

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	•	PIATT	48	1

• BRS-4&(9,10)RS-3

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 741 (IL 105)
SECTION 8RS-4&(9,10)RS-3
PROJECT ESP-0741(098)
PIATT COUNTY

C-95-121-00

RESURFACING (3P) FROM IL 32 (2 MILES SOUTH OF CERRO GORDO) TO SCL OF BEMENT

D-95-059-00



FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 4-5

(217)465-4181

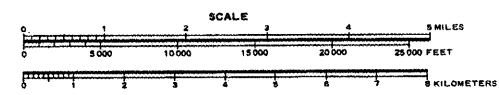
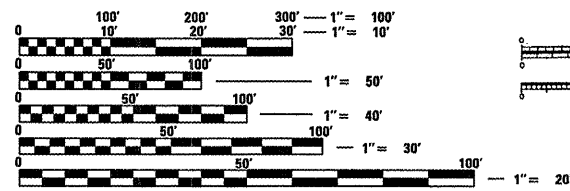
PROJECT ENGINEER: KEVIN TRAPP

SQUAD LEADER: MICHAEL LEROY

DESIGNER: DANA HENNESS

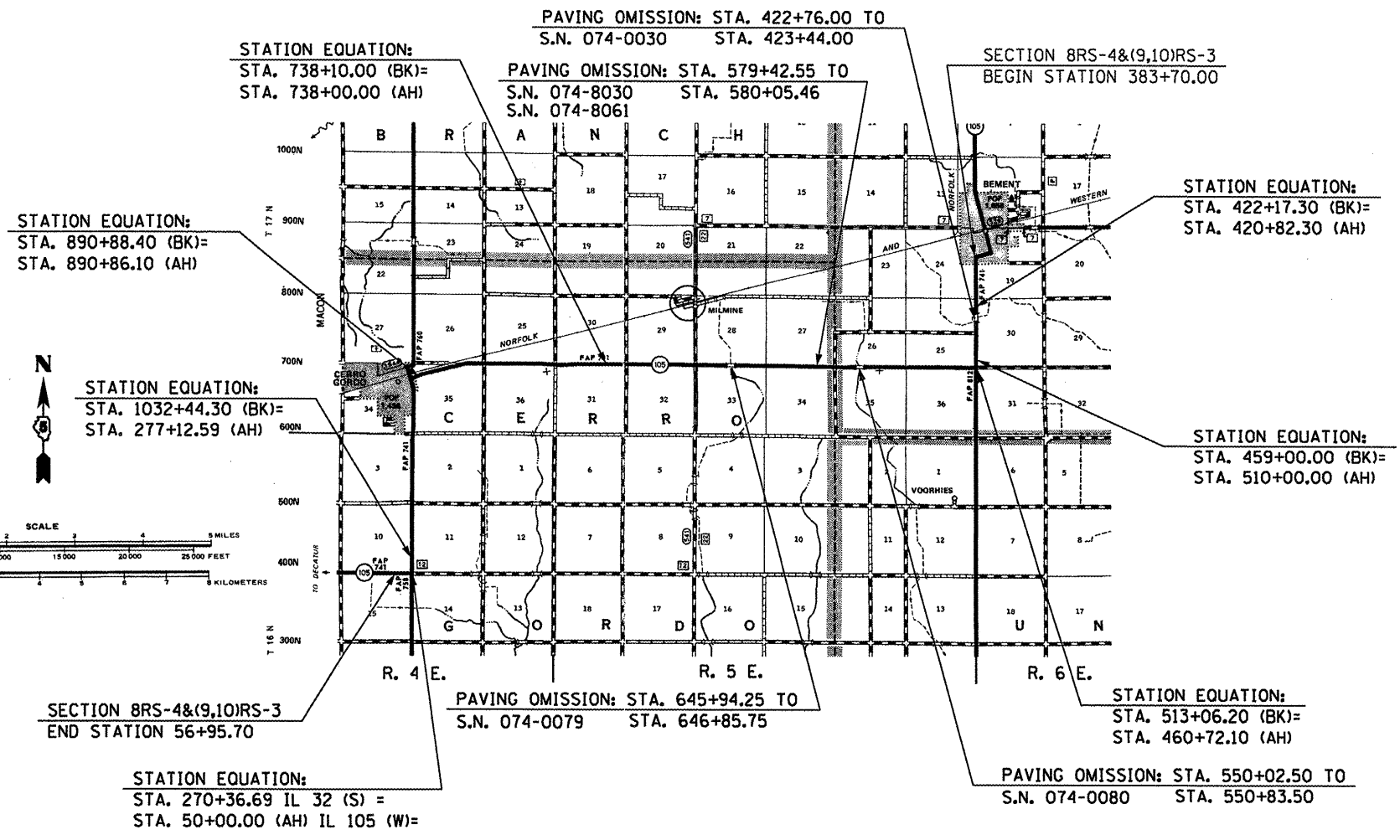
CURRENT ADT		
IL 105 (S)	1,800	2007
IL 105 (E)	900	2007
IL 105 (N)	1,550	2007

DESIGN DESIGNATION
N/A



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED 1/22 20 09
Joseph E. Cooney
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

March 13, 20 09
Charles G. Spradell
ENGINEER OF DESIGN AND ENVIRONMENT

March 13, 20 09
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

CONTRACT NO. 70129

TOTAL LENGTH OF SECTION & PROJECT = 66,527.30 FEET = 12.600 MILES
NET LENGTH OF SECTION & PROJECT = 66,223.89 FEET = 12.542 MILES

PLOT DATE = 1/28/2009
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	2

* BRS-4&(9,10)RS-3

INDEX OF SHEETS

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44	DETAIL OF TRAFFIC CONTROL & PROTECTION DEVICES (ROAD CLOSURE AND SIDEROAD / STREET CLOSURE)
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LIST OF HIGHWAY STANDARDS

000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
406201-01	MAILBOX TURNOUT
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHOULDER STRIPS/SHOULDERS WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
701201-03	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701301-03	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701306-02	LANE CLOSURE, 2L, 2W, SLOW MOVING OPERATIONS, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS, DAY ONLY
701501-05	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-01	TRAFFIC CONTROL DEVICES
780001-02	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	3

* 8RS-4&(9,10)RS-3

GENERAL NOTES

G.N.-100

ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.

G.N.-406

THE QUANTITIES INCLUDED IN THE PLANS FOR HOT-MIX ASPHALT RESURFACING ARE INTENDED TO GIVE THE COVERAGE SHOWN ON THE TYPICAL CROSS SECTIONS. IT IS NOT INTENDED TO INCREASE THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE IN ORDER TO USE ALL OF THE QUANTITIES INCLUDED IN THE CONTRACT.

G.N.-406.05B

ALL LEVELING BINDER OR BINDER SHALL BE GIVEN A FOG COAT OF PRIME BEFORE THE SURFACE COURSE IS PLACED WHEN DIRECTED BY THE ENGINEER.

THE FOG COAT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER GALLON FOR BITUMINOUS MATERIAL (PRIME COAT) AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N. -406H

MIXTURE REQUIREMENTS

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

LOCATION MIXTURE USE	IL 105 LEVEL BINDER	IL 105 SURFACE & INCIDENTAL	IL 105 BIT SHOULDER	IL 105 CLASS "D" PATCHING
AC/PG	PG 64-22	PG 64-22	PG 64-22	PG 64-22
RAP %(MAX)	25	15	30	25
DESIGN AIR Voids	4.0% @ NDES = 50	4.0% @ NDES = 50	2.0% @ NDES = 30	4.0% @ NDES = 50
MIX COMP (GRADATION)	IL 9.5	IL 9.5	OTHER	IL 19.0
FRICTION AGGREGATE	MIX C	MIX C	N/A	N/A

G.N.-440 (SPECIAL)

SPECIAL ATTENTION IS DIRECTED TO THE LAST SENTENCE OF ARTICLE 440.04 WHICH STATES: "MILLED PAVEMENT SHALL BE RESURFACED WITHIN 10 CALENDAR DAYS." THE CONTRACTOR SHALL SCHEDULE ACTIVITIES TO ACCOMPLISH THIS REQUIRED SPECIFICATION.

G.N.-442B -- PATCHING SCHEDULES

THE PATCHING SCHEDULES INCLUDED IN THE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AT THE TIME OF COMPLETION OF THE PLANS FOR LETTING. VARIATIONS IN LOCATION AND SIZES OF BOTH FULL-DEPTH AND PARTIAL-DEPTH PATCHES MAY OCCUR.

G.N.-667

THE RESIDENT ENGINEER SHALL CONTACT THE PROGRAM DEVELOPMENT CHIEF OF SURVEYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE FOR INSTRUCTION AS TO SETTING OF TEMPORARY OR PERMANENT TIES FOR CENTERLINE ALIGNMENT CONTROL SURVEY MARKERS (PC'S, PT'S, AND PI'S). PROJECT IMPLEMENTATION PERSONNEL WILL BE RESPONSIBLE FOR SETTING THESE MARKERS.

G.N.-703A

SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).

G.N.-781

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH STANDARD 781001, AND THE DETAILS SHOWN IN THE PLANS. IF THERE IS ANY DISCREPANCY BETWEEN THE STANDARD AND THE DETAILS IN THE PLANS, THE DETAILS IN THE PLANS SHALL GOVERN. THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS AND THE RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED MIDWAY IN THE 30 FOOT (9 m) SPACE BETWEEN THE DASHED CENTERLINE STRIPES (WHEN APPLICABLE).

G.N.-1004.01

COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS.

NO COMMITMENTS ON THIS PROJECT.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	4

* BRS-4&(9,10)RS-3

SUMMARY OF QUANTITIES

LOCATION OF WORK:

FAP 741 (IL 105)
 PIATT COUNTY
 STATION 383+70.00
 TO
 STATION 56+95.70
 RURAL TWO-LANE
 100% FEDERAL
 I000

CONSTRUCTION TYPE CODE:

CODE NO.	PAY ITEM	UNIT	TOTAL QUANTITY
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	19,441.0
40600300	AGGREGATE (PRIME COAT)	TON	197.0
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	7,849.0
40600895	CONSTRUCTING TEST STRIP	EACH	1.0
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	1,007.0
40600990	TEMPORARY RAMP	SQ YD	294.0
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	16,409.0
40800010	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	676.0
40800030	AGGREGATE (PRIME COAT)	TON	7.0
40800050	INCIDENTAL HOT-MIX ASPHALT SURFACING	TON	503.0
44000152	HOT-MIX ASPHALT SURFACE REMOVAL, 3/4"	SQ YD	185,911.0
44000154	HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/4"	SQ YD	7,489.0
44000196	HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL	SQ YD	1,311.0
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SQ YD	337.0
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SQ YD	48.0
48101200	AGGREGATE SHOULDERS, TYPE B	TON	3,277.0
56109210	WATER VALVES TO BE ADJUSTED	EACH	1.0
60255500	MANHOLES TO BE ADJUSTED	EACH	1.0
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	4.0
67100100	MOBILIZATION	L SUM	1.0
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1.0
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1.0
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1.0

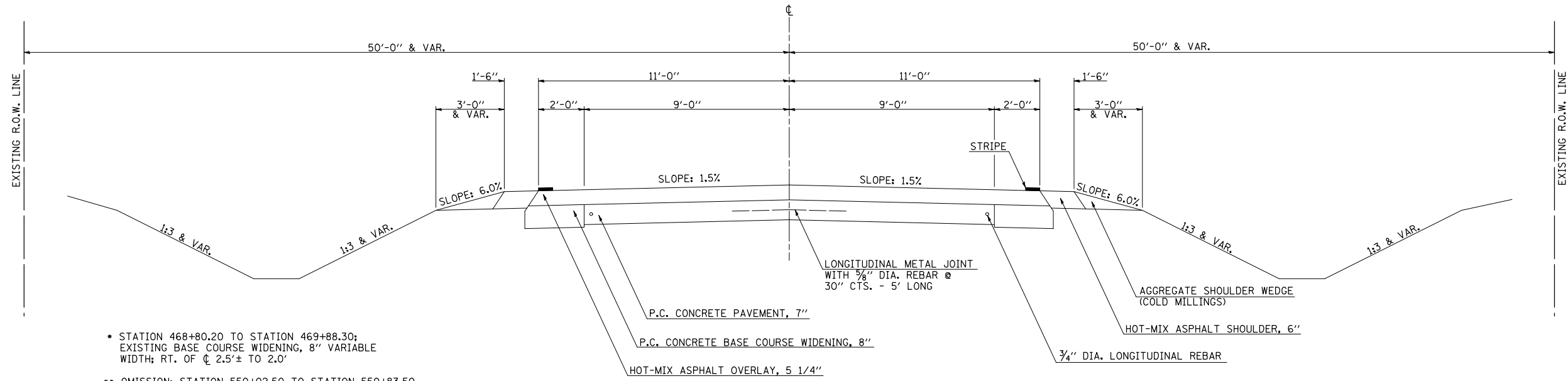
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721	*	PIATT	48	6

* BRS-4 & (9,10)RS-3

EXISTING TYPICAL CROSS SECTION (A)

FAP 721 (IL 105)

STATION	TO	STATION	TO	STATION	TO	STATION	TO	STATION
383+70.00		411+00.00 (B)		648+50.00		663+00.00 (E)		
(B) 435+00.00		452+57.80 (C)		(E) 739+00.00		758+50.00 (E)		
(C) 468+80.20		474+13.00 (E)		(B) 825+95.00		834+00.00 (B)		
(B) 488+76.00		503+00.00 (E)		(B) 838+34.00		846+67.16 (F)		
(E) 515+00.00		550+02.50**		(E) 930+50.00		936+50.00 (E)		
550+83.50		579+42.55 (E)						

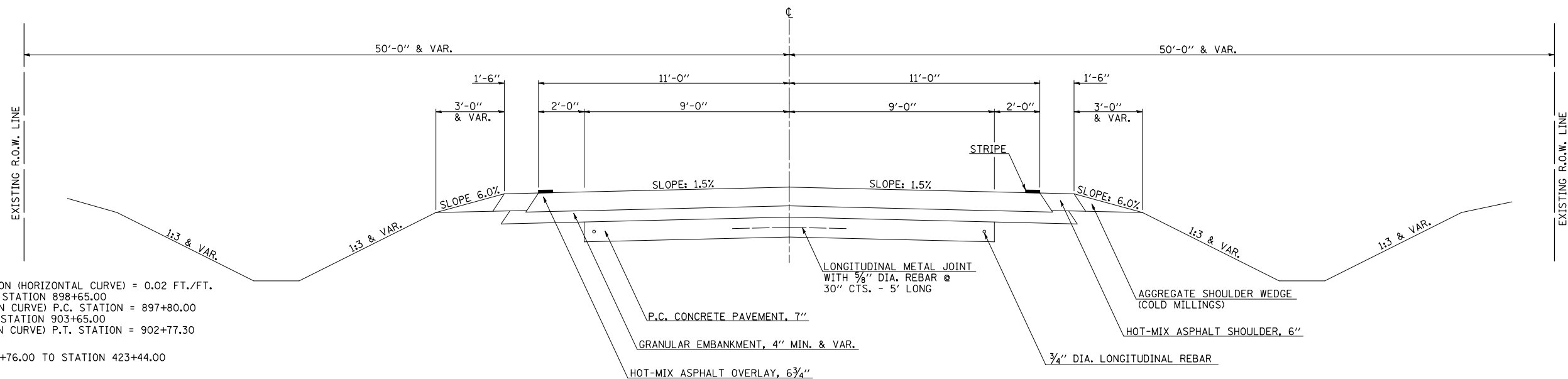


- STATION 468+80.20 TO STATION 469+88.30;
EXISTING BASE COURSE WIDENING, 8" VARIABLE
WIDTH; RT. OF CL 2.5'± TO 2.0'
- ** OMISSION: STATION 550+02.50 TO STATION 550+83.50
- ** OMISSION: STATION 579+42.55 TO STATION 580+05.46

EXISTING TYPICAL CROSS SECTION (B)

FAP 721 (IL 105)

STATION	TO	STATION	TO	STATION	TO	STATION	TO	STATION
(A) 411+00.00		422+17.30 (BK.)***		(E) 678+14.00		703+97.00 (E)		
*** 420+82.30 (AH.)		422+76.00 **		(E) 818+05.00		825+95.00 (A)		
** 423+44.00		435+00.00 (A)		(A) 834+00.00		838+34.00 (A)		
(E) 478+46.00		488+76.00 (A)		(I) 898+03.00		921+90.00 (E)		
(E) 606+83.00		628+12.00 (E)						



- EXISTING S.E. TRANSITION (HORIZONTAL CURVE) = 0.02 FT./FT.
STATION 896+95.00 TO STATION 898+65.00
(85.0' TANGENT; 85.0' IN CURVE) P.C. STATION = 897+80.00
STATION 901+95.00 TO STATION 903+65.00
(87.7' TANGENT; 82.3' IN CURVE) P.T. STATION = 902+77.30
- ** OMISSION: STATION 422+76.00 TO STATION 423+44.00
- *** STATION EQUATION:
STATION 422+17.30 (BK.) =
STATION 420+82.30 (AH.)

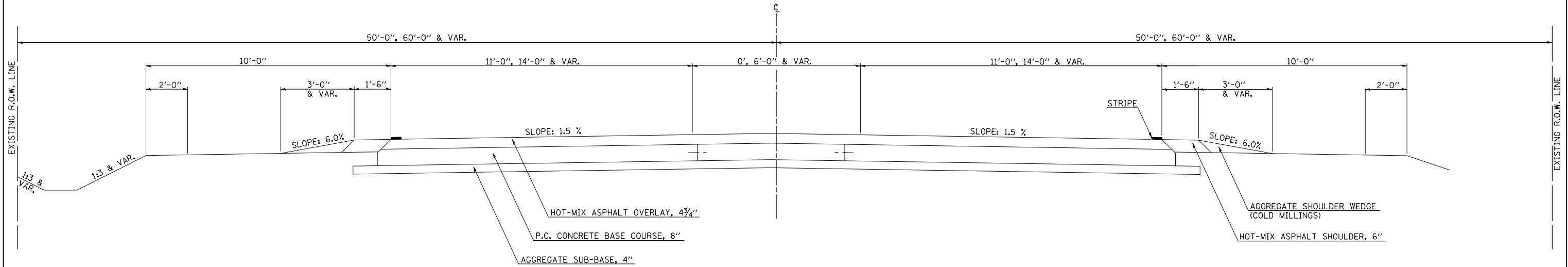
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721	*	PIATT	48	7

* BRS-4 & (9,10)RS-3

EXISTING TYPICAL CROSS SECTION (C)

FAP 721 (IL 105)

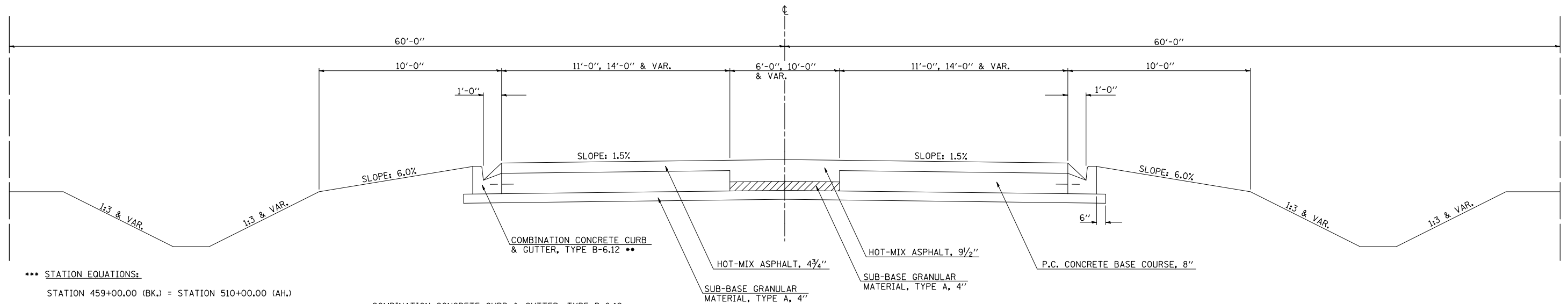
STATION	TO	STATION
(A) 452+57.80		458+74.00 (N. APPROACH) (D)
(D) 463+72.10		468+80.20 (W. APPROACH) (A)
(D) 515+87.60		520+95.70 (S. APPROACH)



EXISTING TYPICAL CROSS SECTION (D)

FAP 721 (IL 105)

STATION	TO	STATION
(C) (N. APPROACH) 458+74.00		459+00.00 (BK.)***
510+00.00 (AH.)		513+06.20 (BK.)
***460+72.10 (AH.)		463+72.10 (WEST) (C)
513+06.20		515+87.60 (SOUTH: F.A.P. RTE. 812) (C)



*** STATION EQUATIONS:

STATION 459+00.00 (BK.) = STATION 510+00.00 (AH.)

