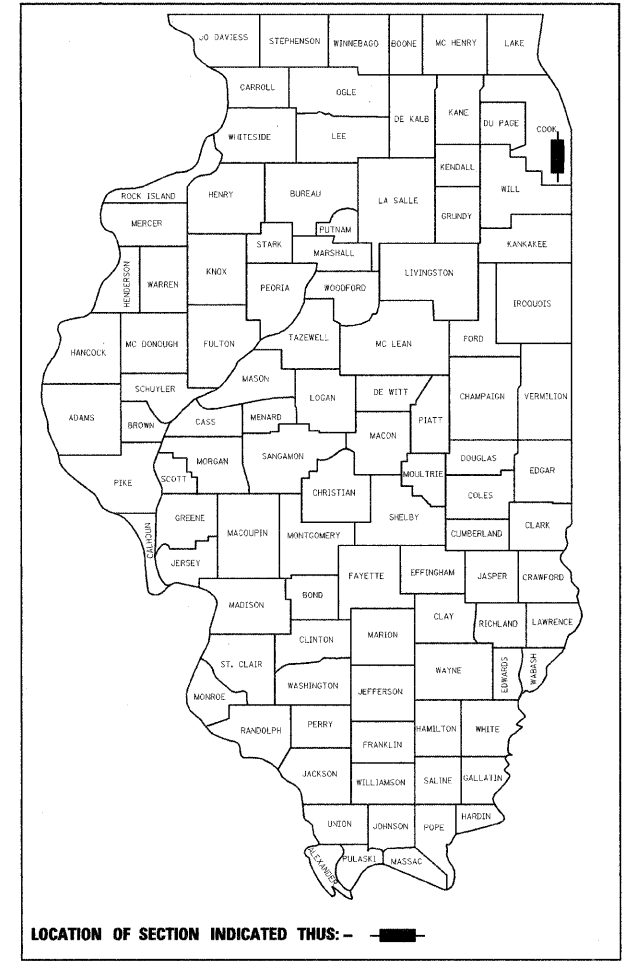


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94		COOK	133	1
FED. ROAD DIST. NO.	ILLINOIS	CONTRACT NO. 60D21		

• (0405,ETC,42-7&0707)RS-11

D-91-114-07



LOCATION OF SECTION INDICATED THUS: - ■ -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

**PROPOSED  
HIGHWAY PLANS**

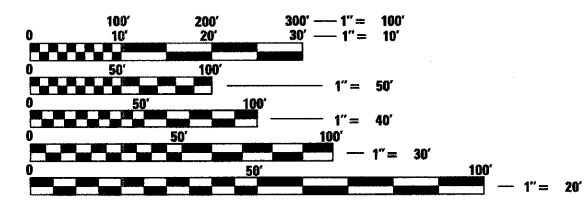
**FAI-94 (BISHOP FORD EXPRESSWAY)  
SOUTH OF DOLTON AVENUE INTERCHANGE TO U.S. 6 (159th ST)  
STRUCTURE AND ROADWAY REPAIRS  
AND RESURFACING  
SECTION (0405,ETC,42-7&0707)RS-11  
COOK COUNTY  
C-91-355-07  
PROJECT: ESP-094-3(417)072**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

ADT = 143,000 VEHICLES  
POSTED SPEED LIMIT = 55 MPH

PROJECT LOCATED IN  
CALUMET CITY AND THE  
VILLAGES OF DOLTON AND  
SOUTH HOLLAND

DISTRICT ONE / DESIGN / CONSULTANT SERVICES SECTION CRAIG BAUER (847) 705-4265



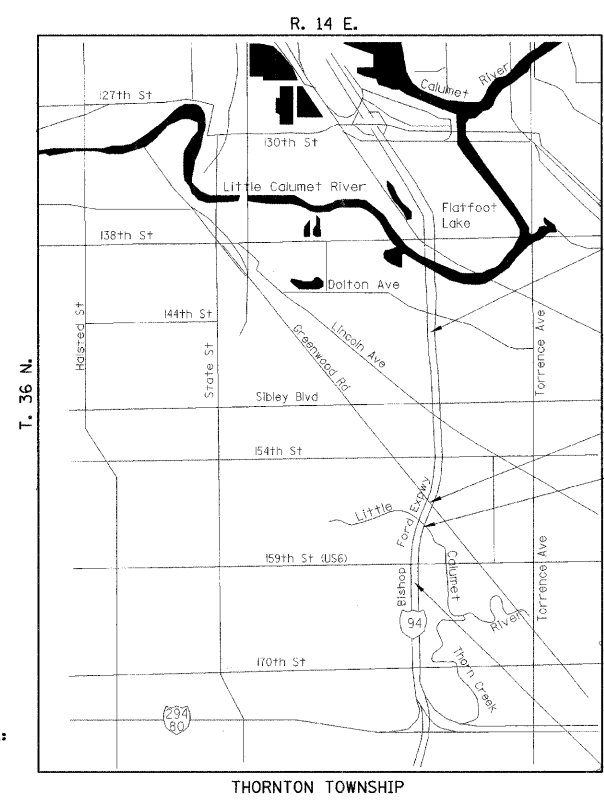
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: CRAIG BAUER  
PROJECT MANAGER: HELEN PAZON

CONTRACT NO. 60D21

APPROX. MAP SCALE:  
1" = 1.2 MILES



PROJECT BEGINS  
STA. 485+91

SN 016-0162  
I-94 OVER GREENWOOD AVE  
SN 016-0163  
I-94 OVER LITTLE CALUMET RIVER

ALL STRUCTURES:  
DECK OVERLAY, BEARING REPLACEMENT,  
JOINT REPAIRS AND REPLACEMENT,  
CONCRETE REPAIRS AND SELECT PAINTING.

PROJECT ENDS  
STA. 607+28

GROSS LENGTH OF PROJECT = 12,137 LIN. FT. = 2.30 MILES  
NET LENGTH OF PROJECT = 12,137 LIN. FT. = 2.30 MILES

*Sealed sheets 68-101*  
1-15-09  
exp. 11-30-10

*Applies to Sheets 1-67 and 102-118*  
1-15-09  
6XP 11-30-09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

SUBMITTED JANUARY 21, 2009

*Diana M. O'Keefe*  
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER  
March 13, 2009

*Charles J. Ingersoll*  
ENGINEER OF DESIGN AND ENVIRONMENT  
March 13, 2009

*Christie M. Reed*  
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY  
OF THE STATE OF ILLINOIS

## INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS, STANDARDS, GENERAL NOTES & HMA REQUIREMENTS
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7 - 10	TYPICAL SECTIONS
11 - 17	SCHEDULES OF QUANTITIES
18 - 19	ALIGNMENT, TIES AND BENCHMARKS
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70 - 103	STRUCTURAL PLANS
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107 - 133	DISTRICT DETAILS

## STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-05	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
420401-07	BRIDGE APPROACH PAVEMENT CONNECTOR
442101-07	CLASS B PATCHES
442201-03	CLASS C AND D PATCHES
606001-04	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
609006-04	BRIDGE APPROACH PAVEMENT (DRAIN DETAIL)
630001-08	STEEL PLATE BEAM GUARDRAIL
631026-05	TRAFFIC BARRIER TERMINAL, TYPE 5
631031-07	TRAFFIC BARRIER TERMINAL, TYPE 6
635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
635011-02	REFLECTOR MARKER AND MOUNTING DETAILS
642001-01	SHOULDER RUMBLE STRIPS
701101-02	OFF-ROAD OPERATIONS, MULTILANE, 4.5 m (15') TO 600 mm (24") FROM PAVEMENT EDGE
701400-03	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-05	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-05	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701421-02	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS ≥ 45 MPH
701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS, FOR SPEEDS ≥ 45 MPH
701446-01	TWO LANE CLOSURE, FREEWAY/EXPRESSWAY
701901-01	TRAFFIC CONTROL DEVICES
704001-05	TEMPORARY CONCRETE BARRIER
720001-01	SIGN PANEL MOUNTING DETAILS
720006-02	SIGN PANEL ERECTION DETAILS
720021-02	SIGN PANELS EXTRUDED ALUMINUM TYPE

## GENERAL NOTES

- BEFORE STARTING WORK THE CONTRACTOR SHALL CALL J.U.L.I.E. AT 811 OR (800) 892-0123 FOR FIELD LOCATIONS OF BURIED UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AS REQUIRED.
- THE CONTRACTOR WILL COORDINATE WORK ACTIVITIES WITH THE ADJACENT I-94 CONTRACT NO. 60C08.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- VERTICAL CLEARANCES OF BRIDGES OVER I-94 SHALL BE MAINTAINED. HMA SURFACE REMOVAL AND PROPOSED HMA SURFACE THICKNESS WILL BE VARIED AS DIRECTED BY THE ENGINEER IF THE EXISTING BITUMINOUS DEPTH IS LESS THAN 4-1/4". THE CONTRACTOR SHALL VERIFY ALL BRIDGE CLEARANCES BEFORE AND AFTER CONSTRUCTION AND FURNISH RESULTS TO THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE LUMP SUM PRICE FOR CONSTRUCTION LAYOUT.
- THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS WHEN USING ARTIFICIAL LIGHTING DURING NIGHT OPERATIONS TO PREVENT ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
- PATCHING, SHOULDER REMOVAL AND REPLACEMENT, AND SHOULDER RECONSTRUCTION LIMITS ARE SHOWN ON THE PLANS AND ARE SUBJECT TO CHANGE AS DIRECTED BY THE ENGINEER.
- BUTT JOINTS WILL BE INSTALLED AT THE END OF ALL RESURFACING SECTIONS TO MATCH EXISTING PAVEMENT. SEE DISTRICT DETAIL BD400-05 - BUTT JOINT AND HMA TAPER DETAILS FOR ALL PCC AND HMA BUTT JOINTS IDENTIFIED IN THE RESURFACING PLANS.
- CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER AT (847) 715-8422 TWO WEEKS BEFORE INSTALLING FINAL PAVEMENT MARKINGS.
- TRAFFIC CONDITIONS, ACCIDENTS AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL RESPOND TO THE ENGINEER'S REQUEST WITHIN THIRTY (30) MINUTES FROM THE TIME OF THE NOTIFICATION.
- THE CONTRACTOR MAY WORK IN THE EASTBOUND AND WESTBOUND LANES SIMULTANEOUSLY.
- THE CONTRACTOR SHALL SWEEP THE PAVEMENT SURFACE, PLACE TEMPORARY PAVEMENT MARKING AND REMOVE TRAFFIC CONTROL AT THE END OF EACH NIGHT SHIFT PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS. VERTICAL BARRICADES WILL REMAIN IN PLACE ALONG THE EDGES OF PAVEMENT AS SHOWN IN THE SUGGESTED MAINTENANCE OF TRAFFIC PLANS UNTIL THE SURFACE COURSE AND PROPOSED PAVEMENT MARKING EDGE LINES HAVE BEEN COMPLETED.
- THE CONTRACTOR SHALL FURNISH AND INSTALL TWO WEIGHTED SANDBAGS ON ALL TYPE I AND TYPE II BARRICADES USED.
- TEMPORARY EDGE LINES SHOWN IN THE SUGGESTED MAINTENANCE OF TRAFFIC SHALL BE INSTALLED WITHIN 24 HOURS OF MILLING OR BINDER PLACEMENT.
- STRIP REFLECTIVE CRACK CONTROL TREATMENT WILL BE INSTALLED IMMEDIATELY IN FRONT OF THE HMA SURFACING OPERATION SO TRAFFIC WILL NOT BE DISTURBED BEFORE BEING COVERED BY HMA. ANY CRACK CONTROL TREATMENT DISTURBED BY TRAFFIC WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR WILL MILL 4 FOOT LONG TRENCHES IN THE SHOULDER TO FACILITATE MAINLINE DRAINAGE WHEN THE MAINLINE SURFACE HAS BEEN REMOVED. THE TRENCH WILL BE LEVEL FROM THE OUTSIDE EDGE OF PAVEMENT AND TAPER 2 FEET AT EACH END TO MATCH THE MILLED SHOULDER ELEVATION. TRENCHES WILL BE CONSTRUCTED AT ALL SAG LOCATIONS AND SPACED AT 1000 FOOT CENTERS MINIMUM. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR "HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4 INCH".
- THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS WAS DEVELOPED FROM PREVIOUS PLANIMETRICS AND AERIAL PHOTOGRAPHY FURNISHED BY THE DEPARTMENT AND IS NOT THE RESULT OF A GROUND SURVEY. THEREFORE, THE ALIGNMENTS AND SUPPORTING DATA SHOWN IN THE PLANS IS FOR REFERENCE PURPOSES ONLY. THE RELATIVE ACCURACY OF THE INFORMATION IS UNKNOWN AND CANNOT BE GUARANTEED. THE CONTRACTOR MAY BE REQUIRED TO ADJUST LAYOUT TO MATCH ACTUAL FIELD CONDITIONS AND THE INTENT OF THE PLANS.
- AN EXISTING CURB AND GUTTER UNDER THE HMA SURFACE ALONG THE INSIDE SHOULDER MAY BE ENCOUNTERED DURING HMA SURFACE REMOVAL. IF ENCOUNTERED, THE MILLING DEPTH WILL BE ADJUSTED AT THE CURB AND THE HMA WILL BE REMOVED IN THE FLAG AS DIRECTED BY THE ENGINEER. COSTS SHALL BE INCLUDED IN THE UNIT PRICE FOR THE HMA SURFACE REMOVAL.

### HOT-MIX ASPHALT REQUIREMENTS

MIXTURE TYPE	THICKNESS	AC TYPE	VOIDS
<b>MAINLINE RESURFACING</b>			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	1-3/4"	SBS PG 76-28/-22	4% @ 80 Gyr
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80	1-3/4"	SBS PG 76-28/-22	4% @ 80 Gyr
POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50	3/4"	SBS/SBR PG 76-28/-22	4% @ 50 Gyr
<b>MAINLINE SHOULDER RESURFACING</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	1-3/4"	PG 64-22	4% @ 70 Gyr
<b>US 6 RAMP &amp; RAMP SHOULDER RESURFACING</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	2"	PG 64-22	4% @ 70 Gyr
POLYMERIZED LEVELING BINDER (MACHINE METHOD) IL-4.75, N50	1"	SBS/SBR PG 76-28/-22	4% @ 50 Gyr
<b>ALL OTHER RAMP &amp; RAMP SHOULDER RESURFACING</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	1-3/4"	PG 64-22	4% @ 70 Gyr
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	2-1/4"	PG 64-22 <sup>1</sup>	4% @ 70 Gyr
<b>MAINLINE SHOULDER RECONSTRUCTION</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	1-3/4"	PG 64-22	4% @ 70 Gyr
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	2-1/4"	PG 64-22 <sup>1</sup>	4% @ 70 Gyr
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm)	10"	PG 64-22 <sup>1</sup>	4% @ 70 Gyr
<b>RAMP SHOULDER RECONSTRUCTION</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL 9.5 mm)	1-3/4"	PG 64-22	4% @ 70 Gyr
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	2-1/4"	PG 64-22 <sup>1</sup>	4% @ 70 Gyr
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm)	6"	PG 64-22 <sup>1</sup>	4% @ 70 Gyr
<b>BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)</b>			
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	1-3/4"	SBS PG 76-28/-22	4% @ 80 Gyr
POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80	1-3/4"	SBS PG 76-28/-22	4% @ 80 Gyr
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm)	10-1/2"	PG 64-22 <sup>1</sup>	4% @ 70 Gyr
<b>PATCHING</b>			
CLASS D PATCHES OF TYPE AND THICKNESS SPECIFIED (HMA BINDER IL-25 mm)	10"	PG 64-22	4% @ 105 Gyr
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-25 mm)	4" OR 6"	PG 64-22 <sup>1</sup>	4% @ 105 Gyr
<b>HMA SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)</b>			
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	8" (MIN) <sup>2</sup>	PG 64-22 <sup>1</sup>	4% @ 70 Gyr
<b>STABILIZED MEDIAN SURFACE</b>			
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50	4"	PG 64-22	4% @ 50 Gyr

UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SMA SURFACE MIXTURES IS 135 LBS/SQ YD/IN.  
UNIT WEIGHT USED TO CALCULATE ALL OTHER HOT-MIX ASPHALT SURFACE MIXTURES IS 112 LBS/SQ YD/IN.

<sup>1</sup> WHEN RAP EXCEEDS 20%, THE NEW ASPHALT BINDER SHOULD BE PG 58-22.

<sup>2</sup> ACTUAL THICKNESS SHALL BE THE GREATER OF THE EXISTING THICKNESS REMOVED OR THE MINIMUM THICKNESS SHOWN ABOVE.

### INDEX OF SHEETS, STANDARDS, GENERAL NOTES & HMA REQUIREMENTS

FILE NAME = D:\60D21-002-GENNOTE.dgn	USER NAME = dwozniarski	DESIGNED - DRAWN - DMW	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>		BISHOP FORD EXPRESSWAY (I-94)		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEET NO.	SHEET NO.
	PLOT SCALE = N/A	CHECKED - DATE - 2/7/2009	REVISED - REVISED -					94	(0405,ETC,42-7&07)RS-11	COOK	133	2
				SCALE: N/A	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 60D21		

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM	UNITS	URBAN 100% P20 TOTAL	EB	WB	SFTY-3J	SN 016-0162 BRIDGES X231-2A	SN 016-0163 BRIDGES X031-2A
				ROADWAY	ROADWAY			
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	95	45	50			
25000210	SEEDING, CLASS 2 A	ACRE	2.5	1.25	1.25			
28000400	PERIMETER EROSION BARRIER	FOOT	9,617	4,640	4,977			
31101900	SUB-BASE GRANULAR MATERIAL, TYPE C	TON	3,710	3,434	276			
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	58	29	29			
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGWAYS	TON	31	15	16			
40600895	CONSTRUCTING TEST STRIP	EACH	2	1	1			
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	590	293	297			
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	675	275	400			
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	771	397	374			
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	8,883	4,396	4,487			
42001430	BRIDGE APPROACH PAVEMENT CONNECTOR (FLEXIBLE)	SQ YD	840	420	420			
44000100	PAVEMENT REMOVAL	SQ YD	655	333	322			
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SQ YD	49,908	23,680	26,228			
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SQ YD	30,289	15,774	14,515			
44000165	HOT-MIX ASPHALT SURFACE REMOVAL, 4"	SQ YD	6,111	3,146	2,965			
44000166	HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4"	SQ YD	100,018	49,770	50,248			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	381	161	220			
44001980	CONCRETE BARRIER REMOVAL	FOOT	62	62				
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SQ YD	15		15			
44002224	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 6"	SQ YD	1,995	818	1,177			
44004250	PAVED SHOULDER REMOVAL	SQ YD	15,938	10,058	5,880			
44200970	CLASS B PATCHES, TYPE II, 10 INCH	SQ YD	10		10			
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SQ YD	872	327	545			
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SQ YD	321	179	142			
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	500	182	318			
44213200	SAW CUTS	FOOT	60		60			
44300200	STRIP REFLECTIVE CRACK CONTROL TREATMENT	FOOT	114,238	57,092	57,146			
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	269	77	192			
48203037	HOT-MIX ASPHALT SHOULDERS, 10"	SQ YD	2,358	676	1,682			
48203053	HOT-MIX ASPHALT SHOULDERS, 14"	SQ YD	8,715	4,821	3,894			
50102400	CONCRETE REMOVAL	CU YD	80.0				40.0	40.0

\* SPECIALTY ITEM

FILE NAME =  
D:\60021-003-500.dgn

USER NAME = dwozniarski

DESIGNED -

REVISED -

DRAWN - DMW

REVISED -

PLOT SCALE = N/A

CHECKED -

REVISED -

PLOT DATE = 2/7/2009

DATE - 2/7/2009

REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BISHOP FORD EXPRESSWAY (I-94)  
SUMMARY OF QUANTITIES

SCALE: N/A

SHEET NO. 1 OF 4 SHEETS STA. TO STA.

F.A.I.  
RTE.  
94

SECTION  
(0405,ETC,42-7&0707)RS-II

COUNTY  
COOK

TOTAL SHEETS  
133

SHEET NO.  
3

CONTRACT NO. 60D21

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM	UNITS	URBAN 100% FED. TOTAL	EB	WB	SFTY-3J	SN 016-0162	SN 016-0163
				ROADWAY	ROADWAY		BRIDGES	BRIDGES
				1000 - 2A			X231-2A	X031-2A
50300225	CONCRETE STRUCTURES	CU YD	154.2				80.0	74.2
50300255	CONCRETE SUPERSTRUCTURE	CU YD	811.8				406.8	405.0
50300300	PROTECTIVE COAT	SQ YD	230				120	110
50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	12,170				5,210	6,960
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	201,440				102,580	99,860
50606401	CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES NO.1	L SUM	1					1
50800515	BAR SPLICERS	EACH	1,016				508	508
52000110	PREFORMED JOINT STRIP SEAL	FOOT	504				242	242
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	18					18
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	54				36	18
52100520	ANCHOR BOLTS, 1"	EACH	144				72	72
NP 55039700	STORM SEWERS TO BE CLEANED	FOOT	24,098	19,550	4,548			
58700300	CONCRETE SEALER	SQ FT	1,191				581	610
60261540	INLETS TO BE ADJUSTED WITH NEW TYPE 24 FRAME AND GRATE	EACH	79	51	28			
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	51	28	23			
60600605	CONCRETE CURB, TYPE B	FOOT	240	180	60			
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	56	28	28			
60626300	STABILIZED MEDIAN SURFACE	SQ YD	4,794	4,794				
60900315	TYPE D INLET BOX, STANDARD 609006	EACH	6	3	3			
* 63000001	STEEL PLATE BEAM GUARD RAIL, TYPE A, 6 FOOT POSTS	FOOT	4,413			4,413		
* 63000005	STEEL PLATE BEAM GUARD RAIL, TYPE B	FOOT	626			626		
* 63000015	STEEL PLATE BEAM GUARD RAIL, TYPE D	FOOT	8,900			8,900		
* 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	1			1		
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	8			8		
* 63100089	TRAFFIC BARRIER TERMINAL, TYPE 6B	EACH	16			16		
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	7			7		
* 63100215	TRAFFIC BARRIER TERMINAL, TYPE 6 (SPECIAL)	EACH	5			5		
63200305	STEEL PLATE BEAM GUARD RAIL REMOVAL	FOOT	16,388	14,260	2,128			
64200105	SHOULDER RUMBLE STRIP	FOOT	45,080	22,540	22,540			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	16	8	8			
67100100	MOBILIZATION	L SUM	1	0.5	0.5			
70101900	TRAFFIC CONTROL AND PROTECTION (DETOUR 1)	L SUM	1		1			
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	8	4	4			
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	25,558	12,785	12,773			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	23,706	13,076	10,630			

NP = Non-participating  
\* SPECIALTY ITEM

FILE NAME =  
0169021-004-S00.dgn

USER NAME = dwozniarski

DESIGNED -

REVISED -

DRAWN - DMW

REVISED -

CHECKED -

REVISED -

DATE - 2/7/2009

REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BISHOP FORD EXPRESSWAY (I-94)  
SUMMARY OF QUANTITIES

SCALE: N/A SHEET NO. 2 OF 4 SHEETS STA. TO STA.

F.A.I.  
RTE.  
94

SECTION  
(0405,ETC,42-7&07)RS-11

COUNTY  
COOK

TOTAL SHEETS  
133

SHEET NO.  
4

CONTRACT NO. 60D21

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

Rev

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM	UNITS	URBAN 100% FED. TOTAL	EB	WB	SFTY-3J	SN 016-0162	SN 016-0163
				ROADWAY	ROADWAY		BRIDGES	BRIDGES
				1000 - 2A			X231-2A	X031-2A
70400100	TEMPORARY CONCRETE BARRIER	FOOT	22,319	11,160	11,159			
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	3,238	1,625	1,613			
* 72000300	SIGN PANEL - TYPE 3	SQ FT	2,906	1,831	1,075			
* 72400320	REMOVE SIGN PANEL - TYPE 2	SQ FT	140	70	70			
* 72400330	REMOVE SIGN PANEL - TYPE 3	SQ FT	2,693	1,765	928			
* 73000100	WOOD SIGN SUPPORT	FOOT	288	144	144			
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	54,362	27,319	27,043			
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	13,769	7,188	6,601			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2,568	1,411	1,157			
* 78004210	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 4"	FOOT	1,226	655	571			
* 78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	FOOT	11,500	5,719	5,781			
* 78005110	EPOXY PAVEMENT MARKING - LINE 4"	FOOT	24,913	14,661	10,252			
* 78005120	EPOXY PAVEMENT MARKING - LINE 5"	FOOT	6,372	3,767	2,605			
* 78005140	EPOXY PAVEMENT MARKING - LINE 8"	FOOT	453		453			
* 78005150	EPOXY PAVEMENT MARKING - LINE 12"	FOOT	2,144	1,032	1,112			
* 78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	5,595	2,198	3,397			
* 78008220	POLYUREA PAVEMENT MARKING TYPE I - LINE 5"	FOOT	702	351	351			
* 78008240	POLYUREA PAVEMENT MARKING TYPE I - LINE 8"	FOOT	2,015	579	1,436			
* 78008250	POLYUREA PAVEMENT MARKING TYPE I - LINE 12"	FOOT	555	26	529			
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1,277	635	642			
* 78100105	RAISED REFLECTIVE PAVEMENT MARKER (BRIDGE)	EACH	60	30	30			
* 78200100	MONODIRECTIONAL PRISMATIC BARRIER REFLECTOR	EACH	180			180		
* 78200450	MONODIRECTIONAL GUARD RAIL REFLECTORS	EACH	188			188		
* 78200530	BARRIER WALL MARKER, TYPE C	EACH	2,093	1,047	1,046			
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	7			7		
78300100	PAVEMENT MARKING REMOVAL	SQ FT	10,226	6,140	4,086			
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1,337	665	672			
X0321744	SILICONE JOINT SEALER, 2"	FOOT	588				313	275
* X0322247	MAINTENANCE OF EXISTING TRAFFIC SURVEILLANCE	L SUM	1	0.5	0.5			
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	52	26	26			
X0322729	MATERIAL TRANSFER DEVICE	TON	26,358	13,283	13,075			
X0325303	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	18				14	4

\* SPECIALTY ITEM

FILE NAME =  
D:\62021-005-500.dgn

USER NAME = dwoznis-ski

DESIGNED -

REVISED -

DRAWN - DMW

REVISED -

CHECKED -

REVISED -

DATE = 2/7/2009

REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BISHOP FORD EXPRESSWAY (I-94)  
SUMMARY OF QUANTITIES

SCALE: N/A

SHEET NO. 3 OF 4 SHEETS STA. TO STA.

F.A.I.  
RTE.

SECTION  
94 (0405,ETC,42-7&0707)RS-11

COUNTY  
COOK

TOTAL SHEETS  
133

SHEET NO.  
5

CONTRACT NO. 60D21

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT

SUMMARY OF QUANTITIES

CODE NUMBER	ITEM	UNITS	URBAN 100% P&D TOTAL	1000 - 2A		SFTY-3J	SN 016-0162 BRIDGES X231 - 2A	SN 016-0163 BRIDGES X031 - 2A
				EB ROADWAY	WB ROADWAY			
X0325349	TEMPORARY CONCRETE BARRIER (TO REMAIN PERMANENTLY)	FOOT	282			282		
X0325702	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	0.5	0.5			
X0325739	HOT-MIX ASPHALT SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)	SQ YD	224	190	34			
X0325765	ANTHCING SURFACE OVERLAY	SQ YD	7,210				3,860	3,350
X0325775	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4 INCH	FOOT	177,650	92,398	85,252			
X0325864	BRIDGE APPROACH PAVEMENT REMOVAL	SQ YD	1,156	565	591			
X0325876	WET REFLECTIVE TEMPORARY TAPE TYPE III, 8 INCH	FOOT	27,843	14,379	13,464			
X0326107	WET REFLECTIVE TEMPORARY TAPE, TYPE III, 5 INCH	FOOT	45,557	23,307	22,250			
50600300	CLEANING AND PAINTING STEEL BRIDGE	L SUM	1					1
X4066580	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80	TON	11,816	5,880	5,936			
X4066685	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80	TON	11,774	5,859	5,915			
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	5,898	2,974	2,924			
50500715	JACK AND REMOVE EXISTING BEARINGS	EACH	72				36	36
X6050700	REMOVE INLET BOX	EACH	6	3	3			
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	0.5	0.5			
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	250	125	125			
* X8730312	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 18 4/C, TWISTED, SHIELDED	FOOT	2,018	1,165	853			
* X8850102	INDUCTION LOOP	FOOT	710	355	355			
XX003988	TEMPORARY CONCRETE BARRIER REMOVAL	FOOT	682	682				
Z0010400	CLEANING BRIDGE SEATS	SQ FT	1,191				581	610
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	0.5	0.5			
Z0016200	DECK SLAB REPAIR (PARTIAL)	SQ YD	361				193	168
Z0017202	DOWEL BARS 1 1/2"	EACH	24		24			
NP Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	201	110	91			
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	11	6	5			
Z0030260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	12	6	6			
Z0030330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	2	1	1			
Z0076600	TRAINEES	hour	3500	3500				

\* SPECIALTY ITEM

NP = Non-participating  
 © Y080

FILE NAME = D:\62021-006-900.dgn

USER NAME = dwozniarski

DESIGNED -

REVISED -

DRAWN - DMW

REVISED -

CHECKED -

REVISED -

DATE - 2/7/2009

REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BISHOP FORD EXPRESSWAY (I-94)  
 SUMMARY OF QUANTITIES

SCALE: N/A

SHEET NO. 4 OF 4 SHEETS STA. TO STA.

F.A.I.  
 RTE.

SECTION

COUNTY

TOTAL SHEETS

SHEET NO.

94

(0405,ETC,42-7&0707)RS-11

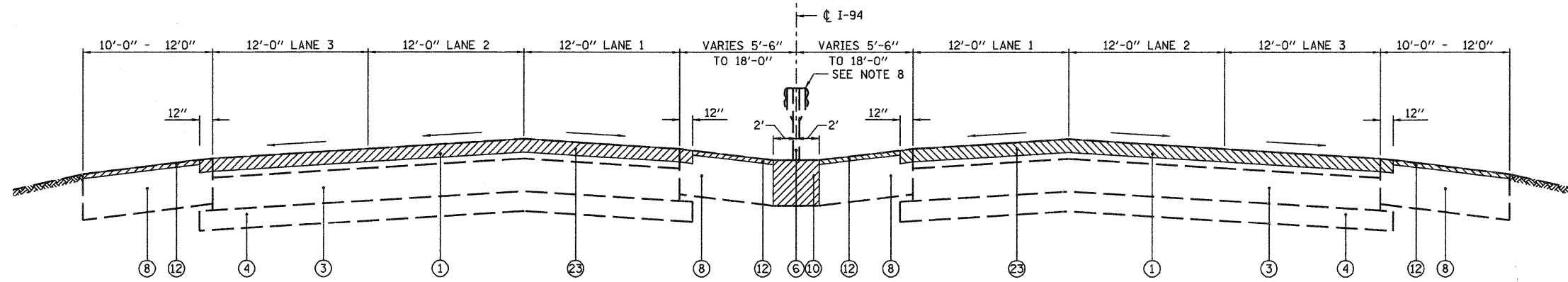
COOK

133

6

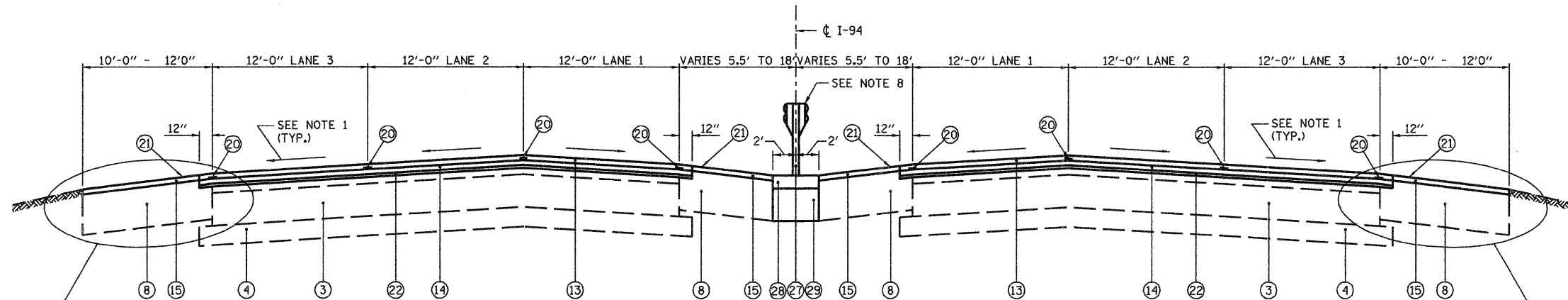
CONTRACT NO. 60D21

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



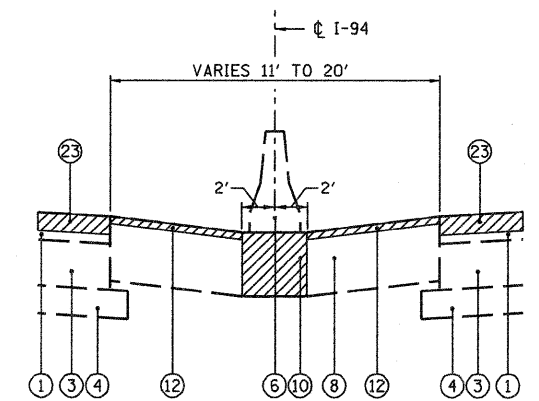
**I-94 EXISTING TYPICAL SECTION**

STA 485+91 TO 568+88  
STA 583+27 TO 607+28

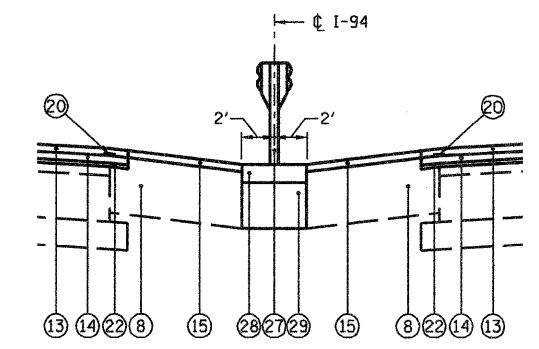


**I-94 PROPOSED TYPICAL SECTION**

STA 485+91 TO 568+88  
STA 583+27 TO 607+28



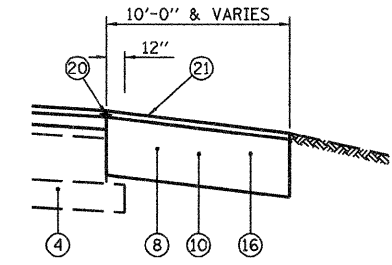
**EXISTING**



**PROPOSED**

**MEDIAN TREATMENT DETAIL**

STA 489+09± TO 491+09±  
STA 491+47± TO STA 493+47±  
STA 485+91± TO 488+71± EXISTING TEMPORARY CONCRETE BARRIER TO BE REMOVED AND REPLACED WITH NEW F-SHAPE. SEE BARRIER SCHEDULE.



