

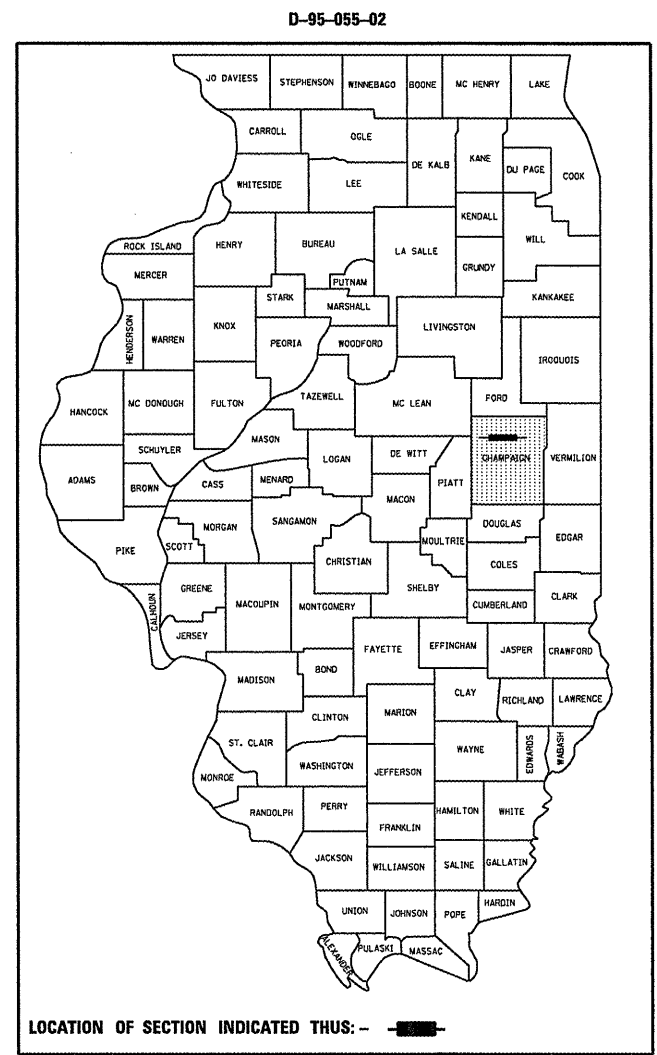
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
709	105RS-2	Champaign	19	1
		ILLINOIS	CONTRACT NO. 70261	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED HIGHWAY PLANS

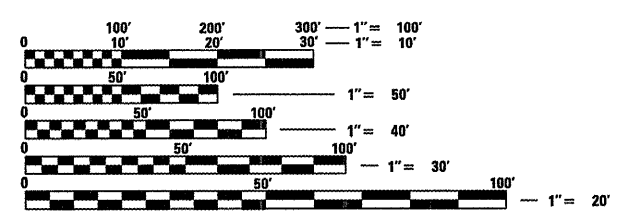
F.A.P. ROUTE 709 (US 136)
SECTION 105RS-2
PROJECT ESP-0709(024)
RESURFACING (3P)
CHAMPAIGN COUNTY
DEWEY-FISHER RD TO S. BRANCH BIG DITCH W. OF RANTOUL
C-95-103-02

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 4



CURRENT ADT
LEG "A" = 2,800
LEG "B" = 4,300

DESIGN DESIGNATION
N/A

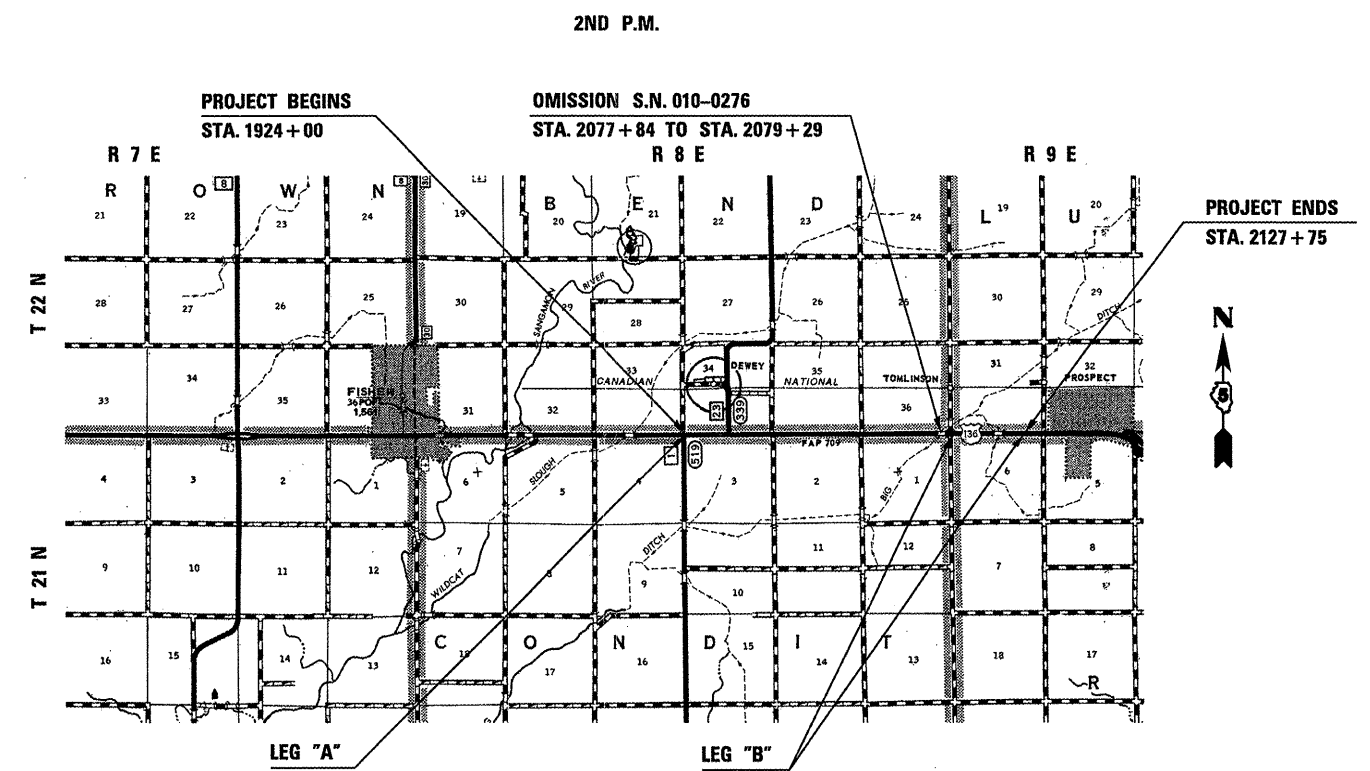


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811
EAST BEND TOWNSHIP
CONDIT TOWNSHIP

PROJECT ENGINEER: NANCY FASIG
DESIGNER: WILLIAM PARKER, JR.

CONTRACT NO. 70261



GROSS LENGTH = 20,375.00 FT. = 3.859 MILE
NET LENGTH = 20,233 FT. = 3.830 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED 2/6 20 09

Joseph E. Houder
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 13, 20 09
Charles J. Imersole
ENGINEER OF DESIGN AND ENVIRONMENT

March 13, 20 09
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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- 7-11 SCHEDULE OF QUANTITIES
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- 12 DETAIL OF PRIVATE AND COMMERCIAL ENTRANCE - BUTT JOINT
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- 14 DETAIL OF PARTIAL DEPTH HOT-MIX ASPHALT PATCHING
- 15-18 TYPICAL APPLICATIONS OF PERMANENT MARKINGS AND MARKERS
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HIGHWAY STANDARDS

- 000001-05 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001006 DECIMAL OF AN INCH AND A FOOT
- 406201-01 MAILBOX TURNOUT
- 442201-03 CLASS C AND D PATCHES
- 701006-03 OFF-ROAD OPERATIONS, 2L, 2W, 15' TO 2' FROM PAVEMENT EDGE
- 701201-03 LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS \geq 45 MPH
- 701301-03 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-02 LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS \geq 45 MPH
- 701311-03 LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
- 701901-01 TRAFFIC CONTROL DEVICES
- 780001-02 TYPICAL PAVEMENT MARKING
- 781001-03 TYPICAL APP. RAISED REFLECTIVE PAVMENT MARKERS

FILE NAME : c:\pwwork\PIERSONBR\dms87996\DE	USER NAME : piersonbr 70261-shr-gennote.dgn	DESIGNED - DRAWN -	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS /HIGHWAY STANDARDS	F.A.P. RTE. 709	SECTION 105RS-2	COUNTY Champaign	TOTAL SHEETS 19	SHEET NO. 2
PLOT SCALE = 40.0000' / IN. PLOT DATE = 2/6/2009		CHECKED - DATE -	REVISED - REVISED -	SCALE: SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 70261 ILLINOIS FED. AID PROJECT				

G.N.-100
ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.

G.N.-406
THE QUANTITIES INCLUDED IN THE PLANS FOR HOT-MIX ASPHALT RESURFACING ARE INTENDED TO GIVE THE COVERAGE SHOWN ON THE TYPICAL CROSS SECTIONS. IT IS NOT INTENDED TO INCREASE THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE IN ORDER TO USE ALL OF THE QUANTITIES INCLUDED IN THE CONTRACT.

G.N. -406H

MIXTURE REQUIREMENTS

The following mixture requirements are applicable for this project:

LOCATION	US 136	US 136	US 136
MIXTURE USE	LEVEL BINDER & PARTIAL DEPTH PATCHES	SURFACE & INCIDENTAL	CLASS D PATCHES
AC/PG	PG 64-22	PG 64-22	PG 64-22
RAP % (MAX)	25	15	25
DESIGN AIR VOIDS	4.0% @ Ndes=50	4.0% @ Ndes=50	4.0% @ Ndes=50
MIX COMP(GRADATION)	IL 9.5	IL 9.5	IL 19.0
FRICTION AGGREGATE	MIX C	MIX C	N.A.

G.N.-406.05B
ALL LEVELING BINDER OR BINDER SHALL BE GIVEN A FOG COAT OF PRIME BEFORE THE SURFACE COURSE IS PLACED WHEN DIRECTED BY THE ENGINEER.

THE FOG COAT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER GALLON FOR BITUMINOUS MATERIAL (PRIME COAT) AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-408B
THE INCIDENTAL HOT-MIX ASPHALT SURFACING SHALL BE COMPACTED AS REQUIRED BY THE SPECIFICATIONS FOR DESIGN NUMBER OF GYRATIONS BEING USED,

AT THE FOLLOWING LOCATIONS:

Dewey/Fisher Road
Dewey/Elliott Road

G.N.-440C
THE MACHINE USED FOR HOT-MIX ASPHALT SURFACE REMOVAL ON THE THROUGH TRAFFIC LANES ON THIS JOB SHALL BE CAPABLE OF REMOVING A LAYER OF BITUMINOUS MATERIAL AT LEAST 12 FT. IN WIDTH AND 1 1/2 INCHES IN DEPTH IN A SINGLE PASS.

G.N.-442B -- PATCHING SCHEDULES
THE PATCHING SCHEDULES INCLUDED IN THE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AT THE TIME OF COMPLETION OF THE PLANS FOR LETTING. VARIATIONS IN LOCATION AND SIZES OF BOTH FULL-DEPTH AND PARTIAL-DEPTH PATCHES MAY OCCUR.

G.N.-482
ALL MATERIAL PLACED AS HOT-MIX ASPHALT SHOULDERS SHALL BE COMPACTED TO 94.0 - 98.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY. THIS REQUIREMENT SHALL APPLY TO IL 9.5L GRADATION SHOULDER MIXES AND OTHER MIXES (BOTTOM LIFT OF SHOULDERS). THIS MAXIMUM DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE OF FOUR TESTS AS IN OTHER QC/QA TESTING. A NUCLEAR GAUGE DENSITY/CORE CORRELATION SHALL BE PERFORMED FOR THE IL 9.5L MIXES AND OTHER MIXES USING STANDARD CORRELATION PROCEDURES.

G.N.-667
THE RESIDENT ENGINEER SHALL CONTACT THE PROGRAM DEVELOPMENT CHIEF OF SURVEYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE FOR INSTRUCTION AS TO SETTING OF TEMPORARY OR PERMANENT TIES FOR CENTERLINE ALIGNMENT CONTROL SURVEY MARKERS (PC'S, PT'S, AND PI'S). PROJECT IMPLEMENTATION PERSONNEL WILL BE RESPONSIBLE FOR SETTING THESE MARKERS.

G.N.-703A
SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).

G.N.-781
RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH STANDARD 781001, AND THE DETAILS SHOWN IN THE PLANS. IF THERE IS ANY DISCREPANCY BETWEEN THE STANDARD AND THE DETAILS IN THE PLANS, THE DETAILS IN THE PLANS SHALL GOVERN. THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS AND THE RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED MIDWAY IN THE 30 FOOT (9 m) SPACE BETWEEN THE DASHED CENTERLINE STRIPES (WHEN APPLICABLE).

G.N.-1004.01
COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS.

FILE NAME *	USER NAME *	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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	PLOT SCALE = 40.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 70261		ILLINOIS FED. AID PROJECT			
	PLOT DATE = 2/6/2009	DATE -	REVISED -			SCALE:	SHEET NO. OF SHEETS	STA. TO STA.			