F.A. RTE. 741 (NORTH APPROACH) STATION 512+57.60 (BK.) = T.R. 167 (EAST APPROACH) STATION 600+00.00 (AH.)

F.A. RTE.741 (N/S) STATION 513+06.20 (BK.) = F.A. RTE. 741 (WEST) STATION 460+72.10 (AH.)

** COMBINATION CONCRETE CURB & GUTTER, TYPE B-6.12

LT. & RT. STATION 510+89.00 TO STATION 514+98.00 (FAP 812) & LT. & RT. STATION 462+72.40 (IL 105 WEST)

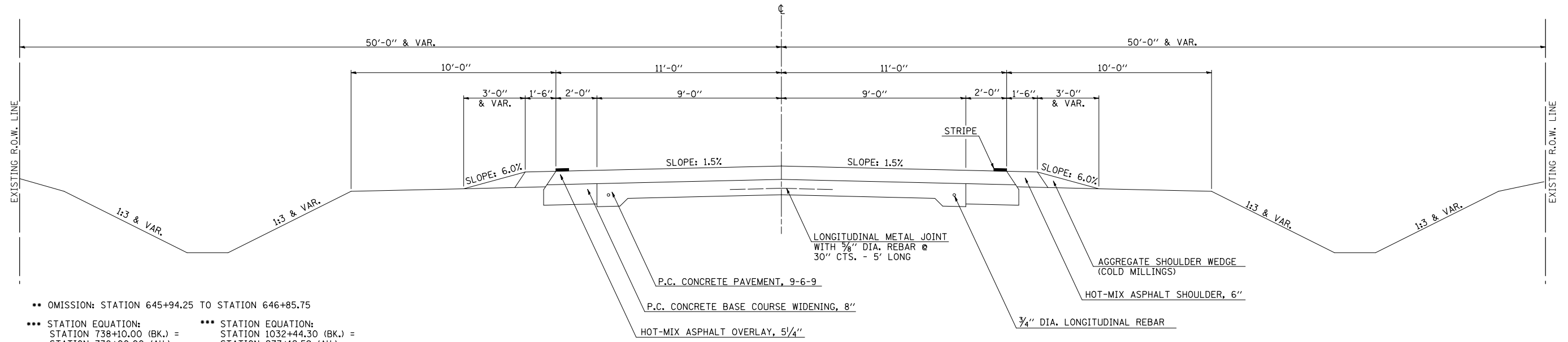
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721	*	PIATT	48	8

* BRS-4 & (9,10)RS-3

EXISTING TYPICAL CROSS SECTION (E)

FAP 721 (IL 105)

STATION	TO	STATION	TO	STATION	TO	STATION
(A) 474+13.00		478+46.00 (B)		(B) 703+97.00		738+10.00 (BK.)***
(A) 503+00.00		515+00.00 (A)		*** 738+00.00 (AH.)		739+00.00 (A)
(A)** 580+05.46		606+83.00 (B)		(A) 758+50.00		818+05.00 (B)
(B) 628+12.00		645+94.25**		(B) 921+90.00		930+50.00 (A)
** 646+85.75		648+50.00 (A)		(A) 936+50.00		1032+44.30 (BK.)*** (J)
(A) 663+00.00		678+14.00 (B)				

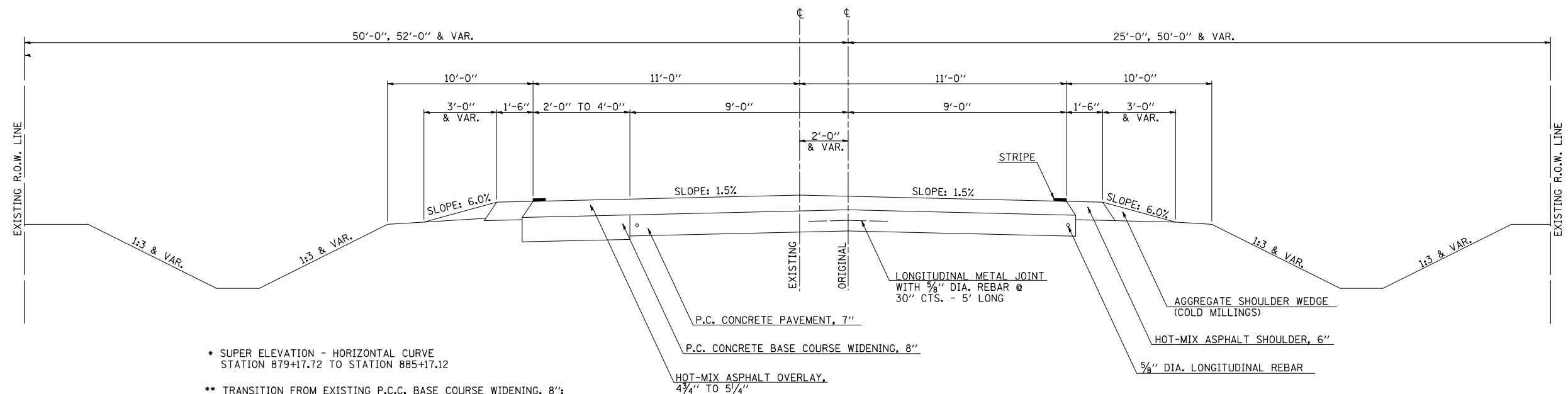


** OMISSION: STATION 645+94.25 TO STATION 646+85.75
 *** STATION EQUATION: STATION 738+10.00 (BK.) = STATION 738+00.00 (AH.)
 *** STATION EQUATION: STATION 1032+44.30 (BK.) = STATION 277+12.59 (AH.)

EXISTING TYPICAL CROSS SECTION (F)

FAP 721 (IL 105)

STATION	TO	STATION
(A) 846+67.16		850+64.96**
850+64.96		878+17.42
*** 878+17.42		885+16.82 (C)



* SUPER ELEVATION - HORIZONTAL CURVE STATION 879+17.72 TO STATION 885+17.12
 ** TRANSITION FROM EXISTING P.C.C. BASE COURSE WIDENING, 8"; 2' LT. & RT. TO 4' LT. & 0' RT.
 *** TRANSITION BACK TO 2' LT. & RT.

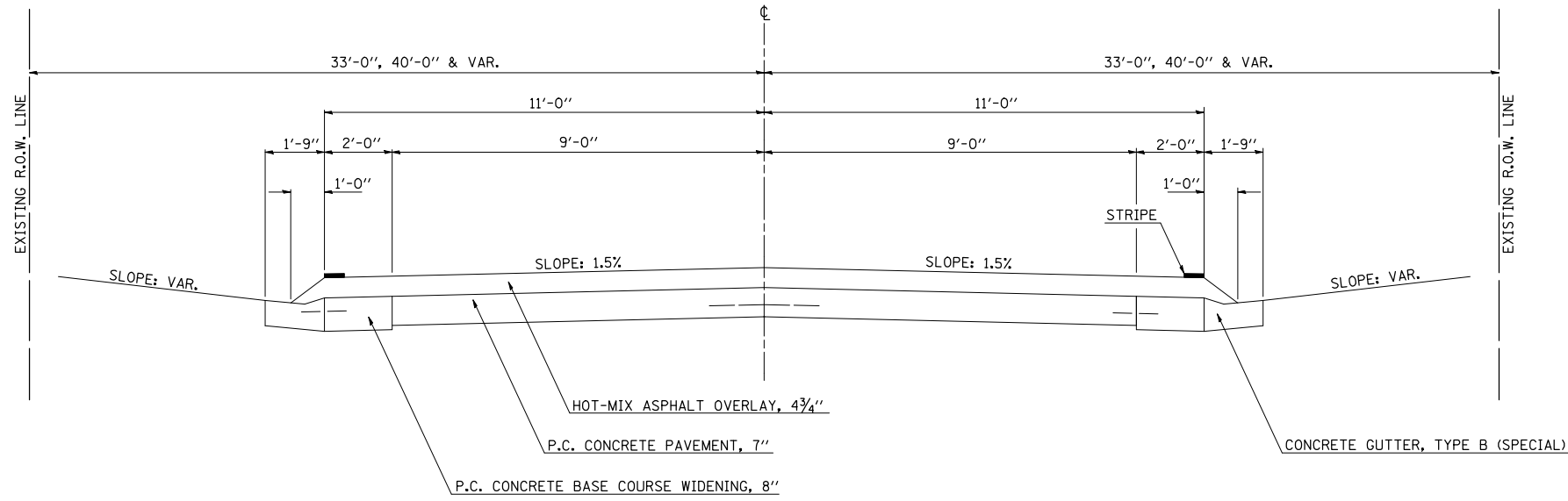
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721	*	PIATT	48	10

* BRS-4 & (9,10)RS-3

EXISTING TYPICAL CROSS SECTION I

FAP 721 (IL 105)

STATION TO STATION
 (H) 893+50.00 (B) 898+03.00

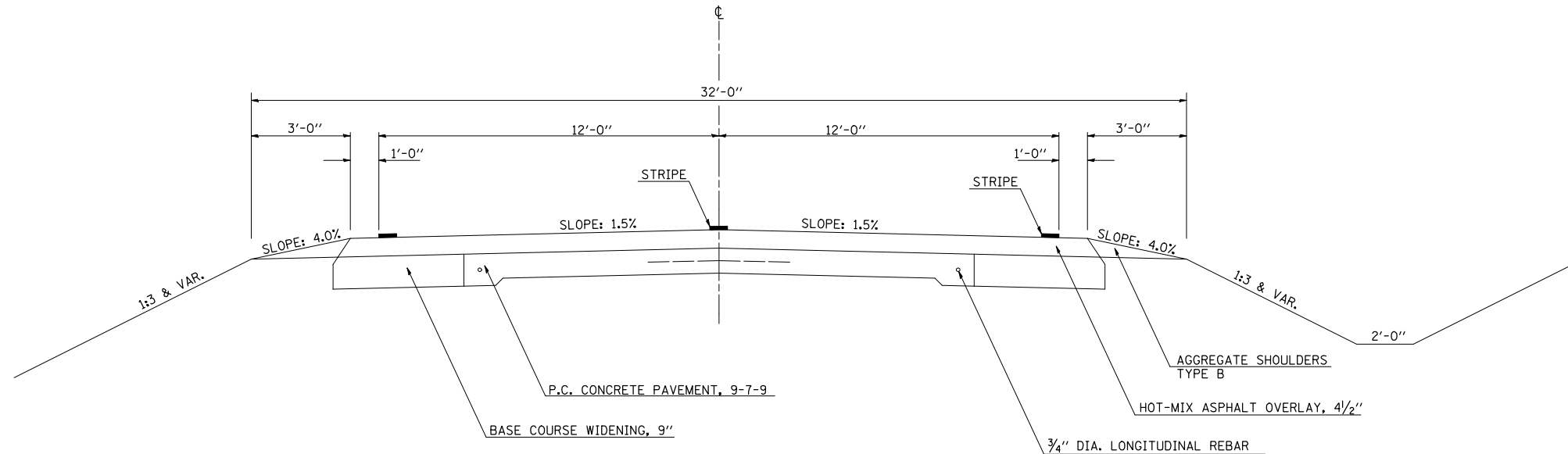


* SUPER ELEVATION: HORIZONTAL CURVE:
 STATION 897+80.00 TO STATION 901+77.30

EXISTING TYPICAL CROSS SECTION J

FAP 758 (IL 32)

STATION TO STATION
 263+11.00 (IL 32) 268+34.00 (K)



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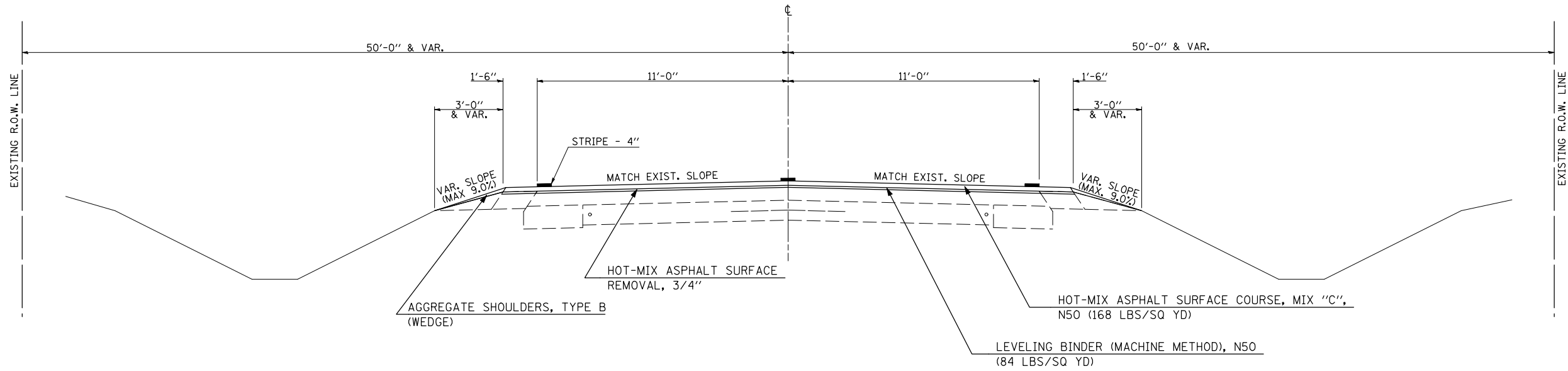
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721	.	PIATT	48	12

• BRS-4 & (9,10)RS-3

PROPOSED TYPICAL CROSS SECTION ①

FAP 721 (IL 105)

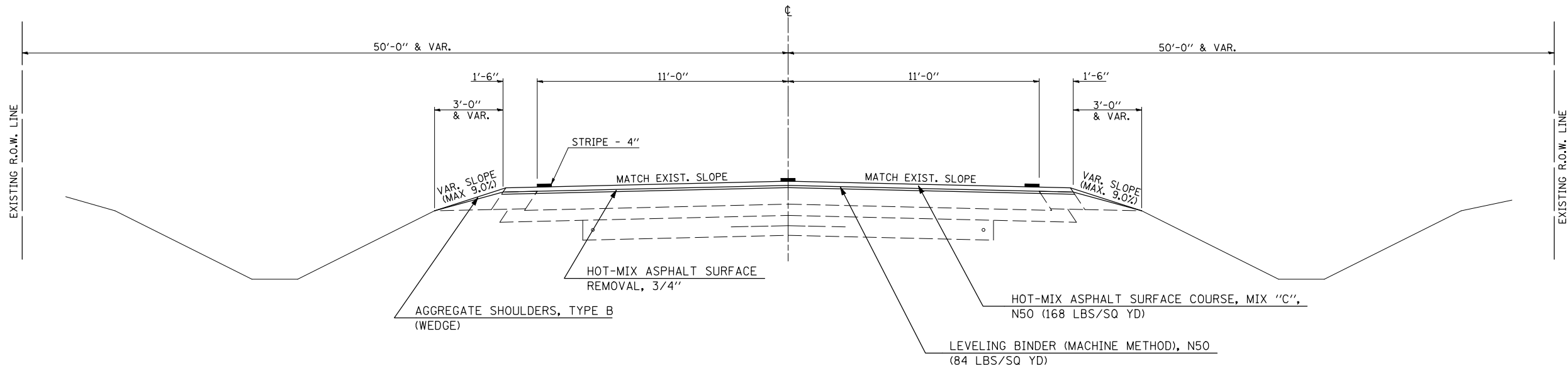
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383+70.00		411+00.00 ②	⑤ 648+50.00		663+00.00 ⑤
② 435+00.00		452+57.80 ③	⑤ 739+00.00		758+50.00 ⑤
③ 468+80.20		474+13.00 ⑤	② 825+95.00		834+00.00 ②
② 488+76.00		503+00.00 ⑤	② 838+34.00		846+67.16 ⑥
⑤ 515+00.00		550+02.50**	① 930+50.00		936+50.00 ⑤
550+83.50		579+42.55 ⑤			



PROPOSED TYPICAL CROSS SECTION ②

FAP 721 (IL 105)

STATION	TO	STATION	STATION	TO	STATION
① 411+00.00		422+17.30 (BK.)***	⑤ 678+14.00		703+97.00 ⑤
*** 420+82.30 (AH.)		422+76.00 **	⑤ 818+05.00		825+95.00 ①
** 423+44.00		435+00.00 ①	① 834+00.00		838+34.00 ①
⑤ 478+46.00		488+76.00 ①	⑨ 898+03.00		921+90.00 ⑤
⑤ 606+83.00		628+12.00 ⑤			



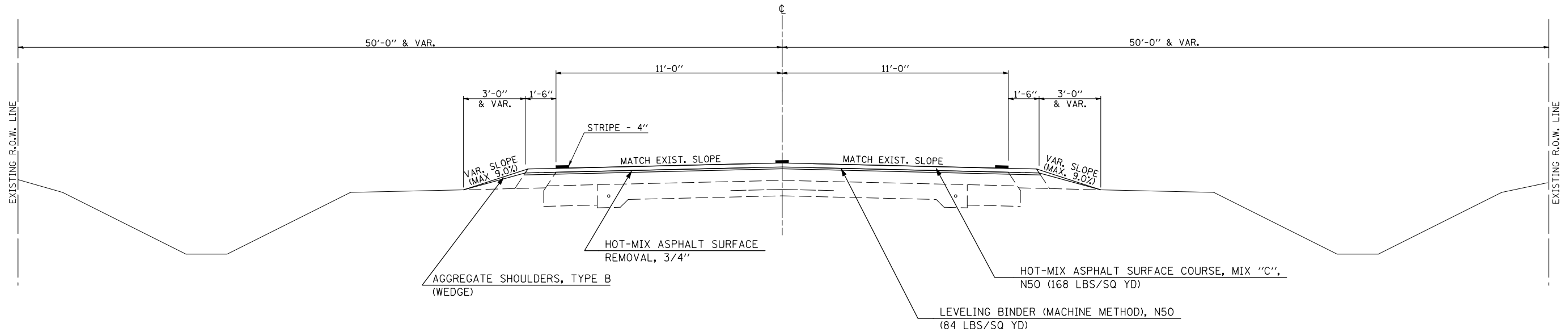
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* BRS-4 & (9,10)RS-3

PROPOSED TYPICAL CROSS SECTION ⑤

FAP 721 (IL 105)

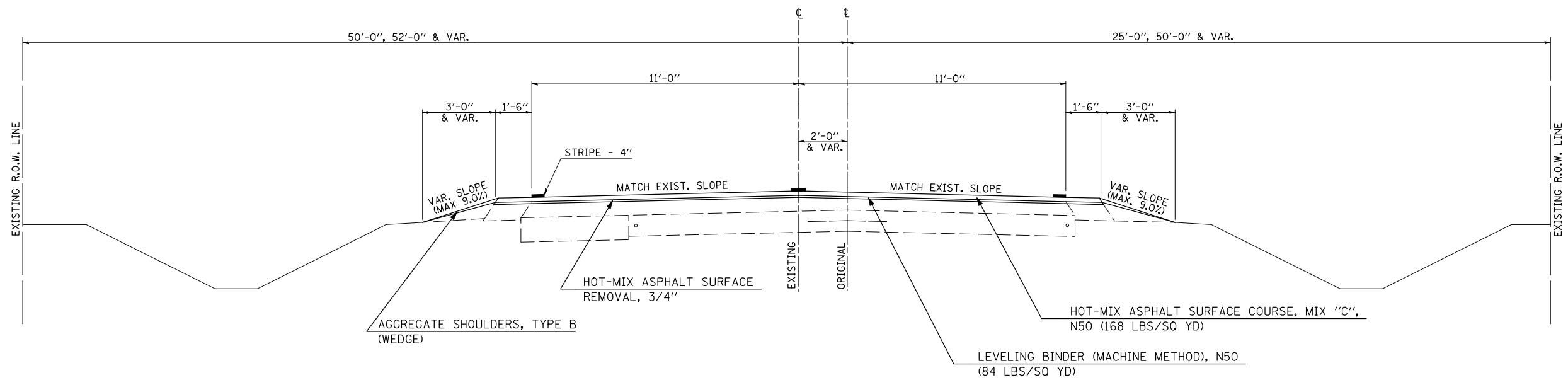
STATION	TO	STATION	TO	STATION	TO	STATION
① 474+13.00		478+46.00	②	② 703+97.00		738+10.00 (BK.)***
① 503+00.00		515+00.00	①	*** 738+00.00 (AH.)		739+00.00 ①
① ** 580+05.46		606+83.00	②	① 758+50.00		818+05.00 ②
② 628+12.00		645+94.25 **		② 921+90.00		930+50.00 ①
** 646+85.75		648+50.00	①	① 936+50.00		1032+44.30 (BK.)*** ⑩
① 663+00.00		678+14.00	②			