**AREAS OF SHOULDER RECONSTRUCTION**

STA 545+00 TO 590+00 RT  
STA 560+00 TO 598+00 LT (MIRROR IMAGE)

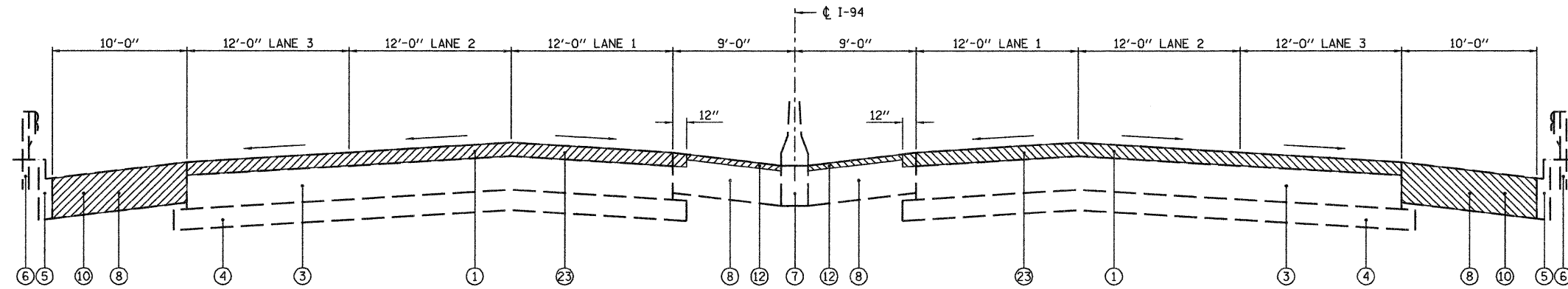
**LEGEND**

- ① EXISTING HMA OVERLAY, VARIES 4"-6"
- ② EXISTING HMA OVERLAY, 3"
- ③ EXISTING P.C.C. BASE, 10"
- ④ EXISTING GRANULAR SUB-BASE
- ⑤ EXISTING CURB & GUTTER, TYPE VARIES
- ⑥ EXISTING STEEL PLATE BEAM GUARDRAIL OR TEMPORARY CONCRETE BARRIER
- ⑦ EXISTING MEDIAN BARRIER WALL, WIDTH VARIES
- ⑧ EXISTING BITUMINOUS SHOULDER, VARIES 10"-17"
- ⑨ EXISTING AGGREGATE SHOULDER, VARIES 10"-13"
- ⑩ PAVED SHOULDER REMOVAL
- ⑪ HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"
- ⑬ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- ⑭ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- ⑮ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- ⑯ HOT-MIX ASPHALT SHOULDERS, 14" (IN 5 LIFTS)
- ⑰ HOT-MIX ASPHALT SHOULDERS, 10" (IN 3 LIFTS)
- ⑱ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/4"
- ⑲ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑳ STRIP REFLECTIVE CRACK CONTROL TREATMENT SYSTEM A
- ㉑ PROPOSED SHOULDER RUMBLE STRIPS (STD. 642001-01)
- ㉒ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ㉓ HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4"
- ㉔ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ㉕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ㉖ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ㉗ PROPOSED STEEL PLATE BEAM GUARDRAIL (SEE SCHEDULE)
- ㉘ STABILIZED MEDIAN SURFACE
- ㉙ SUB-BASE GRANULAR MATERIAL, TYPE C

**NOTES:**

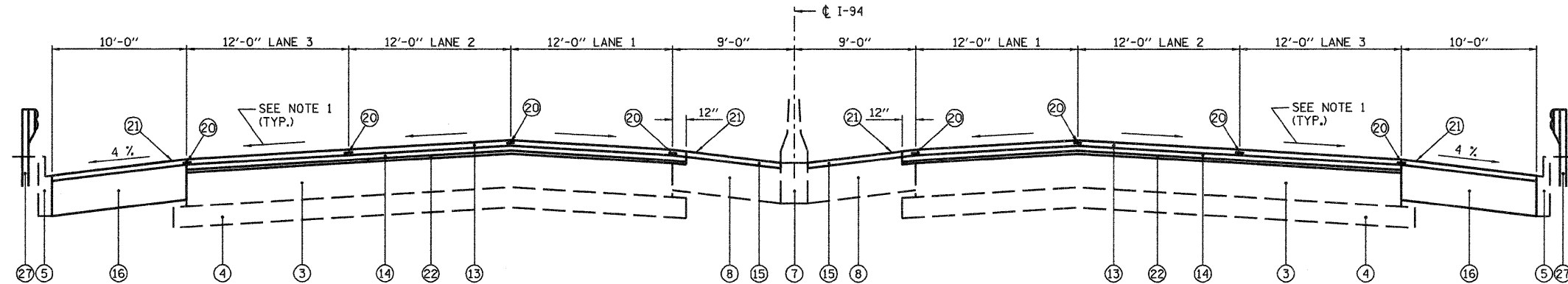
1. THE CONTRACTOR SHALL MAINTAIN THE EXISTING CROSS SLOPES.
2. THE EXISTING HMA IN THE FLAG OF THE CURB AND GUTTER SHALL BE REMOVED AND REPLACED AT NO ADDITIONAL COST. THESE COSTS SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS MILLING AND RESURFACING OPERATIONS MEASURED TO THE EDGE OF THE SHOULDER.
3. THE COST OF HAND REMOVAL OF HMA SURFACE AROUND CASTINGS SHALL BE INCLUDED IN THE COST OF HMA SURFACE REMOVAL OF THE THICKNESS SPECIFIED.
4. HOT-MIX ASPHALT SHOULDERS, 14" CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4" AND HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4".
5. HOT-MIX ASPHALT SHOULDERS, 10" CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4" AND HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4".
6. STABILIZED MEDIAN SURFACE CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 4".
7. TYPICAL SECTION OMISSION STA 568+88 TO 572+91 (SN 016-0162 AND APPROACHES) AND STA 579+59 TO 583+27 (SN 016-0163 AND APPROACHES).
8. SEE MEDIAN TREATMENT DETAIL FOR VARYING CONDITIONS.

FILE NAME = D168021-007-TYPICAL.dgn	USER NAME = dwozmlarski	DESIGNED - RAC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) EXISTING &amp; PROPOSED TYPICAL SECTIONS</b>		F.A.I. RTE. 94	SECTION 10312-708W, ETC. & 13151RS-4	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 7	
PLOT SCALE = 10.0000' / IN.	CHECKED - RAC	DATE - 02/06/09	REVISED -		SCALE:	SHEET NO. 1 OF 4 SHEETS	STA. 485+91 TO STA. 607+28	CONTRACT NO. 60D21				
PLOT DATE = 2/8/2009	DATE - 02/06/09	REVISED -	REVISED -		FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT							



**I-94 EXISTING TYPICAL SECTION**

STA 572+91 TO 579+59



**I-94 PROPOSED TYPICAL SECTION**

STA 572+91 TO 579+59

**LEGEND**

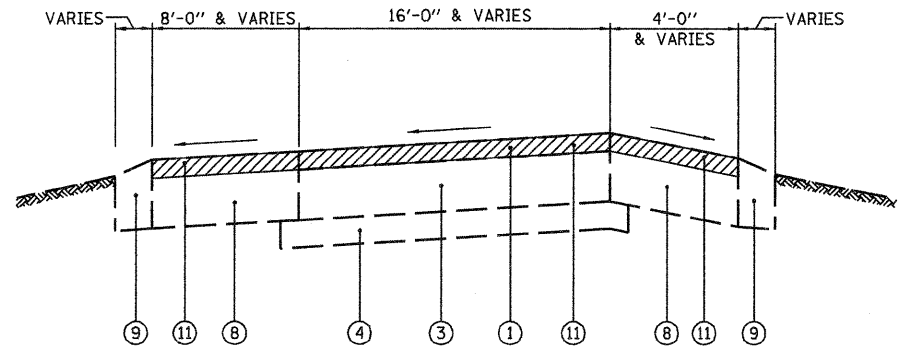
- ① EXISTING HMA OVERLAY, VARIES 4"-6"
- ② EXISTING HMA OVERLAY, 3"
- ③ EXISTING P.C.C. BASE, 10"
- ④ EXISTING GRANULAR SUB-BASE
- ⑤ EXISTING CURB & GUTTER, TYPE VARIES
- ⑥ EXISTING STEEL PLATE BEAM GUARDRAIL OR TEMPORARY CONCRETE BARRIER
- ⑦ EXISTING MEDIAN BARRIER WALL, WIDTH VARIES
- ⑧ EXISTING BITUMINOUS SHOULDER, VARIES 10"-17"
- ⑨ EXISTING AGGREGATE SHOULDER, VARIES 10"-13"
- ⑩ PAVED SHOULDER REMOVAL
- ⑪ HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"
- ⑬ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- ⑭ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- ⑮ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- ⑯ HOT-MIX ASPHALT SHOULDERS, 14" (IN 5 LIFTS)
- ⑰ HOT-MIX ASPHALT SHOULDERS, 10" (IN 3 LIFTS)
- ⑱ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/4"
- ⑲ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑳ STRIP REFLECTIVE CRACK CONTROL TREATMENT SYSTEM A
- ㉑ PROPOSED SHOULDER RUMBLE STRIPS (STD. 642001-01)
- ㉒ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ㉓ HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4"
- ㉔ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ㉕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ㉖ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ㉗ PROPOSED STEEL PLATE BEAM GUARDRAIL (SEE SCHEDULE)
- ㉘ STABILIZED MEDIAN SURFACE
- ㉙ SUB-BASE GRANULAR MATERIAL, TYPE C

**NOTES:**

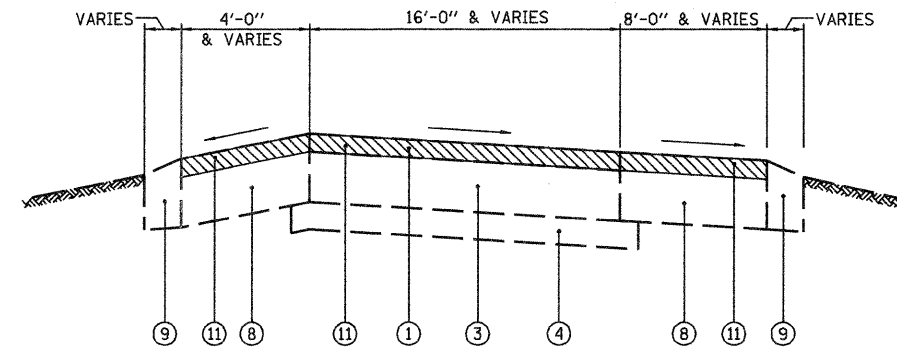
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2. THE EXISTING HMA IN THE FLAG OF THE CURB AND GUTTER SHALL BE REMOVED AND REPLACED AT NO ADDITIONAL COST. THESE COSTS SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS MILLING AND RESURFACING OPERATIONS MEASURED TO THE EDGE OF THE SHOULDER.
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6. STABILIZED MEDIAN SURFACE CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 4".
7. TYPICAL SECTION OMISSION STA 568+88 TO 572+91 (SN 016-0162 AND APPROACHES) AND STA 579+59 TO 583+27 (SN 016-0163 AND APPROACHES).
8. SEE MEDIAN TREATMENT DETAIL FOR VARYING CONDITIONS.

FILE NAME = D168021-088-TYPICAL.dgn	USER NAME = dwozmlarski	DESIGNED - RAC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) EXISTING &amp; PROPOSED TYPICAL SECTIONS</b>	F.A.I. RTE. 94	SECTION 10312-708W. ETC. & 1315RS-4	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 8	
PLOT SCALE = 10.0000' / 1" IN.		CHECKED - RAC	REVISED -		SCALE:	SHEET NO. 2 OF 4 SHEETS	STA. 485+91 TO STA. 607+28		CONTRACT NO. 60D21		
PLOT DATE = 2/8/2009		DATE - 02/06/09	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT						





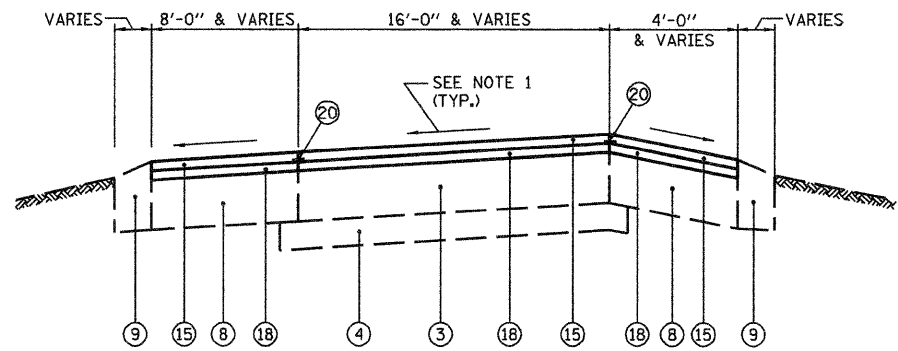
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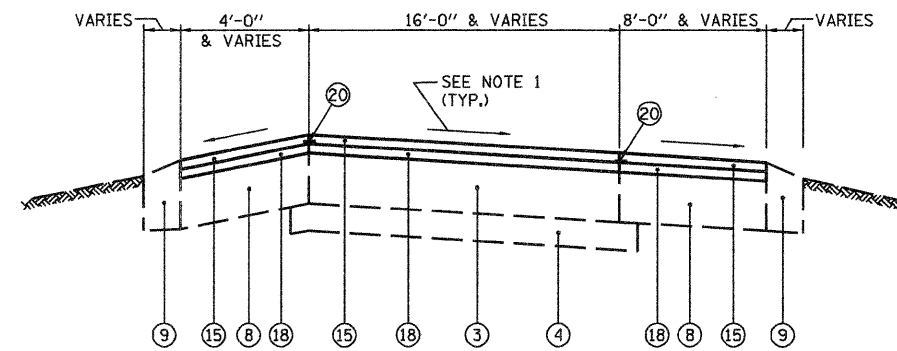
**INTERIOR RAMP**

**I-94 EXISTING RAMP TYPICAL SECTIONS**

SIBLEY BLVD RAMPS



**EXTERIOR RAMP**



**INTERIOR RAMP**

**I-94 PROPOSED RAMP TYPICAL SECTIONS**

SIBLEY BLVD RAMPS

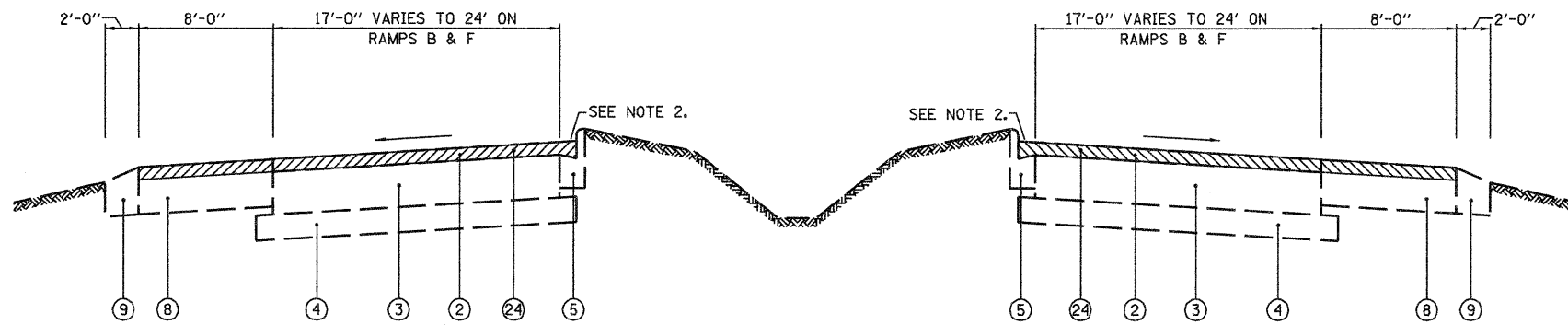
**LEGEND**

- |   |  |
|---|--|
| <ul style="list-style-type: none"> <li>① EXISTING HMA OVERLAY, VARIES 4"-6"</li> <li>② EXISTING HMA OVERLAY, 3"</li> <li>③ EXISTING P.C.C. BASE, 10"</li> <li>④ EXISTING GRANULAR SUB-BASE</li> <li>⑤ EXISTING CURB &amp; GUTTER, TYPE VARIES</li> <li>⑥ EXISTING STEEL PLATE BEAM GUARDRAIL OR TEMPORARY CONCRETE BARRIER</li> <li>⑦ EXISTING MEDIAN BARRIER WALL, WIDTH VARIES</li> <li>⑧ EXISTING BITUMINOUS SHOULDER, VARIES 10"-17"</li> <li>⑨ EXISTING AGGREGATE SHOULDER, VARIES 10"-13"</li> <li>⑩ PAVED SHOULDER REMOVAL</li> <li>⑪ HOT-MIX ASPHALT SURFACE REMOVAL, 4"</li> <li>⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"</li> <li>⑬ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"</li> <li>⑭ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"</li> <li>⑮ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"</li> </ul> | <ul style="list-style-type: none"> <li>⑯ HOT-MIX ASPHALT SHOULDERS, 14" (IN 5 LIFTS)</li> <li>⑰ HOT-MIX ASPHALT SHOULDERS, 10" (IN 3 LIFTS)</li> <li>⑱ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/4"</li> <li>⑲ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B</li> <li>⑳ STRIP REFLECTIVE CRACK CONTROL TREATMENT SYSTEM A</li> <li>㉑ PROPOSED SHOULDER RUMBLE STRIPS (STD. 642001-01)</li> <li>㉒ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"</li> <li>㉓ HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4"</li> <li>㉔ HOT-MIX ASPHALT SURFACE REMOVAL, 3"</li> <li>㉕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"</li> <li>㉖ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"</li> <li>㉗ PROPOSED STEEL PLATE BEAM GUARDRAIL (SEE SCHEDULE)</li> <li>㉘ STABILIZED MEDIAN SURFACE</li> <li>㉙ SUB-BASE GRANULAR MATERIAL, TYPE C</li> </ul> |
|---|--|

**NOTES:**

1. THE CONTRACTOR SHALL MAINTAIN THE EXISTING CROSS SLOPES.
2. THE EXISTING HMA IN THE FLAG OF THE CURB AND GUTTER SHALL BE REMOVED AND REPLACED AT NO ADDITIONAL COST. THESE COSTS SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS MILLING AND RESURFACING OPERATIONS MEASURED TO THE EDGE OF THE SHOULDER.
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5. HOT-MIX ASPHALT SHOULDERS, 10" CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4" AND HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4".
6. STABILIZED MEDIAN SURFACE CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 4".
7. TYPICAL SECTION OMISSION STA 568+88 TO 572+91 (SN 016-0162 AND APPROACHES) AND STA 579+59 TO 583+27 (SN 016-0163 AND APPROACHES).
8. SEE MEDIAN TREATMENT DETAIL FOR VARYING CONDITIONS.

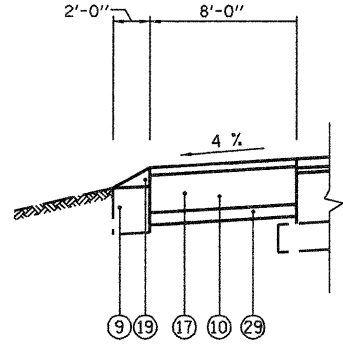
FILE NAME = D168021-809-TYPICAL.dgn	USER NAME = dwoznaraki	DESIGNED - RAC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) EXISTING &amp; PROPOSED TYPICAL SECTIONS</b>	F.A.I. RTE. 94	SECTION 10312-708W, ETC. & 1315RS-4	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 9	
PLOT SCALE = 10.0000' / 1" / IN.					DRAWN - SAW	SCALE:	SHEET NO. 3 OF 4 SHEETS		STA. 485+91 TO STA. 607+28		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT
PLOT DATE = 2/8/2009					CHECKED - RAC			CONTRACT NO. 60D21			
					DATE - 02/06/09						



**EXTERIOR RAMPS AT 159TH ST.  
RAMPS B, D, F, & H**

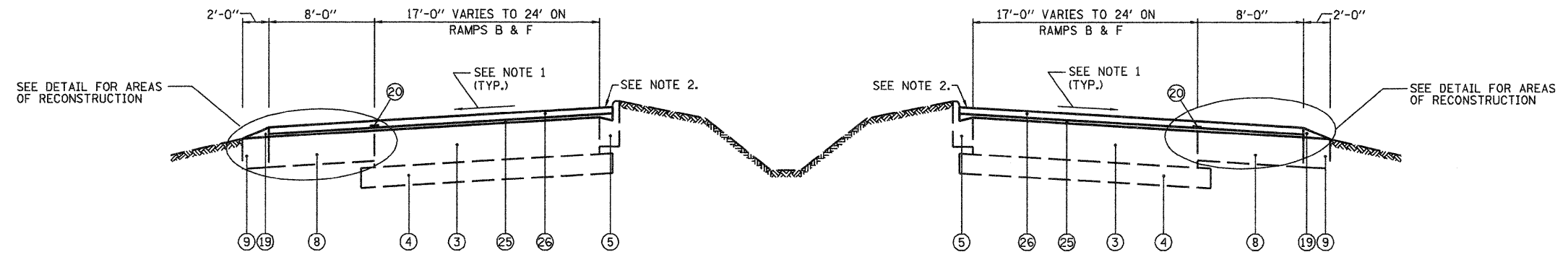
**INTERIOR RAMPS AT 159TH ST.  
RAMPS A, C, E, & G**

**I-94 EXISTING TYPICAL SECTIONS**  
US 6/159TH ST. RAMPS



**AREAS OF SHOULDER RECONSTRUCTION  
EXTERIOR RAMPS AT 159TH ST.  
RAMPS D & F**

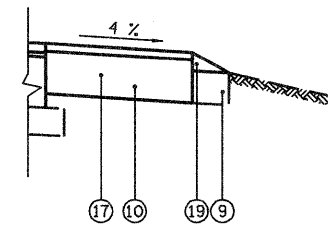
FOR LOCATIONS OF SHOULDER RECONSTRUCTION, SEE THE RESURFACING PLAN SHEETS.



**EXTERIOR RAMPS AT 159TH ST.  
RAMPS B, D, F, & H**

**INTERIOR RAMPS AT 159TH ST.  
RAMPS A, C, E, & G**

**I-94 PROPOSED TYPICAL SECTIONS**  
US 6/159TH ST. RAMPS



**AREAS OF SHOULDER RECONSTRUCTION  
INTERIOR RAMPS AT 159TH ST.  
RAMPS A, C, E, & G**

FOR LOCATIONS OF SHOULDER RECONSTRUCTION, SEE THE RESURFACING PLAN SHEETS.

**LEGEND**

- ① EXISTING HMA OVERLAY, VARIES 4"-6"
- ② EXISTING HMA OVERLAY, 3"
- ③ EXISTING P.C.C. BASE, 10"
- ④ EXISTING GRANULAR SUB-BASE
- ⑤ EXISTING CURB & GUTTER, TYPE VARIES
- ⑥ EXISTING STEEL PLATE BEAM GUARDRAIL OR TEMPORARY CONCRETE BARRIER
- ⑦ EXISTING MEDIAN BARRIER WALL, WIDTH VARIES
- ⑧ EXISTING BITUMINOUS SHOULDER, VARIES 10"-17"
- ⑨ EXISTING AGGREGATE SHOULDER, VARIES 10"-13"
- ⑩ PAVED SHOULDER REMOVAL
- ⑪ HOT-MIX ASPHALT SURFACE REMOVAL, 4"
- ⑫ HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"
- ⑬ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- ⑭ POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1 3/4"
- ⑮ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- ⑯ HOT-MIX ASPHALT SHOULDERS, 14" (IN 5 LIFTS)
- ⑰ HOT-MIX ASPHALT SHOULDERS, 10" (IN 3 LIFTS)
- ⑱ HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2 1/4"
- ⑲ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- ⑳ STRIP REFLECTIVE CRACK CONTROL TREATMENT SYSTEM A
- ㉑ PROPOSED SHOULDER RUMBLE STRIPS (STD. 642001-01)
- ㉒ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- ㉓ HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4"
- ㉔ HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ㉕ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- ㉖ HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
- ㉗ PROPOSED STEEL PLATE BEAM GUARDRAIL (SEE SCHEDULE)
- ㉘ STABILIZED MEDIAN SURFACE
- ㉙ SUB-BASE GRANULAR MATERIAL, TYPE C

**NOTES:**

1. THE CONTRACTOR SHALL MAINTAIN THE EXISTING CROSS SLOPES.
2. THE EXISTING HMA IN THE FLAG OF THE CURB AND GUTTER SHALL BE REMOVED AND REPLACED AT NO ADDITIONAL COST. THESE COSTS SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS MILLING AND RESURFACING OPERATIONS MEASURED TO THE EDGE OF THE SHOULDER.
3. THE COST OF HAND REMOVAL OF HMA SURFACE AROUND CASTINGS SHALL BE INCLUDED IN THE COST OF HMA SURFACE REMOVAL OF THE THICKNESS SPECIFIED.
4. HOT-MIX ASPHALT SHOULDERS, 14" CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4" AND HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4".
5. HOT-MIX ASPHALT SHOULDERS, 10" CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4" AND HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4".
6. STABILIZED MEDIAN SURFACE CONSISTS OF HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 4".
7. TYPICAL SECTION OMISSION STA 568+88 TO 572+91 (SN 016-0162 AND APPROACHES) AND STA 579+59 TO 583+27 (SN 016-0163 AND APPROACHES).
8. SEE MEDIAN TREATMENT DETAIL FOR VARYING CONDITIONS.

FILE NAME = D168021-010-TYPICAL.dgn	USER NAME = dwoznibrski	DESIGNED - RAC	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) EXISTING &amp; PROPOSED TYPICAL SECTIONS</b>			F.A.I. RTE. 94	SECTION 10312-708W, ETC. & 1315RS-4	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 10
	PLOT SCALE = 10.0000' / IN.	DRAWN - SAW	REVISED -		SCALE:	SHEET NO. 4 OF 4 SHEETS	STA. 485+91 TO STA. 607+28	CONTRACT NO. 60D21				
	PLOT DATE = 2/8/2009	CHECKED - RAC	REVISED -					FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				
		DATE - 02/06/09	REVISED -									





CONTRACT 60D21  
MAINLINE CLASS D PATCHING SCHEDULE

WESTBOUND MAINLINE CONTINUED

PATCH NUMBER	STATION	LANE NUMBER	LENGTH (FT)	WIDTH (FT)	LANE 1			LANE 2			LANE 3			AUX			TOTAL					
					CLASS D, 10"			CLASS D, 10"			CLASS D, 10"			CLASS D, 10" TYPE II	HMA RMOV PAT 6	HMA REPL OVER PATCH	CLASS D, 10"			HMA RMOV PAT 6	HMA REPL OVER PATCH	
					TYPE II	TYPE III	TYPE IV	TYPE II	TYPE III	TYPE IV	TYPE II	TYPE III	TYPE IV				TYPE II	TYPE III	TYPE IV			
59W	591+37	1	8	12														10.7			13.3	4.5
60W	591+37	2	8	12				10.7			13.3	4.5						10.7			13.3	4.5
61W	591+37	3	8	12						10.7			13.3	4.5				10.7			13.3	4.5
62W	602+47	1	8	12	10.7													10.7			13.3	4.5
63W	602+47	2	8	12				10.7			13.3	4.5						10.7			13.3	4.5
64W	602+47	3	8	12						10.7			13.3	4.5				10.7			13.3	4.5
65W	602+47	4/Aux	8	12											10.7	13.3	4.5	10.7			13.3	4.5
66W	603+09	1	10	12	13.4													13.4			16.0	5.4
67W	603+09	2	10	12				13.4			16.0	5.4						13.4			16.0	5.4
68W	604+05	1	6	12	8.0													8.0			10.7	3.6
69W	604+05	2	6	12				8.0			10.7	3.6						8.0			10.7	3.6
70W	604+85	2	4	12				5.4			8.0	2.7						5.4			8.0	2.7
71W	604+85	3	4	12						5.4			8.0	2.7				5.4			8.0	2.7
72W	604+85	4/Aux	4	12											5.4	8.0	2.7	5.4			8.0	2.7
SUBTOTAL WESTBOUND																	533.5	141.6	317.6	1,176.7	395.4	
ROUNDED SUBTOTAL WESTBOUND																	534	142	318	1,177	396	

CONTRACT 60D21  
RAMPS CLASS D PATCHING SCHEDULE

WESTBOUND RAMPS

Patch Number	LOCATION	LENGTH (FT)	WIDTH (FT)	CLASS D, 10"	HMA RMOV PAT 4	HMA REPL OVER PATCH
				TYPE II (SY)	(SY)	(TON)
R1	US 6 - RAMP D	5	19	10.6	14.8	3.3
ROUNDED SUBTOTAL WESTBOUND RAMPS				11	15	4

CONTRACT 60D21  
CLASS D PATCHING SUMMARY

SCHEDULE	CLASS D, 10"			HMA RMOV PAT 4	HMA RMOV PAT 6	HMA REPL OVER PATCH
	TYPE II (SY)	TYPE III (SY)	TYPE IV (SY)	(SY)	(SY)	(TON)
EASTBOUND MAINLINE (SHEET 1 OF 6)	327	179	182		818	275
WESTBOUND MAINLINE	534	142	318		1,177	396
WESTBOUND RAMPS	11			15		4
TOTAL	872	321	500	15	1,995	675

CONTRACT 60D21  
CLASS B PATCHES, 10"

WESTBOUND RAMPS

Patch Number	LOCATION	LENGTH (FT)	WIDTH (FT)	TYPE II (SY)	SAW CUTS (FT)	DOWEL (EA)
R2	US 6 - RAMP D	6	13.75	9.2	59.3	24
ROUNDED TOTAL				10	60	24

**CONTRACT 60D21  
RESURFACING SCHEDULE**

**EASTBOUND**

LOCATION		POLYMERIZED HMA SURFACE COURSE, SMA, N80	HMA SURFACE COURSE, MIX "D", N70	POLYMERIZED HMA BINDER COURSE, SMA, N80	HMA BINDER COURSE, IL-19, N70	POLYMERIZED LEVELING BINDER(MACHINE METHOD), IL- 4.75,N50	HMA SURFACE REMOVAL, 1 3/4"	HMA SURFACE REMOVAL, 3"	HMA SURFACE REMOVAL, 4"	HMA SURFACE REMOVAL, 4 1/4"	DESCRIPTION
STA	STA	(TON)	(TON)	(TON)	(TON)	(TON)	(SY)	(SY)	(SY)	(SY)	
485+91	500+00		168.8				1,722.1				OUTSIDE SHOULDER
500+00	512+36		140.0				1,428.4				OUTSIDE SHOULDER
513+63	517+88		50.9				519.1				OUTSIDE SHOULDER
524+00	531+05		100.8				1,028.3				OUTSIDE SHOULDER
531+98	568+58		436.8				4,457.5				OUTSIDE SHOULDER
572+63.1	579+41.6		73.9				753.9				OUTSIDE SHOULDER
583+08	592+26		115.0				1,173.1				OUTSIDE SHOULDER
593+30	604+23		105.0				1,071.3				OUTSIDE SHOULDER
606+05	607+28		25.5				259.7				OUTSIDE SHOULDER
485+91	569+04		582.8				5,947.1				INSIDE SHOULDER
572+45	580+00		57.5				587.2				INSIDE SHOULDER
583+06	607+28		278.7				2,843.9				INSIDE SHOULDER
GORE AREAS			201.0			8.0	1,887.8	142.7			
US 6 RAMP A			375.0			187.5		3,348.4			RAMPS & RAMP AREA
US 6 RAMP G			475.4			237.7		4,244.7			RAMPS & RAMP AREA
US 6 RAMP H			300.5			150.2		2,682.8			RAMPS & RAMP AREA
US 6 RAMP B			429.2			214.6		3,832.3			RAMPS & RAMP AREA
SIBLEY RAMP A			39.6		50.9				404.1		RAMPS & RAMP AREA
SIBLEY RAMP B			66.3		85.3				676.7		RAMPS & RAMP AREA
SIBLEY RAMP G			77.8		100.0				793.8		RAMPS & RAMP AREA
SIBLEY RAMP H			124.5		160.1				1,270.5		RAMPS & RAMP AREA
US 6 AREAS ADJACENT TO RAMPS			170.5			85.2		1,522.2			
485+91	568+55	4,110.4		4,110.4		1,461.5				34,797.4	MAINLINE
573+21	579+41	301.1		301.1		107.1				2,548.9	MAINLINE
583+42	607+28	1,181.4		1,181.4		420.0				10,001.1	MAINLINE
TAPERED AREA		147.7		147.7		52.5				1,250.5	MAINLINE
AUXILIARY LANES		138.4		138.4		49.2				1,171.9	MAINLINE
DEDUCTION				(20.8)							FOR BUTT JOINT
SUBTOTAL		5,879.1	4,395.4	5,858.2	396.3	2,973.6	23,679.4	15,773.1	3,145.1	49,769.8	
ROUNDED SUBTOTAL		5,880	4,396	5,859	397	2,974	23,680	15,774	3,146	49,770	

**WESTBOUND**

LOCATION		POLYMERIZED HMA SURFACE COURSE, SMA, N80	HMA SURFACE COURSE, MIX "D", N70	POLYMERIZED HMA BINDER COURSE, SMA, N80	HMA BINDER COURSE, IL-19, N70	POLYMERIZED LEVELING BINDER(MACHINE METHOD), IL- 4.75,N50	HMA SURFACE REMOVAL, 1 3/4"	HMA SURFACE REMOVAL, 3"	HMA SURFACE REMOVAL, 4"	HMA SURFACE REMOVAL, 4 1/4"	DESCRIPTION
STA	STA	(TON)	(TON)	(TON)	(TON)	(TON)	(SY)	(SY)	(SY)	(SY)	
485+91	515+39		342.0				3,489.7				OUTSIDE SHOULDER
515+39	569+15		605.2				6,175.0				OUTSIDE SHOULDER
573+18	579+75		85.8				875.9				OUTSIDE SHOULDER
583+43	607+28		267.0				2,724.4				OUTSIDE SHOULDER
485+91	569+04		693.5				7,076.2				INSIDE SHOULDER
572+45	580+00		65.8				671.1				INSIDE SHOULDER
583+06	607+28		355.6				3,628.7				INSIDE SHOULDER
GORE AREAS			146.1			11.1	1,264.9	197.9			
SHLDR AREAS			31.6				322.0				
US 6 RAMP E			438.4			219.2		3,914.4			RAMPS & RAMP AREA
US 6 RAMP C			417.7			208.8		3,729.4			RAMPS & RAMP AREA
US 6 RAMP D			392.9			196.5		3,508.4			RAMPS & RAMP AREA
US 6 Ramp F			218.1			109.1		1,947.7			RAMPS & RAMP AREA
SIBLEY RAMP C			67.1		86.3				685.1		RAMPS & RAMP AREA
SIBLEY RAMP D			114.4		147.0				1,167.0		RAMPS & RAMP AREA
SIBLEY RAMP E			46.2		59.4				471.2		RAMPS & RAMP AREA
SIBLEY RAMP F			62.8		80.8				641.0		RAMPS & RAMP AREA
US 6 AREAS ADJACENT TO RAMPS			136.2			68.1		1,216.5			
485+91	568+55	4,110.4		4,110.4		1,461.5				34,797.4	MAINLINE
573+21	579+41	301.1		301.1		107.1				2,548.9	MAINLINE
583+42	607+28	1,181.4		1,181.4		420.0				10,001.1	MAINLINE
TAPERED AREA		208.8		208.8		74.2				1,767.3	MAINLINE
AUXILIARY LANES		133.8		133.8		47.6				1,132.6	MAINLINE
DEDUCTION				(20.8)							FOR BUTT JOINT
SUBTOTAL		5,935.5	4,486.4	5,914.6	373.5	2,923.2	26,227.9	14,514.3	2,964.3	50,247.3	
ROUNDED SUBTOTAL		5,936	4,487	5,915	374	2,924	26,228	14,515	2,965	50,248	

TOTAL	11,816	8,883	11,774	771	5,898	49,908	30,289	6,111	100,018
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FILE NAME =  
D168021-014-SCHEDULE.dgn

