

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	001

CONTRACT NO. 62533

FOR INDEX OF SHEETS, SEE SHEET NO. 2

LIST OF BRIDGE WORK:

- S.N. 016-0988 112TH PLACE OVER NB AND SB I-57
- * REMOVE AND REPLACE EXISTING CONCRETE DECK.
- * REMOVE AND REPLACE EXISTING ABUTMENT BACKWALL AND PORTIONS OF RETAINING WALL.
- * REMOVE AND REPLACE APPROACH SLABS.
- * REMOVE EXISTING EXPANSION BEARINGS AND REPLACE WITH ELASTOMERIC BEARINGS.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

FAI ROUTE 57
(I-57)
COOK COUNTY
112TH PLACE BRIDGE DECK REPLACEMENT
OVER NORTHBOUND AND SOUTHBOUND INTERSTATE 57
SECTION 1920.1B-R
PROJECT: ESP-057-7 (277)356
C-91-337-02

SEALS

COMPANY:
TENG & ASSOCIATES INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N MICHIGAN AVE.
CHICAGO IL 60601

SIGNATURE:
NAME: BYRON DANLEY
SIGNATURE & SEAL APPLY TO DRAWING
NOS. 1 TO 28 AND 36 TO 89
DATE: 11/16/09

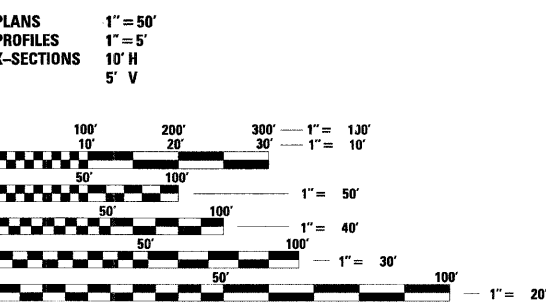
SEALS

COMPANY:
TENG & ASSOCIATES INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N MICHIGAN AVE.
CHICAGO IL 60601

SIGNATURE: Paul A. Rogas
NAME: PAUL A. ROGAS
SIGNATURE & SEAL APPLY TO DRAWING
NOS. 29 TO 35
DATE: 11/30/2009 (EXP.)

TRAFFIC DATA
2008 ADT = 11,700 (112TH PLACE)
POSTED SPEED LIMIT
30 MPH (112TH PLACE)

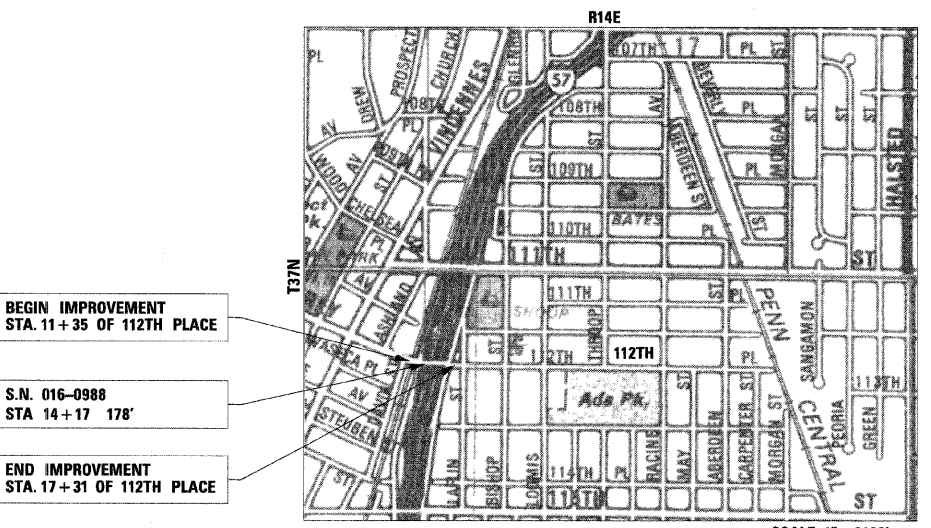
PROJECT LOCATED IN THE CITY OF CHICAGO



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

FOR UTILITY INFORMATION CALL C.U.A.N.
(CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000
48 HOURS BEFORE DIGGING

CONTRACT NO. 62533



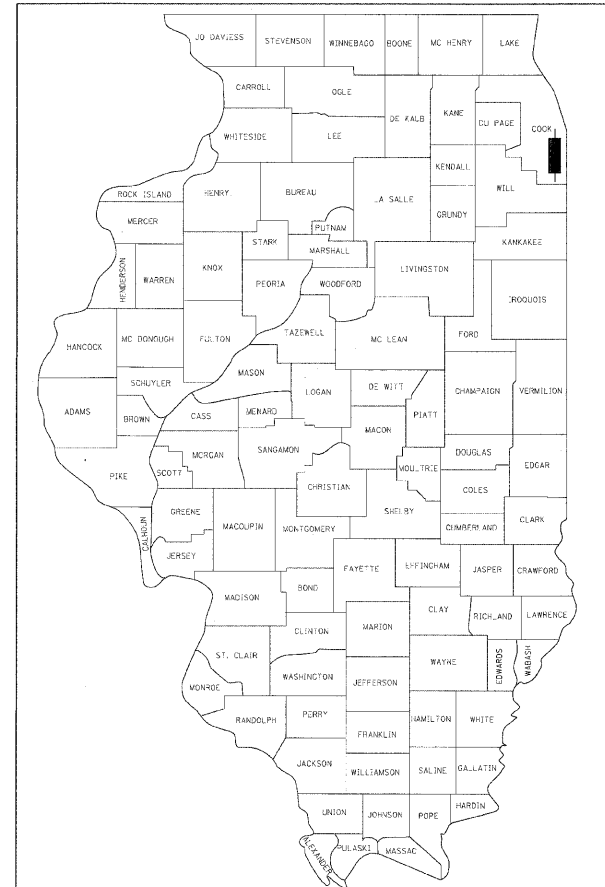
BEGIN IMPROVEMENT
STA. 11 + 35 OF 112TH PLACE

S.N. 016-0988
STA 14 + 17 178'

END IMPROVEMENT
STA. 17 + 31 OF 112TH PLACE

CALUMET TOWNSHIP
GROSS LENGTH OF PROJECT: 596' = 0.11 MILES
NET LENGTH OF PROJECT: 596' = 0.11 MILES

D-91-337-02



LOCATION OF SECTION INDICATED THUS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 12, 2009

Diana M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAY, REGION 1

March 13, 2009
Charles A. Jorgensen
ENGINEER OF DESIGN AND ENVIRONMENT

March 13, 2009
Christine M. Reed
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

TENG

TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE. CHICAGO, IL 60601
TELEPHONE: 312/616-0000

DISTRICT 1 (847) 705-4431 PROJECT MANAGER: BRIAN KUTTAB

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GENERAL NOTES [] = INCLUDED IN THE COST OF

1. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "CUAN" (CHICAGO UTILITY ALERT NETWORK), 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRICAL, TELEPHONE AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).
2. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
3. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO THE COMED CONDUITS ATTACHED TO THE BOTTOM OF 112TH BRIDGE DECKS. THE CONTRACTOR SHALL CONTACT CUAN FOR EXACT LOCATION OF THESE FACILITIES PRIOR TO ANY DIGGING IN THE ADJACENT AREA OR BRIDGE DECK DEMOLITION.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. BARRICADES: WHEN APPLICABLE THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED-ONE (1) WEIGHTED SAND BAG ACROSS EACH BOTTOM RAIL. ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR (4) SANDBAGS PER BARRICADE.
6. WHEN ARTIFICIAL LIGHTING IS USED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
7. BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE DISTRICT ONE STANDARD BD32 INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED. BUTT JOINTS SHALL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE CONSIDERED INCLUDED IN THE UNIT COST OF HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH.
8. THE CONTRACTOR SHALL NOT USE CONCRETE MIXTURE PP-2 FOR ANY ROADWAY OR BRIDGE DECK CONCRETE PATCHING.
9. SURFACE ADJACENT TO RETAINING OR PARAPET WALLS AFFECTED BY THE RECONSTRUCTION SHALL BE SURFACED WITH BITUMINOUS AND SHALL BE PAID FOR AS INCIDENTAL BITUMINOUS SURFACING. THE COST OF ALL INCIDENTAL BITUMINOUS SURFACING SHALL INCLUDE ALL PREPARATION, EXCAVATION, CLEANUP, RESTORATION, REMOVAL AND SATISFACTORY DISPOSAL OF THE EXISTING SURFACE.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL OVERHEAD, SURFACE, AND UNDERGROUND UTILITIES WITHIN THE PROJECT LIMITS WHETHER OR NOT THE UTILITIES ARE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPLACED OR RELOCATED BY THE CONTRACTOR AT HIS/ HER OWN EXPENSE.
11. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH WILL NOT BE REMOVED. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE CONTRACTORS' OWN EXPENSE.
12. 3 METER (10 FEET) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB & GUTTERS AND MEDIAN ITEMS OF WORK TO EXISTING CURBS & GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
13. CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE CHICAGO TRANSIT AUTHORITY (CTA) AND SHOOP ACADEMY OF MATH, SCIENCE, AND TECHNOLOGY ((773) 535-2715) 28 WORKING DAYS PRIOR TO START OF CONSTRUCTION IN ORDER TO TO REVIEW AND COORDINATE ANY CONFLICTS WITH BUS ROUTES.
14. STRIP REFLECTIVE CRACK CONTROL TREATMENT SHALL BE PLACED ON MILLED PCC PAVEMENT PRIOR TO BITUMINOUS OVERLAY ON THE EAST AND WEST FRONTAGE ROADS.
15. SPECIAL ATTENTION IS CALLED TO ARTICLE 107.12 REGARDING RAILROAD FLAGGERS. THE NAME AND TELEPHONE NUMBER OF THE RAILROAD ENGINEER IS MR. MIR ALI (847) 705-4248.
16. THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF .0004 TONS/SQ YD.
17. SIDEWALK ACCESSIBILITY RAMPS SHALL NOT BE CONSTRUCTED DIRECTLY OVER EXISTING OR PROPOSED DRAINAGE STRUCTURES.
18. BACKFILL MATERIALS UNDER SIDEWALKS SHALL BE CA-6 AND SHALL BE INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH.
19. THE COST OF THE FULL DEPTH SAW CUT REQUIRED FOR PAVEMENT REMOVAL SHALL BE INCLUDED IN THE UNIT PRICE FOR PAVEMENT REMOVAL.

20. EXISTING MUDJACK REMOVAL BENEATH APPROACH PAVEMENTS SHALL BE INCLUDED IN THE COST OF APPROACH PAVEMENT REMOVAL.
21. THE PROPOSED BRIDGE APPROACH PAVEMENTS SHALL BE GROOVED AND THE QUANTITY MEASURED FOR PAYMENT AS BRIDGE DECK GROOVING.
22. THE CONTRACTOR SHALL BE RESPONSIBLE, AS PART OF THIS PROJECT, TO REMOVE, TEMPORARILY SUPPORT, PROTECT, AND REINSTALL THE EXISTING 911 CABLE DUCT OF THE CITY OF CHICAGO OFFICE OF EMERGENCY COMMUNICATIONS. THE EXISTING DUCT IS LOCATED UNDERNEATH THE BRIDGE DECK AS SHOWN ON THE STRUCTURAL PLANS. THIS SHALL BE APPLICABLE FOR THE ENTIRE DURATION OF CONSTRUCTION. THE CONTRACTOR SHALL REINSTALL THE DUCT IN ITS ORIGINAL POSITION UNDERNEATH THE DECK IN ACCORDANCE WITH THE APPROVAL OF THE ENGINEER AND THE REQUIREMENTS OF THE OFFICE OF EMERGENCY COMMUNICATIONS. THE DUCT PACKAGE HANDLING MAY INCLUDE ASBESTOS WHICH THE CONTRACTOR SHALL HANDLE PER CURRENT APPROVED ASBESTOS HANDLING PROCEDURES. THE CONTRACTOR SHALL ALSO SUPPLY AND INSTALL ALL NECESSARY HARDWARE, BRACKETS AND INSERTS IN THE NEW BRIDGE DECK TO ENABLE THE REINSTALLATION OF THE DUCT PACKAGE. CONTRACTOR TO PROVIDE THE DETAILS FOR DUCT TEMPORARY SUPPORT AND PROTECTION TO THE ENGINEER FOR APPROVAL PRIOR TO ANY CONSTRUCTION OR DECK REMOVAL ANY DAMAGE TO THE DUCT SHALL BE REPLACED TO THE SATISFACTION OF THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR. THE COST OF ALL OF THIS WORK, INCLUDING ALL MATERIALS, SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT COST OF REMOVAL OF EXISTING CONCRETE DECK.
23. TREES ALONG HAMLET AVE BETWEEN 112TH PLACE TO 111TH STREET SHALL BE PROTECTED DURING CONSTRUCTION. IF ANY TREES ARE DAMAGED, THE CITY OF CHICAGO FORESTRY RESERVES THE RIGHT TO COLLECT FINES FROM THE CONTRACTOR FOR THE COST OF REPAIR, OR REMOVAL AND REPLACEMENT AND THE DIFFERENCE OF THE AFOREMENTIONED COSTS FROM THE TOTAL APPRAISED VALUE OF THE TREES IN QUESTION. THE CONTRACTOR SHALL OBTAIN A PERMIT TO REMOVE, PLANT, TRIM, SPRAY, OR IN ANY WAY AFFECT THE GENERAL HEALTH OR STRUCTURE OF TREES IN THE PUBLIC WAY. THIS INCLUDES THE DISTURBANCE OR REMOVAL OF ROOT SYSTEMS OF EXISTING TREES WITHIN THE PROJECT AREA. PERMITS FOR TREE RELATED ACTIVITIES WILL BE ISSUED TO INSURED TREE SERVICE COMPANIES BY THE BUREAU OF FORESTRY, 3200 SOUTH KEDZIE AVENUE, CHICAGO, IL 60613, (312) 747-2098.
24. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS AT THE CONTRACTOR'S EXPENSE.
25. THE CONTRACTOR MUST NOTIFY THE CHICAGO DEPARTMENT OF TRANSPORTATION BUREAU OF TRAFFIC (312) 744-0330, 72 HOURS BEFORE COMMENCING CONSTRUCTION OR CHANGING TRAFFIC FLOW.
26. ALL IMPROVEMENTS OR ITEMS TO BE REMOVED AND REPLACED DURING CONSTRUCTION SHOULD CONFORM TO THE CHICAGO'S CONSTRUCTION STANDARDS FOR WORK IN THE PUBLIC WAY MANUAL.
27. CURB AND GUTTER CONSTRUCTION SHALL PROVIDE A MINIMUM CURB HEIGHT OF 3" AND A MAXIMUM CURB HEIGHT OF 9" AT DRAINAGE STRUCTURES.
28. AN ADDITIONAL QUANTITY (10) FOR NEW FRAMES AND LIDS TYPE 1 OPEN LID AND CLOSED LID HAS BEEN ADDED TO THE CONTRACT FOR EACH AND THE LOCATIONS ARE TO BE DETERMINED BY THE ENGINEER.
29. THE CONTRACTOR SHALL CALL STAN JONES LOCATED AT THE SOUTH DISTRICT SHOP OF PEOPLES ENERGY AT (773) 962-4841 AND DIGGER AT (312) 744-7000 AT LEAST 48 HOURS PRIOR TO START OF CONSTRUCTION. PEOPLES GAS WILL MARK LOCATIONS OF ALL GAS FACILITIES IN ADVANCE OF CONSTRUCTION. IN THE EVENT OF AN EMERGENCY CALL (312) 240-7001.
30. ALL PAVEMENT PATCHING SHALL BE CLASS C AT LOCATIONS DIRECTED BY THE ENGINEER.
31. THE CONTRACTOR SHALL MAINTAIN ACCESS CONTROL AT ALL TIMES DURING CONSTRUCTION. TEMPORARY FENCE SHALL BE PROVIDED TO MAINTAIN ACCESS CONTROL.
31. SLIPFORMING OF THE PARPETS IS NOT ALLOWED.

COMMITMENTS:

NONE

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	003
STA.	TO STA.		FED. AID PROJECT	
CONTRACT NO. 62533				

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

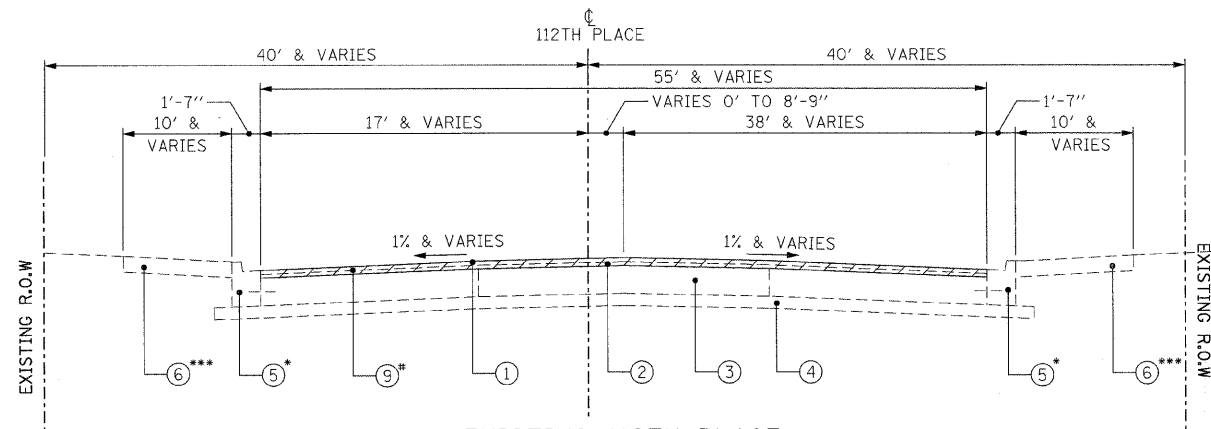
GENERAL NOTES
AND COMMITMENTS

SCALE: NONE
DATE: 01/16/09

DRAWN BY: BRH
CHECKED BY: ACL

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
235 N. MICHIGAN AVE. CHICAGO, IL 60601
TELEPHONE 312/616-0000

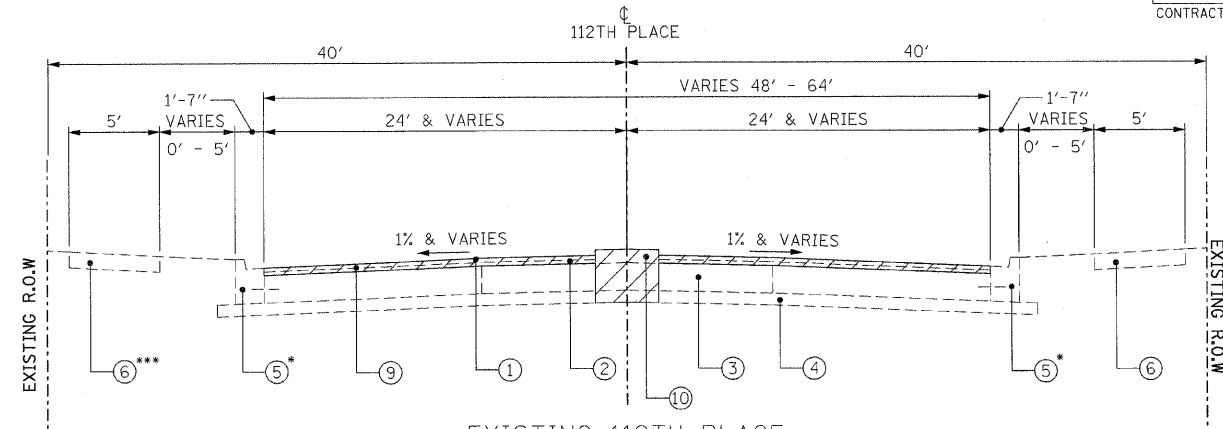
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EXISTING 112TH PLACE

STA. 11+35 TO STA. 13+09

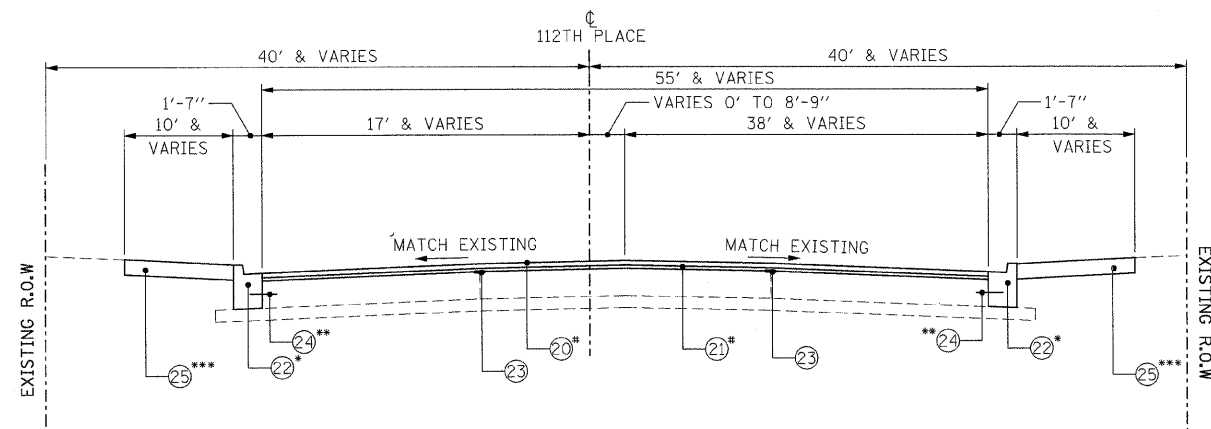
- * REMOVAL OMISSION 11+94.50 TO STA. 12+39.95
- * REMOVE AND REPLACE PER SCHEDULE
- *** SEE REMOVAL PLANS FOR LIMITS OF SIDEWALK REMOVAL



EXISTING 112TH PLACE

STA. 15+37 TO STA. 17+31

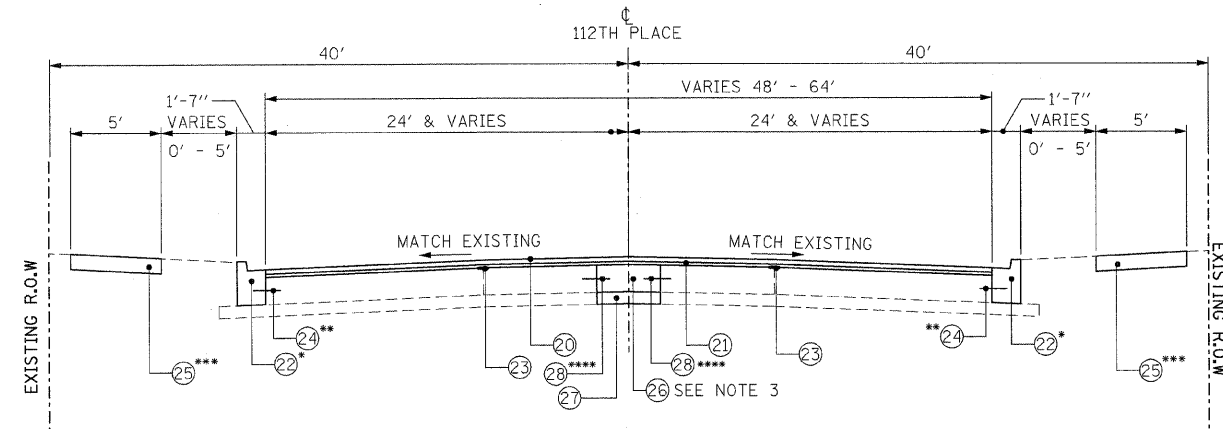
- * REMOVE AND REPLACE PER SCHEDULE
- *** SEE REMOVAL PLANS FOR LIMITS OF SIDEWALK REMOVAL



PROPOSED 112TH PLACE

STA. 11+35 TO STA. 13+09

- * RESURFACING OMISSION STA. 11+94.50 TO STA. 12+39.95
- * REMOVE AND REPLACE PER SCHEDULE
- ** INCLUDED IN THE COST OF CONC CURB & GUTTER TYPE B-V.12
- *** SEE PROPOSED PLANS FOR LIMITS OF P.C.C. SIDEWALK 8" LIMITS



PROPOSED 112TH PLACE

STA. 15+37 TO STA. 17+31

- * REMOVE AND REPLACE PER SCHEDULE
- ** INCLUDED IN THE COST OF CONC CURB & GUTTER TYPE B-V.12
- *** SEE PROPOSED PLANS FOR LIMITS OF P.C.C. SIDEWALK 8" LIMITS
- **** INCLUDED IN THE COST OF PORTLAND CEMENT CONCRETE BASE COURSE, 9"

NOTES:

1. EXISTING DIMENSIONS AND THICKNESSES SHOWN ARE FROM RECORD PLANS. ACTUAL DIMENSIONS AND THICKNESSES MAY VARY.
2. ADDITIONAL STRIP REFLECTIVE CRACK CONTROL TREATMENT PLACEMENT LOCATIONS MY BE REQUIRED AT PAVEMENT JOINTS AND AS DIRECTED BY THE ENGINEER.
3. THICKNESS OF PROPOSED P.C.C. BASE COURSE SHALL MATCH EXISTING P.C.C. PAVEMENT THICKNESS AND WILL BE PAID FOR PORTLAND CEMENT CONCRETE BASE COURSE 9".
4. SEE SHEET 011 FOR MIX DESIGN CHART.

EXISTING LEGEND [diagonal lines] REMOVAL

- ① BIT. CON SURFACE COURSE 1 1/4"
- ② BIT. CON BINDER COURSE 1 1/4"
- ③ P.C.C. BASE COURSE 9"
- ④ STABILIZED SUB-BASE 4"
- ⑤ CONC CURB & GUTTER TYPE BV.12
- ⑥ P.C.C. SIDEWALK 5"
- ⑦ CHAIN LINK FENCE
- ⑧ RETAINING WALL
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (1"-5")
- ⑩ CONCRETE MEDIAN (CORRUGATED & RAISED)

PROPOSED LEGEND [solid box] = INCLUDED IN THE COST OF

- ⑳ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (1 1/2")
- ㉑ LEVELING BINDER (MACHINE METHOD), N70 (1")
- ㉒ CONC CURB & GUTTER TYPE B-V.12
- ㉓ STRIP REFLECTIVE CRACK CONTROL
- ㉔ NO. 6 EPOXY COATED TIE BAR, 30" LONG @ 30" CENTERS
- ㉕ PORTLAND CEMENT CONCRETE SIDEWALK, 8"
- ㉖ PORTLAND CEMENT CONCRETE BASE COURSE 9"
- ㉗ STABILIZED SUB-BASE 4"
- ㉘ NO. 8 EPOXY COATED TIE BAR, 24" LONG @ 24" CENTERS, DRILLED AND GOUTED

REVISIONS	
NAME	DATE

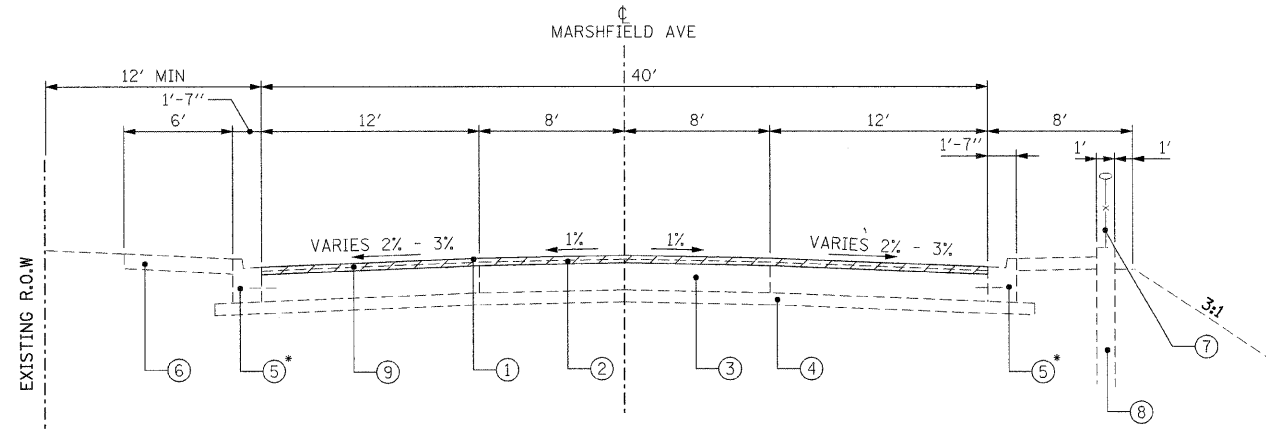
ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I. ROUTE 57 (INTERSTATE 57)

**TYPICAL SECTIONS
112TH PLACE**

SCALE: N.T.S. DRAWN BY: BRH
DATE: 01/16/09 CHECKED BY: ACL

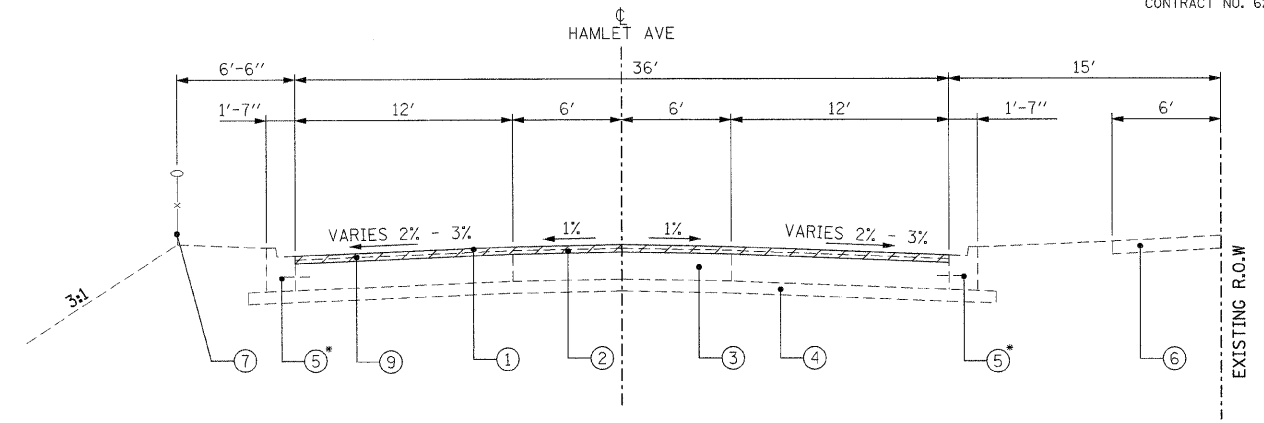
TENG
TENGE & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/457-0000

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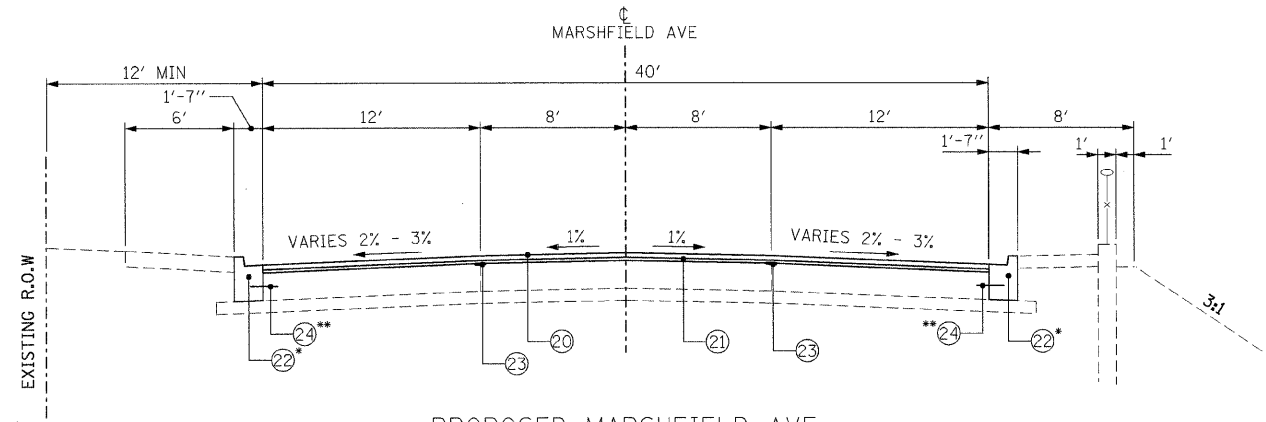
EXISTING MARSHFIELD AVE

* REMOVE AND REPLACE PER SCHEDULE



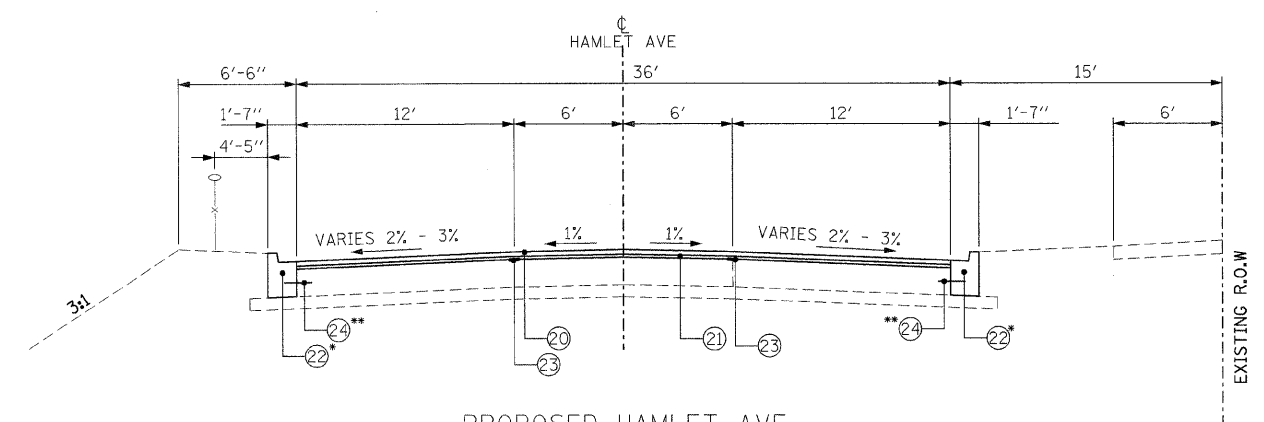
EXISTING HAMLET AVE

* REMOVE AND REPLACE PER SCHEDULE



PROPOSED MARSHFIELD AVE

* REMOVE AND REPLACE PER SCHEDULE
 ** [INCLUDED IN THE COST OF CONC CURB & GUTTER TYPE B-V.12]



PROPOSED HAMLET AVE

* REMOVE AND REPLACE PER SCHEDULE
 ** [INCLUDED IN THE COST OF CONC CURB & GUTTER TYPE B-V.12]

HOT-MIX ASPHALT MIX DESIGN CHART

PAY CODE	PAY ITEM DESCRIPTION	UNIT	AC TYPE	PERCENTAGE AIR VOIDS	MIX TYPE
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	PG 64-22	4% @ 70 GYR	IL 9.5MM
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	PG 64-22*	4% @ 70 GYR	IL 9.5MM

*WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22

NOTES:

1. EXISTING DIMENSIONS AND THICKNESSES SHOWN ARE FROM RECORD PLANS. ACTUAL DIMENSIONS AND THICKNESSES MAY VARY.

EXISTING LEGEND [Hatched Box] REMOVAL

- ① BIT. CON SURFACE COURSE 1 1/4"
- ② BIT. CON BINDER COURSE 1 1/4"
- ③ P.C.C. BASE COURSE 9"
- ④ STABILIZED SUB-BASE 4"
- ⑤ CONC CURB & GUTTER TYPE BV.12
- ⑥ P.C.C. SIDEWALK 5"
- ⑦ CHAIN LINK FENCE
- ⑧ RETAINING WALL
- ⑨ HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH (1'-5")
- ⑩ CONCRETE MEDIAN (CORRUGATED & RAISED)

PROPOSED LEGEND [White Box] = INCLUDED IN THE COST OF

- ⑳ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (1 1/2")
- ㉑ LEVELING BINDER (MACHINE METHOD), N70 (1")
- ㉒ CONC CURB & GUTTER TYPE B-V.12
- ㉓ STRIP REFLECTIVE CRACK CONTROL
- ㉔ NO. 6 EPOXY COATED TIE BAR, 30" LONG @ 30" CENTERS
- ㉕ PORTLAND CEMENT CONCRETE SIDEWALK, 8"
- ㉖ PORTLAND CEMENT CONCRETE BASE COURSE 9"
- ㉗ STABILIZED SUB-BASE 4"
- ㉘ NO. 8 EPOXY COATED TIE BAR, 24" LONG @ 24" CENTERS, DRILLED AND GOUTED

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I. ROUTE 57 (INTERSTATE 57)
**TYPICAL SECTIONS
 FRONTAGE ROADS AND
 BIT. CONC MIXTURE SCHEDULE**
 SCALE: N.T.S. DRAWN BY: BRH
 DATE: 01/16/09 CHECKED BY: ACL
TENG
TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312/456-0000

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COMBINATION CURB AND GUTTER REMOVAL				
FROM STATION	TO STATION	RT/LT	LOCATION	FOOT
12+88.00	12+97.00	RT	SE CORNER 112 & MARSHFIELD	23.0
13+23.23	13+24.23	RT	NE CORNER 112 & MARSHFIELD	20.0
15+36.66	15+46.95	LT	SW CORNER 112 & HAMLET	69.0
15+35.74	15+81.87	LT	NW CORNER 112 & HAMLET	58.0
12+46	12+40	RT	SW CORNER 112 & MARSHFIELD	34.5
12+69	12+57	LT	NW CORNER 112 & MARSHFIELD	20.3
15+99	16+08	RT	SE CORNER 112 & HAMLET	16.1
16+28	16+47	LT	NE CORNER 112 & HAMLET	45.6
DAMAGED CURB AND GUTTER DETERMINED BY THE ENGINEER				150.0
TOTAL=				436

SIDEWALK REMOVAL				
FROM STATION	TO STATION	SO FT	LOCATION	SQ FT
12+89.51	13+19.00	231.2	SE CORNER 112 & MARSHFIELD	231.2
13+25.16	13+39.29	157.3	NE CORNER 112 & MARSHFIELD	157.3
14+95.62	15+45.65	499.0	SW CORNER 112 & HAMLET	499.0
15+15.69	15+80.35	470.1	NW CORNER 112 & HAMLET	470.1
12+42.84	12+33.32	177.58	SW CORNER 112 & MARSHFIELD	177.6
12+71.79	12+57.16	118.83	NW CORNER 112 & MARSHFIELD	118.8
15+98.84	16+15.07	120.21	SE CORNER 112 & HAMLET	120.2
16+28.45	16+46.88	220.41	NE CORNER 112 & HAMLET	220.4
TOTAL=				1995

APPROACH SLAB REMOVAL			
FROM STATION	TO STATION	SQ FT	SQ YD
13+08.70	13+29.10	1394.2	154.9
15+05.57	15+34.80	1919.1	213.2
TOTAL=			368

MEDIAN REMOVAL		
FROM STATION	TO STATION	SQ FT
16+20.00	17+31.00	400.0
TOTAL=		400.0

STRIP REFLECTIVE CRACK CONTROL TREATMENT				
FROM STATION	TO STATION	SQ FT	LOCATION	FOOT
15+70.00	16+44.00	2.00	FRONTAGE ROAD "B"	148.0
0+00.00	9+00.00	2.00	HAMELT AVE	1800.0
15+80.37	16+57.00	2.00	FRONTAGE ROAD "A"	153.3
0+00.00	1+79.15	2.00	MARSHFIELD AVE	358.3
TOTAL=				2460

TEMPORARY PAVEMENT MARKING- LETTERS AND SYMBOLS				
LOCATION	DESCRIPTION	SQ FT	LANES	SQ FT
WB 112TH AT HAMLET	RT TURN ARROW	15.2	2.00	30.4
EB 112TH AT MARSHFIELD	RT TURN ARROW	15.2	2.00	30.4
TOTAL=				61

TEMPORARY PAVEMENT MARKING - LINE 4"		
LOCATION	FOOT	
MARSHFIELD RD NEXT TO TEMPORARY BARRIER WALL	272.0	
HAMELT AVE NEXT TO TEMPORARY BARRIER WALL	230.0	
TOTAL=		502

TEMPORARY PAVEMENT MARKING - LINE 6"			
FROM STATION	TO STATION	LOCATION	FOOT
16+50.00	17+31.00	WB 112TH	81.0
11+36.95	11+80.64	EB 112TH	43.7
		CONCRETE BARRIER WALL MARSHFIELD RD	270.0
		CONCRETE BARRIER WALL OUTSIDE SB I-57	270.0
		CONCRETE BARRIER WALL INSIDE SB I-57	290.0
		CONCRETE BARRIER WALL INSIDE NB I-57	270.0
		CONCRETE BARRIER WALL OUTSIDE NB I-57	270.0
		CONCRETE BARRIER WALL HAMELET AVE	230.0
TOTAL=			1725

WORK ZONE PAVEMENT MARKING REMOVAL		
PAY ITEM #	PAY ITEM	FOOT
70300100	SHORT-TERM PAVEMENT MARKING	289.1
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	61.0
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	165.7
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	862.5
TOTAL=		1378

TEMPORARY CONCRETE BARRIER		
LOCATION	FOOT	
MARSHFIELD RD	270.0	
OUTSIDE SB I-57	270.0	
INSIDE SB I-57	290.0	
INSIDE NB I-57	270.0	
OUTSIDE NB I-57	270.0	
HAMELET AVE	230.0	
TOTAL=		1600

THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS				
LOCATION	DESCRIPTION	SQ FT	SIZE	SQ FT
SB MARSHFIELD AVE	RT ARROW ONLY	36.4	LARGE	36.4
SB MARSHFIELD AVE	LT ARROW ONLY	36.4	LARGE	36.4
112TH WEST OF R.R.	RT ARROW ONLY	36.4	LARGE	36.4
112TH EAST OF R.R.	RT ARROW ONLY	15.6	LARGE	15.6
FRONTAGE RD B	LT ARROW ONLY	36.4	LARGE	36.4
FRONTAGE RD B	LT THRU ARROW ONLY	46.8	LARGE	46.8
FRONTAGE RD B	RT THRU ARROW ONLY	46.8	LARGE	46.8
TOTAL=				255

STABILIZED SUB-BASE 4"			
FROM STATION	TO STATION	SQ FT	SQ YD
16+20.00	17+31.00	400.0	44.44
TOTAL=			44

THERMOPLASTIC PAVEMENT MARKING - LINE 4"							
LOCATION	FROM STATION	TO STATION	SKIP DASH	COLOR	LANES	FOOT	
MARSHFIELD AVE	0+52.62	1+79.25	6'-18'	WHITE	1.00	31.7	
FRONTAGE RD A	15+80.37	16+15.00	6'-18'	WHITE	2.00	17.3	
HAMELT AVE	0+41.00	9+00.00	6'-18'	WHITE	2.00	429.5	
EB 112TH	11+38.75	12+27.25	6'-18'	WHITE	1.00	22.1	
WB 112TH	11+38.75	12+27.25	6'-18'	WHITE	1.00	22.1	
WB 112TH	16+50.00	17+31.00	6'-18'	WHITE	1.00	20.3	
MARSHFIELD AVE	0+58.00	1+79.25	N/A	YELLOW		121.3	
FRONTAGE RD A	15+80.37	16+21.50	N/A	YELLOW		41.1	
HAMELT AVE	0+36.00	9+00.00	N/A	YELLOW		864.0	
FRONTAGE RD B	15+70.00	15+93.00	N/A	YELLOW		23.0	
MEDIAN W. R.R.	11+35.00	11+94.00	N/A	YELLOW		240.0	
MEDIAN E. R.R.	12+16.00	12+43.00	N/A	YELLOW		105.0	
MEDIAN HAMLET/112TH	16+36.00	17+31.00	N/A	YELLOW		388.0	
RAILROAD DIAGONALS				WHITE		165.0	
TOTAL=						2490	

THERMOPLASTIC PAVEMENT MARKING - LINE 6"					
LOCATION	FROM STATION	TO STATION	FOOT	LANES	FOOT
MARSHFIELD AVE	0+52.62	1+79.25	126.6	2.00	253.3
FRONTAGE RD B	15+45.00	15+98.00	53.0	2.00	106.0
CROSSWALK MARSH/112TH			358.0		358.0
CROSSWALK HAMLET/112TH			367.0		367.0
TOTAL=					1084

THERMOPLASTIC PAVEMENT MARKING - LINE 12"		
LOCATION	FOOT	
MEDIAN DIAGONALS MARSHFIELD/112TH	45.0	
MEDIAN DIAGONALS HAMLET/112TH	75.0	
RAILROAD STRIPE	24.0	
TOTAL=		144

THERMOPLASTIC PAVEMENT MARKING - LINE 24"		
LOCATION	STATION	FOOT
SB MARSHFIELD	0+46.41	45.0
NB FRONTAGE RD B	16+03.92	39.0
EB 112TH	11+74.88	25.0
WB 112TH	12+41.31	25.0
WB 112TH	16+52.76	22.0
TOTAL=		156

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I. ROUTE 57 (INTERSTATE 57)

SCHEDULE OF QUANTITIES

SCALE: N.T.S. DRAWN BY: BRH
DATE: 01/16/09 CHECKED BY: ACL

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/516-5000

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PERFORMED PLASTIC PAVEMENT MARKING, TYPE B - LETTERS AND SYMBOLS					
LOCATION	DESCRIPTION	SQ FT	SIZE	LANES	SQ FT
WB 112TH BRIDGE	R.R. CROSSING	61.2	LARGE	2	122.4
WB 112TH BRIDGE	LT ARROW ONLY	36.4	LARGE	1	36.4
EB 112TH BRIDGE	LT ARROW ONLY	36.4	LARGE	1	36.4
TOTAL=					195

PAVEMENT MARKING REMOVAL					
LOCATION	DESCRIPTION	FROM STATION	TO STATION	SQ FT	
FRONTAGE RD B	LT ARROW ONLY			20.6	
FRONTAGE RD B	LT THRU ARROW ONLY			26.5	
FRONTAGE RD B	RT THRU ARROW ONLY			26.5	
SB MARSHFIELD	LT ARROW ONLY			26.5	
SB MARSHFIELD	4" SKIP DASH (6'-18')	0+54.00	1+79.00	13.8	
SB MARSHFIELD	6" SOLID LEFT TURN LANE	0+54.00	1+62.00	54.0	
TOTAL=					168

PERFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4"						
LOCATION	FROM STATION	TO STATION	SKIP DASH	COLOR	LANES	FOOT
WB 112TH BRIDGE	13+45.00	15+19.00	6'-18'	WHITE	1.00	43.5
BRIDGE MEDIAN	13+30.41	15+19.00	N/A	YELLOW		817.0
TOTAL=						861

HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70						112 LBS/SQ YD/IN
FROM STATION	TO STATION	SQ FT	THICKNESS (IN)	LOCATION	TON	
11+35.46	11+88.63	2933.84	1.50	WEST OF R.R.	27.4	
12+33.52	13+02.96	12099.39	1.50	WEST OF BRIDGE	112.9	
15+45.96	17+31.70	14956.11	1.50	EAST OF BRIDGE	139.6	
1+00.00	9+00.00	29059.70	1.50	HAMLET AVE	271.2	
TOTAL=					551	

PERFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 6"						
LOCATION	FROM STATION	TO STATION	SKIP DASH	COLOR	FOOT	
112TH BRIDGE DECK	13+41.80	14+01.50	N/A	WHITE	59.7	
112TH BRIDGE DECK	14+48.13	15+10.73	N/A	WHITE	62.6	
112TH BRIDGE DECK	14+48.13	15+14.13	N/A	WHITE	66.0	
112TH BRIDGE DECK	13+54.58	14+48.13	2'-6'	WHITE	30.9	
112TH BRIDGE DECK	14+01.50	14+48.13	2'-6'	WHITE	15.4	
TOTAL=						235

LEVELING BINDER (MACHINE METHOD), N70					
FROM STATION	TO STATION	SQ FT	THICKNESS (IN)	LOCATION	TON
11+35.46	11+88.63	2933.84	1.00	WEST OF R.R.	18.3
12+33.52	13+02.97	12099.39	1.00	WEST OF BRIDGE	75.3
15+45.95	17+31.70	14956.11	1.00	EAST OF BRIDGE	93.1
1+00.00	9+00.00	29059.70	1.00	HAMLET AVE	180.8
TOTAL=					367

PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-LINE 12"		
LOCATION		FOOT
DIAGONALS MEDIAN 112TH BRIDGE		60.0
TOTAL=		60

COMBINATION CONCRETE CURB AND GUTTER, TYPE B-V.12					
FROM STATION	TO STATION	RT/LT	LOCATION	FOOT	
12+88.21	12+99.22	RT	SE CORNER 112 & MARSHFIELD	25.0	
13+23.63	13+20.50	LT	NE CORNER 112 & MARSHFIELD	18.0	
15+26.15	15+46.21	RT	SW CORNER 112 & HAMLET	54.0	
15+45.68	15+80.91	LT	NW CORNER 112 & HAMLET	50.0	
12+46.00	12+40.00	RT	SW CORNER 112 & MARSHFIELD	23.0	
12+69.21	12+57.33	LT	NW CORNER 112 & MARSHFIELD	13.5	
15+99.00	16+08.00	RT	SE CORNER 112 & HAMLET	10.7	
16+28.00	16+47.00	LT	NE CORNER 112 & HAMLET	30.4	
TOTAL DAMAGED FROM ITEM 44000500 (C&G REMOVAL)				150.0	
TOTAL=					375

PERFORMED PLASTIC PAVEMENT MARKING, TYPE B-LINE 24"		
LOCATION		FOOT
13+49.49		39.5
13+17.55		38.5
RAILROAD BARS		52.0
TOTAL=		130

SELECTIVE CLEARING		
LOCATION	SQ FT	UNIT
NB I-57 OUTSIDE SHLDR (SOUTH)	472.71	0.47
NB I-57 OUTSIDE SHLDR (NORTH)	180.11	0.18
TOTAL=		1

BARRIER WALL MARKERS, TYPE C				
LOCATION	LENGTH (FT)	SPACING (FT)	# OF REFLECTORS	EACH
MARSHFIELD RD	270.0	25	2.0	22
OUTSIDE SB I-57	270.0	25	2.0	22
INSIDE SB I-57	290.0	25	2.0	23
INSIDE NB I-57	270.0	25	2.0	22
OUTSIDE NB I-57	270.0	25	2.0	22
HAMELET AVE	230.0	25	2.0	18
TOTAL=				

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

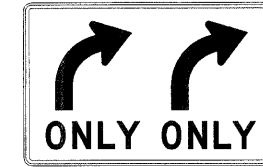
SCHEDULE OF QUANTITIES

SCALE: N.T.S. DRAWN BY: BRH
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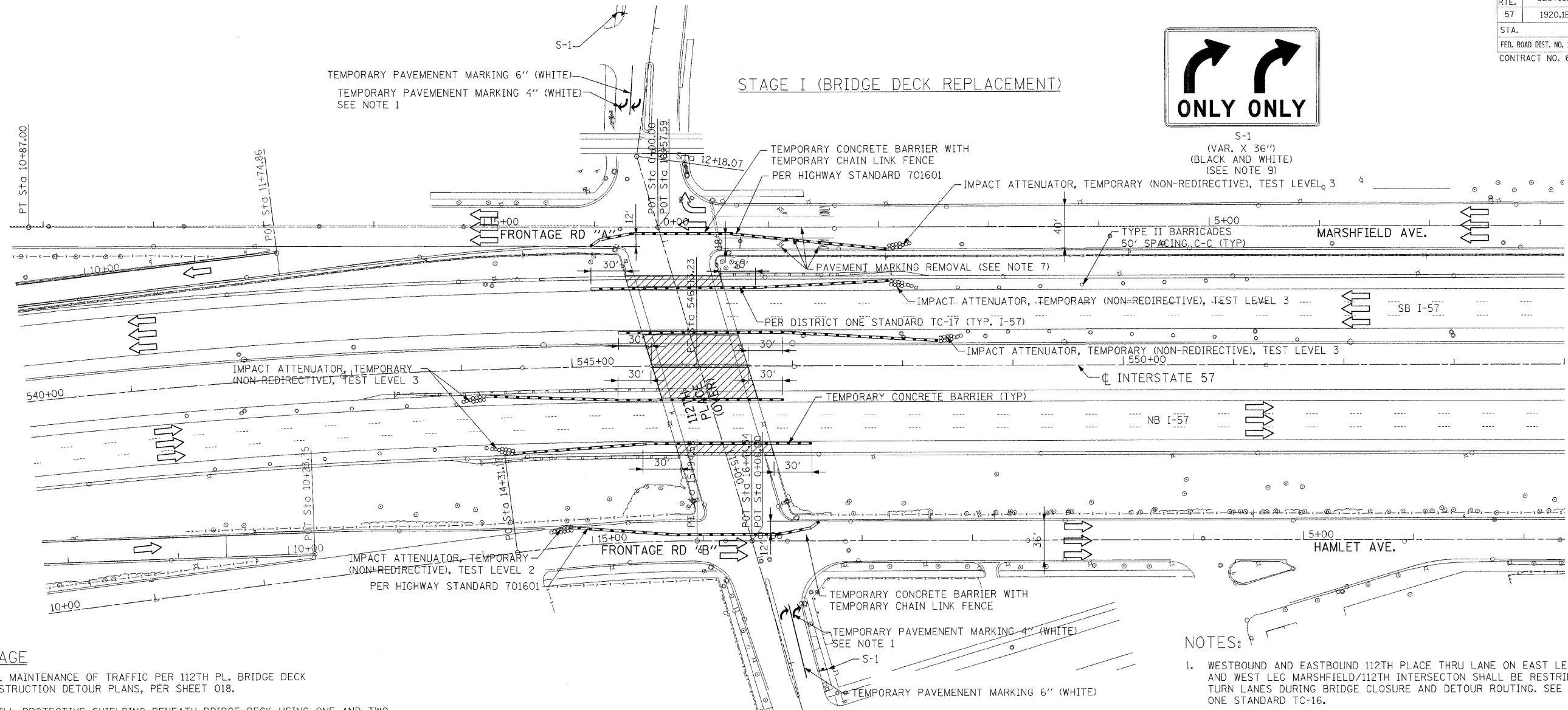
TENG

TENG & ASSOCIATES, INC.
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205 N. MICHIGAN AVE. CHICAGO, IL 60601
TELEPHONE: 312/616-0000

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S-1
(VAR. X 36")
(BLACK AND WHITE)
(SEE NOTE 9)



PRESTAGE

1. INSTAL MAINTENANCE OF TRAFFIC PER 112TH PL. BRIDGE DECK RECONSTRUCTION DETOUR PLANS, PER SHEET 018.

2. INSTALLL PROTECTIVE SHIELDING BENEATH BRIDGE DECK USING ONE AND TWO LANE CLOSURES ON I-57 PER DISTRICT ONE HIGHWAY STANDARD TC-6 AND TC-7.