PROPOSED TYPICAL CROSS SECTION ⑥

FAP 721 (IL 105)

STATION	TO	STATION	TO
① 846+67.16		850+64.96 **	
		878+17.42	
*** 878+17.42		885+16.82 ⑦	



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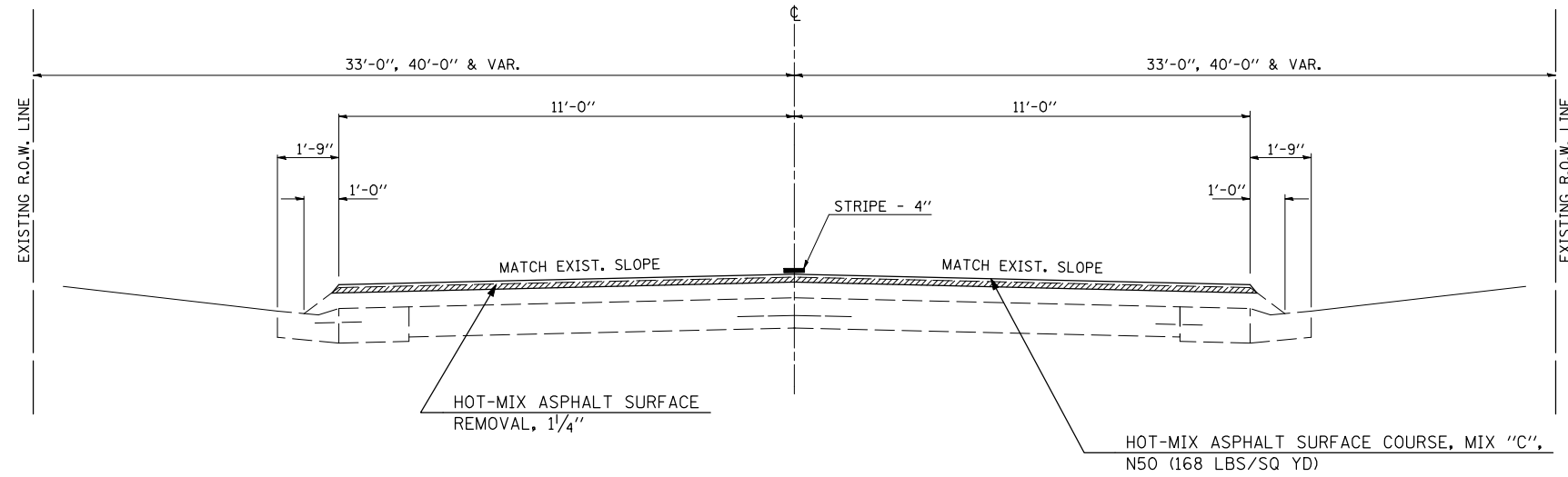
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
721	*	PIATT	48	16

* BRS-4 & (9,10)RS-3

PROPOSED TYPICAL CROSS SECTION ⑨

FAP 721 (IL 105)

STATION TO STATION
 ⑧ 892+40.00 898+03.00 ②

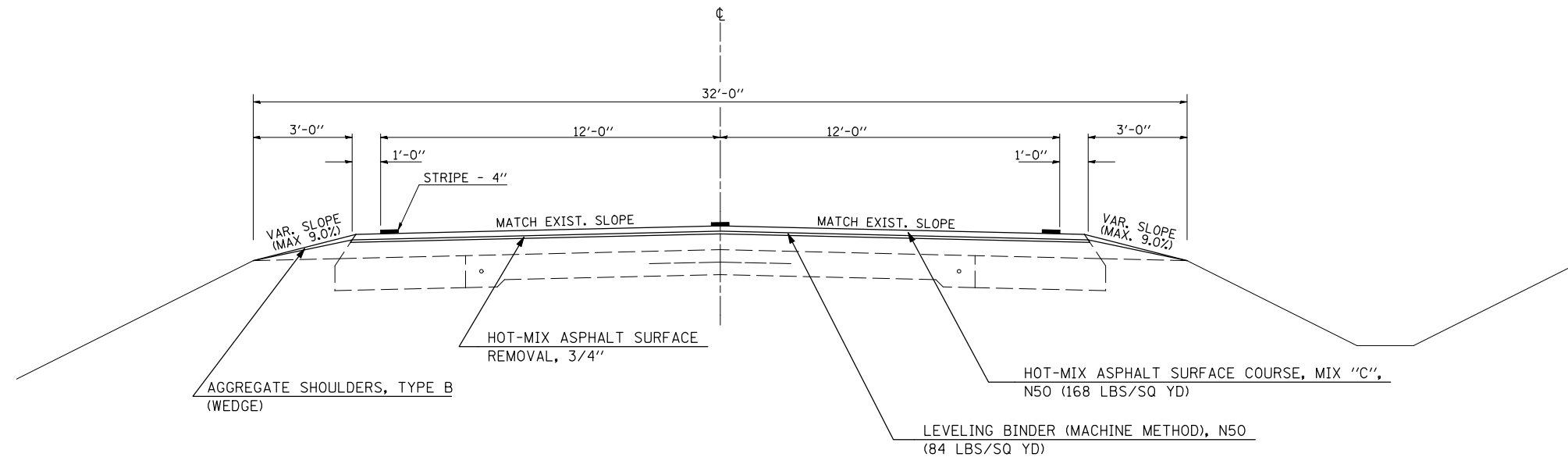


SEE "DETAIL OF MILLING TRANSITION" SHEET FOR TRANSITION LIMITS

PROPOSED TYPICAL CROSS SECTION ⑩

FAP 758 (IL 32)

STATION TO STATION
 263+11.00 (IL 32) 268+34.00 ⑪



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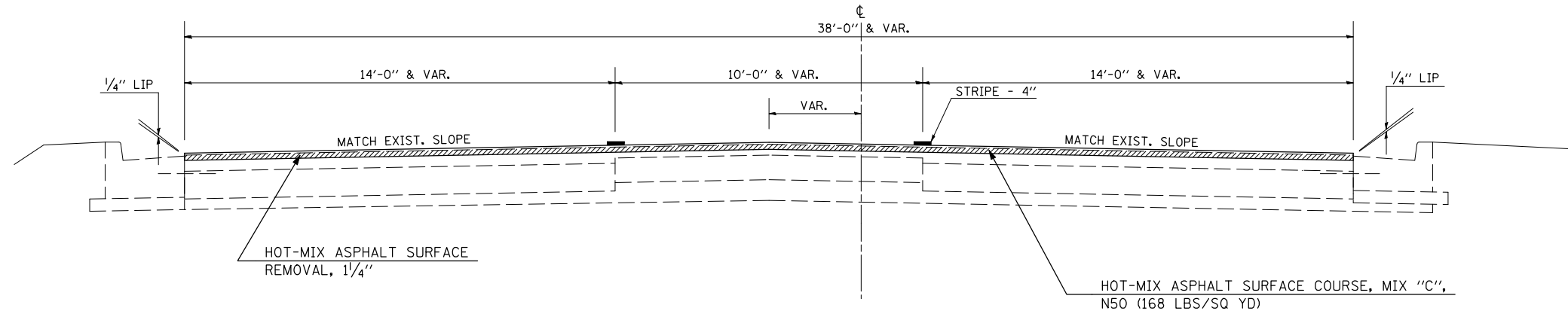
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
721	*	PIATT	48	17

• 8RS-4 & (9,10)RS-3

PROPOSED TYPICAL CROSS SECTION ⑪

FAP 721 (IL 105)

STATION TO STATION
 ⑩ 268+34.00 (IL 32) 277+12.59 (IL 105) *

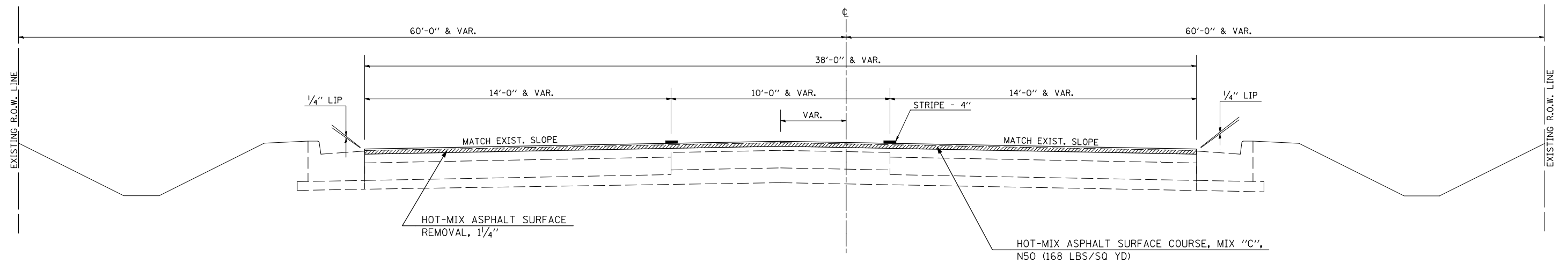


SEE "DETAIL OF MILLING TRANSITION" SHEET FOR TRANSITION LIMITS

PROPOSED TYPICAL CROSS SECTION ⑫

FAP 721 (IL 105)

STATION TO STATION
 50+18.30 56+95.70 IL 105 (W)

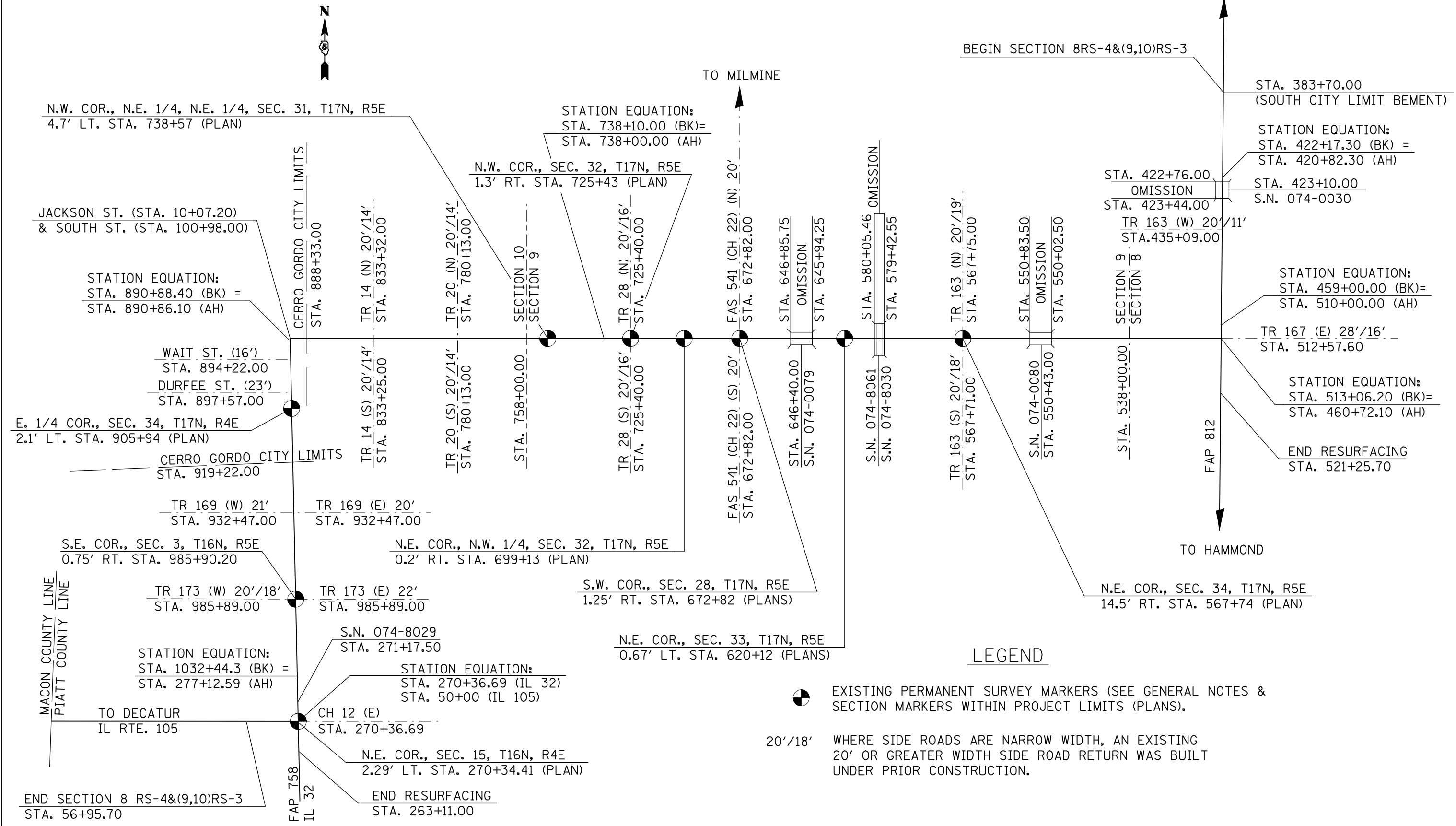


SEE "DETAIL OF MILLING TRANSITION" SHEET FOR TRANSITION LIMITS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	18

* 8RS-4&(9,10)RS-3

SCHEMATIC DETAIL OF SECTION



LEGEND

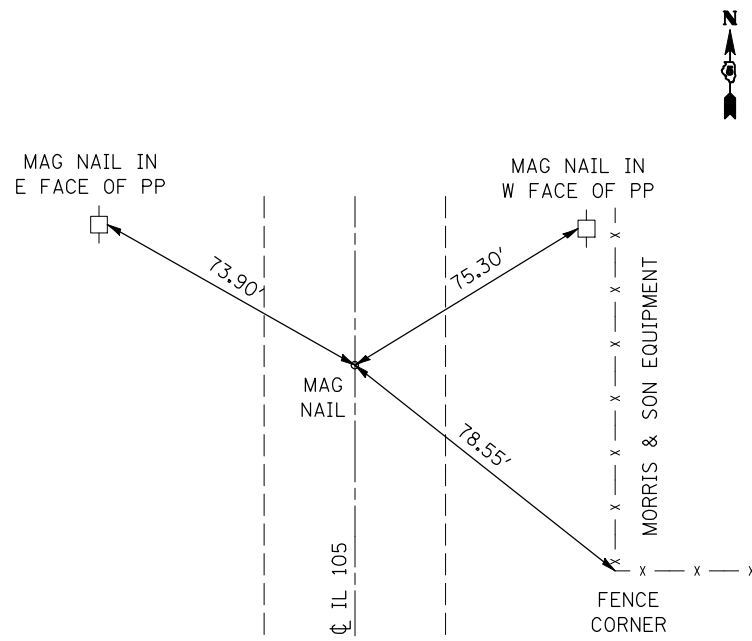
- EXISTING PERMANENT SURVEY MARKERS (SEE GENERAL NOTES & SECTION MARKERS WITHIN PROJECT LIMITS (PLANS)).
- 20'/18' WHERE SIDE ROADS ARE NARROW WIDTH, AN EXISTING 20' OR GREATER WIDTH SIDE ROAD RETURN WAS BUILT UNDER PRIOR CONSTRUCTION.

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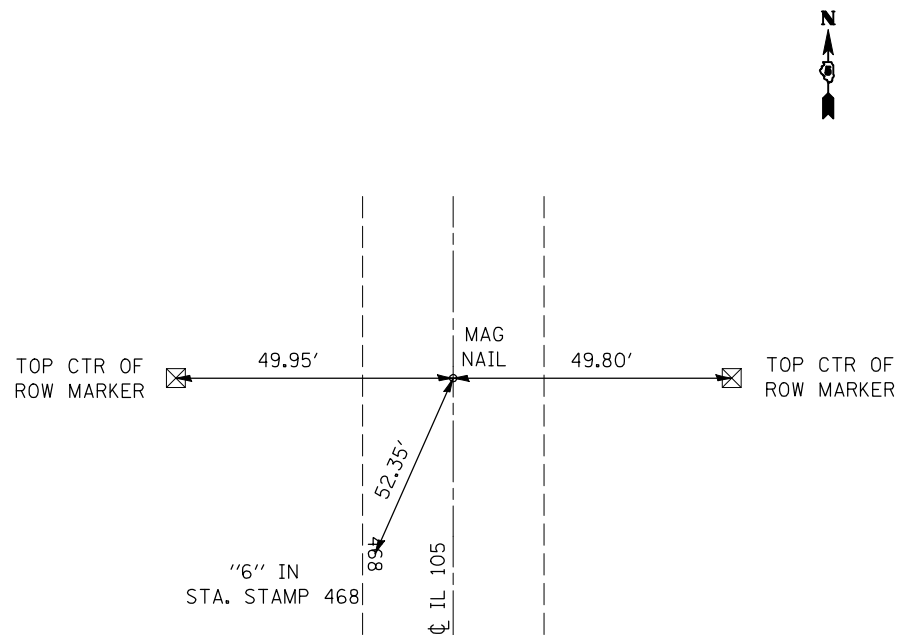
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	.	PIATT	48	19