USER NAME = dwozmarski  
DESIGNED -  
DRAWN - DMW  
CHECKED -  
PLOT SCALE = N/A  
PLOT DATE = 2/6/2009

REVISED -  
REVISED -  
REVISED -  
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BISHOP FORD EXPRESSWAY (I-94)  
SCHEDULES OF QUANTITIES**  
SCALE: N/A SHEET NO. 4 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	(0405,ETC,42-7&0707RS-11)	COOK	133	14
CONTRACT NO. 60D21				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

CONTRACT 60D21  
HOT MIX ASPHALT SHOULDER REMOVAL AND REPLACEMENT (SPECIAL)

EASTBOUND RAMPS

DESCRIPTION	LENGTH (FT)	WIDTH (FT)	AREA (SY)
US 6 - RAMP G SHOULDER	569	3	189.7
ROUNDED SUBTOTAL			190

WESTBOUND RAMPS

DESCRIPTION	LENGTH (FT)	WIDTH (FT)	AREA (SY)
US 6 - RAMP D SHOULDER	100	3	33.4
ROUNDED SUBTOTAL			34

TOTAL	224
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CONTRACT 60D21  
PAVED SHOULDER REMOVAL

EASTBOUND MAINLINE OUTSIDE SHOULDER

FROM STA	TO STA	AREA (SY)
545+00.0	568+85.0	2,639.7
572+29.3	579+70.0	880.6
582+66.6	590+00.0	955.8

EASTBOUND MAINLINE INSIDE SHOULDER

FROM STA	TO STA	AREA (SY)
568+84.1	569+16.3	25.6
572+55.6	572+87.6	26.3
579+56.5	579+87.1	24.5
582+80.7	583+23.6	35.0

EASTBOUND RAMPS

LOCATION	AREA (SY)
US 6 - RAMP A	648.9
US 6 - RAMP G	26.7

FROM BARRIER AND GUARD RAIL SCHEDULE	4,794
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SUBTOTAL EASTBOUND	10,057.1
ROUNDED SUBTOTAL EB	10,058

WESTBOUND MAINLINE OUTSIDE SHOULDER

FROM STA	TO STA	AREA (SY)
560+00.0	569+49.0	1,300.4
572+92.2	580+03.0	797.7
583+04.6	591+20.5	1,064.7
592+31.7	598+00.0	757.7

WESTBOUND MAINLINE INSIDE SHOULDER

FROM STA	TO STA	AREA (SY)
568+90.3	569+22.1	25.8
572+57.3	572+93.1	28.2
579+59.8	579+88.9	23.3
582+81.6	583+27.1	38.7

WESTBOUND RAMPS

LOCATION	AREA (SY)
US 6 - RAMP C	586.7
US 6 - RAMP D	240.0
I-94 / US 6 - RAMP D GORE	161.0
US 6 - RAMP E	608.9
US 6 - RAMP F	246.2

SUBTOTAL WESTBOUND	5,879.3
ROUNDED SUBTOTAL WB	5,880

SCHEDULE TOTAL	15,938
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CONTRACT 60D21  
HOT - MIX ASPHALT SHOULDERS, 14"

EASTBOUND MAINLINE

FROM STA	TO STA	AREA (SY)
545+00.0	568+58.0	3,144.0
572+63.1	579+41.6	753.9
583+08.3	590+00.0	922.3
SUBTOTAL		4,820.2
ROUNDED SUBTOTAL		4,821

WESTBOUND MAINLINE

FROM STA	TO STA	AREA (SY)
560+00.0	569+15.2	1,220.3
573+17.7	579+74.6	729.9
583+42.5	591+20.5	1,025.0
592+31.7	598+00.0	757.7
RAMP D US 6 GORE		161.0
SUBTOTAL		3,893.9
ROUNDED SUBTOTAL		3,894

SCHEDULE TOTAL	8,715
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CONTRACT 60D21  
HOT - MIX ASPHALT SHOULDERS, 10"

EASTBOUND RAMPS

LOCATION	AREA (SY)
US 6 - RAMP A	648.9
US 6 - RAMP G	26.7
SUBTOTAL	675.6
ROUNDED SUBTOTAL	676

WESTBOUND RAMPS

LOCATION	AREA (SY)
US 6 - RAMP C	586.7
US 6 - RAMP D	240.0
US 6 - RAMP E	608.9
US 6 - RAMP F	246.2
SUBTOTAL	1,681.8
ROUNDED SUBTOTAL	1,682

SCHEDULE TOTAL	2,358
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CONTRACT 60D21  
TEMPORARY PAVEMENT MARKING SCHEDULE

STAGE	WET REFL. TEMP TAPE, TYPE III 4"		WET REFL. TEMP TAPE, TYPE III 5"		WET REFL. TEMP TAPE, TYPE III 8"		TEMPORARY PM - LINE 6"		EPOXY PM LINE - 4"		EPOXY PM LINE - 5"		EPOXY PM LINE - 8"		EPOXY PM LINE - 12"		WORK ZONE PM REMOVAL		PAVEMENT MARKING REMOVAL	
	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB	EB	WB
STAGE 1	19,766	12,610	2,307	1,505	2,503	3,558	1,625	1,613									10,086	7,895	4,107	2,649
STAGE 2	6,821	6,435	1,113	863			11,160	11,160									2,990	2,735	2,033	1,437
STAGE 3	65,811	66,207	19,887	19,882	11,876	9,906														
WINTER SHUTDOWN									14,661	10,252	3,767	2,605		453	1,032	1,112				
SUBTOTAL	92,398	85,252	23,307	22,250	14,379	13,464	12,785	12,773	14,661	10,252	3,767	2,605	0	453	1,032	1,112	13,076	10,630	6,140	4,086
TOTAL	177,650		45,557		27,843		25,558		24,913		6,372		453		2,144		23,706		10,226	

FILE NAME =  
D:\60D21-015-SCHEDULE.dgn

USER NAME = dwozniarski  
PLOT SCALE = N/A  
PLOT DATE = 2/6/2009

DESIGNED -  
DRAWN - DMW  
CHECKED -  
DATE - 2/6/2009

REVISED -  
REVISED -  
REVISED -  
REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

BISHOP FORD EXPRESSWAY (I-94)  
SCHEDULES OF QUANTITIES

SCALE: N/A SHEET NO. 5 OF 7 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	10405.ETC.42-T&O707RS-11	COOK	133	15

CONTRACT NO. 60D21  
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT





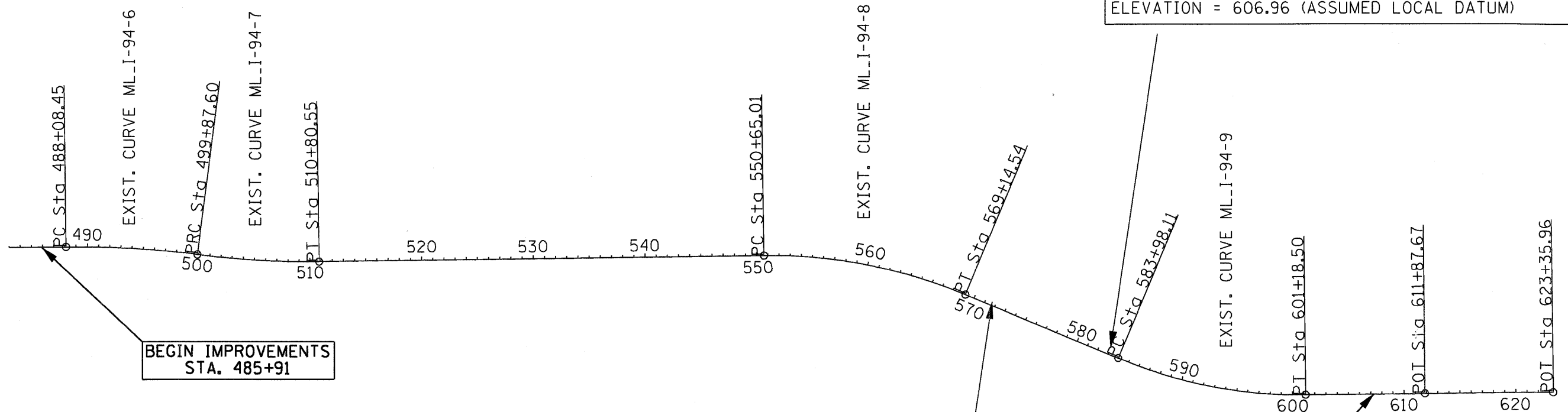
CONTRACT 60D21  
BARRIER AND GUARD RAIL SCHEDULE

LOCATION	LOCATION		(SQ YD)	(FT)	(FT)	(FT)	(FT)	(FT)	(FT)	(EACH)	(EACH)	(EACH)	(EACH)	(EACH)	(FOOT)	(EACH)	(EACH)	(EACH)	(TON)	(SQ YD)	REMARKS	
	STATION	STATION																				
MEDIAN	485+89	488+71			282.0					282.0									24			
	488+71	489+09																	6		EXISTING F-SHAPED SIGN FOUNDATION	
	489+09	491+09	88.9		200.0										2				8	70.9	88.9	WITHOUT BARRIER CURB, TYPE B - 6"
	491+09	491+47																	6		EXISTING F-SHAPED SIGN FOUNDATION	
	491+47	506+74	678.7	1,784.0	200.0										1				16	541.2	678.7	WITHOUT BARRIER CURB, TYPE B - 6"
	506+74	509+87	139.1	550.0				626.0											12	110.9	139.1	SEE GUARD RAIL TRANSITION AT SIGN FOUNDATION DETAIL
	509+87	520+85	976.0	1,363.0											2				14	389.2	976.0	
	520+85	521+67																				SIBLEY BLVD. BRIDGE
	521+67	527+35	252.4	593.0											4				12	201.3	252.4	
	527+35	528+12																				MICHIGAN CITY BRIDGE
	528+12	547+38	856.0	2,102.0											4				18	682.6	856.0	
	547+38	548+02																				154TH ST BRIDGE
	548+02	569+34	947.6	2,132.0											2	1	60.0		20	755.6	947.6	
	569+34	583+27																	112			EXISTING MEDIAN BARRIER
583+27	602+50	854.7	2,915.0											2	1	60.0		18	681.6	854.7		
602+50	603+38																				159TH ST BRIDGE	
603+38	607+28																	32			EXISTING MEDIAN BARRIER	
EASTBOUND	519+42	520+85		143.0		62.5				1			1					1	4			APPROACH TO SIBLEY BLVD BRIDGE
	524+77	526+93								1								1	4			APPROACH TO MICHIGAN CITY BRIDGE - TERMINAL REPLACEMENT ONLY
	545+69	547+37								1								1	4			APPROACH TO 154TH ST BRIDGE - TERMINAL REPLACEMENT ONLY
	559+03	569+02		999.0		925.0				1		1		30.0				1	7			APPROACH TO GREENWOOD AVE BRIDGE
	572+19	579+85		766.0		687.5							2						6			BETWEEN GREENWOOD AVE & LITTLE CALUMET RIVER BRIDGES - WITHOUT BARRIER CURB, TYPE B - 6"
	582+47	591+60		913.0		875.0							1		30.0				7			LITTLE CALUMET RIVER BRIDGE TO 159TH ST RAMP B - ATTACH PROPOSED GUARD RAIL TO EXISTING GUARD RAIL AT RAMP B
	549+42	602+47																	4			APPROACH TO 159TH ST - REFLECTORS ONLY
WESTBOUND	521+67	522+87								1								1	4			APPROACH TO SIBLEY BLVD BRIDGE - TERMINAL REPLACEMENT ONLY
	528+58	530+69																	4			APPROACH TO MICHIGAN CITY RD BRIDGE - REFLECTORS ONLY
	548+01	549+69																	4			APPROACH TO 154TH ST BRIDGE - REFLECTORS ONLY
	559+01	569+34		1,033.0		1,000.0					1	1		30.0					8			DEPARTURE GREENWOOD AVE BRIDGE
	572+75	580+23		748.0		662.5							2						6			BETWEEN GREENWOOD AVE & LITTLE CALUMET RIVER BRIDGES - WITHOUT BARRIER CURB, TYPE B - 6"
	582+84	584+73		189.0		112.5				1				30.0				1	4			APPROACH TO LITTLE CALUMET RIVER BRIDGE
603+38	604+96		158.0		87.5				1									1	4		APPROACH TO 159TH ST	

US 6 RAMPS D & F SHOULDERS 276

SUBTOTAL	4,793.4	16,388.0	682.0	4,412.5	626.0	8,900.0	282.0	7	1	8	16	5	240.0	7	188	180	3,709.3	4,793.4
ROUNDED TOTAL	4,794	16,388	682	4,413	626	8,900	282	7	1	8	16	5	240	7	188	180	3,710	4,794

- NOTES:**
- THE STATIONING SHOWN IN THE BARRIER AND GUARD RAIL SCHEDULE WAS DEVELOPED FROM PREVIOUS PLANIMETRICS AND AERIAL PHOTOGRAPHY FURNISHED BY THE DEPARTMENT AND IS NOT THE RESULT OF A GROUND SURVEY. THE CONTRACTOR SHALL FIELD VERIFY THE ACTUAL LOCATIONS OF BRIDGE PIERS, SIGN FOUNDATIONS, ETC. AND ADJUST THE LENGTH OF GUARD RAIL AS REQUIRED TO MAINTAIN THE INTENT OF THE PLANS.
  - THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES BEFORE BEGINNING GUARD RAIL WORK.
  - THE CONTRACTOR SHALL SUBMIT A PROPOSED METHOD TO ATTACH PROPOSED TEMPORARY CONCRETE BARRIER TO EXISTING CONCRETE PARAPETS.
  - CUTTING OF EITHER EXISTING OR PROPOSED GUARD RAIL AS REQUIRED TO FACILITATE A SPLICE OF GUARD RAIL OR TERMINAL SHALL BE INCLUDED IN THE UNIT PRICE FOR GUARD RAIL OF THE TYPE BEING INSTALLED.



BENCHMARK  
 STRUCTURE NO. 016-0163 (FAI-94 OVER LITTLE CALUMET RIVER) APPROX. STATION 583+00, 56' RT  
 X SCRIBED IN CHISELED BOX ON TOP OF CONCRETE BARRIER WALL AT THE SOUTHEAST CORNER OF BRIDGE STRUCTURE FOR WESTBOUND FAI-94 OVER THE LITTLE CALUMET RIVER. ELEVATION = 606.96 (ASSUMED LOCAL DATUM)

BEGIN IMPROVEMENTS  
 STA. 485+91

END IMPROVEMENTS  
 STA. 607+28

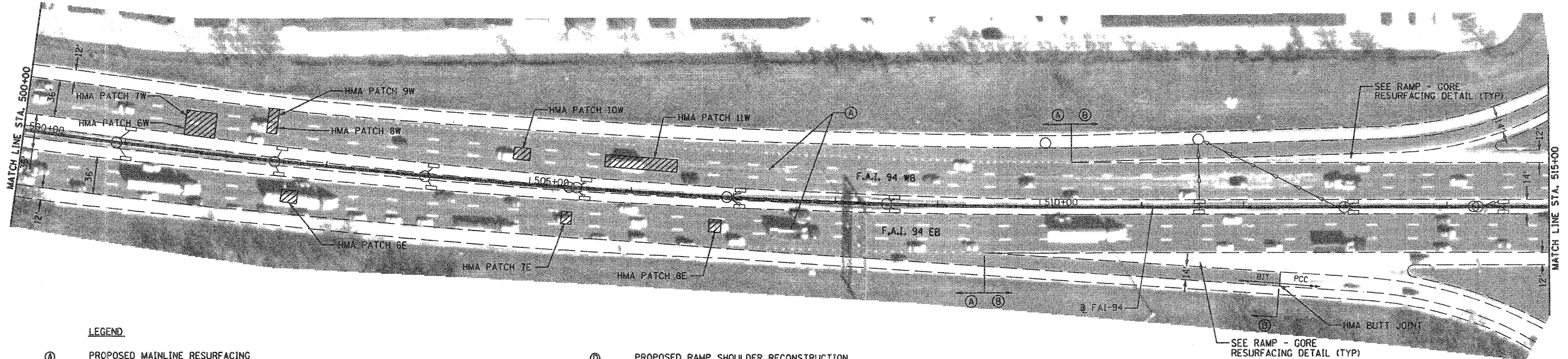
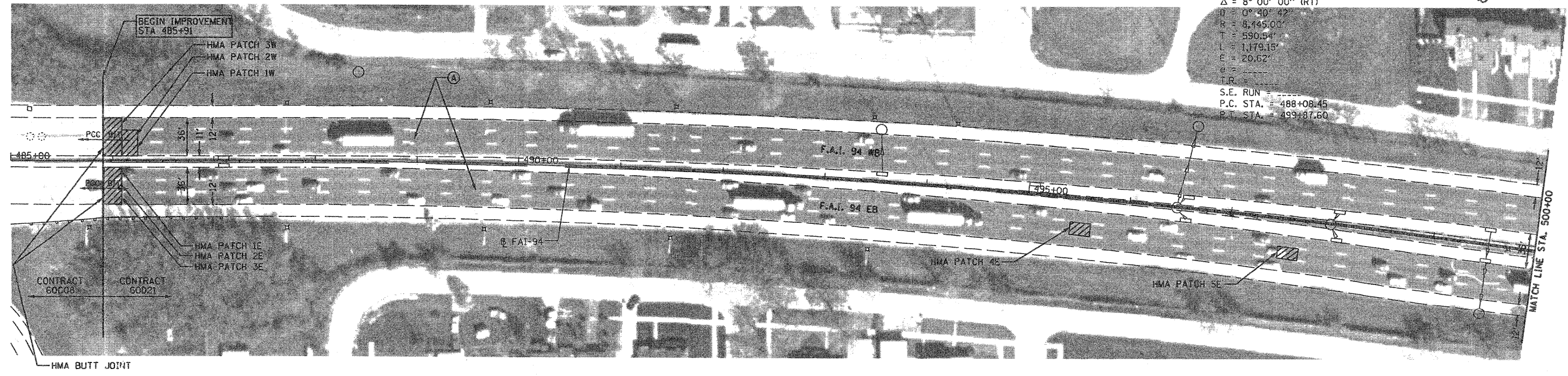
BENCHMARK  
 STRUCTURE NO. 016-0162 (FAI-94 OVER GREENWOOD AVENUE) APPROX. STATION 572+75, 57' RT.  
 X SCRIBED IN CHISELED BOX ON TOP OF CONCRETE BARRIER WALL AT THE SOUTHEAST CORNER OF BRIDGE STRUCTURE FOR WESTBOUND FAI-94 OVER GREENWOOD AVENUE. ELEVATION = 627.12 (ASSUMED LOCAL DATUM)

COORDINATES, BEARINGS AND DISTANCES  
 BASED ON ILLINOIS STATE PLANE (NAD83) ZONE 1201  
 (GPS DERIVED)

FILE NAME = D168021-018-ATB.dgn	USER NAME = dwozniarski	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BISHOP FORD EXPRESSWAY (I-94) ALIGNMENT, TIES AND BENCHMARKS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = N/A	DRAWN - EJS	REVISED -			94	(0405,ETC,A2-780707RS-1I)	COOK	133	18
PLOT DATE = 2/5/2009	CHECKED -	REVISED -	REVISED -	SCALE: N/A	SHEET NO. 1 OF 2 SHEETS	STA.	TO STA.	CONTRACT NO. 60021		
	DATE - 2/5/2009	REVISED -	REVISED -	FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT			



EXIST. CURVE ML\_I-94-6  
 PI STA. = 493+98.98  
 $\Delta = 8^{\circ} 00' 00''$  (RT)  
 $\theta = 0^{\circ} 40' 42''$   
 $R = 8,445.00'$   
 $T = 590.54'$   
 $L = 1,179.15'$   
 $E = 20.62'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 488+08.45  
 P.T. STA. = 499+87.60



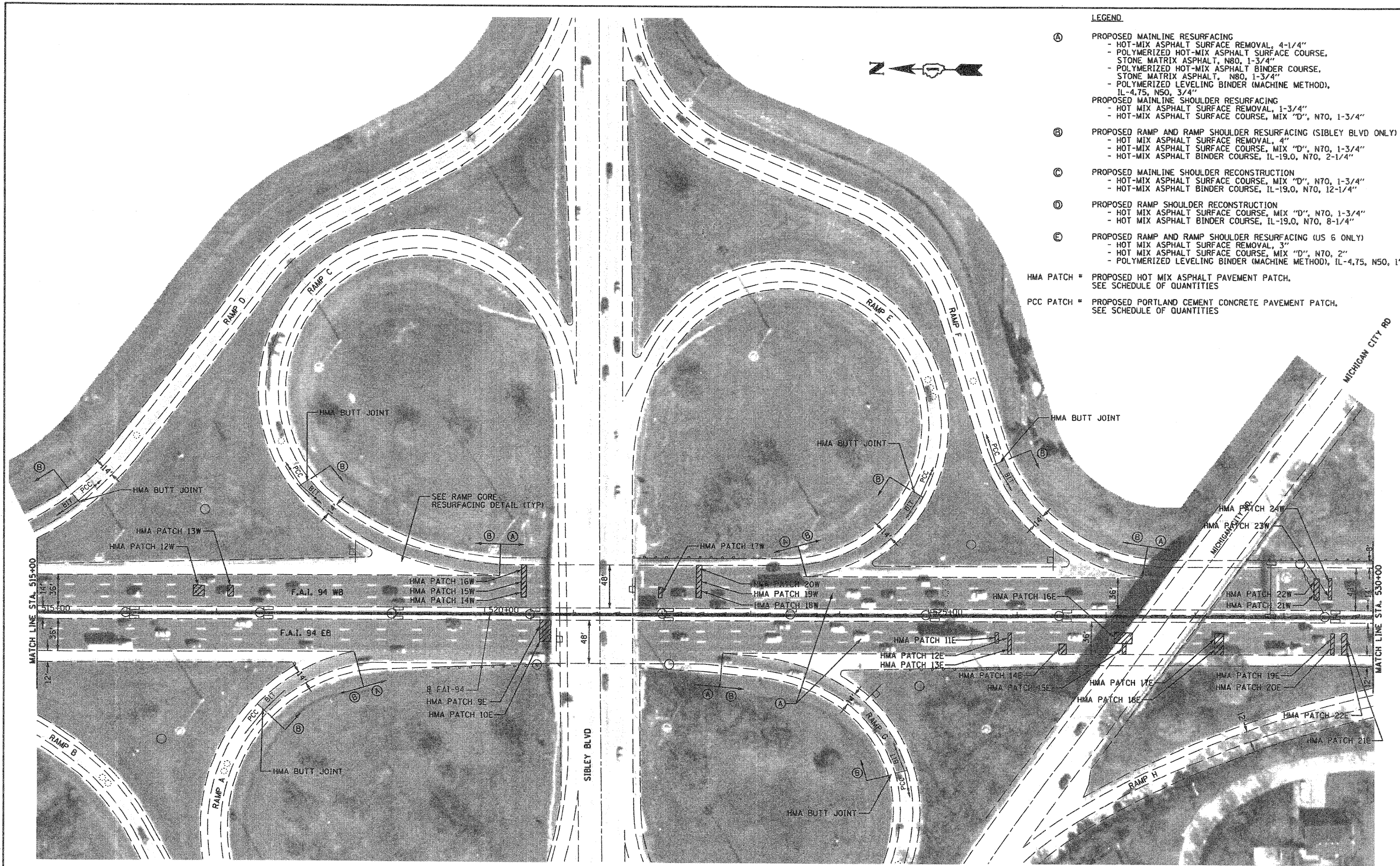
**LEGEND**

- (A)** PROPOSED MAINLINE RESURFACING
  - HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4"
  - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
  - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
  - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (B)** PROPOSED RAMP AND RAMP SHOULDER RESURFACING (SIBLEY BLVD ONLY)
  - HOT MIX ASPHALT SURFACE REMOVAL, 4"
  - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2-1/4"
- (C)** PROPOSED MAINLINE SHOULDER RECONSTRUCTION
  - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4"
- (D)** PROPOSED RAMP SHOULDER RECONSTRUCTION
  - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4"
- (E)** PROPOSED RAMP AND RAMP SHOULDER RESURFACING (US 6 ONLY)
  - HOT MIX ASPHALT SURFACE REMOVAL, 3"
  - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
  - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- HMA PATCH = PROPOSED HOT MIX ASPHALT PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES
- PCC PATCH = PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES

EXIST. CURVE ML\_I-94-6  
 PI STA. = 493+98.98  
 $\Delta = 8^{\circ} 00' 00''$  (RT)  
 $\theta = 0^{\circ} 40' 42''$   
 $R = 8,445.00'$   
 $T = 590.54'$   
 $L = 1,179.15'$   
 $E = 20.62'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 488+08.45  
 P.T. STA. = 499+87.60

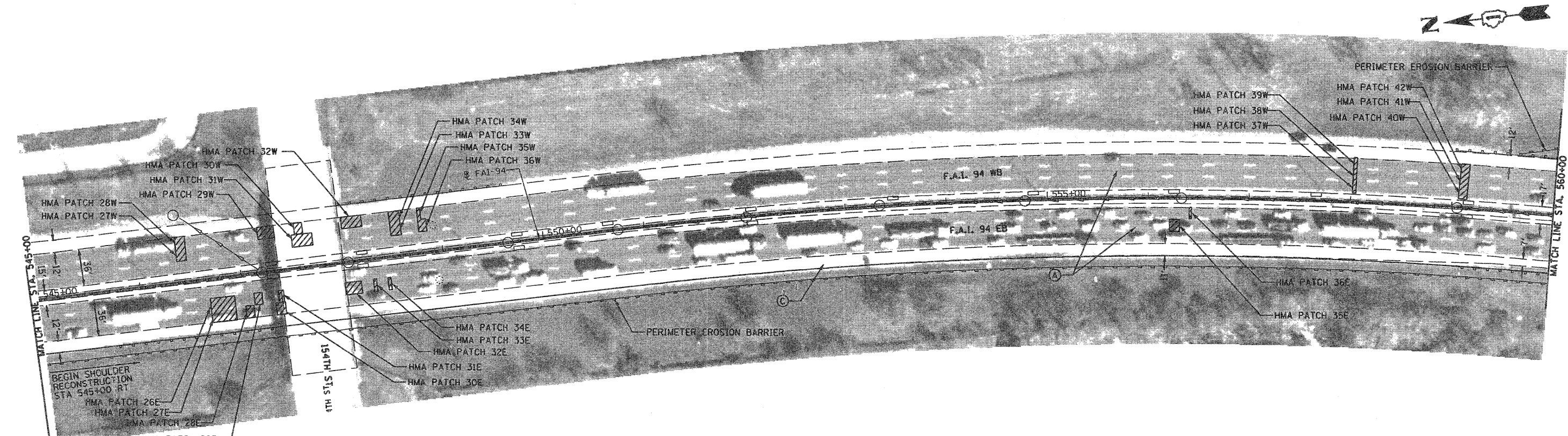
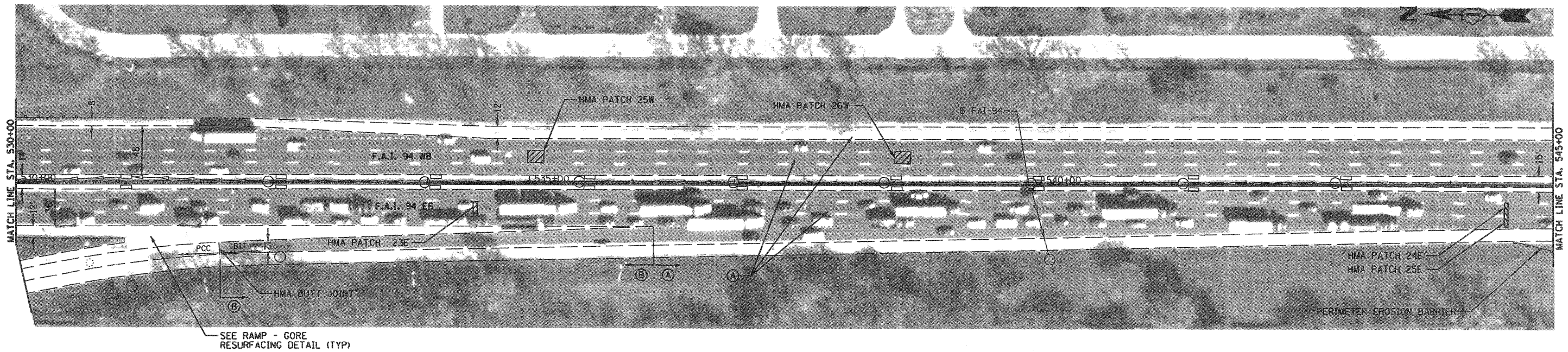
EXIST. CURVE ML\_I-94-7  
 PI STA. = 505+35.01  
 $\Delta = 8^{\circ} 12' 45''$  (LT)  
 $\theta = 0^{\circ} 45' 05''$   
 $R = 7,625.00'$   
 $T = 547.41'$   
 $L = 1,092.95'$   
 $E = 19.62'$   
 $e = \text{---}$   
 T.R. =  $\text{---}$   
 S.E. RUN =  $\text{---}$   
 P.C. STA. = 499+87.60  
 P.T. STA. = 510+80.55

FILE NAME = 0160021-020-PLAN-1.dgn	USER NAME = dwoznarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) RESURFACING PLAN</b>	F.A.I. RTE. 94	SECTION 10405.ETC.42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 20	
PLOT SCALE = 100.00' / IN.	CHECKED -	REVISED -	REVISED -			SCALE: 1" = 50'	SHEET NO. 1 OF 9 SHEETS	STA. 485+00	TO STA. 515+00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -	REVISED -			CONTRACT NO. 60D21					
						CONTRACT NO. 60D21					



- LEGEND**
- Ⓐ PROPOSED MAINLINE RESURFACING
    - HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4"
    - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
  - Ⓑ PROPOSED MAINLINE SHOULDER RESURFACING
    - HOT MIX ASPHALT SURFACE REMOVAL, 1-3/4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - Ⓒ PROPOSED RAMP AND RAMP SHOULDER RESURFACING (SIBLEY BLVD ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2-1/4"
  - Ⓓ PROPOSED MAINLINE SHOULDER RECONSTRUCTION
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4"
  - Ⓔ PROPOSED RAMP SHOULDER RECONSTRUCTION
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4"
  - Ⓕ PROPOSED RAMP AND RAMP SHOULDER RESURFACING (US 6 ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 3"
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
  - HMA PATCH \* PROPOSED HOT MIX ASPHALT PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES
  - PCC PATCH \* PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES

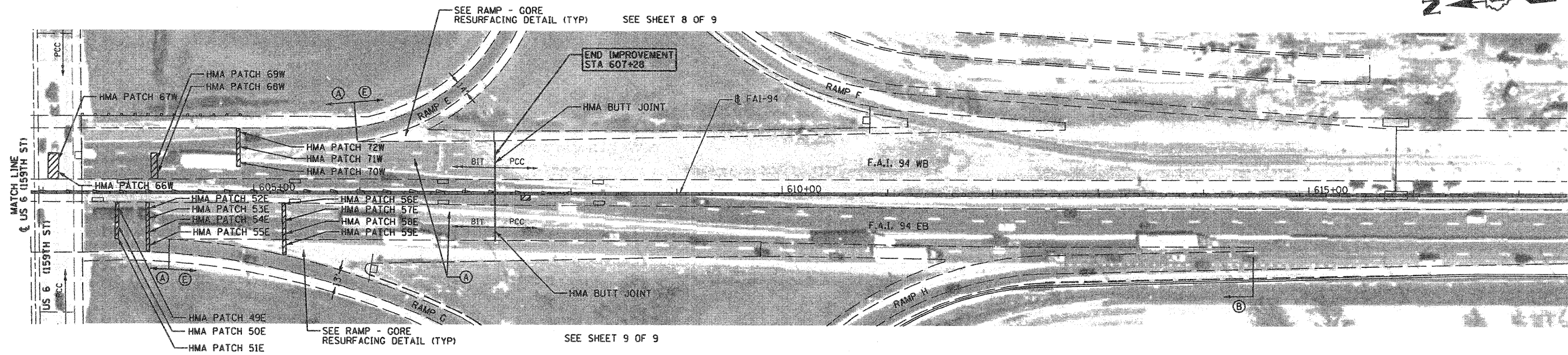
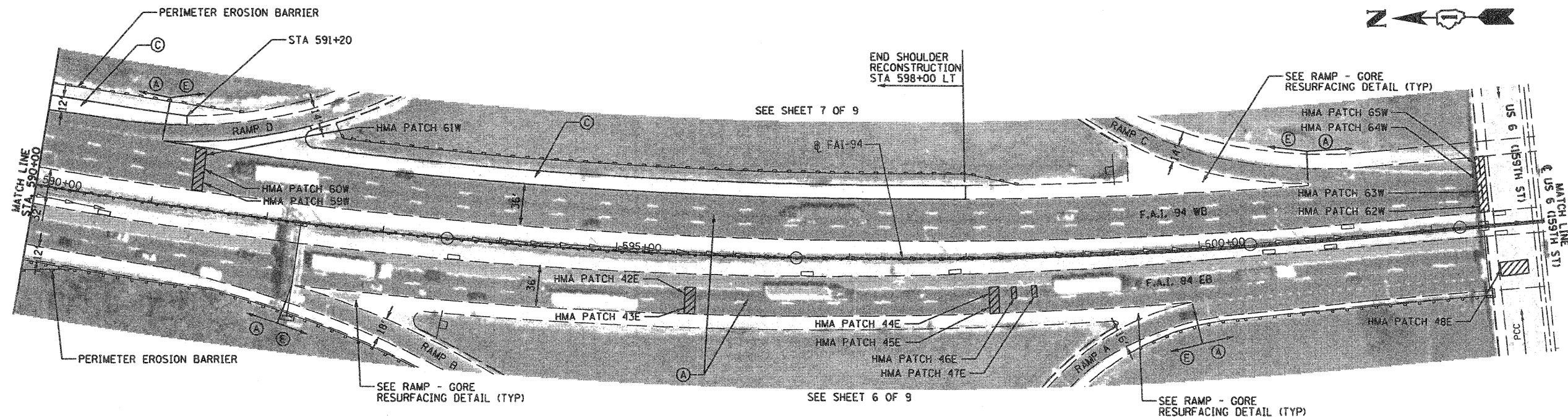
FILE NAME = D168021-821-PLAN-2.dgn	USER NAME = dwoznaraki	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) RESURFACING PLAN</b>	F.A.I. RTE. 94	SECTION 0405,ETC,42-T&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 21		
PLOT SCALE = 100.00' / IN.	DRAWN - DMW	CHECKED -	REVISED -			SCALE: 1" = 50'	SHEET NO. 2 OF 9 SHEETS	STA. 515+00	TO STA. 530+00	CONTRACT NO. 60D21		
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT						



EXIST. CURVE ML\_I-94-8  
 PI STA. = 560+02.96      E = 96.50'  
 $\Delta$  = 23° 29' 48" (RT)      e = -----  
 D = 1° 16' 13"      T.R. = -----  
 R = 4,510.00'      S.E. RUN = -----  
 T = 937.95'      P.C. STA. = 550+65.01  
 L = 1,849.53'      P.T. STA. = 569+14.54

- LEGEND**
- Ⓐ PROPOSED MAINLINE RESURFACING
    - HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4"
    - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED HOT-MIX ASPHALT COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
  - Ⓑ PROPOSED MAINLINE SHOULDER RESURFACING
    - HOT MIX ASPHALT SURFACE REMOVAL, 1-3/4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - Ⓒ PROPOSED RAMP AND RAMP SHOULDER RESURFACING (SIBLEY BLVD ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2-1/4"
  - Ⓓ PROPOSED MAINLINE SHOULDER RECONSTRUCTION
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4"
  - Ⓔ PROPOSED RAMP SHOULDER RECONSTRUCTION
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4"
  - Ⓕ PROPOSED RAMP AND RAMP SHOULDER RESURFACING (US 6 ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 3"
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- HMA PATCH = PROPOSED HOT MIX ASPHALT PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES  
 PCC PATCH = PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES



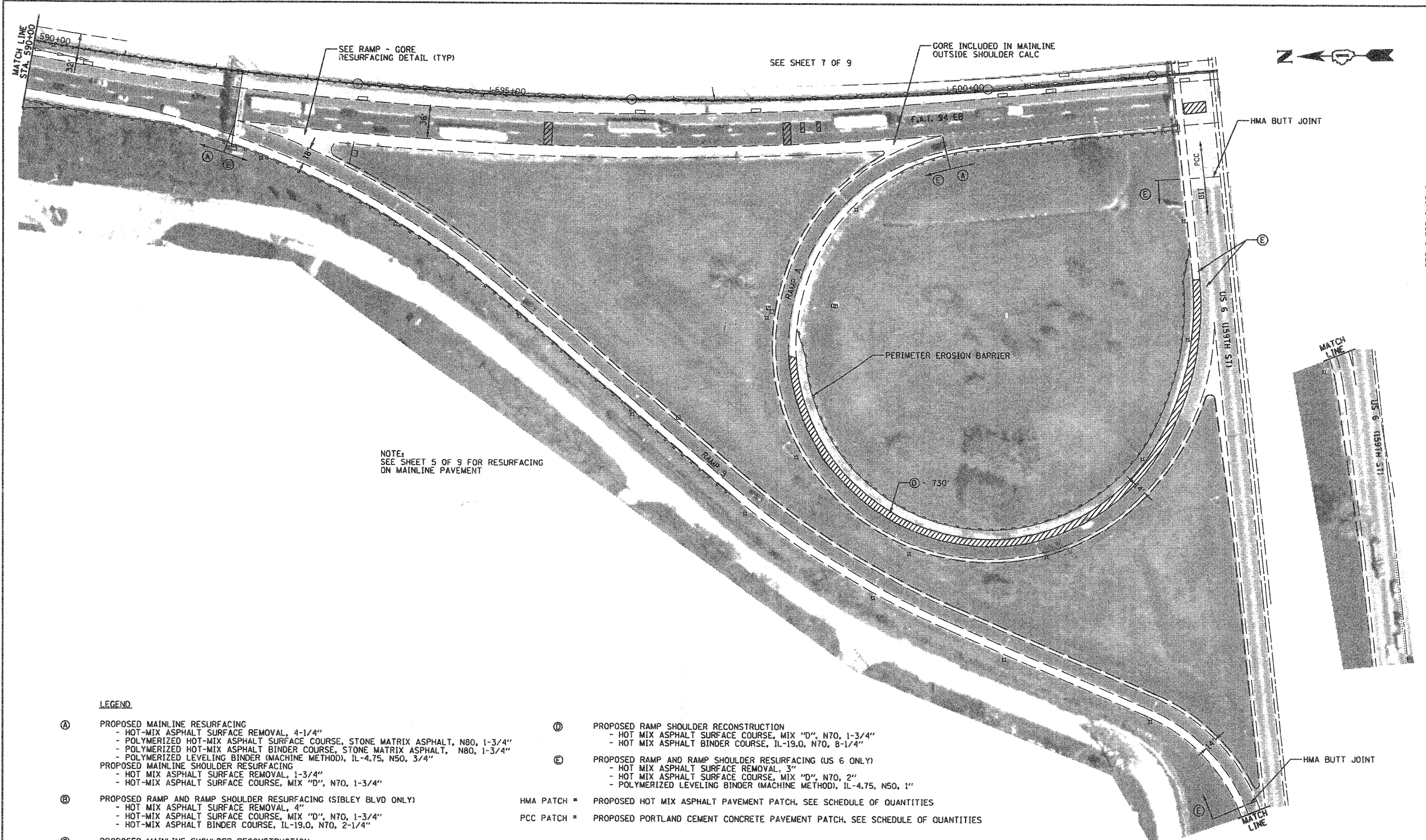


**LEGEND**

- (A)** PROPOSED MAINLINE RESURFACING
  - HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4"
  - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
  - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
  - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (B)** PROPOSED RAMP AND RAMP SHOULDER RESURFACING (SIBLEY BLVD ONLY)
  - HOT MIX ASPHALT SURFACE REMOVAL, 4"
  - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2-1/4"
- (C)** PROPOSED MAINLINE SHOULDER RECONSTRUCTION
  - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4"
- (D)** PROPOSED RAMP SHOULDER RECONSTRUCTION
  - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4"
- (E)** PROPOSED RAMP AND RAMP SHOULDER RESURFACING (US 6 ONLY)
  - HOT MIX ASPHALT SURFACE REMOVAL, 3"
  - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
  - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- HMA PATCH** = PROPOSED HOT MIX ASPHALT PAVEMENT PATCH, SEE SCHEDULE OF QUANTITIES
- PCC PATCH** = PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT PATCH, SEE SCHEDULE OF QUANTITIES

FILE NAME = D168021-824-PLAN-5.dgn	USER NAME = dwoznarski	DESIGNED - DRAWN - DMW	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) RESURFACING PLAN</b>	F.A.I. RTE. 94	SECTION 10405.ETC.42-T&O707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 24
PLOT SCALE = 100.00' / IN. PLOT DATE = 2/5/2009				SCALE: 1" = 50'		SHEET NO. 5 OF 9 SHEETS		STA. 590+00 TO STA. 605+00		
DATE = 2/5/2009				CONTRACT NO. 60021		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				





NOTE:  
SEE SHEET 5 OF 9 FOR RESURFACING  
ON MAINLINE PAVEMENT

**LEGEND**

- (A) PROPOSED MAINLINE RESURFACING
  - HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4"
  - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
  - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
  - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
- (B) PROPOSED RAMP AND RAMP SHOULDER RESURFACING (SIBLEY BLVD ONLY)
  - HOT MIX ASPHALT SURFACE REMOVAL, 4"
  - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2-1/4"
- (C) PROPOSED MAINLINE SHOULDER RECONSTRUCTION
  - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4"
- (D) PROPOSED RAMP SHOULDER RECONSTRUCTION
  - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4"
- (E) PROPOSED RAMP AND RAMP SHOULDER RESURFACING (US 6 ONLY)
  - HOT MIX ASPHALT SURFACE REMOVAL, 3"
  - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
  - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- HMA PATCH = PROPOSED HOT MIX ASPHALT PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES
- PCC PATCH = PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES

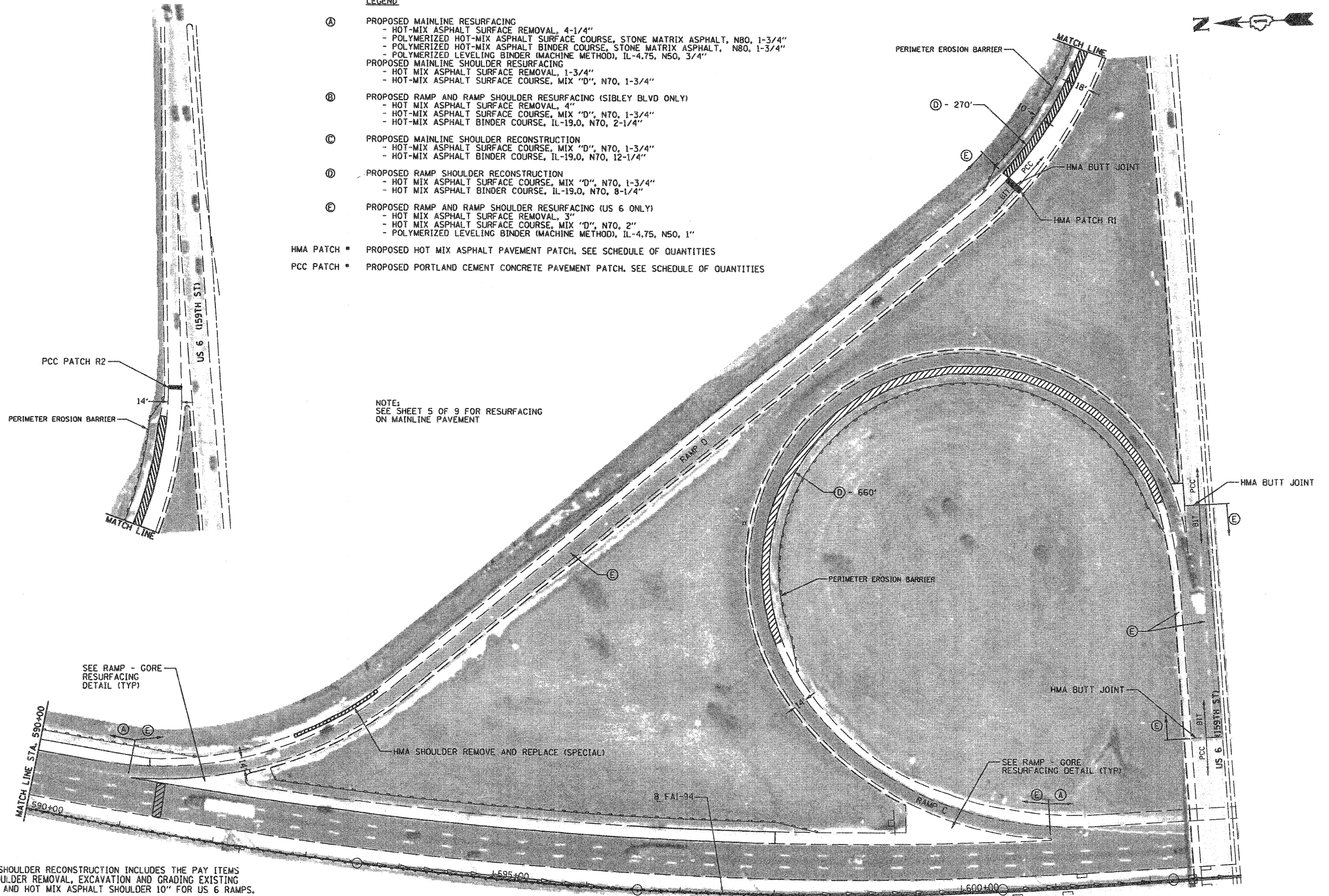
NOTE: AREAS OF SHOULDER RECONSTRUCTION INCLUDES THE PAY ITEMS PAVED SHOULDER REMOVAL, EXCAVATION AND GRADING EXISTING SHOULDER, AND HOT MIX ASPHALT SHOULDER 10" FOR US 6 RAMPS.

FILE NAME = D168021-025-PLAN-6.dgn	USER NAME = dwoznarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) RESURFACING PLAN</b>		F.A.I. RTE. 94	SECTION 10405.ETC.42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 25	
	PLOT SCALE = 100.00' / IN.	DRAWN - DMW	REVISED -		SCALE: 1" = 50'		SHEET NO. 6 OF 9 SHEETS		STA. 590+00 TO STA. 605+00		CONTRACT NO. 60021	
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							
		DATE - 2/5/2009	REVISED -									

**LEGEND**

- Ⓐ PROPOSED MAINLINE RESURFACING
    - HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4"
    - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
  - Ⓑ PROPOSED MAINLINE SHOULDER RESURFACING
    - HOT MIX ASPHALT SURFACE REMOVAL, 1-3/4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - Ⓒ PROPOSED RAMP AND RAMP SHOULDER RESURFACING (SIBLEY BLVD ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2-1/4"
  - Ⓓ PROPOSED RAMP SHOULDER RECONSTRUCTION
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4"
  - Ⓔ PROPOSED RAMP AND RAMP SHOULDER RESURFACING (US 6 ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 3"
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- HMA PATCH \* PROPOSED HOT MIX ASPHALT PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES  
 PCC PATCH \* PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES

NOTE:  
SEE SHEET 5 OF 9 FOR RESURFACING  
ON MAINLINE PAVEMENT



SEE RAMP - GORE  
RESURFACING  
DETAIL (TYP)

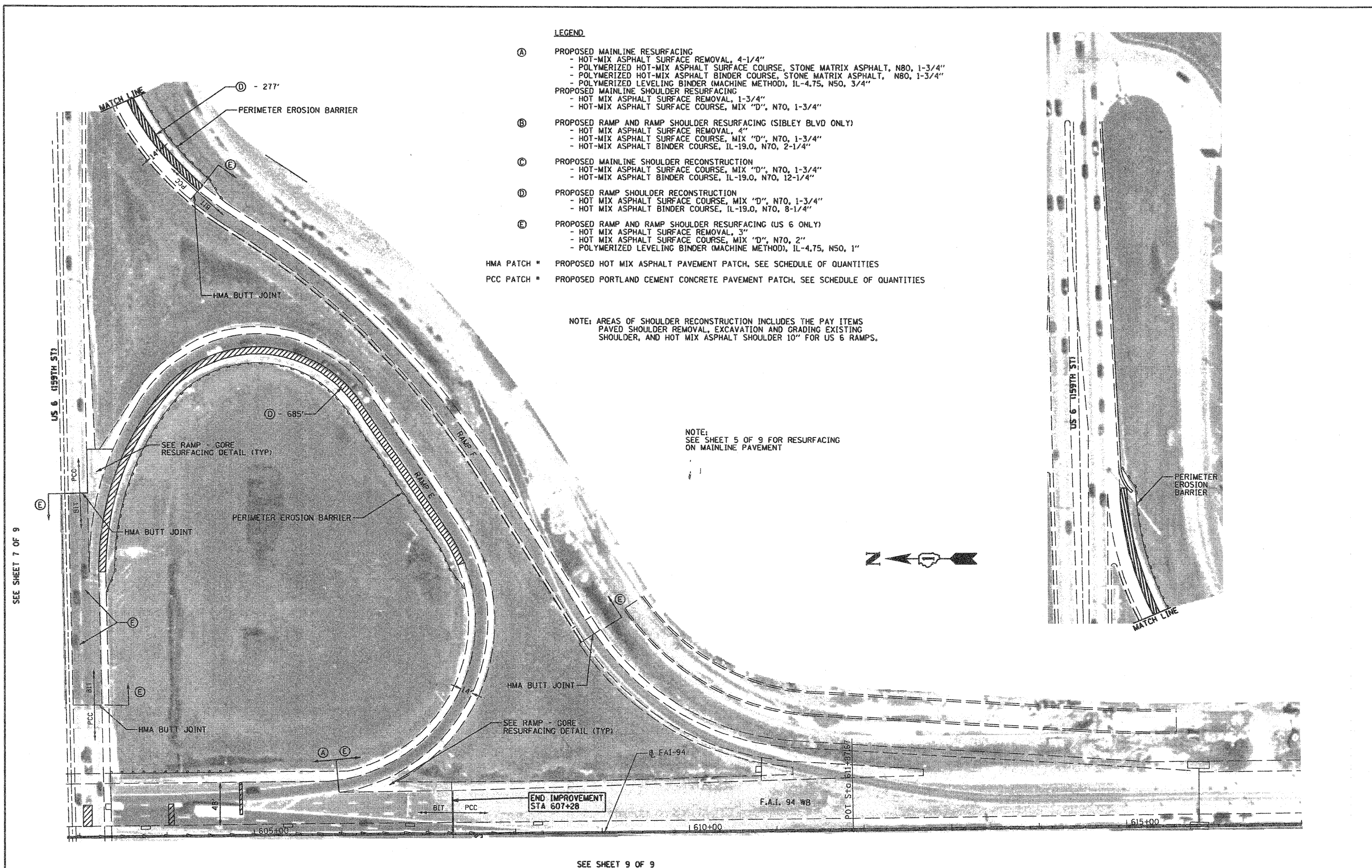
MATCH LINE STA. 590+00

SEE SHEET 6 OF 9

NOTE: AREAS OF SHOULDER RECONSTRUCTION INCLUDES THE PAY ITEMS  
PAVED SHOULDER REMOVAL, EXCAVATION AND GRADING EXISTING  
SHOULDER, AND HOT MIX ASPHALT SHOULDER 10" FOR US 6 RAMPS.

SEE SHEET 8 OF 9

FILE NAME = D:\68021-026-PLAN-7.dgn	USER NAME = dvoznierski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) RESURFACING PLAN</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN - DMW	REVISED -			94	10405,ETC,42-T&0707RS-11	COOK	133	26	
		PLOT SCALE = 1/8" = 1'-0"	REVISED -			SCALE: 1" = 50' SHEET NO. 7 OF 9 SHEETS STA. 590+00 TO STA. 605+00					
		PLOT DATE = 2/5/2009	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT CONTRACT NO. 60021					



**LEGEND**

- Ⓐ PROPOSED MAINLINE RESURFACING
    - HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4"
    - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
  - Ⓑ PROPOSED MAINLINE SHOULDER RESURFACING
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - Ⓒ PROPOSED RAMP AND RAMP SHOULDER RESURFACING (SIBLEY BLVD ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2-1/4"
  - Ⓓ PROPOSED MAINLINE SHOULDER RECONSTRUCTION
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4"
  - Ⓔ PROPOSED RAMP SHOULDER RECONSTRUCTION
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4"
  - Ⓕ PROPOSED RAMP AND RAMP SHOULDER RESURFACING (US 6 ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 3"
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- HMA PATCH ■ PROPOSED HOT MIX ASPHALT PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES  
 PCC PATCH ■ PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES

NOTE: AREAS OF SHOULDER RECONSTRUCTION INCLUDES THE PAY ITEMS PAVED SHOULDER REMOVAL, EXCAVATION AND GRADING EXISTING SHOULDER, AND HOT MIX ASPHALT SHOULDER 10" FOR US 6 RAMPS.

NOTE: SEE SHEET 5 OF 9 FOR RESURFACING ON MAINLINE PAVEMENT

SEE SHEET 7 OF 9

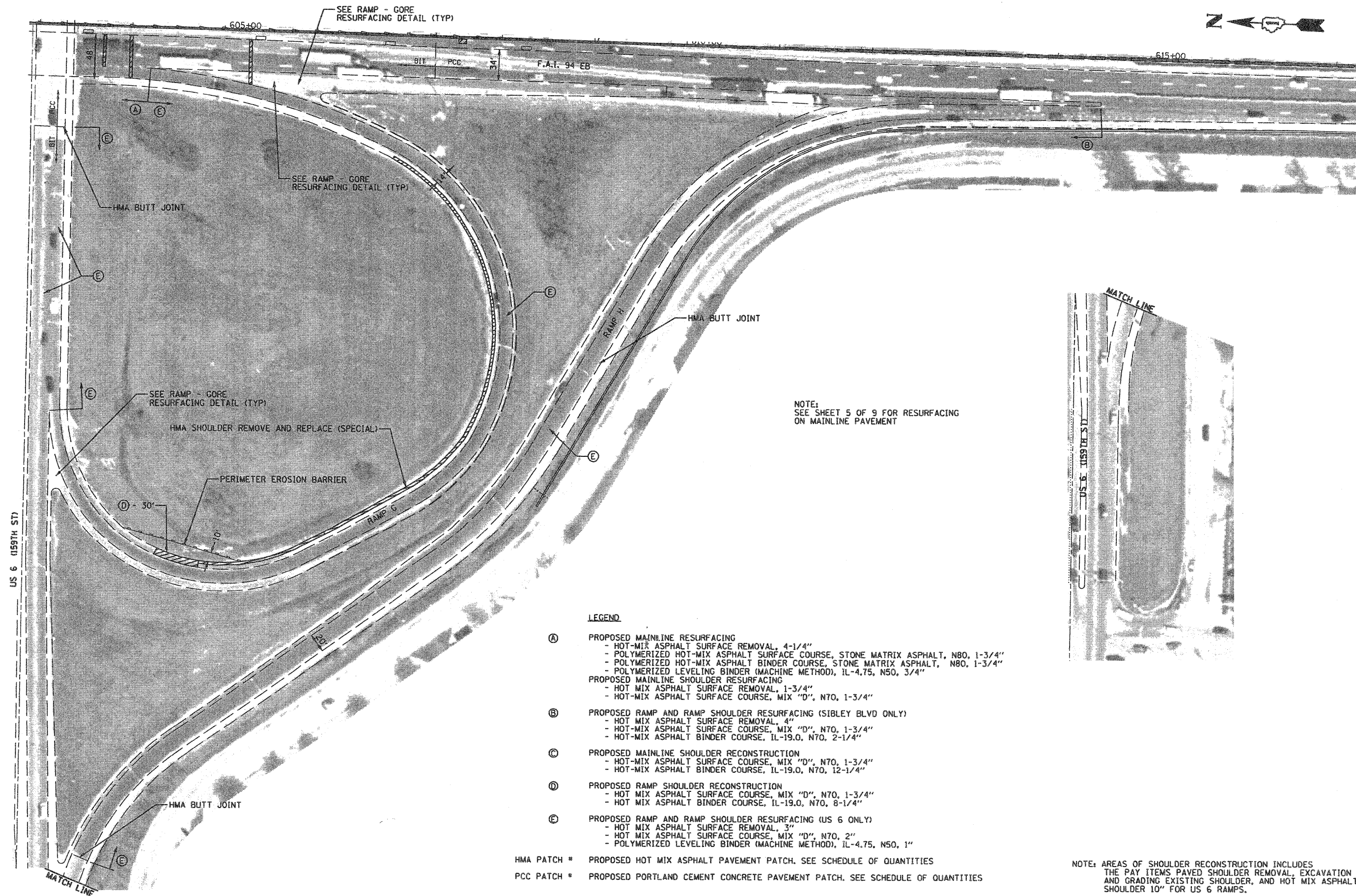
SEE SHEET 9 OF 9

FILE NAME = 0160021-027-PLAN-8.dgn	USER NAME = dwozniarski	DESIGNED - DRAWN - DMW	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BISHOP FORD EXPRESSWAY (I-94) RESURFACING PLAN	F.A.I. RTE. 94	SECTION 10405.ETC.42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 27
PLOT SCALE = 1/8" = 1'-0"	PLOT DATE = 2/5/2009	CHECKED - DATE - 2/5/2009	SCALE: 1" = 50'			SHEET NO. 8 OF 9 SHEETS	STA. 605+00 TO STA. 607+28	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT CONTRACT NO. 60021		

SEE SHEET 8 OF 9



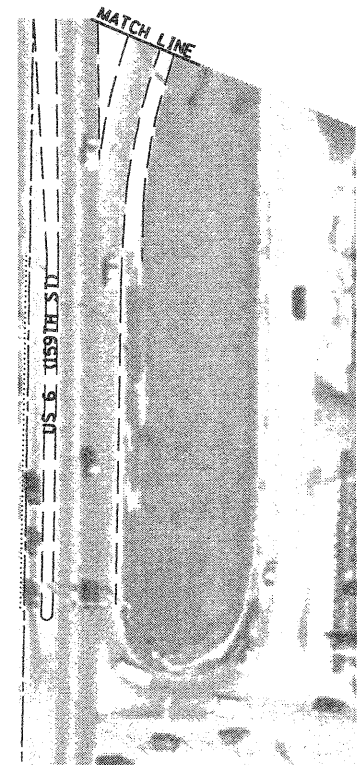
SEE SHEET 6 OF 9



NOTE:  
SEE SHEET 5 OF 9 FOR RESURFACING  
ON MAINLINE PAVEMENT

**LEGEND**

- (A) PROPOSED MAINLINE RESURFACING
    - HOT-MIX ASPHALT SURFACE REMOVAL, 4-1/4"
    - POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, STONE MATRIX ASPHALT, N80, 1-3/4"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4"
  - (B) PROPOSED MAINLINE SHOULDER RESURFACING
    - HOT MIX ASPHALT SURFACE REMOVAL, 1-3/4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
  - (C) PROPOSED RAMP AND RAMP SHOULDER RESURFACING (SIBLEY BLVD ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 4"
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2-1/4"
  - (D) PROPOSED MAINLINE SHOULDER RECONSTRUCTION
    - HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 12-1/4"
  - (E) PROPOSED RAMP SHOULDER RECONSTRUCTION
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1-3/4"
    - HOT MIX ASPHALT BINDER COURSE, IL-19.0, N70, 8-1/4"
  - (F) PROPOSED RAMP AND RAMP SHOULDER RESURFACING (US 6 ONLY)
    - HOT MIX ASPHALT SURFACE REMOVAL, 3"
    - HOT MIX ASPHALT SURFACE COURSE, MIX "D", N70, 2"
    - POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 1"
- HMA PATCH \* PROPOSED HOT MIX ASPHALT PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES  
 PCC PATCH \* PROPOSED PORTLAND CEMENT CONCRETE PAVEMENT PATCH. SEE SCHEDULE OF QUANTITIES



NOTE: AREAS OF SHOULDER RECONSTRUCTION INCLUDES THE PAY ITEMS PAVED SHOULDER REMOVAL, EXCAVATION AND GRADING EXISTING SHOULDER, AND HOT MIX ASPHALT SHOULDER 10" FOR US 6 RAMPS.

FILE NAME = 0160021-026-PLAN-9.dgn	USER NAME = dvozmarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) RESURFACING PLAN</b>	F.A.I. RTE. 94	SECTION (0405.ETC.42-7&0707RS-11)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 28		
PLOT SCALE = 100.00' / IN.	DATE = 2/5/2009	DRAWN - DMW	REVISED -			SCALE: 1" = 50'	SHEET NO. 9 OF 9 SHEETS	STA. 605+00	TO STA. 607+28	CONTRACT NO. 60021		
PLOT DATE = 2/5/2009	DATE = 2/5/2009	CHECKED -	REVISED -			FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT						

# STAGE I AND II: YEAR 2009

## STAGE I NOTES

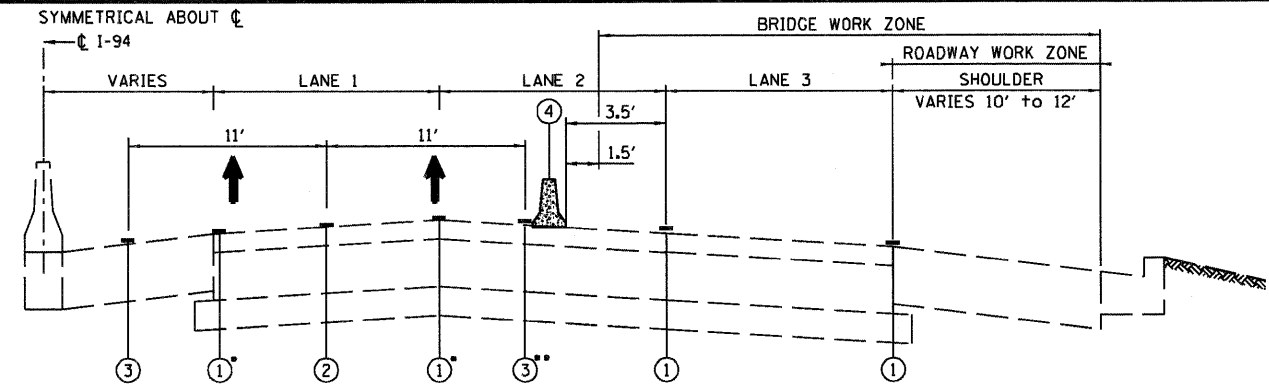
1. WORK ITEMS:
  - A. APPLY TEMPORARY PAVEMENT MARKINGS LANES 1 & 2.
  - B. COMPLETE STRUCTURAL REPAIRS DETAILED IN STRUCTURE PLANS.
  - C. REMOVE AND REPLACE BRIDGE APPROACH PAVEMENT AND CONNECTOR.
  - D. RECONSTRUCTION OF OUTSIDE SHOULDER AS SHOWN IN RESURFACING PLANS.
  - E. OUTSIDE SHOULDER GUARD RAIL REMOVAL AND REPLACEMENT.
2. WORK CAN OCCUR SIMULTANEOUSLY.
3. SEE STAGE I MAINTENANCE OF TRAFFIC PLANS AND BRIDGE PLANS FOR SN 016-0162 AND SN 016-0163.
4. PERMANENT LANE CLOSURES SHOWN IN STAGE I SHALL NOT EXCEED 60 CALENDAR DAYS. TEMPORARY LANE CLOSURES OUTSIDE THE 60 CALENDAR DAYS ALLOWED AS STIPULATED IN THE SPECIAL PROVISIONS.
5. LANE CLOSURES PER STANDARDS 701400 AND 701401 MULTI-LANE WEAVE PER DISTRICT DETAIL TC-9.
6. GUARD RAIL WORK THAT IS NOT WITHIN THE MAINTENANCE OF TRAFFIC PLANS FOR THE BRIDGE WORK ZONE SHALL BE COMPLETED DURING ALLOWABLE CLOSURES IDENTIFIED IN THE SPECIAL PROVISIONS. THE CONTRACTOR SHALL SCHEDULE THE WORK SO A HAZARD IS PROTECTED DURING OPEN TIMES.
7. REMOVAL OF TEMPORARY PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL".
8. REMOVAL OF EXISTING PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "PAVEMENT MARKING REMOVAL".

## STAGE II NOTES

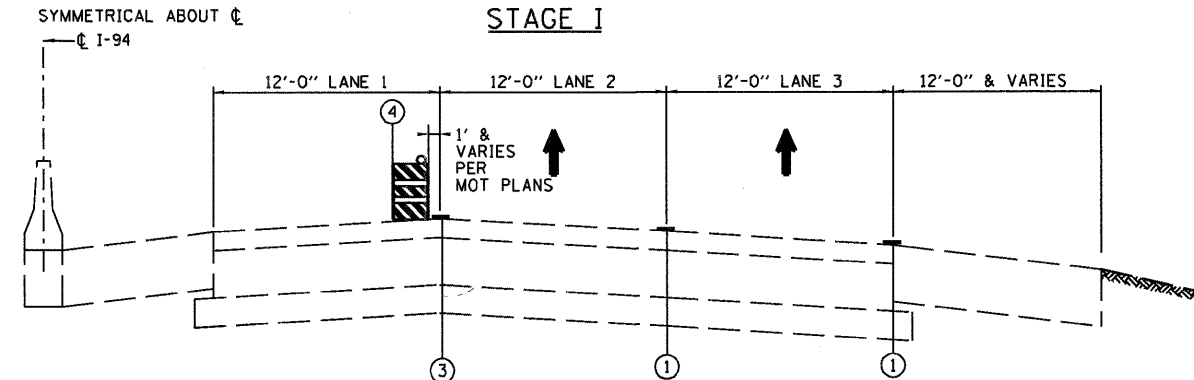
1. WORK ITEMS:
  - A. APPLY TEMPORARY PAVEMENT MARKINGS LANE 3 & OUTSIDE SHOULDER.
  - B. COMPLETE STRUCTURAL REPAIRS DETAILED IN STRUCTURE PLANS.
  - C. REMOVE AND REPLACE BRIDGE APPROACH PAVEMENT AND CONNECTOR.
  - D. MEDIAN GUARD RAIL AND TEMPORARY CONCRETE BARRIER REMOVAL AND REPLACEMENT.
2. WORK CAN OCCUR SIMULTANEOUSLY.
3. SEE STAGE II MAINTENANCE OF TRAFFIC PLANS AND BRIDGE PLANS FOR SN 016-0162 AND SN 016-0163.
4. PERMANENT LANE CLOSURES SHOWN IN STAGE II SHALL NOT EXCEED 60 CALENDAR DAYS. TEMPORARY LANE CLOSURES OUTSIDE THE 60 CALENDAR DAYS ALLOWED AS STIPULATED IN THE SPECIAL PROVISIONS.
5. LANE CLOSURES PER STANDARDS 701400 AND 701401 MULTI-LANE WEAVE PER DISTRICT DETAIL TC-9.
6. REMOVAL OF TEMPORARY PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL".
7. REMOVAL OF EXISTING PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "PAVEMENT MARKING REMOVAL".

## LEGEND

- ① EXISTING PAVEMENT MARKING
- ② WET REFLECTIVE TEMPORARY TAPE, TYPE III, 5" (WHITE 10' DASH - 30' SKIP)
- ③ WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4" (YELLOW LEFT & WHITE RIGHT)
- ④ DRUMS WITH STEADY BURNING MONO DIRECTIONAL LIGHT AT 100' C-C SPACING OR TEMPORARY CONCRETE BARRIER AS DETAILED IN MOT.
- ⑤ VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH CURB AND GUTTER / GUARDRAIL COMBINATION.
- ⑥ TEMPORARY CONCRETE BARRIER - THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING CONSTRUCTION ACCESS POINTS. THE PROPOSED LOCATIONS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL BEFORE USE. THE MINIMUM DISTANCE BETWEEN ACCESS POINTS IS 2,700'. THE MAXIMUM WIDTH OF AN ACCESS POINT IS 50'. THE APPROACH END SHALL BE PROTECTED BY IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3. A QUANTITY FOR TEMPORARY IMPACT ATTENUATORS HAS BEEN INCLUDED IN THE PLANS FOR USE WITH THE CONSTRUCTION ACCESS POINTS. DURING NON-WORKING HOURS THE ACCESS POINTS SHALL BE CLOSE WITH DRUMS AT 10' C-C SPACING. THE CONTRACTOR MAY REQUEST ADDITIONAL ACCESS POINTS AT NO ADDITIONAL COST TO THE DEPARTMENT.
- TO BE REMOVED.
- USED ONLY ADJACENT TO DRUMS. TEMPORARY PAVEMENT MARKINGS, 6" APPLIED TO CONCRETE BARRIER PER THE SPECIAL PROVISIONS.