SUMMARY OF QUANTITIES

LOCATION OF WORK: FAP 709
 US 136
 RURAL TWO LANE
 CHAMPAIGN COUNTY
 STA 1924+00
 TO
 STA. 2127+75

FUNDING BREAKOUT: 100% FEDERAL

CONSTRUCTION TYPE CODE: 1000

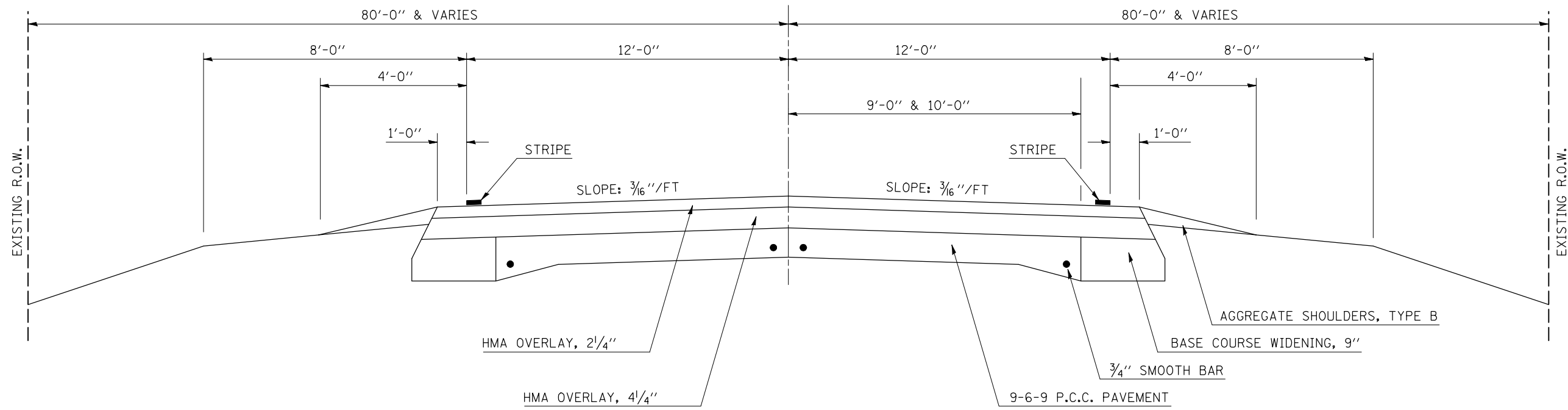
CODE NO	ITEM	UNIT	TOTAL QUANTITY
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL.	11,800.0
40600300	AGGREGATE (PRIME COAT)	TON	120.0
40600625	LEVELING BINDER (MACHINE METHOD), N 50	TON	2,423.0
40600895	CONSTRUCTING TEST STRIP	EACH	1.0
40600982	HMA SURFACE REMOVAL- BUTT JOINT	SQ YD	2,269.0
40600990	TEMPORARY RAMP	SQ YD	72.0
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N 50	TON	4,874.0
40800010	BITUMINOUS MATERIALS (PRIME COAT)	GAL.	312.0
40800030	AGGREGATE (PRIME COAT)	TON	7.0
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	265.0
44000152	HOT-MIX ASPHALT SURFACE REMOVAL 3/4"	SQ YD	57,690.0
44201827	CLASS D PATCHES, TYPE II, 15 INCH	SQ YD	959.0
44201831	CLASS D PATCHES, TYPE III, 15 INCH	SQ YD	138.0
48101200	AGGREGATE SHOULDERS, TYPE B	TON	1,665.0
48203100	HOT-MIX ASPHALT SHOULDERS	TON	164.0
67000500	ENGINEER'S FIELD OFFICE, TYPE B	CAL. MO.	3.0
67100100	MOBILIZATION	L. SUM	1.0
70100450	TRAFFIC CONTROL & PROTECTION STANDARD 701201	L. SUM	1.0
70100460	TRAFFIC CONTROL & PROTECTION STANDARD 701306	L. SUM	1.0
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	6,240.0
70300625	TEMPORARY PAINT PAVEMENT MARKING- LINE 4"	FOOT	44,430.0
70300660	TEMPORARY PAINT PAVEMENT MARKING- LINE 24"	FOOT	60.0
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	733.0
* 78001110	PAINT PAVEMENT MARKING- LINE 4"	FOOT	44,430.0
* 78001180	PAINT PAVEMENT MARKING- LINE 24"	FOOT	60.0
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	255.0
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	255.0
X4401705	PARTIAL DEPTH REMOVAL	SQ YD	304.0
X4421000	PARTIAL DEPTH PATCHING	TON	94.0
XX005938	SOLAR-POWERED FLASHING BEACON ASSEMBLY (COMPLETE)	EACH	4.0

*** SPECIALTY ITEM**

FILE NAME #	USER NAME #	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
es:\pw_work\PIWIDOT\PIERSONBR\dms87996\02	70261-shr-S00.dgn	DRAWN -	REVISED -			709	105RS-2	Champaign	19	4	
	PLOT SCALE : 42.0/23 "/ IN.	CHECKED -	REVISED -			CONTRACT NO. 70261					
	PLOT DATE : 2/6/2009	DATE -	REVISED -			SCALE:	SHEET NO. OF	SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT

EXISTING TYPICAL SECTION ①

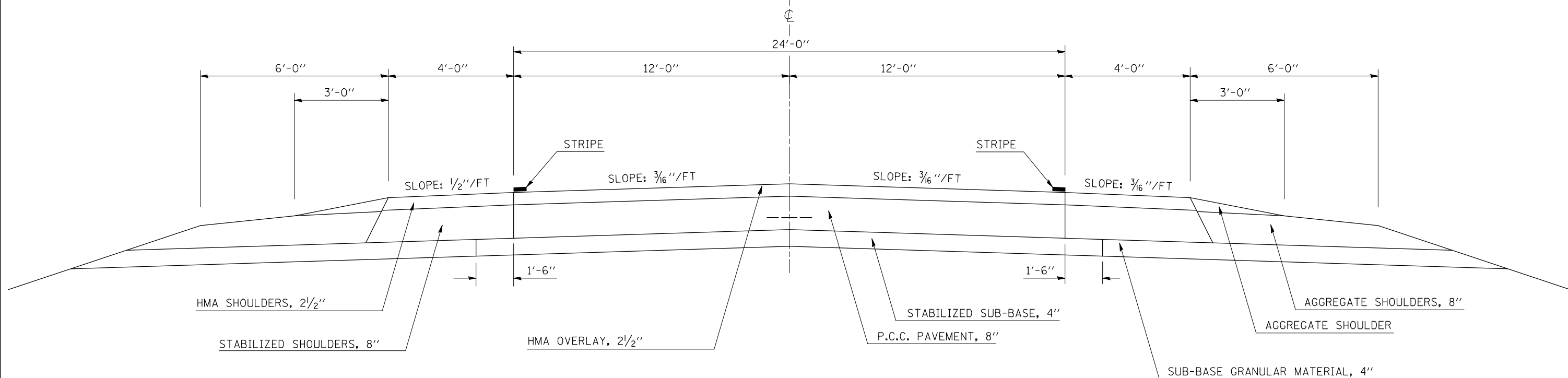
STATION	TO	STATION	
① 1924+00.00		2062+00.00	RT. ②
① 1924+00.00		2061+85.00	LT. ②
① 2082+65.00	⊕	2127+75.00	⊖



EXISTING TYPICAL SECTION ②

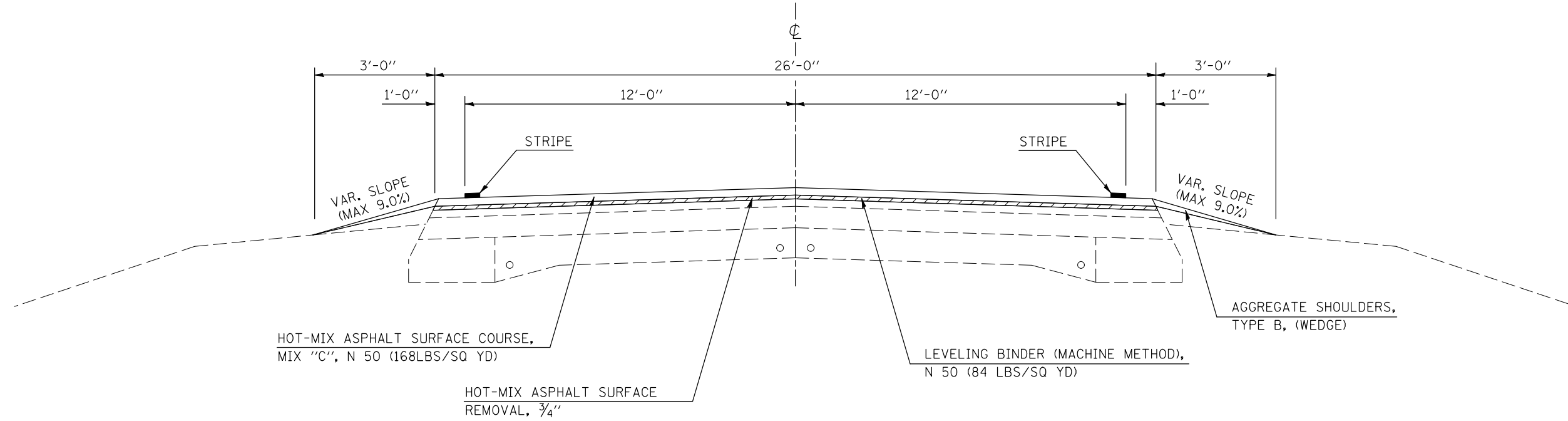
STATION	TO	STATION	
② 2062+00.00	RT.	2082+65.00	①
② 2061+85.00	LT.	2082+65.00	①

BRIDGE OMISSION: STA. 2077+84 TO STA. 2079+29



PROPOSED TYPICAL SECTION ①

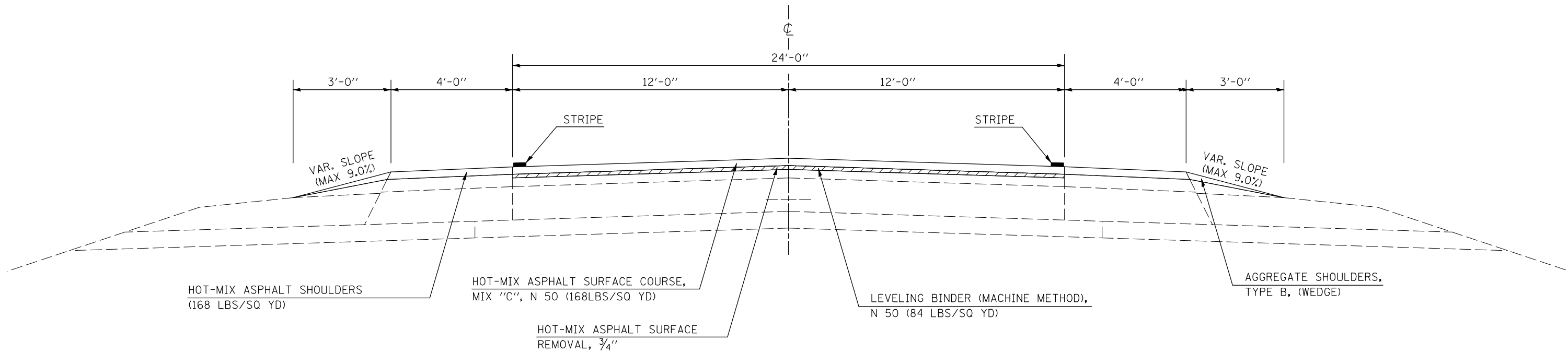
STATION	TO	STATION	
① 1924+00.00		2062+00.00	RT. ②
① 1924+00.00		2061+85.00	LT. ②
① 2082+65.00		2127+75.00	①



PROPOSED TYPICAL SECTION ②

STATION	TO	STATION	
② 2062+00.00	RT.	2082+65.00	①
② 2061+85.00	LT.	2082+65.00	①

BRIDGE OMISSION: STA. 2077+84 TO STA. 2079+29



FILE NAME =	USER NAME = ceorlockjd	DESIGNED - WLP	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PROPOSED TYPICAL SECTION			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
ct:\pw\work\p\dot\ceorlockjd\dms87996\70261\typical1.dgn		DRAWN - JDC	REVISED -		SCALE: N/A	SHEET NO.	OF SHEETS	STA.	TO STA.	709	105RS-2	Champaign	19	6
	PLOT SCALE = 100.0000' / IN.	CHECKED -	REVISED -											
	PLOT DATE = 2/6/2009	DATE - 01/31/09	REVISED -											
										CONTRACT NO. 70261		ILLINOIS FED. AID PROJECT		

40600100 BIT MATERIALS (PRIME COAT)

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ. YD.)	(GAL)
LT	1924+00	LT 2061+85	13785.00	13.00	19911.67	1991.17
RT	1924+00	RT 2062+00	13800.00	13.00	19933.33	1993.33
LT	2082+65	LT 2127+75	4510.00	13.00	6514.44	651.44
RT	2082+65	RT 2127+75	4510.00	13.00	6514.44	651.44
LT	2061+85	LT 2077+87	1602.00	12.00	2136.00	213.60
RT	2062+00	RT 2077+87	1587.00	12.00	2116.00	211.60
BRIDGE OMISSION						
LT	2079+29	LT 2082+65	336.00	12.00	448.00	44.80
RT	2079+29	RT 2082+65	336.00	12.00	448.00	44.80

SUB TOTAL= 5,802.19

SECOND APPLICATION= 5,802.19

HMA SHOULDERS

1951.5 195.15

TOTAL= 11,799.53

USE= 11800.0

40600300 AGGREGATE (PRIME COAT)

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ. YD.)	(TON)
LT	1924+00	LT 2061+85	13785.00	13.00	19911.67	39.82
RT	1924+00	RT 2062+00	13800.00	13.00	19933.33	39.87
LT	2082+65	LT 2127+75	4510.00	13.00	6514.44	13.03
RT	2082+65	RT 2127+75	4510.00	13.00	6514.44	13.03
LT	2061+85	LT 2077+87	1602.00	12.00	2136.00	4.27
RT	2062+00	RT 2077+87	1587.00	12.00	2116.00	4.23
BRIDGE OMISSION						
LT	2079+29	LT 2082+65	336.00	12.00	448.00	0.90
RT	2079+29	RT 2082+65	336.00	12.00	448.00	0.90