STAGE I (BRIDGE DECK REPLACEMENT)

1. INSTALL MAINTENANCE OF TRAFFIC DEVICES PER PLANS AND STANDARDS.

2. INSTALL TEMPORARY TRAFFIC SIGNALS.

3. REMOVE EXISTING TRAFFIC SIGNALS.

4. SHOULDER CLOSURE PER IDOT DISTRICT 1 STANDARD, TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES, PARTIAL RAMP CLOSURES, TC-17.

5. REMOVE AND REPLACE EXISTING CONCRETE DECK.

6. REMOVE AND REPLACE ABUTMENT BACKWALL AND PORTIONS OF RETAINING WALL.

7. REMOVE EXISTING PAVEMENT, APPROACH PAVEMENT, CURB & GUTTER, LIGHTING AND SIDEWALK.

8. CONSTRUCT PROPOSED CURB & GUTTER, PCC BASE COURSE, BRIDGE APPROACH SLAB, BRIDGE APPROACH PAVEMENT, BRIDGE APPROACH PAVEMENT CONNECTOR, LIGHTING, AND SIDEWALK PER PLANS AND STANDARDS.

9. INSTALL PROPOSED TRAFFIC SIGNALS.

10. REMOVE TEMPORARY TRAFFIC SIGNALS.

STAGE II (FRONTAGE ROAD RESURFACING)

1. RESURFACING, CURB AND GUTTER AND SIDEWALK REMOVAL & REPLACEMENT PER ROADWAY PLANS AND STANDARD 701411, 701601, 701701, AND 701801.

2. MEDIAN REMOVAL PARTIAL DEPTH PER ROADWAY PLANS.

3. PLACE PAVEMENT MARKING AND SIGNING PER PAVEMENT MARKING AND SIGNING PLANS.

9. THE COST OF SIGN S-1 WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICE FOR MAINTENANCE OF TRAFFIC.

NOTES:

- WESTBOUND AND EASTBOUND 112TH PLACE THRU LANE ON EAST LEG HAMLET/112TH AND WEST LEG MARSHFIELD/112TH INTERSECTON SHALL BE RESTRIPE FOR TWO RIGHT TURN LANES DURING BRIDGE CLOSURE AND DETOUR ROUTING. SEE DISTRICT ONE STANDARD TC-16.
- ALONG I-57 BEGIN AND END TEMPORARY CONCRETE BARRIER AT A MINIMUM OF 30 FT FROM THE END OF BRIDGE PIER AS SHOWN ON PLAN.
- THE ENGINEER SHALL BE NOTIFIED 48 HOURS IN ADVANCE OF ANY CHANGE IN CONSTRUCTION STAGING ALONG 112TH PLACE, BUT 72 HOURS IN ADVANCE ALONG I-57.
- BARRICADES: THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED - ONE (1) WEIGHTED SANDBAG ACROSS EACH BOTTOM RAIL.
- TEMPORARY FENCE MUST BE A CHAIN LINK FENCE THAT IS A MINIMUM OF 6' TALL. FENCE MAY BE USED NOT NEW.
- TEMPORARY PAVEMENT MARKING 4" (YELLOW) SHALL BE PLACED ALONG SIDE TEMPORARY CONCRETE BARRIER ALONG THE FRONTAGE ROADS AND WILL BE PAID FOR AS TEMPORARY PAVEMENT MARKING 4".
- ALL CONFLICTING PAVEMENT MARKINGS SHALL BE REMOVED AND PAID FOR AS PAVEMENT MARKING REMOVAL.
- RAILROAD FLAGGERS WILL BE REQUIRED ACCORDING TO ARTICLE 107.12 OF THE STANDARD SPECIFICATIONS

LEGEND

- TRAFFIC LANE/DIRECTION OF TRAVEL
- DRUMS (TYPE II BARRICADES) W/ STEADY BURN LIGHTS @ SPACING PER IDOT DISTRICT ONE STANDARDS
- TEMPORARY CONCRETE BARRIER WALL
- WORK ZONE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)
**M.O.T. INTERSTATE 57
AND 112TH PL
STAGING AND TRAFFIC CONTROL**

SCALE: 1"=50'
DATE: 01/16/09
DRAWN BY: BRH
CHECKED BY: ACL

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
255 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-0000

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	020
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62533				

GENERAL NOTES FOR EROSION CONTROL



-PRIOR TO START OF CONSTRUCTION IN ANY AREA INSTALL PERIMETER PERIMETER EROSION BARRIER AT LOCATIONS SHOWN ON THE PLANS.

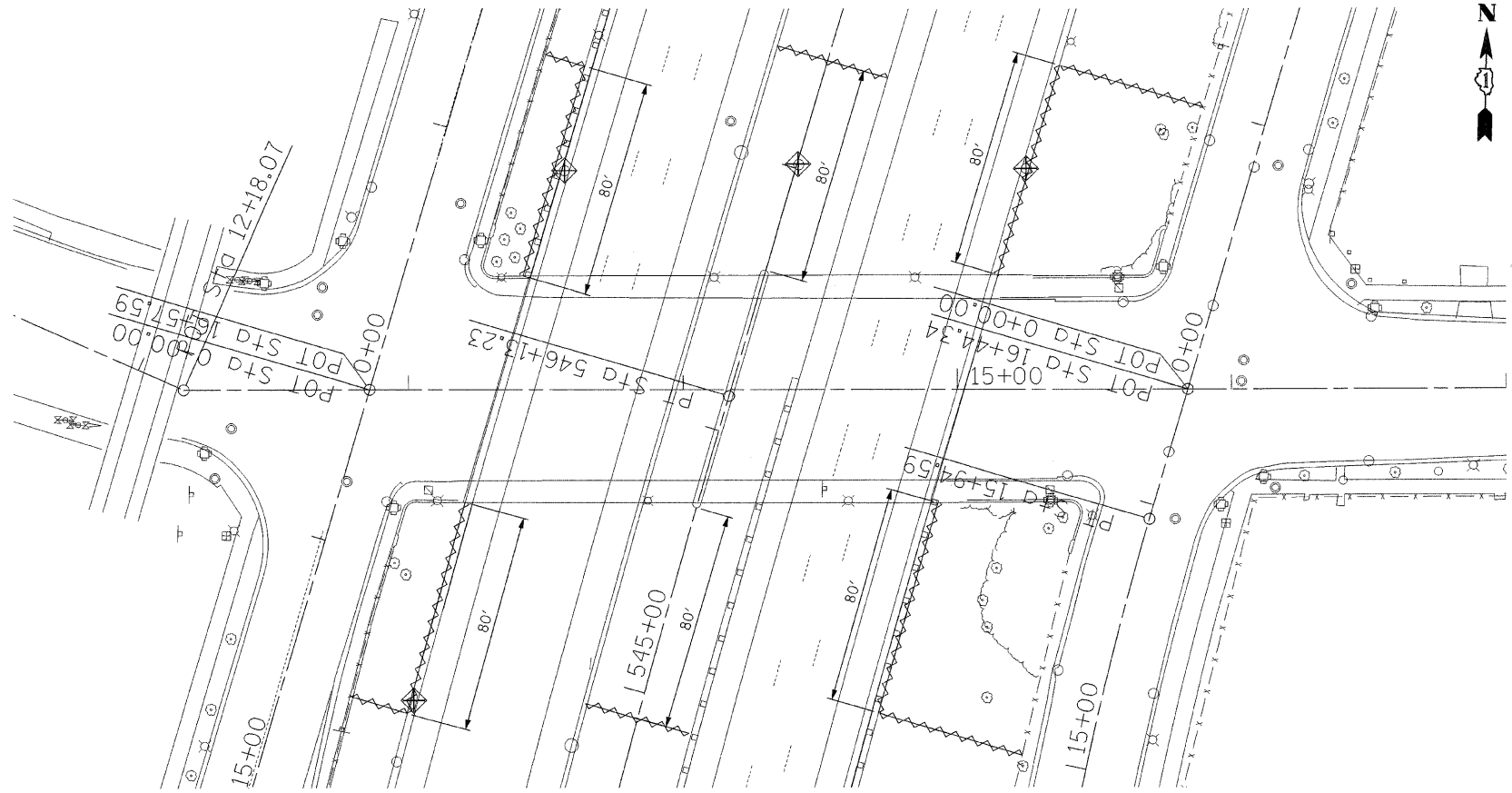
-THE PERIMETER EROSION BARRIER SHALL REMAIN IN PLACE UNTIL ALL DISTURBED AREAS HAVE BEEN STABILIZED WITH VEGETATION. AT THIS TIME, THE PERIMETER EROSION BARRIER SHALL BE REMOVED AND AREAS DAMAGED BY THE FENCE INSTALLATION RESTORED.

-ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. ALL DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.

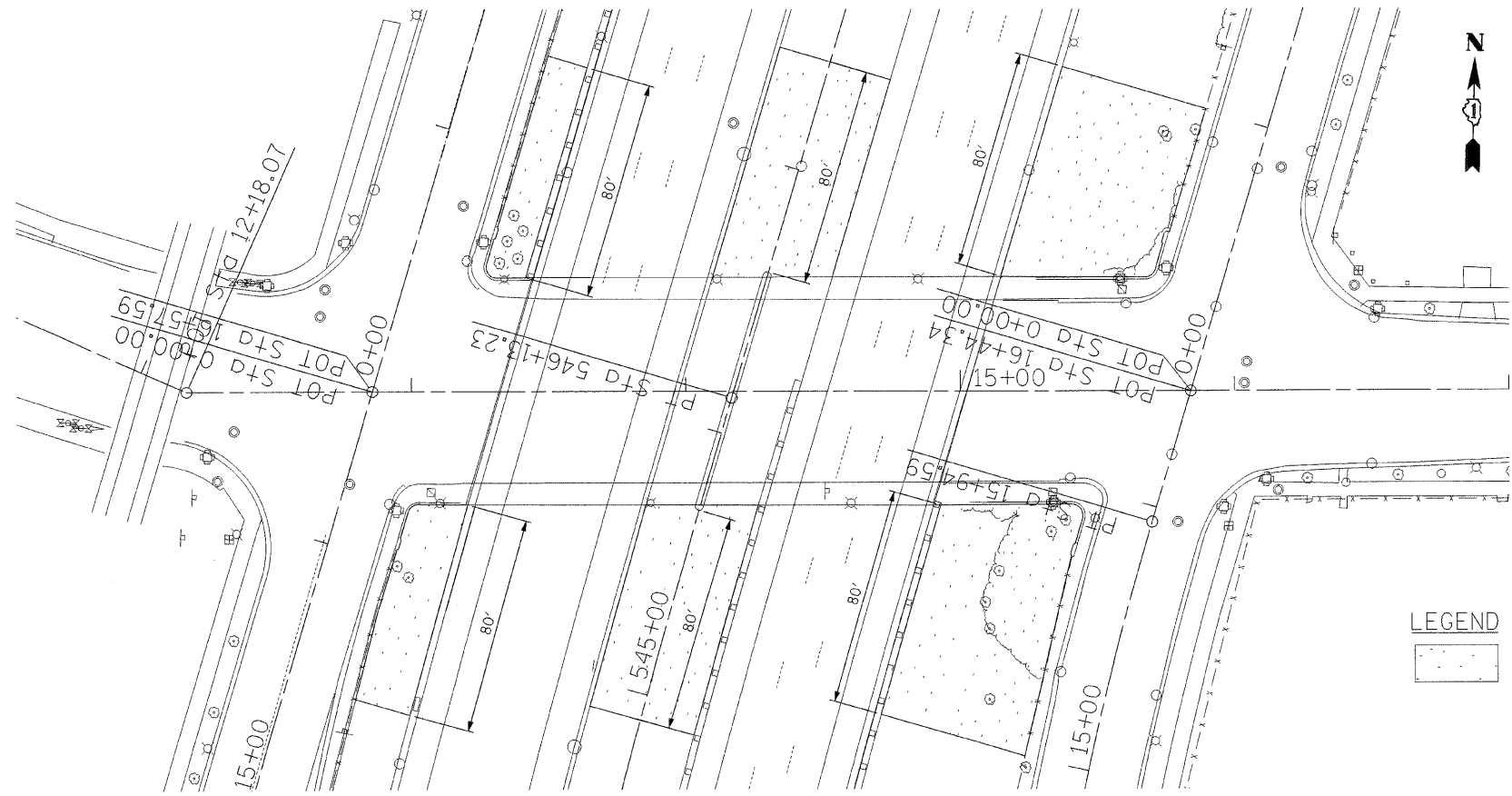
-ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE GENERAL CONTRACTOR WILL BE RESPONSIBLE FOR INSPECTION AND REPAIR DURING CONSTRUCTION.

LEGEND

-  PERIMETER EROSION BARRIER
-  INLET & PIPE PROTECTION



EROSION CONTROL
LANDSCAPING



LEGEND

-  SEEDING, CLASS 2A

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

**EROSION CONTROL
AND LANDSCAPING**

SCALE: 1"=30'
DATE: 01/16/09

DRAWN BY: BRH
CHECKED BY: ACL

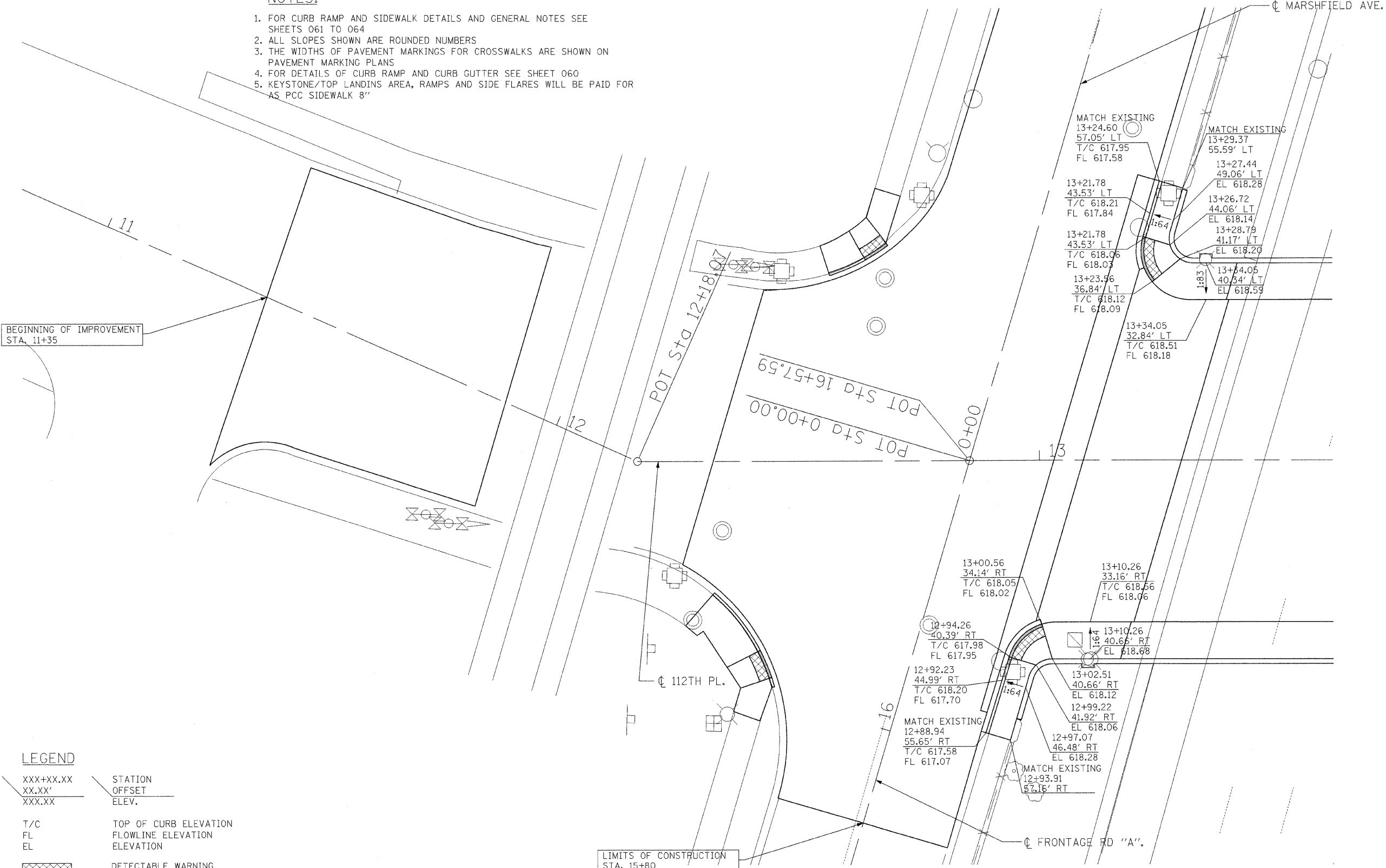
TENG
TENG & ASSOCIATES, INC.
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205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/451-0500

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	024
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62533				

NOTES:

1. FOR CURB RAMP AND SIDEWALK DETAILS AND GENERAL NOTES SEE SHEETS 061 TO 064
2. ALL SLOPES SHOWN ARE ROUNDED NUMBERS
3. THE WIDTHS OF PAVEMENT MARKINGS FOR CROSSWALKS ARE SHOWN ON PAVEMENT MARKING PLANS
4. FOR DETAILS OF CURB RAMP AND CURB GUTTER SEE SHEET 060
5. KEYSTONE/TOP LANDINGS AREA, RAMPS AND SIDE FLARES WILL BE PAID FOR AS PCC SIDEWALK 8"



BEGINNING OF IMPROVEMENT
STA. 11+35

LIMITS OF CONSTRUCTION
STA. 15+80

LEGEND

- XXX+XX.XX STATION
- XX.XX' OFFSET
- XXX.XX ELEV.
- T/C TOP OF CURB ELEVATION
- FL FLOWLINE ELEVATION
- EL ELEVATION
- [Cross-hatched box] DETECTABLE WARNING TILES
- [Solid shaded box] DEPRESSED CURB
- 0.5% SLOPE DIRECTION
- SLOPE IN PERCENT
- 1:6.4 SLOPE DIRECTION
- SLOPE IN RISE:RUN

**PROPOSED ADA RAMP GRADING PLAN
FOR 112TH PLACE AND MARSHFIELD AVE.**

NOTE: ALL STATIONING BASED OFF 112TH PLACE UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)
**112TH PLACE AND
MARSHFIELD AVE
ADA SIDEWALK DETAILS**

SCALE: 1"=10'
DATE: 01/16/09

DRAWN BY: BRH
CHECKED BY: ACL

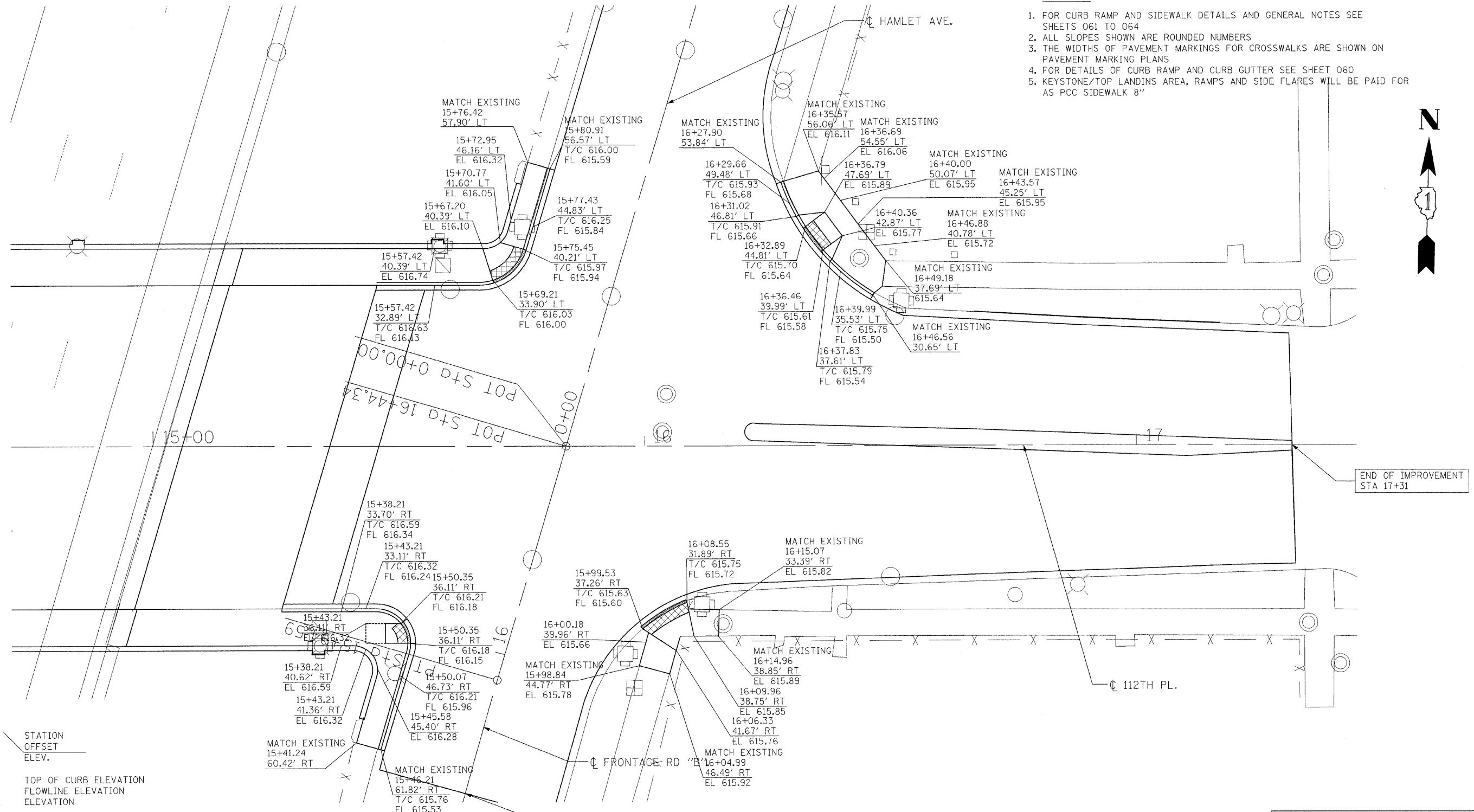
TENG
TENG & ASSOCIATES, INC.
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205 N. MICHIGAN AVE. CHICAGO, IL 60601
TELEPHONE: 312/616-0000

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	025
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62533				

NOTES:

- FOR CURB RAMP AND SIDEWALK DETAILS AND GENERAL NOTES SEE SHEETS 061 TO 064
- ALL SLOPES SHOWN ARE ROUNDED NUMBERS
- THE WIDTHS OF PAVEMENT MARKINGS FOR CROSSWALKS ARE SHOWN ON PAVEMENT MARKING PLANS
- FOR DETAILS OF CURB RAMP AND CURB GUTTER SEE SHEET 060
- KEYSTONE/TOP LANDINGS AREA, RAMPS AND SIDE FLARES WILL BE PAID FOR AS PCC SIDEWALK 8"



LEGEND

- XXX+XX.XX STATION
- XX.XX' OFFSET
- XXX.XX ELEV.
- T/C TOP OF CURB ELEVATION
- FL FLOWLINE ELEVATION
- EL ELEVATION
- [Cross-hatched box] DETECTABLE WARNING TILES
- [Solid shaded box] DEPRESSED CURB
- 0.5% SLOPE DIRECTION
- SLOPE IN PERCENT
- 1:64 SLOPE DIRECTION
- SLOPE IN RISE:RUN

PROPOSED ADA RAMP GRADING PLAN FOR 112TH PLACE AND HAMLET AVE.

NOTE: ALL STATIONING BASED OFF 112TH PLACE UNLESS OTHERWISE NOTED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I. ROUTE 57 (INTERSTATE 57)
**112TH PLACE AND HAMLET AVE
ADA SIDEWALK DETAILS**

SCALE: 1"=10'
DATE: 01/16/09
DRAWN BY: BRH
CHECKED BY: ACL

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TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-9000

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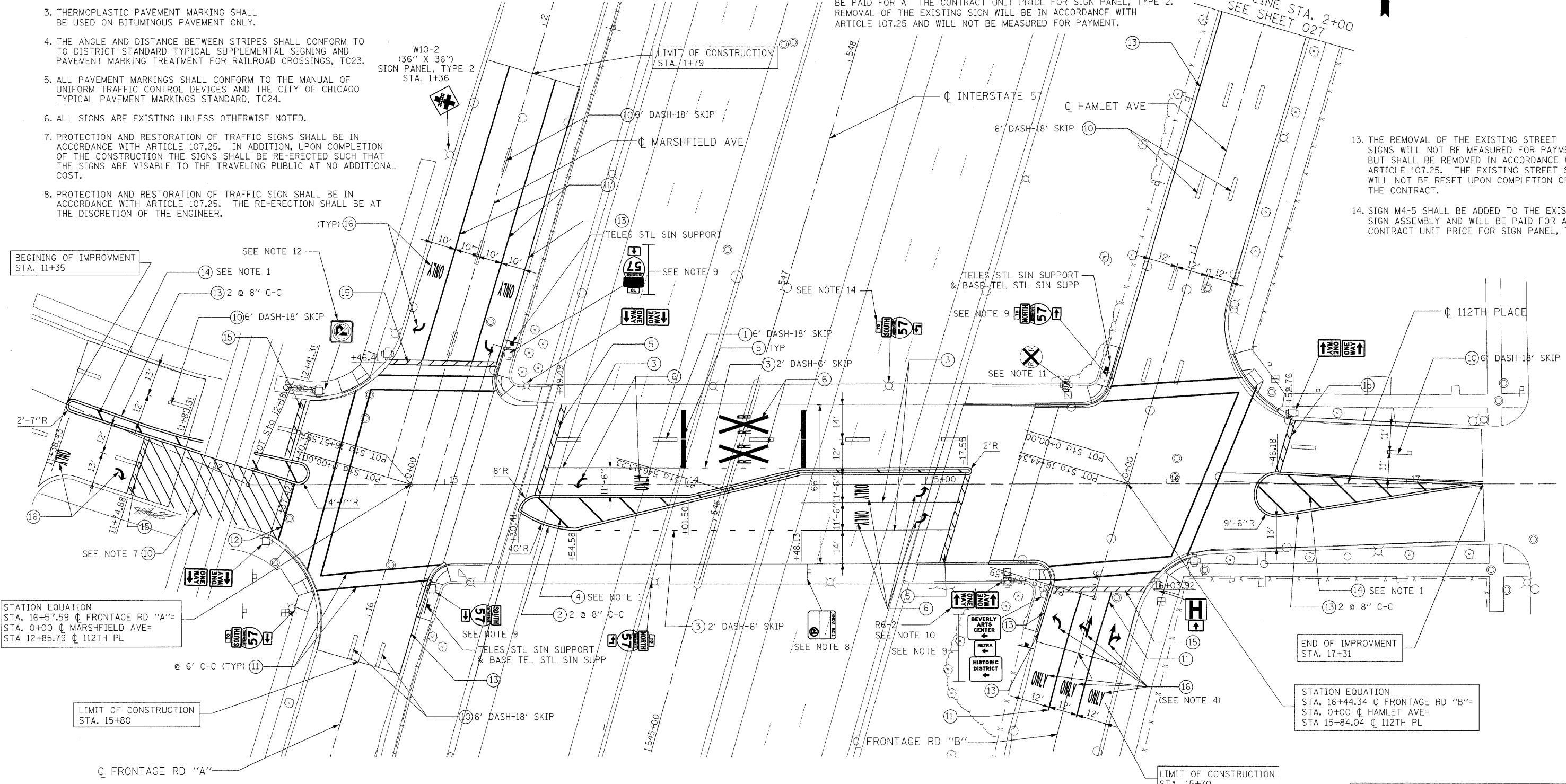
NOTES:

- 12" DIAGONAL LINES PLACED IN PAINTED MEDIAN AREAS SHALL HAVE MINIMUM FIVE (5) EQUALLY SPACED DIAGONAL LINES.
- BRIDGE DECKS REQUIRE PREFORMED PLASTIC PAVEMENT MARKINGS.
- THERMOPLASTIC PAVEMENT MARKING SHALL BE USED ON BITUMINOUS PAVEMENT ONLY.
- THE ANGLE AND DISTANCE BETWEEN STRIPES SHALL CONFORM TO DISTRICT STANDARD TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS, TC23.
- ALL PAVEMENT MARKINGS SHALL CONFORM TO THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES AND THE CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS STANDARD, TC24.
- ALL SIGNS ARE EXISTING UNLESS OTHERWISE NOTED.
- PROTECTION AND RESTORATION OF TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH ARTICLE 107.25. IN ADDITION, UPON COMPLETION OF THE CONSTRUCTION THE SIGNS SHALL BE RE-ERECTED SUCH THAT THE SIGNS ARE VISABLE TO THE TRAVELING PUBLIC AT NO ADDITIONAL COST.
- PROTECTION AND RESTORATION OF TRAFFIC SIGN SHALL BE IN ACCORDANCE WITH ARTICLE 107.25. THE RE-ERECTION SHALL BE AT THE DISCRETION OF THE ENGINEER.

- EXISTING SIGN ASSEMBLIES WILL BE RE-ERECTED PER ARTICLE 107.25 ON PROPOSED TELESCOPING STEEL SIGN SUPPORTS.
- TWO PROPOSED R6-2 (18" X 24") SIGNS SHALL BE INSTALLED ON EITHER THE PROPOSED LIGHT POLE OR TRAFFIC SIGNAL PER DIRECTION OF THE ENGINEER AND WILL BE PAID FOR AS SIGN PANEL, TYPE 1. THE INSTALLATION OF THE SIGNS SHALL BE VISABLE TO THE TRAVELING PUBLIC.

- A PROPOSED W10-1 SIGN (36") SHALL BE FURNISHED, FABRICATED AND INSTALLED ON PROPOSED LIGHT POLE AT STA. 14+11 AND SHALL BE VISIBLE TO THE TRAVELING PUBLIC. THIS WORK WILL BE PAID FOR AS SIGN PANEL, TYPE 1. REMOVAL OF THE EXISTING WILL BE IN ACCORDANCE WITH ARTICLE 107.25 AND NOT BE MEASURED FOR PAYMENT.
- A PROPOSED R8-3A SIGN (24" X 24") SHALL BE FURNISHED, FABRICATED AND INSTALLED ON THE PROPOSED TRAFFIC SIGNAL. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR SIGN PANEL, TYPE 2. REMOVAL OF THE EXISTING SIGN WILL BE IN ACCORDANCE WITH ARTICLE 107.25 AND WILL NOT BE MEASURED FOR PAYMENT.

- THE REMOVAL OF THE EXISTING STREET SIGNS WILL NOT BE MEASURED FOR PAYMENT, BUT SHALL BE REMOVED IN ACCORDANCE WITH ARTICLE 107.25. THE EXISTING STREET SIGNS WILL NOT BE RESET UPON COMPLETION OF THE CONTRACT.
- SIGN M4-5 SHALL BE ADDED TO THE EXISTING SIGN ASSEMBLY AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR SIGN PANEL, TYPE 1.



PAVEMENT MARKING LEGEND

- | | | | | |
|--|--|--|---|---|
| ① PREFORMED PLASTIC PAVEMENT MARKING, TYPE B- LINE 4" (WHITE) | ④ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B- LINE 12" (YELLOW) | ⑩ THERMOPLASTIC PAVEMENT MARKING- LINE 4" (WHITE) | ⑬ THERMOPLASTIC PAVEMENT MARKING- LINE 4" (YELLOW) | ⑯ THERMOPLASTIC PAVEMENT MARKING- LETTERS AND SYMBOLS |
| ② PREFORMED PLASTIC PAVEMENT MARKING, TYPE B- LINE 4" (YELLOW) | ⑤ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B- LINE 24" (WHITE) | ⑪ THERMOPLASTIC PAVEMENT MARKING- LINE 6" (WHITE) | ⑭ THERMOPLASTIC PAVEMENT MARKING- LINE 12" (YELLOW) | |
| ③ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B- LINE 6" (WHITE) | ⑥ PREFORMED PLASTIC PAVEMENT MARKING, TYPE B LETTERS AND SYMBOLS | ⑫ THERMOPLASTIC PAVEMENT MARKING- LINE 12" (WHITE) | ⑮ THERMOPLASTIC PAVEMENT MARKING- LINE 24" (WHITE) | |

LEGEND

- ◻ EXISTING MOUNTED SIGN
- ◻ PROPOSED MOUNTED SIGN

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

112TH PLACE SIGNING AND PROPOSED PAVEMENT MARKINGS

SCALE: 1"=20'
DATE: 01/16/09

DRAWN BY: BRH
CHECKED BY: ACL

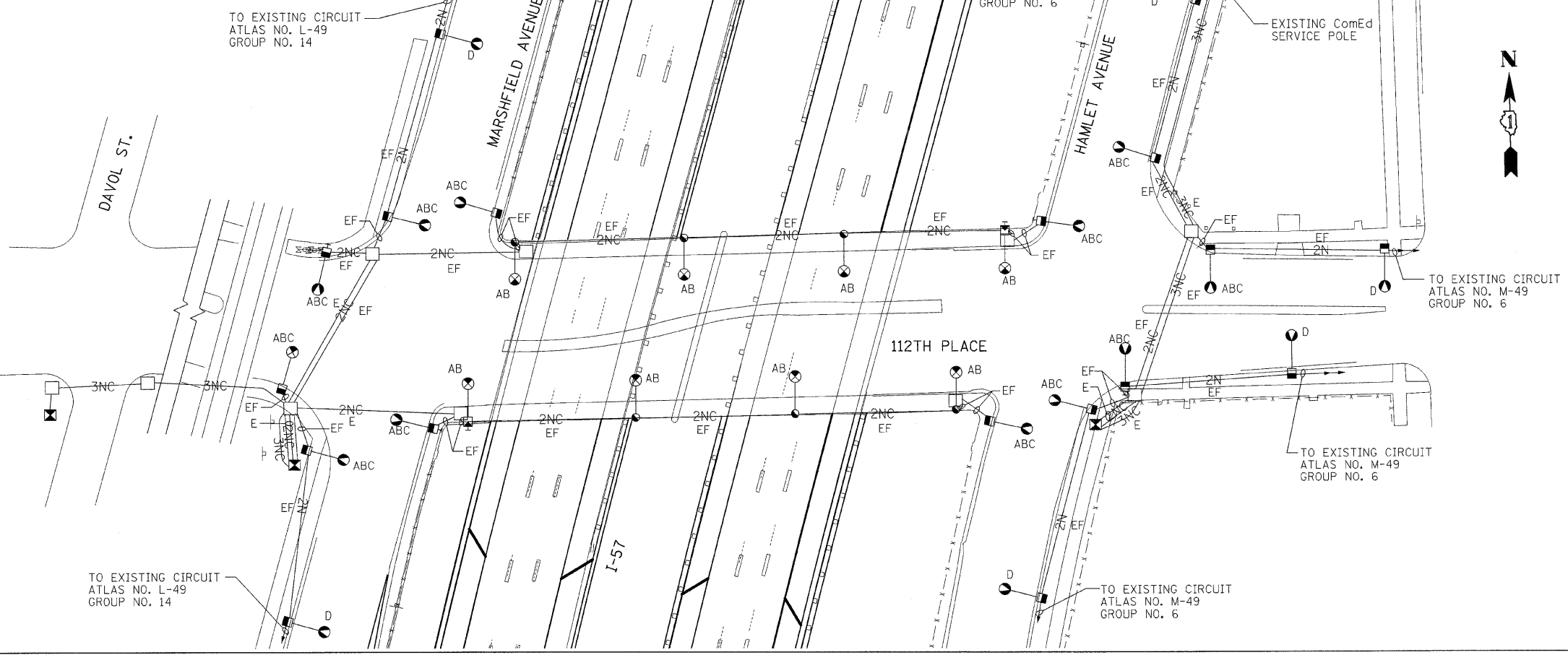


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TELEPHONE: 312/816-0000

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	029
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				

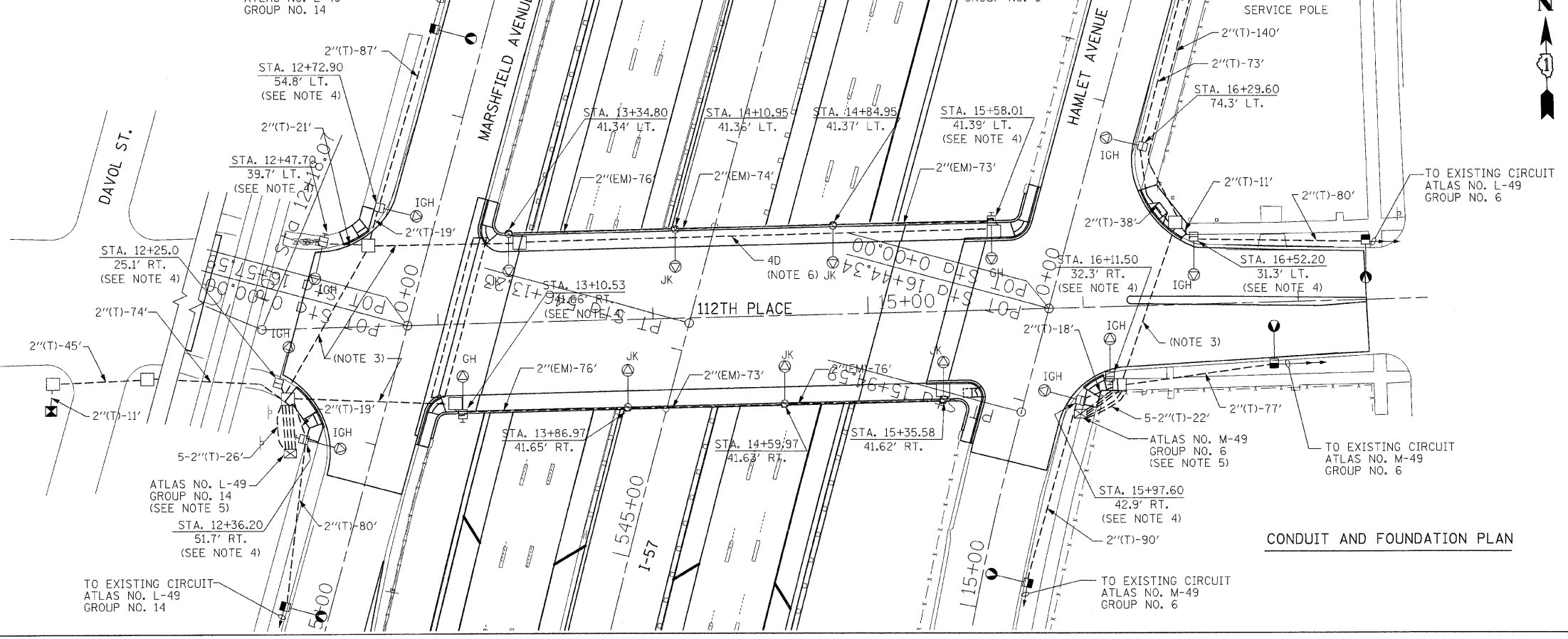
EXISTING LIGHTING SYSTEM



REMOVAL NOTES

- A - REMOVE H.P.S.V. LUMINAIRE AND MAST ARM
- B - REMOVE POLE, CITY STEEL
- C - REMOVE FOUNDATION
- D - EXISTING POLE TO REMAIN
- E - EXISTING CABLE TO BE REMOVED
- F - EXISTING CONDUIT TO BE REMOVED

PROPOSED LIGHTING SYSTEM



INSTALLATION NOTES

- G - INSTALL 400W H.P.S.V. LUMINAIRE AND 12' MAST ARM
- H - INSTALL POLE, CITY STEEL, ANCHOR BASE, SIZE AS INDICATED
- I - INSTALL NEW CONCRETE FOUNDATION
- A - CONDUIT ATTACHED TO STRUCTURE
- EM - CONDUIT EMBEDDED IN BARRIER WALL
- T - CONDUIT IN TRENCH
- J - INSTALL POLE/CITY ALUMINUM ANCHOR BASE SIZE AS INDICATED
- K - INSTALL 400W HPSV LUMINAIRE & DAVIT ARM

NOTE:

1. FOR LEGEND AND NOTES REFER TO DRAWING NO. E-1.

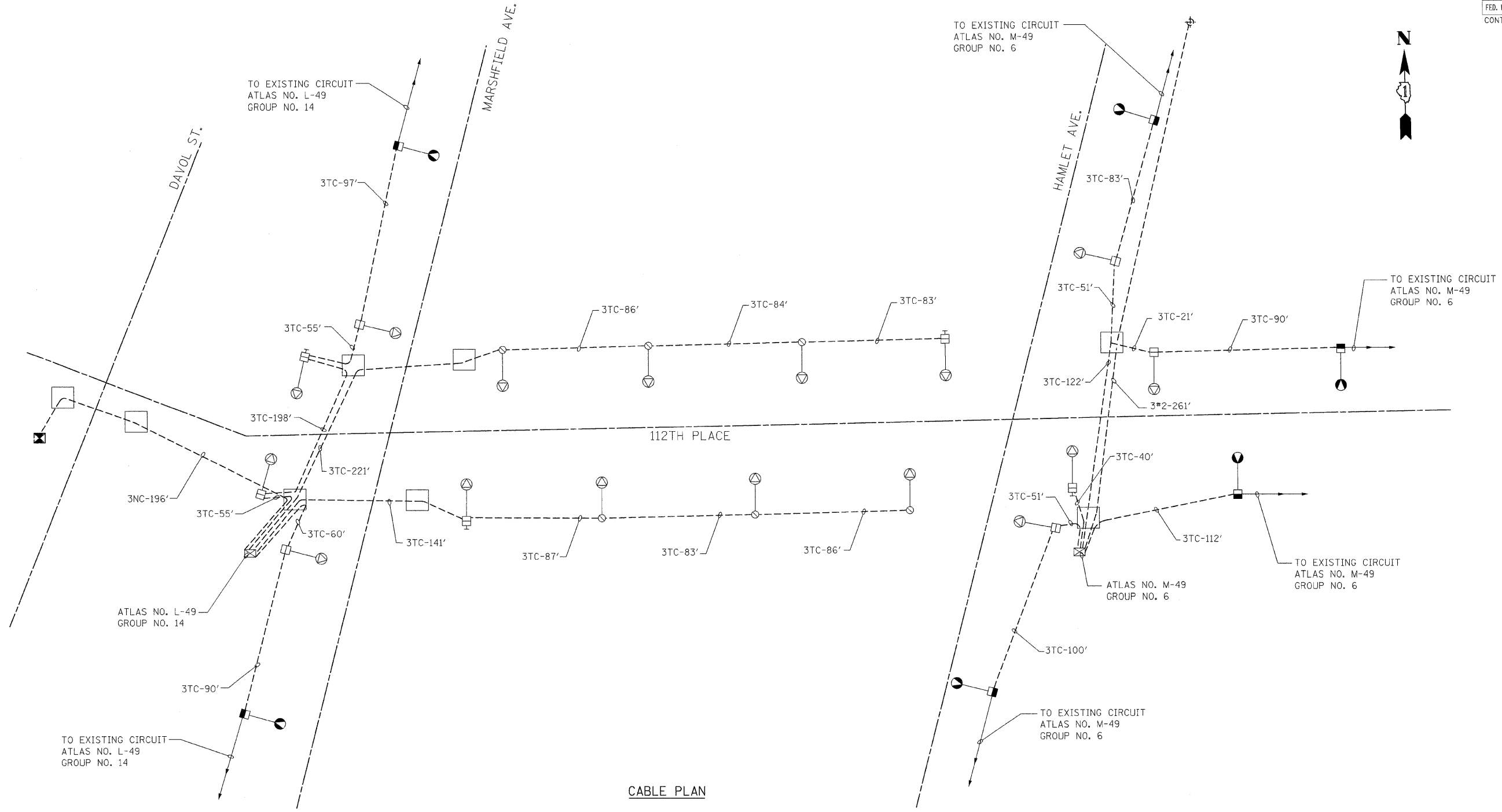
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I ROUTE 57 (INTERSTATE 57)
 CITY OF CHICAGO
 LIGHTING PLAN