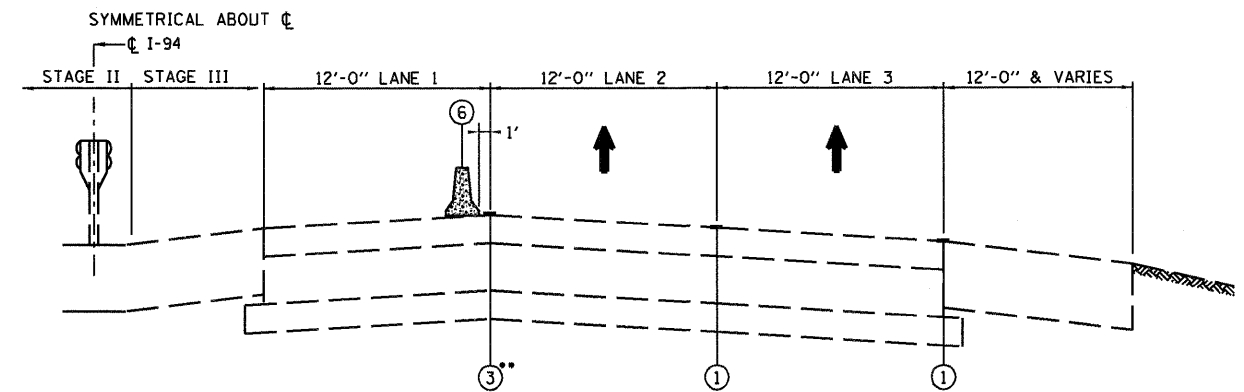


STAGE I



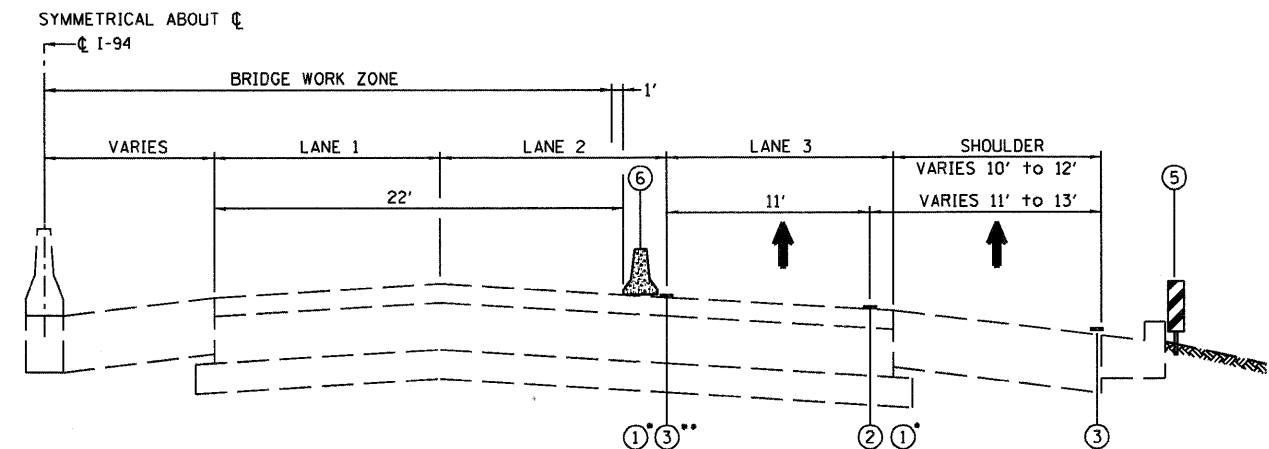
STAGE II

APPROACH TO GUARD RAIL REMOVE AND REPLACE WORK ZONE



STAGE II

GUARD RAIL REMOVE AND REPLACE WORK ZONE



STAGE II  
BRIDGE WORK ZONE



FILE NAME = D160021-029-STAGE_I.1811.dgn	USER NAME = droznierski	DESIGNED - DRAWN - DMW	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC</b>		F.A.I. RTE. = 94	SECTION 0405.ETC.42-7&0707R5-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 29	
	PLOT SCALE = N/A	CHECKED -	REVISED -		SCALE: N/A	SHEET NO. 1 OF 24 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			
	PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -		CONTRACT NO. 60021							

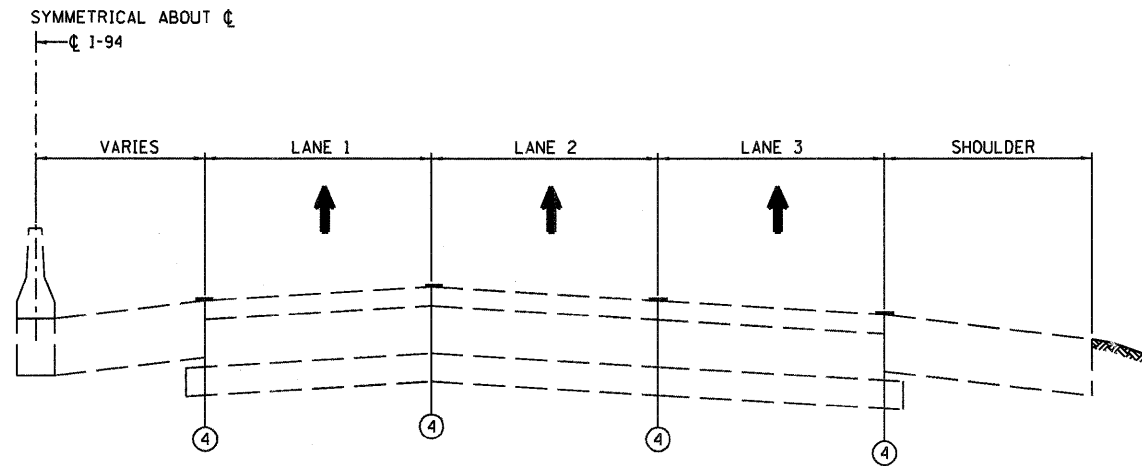
**STAGE III AND IV: YEAR 2010**  
**STAGE III**

**STAGE III NOTES**

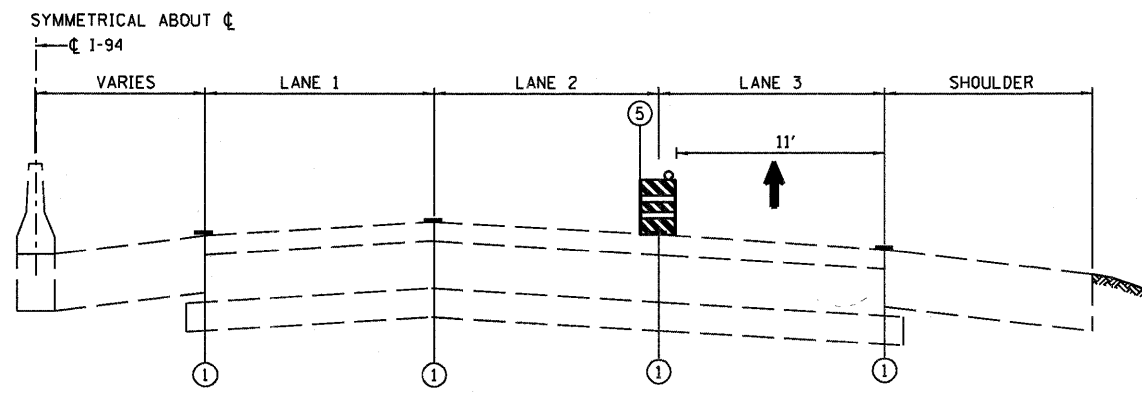
1. SEQUENCE OF CONSTRUCTION: PAVEMENT PATCHING, HMA SURFACE REMOVAL AND PROPOSED HMA COURSES. WORK CAN OCCUR CONCURRENTLY, BUT SEQUENCE SHALL BE MAINTAINED AND NOT CONFLICT. RAMP WORK WILL BE COMPLETED USING DISTRICT DETAIL TC-8 - FREEWAY AND EXIT RAMP CLOSURE DETAILS.
2. LANE AND RAMP CLOSURES ALLOWED AS STIPULATED IN THE SPECIAL PROVISIONS.
3. LANE CLOSURES PER STANDARDS 701400, 701401 AND 701446.
4. REDUCE LANE WIDTH USING 35:1 TAPER PER DISTRICT DETAIL TC-9 - TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE AND MULTI-LANE WEAVE. A W5-1 (48 x 48) ROAD NARROWS SIGN WILL BE ERRECTED 500' IN ADVANCE OF THE TAPER.
5. THE CONTRACTOR SHALL ERECT ROAD CONSTRUCTION AHEAD SIGNS (W20-1103(0-48) WITH FLASHING BEACON ON ALL ARTERIAL ROADWAYS APPROACHING INTERCHANGE RAMP.
6. GRADE DIFFERENTIAL BETWEEN LANES SHALL NOT EXCEED 2-1/2".
7. PARTIAL MILLING OF PAVEMENT FOR AN OPERATION WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE PAID FOR AT THE FINAL MILLING THICKNESS SHOWN ON THE TYPICAL SECTIONS.
8. REMOVAL OF TEMPORARY PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL".
9. REMOVAL OF EXISTING PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "PAVEMENT MARKING REMOVAL".
10. EXISTING OR TEMPORARY PAVEMENT MARKINGS REMOVED DURING MILLING OPERATIONS WILL NOT BE PAID FOR SEPARATELY.
11. CASTINGS EXPOSED IN TRAVEL LANES SHALL BE PROTECTED PER APPLICABLE PORTIONS OF ARTICLE 603.07 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE VARIOUS HMA BINDER AND SURFACE COURSES PLACED.

**LEGEND**

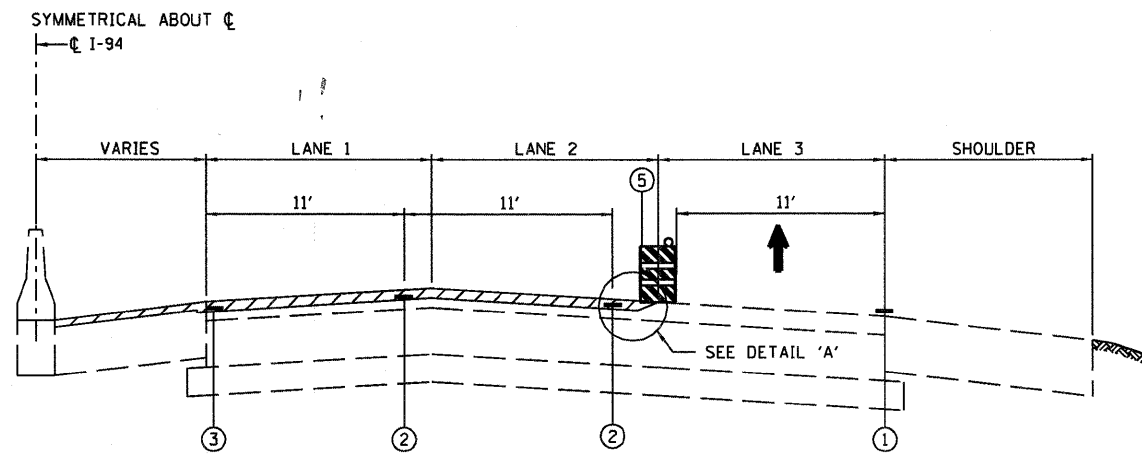
- ① EXISTING PAVEMENT MARKING
  - ② WET REFLECTIVE TEMPORARY TAPE, TYPE III, 5" (WHITE 10' DASH - 30' SKIP)
  - ③ WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4" (YELLOW LEFT & WHITE RIGHT) INSTALLED WITHIN 24 HOURS OF MILLING OR BINDER PLACEMENT.
  - ④ EPOXY PAVEMENT MARKING, LIMITED TO AREA IMPACTED BY STAGE I & II SEE PAVEMENT MARKING PLANS FOR COLOR AND WIDTH.
  - ⑤ VERTICAL BARRICADES WITH STEADY BURNING MONO DIRECTIONAL LIGHT AT 100' C-C SPACING. VERTICAL BARRICADES ALONG SHOULDERS WILL REMAIN IN PLACE BETWEEN OPERATIONS WHEN ALL LANES ARE OPEN TO TRAFFIC. DURING EACH OPERATION VERTICAL BARRICADES WILL THEN BE MOVED TO SEPARATE THE WORK ZONE AND OPERATING LANES.
  - ⑥ PROPOSED PAVEMENT MARKING. SEE PAVEMENT MARKING PLANS.
-  BITUMINOUS SURFACE REMOVAL  
 HMA LEVELING BINDER AND BINDER COURSE



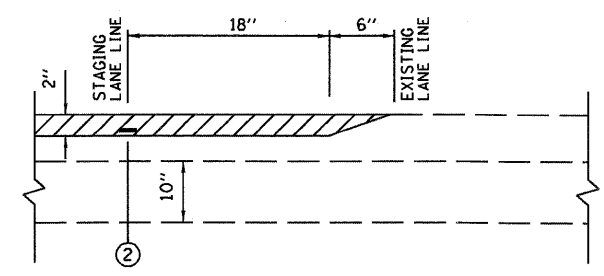
**WINTER SHUTDOWN**  
**PROPOSED PAVEMENT MARKING**  
 LIMITED TO AREA IMPACTED BY STAGES I - II



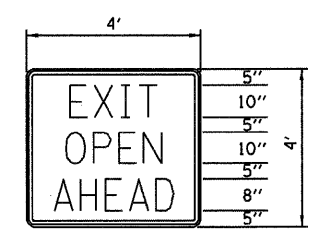
**STAGE III - OPERATION ONE**  
**PATCH LANES 1 & 2 AND INSIDE SHOULDER**



**STAGE III - OPERATION TWO**  
**HMA SURFACE REMOVAL 2" - LANES 1 & 2**  
**HMA SURFACE REMOVAL 1 3/4" - INSIDE SHOULDER**



**DETAIL 'A'**



1" BORDER LINE  
 GREEN REFLECTIVE BACKGROUND  
 WITH WHITE LEGEND

THIS SIGN SHALL BE INSTALLED  
 IN ADVANCE OF ALL OPEN/EXIT  
 RAMP WHEN THE RIGHT  
 LANES ARE CLOSED.



W8-11  
 48" x 48"

NOTE:  
 THIS SIGN SHALL BE INSTALLED  
 ON BOTH SIDES OF THE ROADWAY  
 500' IN ADVANCE OF AREAS WHERE  
 THERE IS A GRADE DIFFERENTIAL  
 BETWEEN LANES, AFTER EACH  
 ENTRANCE RAMP AND MINIMUM  
 OF EVERY MILE..

FILE NAME = D160021-938-STAGE.III.A.dgn	USER NAME = dwozniarski	DESIGNED - DRAWN - DMW	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94)</b> <b>SUGGESTED MAINTENANCE OF TRAFFIC</b>	F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 30
PLOT SCALE = N/A	PLOT DATE = 2/5/2009	CHECKED - DATE - 2/5/2009	REVISED -			SCALE: N/A	SHEET NO. 2 OF 24 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT	<b>CONTRACT NO. 60021</b>





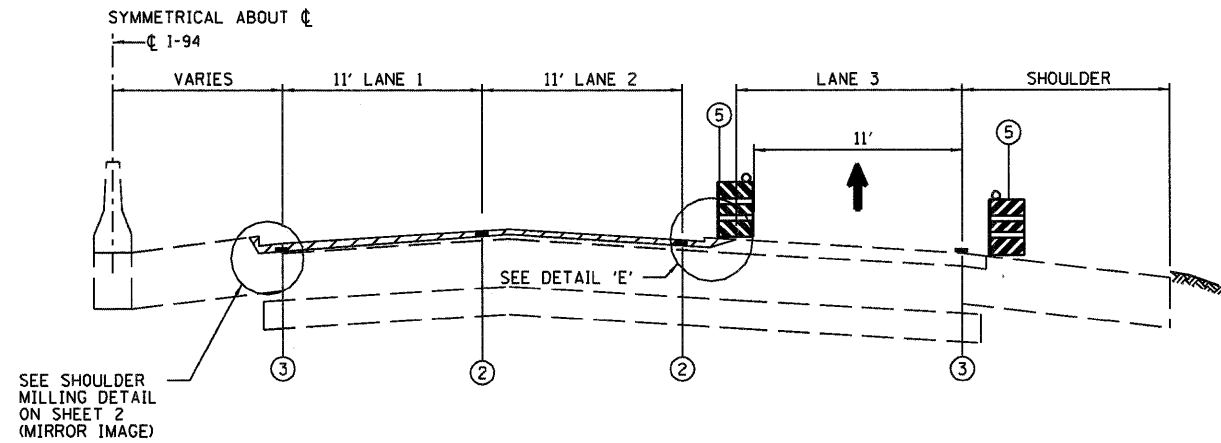
STAGE III AND IV: YEAR 2010  
 STAGE III (CONTINUED)

STAGE III NOTES

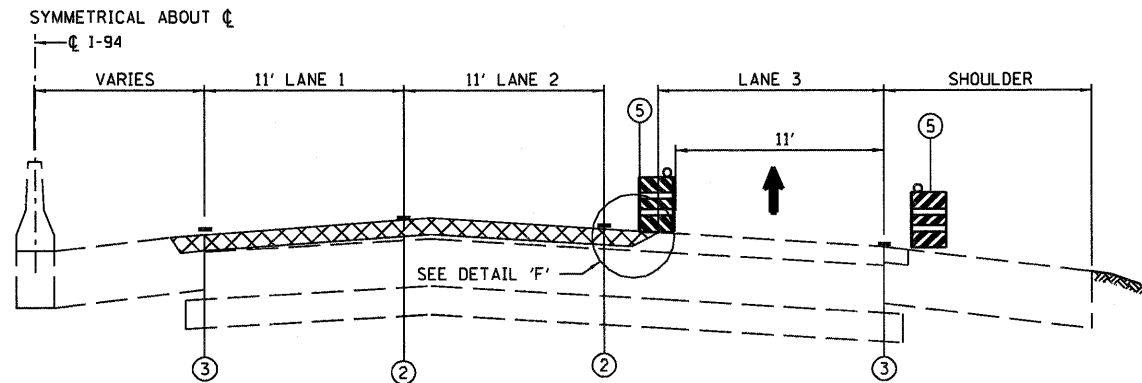
1. SEQUENCE OF CONSTRUCTION: PAVEMENT PATCHING, HMA SURFACE REMOVAL AND PROPOSED HMA COURSES. WORK CAN OCCUR CONCURRENTLY, BUT SEQUENCE SHALL BE MAINTAINED AND NOT CONFLICT. RAMP WORK WILL BE COMPLETED USING DISTRICT DETAIL TC-8 - FREEWAY AND EXIT RAMP CLOSURE DETAILS.
2. LANE AND RAMP CLOSURES ALLOWED AS STIPULATED IN THE SPECIAL PROVISIONS.
3. LANE CLOSURES PER STANDARDS 701400, 701401 AND 701446.
4. REDUCE LANE WIDTH USING 35:1 TAPER PER DISTRICT DETAIL TC-9 - TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE AND MULTI-LANE WEAVE. A W5-1 (48 x 48) ROAD NARROWS SIGN WILL BE ERRECTED 500' IN ADVANCE OF THE TAPER.
5. THE CONTRACTOR SHALL ERRECT ROAD CONSTRUCTION AHEAD SIGNS (W20-I10310-48) WITH FLASHING BEACON ON ALL ARTERIAL ROADWAYS APPROACHING INTERCHANGE RAMPS.
6. GRADE DIFFERENTIAL BETWEEN LANES SHALL NOT EXCEED 2-1/2".
7. PARTIAL MILLING OF PAVEMENT FOR AN OPERATION WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE PAID FOR AT THE FINAL MILLING THICKNESS SHOWN ON THE TYPICAL SECTIONS.
8. REMOVAL OF TEMPORARY PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL".
9. REMOVAL OF EXISTING PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "PAVEMENT MARKING REMOVAL".
10. EXISTING OR TEMPORARY PAVEMENT MARKINGS REMOVED DURING MILLING OPERATIONS WILL NOT BE PAID FOR SEPARATELY.
11. CASTINGS EXPOSED IN TRAVEL LANES SHALL BE PROTECTED PER APPLICABLE PORTIONS OF ARTICLE 603.07 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE VARIOUS HMA BINDER AND SURFACE COURSES PLACED.

LEGEND

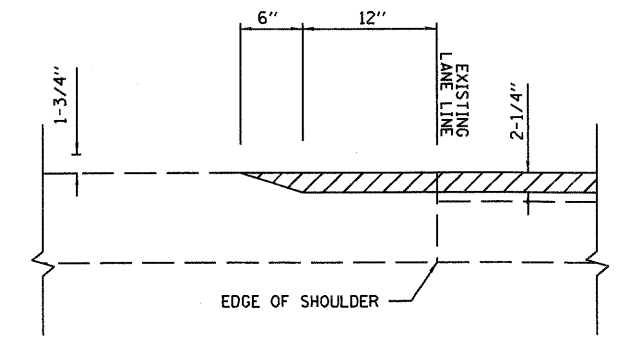
- ① EXISTING PAVEMENT MARKING
  - ② WET REFLECTIVE TEMPORARY TAPE, TYPE III, 5" (WHITE 10' DASH - 30' SKIP)
  - ③ WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4" (YELLOW LEFT & WHITE RIGHT) INSTALLED WITHIN 24 HOURS OF MILLING OR BINDER PLACEMENT.
  - ④ EPOXY PAVEMENT MARKING, LIMITED TO AREA IMPACTED BY STAGE I & II SEE PAVEMENT MARKING PLANS FOR COLOR AND WIDTH.
  - ⑤ VERTICAL BARRICADES WITH STEADY BURNING MONO DIRECTIONAL LIGHT AT 100' C-C SPACING. VERTICAL BARRICADES ALONG SHOULDERS WILL REMAIN IN PLACE BETWEEN OPERATIONS WHEN ALL LANES ARE OPEN TO TRAFFIC. DURING EACH OPERATION VERTICAL BARRICADES WILL THEN BE MOVED TO SEPARATE THE WORK ZONE AND OPERATING LANES.
  - ⑥ PROPOSED PAVEMENT MARKING. SEE PAVEMENT MARKING PLANS.
-  BITUMINOUS SURFACE REMOVAL  
 HMA LEVELING BINDER AND BINDER COURSE



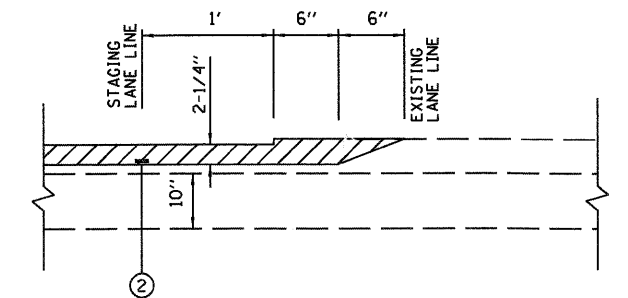
STAGE III - OPERATION SIX  
 HMA SURFACE REMOVAL 2-1/4" - LANES 1 & 2



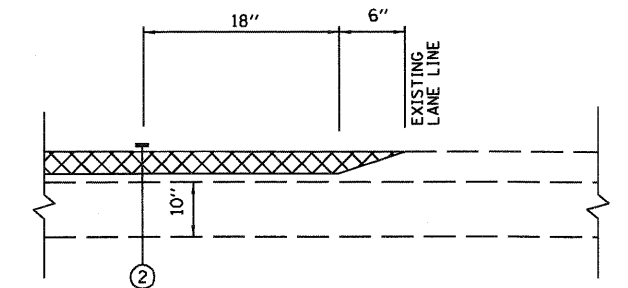
STAGE III - OPERATION SEVEN  
 PLACE 3/4" LEVELING BINDER & 1-3/4" BINDER - LANES 1 & 2



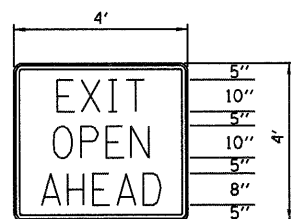
SHOULDER MILLING DETAIL



DETAIL 'E'



DETAIL 'F'



1" BORDER LINE  
 GREEN REFLECTIVE BACKGROUND  
 WITH WHITE LEGEND

THIS SIGN SHALL BE INSTALLED  
 IN ADVANCE OF ALL OPEN/EXIT  
 RAMPS WHEN THE RIGHT  
 LANES ARE CLOSED.



W8-11  
 48" x 48"

NOTE:  
 THIS SIGN SHALL BE INSTALLED  
 ON BOTH SIDES OF THE ROADWAY  
 500' IN ADVANCE OF AREAS WHERE  
 THERE IS A GRADE DIFFERENTIAL  
 BETWEEN LANES, AFTER EACH  
 ENTRANCE RAMP AND MINIMUM  
 OF EVERY MILE..

FILE NAME = D:\60021-032-STAGE.III.C.dgn	USER NAME = droznerski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC</b>	F.A.I. RTE. 94	SECTION 10405,ETC,42-7&0707R5-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 32	
PLOT SCALE = N/A	CHECKED -	REVISED -	REVISED -			SCALE: N/A		SHEET NO. 4 OF 24 SHEETS		STA. TO STA.	
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -	REVISED -			FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		CONTRACT NO. 60021	
						TO STA.		CONTRACT NO. 60021		CONTRACT NO. 60021	



STAGE III AND IV: YEAR 2010

STAGE IV

STAGE IV NOTES

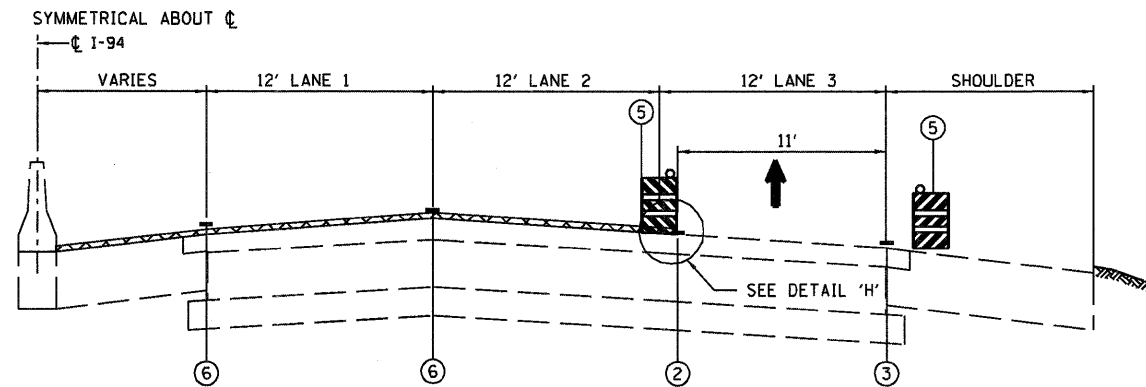
- SEQUENCE OF CONSTRUCTION: PAVEMENT PATCHING, HMA SURFACE REMOVAL AND PROPOSED HMA COURSES. WORK CAN OCCUR CONCURRENTLY, BUT SEQUENCE SHALL BE MAINTAINED AND NOT CONFLICT. RAMP WORK WILL BE COMPLETED USING DISTRICT DETAIL TC-8 - FREEWAY AND EXIT RAMP CLOSURE DETAILS.
- LANE AND RAMP CLOSURES ALLOWED AS STIPULATED IN THE SPECIAL PROVISIONS.
- LANE CLOSURES PER STANDARDS 701400, 701401 AND 701446.
- REDUCE LANE WIDTH USING 35:1 TAPER PER DISTRICT DETAIL TC-9 - TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE AND MULTI-LANE WEAVE. A W5-1 (48 x 48) ROAD NARROWS SIGN WILL BE ERRECTED 500' IN ADVANCE OF THE TAPER.
- THE CONTRACTOR SHALL ERECT ROAD CONSTRUCTION AHEAD SIGNS (W20-1103(0-48)) WITH FLASHING BEACON ON ALL ARTERIAL ROADWAYS APPROACHING INTERCHANGE RAMP.
- GRADE DIFFERENTIAL BETWEEN LANES SHALL NOT EXCEED 2-1/2".
- PARTIAL MILLING OF PAVEMENT FOR AN OPERATION WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE PAID FOR AT THE FINAL MILLING THICKNESS SHOWN ON THE TYPICAL SECTIONS.
- REMOVAL OF TEMPORARY PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "WORK ZONE PAVEMENT MARKING REMOVAL".
- REMOVAL OF EXISTING PAVEMENT MARKING, WHEN REQUIRED, SHALL BE PAID FOR AS "PAVEMENT MARKING REMOVAL".
- EXISTING OR TEMPORARY PAVEMENT MARKINGS REMOVED DURING MILLING OPERATIONS WILL NOT BE PAID FOR SEPARATELY.
- CASTINGS EXPOSED IN TRAVEL LANES SHALL BE PROTECTED PER APPLICABLE PORTIONS OF ARTICLE 603.07 OF THE STANDARD SPECIFICATIONS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE VARIOUS HMA BINDER AND SURFACE COURSES PLACED.

LEGEND

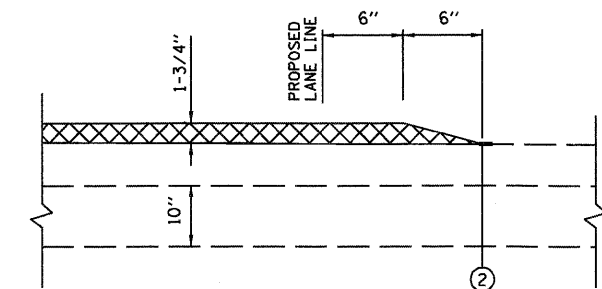
- ① EXISTING PAVEMENT MARKING
- ② WET REFLECTIVE TEMPORARY TAPE, TYPE III, 5" (WHITE 10' DASH - 30' SKIP)
- ③ WET REFLECTIVE TEMPORARY TAPE, TYPE III, 4" (YELLOW LEFT & WHITE RIGHT) INSTALLED WITHIN 24 HOURS OF MILLING OR BINDER PLACEMENT.
- ④ EPOXY PAVEMENT MARKING. LIMITED TO AREA IMPACTED BY STAGE I & II SEE PAVEMENT MARKING PLANS FOR COLOR AND WIDTH.
- ⑤ VERTICAL BARRICADES WITH STEADY BURNING MONO DIRECTIONAL LIGHT AT 100' C-C SPACING. VERTICAL BARRICADES ALONG SHOULDERS WILL REMAIN IN PLACE BETWEEN OPERATIONS WHEN ALL LANES ARE OPEN TO TRAFFIC. DURING EACH OPERATION VERTICAL BARRICADES WILL THEN BE MOVED TO SEPARATE THE WORK ZONE AND OPERATING LANES.
- ⑥ PROPOSED PAVEMENT MARKING. SEE PAVEMENT MARKING PLANS.

▨ BITUMINOUS SURFACE REMOVAL

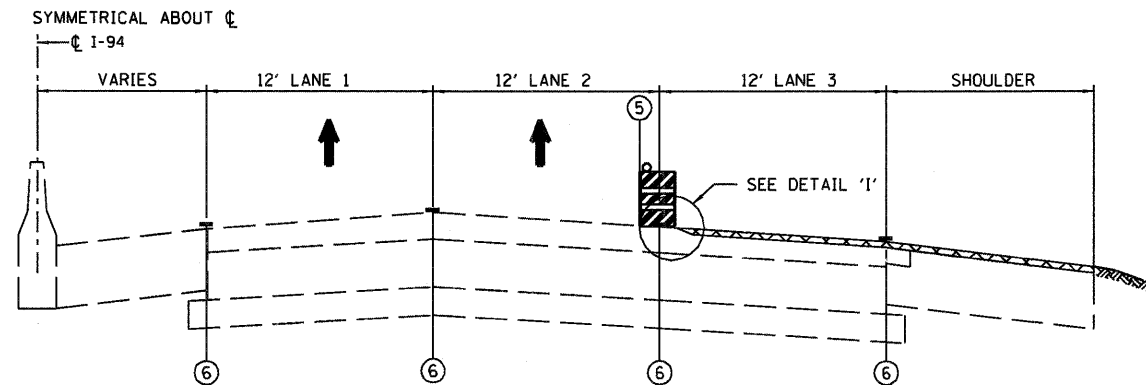
▩ HMA SURFACE COURSE



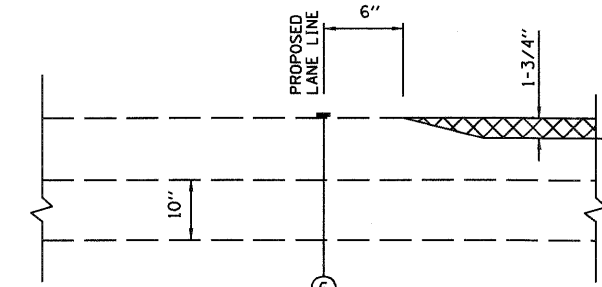
STAGE IV- OPERATION ONE  
PLACE SURFACE 1-3/4" - LANES 1 & 2  
AND INSIDE SHOULDER



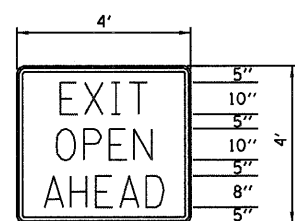
DETAIL 'H'



STAGE IV - OPERATION TWO  
PLACE 1-3/4" SURFACE - LANE 3  
AND OUTSIDE SHOULDER



DETAIL 'I'



1" BORDER LINE  
GREEN REFLECTIVE BACKGROUND  
WITH WHITE LEGEND

THIS SIGN SHALL BE INSTALLED  
IN ADVANCE OF ALL OPEN/EXIT  
RAMPS WHEN THE RIGHT  
LANES ARE CLOSED.



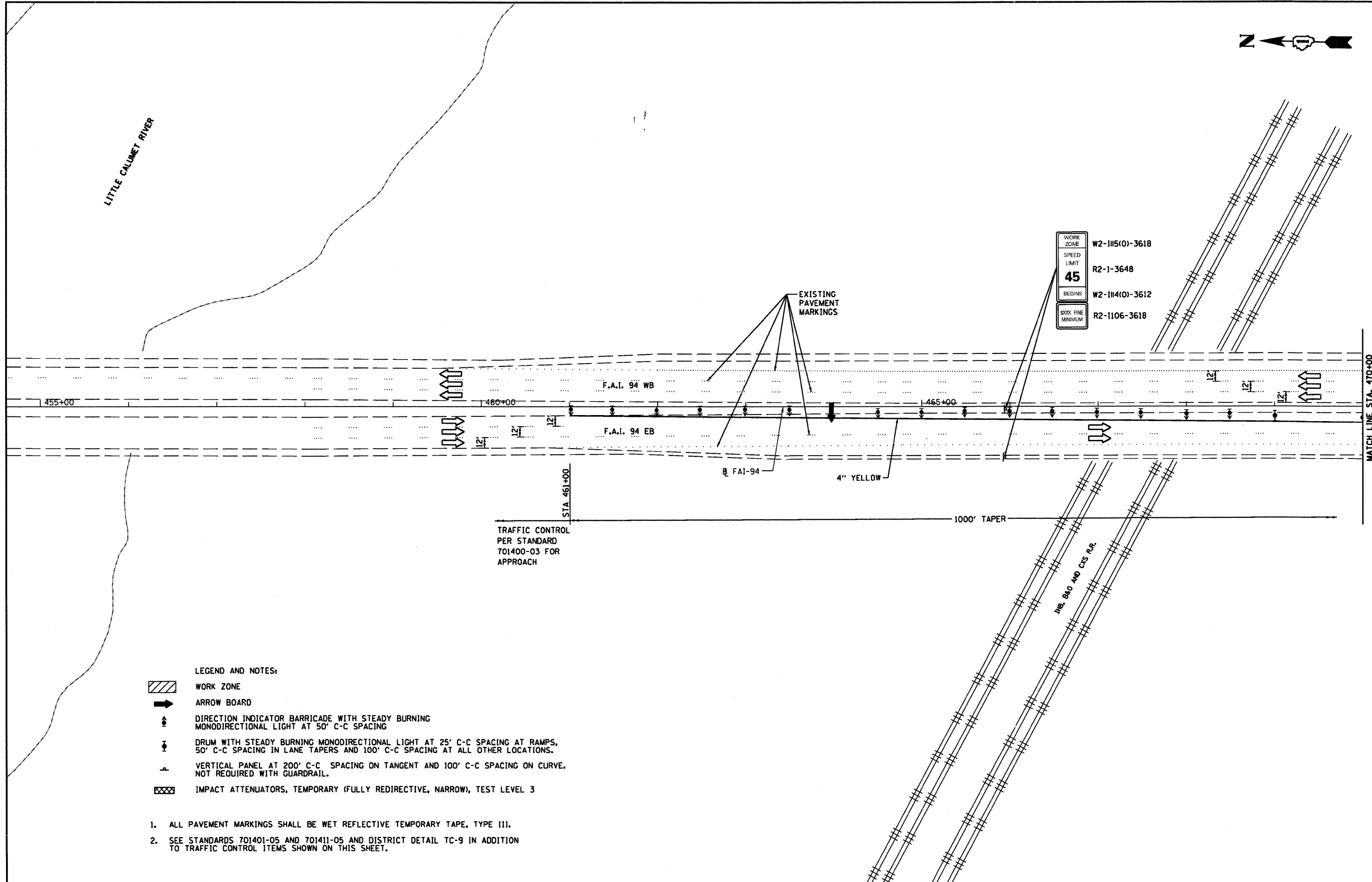
W8-11  
48" x 48"

NOTE:  
THIS SIGN SHALL BE INSTALLED  
ON BOTH SIDES OF THE ROADWAY  
500' IN ADVANCE OF AREAS WHERE  
THERE IS A GRADE DIFFERENTIAL  
BETWEEN LANES, AFTER EACH  
ENTRANCE RAMP AND MINIMUM  
OF EVERY MILE..