HMA SHOULDERS

1951.5 3.90

TOTAL= 119.95

USE= 120.0

40600625 LEVELING BINDER (MACHINE METHOD) N 50

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ. YD.)	(TON)
LT	1924+30	LT 2061+85	13755.00	13.00	19868.33	834.47
RT	1924+30	RT 2062+00	13770.00	13.00	19890.00	835.38
LT	2082+65	LT 2127+45	4480.00	13.00	6471.11	271.79
RT	2082+65	RT 2127+45	4480.00	13.00	6471.11	271.79
LT	2061+85	LT 2077+57	1572.00	12.00	2096.00	88.03
RT	2062+00	RT 2077+57	1557.00	12.00	2076.00	87.19
BRIDGE OMISSION						
LT	2079+59	LT 2082+65	306.00	12.00	408.00	17.14
RT	2079+59	RT 2082+65	306.00	12.00	408.00	17.14

TOTAL= 2422.92

USE= 2423.0

40600990 TEMPORARY RAMP

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ. YD.)
1924+00		1924+05	5.00	26.00	14.44
STR 010-0276	WEST END		5.00	38.00	21.11
STR 010-0276	EAST END		5.00	38.00	21.11
2127+70		2127+75	5.00	26.00	14.44
TOTAL= 71.1					
USE= 72.0					

40603310 HMA SURE CRSE. MIX "C". N 50

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ. YD.)	(TON)
LT	1924+00	LT 2061+85	13785.00	13.00	19911.67	1672.58
RT	1924+00	RT 2062+00	13800.00	13.00	19933.33	1674.40
LT	2082+65	LT 2127+75	4510.00	13.00	6514.44	547.21
RT	2082+65	RT 2127+75	4510.00	13.00	6514.44	547.21
LT	2061+85	LT 2077+87	1602.00	12.00	2136.00	179.42
RT	2062+00	RT 2077+87	1587.00	12.00	2116.00	177.74
BRIDGE OMISSION						
LT	2079+29	LT 2082+65	336.00	12.00	448.00	37.63
RT	2079+29	RT 2082+65	336.00	12.00	448.00	37.63

TOTAL= 4873.84

USE= 4874.0

44000152 HMA SURFACE REMOVAL 3/4"

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ. YD.)
LT	1924+30	LT 2061+85	13755.00	13.00	19868.33
RT	1924+30	RT 2062+00	13770.00	13.00	19890.00
LT	2082+65	LT 2127+45	4480.00	13.00	6471.11
RT	2082+65	RT 2127+45	4480.00	13.00	6471.11
LT	2061+85	LT 2077+57	1572.00	12.00	2096.00
RT	2062+00	RT 2077+57	1557.00	12.00	2076.00
BRIDGE OMISSION					
LT	2079+59	LT 2082+65	306.00	12.00	408.00
RT	2079+59	RT 2082+65	306.00	12.00	408.00

TOTAL= 57,688.56

USE= 57690.0

44201827 CLASS D PATCHES, TYPE II, 15 INCH

STATION	DIRECTION	LENGTH (FT)	WIDTH (FT)	PATCH AREA (SQ YD)
1925+07	EB	5	13	7.2
1925+48	EB	5	13	7.2
1926+01	EB	5	13	7.2
1926+52	EB	5	13	7.2
1927+45	EB	5	13	7.2
1928+10	EB	5	13	7.2
1928+77	EB	5	13	7.2
1929+29	EB	5	13	7.2
1929+76	EB	8	13	11.6
1930+45	EB	5	13	7.2
1932+95	EB	5	13	7.2
1934+29	EB	5	13	7.2
1937+03	EB	5	13	7.2
1937+54	EB	5	13	7.2
1937+66	EB	5	13	7.2
1944+33	EB	8	13	11.6
1951+51	EB	5	13	7.2
1953+73	EB	5	13	7.2
1954+42	EB	5	13	7.2
1955+08	EB	5	13	7.2
1955+75	EB	5	13	7.2
1957+49	EB	10	13	14.4
1959+23	EB	5	13	7.2
1959+51	EB	5	13	7.2
1960+47	EB	5	13	7.2
1961+93	EB	5	13	7.2
1962+57	EB	5	13	7.2
1963+50	EB	5	13	7.2
1965+14	EB	10	13	14.4
1966+10	EB	5	13	7.2
1967+51	EB	8	13	11.6
1970+22	EB	5	13	7.2
1974+38	EB	10	13	14.4
1974+89	EB	5	13	7.2
1977+51	EB	5	13	7.2
1979+50	EB	5	13	7.2
1981+44	EB	12	13	17.3
1982+60	EB	5	13	7.2
1984+78	EB	12	13	17.3
1988+65	EB	8	13	11.6
1992+48	EB	5	13	7.2
1993+69	EB	5	13	7.2

SUB-TOTALS 362.6

44201827 CLASS D PATCHES, TYPE II, 15 INCH

STATION	DIRECTION	LENGTH (FT)	WIDTH (FT)	PATCH AREA (SQ YD)
1993+87	EB	5	13	7.2
1995+49	EB	8	13	11.6
1998+51	EB	5	13	7.2
1998+96	EB	5	13	7.2
1999+55	EB	10	13	14.4
2000+23	EB	5	13	7.2
2001+28	EB	5	13	7.2
2002+01	EB	5	13	7.2
2002+87	EB	5	13	7.2
2003+87	EB	5	13	7.2
2007+62	EB	5	13	7.2
2010+41	EB	5	13	7.2
2013+30	EB	5	13	7.2
2014+45	EB	5	13	7.2
2016+42	EB	5	13	7.2
2019+73	EB	5	13	7.2
2021+41	EB	5	13	7.2
2023+26	EB	5	13	7.2
2023+68	EB	5	13	7.2
2024+46	EB	5	13	7.2
2026+83	EB	5	13	7.2
2027+60	EB	5	13	7.2
2028+61	EB	5	13	7.2
2029+60	EB	5	13	7.2
2032+66	EB	5	13	7.2
2036+37	EB	5	13	7.2
2036+84	EB	5	13	7.2
2038+77	EB	5	13	7.2
2039+11	EB	5	13	7.2
2040+88	EB	5	13	7.2
2043+48	EB	5	13	7.2
2045+05	EB	5	13	7.2
2051+06	EB	8	13	11.6
2051+46	EB	5	13	7.2
2053+46	EB	5	13	7.2
2059+99	EB	5	13	7.2
2061+27	EB	5	13	7.2

SUB-TOTALS 283.1

44201827 CLASS D PATCHES, TYPE II, 15 INCH

STATION	DIRECTION	LENGTH (FT)	WIDTH (FT)	PATCH AREA (SQ YD)
1925+07	WB	5	13	7.2
1925+48	WB	5	13	7.2
1926+01	WB	8	13	11.6
1928+10	WB	5	13	7.2
1928+77	WB	5	13	7.2
1929+29	WB	5	13	7.2
1930+45	WB	5	13	7.2
1931+69	WB	5	13	7.2
1934+29	WB	5	13	7.2
1937+03	WB	5	13	7.2
1937+54	WB	5	13	7.2
1939+91	WB	5	13	7.2
1944+33	WB	8	13	11.6
1953+73	WB	5	13	7.2
1954+21	WB	10	13	14.4
1955+08	WB	5	13	7.2
1955+75	WB	5	13	7.2
1957+49	WB	5	13	7.2
1960+47	WB	5	13	7.2
1961+93	WB	5	13	7.2
1962+57	WB	5	13	7.2
1963+50	WB	5	13	7.2
1965+14	WB	5	13	7.2
1970+30	WB	5	13	7.2
1974+38	WB	5	13	7.2
1974+89	WB	5	13	7.2
1977+51	WB	5	13	7.2
1979+50	WB	5	13	7.2
1981+44	WB	5	13	7.2
1982+60	WB	5	13	7.2
1988+65	WB	8	13	11.6
1992+48	WB	5	13	7.2
1995+49	WB	5	13	7.2
1998+51	WB	5	13	7.2
1999+55	WB	5	13	7.2
2002+01	WB	5	13	7.2
2002+87	WB	5	13	7.2
2003+87	WB	5	13	7.2
2007+62	WB	5	13	7.2
2013+84	WB	5	13	7.2
2016+67	WB	5	13	7.2
2017+17	WB	8	13	11.6

SUB-TOTALS 327.9

44201827 CLASS D PATCHES, TYPE II, 15 INCH

STATION	DIRECTION	LENGTH (FT)	WIDTH (FT)	PATCH AREA (SQ YD)
2019+73	WB	5	13	7.2
2023+68	WB	5	13	7.2
2024+46	WB	5	13	7.2
2026+83	WB	5	13	7.2
2027+60	WB	5	13	7.2
2029+19	WB	5	13	7.2
2029+60	WB	5	13	7.2
2032+53	WB	5	13	7.2
2045+05	WB	5	13	7.2
2045+55	WB	10	13	14.4
2047+08	WB	5	13	7.2
2051+06	WB	8	13	11.6
2051+46	WB	5	13	7.2
2053+46	WB	5	13	7.2
2061+27	WB	5	13	7.2
2098+98	WB	8	12	10.7
2101+23	WB	5	12	6.7
2107+08	WB	5	12	6.7
2120+35	WB	5	12	6.7
2120+87	WB	5	12	6.7
2122+78	WB	5	12	6.7
2124+87	WB	5	12	6.7

SUB-TOTALS 170.6

FINAL-TOTALS 958.2

USE 959.0

44201831 CLASS D PATCHES, TYPE III, 15 INCH

STATION	DIRECTION	LENGTH (FT)	WIDTH (FT)	PATCH AREA (SQ YD)
1981+44	EB	12	13	17.3
1984+78	EB	12	13	17.3
1997+19	EB	12	13	17.3
2044+50	WB	17	13	24.6
2054+67	WB	17	13	24.6
2070+73	WB	15	12	20.0
2093+58	WB	12	12	16.0

TOTAL 137.1

USE 138.0

48101200 AGGREGATE SHOULDERS, TYPE B

STATION	TO	STATION	DESCRIPTION	LENGTH (FT)	WIDTH (FT)	AVERAGE THICKNESS	(TON)
1924+00		2127+75	WESTBOUND	20,375.00	3.00	1.9"	
			STR 010-0276	OMISSION	-142.00	3.00	
1924+00		2127+75	EASTBOUND	20,375.00	3.00	1.4"	
			STR 010-0276	OMISSION	-142.00	3.00	
TOTAL=							1,663.7
USE=							1,665.0

CONVERSION FACTOR: 1.85 TON/ CY

48203100 HMA SHOULDERS

STATION	TO	STATION	LENGTH (FT)	WIDTH (FT)	AREA (SQ. YD.)	(TON)
LT 2061+85		LT 2074+65	1280.00	4.00	568.89	47.79
RT 2062+00		RT 2074+51	1251.00	4.00	556.00	46.70
LT 2074+65		LT 2077+93	328.00	5.00	182.22	15.31
RT 2074+51		RT 2077+79	328.00	7.00	255.11	21.43
LT 2079+35		LT 2082+65	330.00	5.14	188.43	15.83
RT 2079+22		RT 2082+64	342.00	5.29	200.87	16.87
TOTAL=						163.93
USE=						164.0

70300100 SHORT TERM PAVEMENT MARKING

STATION	TO	STATION	APPLY AFTER:	QUANTITY (FT)	COLOR	DESCRIPTION
1924+00		2127+75	MILLING	1,840.0	YELLOW	SKIP DASH
1924+00		2127+75	LEVEL BINDER	1,840.0	YELLOW	SKIP DASH
1924+00		2127+75	HMA SURFACE	1,840.0	YELLOW	SKIP DASH
US 136/ DEWEY FISHER RD. INTERSECTION			MILLING	180.0	WHITE	STOP BAR
US 136/ DEWEY FISHER RD. INTERSECTION			HMA SURFACE	180.0	WHITE	STOP BAR
US 136/ DEWEY ELLIOT RD. INTERSECTION			MILLING	180.0	WHITE	STOP BAR
US 136/ DEWEY ELLIOT RD. INTERSECTION			HMA SURFACE	180.0	WHITE	STOP BAR
TOTAL=				6,240.0	FEET	

70300625 TEMP. PAINT PAVEMENT MARKING- LINE 4"

STATION	TO	STATION	LOCATION	QUANTITY (FT)	COLOR
1924+00		2127+75	EDGE LINE	39,372.0	WHITE
1924+00		2127+75	SKIP-DASH	5,058.0	YELLOW
TOTAL=				44,430.0	FEET

70300660 TEMP PAINT PAVEMENT MARKING- LINE 24"

STATION	LOCATION	QUANTITY (FT)	COLOR
US 136/ DEWEY FISHER RD. INTERSECTION	STOP BAR	30.0	WHITE
US 136/ DEWEY ELLIOT RD. INTERSECTION	STOP BAR	30.0	WHITE
TOTAL=		60.0	FEET

70301000 WORK ZONE PAVEMENT MARKING REMOVAL

STATION	TO	STATION	QUANTITY (SQ. FT)	DESCRIPTION
1924+00		2127+75	613.0	SKIP DASH
US 136/ DEWEY FISHER RD. INTERSECTION			60.0	STOP BAR
US 136/ DEWEY ELLIOT RD. INTERSECTION			60.0	STOP BAR
TOTAL=			733.0	SQ. FEET

78001110 PAINT PAVEMENT MARKING- LINE 4"

STATION	TO	STATION	LOCATION	QUANTITY (FT)	COLOR
1924+00		2127+75	EDGE LINE	39,372.0	WHITE
1924+00		2127+75	SKIP-DASH	5,058.0	YELLOW
TOTAL=				44,430.0	FEET