SCALE: 1" = 30'
 DATE: 01/16/09
 DRAWN BY: ACL
 CHECKED BY: PR
TENG
TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312/616-0000

CONDUIT AND FOUNDATION PLAN

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	030
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				



CABLE PLAN

112TH ST BORDER.DGN
 11-16-2009 10:51:53
 T:\DOCUMENT\932086\01\CIVIL\ADGN\EXH00\FB.DGN
 1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63
 HECHTBR

NOTE:
FOR LEGEND REFER TO DRAWING NO. E-1.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I. ROUTE 57 (INTERSTATE 57)

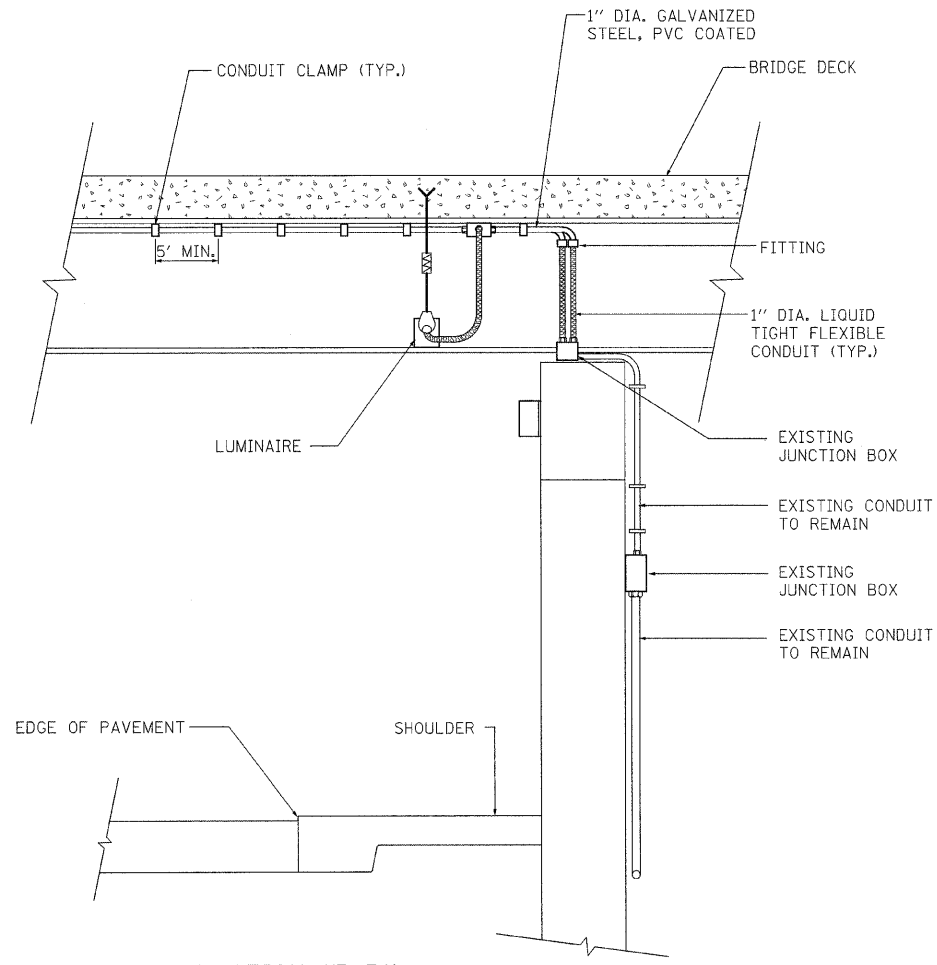
WIRING DIAGRAM

SCALE: NONE
DATE: 01/16/09

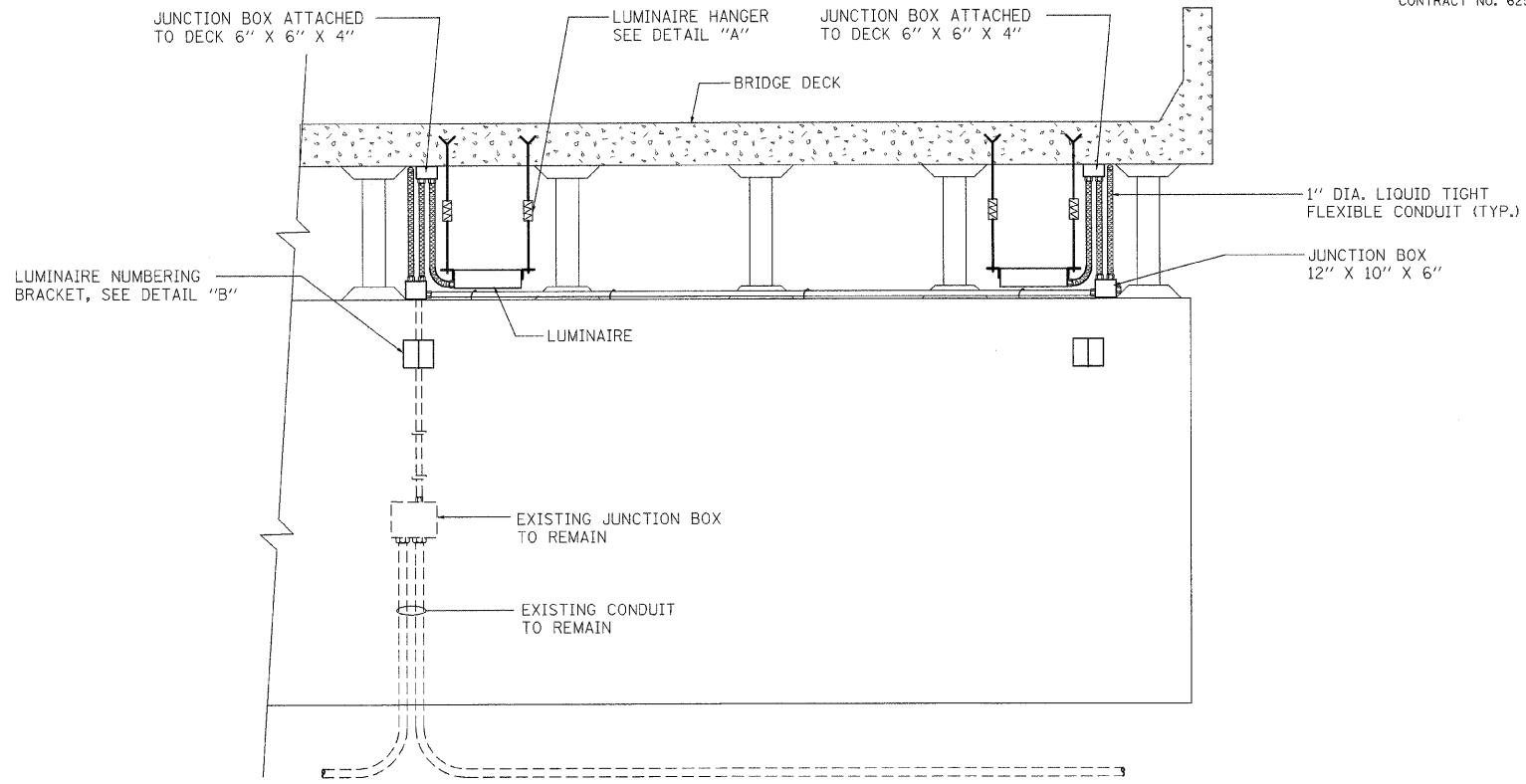
DRAWN BY: ACL
CHECKED BY: PR

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-0000

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	032
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62533				

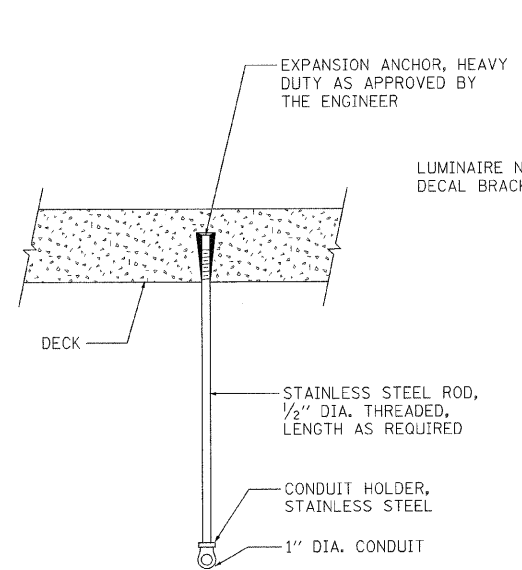


ELEVATION "B-B"
NOT TO SCALE

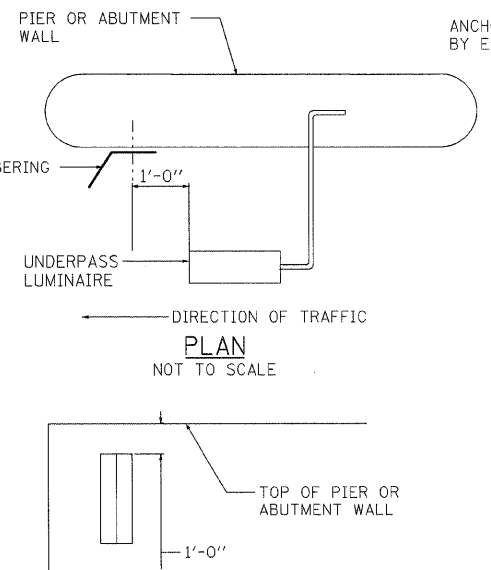


ELEVATION "A-A"

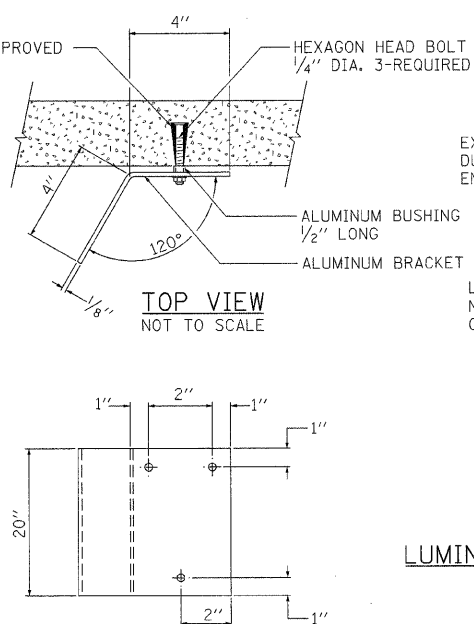
NOT TO SCALE



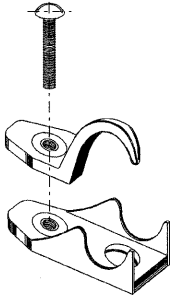
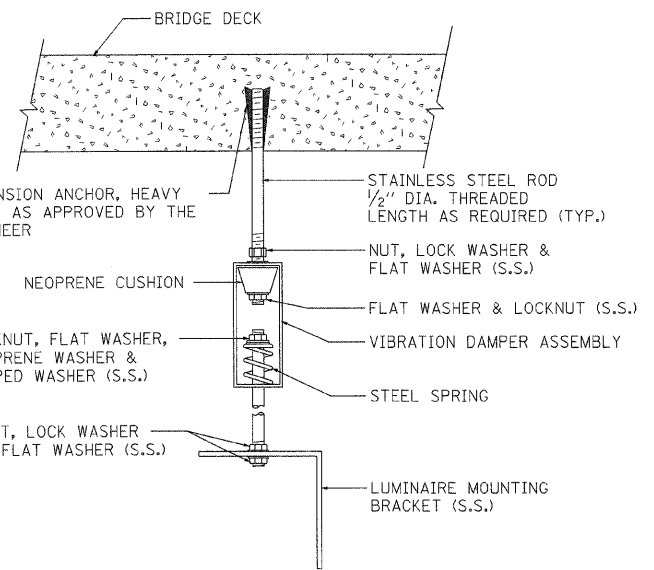
CONDUIT HANGER, DETAIL "C"
NOT TO SCALE



ELEVATION LUMINAIRE NUMBERING DECAL BRACKET, DETAIL "B"
NOT TO SCALE



LUMINAIRE HANGER ASSEMBLY, DETAIL "A"
NOT TO SCALE



DETAIL "D"
(CONDUIT CLAMP WITH CLAMP BACK)

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I. ROUTE 57 (INTERSTATE 57)
IDOT UNDERPASS LIGHTING DETAILS






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DATE: 01/16/09

DRAWN BY: ACL
CHECKED BY: PR

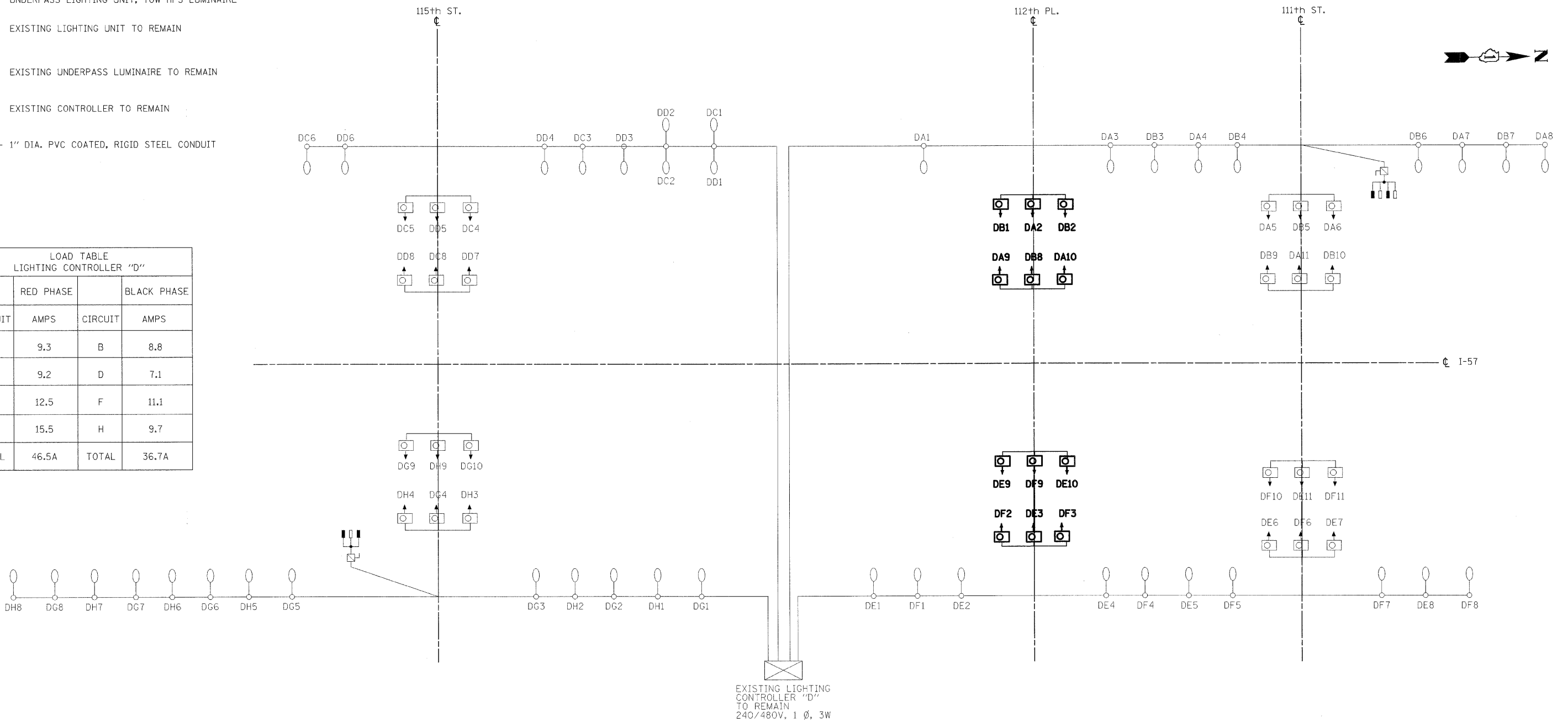
TENG
TENGE & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
209 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-2000

NORTH ST. BORDER/DJDN
 I-16-2009-1317251
 T:\DOCUMENT\931086\N\C\W\ADON\EXH008\FD.DGN
 HECHTER
 1 2 3 4 5 6 7 8 9 10 11 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63

LEGEND:

-  UNDERPASS LIGHTING UNIT, 70W HPS LUMINAIRE
-  EXISTING LIGHTING UNIT TO REMAIN
-  EXISTING UNDERPASS LUMINAIRE TO REMAIN
-  EXISTING CONTROLLER TO REMAIN
-  1" DIA. PVC COATED, RIGID STEEL CONDUIT

LOAD TABLE LIGHTING CONTROLLER "D"			
	RED PHASE		BLACK PHASE
CIRCUIT	AMPS	CIRCUIT	AMPS
A	9.3	B	8.8
C	9.2	D	7.1
E	12.5	F	11.1
G	15.5	H	9.7
TOTAL	46.5A	TOTAL	36.7A



DRAWING FOR INFORMATION ONLY

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

WIRING DIAGRAM

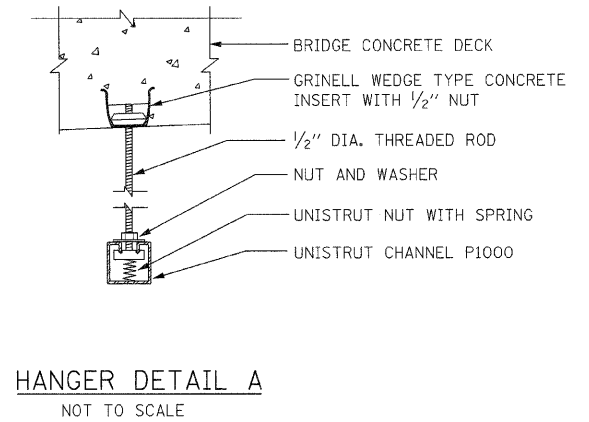
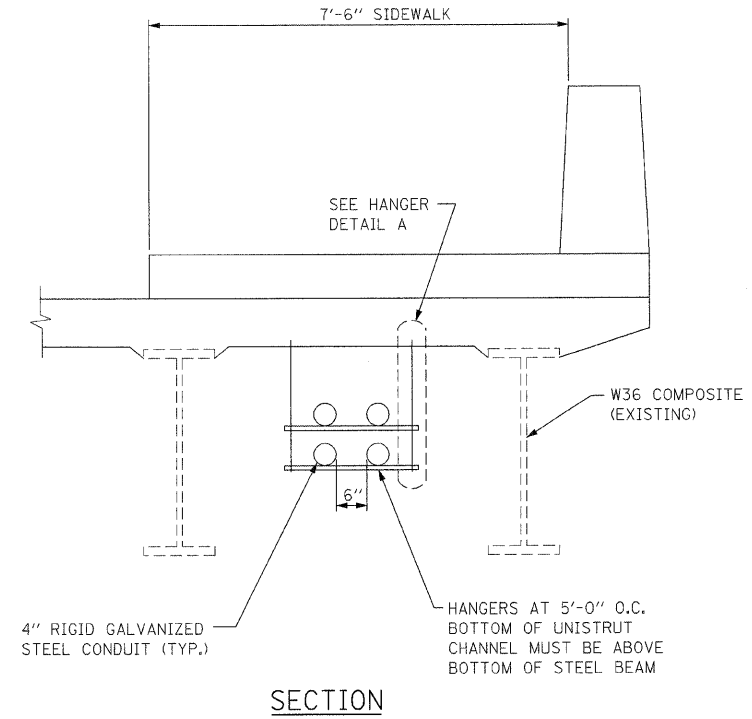
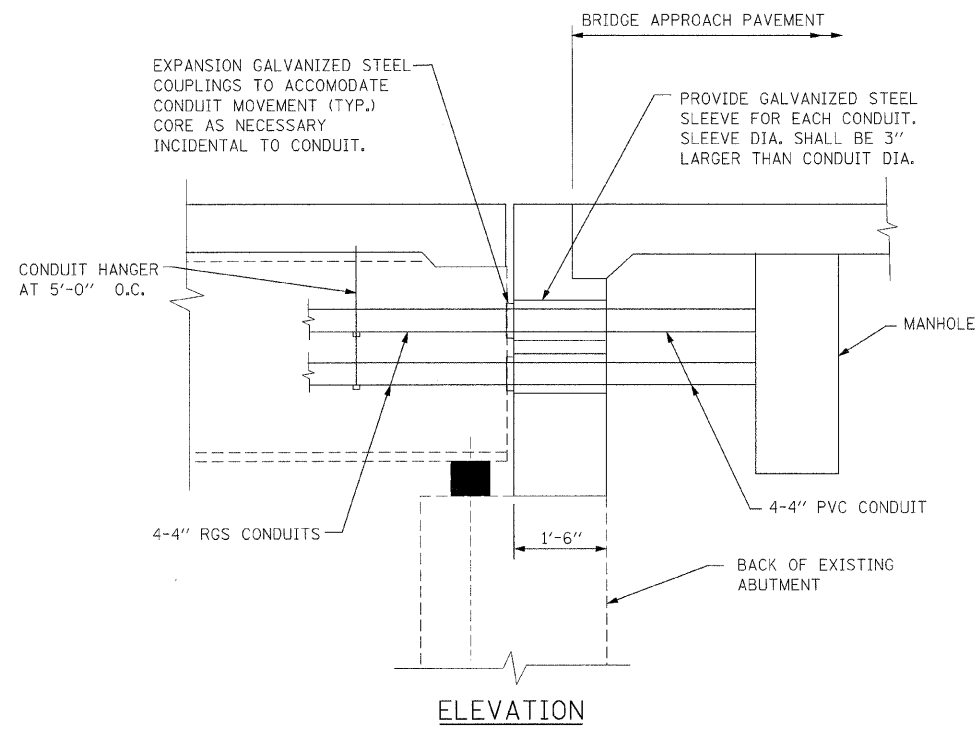
SCALE: NONE
DATE: 01/16/09

DRAWN BY: ACL
CHECKED BY: PR

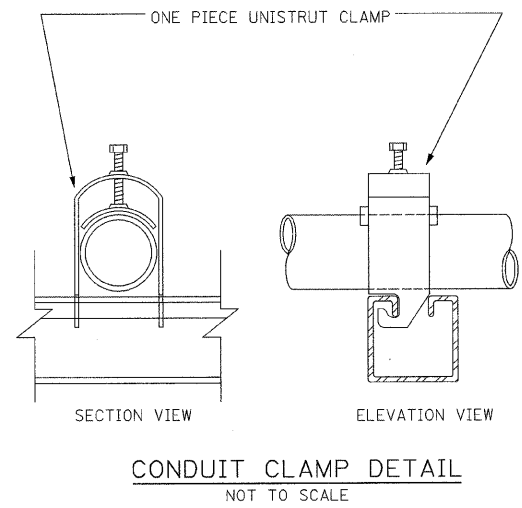
TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-0000

..\112TH ST_BORDER.DGN
 I-6-2009_131150
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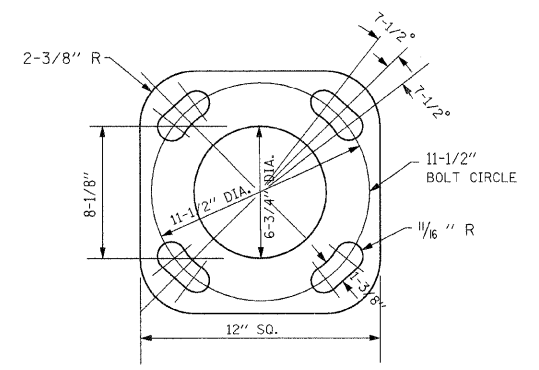
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	034
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				



EXPANSION COUPLING DETAIL
NOT TO SCALE



CONDUIT CLAMP DETAIL
NOT TO SCALE



VIBRATION ISOLATION PAD DETAIL
NOTE 1

NOTE 1:
BRIDGE MOUNTED POLES WILL HAVE 1/2 in. THICK VIBRATION ISOLATION MOUNTING PAD INCLUDED WITH THE POLE. THE PAD SHALL HAVE THE SAME SHAPE AS THE BOTTOM OF THE POLE BASE WITH APPROPRIATE BOLT HOLES AND OPENING FOR THE CENTER OF THE POLE. INCLUDED WITH THE PAD SHALL BE FOUR 10 mm (3/8") THICK WASHERS. THE PAD AND WASHERS SHALL BE MADE FROM THE RUGGED ELASTOMERIC MATERIAL. THE ULTIMATE BREAKDOWN OF THE PAD AND WASHERS UNDER COMPRESSIVE LOAD SHALL BE NOT LESS THAN 69,000 KPA (10,000 PSI) FOR THE SPECIFIED THICKNESS WITHOUT EXTRUSION OR DETRIMENTAL REDUCTION IN THICKNESS. THE MATERIAL SHALL ALSO HAVE A SHORE A DUROMETER READING OF NOT LESS THAN 80.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

MISCELLANEOUS DETAILS

SCALE: NONE
DATE: 01/16/09

DRAWN BY: ACL
CHECKED BY: PR

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/416-9000

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	035
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				

BILL OF MATERIALS - TRAFFIC SIGNAL WORK

DESCRIPTION	TOTAL	TOTAL	112TH ST & MARSHFIELD	112TH ST & HAMLET AVE
COILABLE CONDUIT INSTALLATION BY DIRECTIONAL BORING	FOOT	1276	624	652
PVC CONDUIT IN TRENCH 3" (SCHEDULE #80)	FOOT	84	84	0
PVC CONDUIT IN TRENCH 4" (SCHEDULE #80)	FOOT	151	89	62
CONDUIT ATTACHED TO STRUCTURE, 4" DIA., GALVANIZED STEEL	FOOT	920	920	-
CONCRETE FOUNDATION 24" DIA.	FOOT	45	27	18
POLE, CITY, STEEL ANCHOR BASE 10" DIAMETER, 3 GAUGE, 32'-6"	EACH	2	1	1
POLE, CITY, STEEL ANCHOR BASE 10" DIAMETER, 3 GAUGE, 34'-6"	EACH	11	6	5
SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	12	6	6
SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 3-SECTION, MAST ARM MOUNTED	EACH	8	5	3
SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 4-SECTION, BRACKET MOUNTED	EACH	2	1	1
SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, 4-SECTION, MAST ARM MOUNTED	EACH	2	1	1
PEDESTRIAN SIGNAL HEAD, POLYCARBONATE, LED, 1-FACE, BRACKET MOUNTED	EACH	12	6	6
JUNCTION BOX, POLE OR POST MOUNTED	EACH	13	7	6
MAST ARM, STEEL, MONOTUBE 20 FT.	EACH	2	1	1
MAST ARM, STEEL, MONOTUBE 26 FT.	EACH	3	1	2
ELECTRIC CABLE IN CONDUIT NO. 4 2/C	FOOT	130	30	100
ELECTRIC CABLE IN CONDUIT NO. 12 7/C	FOOT	520	370	150
ELECTRIC CABLE IN CONDUIT NO. 12 19/C	FOOT	1358	758	600
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	2	1	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	L SUM	1	0.5	0.5
TRENCH AND BACKFILL WITH SCREENING	FOOT	151	89	62
STREET NAME SIGN	EACH	7	4	3
REMOVE EXISTING TRAFFIC SIGNAL FOUNDATION	EACH	4	2	2
ROD AND CLEAN DUCT IN EXISTING CONDUIT SYSTEM	FOOT	1870	930	940
SIGN, MESSAGE, ELECTRICALLY ILLUMINATED, FIBER OPTIC, BRACKET MOUNTED	EACH	9	5	4
DRILL EXISTING MANHOLE OR HANDHOLE	EACH	15	8	7
MAST ARM, STEEL, MONOTUBE 35 FT.	EACH	1	1	0
CONCRETE FOUNDATION 30" DIA.	FOOT	11	11	0

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 HECHTER

TS-1

REVISIONS	
NAME	DATE

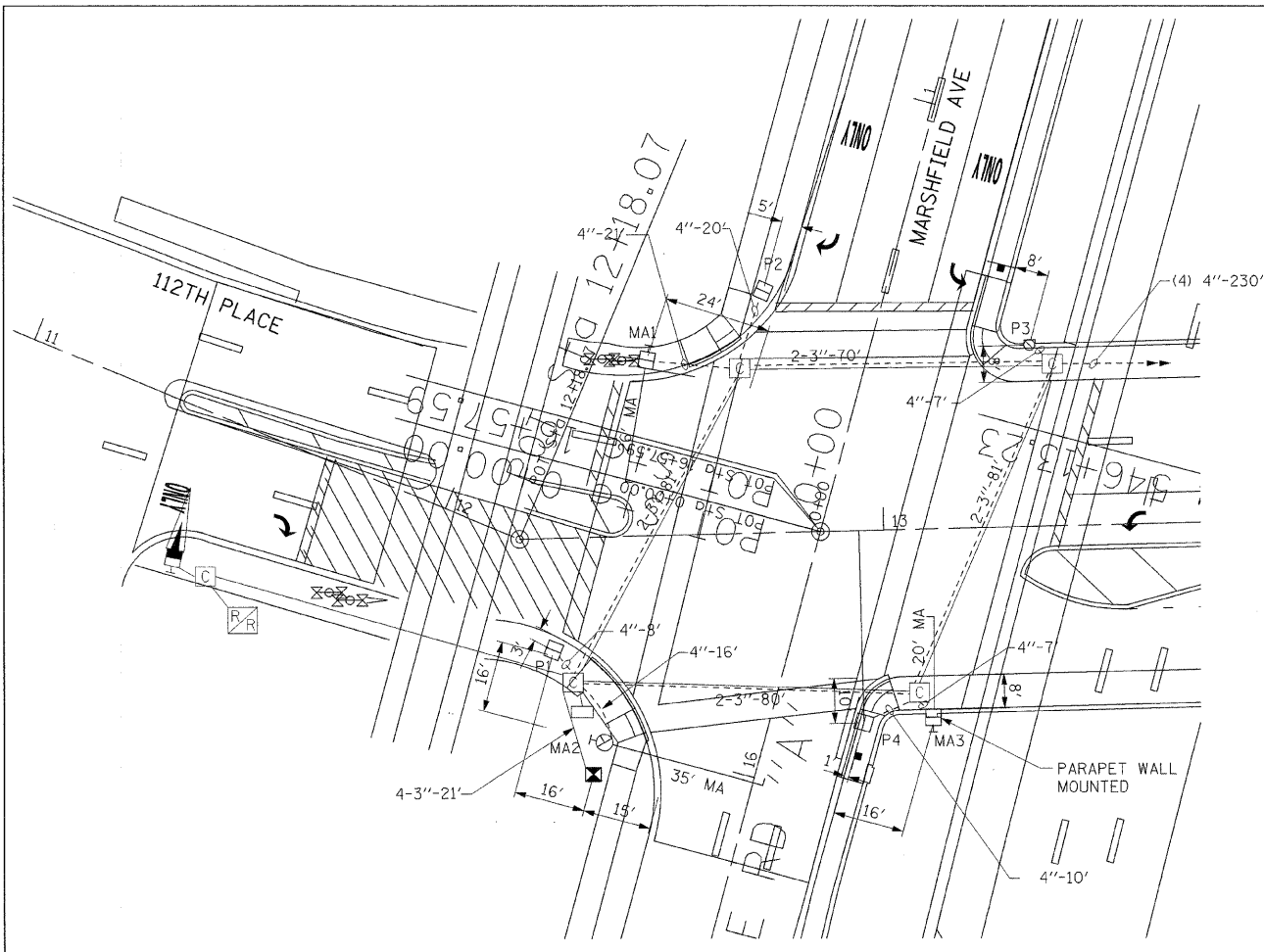
ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I ROUTE 57 (INTERSTATE 57)

TRAFFIC SIGNAL
BILL OF MATERIALS

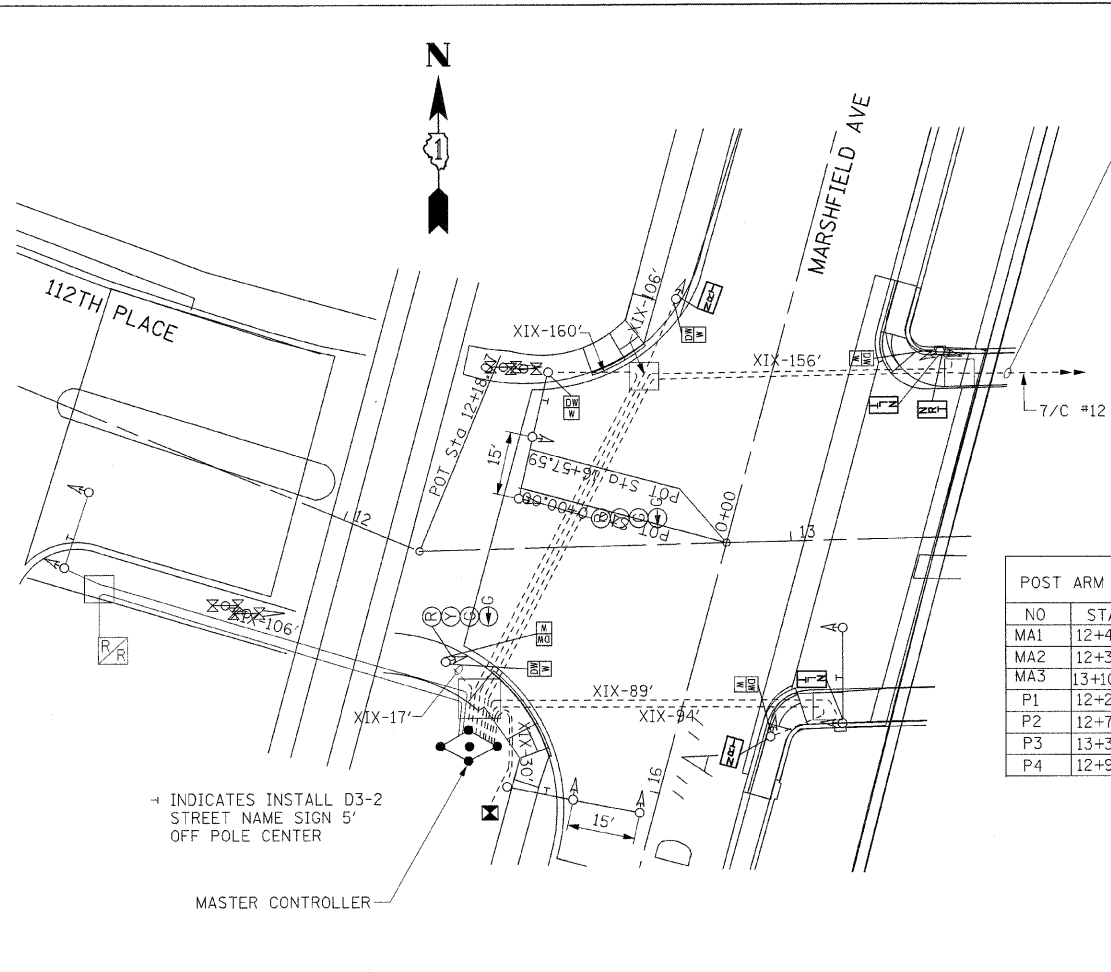
SCALE: NONE
 DATE: 01/16/09
 DRAWN BY: ACL
 CHECKED BY: RMW

TENG
TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312/616-0900

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	036
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62533				



FOUNDATION AND CONDUIT PLAN
1" = 20'



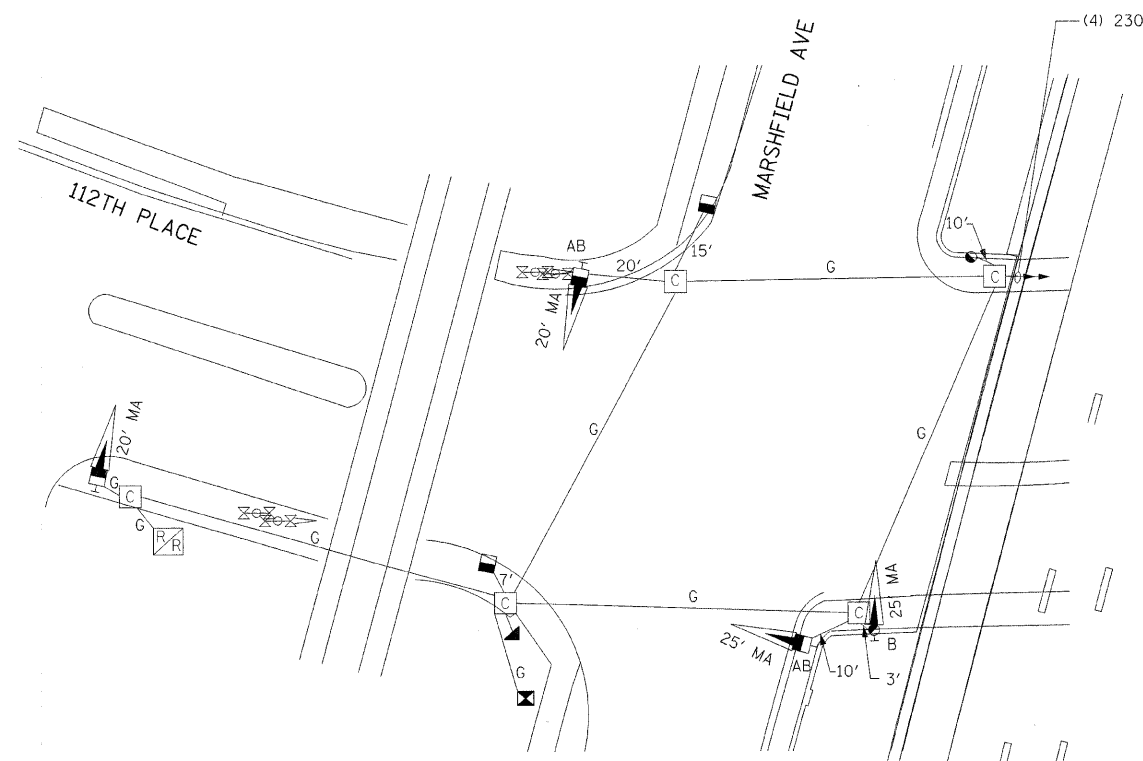
INDICATES INSTALL D3-2 STREET NAME SIGN 5' OFF POLE CENTER

SIGNAL AND CABLE PLAN
SCALE : NONE

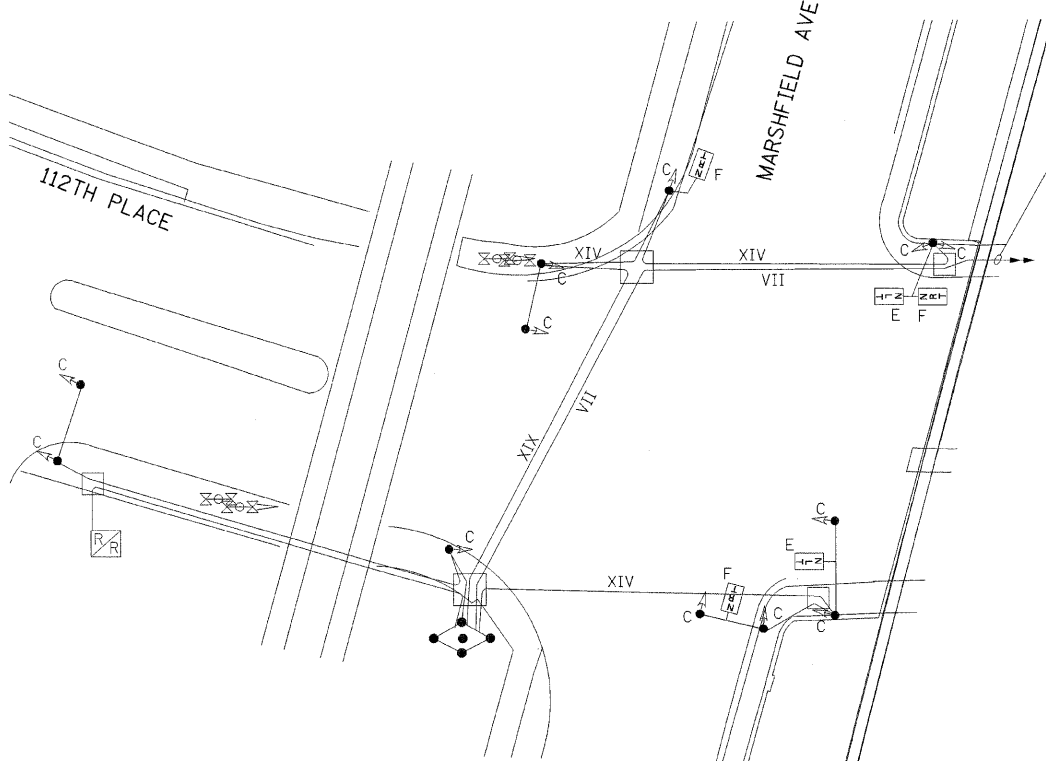
- NOTES:
- CONTRACTOR TO CONTACT ALL UTILITIES PRIOR TO EXCAVATION FOR EXACT LOCATION OF THEIR UNDERGROUND FACILITIES.
 - MINIMUM DISTANCE OF 3 FEET IS DESIRABLE FROM CURB FACE TO POLE/PEDESTAL CENTERLINE.
 - ALL FOUNDATIONS IN SIDEWALK TO BE INSTALLED PER DRAWING #828, #837 AND 854.
 - MOUNT ALL PEDESTRIAN SIGNALS BELOW TRAFFIC SIGNALS WHERE APPLICABLE.
 - ALL TRAFFIC SIGNALS TO BE 12" LED.
 - FOR CODE SEE DRAWING # 826.

NO	STATION OFFSET	DESCRIPTION
MA1	12+47.7, 39.7' LT	POLE, CITY STEEL, ANCHOR BASE, 34'-6", 26' M.A.
MA2	12+36.2, 51.7' RT	POLE, CITY STEEL, ANCHOR BASE, 34'-6", 35' M.A.
MA3	13+10.53, 41.66' LT	POLE, CITY STEEL, ANCHOR BASE, 34'-6", 20' M.A.
P1	12+25.0, 25.1' RT	POLE, CITY STEEL, ANCHOR BASE 34"-6"
P2	12+72.9, 54.8' LT	POLE, CITY STEEL, ANCHOR BASE 34"-6"
P3	13+33.9, 40.8' LT	POLE, CITY STEEL, ANCHOR BASE 32"-6"
P4	12+94.4, 43.3' RT	POLE, CITY STEEL, ANCHOR BASE 34"-6"

- LEGEND:
- A - BREAK DOWN FOUNDATION (2)
 - B - REMOVE M.A. ASSEMBLY (3)
 - C - REMOVE TRAFFIC SIGNALS (12)
 - D - REMOVE PEDESTRIAN "WALK/DON'T WALK" SIGNAL (8)
 - E - REMOVE ILLUMINATED - NO LEFT TURN SIGN (2)
 - F - REMOVE ILLUMINATED - NO RIGHT TURN SIGN (3)
 - G - EXISTING CONDUIT TO REMAIN
- REMOVE CABLES AS SHOWN: VII - 385', XIV-140, XIX-95



FOUNDATION REMOVAL PLAN
SCALE : NONE



SIGNAL AND CABLE REMOVAL PLAN
SCALE : NONE

REVISIONS	NAME	DATE

DATE	REVISION
WORK ORDER NO.	DATE
COST ALLOCATION ACCOUNT	MATERIAL
APPROPRIATION ACCOUNT	LABOR
F.A.I. ROUTE 57 (INTERSTATE 57) 112th PL. CONTRACT	
TRAFFIC SIGNAL PLAN 112th PL. AND MARSHFIELD AVE.	
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICAL ENGINEERING	
DRAFTSMAN:	CHIEF DRAFTSMAN: ENGINEER:
SUPERVISING ENGINEER	DRAWING NO.
ENGINEER OF ELECTRICITY	
SUPT. OF CONSTRUCTION	
DEPUTY COMMISSIONER	
SIZE:	SCALE: 1" = 20' DATE: MAY 2003

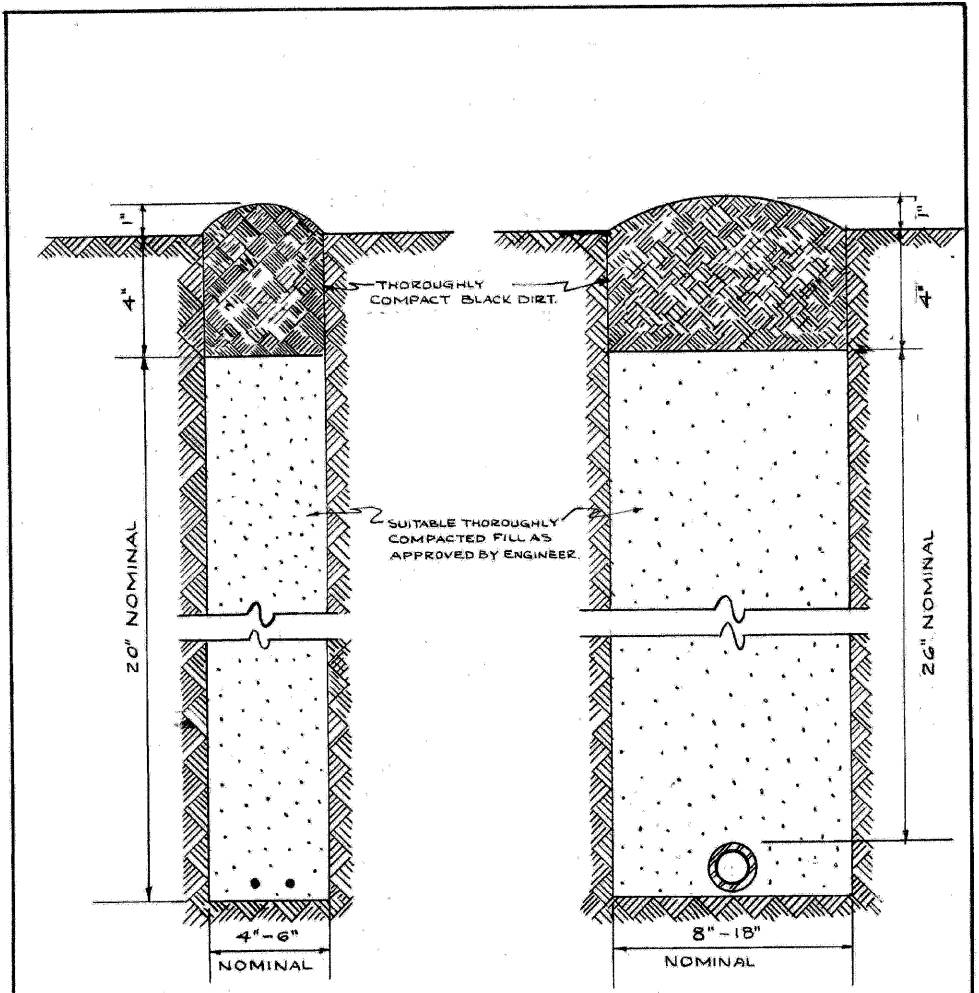
TS-2
ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)
TRAFFIC SIGNAL PLAN
112TH PL. AND MARSHFIELD AVE.

