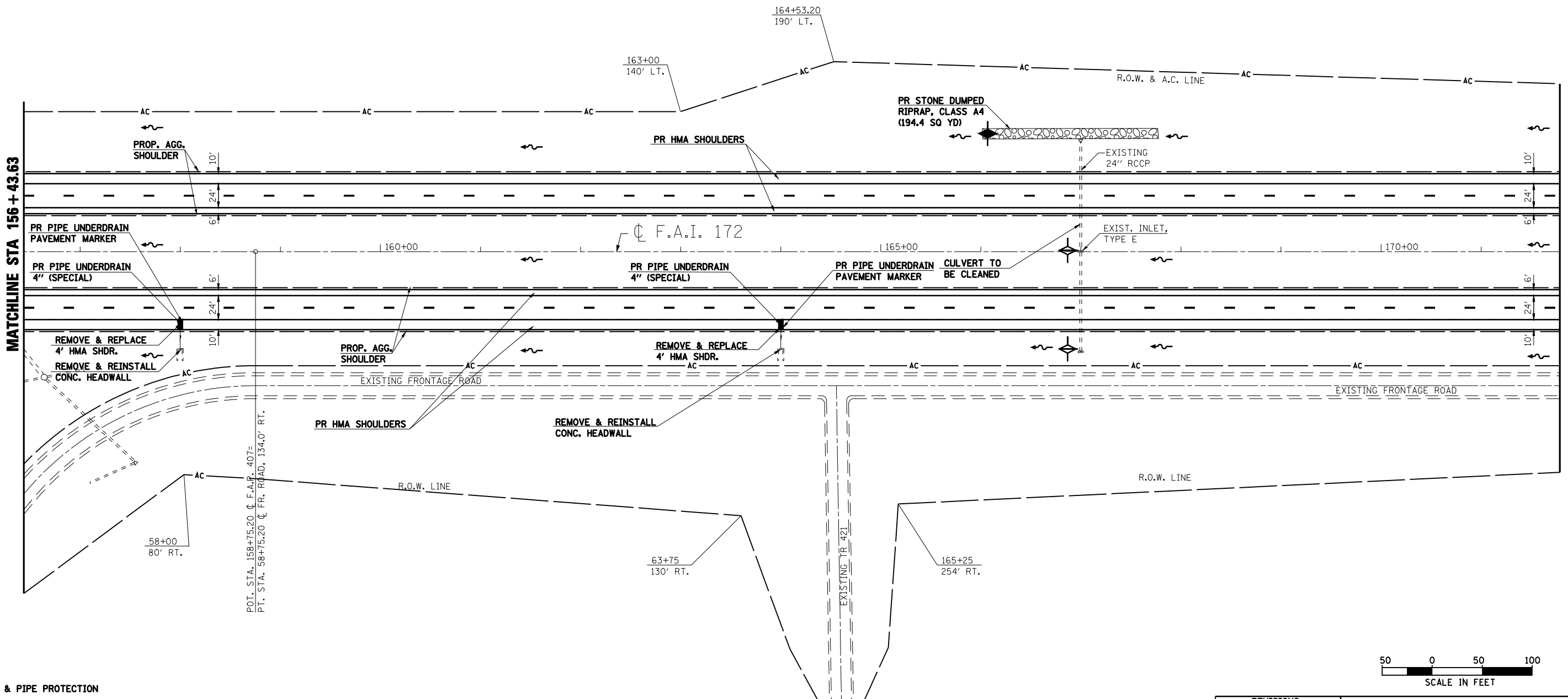
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 141+08.63 TO STA. 156+43.63**  
F.A.I. 172  
ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE: 08/09/04

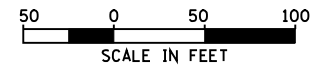
DRAWN BY: JJS  
CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	46
STA. 156+43.63		TO STA. 171+78.63		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



- ◆ INLET & PIPE PROTECTION
- ▨ BITUMINOUS SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ↔ DITCH FLOW LINE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 156+43.63 TO STA. 171+78.63**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/09/04

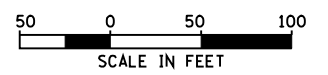
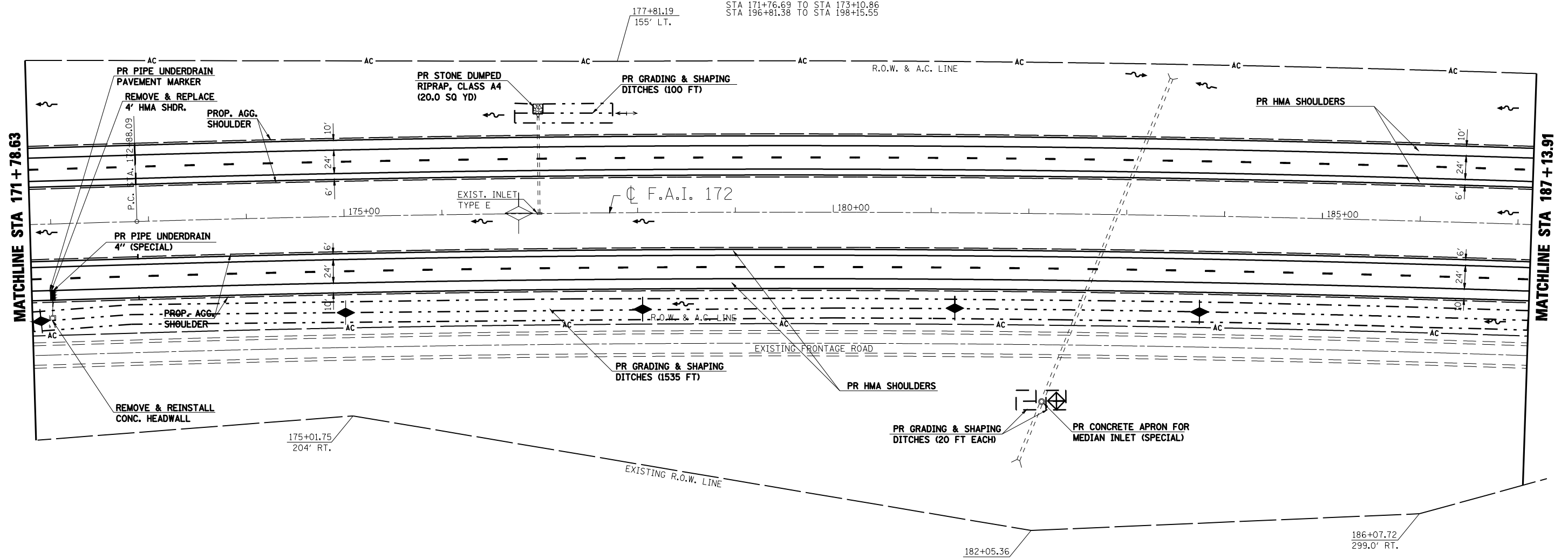
DRAWN BY JJS  
 CHECKED BY LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	47
STA. 171+78.63		TO STA. 187+13.91		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

**\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694**



CURVE  
P.I. STA= 184+97.19  
 $\Delta$ = 6°02'20.57" (RT)  
D= 0°15'00"  
R= 22,918.31'  
T= 1208.93'  
L= 2,415.62'  
E= 31.86'  
EX & PR SE= 1.5%/R.C. (+70 MPH DESIGN)  
T.R.= 66.67'  
S.E. RUN= 67.50'  
P.C. STA= 172+88.09  
P.T. STA= 197+03.71  
SE TRANS.:  
STA 171+76.69 TO STA 173+10.86  
STA 196+81.38 TO STA 198+15.55



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

REVISIONS	
NAME	DATE

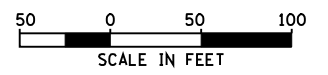
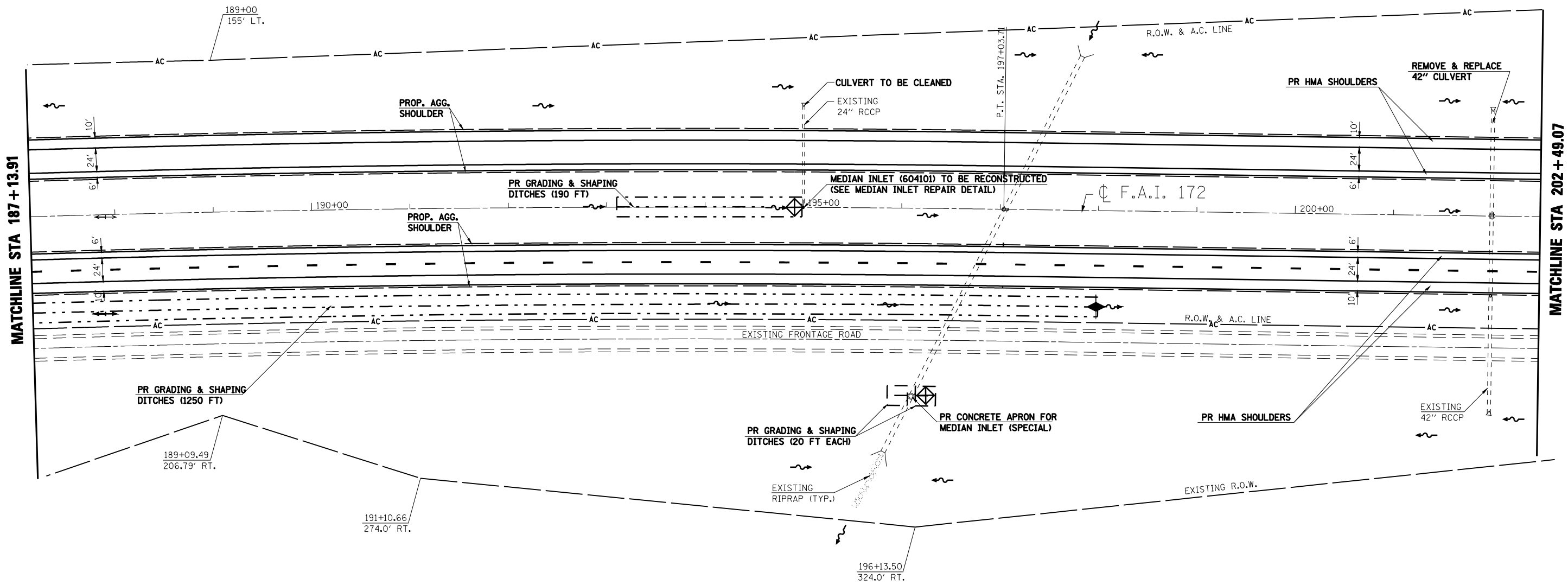
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 171+78.63 TO STA. 187+13.91**  
F.A.I. 172  
ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE: 08/09/04

DRAWN BY: JJS  
CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	48
STA. 187+13.91		TO STA. 202+49.07		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



- ◆ INLET & PIPE PROTECTION
- ▨ BITUMINOUS SURFACE REMOVAL - BUTT JOINT
- ▧ PCC SURFACE REMOVAL - BUTT JOINT
- ▩ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

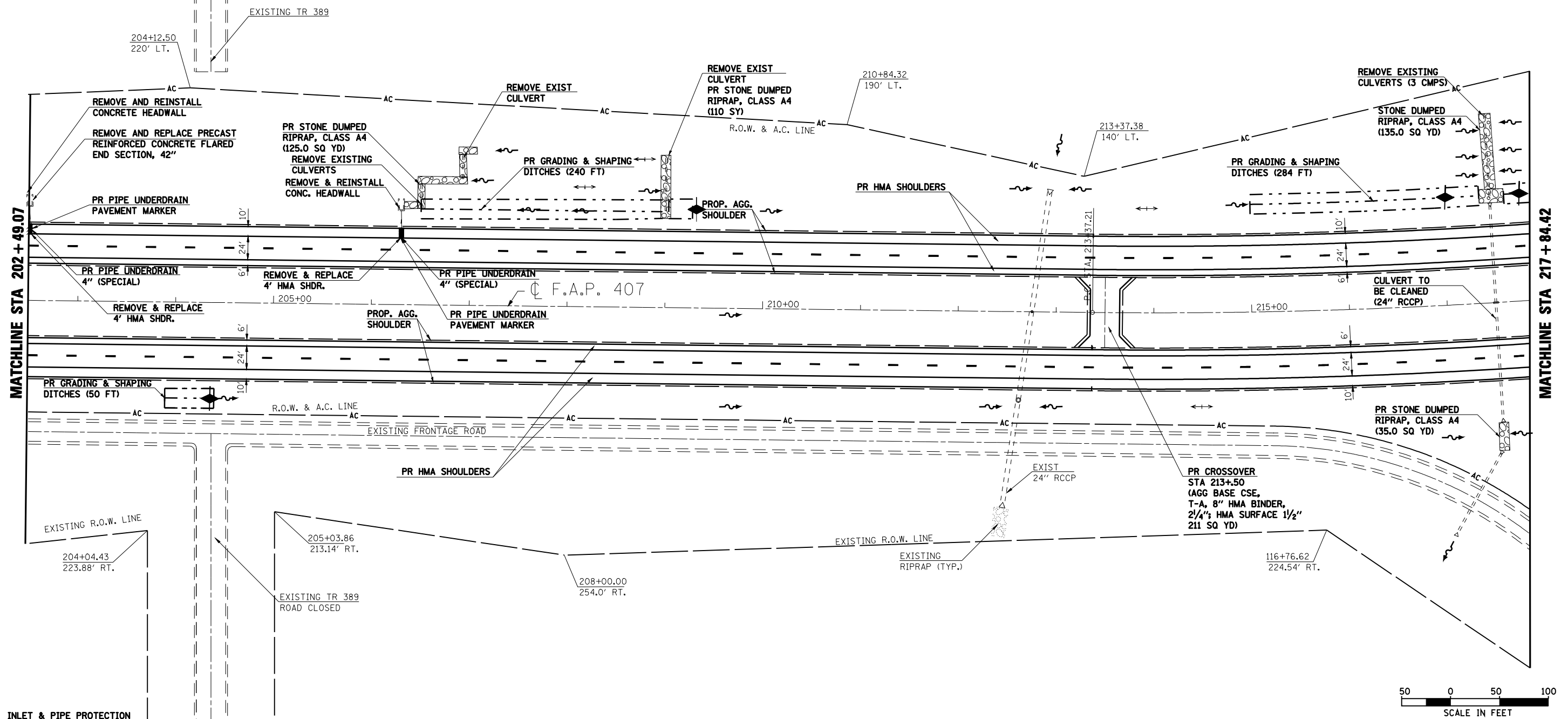
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 187 + 13.91 TO STA. 202 + 49.07**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 08/09/04  
 DRAWN BY: JJS  
 CHECKED BY: LLQ

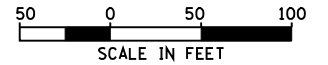


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	49
STA. 202+49.07 TO STA. 217+84.42			FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT	

\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 202+49.07 TO STA. 217+84.42**  
F.A.I. 172  
ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 08/09/04

DRAWN BY JJS  
CHECKED BY LLQ

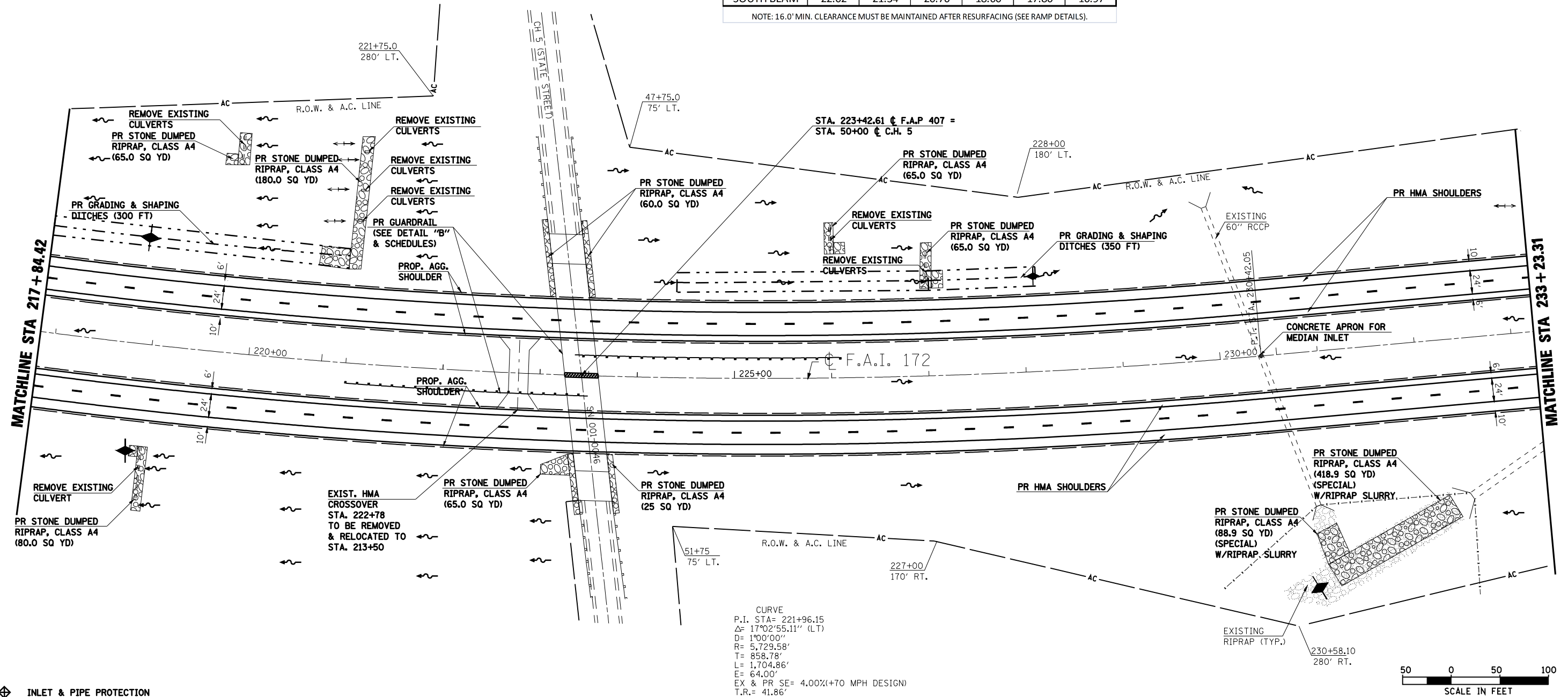
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	50
STA. 217+84.42		TO STA. 233+23.31		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-8)RS  
CONTRACT NO. 72894



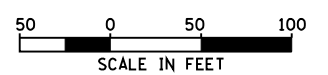
S.N. 001-0046	SB			NB		
	EOP	CL	EOP	EOP	CL	EOP
NORTH BEAM	21.99	21.34	20.68	18.53	17.75	17.00
SOUTH BEAM	22.02	21.34	20.70	18.60	17.80	16.97

NOTE: 16.0' MIN. CLEARANCE MUST BE MAINTAINED AFTER RESURFACING (SEE RAMP DETAILS).



CURVE  
P.I. STA= 221+96.15  
Δ= 17°02'55.11" (LT)  
D= 1°00'00"  
R= 5,729.58'  
T= 858.78'  
L= 1,704.86'  
E= 64.00'  
EX & PR SE= 4.00% (+70 MPH DESIGN)  
T.R.= 41.86'  
S.E. RUN= 180.00'  
P.C. STA= 213+37.21  
P.T. STA= 230+42.05  
SE TRANS.:  
STA 211+75.52 TO STA 213+04.10  
STA 229+82.24 TO STA 232+04.10

- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

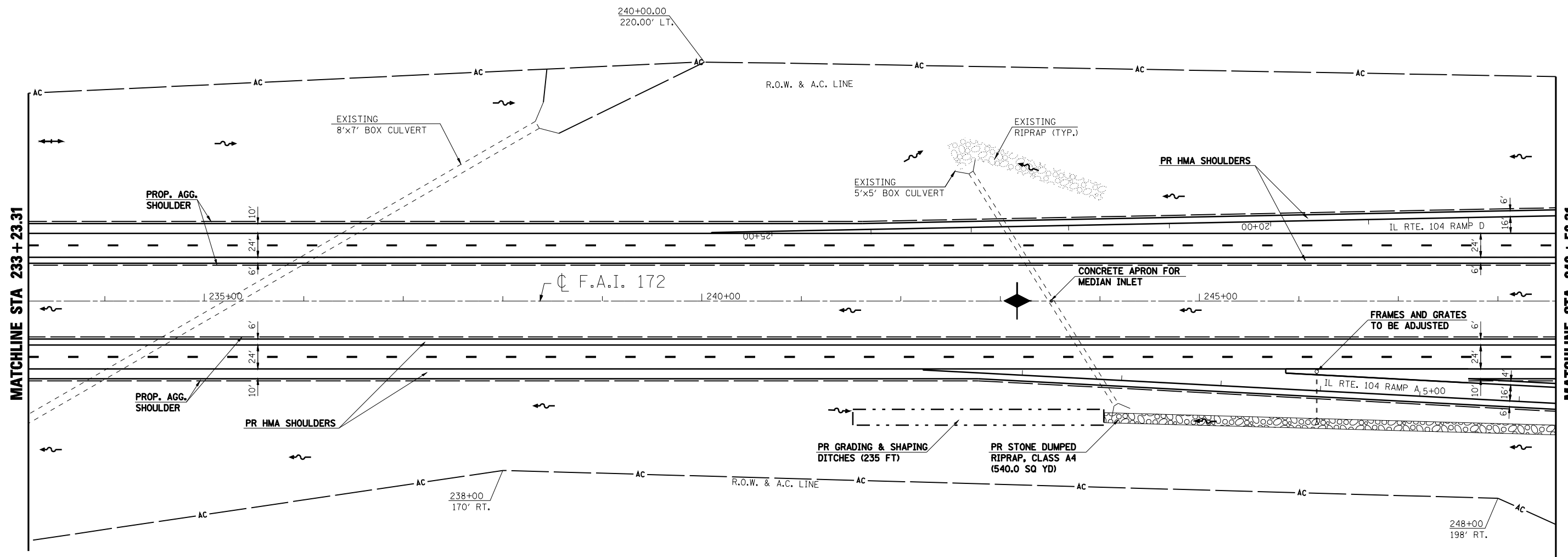


REVISIONS	
NAME	DATE

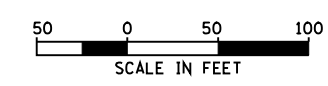
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 217 + 84.42 TO STA. 233 + 23.31**  
F.A.I. 172  
ADAMS COUNTY  
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE: 08/09/04  
DRAWN BY: JJS  
CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	51
STA. 233+23.31		TO STA. 248+58.31		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 233 + 23.31 TO STA. 248 + 58.31**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/09/04

DRAWN BY JJS  
 CHECKED BY LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	52
STA. 248+58.31		TO STA. 263+93.90		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

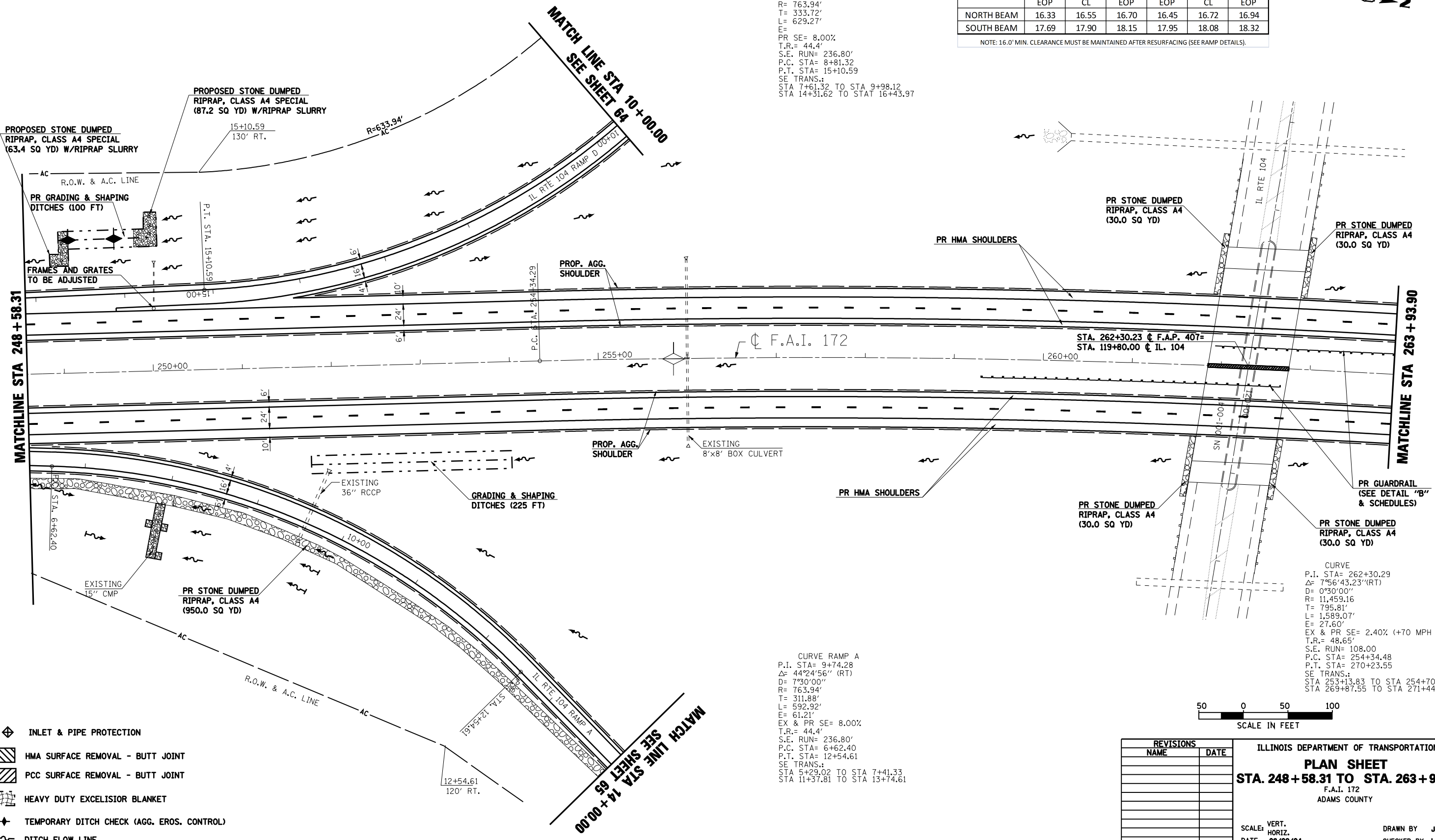
**\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694**



**CURVE RAMP D**  
P.I. STA= 12+15.04  
 $\Delta$ = 47°11'43" RT  
D= 7°30'00"  
R= 763.94'  
T= 333.72'  
L= 629.27'  
E=  
PR SE= 8.00%  
T.R.= 44.4'  
S.E. RUN= 236.80'  
P.C. STA= 8+81.32  
P.T. STA= 15+10.59  
SE TRANS.:  
STA 7+61.32 TO STA 9+98.12  
STA 14+31.62 TO STA 16+43.97

S.N. 001-0047	EXISTING BRIDGE CLEARANCES					
	SB			NB		
	EOP	CL	EOP	EOP	CL	EOP
NORTH BEAM	16.33	16.55	16.70	16.45	16.72	16.94
SOUTH BEAM	17.69	17.90	18.15	17.95	18.08	18.32

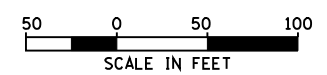
NOTE: 16.0' MIN. CLEARANCE MUST BE MAINTAINED AFTER RESURFACING (SEE RAMP DETAILS).



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

**CURVE RAMP A**  
P.I. STA= 9+74.28  
 $\Delta$ = 44°24'56" (RT)  
D= 7°30'00"  
R= 763.94'  
T= 311.88'  
L= 592.92'  
E= 61.21'  
EX & PR SE= 8.00%  
T.R.= 44.4'  
S.E. RUN= 236.80'  
P.C. STA= 6+62.40  
P.T. STA= 12+54.61  
SE TRANS.:  
STA 5+29.02 TO STA 7+41.33  
STA 11+37.81 TO STA 13+74.61

**CURVE**  
P.I. STA= 262+30.29  
 $\Delta$ = 7°56'43.23" (RT)  
D= 0°30'00"  
R= 11,459.16'  
T= 795.81'  
L= 1,589.07'  
E= 27.60'  
EX & PR SE= 2.40% (+70 MPH DESIGN)  
T.R.= 48.65'  
S.E. RUN= 108.00'  
P.C. STA= 254+34.48  
P.T. STA= 270+23.55  
SE TRANS.:  
STA 253+13.83 TO STA 254+70.48  
STA 269+87.55 TO STA 271+44.20



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 248+58.31 TO STA. 263+93.30**  
F.A.I. 172  
ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE: 08/09/04  
DRAWN BY: JJS  
CHECKED BY: LLQ

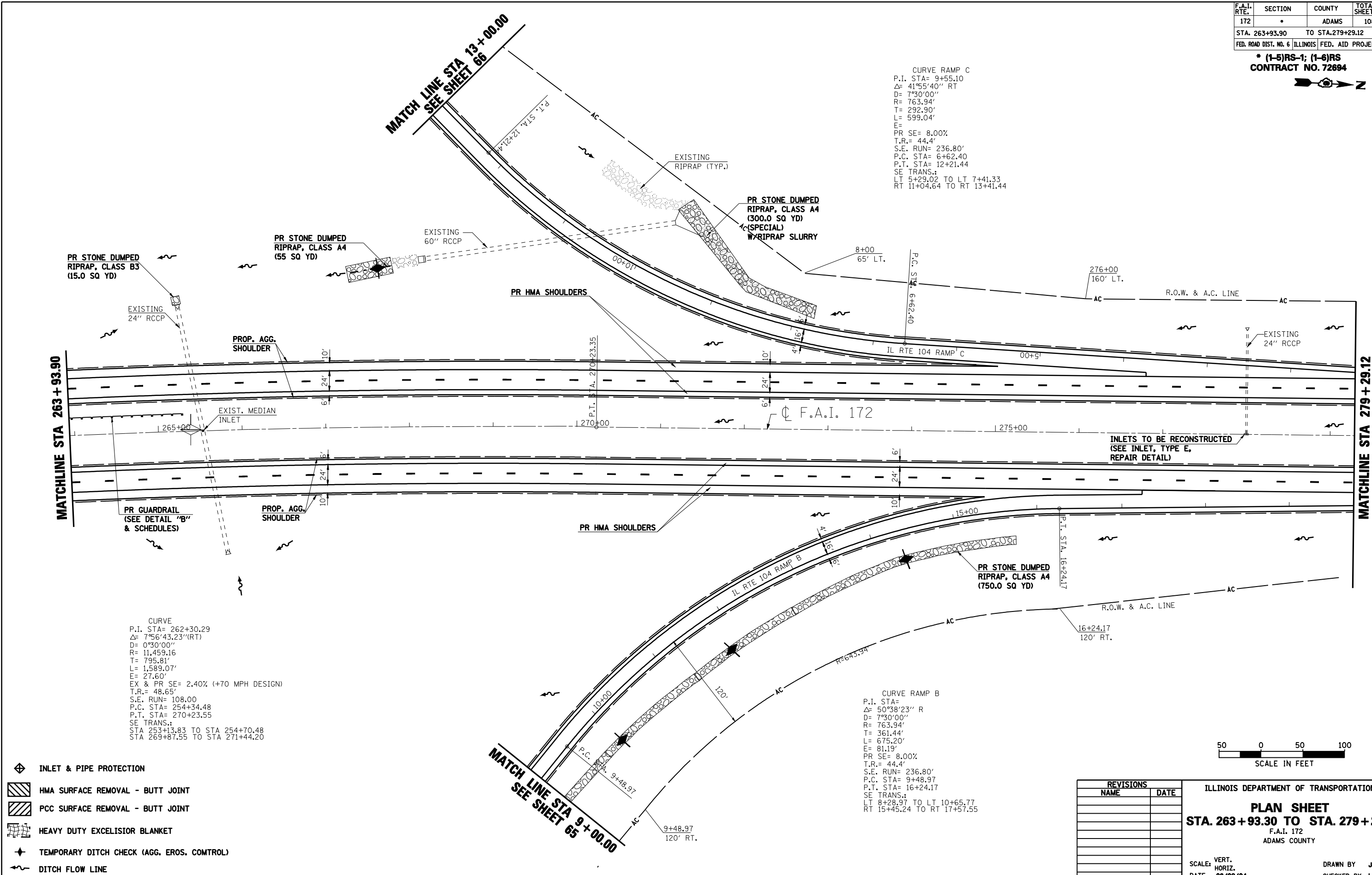
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	53

STA. 263+93.90 TO STA. 279+29.12  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694

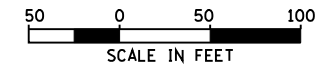


CURVE RAMP C  
 P.I. STA= 9+55.10  
 $\Delta$ = 41°55'40" RT  
 D= 7°30'00"  
 R= 763.94'  
 T= 292.90'  
 L= 599.04'  
 E=  
 PR SE= 8.00%  
 T.R.= 44.4'  
 S.E. RUN= 236.80'  
 P.C. STA= 6+62.40  
 P.T. STA= 12+21.44  
 SE TRANS.:  
 LT 5+29.02 TO LT 7+41.33  
 RT 11+04.64 TO RT 13+41.44



CURVE  
 P.I. STA= 262+30.29  
 $\Delta$ = 7°56'43.23"(RT)  
 D= 0°30'00"  
 R= 11,459.16  
 T= 795.81'  
 L= 1,589.07'  
 E= 27.60'  
 EX & PR SE= 2.40% (+70 MPH DESIGN)  
 T.R.= 48.65'  
 S.E. RUN= 108.00  
 P.C. STA= 254+34.48  
 P.T. STA= 270+23.55  
 SE TRANS.:  
 STA 253+13.83 TO STA 254+70.48  
 STA 269+87.55 TO STA 271+44.20

CURVE RAMP B  
 P.I. STA=  
 $\Delta$ = 50°38'23" R  
 D= 7°30'00"  
 R= 763.94'  
 T= 361.44'  
 L= 675.20'  
 E= 81.19'  
 PR SE= 8.00%  
 T.R.= 44.4'  
 S.E. RUN= 236.80'  
 P.C. STA= 9+48.97  
 P.T. STA= 16+24.17  
 SE TRANS.:  
 LT 8+28.97 TO LT 10+65.77  
 RT 15+45.24 TO RT 17+57.55



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▨ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

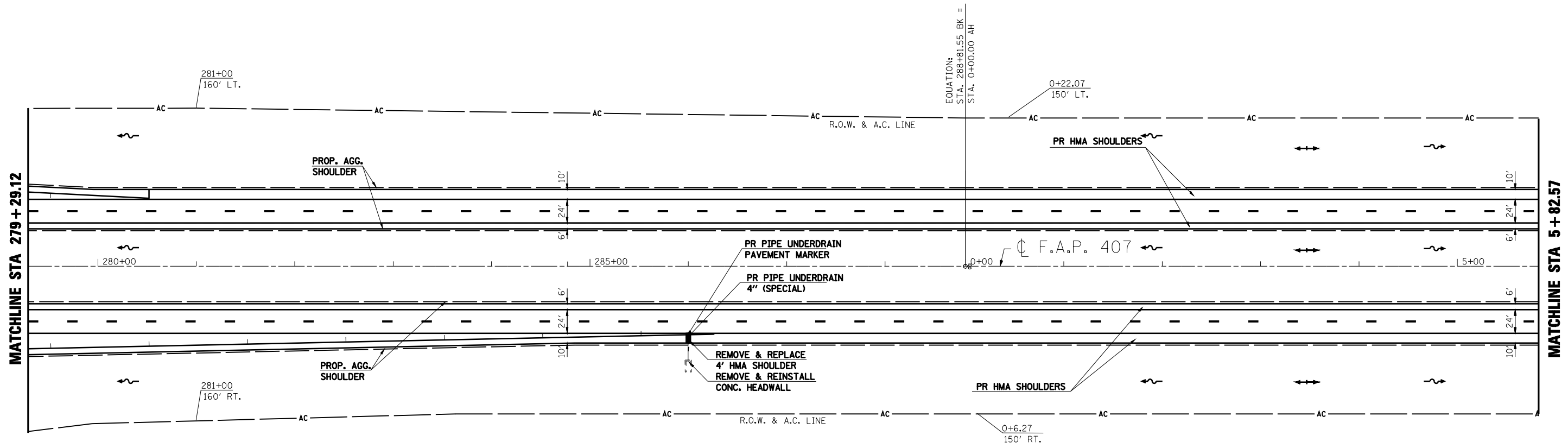
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 263 + 93.30 TO STA. 279 + 29.12**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/09/04  
 DRAWN BY JJS  
 CHECKED BY LLQ

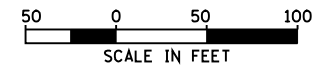
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	54

STA. 279+29.12 TO STA. 5+82.57  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ⚡ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

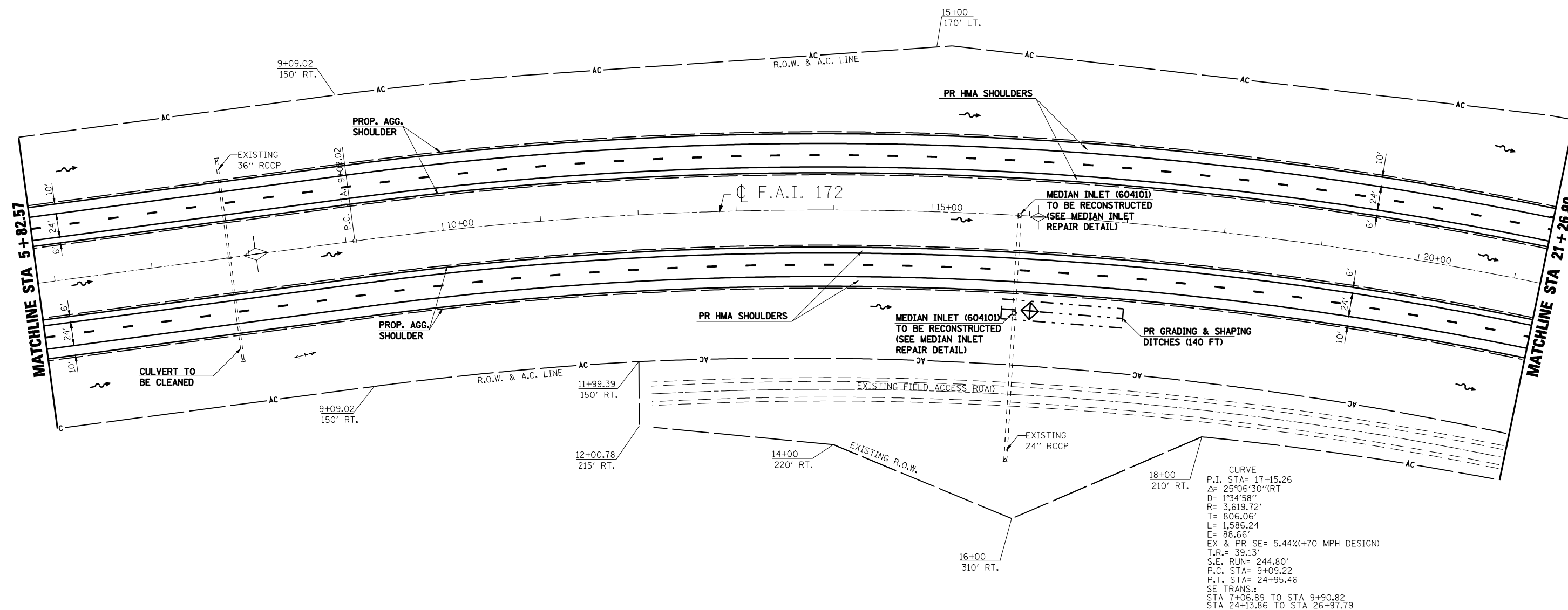


REVISIONS	
NAME	DATE

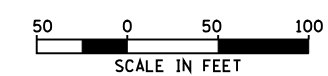
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 279+29.12 TO STA. 5+82.57**  
**STATION EQUATION**  
 F.A.I. 172  
 ADAMS COUNTY  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/09/04  
 DRAWN BY JJS  
 CHECKED BY LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	55
STA. 5+82.57		TO STA. 21+26.90		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 5+82.57 TO STA. 21+26.90**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 08/09/04  
 DRAWN BY: JJS  
 CHECKED BY: LLQ

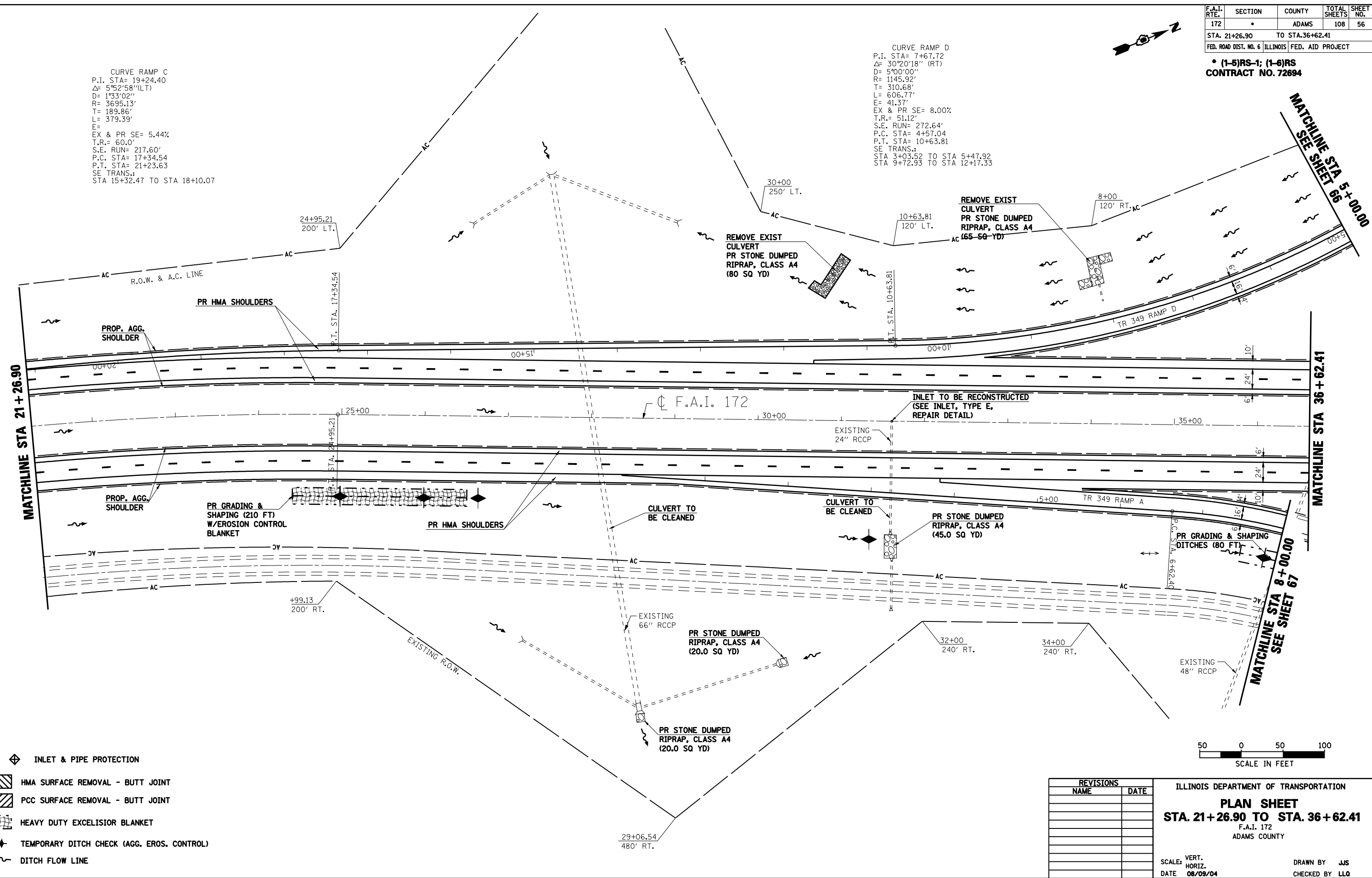
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	56

STA. 21+26.90 TO STA. 36+62.41  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

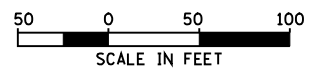
\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694

CURVE RAMP C  
 P.I. STA= 19+24.40  
 $\Delta$ = 5°52'58" (LT)  
 D= 1°33'02"  
 R= 3695.13'  
 T= 189.86'  
 L= 379.39'  
 E= 41.37'  
 EX & PR SE= 5.44%  
 T.R.= 60.0'  
 S.E. RUN= 217.60'  
 P.C. STA= 17+34.54  
 P.T. STA= 21+23.63  
 SE TRANS.:  
 STA 15+32.47 TO STA 18+10.07

CURVE RAMP D  
 P.I. STA= 7+67.72  
 $\Delta$ = 30°20'18" (RT)  
 D= 5°00'00"  
 R= 1145.92'  
 T= 310.68'  
 L= 606.77'  
 E= 41.37'  
 EX & PR SE= 8.00%  
 T.R.= 51.12'  
 S.E. RUN= 272.64'  
 P.C. STA= 4+57.04  
 P.T. STA= 10+63.81  
 SE TRANS.:  
 STA 3+03.52 TO STA 5+47.92  
 STA 9+72.93 TO STA 12+17.33



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 21+26.90 TO STA. 36+62.41**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 08/09/04

DRAWN BY: JJS  
 CHECKED BY: LLQ



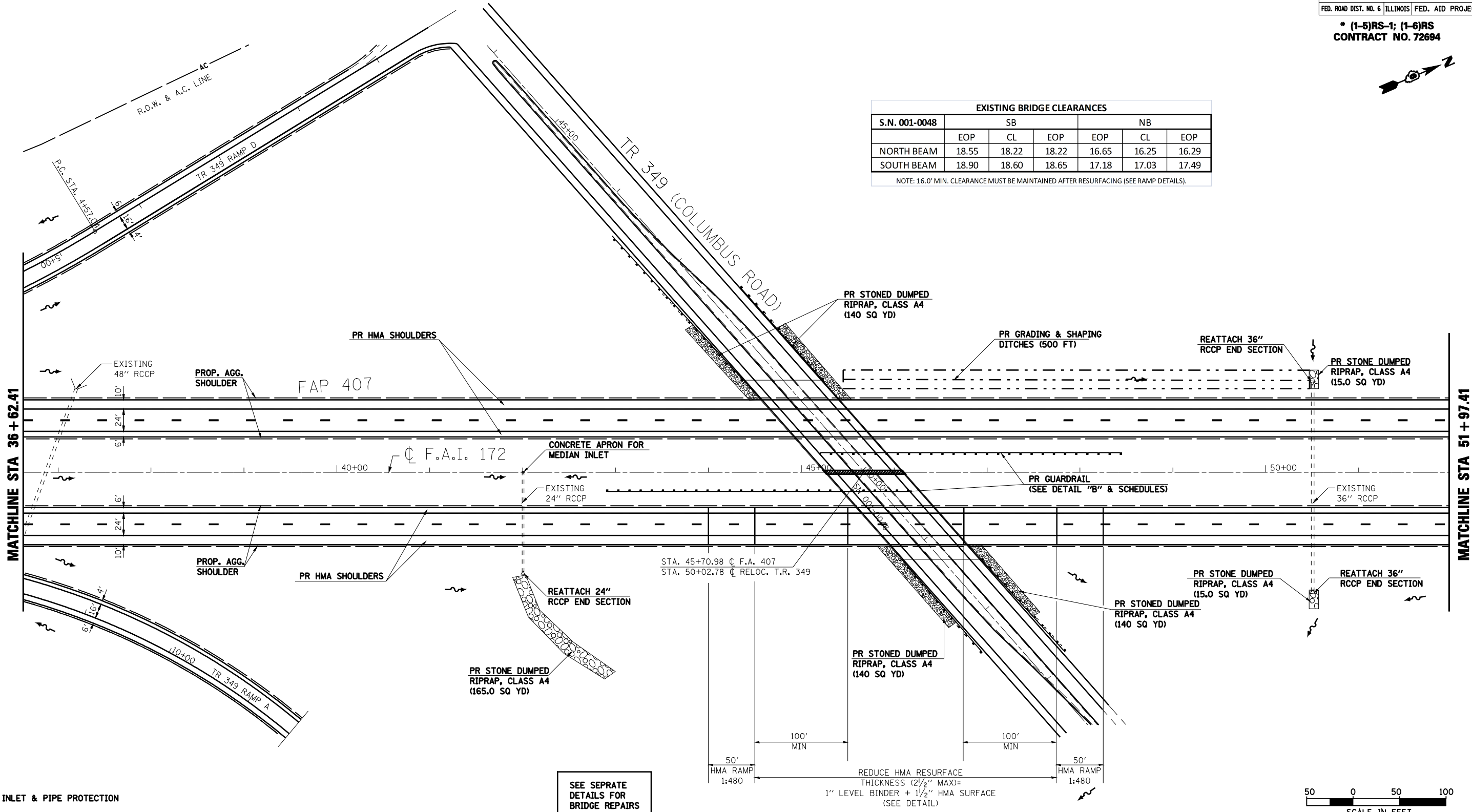
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	57
STA. 36+62.41		TO STA. 51+97.41		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

**\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694**



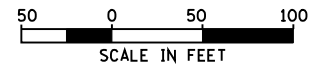
EXISTING BRIDGE CLEARANCES						
S.N. 001-0048	SB			NB		
	EOP	CL	EOP	EOP	CL	EOP
NORTH BEAM	18.55	18.22	18.22	16.65	16.25	16.29
SOUTH BEAM	18.90	18.60	18.65	17.18	17.03	17.49

NOTE: 16.0' MIN. CLEARANCE MUST BE MAINTAINED AFTER RESURFACING (SEE RAMP DETAILS).