FILE NAME = D:\60021-033-STAGE.IV.dgn	USER NAME = dvoznarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC</b>	F.A.I. RTE. 94	SECTION (0405.ETC.42-7&0707RS-11)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 33	
	PLOT SCALE = N/A	DRAWN - DMW	REVISED -			SCALE: N/A	SHEET NO. 5 OF 24 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -								
		DATE - 2/5/2009	REVISED -								



LITTLE CALUMET RIVER



WORK ZONE  
SPEED LIMIT  
**45**  
BEGINS  
W2-1115(0)-3618  
R2-1-3648  
W2-1114(0)-3612  
R2-1106-3618

- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

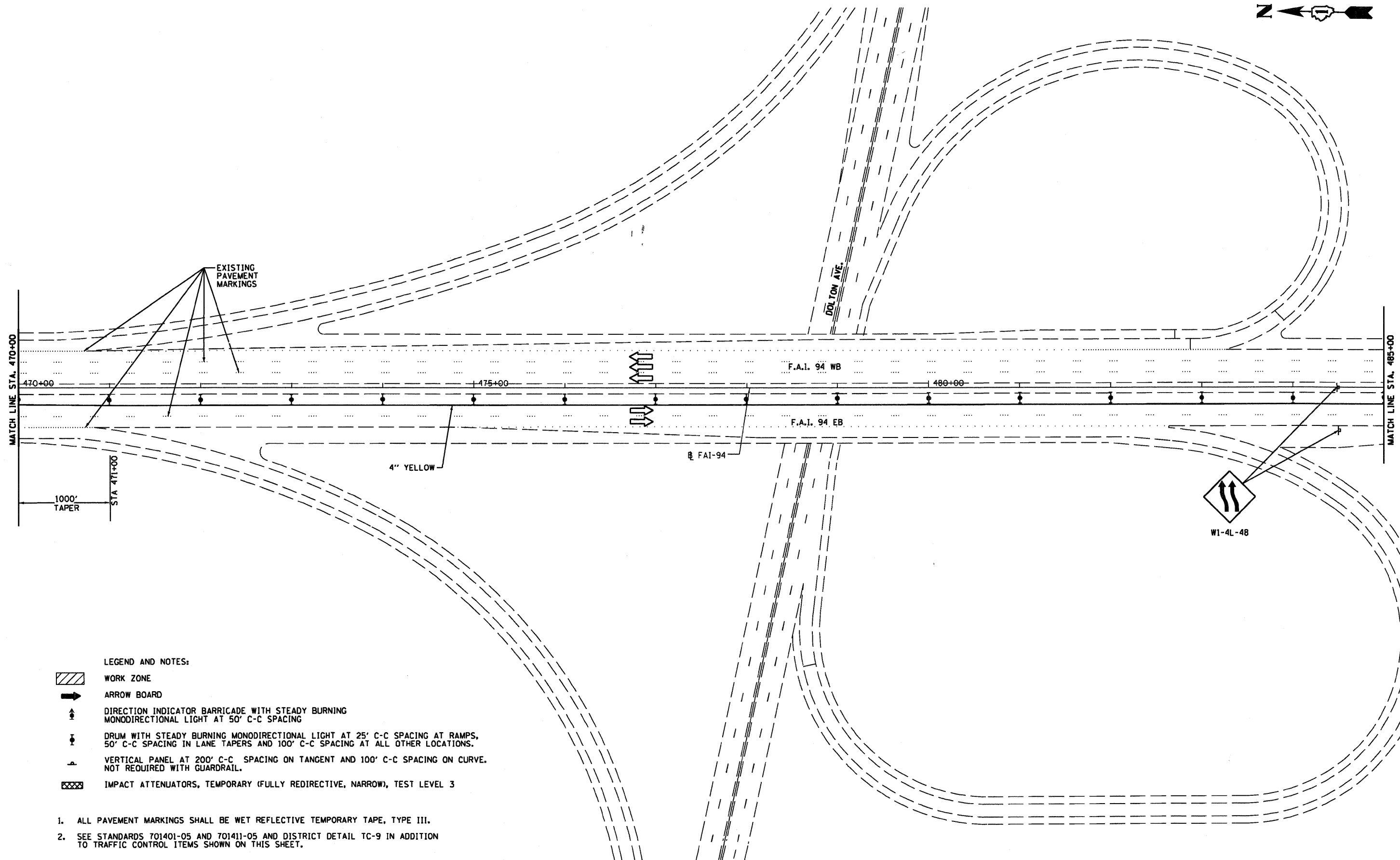
FILE NAME = D160021-034-STAGE_PLAN.STG1.dgn	USER NAME = dwozniarski	DESIGNED - DRAWN - DMW	REVISED - REVISED -
PLOT SCALE = 100.00' / IN.	CHECKED -	REVISED -	REVISED -
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BISHOP FORD EXPRESSWAY (I-94)  
SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I PLAN**

SCALE: 1" = 50'    SHEET NO. 6 OF 24 SHEETS    STA. 455+00 TO STA. 470+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	(0405,ETC,42-7&0707RS-11)	COOK	133	34
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60021	

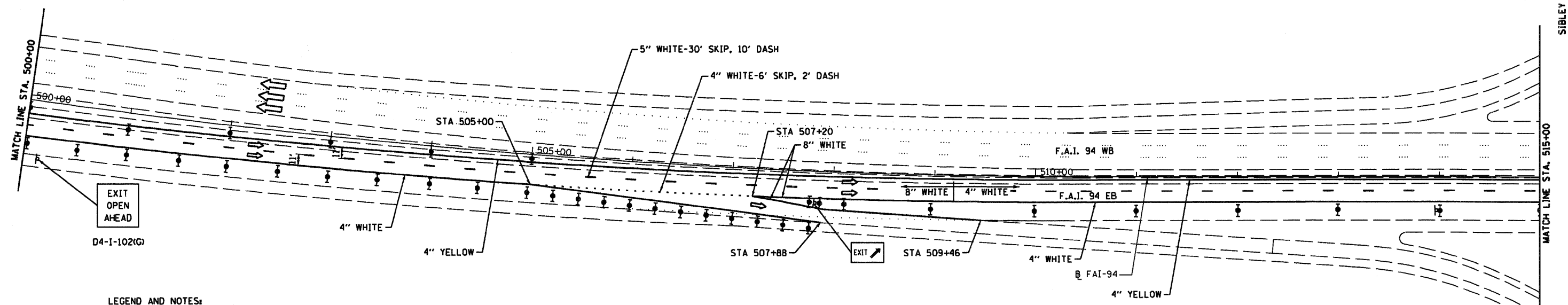
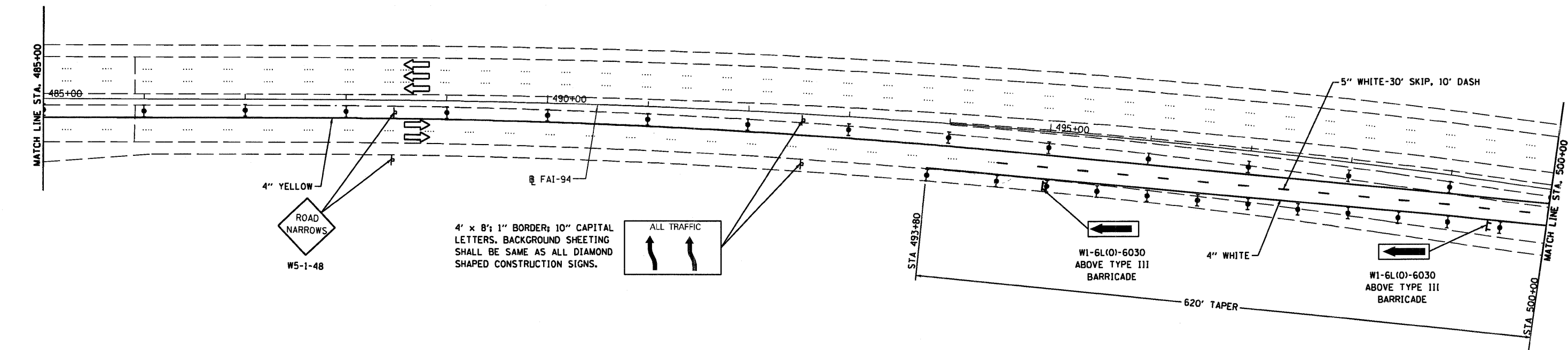


**LEGEND AND NOTES:**

- WORK ZONE
- ARROW BOARD
- DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
- DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMPS, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
- VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
- IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

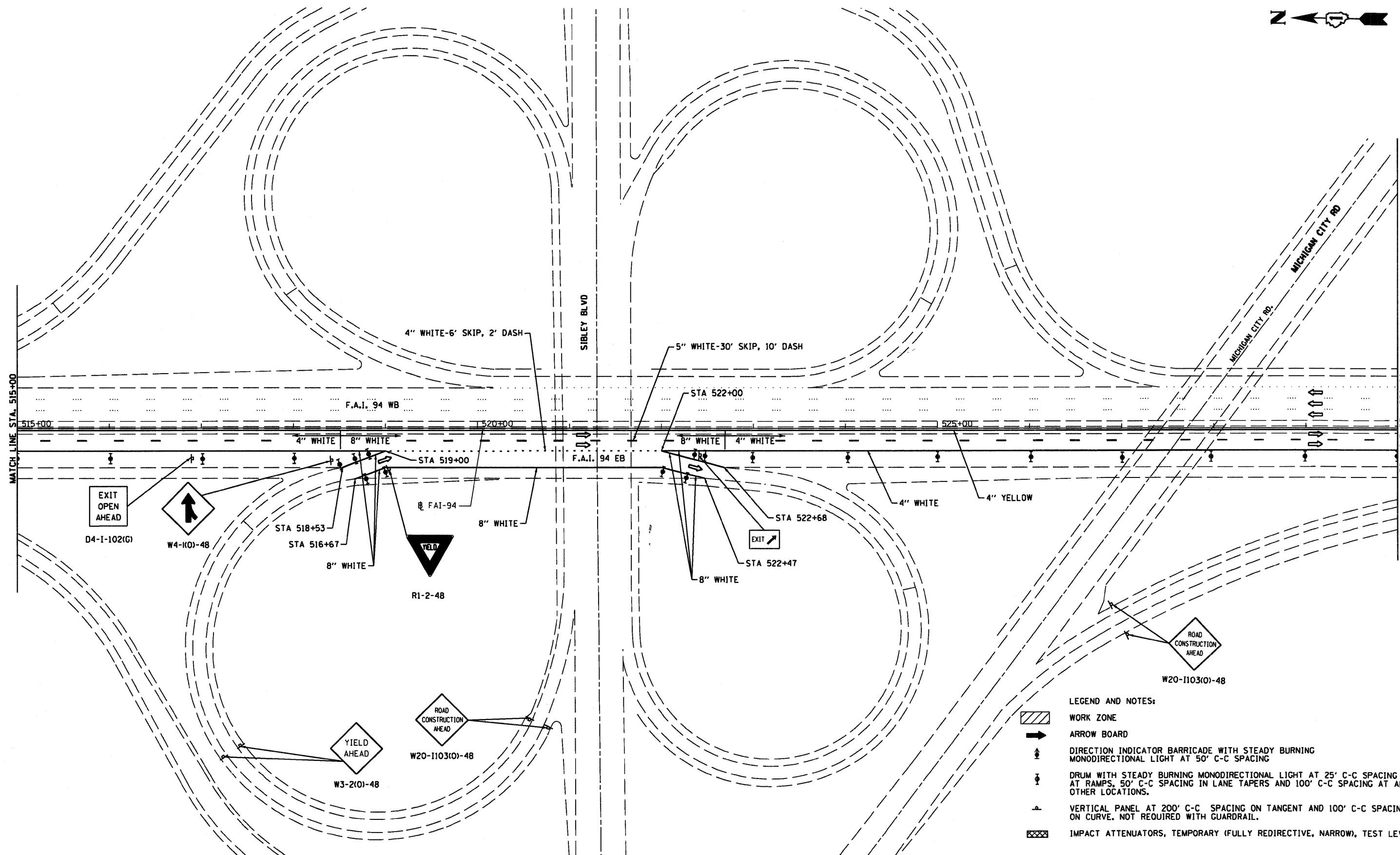
FILE NAME = D168021-835-STAGE.PLAN.STG1.dgn	USER NAME = dwozniarski	DESIGNED - DRAWN - DMW	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I PLAN</b>	F.A.I. RTE. = 94	SECTION ID405,ETC,42-7&0707R5-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 35
PLOT SCALE = 100.00' / IN.		CHECKED - DATE - 2/5/2009		SCALE: 1" = 50'		SHEET NO. 7 OF 24 SHEETS		STA. 470+00 TO STA. 485+00		CONTRACT NO. 60021
PLOT DATE = 2/5/2009		DATE - 2/5/2009		FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT				



- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

FILE NAME = D:\60021-936-STAGE_PLAN\STG1.dgn	USER NAME = dwozmirski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I PLAN</b>			F.A.I. RTE. 94	SECTION (0405,ETC.42-7&0707RS-1)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 36	
	PLOT SCALE = 100.00' / IN.	DRAWN - DMW	REVISED -		SCALE: 1" = 50'			SHEET NO. 8 OF 24 SHEETS		STA. 485+00 TO STA. 515+00		CONTRACT NO. 60021	
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -		FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT					
		DATE - 2/5/2009	REVISED -										



- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMPS, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

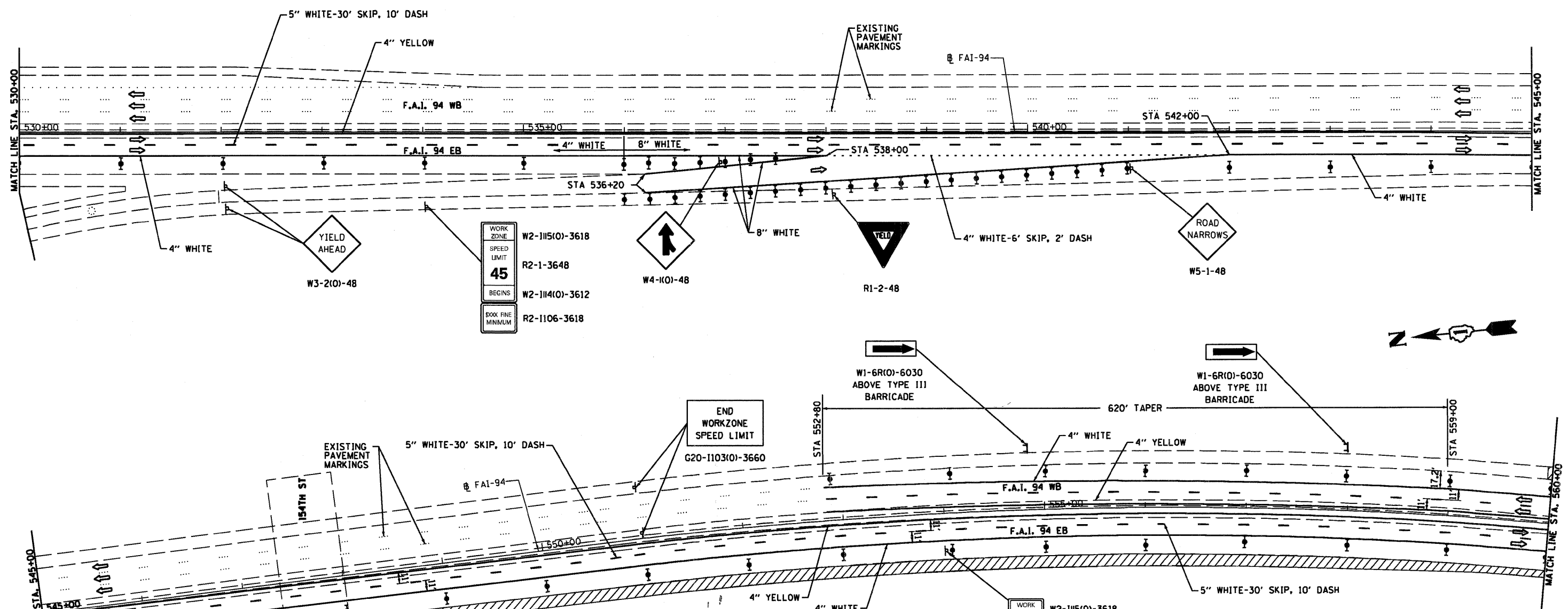
FILE NAME = D:\68021-837-STAGE_PLAN.STG1.dgn	USER NAME = dwozniarski	DESIGNED - DRAWN - DMW	REVISED - REVISED -
PLOT SCALE = 100.00' / IN.	CHECKED -	REVISED -	REVISED -
PLOT DATE = 2/5/2009	DATE = 2/5/2009	REVISED -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BISHOP FORD EXPRESSWAY (I-94)  
SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I PLAN**

SCALE: 1" = 50'    SHEET NO. 9 OF 24 SHEETS    STA. 515+00 TO STA. 530+00

F.A.I. RTE. 94	SECTION 10405.ETC.AZ-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 37
FED. ROAD DIST. NO.    ILLINOIS FED. AID PROJECT			CONTRACT NO. 60D21	



WORK ZONE	W2-1115(O)-3618
SPEED LIMIT	R2-1-3648
BEGINS	W2-1114(O)-3612
800X FINE MINIMUM	R2-1106-3618

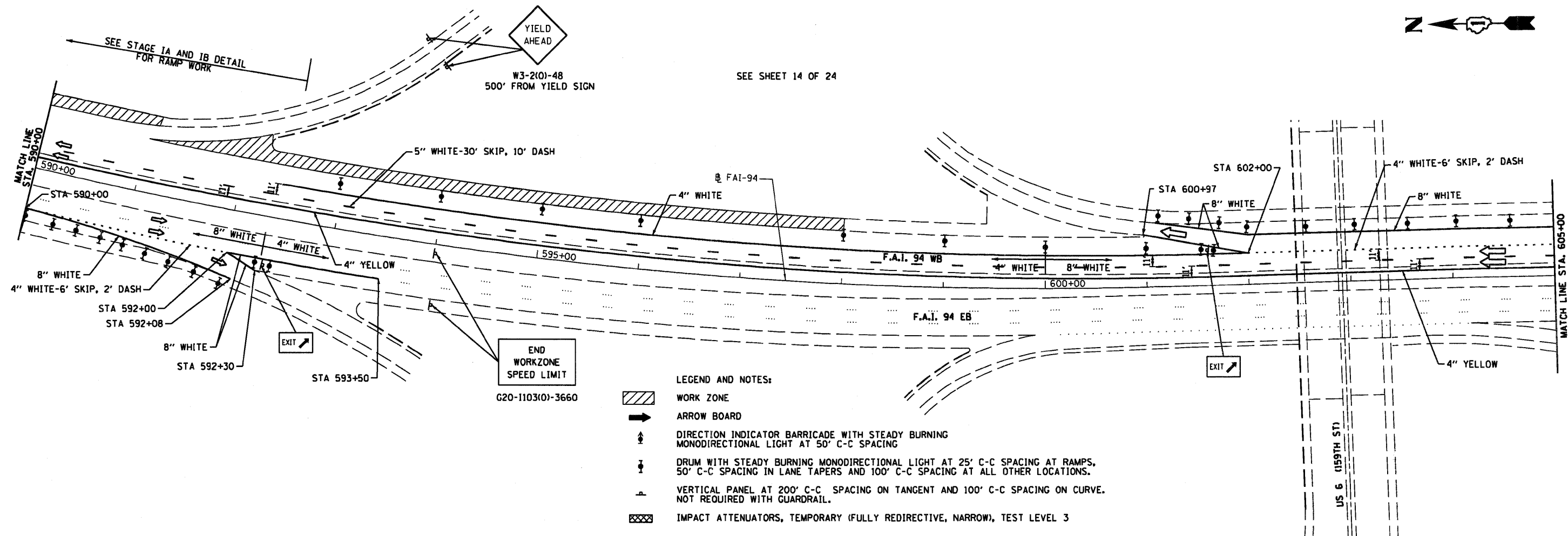
WORK ZONE	W2-1115(O)-3618
SPEED LIMIT	R2-1-3648
800X FINE MINIMUM	W2-1106(O)-3618

- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMPS, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

FILE NAME = D160021-038-STAGE.PLAN.STG1.dgn	USER NAME = dwoznierski	DESIGNED - 2/5/2009	REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94)</b> <b>SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I PLAN</b>		F.A.I. RTE. 94	SECTION 0405,ETC,42-T&O707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 38
	PLOT SCALE = 100.00' / IN.	CHECKED - DMW	REVISED -				SCALE: 1" = 50'	SHEET NO. 10 OF 24 SHEETS	STA. 530+00 TO STA. 560+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	CONTRACT NO. 60021
PLOT DATE = 2/5/2009	DATE -	REVISED -	REVISED -								



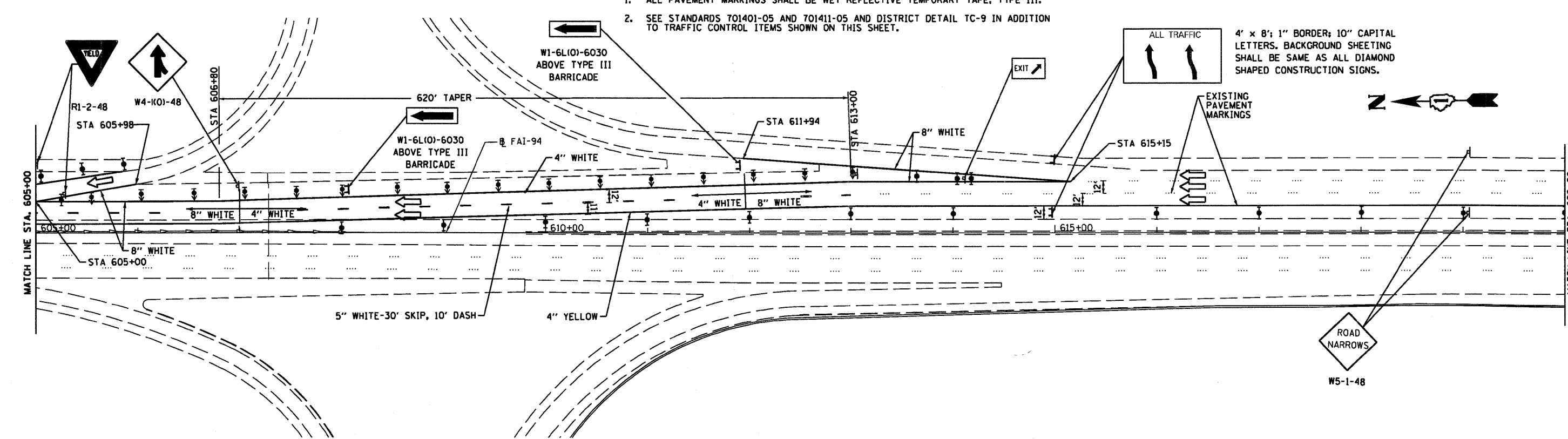


SEE SHEET 14 OF 24

- LEGEND AND NOTES:
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

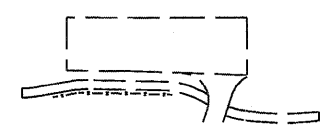
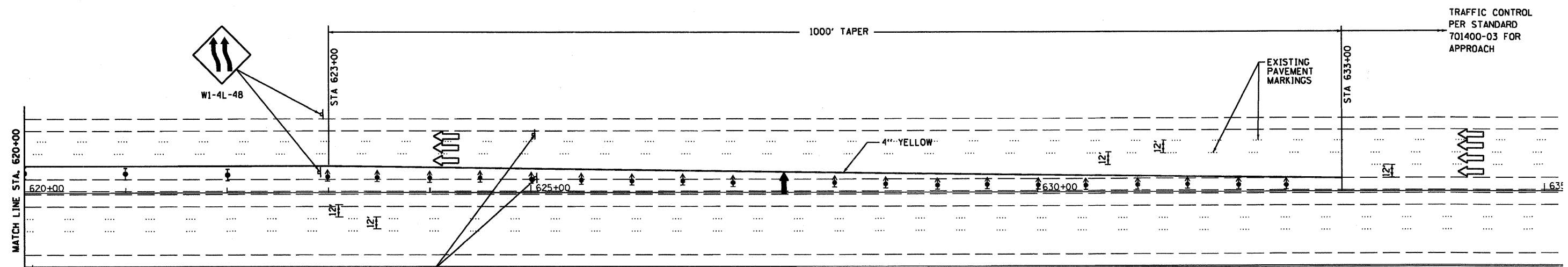
SEE SHEET 14 OF 24

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.



FILE NAME = D168021-848-STAGE_PLAN.STG1.dgn	USER NAME = dwoznarski	DESIGNED - DRAWN - CHECKED - DATE -	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I PLAN</b>		F.A.I. RTE. 94	SECTION (0405,ETC,42-780707RS-11)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 40
					SCALE: 1" = 50'	SHEET NO. 12 OF 24 SHEETS	STA. 590+0 TO STA. 620+00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60021	











WORK ZONE	W2-1115(O)-3618
SPEED LIMIT	R2-1-3648
BEGINS	W2-1114(O)-3612
SOX FINE MINIMUM	R2-1106-3618

TRAFFIC CONTROL PER STANDARD 701400-03 FOR APPROACH

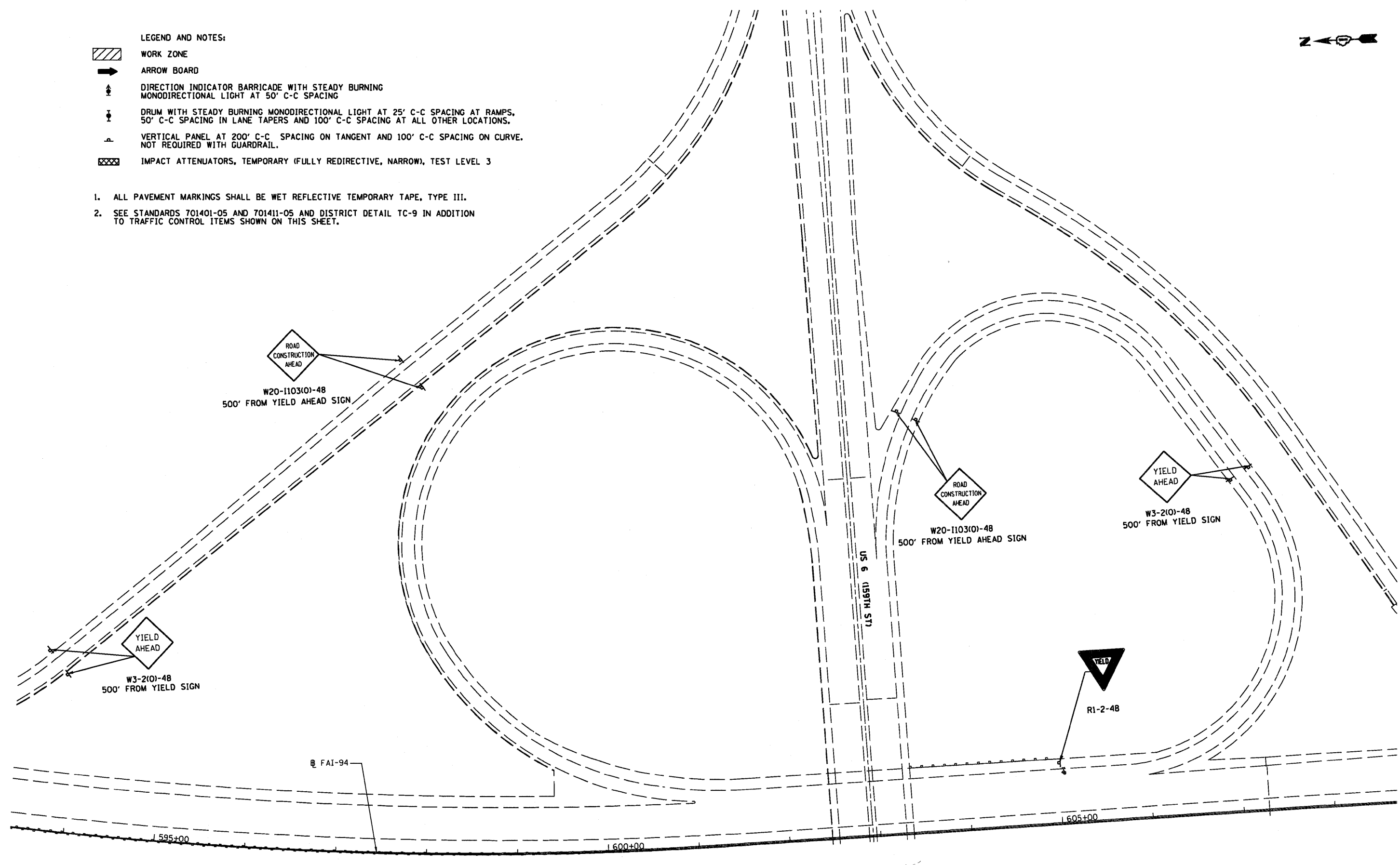
- LEGEND AND NOTES:
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

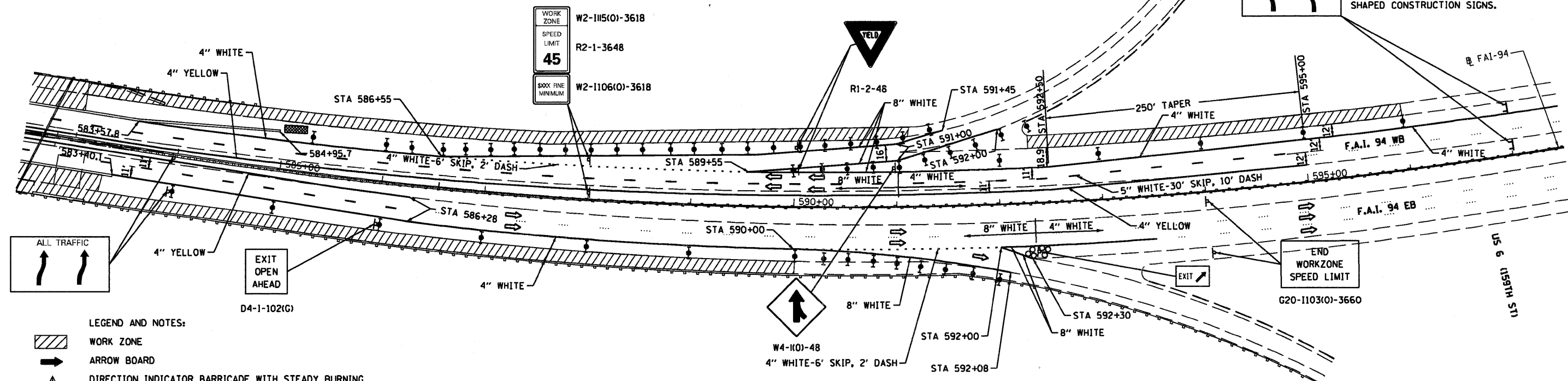
FILE NAME = D168021-B41-STAGE.PLAN.STG1.dgn	USER NAME = dwozn1@rsk1	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS</b> DEPARTMENT OF TRANSPORTATION	<b>BISHOP FORD EXPRESSWAY (I-94)</b> <b>SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I PLAN</b>		F.A.I. RTE. = 94	SECTION = (0405,ETC,42-7&0707)RS-11	COUNTY = COOK	TOTAL SHEETS = 133	SHEET NO. = 41			
	PLOT SCALE = 100.00' / IN.	DRAWN -	REVISED -				SCALE: 1" = 50'		SHEET NO. 13 OF 24 SHEETS	STA. 620+00	TO STA. 635+00	FED. ROAD DIST. NO.   ILLINOIS FED. AID PROJECT		
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -				CONTRACT NO. 60D21							
		DATE -	REVISED -											

- LEGEND AND NOTES:**
-  WORK ZONE
  -  ARROW BOARD
  -  DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  -  DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  -  VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  -  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.