SCALE: 1" = 20'
DATE: 01/16/09

DRAWN BY: ACL
CHECKED BY: RMW

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-0000

...VAL TOON88.DGN, ...RDW00003.DGN, ...APM00005.DGN, ...N12TH ST BORDER.DGN, ...VAL TOON88.DGN, ...VAL TOON88.DGN, ...VAL TOON88.DGN, ...HECHTBR
 H-2009, 131149
 T:\DOCUMENTS\1930860\CIVIL\DWG\EXH004P.DGN
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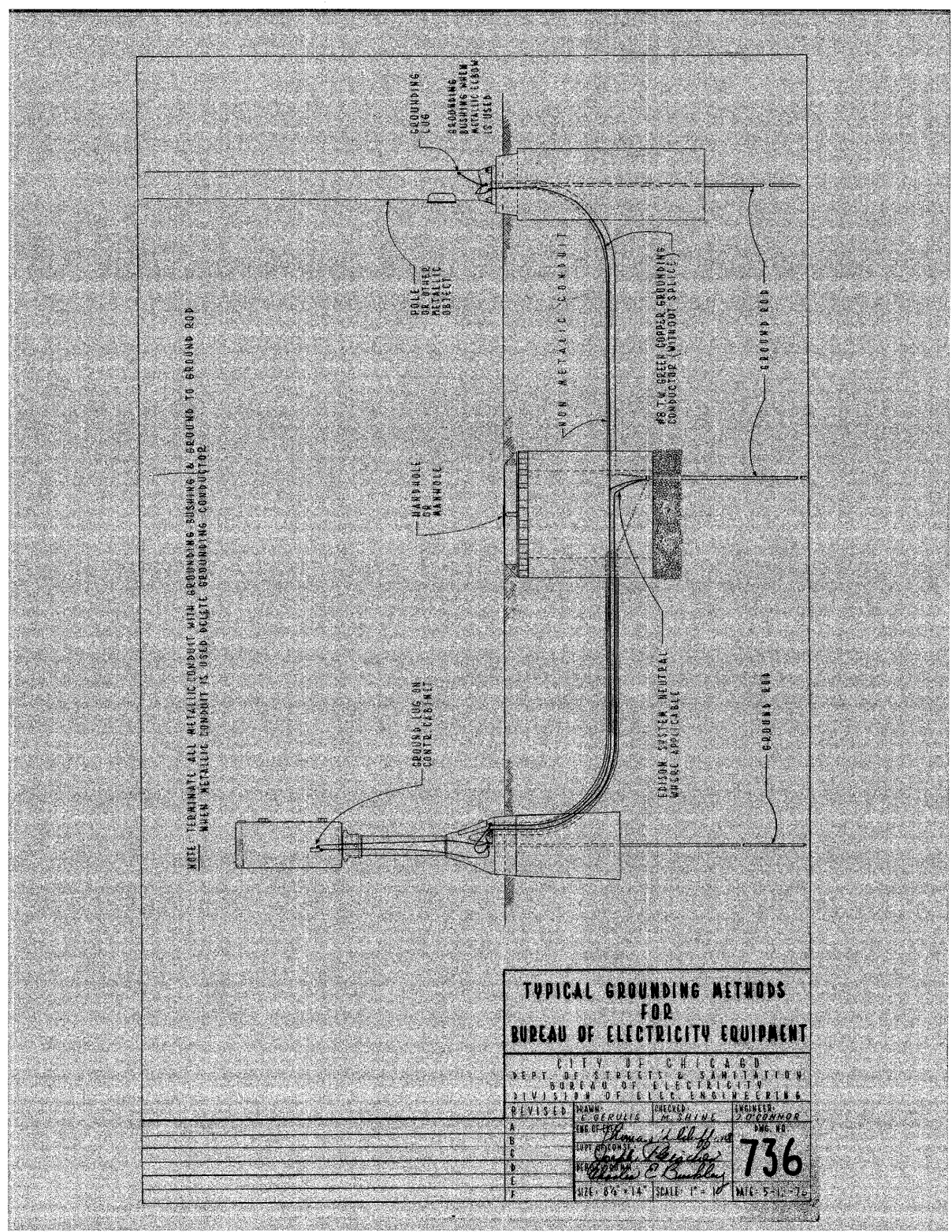


CABLE TRENCH

CONDUIT TRENCH

NOTE
 EXCESS SOIL FROM TRENCH TO BE COMPLETELY REMOVED FROM SITE AS SOON AS PRACTICABLE.
 BLACK DIRT TO BE TAMPED & THOROUGHLY COMPACTED AS SHOWN.

STANDARD METHOD FOR BACKFILLING CABLE & CONDUIT TRENCHES IN SODDED PARKWAY & LAWNS			
CITY OF CHICAGO			
DEPT. OF STREETS & SANITATION			
DIVISION OF ELECTRICAL ENGINEERING			
REVISION	DRAWN	CHECKED	ENGINEER
A	W. E. HARR	J. J. CONNOR	J. J. CONNOR
B			
C	ENGINEER IN CHARGE		DRG. NO.
D			
E	SUPT. OF CONET.		579
F	DR. JOHN COMM.		
G			
H	SIZE 8 1/2 X 14	SCALE: N	DATE: 7-14-01



TYPICAL GROUNDING METHODS FOR BUREAU OF ELECTRICITY EQUIPMENT

CITY OF CHICAGO			
DEPT. OF STREETS & SANITATION			
BUREAU OF ELECTRICITY			
DIVISION OF ELEC. ENGINEERING			
REVISED	DRAWN	CHECKED	ENGINEER
A	W. E. HARR	J. J. CONNOR	J. J. CONNOR
B			
C			
D			
E			
F			
SIZE: 8 1/2 X 14		SCALE: 1" = 10'	DATE: 5-1-76

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I. ROUTE 57 (INTERSTATE 57)

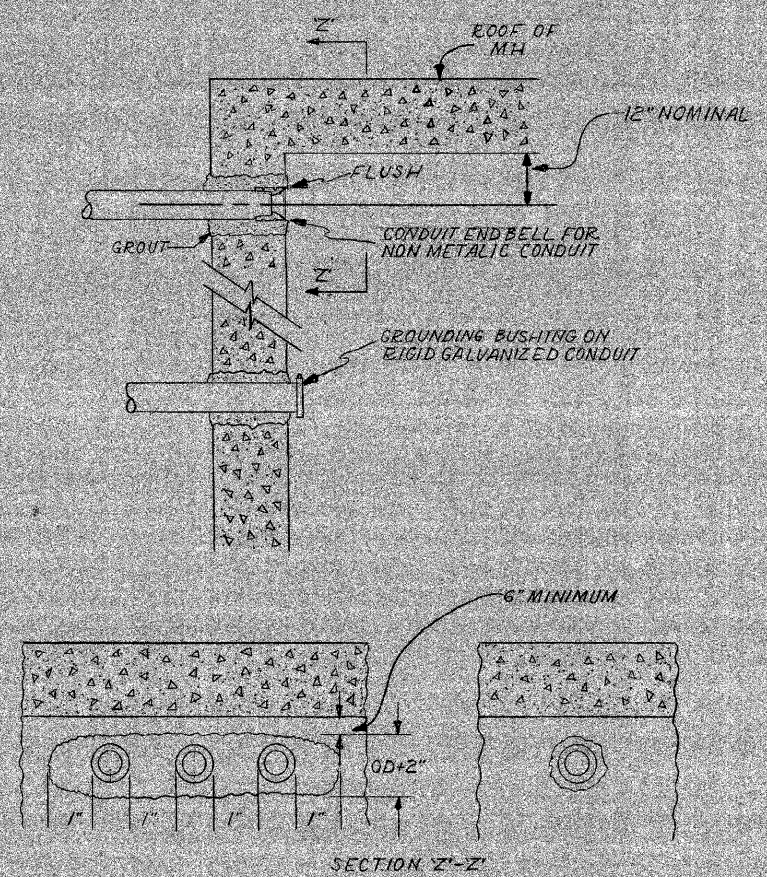
ELECTRICAL STANDARD DETAILS

SCALE: NONE
 DATE: 01/16/09
 DRAWN BY: N/A
 CHECKED BY: N/A

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS, ARCHITECTS, PLANNERS
 225 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.7616-0000

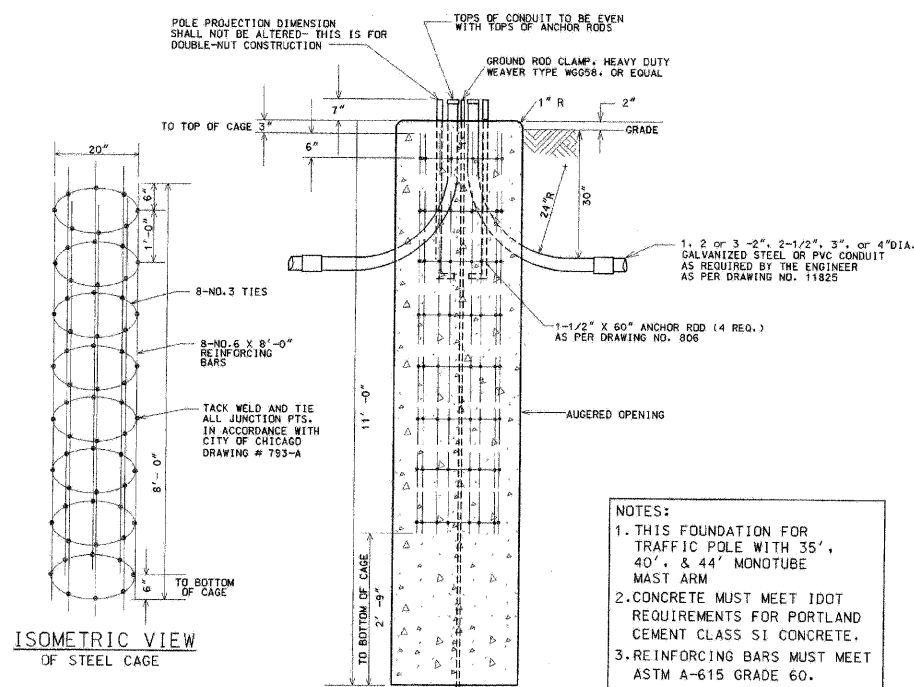
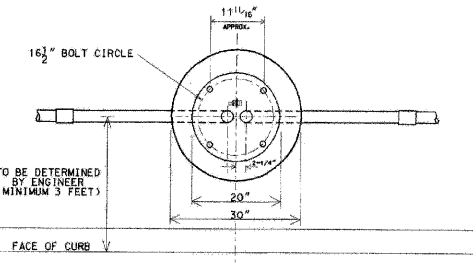
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	043
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				

CONDUIT INSTALLATION THROUGH EXISTING MANHOLE OR HANDHOLE WALL



OPENING THROUGH WALL TO BE KEPT TO MINIMUM SIZE TO ADMIT CONDUIT AND SUFFICIENT GROUT TO ASSURE SEALING WALL.

INSTALLATION METHOD OF INSTALLING CONDUIT THRU MANHOLE WALL		
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING		
DRAWN A.M. JOHNSON	CHECKED R. SICKOWSKI	ENGINEER R.L. MARTIN
 CHARLES E. BUCKLEY ENGINEER OF ELECTRICITY REG. NO. 814		REG. NO. 814 EXP. DATE N.P. SCALE N.P. DATE 7-18-02



ISOMETRIC VIEW OF STEEL CAGE

ELEVATION

NOTE: HOLE FOR FOUNDATION MUST BE AUGERED IN UNDISTURBED SOIL

CODE	COMMODITY	SIZE	QUANTITY
05-3267-2940	REDI-MIX CONCRETE	CU. YD.	2.20
09-4001-	ELBOW, LARGE RADIUS	2", 2-1/2", 3", 4"	AS REQUIRED
37-8180-0200	ANCHOR ROD	1-1/2" X 60"	4
05-5054-6910	RE-BAR CAGE	20" R. X 8'-0"	1
09-7796	GROUND ROD	3/4" X 14'-0"	1
09-2636-	GROUND ROD CLAMP	3/4"	1
09-2092-	GROUND BUSHING	2" x 2-1/2", 3", 4"	AS REQUIRED

- NOTES:
- THIS FOUNDATION FOR TRAFFIC POLE WITH 35', 40', & 44' MONOTUBE MAST ARM
 - CONCRETE MUST MEET IDOT REQUIREMENTS FOR PORTLAND CEMENT CLASS S1 CONCRETE.
 - REINFORCING BARS MUST MEET ASTM A-615 GRADE 60.

8/21/02	SUPERCEDES DWG 4817 DRAWN 4/20/81
DATE REVISION	
FOUNDATION FOR 34'-6" TRAFFIC SIGNAL POLE WITH 16 1/2" BOLT CIRCLE	
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING	
DRAFTSMAN S. GARNSEY	DATE 8/21/02
ELECTRICAL ENGINEER R. CARTER	ENGINEER S. GARNSEY
REG. NO. 817	REG. NO. 817
SIZE: 11" X 17"	SCALE: NONE

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I. ROUTE 57 (INTERSTATE 57)
ELECTRICAL STANDARD DETAILS

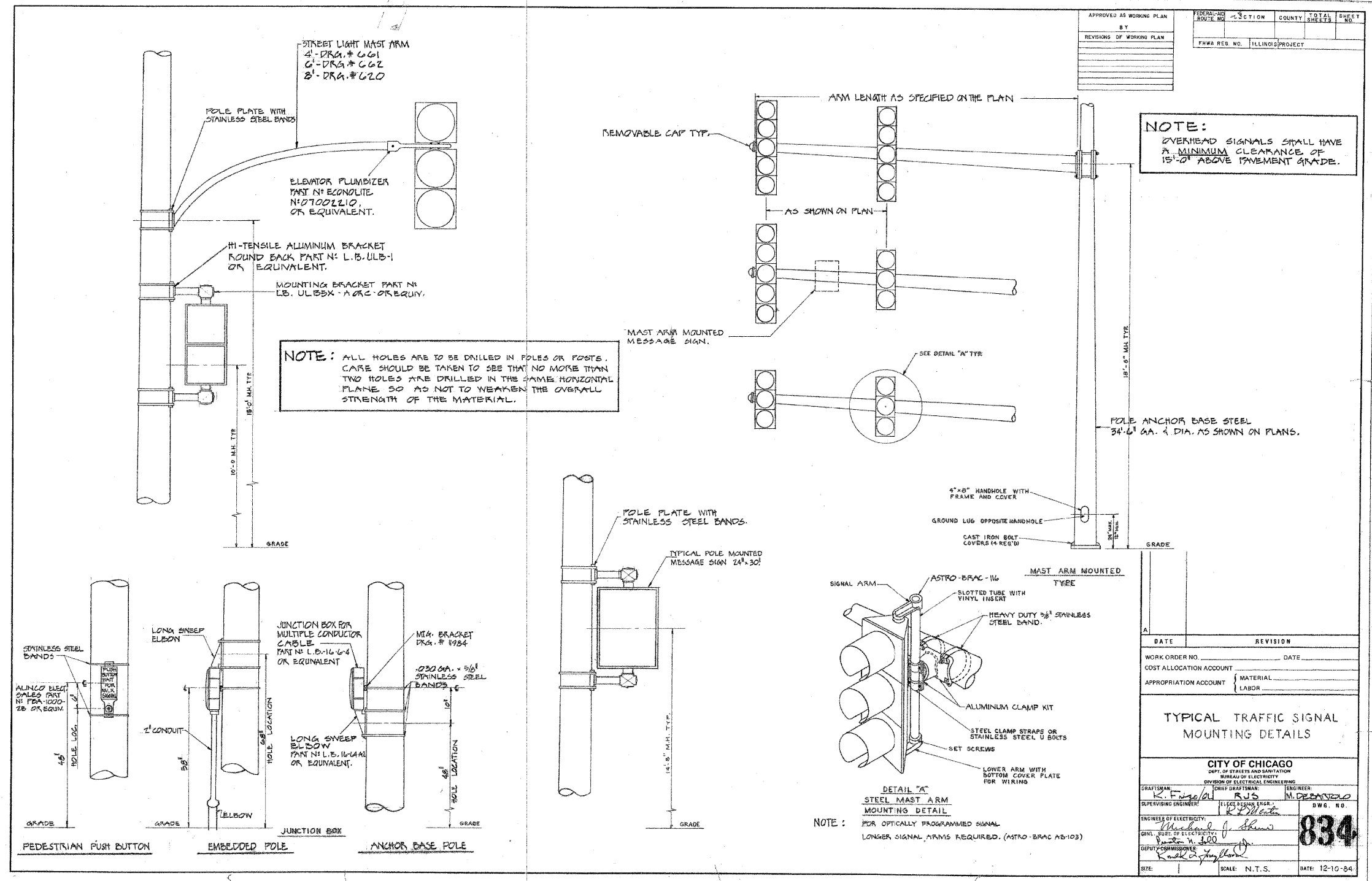
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TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS-PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-0000

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 HECHTER

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	046
STA.	TO STA.			
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				

APPROVED AS WORKING PLAN	FEDERAL AID SECTION COUNTY TOTAL SHEETS
BY	NO. NO. NO. NO.
REVISIONS OF WORKING PLAN	FHWA REG. NO. ILLINOIS PROJECT



NOTE: ALL HOLES ARE TO BE DRILLED IN POLES OR POSTS. CARE SHOULD BE TAKEN TO SEE THAT NO MORE THAN TWO HOLES ARE DRILLED IN THE SAME HORIZONTAL PLANE, SO AS NOT TO WEAKEN THE OVERALL STRENGTH OF THE MATERIAL.

NOTE: OVERHEAD SIGNALS SHALL HAVE A MINIMUM CLEARANCE OF 15'-0" ABOVE PAVEMENT GRADE.

NOTE: FOR OPTICALLY PROGRAMMED SIGNAL LONGER SIGNAL ARMS REQUIRED. (ASTRO-BRAC AS103)

DATE	REVISION

WORK ORDER NO. _____ DATE _____
 COST ALLOCATION ACCOUNT _____
 APPROPRIATION ACCOUNT { MATERIAL _____
 LABOR _____

TYPICAL TRAFFIC SIGNAL MOUNTING DETAILS

CITY OF CHICAGO
 DEPT. OF STREETS AND SANITATION
 BUREAU OF ELECTRICITY
 DIVISION OF ELECTRICAL ENGINEERING

CHIEF DRAFTSMAN: *M. DeBenedetto* ENGINEER: _____
 SUPERVISING ENGINEER: *R. F. ...* ELECTRICAL ENGR.: *R. ...* DWG. NO. _____
 ENGINEER OF ELECTRICITY: *Michael J. ...*
 GENL. SGT. OF ELECTRICITY: *...*
 DEPUTY COMMISSIONER: *...*

SCALE: N.T.S. DATE: 12-10-84

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I ROUTE 57 (INTERSTATE 57)

ELECTRICAL STANDARD DETAILS

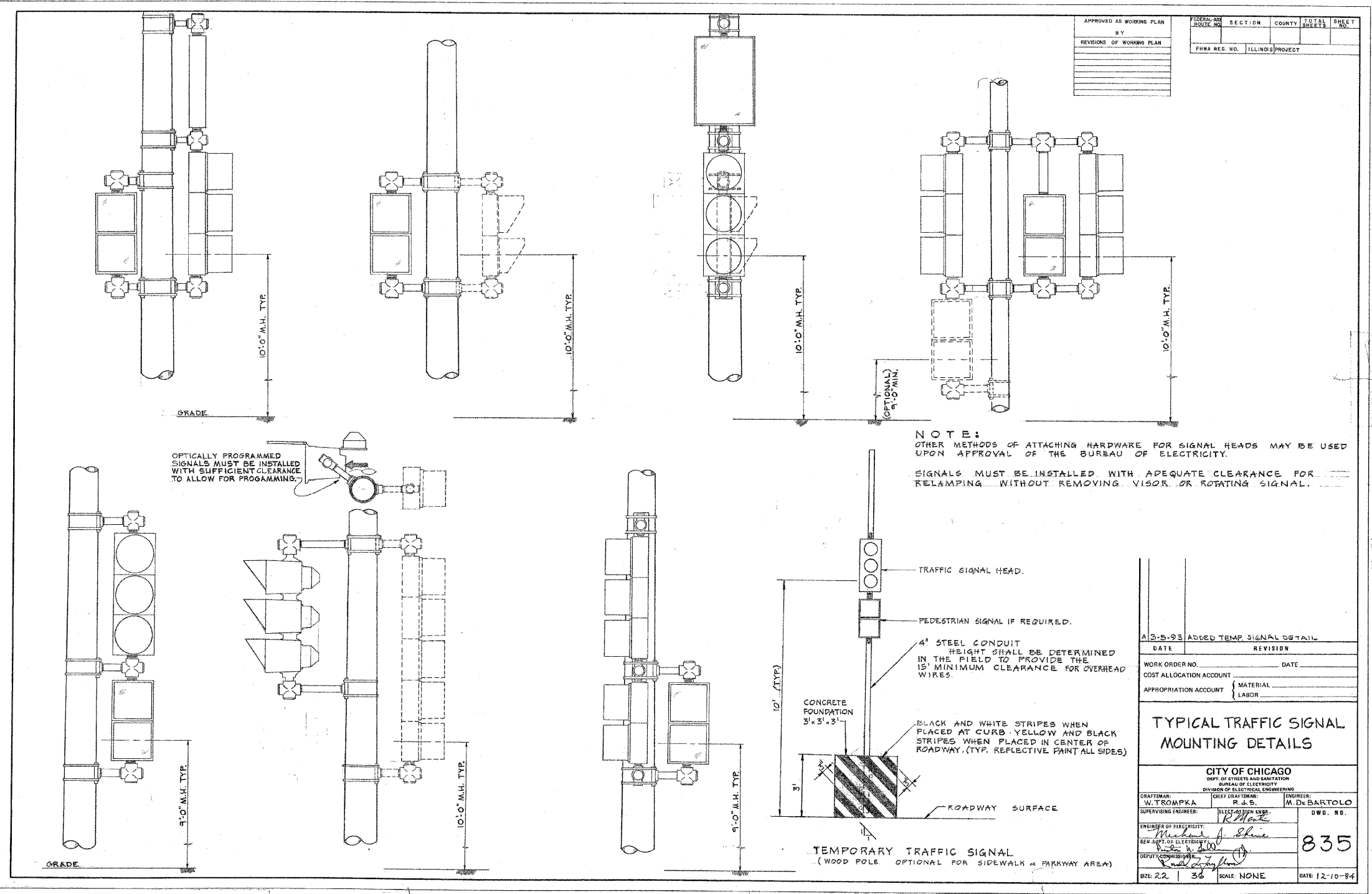
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 DATE: 01/16/09 CHECKED BY: N/A

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 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE. CHICAGO, IL 60601
 TELEPHONE: 312/616-0000

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ES-08

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	047
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62533				



NOTE:
 OTHER METHODS OF ATTACHING HARDWARE FOR SIGNAL HEADS MAY BE USED UPON APPROVAL OF THE BUREAU OF ELECTRICITY.
 SIGNALS MUST BE INSTALLED WITH ADEQUATE CLEARANCE FOR RELAMPING WITHOUT REMOVING VISIBLE OR ROTATING SIGNAL.

APPROVED AS WORKING PLAN	FEDERAL AID ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
BY					
REVISIONS OF WORKING PLAN	FHWA REG. NO.	ILLINOIS PROJECT			

A 2-5-93 ADDED TEMP. SIGNAL DETAIL	
DATE	REVISION
WORK ORDER NO.	DATE
COST ALLOCATION ACCOUNT	
APPROPRIATION ACCOUNT	MATERIAL
	LABOR
TYPICAL TRAFFIC SIGNAL MOUNTING DETAILS	
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING	
ENGINEER: M. De BARTOLO	DWG. NO. 835
DATE: 12-10-84	

NORTH ST. BORDER: JCV
 FILE: 2008_182023
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REVISIONS	
NAME	DATE

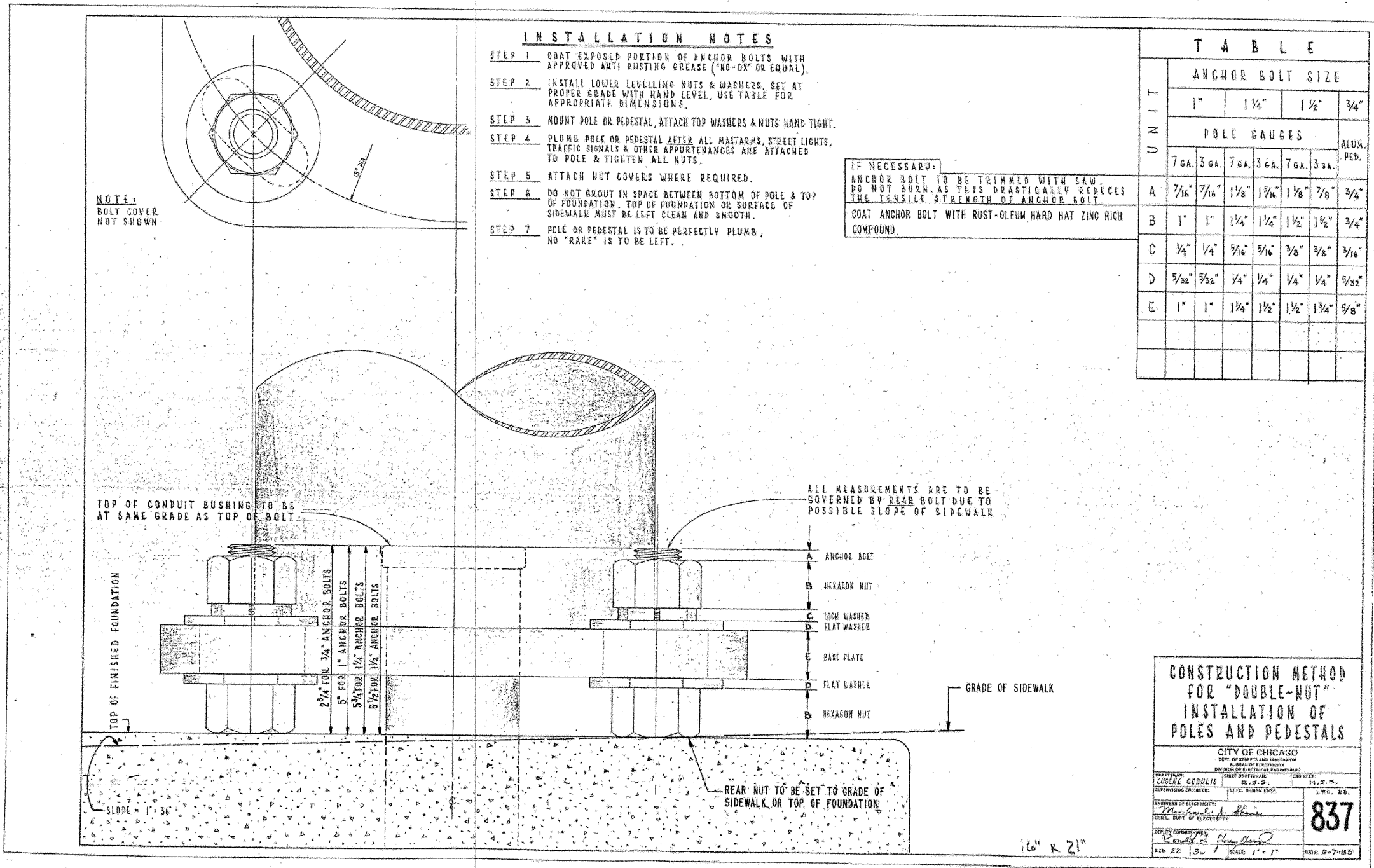
ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I. ROUTE 57 (INTERSTATE 57)
 ELECTRICAL STANDARD DETAILS

SCALE: NONE
 DATE: 01/16/09
 DRAWN BY: N/A
 CHECKED BY: N/A

TENG

TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312/616-0000

ES-09



CONSTRUCTION METHOD FOR "DOUBLE-NUT" INSTALLATION OF POLES AND PEDESTALS

CITY OF CHICAGO
DEPT. OF STREETS AND SANITATION
BUREAU OF ELECTRICITY
DIVISION OF ELECTRICAL ENGINEERING

DESIGNED BY: EUGENE GEBULIS
SUPERVISING ENGINEER: ELEC. DESIGN ENGR. P.E. S. S.

ENGINEER: P.E. S. S.

SCALE: 1" = 1"

DATE: 6-7-85

837

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

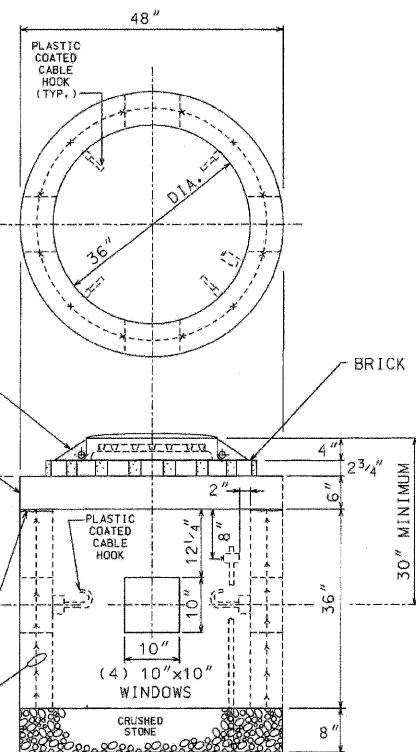
ELECTRICAL STANDARD DETAILS

SCALE: NONE
DATE: 01/16/09

DRAWN BY: N/A
CHECKED BY: N/A

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-0000

TOP VIEW
FRAME, COVER & TOP
OF HANDHOLE REMOVED



FRAME & COVER
DRG. #872

SEE DETAIL
ROOF OF
HANDHOLE

BRICK

INSTALL BED OF MORTAR
BETWEEN ROOF & HANDHOLE

REINF. WIRE
MESH-6"X6"

SIDE
ELEVATION

COMPLETE COMMODITY CODE NO. 05-6610-5310M

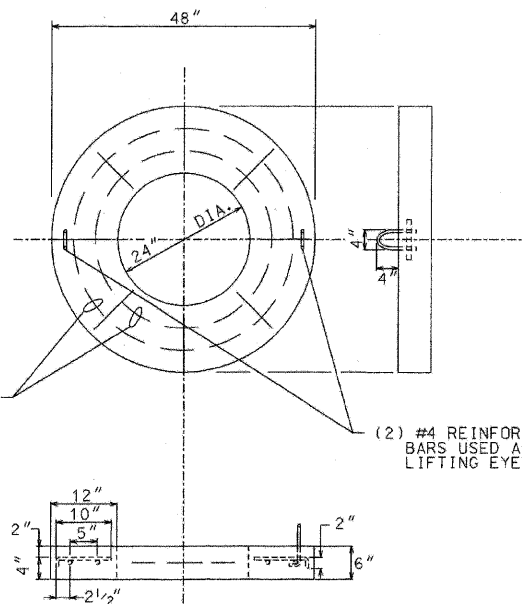
CODE NO.	MATERIALS	SIZE	QUAN.
(1) 05-6610-5312	PRE-CAST HANDHOLE	36" X 36"	
(2) 05-6610-5312	PRE-CAST ROOF	SEE DETAIL	1
05-9075-5470	STONE 3/4" CRUSHED SCREENINGS	BAG	5
05-1452-9720	BRICK		24
02-4299-5524	FRAME, MANHOLE	24"	1
02-4574-5624	COVER, MANHOLE	24"	1
09-7796-9312	GROUND ROD	3/4" X 12'	1
09-2636-3240	GROUND CLAMP		1
(3) 05-5082-5330	SONO TUBE	36"	1
(3) 05-5082-5342	SONO TUBE	48"	1
(3) 05-3267-2940	CONC. REDI-MIX	CU. YD.	3/4
(3) 20-5472-9630	REINFORCING BAR	#3 (3/8")	20'
(3) 20-5472-9630	REINFORCING BAR	#3 (3/8")	8'
(3) 20-5472-9640	REINFORCING BAR	#4 (1/2")	4'
(3) 57-0770-0000	(MESH (6" X 6"))	36" X 11'	1

- (1) PRE-CAST HANDHOLE SHALL INCLUDE CABLE HOOKS AND CONDUIT KNOCK-OUTS.
 (2) PRE-CAST ROOF SHALL INCLUDE LIFTING EYES.
 (3) THESE ITEMS ARE FOR POURED-IN-PLACE HANDHOLES ONLY.

CONSTRUCTION NOTES:

- 1 - 8" BED OF STONE FOR DRAINAGE.
 2 - ALL METALLIC CONDUIT(S) ENTERING HANDHOLE SHALL EXTEND MIN. 1" & MAX. OF 3" INSIDE INNER WALL & BE EQUIPPED WITH AN APPROVED TYPE THREADED GROUNDING BUSHING.

(2) #3 REINFORCING
BARS WITH (8) #3
CROSS TIES



(2) #4 REINFORCING
BARS USED AS
LIFTING EYES

DETAIL-ROOF OF HANDHOLE

DATE	REVISION
B 01-23-00	ADDED CABLE HOOKS PER COMMISSIONER MURPHY (RC/RE)
A	REDRAW (CAD.) (MP)

36" DIA. HEAVY DUTY
CONCRETE HANDHOLE
WITH 24" FRAME &
COVER

CITY OF CHICAGO
DEPT. OF STREETS AND SANITATION
BUREAU OF ELECTRICAL ENGINEERING

DRAFTSMAN M. PATTON	CHIEF DRAFTSMAN B. CARTER	ENGINEER W. THOMPKA
SUPERVISING ENGINEER N. DEBARTOLO	ELECT. DESIGN ENGINEER	
ENGINEER OF ELECTRICALITY		
DEPT. CHIEF OF ELECTRICALITY		
DEPUTY COMMISSIONER		
SIZE: 17" x 22"	SCALE: NONE	DATE: 1-12-96

DWG. NO. 866

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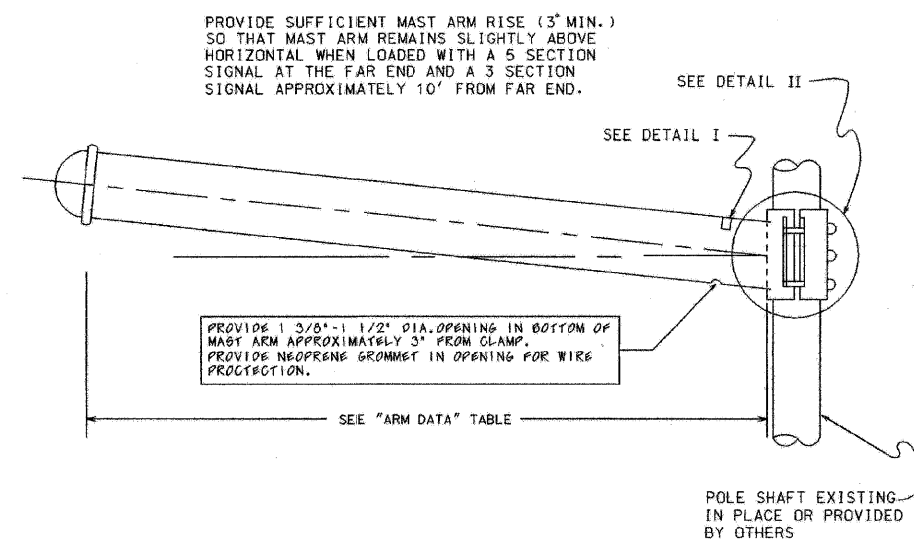
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I ROUTE 57 (INTERSTATE 57)

ELECTRICAL STANDARD DETAILS

SCALE: NONE DRAWN BY: N/A
 DATE: 01/16/09 CHECKED BY: N/A

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312/678-0000



PROVIDE 1 3/8" - 1 1/2" DIA. OPENING IN BOTTOM OF MAST ARM APPROXIMATELY 3" FROM CLAMP. PROVIDE NEOPRENE GROMMET IN OPENING FOR WIRE PROTECTION.

SEE "ARM DATA" TABLE

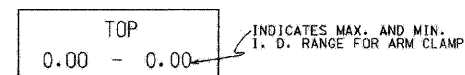
POLE SHAFT EXISTING IN PLACE OR PROVIDED BY OTHERS

ARM DATA

CLAMP RANGE (INCHES)	POLE END (INCHES)	SIGNAL END (INCHES)	LENGTH (FEET)	GAUGE	WEIGHT (POUNDS)	TEST LOAD (POUNDS)	MAX. DEFLECTION (INCHES)
7.45-7.95	7.0	4.76	16	7	241	1700	6.5
7.45-7.95	7.0	4.20	20	7	274	1300	12.0
7.45-7.95	7.0	3.36	26	7	315	1000	24.0
8.45-8.95	8.0	3.80	30	7	409	1100	29.0
9.95-10.45	9.0	4.10	35	7	529	1200	36.0
9.95-10.45	9.0	3.40	40	7	559	1000	52.0
9.95-10.45	10.0	3.84	44	7		1200	57.0

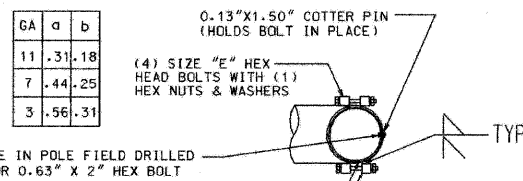
NOTES:

- TRAFFIC SIGNAL ARM SHAFT ASTM DESIGNATION: A595 GRADE C, 60,000 PSI MINIMUM YIELD STRENGTH WITH A LINEAR TAPER -0.14"/FT.
- TRAFFIC SIGNAL ARM END CAP SECURED IN PLACE WITH 3 SET SCREWS AND 1 THRU ARM END BOLT. (PLATED HARDWARE)
- ALL THREADED FASTENERS TO BE GALVANIZED TO ASTM DESIGNATION: A153
- ALL VEHICULAR AND/OR PEDESTRIAN SIGNAL LIGHTS AND NECESSARY HARDWARE FOR ATTACHMENT TO BE FIELD LOCATED AND FURNISHED BY OTHERS.
- ALL ARM END CAPS AND ARM CLAMPS TO BE FULLY ASSEMBLED AND ATTACHED TO THE ARM PRIOR TO SHIPPING.
- ARM ASSEMBLY TO BE DEGREASED; CLEANED; CHEMICALLY PRETREATED; GIVEN AN EXTERIOR THERMOSETTING POLYESTER POWDER COAT; AND AN INTERIOR THERMOPLASTIC HYDROCARBON RESIN POWDER COAT. ALL PAINTING TO BE IN ACCORDANCE WITH SPECIFICATION 1454, SECTION 8, "PAINTING".
- MAST ARM SHALL BE TESTED IN ACCORDANCE WITH SPECIFICATION 1454, SECTION 9 WITH TEST VALUES AS SHOWN ON THE "ARM DATA" TABLE.
- HOLE IN CLAMP TO BE 4.0" (MIN.). HOLE TO BE GROUND SMOOTH AND DEBURRED TO PROVIDE A SMOOTH WIRE ENTRY FROM POLE TO MAST ARM.



I. D. TAG TO BE MOUNTED ON THE TOP OF THE POLE AS INDICATED, USING 2 "GRIP-NAIL" FASTENERS.

DETAIL I
I. D. TAG



HOLE IN POLE FIELD DRILLED FOR 0.63" X 2" HEX BOLT

SEE NOTE 8

DETAIL II
ARM ATTACHMENT

SIGNAL ARM ATTACHMENT DATA					
CLAMP RANGE	"A"	"B"	"C"	"D"	"E"
7.45" - 7.95"	7.25"	9.92"	10.80"	10.32"	1.00" X 7.50"
8.45" - 8.95"	9.00"	10.66"	13.06"	12.50"	1.00" X 8.00"
9.95" - 10.45"	10.25"	12.66"	15.30"	13.50"	1.25" X 8.50"

B	10-29-02	REFERENCES SPEC. 1454
A	8-6-93	WIRE OPENING ON MAST ARM
DATE		REVISION
SUPERSEDES DWG. DATED DEC. 1991		
STEEL TRAFFIC SIGNAL MAST ARM-MONOTUBE		
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING		
DRAFTSMAN: R. IVY	CHIEF DRAFTSMAN: ROBERT CARTER	ENGINEER: RON POOL
SUPERVISING ENGINEER	ELECTRICAL ENGINEER	
ENGINEER IN CHARGE <i>[Signature]</i>		870
DEPUTY COMM. ENGINEER <i>[Signature]</i>		
SCALE: 1/8"	SCALE: 1/8"	DATE: DEC. 1991

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

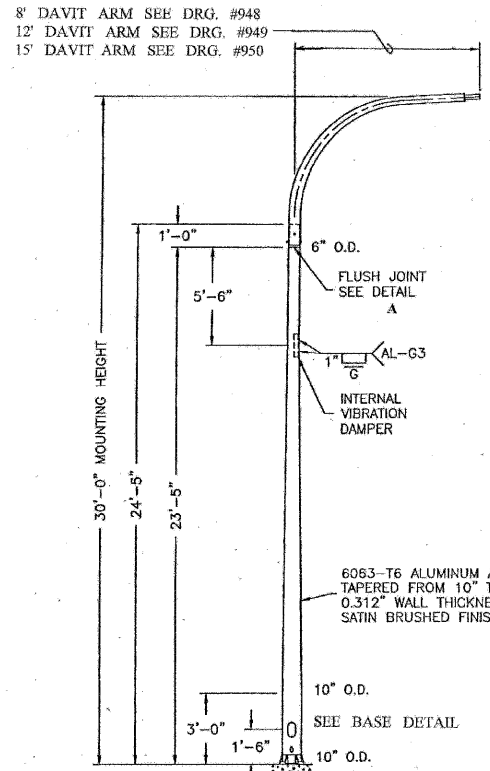
ELECTRICAL STANDARD DETAILS

SCALE: DATE: 10/16/09

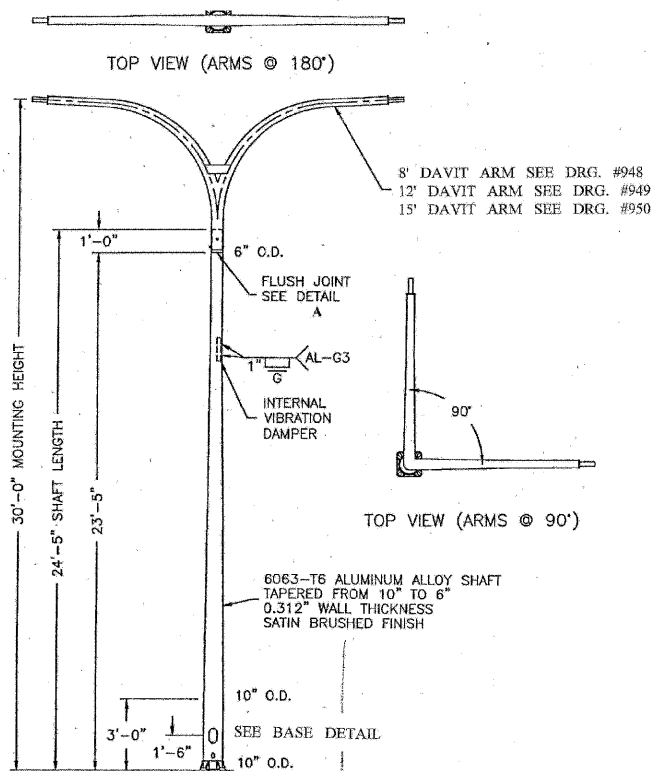
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CHECKED BY: N/A

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TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
225 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/616-0000

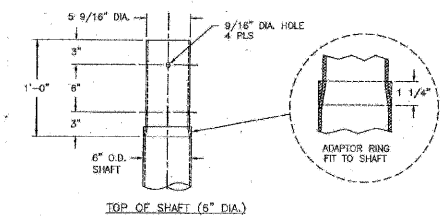
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.IB-R	COOK	101	055
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				



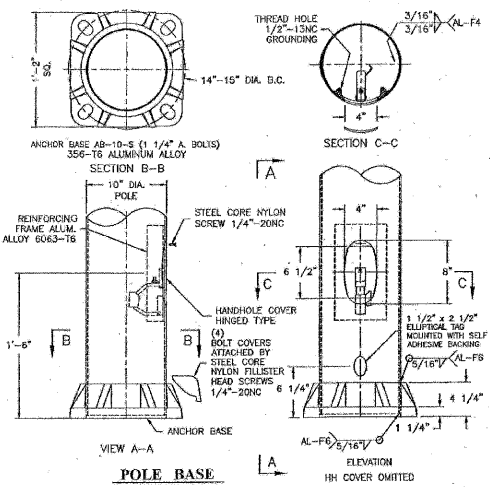
POLE WITH SINGLE DAVIT ARM



POLE WITH TWIN DAVIT ARMS

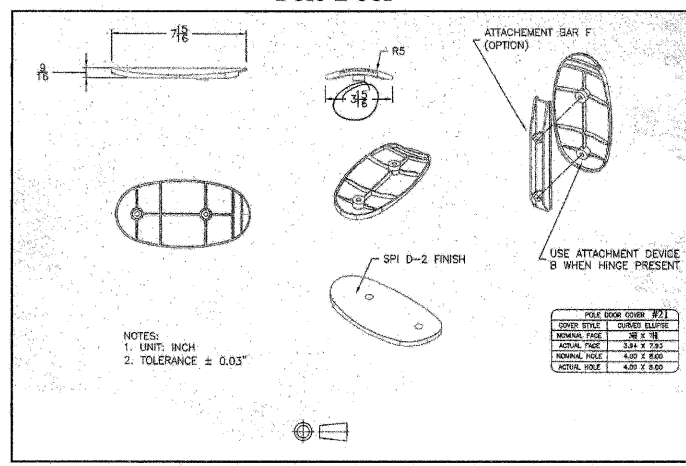


DETAIL A



POLE BASE

Pole Door



ALUMINUM DAVIT POLE
10" x 6" x 24'-5"
FOR ARTERIAL STREETS

CITY OF CHICAGO
DEPT. OF STREETS AND SANITATION
BUREAU OF ELECTRICITY
DIVISION OF ELECTRICAL ENGINEERING

DRAFTSMAN:	CHIEF DRAFTSMAN:	ENGINEER:
SUPERVISING ENGINEER:	ELTC. DESIGN ENGINEER:	DWG. NO.:
ENGINEER OF ELECTRICITY:		941
DEPUTY PROFESSIONAL ENGINEER:		DATE: 1-29-02

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REVISIONS	
NAME	DATE

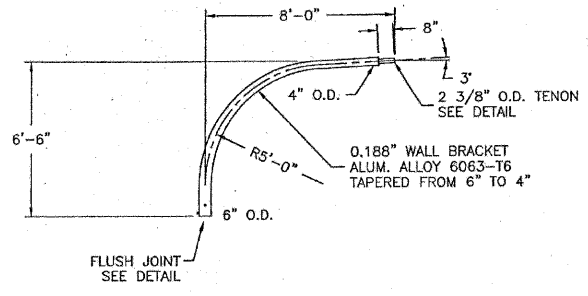
ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

ELECTRICAL STANDARD DETAILS

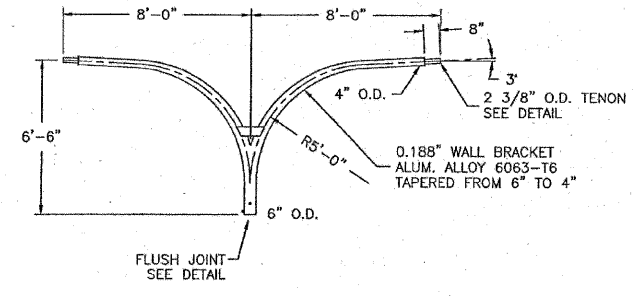
SCALE: DRAWN BY: N/A
DATE: 01/16/09 CHECKED BY: N/A

TENG
TENG & ASSOCIATES, INC.
ENGINEERS-ARCHITECTS-PLANNERS
295 N. MICHIGAN AVE. CHICAGO, IL 60601
TELEPHONE: 312/615-0000

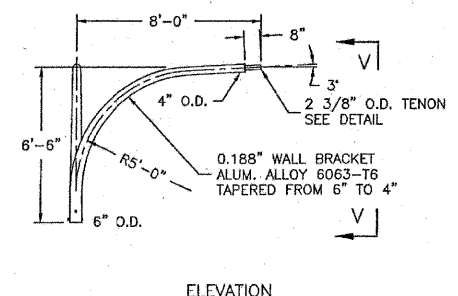
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	056
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				



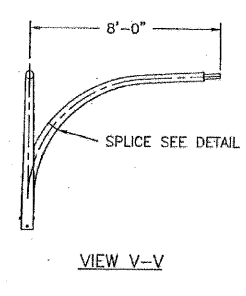
SINGLE DAVIT ARM



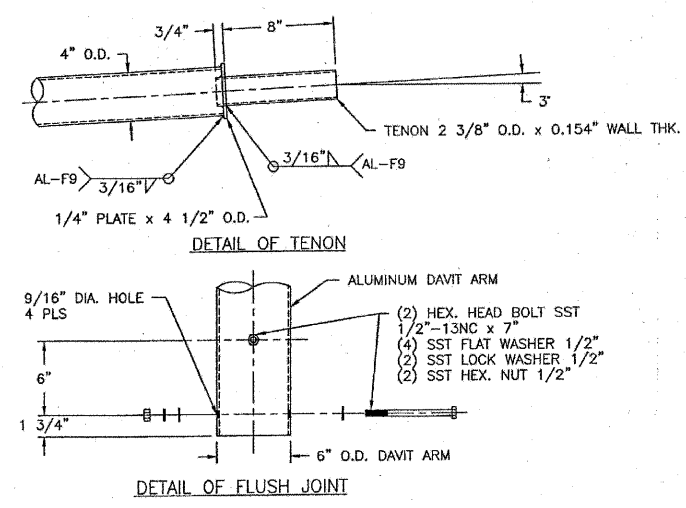
TWIN DAVIT ARM AT 180°



TWIN DAVIT ARM AT 90°

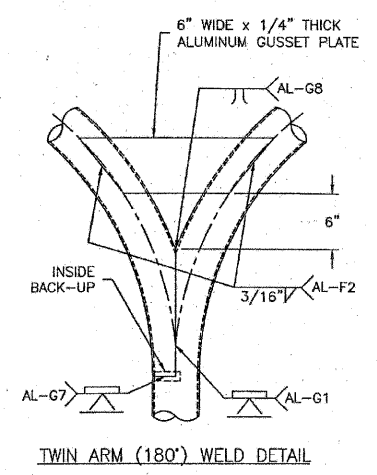


VIEW V-V

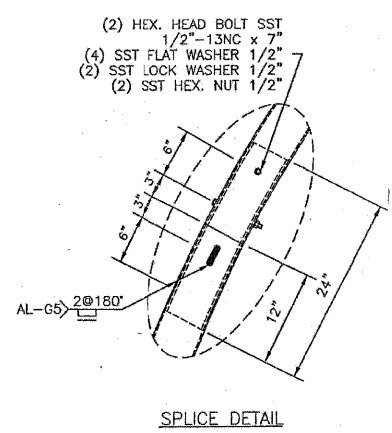


DETAIL OF TENON

DETAIL OF FLUSH JOINT



TWIN ARM (180°) WELD DETAIL



SPLICE DETAIL

ALUMINUM DAVIT ARM 6" x 8'-0"		
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION DIVISION OF ELECTRICITY BUREAU OF ELECTRICAL ENGINEERING		
DRAFTSMAN	CHIEF DRAFTSMAN	ENGINEER
SUPERVISING ENGINEER	ELEC. DESIGN ENGR.	DWG. NO.
ENGINEER OF ELECTRICITY		948
DEPT. IN CHARGE	CONSTRUCTION	
DEPUTY COMMISSIONER		
DATE	SCALE	DATE
1-23-02		

NORTH ST. BORDER ROOM
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 T:\DOCUMENTS\950860\CIVIL\DWG\4-CORR\HW.DWG
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ES-18

REVISIONS	
NAME	DATE

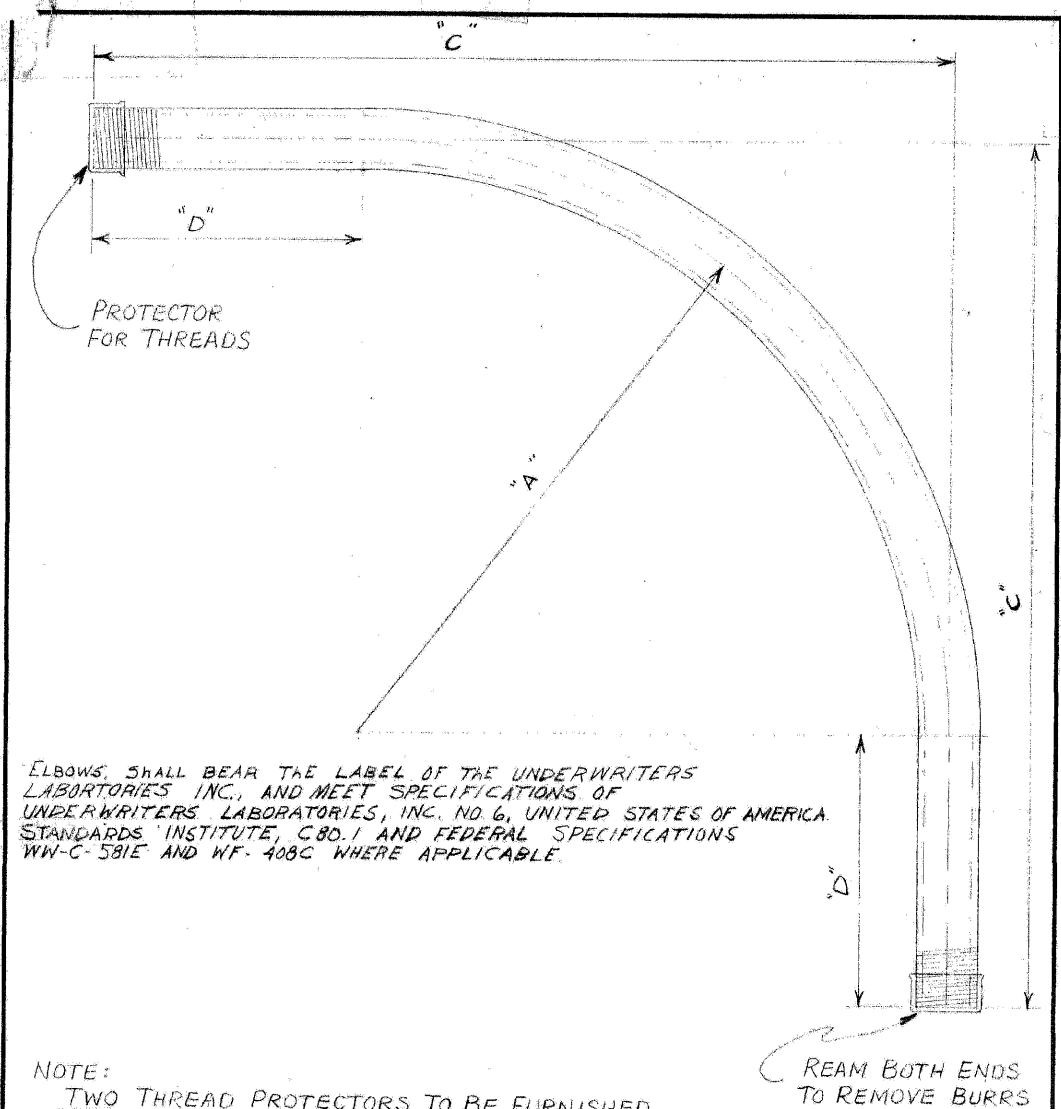
ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I. ROUTE 57 (INTERSTATE 57)

ELECTRICAL STANDARD DETAILS

SCALE: NONE
DATE: 01/16/09
DRAWN BY: N/A
CHECKED BY: N/A

TENG

TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS-PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/678-0000



ELBOWS SHALL BEAR THE LABEL OF THE UNDERWRITERS LABORATORIES INC., AND MEET SPECIFICATIONS OF UNDERWRITERS LABORATORIES, INC. NO. 6, UNITED STATES OF AMERICA STANDARDS INSTITUTE, C80.1 AND FEDERAL SPECIFICATIONS WW-C-581E AND WF-408C WHERE APPLICABLE.