78001180 PAINT PAVEMENT MARKING- LINE 24"

STATION	LOCATION	QUANTITY (FT)	COLOR
US 136/ DEWEY FISHER RD. INTERSECTION	STOP BAR	30.0	WHITE
US 136/ DEWEY ELLIOT RD. INTERSECTION	STOP BAR	30.0	WHITE
TOTAL=		60.0	FEET

78100100 RAISED REFLECTIVE PAVEMENT MARKER

STATION	TO	STATION	LOCATION	QUANTITY (EACH)	TYPE
1924+00		2127+75	CENTERLINE	255.0	TWO-WAY AMBER
TOTAL=				255.0	EACH

78300200 RAISED REFLECTIVE PAVEMENT MARKER REMOVAL

STATION	TO	STATION	LOCATION	QUANTITY (EACH)
1924+00		2127+75	CENTERLINE	255.0
TOTAL=				255.0

TABULATION OF PARTIAL DEPTH PAVEMENT PATCHING

STATION	DIRECTION	LENGTH (FT)	WIDTH (FT)	EXISTING OVERLAY THICKNESS (INCH)	PARTIAL DEPTH REMOVAL X4401705 (SQ YD)	PARTIAL DEPTH PATCHING X4421000 (TON)
2062+10	EB	5	12	2.5	6.7	2.1
2062+66	EB	5	12	2.5	6.7	2.1
2063+18	EB	5	12	2.5	6.7	2.1
2063+56	EB	5	12	2.5	6.7	2.1
2064+19	EB	5	12	2.5	6.7	2.1
2065+19	EB	8	12	2.5	10.7	3.3
2066+19	EB	5	12	2.5	6.7	2.1
2067+22	EB	5	12	2.5	6.7	2.1
2068+22	EB	5	12	2.5	6.7	2.1
2069+73	EB	5	12	2.5	6.7	2.1
2070+73	EB	15	12	2.5	20.0	6.2
2072+74	EB	5	12	2.5	6.7	2.1
2073+73	EB	5	12	2.5	6.7	2.1
2080+39	EB	5	12	2.5	6.7	2.1
2083+16	EB	5	12	2.5	6.7	2.1
2087+72	EB	5	12	2.5	6.7	2.1
2091+08	EB	5	12	2.5	6.7	2.1
2091+85	EB	5	12	2.5	6.7	2.1
2093+58	EB	5	12	2.5	6.7	2.1
2101+23	EB	5	12	2.5	6.7	2.1
2101+62	EB	5	12	2.5	6.7	2.1
2105+83	EB	5	12	2.5	6.7	2.1
2107+94	EB	5	12	2.5	6.7	2.1
2108+88	EB	5	12	2.5	6.7	2.1
2117+60	EB	5	12	2.5	6.7	2.1
2120+35	EB	5	12	2.5	6.7	2.1
2120+87	EB	5	12	2.5	6.7	2.1
2122+78	EB	5	12	2.5	6.7	2.1
2124+87	EB	5	12	2.5	6.7	2.1
2062+10	WB	5	12	2.5	6.7	2.1
2062+66	WB	5	12	2.5	6.7	2.1
2064+19	WB	5	12	2.5	6.7	2.1
2065+19	WB	5	12	2.5	6.7	2.1
2066+19	WB	5	12	2.5	6.7	2.1
2067+22	WB	5	12	2.5	6.7	2.1
2068+22	WB	5	12	2.5	6.7	2.1
2069+73	WB	5	12	2.5	6.7	2.1
2070+73	WB	15	12	2.5	20.0	6.2
2072+74	WB	5	12	2.5	6.7	2.1
2073+73	WB	5	12	2.5	6.7	2.1
2080+39	WB	5	12	2.5	6.7	2.1

TOTAL 304.0 93.6

USE 304.0 94.0

XX005938 SOLAR-POWERED FLASHING BEACON ASSEMBLY (COMPLETE)

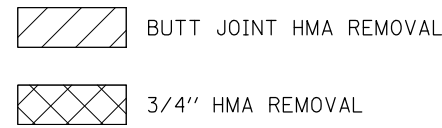
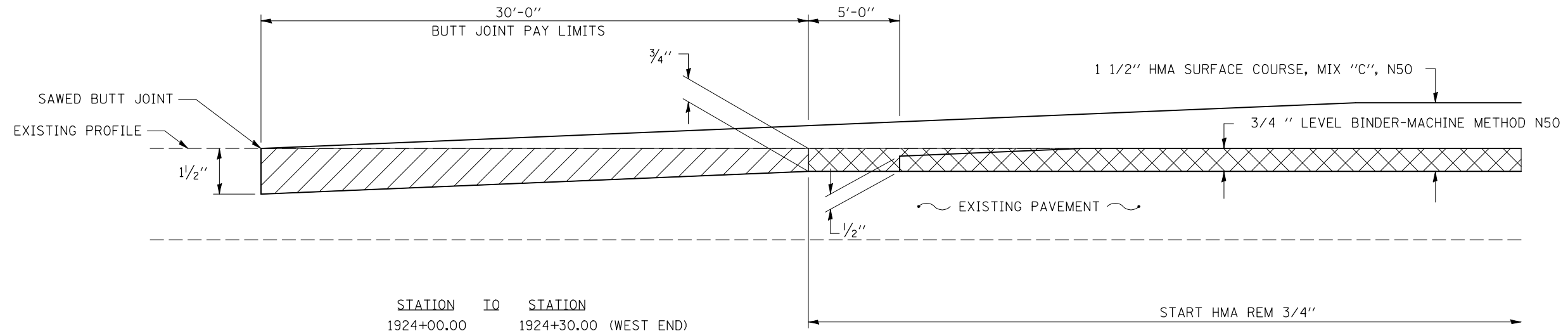
STATION	LOCATION	QUANTITY (FT)
US 136/ DEWEY FISHER RD. INTERSECTION	1 RED @ NB STOP SIGN IN ISLAND	1.0
US 136/ DEWEY FISHER RD. INTERSECTION	1 RED @ SB STOP SIGN	1.0
RT 1917+75	1 YELLOW @ EB INTERSECTION ADVANCE WARNING SIGN	1.0
LT 1933+12	1 YELLOW @ WB INTERSECTION ADVANCE WARNING SIGN	1.0
TOTAL=		4.0 EACH

INCIDENTAL HMA SURFACING

40800050-INCIDENTAL HMA SURFACING											
STATION	AREA	AV.THICK.	Commercial ENT.	FIELD ENT.	PRIVATE ENT. W/ MBTO	PRIVATE ENT.	SIDE ROADS	40600982	40800010	40800030	
								HMA SURF REMOVAL BUTT JOINT	BIT.MTLS (PR CT) RATE=0.1gal	AGG.(PR CT) RATE=4lb	
	SQ YD	INCHES	TON	TON	TON	TON	TON	SQ YD	GAL	TON	
LT 1925+53.5	202.89	1.50					17.04	227.10	20.29	0.41	
RT 1925+69.87	567.42	1.50					47.86	195.00	56.74	1.13	
LT 1932+25.4	13.00	1.50		1.09				1.30	1.30	0.03	
LT 1938+65	13.67	1.50		1.15				1.37	1.37	0.03	
RT 1938+92	13.50	1.50		1.13				1.35	1.35	0.03	
RT 1940+04.3	55.44	1.50			4.66			5.54	5.54	0.11	
RT 1941+55.8	35.28	1.50				2.96		3.53	3.53	0.07	
RT 1942+68	33.33	1.50			2.80			3.33	3.33	0.07	
RT 1942+68	11.67	5.00			3.27			1.17	1.17	0.02	
LT 1945+76	10.67	1.50		0.90				1.07	1.07	0.02	
LT 1947+62	55.12	1.50			4.63			5.51	5.51	0.11	
RT 1950+87.6	13.00	1.50		1.09				1.30	1.30	0.03	
LT 1952+18.15	404.44	1.50					33.97	404.40	40.44	0.81	
LT 1958+58.2	14.17	1.50		1.19				1.42	1.42	0.03	
RT 1960+76.8	14.17	1.50		1.19				1.42	1.42	0.03	
LT 1961+02	66.56	1.50			5.59			6.66	6.66	0.13	
LT 1965+34	12.67	1.50		1.06				1.27	1.27	0.03	
LT 1978+33.2	129.44	1.50					10.87	157.10	12.94	0.26	
RT 1978+64.5	126.44	1.50					10.62	158.70	12.64	0.25	
RT 1981+53	12.83	1.50		1.08				1.28	1.28	0.03	
RT 1983+58.1	53.89	1.50	4.53					5.39	5.39	0.11	
RT 1984+80	62.78	1.50	5.27					6.28	6.28	0.13	
RT 1986+60	71.75	1.50	6.03					7.18	7.18	0.14	
RT 1991+30.3	12.50	1.50		1.05				1.25	1.25	0.03	
LT 1997+61.7	35.07	1.50				2.95		3.51	3.51	0.07	
RT 2004+78	13.83	1.50		1.16				1.38	1.38	0.03	
LT 2004+84	13.17	1.50		1.11				1.32	1.32	0.03	
RT 2017+71.3	18.33	1.50		1.54				1.83	1.83	0.04	
LT 2017+74.1	14.00	1.50		1.18				1.40	1.40	0.03	
RT 2024+00	13.17	1.50		1.11				1.32	1.32	0.03	
LT 2024+36	12.33	1.50		1.04				1.23	1.23	0.02	
RT 2024+56	40.56	1.50	3.41					4.06	4.06	0.08	
RT 2026+52	53.89	1.50			4.53			5.39	5.39	0.11	
LT 2029+27.8	32.00	1.50			2.69			3.20	3.20	0.06	
RT 2030+85	158.67	1.50					13.33	174.20	15.87	0.32	
LT 2031+34.95	118.00	1.50					9.91	146.20	11.80	0.24	
RT 2044+46	13.50	1.50		1.13				1.35	1.35	0.03	
LT 2044+70	14.00	1.50		1.18				1.40	1.40	0.03	
RT 2049+61.8	38.89	1.50				3.27		3.89	3.89	0.08	
LT 2049+63.7	13.83	1.50		1.16				1.38	1.38	0.03	
RT 2051+08	9.50	1.50		0.80				0.95	0.95	0.02	
LT 2057+86.4	12.22	1.50		1.03				1.22	1.22	0.02	
RT 2065+29.1	13.83	1.50		1.16				1.38	1.38	0.03	
RT 2069+33.8	21.33	1.50			1.79			2.13	2.13	0.04	
LT 2071+24.8	18.89	1.50		1.59				1.89	1.89	0.04	
RT 2071+34	27.22	1.50		2.29				2.72	2.72	0.05	
LT 2084+09	9.00	1.50		0.76				0.90	0.90	0.02	
RT 2084+09	12.33	1.50		1.04				1.23	1.23	0.02	
RT 2084+49.87	96.67	1.50					8.12	108.90	9.67	0.19	
LT 2084+52.8	136.33	1.50					11.45	154.00	13.63	0.27	
RT 2088+54.5	13.17	1.50		1.11				1.32	1.32	0.03	
LT 2099+73.4	13.33	1.50		1.12				1.33	1.33	0.03	
RT 2104+57.7	11.83	1.50		0.99				1.18	1.18	0.02	
RT 2106+29.8	13.33	1.50		1.12				1.33	1.33	0.03	
COLUMN 1 TOTALS				19.23	34.52	20.95	18.18	162.99	1784.30	301.89	6.04

40800050-INCIDENTAL HMA SURFACING											
STATION	AREA	AV.THICK.	MAILBOX TURNOUTS	FIELD ENT.	PRIVATE ENT. W/ MBTO	PRIVATE ENT.	SIDE ROADS	40600982	40800010	40800030	
								HMA SURF REMOVAL BUTT JOINT	BIT.MTLS (PR CT) RATE=0.1gal	AGG.(PR CT) RATE=4lb	
LT/RT	SQ YD	INCHES	TON	TON	TON	TON	TON	SQ YD	GAL	TON	
LT 2112+76	11.00	1.50		0.92						1.10	0.02
RT 2118+53	16.83	1.50		1.41						1.68	0.03
LT 2124+66	57.63	1.50			4.84			57.60	5.76	0.12	
RT 2126+58	12.83	1.50		1.08					1.28	0.03	
									0.00	0.00	
COLUMN TOTALS	0.00	3.42	4.84	0.00	0.00	0.00	0.00	57.60	9.83	0.20	
COLUMN 1 TOTALS	19.23	34.52	20.95	18.18	162.99	1784.30	301.89	6.04			
COLUMN 1 & 2 TOTALS	19.23	37.94	25.79	18.18	162.99	1841.90	311.71	6.23			
GRAND TOTALS		265.00	Tons HMA			1841.9	SqYd	312.0	Gallon	7.0	Tons