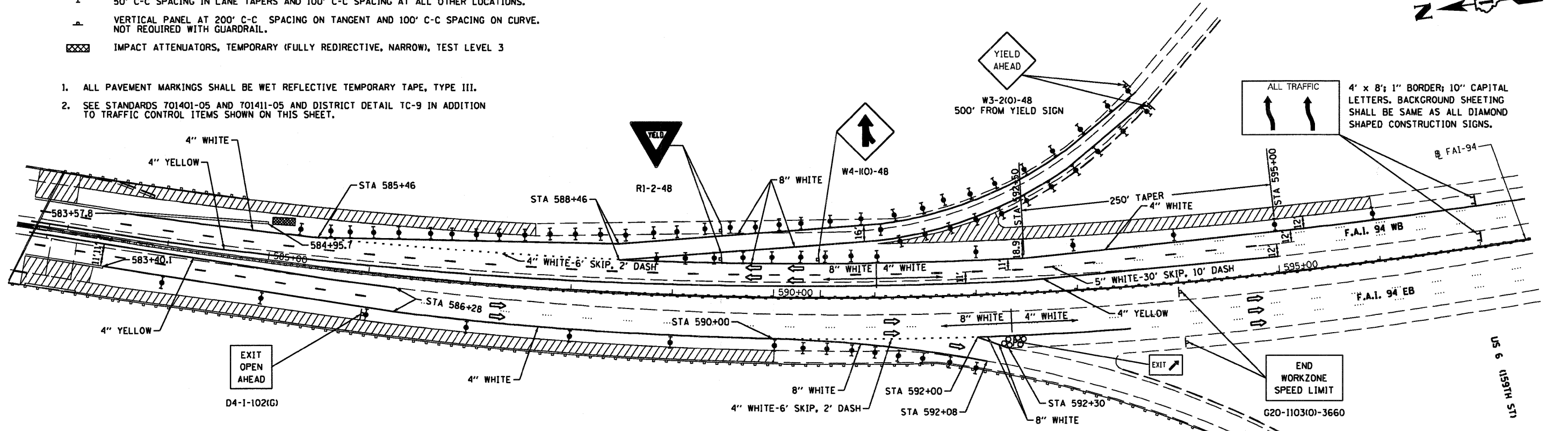
FILE NAME = D:\68021-842-STAGE.PLAN.STG1.dgn	USER NAME = dwoznieraki	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE I PLAN</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.00' / IN.	DRAWN - DMW	REVISED -					94	(D405,ETC,A2-T&0707RS-11)	COOK	133	42
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -					CONTRACT NO. 60021			FED. ROAD DIST. NO.	ILLINOIS
	DATE = 2/5/2009	DATE -	REVISED -	SCALE: 1" = 50'			SHEET NO. 14 OF 24 SHEETS		STA. 590+00 TO STA. 605+00			



- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

**STAGE IA**

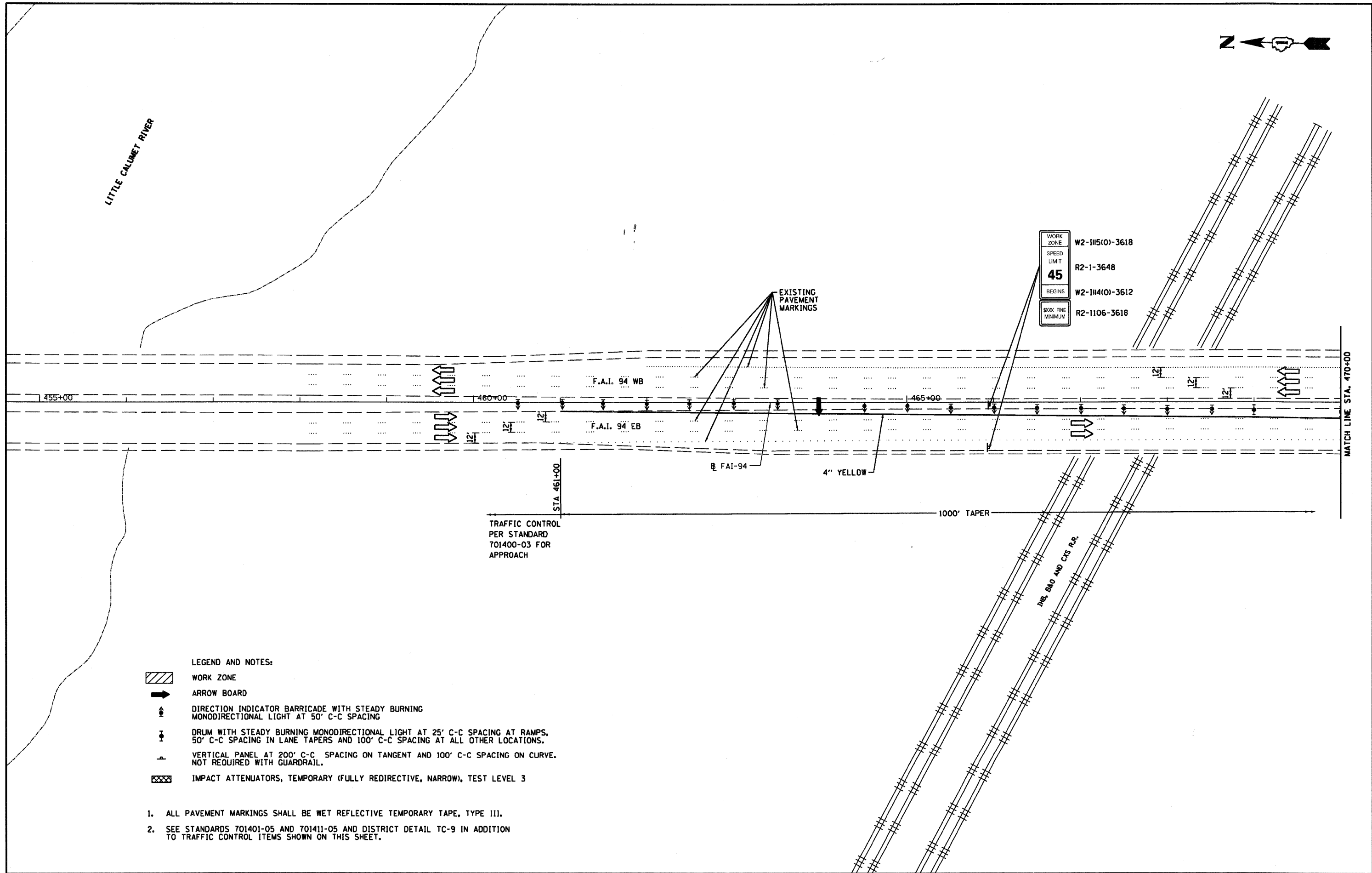


**STAGE IB**

FILE NAME = D:\60021-043-STAGE_PLAN_STGIA&B.dgn	USER NAME = dwoznarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE IA &amp; IB PLANS</b>	F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 43		
PLOT SCALE = 100.00' / IN.	CHECKED -	REVISED -	SCALE: 1" = 50'			SHEET NO. 15 OF 24 SHEETS	STA. 590+0 TO STA. 605+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	CONTRACT NO. 60021			
PLOT DATE = 2/5/2009	DATE -	REVISED -										



LITTLE CALUMET RIVER



WORK ZONE	W2-1115(0)-3618
SPEED LIMIT	R2-1-3648
BEGINS	W2-1114(0)-3612
500X FINE MINIMUM	R2-1106-3618

- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

FILE NAME =  
D160021-044-STAGE.PLAN.STG2.dgn

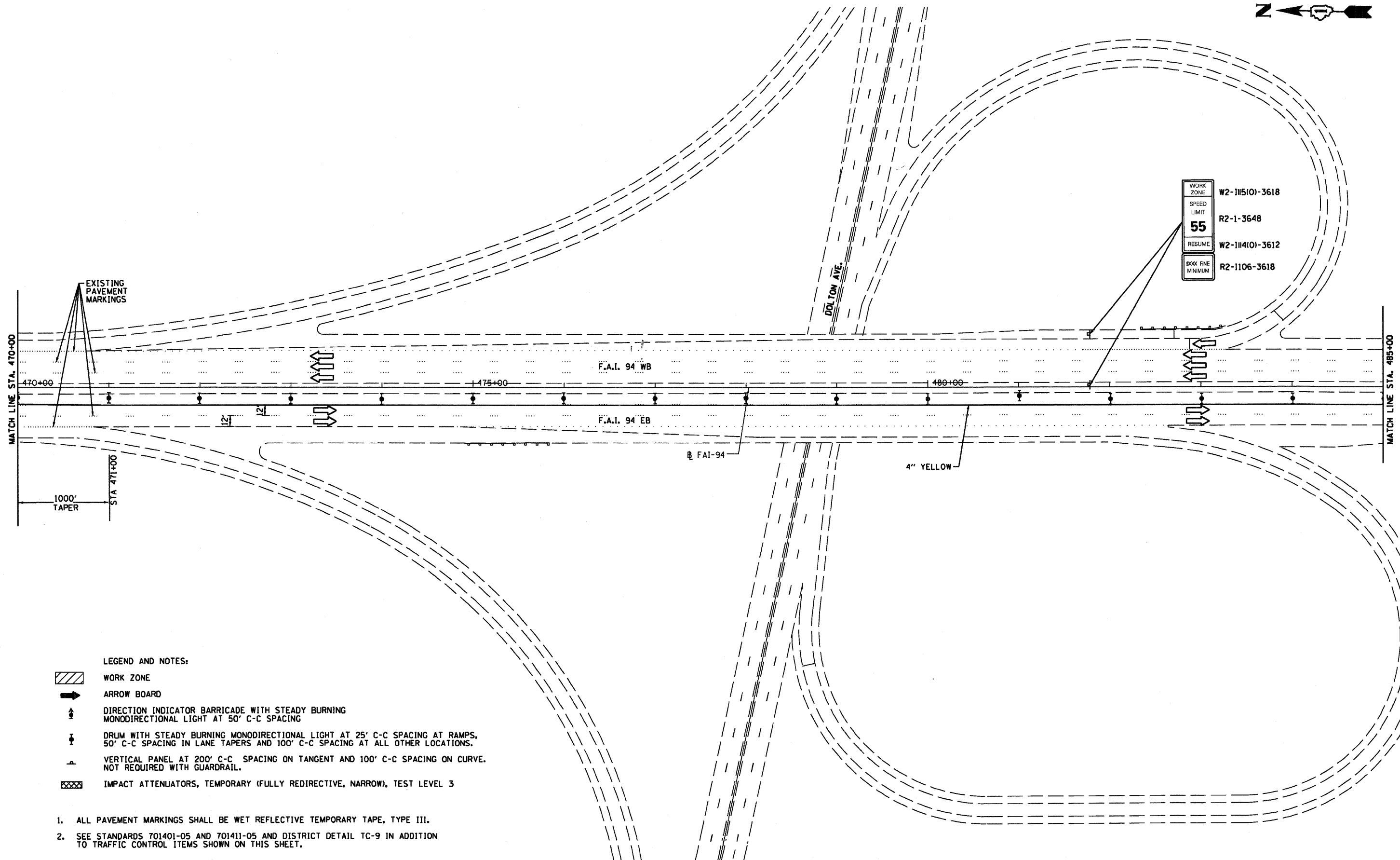
USER NAME = dwozniaraki	DESIGNED -	REVISED -
	DRAWN - DMW	REVISED -
PLOT SCALE = 100.00' / IN.	CHECKED -	REVISED -
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BISHOP FORD EXPRESSWAY (I-94)  
SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN**

SCALE: 1" = 50'    SHEET NO. 16 OF 24 SHEETS    STA. 455+00 TO STA. 470+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	(0405,ETC,42-T&0707)RS-11	COOK	133	44
CONTRACT NO. 60021			FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT	

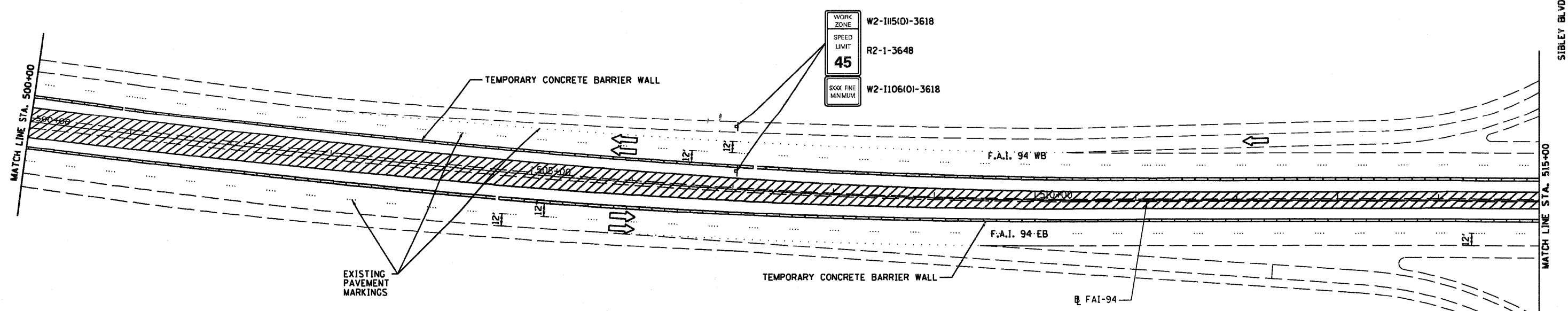
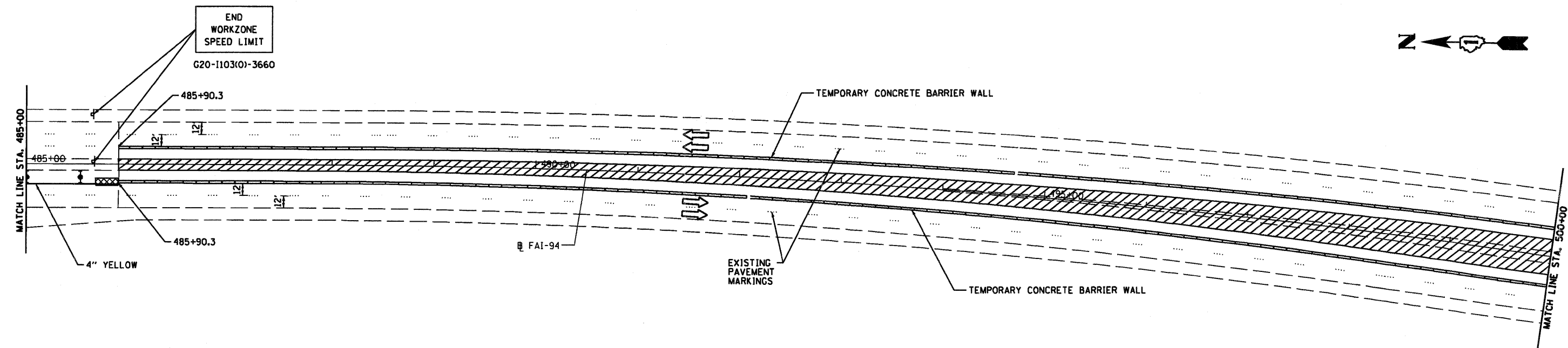


WORK ZONE	W2-1115(10)-3618
SPEED LIMIT	R2-1-3648
RESUME	W2-1114(10)-3612
BOOK FINE MINIMUM	R2-1106-3618

- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMPS, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

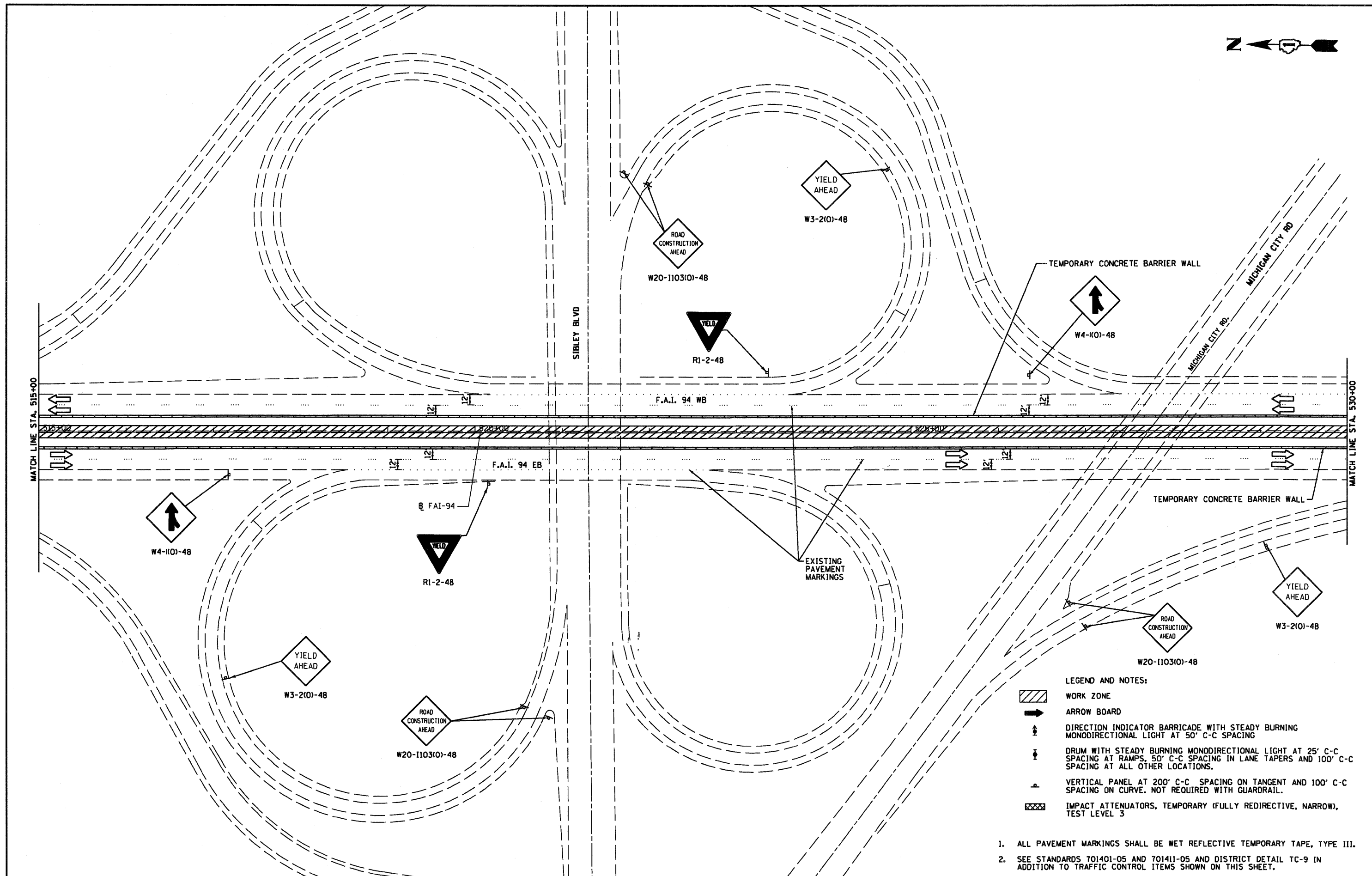
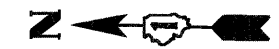
FILE NAME = D:\60021-045-STAGE_PLAN.STG2.dgn	USER NAME = dwozniczski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN</b>		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.00' / IN.	DRAWN - DMW	REVISED -				94	10405,ETC,42-T&O707RS-11	COOK	133	45
PLOT DATE = 2/5/2009	CHECKED -	DATE - 2/5/2009	REVISED -	SCALE: 1" = 50'		SHEET NO. 17 OF 24 SHEETS	STA. 470+00	TO STA. 485+00	CONTRACT NO. 60021		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT											



- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

FILE NAME = D:\60021-946-STAGE_PLAN.STG2.dgn	USER NAME = dvoznirski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN</b>	F.A.I. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 100.00' / IN.	DRAWN - DMW	REVISED -			94	(0405.ETC,42-T&0707RS-11)	COOK	133	46	
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -			SCALE: 1" = 50'		SHEET NO. 18 OF 24 SHEETS		STA. 485+00 TO STA. 515+00	
		DATE - 2/5/2009	REVISED -			CONTRACT NO. 60021					
						FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					



- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMPS, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3







1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

FILE NAME = D:\60021-847-STAGE.PLAN.STG2.dgn	USER NAME = dwoznierski	DESIGNED -	REVISED -
		DRAWN - DMW	REVISED -
		CHECKED -	REVISED -
		DATE - 2/5/2009	REVISED -

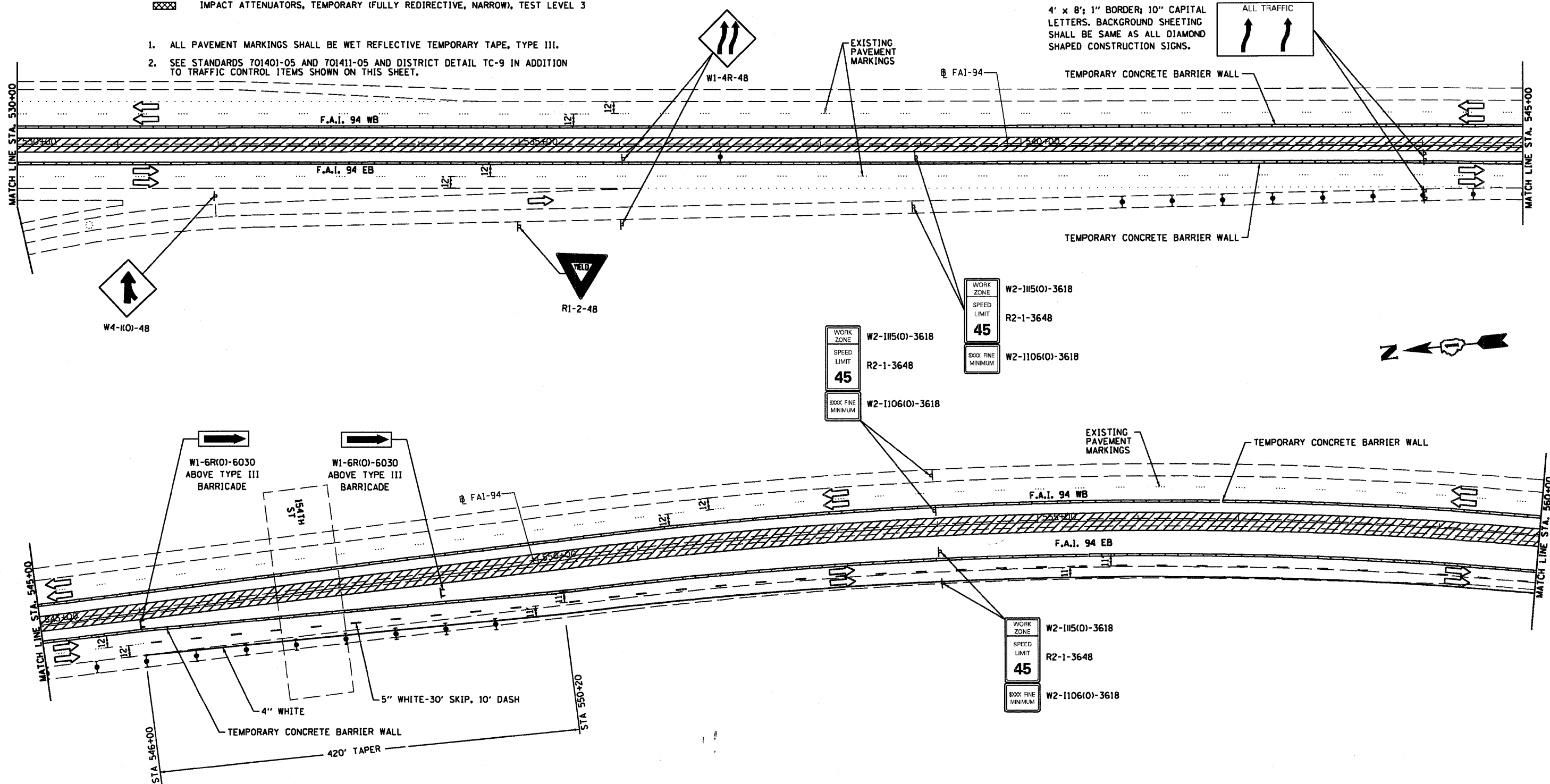
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BISHOP FORD EXPRESSWAY (I-94)  
SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	(0405,ETC,42-7&0707RS-II)	COOK	133	47
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60021	

- LEGEND AND NOTES:**
-  WORK ZONE
  -  ARROW BOARD
  -  DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  -  DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  -  VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  -  IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.



FILE NAME = D:\60021-848-STAGE\_PLAN.STG2.dgn

USER NAME = dwozniarski  
 PLOT SCALE = 100.00' / IN.  
 PLOT DATE = 2/5/2009

DESIGNED -  
 DRAWN - DMW  
 CHECKED -  
 DATE - 2/5/2009

REVISED -  
 REVISED -  
 REVISED -  
 REVISED -

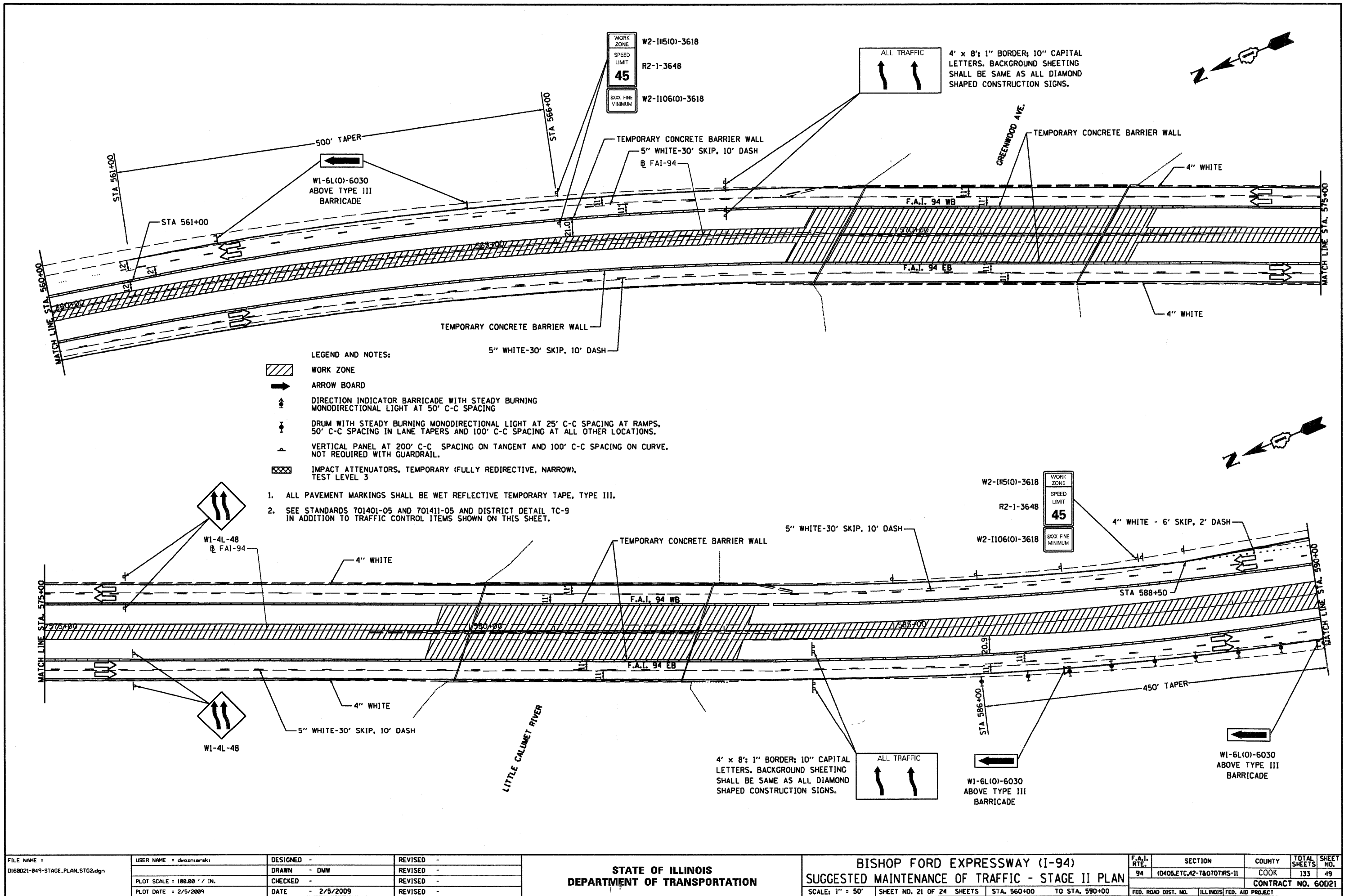
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BISHOP FORD EXPRESSWAY (I-94)  
 SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN

SCALE: 1" = 50' | SHEET NO. 20 OF 24 SHEETS | STA. 530+00 TO STA. 560+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	(0405,ETC,42-7&0707RS-11	COOK	133	48
FED. ROAD DIST. NO.   ILLINOIS FED. AID PROJECT			CONTRACT NO. 60021	

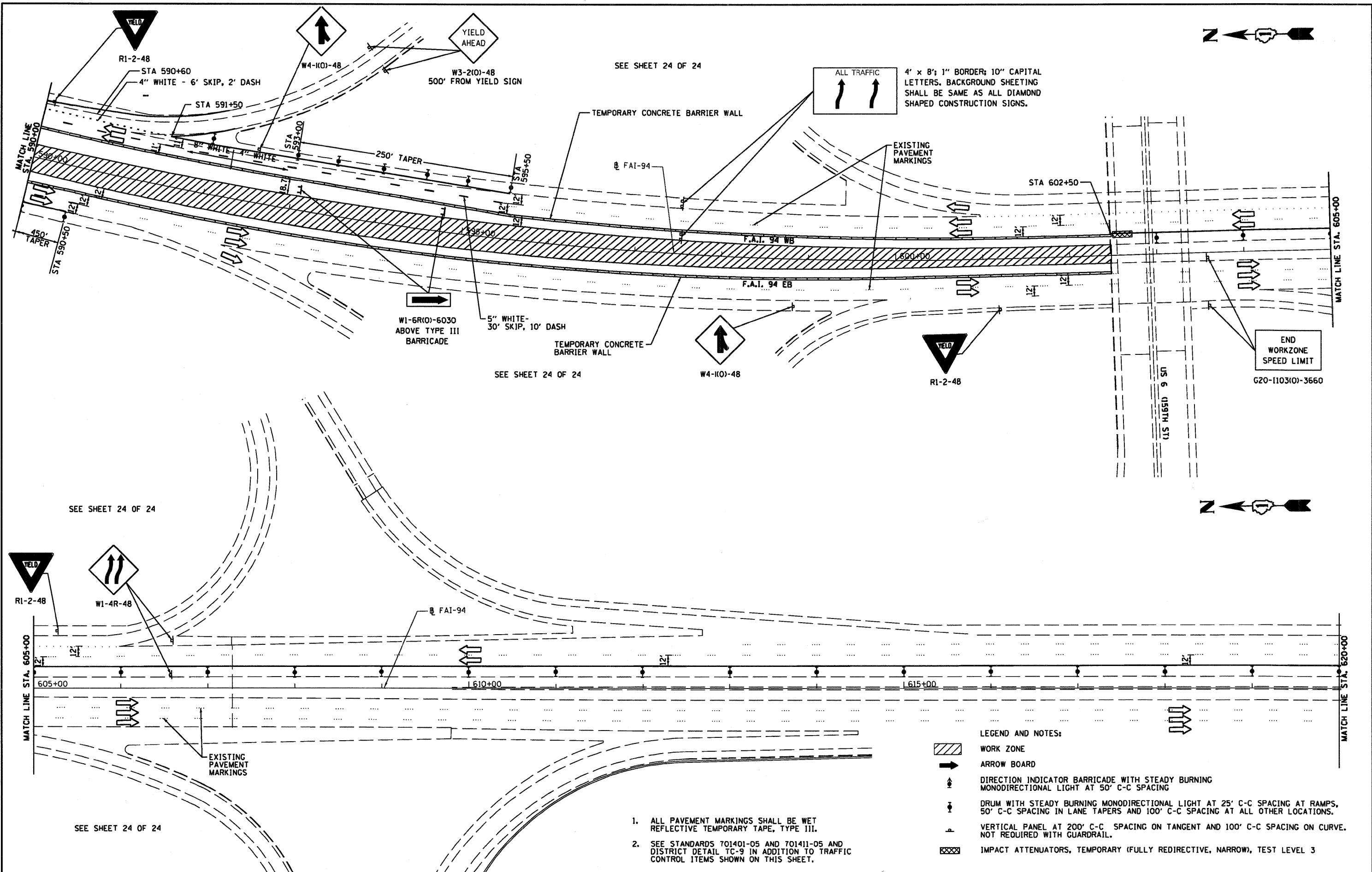




- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

FILE NAME = D168021-849-STAGE_PLAN.STG2.dgn	USER NAME = dwoznarski	DESIGNED - DRAWN - DMW	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>		<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN</b>		F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 49
PLOT SCALE = 100.00' / IN.	CHECKED -	DATE - 2/5/2009	REVISED -	SCALE: 1" = 50'		SHEET NO. 21 OF 24 SHEETS		STA. 560+00 TO STA. 590+00		CONTRACT NO. 60D21 FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		



SEE SHEET 24 OF 24

ALL TRAFFIC  
 4' x 8'; 1" BORDER; 10" CAPITAL LETTERS. BACKGROUND SHEETING SHALL BE SAME AS ALL DIAMOND SHAPED CONSTRUCTION SIGNS.

SEE SHEET 24 OF 24

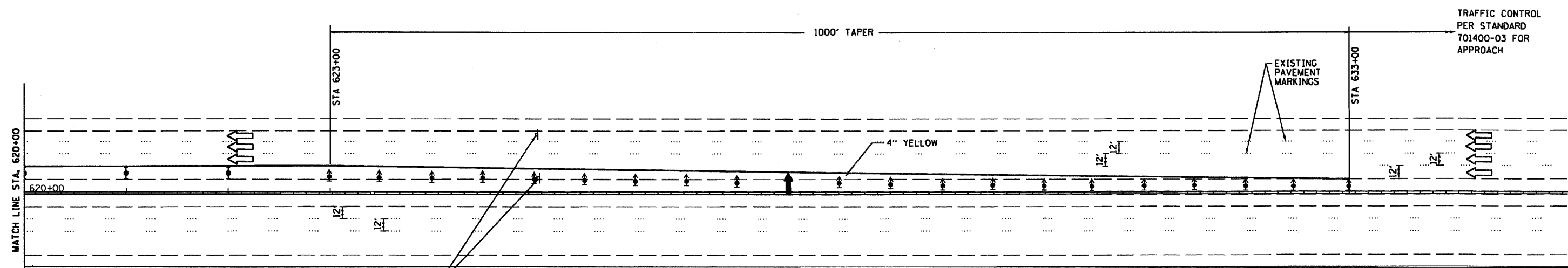
SEE SHEET 24 OF 24

SEE SHEET 24 OF 24

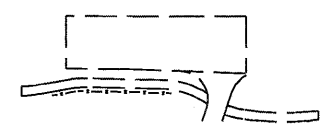
1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

- LEGEND AND NOTES:
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

FILE NAME = D:\60021-050-STAGE.PLAN.STG2.dgn	USER NAME = dvoznitski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN</b>		F.A.I. RTE. 94	SECTION (D405,ETC,42-780707RS-11)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 50
	PLOT SCALE = 100.00' / IN.	DRAWN -	REVISED -		SCALE: 1" = 50'	SHEET NO. 22 OF 24 SHEETS	STA. 590+00 TO STA. 620+00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60021	
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -								
		DATE -	REVISED -								



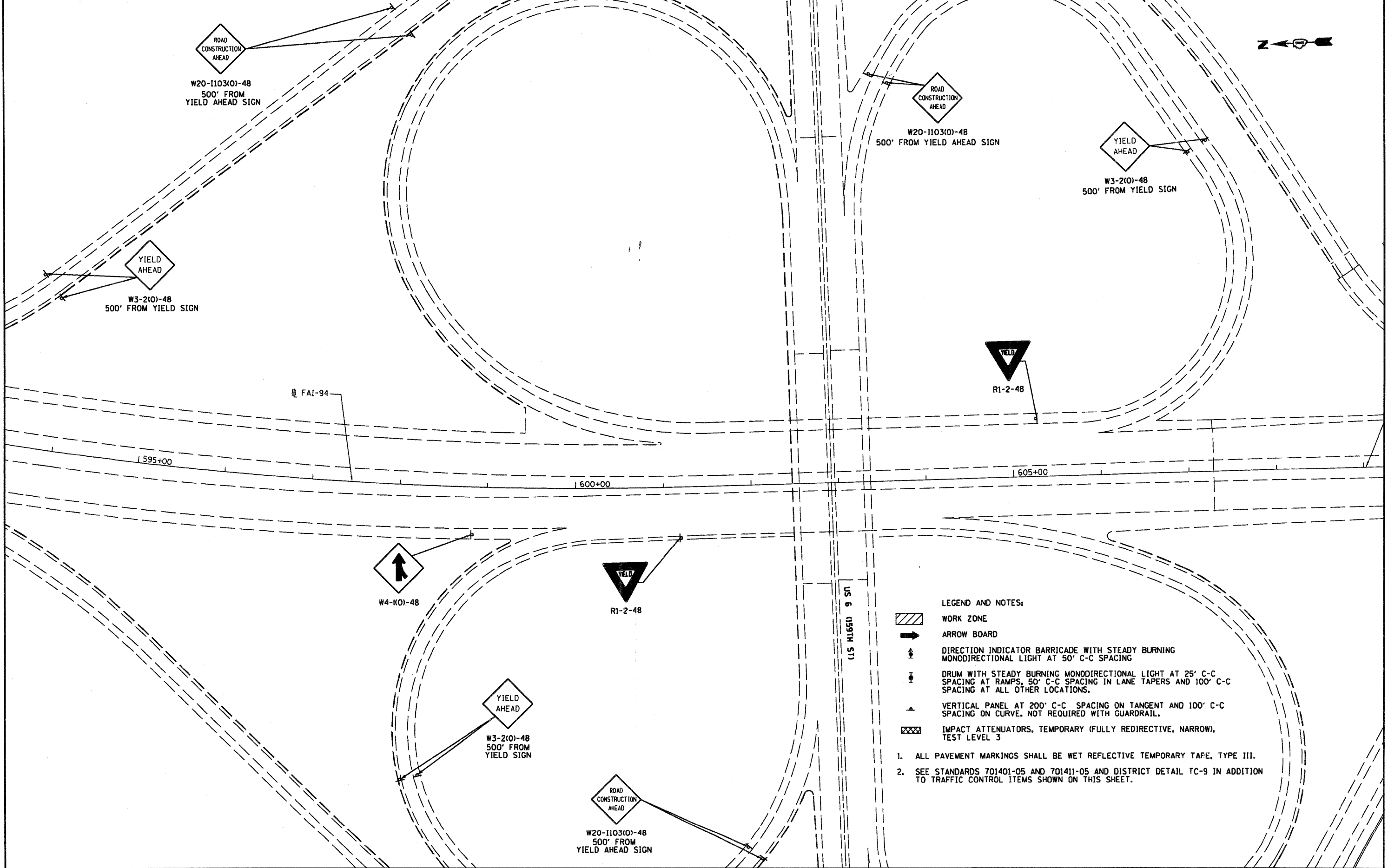
WORK ZONE	W2-II1510-3618
SPEED LIMIT	R2-1-3648
<b>45</b>	
BEGINS	W2-II1410-3612
XXXX FINE MINIMUM	R2-1106-3618



- LEGEND AND NOTES:
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMP, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3

1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

FILE NAME = DI68021-051-STAGE.PLAN.STG2.dgn	USER NAME = dwoznarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN</b>	F.A.J. RTE. 94	SECTION 10405.ETC.42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 51	
	PLOT SCALE = 100.00' / IN.	DRAWN -	REVISED -			SCALE: 1" = 50'	SHEET NO. 23 OF 24 SHEETS	STA. 620+00	TO STA. 634+00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -			CONTRACT NO. 60021					
		DATE -	REVISED -								




- LEGEND AND NOTES:**
- WORK ZONE
  - ARROW BOARD
  - DIRECTION INDICATOR BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 50' C-C SPACING
  - DRUM WITH STEADY BURNING MONODIRECTIONAL LIGHT AT 25' C-C SPACING AT RAMPS, 50' C-C SPACING IN LANE TAPERS AND 100' C-C SPACING AT ALL OTHER LOCATIONS.
  - VERTICAL PANEL AT 200' C-C SPACING ON TANGENT AND 100' C-C SPACING ON CURVE. NOT REQUIRED WITH GUARDRAIL.
  - IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3
1. ALL PAVEMENT MARKINGS SHALL BE WET REFLECTIVE TEMPORARY TAPE, TYPE III.
  2. SEE STANDARDS 701401-05 AND 701411-05 AND DISTRICT DETAIL TC-9 IN ADDITION TO TRAFFIC CONTROL ITEMS SHOWN ON THIS SHEET.