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

SEE SEPRATE  
DETAILS FOR  
BRIDGE REPAIRS  
SN 001-0048



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 36+62.41 TO STA. 51+97.41**  
F.A.I. 172  
ADAMS COUNTY

SCALE: VERT.      DRAWN BY JJS  
HORIZ.              CHECKED BY LLQ  
DATE 08/09/04

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	58
STA. 51+97.41		TO STA. 67+32.41		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694

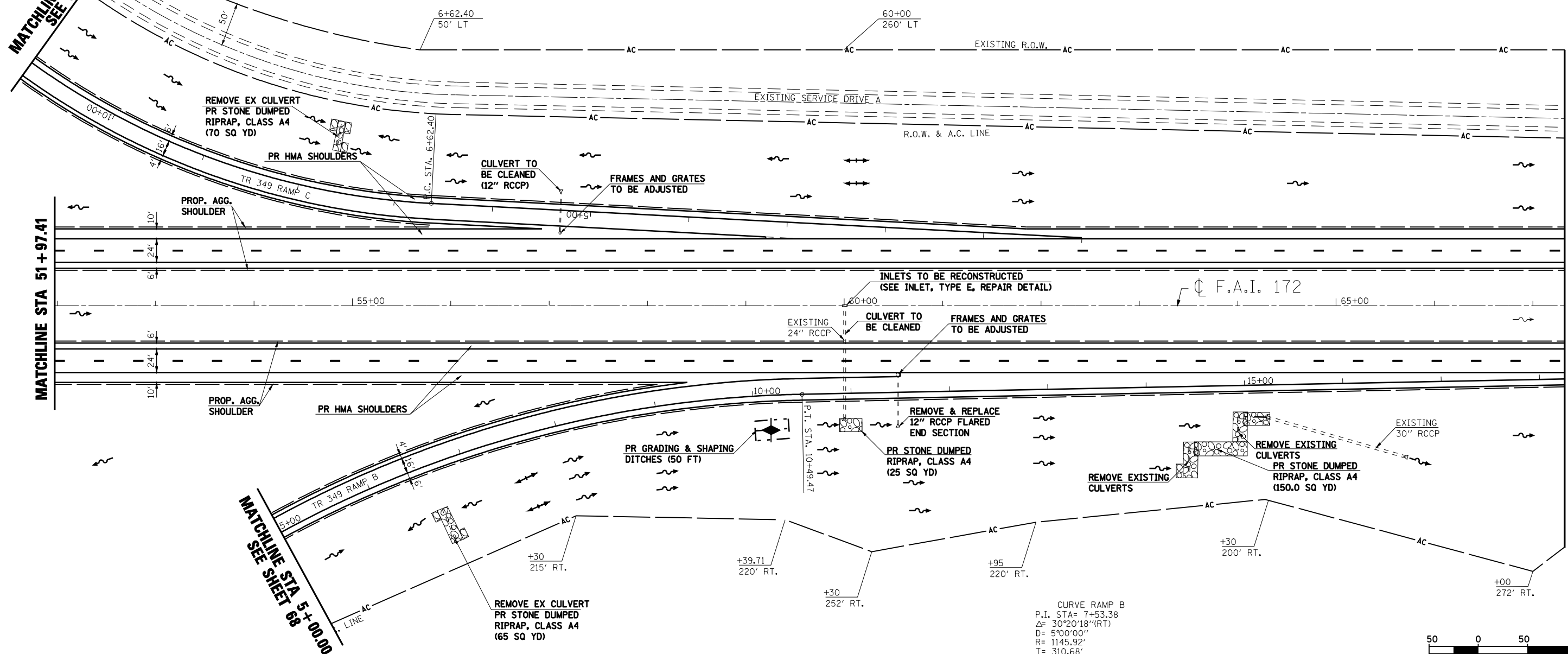


CURVE RAMP C  
P.I. STA= 9+74.65  
Δ= 44°27'46"(RT)  
D= 7°30'00"  
R= 763.94'  
T= 312.25'  
L= 592.84'  
E= 61.35'  
EX & PR SE= 8.00%  
T.R.= 44.4'  
S.E. RUN= 236.80'  
P.C. STA= 6+62.40  
P.T. STA= 12+55.24  
SE TRANS.:  
STA 5+29.02 TO STA 7+41.33  
STA 11+38.44 TO STA 13+75.24

MATCHLINE STA 11+00.00  
SEE SHEET 69

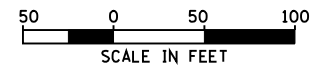
MATCHLINE STA 51+97.41

MATCHLINE STA 67+32.41



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ⚡ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

CURVE RAMP B  
P.I. STA= 7+53.38  
Δ= 30°20'18"(RT)  
D= 5°00'00"  
R= 1145.92'  
T= 310.68'  
L= 606.77'  
E= 41.37'  
EX & PR SE= 8.00%  
T.R.= 48.0'  
S.E. RUN= 256.00'  
P.C. STA= 4+42.70  
P.T. STA= 10+49.47  
SE TRANS.:  
STA 2+98.40 TO STA 5+28.03  
STA 9+64.14 TO STA 11+93.77



REVISIONS	
NAME	DATE

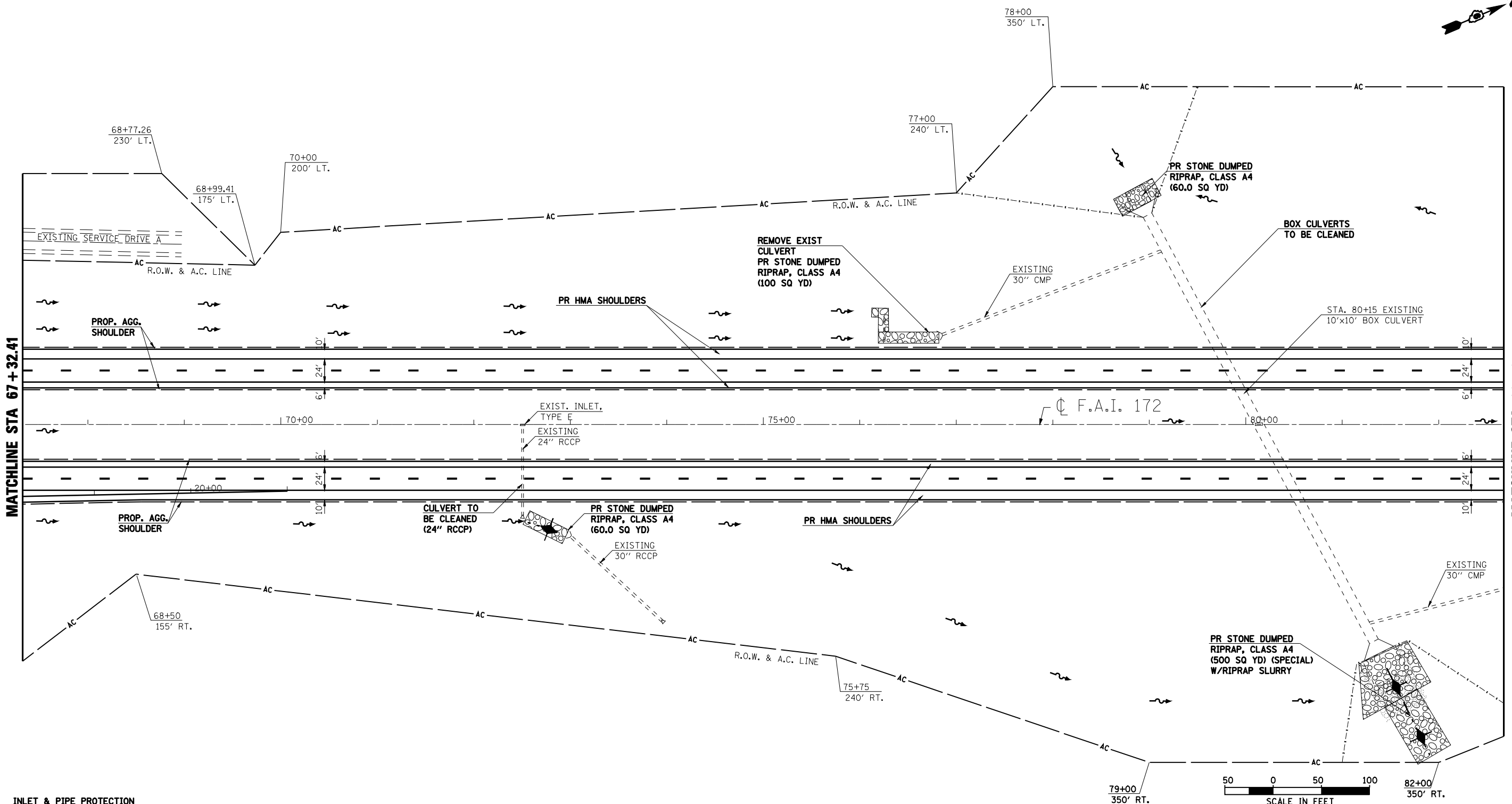
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
STA. 51+97.41 TO STA. 67+32.41  
F.A.I. 172  
ADAMS COUNTY

SCALE: VERT.     DRAWN BY JJS  
          HORIZ.     CHECKED BY LLQ  
DATE 08/09/04

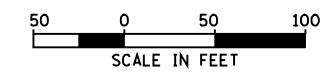
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	59

STA. 67+32.41 TO STA. 82+67.41  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

**\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694**



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 67+32.41 TO STA. 82+67.41**  
 F.A.I. 172  
 ADAMS COUNTY

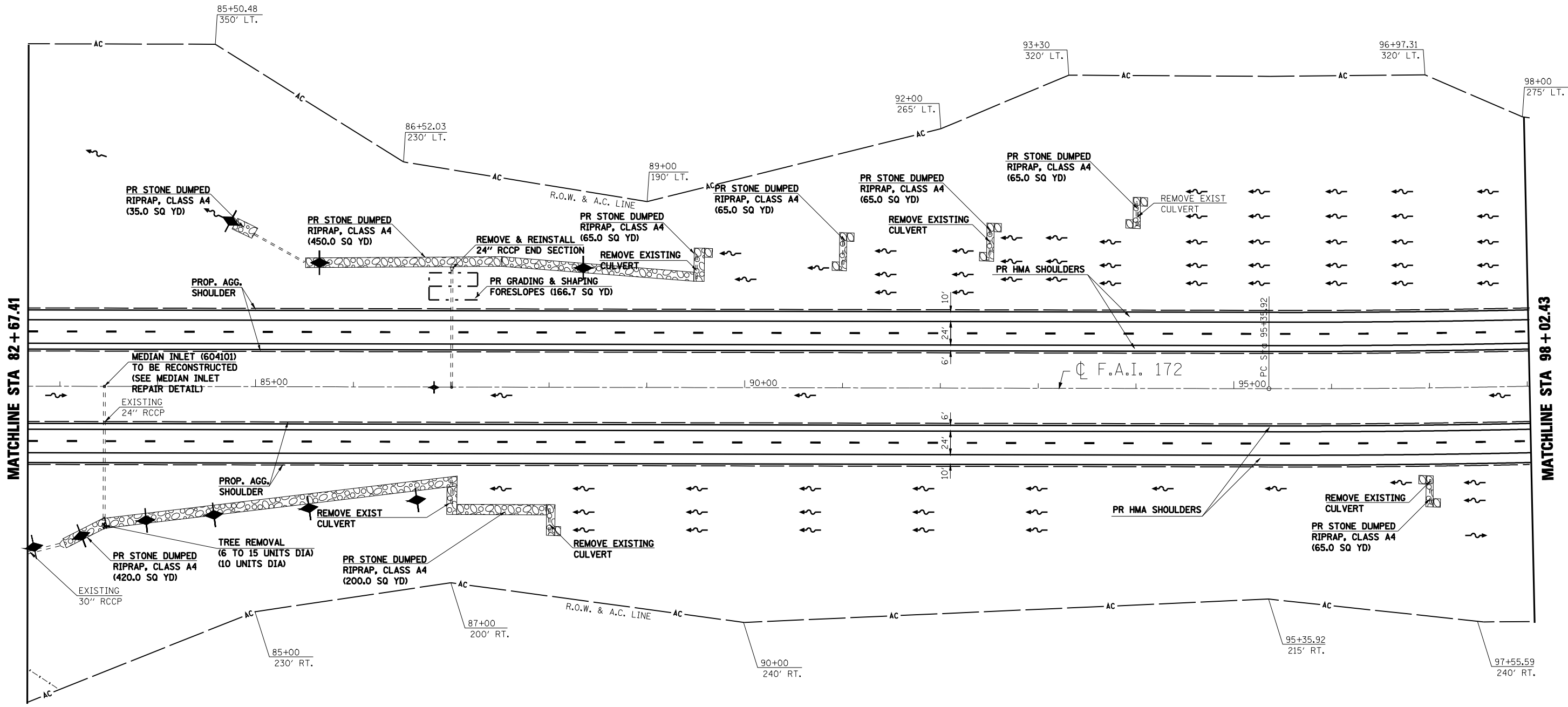
SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 08/09/04

DRAWN BY: JJS  
 CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	60

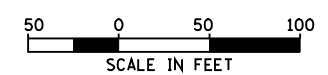
STA. 82+67.41 TO STA. 98+02.43  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



MATCHLINE STA 82+67.41

MATCHLINE STA 98+02.43



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 82+67.41 TO STA. 98+02.43**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/09/04

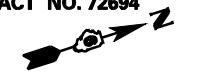
DRAWN BY JJS  
 CHECKED BY LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	61

STA. 98+02.43 TO STA. 113+38.56  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694

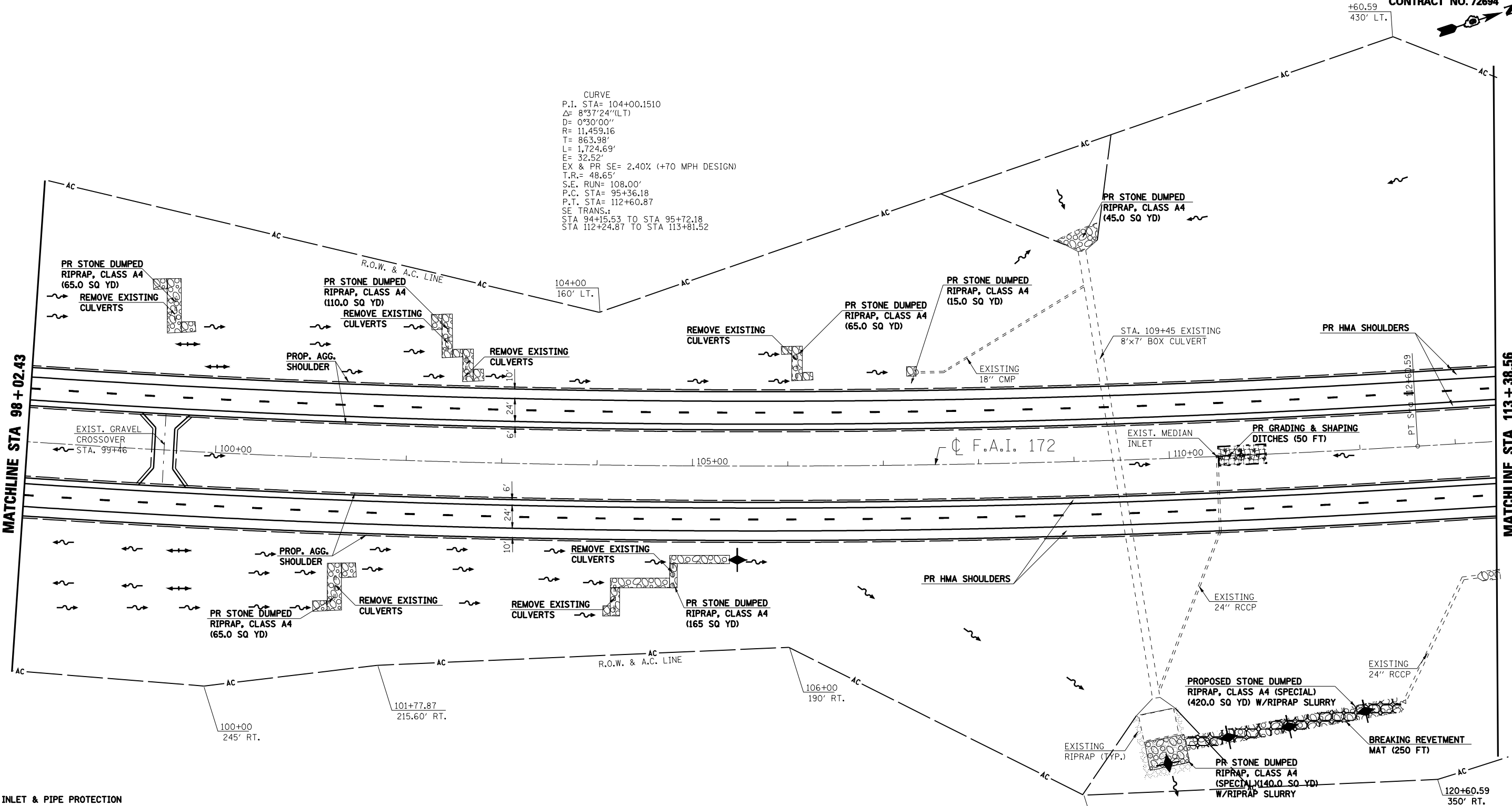
+60.59  
 430' LT.









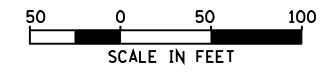
CURVE  
 P.I. STA= 104+00.1510  
 $\Delta = 8^{\circ}37'24''$ (LT)  
 $D = 0^{\circ}30'00''$   
 $R = 11,459.16$   
 $T = 863.98'$   
 $L = 1,724.69'$   
 $E = 32.52'$   
 $EX \ \& \ PR \ SE = 2.40\%$  (+70 MPH DESIGN)  
 $T.R. = 48.65'$   
 $S.E. \ RUN = 108.00'$   
 $P.C. \ STA = 95+36.18$   
 $P.T. \ STA = 112+60.87$   
 $SE \ TRANS.: \ STA \ 94+15.53 \ TO \ STA \ 95+72.18$   
 $STA \ 112+24.87 \ TO \ STA \ 113+81.52$

MATCHLINE STA 98 + 02.43

MATCHLINE STA 113 + 38.56



-  INLET & PIPE PROTECTION
-  BITUMINOUS SURFACE REMOVAL - BUTT JOINT
-  PCC SURFACE REMOVAL - BUTT JOINT
-  HEAVY DUTY EXCELSIOR BLANKET
-  TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
-  DITCH FLOW LINE



REVISIONS	
NAME	DATE

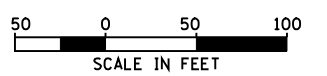
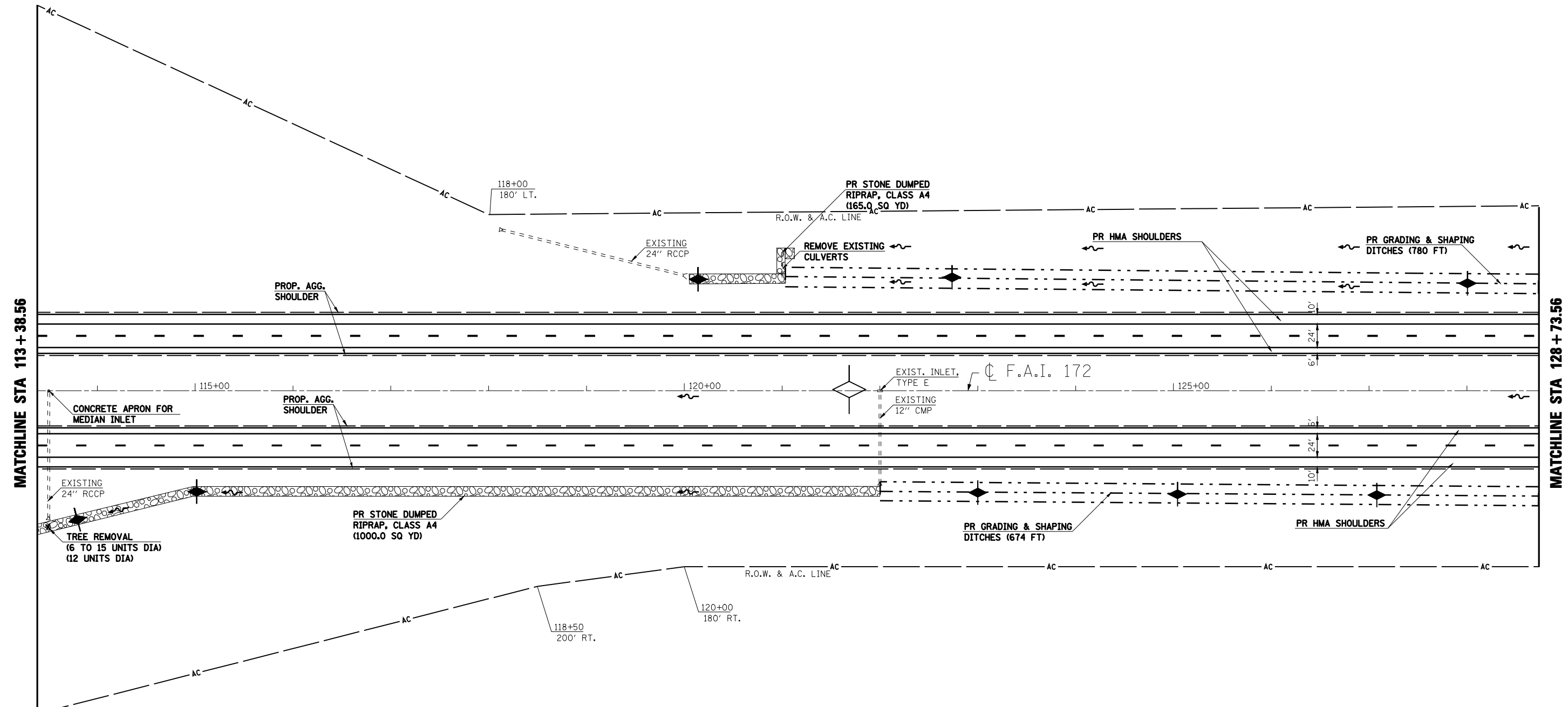
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 98+02.43 TO STA. 113+38.56**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 08/09/04  
 DRAWN BY: JJS  
 CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	62

STA. 113+38.56 TO STA. 128+73.56  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

**\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694**



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 113+38.56 TO STA. 128+73.56**  
 F.A.I. 172  
 ADAMS COUNTY

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/09/04

DRAWN BY JJS  
 CHECKED BY LLQ

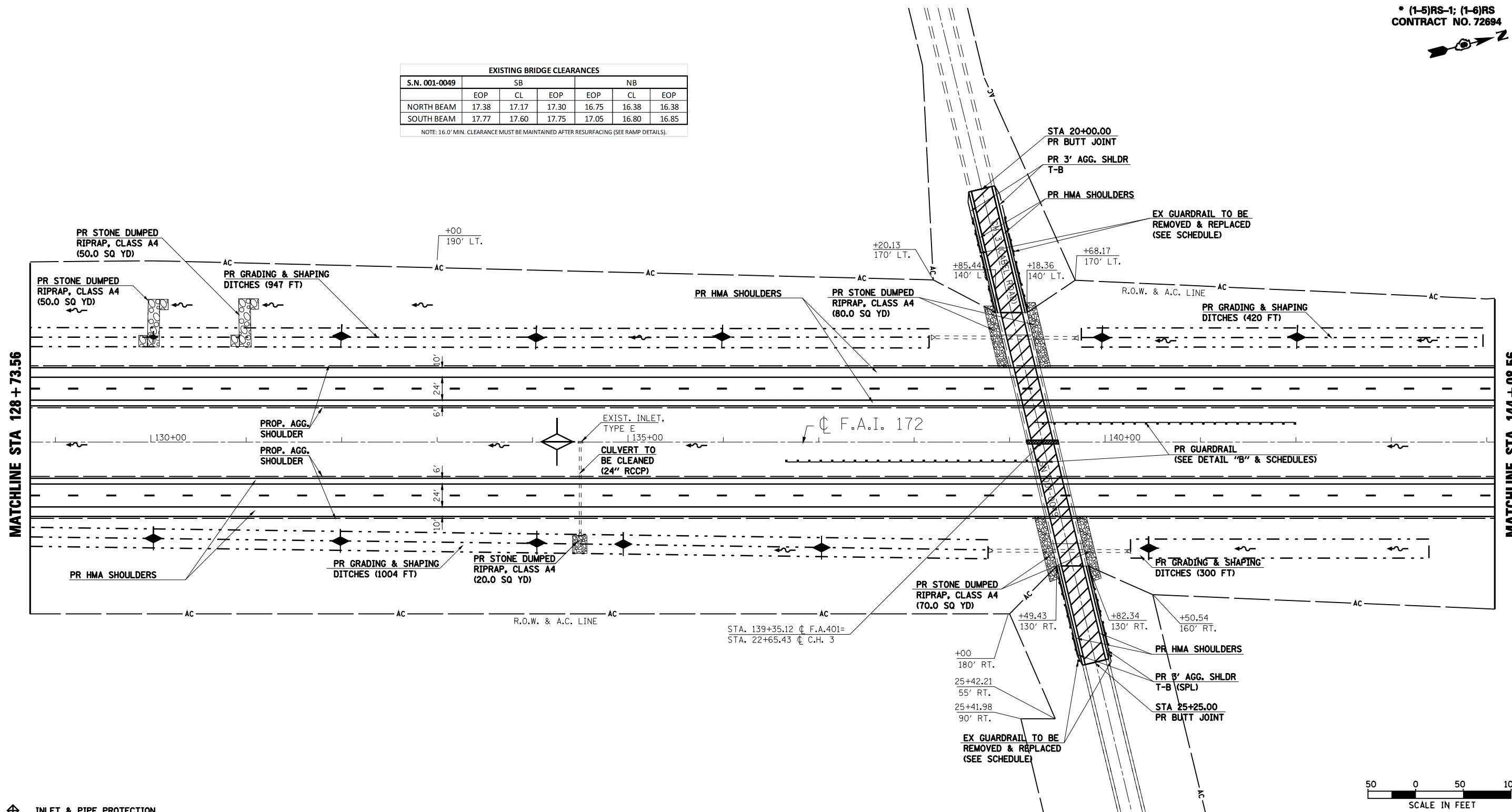
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	63
STA. 128+73.56		TO STA. 144+08.56		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

**\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694**



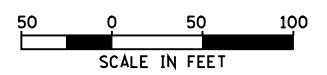
EXISTING BRIDGE CLEARANCES						
S.N. 001-0049	SB			NB		
	EOP	CL	EOP	EOP	CL	EOP
NORTH BEAM	17.38	17.17	17.30	16.75	16.38	16.38
SOUTH BEAM	17.77	17.60	17.75	17.05	16.80	16.85

NOTE: 16.0' MIN. CLEARANCE MUST BE MAINTAINED AFTER RESURFACING (SEE RAMP DETAILS).



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

SEE SEPARATE  
DETAILS FOR  
BRIDGE REPAIRS  
SN 001-0049



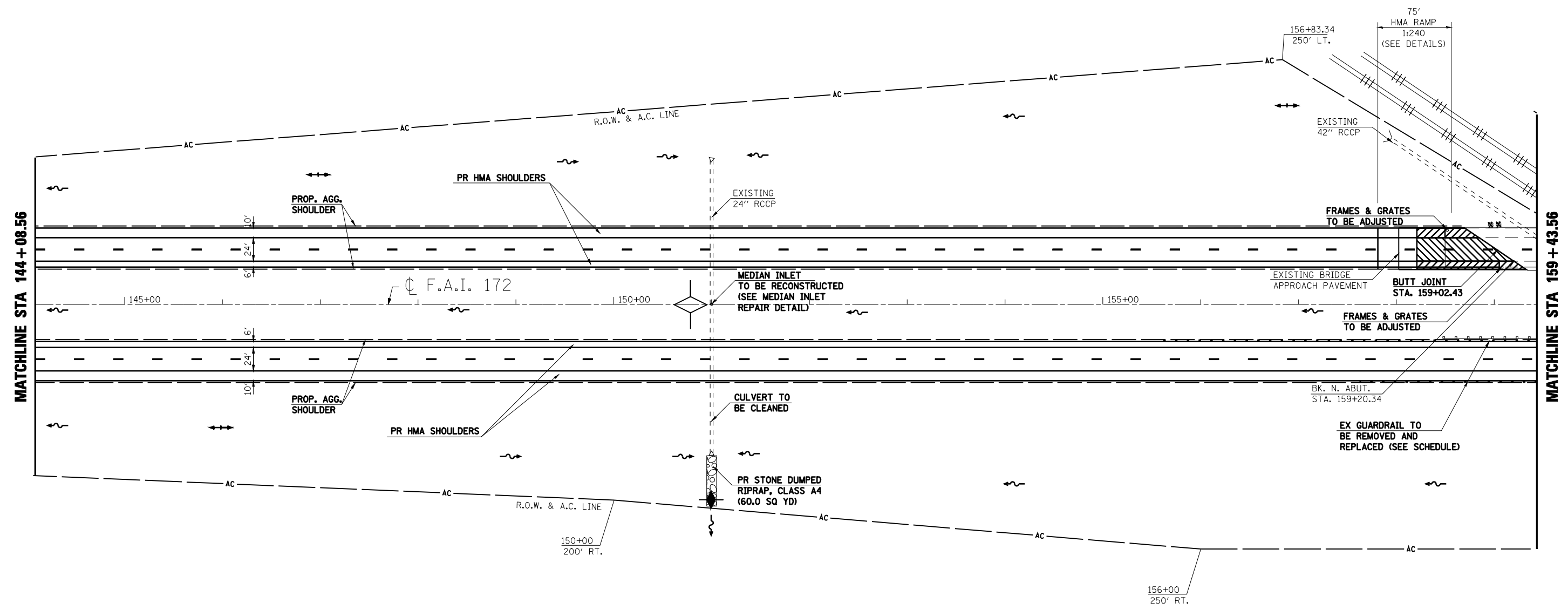
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 128+73.56 TO STA. 144+08.56**  
F.A.I. 172  
ADAMS COUNTY

SCALE: VERT.    DRAWN BY JJS  
          HORIZ.    CHECKED BY LLQ  
DATE    08/09/04

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	64
STA. 144+08.56		TO STA. 159+43.56		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694



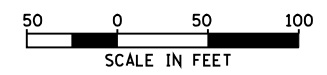
- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

NOTE:  
SEE GUARDRAIL SCHEDULE FOR  
GUARDRAIL REMOVAL & REPLACEMENT  
AT B.N.R.R. BRIDGE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 144+08.56 TO STA. 159+43.56**  
F.A.I. 172  
ADAMS COUNTY

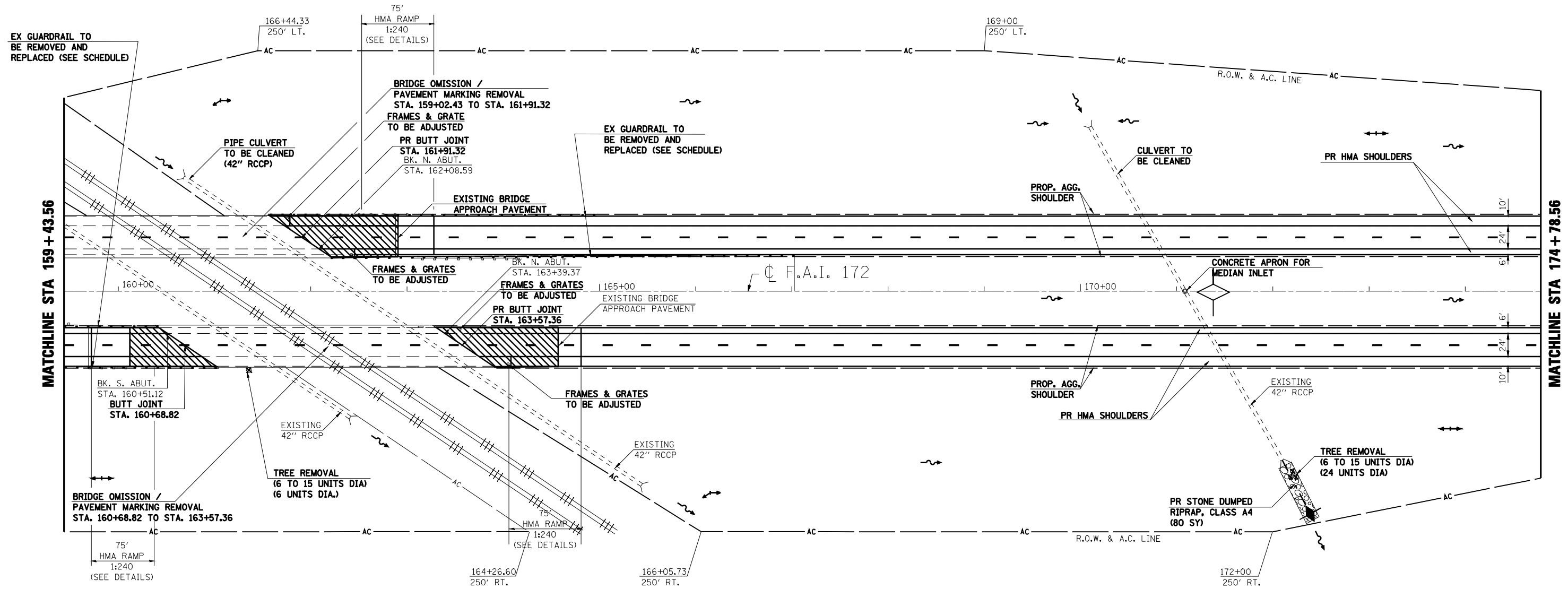
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 08/09/04  
DRAWN BY JJS  
CHECKED BY LLQ





F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	65
STA. 159+43.56		TO STA. 174+78.56		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

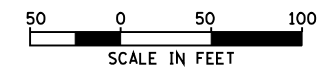
\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

SEE SEPERATE BRIDGE REPAIR DETAILS FOR SN 001-0051

SEE SCHEDULE FOR PROPOSED REMOVAL AND INSTALLATION OF NEW GUARDRAIL



REVISIONS	
NAME	DATE

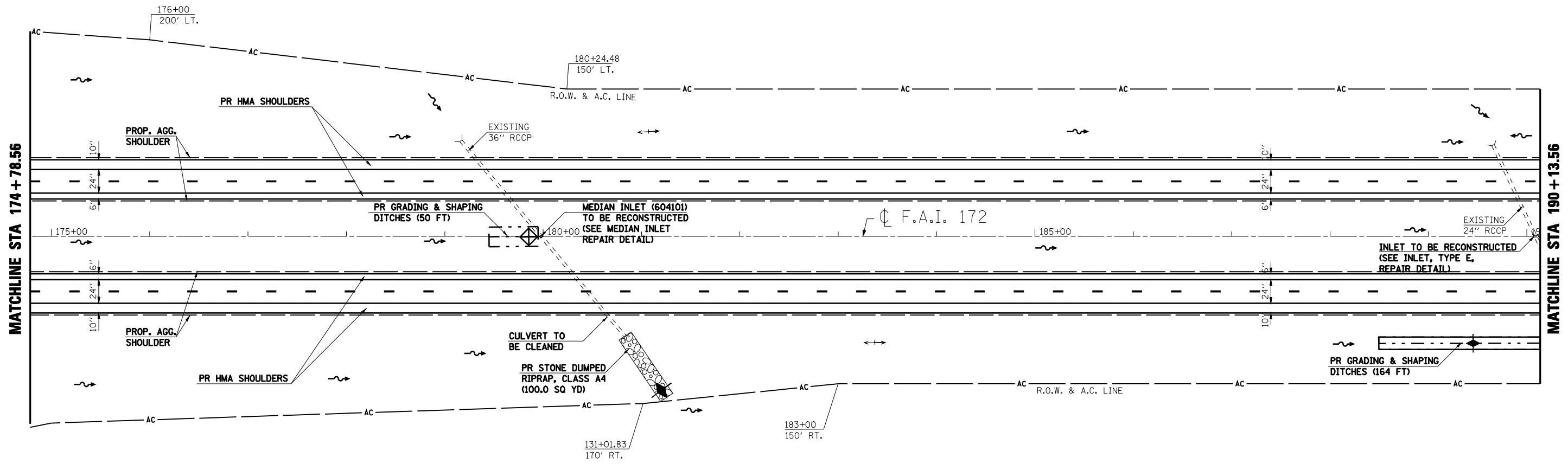
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
STA. 159+43.56 TO STA. 174+78.56  
F.A.I. 172  
ADAMS COUNTY







NOTE:  
SEE GUARDRAIL SCHEDULE FOR GUARDRAIL REMOVAL & REPLACEMENT AT B.N.R.R. BRIDGE

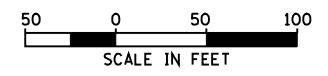
SCALE: VERT. HORIZ. DATE 08/09/04  
DRAWN BY JJS  
CHECKED BY LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	66
STA. 174+78.56		TO STA. 190+13.56		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



-  INLET & PIPE PROTECTION
-  HMA SURFACE REMOVAL - BUTT JOINT
-  PCC SURFACE REMOVAL - BUTT JOINT
-  HEAVY DUTY EXCELSIOR BLANKET
-  TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
-  DITCH FLOW LINE



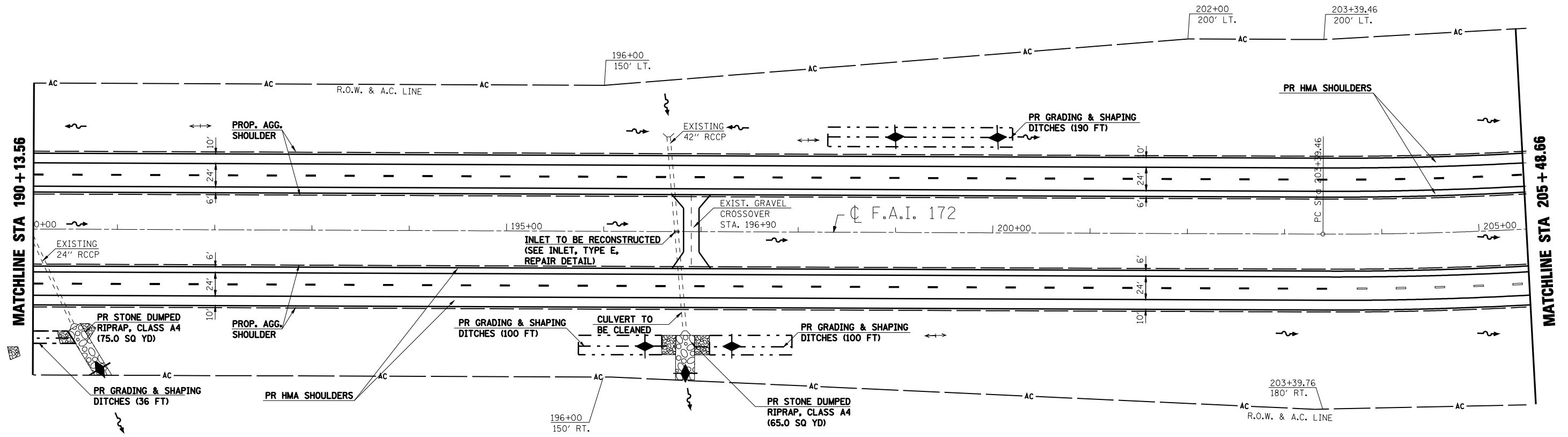
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 174+78.56 TO STA. 190+13.56**  
 F.A.I. 172  
 ADAMS COUNTY

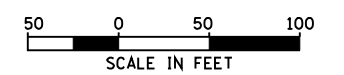
SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE: 08/09/04  
 DRAWN BY: JJS  
 CHECKED BY: LLQ

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	67
STA. 190+13.56		TO STA. 205+48.66		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▨ PCC SURFACE REMOVAL - BUTT JOINT
- ▨ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE