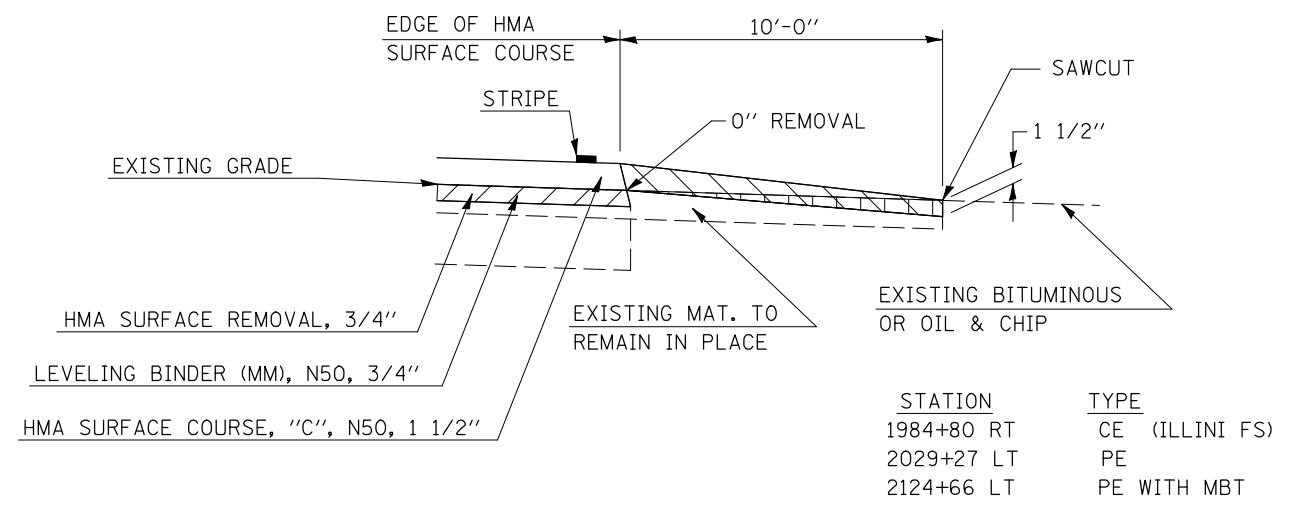
DETAIL OF HOT-MIX ASPHALT SURFACE REMOVAL – BUTT JOINT



STATION	TO	STATION
1924+00.00		1924+30.00 (WEST END)
* 2077+57.00		2077+87.00 (C S.N. 010-0276)
* 2079+29.00		2079+59.00 (C S.N. 010-0276)
2127+45.00		2127+75.00 (EAST END)

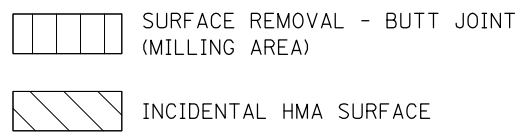
* APPROACH SKEWED @ STRUCTURE STATIONS ARE @ C

TYPICAL PE /CE ENTRANCE HMA SURFACE REMOVAL-BUTT JOINT (HMA OR OIL & CHIP)

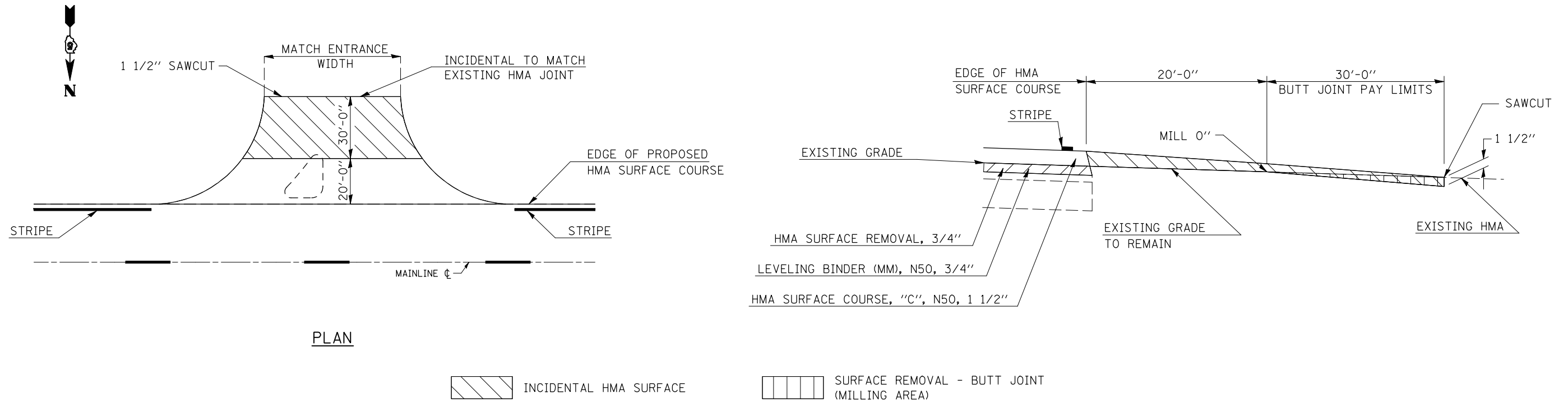


STATION	TYPE
1984+80 RT	CE (ILLINI FS)
2029+27 LT	PE
2124+66 LT	PE WITH MBT

NOTES: THE INCIDENTAL HMA SURFACING IS NOT INTENDED TO RESURFACE THE EXISTING ENTRANCES IN THEIR ENTIRETY.

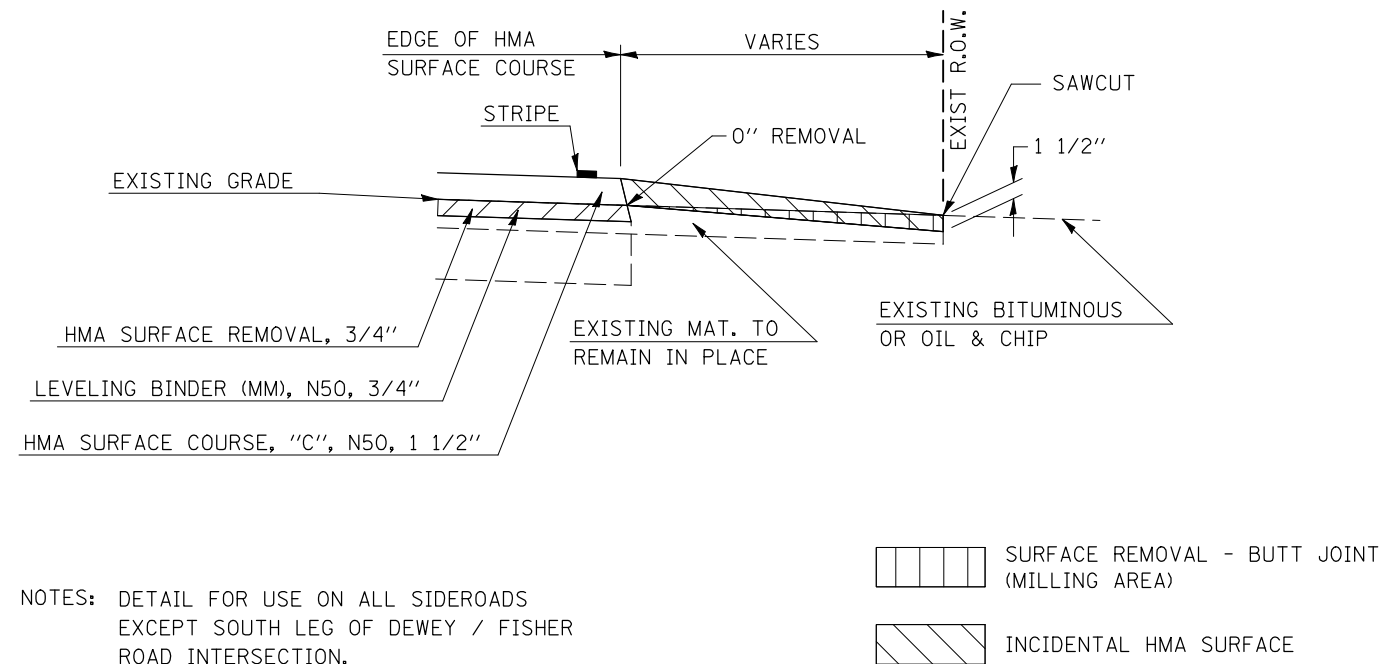


DETAIL FOR MILLING AND RESURFACING @ DEWEY / FISHER ROAD STA. 1925+69 RT.

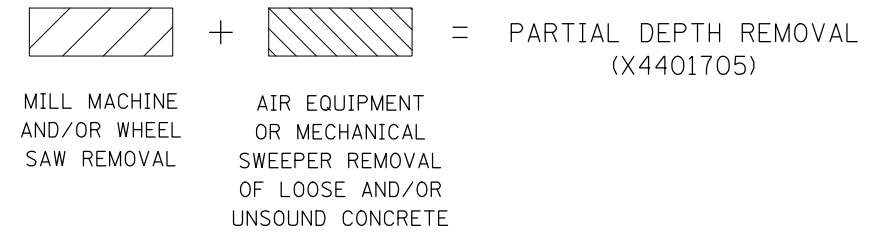
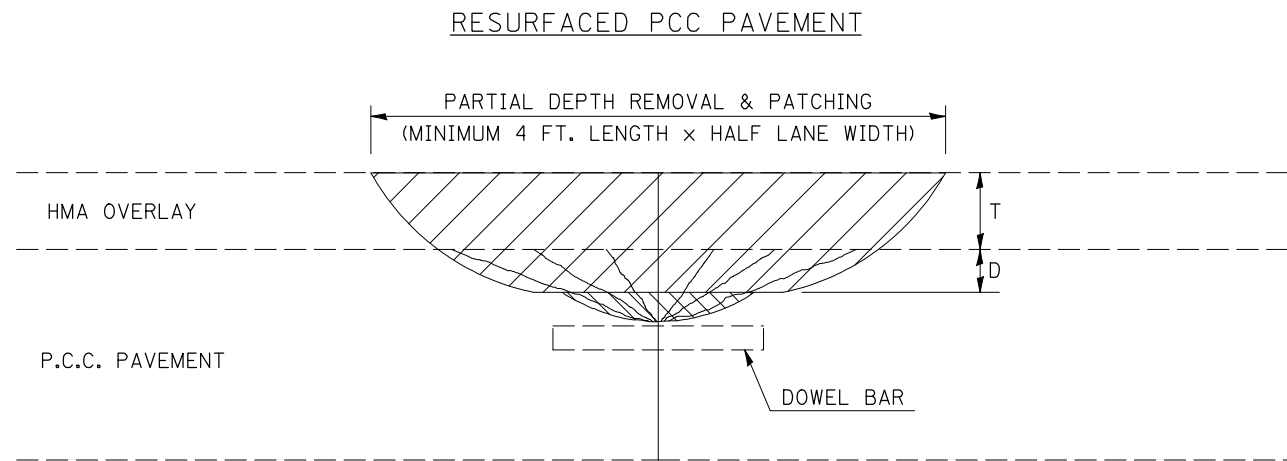


TYPICAL SIDEROAD HMA SURFACE REMOVAL-BUTT JOINT

(HMA OR OIL & CHIP)



FILE NAME =	USER NAME = ceorlockjd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	ct:\pw\work\pwwork\ceorlockjd\dms87996\0670261-sht-details.dgn	DRAWN -	REVISED -									
	PLOT SCALE = 40.0000' / IN.	CHECKED -	REVISED -									
	PLOT DATE = 2/6/2009	DATE -	REVISED -			SCALE:	SHEET NO.	OF SHEETS	STA.	TO STA.		
											CONTRACT NO. 70261	ILLINOIS FED. AID PROJECT



DESIGN NOTES

PARTIAL DEPTH HMA PATCHING SHALL NOT BE USED WHEN HMA OVERLAY THICKNESS (T) ON PCC PAVEMENT EXCEEDS 4 3/4 INCHES (145 mm) OR ON CRC PAVEMENT.

GENERAL NOTES

ALL VOLUME OF PARTIAL DEPTH REMOVAL SHALL BE REPLACED WITH HOT-MIX ASPHALT (HMA) AND PAID FOR AT THE CONTRACT UNIT PRICE PER TON FOR PARTIAL DEPTH PATCHING.

T = THICKNESS OF HMA OVERLAY(S). SEE EXISTING TYPICAL CROSS SECTION.

GENERAL NOTE 406H SHALL INCLUDE MIXTURE REQUIREMENTS FOR PARTIAL DEPTH PATCHING.

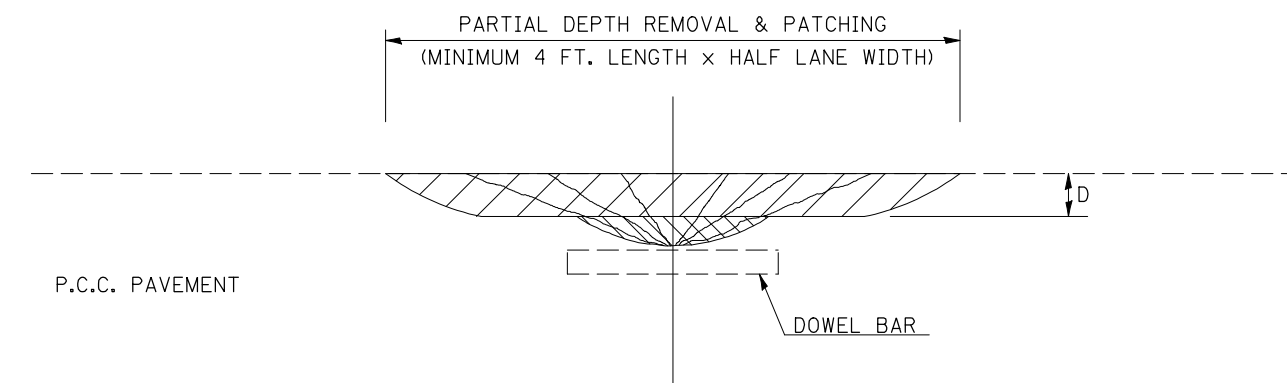
HMA REPLACEMENT IN RESURFACED PCC PAVEMENT LOCATIONS SHALL BE ACCORDING TO SECTION 406 OF THE STANDARD SPECIFICATIONS.