* 8RS-4&(9,10)RS-3

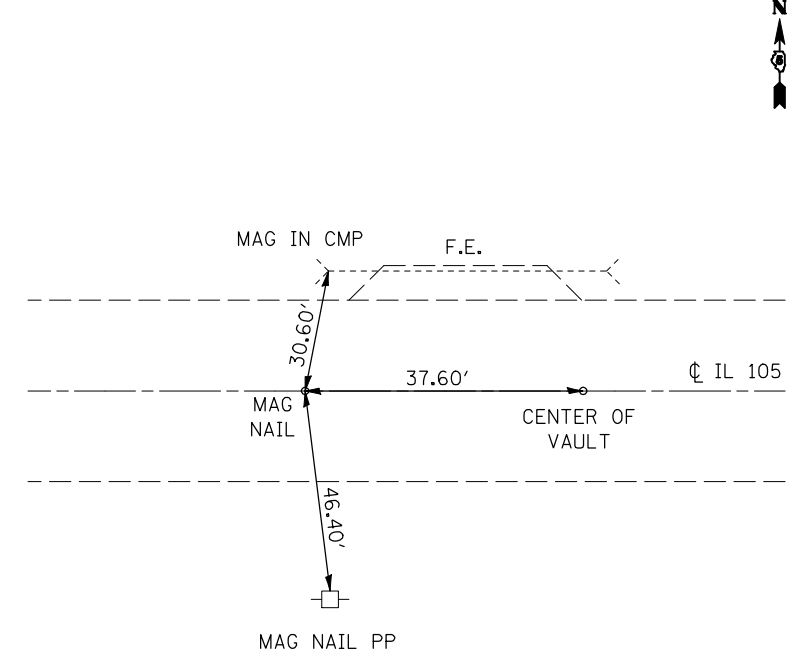
P.O.T. #1
STA. 443 + 78.38



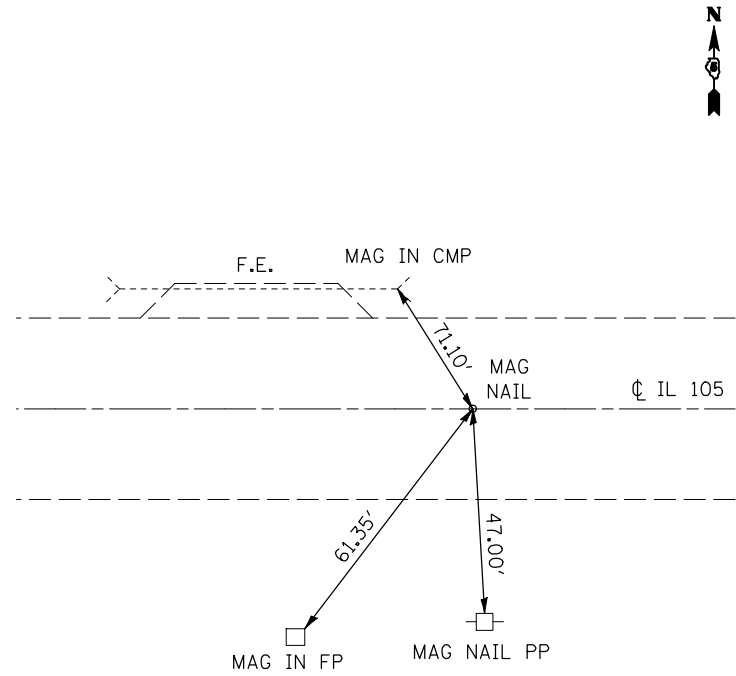
P.O.T. #3
STA. 457 + 44.29



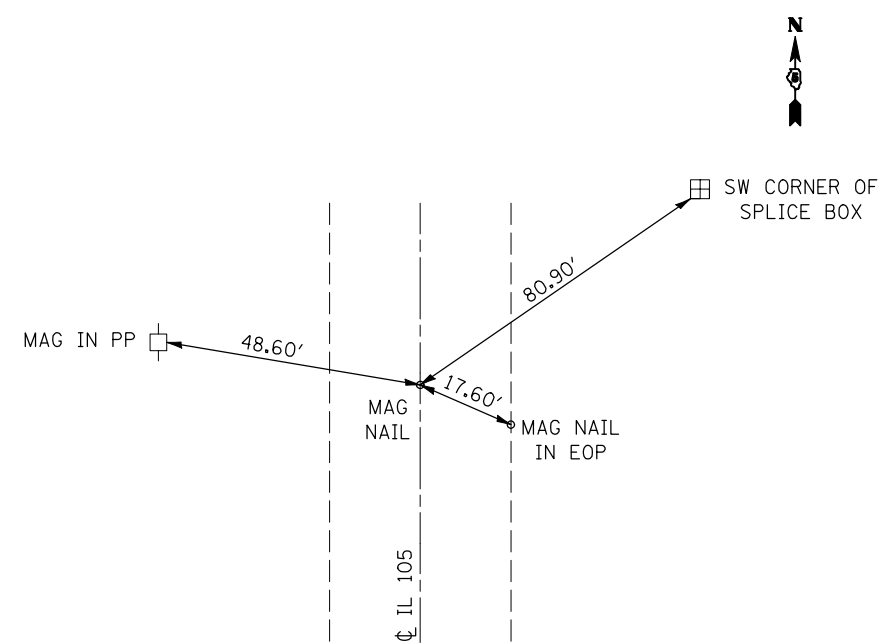
P.O.T. #4
STA. 620 + 49.06



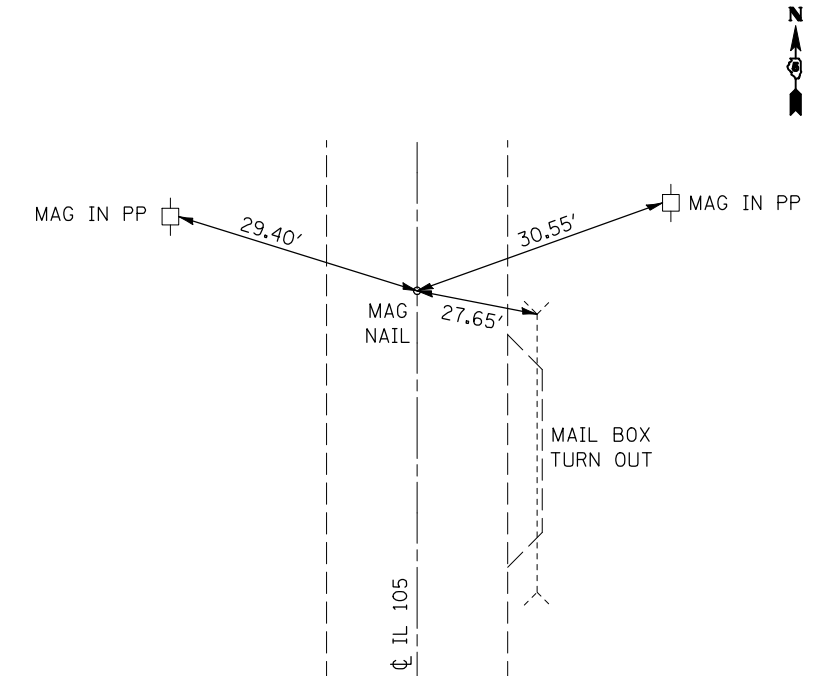
P.O.T. #6
STA. 632 + 93.39



P.O.T. #7
STA. 281 + 35.34



P.O.T. #11
STA. 261 + 08.36



F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	21

* 8RS-4&(9,10)RS-3

TABULATION OF RESURFACING QUANTITIES

LOCATION	TOTAL ROADWAY WIDTH	LENGTH	AREA	40600982	40600990	44000152	44000154	40600100	40600300	40600625	40603310	LOCATION
				HMA SURFACE REMOVAL BUTT JOINT	TEMPORARY RAMP	HMA SURFACE REMOVAL 0.75 IN	HMA SURFACE REMOVAL 1.25 IN	BITUMINOUS MATERIALS PRIME COAT	AGGREGATE MATERIALS PRIME COAT	LEVELING BINDER (MM) MIX C N 50 0.75 IN	HMA SURF CSE MIX C, N 50 IL 9.5, 1.5 IN	
				SQ YD	SQ YD	SQ YD	SQ YD	GALLON	TON	TON	TON	
STA. 383 + 70 TO 384 + 00	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	BEGINNING OF SECTION
STA. 384 + 00 TO 422 + 17	25.00	3,817.30	10,603.61			10,603.61		1,060.36	10.60	446.47	895.16	
STA. 420 + 82 TO 422 + 46	25.00	163.70	454.72			454.72		45.47	0.45	19.15	38.39	
STA. 422 + 46 TO 422 + 76	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	S.N. 074-0030 (NORTH SIDE)
STA. 423 + 44 TO 423 + 74	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	S.N. 074-0030 (SOUTH SIDE)
STA. 423 + 74 TO 452 + 58	25.00	2,883.80	8,010.56			8,010.56		801.06	8.01	337.28	676.25	
STA. 452 + 58 TO 458 + 74	31.00	616.20	2,122.47			2,122.47		212.25	2.12	89.32	179.01	
STA. 458 + 74 TO 459 + 00	37.00	26.00	106.89			106.89		10.69	0.11	4.50	9.01	
STA. 510 + 00 TO 510 + 89	37.00	89.00	365.89			365.89		36.59	0.37	15.39	30.84	
STA. 510 + 89 TO 514 + 98	34.00	409.00	1,545.11				1,545.11	154.51	1.55		130.27	BEMENT-HAMMOND ROAD
STA. 514 + 98 TO 515 + 88	37.00	89.60	368.36			368.36		36.84	0.37	15.50	31.05	(FAP 812)
STA. 515 + 88 TO 520 + 96	31.00	508.10	1,750.12			1,750.12		175.01	1.75	73.65	147.60	
STA. 520 + 96 TO 521 + 26	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	FAP 812
STA. 600 + 00 TO 600 + 55	38.00	54.60	230.53		21.11		230.53	23.05	0.23		19.43	T.R. 167 (700N)
STA. 460 + 72 TO 462 + 72	38.00	200.30	845.71				845.71	84.57	0.85		71.27	IL 105 (WEST LEG)
STA. 462 + 72 TO 468 + 80	33.00	607.80	2,228.60			2,228.60		222.86	2.23	93.78	187.91	
STA. 468 + 80 TO 549 + 73	25.00	8,092.30	22,478.61			22,478.61		2,247.86	22.48	946.46	1,897.64	
STA. 549 + 73 TO 550 + 03	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	S.N. 074-0080 (EAST)
STA. 550 + 84 TO 551 + 14	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	S.N. 074-0080 (WEST)
STA. 551 + 14 TO 579 + 13	25.00	2,799.05	7,775.14			7,775.14		777.51	7.78	327.37	656.38	
STA. 579 + 13 TO 579 + 43	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	S.N. 074-8061 (EAST)
STA. 580 + 05 TO 580 + 35	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	S.N. 074-8030 (WEST)
STA. 580 + 35 TO 645 + 64	25.00	6,528.79	18,135.53			18,135.53		1,813.55	18.14	763.60	1,531.00	
STA. 645 + 64 TO 645 + 94	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	S.N. 074-0079 (EAST)
STA. 646 + 86 TO 647 + 16	25.00	30.00	83.33	83.33	13.89			8.33	0.08	1.75	7.04	S.N. 074-0079 (WEST)
STA. 647 + 16 TO 738 + 10	25.00	9,094.25	25,261.81			25,261.81		2,526.18	25.26	1,063.65	2,132.60	
STA. 738 + 00 TO 885 + 17	25.00	14,716.82	40,880.06			40,880.06		4,088.01	40.88	1,721.25	3,451.09	
STA. 885 + 17 TO 888 + 47	33.00	329.68	1,208.83			1,208.83		120.88	1.21	50.87	101.93	
STA. 888 + 47 TO 889 + 22	41.00	75.50	343.94			343.94		34.39	0.34	14.47	28.98	
STA. 889 + 22 TO 892 + 40		318.00	1,438.62				1,438.62	143.86	1.44		121.22	CERRO GORDO INTERSECTION
STA. 892 + 40 TO 898 + 03	22.00	563.00	1,376.22				1,376.22	137.62	1.38		116.26	
STA. 898 + 03 TO 1032 + 44	25.00	13,441.30	37,336.94			37,336.94		3,733.69	37.34	1,572.07	3,151.98	
STA. 263 + 11 TO 263 + 41	26.00	30.00	86.67	86.67	14.44			8.67	0.09	1.82	7.32	END OF SECTION (IL 32)
STA. 263 + 41 TO 268 + 34	26.00	493.00	1,424.22			1,424.22		142.42	1.42	59.96	120.21	
STA. 268 + 34 TO 269 + 39	38.00	104.90	442.91			442.91		44.29	0.44	18.63	37.33	
STA. 269 + 39 TO 271 + 18	38.00	178.90	755.36				755.36	75.54	0.76		63.66	IL 105/32 INTERSECTION
STA. 271 + 18 TO 277 + 13	32.00	594.79	2,114.81			2,114.81		211.48	2.11	89.00	178.34	
STA. 48 + 87 TO 49 + 81		94.00	622.22		24.06			622.22	0.62		52.38	C.H. 12
STA. 50 + 18 TO 50 + 87		68.70	675.73					675.73	0.68		56.84	IL 105 (WEST)
STA. 50 + 87 TO 52 + 35	48.00	148.30	790.93			790.93		79.09	0.79	33.26	66.61	
STA. 52 + 35 TO 54 + 17	42.00	182.00	849.33			849.33		84.93	0.85	35.73	71.56	
STA. 54 + 17 TO 56 + 66	31.00	248.70	856.63			856.63		85.66	0.86	36.05	72.25	
STA. 56 + 66 TO 56 + 96	26.00	30.00	86.67	86.67	14.44			8.67	0.09	1.83	7.32	END OF SECTION (IL 105 W)
STA. 268 + 34 TO 272 + 34	38.00	400.00	1,688.89						1.69			
STA. 50 + 18 TO 51 + 18		100.00	807.88						0.81			
GRAND-TOTAL				1,007.0	213.0	185,911.0	7,489.0	19,441.0	197.0	7,849.0	16,409.0	

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	24

* 8RS-4&(9,10)RS-3

SCHEDULE OF QUANTITIES

SURVEY MONUMENT COVER ASSEMBLY (20070100)

STATION	OFFSET	EACH
567+74.00	14.5' RT	1.0
620+12.00	0.67' LT	1.0
672+82.00	1.25' RT	1.0
699+13.00	0.2' RT	1.0
725+43.00	1.3' RT	1.0
738+57.00	4.7' LT	1.0
905+94.00	2.1' LT	1.0
985+90.20	0.75' RT	1.0
270+34.41	2.29' LT	1.0
TOTAL =		9.0

MANHOLES TO BE ADJUSTED (60255500)

STATION	OFFSET	EACH
890+88.40 RT	4.5' RT	1.0
TOTAL =		1.0

WATER VALVES TO BE ADJUSTED (56109210)

STATION	OFFSET	EACH
891+33.00	10.3' RT	1.0
TOTAL =		1.0

TEMPORARY RAMP (40600990)

STATION	L (FT)	W (FT)	SQ YDS
383+70.00	5.0	25.0	13.9
422+76.00	5.0	25.0	13.9
423+44.00	5.0	25.0	13.9
521+25.70 (FAP 812)	5.0	25.0	13.9
600+54.60 (TR 167)	5.0	38.0	21.1
550+02.50	5.0	25.0	13.9
550+83.50	5.0	25.0	13.9
579+42.55	5.0	25.0	13.9
580+05.46	5.0	25.0	13.9
645+94.25	5.0	25.0	13.9
646+85.75	5.0	25.0	13.9
10+20.00 (JACKSON ST.)	5.0	97.0	53.9
101+32.00 (SOUTH ST.)	5.0	49.0	27.2
263+11.00 (IL 32)	5.0	26.0	14.4
48+87.00 (CH 12)	5.0	43.3	24.1
56+95.70	5.0	26.0	14.4
TOTAL =			294.0

*PLAN NOTE: TEMPORARY RAMPS SHALL BE USED AT LOCATIONS SHOWN IN THE PLANS. IT IS THE INTENTION TO USE TEMPORARY RAMPS FOR MAJOR SIDE ROADS. TEMPORARY RAMPS SHALL NOT BE USED AT MINOR TOWNSHIP ROADS, PRIVATE ENTRANCES, AND COMMERCIAL ENTRANCES.

HOT-MIX ASPHALT SURFACE REMOVAL, SPECIAL (44000196)

STATION	ENT. TYPE	SQ YDS
395+56 RT	PE	33.1
410+04 RT	PE	30.0
441+70 LT	CE	65.1
627+50 RT	PE / MBTO	56.5
635+50 LT	PE	28.3
791+05 RT	PE	26.8
792+17 RT	PE	43.3
816+73 LT	PE	63.8
889+50 LT	CE	61.8
891+66 LT	CE	31.7
892+10 LT	PE	25.2
892+87 RT	CE	84.4
893+15 LT	PE	22.8
893+50 RT	CE	96.3
893+85 LT	PE	25.0
894+22 LT	PE	28.1
894+95 RT	CE	74.7
895+40 LT	PE	40.2
895+52 RT	CE	79.6
896+00 RT	PE	37.9
896+66 LT	CE	59.0
900+53 LT	CE	71.7
901+29 RT	CE	157.9
901+35 LT	CE	67.8
TOTAL =		1,311.0

HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT (40600982)

STATION	STATION	L (FT)	W (FT)	SQ YDS
383+70.00	384+00.00	30.0	25.0	83.3
422+46.00	422+76.00	30.0	25.0	83.3
423+44.00	423+74.00	30.0	25.0	83.3
520+95.70	521+25.70	30.0	25.0	83.3
549+72.50	550+02.50	30.0	25.0	83.3
550+83.50	551+13.50	30.0	25.0	83.3
579+12.55	579+42.55	30.0	25.0	83.3
580+05.46	580+35.46	30.0	25.0	83.3
645+64.25	645+94.25	30.0	25.0	83.3
646+85.75	647+15.75	30.0	25.0	83.3
263+11.00	263+41.00	30.0	26.0	86.7
56+65.70	56+95.70	30.0	26.0	86.7
TOTAL =				1,007.0

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	27

* 8RS-4&(9,10)RS-3

ONE-WAY AMBER -

PAINT PAVEMENT MARKING - LINE 4" (78001110)

4" WHITE LINE - (OMIT AT SIDE ROADS FROM RAD. PT. TO RAD. PT.)

STATION	STATION	LOCATION	FEET
383+70.00	422+17.30 (BK)	LT EDGE	3,847.3
383+70.00	422+17.30 (BK)	RT EDGE	3,847.3
420+82.30 (AH)	459+00.00 (BK)	LT EDGE	3,817.7
420+82.30 (AH)	459+00.00 (BK)	RT EDGE	3,817.7
510+00.00 (AH)	510+83.00	LT EDGE	83.0
510+00.00 (AH)	510+83.00	RT EDGE	83.0
514+98.00	521+25.70	LT EDGE (FAP 812)	627.7
514+98.00	521+25.70	RT EDGE (FAP 812)	627.7
462+72.00	738+10.00 (BK)	LT EDGE	27,538.0
462+72.00	738+10.00 (BK)	RT EDGE	27,538.0
738+00.00 (AH)	888+97.00	LT EDGE	15,097.0
738+00.00 (AH)	888+06.50	RT EDGE	15,006.5
898+03.00	1027+44.30	LT EDGE	12,941.3
898+03.00	1027+44.30	RT EDGE	12,941.3
TOTAL =			127,814.0

STATION	STATION	LOCATION	EACH
458+74.00	512+42.00	MEDIAN (N)	16.0
513+11.00	515+90.00	MEDIAN (FAP 812)	16.0
600+28.50	601+45.20	MEDIAN (TR 167)	8.0
461+01.00	463+74.00	MEDIAN (W)	16.0
271+83.00	277+12.59 (AH)	MEDIAN (N)	27.0
263+85.00	268+34.00	MEDIAN (S)	23.0
47+94.00	49+69.00	MEDIAN (CH 12)	10.0
50+41.00	54+50.00	MEDIAN (IL 105 W)	22.0
TOTAL =			138.0

ONE-WAY CRYSTAL -

STATION	STATION	LOCATION	EACH
270+85.00	271+83.00	LT TURN LANE	4.0
268+34.00	269+82.00	LT TURN LANE	5.0
50+36.00	52+35.00	RT TURN LANE	6.0
TOTAL =			15.0

4" YELLOW LINE -

STATION	STATION	LOCATION	FEET
383+70.00	422+17.30 (BK)	SKIP-DASH	961.8
420+82.30 (AH)	458+74.00	SKIP-DASH	947.9
515+90.00	521+25.70	SKIP-DASH	133.9
463+74.00	738+10.00 (BK)	SKIP-DASH	6,859.0
487+49.40	493+83.00	SOLID (WB)	633.6
498+05.40	503+86.20	SOLID (EB)	580.8
738+00.00 (AH)	890+32.30	SKIP-DASH	3,808.1
891+50.00	1027+44.30	SKIP-DASH	3,398.6
TOTAL =			17,324.0

RAISED REFLECTIVE PAVEMENT MARKER (78100100)