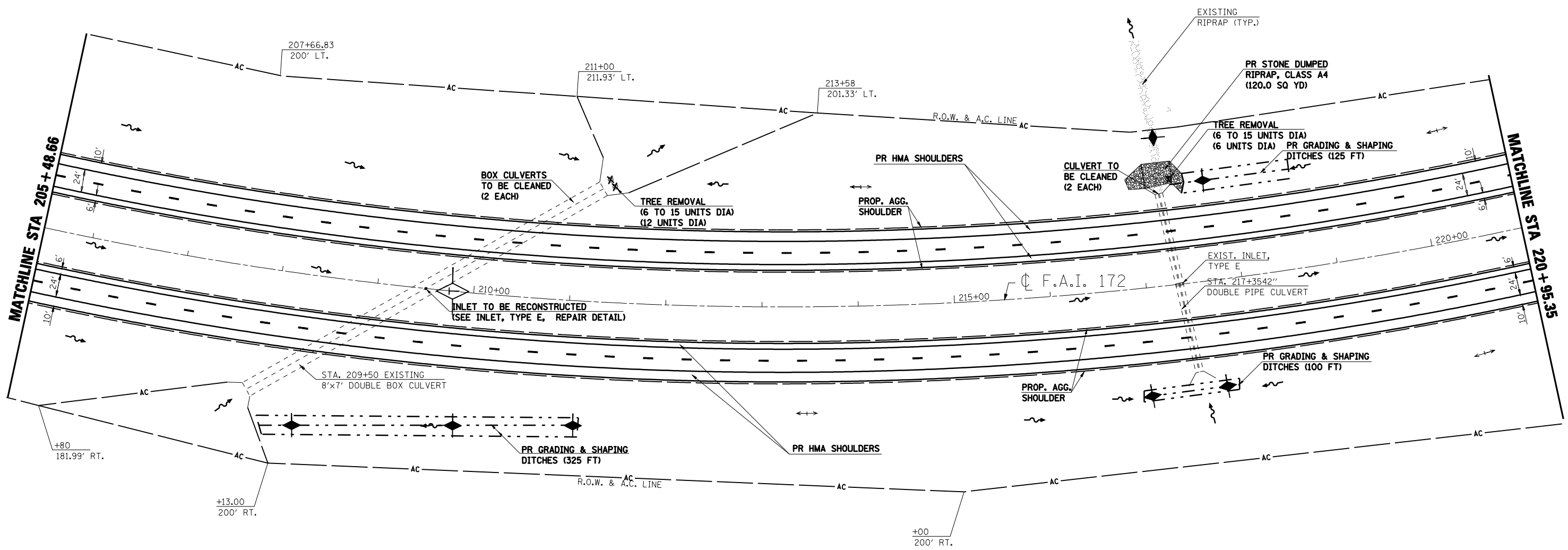
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 190+13.56 TO STA. 205+48.66**  
F.A.I. 172  
ADAMS COUNTY

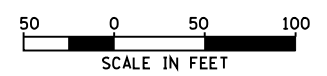
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE: 08/09/04  
DRAWN BY: JJS  
CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	68
STA. 205+48.66		TO STA. 220+95.35		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72894



CURVE  
 P.I. STA= 212+88.83  
 $\Delta = 29^{\circ}23'03.9''$ (LT)  
 D= 1'34'58"  
 R= 3,619.72'  
 T= 949.07'  
 L= 1,856.38'  
 E= 122.36'  
 EX & PR SE= 5.44%  
 T.R.= 48.65'  
 S.E. RUN= 108.00'  
 P.C. STA= 203+39.76  
 P.T. STA= 221+96.12  
 SE TRANS.:  
 STA 202.19.11 TO STA 203+75.76  
 STA 221+60.12 TO STA 223+16.77



- INLET & PIPE PROTECTION
- HMA SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT
- HEAVY DUTY EXCELSIOR BLANKET
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 205+48.66 TO STA. 220+95.35**  
 F.A.I. 172  
 ADAMS COUNTY

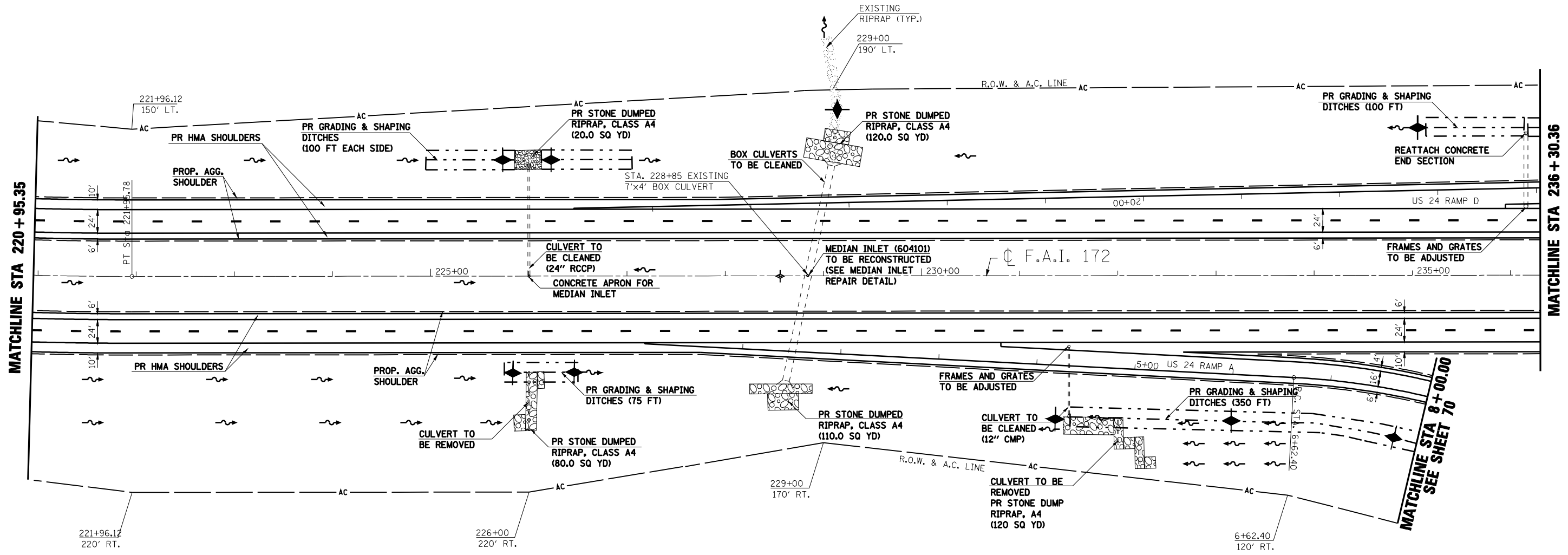
SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/09/04

DRAWN BY JJS  
 CHECKED BY LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	69

STA. 220+95.35 TO STA. 236+30.36  
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

**\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694**

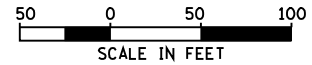


MATCHLINE STA 220 + 95.35

MATCHLINE STA 236 + 30.36

MATCHLINE STA 8 + 00.00  
SEE SHEET 70

CURVE RAMP A  
P.I. STA= 8+15.80  
Δ= 22°42'31"(RT)  
D= 7°30'00"  
R= 763.94'  
T= 153.40'  
L= 302.78'  
E= 15.25'  
EX & PR SE= 8.00%  
T.R.= 44.4'  
S.E. RUN= 236.80'  
P.C. STA= 6+62.40  
P.T. STA= 9+65.18  
SE TRANS.:  
STA 5+29.02 TO STA 7+41.33  
STA 8+86.25 TO STA 10+98.56



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE

REVISIONS	
NAME	DATE

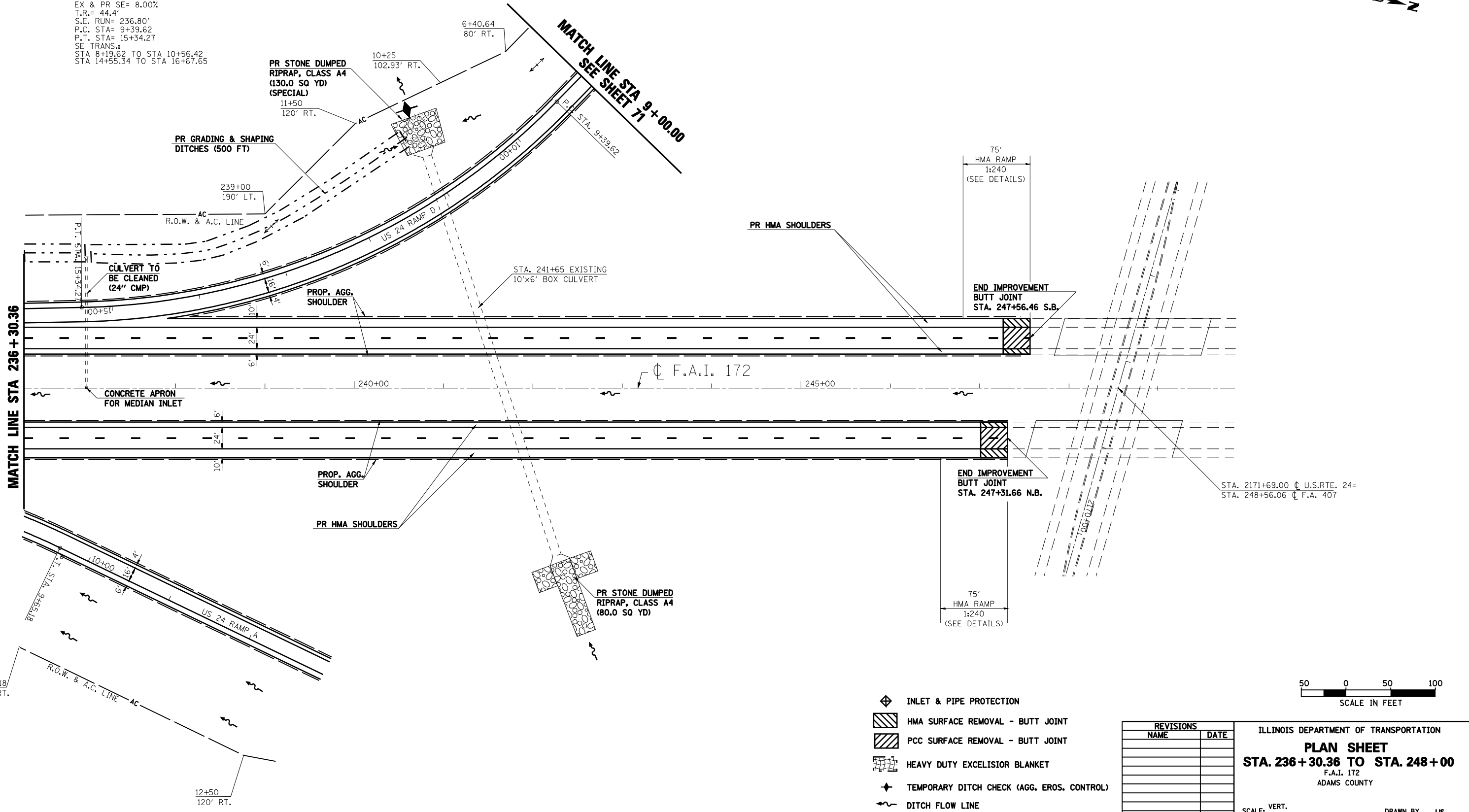
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 220 + 95.35 TO STA. 236 + 30.36**  
F.A.I. 172  
ADAMS COUNTY  
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE: 08/09/04  
DRAWN BY: JJS  
CHECKED BY: LLQ

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	70
STA. 236+30.36		TO STA. 248+00		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

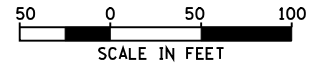
**\* (1-5)RS-1; (1-6)RS  
CONTRACT NO. 72694**



CURVE RAMP D  
P.I. STA= 12+52.93  
Δ= 44°35'54" (RT)  
D= 7°30'00"  
R= 763.94'  
T= 313.30'  
L<sub>1</sub>= 594.65'  
E= 61.75'  
EX & PR SE= 8.00%  
T.R.= 44.4'  
S.F. RUN= 236.80'  
P.C. STA= 9+39.62  
P.T. STA= 15+34.27  
SE TRANS.:  
STA 8+19.62 TO STA 10+56.42  
STA 14+55.34 TO STA 16+67.65



- ◆ INLET & PIPE PROTECTION
- ▨ HMA SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ HEAVY DUTY EXCELISIOR BLANKET
- ◆ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**STA. 236+30.36 TO STA. 248+00**  
F.A.I. 172  
ADAMS COUNTY  
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 08/09/04  
DRAWN BY JJS  
CHECKED BY LLQ

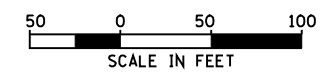
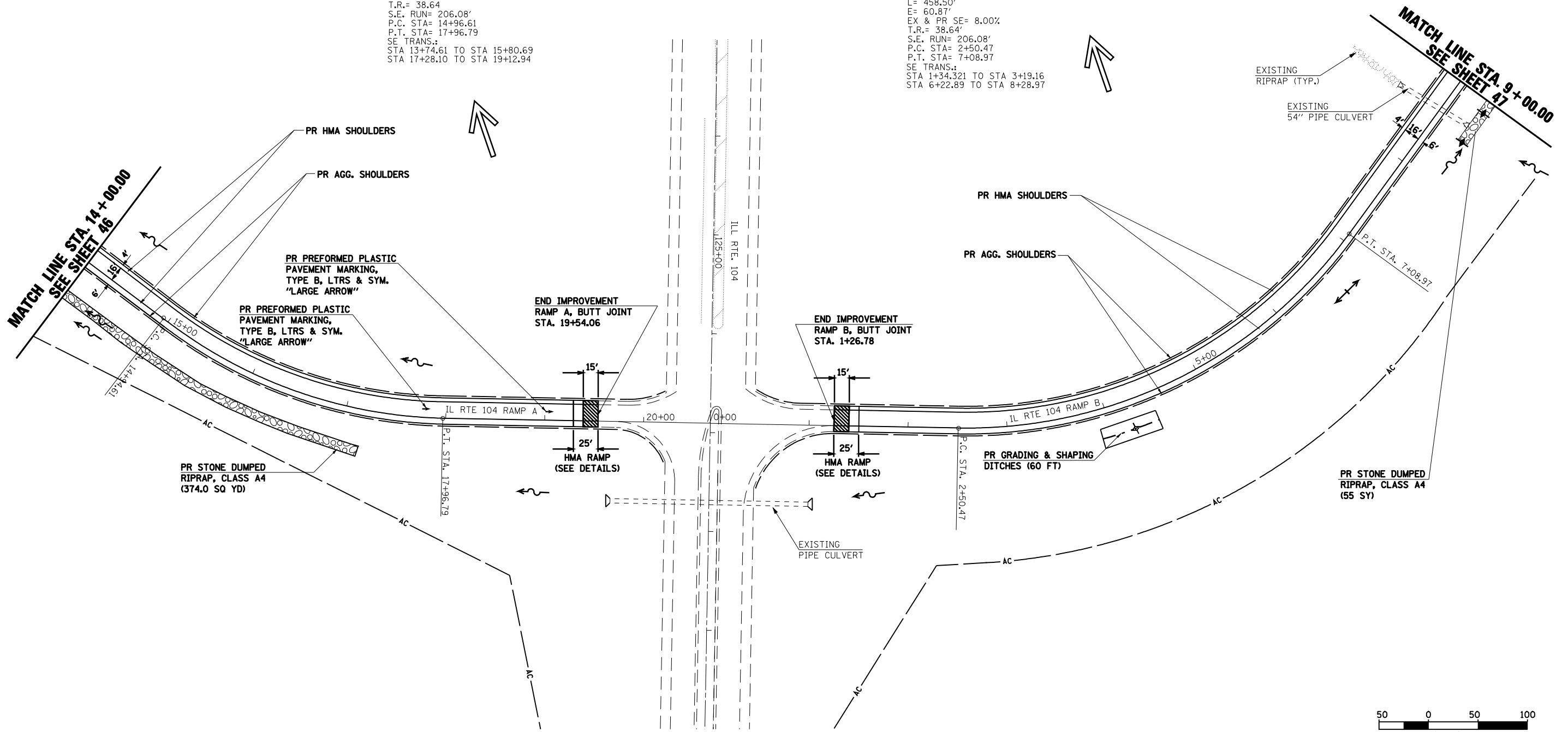
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	71
STA. 104/RAMP A		TO STA.104/RAMP B		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



CURVE RAMP A  
P.I. STA= 16+50.96  
 $\Delta= 36^{\circ}15'43''$  (LT)  
D= 12 $^{\circ}00'00''$   
R= 477.47'  
T= 155.35'  
L= 302.18'  
E= 24.95'  
EX & PR SE= 8.00%  
T.R.= 38.64'  
S.E. RUN= 206.08'  
P.C. STA= 14+96.61  
P.T. STA= 17+96.79  
SE TRANS.:  
STA 13+74.61 TO STA 15+80.69  
STA 17+28.10 TO STA 19+12.94

CURVE RAMP B  
P.I. STA= 4+99.13  
 $\Delta= 55^{\circ}01'10''$  (LT)  
D= 12 $^{\circ}00'00''$   
R= 477.47'  
T= 248.66'  
L= 458.50'  
E= 60.87'  
EX & PR SE= 8.00%  
T.R.= 38.64'  
S.E. RUN= 206.08'  
P.C. STA= 2+50.47  
P.T. STA= 7+08.97  
SE TRANS.:  
STA 1+34.321 TO STA 3+19.16  
STA 6+22.89 TO STA 8+28.97



- BITUMINOUS SURFACE REMOVAL - BUTT JOINT
- TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- DITCH FLOW LINE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PLAN SHEET**  
**IL RTE 104 RAMP A & B**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 06/08/2004

DRAWN BY JJS  
CHECKED BY LLO

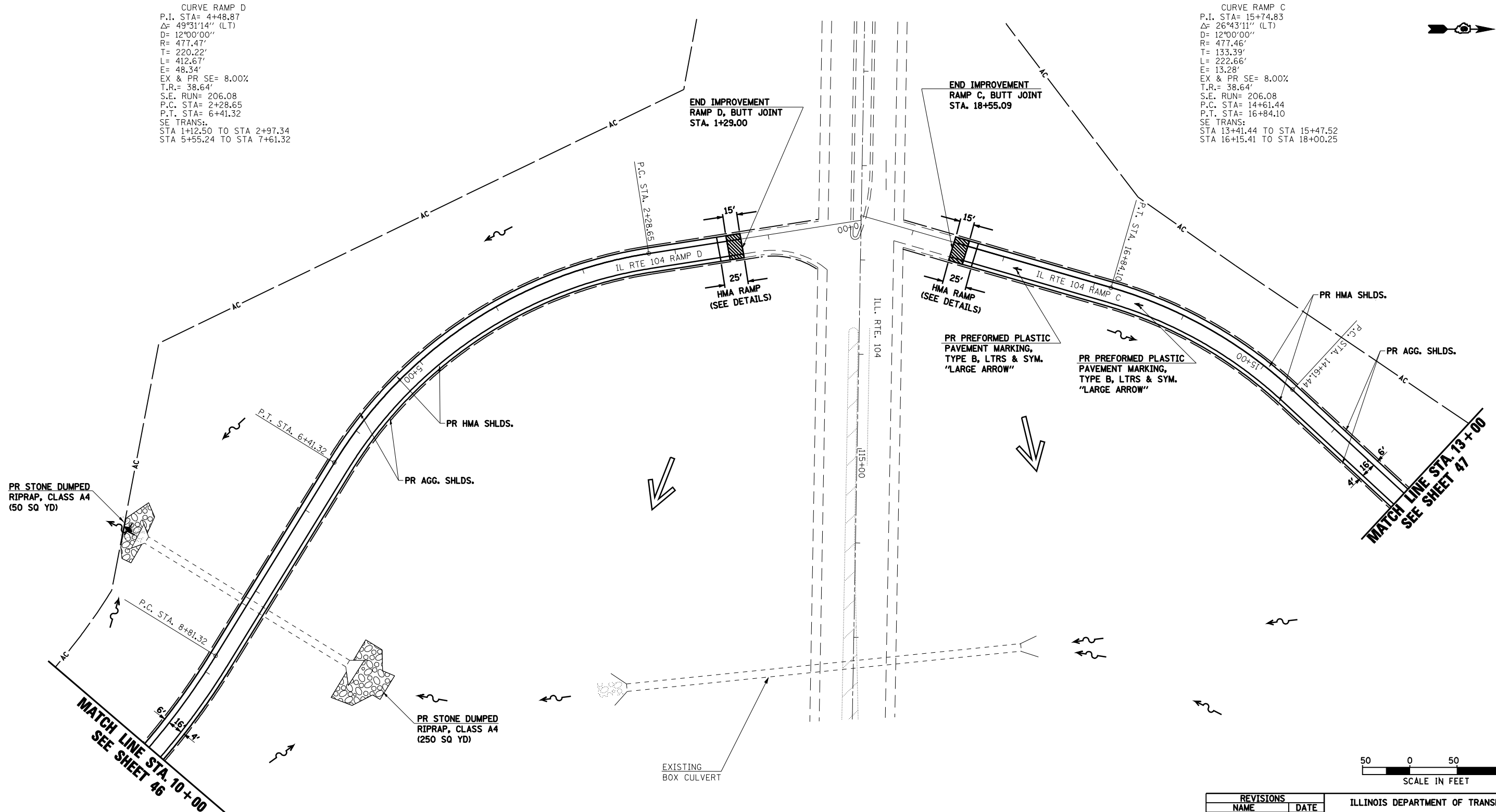
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	72
STA. 104/RAMP C		TO STA.104/RAMP D		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



**CURVE RAMP D**  
P.I. STA= 4+48.87  
 $\Delta$ = 49°31'14" (LT)  
D= 12°00'00"  
R= 477.47'  
T= 220.22'  
L= 412.67'  
E= 48.34'  
EX & PR SE= 8.00%  
T.R.= 38.64'  
S.E. RUN= 206.08  
P.C. STA= 2+28.65  
P.T. STA= 6+41.32  
SE TRANS:  
STA 1+12.50 TO STA 2+97.34  
STA 5+55.24 TO STA 7+61.32

**CURVE RAMP C**  
P.I. STA= 15+74.83  
 $\Delta$ = 26°43'11" (LT)  
D= 12°00'00"  
R= 477.46'  
T= 133.39'  
L= 222.66'  
E= 13.28'  
EX & PR SE= 8.00%  
T.R.= 38.64'  
S.E. RUN= 206.08  
P.C. STA= 14+61.44  
P.T. STA= 16+84.10  
SE TRANS:  
STA 13+41.44 TO STA 15+47.52  
STA 16+15.41 TO STA 18+00.25



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**IL RTE 104 RAMP C & D**  
SCALE: VERT.      DRAWN BY JJS  
          HORIZ.      CHECKED BY LLQ  
DATE 06/08/2004



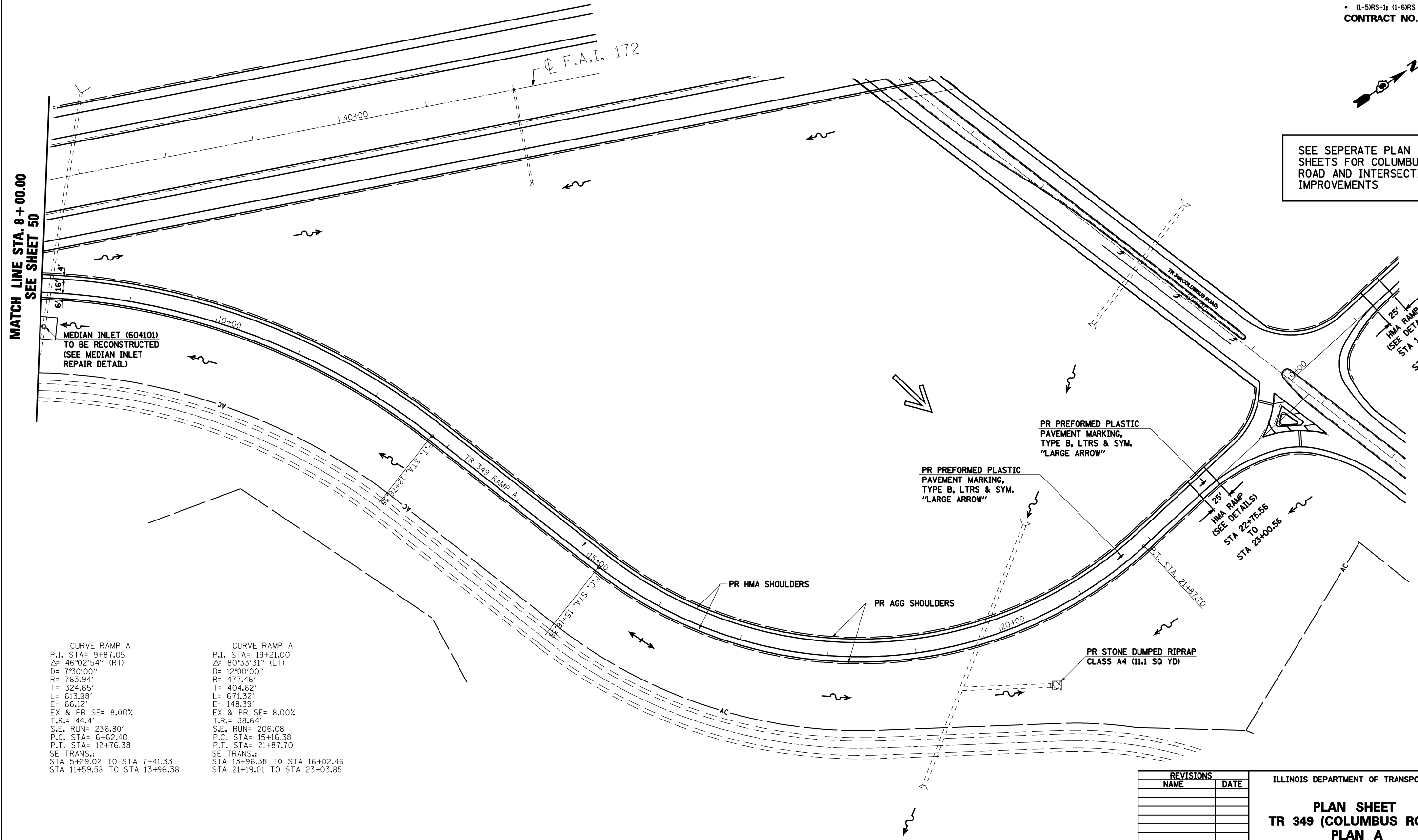
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	73

STA. 349 RAMP A TO STA. 349 RAMP A  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

(1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



SEE SEPERATE PLAN SHEETS FOR COLUMBUS ROAD AND INTERSECTION IMPROVEMENTS



CURVE RAMP A  
 P.I. STA= 9+87.05  
 $\Delta = 46^{\circ}02'54''$  (RT)  
 D= 7 $^{\circ}30'00''$   
 R= 763.94'  
 T= 324.65'  
 L= 613.98'  
 E= 66.12'  
 EX & PR SE= 8.00%  
 T.R.= 44.4'  
 S.E. RUN= 236.80'  
 P.C. STA= 6+62.40  
 P.T. STA= 12+76.38  
 SE TRANS.:  
 STA 5+29.02 TO STA 7+41.33  
 STA 11+59.58 TO STA 13+96.38

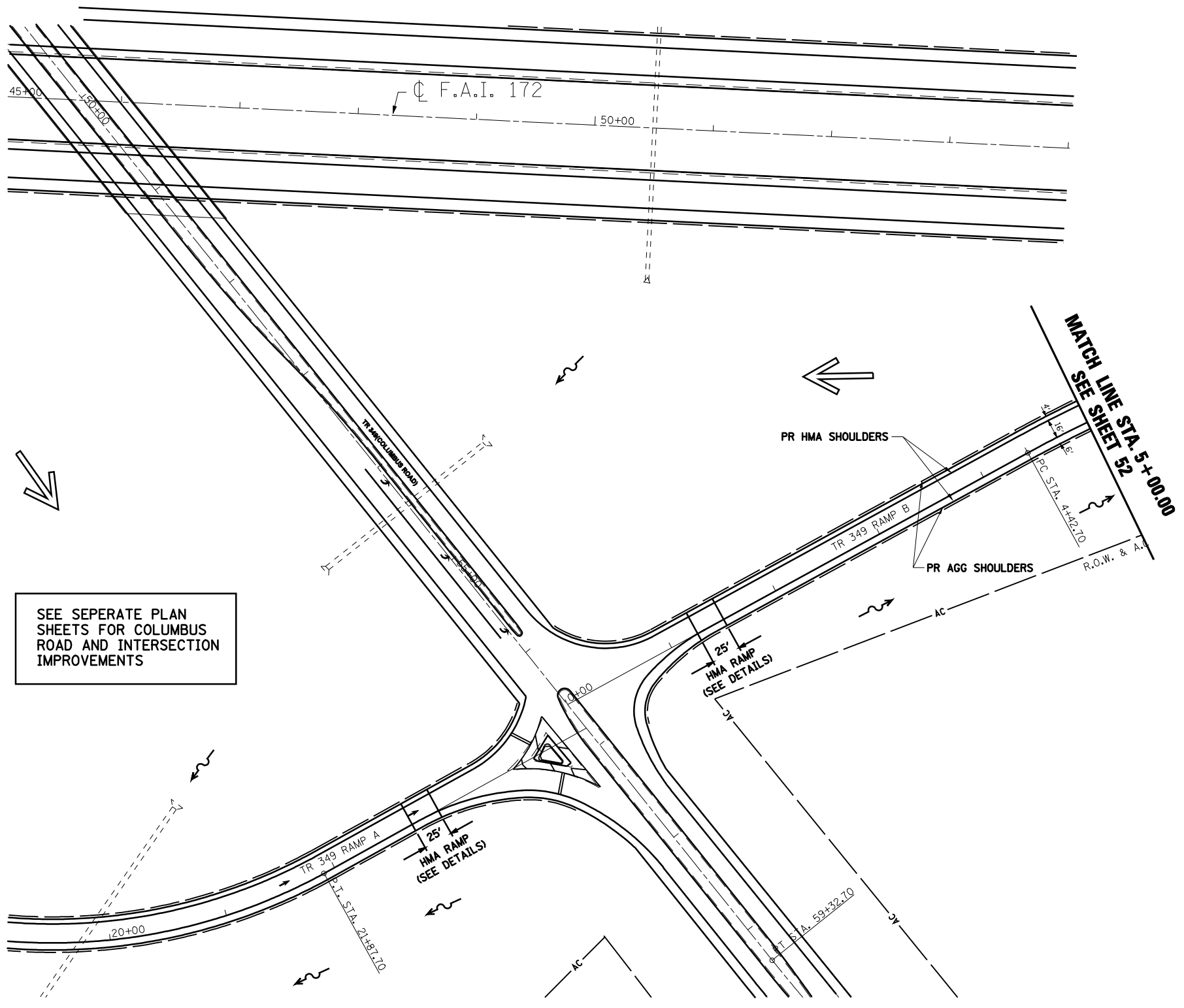
CURVE RAMP A  
 P.I. STA= 19+21.00  
 $\Delta = 80^{\circ}33'31''$  (LT)  
 D= 12 $^{\circ}00'00''$   
 R= 477.46'  
 T= 404.62'  
 L= 671.32'  
 E= 148.39'  
 EX & PR SE= 8.00%  
 T.R.= 38.64'  
 S.E. RUN= 206.08  
 P.C. STA= 15+16.38  
 P.T. STA= 21+87.70  
 SE TRANS.:  
 STA 13+96.38 TO STA 16+02.46  
 STA 21+19.01 TO STA 23+03.85

REVISIONS	
NAME	DATE

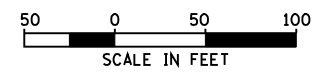
ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**TR 349 (COLUMBUS ROAD)**  
**PLAN A**  
 SCALE: VERT.      DRAWN BY JJS  
           HORIZ.      CHECKED BY LLQ  
 DATE 06/08/2004

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	74
STA. 349 RAMP B		TO STA. 349 RAMP B		
FED. ROAD DIST. NO. 6		ILLINOIS FED. AID PROJECT		

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



SEE SEPERATE PLAN SHEETS FOR COLUMBUS ROAD AND INTERSECTION IMPROVEMENTS



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PLAN SHEET**  
**TR 349 (COLUMBUS ROAD)**  
**RAMP B**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 06/09/2004

DRAWN BY JJS  
 CHECKED BY LLO

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	75

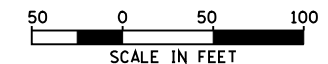
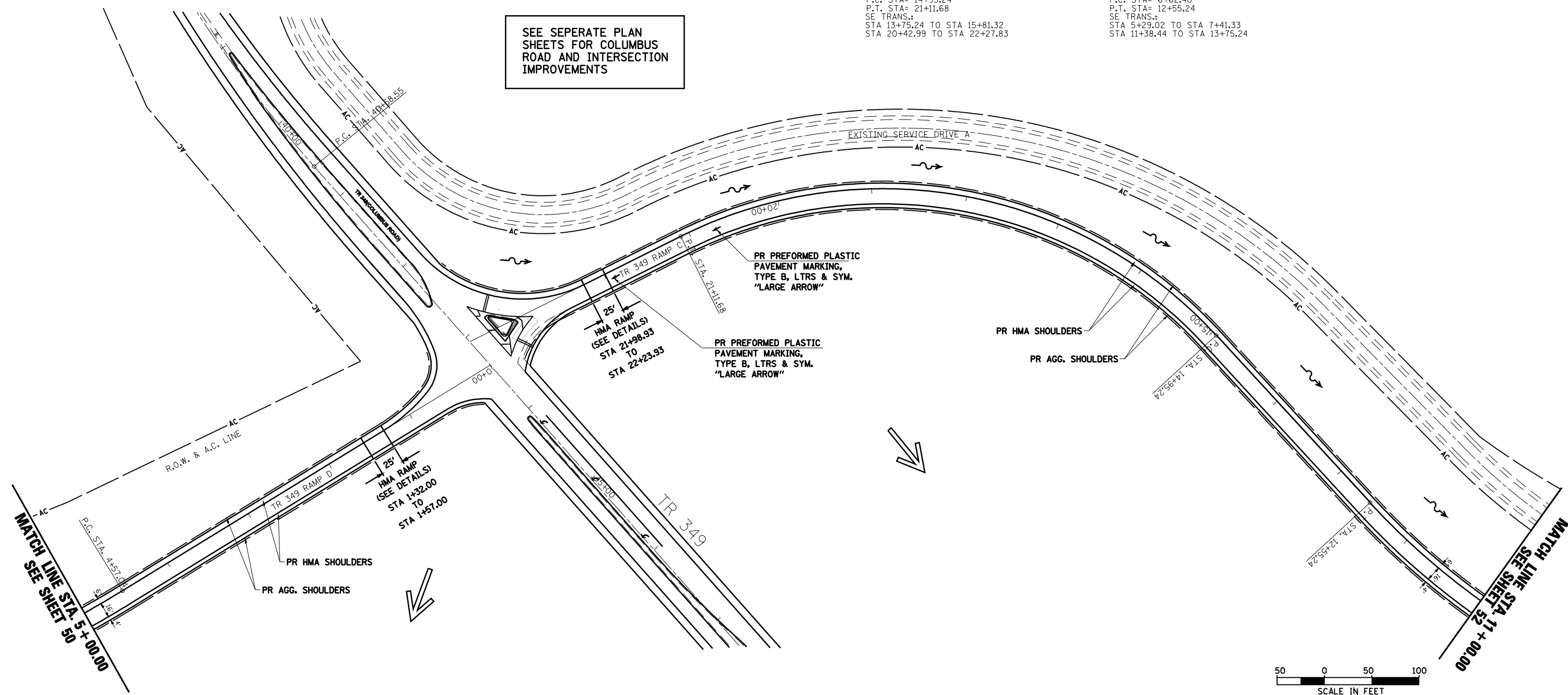
STA. 349 RAMP C TO STA. 349 RAMP D  
 FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT  
 • (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



CURVE RAMP C  
 P.I. STA= 18+54.86  
 $\Delta$ = 73°58'23" (LT)  
 D= 12°00'00"  
 R= 477.46'  
 T= 359.62'  
 L= 616.44'  
 E= 120.28'  
 EX & PR SE= 8.00%  
 T.R.= 21.24'  
 S.E. RUN= 38.64'  
 P.C. STA= 14+95.24  
 P.T. STA= 21+11.68  
 SE TRANS.:  
 STA 13+75.24 TO STA 15+81.32  
 STA 20+42.99 TO STA 22+27.83

CURVE RAMP D  
 P.I. STA= 9+74.65  
 $\Delta$ = 44°27'46" (RT)  
 D= 7°30'00"  
 R= 763.94'  
 T= 312.25'  
 L= 592.84'  
 E= 61.35'  
 EX & PR SE= 8.00%  
 T.R.= 44.4'  
 S.E. RUN= 236.80'  
 P.C. STA= 6+62.40  
 P.T. STA= 12+55.24  
 SE TRANS.:  
 STA 5+29.02 TO STA 7+41.33  
 STA 11+38.44 TO STA 13+75.24

SEE SEPERATE PLAN SHEETS FOR COLUMBUS ROAD AND INTERSECTION IMPROVEMENTS

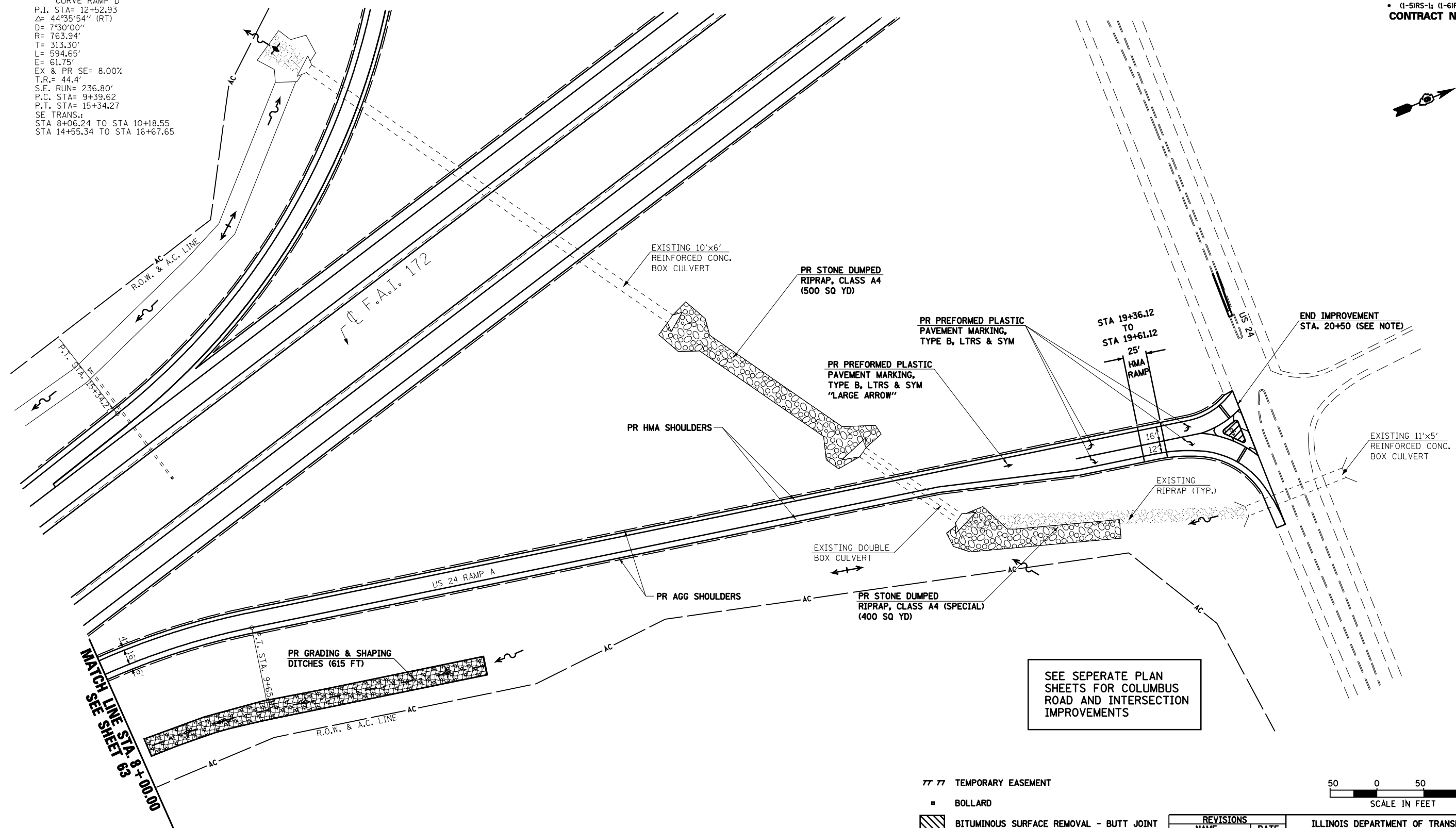


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**TR 349 (COLUMBUS ROAD)**  
**RAMP C & D**  
 SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 06/09/2004  
 DRAWN BY JJS  
 CHECKED BY LLO

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	76
STA. 24/RAMP A		TO STA.24/RAMP A		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				
• (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				

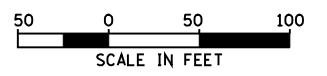
CURVE RAMP D  
P.I. STA= 12+52.93  
 $\Delta = 44^{\circ}35'54''$  (RT)  
D= 730'00"  
R= 763.94'  
T= 313.30'  
L= 594.65'  
E= 61.75'  
EX & PR SE= 8.00%  
T.R.= 44.4'  
S.E. RUN= 236.80'  
P.C. STA= 9+39.62  
P.T. STA= 15+34.27  
SE TRANS.:  
STA 8+06.24 TO STA 10+18.55  
STA 14+55.34 TO STA 16+67.65



SEE SEPERATE PLAN SHEETS FOR COLUMBUS ROAD AND INTERSECTION IMPROVEMENTS

NOTE:  
SEE INTERCHANGE DETAILS (I-172 & RAMP A) FOR WIDENING AND HMA BUTT JOINT REMOVAL AT INTERSECTION

- TEMPORARY EASEMENT
- BOLLARD
- ▨ BITUMINOUS SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ EROSION CONTROL BLANKET
- ⊕ TEMPORARY DITCH CHECK
- ~ DITCH FLOW LINE

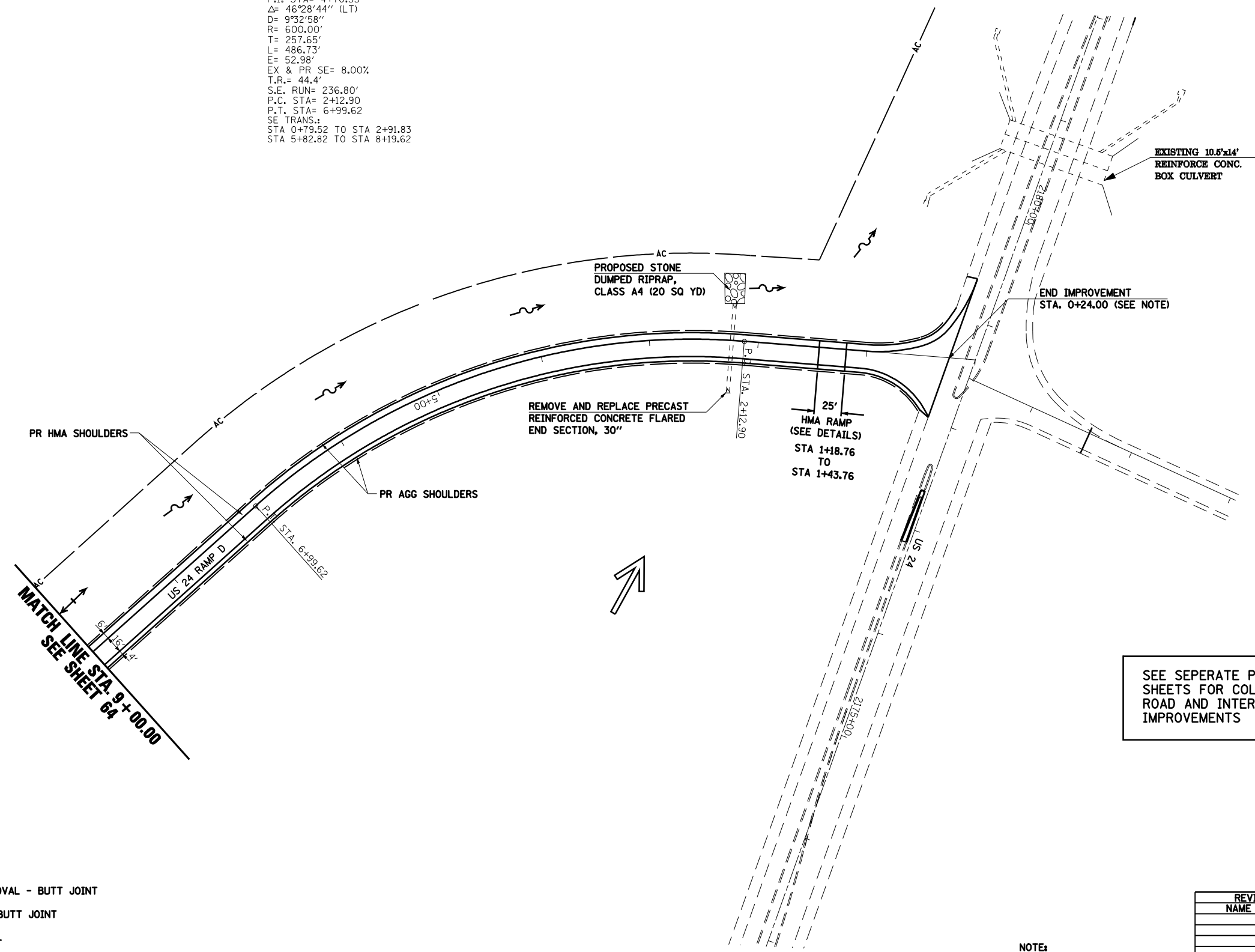


REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**PLAN SHEET**  
**US 24 RAMP A**  
SCALE: VERT.     DRAWN BY JJS  
HORIZ.             CHECKED BY LLQ  
DATE 06/11/2004