D = DEPTH OF PARTIAL DEPTH REMOVAL INTO EXISTING PCC PAVEMENT. (3" (75 mm) ± OR TO SOUND CONCRETE)

TRANSVERSE CONTRACTION JOINT SHOWN - OTHER LOCATIONS SIMILAR.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

BARE PCC PAVEMENT



DISTRICT 5 DETAIL NO. XX442100

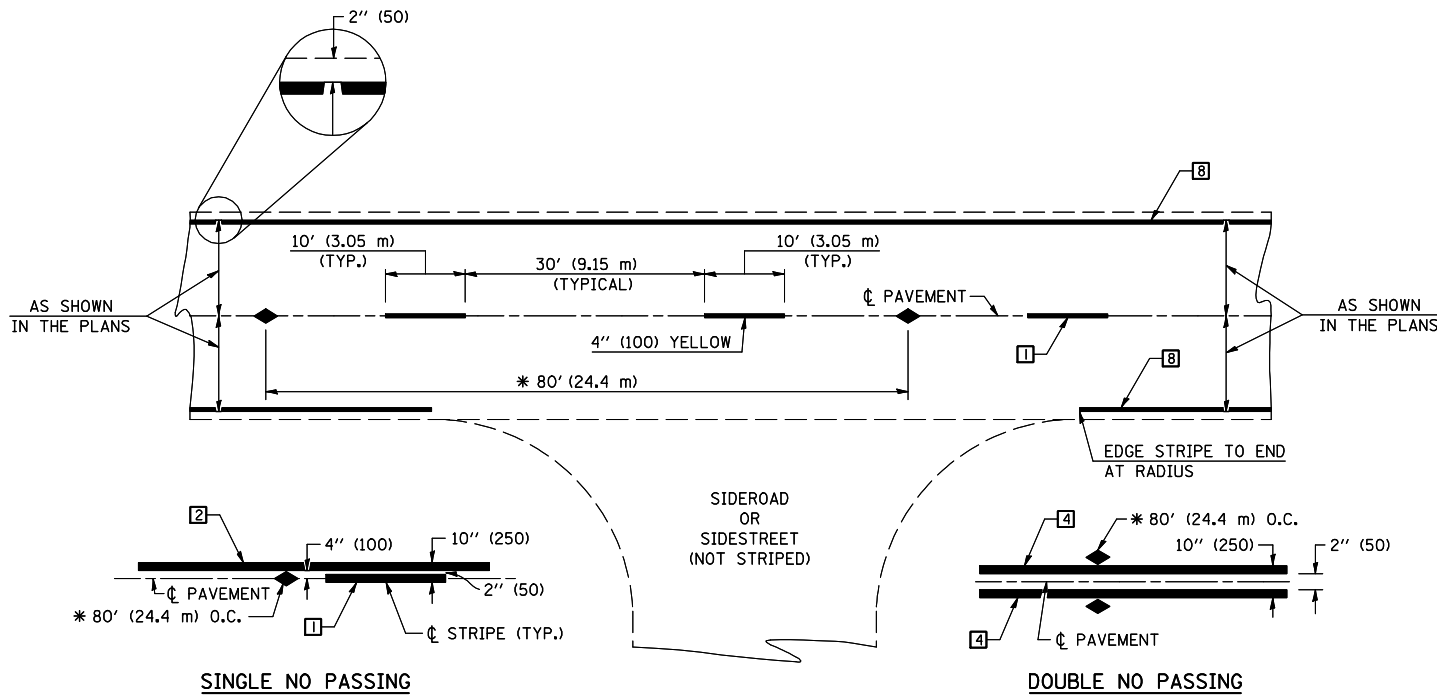
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	PLOT SCALE = 40.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/6/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PCC PARTIAL DEPTH HOT-MIX ASPHALT PATCHING

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
709	105RS-2	Champaign	19	14
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 70261	



* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

TWO LANE/TWO WAY

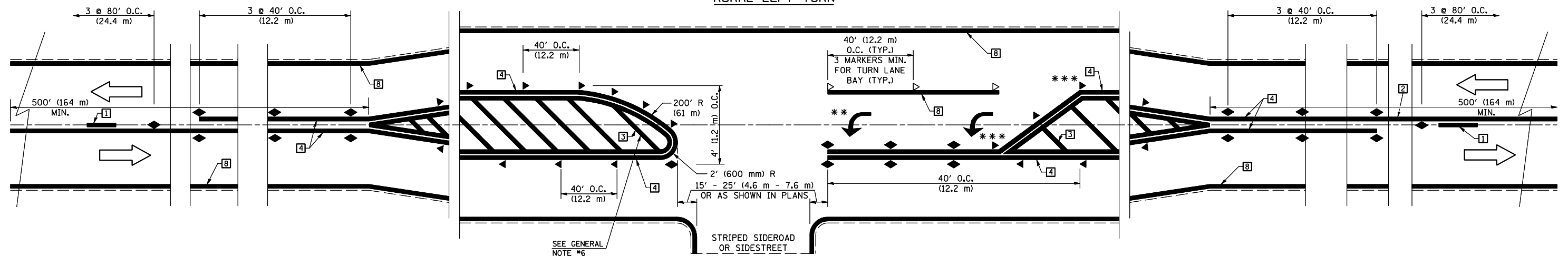
TYPICAL PAVEMENT MARKING LEGEND

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 4" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

TYPICAL PAVEMENT MARKERS LEGEND

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

RURAL LEFT TURN



*** REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

** TURN ARROWS SHALL BE PLACED AS SHOWN ON SHEET #2.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 5 DETAIL NO. 7800AAA

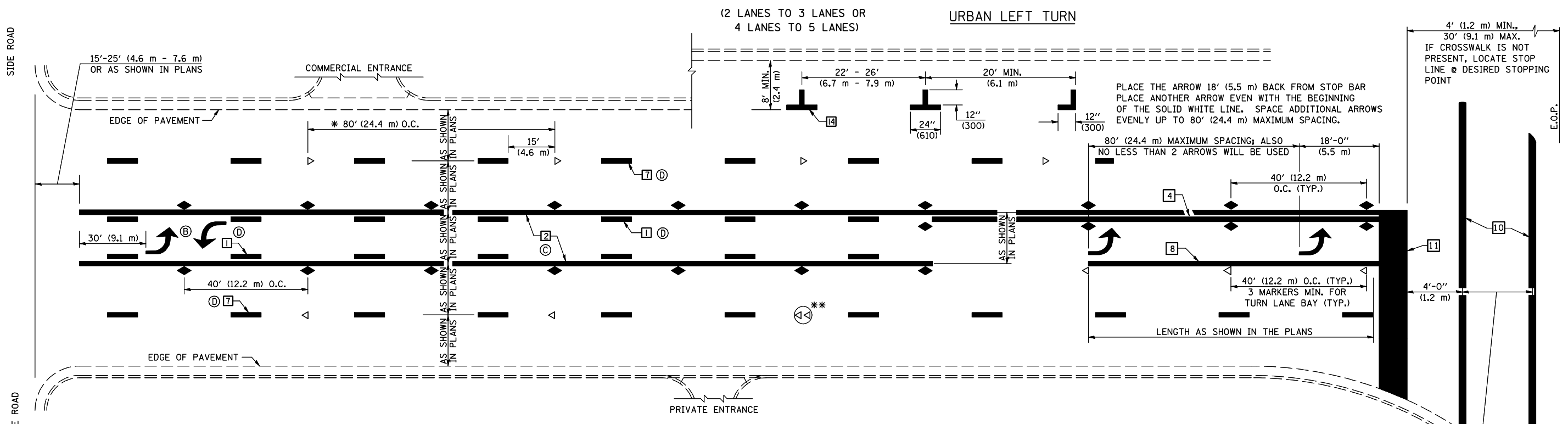
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PLOT SCALE = 40.0000' / IN.		CHECKED -	REVISED -
PLOT DATE = 2/6/2009		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS
(RURAL & URBAN APPLICATIONS)**

SCALE: SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
709	105RS-2	Champaign	19	15
				CONTRACT NO. 70261
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

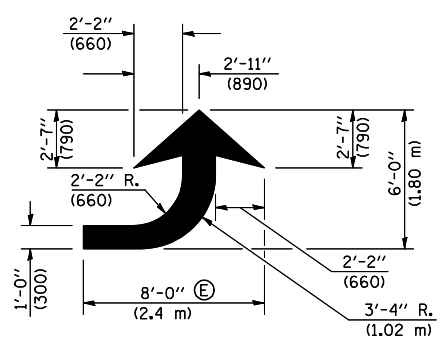


* REDUCE TO 40 FEET (12.2 METERS) ON CENTER ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

** DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

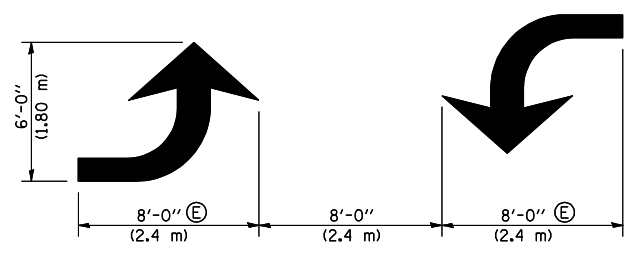
GENERAL NOTES:

- ⓑ TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE.
- ⓒ THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
- ⓓ THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER. SEE EXAMPLE ON SHEET 2 OF 3.
- ⓔ USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)



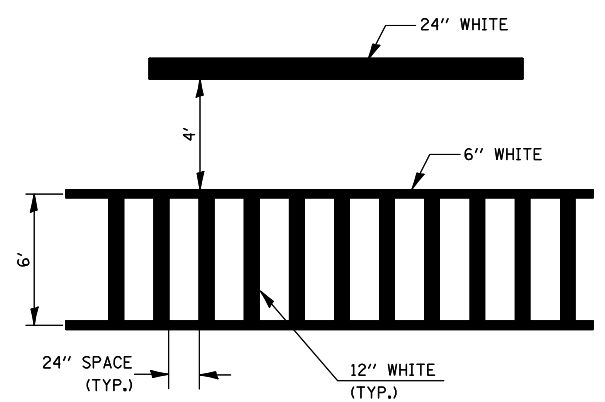
LEFT ARROW

REVERSE FOR RIGHT ARROW
AREA = 15.6 SQ. FT. (1.47 m²)
(WHITE)

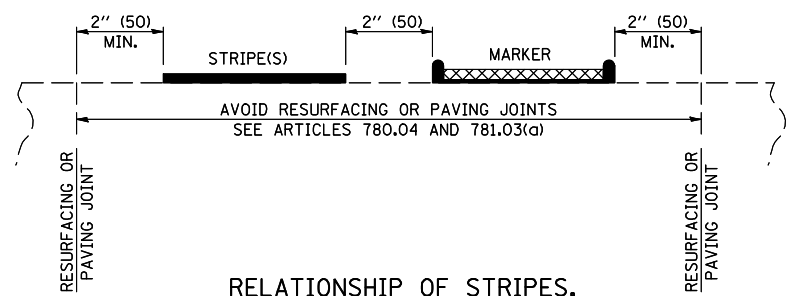


TYPICAL DOUBLE TURN ARROWS (WHITE)

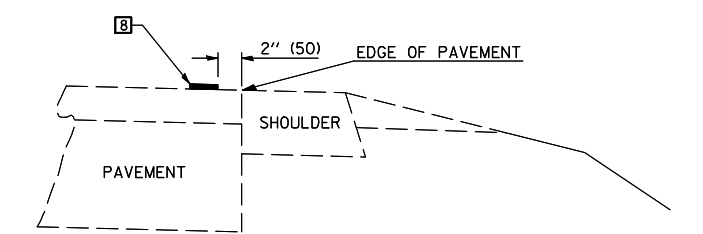
BLOOMINGTON-NORMAL CITY LIMITS ONLY



TYPICAL SPACING FOR CROSSWALKS & STOP BARS



RELATIONSHIP OF STRIPES, MARKERS AND JOINTS



RELATIONSHIP OF EDGE LINE TO EDGE OF PAVEMENT
(SAFETY SHOULDER OR PAVED SURFACE)
SEE ARTICLE 780.04

CROSSWALK WIDTH 6'-0" (1.8 m)
OR AS SHOWN IN THE PLANS

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

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	PLOT DATE = 2/6/2009	DATE -	REVISED -

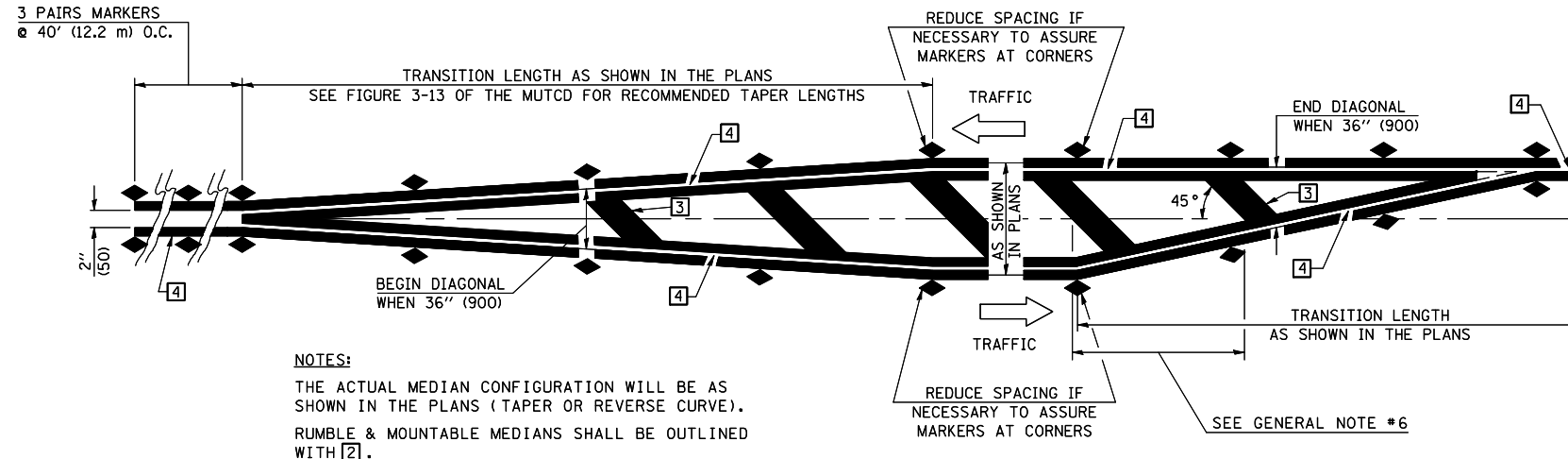
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS
(RURAL & URBAN APPLICATIONS)**

SCALE: SHEET NO. 2 OF 4 SHEETS STA. TO STA.