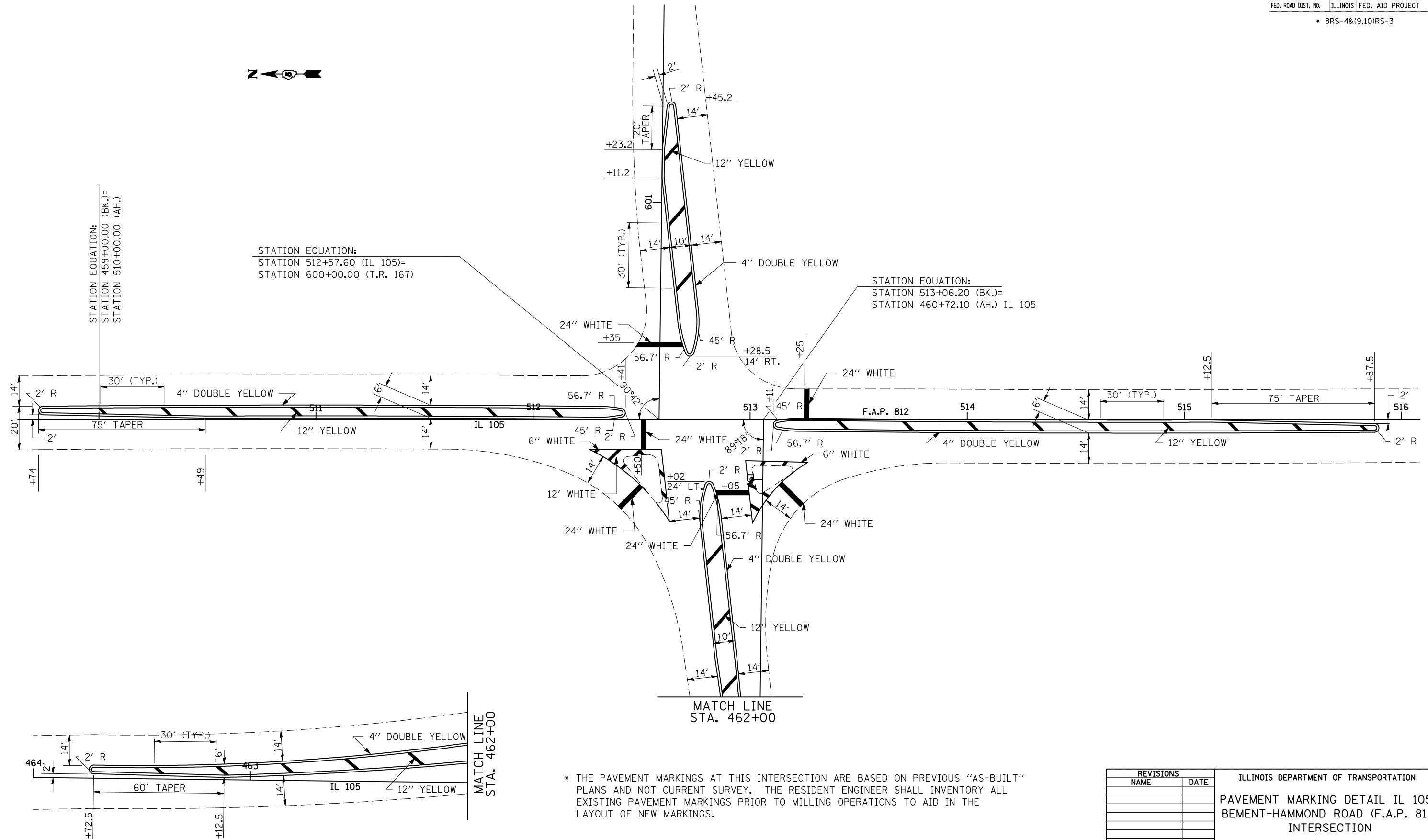
TWO-WAY AMBER -

STATION	STATION	LOCATION	EACH
383+70.00	422+17.30 (BK)	CENTERLINE	48.0
420+82.30 (AH)	458+74.00	CENTERLINE	47.0
515+90.00	521+25.70	CENTERLINE	7.0
463+74.00	738+10.00 (BK)	CENTERLINE	343.0
738+00.00 (AH)	890+32.30	CENTERLINE	190.0
891+50.00	1032+44.30 (BK)	CENTERLINE	182.0
263+11.00	263+85.00	DBL YELLOW	6.0
268+34.00	269+82.00	DBL YELLOW	8.0
270+85.00	271+83.00	DBL YELLOW	6.0
54+50.00	56+95.70	DBL YELLOW	7.0
TOTAL =			844.0

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 PLOT SCALE = 4.23629' / IN.
 USER NAME = cearlockjd

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	28
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* 8RS-4&(9,10)RS-3



* THE PAVEMENT MARKINGS AT THIS INTERSECTION ARE BASED ON PREVIOUS "AS-BUILT" PLANS AND NOT CURRENT SURVEY. THE RESIDENT ENGINEER SHALL INVENTORY ALL EXISTING PAVEMENT MARKINGS PRIOR TO MILLING OPERATIONS TO AID IN THE LAYOUT OF NEW MARKINGS.

REVISIONS	
NAME	DATE

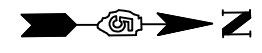
ILLINOIS DEPARTMENT OF TRANSPORTATION
 PAVEMENT MARKING DETAIL IL 105/
 BEMENT-HAMMOND ROAD (F.A.P. 812)
 INTERSECTION

SCALE: VERT. _____
 HORIZ. _____
 DATE _____ DRAWN BY _____
 CHECKED BY _____

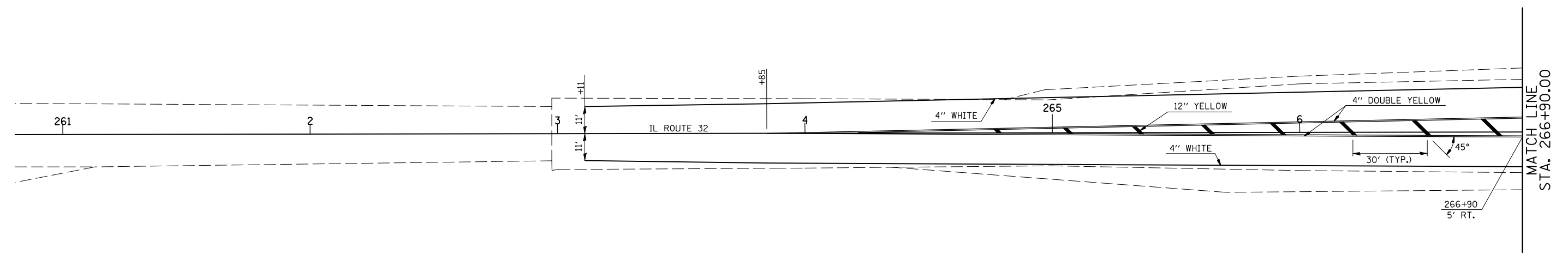
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 PLOT SCALE = 42.3529 / IN.
 USER NAME = cearlock_jd

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	29
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* 8RS-4&(9,10)RS-3



SEC. 15, T-16-N, R-4-E, 3rd P.M.



SEC. 14, T-16-N, R-4-E, 3rd P.M.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 PAVEMENT MARKING DETAIL
 IL 105/32 INTERSECTION

SCALE: VERT. _____
 HORIZ. _____
 DATE _____ DRAWN BY _____
 CHECKED BY _____

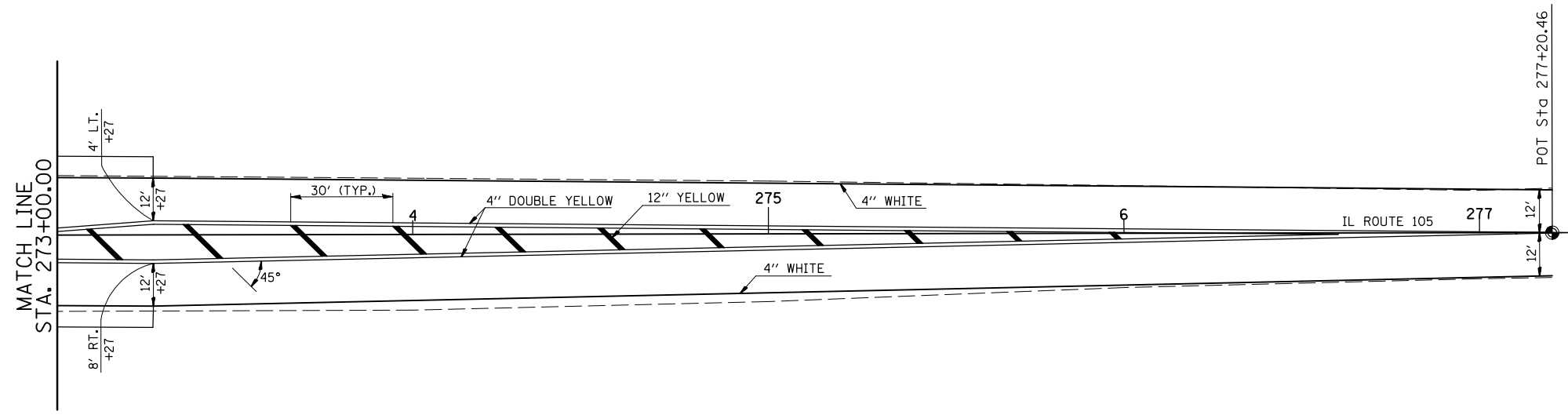
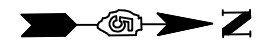
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SEC. 10, T-16-N, R-4-E, 3rd P.M.

CONTRACT NO. 70129

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	31
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

* 8RS-4&(9,10)RS-3



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SEC. 11, T-16-N, R-4-E, 3rd P.M.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING DETAIL
 IL 105/32 INTERSECTION

SCALE: VERT. _____
 HORIZ. _____

DATE _____ DRAWN BY _____
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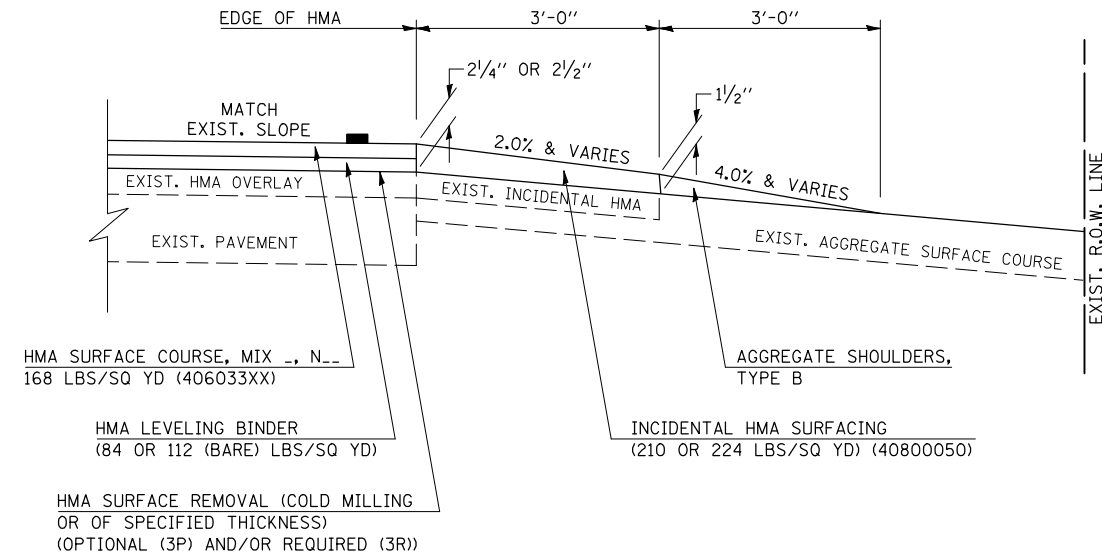
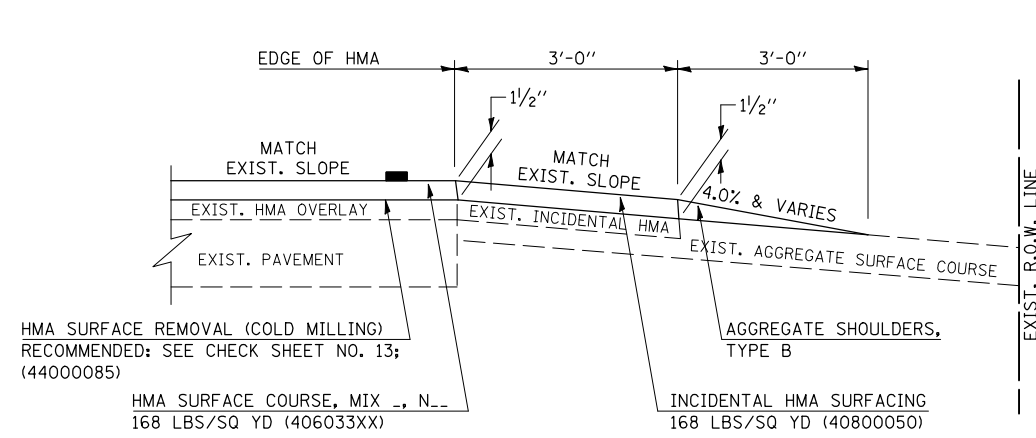
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	38

PROJECTS WITHOUT RECONSTRUCTION

("3R" WITHOUT RECONSTRUCTION, 3P, SMART AND CM)

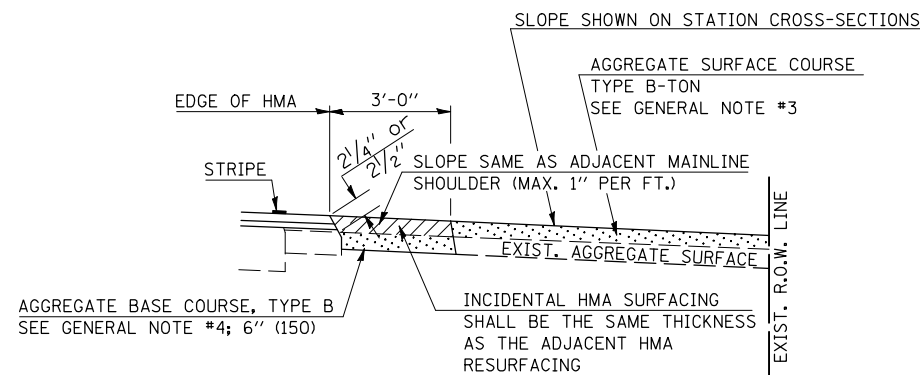
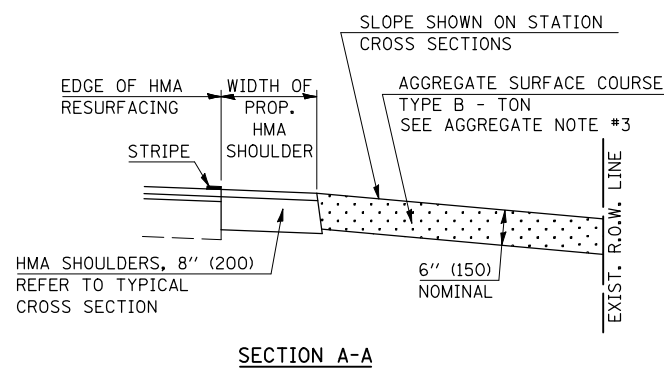
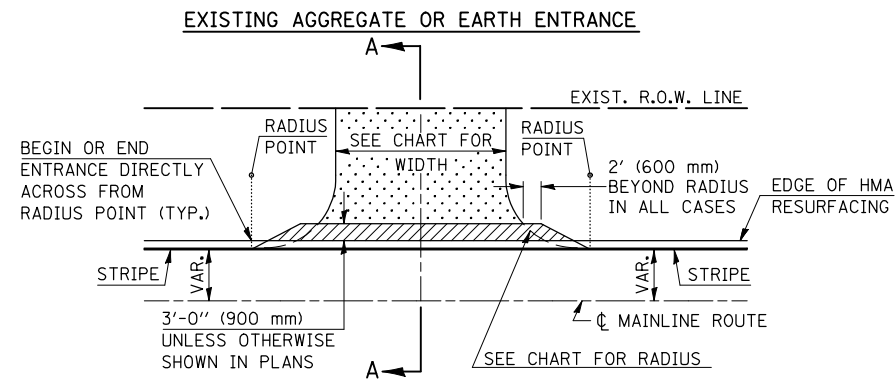
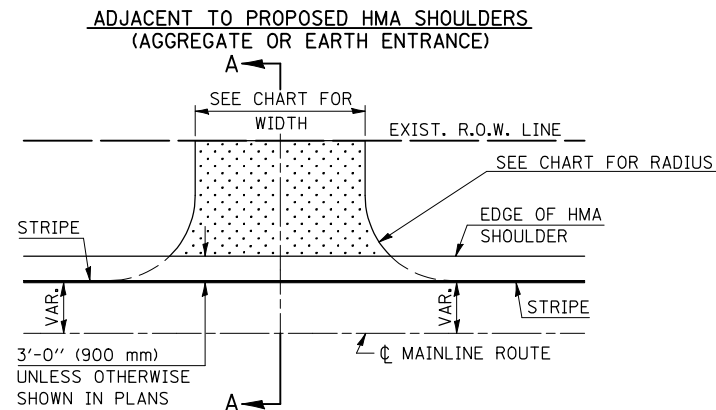
S.M.A.R.T. IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.03; 1/2")

"3P" OR "3R" IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.02; 2/4" OR 2/2" ON BARE CONCRETE)



PROJECTS WITH RECONSTRUCTION

("3R" IMPROVEMENTS AND SMART/3P "SPOT" LOCATIONS)



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
12-01-06	RENUMBERED/COMBINED C-1.32 & C-1.26 WITH ADDITIONS OF S.M.A.R.T. & 3P & 3R IMPROV.	T.J.B.

ILLINOIS DEPARTMENT OF TRANSPORTATION

FIELD ENTRANCES (NONCOMMERCIAL RURAL)

DISTRICT 5 DETAIL NO. 40800050A

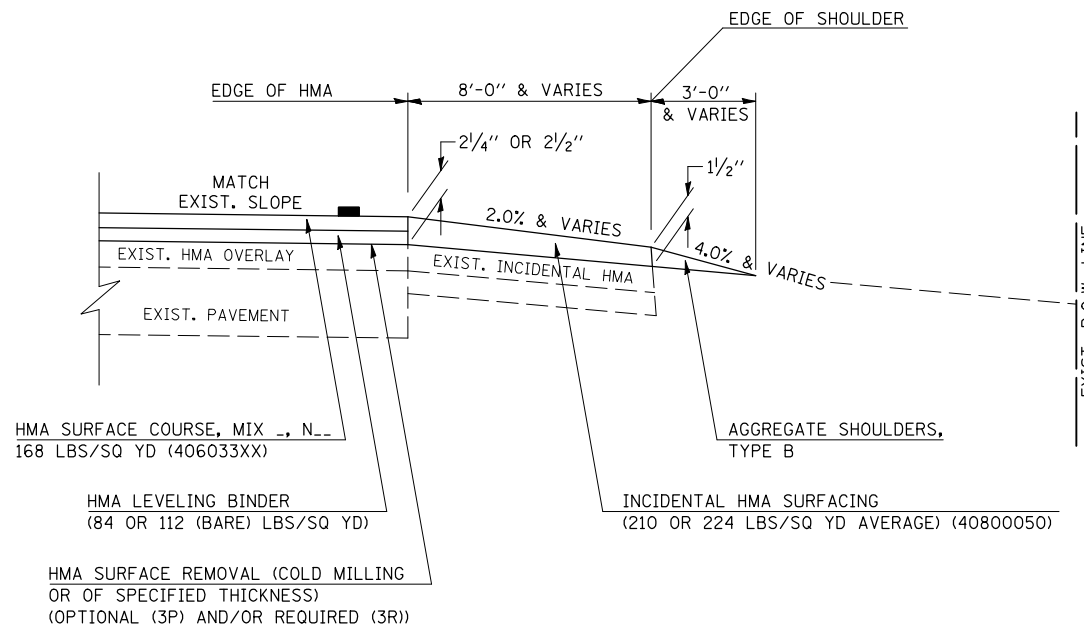
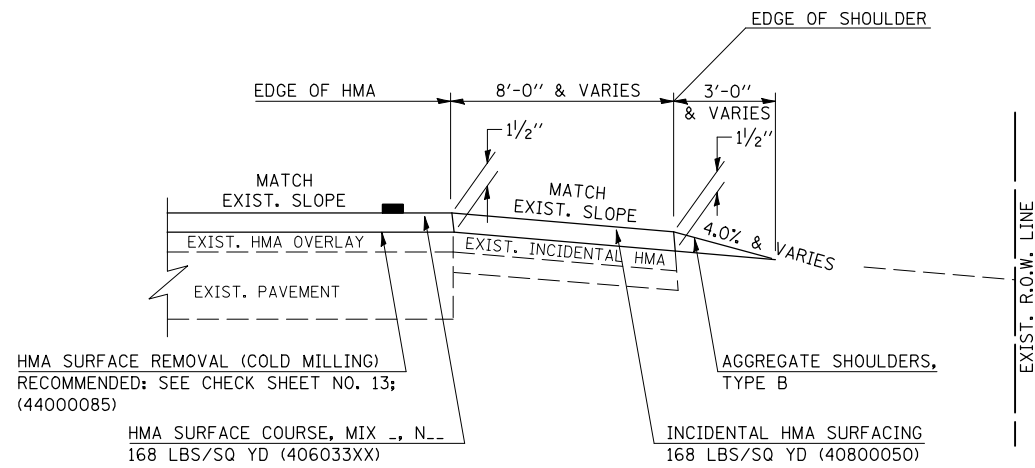
PLOT DATE = 1/22/2009
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USER NAME = ceerlock_jd