NOTE: TWO THREAD PROTECTORS TO BE FURNISHED ON EACH ELBOW, PROTECTOR TO COVER A MINIMUM OF TEN THREADS.

CONDUIT SIZE	DIMENSIONS			COMMODITY CODE
	"A"	"C"	"D"	
1 1/4"	24"	35"	11"	09-4001-0510
1 1/2"	24"	35"	11"	09-4001-0520
2"	24"	35"	11"	09-4001-4126
2 1/2"	24"	35"	11"	09-4001-4128
3"	24"	35"	11"	09-4001-4230
4"	24"	35"	11"	09-4001-0000

B SPECIFICATIONS REVISED
A REVISED DIMENSIONS ON 3" x 4" CONDUIT L.P.
ELBOW, CONDUIT, RIGID GALVANIZED STEEL, LARGE RADIUS

REVISED	CITY OF CHICAGO		
A	7-22-71	DEPT. OF STREETS AND SANITATION	
B	4-3-73	BUREAU OF ELECTRICITY	
C		DIVISION OF ELECTRICAL ENGINEERING	
D		DRAWN	CHECKED
E		LON BURDY	M.S.
F		ENGINEER	
G		M. SHINE	
		DRG. NO.	
		11825	
		DEPUTY COMM.	
		DATE 6-2-71	

SIZE 8 1/2" x 14" SCALE: 3/16"

<p>7 CONDUCTOR - 7 POINT STRIP</p> <ul style="list-style-type: none"> 7 - COMMON WHITE 6 - WHITE BLK. TR. E & W RED 5 - BLACK - SOLID " AMBER 4 - BLUE - " " GREEN 3 - RED - " " N & S RED 2 - ORANGE - " " AMBER 1 - GREEN - " " GREEN 	<p>14 CONDUCTOR - 10 POINT STRIP</p> <ul style="list-style-type: none"> 10 - COMMON WHITE 9 - RED WHITE TRACER 8 - ORANGE " " 7 - GREEN " " 6 - RED BLACK TRACER 5 - ORANGE " " 4 - GREEN " " 3 - RED - SOLID 2 - ORANGE - SOLID 1 - GREEN - SOLID <p>GREEN BLUE TRACER } ORANGE " " } SPLICED & TAPED RED " " } BLACK - SOLID }</p>	<p>22 CONDUCTOR - CODE 1/2 CODE - OMIT *19 *21 *22</p> <ul style="list-style-type: none"> 1 - WH RED/BLACK TR. COMMON 2 - WH RED/GREEN TR. COMMON (SPARE) 3 - R BLACK TR. NO. BOUND 4 - A " " " " 5 - G " " " " 6 - R SOLIDS SO. BOUND 7 - A " " " " 8 - G " " " " 9 - BK " " NEON OR ARROW 10 - R BLUE TR. EAST BOUND 11 - A " " " " 12 - G " " " " 13 - R WHITE TR. WEST BOUND 14 - A " " " " 15 - G " " " " 16 - BL SOLID - SPECIAL 17 - BL AMBER TR - " 18 - BL WHITE TR - " 19 - WH RED TR - " 20 - WH SOLID - " 21 - WH BLACK TR - " 22 - BK WHITE TR - " 																																
<p>10 CONDUCTOR - 7 POINT STRIP</p> <ul style="list-style-type: none"> 7 - COMMON - WHITE 6 - RED BLK. TR. E & W RED 5 - ORANGE BLK. TR. " AMBER 4 - GREEN BLK. TR. " GREEN 3 - RED - SOLID N & S RED 2 - ORANGE - " " AMBER 1 - GREEN - " " GREEN <p>SOLID BLUE } SOLID BLACK } SPLICED & TAPED BLUE BLK. TR. }</p>	<p>14 CONDUCTOR - 14 POINT STRIP</p> <ul style="list-style-type: none"> 14 - COMMON WHITE 13 - BLACK 12 - RED - BLUE TRACER 11 - ORANGE - " " 10 - GREEN - " " 9 - RED - WHITE TRACER 8 - ORANGE - " " 7 - GREEN - " " 6 - RED - BLACK TRACER 5 - ORANGE - " " 4 - GREEN - " " 3 - SOLID RED 2 - SOLID ORANGE 1 - SOLID GREEN 	<p>SPLIT CORNER</p>																																
<p>10 CONDUCTOR - 10 POINT STRIP</p> <ul style="list-style-type: none"> 10 - COMMON - WHITE 9 - BLUE BLK. TRACER * 8 - BLACK - SOLID * 7 - BLUE - SOLID * 6 - RED BLK. TRACER E & W RED 5 - ORANGE BLK. " " AMBER 4 - GREEN BLK. " " GREEN 3 - RED - SOLID N & S RED 2 - ORANGE - SOLID " AMBER 1 - GREEN - SOLID " GREEN <p>* - SOLID BLUE - GREEN LIGHT } IF USED * - SOLID BLACK - AMBER " } * - BLUE BLK. TR. RED " }</p>	<p>STRAIGHT CORNER</p>	<p>TRAFFIC CONTROL SIGNALS STRIP WIRING LAYOUT</p> <table border="1"> <tr> <td>REVISED</td> <td colspan="3">CITY OF CHICAGO</td> </tr> <tr> <td>A</td> <td>5-4-61</td> <td colspan="2">DEPT. OF STREETS AND SANITATION</td> </tr> <tr> <td>B</td> <td>11-10-64</td> <td colspan="2">DIVISION OF ELECTRICAL OPERATION</td> </tr> <tr> <td>C</td> <td></td> <td>DRAWN</td> <td>CHECKED</td> </tr> <tr> <td>D</td> <td></td> <td>W.S.M. CARR</td> <td>12268-A</td> </tr> <tr> <td>E</td> <td></td> <td colspan="2">SUPERINTENDENT</td> </tr> <tr> <td>F</td> <td></td> <td colspan="2">COMMISSIONER</td> </tr> <tr> <td>G</td> <td></td> <td>FILE 16*21</td> <td>DATE 11-8-56</td> </tr> </table>	REVISED	CITY OF CHICAGO			A	5-4-61	DEPT. OF STREETS AND SANITATION		B	11-10-64	DIVISION OF ELECTRICAL OPERATION		C		DRAWN	CHECKED	D		W.S.M. CARR	12268-A	E		SUPERINTENDENT		F		COMMISSIONER		G		FILE 16*21	DATE 11-8-56
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F		COMMISSIONER																																
G		FILE 16*21	DATE 11-8-56																															

NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I. ROUTE 57 (INTERSTATE 57)

ELECTRICAL STANDARD DETAILS

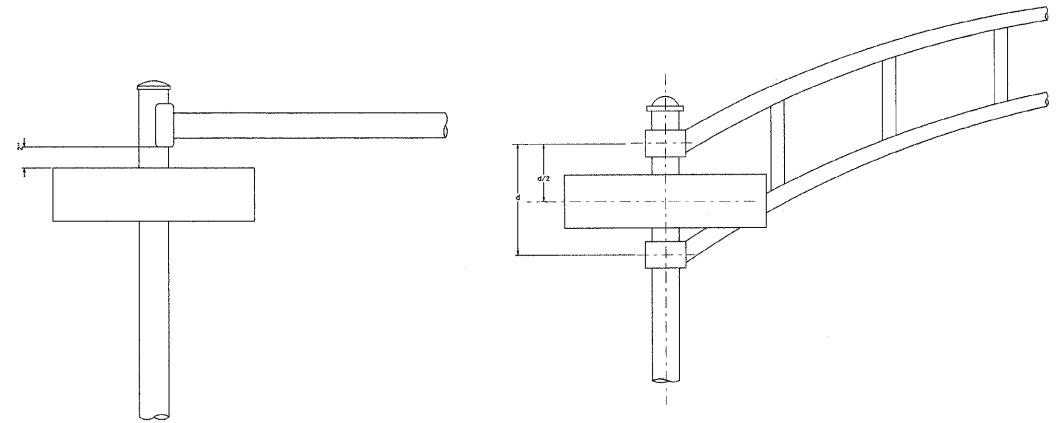
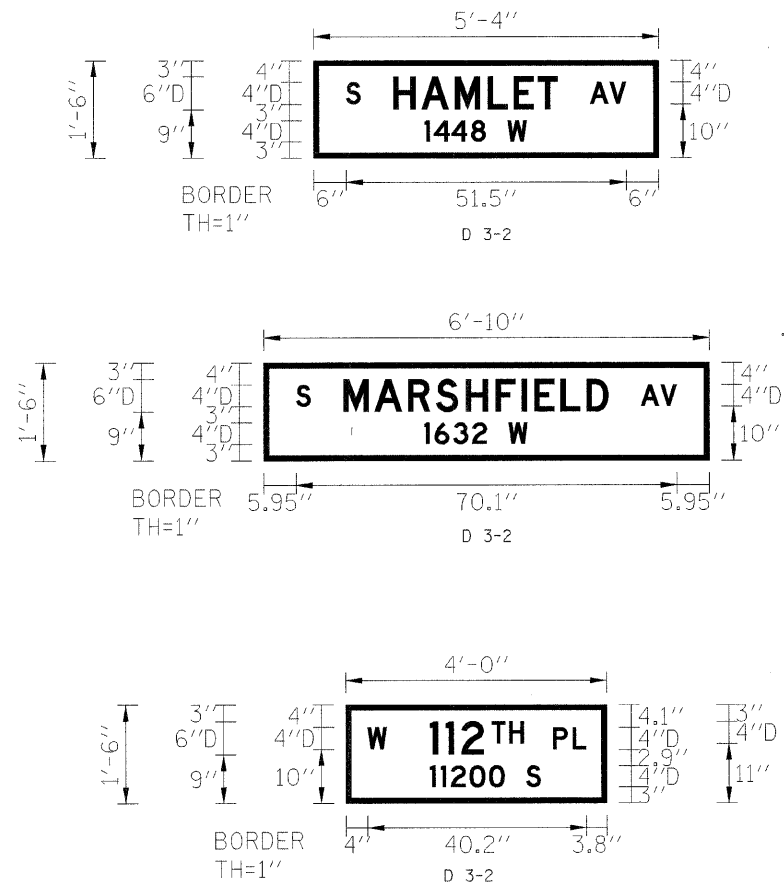
SCALE: NONE DRAWN BY: N/A
 DATE: 01/16/09 CHECKED BY: N/A

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312/416-0000

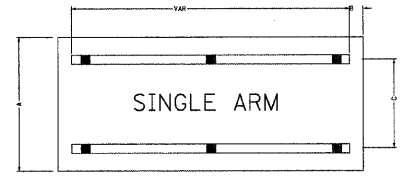
NORTH ST. BORGERSON
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 HECHTBR
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	059
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				

MOUNTING LOCATIONS

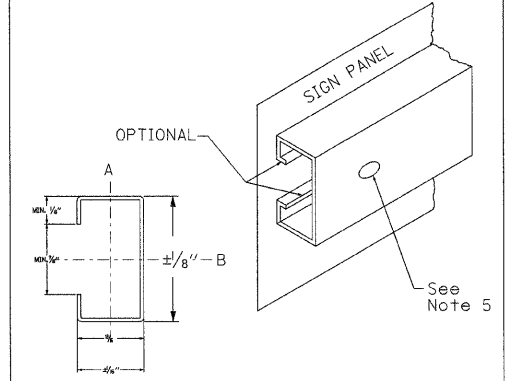


DETAIL "A"
SUPPORTING CHANNELS



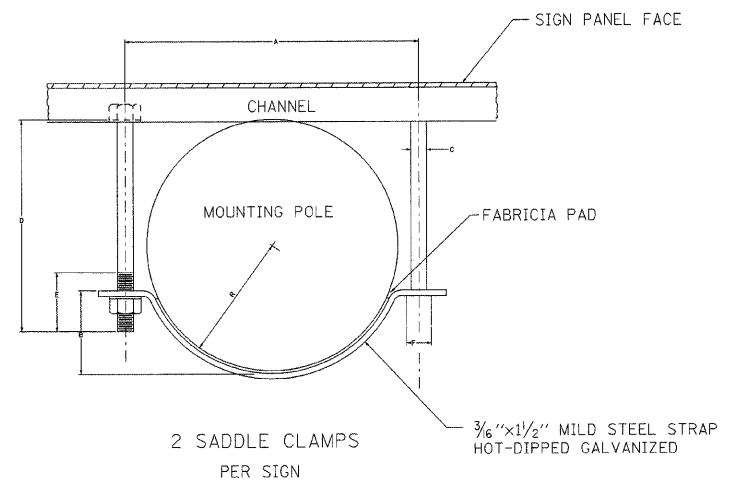
A	B	C
18"	2"	12"
24"	2"	16"

STEEL (MILD STRIP, ASTM A.525)
Section modulus, axle A - .050 in³ minimum
Section modulus, axle B - .105 in³ minimum
Galvanized before forming in accordance with ASTM A.525, Coating Designation G 90 or after forming in accordance with AASHTO M-232, class B-2
or
ALUMINUM (6061 T6 or 6063 T5 or T6)
Section modulus, axle A - .150 in³ minimum
Section modulus, axle B - .318 in³ minimum



- All mounting hardware, exclusive of supporting channels shall be zinc or cadmium plated steel or stainless steel.
- All bolts and nuts shall have National Coarse Thread (UNC).
- All signs mounted to supporting channels shall be attached with at least 5/16" diameter hex bolts.
- Horizontal supporting channels used to brace individual signs shall be located using the mounting holes prepunched in the sign blank.
- Mounting holes in the supporting channel shall be 5/16" diameter. Channel material with prepunched holes at 2" centers may be used.
- Sign panel to channel bolt centers should not exceed 2'-0".

DETAIL "B"



SIGN TYPE	SIGN SIZE	D3-2 TYPE SIGN DIMENSIONS								LETTER SIZE LINES			BORDER
		A	B	C	D	E	F	G	H	1	2	3	
D3-2	VAR x 18	VAR	18"	1.0"	6.0"	3.0"	2.0"	4D	4" T06"	6D	4D	-	1.0"
D3-2(a)	VAR x 24	VAR	24"	1.0"	6.0"	2.5"	1.5"	4D	4" T06"	6D	4D	4D	1.0"

MOUNTING POLE	D3-2 TYPE MOUNT DIMENSIONS						
	R	A	B	C	D	E	F
10"	4"	9"	2 5/8"	7/16"	7"	2 1/2"	1 1/2"
11"	4 1/2"	10"	3"	7/16"	7"	2 1/2"	1 1/2"
12 1/2"	5 1/4"	11 1/2"	3 1/2"	1/2"	8"	2"	3/16"

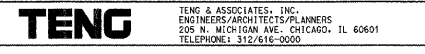
SPECIAL DESIGN SPECIFICATIONS

- The State of Illinois Department of Transportation standard specifications for traffic control items shall apply except where noted herein.
- The D3-2 and D3-2a sign length shall not be less than 4'-0" or greater than 6'-0". Sign length shall be determined by message copy, dictated by 6" capital letter series D, appropriate space chart layout, and the table of dimensions, where a required message layout would exceed maximum length of panel allowed the contractor shall layout to a reduced series stroke width. Letter series height shall remain as 6" capitals.
- Reflective sheeting shall conform to T602.01 TYPE A with green background and white letter and border.
- The sign panel shall conform to 601.01 TYPE 1. Supporting channels and panel to channel attachment hardware shall be in conformance to detail "A" of this plan sheet.
- Sign installation hardware shall be in conformance to detail "B" of this plan sheet.

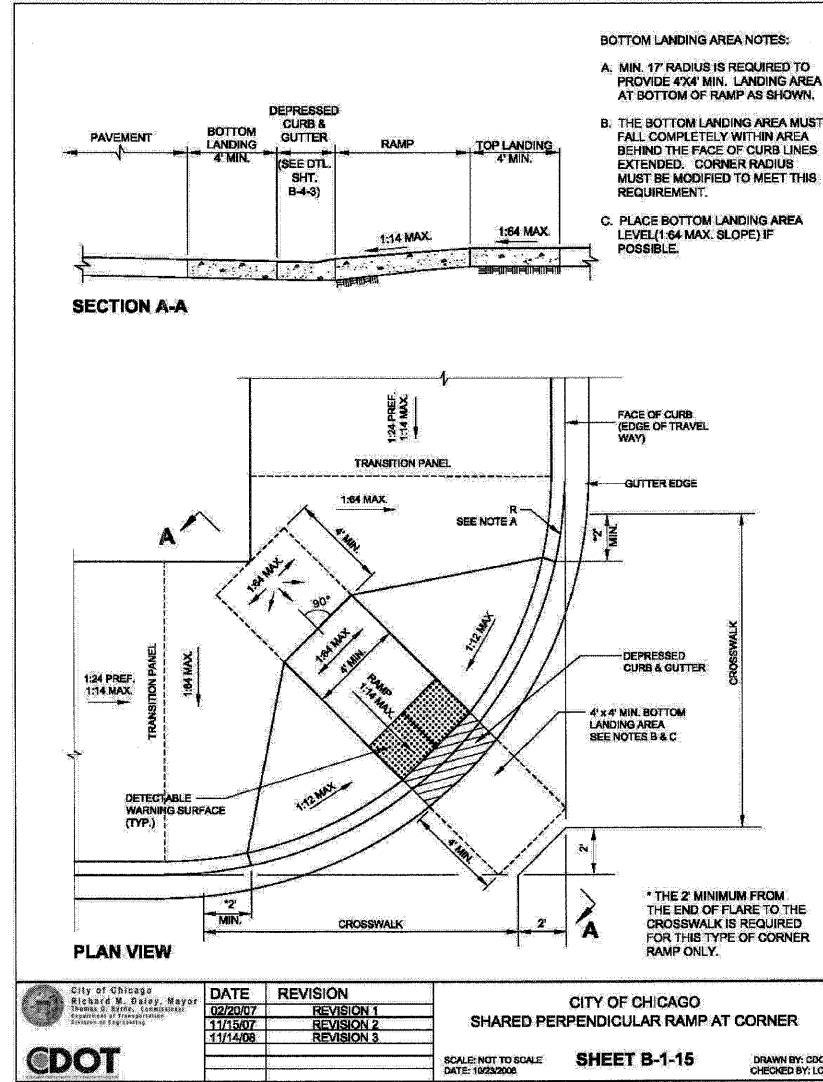
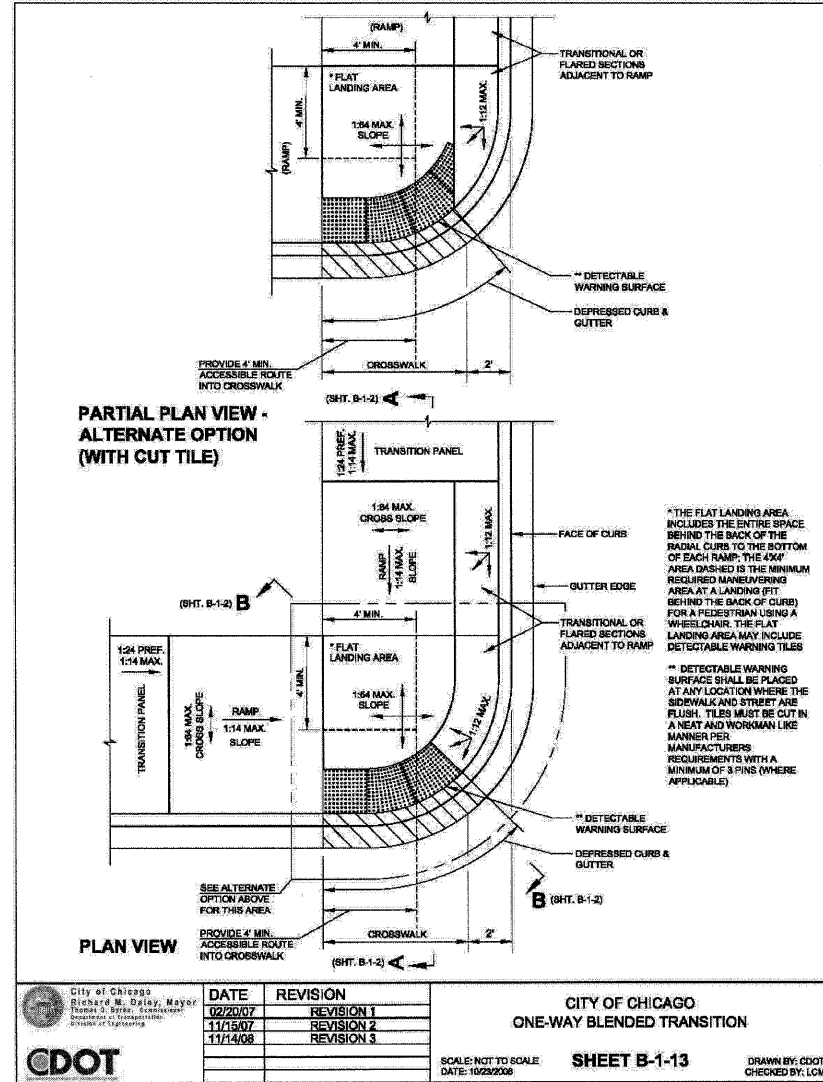
ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)
ELECTRICAL STANDARD DETAILS

REVISIONS	
NAME	DATE

SCALE: NONE
DATE: 01/16/09
DRAWN BY: N/A
CHECKED BY: N/A



\\N12TH ST BORDER.DGN, ..\EXH0007.DGN, ..\P\WOODTOD.DGN
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 12 13 14 15 16 17 18 19 20 21 22 23 24 25 26 27 28 29 30 31 32 33 34 35 36 37 38 39 40 41 42 43 44 45 46 47 48 49 50 51 52 53 54 55 56 57 58 59 60 61 62 63
 HECHTBR



SLOPE CONVERSION CHART			
% SLOPE	SLOPE RATIO	INCHES PER FOOT	DECIMAL FEET PER FOOT
16.67%	1:6	2"	0.167'
10%	1:10	1 1/4"	0.104'
8.33%	1:12	1"	0.083'
7.14%	1:14	7/8"	0.073'
5%	1:20	5/8"	0.052'
4.17%	1:24	1/2"	0.042'
2%	1:50	1/4"	0.021'
1.56%	1:64	3/16"	0.016'

CITY OF CHICAGO
Richard M. Daley, Mayor
Thomas S. Byrne, Commissioner
Department of Transportation
Division of Engineering

DATE	REVISION
02/20/07	REVISION 1
11/15/07	REVISION 2
11/14/08	REVISION 3

SCALE: NOT TO SCALE
DATE: 10/29/08

SHEET B-3-1 DRAWN BY: CDOT CHECKED BY: LGM

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REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)

**CITY OF CHICAGO DETAIL
ADA STANDARDS - II**


SCALE: N.T.S. DRAWN BY: N/A
DATE: 01/16/09 CHECKED BY: N/A

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE. - CHICAGO, IL 60601
TELEPHONE: 312/618-0000

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	063
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				


GENERAL NOTES

- THE DETECTABLE WARNING TILES SHALL BE CERAMIC CEMENT OR COMPOSITE POLYMER CONCRETE (CRC) TILES WHICH MEET THE CHICAGO DEPARTMENT OF TRANSPORTATION SPECIFICATION (AVAILABLE ON THE CITY OF CHICAGO WEBSITE). IN THE CENTRAL BUSINESS DISTRICT, GRANITE OR OTHER SPECIALTY PAVING MATERIALS MAY BE SUBMITTED TO THE COMMISSIONER FOR APPROVAL.
- TILES MUST BE INSTALLED A MAXIMUM OF 8" OR LESS FROM FACE OF CURB (SEE DETAIL SHEET B-4-2).
- TILES MUST COVER FULL WIDTH OF RAMP EXCLUDING SIDE FLARES FOR A MINIMUM UNOBSTRUCTED DEPTH OF 24". TILES LOCATED ON THE SURFACES OF RAMPS ARE TYPICALLY ORIENTED PERPENDICULAR TO THE RUN OF THE RAMP UNLESS SPECIAL CIRCUMSTANCES OCCUR (SEE DETAIL SHEET B-1-6). TILES MUST BE PROVIDED FOR A MINIMUM DEPTH OF 24" FOR THE ENTIRE LENGTH OF THE SIDEWALK WHERE THE SIDEWALK IS FLUSH WITH THE STREET (DEPRESSED CURB OR FLUSH TRANSITION).
- WHERE APPLICABLE, A COMBINATION OF DIFFERING SIZE TILES MAY BE USED ON COMPOUND AND LARGE RADII. CONTRACTOR MUST MAKE THIS DETERMINATION AND VERIFY IN FIELD.
- TILES MUST CONTRAST WITH ADJACENT PAVEMENT. IF LIGHT COLORED PAVEMENT IS USED THE TILE COLOR SHALL BE RED. IF A DARK COLORED PAVEMENT IS USED THE TILE COLOR SHALL BE YELLOW. CONTRACTOR TO VERIFY THAT PROPER CONTRAST IS OBTAINED.
- RAMP WIDTH MUST BE A MINIMUM OF 6'-0" AND IN INCREMENTS OF 1'-0", EXCEPT WHEN USING THE PERPENDICULAR RAMP AT CORNER, WHICH HAS A MINIMUM WIDTH OF 4'-0".
- PRIOR TO PLACING CONCRETE FOR DEPRESSED CURBS AND RAMPS THE CONTRACTOR SHALL NOTIFY THE COMMISSIONER TO RECEIVE LAYOUT APPROVAL. A PRE-MANUFACTURED TILE, APPROVED BY THE COMMISSIONER, MUST BE USED TO ACHIEVE THE DETECTABLE WARNING SURFACE.
- THE MAXIMUM ALLOWABLE RAMP RUNNING SLOPE IS 1:14, MEASURED AT ANY PORTION OF THE RAMP. IF POSSIBLE, A MORE GRADUAL SLOPE SHALL BE USED. GRADE BREAKS AT THE TOP AND BOTTOM OF RAMPS SHALL BE PERPENDICULAR TO THE DIRECTION OF RAMP RUN.
- THE MAXIMUM ALLOWABLE RAMP CROSS SLOPE IS 1:64, MEASURED AT ANY PORTION OF THE RAMP. IF POSSIBLE, A MORE GRADUAL SLOPE SHALL BE USED.
- THE MAXIMUM ALLOWABLE RAMP LANDING SLOPE IS 1:64, MEASURED AT ANY LOCATION AND IN ANY DIRECTION ON THE LANDING. THE RAMP LANDING WIDTH SHALL MATCH THE FULL WIDTH OF THE RAMP FOR A MINIMUM UNOBSTRUCTED DEPTH OF 4'-0". RAMP LANDINGS SHALL BE PROVIDED AT THE TOP AND/OR BOTTOM OF RAMPS WHERE TURNING IS REQUIRED.
- RAMP SIDE FLARES SHALL BE INSTALLED AT ANY LOCATION WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS INTENDED FOR PEDESTRIAN USE. TRIPPING HAZARDS, INCLUDING STEPS, DROP-OFFS, OR SIDE CURBS SHALL NOT BE LOCATED WITHIN THE LIMITS OF THE SIDEWALK.
- UTILITIES, SUCH AS LIGHT POLES, TRAFFIC POLES AND HYDRANTS, MAY BE LOCATED IN THE FLARE OF THE RAMP BUT ARE NOT ALLOWED ON THE RAMP SURFACE OR LANDING AREAS.
- ALL LOCATIONS WITH TYPE 4 OR TYPE B CURBS (EXCEPT ALLEY APRONS) SHALL BE CONSTRUCTED AS CURB AND GUTTER TYPE BV.12 THROUGH THE LIMITS OF THE CORNER AND THE CURB RAMPS.
- ALTERATIONS SHALL NOT DECREASE THE ACCESSIBILITY TO EXISTING FACILITIES, SIDEWALKS LEADING TO EXISTING FACILITIES OR DOOR OR GATE ACCESS POINTS TO FACILITIES. THE ELEVATION AT THE EXISTING PROPERTY LINE OR FACILITY ACCESS POINT SHALL BE MAINTAINED AT A MINIMUM. ANY ALTERATIONS ADJACENT TO OR AFFECTING A FACILITY ACCESS POINT SHALL RESULT IN IMPROVED ACCESS OR AT A MINIMUM A REPLICATION OF EXISTING CONDITIONS, INCLUDING SIDEWALK SLOPES AND SURFACE CONDITIONS. FACILITIES INCLUDE, BUT ARE NOT LIMITED TO PRIVATE BUSINESSES, PUBLIC BUILDINGS, RESIDENCES, BUS STOPS, PUBLIC BENCHES, PAY PHONES, AND PARKING METERS.
- THE MINIMUM CROSSWALK WIDTH IS 6'-0". CROSSWALKS SHALL BE LOCATED AS SHOWN IN THE PLAN SHEETS DEPENDING ON THE TYPE OF CURB RAMP USED. BEYOND THE CURB FACE AT THE BASE OF CURB RAMPS, A CLEAR SPACE OF 4'-0" BY 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE WIDTH OF THE CROSSWALK.

 City of Chicago Richard M. Daley, Mayor Thomas S. Byrne, Commissioner Department of Transportation Division of Engineering	DATE	REVISION	CITY OF CHICAGO GENERAL NOTES SCALE: NOT TO SCALE DATE: 10/29/2008 SHEET B-3-2 DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	

GENERAL NOTES (CONTINUED)

- IF SIDEWALK AND ALLEY ARE AT THE SAME GRADE, A RAMP IS NOT REQUIRED. IF SIDEWALK AND DRIVEWAY ARE AT THE SAME GRADE, A RAMP IS NOT REQUIRED BUT DETECTABLE WARNING TILES ARE STILL REQUIRED IF THE DRIVEWAY HAS TRAFFIC CONTROL DEVICES (I.E. TRAFFIC SIGNALS).
- MAIN LINE SIDEWALK SHALL HAVE A MAXIMUM CROSS SLOPE NOT TO EXCEED 1:64 FOR THE FULL WIDTH OF WALK UNLESS OTHERWISE APPROVED BY THE COMMISSIONER. WHERE TURNING IS REQUIRED AND WHERE SIDEWALKS INTERSECT, THE SLOPE OF THE SIDEWALK SHALL NOT EXCEED 1:64 IN ANY DIRECTION.
- MAIN LINE SIDEWALK RUNNING SLOPES SHALL NOT EXCEED 1:24 OR THE GENERAL GRADE ESTABLISHED FOR THE ADJACENT STREET, WHICH EVER IS HIGHER.
- THERE SHALL BE NO VERTICAL LEVEL DIFFERENCES BETWEEN SURFACES GREATER THAN 1/4" ON THE MAIN LINE SIDEWALK. THERE SHALL BE NO HORIZONTAL GAPS OR OPENINGS GREATER THAN 1/2" ON THE MAIN LINE SIDEWALK.
- CURB RAMPS AND KEYSTONE TO BE CONSTRUCTED WITH 8" THICK CONCRETE AT ALL TRAFFIC SIGNALIZED INTERSECTIONS AND INDUSTRIAL STREET INTERSECTIONS. AT ALL OTHER LOCATIONS, 5" THICK CONCRETE TO BE USED.
- NO DEVIATIONS FROM THESE STANDARDS ARE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COMMISSIONER.
- ALL CONSTRUCTION DOCUMENTS MUST BE STAMPED BY A LICENSED ARCHITECT/LANDSCAPE ARCHITECT/ENGINEER TO CERTIFY THAT THEY ARE IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA) AND ALL CODES AND BUILDING ORDINANCES OF THE CITY OF CHICAGO AND STATE OF ILLINOIS.

 City of Chicago Richard M. Daley, Mayor Thomas S. Byrne, Commissioner Department of Transportation Division of Engineering	DATE	REVISION	CITY OF CHICAGO GENERAL NOTES SCALE: NOT TO SCALE DATE: 10/29/2008 SHEET B-3-3 DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	

ADA COMPLIANCE AND TRANSITION GUIDELINES

POLICY STATEMENT: ANY ALTERATION OF THE PUBLIC WAY MUST BE RESTORED IN AN ADA COMPLIANT MANNER

I. STREET RESURFACING / RECONSTRUCTION *

FOR ANY RESURFACING/RECONSTRUCTION PROJECT WHERE, WITHIN THE PROJECT LIMITS, A CROSSWALK IS ENCOUNTERED OR WHERE THE PROJECT LIMITS TERMINATE WITHIN 4' OR LESS OF A CROSSWALK, THOSE CROSSWALKS AND THE ASSOCIATED CURB RAMPS MUST BE IMPROVED TO CURRENT ADA STANDARDS. WHERE RESURFACING/RECONSTRUCTION ENCOUNTERS LESS THAN 1/4 POINT OF THE STREET SURFACE (LONGITUDINAL CUT), IT IS ACCEPTABLE PRACTICE TO EXCLUDE IMPROVEMENTS TO THE ASSOCIATED CROSSWALKS AND CURB RAMPS.

WHEN A PROJECT SCOPE OF WORK CALLS FOR ONLY AN INTERSECTION TO BE REPAVED, THE INTERSECTION LIMITS AS DEFINED BY THE AREA OUTLINED BY OUTERMOST CROSSWALK LINES AND ADJACENT CURB FACES AND ALL ADJOINING CROSSWALKS AND CURB RAMPS MUST BE IMPROVED TO CURRENT ADA STANDARDS.

WHEN A PROJECT SCOPE OF WORK IS LIMITED TO A SINGLE CORNER OF AN INTERSECTION, THE CURB RAMP MUST BE IMPROVED TO CURRENT ADA STANDARDS AND THE ADJACENT PAVEMENT MUST BE RESURFACED, AS NECESSARY TO PROVIDE FOR A FLUSH TRANSITION.

FOR ANY RESURFACING / RECONSTRUCTION PROJECT WHERE, WITHIN THE PROJECT LIMITS, AN ALLEY APRON IS ENCOUNTERED, THE ASSOCIATED CURB RAMPS, ALLEY APRON, AND SIDEWALKS MUST BE IMPROVED TO CURRENT ADA STANDARDS.

II. SIDEWALK INSTALLATION / REPAIRS / RECONSTRUCTION

THE LIMITS OF ANY MAINLINE SIDEWALK REPLACEMENT, GREATER THAN TEN FEET (10') IN LENGTH, THAT ABUT AN EXISTING RAMP LOCATION (INCLUDING THE FOUR FOOT (4') LANDING AREA AND/OR THE "KEYSTONE"), SHALL BE EXTENDED TO INCLUDE THE AFFECTED RAMPS AND THESE RAMPS SHALL BE RECONSTRUCTED TO CURRENT ADA STANDARDS. IN ADDITION, ALL NEWLY PLACED SIDEWALK TEN FEET (10') OR MORE IN LENGTH SHALL BE CONSTRUCTED IN ACCORDANCE WITH ALL CURRENT APPLICABLE STANDARDS WHICH INCLUDE PROVIDING A MINIMUM FOUR FEET (4') WIDTH ACCESSIBLE PATHWAY WITH A CROSS SLOPE NOT TO EXCEED 1:64.

III. GUIDELINES FOR TRANSITIONING TO EXISTING NON-COMPLIANT CONDITION


NEW SIDEWALK PLACEMENTS GREATER THAN TEN FEET IN CONTIGUOUS LENGTH:
 THE LIMITS OF ANY MAINLINE SIDEWALK REPLACEMENT, GREATER THAN TEN FEET (10') IN LENGTH, MUST BE EXTENDED FOR A MINIMUM FIVE ADDITIONAL FEET (5') EITHER SIDE IN ORDER TO PROVIDE A REASONABLE TRANSITION TO MATCH THE EXISTING SIDEWALK. THE LENGTH OF TRANSITION SHALL BE LENGTHENED AS NECESSARY TO ENSURE THAT THE RUNNING SLOPE OF THE TRANSITION DOES NOT EXCEED A SLOPE OF 1:24 (PREFERRED) OR 1:14 (MAXIMUM) AT ANY POINT.

NEW SIDEWALK REPLACEMENTS TEN FEET OR LESS IN CONTIGUOUS LENGTH (REPAIRS):
 IT IS ACCEPTABLE PRACTICE TO MATCH ADJACENT SIDEWALKS AT THE EXISTING SLOPE.

CURB RAMP REPLACEMENTS:
 SIDEWALK REPLACEMENT BEYOND THE LIMITS OF THE LANDING AREA AND/OR THE "KEYSTONE" MUST BE EXTENDED A MINIMUM OF AN ADDITIONAL FIVE FEET (5') EITHER SIDE IN ORDER TO PROVIDE A REASONABLE TRANSITION TO MATCH THE EXISTING SIDEWALK. THE TRANSITION SHALL BE LENGTHENED AS NECESSARY TO ENSURE THAT THE RUNNING SLOPE OF THE TRANSITION DOES NOT EXCEED A SLOPE OF 1:24 (PREFERRED) OR 1:14 (MAXIMUM) AT ANY POINT.

NO EXCEPTIONS TO THE ABOVE WILL BE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COMMISSIONER.

* THE REQUIREMENTS OF SECTION I. SHALL APPLY ONLY TO CITY AGENCIES

 City of Chicago Richard M. Daley, Mayor Thomas S. Byrne, Commissioner Department of Transportation Division of Engineering	DATE	REVISION	CITY OF CHICAGO ADA COMPLIANCE AND TRANSITION GUIDELINES SCALE: NOT TO SCALE DATE: 10/29/2008 SHEET B-3-4 DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	

REVISIONS	
NAME	DATE

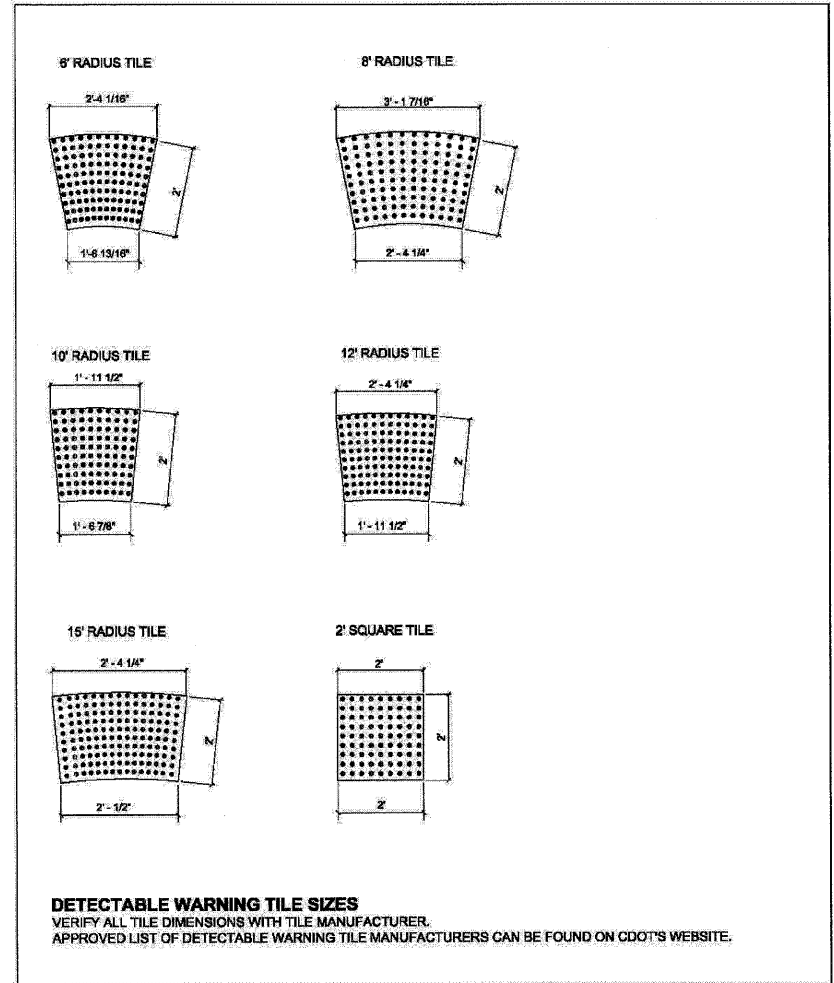
ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I ROUTE 57 (INTERSTATE 57)

**CITY OF CHICAGO DETAIL
 ADA STANDARDS - III**

SCALE: N.T.S. DRAWN BY: N/A
 DATE: 01/16/09 CHECKED BY: N/A

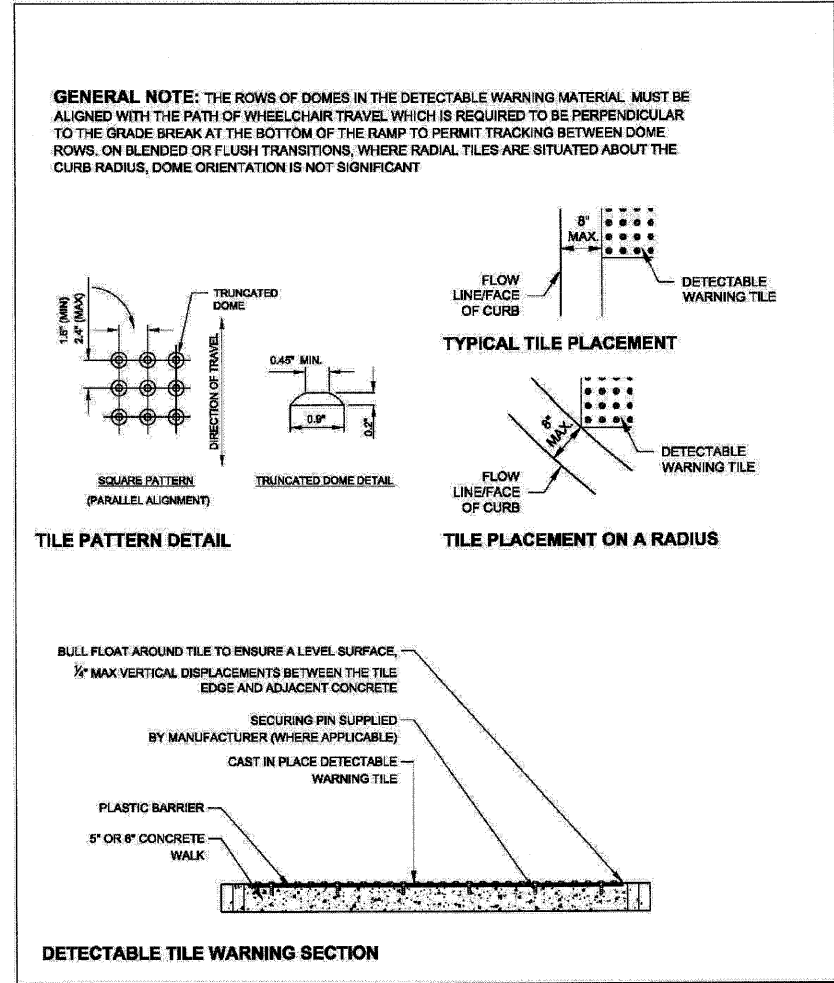
TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.616.9000

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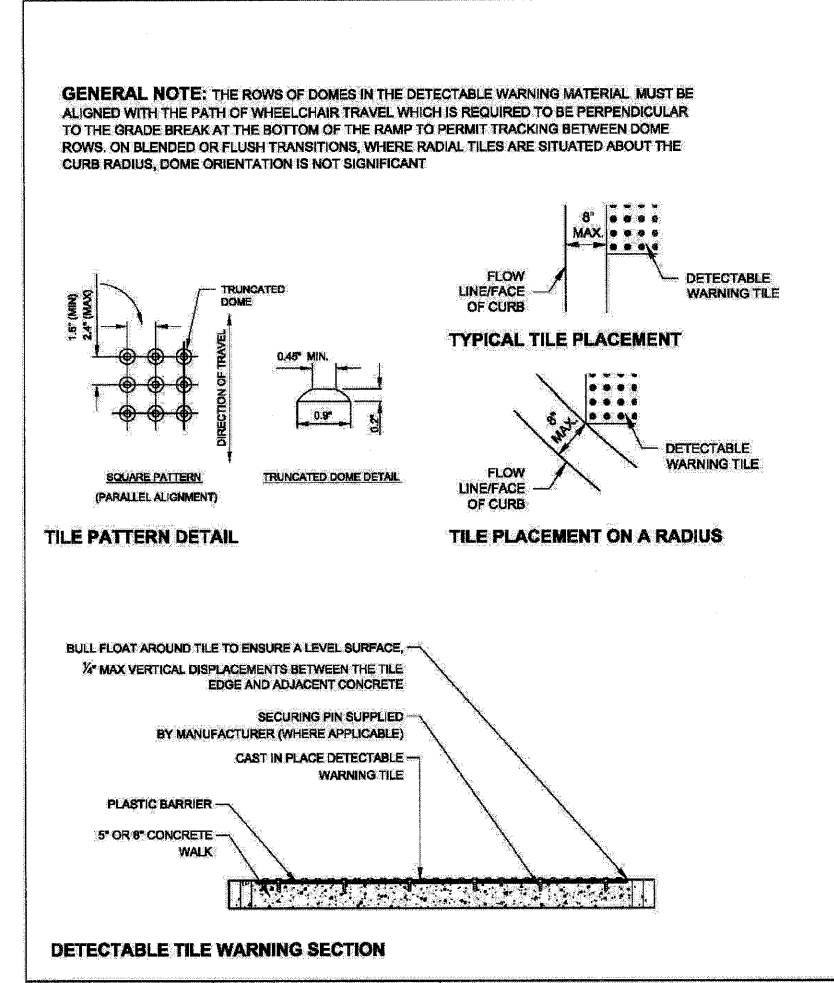


DETECTABLE WARNING TILE SIZES
 VERIFY ALL TILE DIMENSIONS WITH TILE MANUFACTURER.
 APPROVED LIST OF DETECTABLE WARNING TILE MANUFACTURERS CAN BE FOUND ON CDOT'S WEBSITE.