DISTRICT 5 DETAIL NO. 7800AAA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
709	105RS-2	Champaign	19	16
				CONTRACT NO. 70261
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		

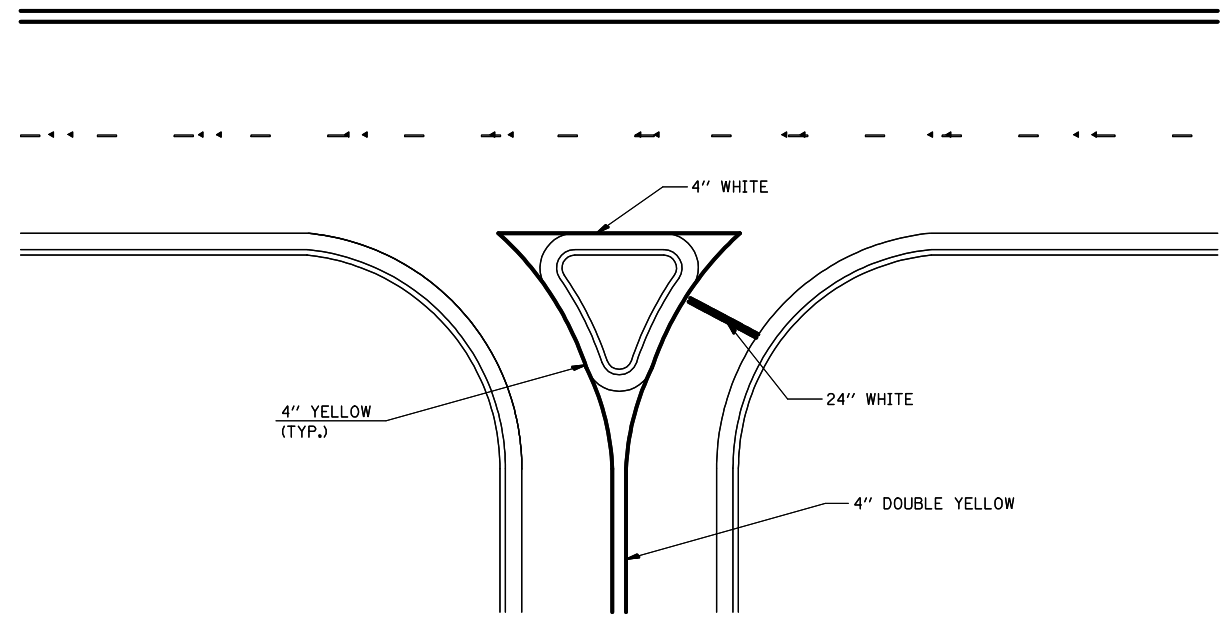


NOTES:
 THE ACTUAL MEDIAN CONFIGURATION WILL BE AS SHOWN IN THE PLANS (TAPER OR REVERSE CURVE).
 RUMBLE & MOUNTABLE MEDIANS SHALL BE OUTLINED WITH [2].

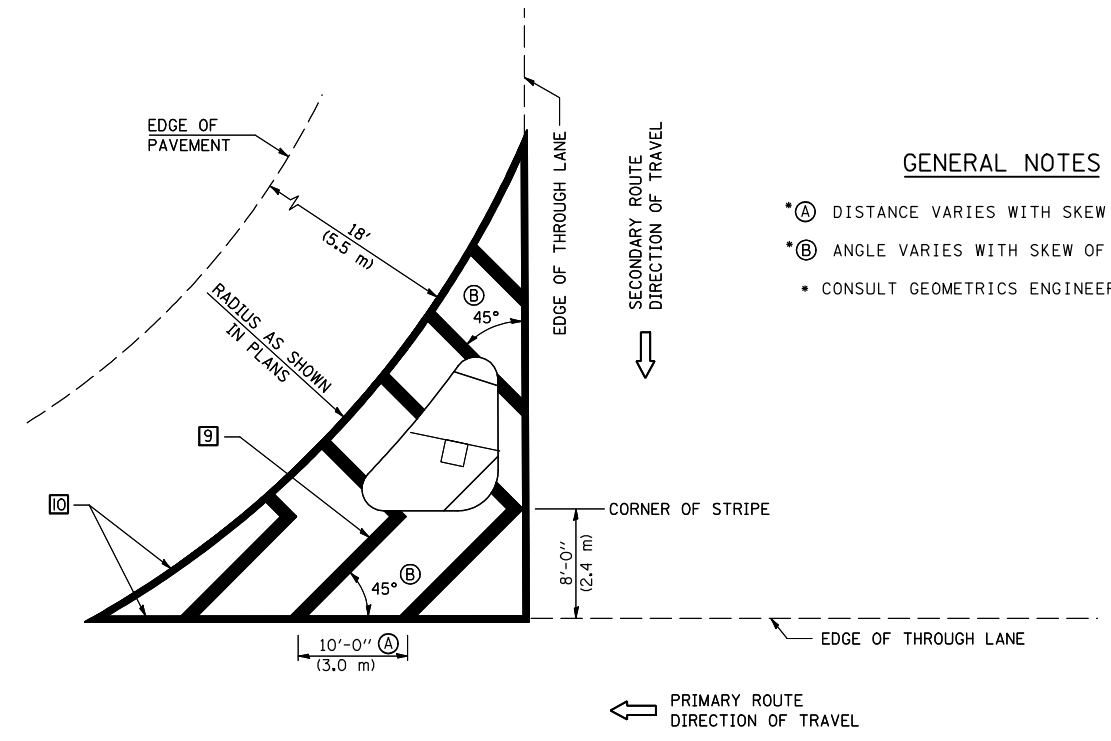
TYPICAL MEDIAN TRANSITIONS

GENERAL NOTES

1. WHEN MEDIANS ARE PRESENT, PAVEMENT MARKINGS ARE TO BE PLACED ADJACENT TO MEDIANS.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. A STRIPING KEY IS AVAILABLE ELSEWHERE AND SHALL BE SHOWN WHERE THE QUANTITIES ARE LISTED.
5. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
6. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING,
 < 30 MPH USE 15' (< 50 km/h USE 4.5 m)
 30-45 MPH USE 20' (50-75 km/h USE 6.0 m)
 > 45 MPH USE 30' (> 75 km/h USE 9.0 m)



RIGHT IN - RIGHT OUT ACCESS



GENERAL NOTES

- *A DISTANCE VARIES WITH SKEW OF INTERSECTION.
- *B ANGLE VARIES WITH SKEW OF INTERSECTION.
- CONSULT GEOMETRICS ENGINEER

ISLAND

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

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	PLOT SCALE = 40.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 2/6/2009	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS
 (RURAL & URBAN APPLICATIONS)**

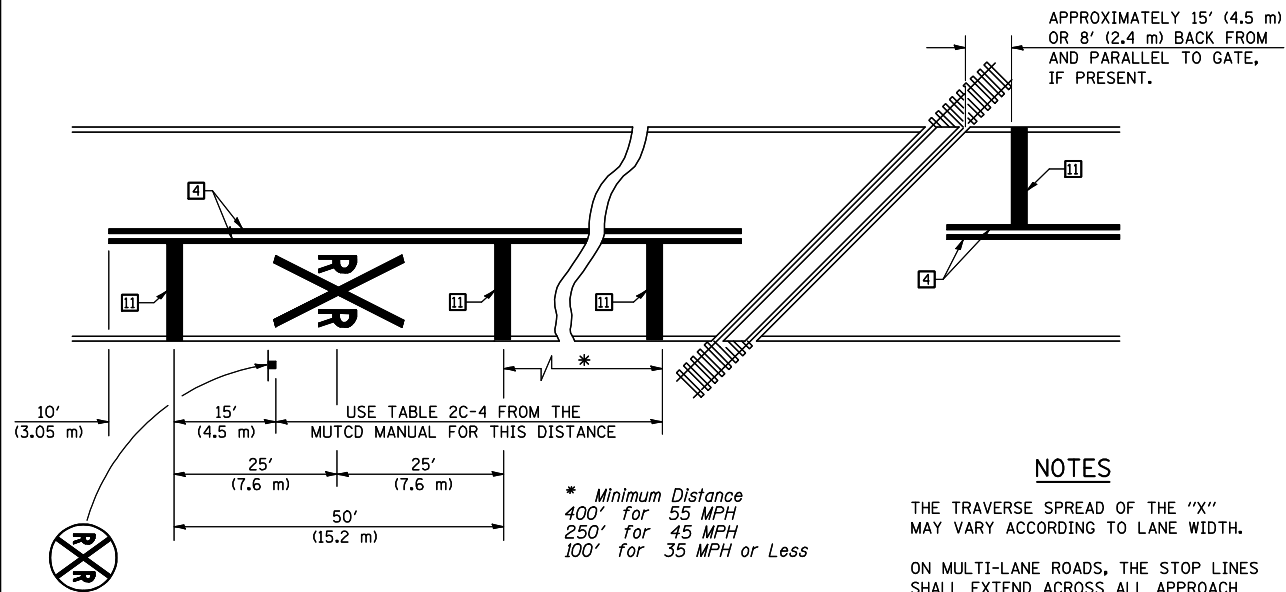
SCALE: SHEET NO. 3 OF 4 SHEETS STA. TO STA.

DISTRICT 5 DETAIL NO. 7800AAAA

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
709	105RS-2	Champaign	19	17
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 70261	

RAILROAD CROSSING WITH INTERCONNECT ONLY

RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

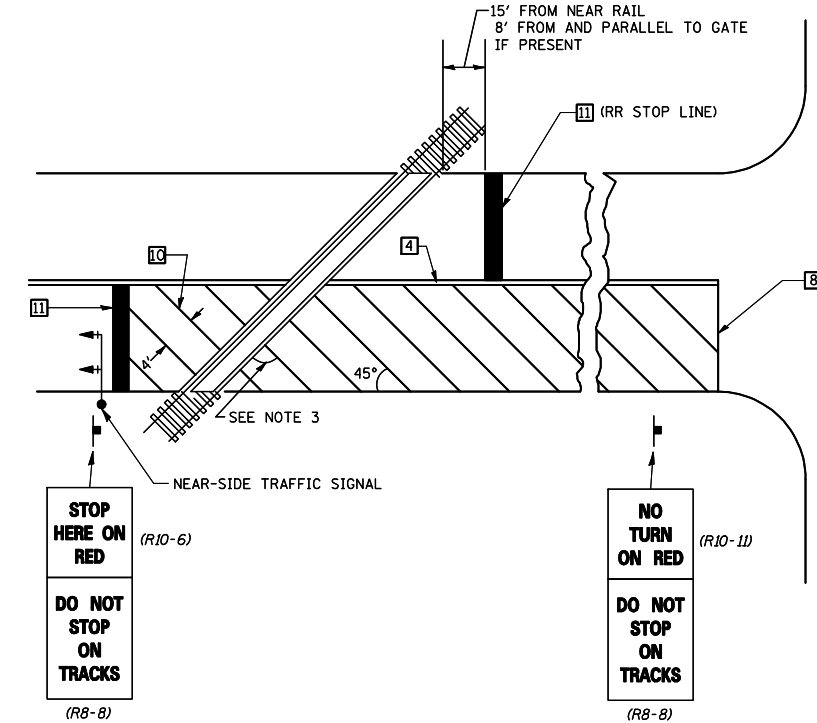
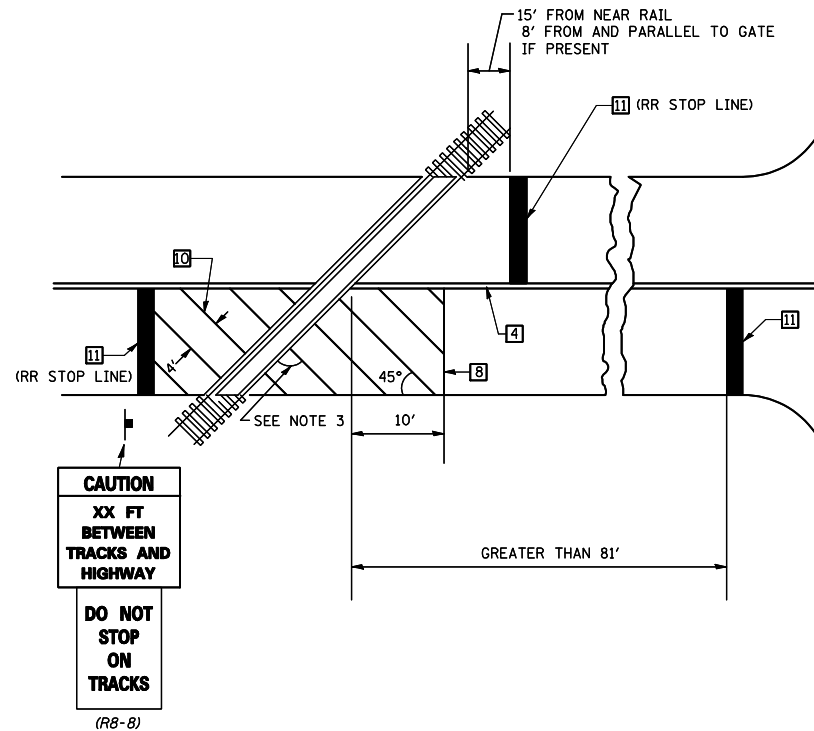
NOTES

APPROXIMATELY 15' (4.5 m) OR 8' (2.4 m) BACK FROM AND PARALLEL TO GATE, IF PRESENT.