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	40

PROJECTS WITHOUT RECONSTRUCTION

S.M.A.R.T. IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.03; 1 1/2")

"3P" OR "3R" IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.02; 2 1/4" OR 2 1/2" ON BARE CONCRETE)



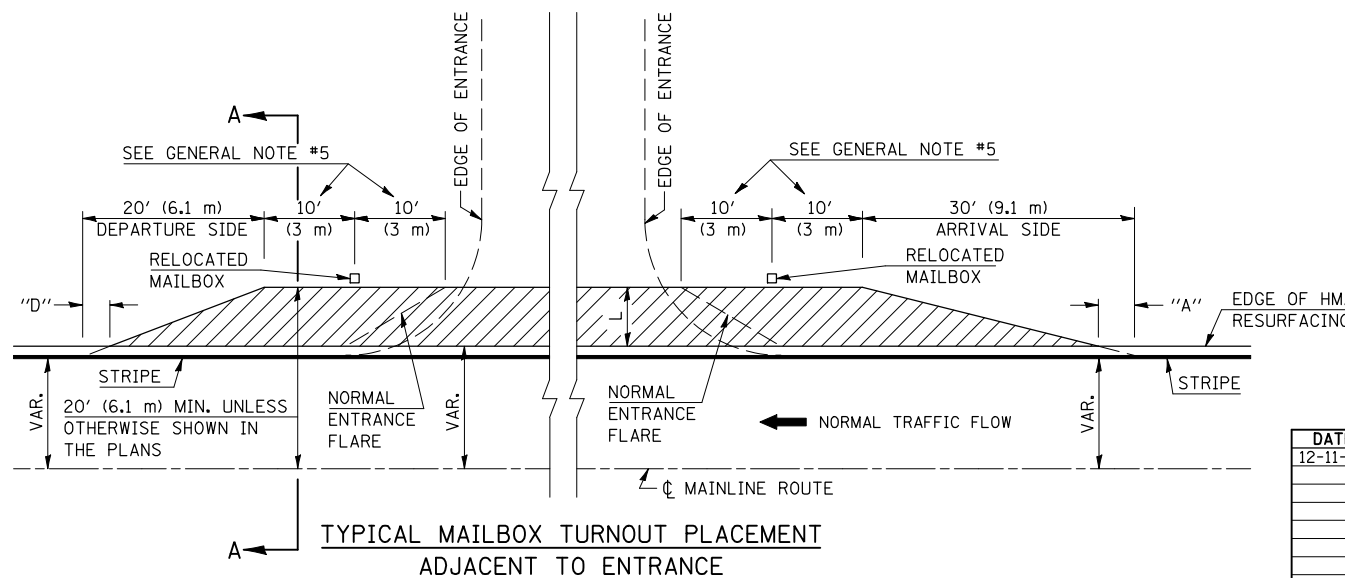
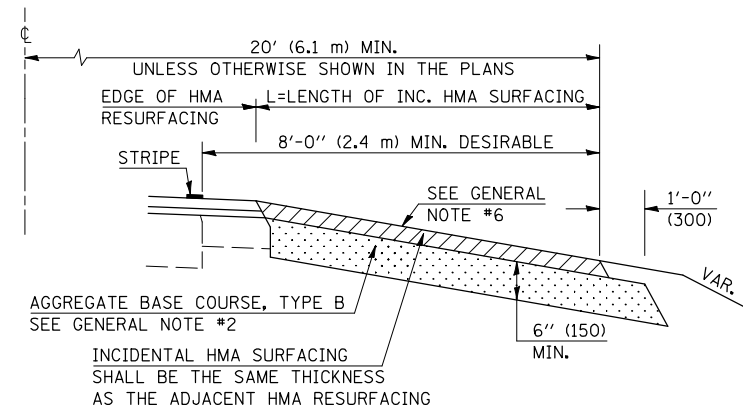
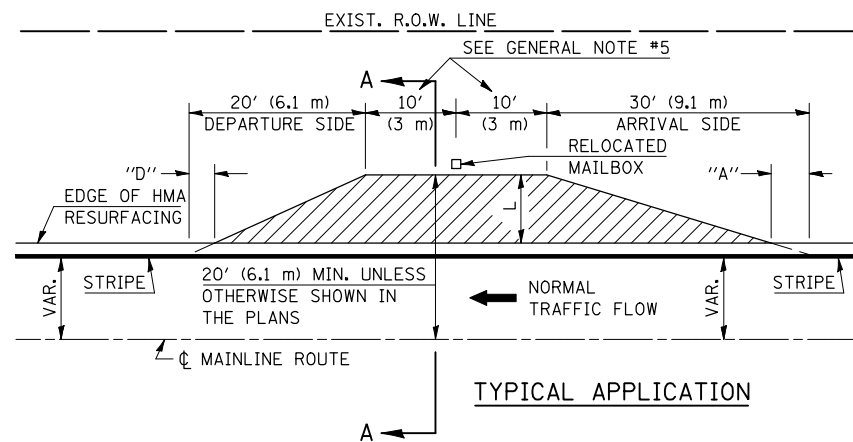
GENERAL NOTES

1. THE EXISTING SURFACE SHALL BE PREPARED IN ACCORDANCE WITH SECTION 408 OF THE STANDARD SPECIFICATIONS.
2. AGGREGATE BASE COURSE, TYPE B, 6" (150) MIN. SHALL BE USED WHERE IN THE OPINION OF THE ENGINEER THERE IS NOT SUFFICIENT BASE MATERIAL FOR THE PROPOSED MAILBOX TURNOUTS. THIS MATERIAL SHALL GENERALLY BE USED TO WIDEN ALL EXISTING MAILBOX TURNOUTS OR TO CONSTRUCT NEW MAILBOX TURNOUTS WHERE NONE NOW EXISTS.
3. ANY NECESSARY WORK BEHIND THE INCIDENTAL HMA SURFACING SHALL BE AS SHOWN IN THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
4. THE TEMPORARY RELOCATION OF EXISTING MAILBOXES SHALL BE IN ACCORDANCE WITH ARTICLE 107.20 OF THE STANDARD SPECIFICATIONS.
5. WHEN MORE THAN ONE RELOCATED MAILBOX IS INCLUDED IN A PARTICULAR LOCATION THE TWO 10' (3 m) DIMENSIONS AS SHOWN ABOVE SHALL BE FROM THE END MAILBOX.
6. CROSS SLOPE SHALL BE AS SHOWN ON THE STATION CROSS SECTIONS AND/OR AS DIRECTED BY THE ENGINEER. MINIMUM 4% (1/2"/') DESIRABLE; MAXIMUM 8% (1"/')
7. WHEN MAILBOX TURNOUTS ARE CONSTRUCTED ADJACENT TO FIELD ENTRANCES, THE WIDTH OF THE INCIDENTAL HMA SURFACING CONSTRUCTED FOR THE FIELD ENTRANCE SHALL MATCH THE WIDTH OF THE PROPOSED MAILBOX TURNOUT SURFACING.
8. THE TOTAL SHOULDER WIDTH, 2.4 m (8') MINIMUM, SHALL BE PAVED BETWEEN SIDEROADS ENTRANCES AND/OR MAILBOX TURNOUTS AT LOCATIONS WHERE THE DISTANCE BETWEEN RADIUS OR TAPER CONTROL POINTS IS LESS THAN 15.0 m (50').
9. MAILBOXES SHALL BE MOUNTED SUCH THAT THE FACE OF THE MAILBOX IS 150 (6) TO 300 (12) AND THE POST A MINIMUM OF 600 (24) FROM THE EDGE OF THE TURNOUT SURFACING.

DISTANCE FROM STRIPE TO HMA RESURFACING	"D"	DISTANCE FROM STRIPE TO HMA RESURFACING	"A"
1.0' (300 mm)	2.5'	1.0' (300 mm)	3.8'
1.5' (450 mm)	3.8'	1.5' (450 mm)	5.6'
2.0' (600 mm)	5.0'	2.0' (600 mm)	7.5'
2.5' (750 mm)	6.3'	2.5' (750 mm)	9.4'
3.0' (900 mm)	7.5'	3.0' (900 mm)	11.3'

PROJECTS WITH RECONSTRUCTION

("3R" IMPROVEMENTS)



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
12-11-06	RENUMBERED/COMBINED C-1.25 WITH ADDITIONS OF S.M.A.R.T & 3P & 3R IMPROVEMENTS	T.J.B.

ILLINOIS DEPARTMENT OF TRANSPORTATION

MAILBOX TURNOUT (RURAL)

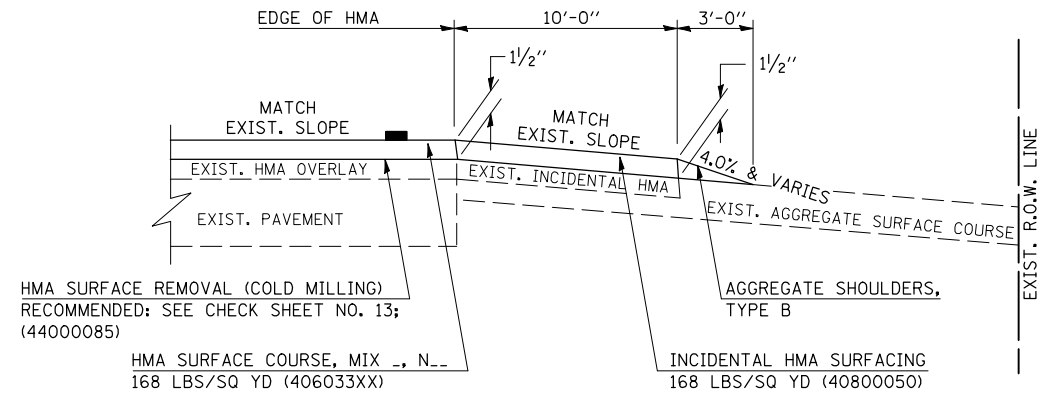
DISTRICT 5 DETAIL NO. 40800050B

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	41

* 8RS-4&(9,10)RS-3

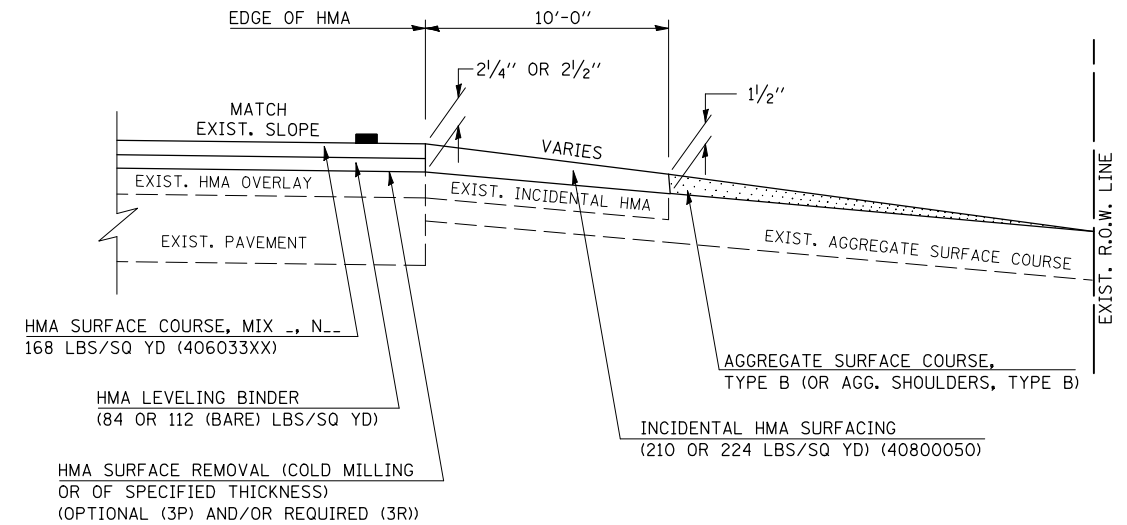
PROJECTS WITHOUT RECONSTRUCTION

S.M.A.R.T. IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.03; 1/2")

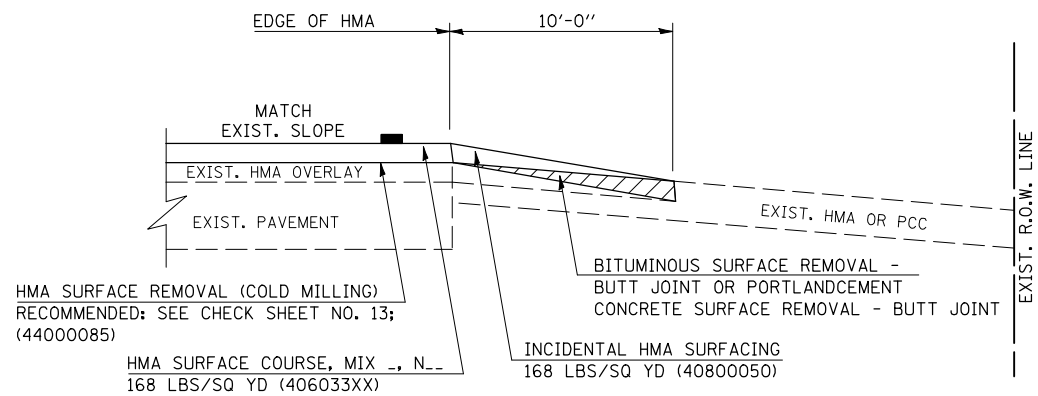


EXISTING AGGREGATE ENTRANCE

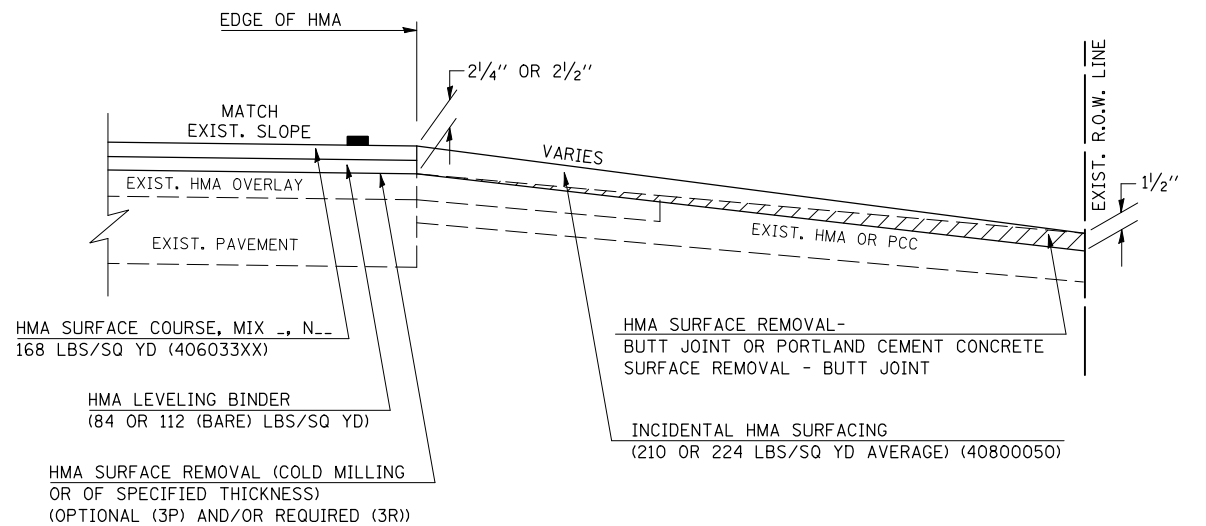
"3P" OR "3R" IMPROVEMENTS
(POLICY RESURFACING; BDE 53-4.02; 2/4" OR 2/2" ON BARE CONCRETE)



EXISTING AGGREGATE ENTRANCE



EXISTING HMA OR PCC ENTRANCE



EXISTING HMA OR PCC ENTRANCE

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

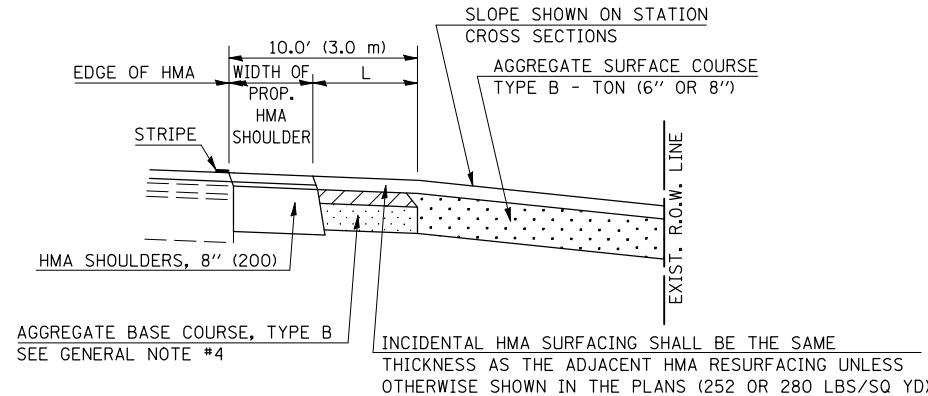
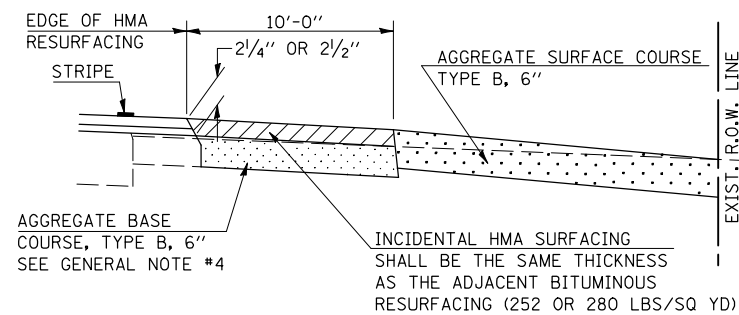
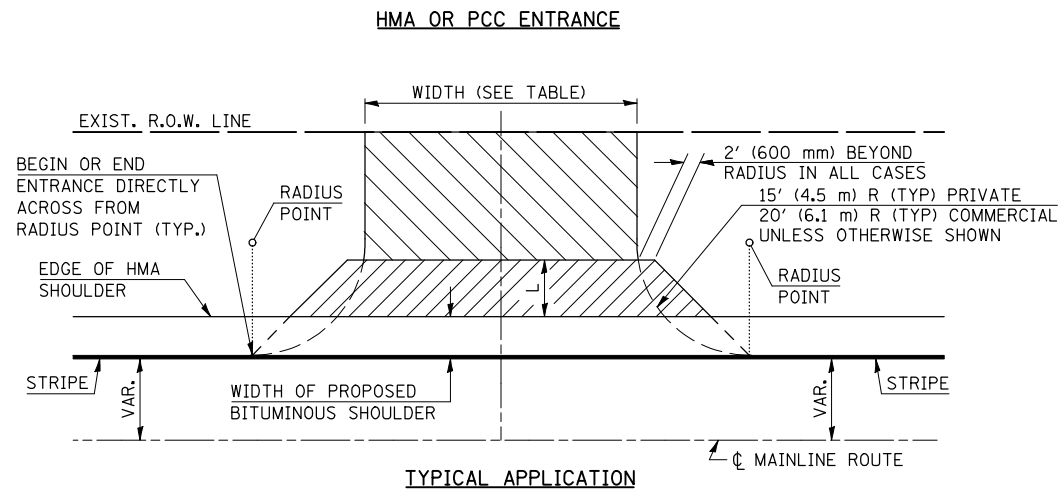
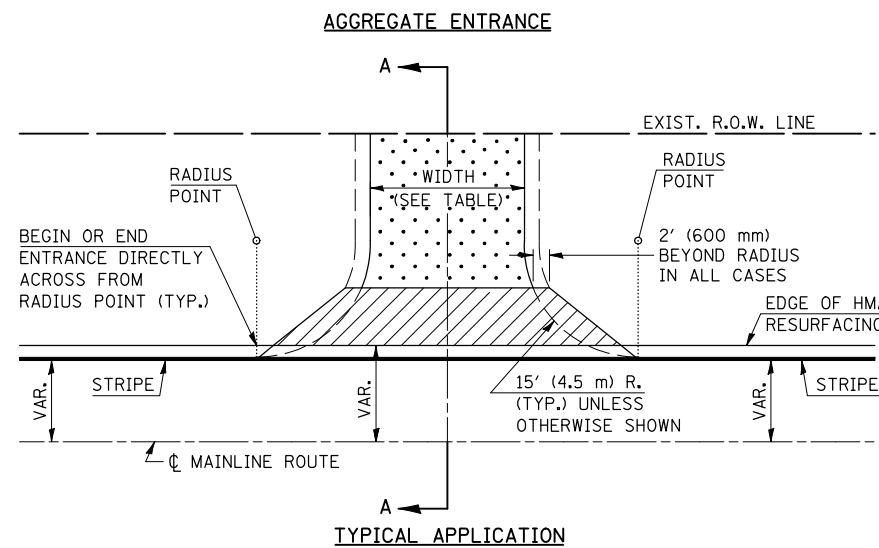
DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
12-08-06	RENUMBERED/COMBINED C-1.24 & C-1.33 WITH ADDITIONS OF S.M.A.R.T. & 3P & 3R IMPROV.	T.J.B.	PRIVATE AND COMMERCIAL ENTRANCES (NONCOMMERCIAL AND COMMERCIAL RURAL)
			DISTRICT 5 DETAIL NO. 40800050C