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	77
STA. 24/RAMP D		TO STA.24/RAMP D		
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT				
• (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				

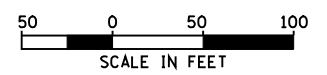
CURVE RAMP D  
P.I. STA= 4+70.55  
Δ= 46°28'44" (LT)  
D= 9°32'58"  
R= 600.00'  
T= 257.65'  
L= 486.73'  
E= 52.98'  
EX & PR SE= 8.00%  
T.R.= 44.4'  
S.E. RUN= 236.80'  
P.C. STA= 2+12.90  
P.T. STA= 6+99.62  
SE TRANS.:  
STA 0+79.52 TO STA 2+91.83  
STA 5+82.82 TO STA 8+19.62



MATCH LINE STA. 9+00.00  
SEE SHEET 64

SEE SEPERATE PLAN SHEETS FOR COLUMBUS ROAD AND INTERSECTION IMPROVEMENTS

- TEMPORARY EASEMENT
- BOLLARD
- ▨ BITUMINOUS SURFACE REMOVAL - BUTT JOINT
- ▩ PCC SURFACE REMOVAL - BUTT JOINT
- ▧ EROSION CONTROL BLANKET
- ⚡ TEMPORARY DITCH CHECK (AGG. EROS. CONTROL)
- ~ DITCH FLOW LINE



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**PLAN SHEET  
US 24 RAMP D**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 06/11/2004

DRAWN BY JJS  
CHECKED BY LLQ

NOTE:  
SEE INTERCHANGE DETAILS (I-172 & U.S. 24 RAMP D) FOR WIDENING AND HMA BUTT JOINT REMOVAL AT INTERSECTION

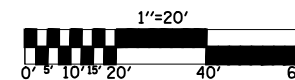
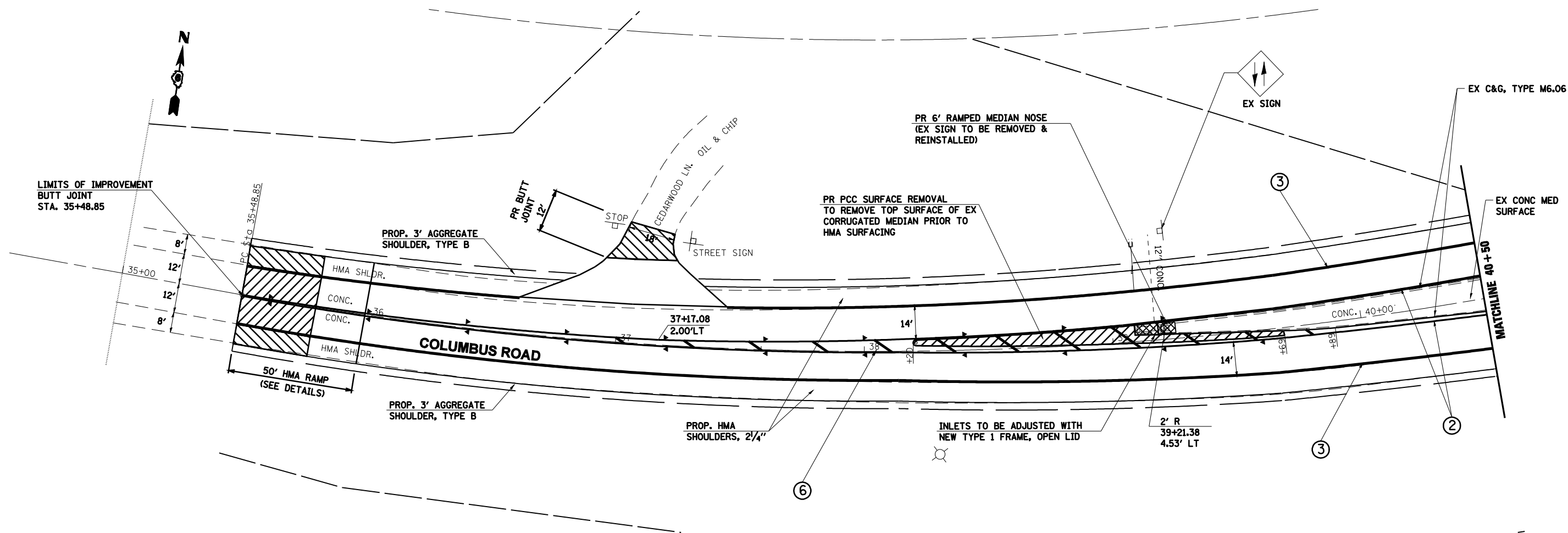
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	78
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		• (1-5)RS-1; (1-6)RS		

**CONTRACT NO. 72694**

**CURVE**  
P.I. STA= 38+06.42  
 $\Delta$ = 20°23'20" (LT)  
D= 4°00'00"  
R= 1432.40'  
T= 257.58'  
L= 509.72'  
E= 22.98'  
EX & PR SE= 4.70%  
T.R.= 36.00'  
S.E. RUN= 112.80'  
P.C. STA= 35+48.83  
P.T. STA= 40+58.55  
SE TRANS.:  
STA 35+48.85 TO STA 35+86.43  
STA 40+20.95 TO STA 41+69.75

**PAVEMENT MARKING LEGEND**

- ① PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 5" (30' SKIP, 10' WHITE DASH)
- ② PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID YELLOW)
- ③ PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID WHITE)
- ④ PR URETHANE PAVEMENT MARKING, LINE 6" (SOLID WHITE)
- ⑤ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 8" (SOLID WHITE)
- ⑥ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID YELLOW)
- ⑦ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID WHITE)
- ⑧ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 24" (SOLID WHITE)
- ⑨ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LETTERS & SYMBOLS (SOLID WHITE)
- ◀ PR RAISED PAVEMENT MARKERS, ONE-WAY CRYSTAL
- ◀ PR RAISED PAVEMENT MARKERS, ONE-WAY AMBER
- ◆ PR RAISED PAVEMENT MARKERS, TWO-WAY AMBER



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERCHANGE DETAILS**  
**I-172 & COLUMBUS ROAD**  
**ADAMS COUNTY**

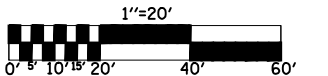
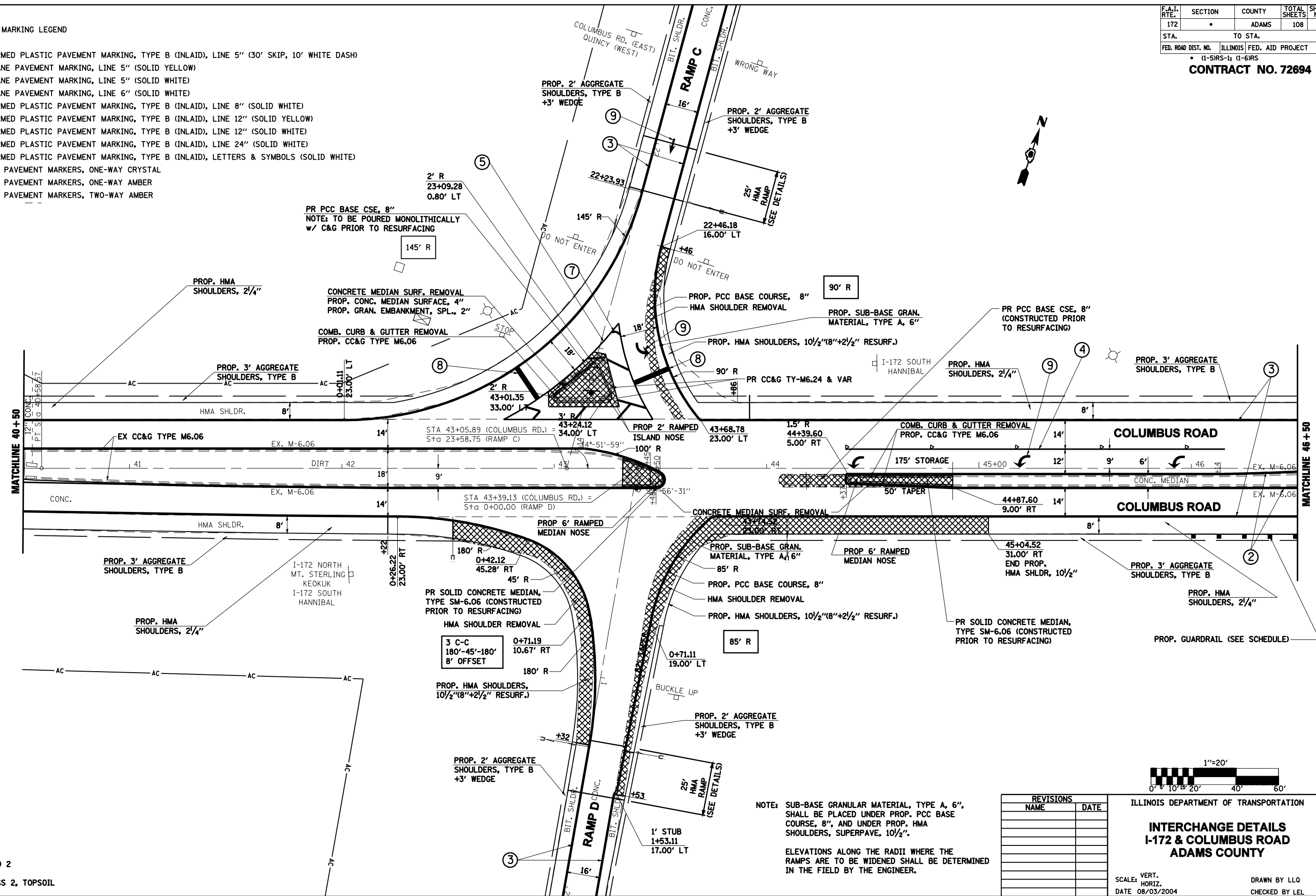
SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 08/03/2004  
DRAWN BY LLO  
CHECKED BY LEL

c:\p\work\p\DOT\LAUGH\INR\lma26563\078-082 PROPOSED.COLUMBUS Interchange Plans\_IDOT\_REV.dgn  
Feb-11-2009 12:48:40PM  
\*REF01

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172		ADAMS	108	79
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		• (1-5)RS-1; (1-6)RS		
<b>CONTRACT NO. 72694</b>				

**PAVEMENT MARKING LEGEND**

- ① PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 5" (30' SKIP, 10' WHITE DASH)
- ② PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID YELLOW)
- ③ PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID WHITE)
- ④ PR URETHANE PAVEMENT MARKING, LINE 6" (SOLID WHITE)
- ⑤ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 8" (SOLID WHITE)
- ⑥ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID YELLOW)
- ⑦ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID WHITE)
- ⑧ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 24" (SOLID WHITE)
- ⑨ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LETTERS & SYMBOLS (SOLID WHITE)
- ◀ PR RAISED PAVEMENT MARKERS, ONE-WAY CRYSTAL
- ▲ PR RAISED PAVEMENT MARKERS, ONE-WAY AMBER
- ◆ PR RAISED PAVEMENT MARKERS, TWO-WAY AMBER



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERCHANGE DETAILS**  
**I-172 & COLUMBUS ROAD**  
**ADAMS COUNTY**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/03/2004

DRAWN BY LLO  
 CHECKED BY LEL

NOTE: SUB-BASE GRANULAR MATERIAL, TYPE A, 6", SHALL BE PLACED UNDER PROP. PCC BASE COURSE, 8", AND UNDER PROP. HMA SHOULDERS, SUPERPAVE, 10 1/2".

ELEVATIONS ALONG THE RADII WHERE THE RAMP ARE TO BE WIDENED SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

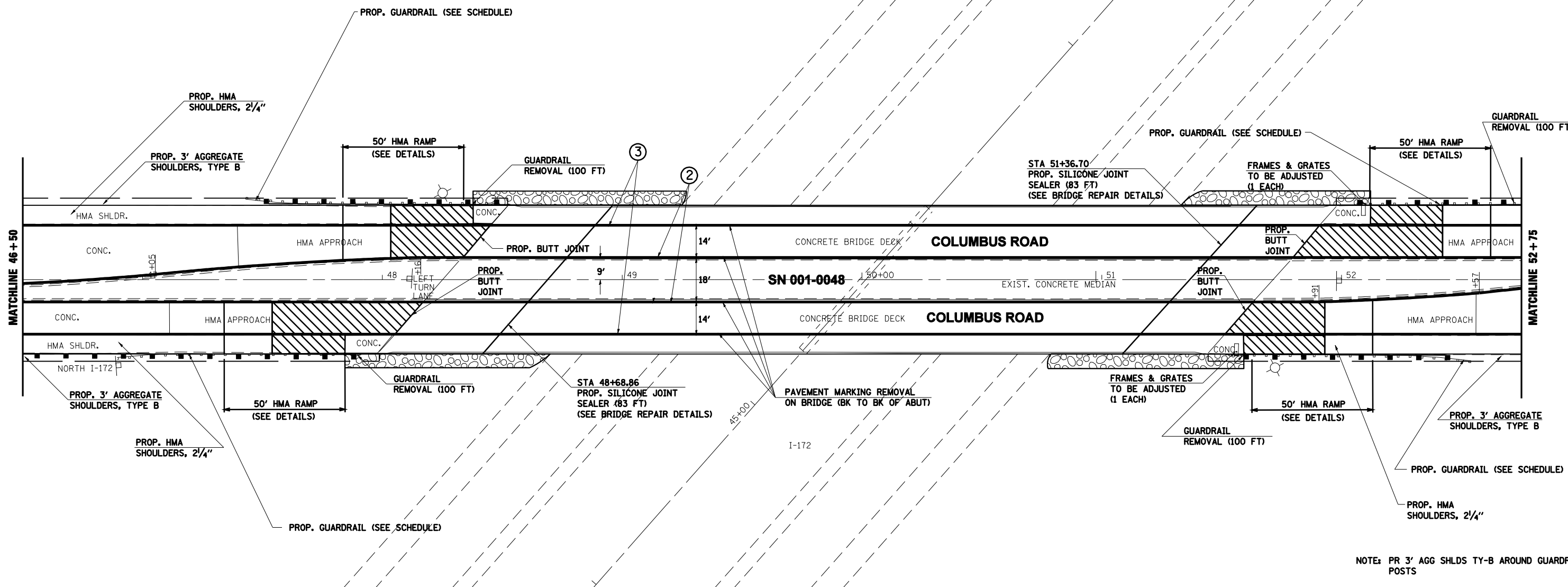
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	80
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		• (1-5)RS-1; (1-6)RS		

**CONTRACT NO. 72694**

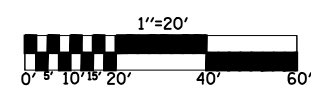
**PAVEMENT MARKING LEGEND**

- ① PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 5" (30' SKIP, 10' WHITE DASH)
- ② PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID YELLOW)
- ③ PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID WHITE)
- ④ PR URETHANE PAVEMENT MARKING, LINE 6" (SOLID WHITE)
- ⑤ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 8" (SOLID WHITE)
- ⑥ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID YELLOW)
- ⑦ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID WHITE)
- ⑧ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 24" (SOLID WHITE)
- ⑨ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LETTERS & SYMBOLS (SOLID WHITE)
- ▲ PR RAISED PAVEMENT MARKERS, ONE-WAY CRYSTAL
- ▲ PR RAISED PAVEMENT MARKERS, ONE-WAY AMBER
- ◆ PR RAISED PAVEMENT MARKERS, TWO-WAY AMBER



SEE SEPERATE DETAILS FOR BRIDGE REPAIRS SN 001-0048

NOTE: PR 3' AGG SHLDS TY-B AROUND GUARDRAIL POSTS



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERCHANGE DETAILS**  
**I-172 & COLUMBUS ROAD**  
**ADAMS COUNTY**

SCALE: VERT. \_\_\_\_\_  
 HORIZ. \_\_\_\_\_  
 DATE 08/03/2004  
 DRAWN BY LLQ  
 CHECKED BY LEL

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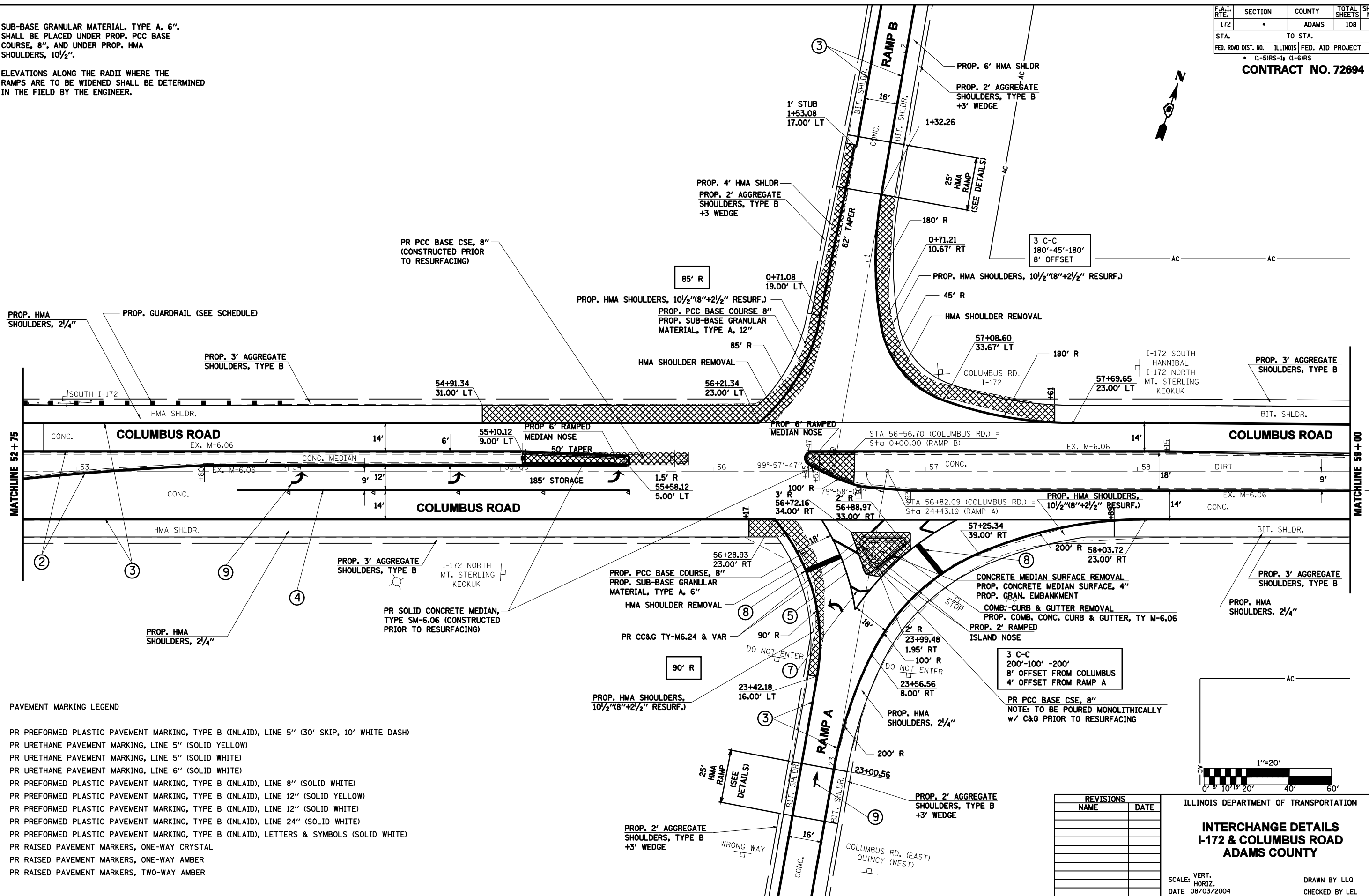


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172		ADAMS	108	81
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		• (1-5)RS-1; (1-6)RS		

**CONTRACT NO. 72694**

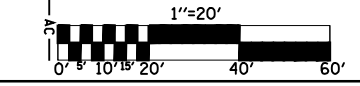
NOTE: SUB-BASE GRANULAR MATERIAL, TYPE A, 6", SHALL BE PLACED UNDER PROP. PCC BASE COURSE, 8", AND UNDER PROP. HMA SHOULDERS, 10 1/2".

ELEVATIONS ALONG THE RADII WHERE THE RAMPS ARE TO BE WIDENED SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.



- PAVEMENT MARKING LEGEND
- ① PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 5" (30' SKIP, 10' WHITE DASH)
  - ② PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID YELLOW)
  - ③ PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID WHITE)
  - ④ PR URETHANE PAVEMENT MARKING, LINE 6" (SOLID WHITE)
  - ⑤ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 8" (SOLID WHITE)
  - ⑥ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID YELLOW)
  - ⑦ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID WHITE)
  - ⑧ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 24" (SOLID WHITE)
  - ⑨ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LETTERS & SYMBOLS (SOLID WHITE)
  - ▲ PR RAISED PAVEMENT MARKERS, ONE-WAY CRYSTAL
  - ▲ PR RAISED PAVEMENT MARKERS, ONE-WAY AMBER
  - ◆ PR RAISED PAVEMENT MARKERS, TWO-WAY AMBER

REVISIONS	
NAME	DATE



ILLINOIS DEPARTMENT OF TRANSPORTATION

**INTERCHANGE DETAILS  
I-172 & COLUMBUS ROAD  
ADAMS COUNTY**

SCALE: VERT. \_\_\_\_\_  
HORIZ. \_\_\_\_\_  
DATE 08/03/2004

DRAWN BY LLQ  
CHECKED BY LEL

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 \*REF01

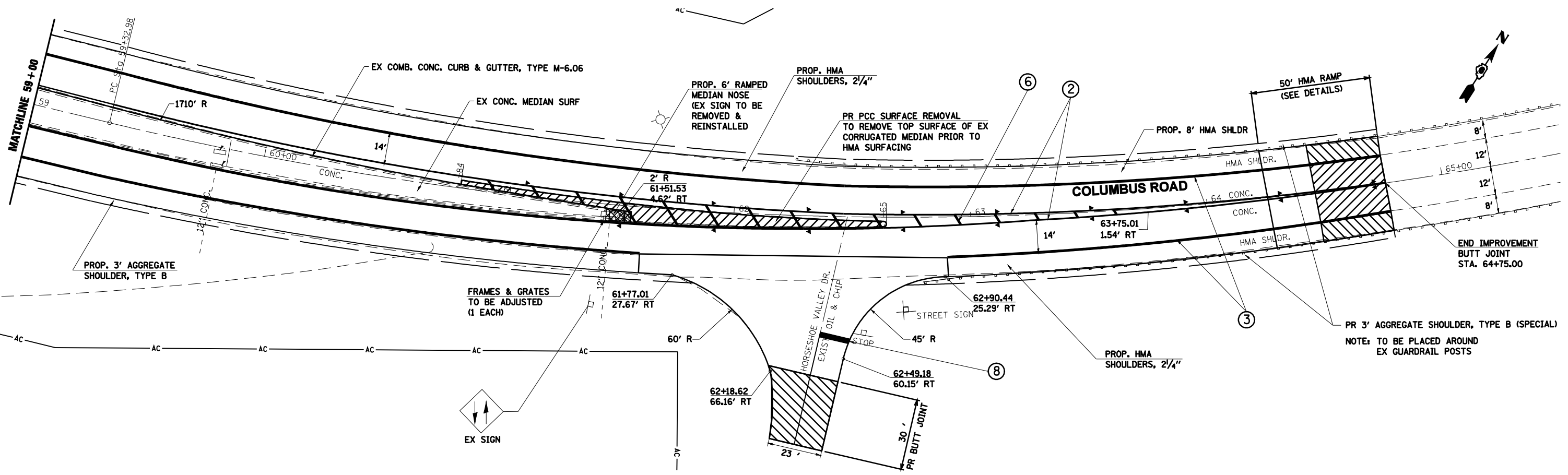
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172		ADAMS	108	82
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		
		• (1-5)RS-1; (1-6)RS		

**CONTRACT NO. 72694**

**CURVE**  
 P.I. STA= 62+78.35  
 $\Delta = 27^{\circ}7'59''$  (LT)  
 $D = 4^{\circ}00'00''$   
 $R = 1432.40'$   
 $T = 345.65'$   
 $L = 678.33'$   
 $E = 41.11'$   
 $EX \ \& \ PR \ SE = 6.30\%$   
 $T.R. = 36.00'$   
 $S.E. \ RUN = 151.20'$   
 $P.C. \ STA = 59+32.70$   
 $P.T. \ STA = 66+11.03$   
 $SE \ TRANS.:$   
 $STA \ 57+90.00 \ TO \ STA \ 59+77.20$

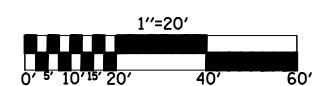
**PAVEMENT MARKING LEGEND**

- ① PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 5" (30' SKIP, 10' WHITE DASH)
- ② PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID YELLOW)
- ③ PR URETHANE PAVEMENT MARKING, LINE 5" (SOLID WHITE)
- ④ PR URETHANE PAVEMENT MARKING, LINE 6" (SOLID WHITE)
- ⑤ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 8" (SOLID WHITE)
- ⑥ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID YELLOW)
- ⑦ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 12" (SOLID WHITE)
- ⑧ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LINE 24" (SOLID WHITE)
- ⑨ PR PREFORMED PLASTIC PAVEMENT MARKING, TYPE B (INLAID), LETTERS & SYMBOLS (SOLID WHITE)
- ▲ PR RAISED PAVEMENT MARKERS, ONE-WAY CRYSTAL
- ▲ PR RAISED PAVEMENT MARKERS, ONE-WAY AMBER
- ◆ PR RAISED PAVEMENT MARKERS, TWO-WAY AMBER



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 \*REF01

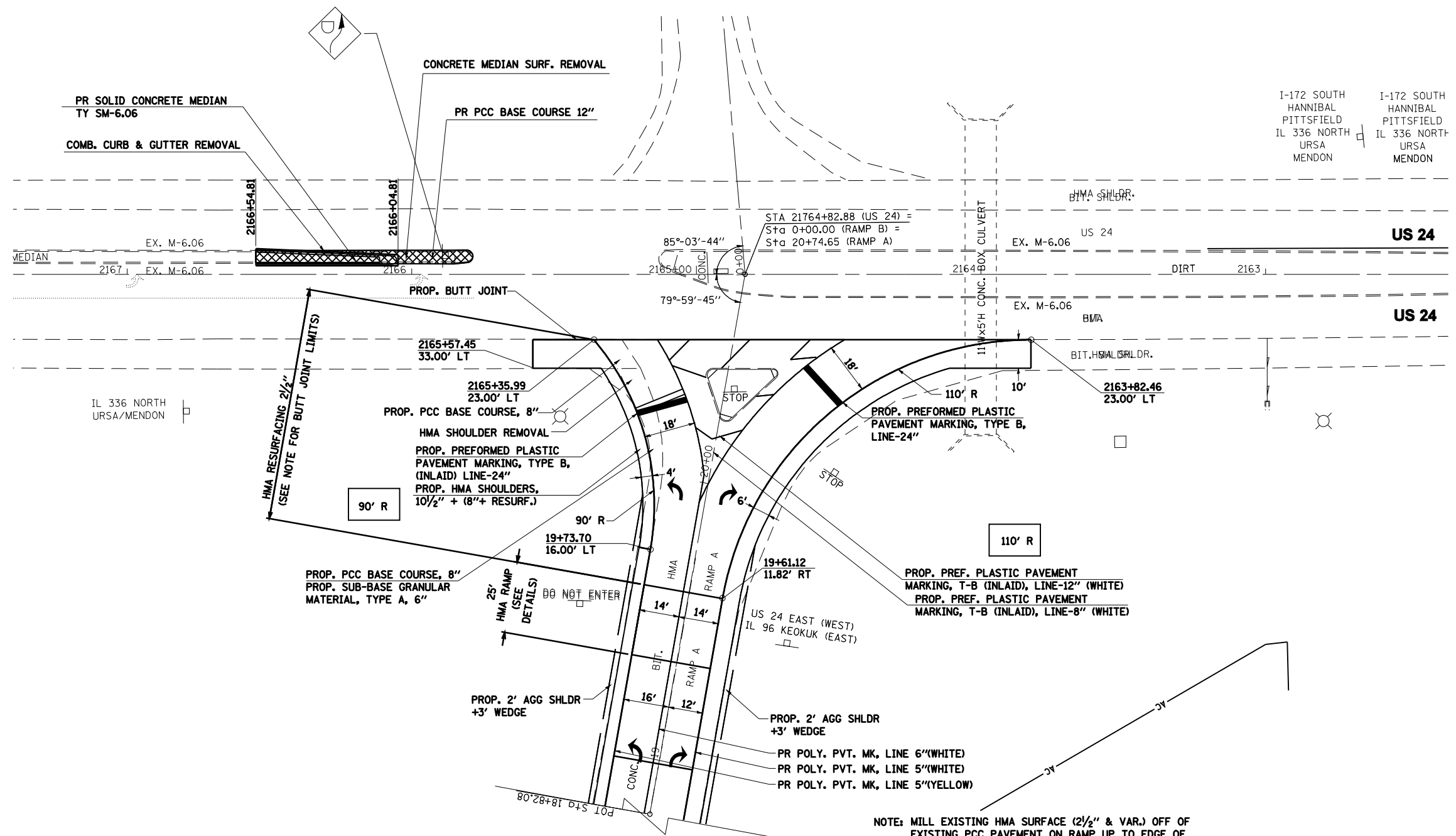
- BITUMINOUS SURFACE REMOVAL - BUTT JOINT
- PCC SURFACE REMOVAL - BUTT JOINT



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERCHANGE DETAILS**  
**I-172 & COLUMBUS ROAD**  
**ADAMS COUNTY**  
 SCALE: VERT. 1"=20'  
 HORIZ. 1"=40'  
 DATE 08/03/2004  
 DRAWN BY LLO  
 CHECKED BY LEL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	83
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS	FED. AID PROJECT	
• (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				



I-172 SOUTH HANNIBAL PITTSFIELD IL 336 NORTH URSA MENDON  
I-172 SOUTH HANNIBAL PITTSFIELD IL 336 NORTH URSA MENDON

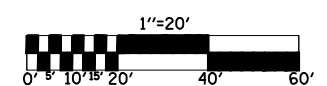
IL 336 NORTH URSA/MENDON

NOTE: MILL EXISTING HMA SURFACE (2 1/2\"/>

RESURFACE (1\"/>

SUB-BASE GRANULAR MATERIAL, TYPE A, 6\", SHALL BE PLACED UNDER PROP. PCC BASE COURSE, 8\", AND UNDER PROP. BITUMINOUS SHOULDERS, SUPERPAVE, 10 1/2\"/>

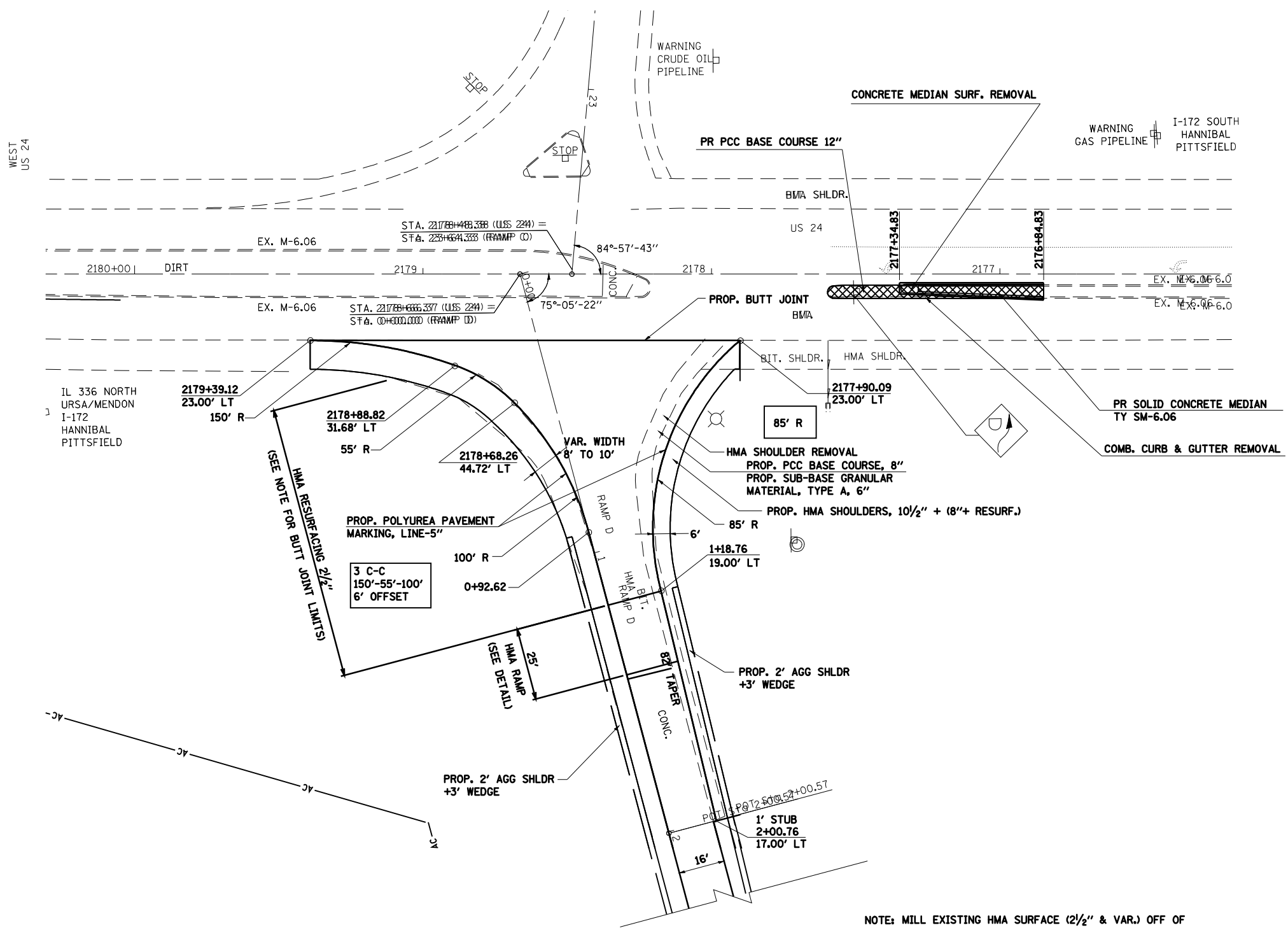
ELEVATIONS ALONG THE RADII WHERE THE RAMPS ARE TO BE WIDENED SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
**INTERCHANGE DETAILS**  
**I-172 & US 24 RAMP A**  
SCALE: VERT. HORIZ. DATE: 08/03/2004  
DRAWN BY: LLO  
CHECKED BY: LEL

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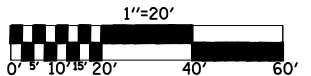


NOTE: MILL EXISTING HMA SURFACE (2 1/2\"/>

RESURFACE (1\"/>

SUB-BASE GRANULAR MATERIAL, TYPE A, 6\", SHALL BE PLACED UNDER PROP. PCC BASE COURSE, 8\", AND UNDER PROP. BITUMINOUS SHOULDERS, SUPERPAVE, 10 1/2\"/>

ELEVATIONS ALONG THE RADII WHERE THE RAMPS ARE TO BE WIDENED SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER



REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

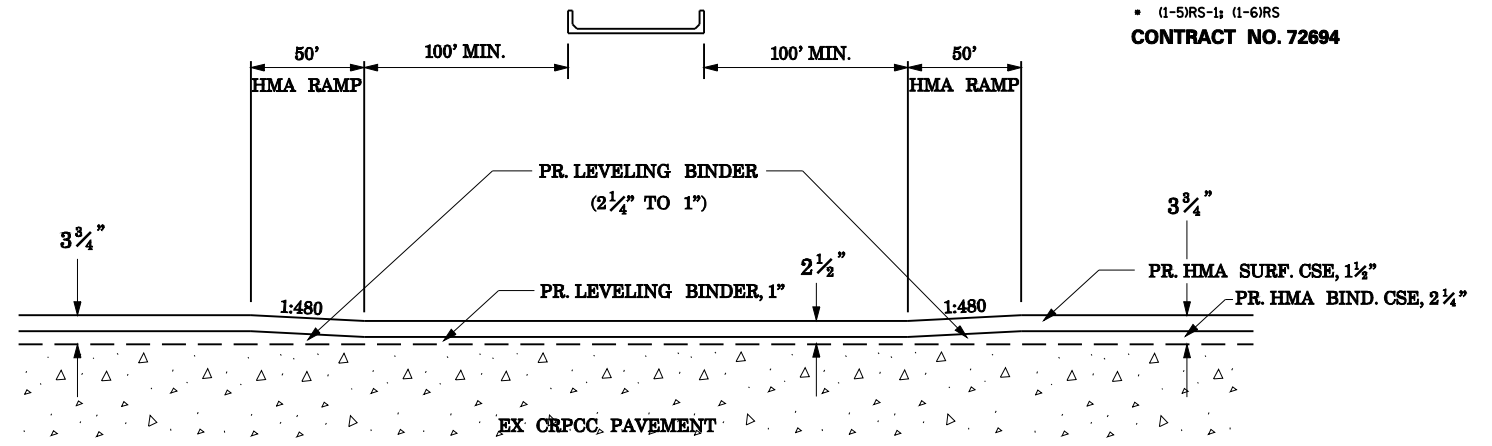
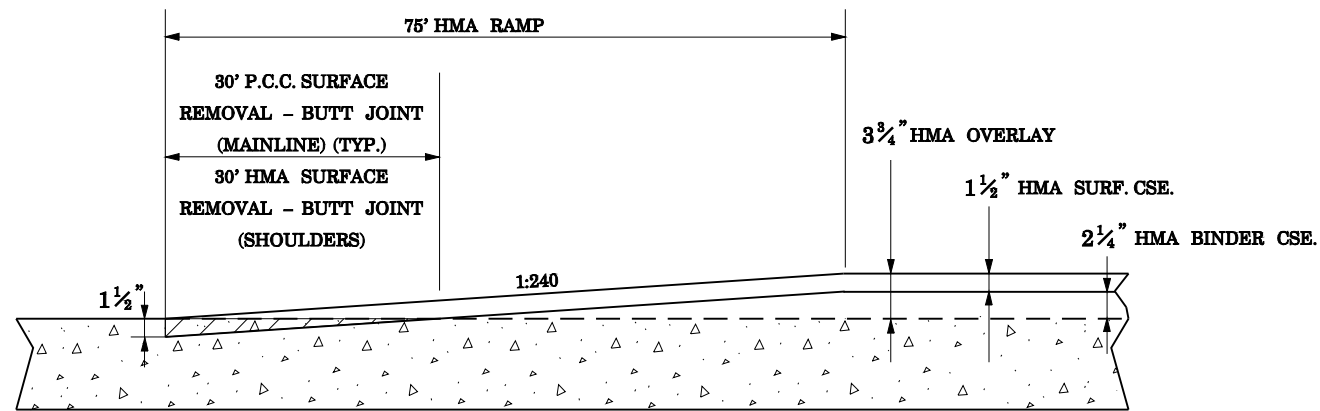
**INTERCHANGE DETAILS  
I-172 & US 24 RAMP D**

SCALE: VERT.  
HORIZ.  
DATE 08/03/2004

DRAWN BY LLQ  
CHECKED BY LEL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	85
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS FED. AID PROJECT		
• (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				

### BUTT JOINT DETAIL - FAI 172 & RAMPS



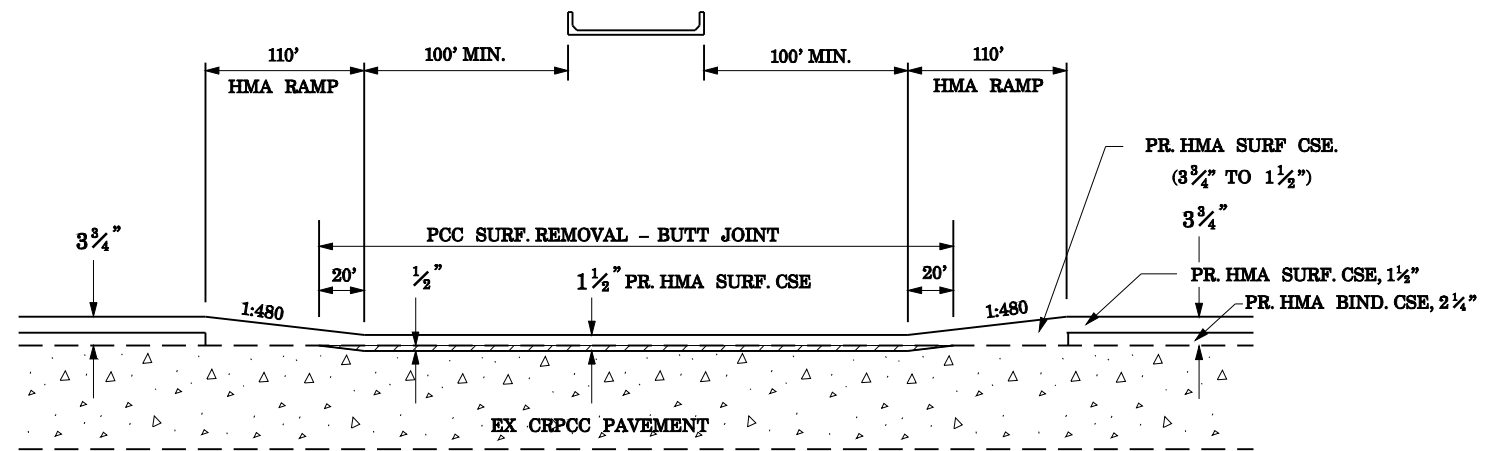
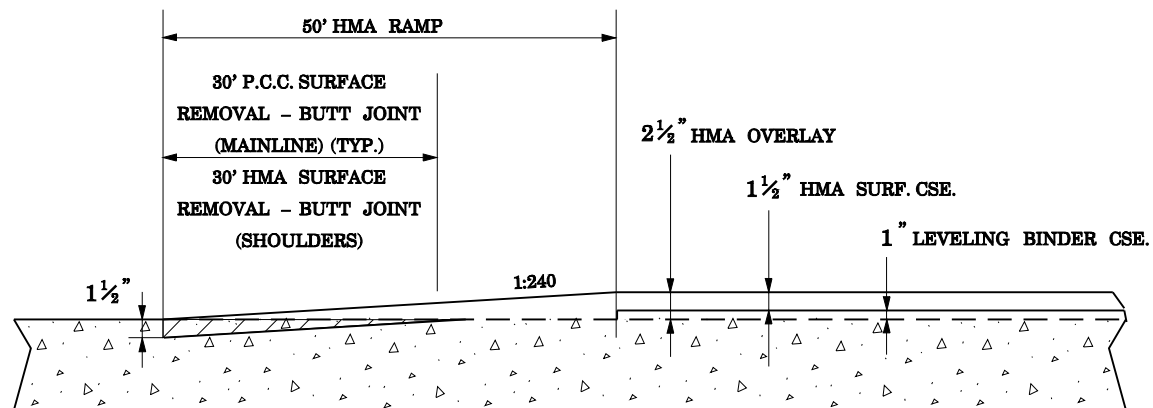
### HMA RAMP LOCATIONS