City of Chicago Richard M. Daley, Mayor Thomas A. Byrne, Commissioner Department of Transportation Division of Engineering	DATE	REVISION	CITY OF CHICAGO DETECTABLE WARNING TILE DIMENSIONS SCALE: NOT TO SCALE DATE: 10/29/2008 SHEET B-4-1 DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	



City of Chicago Richard M. Daley, Mayor Thomas A. Byrne, Commissioner Department of Transportation Division of Engineering	DATE	REVISION	CITY OF CHICAGO DETECTABLE WARNING TILE DETAILS SCALE: NOT TO SCALE DATE: 10/29/2008 SHEET B-4-2 DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	



City of Chicago Richard M. Daley, Mayor Thomas A. Byrne, Commissioner Department of Transportation Division of Engineering	DATE	REVISION	CITY OF CHICAGO DETECTABLE WARNING TILE DETAILS SCALE: NOT TO SCALE DATE: 10/29/2008 SHEET B-4-2 DRAWN BY: CDOT CHECKED BY: LCM
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	11/15/07	REVISION 2	
	11/14/08	REVISION 3	

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REVISIONS	
NAME	DATE

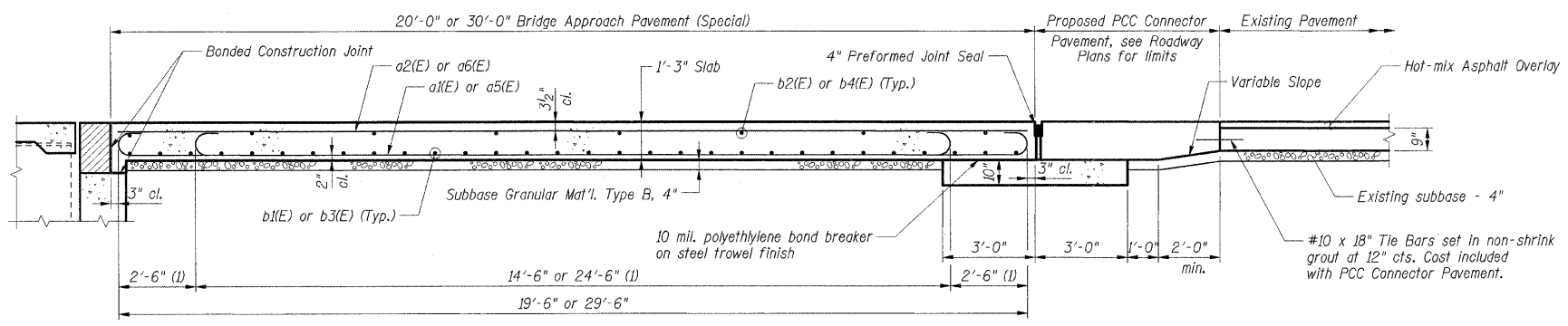
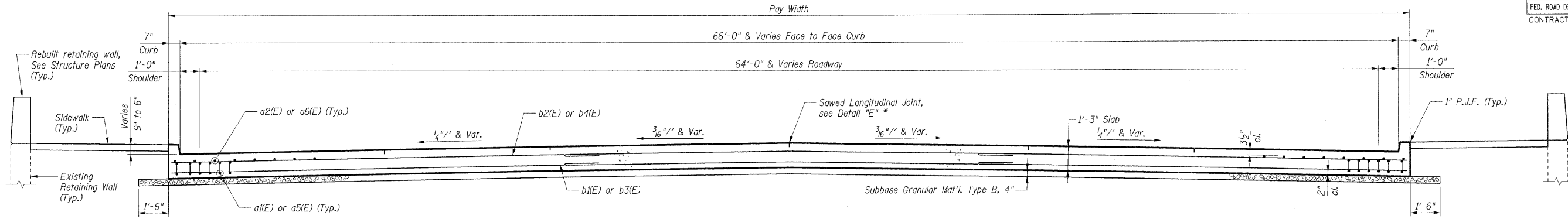
ILLINOIS DEPARTMENT OF TRANSPORTATION
 112TH PLACE OVER
 F.A.I ROUTE 57 (INTERSTATE 57)

**CITY OF CHICAGO DETAIL
 ADA STANDARDS - IV**

SCALE: N.T.S. DRAWN BY: N/A
 DATE: 01/16/09 CHECKED BY: N/A

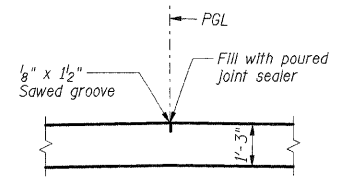
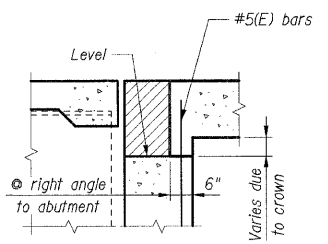
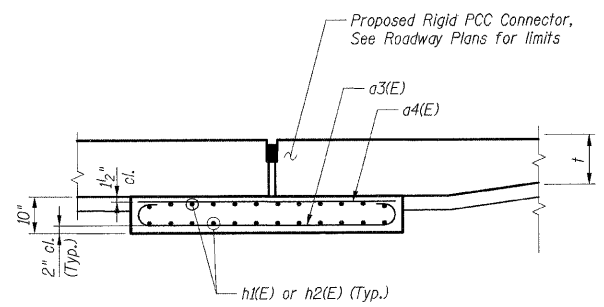
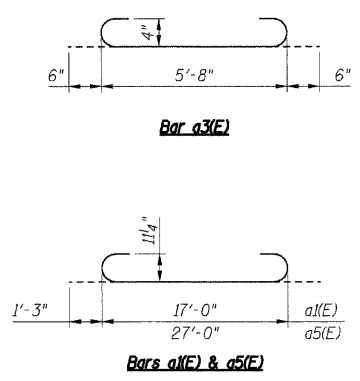
TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE. CHICAGO, IL 60601
 TELEPHONE: 312/616-0000

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	066
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				



MATERIAL REQUIRED FOR BRIDGE APPROACH PAVEMENT (SPECIAL)

Bar	No.	Size	Length	Shape	
a1(E)	134	#9	19'-6"	U	
a2(E)	67	#5	19'-6"	—	
a3(E)	152	#4	6'-8"	U	
a4(E)	152	#4	5'-8"	—	
a5(E)	134	#9	29'-6"	U	
a6(E)	67	#5	29'-6"	—	
a7(E)	8	#5	12'-0"	—	
b1(E)	60	#5	25'-7"	—	
b2(E)	18	#4	25'-4"	—	
b3(E)	90	#5	23'-5"	—	
b4(E)	24	#4	23'-2"	—	
h1(E)	72	#5	26'-9"	—	
h2(E)	72	#5	24'-6"	—	
Item				Unit	Total
Concrete Structures		Cu Yd	160.1	**	
Reinforcement Bars, Epoxy Coated		Lbs	35,420	**	
Polyethylene Bond Breaker		Sq Yd	99	**	
Concrete Pad		Sq Yd	99	**	



- Notes:**
1. Reinforcement bars designated (E) shall be epoxy coated.
 2. Work this sheet with Sht. 065.
 3. Thickness "t" = Thickness of Pavement.
 4. See Standard 42.1001 for reinforcement details not shown.
 5. See Standard 42.0001 for details of joints not shown.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
112TH PLACE OVER
F.A.I ROUTE 57 (INTERSTATE 57)
**BRIDGE APPROACH PAVEMENT,
SPECIAL AT SN 016-0988
DETAILS**

SCALE: N.T.S. DRAWN BY: RDS
DATE: 01/16/09 CHECKED BY: ACL

TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312/615-0000

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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	068
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 62533				

GENERAL NOTES

- Fasteners shall be AASHTO M164 Type 1, mechanically galvanized bolts. Bolts 7/16 in. ϕ , holes 5/16 in. ϕ , unless otherwise noted.
- No field welding is permitted except as specified in the contract documents.
- The Contractor shall test the existing welds by non-destructive methods within 2 ft. of the end of the existing cover plates for cracks after removal of the existing concrete deck. Dye penetrant (PT), magnetic particle (MT), or other approved testing method shall be performed by qualified personnel approved by the Engineer. If cracks are found, report them to the Bureau of Bridges and Structures for disposition. The cost of testing is included in Removal of Existing Concrete Deck. The cost of crack repair, if necessary, will be paid for according to Article 109.04 of the Standard Specifications.
- Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
- Reinforcement bars designated (E) shall be epoxy coated.
- Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

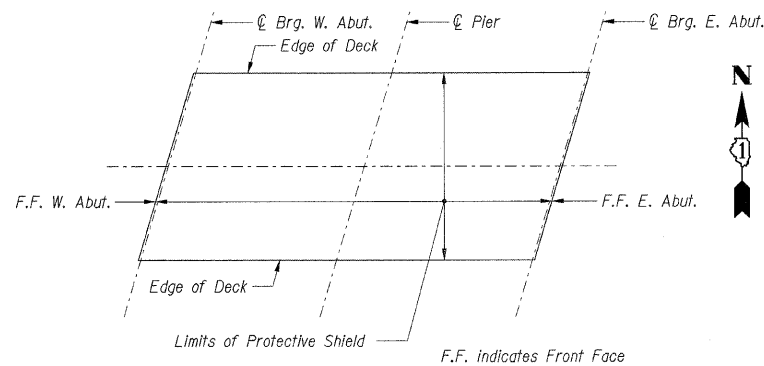
Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Concrete Sealer shall be applied to the designated areas of the abutments.
- The Organic Zinc Rich Primer/ Epoxy/ Urethane Paint System shall be used for painting of new structural steel except where otherwise noted. The entire system shall be shop applied, with the exception that masked off connection surfaces, field installed fasters and damaged areas shall be touched up in the field. The color of the final finish coat for all steel surfaces shall be Gray, Munsell No. 5B 7/1. See Special Provision for "Cleaning and Painting New Metal Structures".
- The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
- The structural steel plates of the Bearing Assembly shall conform to the requirements of AASHTO M 270 Grade 50.
- Existing structural steel shall only be cleaned and painted as required by the special provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures."

INDEX OF SHEETS

- S-001 GENERAL PLAN AND ELEVATION
- S-002 GENERAL NOTES, INDEX OF SHEETS; AND BILL OF MATERIAL
- S-003 SCREED PLAN
- S-004 TOP OF DECK ELEVATIONS I
- S-005 TOP OF DECK ELEVATIONS II
- S-006 DECK PLAN
- S-007 SUPERSTRUCTURE DETAILS 1
- S-008 SUPERSTRUCTURE DETAILS 2
- S-009 PREFORMED JOINT STRIP SEAL
- S-010 DRAINAGE SCUPPER; DS-12
- S-011 BRIDGE FENCE RAILING; PARAPET MOUNTED 1
- S-012 BRIDGE FENCE RAILING; PARAPET MOUNTED 2
- S-013 STEEL FRAMING PLAN
- S-014 STRUCTURAL STEEL DETAILS
- S-015 WEST ABUTMENT
- S-016 WEST ABUTMENT; DETAILS AND BILL OF MATERIAL
- S-017 PIER REPAIRS
- S-018 EAST ABUTMENT
- S-019 EAST ABUTMENT; DETAILS
- S-020 EAST ABUTMENT; DETAILS AND BILL OF MATERIAL
- S-021 BRIDGE DRAINAGE SYSTEM DETAILS
- S-022 BAR SPLICER ASSEMBLY DETAILS

TOTAL BILL OF MATERIAL

Item	Unit	Sub	Super	Total
POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	201		201
CONCRETE REMOVAL	CU YD	64.1		64.1
REMOVAL OF EXISTING CONCRETE DECK	L SUM		1	1
PROTECTIVE SHIELD	SQ YD		1,613	1,613
STRUCTURE EXCAVATION	CU YD	201		201
CONCRETE STRUCTURES	CU YD	62.6		62.6
CONCRETE SUPERSTRUCTURE	CU YD		512.9	512.9
BRIDGE DECK GROOVING	SQ YD		1,596	1,596
PROTECTIVE COAT	SQ YD		2,104	2,104
FURNISHING AND ERECTING STRUCTURAL STEEL	POUND		8,210	8,210
JACK AND REMOVE EXISTING BEARINGS	EACH		28	28
REINFORCEMENT BARS, EPOXY COATED	POUND	7,400	113,140	120,540
BAR SPLICERS	EACH	134		134
BRIDGE FENCE RAILING	FOOT		516	516
NAME PLATES	EACH		2	2
PREFORMED JOINT STRIP SEAL	FOOT		171	171
ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH		28	28
ANCHOR BOLTS, 1"	EACH		112	112
CONCRETE SEALER	SQ FT	346		346
EPOXY CRACK INJECTION	FOOT	55		55
DRAINAGE SCUPPERS, DS-12	EACH		7	7
STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	1566		1566
DRAINAGE SYSTEM	L SUM		1	1
TEMPORARY SHORING AND CRIBBING	L SUM		1	1



LIMITS OF PROTECTIVE SHIELD

STATION 546+15.56
 RE-BUILT 20 BY
 STATE OF ILLINOIS
 F.A.I. RT. 57 SEC 1920.1B-R
 LOADING HS20 & ALT.
 STR. NO. 016-0988

NAME PLATE

See Std. 515001
 (See sheets S-015 and S-018
 for Name Plate locations.)

SHT. S-002

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 112TH PLACE BRIDGE DECK REPLACEMENT
 112th PLACE OVER I-57
 SN 016-0988 STA. 546+15.56
 COOK COUNTY

GENERAL NOTES, INDEX OF SHEETS
 AND BILL OF MATERIAL

DATE: 01/16/09
 DRAWN BY: TMH
 CHECKED BY: JLR

TENG

TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/P.LANNERS
 209 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.464.0000

GIRDER 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+16.74	39.50	617.91	617.91
⊕ Brg. W. Abut.	13+19.30	39.50	617.92	617.92
A	13+29.30	39.50	617.98	618.02
B	13+39.30	39.50	618.02	618.10
C	13+49.30	39.50	618.05	618.14
D	13+59.30	39.50	618.07	618.16
E	13+69.30	39.50	618.07	618.15
F	13+79.30	39.50	618.06	618.11
G	13+89.30	39.50	618.04	618.06
H	13+99.30	39.50	618.00	618.00
⊕ Brg. Pier	14+05.76	39.50	617.97	617.97
J	14+15.76	39.50	617.91	617.92
K	14+25.76	39.50	617.83	617.87
L	14+35.76	39.50	617.74	617.81
M	14+45.76	39.50	617.64	617.73
N	14+55.76	39.50	617.53	617.62
P	14+65.76	39.50	617.40	617.49
R	14+75.76	39.50	617.26	617.32
S	14+85.76	39.50	617.11	617.13
⊕ Brg. E. Abut.	14+92.22	39.50	617.00	617.00
Bk. E. Abut.	14+94.78	39.50	616.96	616.96

SOUTH LONGITUDINAL JOINT

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+17.95	35.42	618.00	618.00
⊕ Brg. W. Abut.	13+20.51	35.42	618.02	618.02
A	13+30.51	35.42	618.07	618.11
B	13+40.51	35.42	618.11	618.18
C	13+50.51	35.42	618.14	618.22
D	13+60.51	35.42	618.15	618.24
E	13+70.51	35.42	618.15	618.23
F	13+80.51	35.42	618.14	618.19
G	13+90.51	35.42	618.12	618.14
H	14+00.51	35.42	618.08	618.08
⊕ Brg. Pier	14+06.97	35.42	618.04	618.04
J	14+16.97	35.42	617.98	617.99
K	14+26.97	35.42	617.91	617.94
L	14+36.97	35.42	617.82	617.88
M	14+46.97	35.42	617.72	617.79
N	14+56.97	35.42	617.60	617.69
P	14+66.97	35.42	617.47	617.55
R	14+76.97	35.42	617.33	617.39
S	14+86.97	35.42	617.17	617.20
⊕ Brg. E. Abut.	14+93.43	35.42	617.06	617.06
Bk. E. Abut.	14+95.99	35.42	617.02	617.02

GIRDER 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+18.62	33.17	618.05	618.05
⊕ Brg. W. Abut.	13+21.18	33.17	618.07	618.07
A	13+31.18	33.17	618.12	618.16
B	13+41.18	33.17	618.16	618.23
C	13+51.18	33.17	618.19	618.27
D	13+61.18	33.17	618.20	618.28
E	13+71.18	33.17	618.20	618.27
F	13+81.18	33.17	618.19	618.24
G	13+91.18	33.17	618.16	618.18
H	14+01.18	33.17	618.12	618.12
⊕ Brg. Pier	14+07.64	33.17	618.09	618.09
J	14+17.64	33.17	618.02	618.03
K	14+27.64	33.17	617.95	617.98
L	14+37.64	33.17	617.86	617.92
M	14+47.64	33.17	617.76	617.83
N	14+57.64	33.17	617.64	617.72
P	14+67.64	33.17	617.51	617.59
R	14+77.64	33.17	617.36	617.42
S	14+87.64	33.17	617.21	617.23
⊕ Brg. E. Abut.	14+94.10	33.17	617.10	617.10
Bk. E. Abut.	14+96.66	33.17	617.05	617.05

GIRDER 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+20.49	26.83	618.19	618.19
⊕ Brg. W. Abut.	13+23.05	26.83	618.21	618.21
A	13+33.05	26.83	618.26	618.30
B	13+43.05	26.83	618.30	618.36
C	13+53.05	26.83	618.32	618.40
D	13+63.05	26.83	618.33	618.42
E	13+73.05	26.83	618.33	618.40
F	13+83.05	26.83	618.32	618.36
G	13+93.05	26.83	618.29	618.31
H	14+03.05	26.83	618.24	618.25
⊕ Brg. Pier	14+09.51	26.83	618.21	618.21
J	14+19.51	26.83	618.14	618.15
K	14+29.51	26.83	618.06	618.10
L	14+39.51	26.83	617.97	618.03
M	14+49.51	26.83	617.87	617.94
N	14+59.51	26.83	617.75	617.83
P	14+69.51	26.83	617.61	617.69
R	14+79.51	26.83	617.47	617.52
S	14+89.51	26.83	617.31	617.33
⊕ Brg. E. Abut.	14+95.97	26.83	617.20	617.20
Bk. E. Abut.	14+98.53	26.83	617.15	617.15

GIRDER 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+22.37	20.50	618.34	618.34
⊕ Brg. W. Abut.	13+24.93	20.50	618.35	618.35
A	13+34.93	20.50	618.40	618.44
B	13+44.93	20.50	618.44	618.50
C	13+54.93	20.50	618.46	618.54
D	13+64.93	20.50	618.47	618.55
E	13+74.93	20.50	618.46	618.53
F	13+84.93	20.50	618.44	618.49
G	13+94.93	20.50	618.41	618.43
H	14+04.93	20.50	618.37	618.37
⊕ Brg. Pier	14+11.39	20.50	618.33	618.33
J	14+21.39	20.50	618.26	618.27
K	14+31.39	20.50	618.18	618.21
L	14+41.39	20.50	618.09	618.14
M	14+51.39	20.50	617.98	618.05
N	14+61.39	20.50	617.86	617.94
P	14+71.39	20.50	617.72	617.80
R	14+81.39	20.50	617.57	617.63
S	14+91.39	20.50	617.41	617.43
⊕ Brg. E. Abut.	14+97.85	20.50	617.30	617.30
Bk. E. Abut.	15+00.41	20.50	617.25	617.25

GIRDER 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+24.24	14.17	618.48	618.48
⊕ Brg. W. Abut.	13+26.80	14.17	618.49	618.49
A	13+36.80	14.17	618.54	618.58
B	13+46.80	14.17	618.57	618.64
C	13+56.80	14.17	618.59	618.67
D	13+66.80	14.17	618.60	618.68
E	13+76.80	14.17	618.59	618.66
F	13+86.80	14.17	618.57	618.62
G	13+96.80	14.17	618.54	618.56
H	14+06.80	14.17	618.49	618.49
⊕ Brg. Pier	14+13.26	14.17	618.45	618.45
J	14+23.26	14.17	618.38	618.39
K	14+33.26	14.17	618.30	618.33
L	14+43.26	14.17	618.20	618.25
M	14+53.26	14.17	618.09	618.16
N	14+63.26	14.17	617.96	618.05
P	14+73.26	14.17	617.83	617.90
R	14+83.26	14.17	617.67	617.73
S	14+93.26	14.17	617.51	617.53
⊕ Brg. E. Abut.	14+99.72	14.17	617.40	617.40
Bk. E. Abut.	15+02.28	14.17	617.35	617.35

GIRDER 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+26.12	7.83	618.60	618.60
⊕ Brg. W. Abut.	13+28.68	7.83	618.61	618.61
A	13+38.68	7.83	618.66	618.69
B	13+48.68	7.83	618.69	618.75
C	13+58.68	7.83	618.71	618.79
D	13+68.68	7.83	618.71	618.79
E	13+78.68	7.83	618.70	618.77
F	13+88.68	7.83	618.67	618.72
G	13+98.68	7.83	618.64	618.66
H	14+08.68	7.83	618.59	618.59
⊕ Brg. Pier	14+15.14	7.83	618.55	618.55
J	14+25.14	7.83	618.47	618.48
K	14+35.14	7.83	618.39	618.42
L	14+45.14	7.83	618.29	618.35
M	14+55.14	7.83	618.18	618.25
N	14+65.14	7.83	618.05	618.13
P	14+75.14	7.83	617.91	617.99
R	14+85.14	7.83	617.75	617.81
S	14+95.14	7.83	617.59	617.61
⊕ Brg. E. Abut.	15+01.60	7.83	617.47	617.47
Bk. E. Abut.	15+04.16	7.83	617.42	617.42

GIRDER 7

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+28.00	1.50	618.71	618.71
⊕ Brg. W. Abut.	13+30.56	1.50	618.72	618.72
A	13+40.56	1.50	618.76	618.79
B	13+50.56	1.50	618.79	618.85
C	13+60.56	1.50	618.81	618.87
D	13+70.56	1.50	618.81	618.87
E	13+80.56	1.50	618.79	618.85
F	13+90.56	1.50	618.77	618.81
G	14+00.56	1.50	618.73	618.75
H	14+10.56	1.50	618.68	618.68
⊕ Brg. Pier	14+17.02	1.50	618.63	618.63
J	14+27.02	1.50	618.56	618.57
K	14+37.02	1.50	618.47	618.50
L	14+47.02	1.50	618.37	618.41
M	14+57.02	1.50	618.25	618.31
N	14+67.02	1.50	618.12	618.19
P	14+77.02	1.50	617.98	618.04
R	14+87.02	1.50	617.82	617.87
S	14+97.02	1.50	617.65	617.67
⊕ Brg. E. Abut.	15+03.48	1.50	617.54	617.54
Bk. E. Abut.	15+06.04	1.50	617.49	617.49

PROFILE GRADE LINE 112TH PL.

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+28.44	0.00	618.74	618.74
⊕ Brg. W. Abut.	13+31.00	0.00	618.75	618.75
A	13+41.00	0.00	618.79	618.82
B	13+51.00	0.00	618.82	618.87
C	13+61.00	0.00	618.83	618.89
D	13+71.00	0.00	618.83	618.89
E	13+81.00	0.00	618.82	618.87
F	13+91.00	0.00	618.79	618.83
G	14+01.00	0.00	618.75	618.77
H	14+11.00	0.00	618.70	618.70
⊕ Brg. Pier	14+17.46	0.00	618.65	618.65
J	14+27.46	0.00	618.58	618.59
K	14+37.46	0.00	618.49	618.51
L	14+47.46	0.00	618.39	618.43
M	14+57.46	0.00	618.27	618.33
N	14+67.46	0.00	618.14	618.21
P	14+77.46	0.00	618.00	618.06
R	14+87.46	0.00	617.84	617.88
S	14+97.46	0.00	617.67	617.69
⊕ Brg. E. Abut.	15+03.92	0.00	617.55	617.55
Bk. E. Abut.	15+06.48	0.00	617.50	617.50

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GIRDER 8

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+28.88	-1.50	618.71	618.71
⊕ Brg. W. Abut.	13+31.44	-1.50	618.73	618.73
A	13+41.44	-1.50	618.77	618.80
B	13+51.44	-1.50	618.79	618.85
C	13+61.44	-1.50	618.81	618.87
D	13+71.44	-1.50	618.81	618.87
E	13+81.44	-1.50	618.79	618.85
F	13+91.44	-1.50	618.77	618.80
G	14+01.44	-1.50	618.72	618.74
H	14+11.44	-1.50	618.67	618.67
⊕ Brg. Pier	14+17.90	-1.50	618.63	618.63
J	14+27.90	-1.50	618.55	618.56
K	14+37.90	-1.50	618.46	618.49
L	14+47.90	-1.50	618.36	618.40
M	14+57.90	-1.50	618.24	618.30
N	14+67.90	-1.50	618.11	618.18
P	14+77.90	-1.50	617.97	618.03
R	14+87.90	-1.50	617.81	617.86
S	14+97.90	-1.50	617.64	617.66
⊕ Brg. E. Abut.	15+04.36	-1.50	617.52	617.52
Bk. E. Abut.	15+06.92	-1.50	617.47	617.47

GIRDER 9

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+30.76	-7.83	618.62	618.62
⊕ Brg. W. Abut.	13+33.32	-7.83	618.64	618.64
A	13+43.32	-7.83	618.67	618.71
B	13+53.32	-7.83	618.70	618.76
C	13+63.32	-7.83	618.71	618.79
D	13+73.32	-7.83	618.71	618.79
E	13+83.32	-7.83	618.69	618.76
F	13+93.32	-7.83	618.66	618.71
G	14+03.32	-7.83	618.62	618.64
H	14+13.32	-7.83	618.56	618.56
⊕ Brg. Pier	14+19.78	-7.83	618.52	618.52
J	14+29.78	-7.83	618.44	618.45
K	14+39.78	-7.83	618.34	618.38
L	14+49.78	-7.83	618.24	618.30
M	14+59.78	-7.83	618.12	618.19
N	14+69.78	-7.83	617.98	618.07
P	14+79.78	-7.83	617.84	617.92
R	14+89.78	-7.83	617.68	617.74
S	14+99.78	-7.83	617.50	617.53
⊕ Brg. E. Abut.	15+06.24	-7.83	617.39	617.39
Bk. E. Abut.	15+08.80	-7.83	617.34	617.34

GIRDER 10

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+32.64	-14.17	618.52	618.52
⊕ Brg. W. Abut.	13+35.20	-14.17	618.53	618.53
A	13+45.20	-14.17	618.57	618.61
B	13+55.20	-14.17	618.59	618.66
C	13+65.20	-14.17	618.60	618.68
D	13+75.20	-14.17	618.59	618.68
E	13+85.20	-14.17	618.57	618.65
F	13+95.20	-14.17	618.54	618.59
G	14+05.20	-14.17	618.50	618.52
H	14+15.20	-14.17	618.44	618.44
⊕ Brg. Pier	14+21.66	-14.17	618.39	618.39
J	14+31.66	-14.17	618.31	618.32
K	14+41.66	-14.17	618.21	618.25
L	14+51.66	-14.17	618.11	618.16
M	14+61.66	-14.17	617.98	618.06
N	14+71.66	-14.17	617.85	617.93
P	14+81.66	-14.17	617.70	617.78
R	14+91.66	-14.17	617.54	617.59
S	15+01.66	-14.17	617.36	617.38
⊕ Brg. E. Abut.	15+08.12	-14.17	617.24	617.24
Bk. E. Abut.	15+10.68	-14.17	617.19	617.19

GIRDER 11

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+34.51	-20.50	618.40	618.40
⊕ Brg. W. Abut.	13+37.07	-20.50	618.41	618.41
A	13+47.07	-20.50	618.44	618.48
B	13+57.07	-20.50	618.46	618.53
C	13+67.07	-20.50	618.47	618.55
D	13+77.07	-20.50	618.46	618.54
E	13+87.07	-20.50	618.44	618.51
F	13+97.07	-20.50	618.40	618.45
G	14+07.07	-20.50	618.35	618.38
H	14+17.07	-20.50	618.29	618.30
⊕ Brg. Pier	14+23.53	-20.50	618.25	618.25
J	14+33.53	-20.50	618.16	618.17
K	14+43.53	-20.50	618.06	618.09
L	14+53.53	-20.50	617.95	618.01
M	14+63.53	-20.50	617.83	617.90
N	14+73.53	-20.50	617.69	617.77
P	14+83.53	-20.50	617.54	617.62
R	14+93.53	-20.50	617.37	617.43
S	15+03.53	-20.50	617.19	617.22
⊕ Brg. E. Abut.	15+09.99	-20.50	617.07	617.07
Bk. E. Abut.	15+12.55	-20.50	617.02	617.02

GIRDER 12

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+36.39	-26.83	618.27	618.27
⊕ Brg. W. Abut.	13+38.95	-26.83	618.28	618.28
A	13+48.95	-26.83	618.31	618.35
B	13+58.95	-26.83	618.33	618.40
C	13+68.95	-26.83	618.33	618.42
D	13+78.95	-26.83	618.32	618.41
E	13+88.95	-26.83	618.30	618.37
F	13+98.95	-26.83	618.26	618.31
G	14+08.95	-26.83	618.21	618.23
H	14+18.95	-26.83	618.15	618.15
⊕ Brg. Pier	14+25.41	-26.83	618.10	618.10
J	14+35.41	-26.83	618.01	618.02
K	14+45.41	-26.83	617.91	617.94
L	14+55.41	-26.83	617.80	617.86
M	14+65.41	-26.83	617.67	617.75
N	14+75.41	-26.83	617.53	617.61
P	14+85.41	-26.83	617.38	617.45
R	14+95.41	-26.83	617.21	617.27
S	15+05.41	-26.83	617.03	617.05
⊕ Brg. E. Abut.	15+11.87	-26.83	616.90	616.90
Bk. E. Abut.	15+14.43	-26.83	616.85	616.85

GIRDER 13

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+38.26	-33.17	618.15	618.15
⊕ Brg. W. Abut.	13+40.82	-33.17	618.16	618.16
A	13+50.82	-33.17	618.19	618.22
B	13+60.82	-33.17	618.20	618.27
C	13+70.82	-33.17	618.20	618.28
D	13+80.82	-33.17	618.19	618.27
E	13+90.82	-33.17	618.16	618.23
F	14+00.82	-33.17	618.12	618.17
G	14+10.82	-33.17	618.07	618.09
H	14+20.82	-33.17	618.00	618.01
⊕ Brg. Pier	14+27.28	-33.17	617.95	617.95
J	14+37.28	-33.17	617.86	617.87
K	14+47.28	-33.17	617.76	617.79
L	14+57.28	-33.17	617.64	617.70
M	14+67.28	-33.17	617.51	617.59
N	14+77.28	-33.17	617.37	617.46
P	14+87.28	-33.17	617.21	617.29
R	14+97.28	-33.17	617.04	617.10
S	15+07.28	-33.17	616.86	616.88
⊕ Brg. E. Abut.	15+13.74	-33.17	616.73	616.73
Bk. E. Abut.	15+16.30	-33.17	616.68	616.68

NORTH LONGITUDINAL JOINT

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+38.93	-35.42	618.11	618.11
⊕ Brg. W. Abut.	13+41.49	-35.42	618.11	618.11
A	13+51.49	-35.42	618.14	618.18
B	13+61.49	-35.42	618.15	618.22
C	13+71.49	-35.42	618.15	618.24
D	13+81.49	-35.42	618.14	618.23
E	13+91.49	-35.42	618.11	618.19
F	14+01.49	-35.42	618.07	618.12
G	14+11.49	-35.42	618.02	618.04
H	14+21.49	-35.42	617.95	617.95
⊕ Brg. Pier	14+27.95	-35.42	617.90	617.90
J	14+37.95	-35.42	617.81	617.82
K	14+47.95	-35.42	617.71	617.74
L	14+57.95	-35.42	617.59	617.65
M	14+67.95	-35.42	617.46	617.54
N	14+77.95	-35.42	617.31	617.40
P	14+87.95	-35.42	617.16	617.24
R	14+97.95	-35.42	616.98	617.04
S	15+07.95	-35.42	616.80	616.83
⊕ Brg. E. Abut.	15+14.41	-35.42	616.67	616.67
Bk. E. Abut.	15+16.97	-35.42	616.62	616.62

GIRDER 14

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted for Dead Load Deflections
Bk. W. Abut.	13+40.14	-39.50	618.03	618.03
⊕ Brg. W. Abut.	13+42.70	-39.50	618.03	618.03
A	13+52.70	-39.50	618.06	618.10
B	13+62.70	-39.50	618.07	618.14
C	13+72.70	-39.50	618.07	618.16
D	13+82.70	-39.50	618.05	618.15
E	13+92.70	-39.50	618.02	618.10
F	14+02.70	-39.50	617.98	618.04
G	14+12.70	-39.50	617.93	617.95
H	14+22.70	-39.50	617.86	617.86
⊕ Brg. Pier	14+29.16	-39.50	617.80	617.80
J	14+39.16	-39.50	617.71	617.72
K	14+49.16	-39.50	617.61	617.64
L	14+59.16	-39.50	617.49	617.55
M	14+69.16	-39.50	617.36	617.44
N	14+79.16	-39.50	617.21	617.31
P	14+89.16	-39.50	617.05	617.14
R	14+99.16	-39.50	616.88	616.94
S	15+09.16	-39.50	616.69	616.72
⊕ Brg. E. Abut.	15+15.62	-39.50	616.56	616.56
Bk. E. Abut.	15+18.18	-39.50	616.51	616.51

Note 2

- Notes:**
1. Work this sheet with Sheet S-003.
 2. Contractor shall set the bottom form to provide a minimum fillet height of 1/2" above the top of beam.

SHT. S-005	
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 112TH PLACE BRIDGE DECK REPLACEMENT
 112th PLACE OVER I-57
 SN 016-0988 STA. 546+15.56
 COOK COUNTY

TOP OF DECK ELEVATIONS II

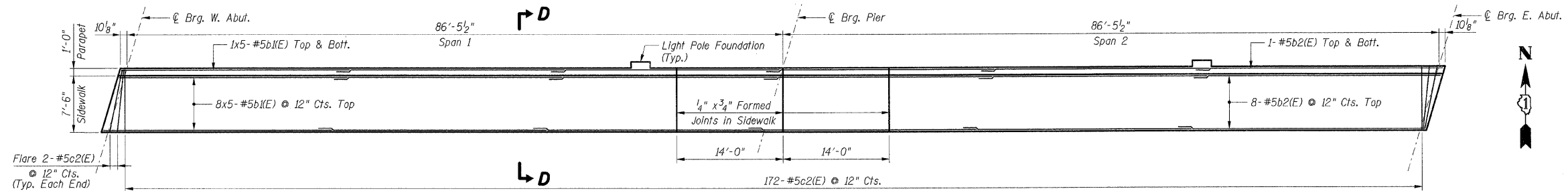
DATE: 01/16/09

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 CHECKED BY: JLR

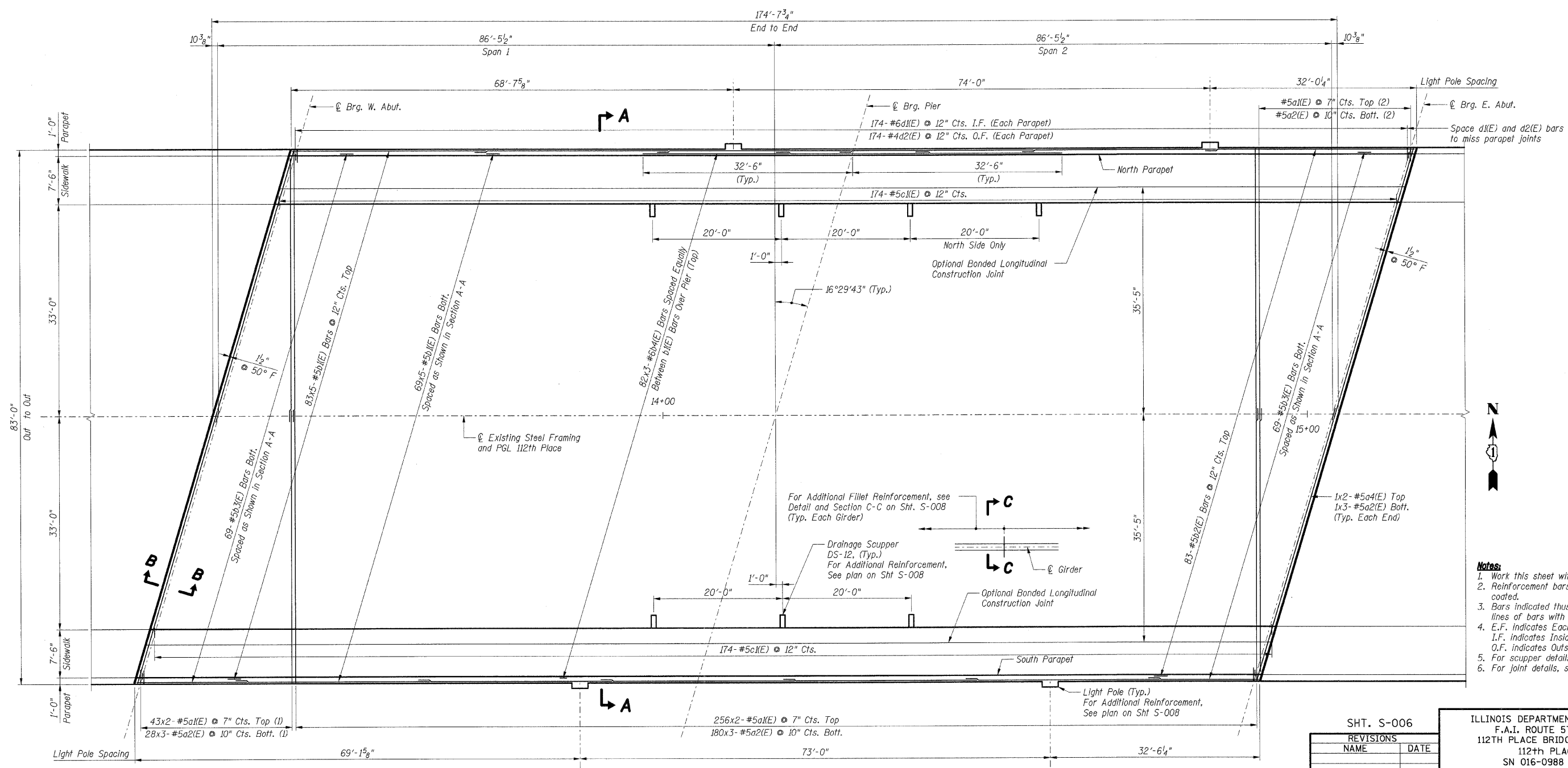
TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.616.0000

I:\BRIDGE\DCN\1\MECON\5.DGN RITCHIE
 16-000_160626
 color table: K:\STANDARD\TENG\DATA\COLORTABLE.TBL
 color table: N/A

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	072
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		
CONTRACT NO. 62533				



SIDEWALK PLAN
(North Sidewalk Shown - South Sidewalk Opposite Hand)



DECK PLAN

- Notes:**
1. Work this sheet with Shts. S-007 & S-008.
 2. Reinforcement bars designated (E) shall be epoxy coated.
 3. Bars indicated thus 30 x 2-#5 etc. indicates 30 lines of bars with 2 lengths per line.
 4. E.F. indicates Each Face
I.F. indicates Inside Face
O.F. indicates Outside Face
 5. For scupper details, see Sht. S-010.
 6. For joint details, see Sht. S-009.

LAP LENGTHS	
Bar Size	Min. Lap
#5	1'-8"
#6	2'-0"

Reinforcing Notes:
(1) Cut bars to fit in field and use remainder in opposite end of deck.
(2) Use remainder of bars from opposite end of deck.

SHT. S-006 REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
F.A.I. ROUTE 57 (INTERSTATE 57)
112TH PLACE BRIDGE DECK REPLACEMENT
112th PLACE OVER I-57
SN 016-0988 STA. 546+15.56
COOK COUNTY

DECK PLAN

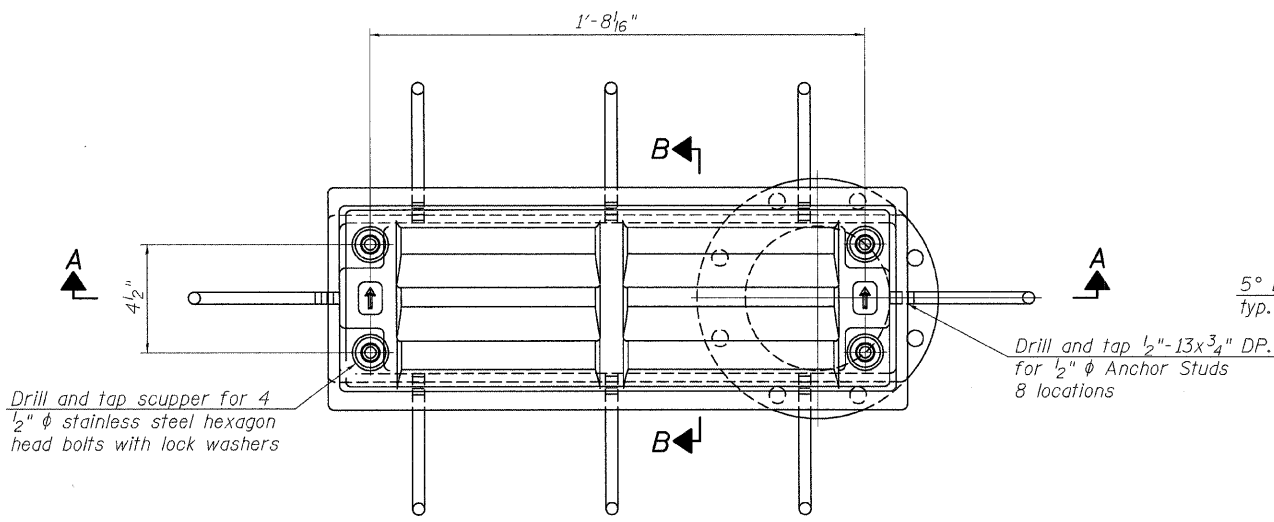
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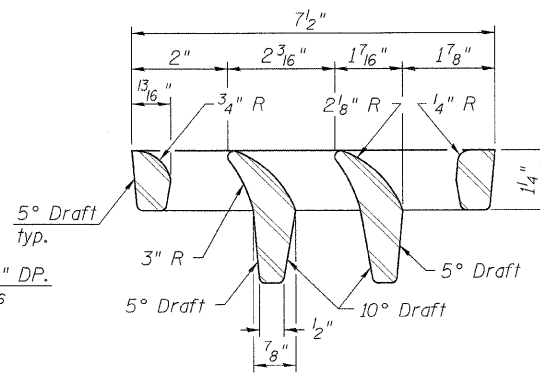
TENG
TENG & ASSOCIATES, INC.
ENGINEERS/ARCHITECTS/PLANNERS
205 N. MICHIGAN AVE., CHICAGO, IL 60601
TELEPHONE: 312.585.0000

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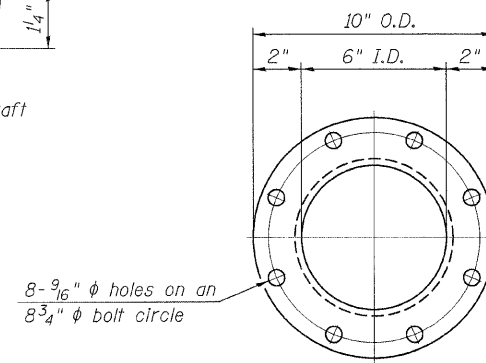
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
57	1920.1B-R	COOK	101	076
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT
CONTRACT NO. 62533				



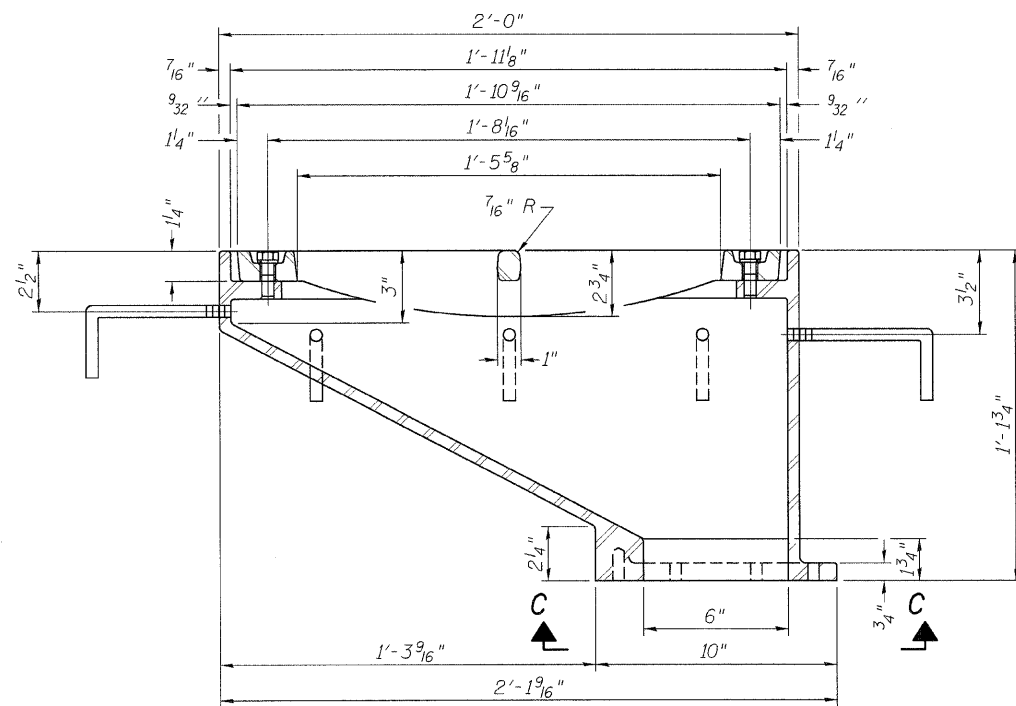
PLAN



VANE GRATE DETAIL

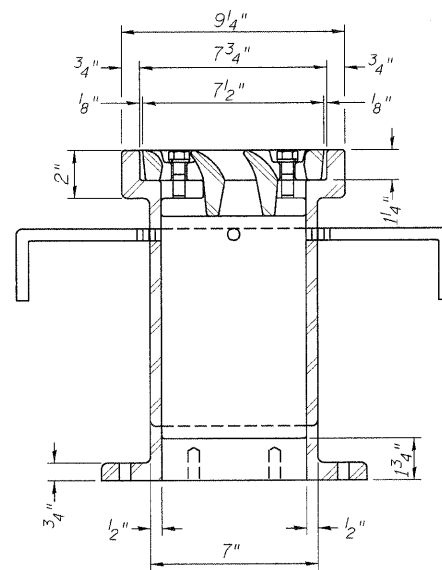


VIEW C-C



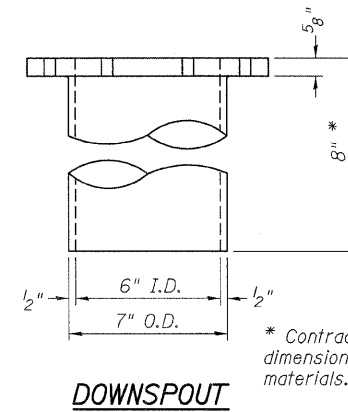
SECTION A-A

See sheet of for scupper location relative to parapet.