PLOT DATE = 1/22/2009
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USER NAME = ceerlockjd

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	42

PROJECTS WITH RECONSTRUCTION

(3R IMPROVEMENTS)



GENERAL NOTES

1. THE EXISTING SURFACE SHALL BE PREPARED IN ACCORDANCE WITH SECTION 408 OF THE STANDARD SPECIFICATIONS.
2. ANY NECESSARY WORK BEHIND THE HMA SHOULDER OR THE INCIDENTAL HMA SURFACING SHALL BE AS SHOWN IN THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.
3. EARTH EXCAVATION REQUIRED FOR THE CONSTRUCTION OF THE AGGREGATE SURFACE COURSE SHALL BE INCLUDED IN THE COST OF AGGREGATE SURFACE COURSE.
4. AGGREGATE BASE COURSE, TYPE B, 6" (150) MIN. SHALL BE USED WHERE IN THE OPINION OF THE ENGINEER THERE IS NOT SUFFICIENT BASE MATERIAL FOR THE PROPOSED ENTRANCES. THIS MATERIAL SHALL GENERALLY BE USED TO WIDEN ANY EXISTING RETURN OR TO CONSTRUCT NEW ENTRANCES WHERE NONE NOW EXISTS.
5. THE AGGREGATE BASE COURSE SHALL BE CONSTRUCTED 1' (0.3 m) WIDER THAN THE SURFACE DIMENSIONS AS SHOWN ABOVE.
6. EXISTING FIELD ENTRANCES OF AGGREGATE OR EARTH WITH NO HMA APRON SHALL NOT RECEIVE A NEW HMA APRON WITHOUT PROPER APPROVAL THROUGH THE BUREAU OF OPERATIONS "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS".
7. TO ASSURE APPROPRIATE ACCESS POLICIES ARE FOLLOWED ALL NEW ACCESS SHALL BE APPLIED FOR THROUGH THE BUREAU OF OPERATIONS PERMIT APPLICATION PROCESS. PLAN PREPARATION MEMORANDUMS 40-09 AND 40-11 ALONG WITH DISTRICT CONSTRUCTION MEMORANDUM 03/14 DISCUSS THIS PROCEDURE.

RURAL ENTRANCE DESIGN STANDARDS (PPM 40-09)															
DESIGN ELEMENT	NEW CONSTRUCTION & 3R with RECONSTRUCTION						3R w/out RECONSTRUCTION, 3P, SMART & CM								
	NONCOMMERCIAL			FIELD W/ FARM IMPLEMENTS			COMMERCIAL			NONCOMMERCIAL			COMMERCIAL		
	PRIVATE & FIELD														
	min.	des.	max.	min.	max.	min.	des.	max.	min.	des.	max.	min.	des.	max.	
SURFACE WIDTH (FT)	12	16	24	24	30	1 LANE, 1 WAY			1 LANE, 1 WAY			1 LANE, 1 WAY			
						14	16	24							
RADIUS (FT)	15	25	40	30		2 LANE, 2 WAY			2 LANE, 2 WAY			2 LANE, 2 WAY			
						24	30	35							
SHOULDER WIDTH (FT)	2	2		2		1	3		resurface existing configuration; existing hma or pcc entrances shall have "butt joints" constructed; existing aggregate or earth entrances shall have the continuation of aggregate shoulders placed behind them						
SHOULDER SLOPE (%)	2	4	6	4		2	4	6							
ENTRANCE GRADE (%)	0	2 to 5	10 or 12	2 to 5	10 or 12	0	2 to 5	8 or 10							
SIDE SLOPE (FT)	4:1	6:1	10:1	4:1	6:1	4:1	6:1	10:1							
SURFACE TYPE															
INCIDENTAL HMA SURFACING (INCH)		2		2		3 or 4			taper from hma resurfacing thickness (2 1/2", 2 1/4" or 1 1/2") to 1 1/2" for "butt joints" and to minimize aggregate shoulder						
AGGREGATE SURFACE COURSE, TYPE B (INCH)		6		6		8			if applicable use items: Preparation of Base & Aggregate Base Repair; see PPM 30-02						
PCC DRIVEWAY PAVEMENT (INCH)		6						6 or 8							

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

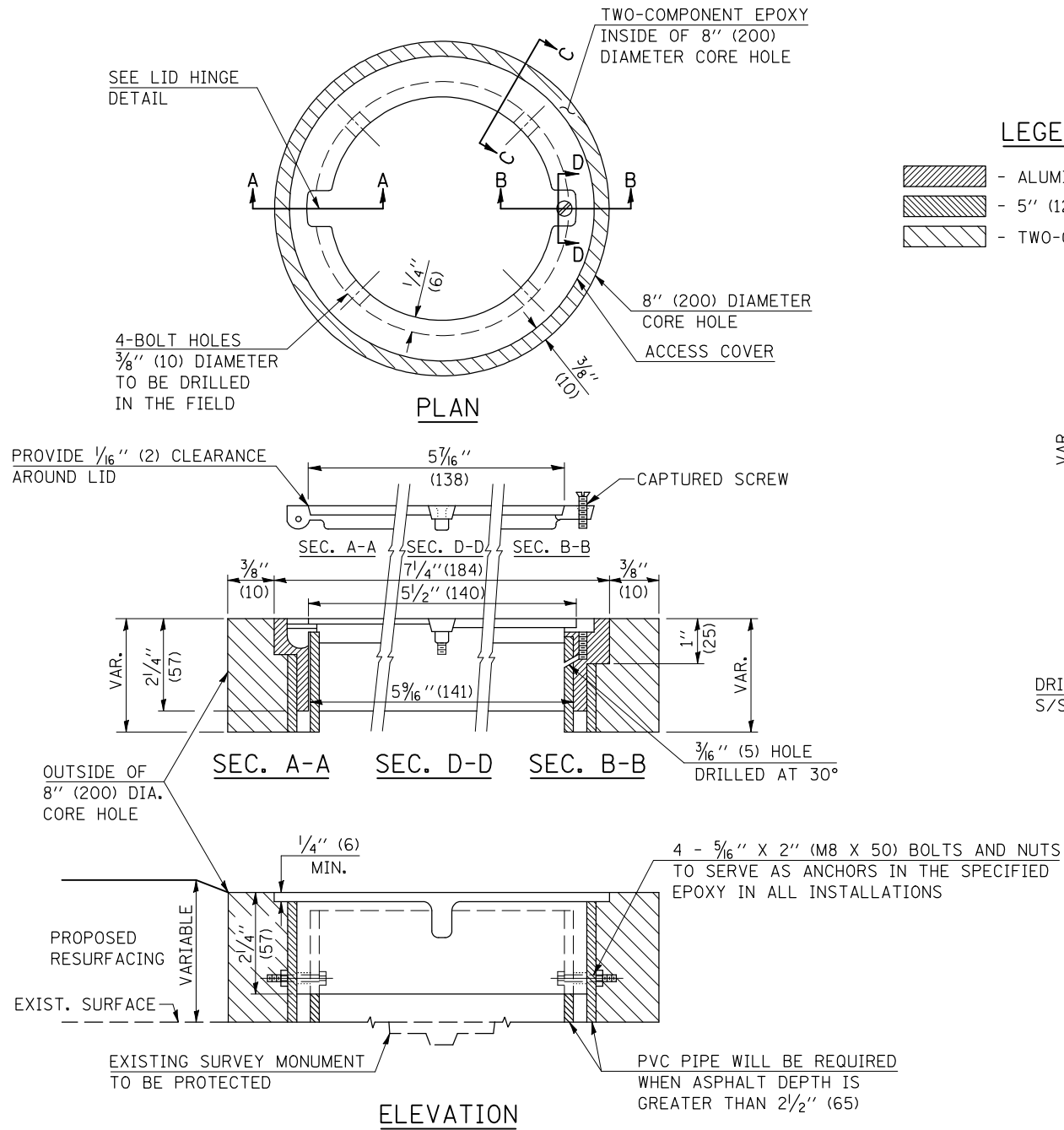
DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
12-08-06	RENUMBERED/COMBINED C-1.24 & C-1.33 WITH ADDITIONS OF S.M.A.R.T. & 3P & 3R IMPROV.		PRIVATE AND COMMERCIAL ENTRANCES (NONCOMMERCIAL AND COMMERCIAL RURAL)
			DISTRICT 5 DETAIL NO. 40800050C

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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	43

* 8RS-4&(9,10)RS-3

TO BE INSTALLED IN ALL PAVEMENT TYPES FOR PRESERVING PERMANENT SURVEY MARKERS (PI'S, PT'S, PC'S, POC'S, & POT'S) AND LAND SURVEY MONUMENTS (SECTION OR SUBSECTION CORNERS)



LEGEND

-  - ALUMINUM CASTING
-  - 5" (125) OR 6" (150) P.V.C. PIPE
-  - TWO-COMPONENT EPOXY

SPECIFICATIONS FOR ACCESS COVER FOR USE WITH SURVEY MARKER VAULT(S) AND SURVEY MARKER COVER ASSEMBLY(S): THE ACCESS COVER WILL BE CAST FROM A SPECIAL ALUMINUM ALLOY THAT IS COMPARABLE TO BRONZE IN HARDNESS. THE ACCESS COVER SHALL BE SPECIALLY ENGINEERED AND DESIGNED TO PROVIDE A SNUG FIT, INCORPORATING EQUIDISTANT LOCKING RIDGES, INSIDE A STANDARD 6" (150 mm) DIAMETER, OR OUTSIDE A STANDARD 5" (125 mm) DIAMETER, SCHEDULE 40 PVC PIPE. THE ACCESS COVER SHALL HAVE SPECIAL UNIFORM 1" (25 mm) THICK TOP SURFACE TO PERMIT INFORMATION TO BE EASILY MACHINE-STAMPED INTO IT. THE ACCESS COVER SHALL INCLUDE A STAINLESS CAPTURED SCREW AND AN OPPOSING RECESSED HINGE ASSEMBLY AS ITS LOCKING MECHANISM. THE ACCESS COVER SHALL INCORPORATE A SPECIAL ACCESS HOLE FOR CLEANING AND DRAINAGE, DRILLED AT 30° INSIDE THE RING OF THE ACCESS COVER, TO THE DRILLED AND TAPPED HOLE PROVIDED FOR THE STAINLESS CAPTURED SCREW. COMPOSITION: ALUMINUM 92-93%; MAGNESIUM 6.5-7.5%. STRENGTH: YIELD - 19,000-21,000 PSI (131-145 MPa); TENSILE - 38,000-44,000 PSI (262-303 MPa); ELONGATION - 10-15% IN 2" (50 mm). SPECIFICATIONS: ALLOY 535.0; QQ-A-601Es. NO EXCEPTIONS.

BILL OF MATERIAL

ALUMINUM CASTING OF THE DIMENSIONS AND SPECIFICATIONS SHOWN OR OTHER SUBJECT TO ENGINEER'S APPROVAL OF SHOP DRAWINGS, 4 EACH - 5/16" X 2" (M8 X 50) BOLTS WITH NUTS, EPOXY, 5" OR 6" (125 mm OR 150 mm) DIAMETER P.V.C. PIPE, SCHEDULE 40 (WHEN REQUIRED).

GENERAL NOTES

1. WORK SHALL NOT START ON THIS ITEM UNTIL THE FINAL LIFT OF SURFACE HAS BEEN COMPLETED.
2. THE SURVEY MONUMENT COVER ASSEMBLY SHALL BE CENTERED ABOVE THE SURVEY MONUMENT TO BE PROTECTED.
3. MODIFICATION OF THE ALUMINUM CASTING SHALL BE DONE BY GRINDING OR SAWING WHEN HEIGHT REDUCTION IS REQUIRED.
4. ALL SURVEY MONUMENT COVER ASSEMBLIES SHALL BE PLACED 1/4" (6 mm) ± BELOW THE FINAL SURFACE.
5. ALUMINUM CASTING SHALL BE PLACED OVER A 5" (125 mm) P.V.C. PIPE OR INSIDE OF A 6" (150 mm) P.V.C. PIPE WHEN AN INCREASE IN HEIGHT IS REQUIRED.
6. THE CASTING SHALL BE ANCHORED IN THE 8" (200 mm) DIAMETER CORE HOLE WITH TWO-COMPONENT EPOXY CONFORMING TO APPLICABLE PORTIONS OF ARTICLE 1025.01 OF THE STANDARD SPECIFICATIONS.
7. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR SURVEY MONUMENT COVER ASSEMBLY WHICH PRICE SHALL INCLUDE ALL LABOR AND MATERIAL AS SPECIFIED INCLUDING CORING THE NEW PAVEMENT SURFACE AND EPOXY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
8. THE 8" (200 mm) DIAMETER CORE HOLE SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

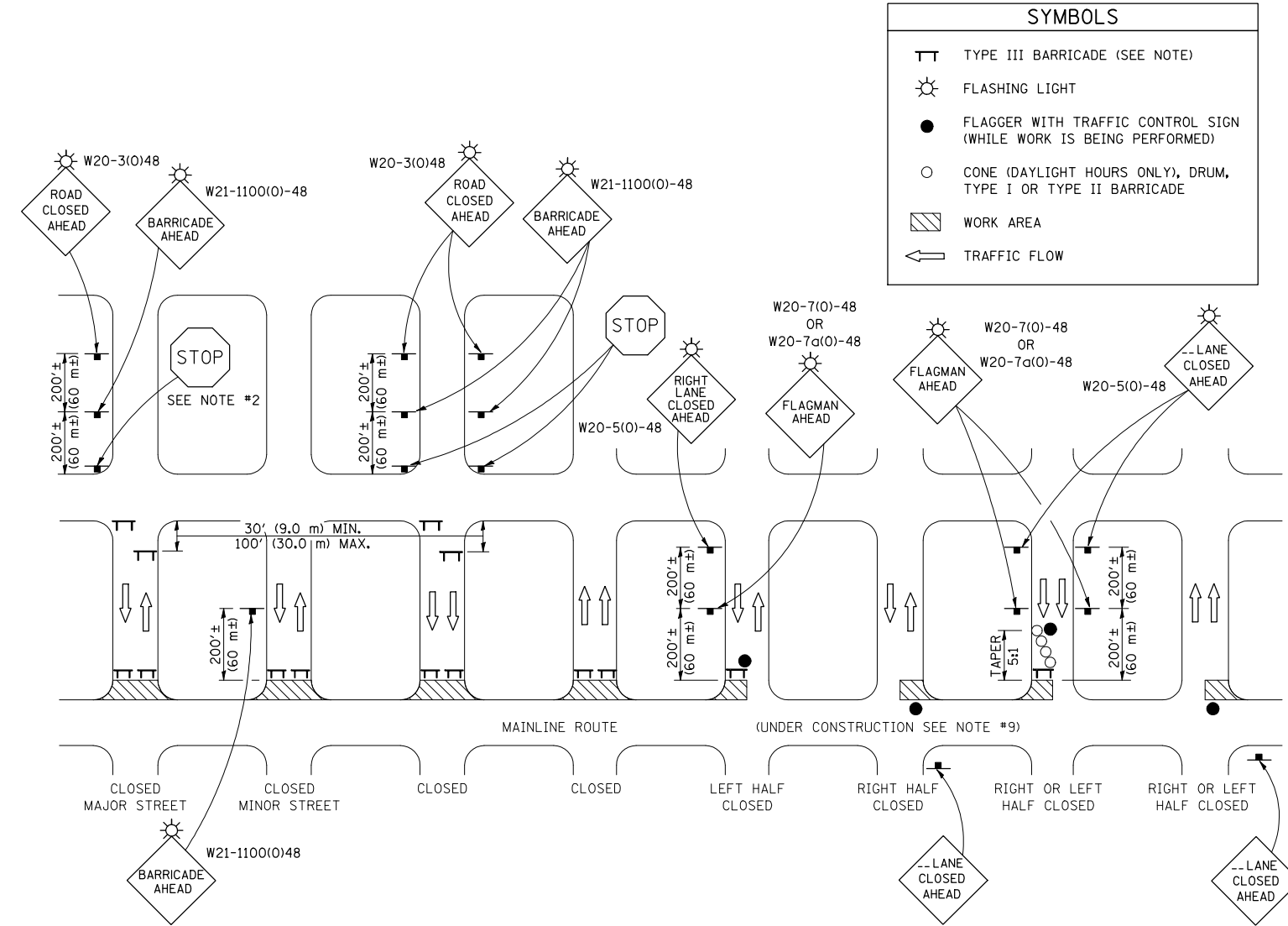
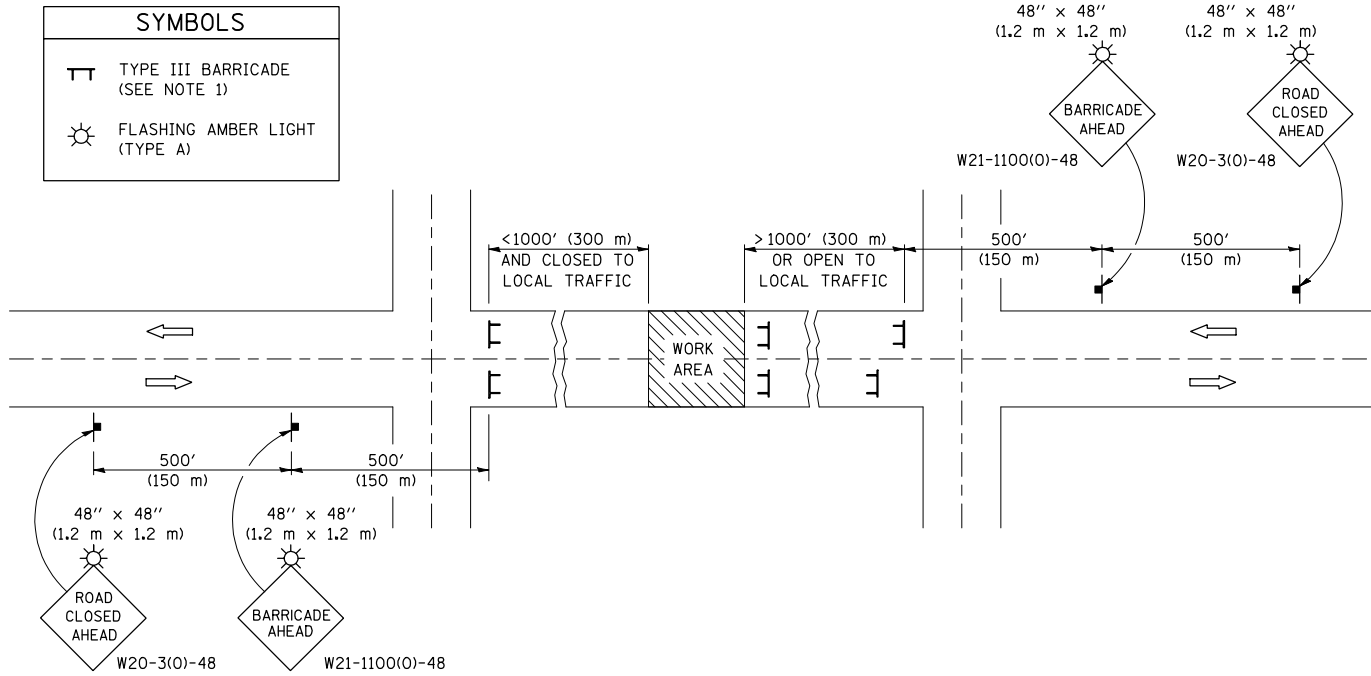
DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
11/06	REPLACED DETAIL D-1.04	TJB	SURVEY MONUMENT COVER ASSEMBLY
			DISTRICT 5 DETAIL NO. Z0070100

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	44

* 8RS-4&(9,10)RS-3

ROAD CLOSURE

SIDEROAD / STREET CLOSURE



GENERAL NOTES

- TYPE III BARRICADES SHALL BE AS SHOWN ON STANDARD 702001 "TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD". EACH TYPE III BARRICADE SHALL HAVE TWO FLASHING AMBER LIGHTS MOUNTED ABOVE IT.
- IF THE ROAD IS OPEN TO LOCAL TRAFFIC OR EXCEEDS 1000' (300 m), ANOTHER SET OF TYPE III BARRICADES, EQUIPPED AS IN NOTE 1 ABOVE, SHALL BE PLACED AT EACH END OF THE WORK AREA.
- WHEN A STOP CONDITION EXISTS, NO SIGNS ARE REQUIRED IN ADVANCE OF THE "STOP" SIGN WHEN THE ROAD IS CLOSED WITHIN 100' (30 m) OF THE INTERSECTION.
- STANDARD 702001 SHALL APPLY FOR THE PLACEMENT & DESIGN OF TYPE III BARRICADES.
- IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON AN NCHRP 350 TEMPORARY SIGN SUPPORT DIRECTLY IN FRONT OF THE BARRICADE.
- REFLECTORIZED STRIPING SHALL APPEAR ON BOTH SIDES OF THE TYPE III BARRICADES IF ROAD IS OPEN TO LOCAL TRAFFIC.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- A MINIMUM OF TWO FLASHING LIGHTS SHALL BE USED AT NIGHT ON EACH APPROACH IN ADVANCE OF THE WORK AREA. FLASHING LIGHTS SHALL BE INSTALLED ABOVE THE FIRST TWO SIGNS IN THE SERIES.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
- FORMS BT. 725 AND BT. 726 ARE REQUIRED.
- WHEN A SIDEROAD INTERSECTS THE HIGHWAY ON WHICH WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC DEVICES SHALL BE ERECTED AND PROVIDED AS DIRECTED BY THE ENGINEER.
- AN ADDITIONAL SIGN MAY BE REQUIRED AT A MAJOR INTERSECTING ROAD IN ADVANCE OF THE CLOSURE. THE ADDITIONAL SIGN SHALL GIVE THE DISTANCE TO THE BARRICADE IN MILES OR FRACTIONS OF A MILE.