- STA 75+24.00 TO STA 75+99.00 (S.B.)
  - STA 78+25.00 TO STA 79+00.00 (N.B.)
  - STA 157+76.00 TO STA 158+51.00 (S.B.)
  - STA 159+67.00 TO STA 160+42.00 (N.B.)
  - STA 162+43.00 TO STA 163+18.00 (S.B.)
  - STA 164+06.00 TO STA 164+81.00 (N.B.)
  - STA 246+81.46 TO STA 247+56.46 (S.B.)
  - STA 246+56.66 TO STA 247+31.66 (N.B.)
- } MILL & RESURFACE  
1 1/2\"/>

### HMA RAMP LOCATIONS

STA 44+00.00 TO STA 48+25.00 (NB - @ COLUMBUS ROAD)

### BUTT JOINT DETAIL - TR 349



### HMA RAMP LOCATIONS

- TR 349 (COLUMBUS ROAD)
- STA 35+48.85 TO STA 35+98.85
  - RT STA 47+34.00 TO RT STA 47+84.00
  - LT STA 47+83.50 TO LT STA 48+33.50
  - RT STA 51+63.00 TO RT STA 52+13.00
  - LT STA 52+24.00 TO LT STA 52+74.00
  - STA 64+25.00 TO STA 64+75.00
- } MILL & RESURFACE  
1 1/2\"/>

### HMA RAMP LOCATIONS

STA 149+00.00 TO STA 153+60.00 (SB - @ ST. ANTHONY RD.)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

### HMA RAMP AND BUTT JOINT DETAIL

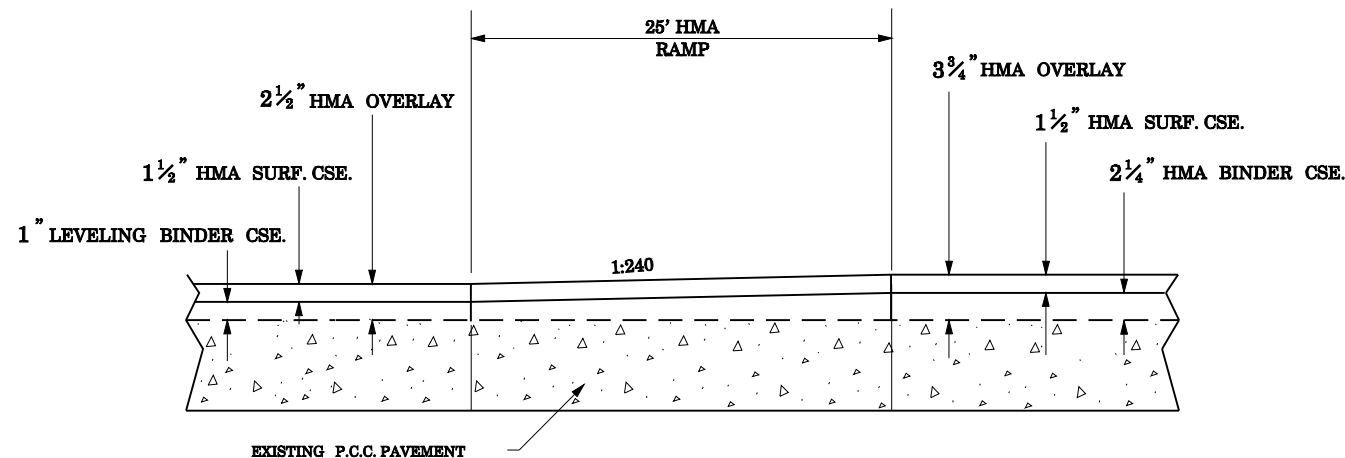
SCALE: VERT. NONE  
HORIZ. NONE  
DATE: 06/09/2004

DRAWN BY LLQ  
CHECKED BY LEL

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	86

STA. TO STA.  
FED. ROAD DIST. NO. 6 ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



**HMA THICKNESS TRANSITIONS  
TR 349 RAMPS & U.S. 24 RAMPS**

**COLUMBUS ROAD**

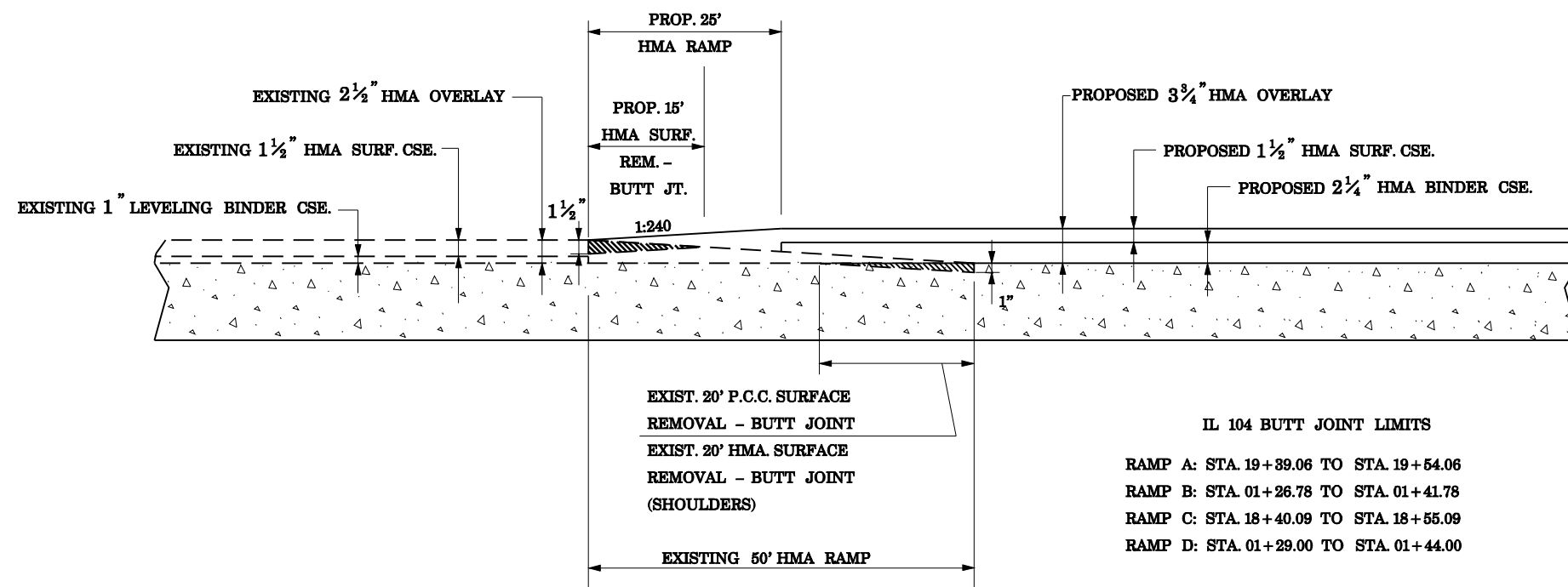
- RAMP A: STA. 22+75.56 (3 3/4") TO STA. 23+00.56 (2 1/2")
- RAMP B: STA. 1+32.26 (2 1/2") TO STA. 1+57.26 (3 3/4")
- RAMP C: STA. 21+98.93 (3 3/4") TO STA. 23+23.93 (2 1/2")
- RAMP D: STA. 1+32.00 (2 1/2") TO STA. 1+52.00 (3 3/4")

**US 24 RAMPS**

- RAMP A: STA. 19+36.12 (3 3/4") TO STA. 19+61.12 (2 1/2")
- RAMP D: STA. 1+18.76 (2 1/2") TO STA. 1+43.76 (3 3/4")

MILL EXIST HMA SURFACE (2 1/2" & VAR.) OFF EXIST PCC PAVEMENT ON RAMPS A & D UP TO EDGE OF PAVEMENT ON US 24, RESURFACE 2 1/2" (1" LEVEL BINDER & 1 1/2" SURFACE) FROM EDGE OF PAVEMENT ON US 24 TO START OF HMA RAMP

**BUTT JOINT DETAIL - IL RTE 104 RAMPS**



**IL 104 BUTT JOINT LIMITS**

- RAMP A: STA. 19+39.06 TO STA. 19+54.06
- RAMP B: STA. 01+26.78 TO STA. 01+41.78
- RAMP C: STA. 18+40.09 TO STA. 18+55.09
- RAMP D: STA. 01+29.00 TO STA. 01+44.00

REVISIONS	
NAME	DATE

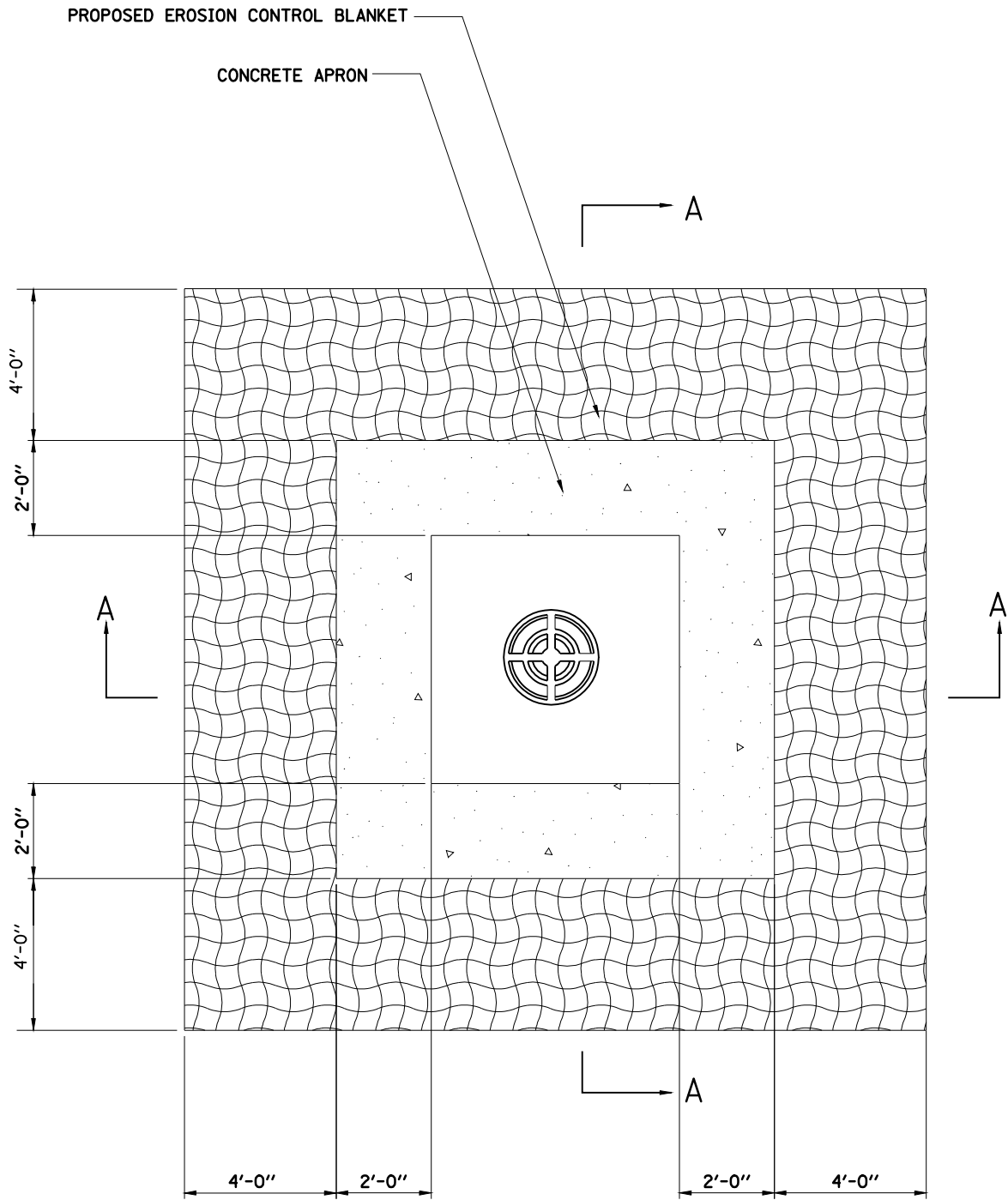
ILLINOIS DEPARTMENT OF TRANSPORTATION

**HMA RAMP AND BUTT JOINT DETAIL**

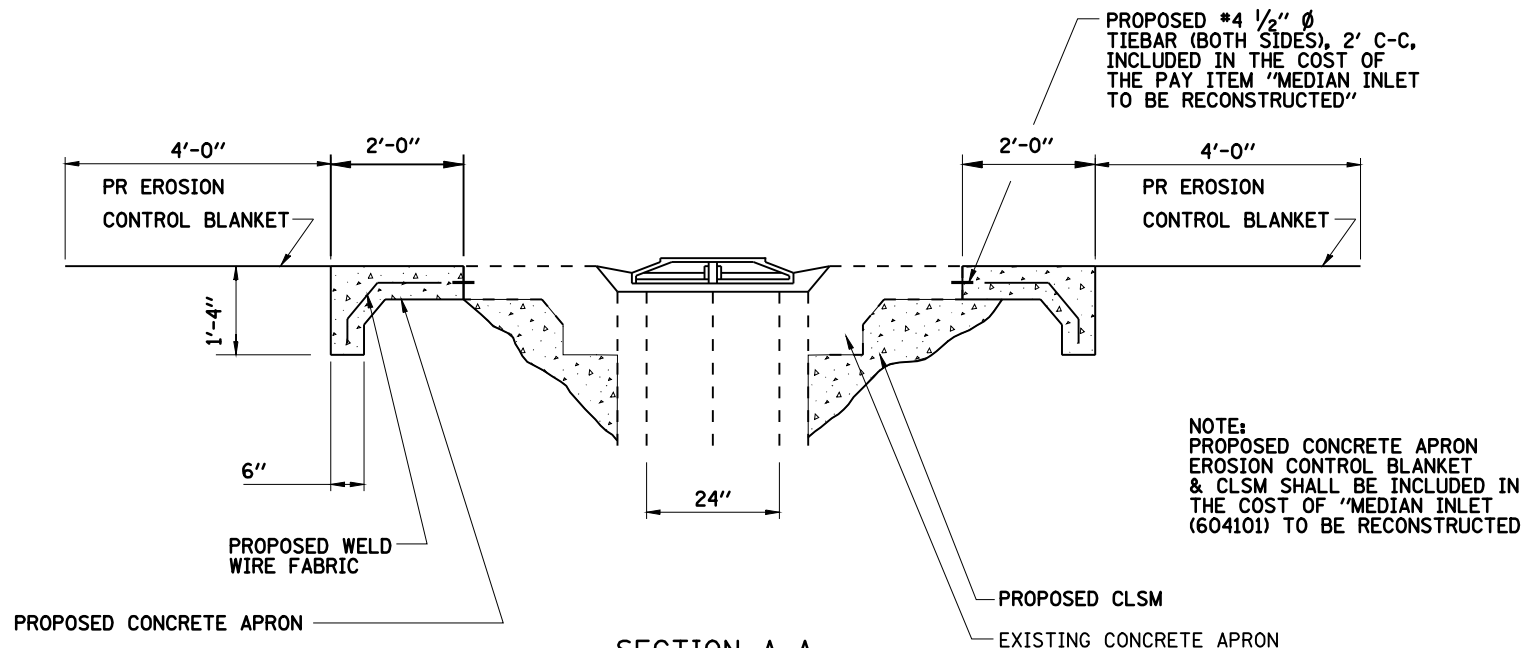
SCALE: VERT. NONE  
HORIZ. NONE  
DATE 06/09/2004

DRAWN BY LLQ  
CHECKED BY LEL

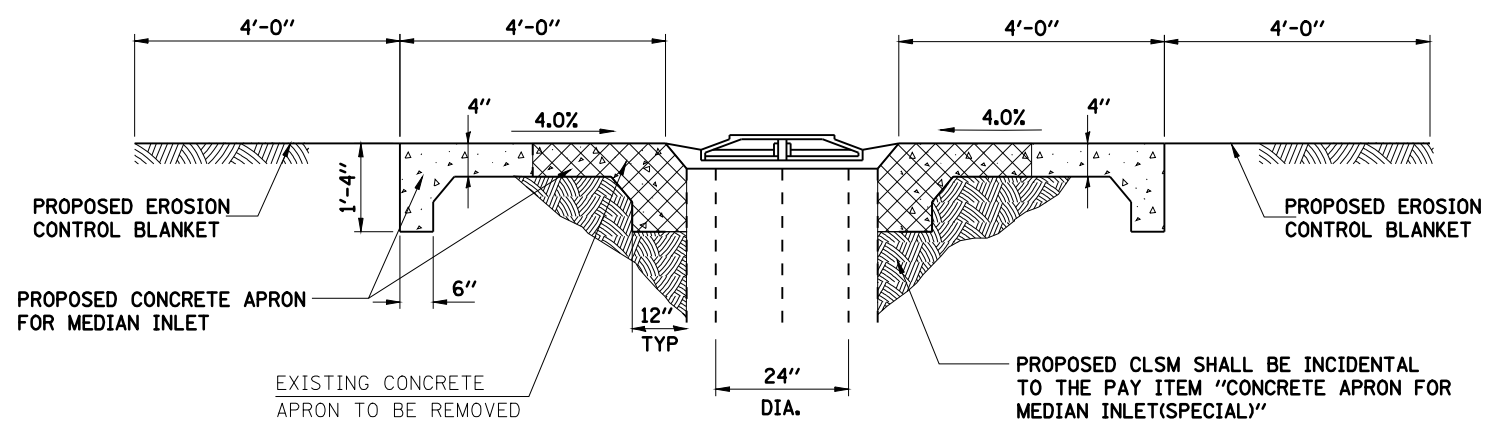
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	87
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
• (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				



PLAN-CONCRETE APRON FOR MEDIAN INLET & MEDIAN INLET REPAIR



SECTION A-A  
MEDIAN INLET REPAIR DETAIL



SECTION A-A  
CONCRETE APRON FOR MEDIAN INLET DETAIL

NOTE:  
REMOVAL OF THE EXISTING CONCRETE APRON,  
PROPOSED CLSM AND EROSION CONTROL BLANKET  
SHALL BE INCLUDED IN THE COST OF "CONCRETE  
APRON FOR MEDIAN INLET"

REVISIONS	
NAME	DATE

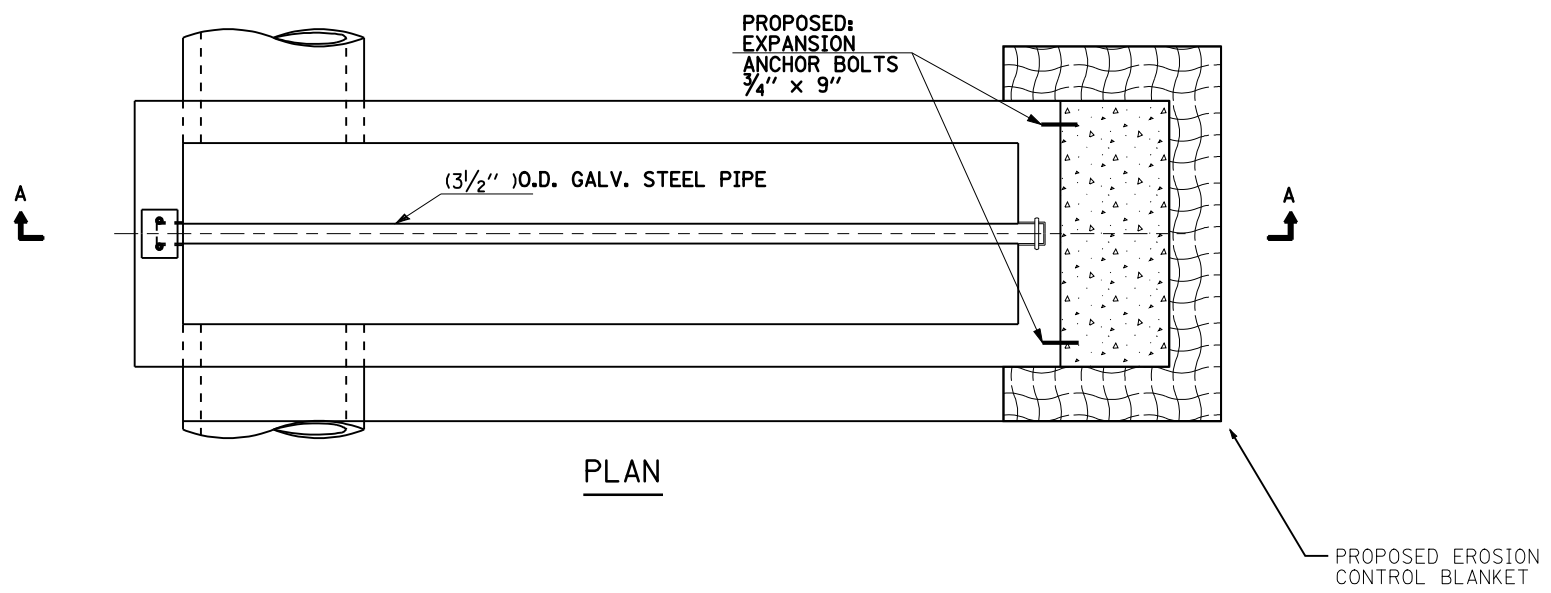
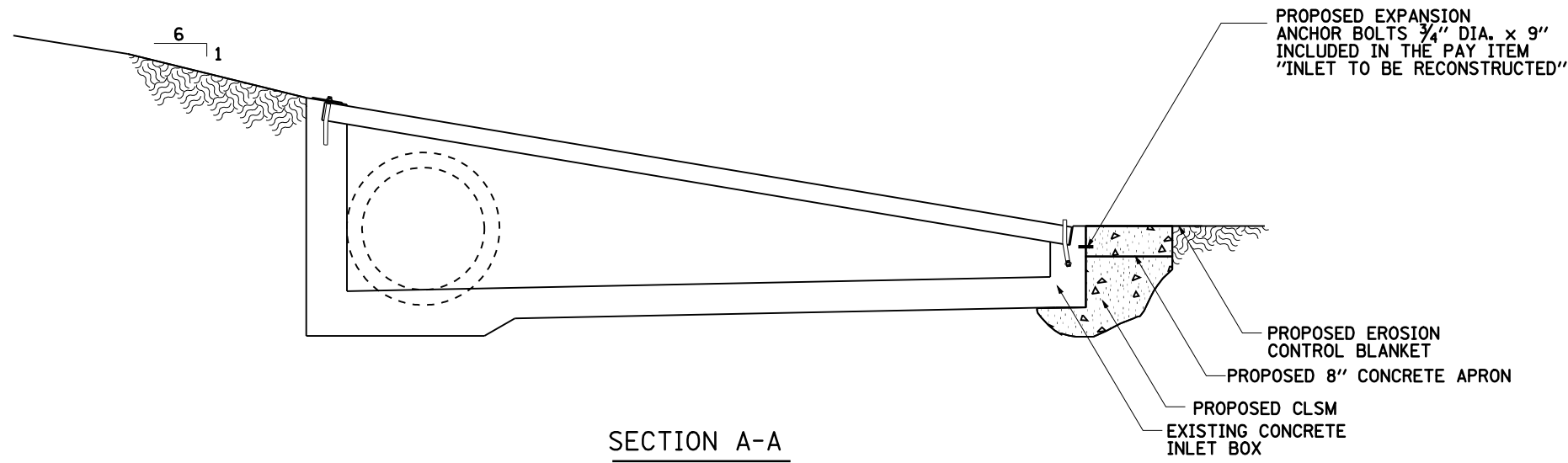
ILLINOIS DEPARTMENT OF TRANSPORTATION

**CONCRETE APRON &  
MEDIAN INLET REPAIR DETAIL**

SCALE: VERT. N.T.S.  
HORIZ. N.T.S.  
DATE 09/10/2004

DRAWN BY JJS  
CHECKED BY LLO

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	88
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		
• (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				



- NOTE:**
1. PROPOSED EROSION CONTROL BLANKET, CLSM, EXPANSION ANCHOR BOLTS  $\frac{3}{4}$ " DIA x 9" AND CONCRETE APRON ARE INCLUDED IN THE PAY ITEM "INLET TO BE RECONSTRUCTED"
  2. USE CLSM AS FILL FOR ERODED SOIL. CONCRETE MAYBE SUBSTITUTED FOR CLSM AT NO ADDITIONAL COST.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**INLET BOX TYPE  
(24) E REPAIR DETAIL**

SCALE: VERT. N.T.S.  
HORIZ. N.T.S.  
DATE 09/10/2004

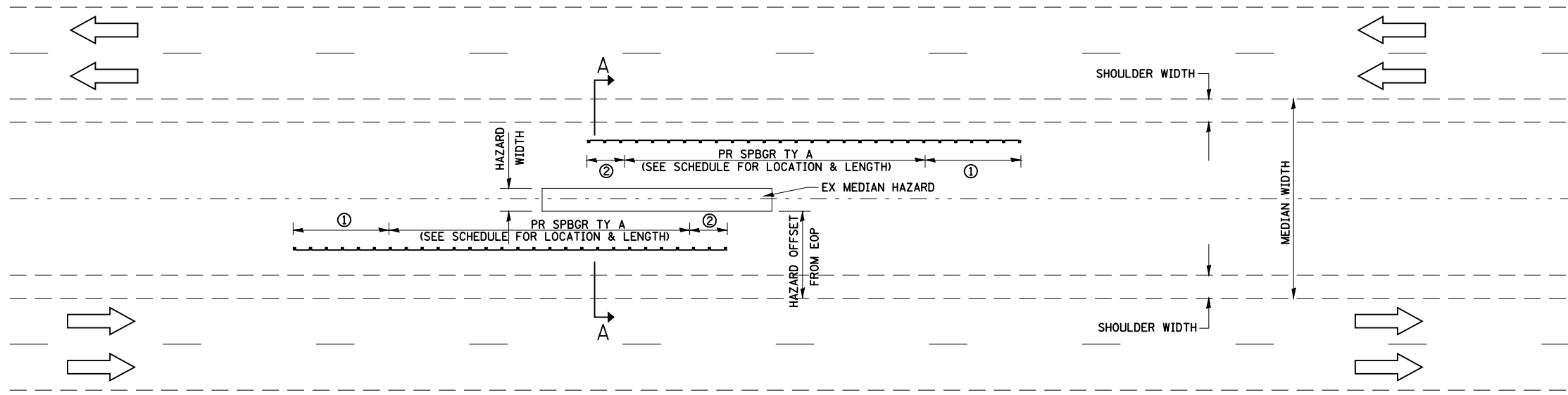
DRAWN BY JJS  
CHECKED BY LLQ



F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	**	ADAMS	108	89

STA. TO STA.  
 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

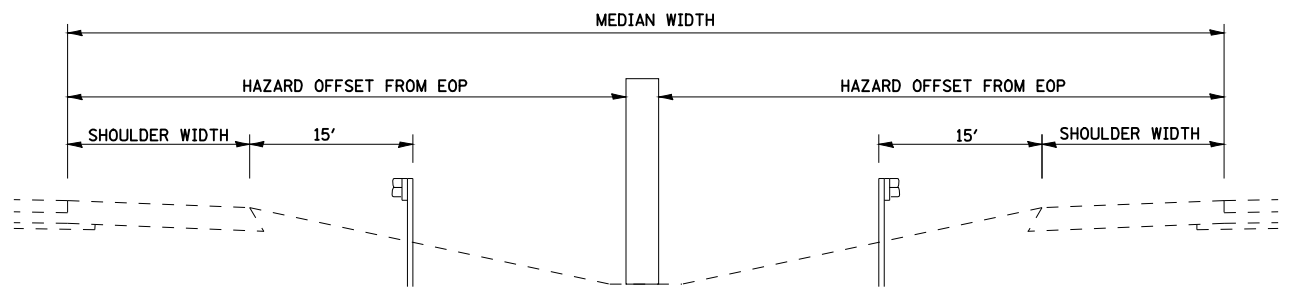
\*\* (1-5)RS-1; (1-6)RS  
 CONTRACT NO. 72694



**DETAIL "B"**

(PROPOSED PROTECTION OF MEDIAN HAZARD WITH PROPOSED GUARDRAIL)

- ① PR TRAFFIC BARRIER TERMINAL, TYPE 1, SPECIAL (TANGENT)
- ② PR TRAFFIC BARRIER TERMINAL, TYPE 2



**SECTION A-A**  
 (TYPICAL CROSS SECTION)

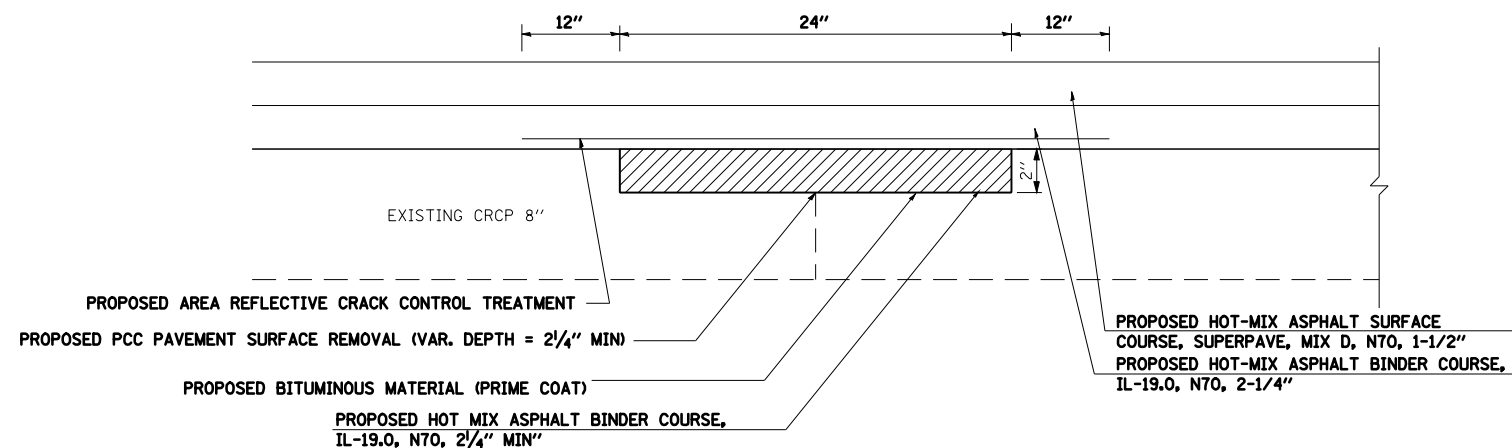
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION  
 GUARDRAIL LAYOUT FOR  
 MEDIAN PIERS  
 DETAIL "B"

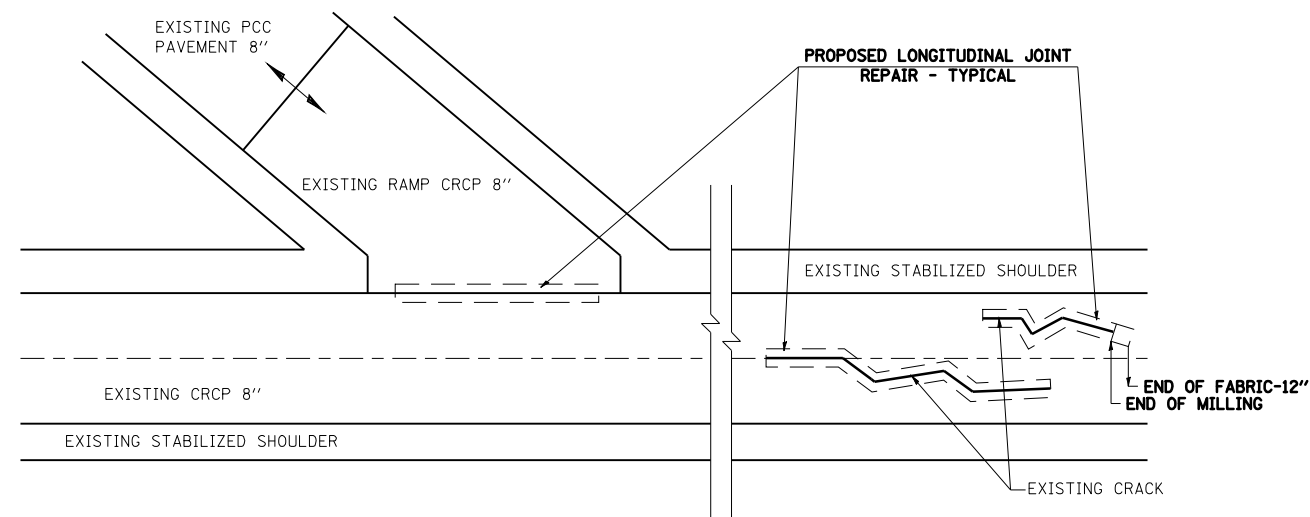
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	90

STA.	TO STA.
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT

• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



**PROPOSED LONGITUDINAL JOINT REPAIR DETAIL  
(TYPICAL)**



**LONGITUDIANL JOINT REPAIR  
(PLAN VIEW)**

NOTE : THE ENGINEER IN THE FIELD SHALL DETERMINE AND LOCATE ALL AREAS FOR REPAIR

REVISIONS	
NAME	DATE

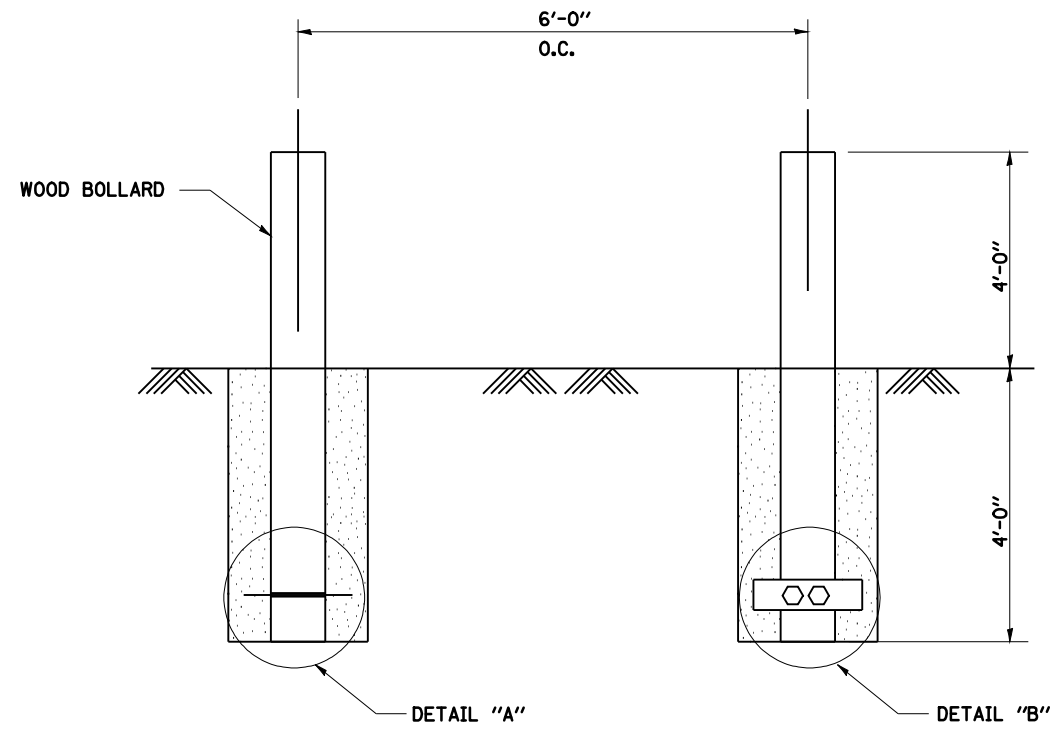
ILLINOIS DEPARTMENT OF TRANSPORTATION

**LONGITUDINAL JOINT REPAIR & SILICONE JOINT SEALER DETAILS**

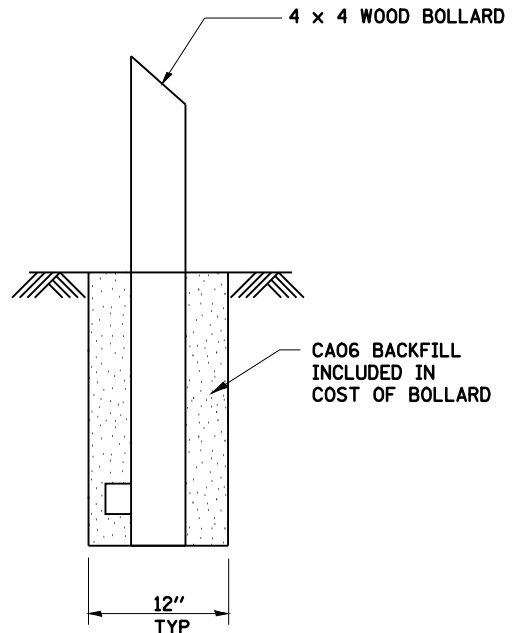
SCALE: VERT.  
 HORIZ.  
 DATE 09/10/2004

DRAWN BY JJS  
 CHECKED BY LLO

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	•	ADAMS	108	91
STA.		TO STA.		
FED. ROAD DIST. NO. 6		ILLINOIS FED. AID PROJECT		
• (1-5)RS-1; (1-6)RS				
<b>CONTRACT NO. 72694</b>				

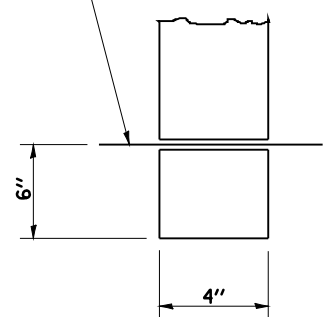


TOPS OF BOLLARDS SHALL BE SLOPED SO WATER DOES NOT PUDDLE

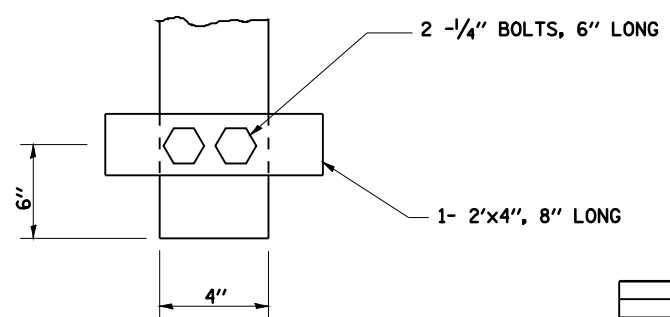


**NOTE:**  
WOOD BOLLARD INSTALLATION SHALL INCLUDE EITHER DETAIL "A" OR "B"

1-5\* BAR 10" LONG, DRIVEN THRU 5/8" HOLE DRILLED THRU WOOD BOLLARD



DETAIL A



DETAIL B

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

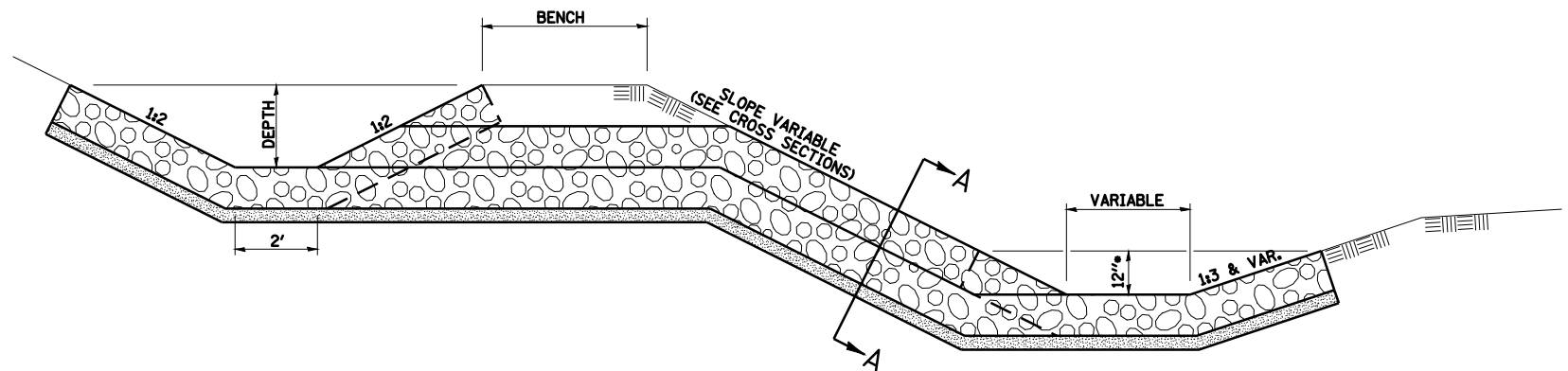
**WOOD BOLLARD DETAIL**

SCALE: DATE: 10-31-02

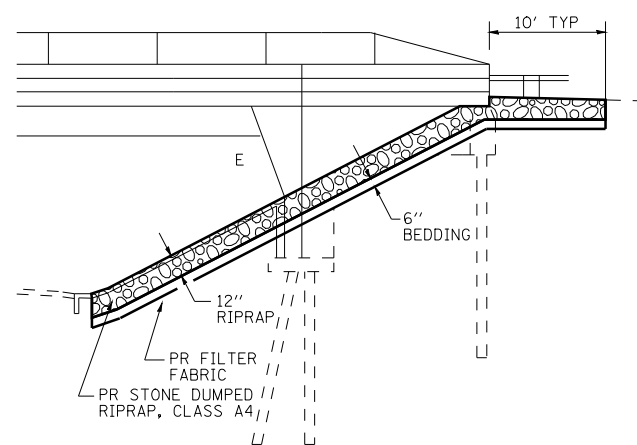
DRAWN BY: TMJ  
CHECKED BY: MEB

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	*	ADAMS	108	92
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

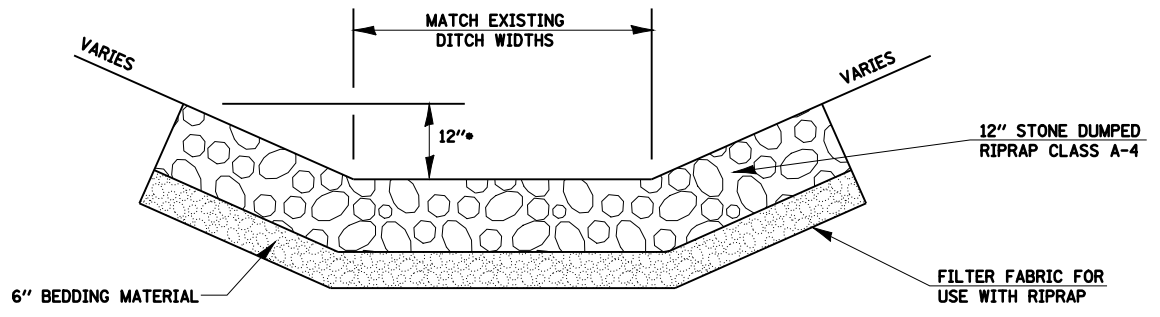
• (1-5)RS-1; (1-6)RS  
**CONTRACT NO. 72694**



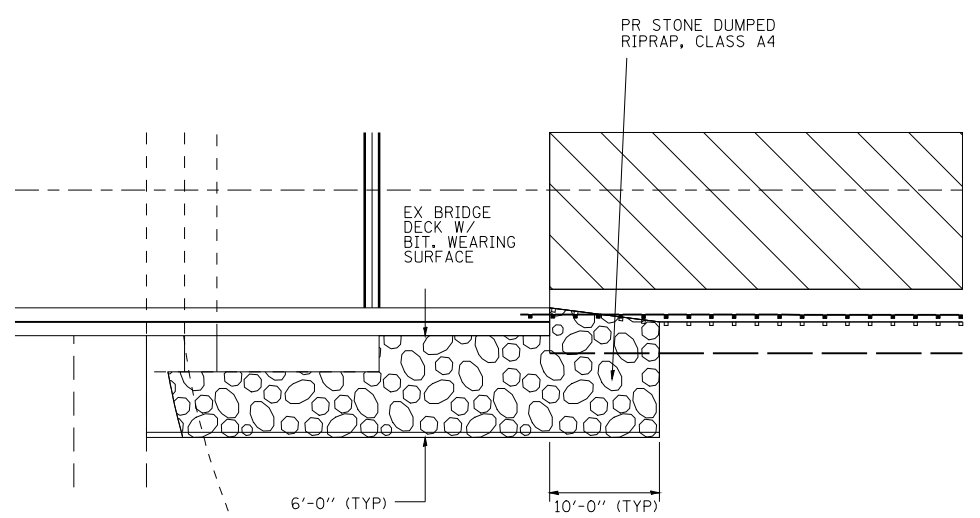
**DETAIL OF BENCH OUTLET AND RIPRAP OUTLET SLOPE**  
 \* OR AS DIRECTED BY ENGINEER



**ELEVATION RIPRAP DETAIL ALONG BRIDGE CURTAIN WALL AND SLOPE WALL**



**TYPICAL STONE RIPRAP - DITCH LINING**  
 \* OR AS DIRECTED BY ENGINEER



**PLAN (RIPRAP DETAILS)**

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

**RIPRAP PLACEMENT DETAILS**

DATE 1/31/06

DRAWN BY  
 CHECKED BY

SN 001-0045

EXISTING STRUCTURE: SN 001-0045 IS 30' - 0" O. TO O. OF DECK AND 263' -0" BK. TO BK. OF APPROACH BENTS, TWO-SPAN CONTINUOUS 54" PPC I BEAMS, 7 1/2" CONCRETE DECK SUPERSTRUCTURE SUPPORTED BY CONCRETE ABUTMENTS AND A CONCRETE PIER. THIS STRUCTURE CARRIES TR 423 (ST. ANTHONY ROAD) OVER FAI 172 AND WAS BUILT UNDER SECTION 1-5HB-1 IN 1975.