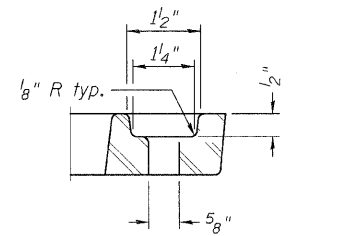


SECTION B-B

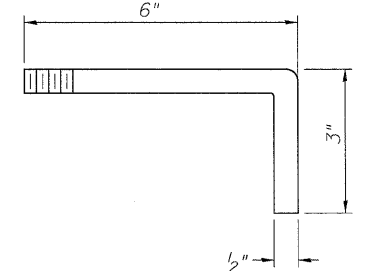
Drill and tap 8 holes for 1/2"-13 bolts on an 8 3/4" φ bolt circle. (2 blind holes are 1/4" deep, 6 thru holes)



DOWNSPOUT



BOLT HOLE DETAIL



ANCHOR STUD DETAIL

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper, DS-12	Each	7

SHT. S-010 REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 112TH PLACE BRIDGE DECK REPLACEMENT
 112TH PLACE OVER I-57
 SN 016-0988 STA. 546+15.56
 COOK COUNTY

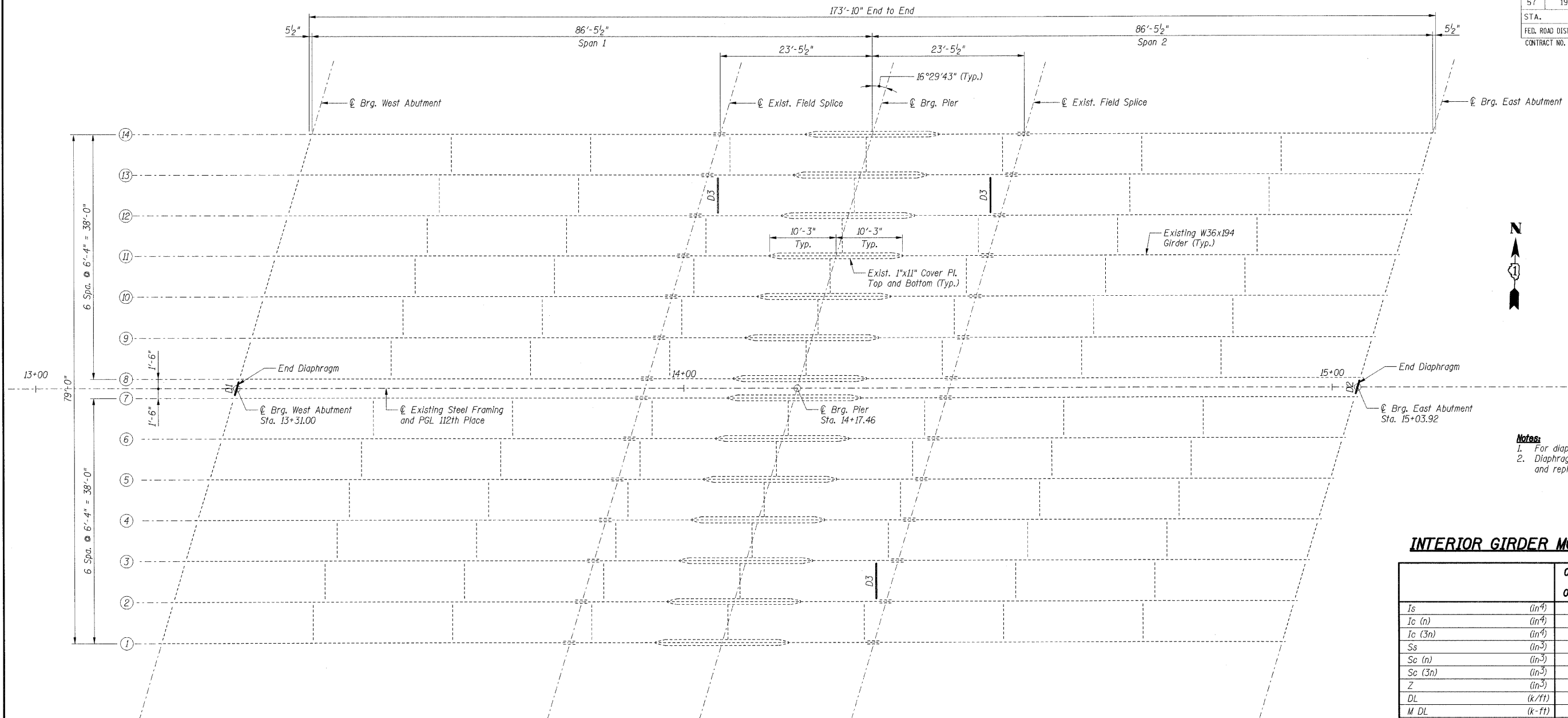
DRAINAGE SCUPPER
 DS-12

DATE: 01/16/09

DRAWN BY: TMH
 CHECKED BY: JLR

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.650.0000

T:\DOCUMENTS\950666\STRUCT\CON\SCDOON\C.DWG HOUGHTON KA STANDARD\TENG\DATA\COLOR\TABLE.TBL
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 PLOT: 10/1-08



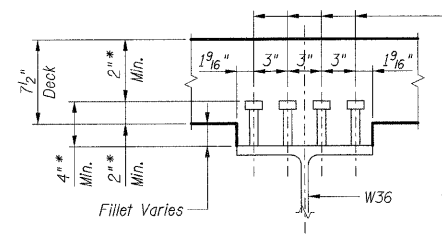
- Notes:**
- For diaphragm details see Sheet S-014.
 - Diaphragms labeled D3 are to be removed and replaced as shown on Sheet S-014.

INTERIOR GIRDER MOMENT TABLE

	0.4 Span 1 or 0.6 Span 2	Pier	
I_s	(in ⁴)	12100	19830
I_c (n)	(in ⁴)	28065	
I_c (3n)	(in ⁴)	20202	
S_s	(in ³)	664	1030
S_c (n)	(in ³)	923	
S_c (3n)	(in ³)	825	
Z	(in ³)		
DL	(k/ft)	0.846	1.350
M DL	(k-ft)	416	1261
sDL	(k/ft)	0.429	
M sDL	(k-ft)	230	
M LL	(k-ft)	613	457
M (Imp)	(k-ft)	145	108
5/3 [M LL + M (Imp)]	(k-ft)	1263	943
M_a	(k-ft)	2481	2865
M_u	(k-ft)	2809	
f_s DL (non-comp)	(ksi)	7.5	14.7
f_s DL (comp)	(ksi)	3.3	
f_s 5/3 [M LL + M (Imp)]	(ksi)	16.4	11.0
f_s (Overload)	(ksi)	27.3	25.7
f_s (Total)	(ksi)		33.4
VR	(k)	54.0	

INTERIOR GIRDER REACTION TABLE

	Abutments	Pier	
R DL	(k)	40.8	140.0
R LL	(k)	40.4	53.9
Imp.	(k)	9.6	12.7
R (Total)	(k)	90.8	206.6



EXISTING SHEAR CONNECTOR DETAIL

* Stud heights and vertical clearances apply to replacement studs (if required).

Existing 7/8" ϕ stud shear connectors to remain. (total number of existing studs = 7168)
 Shear connectors damaged during deck removal shall be replaced by the Contractor at the Contractor's expense with 7/8" ϕ granular or solid flux filled studs automatically end welded to flange.

I_s and S_s are the moment of inertia and section modulus of the steel section used in computing f_s (Total and Overload).
 I_c and S_c are the moment of inertia and section modulus of the composite section used in computing f_s (Total and Overload).
 VR is the maximum Live Load + Impact shear range within the composite portion of the span.
 Z is the plastic section modulus used to determine the fully plastic moments in the non-composite areas.
 M_a (Applied Moment) = 1.3 [M DL + M sDL + 5/3 (M LL + M (Imp))].
 M_u is the maximum bending strength. (compact, braced section)
 f_s (Overload) is the sum of the stresses due to M DL + M sDL + 5/3 (M LL + M (Imp)).
 f_s (Total) is the sum of the stresses due to 1.3 [M DL + M sDL + 5/3 (M LL + M (Imp))]. (non-compact section)

SHT. S-013

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 112TH PLACE BRIDGE DECK REPLACEMENT
 112TH PLACE OVER I-57
 SN 016-0988 STA. 546+15.56
 COOK COUNTY

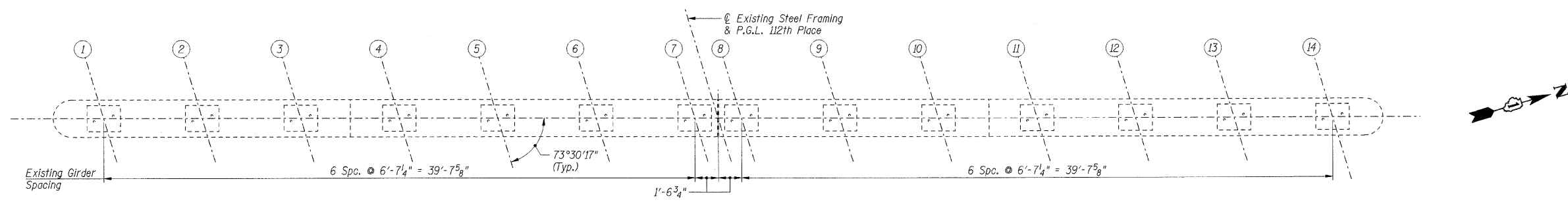
STEEL FRAMING PLAN

DATE: 01/16/09

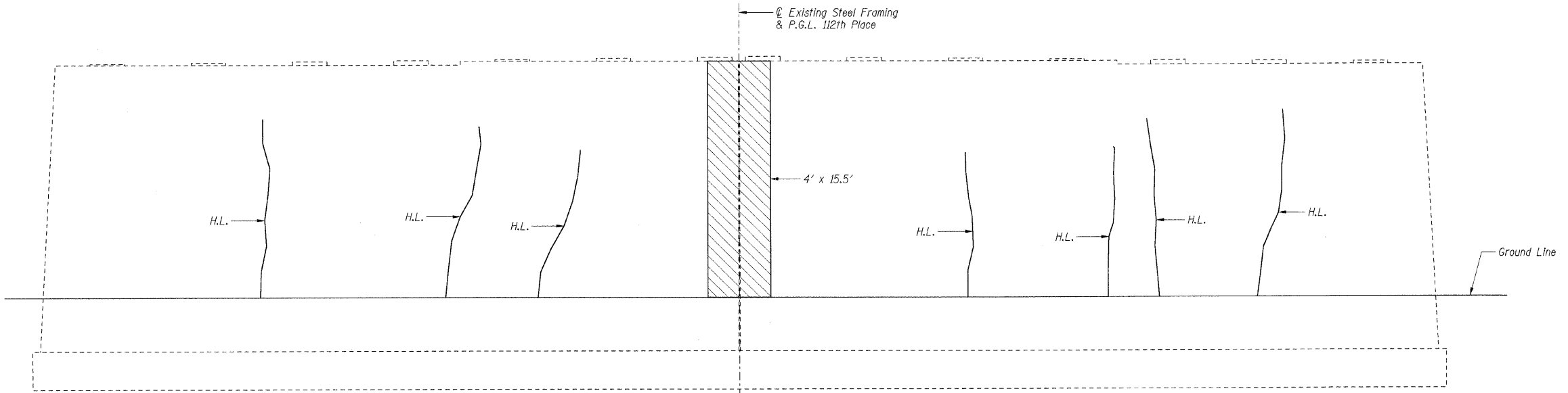
DRAWN BY: CCE
 CHECKED BY: JLR

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS ARCHITECTS PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.616.0000

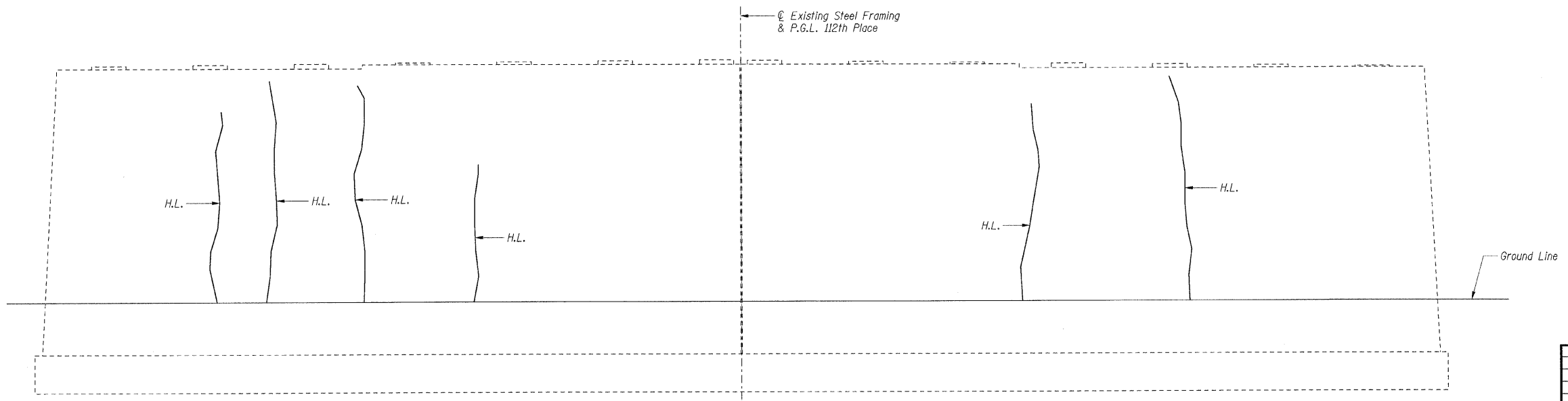
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TOP PLAN



ELEVATION
(Looking East)



ELEVATION
(Looking West)

- Notes:**
- Quantities indicated with a double asterisk (***) are estimated quantities to be determined by the Engineer in the field. Adjustment to the quantities shall not be cause for additional compensation; however the Contractor will be paid for the quantity of work performed at the unit price bid for the work.
 - Girders 7 and 8 shall be supported during concrete repairs in accordance with the Special Provision for Temporary Shoring and Cribbing.

- LEGEND**
- Structural Repair of Concrete (Depth Equal to or Less Than 5")
 - H.L. → Hairline Crack - Not to be Sealed

BILL OF MATERIAL		
Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less Than 5")**	Sq Ft.	96

SHT. S-017

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 112TH PLACE BRIDGE DECK REPLACEMENT
 112TH PLACE OVER I-57
 SN 016-0988 STA. 546+15.56
 COOK COUNTY

PIER REPAIRS

DATE: 01/16/09

DRAWN BY: TMH
 CHECKED BY: JLR

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 205 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.682.0000

\\GPECONE4.DGN, \\BRBORDER.DGN, \\FRP0005.DGN, \\AB100L-INDEN, \\ABTDONB6.DGN, \\HOLGRTM
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The diameter of this part is equal or larger than the diameter of bar spliced.

The diameter of this part is the same as the diameter of the bar spliced.

ROLLED THREAD DOWEL BAR



**** ONE PIECE**

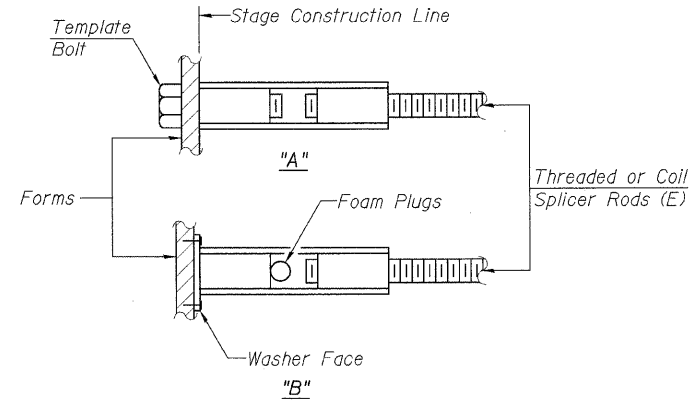
Wire Connector



WELDED SECTIONS

BAR SPLICER ASSEMBLY ALTERNATIVES

**Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.

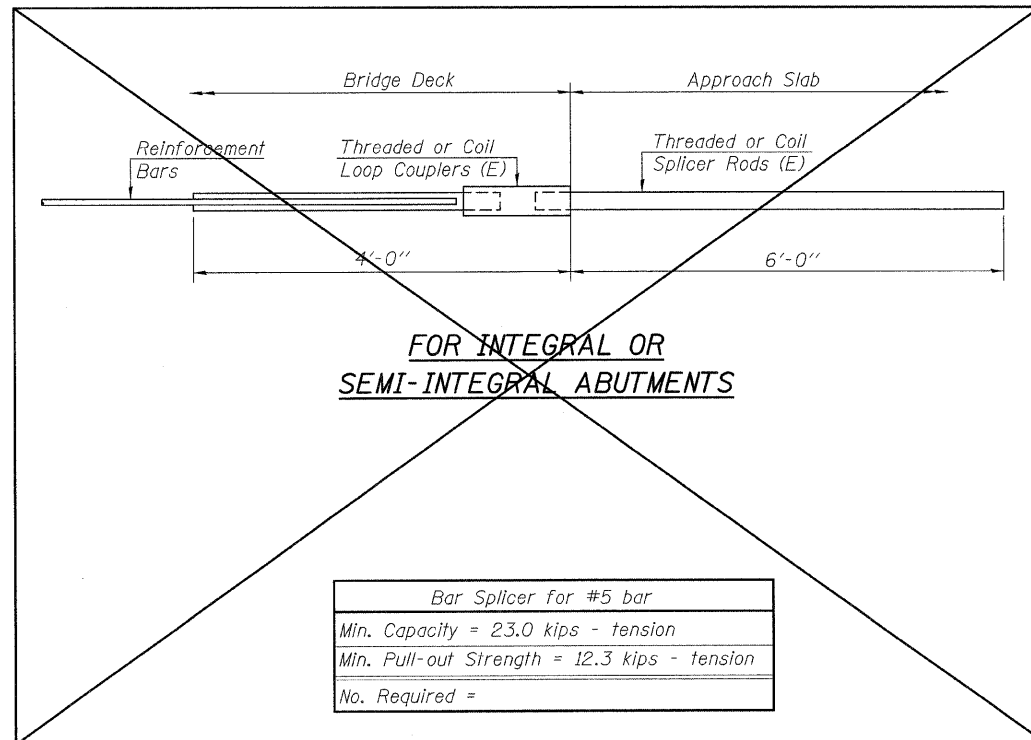
NOTES

Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.
 Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.
 All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.
 Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

- ① Minimum Capacity = $1.25 \times f_y \times A_f$
(Tension in kips)
- ② Minimum *Pull-out Strength = $0.66 \times f_y \times A_f$
(Tension in kips)

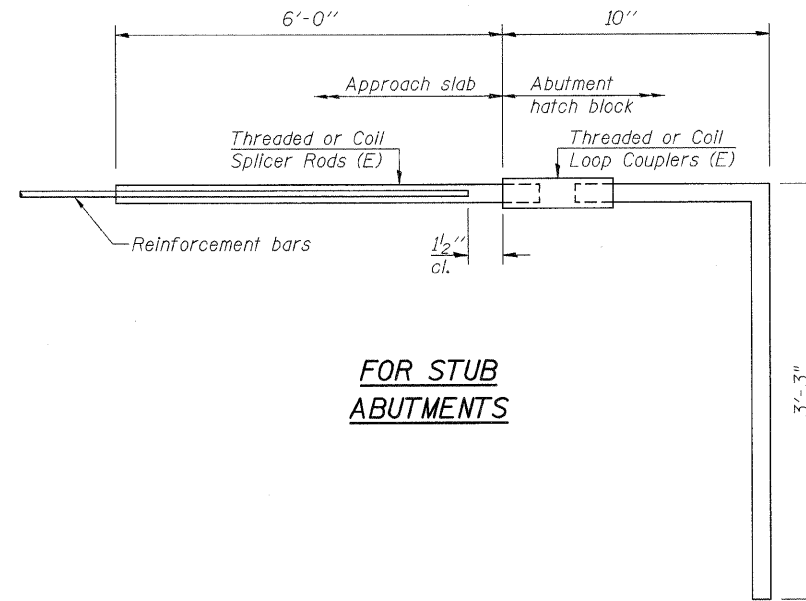
Where f_y = Yield strength of lapped reinforcement bars in ksi.
 A_f = Tensile stress area of lapped reinforcement bars.
 * = 28 day concrete

BAR SPLICER ASSEMBLIES			
Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



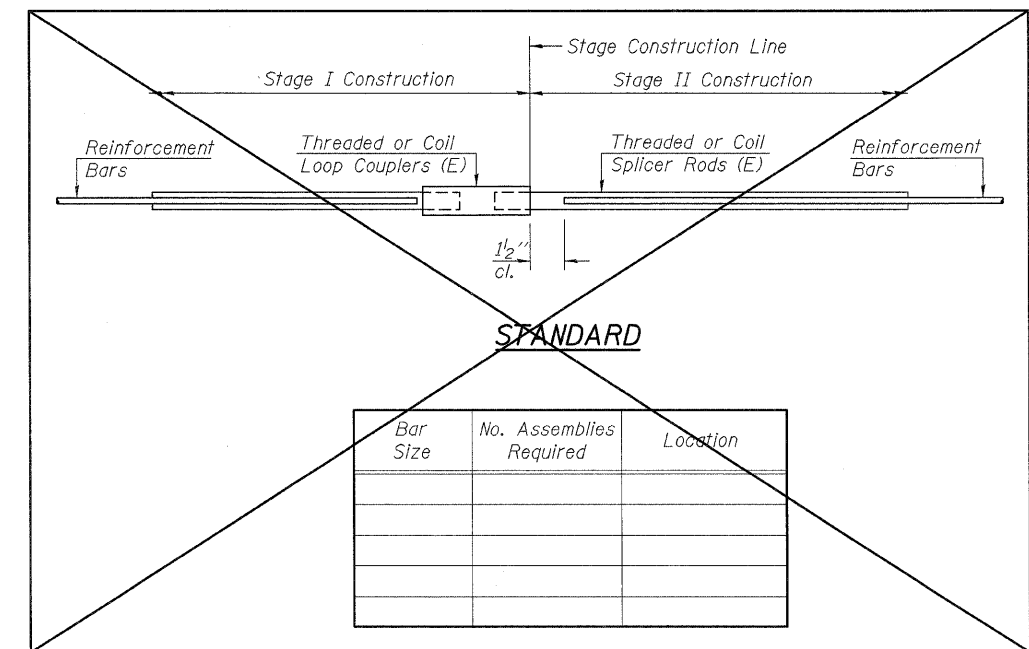
FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



FOR STUB ABUTMENTS

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required = 134



STANDARD

Bar Size	No. Assemblies Required	Location

SHT. S-022	
REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
 F.A.I. ROUTE 57 (INTERSTATE 57)
 112TH PLACE BRIDGE DECK REPLACEMENT
 112TH PLACE OVER I-57
 SN 016-0988 STA. 546+15.56
 COOK COUNTY

BAR SPLICER ASSEMBLY DETAILS

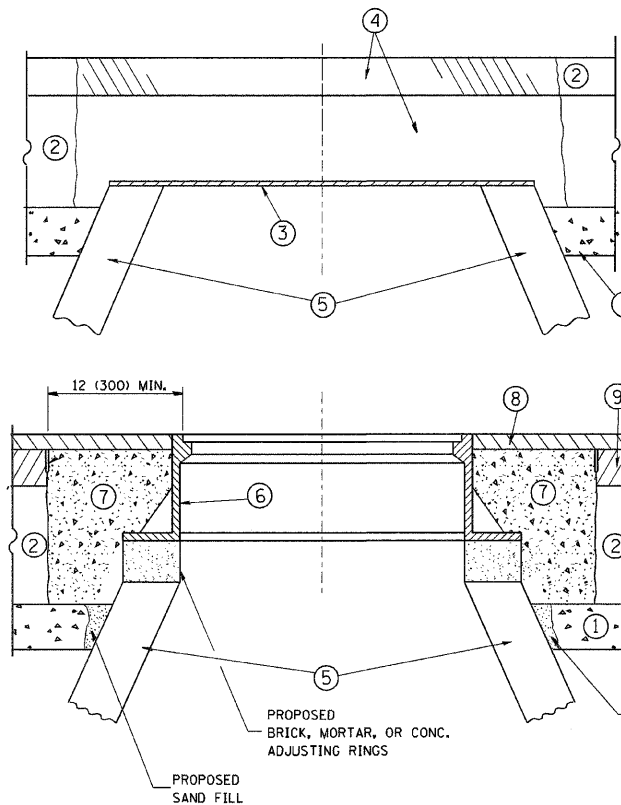
DRAWN BY: TMH
 CHECKED BY: JLR

DATE: 01/16/09

TENG
 TENG & ASSOCIATES, INC.
 ENGINEERS/ARCHITECTS/PLANNERS
 225 N. MICHIGAN AVE., CHICAGO, IL 60601
 TELEPHONE: 312.616.0000

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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	089
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

REVISIONS	
NAME	DATE
R. SHAH	10/25/94
R. SHAH	01/30/95
R. SHAH	03/10/95
A. ABBAS	03/21/97
R. WIEDEMAN	05/14/04
R. BORO	01/01/07

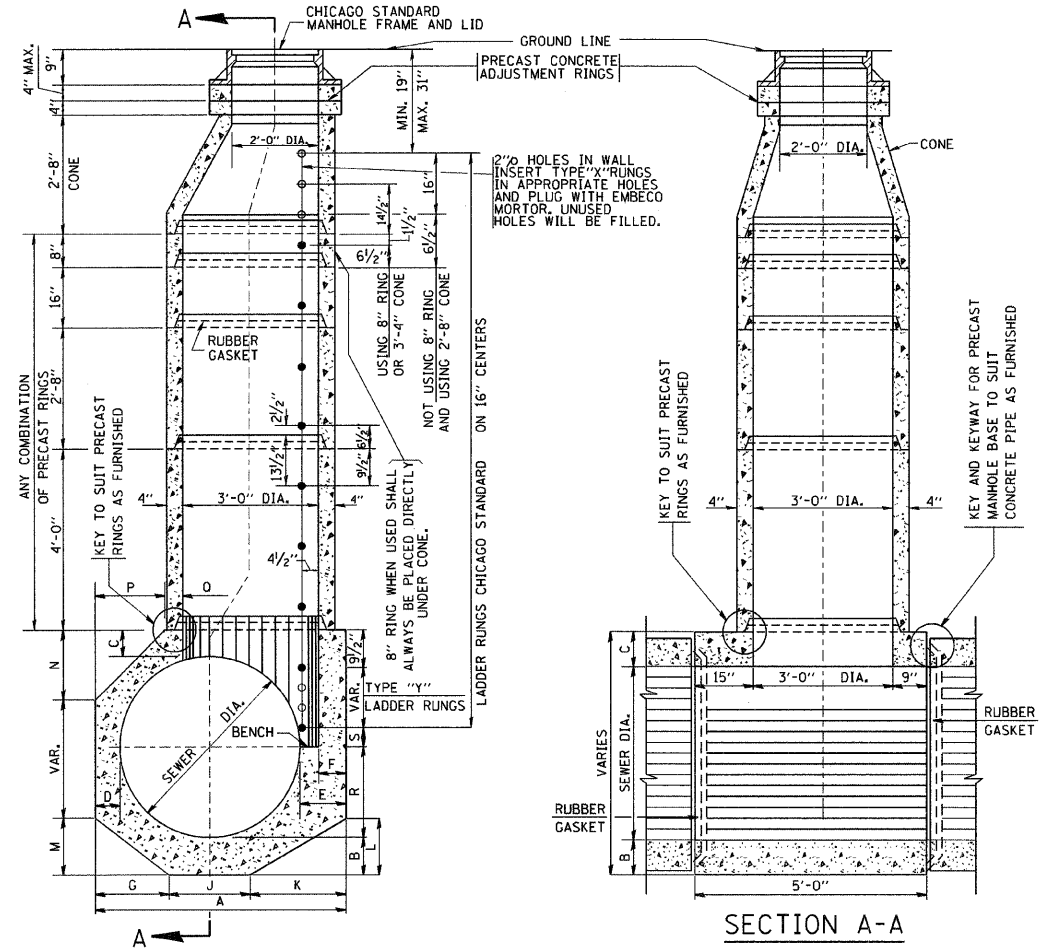
ILLINOIS DEPARTMENT OF TRANSPORTATION
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: VERT. NONE
HORIZ.

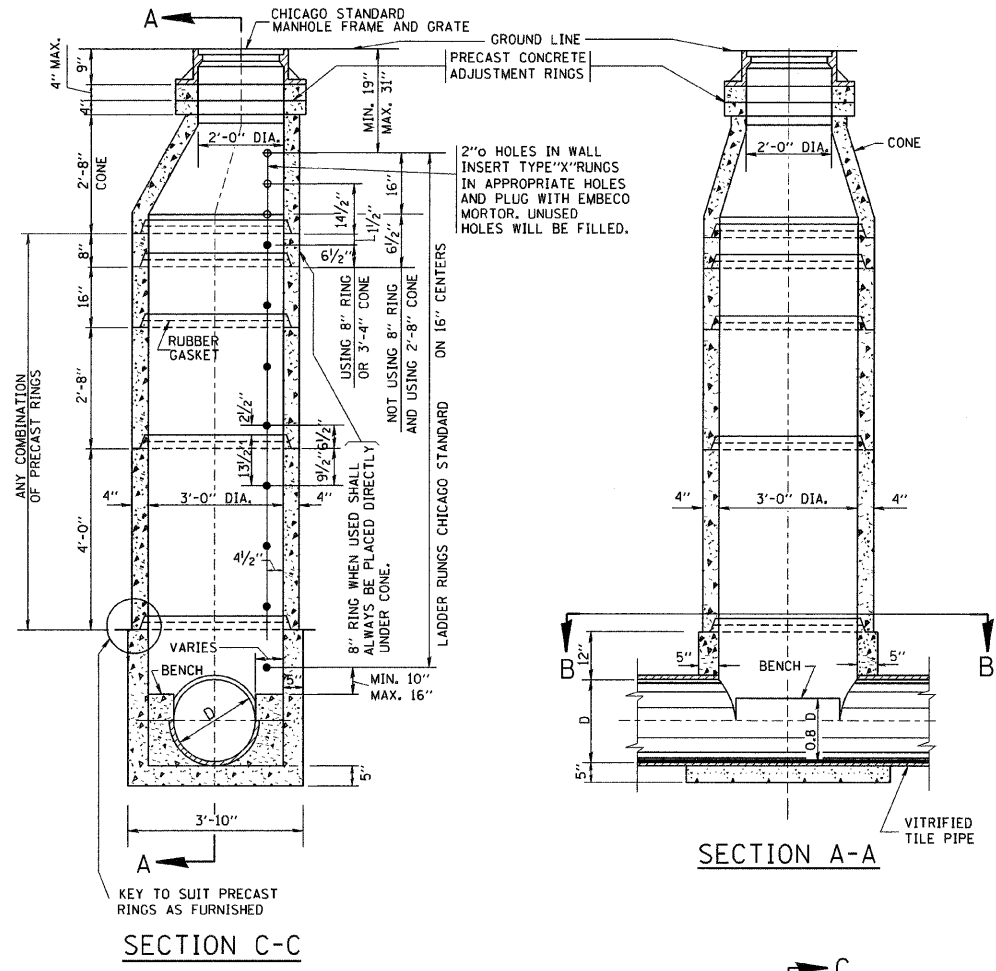
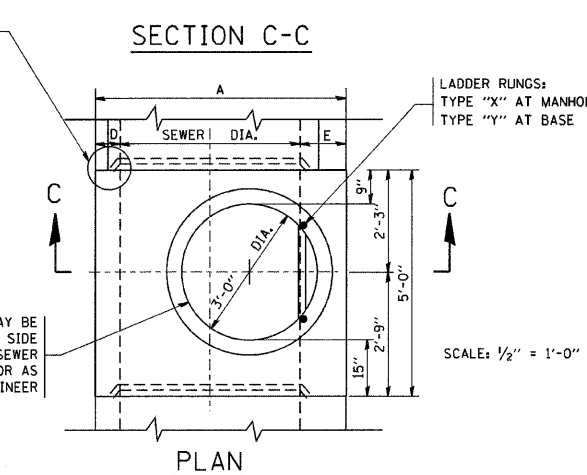
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CHECKED BY

BD600-03 (BD-8)

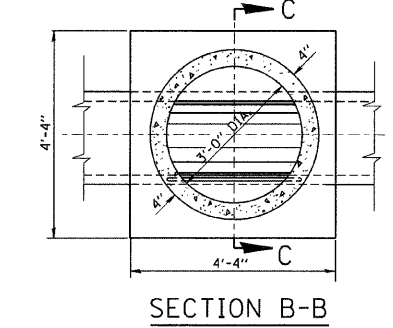
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	090
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



TYPE "A" MANHOLE
FOR SEWERS
24" TO 120" DIAMETER
PRECAST BASES AND RINGS



TYPE "A" MANHOLE
FOR SEWERS
21" DIAMETER AND SMALLER
PRECAST BASES AND RINGS



SEWER DIA.	PART OF ITEM	DIMENSIONS OF PRECAST MANHOLE BASE																NO. "Y" RINGS
		A	B	C	D	E	F	G	J	K	L	M	N	P	O	R		
120"	----	12"-4 1/2"	12"	12"	12"	16 1/2"	12"	4'-0"	4'-0"	4'-4 1/2"	2'-7 1/2"	2'-5"	3'-7"	3'-7"	4'-8 1/2"	2'-0"	2 1/2"	7
108"	----	11"-4 1/2"	12"	12"	12"	16 1/2"	12"	3'-8"	3'-8"	4'-0 1/2"	2'-5"	2'-2"	3'-4"	3'-4"	4'-0 1/2"	2'-0"	6 1/2"	6
102"	----	10"-10 1/2"	12"	12"	12"	16 1/2"	12"	3'-6"	3'-6"	3'-10 1/2"	2'-4"	2'-1"	3'-2"	3'-2"	3'-8 1/2"	2'-0"	16 1/2"	5
96"	10-A	10"-2 1/2"	11"	11"	11"	15 1/2"	11"	3'-3"	3'-3"	3'-8 1/2"	2'-3"	2'-3"	2'-11"	2'-11"	3'-4 1/2"	2'-0"	9 1/2"	5
90"	10-B	9'-8 1/2"	11"	11"	11"	15 1/2"	11"	3'-1"	3'-1"	3'-6 1/2"	2'-1 1/2"	22"	2'-10"	2'-10"	2'-10 1/2"	2'-0"	3 1/2"	5
84"	10-C	9'-0 1/2"	10"	10"	10"	14 1/2"	10"	2'-11"	2'-11"	3'-2 1/2"	23"	21"	2'-7"	2'-7"	2'-7 1/2"	2'-0"	12 1/2"	4
78"	10-D	8'-6 1/2"	10"	10"	10"	14 1/2"	10"	2'-9"	2'-9"	3'-0 1/2"	22"	20"	2'-6"	2'-6"	2'-2 1/2"	2'-0"	6 1/2"	4
72"	10	7'-10 1/2"	9"	9"	9"	13 1/2"	9"	2'-6"	2'-6"	2'-10 1/2"	21"	18"	2'-3"	2'-3"	22 1/2"	2'-0"	15 1/2"	3
66"	11	7'-4 1/2"	9"	9"	9"	13 1/2"	9"	2'-4"	2'-4"	2'-8 1/2"	19 1/2"	17"	2'-1"	2'-1"	18 1/2"	2'-0"	9 1/2"	3
60"	12	6'-8 1/2"	8"	8"	8"	12 1/2"	8"	2'-1 1/2"	2'-1"	2'-6"	18"	15"	23"	23"	13 1/2"	2'-0"	2 1/2"	3
54"	13	6'-2 1/2"	8"	8"	8"	12 1/2"	8"	23 1/2"	23"	2'-4"	17"	14"	21"	21"	9 1/2"	2'-0"	12 1/2"	2
48"	14	5'-6 1/2"	7"	7"	7"	11 1/2"	7"	20 1/2"	21"	2'-1"	15"	12 1/2"	18 1/2"	18 1/2"	5"	2'-0"	5 1/2"	2
42"	15	5'-0 1/2"	7"	7"	7"	11 1/2"	7"	18 1/2"	19"	23"	14"	11"	---	---	17 1/2"	21"	2 1/2"	2
36"	16	4'-4 1/2"	6"	6"	6"	10 1/2"	6"	16"	16"	20 1/2"	12 1/2"	9 1/2"	---	---	10 1/2"	18"	14 1/2"	1
30"	17	4'-0"	6"	6"	6"	10 1/2"	6"	14"	14"	20"	12"	8 1/2"	---	---	6"	15"	11 1/2"	1
24"	18	4'-0"	6"	6"	6"	12"	6"	16"	16"	16"	9 1/2"	9 1/2"	---	---	6"	12"	8 1/2"	1

FOR STATE CONTRACT
ALL DIMENSIONS SHOULD
BE PREPARED IN METRIC
UNITS SOFT CONVERSION
METHOD SHOULD BE USED.

REVISIONS	
NAME	DATE
	6-18-82
	9-22-90

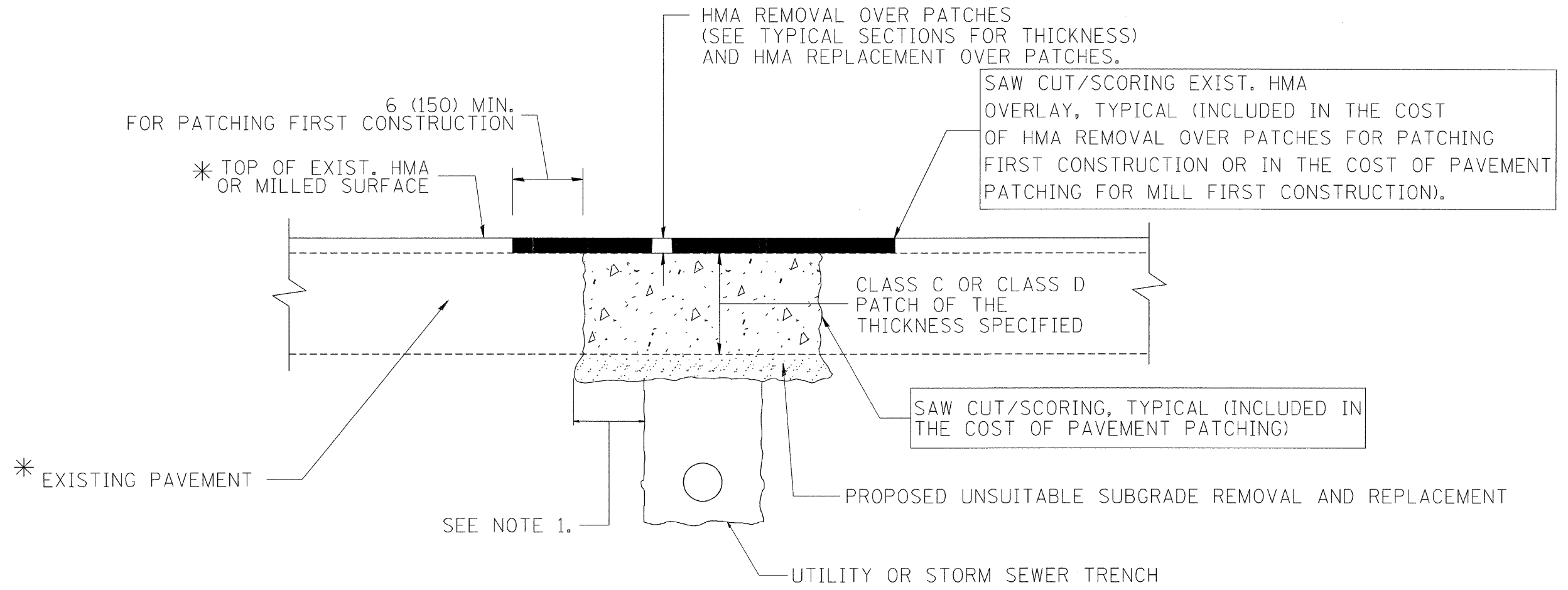
ILLINOIS DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
DRAINAGE
DETAILS

SCALE: VERT. NONE
HORIZ.

DRAWN BY
CHECKED BY

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	091
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

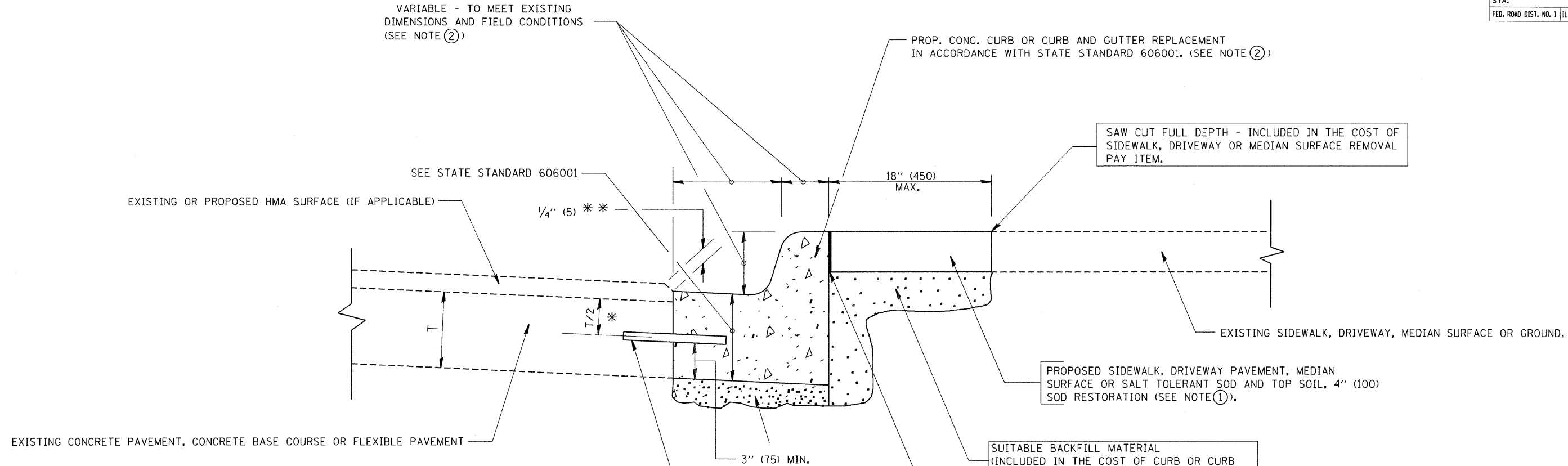
1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
R. SHAH	01/14/95
R. SHAH	03/23/95
R. SHAH	04/24/95
A. HOUSEH	03/15/96
A. ABBAS	03/21/97
A. ABBAS	01/20/98
ART ABBAS	04/27/98
R. BORO	01/01/07
R. BORO	09/04/07
K. ENG	10/27/08

ILLINOIS DEPARTMENT OF TRANSPORTATION
 PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
 SCALE: VERT. NONE
 HORIZ.
 DRAWN BY
 CHECKED BY
 BD400-04 (BD-22)

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	092
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



- * 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- ** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

- ② CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
- ③ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
- ④ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑤ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
- ⑥ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
- ⑦ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

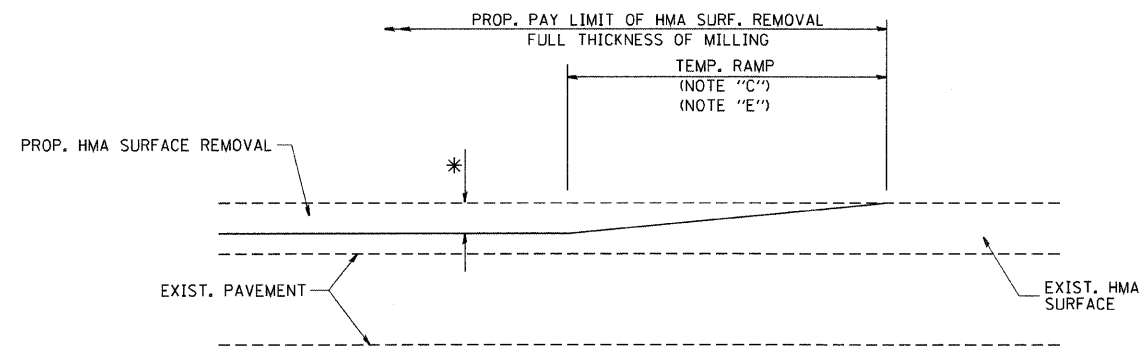
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

SCALE: VERT. NONE
HORIZ. DRAWN BY
CHECKED BY
BD600-06 (BD-24)

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

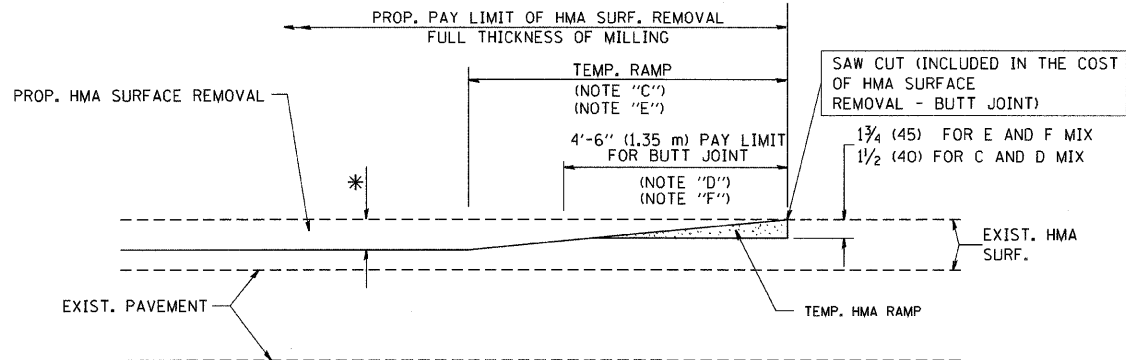
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PLOT SCALE = 50.0000 / IN.
USER NAME = bborndi

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	093
STA.		TO STA.		
FED. ROAD DIST. NO. 1		ILLINOIS FED. AID PROJECT		



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

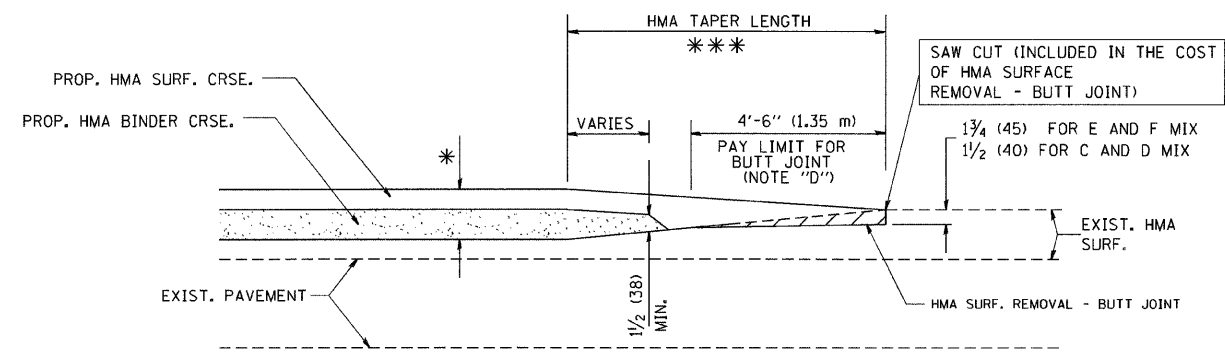
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

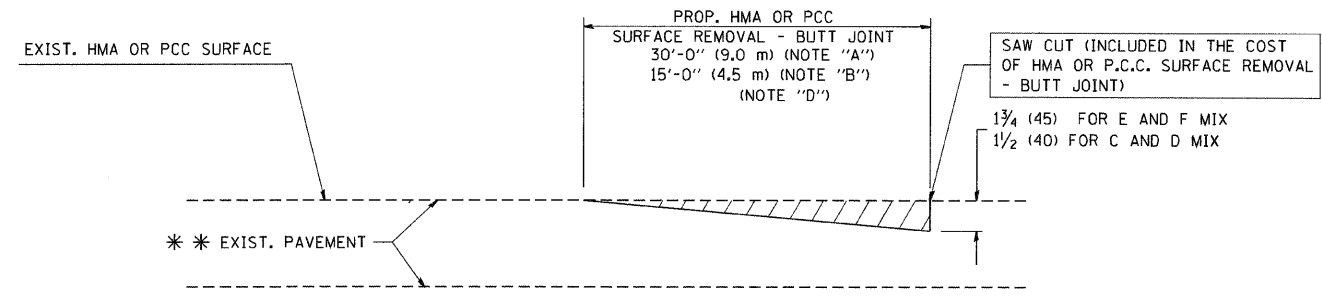
OPTION 2

TYPICAL TEMPORARY RAMP

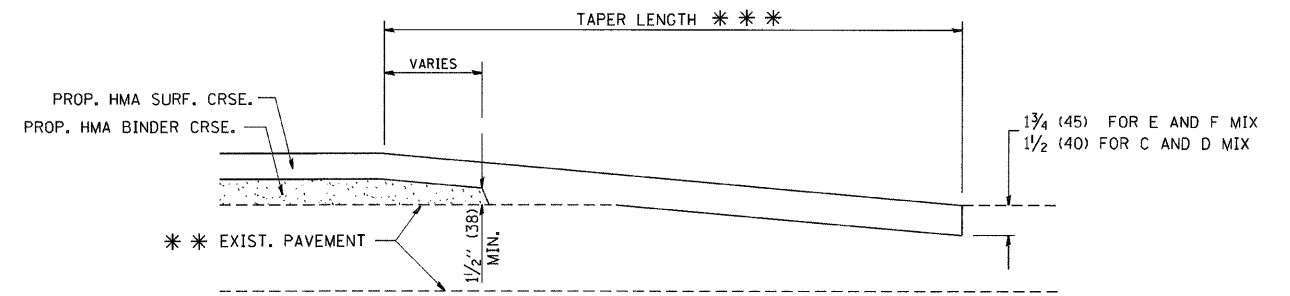


BUTT JOINT AND
HMA TAPER

**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



BUTT JOINT DETAIL



HMA TAPER DETAIL

**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01
R. BORO	01/01/07

ILLINOIS DEPARTMENT OF TRANSPORTATION

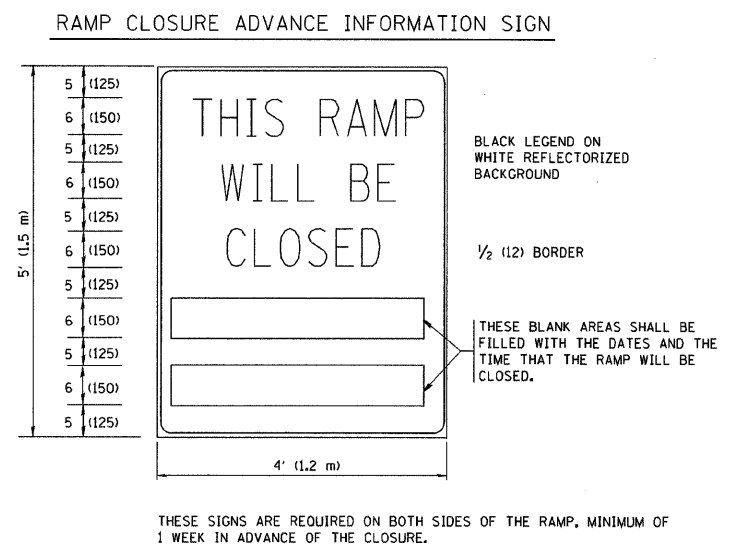
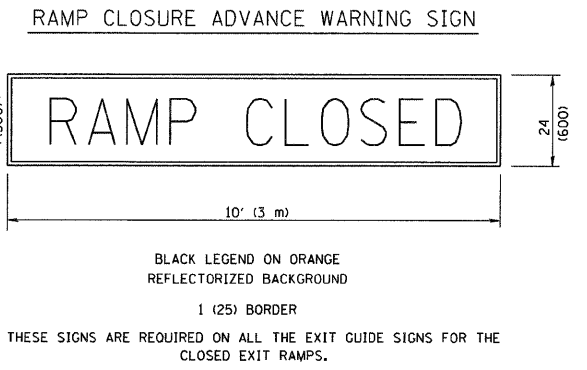
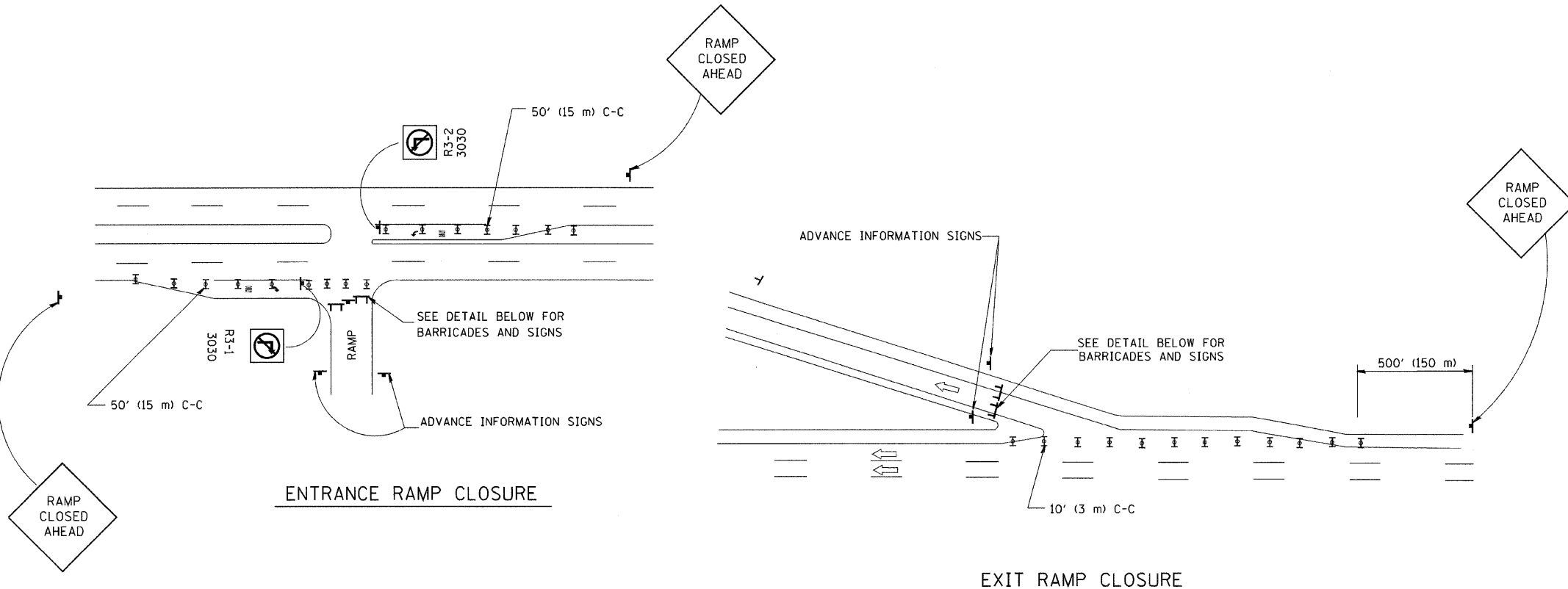
**BUTT JOINT AND
HMA TAPER
DETAILS**

SCALE: VERT. NONE
HORIZ.