GENERAL NOTES

- TYPE III BARRICADES SHALL BE AS SHOWN ON "TYPICAL APPLICATIONS OF TYPE III BARRICADES CLOSING A ROAD". EACH TYPE III BARRICADE SHALL HAVE TWO FLASHING AMBER LIGHTS MOUNTED ABOVE IT.
- WHERE A STOP CONDITION EXISTS, AS SHOWN ABOVE, WARNING SIGNS MAY BE OMITTED IN ADVANCE OF THE "STOP" SIGN.
- STANDARD 702001 SHALL APPLY FOR THE PLACEMENT & MANUFACTURE OF TYPE III BARRICADES.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ONE FLASHING LIGHT IS REQUIRED ABOVE EACH ADVANCE WARNING SIGN DURING HOURS OF DARKNESS.
- LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.
- FORMS BT 725 AND BT 726 ARE REQUIRED.
- THE MAINLINE ROUTE TEMPORARY TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THE PLANS, SPECIAL PROVISIONS AND STANDARD SPECIFICATIONS.
- THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS PAY ITEMS INVOLVING THE RECONSTRUCTION OF ALL APPLICABLE SIDE STREETS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	REPLACED DETAILS F-5.03 & F-5.04	TJB

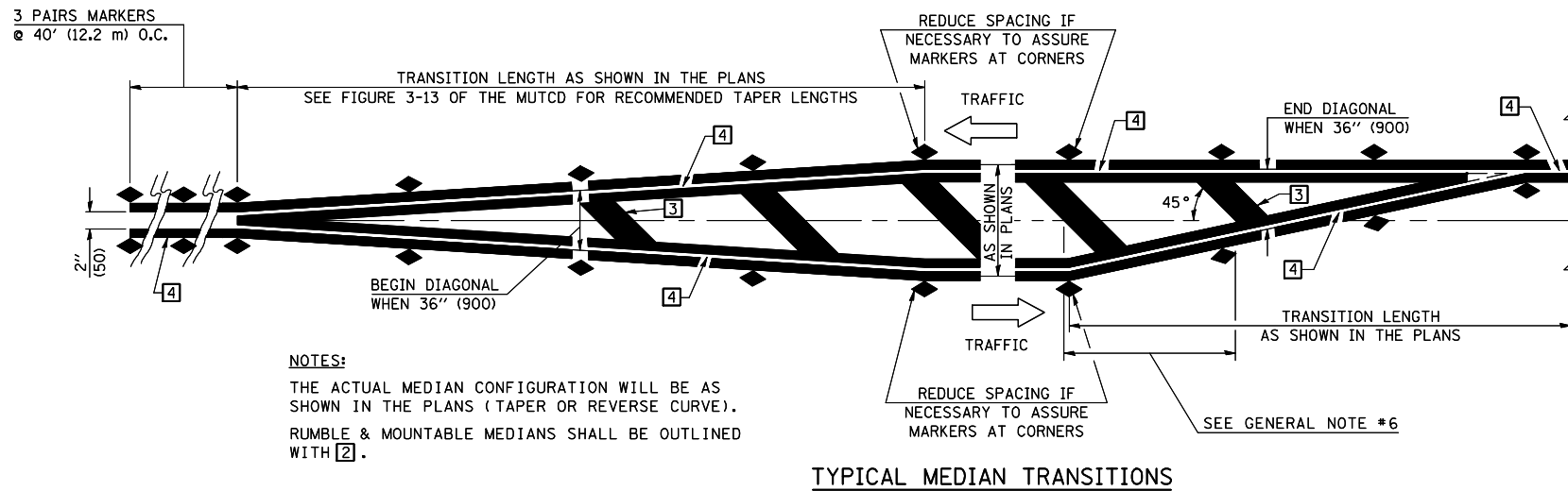
ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL & PROTECTION DEVICES

DISTRICT 5 DETAIL NO. 7020000

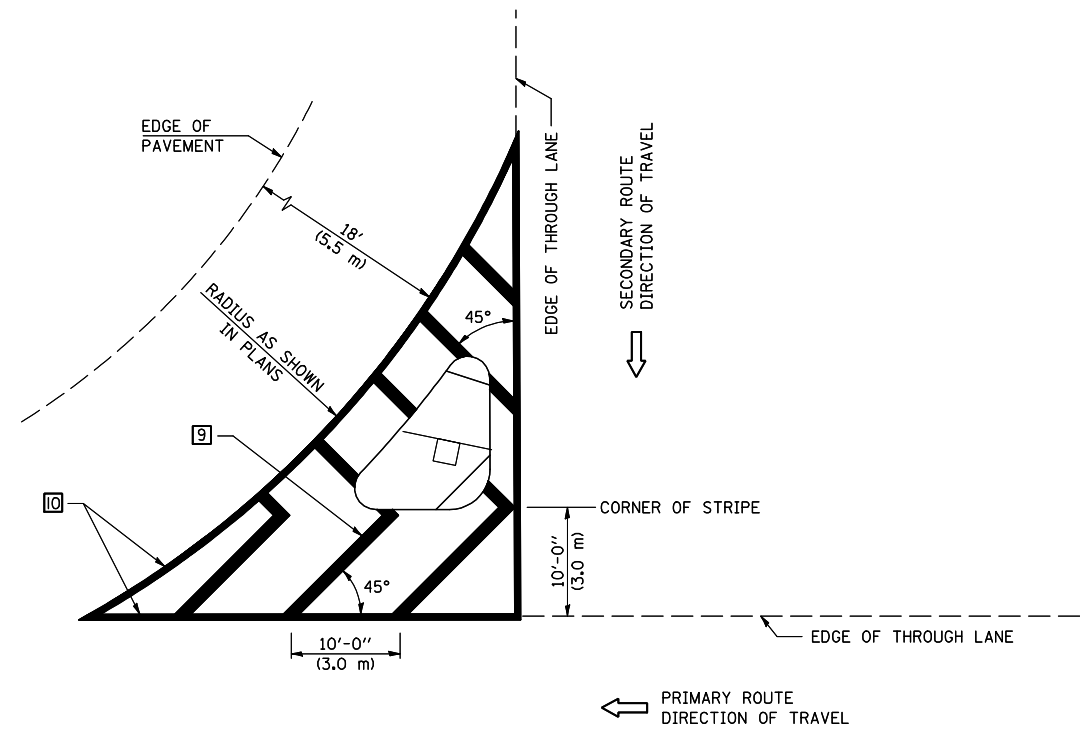
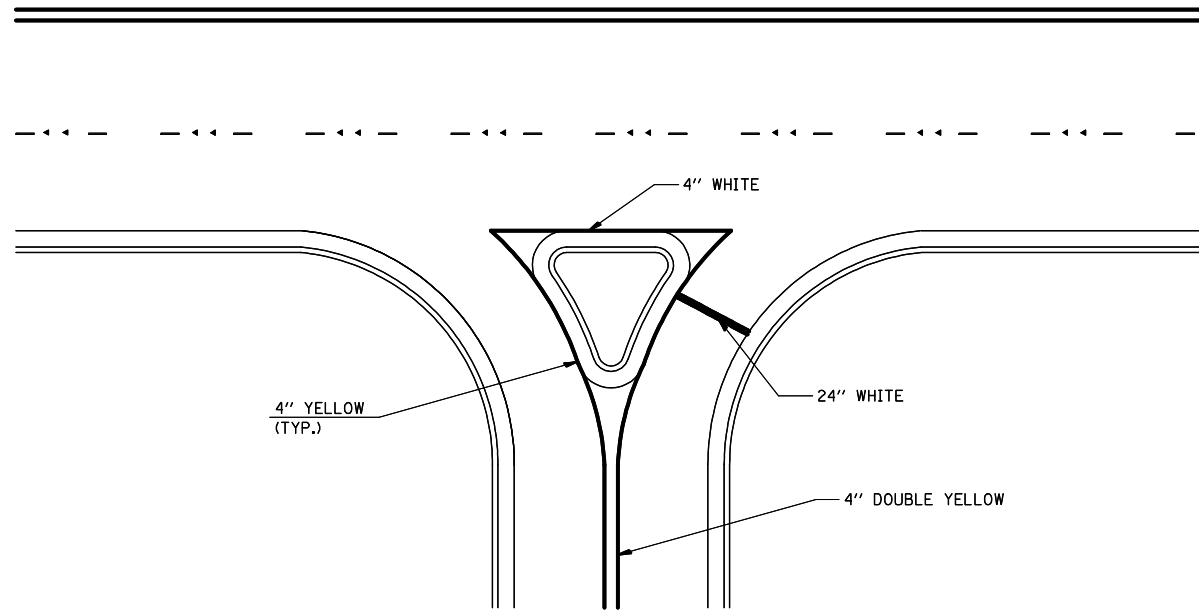
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	47

* 8RS-4&(9,10)RS-3



GENERAL NOTES

1. WHEN MEDIANS ARE PRESENT, PAVEMENT MARKINGS ARE TO BE PLACED ADJACENT TO MEDIANS.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. A STRIPING KEY IS AVAILABLE ELSEWHERE AND SHALL BE SHOWN WHERE THE QUANTITIES ARE LISTED.
5. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
6. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING,
 < 30 MPH USE 15' (< 50 km/h USE 4.5 m)
 30-45 MPH USE 20' (50-75 km/h USE 6.0 m)
 > 45 MPH USE 30' (> 75 km/h USE 9.0 m)



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME	ILLINOIS DEPARTMENT OF TRANSPORTATION
11/06	REPLACED DETAIL F-5.25	TJB	PAVEMENT MARKING AND MARKERS (RURAL & URBAN APPLICATIONS)
			DISTRICT 5 DETAIL NO. 7800AAAA

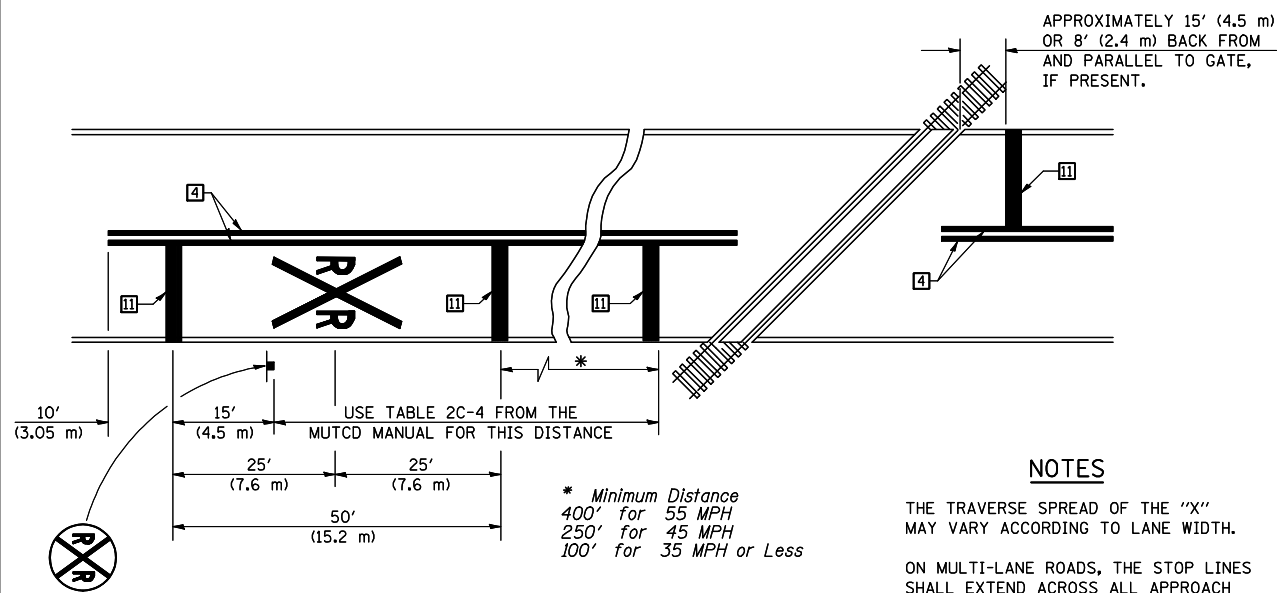
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 USER NAME = ceerlock_jd

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
741	*	PIATT	48	48

* 8RS-4&(9,10)RS-3

RAILROAD CROSSING WITH INTERCONNECT ONLY

RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS



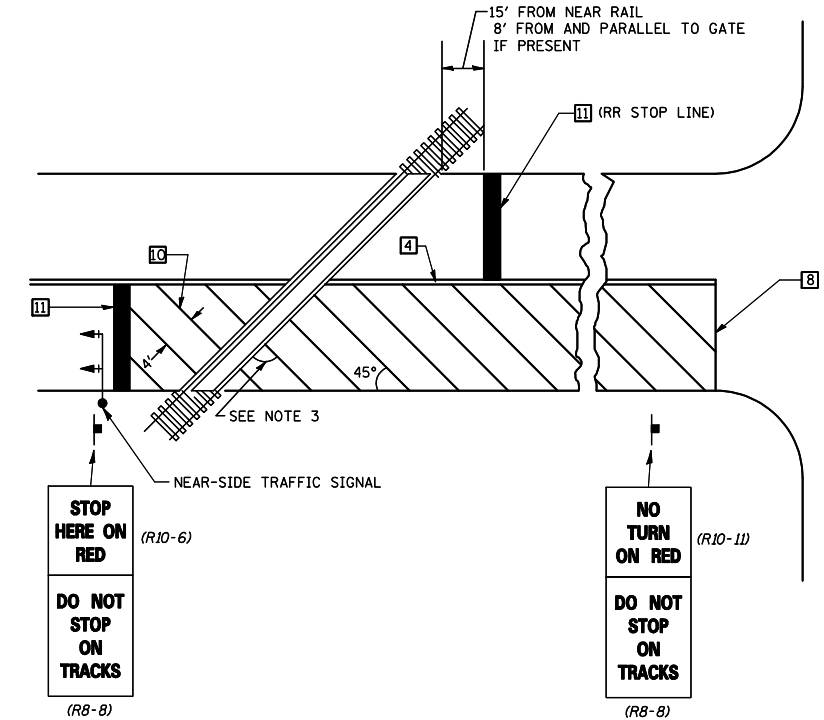
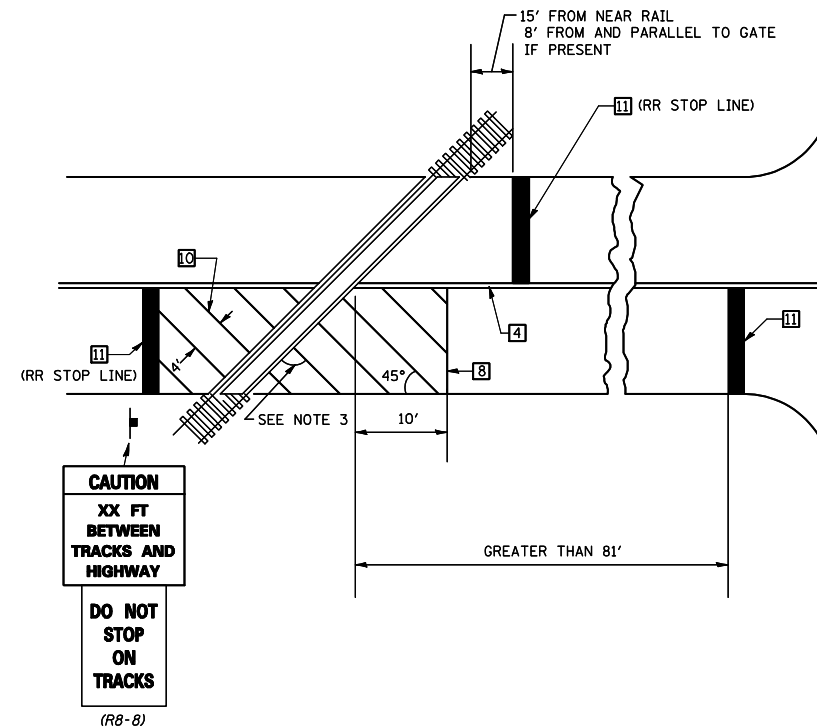
PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

NOTES

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.



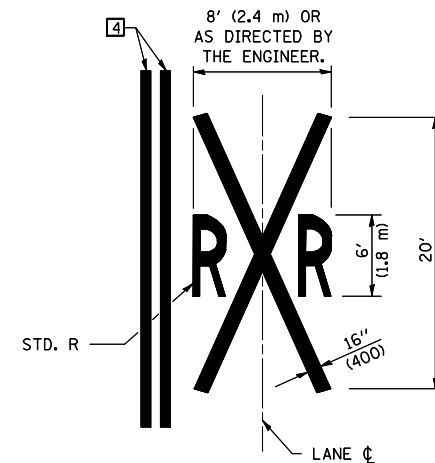
SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING

NOTES

SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.

EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.

WHERE THE ANGLE BETWEEN THE DIAGONAL PAVEMENT MARKINGS AND THE TRACK WOULD BE LESS THAN 20°, THE PAVEMENT MARKINGS SHOULD BE PLACED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DATE	REVISIONS	NAME
11/06	REPLACED DETAIL F-5.25	TJB

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING AND MARKERS (RURAL & URBAN APPLICATIONS)

DISTRICT 5 DETAIL NO. 7800AAAA

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