THIS IMPROVEMENT INCLUDES MILLING AND RESURFACING THE APPROACH ROADWAYS; CONSTRUCTING HOT-MIX ASPHALT AND AGGREGATE SHOULDERS ON THE APPROACH ROADWAYS; ADDING RIPRAP ALONG THE CURTAIN WALLS AND OUTSIDE THE EXISTING SLOPE WALLS; REPLACING EXISTING GUARDRAIL; AND REPLACING PREFORMED JOINT SEALS WITH SILICONE JOINT SEAL POURED AGAINST EXISTING EMBEDDED STEEL PLATES AFTER CLEANING.

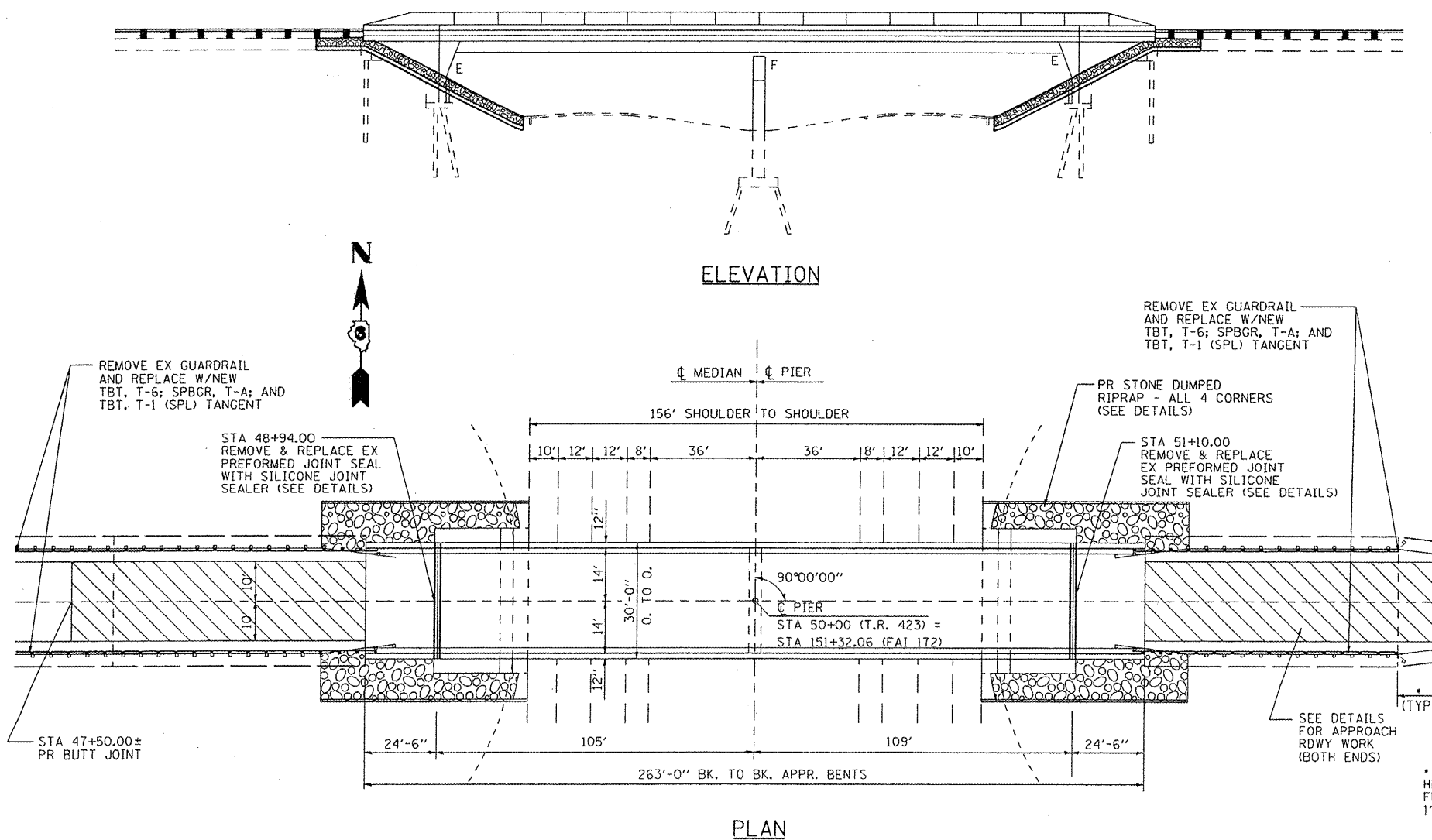
GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

RIPRAP QUANTITIES AND LOCATION OF RIPRAP ALONG CURTAIN WALL AND EXISTING SLOPEWALL ARE APPROXIMATE BASED ON CONDITIONS AT THE TIME OF FIELD INVESTIGATION. THE ENGINEER SHALL DETERMINE FINAL PLACEMENT AND QUANTITY OF THIS WORK IN THE FIELD.

REMOVAL OF THE EXISTING PREFORMED JOINT SEAL WILL BE CONSIDERED AS PART OF THE WORK OF THE INSTALLATION OF THE NEW SILICONE JOINT SEALER ALONG WITH THE CLEANING OF THE EXISTING JOINT.

ALL WORK ASSOCIATED WITH THE INSTALLATION OF THE NEW SILICONE JOINT SEALER WILL BE COMPLETED UTILIZING STAGE CONSTRUCTION CLOSING HALF OF THE BRIDGE TO TRAFFIC FOR CONSTRUCTION AND MAINTAINING ONE-LANE, TWO-WAY, TRAFFIC ON THE OTHER HALF WITH USE OF FLAGGERS AND OTHER REQUIRED TC&P DURING DAYTIME OPERATIONS ONLY AT A TIME.



TOTAL BILL OF MATERIALS		
ITEM	UNIT	TOTAL
SILICONE JOINT SEALER	FOOT	60
<b>ROADWAY ITEMS</b>		
EXCAVATING AND GRADING EXISTING SHOULDERS	UNIT	9
HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	405
HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	525
BITUMINOUS MATERIALS (PRIME COAT)	TON	0.2
AGGREGATE (PRIME COAT)	TON	1.1
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	44
AGGREGATE SHOULDER, TYPE B	TON	37
FILTER FABRIC	SQ YD	150
STONE DUMPED RIPRAP, CLASS A4	TON	88
GUARDRAIL REMOVAL	FOOT	850
TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4
STEEL PLATE BEAM GUARDRAIL, TYPE A	FOOT	450
TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4
GUARDRAIL MARKERS, TYPE A	EACH	10
TERMINAL MARKER - DIRECT APPLIED	EACH	4

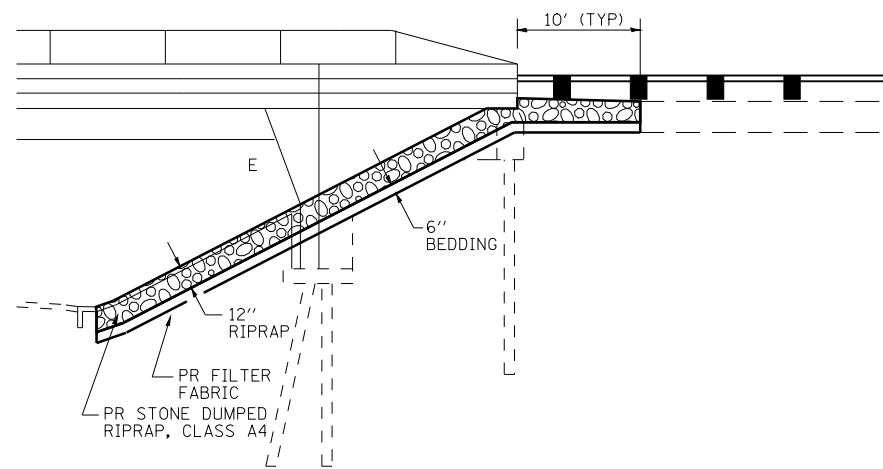


GENERAL PLAN & ELEVATION  
 TR 423 (ST. ANTHONY ROAD) OVER FAI 172  
 FAI ROUTE 172  
 SECTION: (1-5)RS-1; (1-6)RS  
 ADAMS COUNTY  
 STA 151+32.06 (FAI 172)  
 STA 50+00.00 (TR 423)  
 SN 001-0045

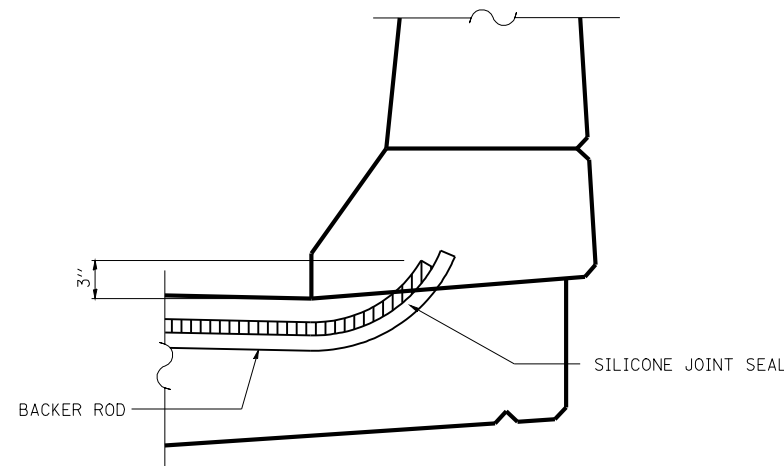
Expires 11/30/2009

SHEET 1 OF 3

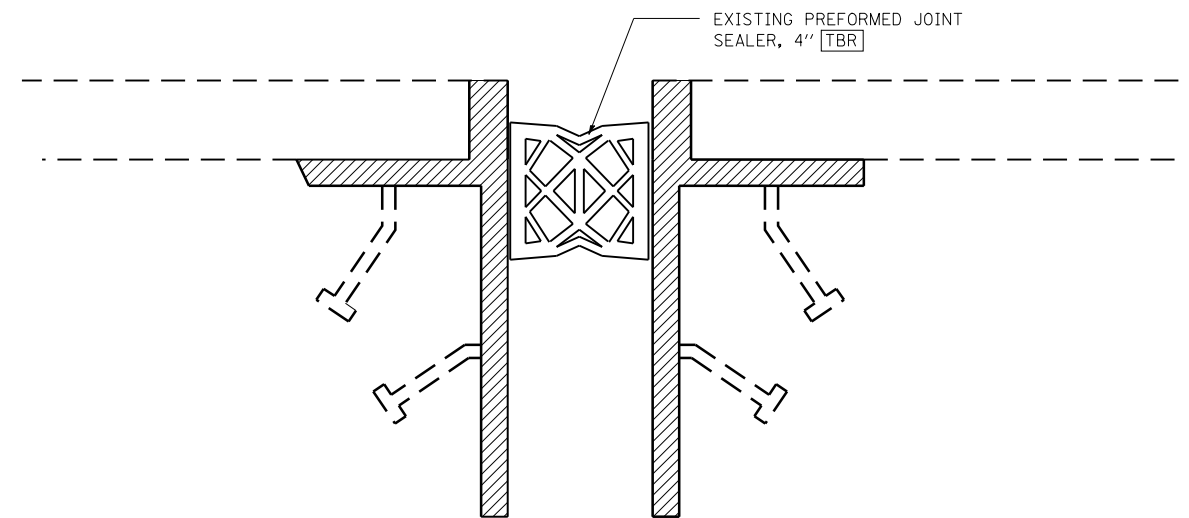
FILE NAME =	USER NAME = laughlinr1	DESIGNED - JCN	REVISED - 02/03/09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BRIDGE REPAIR DETAILS SN-001-0045</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
93-095 SN001-0045.IDOT.REV.dgn	DRAWN -	REVISED -	172			(1-5)RS-1; (1-6)RS	ADAMS	108	93	
PLOT SCALE = 1/8"=1'-0" / IN.	CHECKED -	REVISED -	CONTRACT NO. 72694							
PLOT DATE = Feb-11-2009 11:40:36AM	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
SCALE:		SHEET NO. OF SHEETS		STA.	TO STA.					



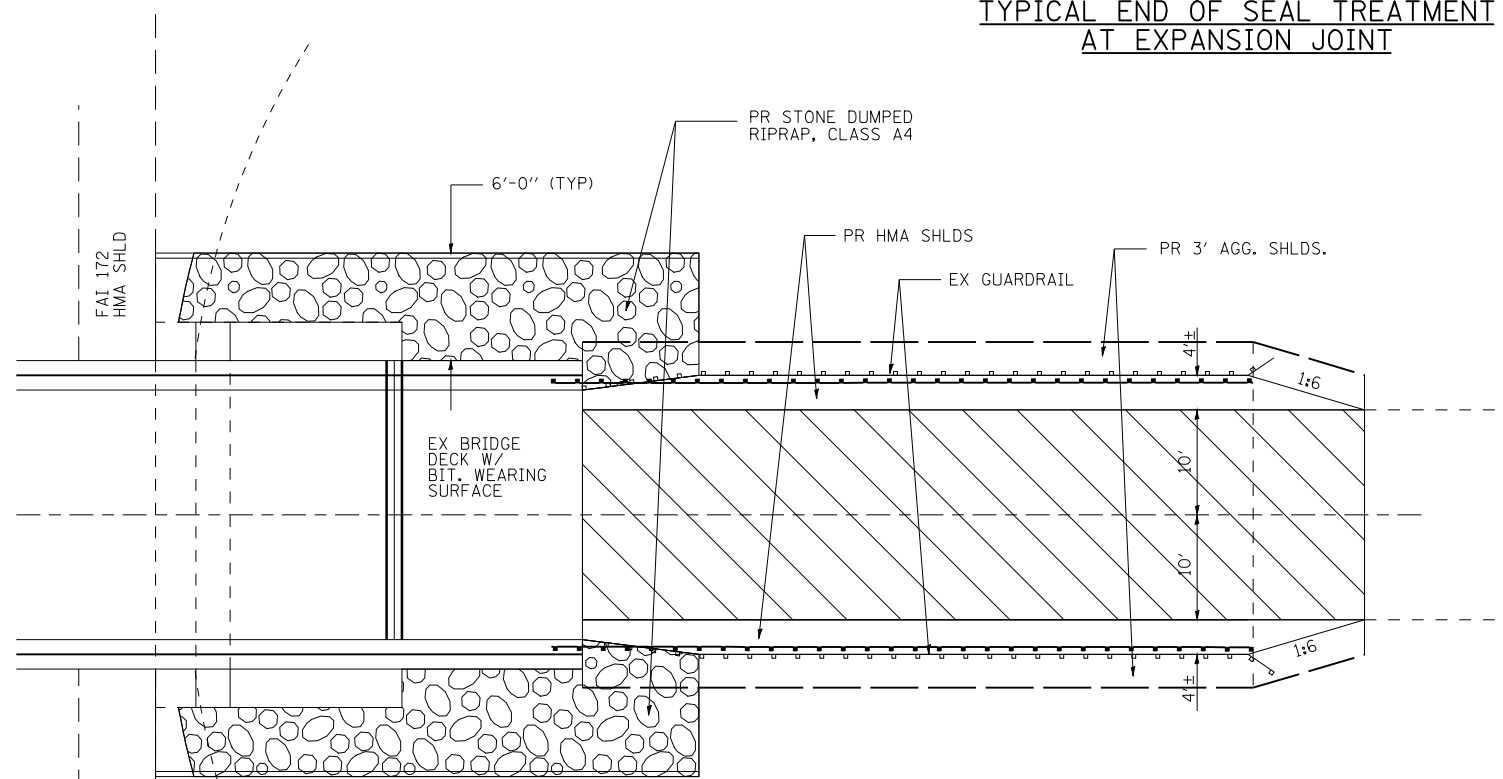
**ELEVATION  
(RIPRAP DETAILS)**



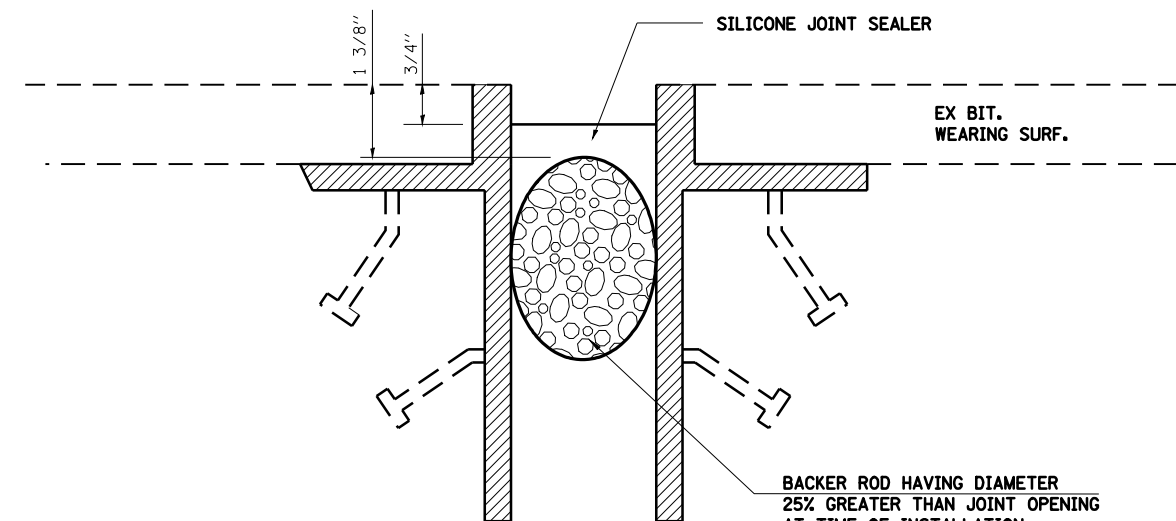
**TYPICAL END OF SEAL TREATMENT  
AT EXPANSION JOINT**



**EXISTING**



**PLAN  
(RIPRAP DETAILS)**



**PROPOSED**

**SN 001-0045**

STA 48+94.00  
STA 51+10.00

RIPRAP & JOINT REPLACEMENT DETAILS  
TR 423 (ST. ANTHONY ROAD) OVER FAI 172  
FAI ROUTE 172  
SECTION: (1-5)RS-1; (1-6)RS  
ADAMS COUNTY  
STA 151+32.06 (FAI 172)  
STA 50+00.00 (TR 423)  
SN 001-0045

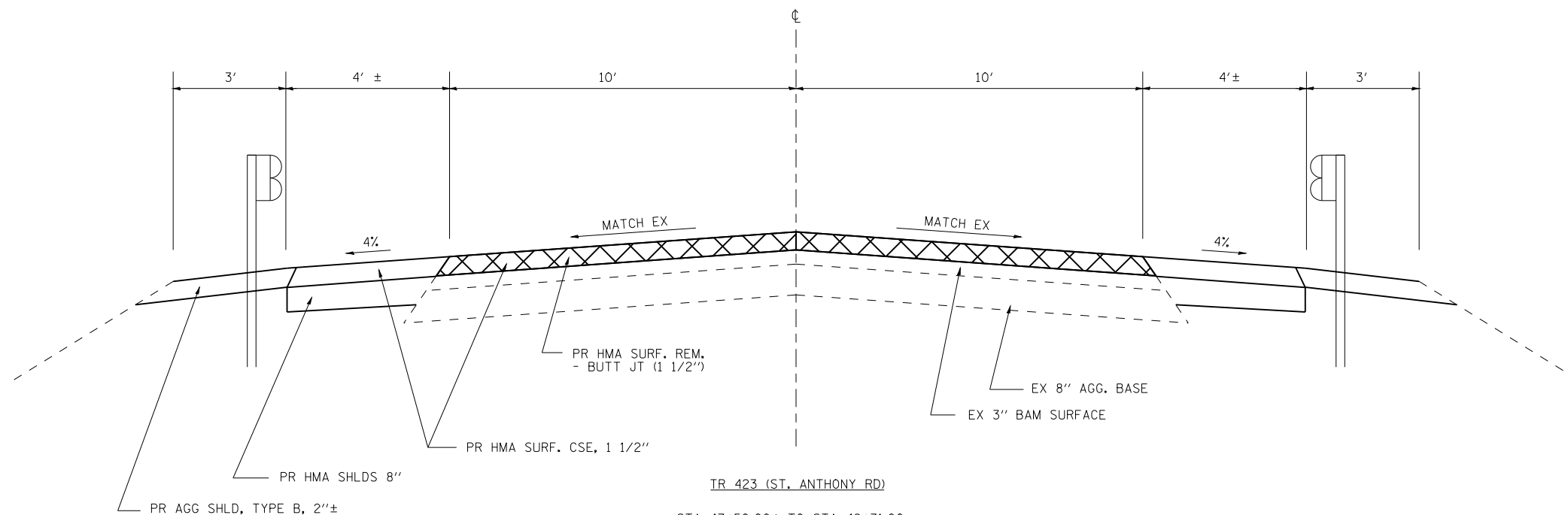
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	PLOT DATE = Feb-11-2009 12:49:20PM	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BRIDGE REPAIR DETAILS  
SN-001-0045**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	94
CONTRACT NO. 72694				
ILLINOIS FED. AID PROJECT				



TR 423 (ST. ANTHONY RD)

STA 47+50.00± TO STA 48+71.00

STA 48+71.00 TO STA 51+33.00 - BRIDGE OMMISION

STA 51+33.00 TO STA 52+50.00±\*

NOTE: \*TAPER ENDS OF SHLDS FROM 4' TO 1' STUB @ 1:6.  
START TAPER AT END OF GUARDRAIL

TYPICAL CROSS SECTION  
 BRIDGE APPROACH DETAILS  
 TR 423 (ST. ANTHONY ROAD) OVER FAI 172  
 FAI ROUTE 172  
 SECTION: (1-5)RS-1; (1-6)RS  
 ADAMS COUNTY  
 STA 151+32.06 (FAI 172)  
 STA 50+00.00 (TR 423)  
 SN 001-0045

SHEET 3 OF 3

FILE NAME =	USER NAME = laughlinr1	DESIGNED - JCN	REVISED - 02/03/09
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PLOT DATE = Feb-11-2009 12:49:22PM		DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS  
 SN-001-0045

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	95
ILLINOIS FED. AID PROJECT			CONTRACT NO. 72694	

SN 001-0048

EXISTING STRUCTURE: SN 001-0048 IS 64' - 0" O. TO O. OF DECK AND 347'-5" BK. TO BK. OF APPROACH BENTS, TWO-SPAN CONTINUOUS 60" STEEL GIRDERS WITH 36" PPC I BEAMS IN THE VAULTED ABUTMENT, 7 1/2" CONCRETE DECK SUPERSTRUCTURE SUPPORTED BY CONCRETE ABUTMENTS AND A CONCRETE PIER. THIS STRUCTURE CARRIES FAS 1604/TR 349 (COLUMBUS ROAD) OVER FAI 172 AND WAS BUILT UNDER SECTION 1-6HB IN 1975.

THIS IMPROVEMENT INCLUDES REPLACING PREFORMED JOINT SEALS WITH SILICONE JOINT SEAL POURED AGAINST EXISTING EMBEDDED STEEL PLATES AFTER CLEANING; MILLING AND RESURFACING THE APPROACH ROADWAYS; ADDING RIPRAP ALONG THE CURTAIN WALLS AND OUTSIDE THE EXISTING SLOPE WALLS; AND REPLACING THE EXISTING GUARDRAIL.

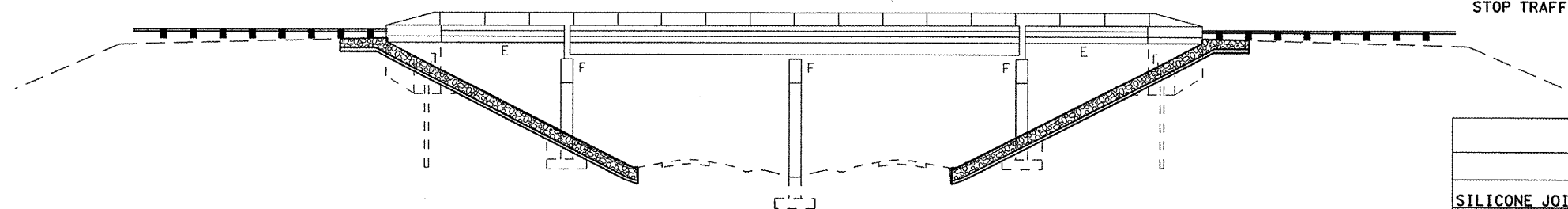
**GENERAL NOTES**

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

RIPRAP QUANTITIES AND LOCATION OF RIPRAP ALONG CURTAIN WALL AND EXISTING SLOPEWALL ARE APPROXIMATE BASED ON CONDITIONS AT THE TIME OF FIELD INVESTIGATION. THE ENGINEER SHALL DETERMINE FINAL PLACEMENT AND QUANTITY OF THIS WORK IN THE FIELD.

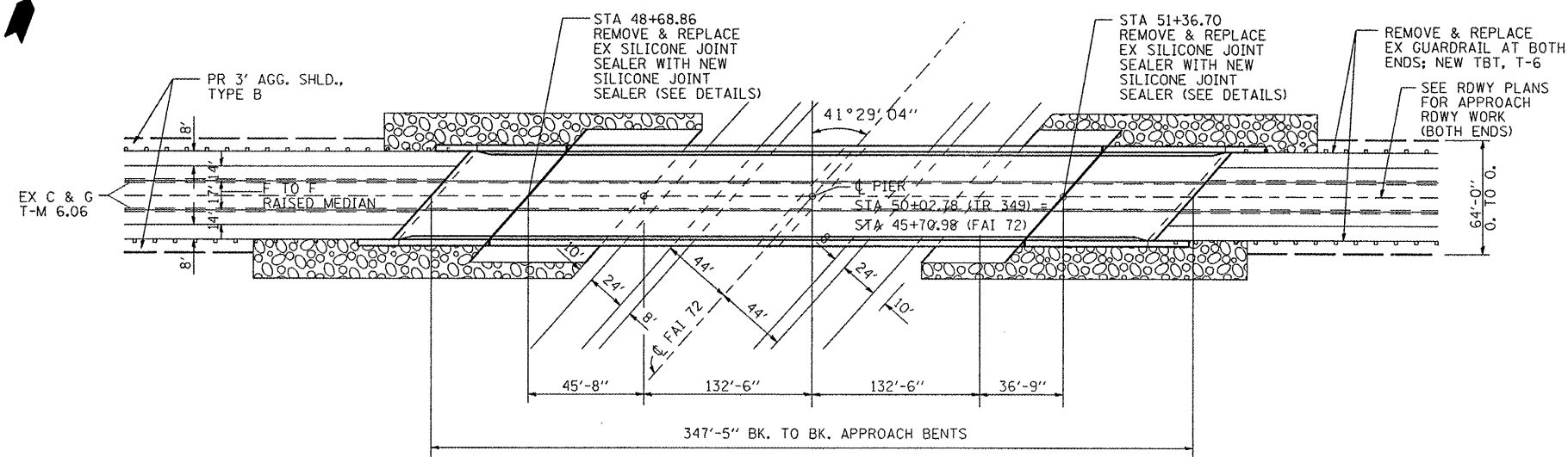
REMOVAL OF THE EXISTING PREFORMED JOINT SEAL WILL BE CONSIDERED AS PART OF THE WORK OF THE INSTALLATION OF THE NEW SILICONE JOINT SEALER ALONG WITH THE CLEANING OF THE EXISTING JOINT.

ALL WORK ASSOCIATED WITH THE INSTALLATION OF THE NEW SILICONE JOINT SEALER WILL BE COMPLETED UTILIZING STAGE CONSTRUCTION CLOSING PART OF THE BRIDGE DECK TO TRAFFIC AT A TIME BY SHIFTING TRAFFIC TO THE OUTSIDE SHOULDER OR TO THE MEDIAN SIDE OF THE EXISTING LANES WITH USE OF CONES AND OTHER TC&P DEVICES DURING DAYTIME OPERATIONS ONLY AS DIRECTED BY THE ENGINEER. FLAGGERS SHALL BE USED TO SLOW TRAFFIC DOWN OR TO STOP TRAFFIC AS NECESSARY.

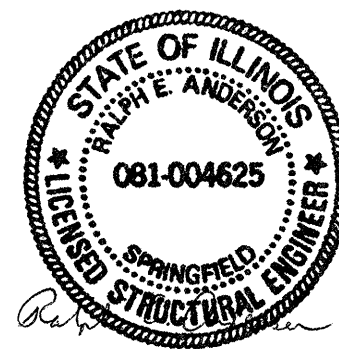


**ELEVATION**

TOTAL BILL OF MATERIALS		
ITEM	UNIT	TOTAL
SILICONE JOINT SEALER	FOOT	173
<b>ROADWAY ITEMS</b>		
FILTER FABRIC	SQ YD	261
STONE DUMPED RIPRAP, CLASS A4	TON	152



**PLAN**



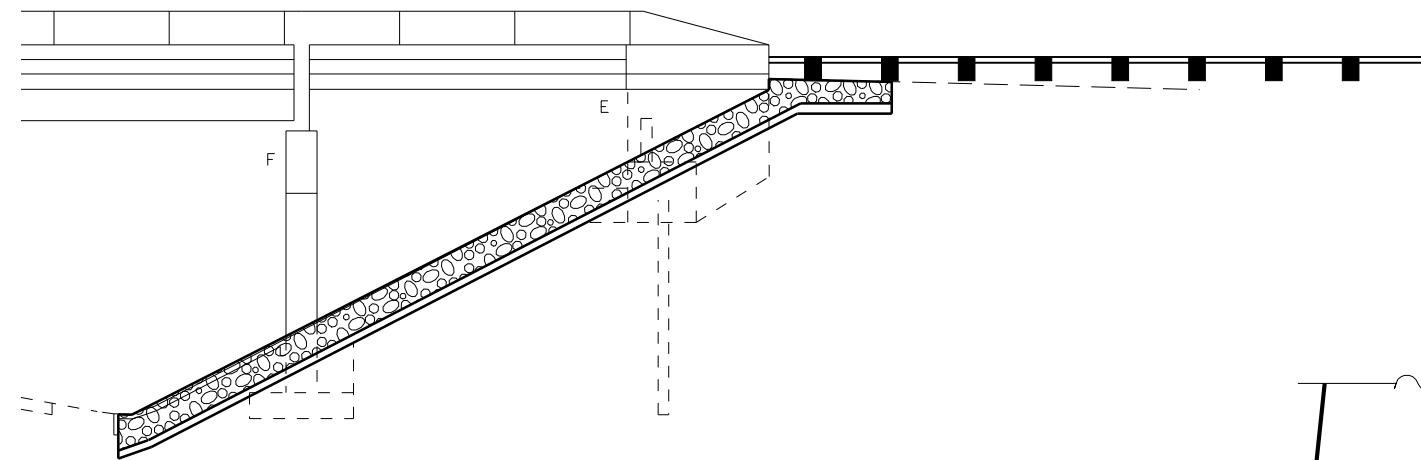
*Expires 11/30/2009*

GENERAL PLAN & ELEVATION  
 FAS 1604/TR 349 (COLUMBUS ROAD)  
 OVER FAI 172  
 FAI ROUTE 172  
 SECTION: (1-5)RS-1; (1-6)RS  
 ADAMS COUNTY  
 STA 45+70.98 (FAI 172)  
 STA 50+02.78 (TR 349)  
 SN 001-0048

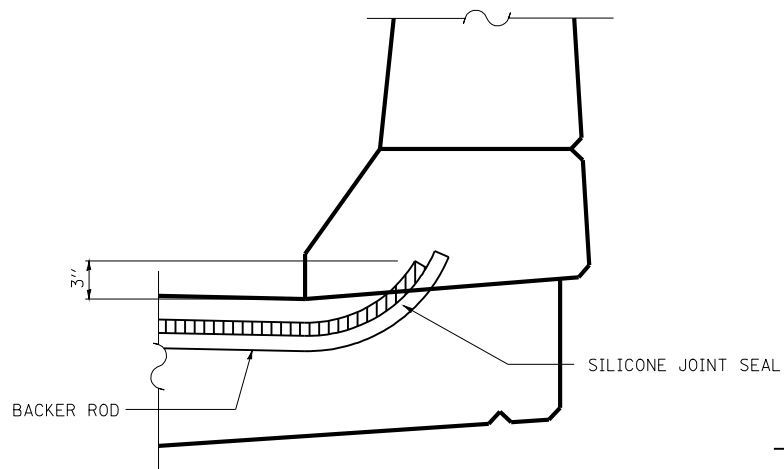
SHEET 1 OF 2

FILE NAME =	USER NAME = laughlinr1	DESIGNED - JCN	REVISED - 02/03/09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BRIDGE REPAIR DETAILS SN-001-0048</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.			
c:\pwwork\pwwid001\LAUGHLINR1\dms26563\	96-097 SN001-0048_I.DOT_REV.dgn	DRAWN -	REVISED -		SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.	172	(1-5)RS-1; (1-6)RS	ADAMS	108	96
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -												
	PLOT DATE = Feb-11-2009 11:40:43AM	DATE -	REVISED -												
											CONTRACT NO. 72694			[ILLINOIS] FED. AID PROJECT	

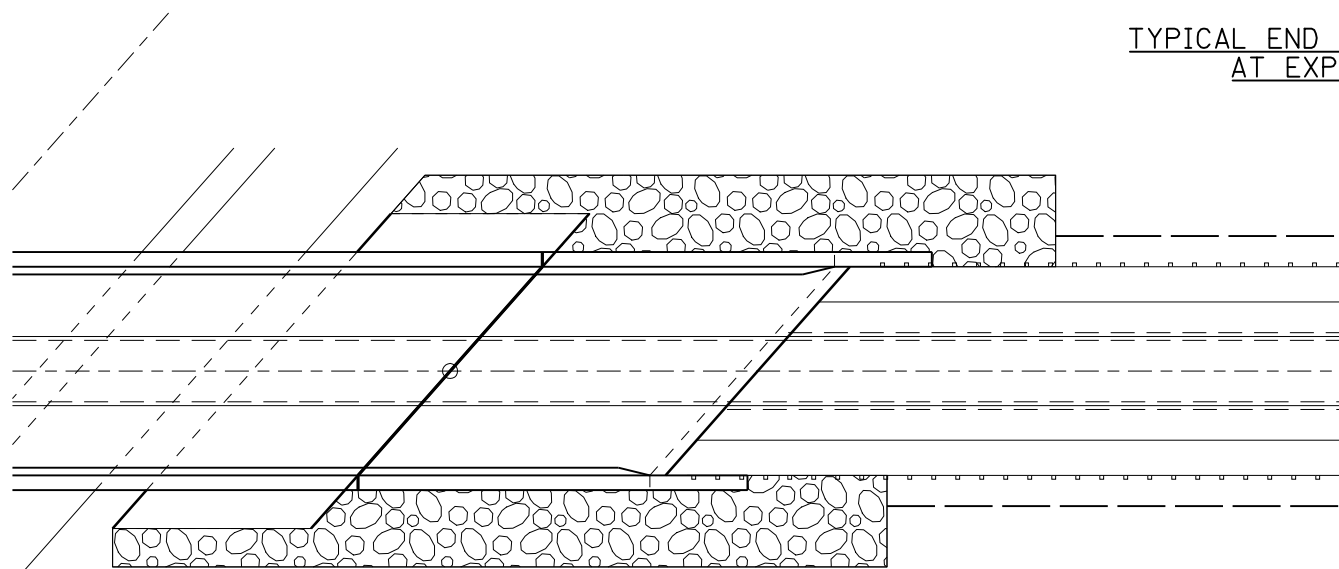




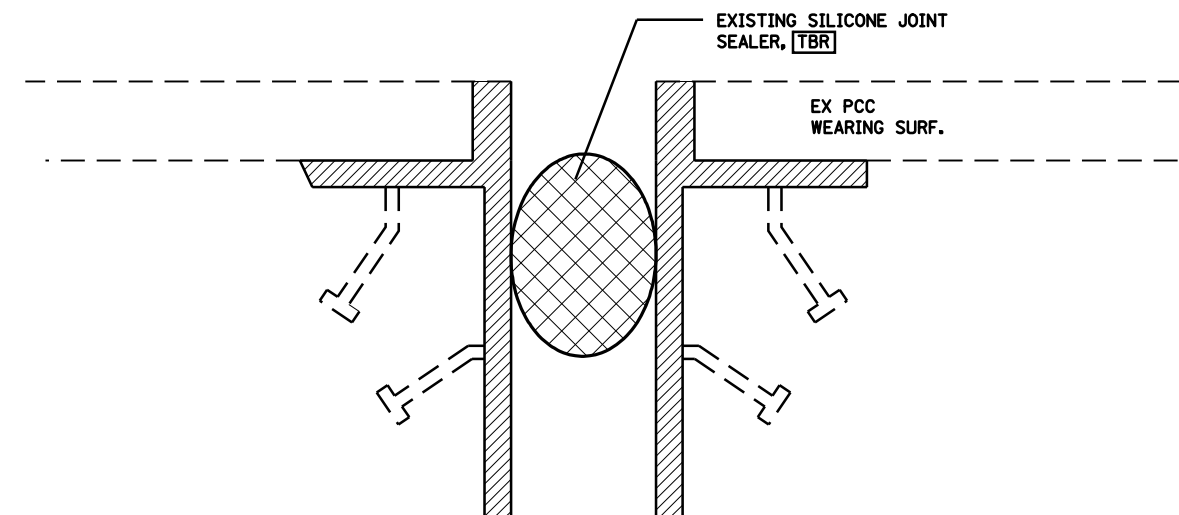
ELEVATION  
(RIPRAP DETAILS)



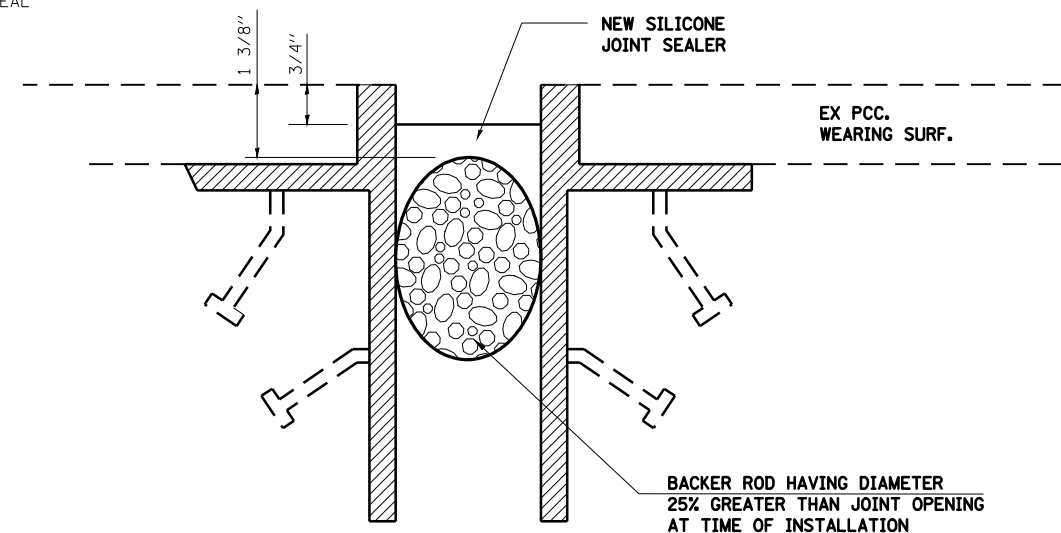
TYPICAL END OF SEAL TREATMENT  
AT EXPANSION JOINT



PLAN  
(RIPRAP DETAILS)



EXISTING



PROPOSED SILICONE JOINT  
SEALER DETAIL

COLUMBUS ROAD  
STA. 48+68.86  
STA. 51+36.70

RIPRAP & JOINT REPLACEMENT DETAILS  
FAS 1604/TR 349 (COLUMBUS ROAD)  
OVER FAI 172  
FAI ROUTE 172  
SECTION: (1-5)RS-1; (1-6)RS  
ADAMS COUNTY  
STA 45+70.98 (FAI 172)  
STA 50+02.78 (TR 349)  
SN 001-0048

SHEET 2 OF 2

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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS  
SN-001-0048

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	97
CONTRACT NO. 72694				
ILLINOIS FED. AID PROJECT				

SN 001-0049

EXISTING STRUCTURE: SN 001-0049 IS 32' - 0" O. TO O. OF DECK AND 267'-3" BK. TO BK. OF APPROACH BENTS, TWO-SPAN CONTINUOUS 54" PPC I BEAMS, 7 1/2" CONCRETE DECK SUPERSTRUCTURE SUPPORTED BY CONCRETE ABUTMENTS AND A CONCRETE PIER. THIS STRUCTURE CARRIES CH 3 (ELLINGTON ROAD) OVER FAI 172 AND WAS BUILT UNDER SECTION 1-6HB-1 IN 1975.

THIS IMPROVEMENT INCLUDES REMOVAL AND REPLACEMENT OF THE EXISTING BITUMINOUS WEARING SURFACE; DECK PATCHING; CONSTRUCTING HOT-MIX ASPHALT AND AGGREGATE SHOULDERS ON THE APPROACH ROADWAYS; ADDING RIPRAP ALONG THE CURTAIN WALLS AND OUTSIDE THE EXISTING SLOPE WALLS; REPLACING GUARDRAIL; AND REPLACING PREFORMED JOINT SEALS WITH SILICONE JOINT SEAL POURED AGAINST EXISTING EMBEDDED STEEL STEEL PLATES AFTER CLEANING.

**GENERAL NOTES**

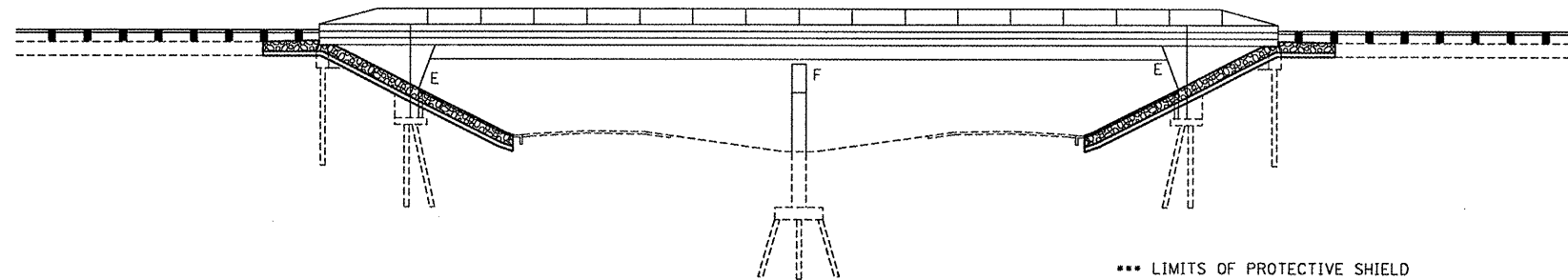
PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

RIPRAP QUANTITIES AND LOCATION OF RIPRAP ALONG CURTAIN WALL AND EXISTING SLOPEWALL ARE APPROXIMATE BASED ON CONDITIONS AT THE TIME OF FIELD INVESTIGATION. THE ENGINEER SHALL DETERMINE FINAL PLACEMENT AND QUANTITY OF THIS WORK IN THE FIELD.