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CHECKED BY

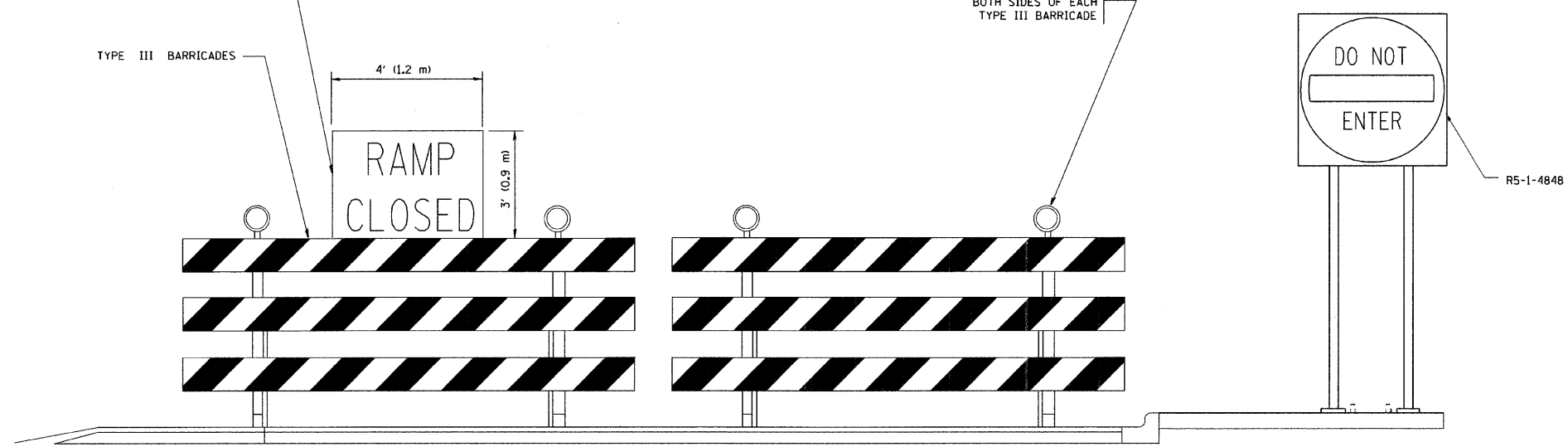
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USER NAME = bboard1

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	095
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



THE "RAMP CLOSED" SIGN SHALL BE B/W WITH 8 (200) CAPS. IF A TYPE III BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 IS NOT AVAILABLE, THE SIGNS MAY BE MOUNTED ON NCHRP 350 TEMPORARY SIGN SUPPORTS DIRECTLY IN FRONT OF THE BARRICADE.

FLASHER UNIT AMBER BOTH SIDES OF EACH TYPE III BARRICADE



DETAIL FOR REQUIRED BARRICADES & SIGNS

- SYMBOLS**
- ▬ TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
 - ▬ TYPE III BARRICADE WITH FLASHING LIGHT

GENERAL NOTES:

1. CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
2. STEADY BURN LIGHTS WILL NOT BE REQUIRED FOR DAY OPERATIONS.
3. A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES.
4. ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED.
5. THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
7. THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED TWENTY-FOUR (24) HOURS IN LENGTH.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
DWS	2-83
DWS	1/90
DWS	9/94
DWS	12/94
DWS/JAF	12/02
JAF	2/06
SPB	1/07
Revise devices to meet NCHRP 350	4/03

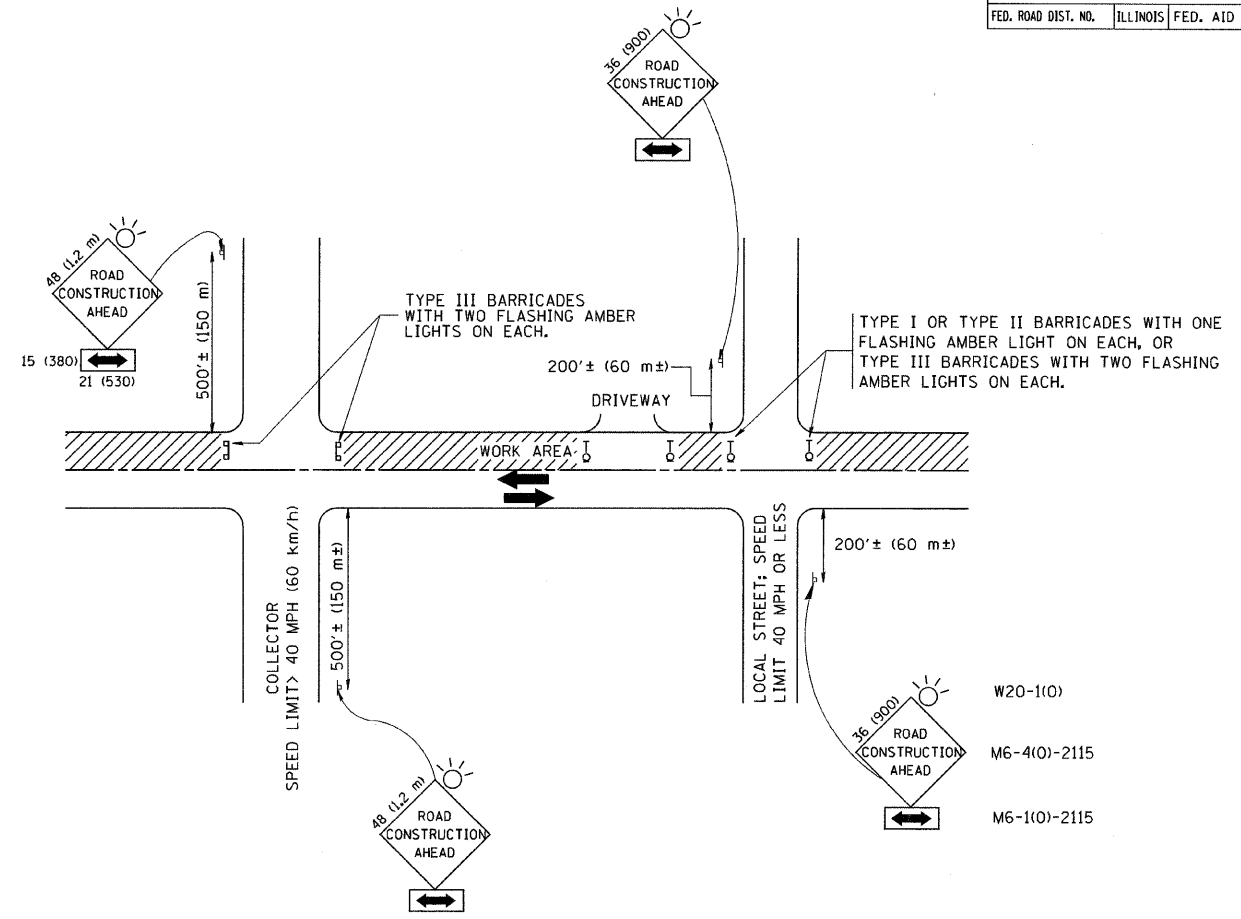
ILLINOIS DEPARTMENT OF TRANSPORTATION
FREWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS

SCALE: NONE

DRAWN BY
CHECKED BY

PLOT DATE = 3/6/2007
FILE NAME = K:\dashed\0808.dgn
PLOT SCALE = 500,000 / 1 IN.
USER NAME = bward

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	096
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 - 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 - USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

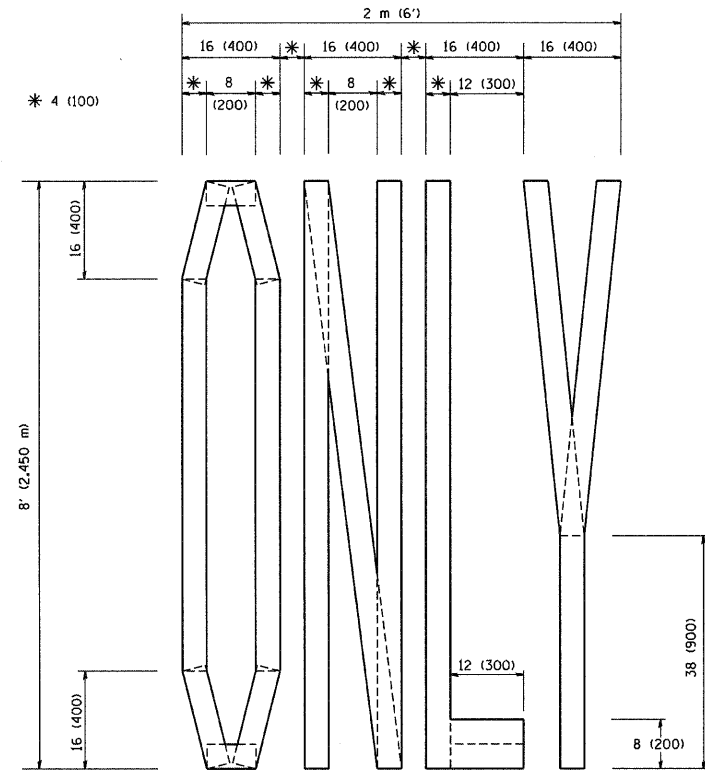
REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/08/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL AND PROTECTION
 FOR
 SIDE ROADS, INTERSECTIONS, AND
 DRIVEWAYS

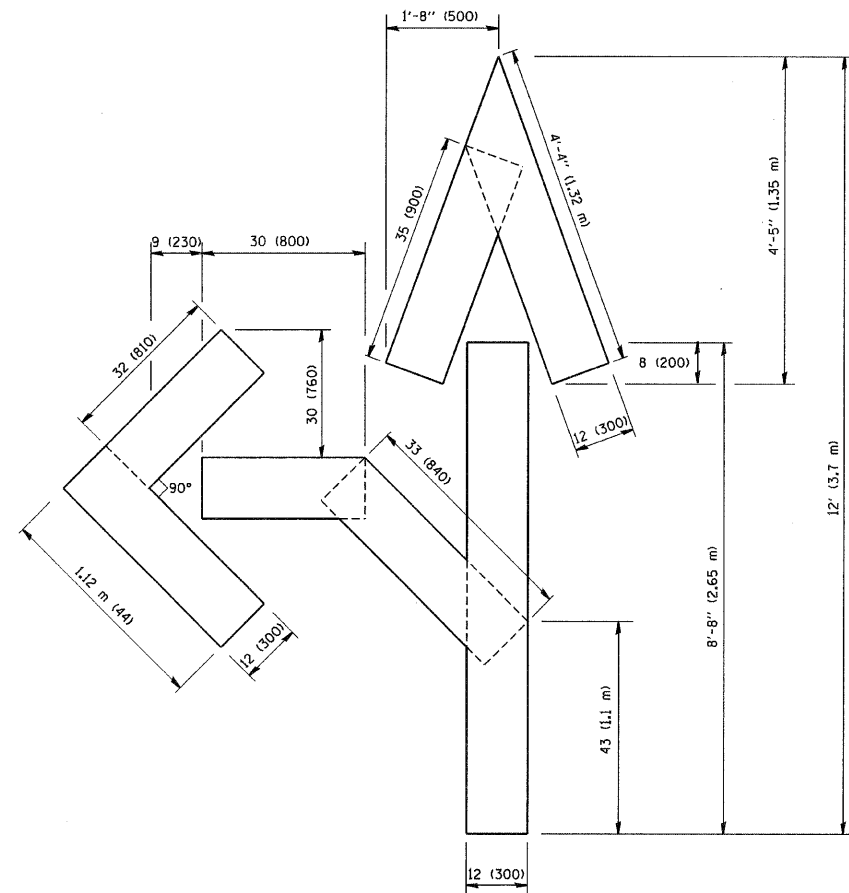
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 USER NAME = bround

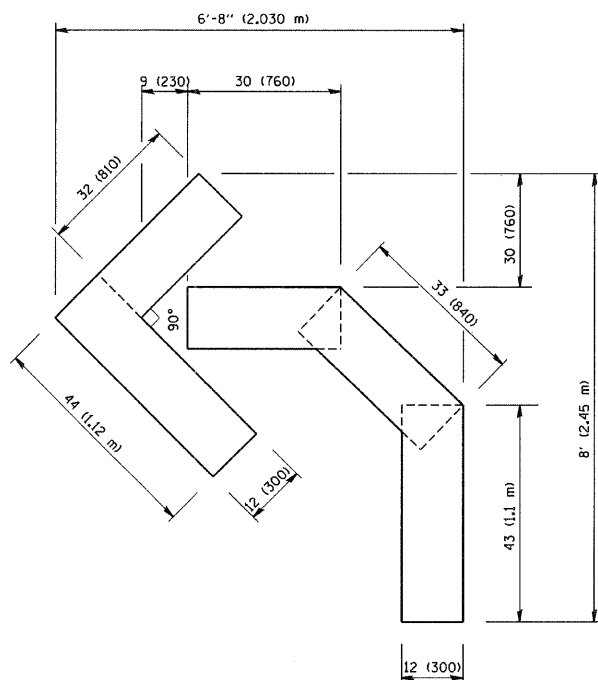
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	097
STA.		TO STA.		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

REVISIONS	
NAME	DATE
T. RAMMACHER	09/18/94
J. OBERLE	06/01/96
T. RAMMACHER	06/05/96
T. RAMMACHER	11/04/97
T. RAMMACHER	03/02/98
E. GOMEZ	08/28/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING
 LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE

DRAWN BY CADD

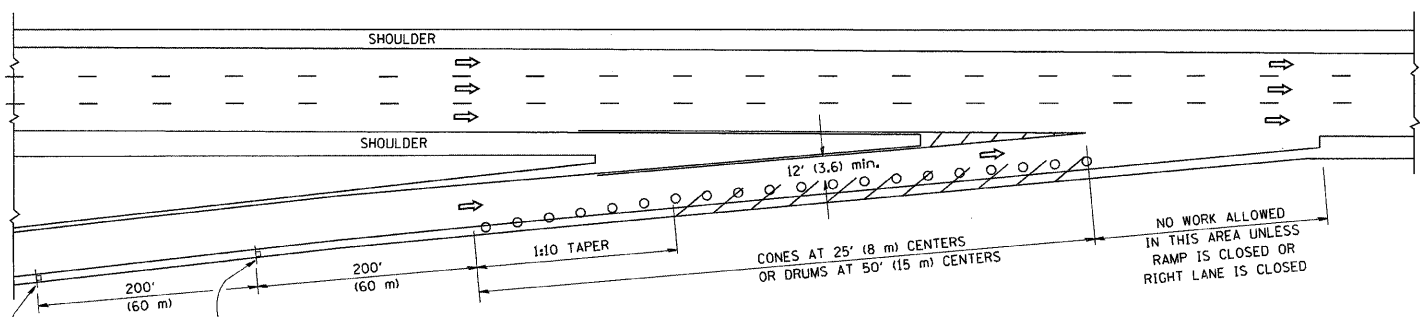
CHECKED BY

TC-16

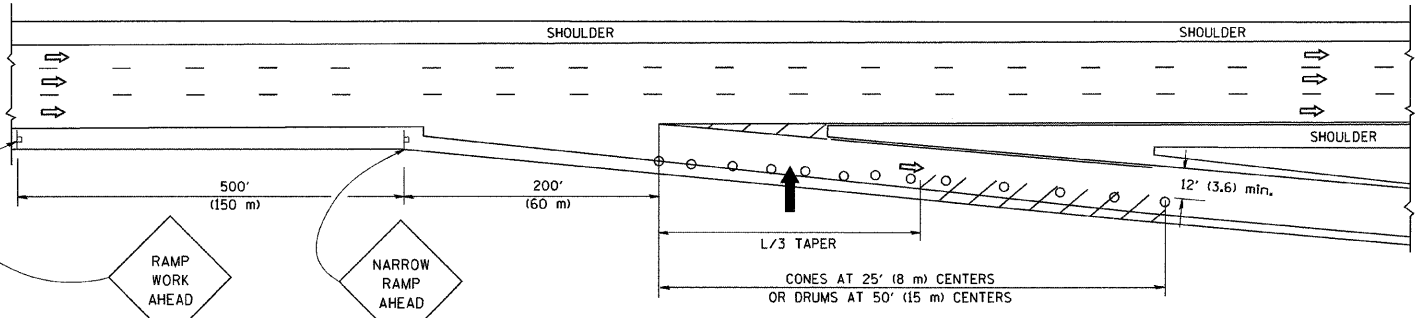
PARTIAL RAMP CLOSURE DETAILS

SHOULDER CLOSURE DETAILS

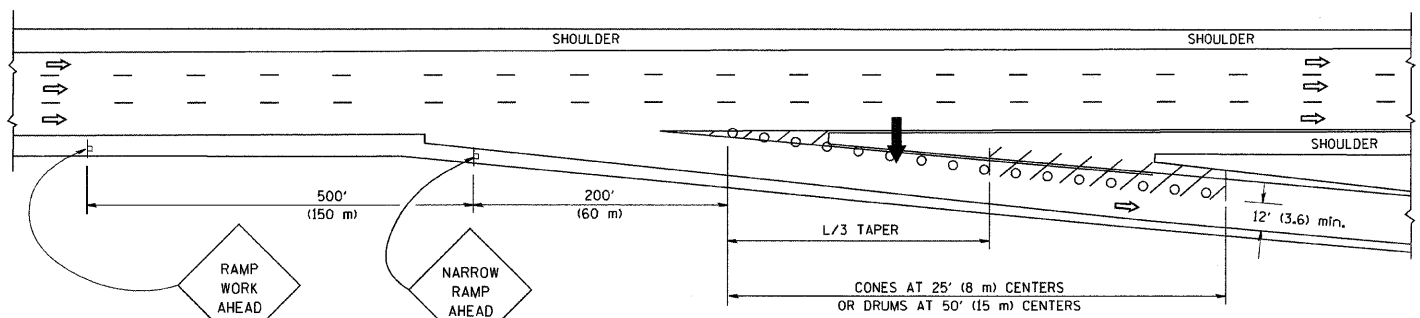
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	098
STA. TO STA.		ILLINOIS FED. AID PROJECT		
FED. ROAD DIST. NO.				



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

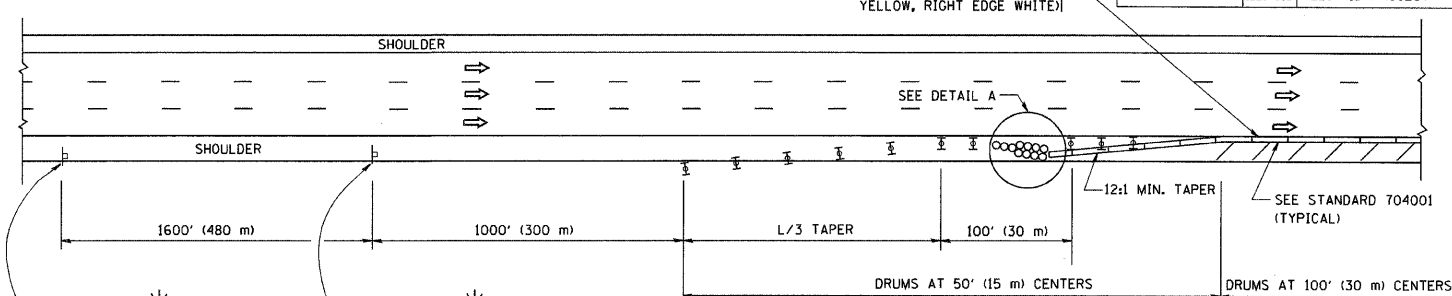
SYMBOLS

- ARROWBOARD
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURN MONO-DIRECTIONAL LIGHT
- CONE, DRUM OR BARRICADE

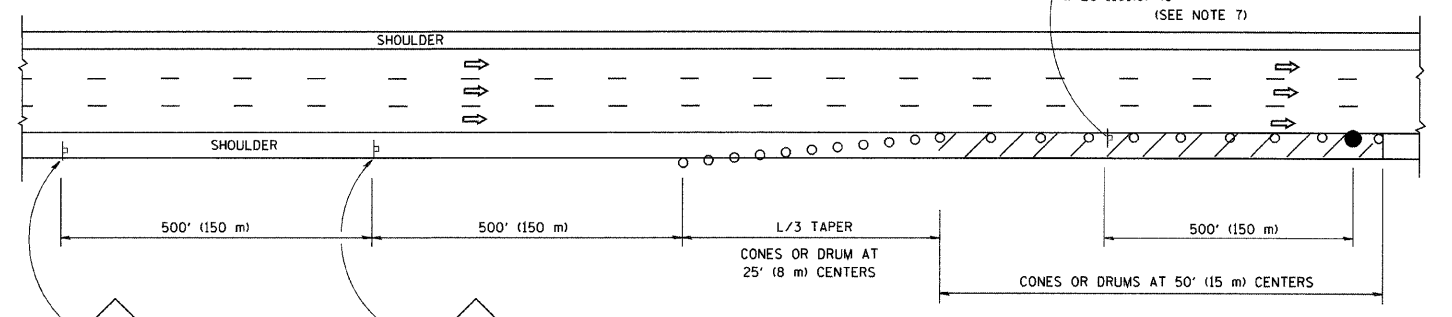
GENERAL NOTES

- THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER:	METRIC: $L=0.65(W)(S)$ ENGLISH: $L=(W)(S)$
	W = WIDTH OF OFFSET IN FEET (METERS)
	S = NORMAL POSTED SPEED MPH (KM/H)
- PLASTIC DRUMS WITH HIGH PERFORMANCE REFLECTIVE SHEETING AND STEADY BURNING LIGHTS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES.
- ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.

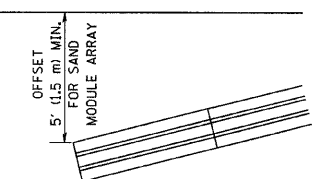


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

ARRAY DESIGN PER MANUFACTURER TO BE NCHRP 350 COMPLIANT FOR POSTED SPEED.



DETAIL "A" IMPACT ATTENUATOR, TEMPORARY (SEE NOTE 5)

- THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE "TRAFFIC BARRIER TERMINAL, TYPE III, TEMPORARY" DEVICE TO MEET NCHRP350 FOR POSTED SPEED.
- AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
- THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.

REVISIONS	
NAME	DATE
DWS	11/96
JAF	12/02
NCHRP 350	04/03
JAF	2/06
SPB	1/07

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

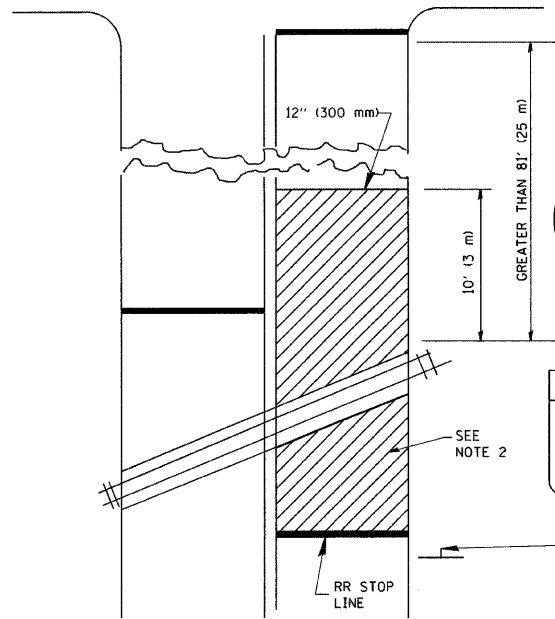
ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES PARTIAL RAMP CLOSURES

DESIGNED BY: DWS
 DRAWN BY:
 CHECKED BY:

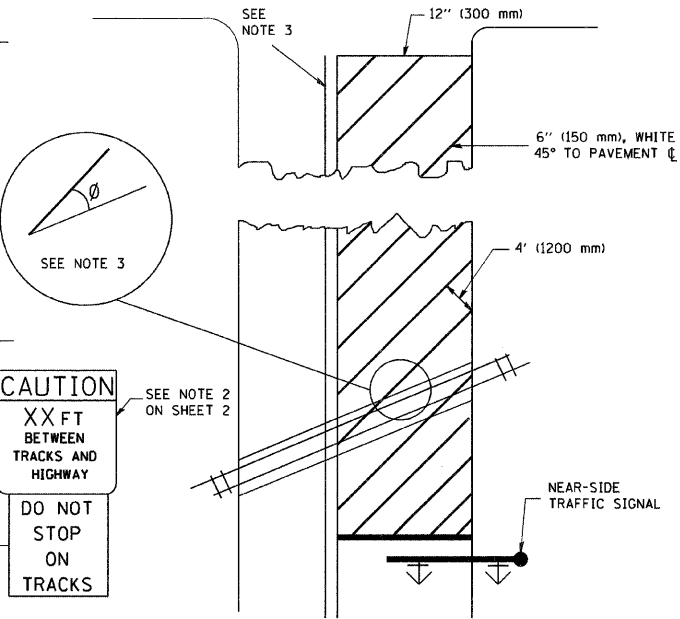
SCALE: NONE

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			101	099
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

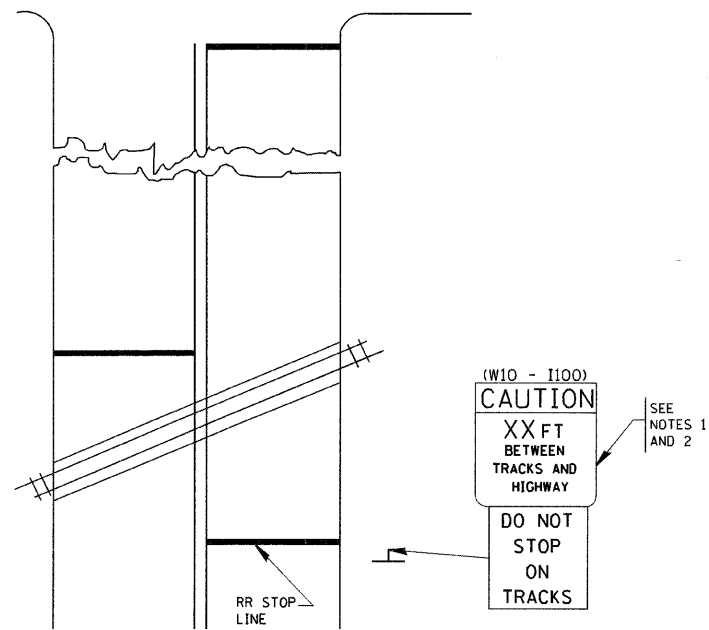
WITH INTERSECTION TRAFFIC SIGNALS



WITH NEAR-SIDE TRAFFIC SIGNALS



WITH NONSIGNALIZED INTERSECTION
81' (25 m) OR LESS TO CLOSEST RAIL



NOTES:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED, THE PAVEMENT MARKINGS EXTENDS TO THE INTERSECTION.
- WHERE THE ANGLE BETWEEN THE DIAGONAL STRIPES AND THE TRACK (θ) WOULD BE LESS THAN APPROXIMATELY 20°, THE STRIPES SHOULD BE SLOPED IN THE OPPOSITE DIRECTION FROM THAT SHOWN.

NOTE :

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET (1.8 m) FROM THE RAIL CLOSEST TO THE INTERSECTION TO THE STOP LINE OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET (1.5 m). WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE THE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6- FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
	01/01/07

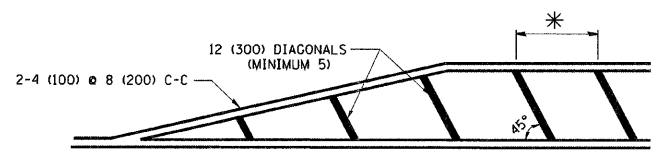
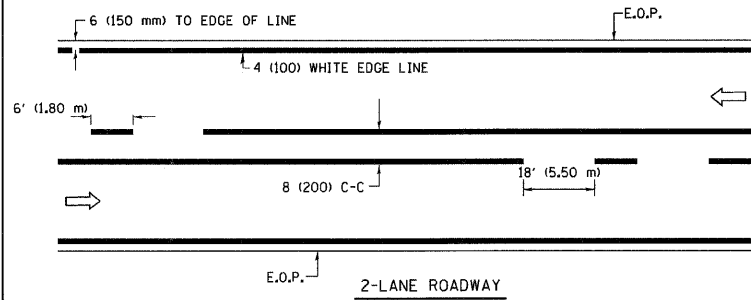
ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SUPPLEMENTAL
SIGNING AND PAVEMENT MARKING
TREATMENT FOR RAILROAD CROSSINGS

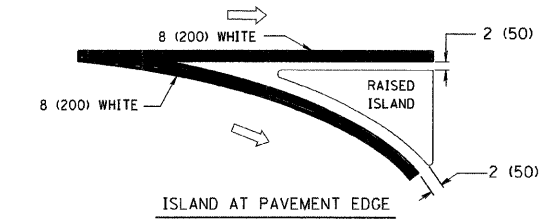
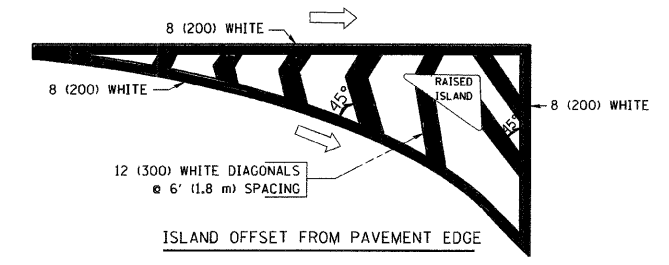
SCALE: NONE

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CHECKED BY
TC23

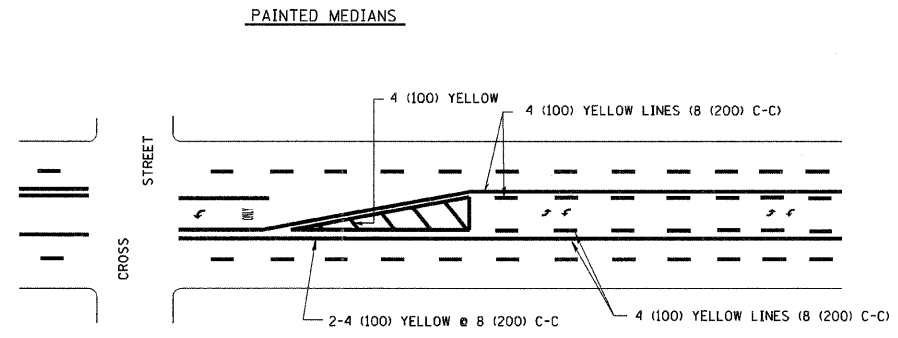
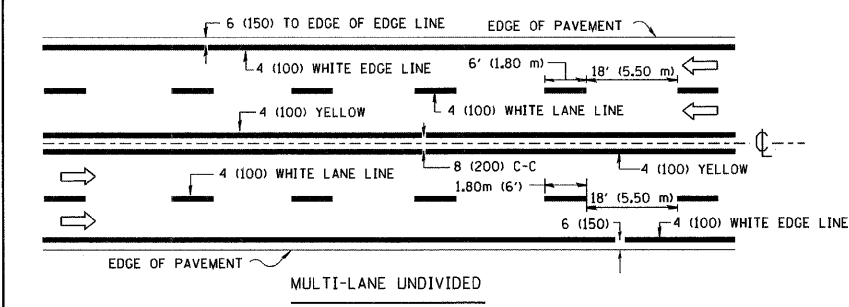
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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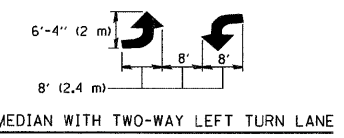
* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
* DIAGONAL LINE SPACING: 20' (6.1 m) C-C



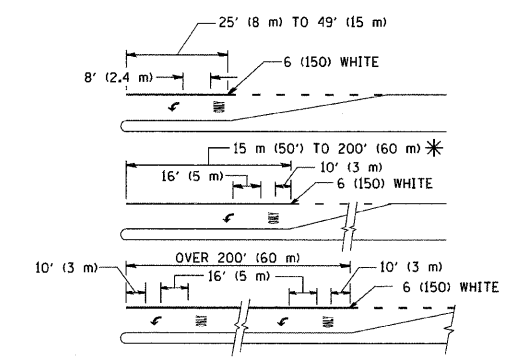
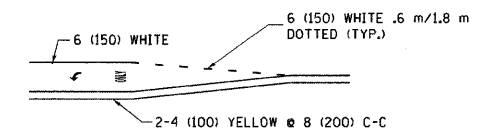
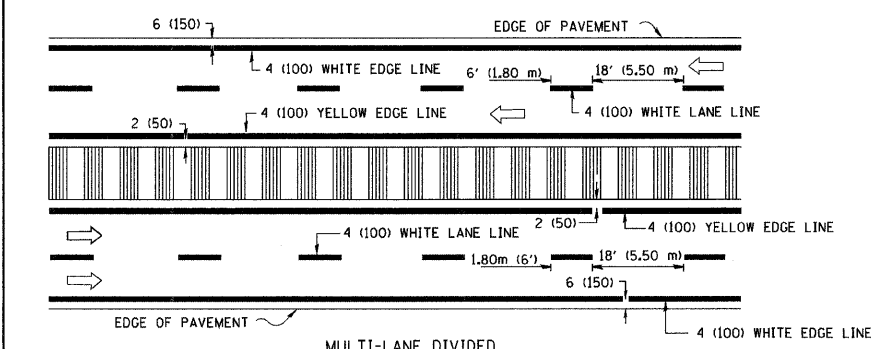
TYPICAL ISLAND MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.8 SQ. FT. (1.47 m²) ONLY AREA = 22.9 SQ. FT. (2.13 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 6 (150) 12 (300) 45° 8 (200) 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2'-4" (700) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK. IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 4 (100) WITH 12 (300) DIAGONALS 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/hr))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

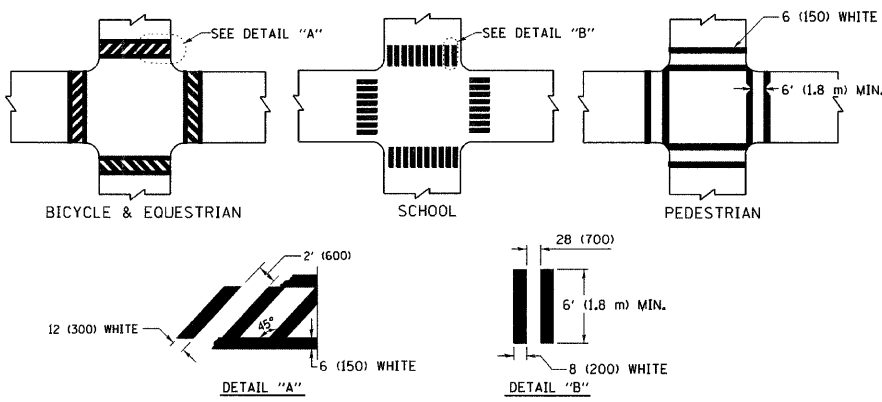
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

REVISIONS	
NAME	DATE
T. RAMMACHER	12/07/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

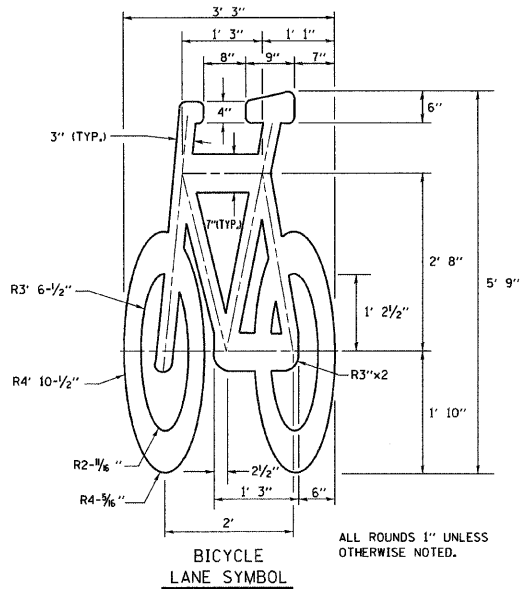
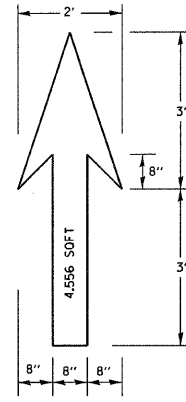
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TC-24

TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

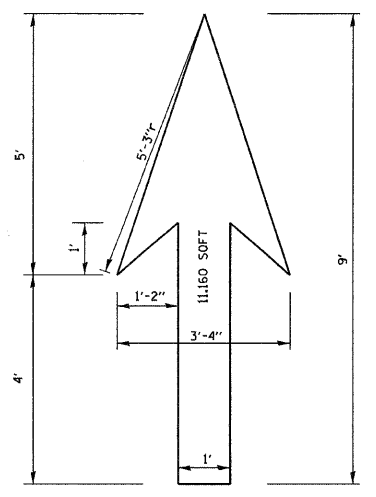
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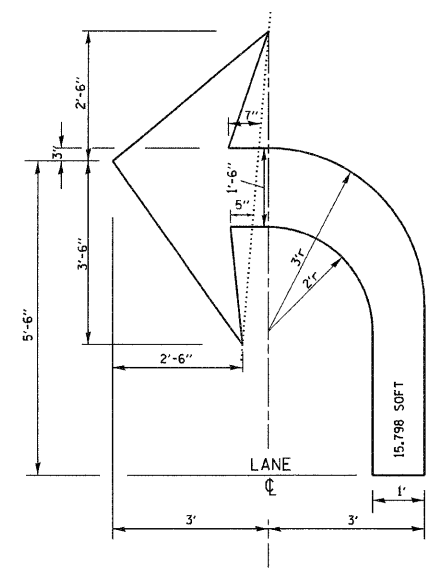
- NOTE:**
- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
 - THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28

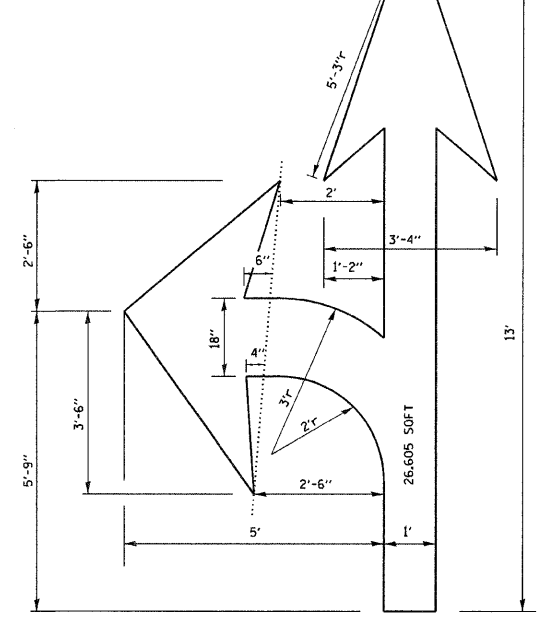
DRAWING #29



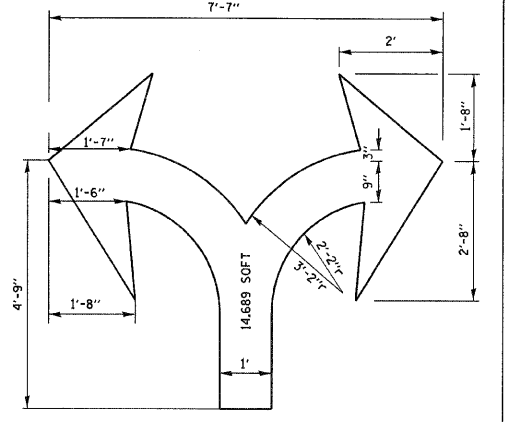
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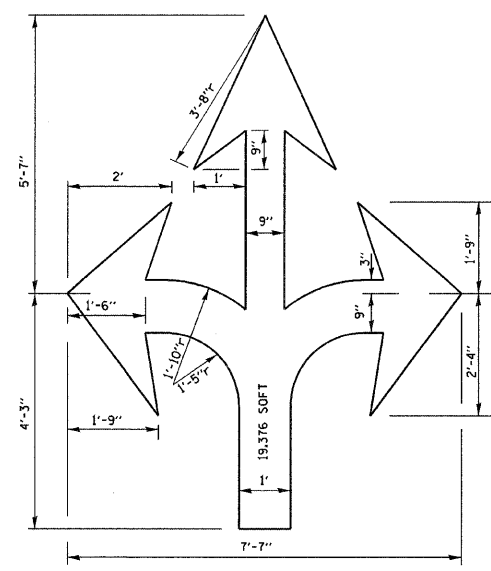
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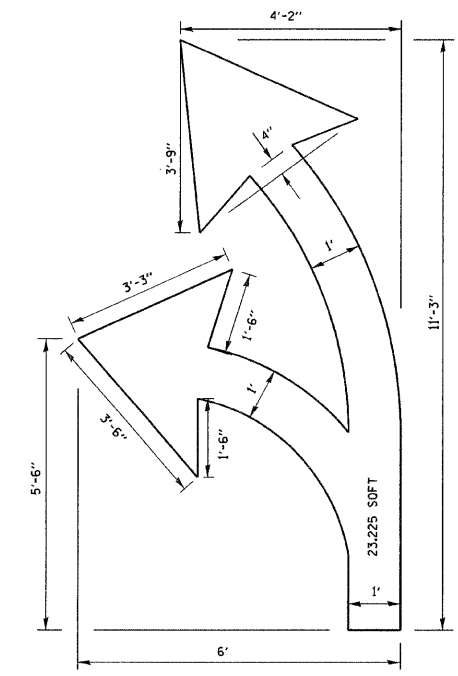
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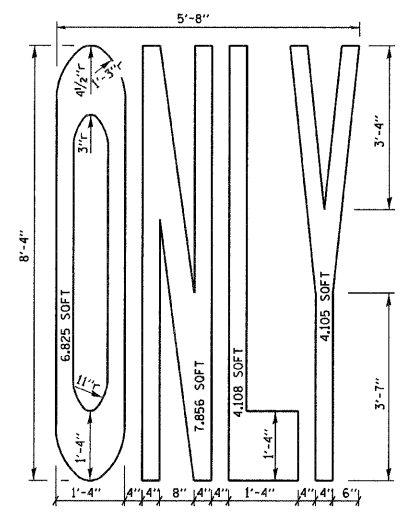
DRAWING #33



DRAWING #34



DRAWING #35



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

REVISIONS	
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T. RAMMACHER	12/07/00

ILLINOIS DEPARTMENT OF TRANSPORTATION
CITY OF CHICAGO
TYPICAL PAVEMENT MARKINGS

SCALE: NONE
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