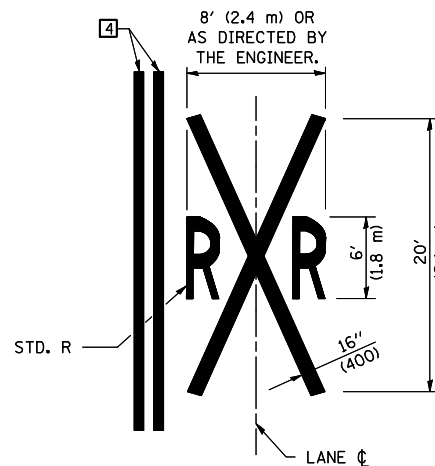
THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.



SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING



GENERAL NOTES

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
- WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

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	PLOT DATE = 2/6/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND MARKERS
(RURAL & URBAN APPLICATIONS)

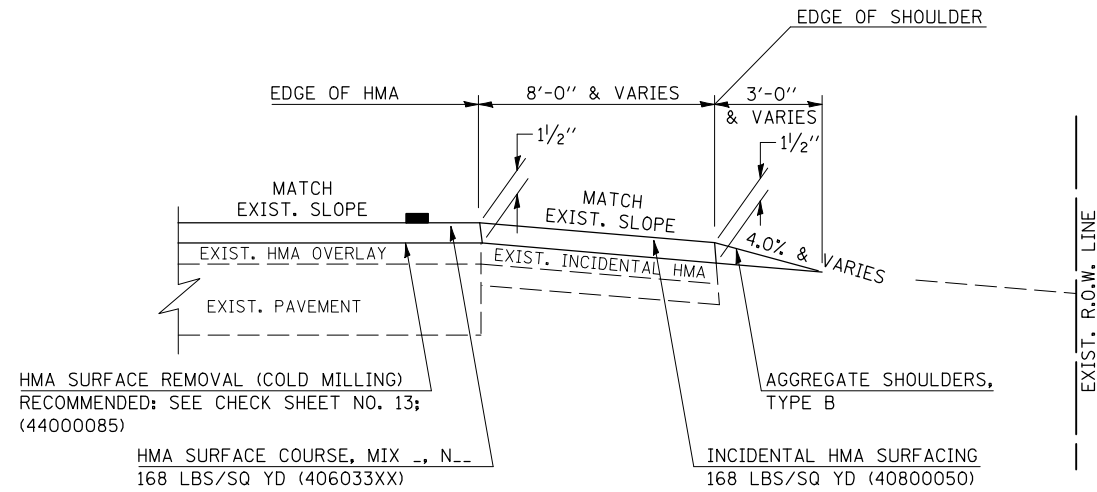
SCALE: SHEET NO. 4 OF 4 SHEETS STA. TO STA.

DISTRICT 5 DETAIL NO. 7800AAAA

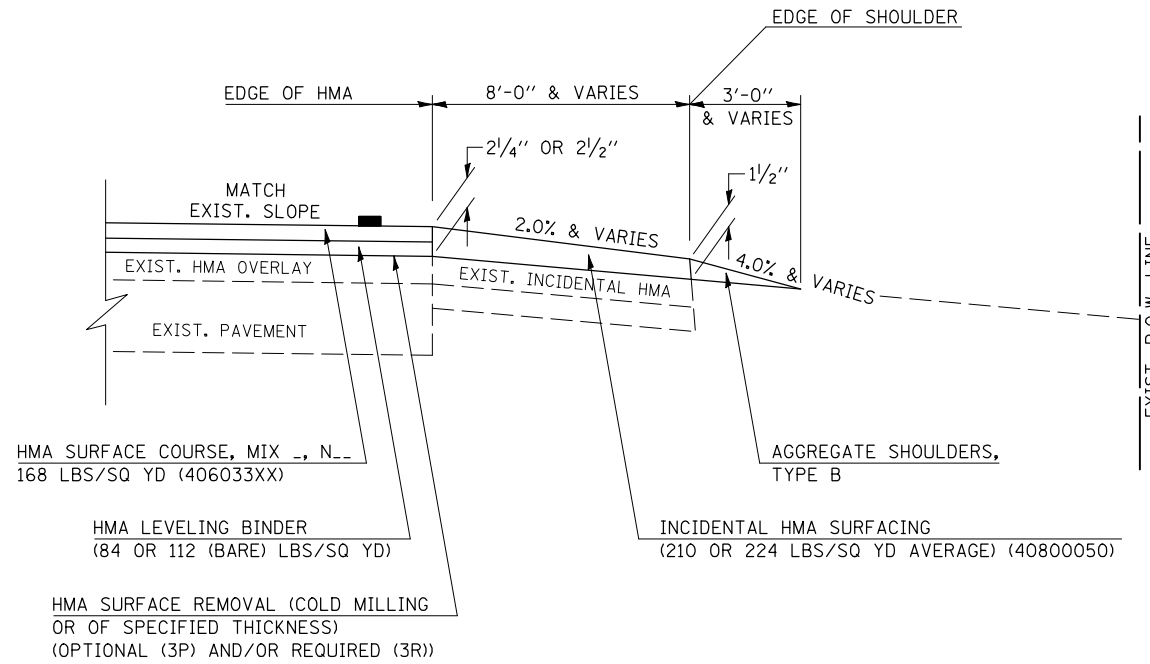
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
709	105RS-2	Champaign	19	18
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 70261	

PROJECTS WITHOUT RECONSTRUCTION

S.M.A.R.T. IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.03; 1 1/2")



"3P" OR "3R" IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.02; 2 1/4" OR 2 1/2" ON BARE CONCRETE)

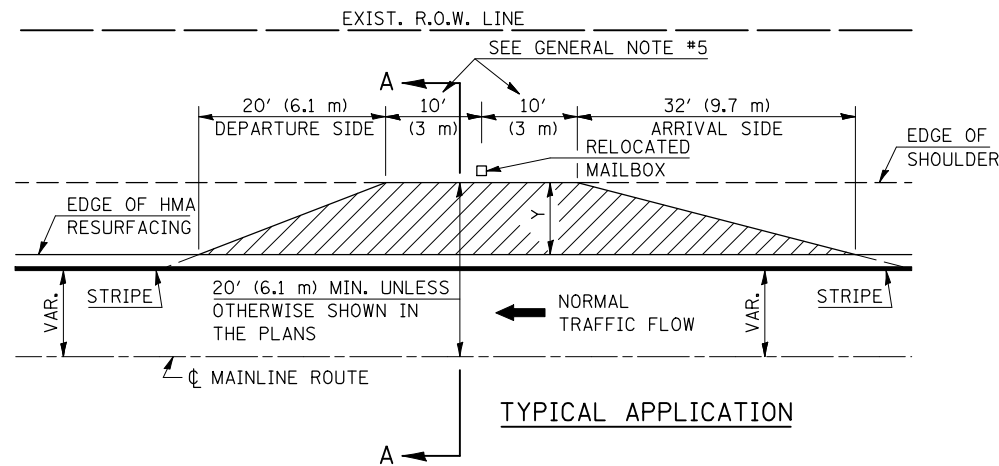


GENERAL NOTES

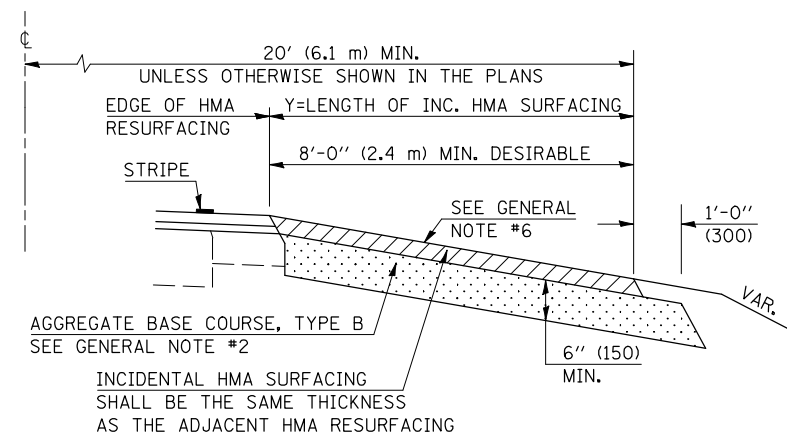
1. THE EXISTING SURFACE SHALL BE PREPARED IN ACCORDANCE WITH SECTION 408 OF THE STANDARD SPECIFICATIONS.
2. AGGREGATE BASE COURSE, TYPE B, 6" (150) MIN. SHALL BE USED WHERE IN THE OPINION OF THE ENGINEER THERE IS NOT SUFFICIENT BASE MATERIAL FOR THE PROPOSED MAILBOX TURNOUTS. THIS MATERIAL SHALL GENERALLY BE USED TO WIDEN ALL EXISTING MAILBOX TURNOUTS OR TO CONSTRUCT NEW MAILBOX TURNOUTS WHERE NONE NOW EXISTS.
3. ANY NECESSARY WORK BEHIND THE INCIDENTAL HMA SURFACING SHALL BE AS SHOWN IN THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
4. THE TEMPORARY RELOCATION OF EXISTING MAILBOXES SHALL BE IN ACCORDANCE WITH ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS.
5. WHEN MORE THAN ONE RELOCATED MAILBOX IS INCLUDED IN A PARTICULAR LOCATION THE TWO 10' (3 m) DIMENSIONS AS SHOWN ABOVE SHALL BE FROM THE END MAILBOX.
6. CROSS SLOPE SHALL BE AS SHOWN ON THE STATION CROSS SECTIONS AND/OR AS DIRECTED BY THE ENGINEER. MINIMUM 4% (1/2"'/') DESIRABLE; MAXIMUM 8% (1"'/')
7. WHEN MAILBOX TURNOUTS ARE CONSTRUCTED ADJACENT TO FIELD ENTRANCES, THE WIDTH OF THE INCIDENTAL HMA SURFACING CONSTRUCTED FOR THE FIELD ENTRANCE SHALL MATCH THE WIDTH OF THE PROPOSED MAILBOX TURNOUT SURFACING.
8. THE TOTAL SHOULDER WIDTH, 2.4 m (8') MINIMUM, SHALL BE PAVED BETWEEN SIDEROADS ENTRANCES AND/OR MAILBOX TURNOUTS AT LOCATIONS WHERE THE DISTANCE BETWEEN RADIUS OR TAPER CONTROL POINTS IS LESS THAN 15.0 m (50').
9. MAILBOXES SHALL BE MOUNTED SUCH THAT THE FACE OF THE MAILBOX IS 6" (150 mm) TO 12" (300 mm) AND THE POST A MINIMUM OF 24" (600 mm) FROM THE EDGE OF THE TURNOUT SURFACING.

PROJECTS WITH RECONSTRUCTION

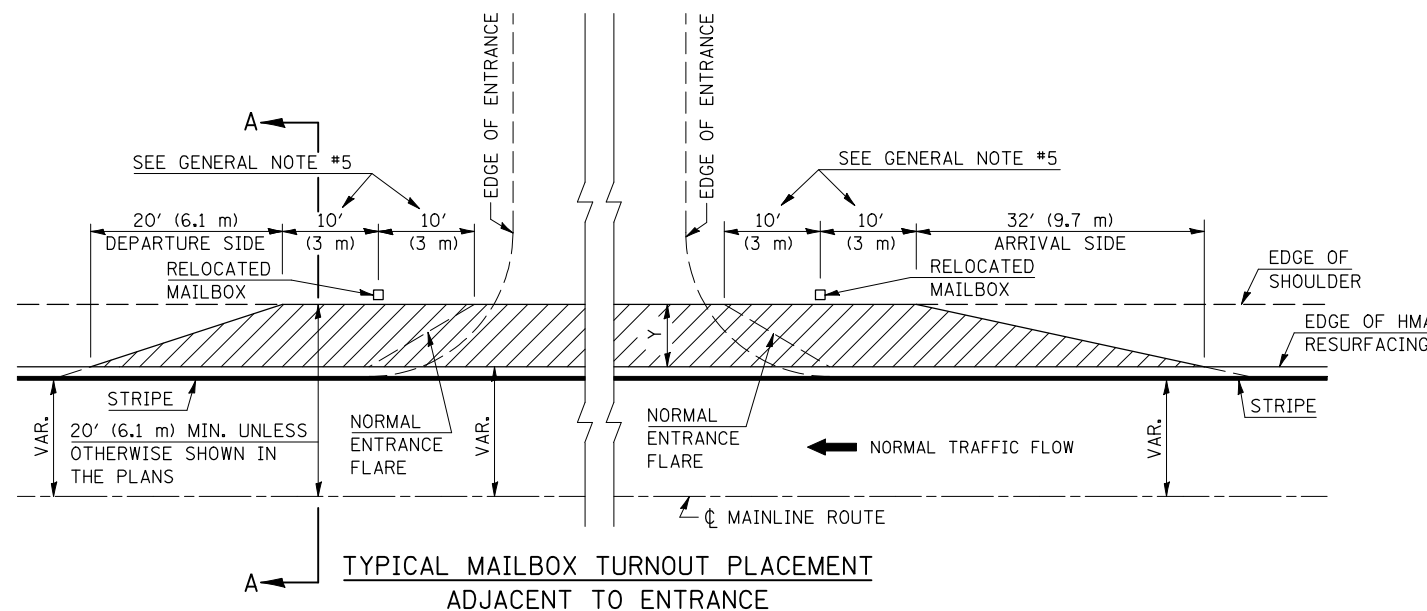
("3R" IMPROVEMENTS)



WIDTH OF SHOULDER	4'-0" - 8'-0" (1.2 m - 2.4 m)	10'-0" (3.0 m)
WIDTH OF TURNOUT "Y"	8'-0" (2.4 m)	8'-0" - 10'-0" (2.4 m - 3.0 m)



SECTION A-A



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 5 DETAIL NO. 40800050B

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		CHECKED -	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAILBOX TURNOUT (RURAL)

SCALE: NA SHEET NO. OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
709	105RS-2	Champaign	19	19
CONTRACT NO. 70261				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

DESIGNER NOTE: SEE HIGHWAY STANDARD 406201