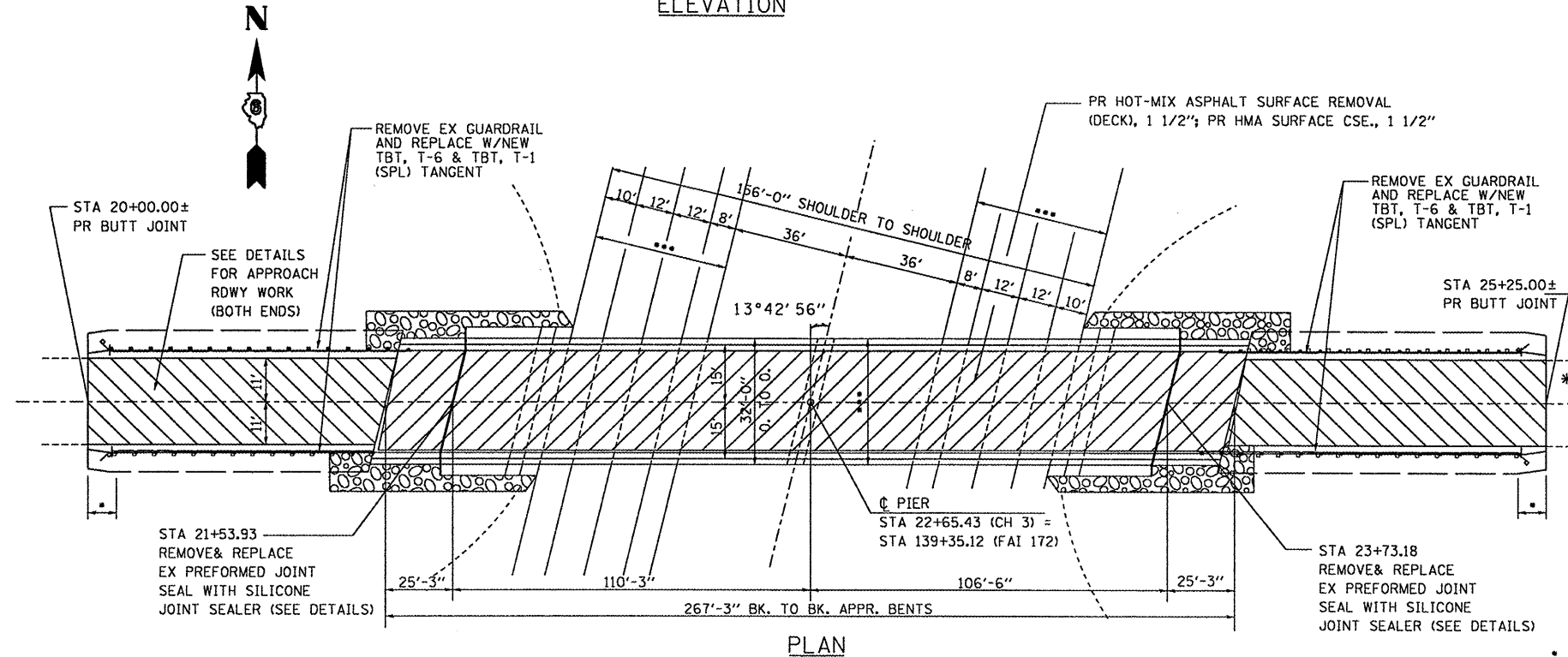
REMOVAL OF THE EXISTING PREFORMED JOINT SEAL WILL BE CONSIDERED AS PART OF THE WORK OF THE INSTALLATION OF THE NEW SILICONE JOINT SEALER ALONG WITH THE CLEANING OF THE EXISTING JOINT.

ALL WORK ASSOCIATED WITH THE INSTALLATION OF THE NEW SILICONE JOINT SEALER WILL BE COMPLETED UTILIZING STAGE CONSTRUCTION CLOSING HALF OF THE BRIDGE TO TRAFFIC FOR CONSTRUCTION AND MAINTAINING ONE-LANE, TWO-WAY, TRAFFIC ON THE OTHER HALF WITH USE OF FLAGGERS AND OTHER REQUIRED TC&P DURING DAYTIME OPERATIONS ONLY AT A TIME.

ALL WORK ASSOCIATED WITH THE REMOVAL AND REPLACEMENT OF THE EXISTING HMA WEARING SURFACE, AND DECK PATCHING SHALL BE COMPLETED UTILIZING STAGE CONSTRUCTION CLOSING HALF OF THE BRIDGE TO TRAFFIC FOR CONSTRUCTION AND MAINTAINING ONE-LANE, TWO-WAY, TRAFFIC ON THE OTHER HALF WITH USE OF TC&P STANDARD 701321, TEMPORARY BRIDGE TRAFFIC SIGNALS, AND TEMPORARY CONCRETE BARRIER.



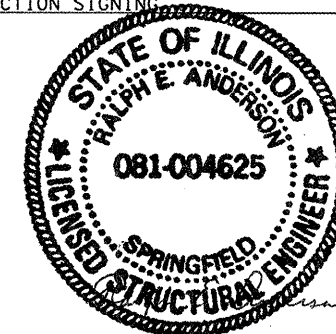
**ELEVATION**



**PLAN**

TOTAL BILL OF MATERIALS		
ITEM	UNIT	TOTAL
HOT-MIX ASPHALT SURFACE REMOVAL (DECK)	SQ YD	846
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	71
SILICONE JOINT SEALER	FOOT	66
<b>ROADWAY ITEMS</b>		
EXCAVATING AND GRADING EXISTING SHOULDERS	UNIT	5
HOT-MIX ASPHALT SHOULDERS, 8"	SQ YD	228
HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	626
BITUMINOUS MATERIALS (PRIME COAT)	TON	0.6
AGGREGATE (PRIME COAT)	TON	3.0
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	53
AGGREGATE SHUOLDERS, TYPE B	TON	20
FILTER FABRIC	SQ YD	150
STONE DUMPED RIPRAP, CLASS A4	TON	88
GUARDRAIL REMOVAL	FOOT	400
TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	4
TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4
GUARDRAIL MARKERS, TYPE A	EACH	4
TERMINAL MARKER - DIRECT APPLIED	EACH	4
DECK SLAB REPAIR (PARTIAL)	SQ YD	36.0
DECK SLAB REPAIR (FULL DEPTH), TYPE 2	SQ YD	9.4
WATERPROOFING MEMBRANE SYSTEM	SQ YD	846
TRAFFIC CONTROL AND PROTECTION STANDARD 701321	EACH	1
TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1
PROTECTIVE SHIELD	L SUM	1
WIDTH RESTRICTION SIGNING	L SUM	1

\*\* IN ACCORDANCE WITH SECTION 501 OF THE STANDARD SPECIFICATIONS



\* TAPER PROPOSED HMA SHLD FROM FULL WIDTH TO 1' STUB @ 1:6 TAPER

EXPIRES 11-30-2010

GENERAL PLAN & ELEVATION  
 CH 3 (ELLINGTON ROAD) OVER FAI 172  
 FAI ROUTE 172  
 SECTION: (1-5)RS-1; (1-6)RS  
 ADAMS COUNTY  
 STA 139+35.12 (FAI 172)  
 STA 22+65.43 (CH 3)  
 SN 001-0049 SHEET 1 OF 5

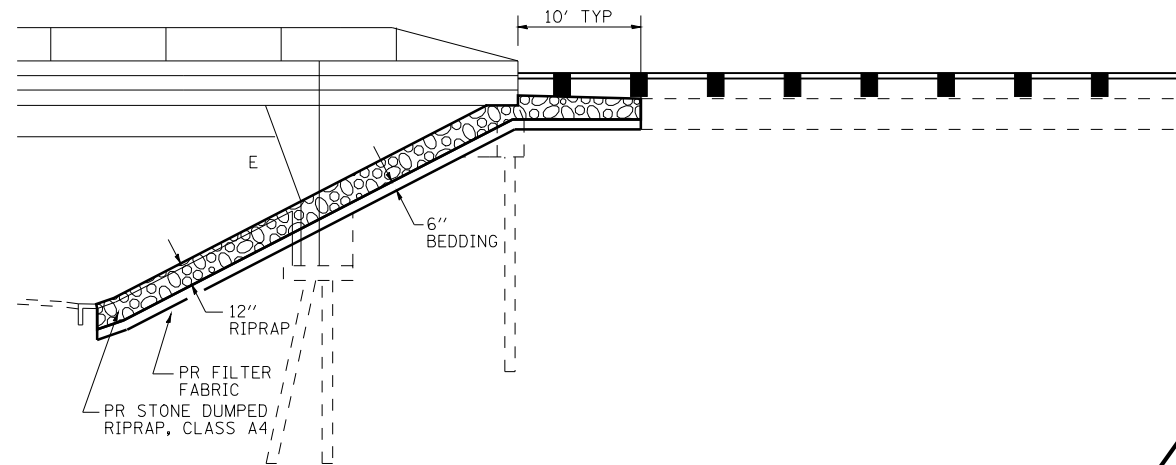
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

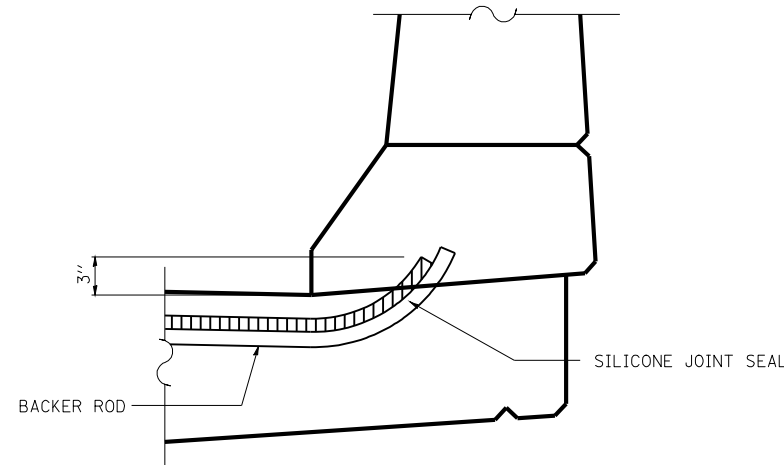
**BRIDGE REPAIR DETAILS  
 SN-001-0049**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

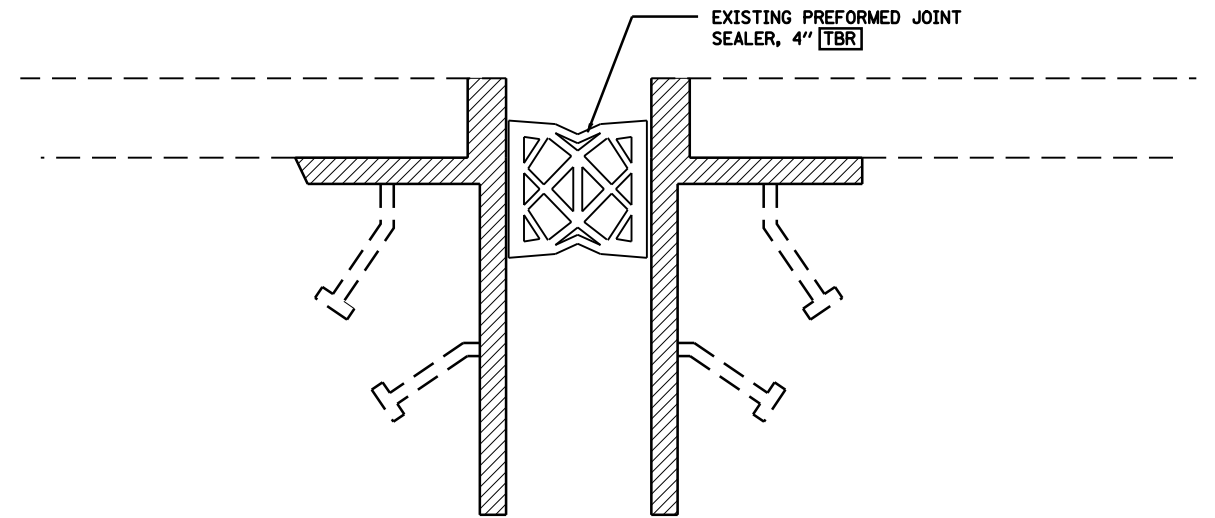
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	98
CONTRACT NO. 72694			ILLINOIS FED. AID PROJECT	



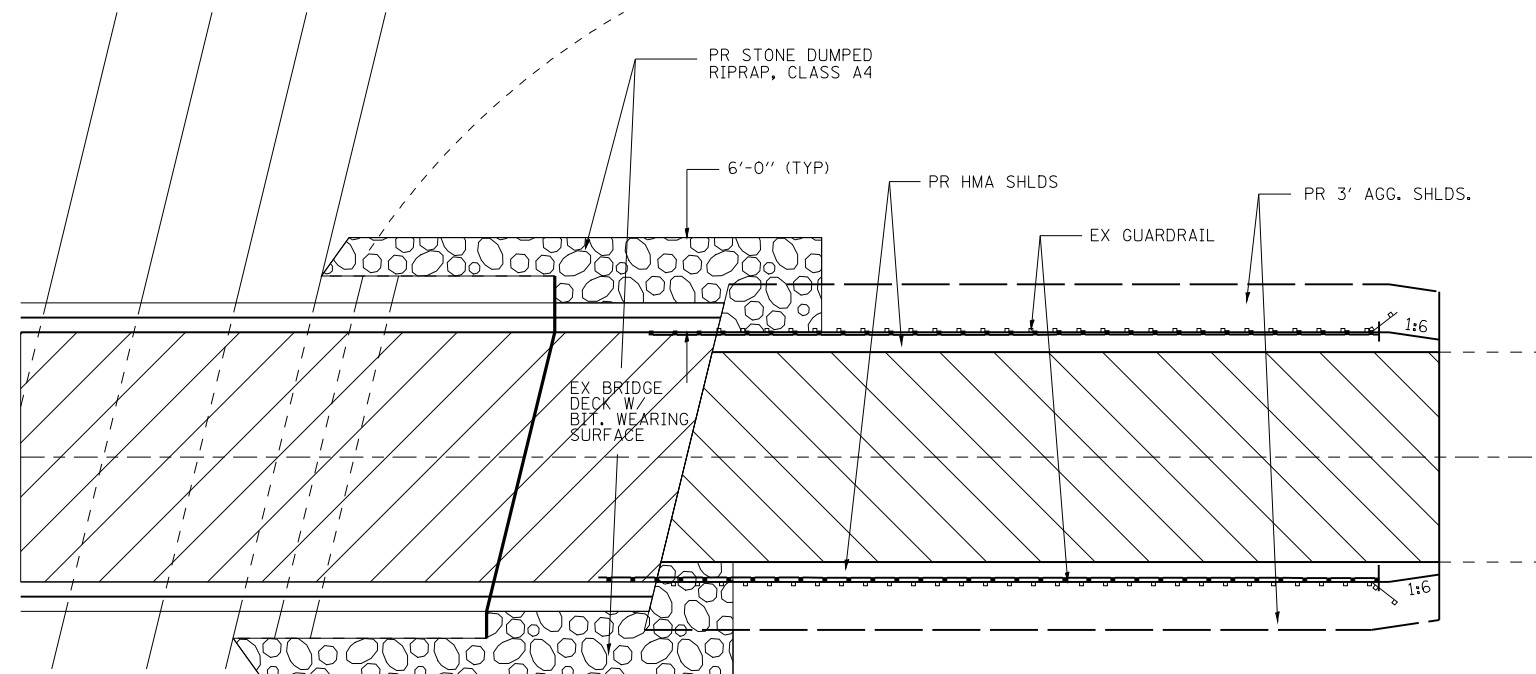
ELEVATION  
(RIPRAP DETAILS)



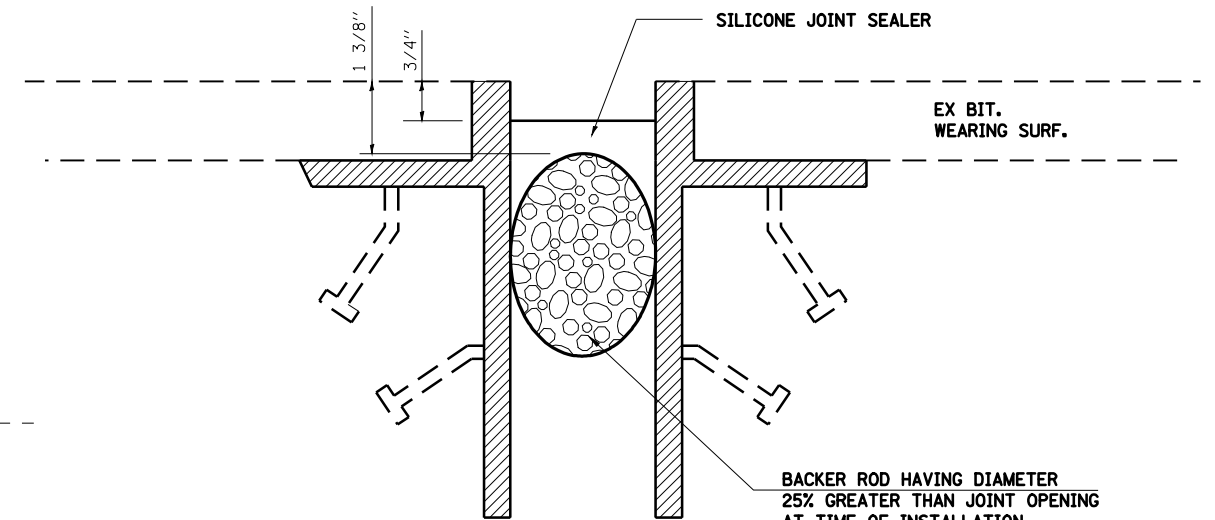
TYPICAL END OF SEAL TREATMENT  
AT EXPANSION JOINT



EXISTING



PLAN  
(RIPRAP DETAILS)



PROPOSED

SN 001-0049

STA 21+53.93  
STA 23+73.18

RIPRAP & JOINT REPLACEMENT DETAILS  
CH 3 (ELLINGTON ROAD) OVER FAI 172  
FAI ROUTE 172  
SECTION: (1-5)RS-1; (1-6)RS  
ADAMS COUNTY  
STA 139+35.12 (FAI 172)  
STA 22+65.43 (CH 3)  
SN 001-0049

SHEET 2 OF 5

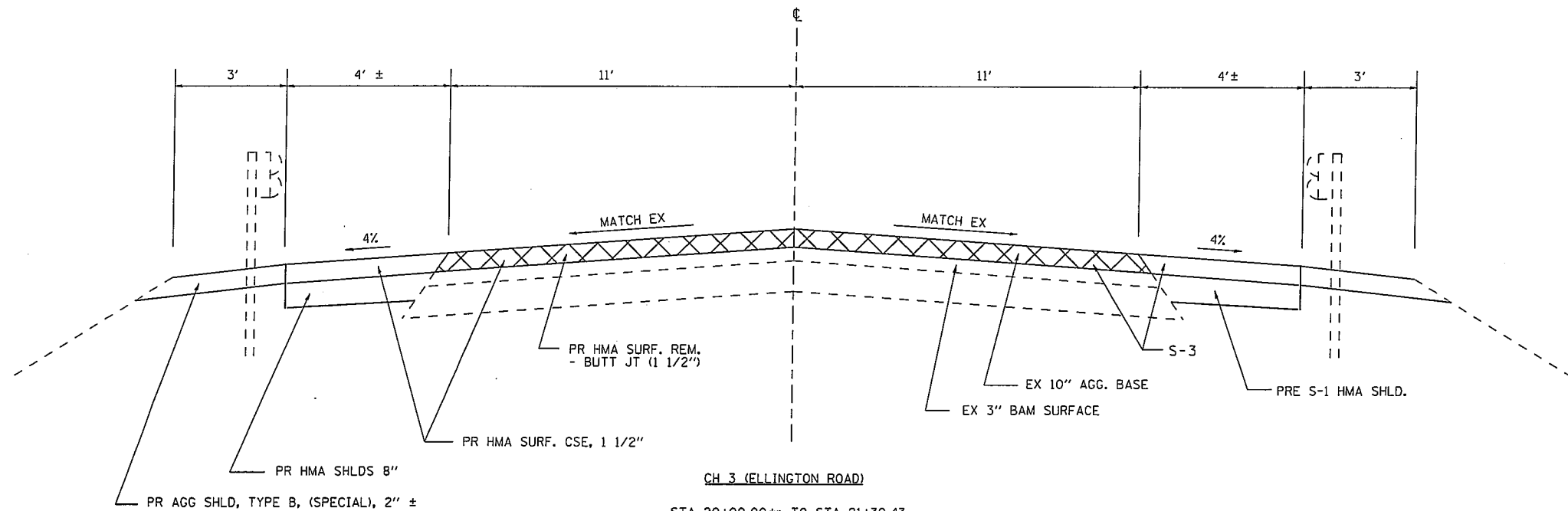
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS  
SN-001-0049

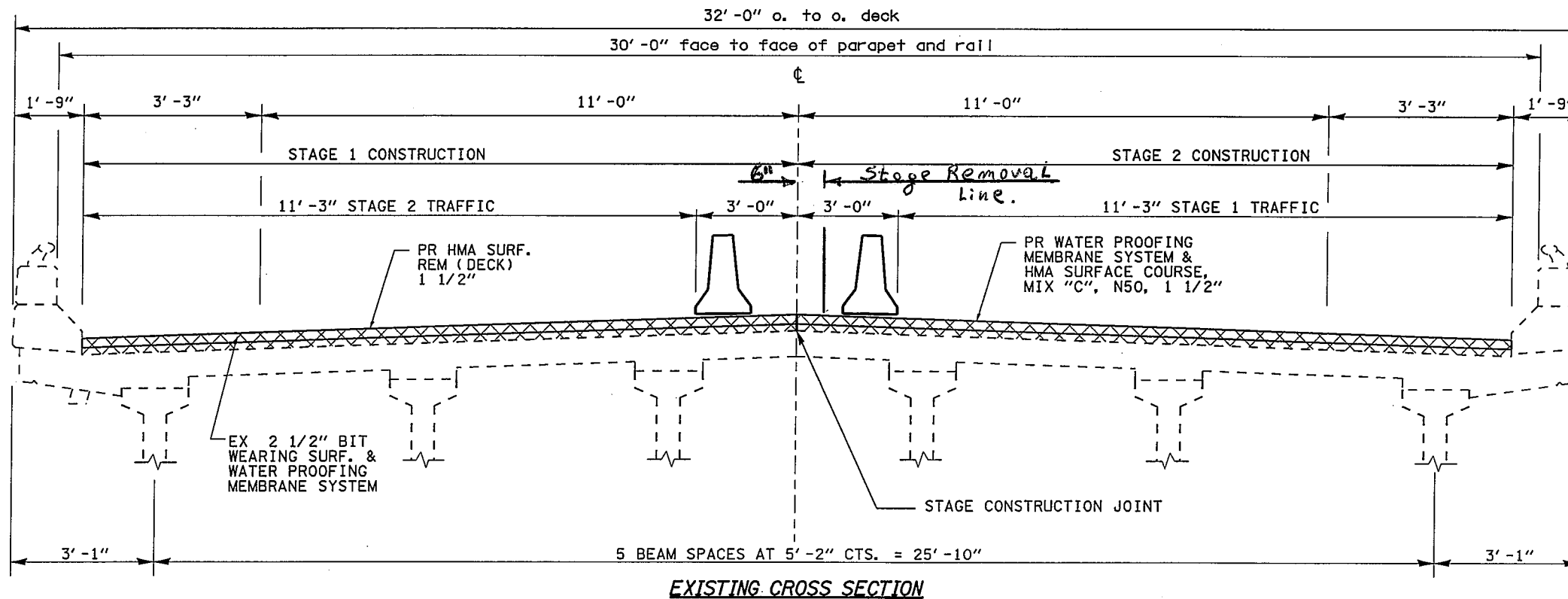
SCALE: SHEET NO. OF SHEETS STA. TO STA.

FAI RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	99
CONTRACT NO. 72694				
ILLINOIS FED. AID PROJECT				



CH 3 (ELLINGTON ROAD)  
 STA 20+00.00± TO STA 21+30.43  
 STA 21+30.43 TO STA 23+96.68 - BRIDGE SECTION (SHOWN BELOW)  
 STA 23+96.68 TO STA 25+25.00±

NOTE: \*TAPER ENDS OF SHLDS FROM 4' TO 1' STUB @ 1:6.  
 START TAPER AT END OF GUARDRAIL

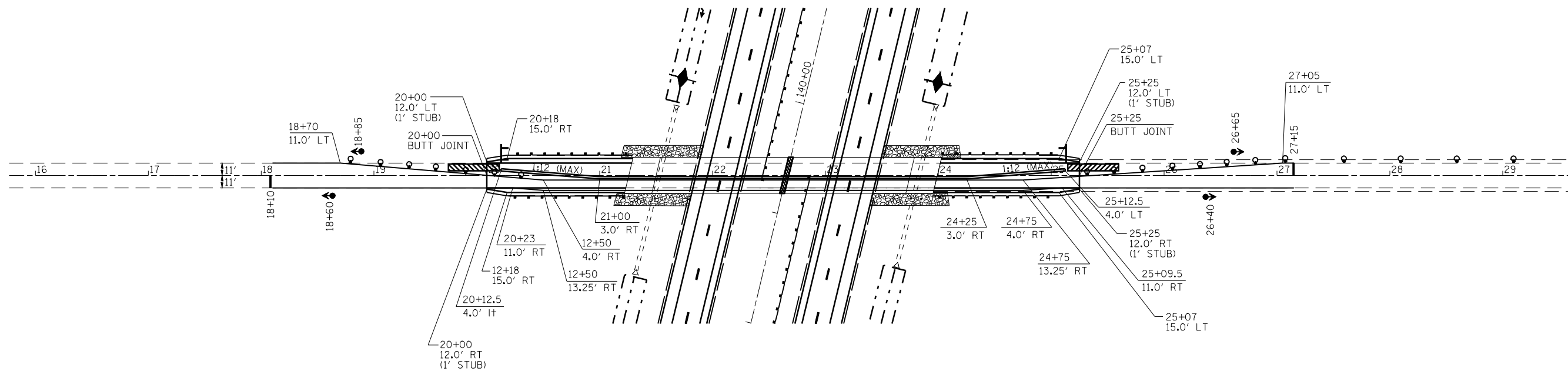


EXISTING CROSS SECTION

TYPICAL CROSS SECTION  
 BRIDGE APPROACH DETAILS  
 CH 3 (ELLINGTON ROAD) OVER FAI 172  
 FAI ROUTE 172  
 SECTION: (1-5)RS-1; (1-6)RS  
 ADAMS COUNTY  
 STA 139+35.12 (FAI 172)  
 STA 22+65.43 (CH 3)  
 SN 001-0049

SHEET 3 OF 5

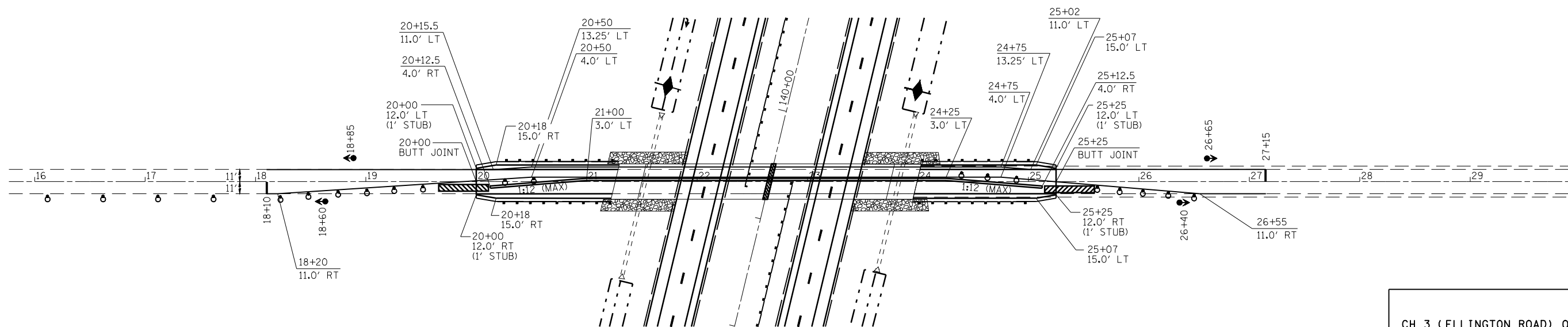
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	PLOT DATE = Feb-11-2009 11:40:52AM	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1
TEMPORARY CONCRETE BARRIER	FOOT	500
IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3	EACH	2

**STAGE 1**

11.25' CLEAR WIDTH  
9.25' LANE WIDTH



RELOCATE TEMPORARY CONCRETE BARRIER	SQ YD	500
IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3	EACH	2

**STAGE 2**

11.25' CLEAR WIDTH  
9.25' LANE WIDTH

CH 3 (ELLINGTON ROAD) OVER FAI 172  
FAI ROUTE 172  
SECTION: (1-5)RS-1; (1-6)RS  
ADAMS COUNTY  
STA 139+35.12 (FAI 172)  
STA 22+65.43 (CH 3)  
SN 001-0049

SHEET 4 OF 5

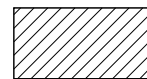
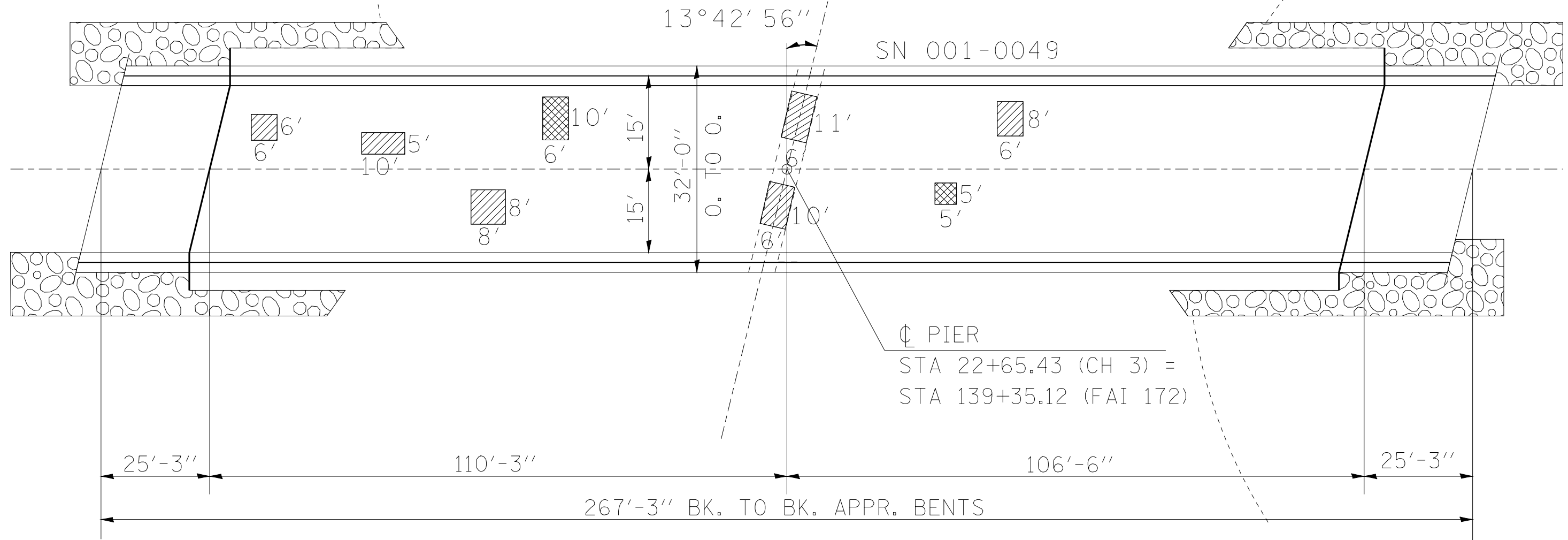
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

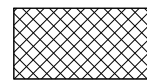
BRIDGE REPAIR DETAILS  
SN-001-0049

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	100A
CONTRACT NO. 72694				
ILLINOIS FED. AID PROJECT				



DECK SLAB REPAIR (PARTIAL DEPTH) - 36 SQ. YD.



DECK SLAB REPAIR (FULL DEPTH, TYPE II) - 9.4 SQ. YD.

DECK PATCHING  
 CH 3 (ELLINGTON ROAD) OVER FAI 172  
 FAI ROUTE 172  
 SECTION: (1-5)RS-1; (1-6)RS  
 ADAMS COUNTY  
 STA 139+35.12 (FAI 172)  
 STA 22+65.43 (CH 3)  
 SN 001-0049

SHEET 5 OF 5

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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS  
 SN-001-0049

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	100B
CONTRACT NO. 72694				
ILLINOIS FED. AID PROJECT				

SN 001-0050, 0051

EXISTING STRUCTURE: SN 001-0050, 0051 ARE DUAL STRUCTURES 42' - 0" O. TO O. OF DECK AND 288' - 3" BK. TO BK. OF ABUTMENTS, THREE-SPAN CONTINUOUS 48" PPC I BEAMS, 7 1/2" CONCRETE DECK SUPERSTRUCTURE SUPPORTED BY CONCRETE ABUTMENTS AND CONCRETE PIERS. THESE STRUCTURES CARRY FAI 172 OVER THE BNRR, AND WERE BUILT UNDER SECTION 1-6VB IN 1975.

THIS IMPROVEMENT INCLUDES REMOVING AND REPLACING EXPANSION JOINTS AT BOTH ENDS OF BOTH BRIDGES WITH A STRIP SEAL; REPAIR OF THE NW FASCIA BEAM ON SN 001-0051; MILLING AND RESURFACING THE APPROACH ROADWAYS; AND REPLACING THE EXISTING GUARDRAIL.

GENERAL NOTES

PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

PRIOR TO BEGINNING ANY REPAIR WORK, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING A PRELOADING SYSTEM ON THE BRIDGE DECK OVER THE EXISTING DAMAGED BEAM AT THE SPECIFIED LOCATION. THE PRELOADING SYSTEM SHOULD PRODUCE A TOTAL MAXIMUM

SERVICE LOAD MOMENT OF 740 FT. KIPS AT THE CENTERLINE OF THE DAMAGED AREA.

PRELOADING SHALL BE KEPT IN PLACE FOR AT LEAST THREE (3) DAYS AFTER COMPLETION OF CONCRETE REPAIR OR UNTIL THE CONCRETE HAS REACHED AN ULTIMATE STRENGTH OF 5,000 PSI.

THE CONTRACTOR'S PROPOSED PRELOADING SYSTEM, WITH COMPUTATIONS, SEALED AND SIGNED BY AN ILLINOIS STRUCTURAL ENGINEER SHALL BE SUBMITTED TO THE BUREAU OF BRIDGES AND STRUCTURES FOR APPROVAL. THE PRELOADING SYSTEM SHALL BE PLACED SHORTLY AFTER BRIDGE CLOSURE FOR REPAIRS.

THE PRELOADING SYSTEM SHALL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE UNIT PRICE FOR PPC I BEAM REPAIRS.

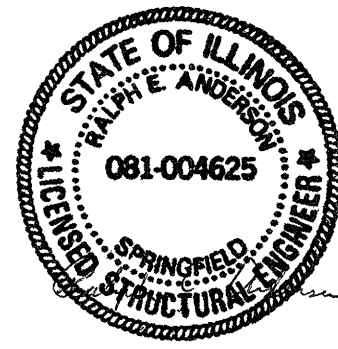
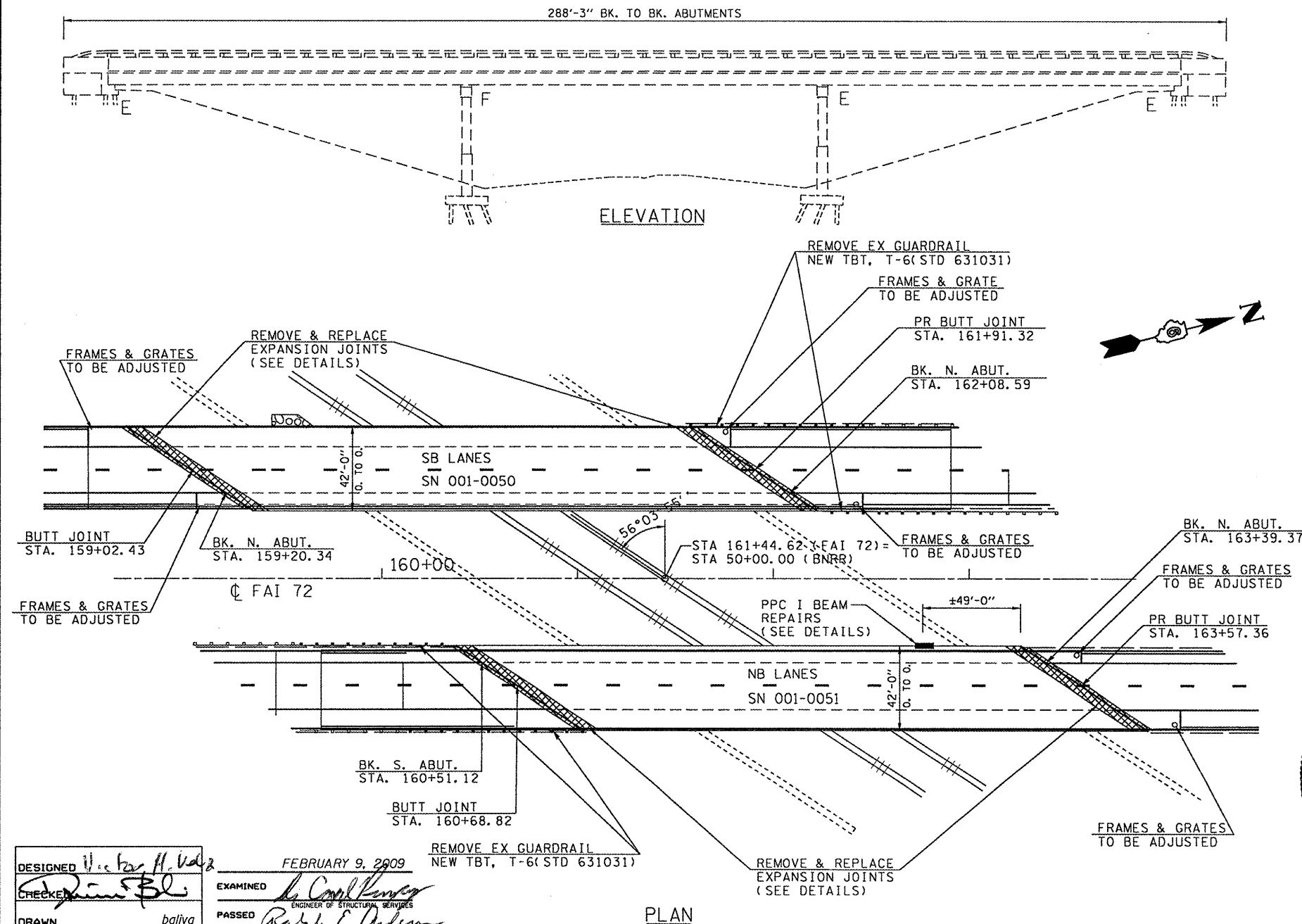
REINFORCEMENT BARS DESIGNATED (E) SHALL BE EPOXY COATED.

REINFORCEMENT BARS SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 706 GR 60. SEE SPECIAL PROVISIONS.

EXISTING REINFORCEMENT BARS EXTENDING INTO THE REMOVAL AREA SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY REINFORCEMENT BARS THAT ARE DAMAGED DURING CONCRETE REMOVAL SHALL BE REPLACED WITH AN APPROVED BAR SPLICER OR ANCHORAGE SYSTEM. COST INCLUDED WITH CONCRETE REMOVAL.

JOINT OPENINGS SHALL BE ADJUSTED ACCORDING TO ARTICLE 520.04 OF THE STANDARD SPECIFICATIONS WHEN THE DECK IS POURED AT AN AMBIENT TEMPERATURE OTHER THAN 50 DEGREES FAHRENHEIT.

ALL REPAIR WORK ASSOCIATED WITH THE BRIDGES SHALL BE COMPLETED BY STAGE CONSTRUCTION UTILIZING TC&P, STANDARD 701402 AND WITH TEMPORARY CONCRETE BARRIER LOCATED AS DETAILED IN THESE REPAIR PLANS.



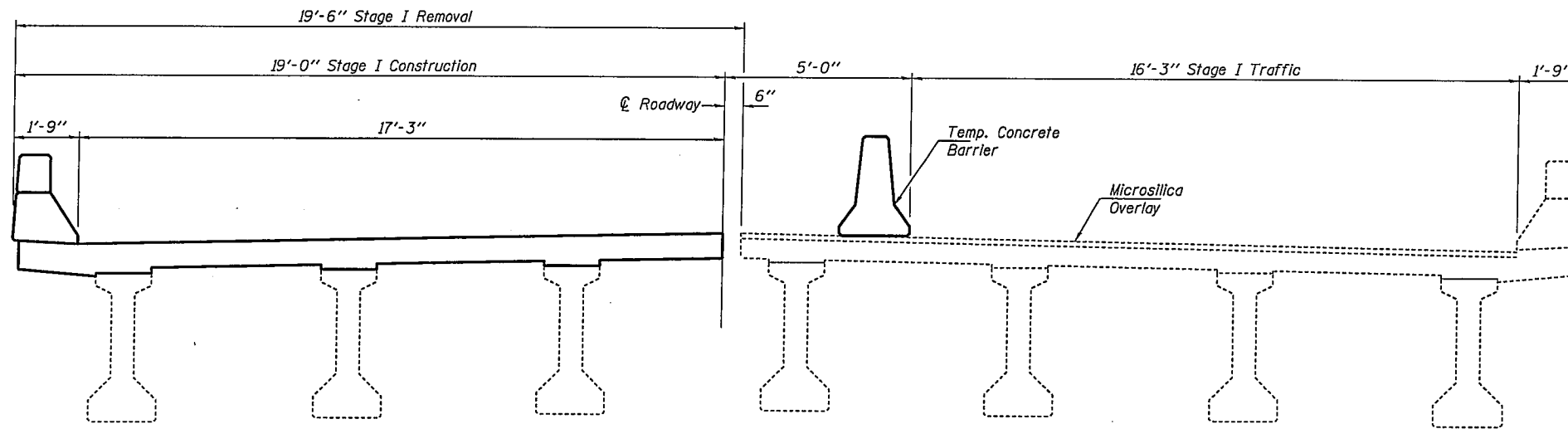
TOTAL BILL OF MATERIALS		
ITEM	UNIT	TOTAL
CONCRETE REMOVAL	CU YD	70.0
REINFORCEMENT BARS, EPOXY COATED	POUND	12,520
BAR SPLICERS	EACH	56
PREFORMED JOINT STRIP SEAL	FOOT	301
CONCRETE SUPERSTRUCTURE	CU YD	70.0
PPC I BEAM REPAIRS	LS	1

DESIGNED: *W. C. ...* FEBRUARY 9, 2009  
 CHECKED: *[Signature]*  
 DRAWN: *balliva*  
 EXAMINED: *[Signature]*  
 PASSED: *Ralph E. Anderson*  
 ENGINEER OF BRIDGES AND STRUCTURES

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION
#FILE#		DRAWN -	REVISED -	
		CHECKED -	REVISED -	
		DATE -	REVISED -	

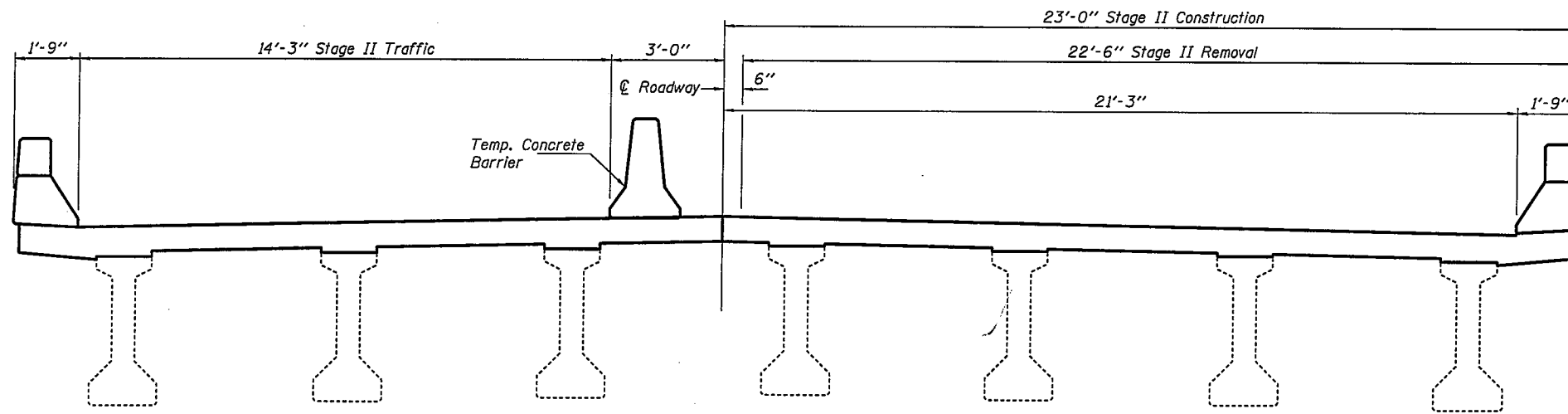
GENERAL PLAN & ELEVATION		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
STA 161+44.62(FAI 172), STA 50+00.00(BNRR)		172	(1-5)RS-1; (1-6)RS	ADAMS	108	101
SN001-0050(SB), SN001-0051(NB)		SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 72694		
ILLINOIS FED. AID PROJECT						

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**STAGE I CONSTRUCTION**

(NB looking North)  
(SB looking South)



**STAGE II CONSTRUCTION**

(NB looking North)  
(SB looking South)

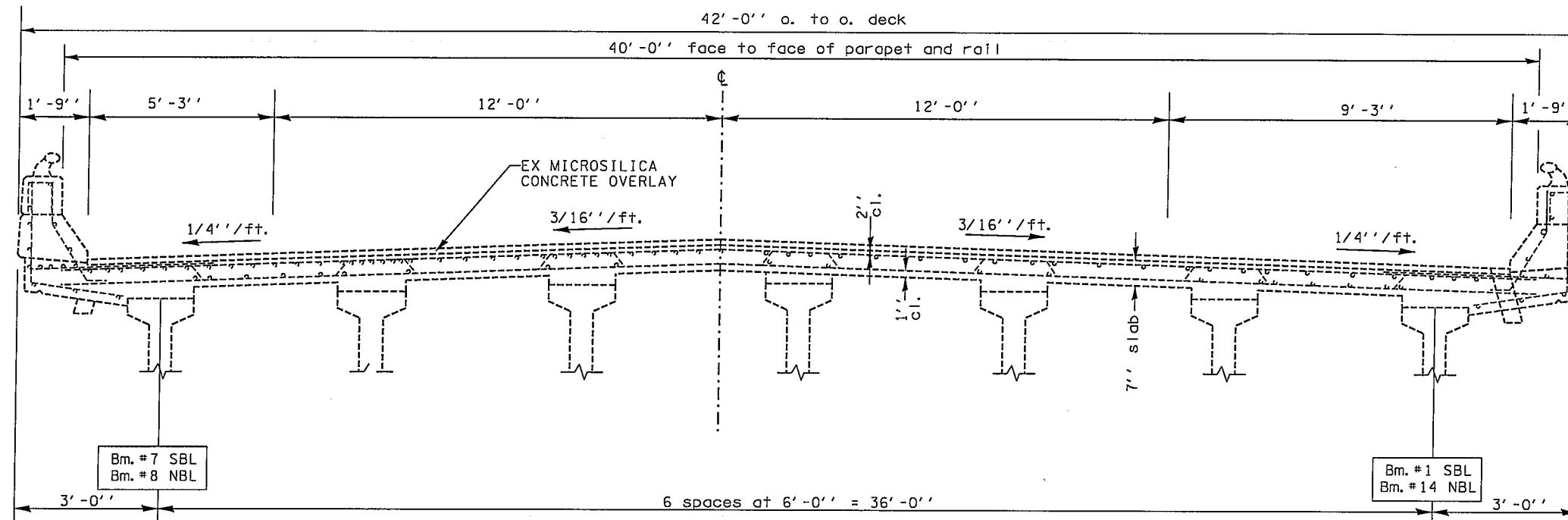
DESIGNED	VHV
CHECKED	DAB
DRAWN	baliva
CHECKED	VHV DAB

FEBRUARY 9, 2009  
EXAMINED *Carl Perry*  
ENGINEER OF STRUCTURAL SERVICES  
PASSED *Ralph E. Anderson*  
ENGINEER OF BRIDGES AND STRUCTURES

**STAGING**  
**FAI 172 OVER BN RR**  
**SN 001-0050 & 0051**

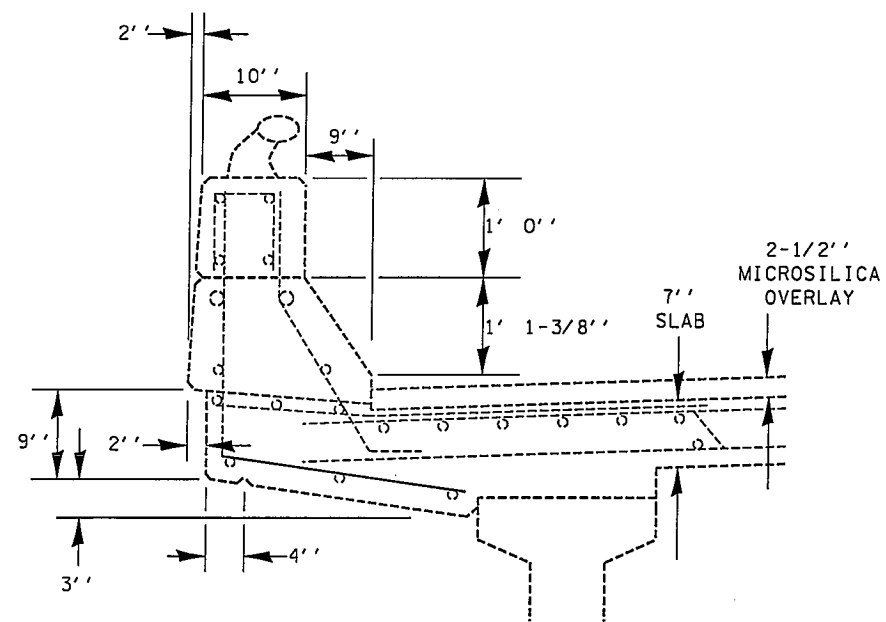
SHEET NO. 2 8 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	172	(1-5)RS-1: (1-6)RS	ADAMS	108	102
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT		
CONTRACT NO. 72694					





**EXISTING CROSS SECTION**

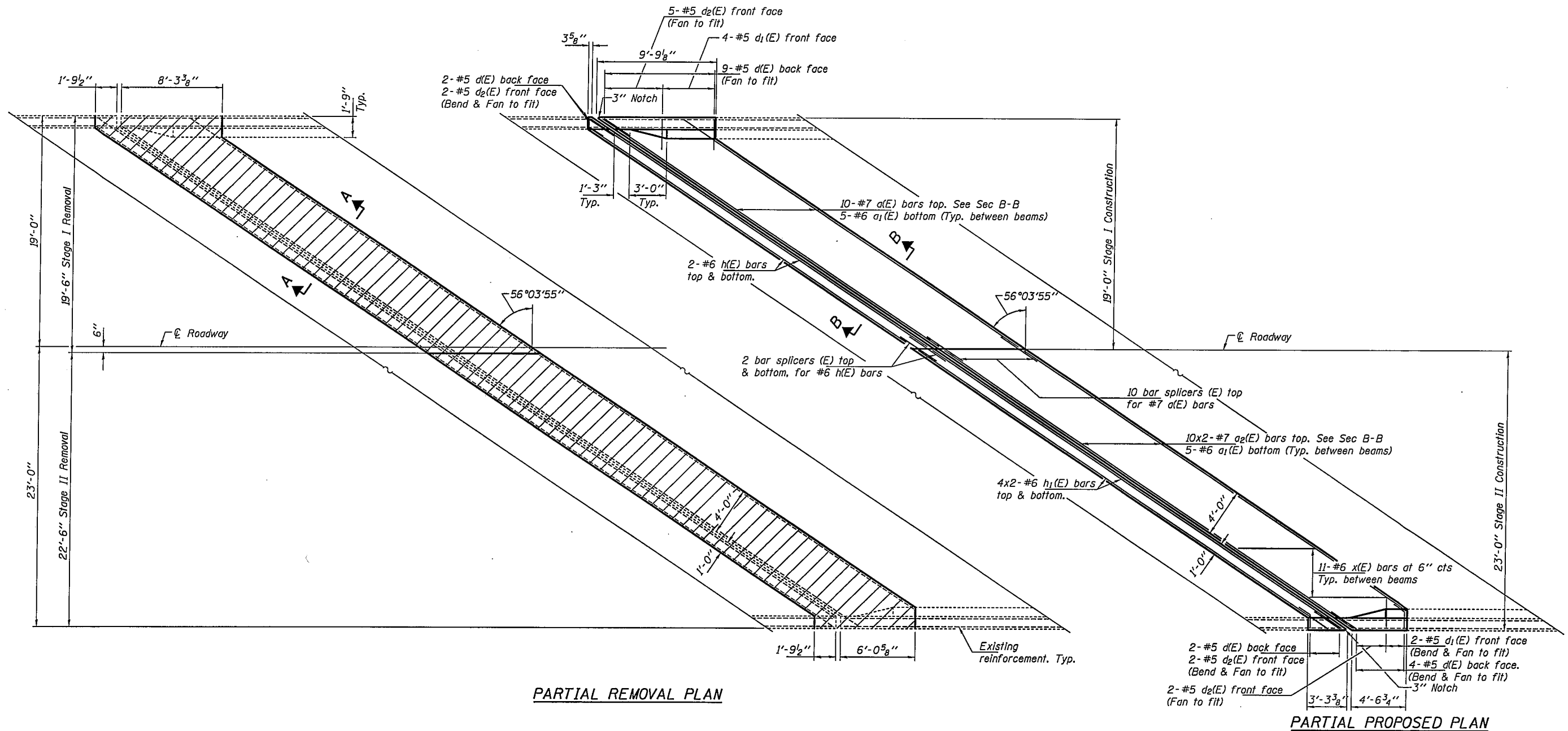
LOOKING N - NBL  
LOOKING S - SBL



**PARAPET CROSS SECTION**

FILE NAME =	USER NAME = #USER#	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BRIDGE REPAIR DETAILS &amp; EXISTING REINFORCEMENT</b> STA 161+44.62(FAI 172), STA 50+00.00(BNRR) SN001-0050(SB), SN001-0051(NB)			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
#FILE#		DRAWN -	REVISED -					172	(1-5)RS-1; (1-6)RS	ADAMS	108	103
	PLOT SCALE = #SCALE#	CHECKED -	REVISED -		SCALE: SHEET NO. OF SHEETS STA. TO STA.			CONTRACT NO. 72694				
	PLOT DATE = #DATE#	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**PARTIAL REMOVAL PLAN**

**PARTIAL PROPOSED PLAN**

DESIGNED	VHV
CHECKED	DAB
DRAWN	baliva
CHECKED	VHV DAB

FEBRUARY 9, 2009  
 EXAMINED *A. Carl Perry*  
 ENGINEER OF STRUCTURAL SERVICES  
 PASSED *Ralph E. Anderson*  
 ENGINEER OF BRIDGES AND STRUCTURES

**MINIMUM BAR LAPS**

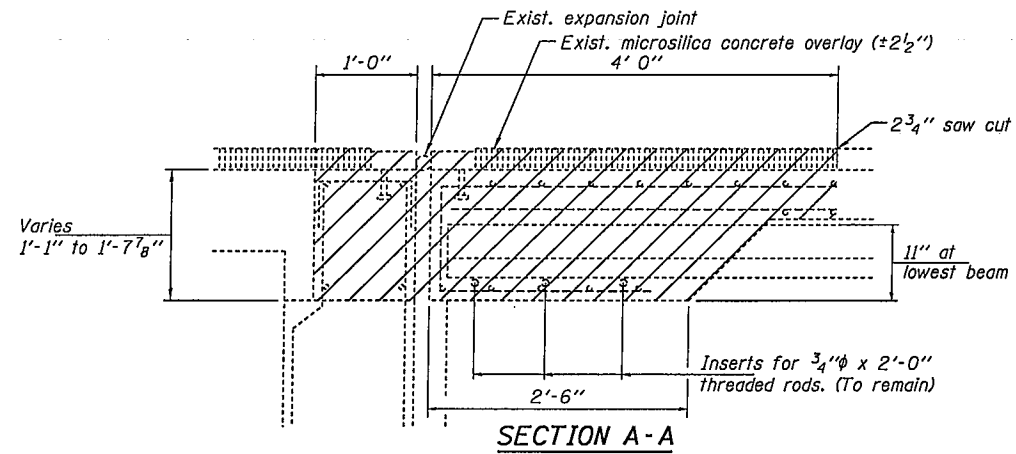
#6 bar = 2'-7"  
 #7 bar = 3'-5"

**Note:**  
 Hatched areas indicate concrete removal.  
 Bars indicated thus 4 x 2-#6 etc. indicates 4 lines of bars with 2 lengths per line.  
 For bar details & Bill of Material, see sheet 5 of 8.  
 Dimensions are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on sheet 6 of 8.

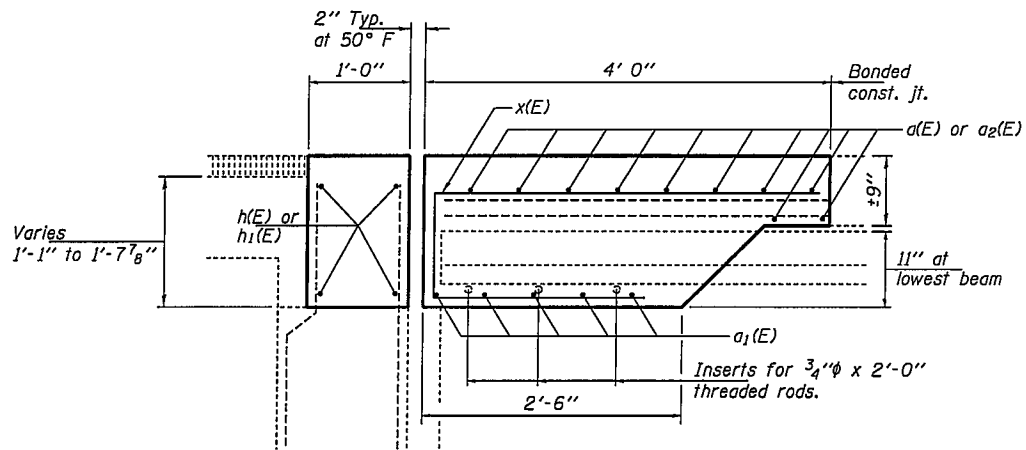
**ABUTMENT DETAILS**  
**FAI 172 OVER BN RR**  
**SN 001-0050 & 0051**

SHEET NO. 4 8 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	172	(1-5)RS-1: (1-6)RS	ADAMS	108	104
FED. ROAD DIST. NO.			ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 72694					

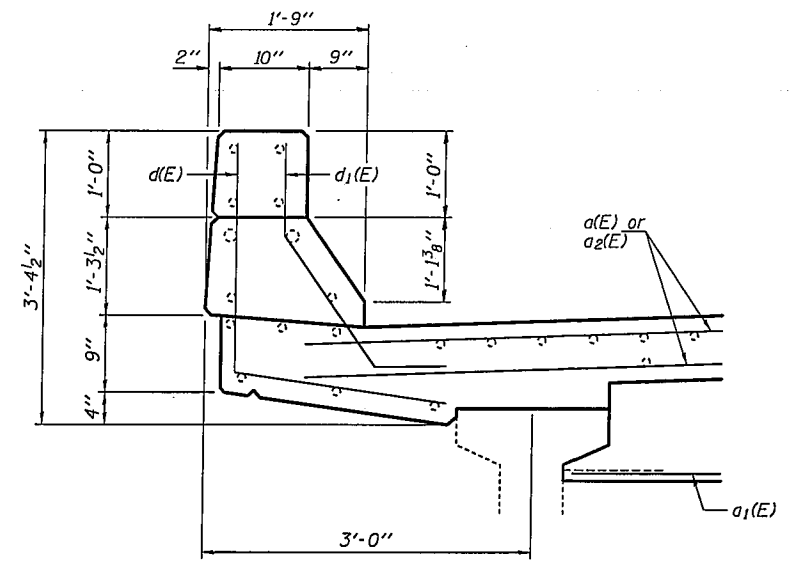
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



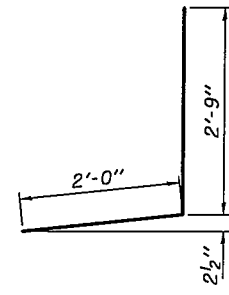
SECTION A-A



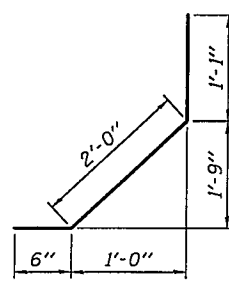
SECTION B-B



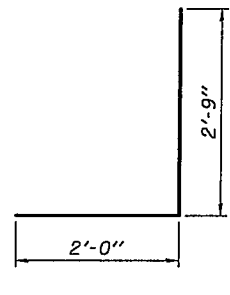
PARAPET CROSS SECTION



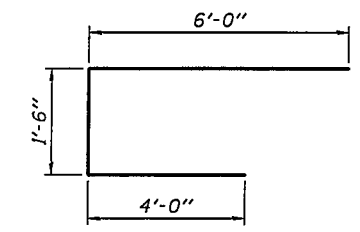
BAR d(E)



BAR d1(E)



BAR d2(E)



BAR x(E)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
d(E)	40	#7	33'-3"	—
d1(E)	120	#6	9'-3"	—
d2(E)	80	#7	22'-6"	—
d(E)	34	#5	4'-9"	J
d1(E)	12	#5	3'-7"	J
d2(E)	22	#5	4'-9"	J
h(E)	16	#6	33'-3"	—
h1(E)	32	#6	22'-0"	—
x(E)	132	#6	11'-6"	—
Bar Splacers			Each	56
Reinforcement Bars, Epoxy Coated			Pound	12520
Concrete Superstructure			Cu. Yd.	70.0

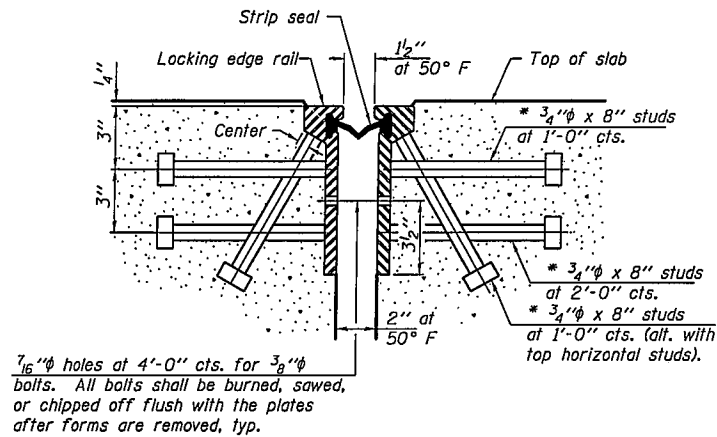
ABUTMENT DETAILS  
FAI 172 OVER BN RR  
SN 001-0050 & 0051

DESIGNED	VHV
CHECKED	DAB
DRAWN	baliva
CHECKED	VHV DAB

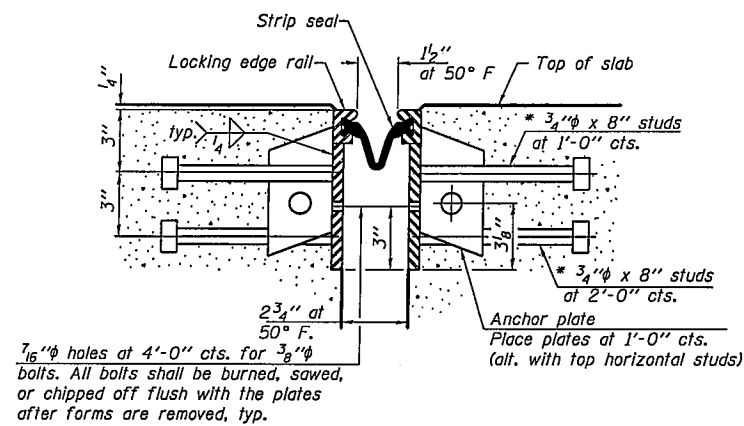
FEBRUARY 9, 2009  
EXAMINED *Carl Proyer*  
ENGINEER OF STRUCTURAL SERVICES  
PASSED *Ralph E. Anderson*  
ENGINEER OF BRIDGES AND STRUCTURES

SHEET NO. 5 8 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	172	(1-5)RS-1: (1-6)RS	ADAMS	108	105
FED. ROAD DIST. NO. ILLINOIS			FED. AID PROJECT		
CONTRACT NO. 72694					

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



**SECTION THRU ROLLED RAIL JOINT**



**SECTION THRU WELDED RAIL JOINT**

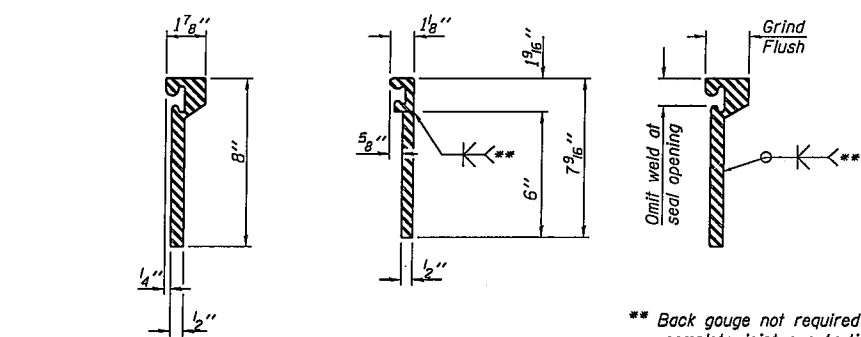
Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

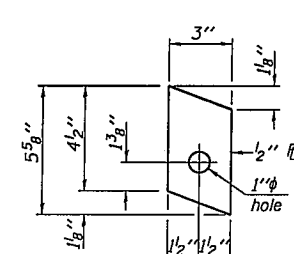


**ROLLED EXTRUDED RAIL**

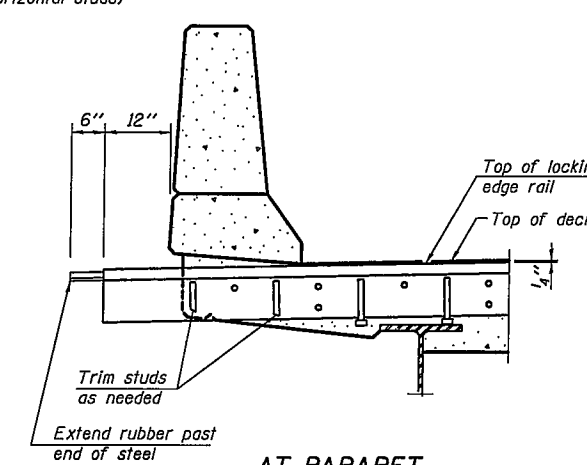
**WELDED RAIL**

**LOCKING EDGE RAIL SPLICE**

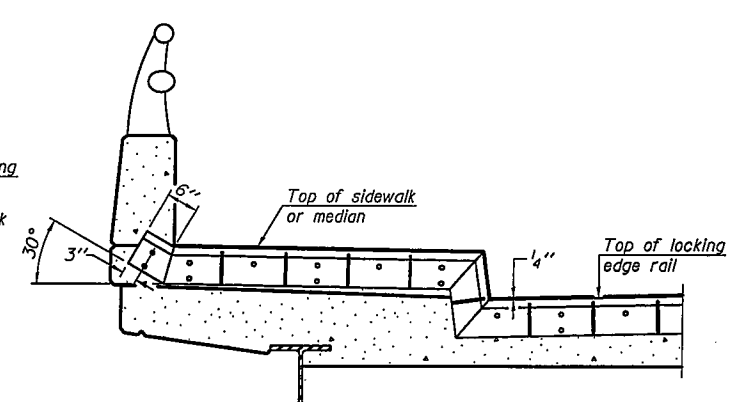
The inside of the locking edge rail groove shall be free of weld residus.



**ANCHOR PLATE (for welded rail)**



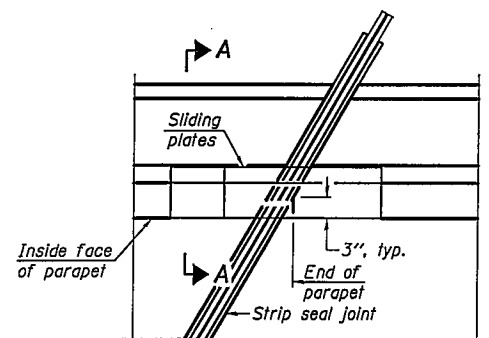
**AT PARAPET**



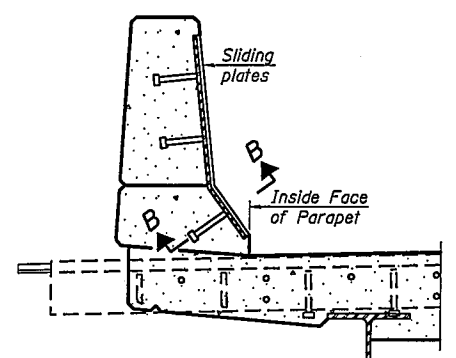
**AT SIDEWALK OR MEDIAN**

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

**LOCKING EDGE RAILS**



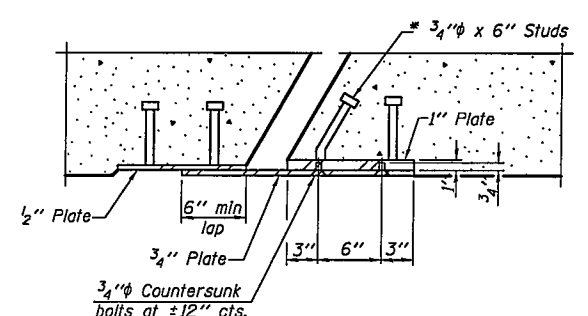
**PLAN**



**SECTION A-A**

**POINT BLOCK DETAILS (for skews > 30°)**

**TYPICAL END TREATMENTS**



**SECTION B-B**

**BILL OF MATERIAL**

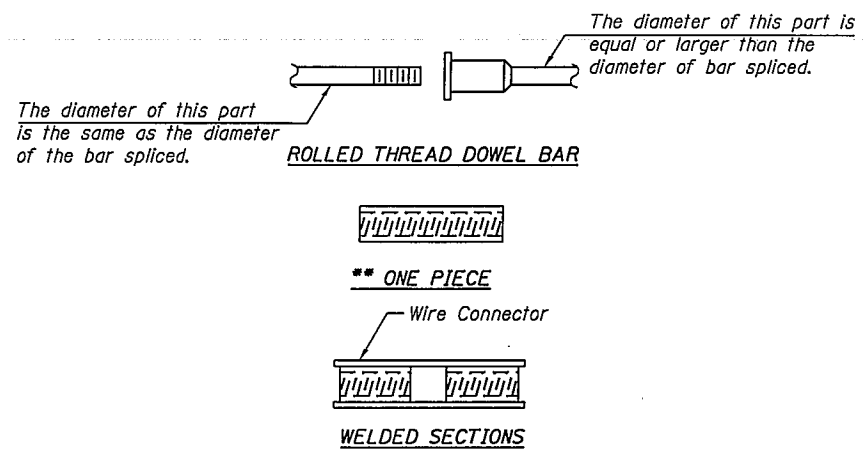
Item	Unit	Total
Preformed Joint Strip Seal	Foot	301

**NOTES**

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

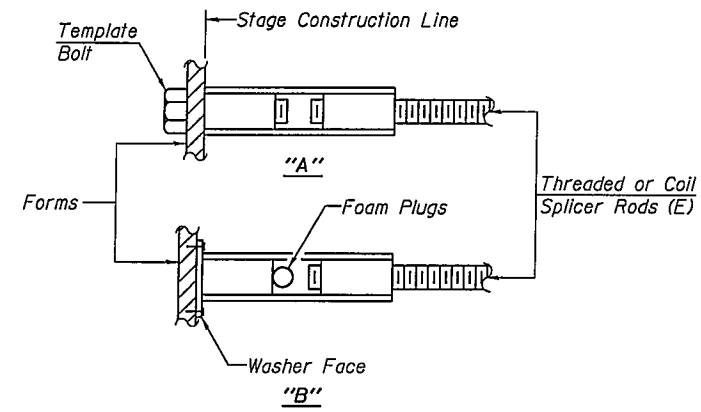
- ① Minimum Capacity (Tension in kips) =  $1.25 \times f_y \times A_l$
  - ② Minimum \*Pull-out Strength (Tension in kips) =  $0.66 \times f_y \times A_l$
- Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $A_l$  = Tensile stress area of lapped reinforcement bars.  
 \* = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



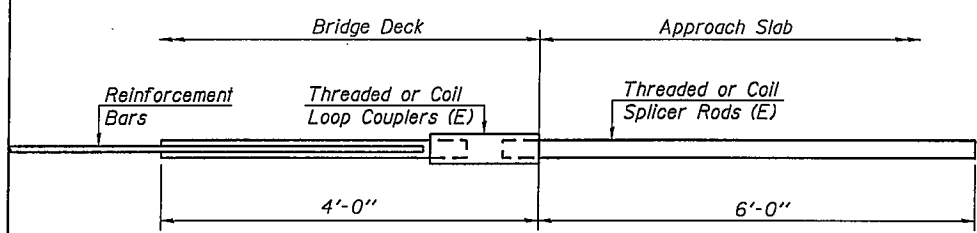
**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\*Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



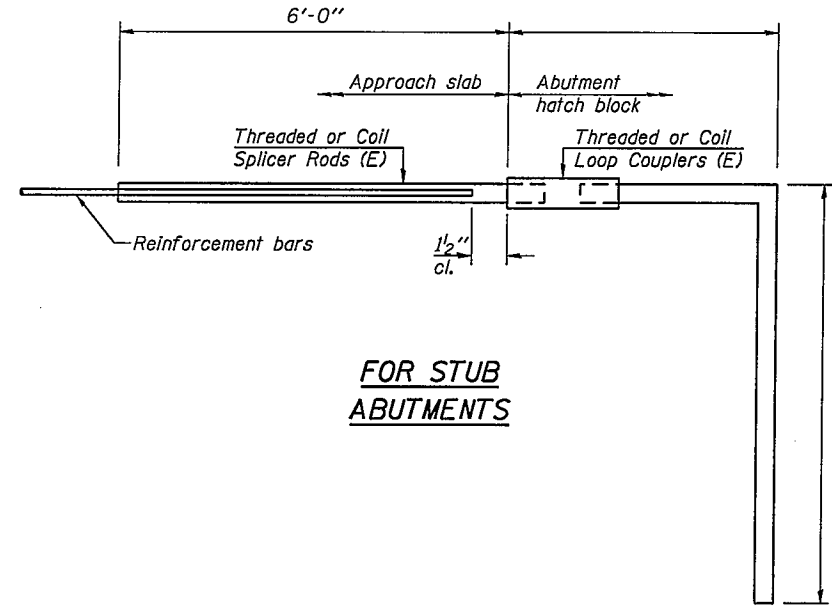
**INSTALLATION AND SETTING METHODS**

"A" :Set bar splicer assembly by means of a template bolt.  
 "B" :Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
 (E) : Indicates epoxy coating.



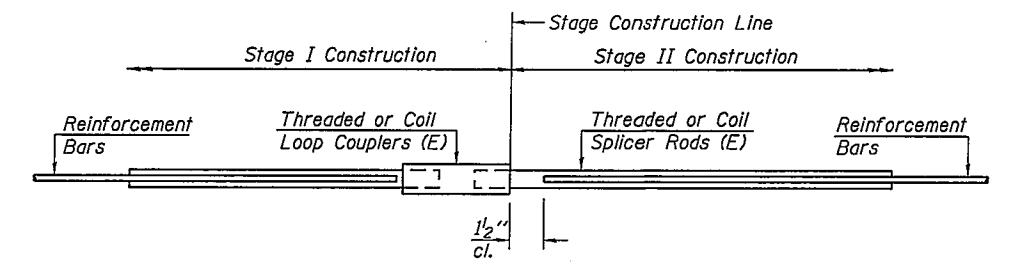
**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

Bar Splicer for #5 bar	
Min. Capacity = 23.0 kips - tension	
Min. Pull-out Strength = 12.3 kips - tension	
No. Required = 0	



**FOR STUB ABUTMENTS**

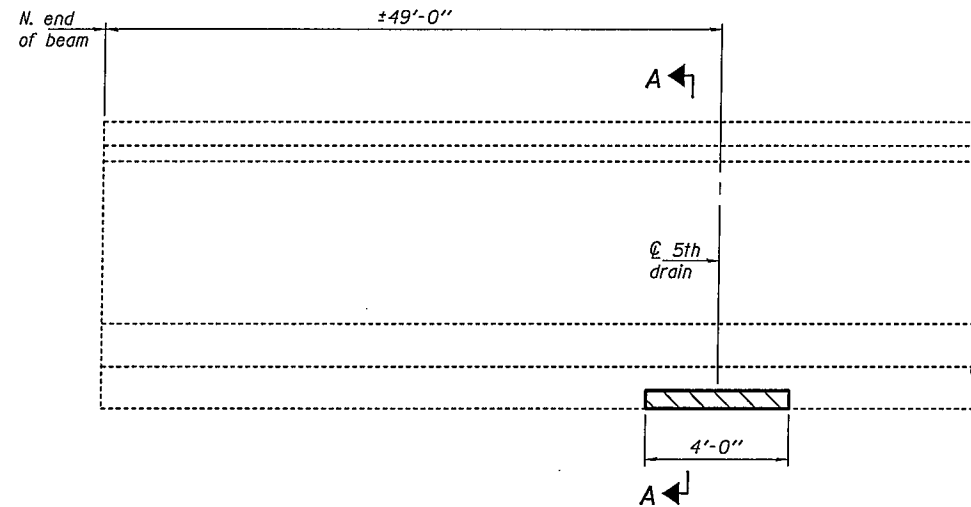
Bar Splicer for #5 bar	
Min. Capacity = 23.0 kips - tension	
Min. Pull-out Strength = 12.3 kips - tension	
No. Required = 0	



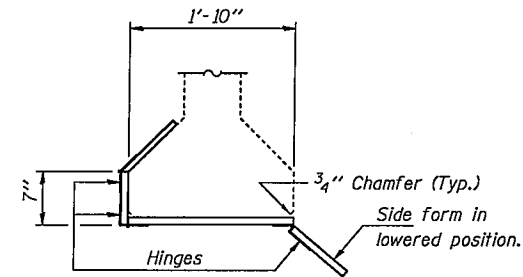
**STANDARD**

Bar Size	No. Assemblies Required	Location
#6	16	Hatch Block
#7	40	Deck

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



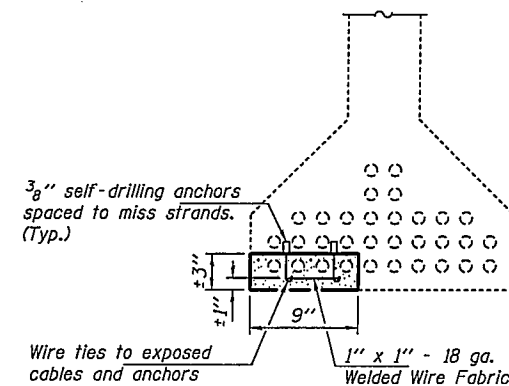
**PARTIAL ELEVATION BEAM 8**  
(Looking East)



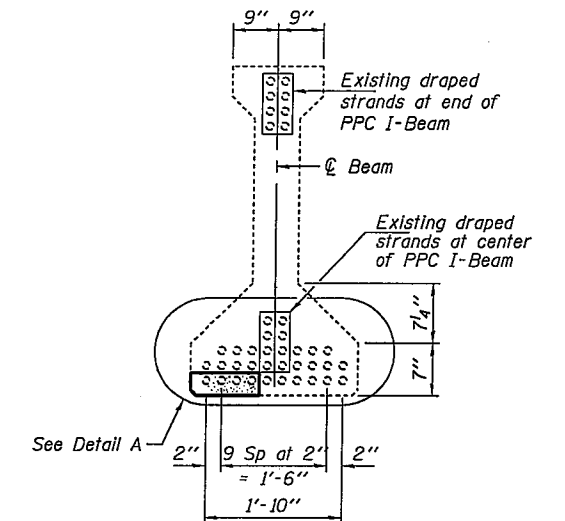
**SUGGESTED FORM DETAIL**

**REPAIR PROCEDURES FOR BEAM 8 SPAN 3**

1. The damaged area of the beam shall be cleaned of all loose and spalled concrete and sealant. All loose material shall be removed to sound concrete until coarse aggregate will break under chipping rather than dislodging. Hand tools shall be used for the removal of concrete adjacent to the prestressing strands. While a 15 pound chipping hammer may be used away from prestressing strands, extreme care shall be taken not to damage the exposed prestressing strands.
2. Using the same tools, remove the existing concrete to sound concrete, as described above, along the edges of the damaged area to a depth of 1" min. to 1 1/2" max. The edges shall be saw cut 3/4" deep. The entire area of existing concrete against which new concrete will be placed and any exposed portions of the prestressing strands shall be sandblasted. The concrete shall be sandblasted to expose clean, well bonded aggregate.
3. Power driven pins as shown in Detail A shall be placed at 9" alternate centers along damaged length of beam at locations shown in Detail A. Place 1" x 1" x 18 gauge welded wire fabric in repair areas and attach it to the pins or strands with wire ties. The clearance between the finished surface of the new concrete and the welded wire fabric shall be 1" minimum. All beams involved in this work shall be rebuilt to their original dimensions.
4. The surface of the existing concrete against which new concrete will be placed shall be prepared as a bonded construction joint according to Article 503.09(b)(2) of the Standard Specifications. Other minor mortar repair, crack sealing or surface sealing of gouges on the beam shall be performed as directed by the Engineer.
5. The repair shall be made using a material from the "Approved List of Non-Shrink Grouts" maintained by the Bureau of Materials and Physical Research. The repair material chosen shall be appropriate for the thickness of repair to be made. Coarse aggregate with maximum size of 3/8" shall be added with the amount as specified by the manufacturer. Place the lower form on the bottom of the beam and compact by vibrating (or other approved methods) the mix into the voids. After accessible voids have been filled and compacted, the side form shall be raised into position and the remaining voids filled and compacted. The sloping upper surface shall be finished to the configuration of the existing PPCI-Beam flange.
6. Preloading, if specified, and forms shall be kept in place for 3 days after completion of concrete repair or until the repair material has reached an ultimate strength of 5,000 psi. Timing of form removal shall be modified as necessary to meet curing requirements as specified by the manufacturer.



**DETAIL A**



**SECTION A-A  
PATCHING DETAIL**  
Beam 8, Span 3

Note:  
The cost of concrete removal, non-shrink grout, power driven pins, wire ties, wire mesh, Epoxy Crack Sealing and all other work required to perform repairs on Beam 8 in Span 3 shall be included in the cost of P.P.C. I Beam Repairs.

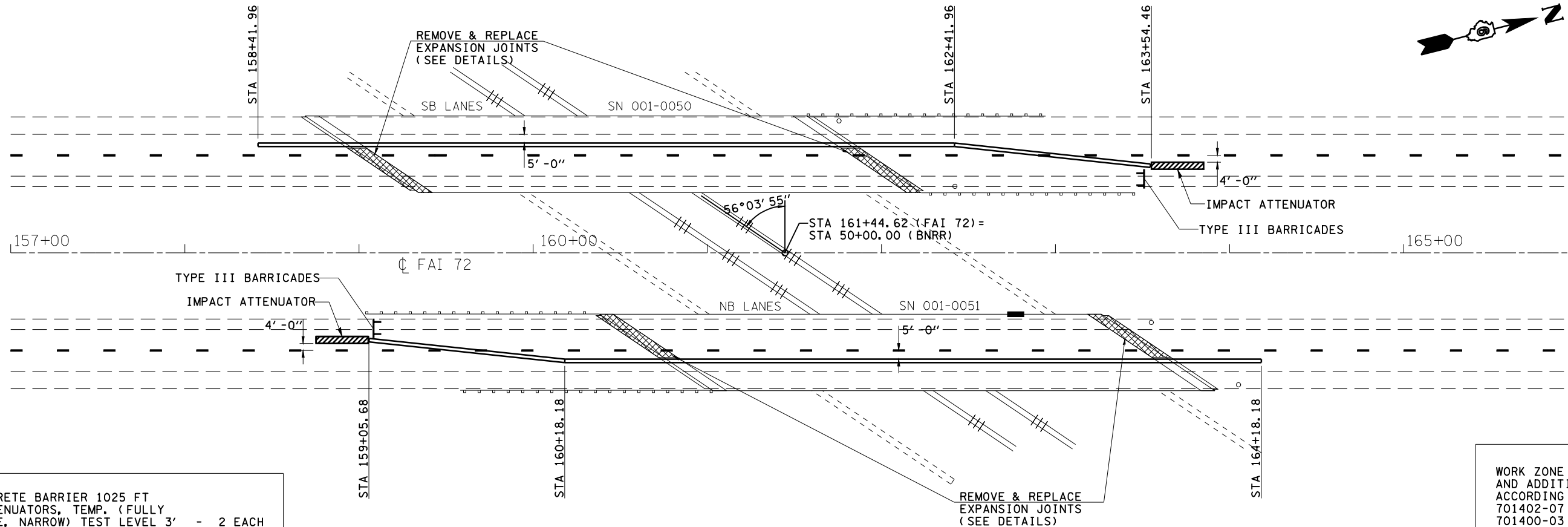
DESIGNED	VHV
CHECKED	DAB
DRAWN	baliva
CHECKED	VHV DAB

FEBRUARY 9, 2009  
EXAMINED *Carl Perry*  
ENGINEER OF STRUCTURAL SERVICES  
PASSED *Ralph E. Anderson*  
ENGINEER OF BRIDGES AND STRUCTURES

**REPAIR DETAILS**  
**FAI 172 OVER BN RR**  
**SN 001-0051**

SHEET NO. 8 8 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	172	(1-5)RS-1; (1-6)RS	ADAMS	108	108
FED. ROAD DIST. NO.			ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 72694					

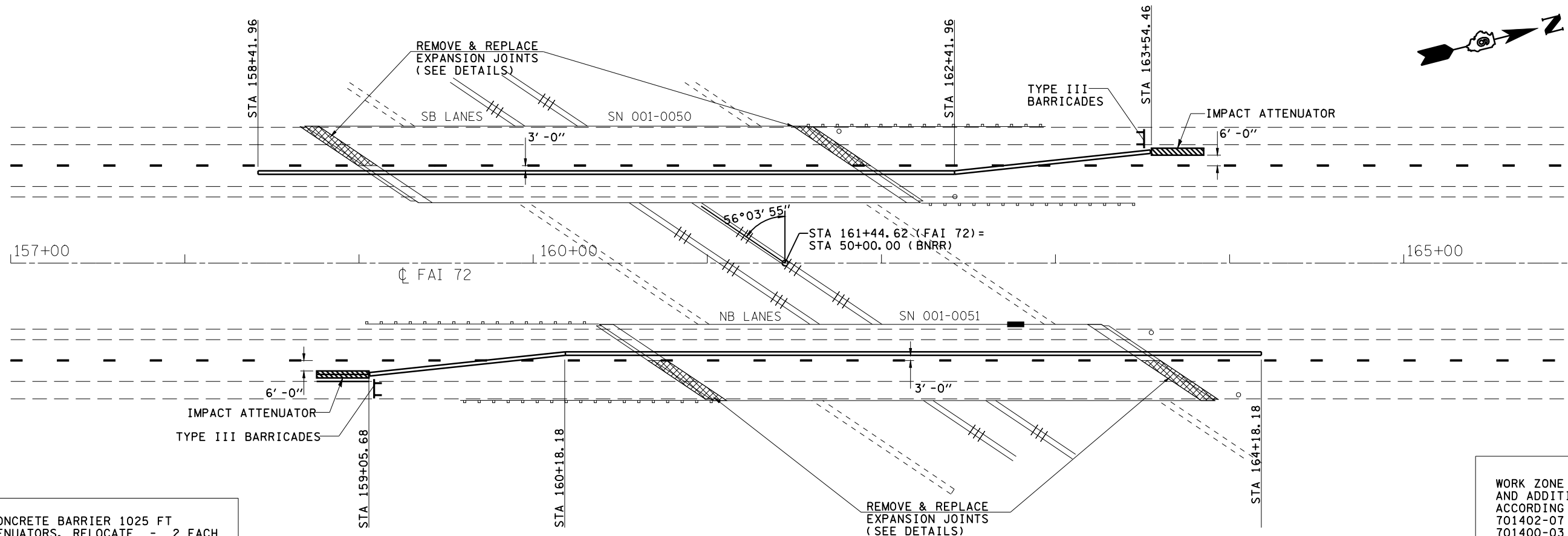
STAGE I



TEMP. CONCRETE BARRIER 1025 FT  
IMPACT ATTENUATORS, TEMP. (FULLY  
REDIRECTIVE, NARROW) TEST LEVEL 3' - 2 EACH

WORK ZONE PAVEMENT MARKING  
AND ADDITIONAL TRAFFIC CONTROL  
ACCORDING TO STANDARD  
701402-07  
701400-03

STAGE II



RELOCATE CONCRETE BARRIER 1025 FT  
IMPACT ATTENUATORS, RELOCATE - 2 EACH

WORK ZONE PAVEMENT MARKING  
AND ADDITIONAL TRAFFIC CONTROL  
ACCORDING TO STANDARD  
701402-07  
701400-03

FILE NAME =	USER NAME = laughlinr1	DESIGNED -	REVISED -
et:\pwork\PWIDOT\LAUGHLINRL\dms26563\108A Temp Barrier Wall Detail SN001-0050.SN001-0050.DRAWN.DOT.REV.dgn		DRAWN -	REVISED -
PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -	REVISED -
PLOT DATE = Feb-11-2009 12:50:05PM	DATE -	REVISED -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TEMPORARY BARRIER WALL DETAIL  
STA 161 + 44.62 (FAI 72), STA 50 + 00.00 (BNRR)  
SN001-0050 (SB), SN001-0051 (NB)

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
172	(1-5)RS-1; (1-6)RS	ADAMS	108	108A
CONTRACT NO. 72694				
ILLINOIS FED. AID PROJECT				