FILE NAME = D160021-052-STAGE_PLAN.STG2.dgn	USER NAME = dwoznieraki	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SUGGESTED MAINTENANCE OF TRAFFIC - STAGE II PLAN</b>	F.A.I. RTE. 94	SECTION 10405.ETC.42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 52		
PLOT SCALE = 100.00' / IN.		CHECKED - DMW	REVISED -			SCALE: 1" = 50'		SHEET NO. 24 OF 24 SHEETS		STA. 590+00 TO STA. 605+00		CONTRACT NO. 60D21
PLOT DATE = 2/5/2009		DATE - 2/5/2009	REVISED -			FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT				


**LEGEND**

① RAMP CLOSED  
PER TC-8: FREEWAY  
ENTRANCE AND  
EXIT RAMP  
CLOSURE DETAILS

 WORK ZONE

 DETOUR

**DETOUR SIGNS**

 W20-2  
48 x 48


 M1-7  
21 x 9


 M4-8a  
24 x 18

 M1-1  
24 x 24

**ARROW SIGNS**

 M5-2R  
21 x 15

 M6-3  
21 x 15

 M6-2R  
21 x 15

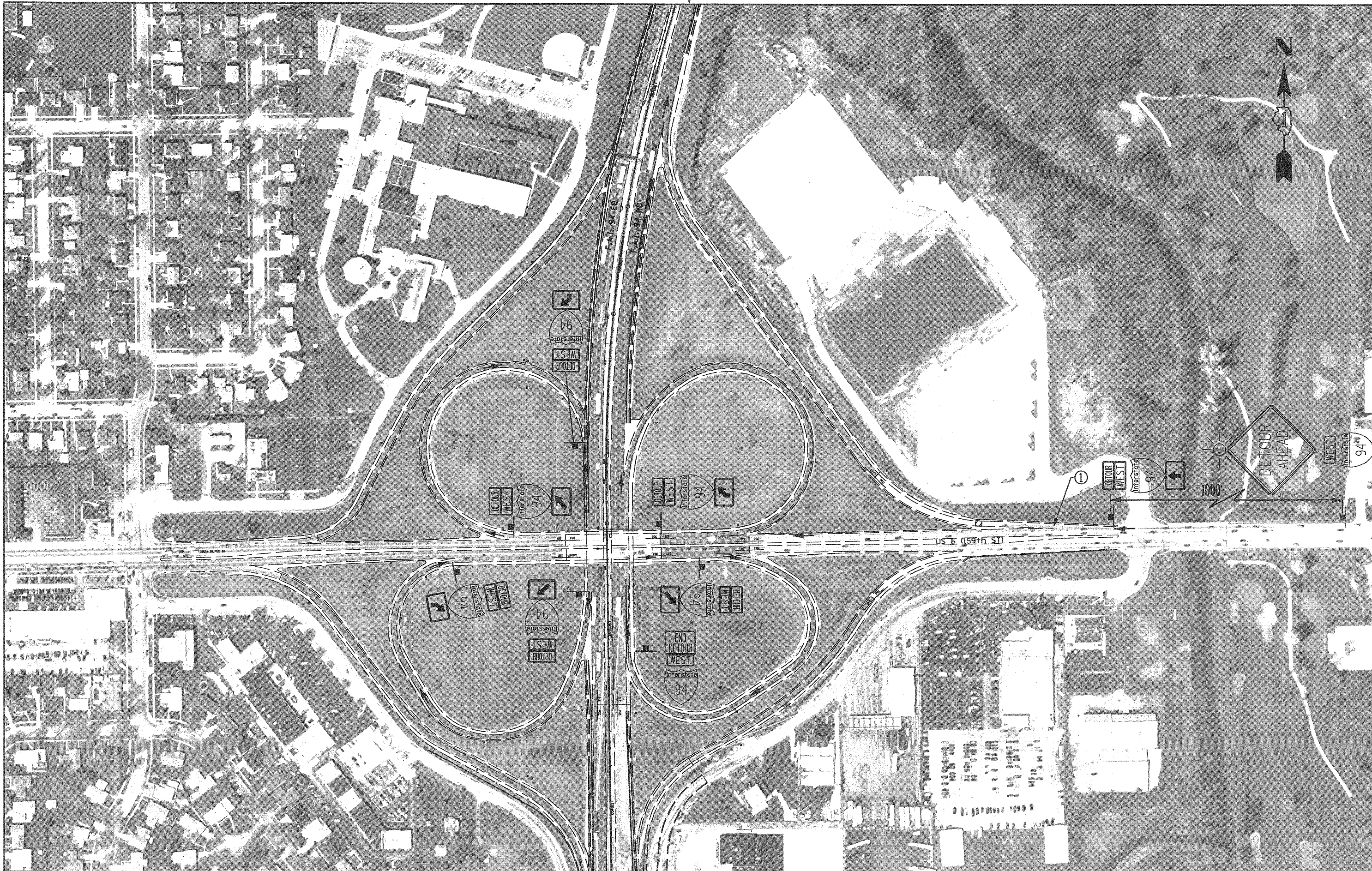
**CARDINAL DIRECTIONS**

 M3-4  
21 x 9

**DETOUR NOTES**

1. THIS WORK SHALL CONSIST OF THE FURNISHING, INSTALLATION, MAINTENANCE, RELOCATION AND REMOVAL OF SIGNS AND SUPPORTS FOR THE DETOURS SHOWN IN THE PLANS. THIS WORK SHALL BE PERFORMED ACCORDING TO SECTION 101 OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, IDOT STANDARDS, DISTRICT DETAILS AND THE DETOUR PLANS CONTAINED HEREIN FOR THE PCC PATCH SHOWN IN RESURFACING PLANS.
2. TRAFFIC CONTROL FOR RAMP CLOSURES WILL BE PERFORMED PER DISTRICT DETAIL TC-8 - FREEWAY ENTRANCE AND EXIT RAMP CLOSURE DETAILS.
3. THE CONTRACTOR SHALL PROVIDE A MINIMUM OF 72 HOURS WRITTEN NOTICE TO THE ENGINEER PRIOR TO CLOSING RAMP.
4. ONE CHANGEABLE MESSAGE SIGN WILL BE USED ON WESTBOUND US 6. TO NOTIFY THE PUBLIC OF THE PENDING RAMP CLOSURES. CHANGEABLE MESSAGE SIGNS WILL OPERATE FOR A PERIOD OF 7 DAYS BEFORE RAMP THE SCHEDULED RAMP CLOSURE.
5. ALL TRAFFIC CONTROL DEVICES PER THE DETOUR PLAN SHEETS WILL BE IN PLACE BEFORE CONSTRUCTION IS STARTED.
6. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND TRAFFIC CONTROL DEVICES MAY BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
7. THE DETOUR FOR WESTBOUND US 6 TO WESTBOUND I-94 WILL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR TRAFFIC CONTROL PROTECTION, (DETOUR 1).

FILE NAME = D:\60021-053-DETOURNOTE.dgn	USER NAME = dwoznieraki	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BISHOP FORD EXPRESSWAY (I-94) DETOUR PLAN NOTES AND LEGEND	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 200.00' / IN.	DRAWN - DMW	REVISED -			94	(0405,ETC,42-7&0707RS-11)	COOK	133	53
PLOT DATE = 2/5/2009	CHECKED -	REVISED -	REVISED -	SCALE: 1" = 50'	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		
	DATE - 2/5/2009	REVISED -	REVISED -					CONTRACT NO. 60021		



FILE NAME = D:\68021-054-US6\_2A\_DETOUR.dgn  
 USER NAME = dwoznarski

DESIGNED -  
 DRAWN - DMW  
 CHECKED -  
 DATE - 2/5/2009

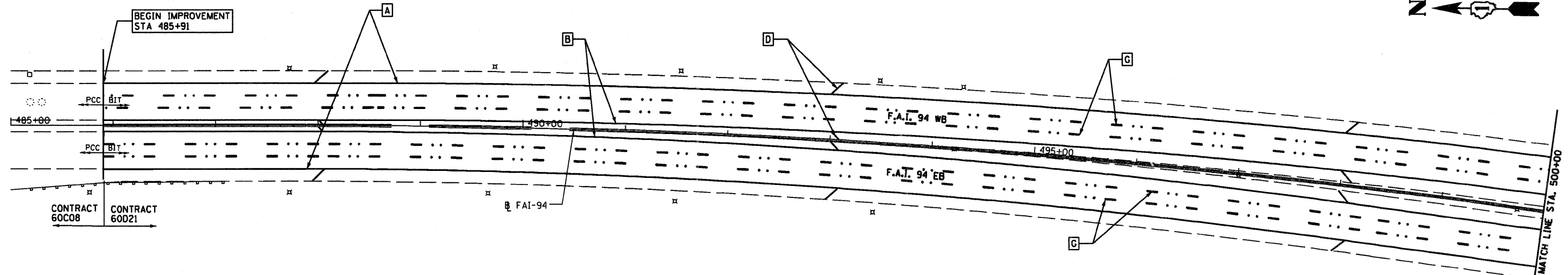
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

BISHOP FORD EXPRESSWAY (I-94)  
 US 6 (159th ST) DETOUR PLAN

SCALE: 1" = 120' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
94	(0405,ETC,42-7&0707RS-11	COOK	133	54
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 60D21	



- PAVEMENT MARKING LEGEND**
- A THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
  - B THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE
  - C THERMOPLASTIC PAVEMENT MARKING - LINE 8" WHITE EDGE LINE
  - D THERMOPLASTIC PAVEMENT MARKING - LINE 12" PAVEMENT DIAGNOLS SPACED @ 500' CENTERS
  - E THERMOPLASTIC PAVEMENT MARKING - LINE 12" GORE DIAGNOLS SPACED @ 30' CENTERS
  - F PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 4" WHITE LANE LINES 2' DASH - 6' SKIP

- G PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 5" WHITE LANE LINES 10' DASH - 30' SKIP
- H POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" WHITE EDGE LINE
- I POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" YELLOW EDGE LINE
- J POLYUREA PAVEMENT MARKING, TYPE I, LINE 5" WHITE LANE LINES 10' DASH - 30' SKIP

**GUIDE SIGN SEQUENCE NUMBERING CODE**

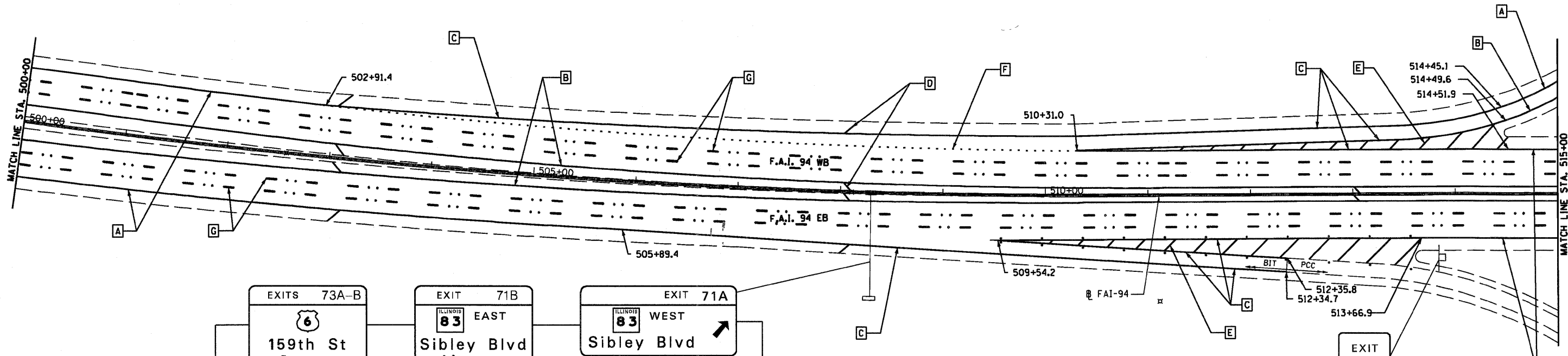
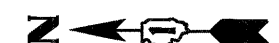
**EXAMPLE**  
EB-TR-03

**DIRECTION OF TRAFFIC**  
EB - EASTBOUND I-94  
WB - WESTBOUND I-94  
SI - STONY ISLAND FEEDER RAMP

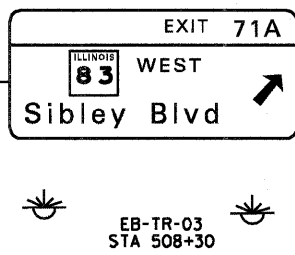
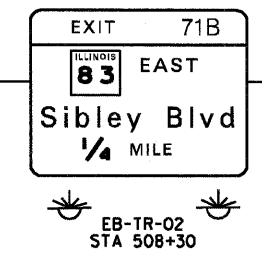
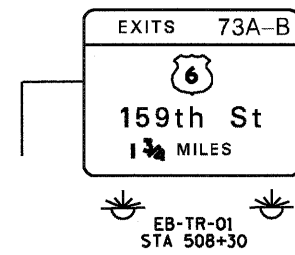
**SIGN PANEL NUMBER**

**MOUNTING TYPE**  
TR - Truss  
CL - Cantilever  
BR - Bridge Mounted  
WP - Wood Post  
BS - Breakaway Steel  
LP - Light Pole

REMOVE AND REPLACE SIGN PANEL  
 EXISTING LUMINAIRE



**NOTE:**  
SEE TC-12 "MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT & TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS



150161094R071.1-000

FILE NAME = DI60021-055-PMK-1.dgn	USER NAME = dvoznirski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) PAVEMENT MARKING AND SIGNING PLANS</b>	F.A.I. RTE. 94	SECTION (D405,ETC,42-7&0707RS-1)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 55
PLOT SCALE = 100.00' / IN.	PLOT DATE = 2/5/2009	DRAWN - DMW	REVISOR -	SCALE: 1" = 50'	SHEET NO. 1 OF 9 SHEETS	STA. 485+91 TO STA. 515+00		CONTRACT NO. 60021		
		CHECKED -	REVISOR -	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT						
		DATE - 2/5/2009	REVISOR -							

GUIDE SIGN SEQUENCE  
NUMBERING CODE

EXAMPLE  
EB-TR-03

DIRECTION OF TRAFFIC

EB - EASTBOUND I-94  
WB - WESTBOUND I-94  
SI - STONY ISLAND FEEDER RAMPS

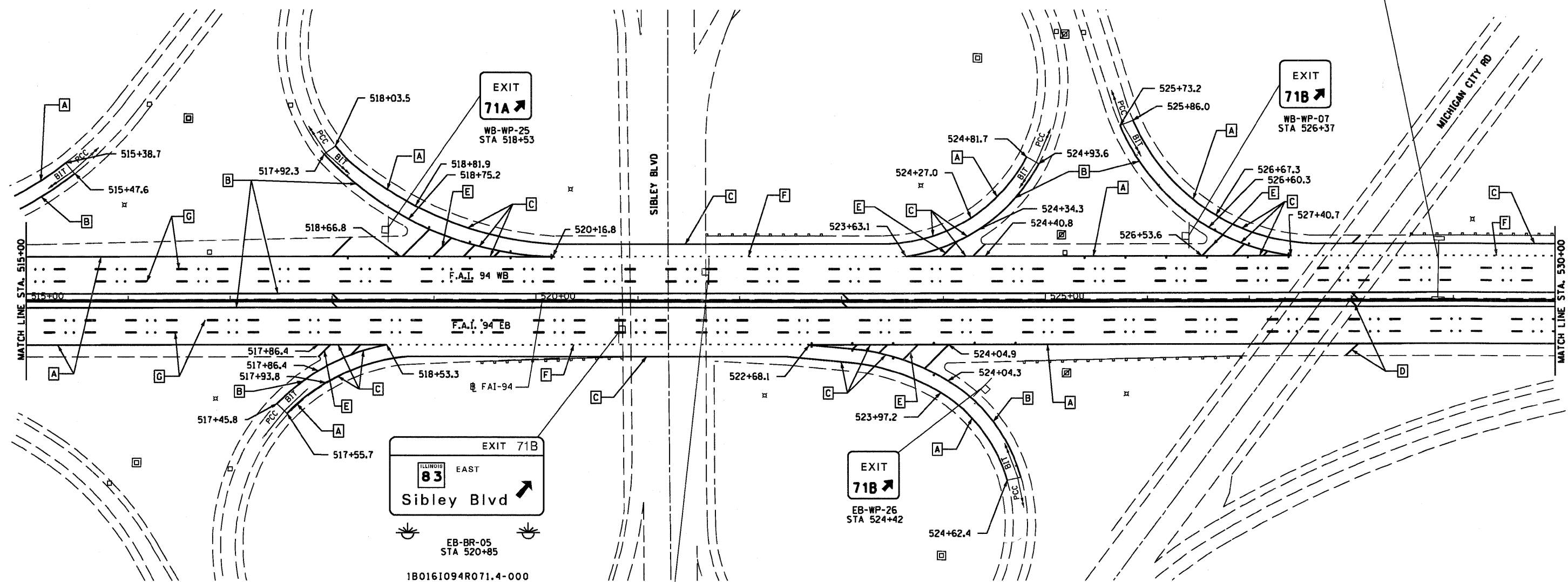
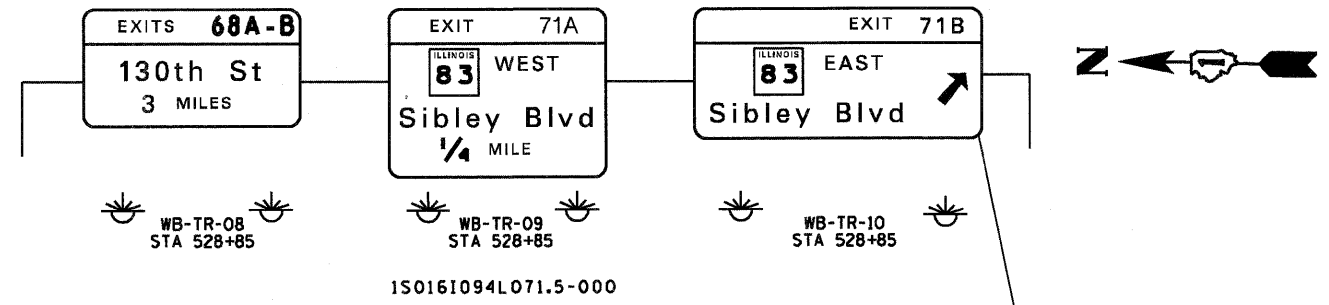
SIGN PANEL NUMBER

MOUNTING TYPE

TR - Truss  
CL - Cantilever  
BR - Bridge Mounted  
WP - Wood Post  
BS - Breakaway Steel  
LP - Light Pole

REMOVE AND REPLACE  
SIGN PANEL

EXISTING LUMINAIRE



PAVEMENT MARKING LEGEND

- A THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
- B THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE
- C THERMOPLASTIC PAVEMENT MARKING - LINE 8" WHITE EDGE LINE
- D THERMOPLASTIC PAVEMENT MARKING - LINE 12" PAVEMENT DIAGONLS SPACED @ 500' CENTERS
- E THERMOPLASTIC PAVEMENT MARKING - LINE 12" CORE DIAGONLS SPACED @ 30' CENTERS
- F PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 4" WHITE LANE LINES 2' DASH - 6' SKIP

- G PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 5" WHITE LANE LINES 10' DASH - 30' SKIP
- H POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" WHITE EDGE LINE
- I POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" YELLOW EDGE LINE
- J POLYUREA PAVEMENT MARKING, TYPE I, LINE 5" WHITE LANE LINES 10' DASH - 30' SKIP

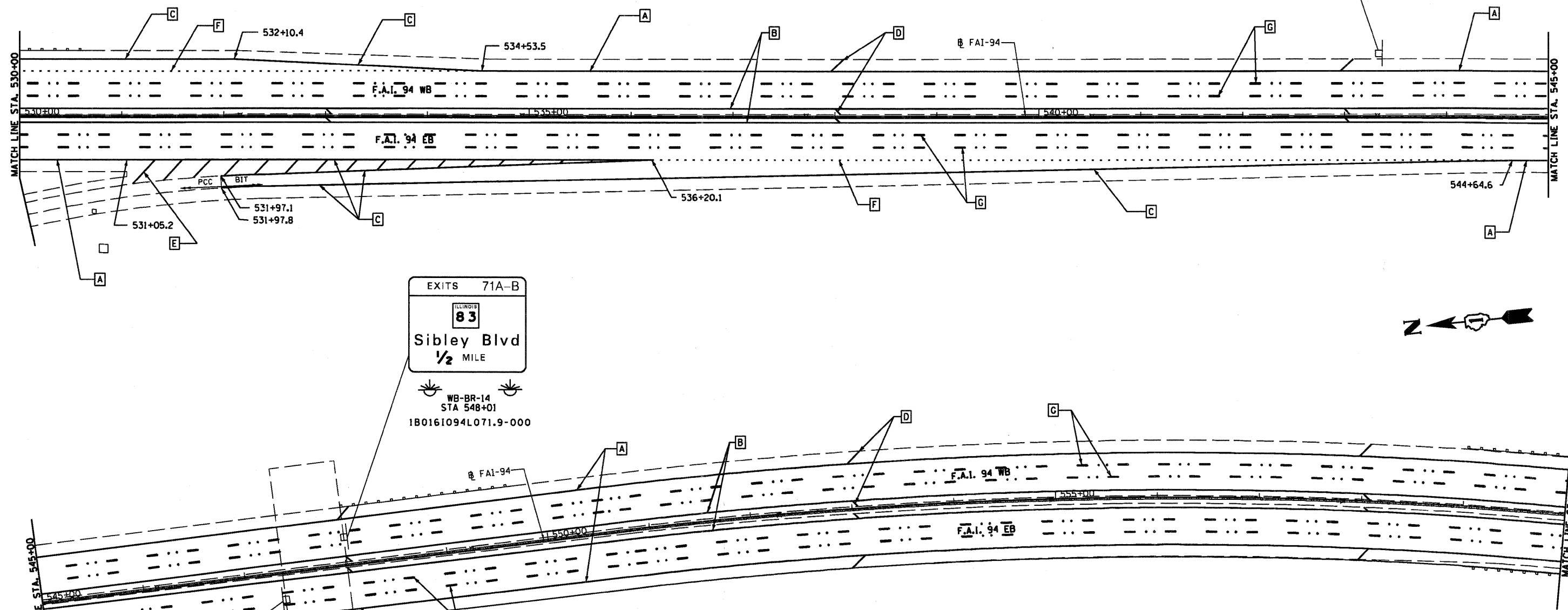
NOTE:  
SEE TC-12 "MULTI-LANE FREEWAY  
PAVEMENT MARKING DETAILS"  
FOR PLACEMENT & TYPE  
OF RAISED REFLECTIVE  
PAVEMENT MARKERS

FILE NAME = D160021-056-PMK-2.dgn	USER NAME = dvoznioraki	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) PAVEMENT MARKING AND SIGNING PLANS</b>	F.A.I. RTE. 94	SECTION 0405.ETC.42-780707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 56	
PLOT SCALE = 100.00' / IN.	CHECKED -	REVISED -	SCALE: 1" = 50'			SHEET NO. 2 OF 9 SHEETS	STA. 515+00 TO STA. 530+00	CONTRACT NO. 60D21		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT	
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -									





Calumet City  
EXIT 71B  
Dolton  
EXIT 71A  
WB-BS-11  
STA 543+34



EXITS 71A-B  
ILLINOIS  
83  
Sibley Blvd  
1/2 MILE

WB-BR-14  
STA 548+01  
1B0161094L071.9-000

EXITS 74A-B  
80 294 394  
2 1/4 MILES

EXITS 73A-B  
6  
159th St  
1 MILE

EB-BR-12  
STA 547+38

EB-BR-13  
STA 547+38

1B0161094R071.9-000

GUIDE SIGN SEQUENCE  
NUMBERING CODE

EXAMPLE  
EB-TR-03

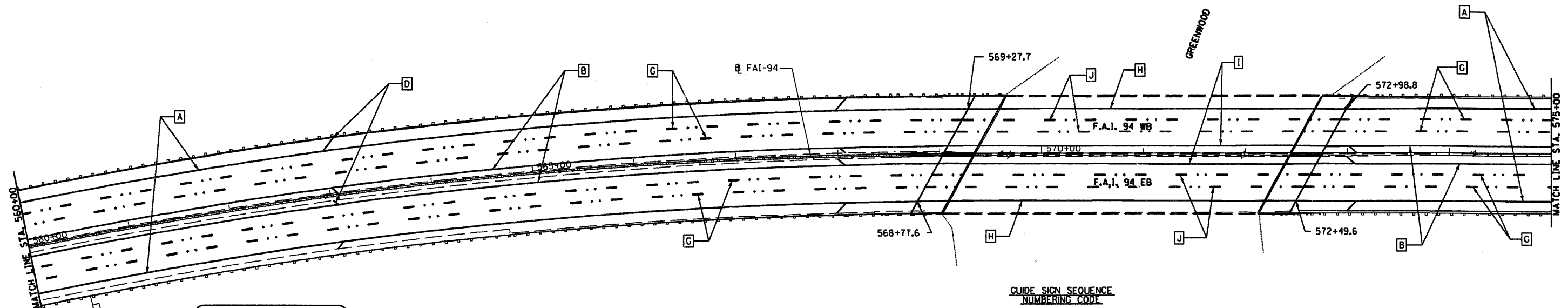
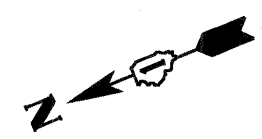
- DIRECTION OF TRAFFIC  
EB - EASTBOUND I-94  
WB - WESTBOUND I-94  
SI - STONY ISLAND FEEDER RAMP
- SIGN PANEL NUMBER
- MOUNTING TYPE  
TR - Truss  
CL - Cantilever  
BR - Bridge Mounted  
WP - Wood Post  
BS - Breakaway Steel  
LP - Light Pole
- REMOVE AND REPLACE SIGN PANEL
- EXISTING LUMINAIRE

PAVEMENT MARKING LEGEND

- A THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
- B THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE
- C THERMOPLASTIC PAVEMENT MARKING - LINE 8" WHITE EDGE LINE
- D THERMOPLASTIC PAVEMENT MARKING - LINE 12" PAVEMENT DIAGNOLS SPACED @ 500' CENTERS
- E THERMOPLASTIC PAVEMENT MARKING - LINE 12" GORE DIAGNOLS SPACED @ 30' CENTERS
- F PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 4" WHITE LANE LINES 2' DASH - 6' SKIP
- G PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 5" WHITE LANE LINES 10' DASH - 30' SKIP
- H POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" WHITE EDGE LINE
- I POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" YELLOW EDGE LINE
- J POLYUREA PAVEMENT MARKING, TYPE I, LINE 5" WHITE LANE LINES 10' DASH - 30' SKIP

NOTE:  
SEE TC-12 "MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT & TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS

FILE NAME = DI68021-057-PMK-3.dgn	USER NAME = dwoznaraki	DESIGNED - 2/5/2009	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) PAVEMENT MARKING AND SIGNING PLANS</b>		F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707RS-11)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 57	
PLOT SCALE = 100.00' / IN.	CHECKED - DMW	REVISED -	REVISED -		SCALE: 1" = 50'	SHEET NO. 3 OF 9 SHEETS	STA. 530+00 TO STA. 560+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 60021		
PLOT DATE = 2/5/2009	DATE -	REVISED -	REVISED -									

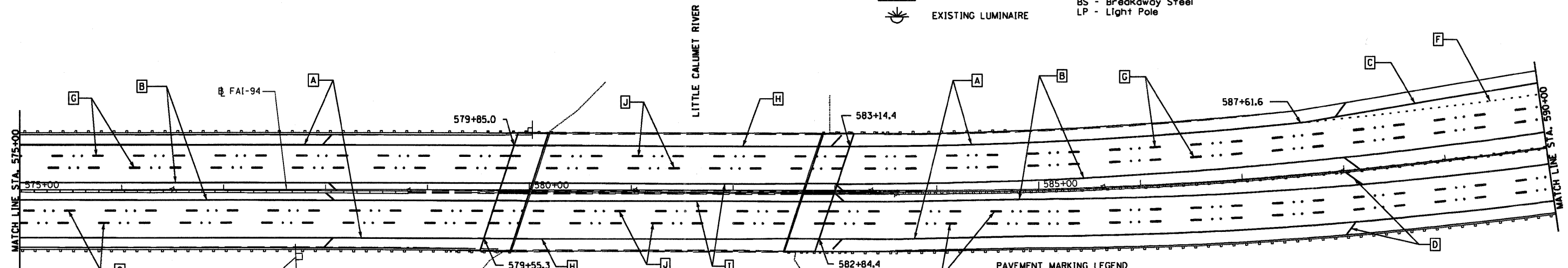
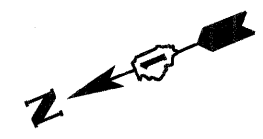


South Holland  
EXIT 73A  
Calumet City  
EXIT 73B  
EB-BS-15  
STA 560+52

**GUIDE SIGN SEQUENCE  
NUMBERING CODE**

EXAMPLE  
EB-TR-Q3

- DIRECTION OF TRAFFIC**  
 EB - EASTBOUND I-94  
 WB - WESTBOUND I-94  
 SI - STONY ISLAND FEEDER RAMPs
- SIGN PANEL NUMBER**
- MOUNTING TYPE**  
 TR - Truss  
 CL - Cantilever  
 BR - Bridge Mounted  
 WP - Wood Post  
 BS - Breakaway Steel  
 LP - Light Pole
- REMOVE AND REPLACE SIGN PANEL
- EXISTING LUMINAIRE



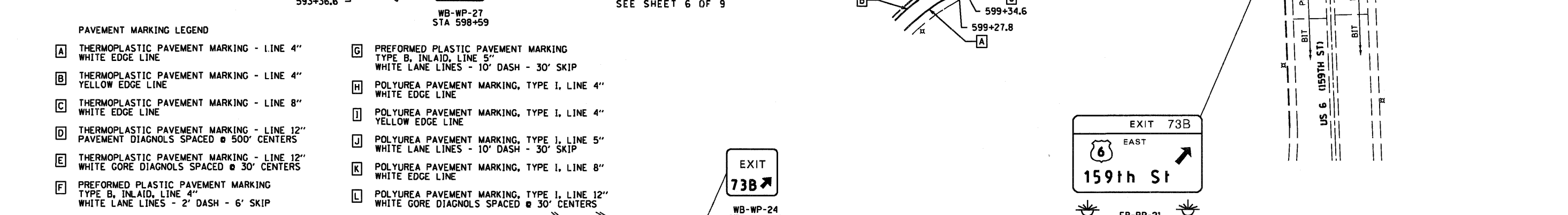
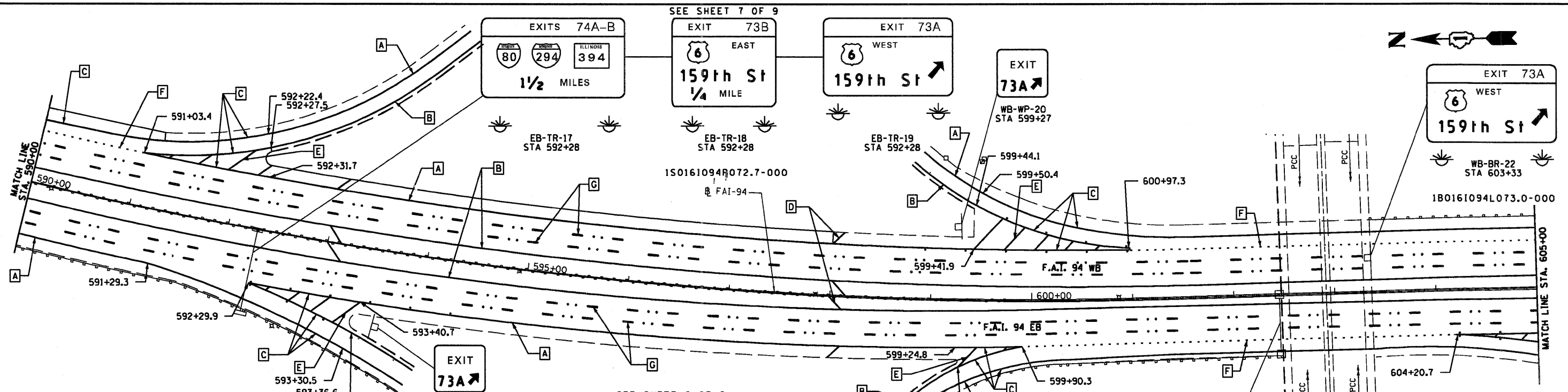
EXIT 73A  
WEST  
6  
159th St  
1/4 MILE  
EB-BS-16  
STA 577+73

NOTE:  
SEE TC-12 "MULTI-LANE FREEWAY  
PAVEMENT MARKING DETAILS"  
FOR PLACEMENT & TYPE  
OF RAISED REFLECTIVE  
PAVEMENT MARKERS

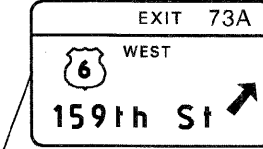
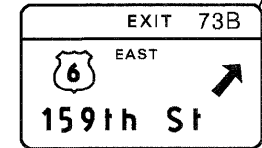
**PAVEMENT MARKING LEGEND**

- A THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
- B THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE
- C THERMOPLASTIC PAVEMENT MARKING - LINE 8" WHITE EDGE LINE
- D THERMOPLASTIC PAVEMENT MARKING - LINE 12" PAVEMENT DIAGNOLS SPACED @ 500' CENTERS
- E THERMOPLASTIC PAVEMENT MARKING - LINE 12" GORE DIAGNOLS SPACED @ 30' CENTERS
- F PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 4" WHITE LANE LINES 2' DASH - 6' SKIP
- G PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 5" WHITE LANE LINES 10' DASH - 30' SKIP
- H POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" WHITE EDGE LINE
- I POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" YELLOW EDGE LINE
- J POLYUREA PAVEMENT MARKING, TYPE I, LINE 5" WHITE LANE LINES 10' DASH - 30' SKIP

FILE NAME = D168021-B58-PMK-4.dgn	USER NAME = dwoznarski	DESIGNED - DRAWN - DMW	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) PAVEMENT MARKING AND SIGNING PLANS</b>	F.A.I. RTE. 94	SECTION (0405.ETC.42-780707RS-II)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 58	
PLOT SCALE = 1/8" = 1' IN.	CHECKED -	REVISED -	SCALE: 1" = 50'			SHEET NO. 4 OF 9 SHEETS	STA. 560+00 TO STA. 590+00	FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 60021	
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -									



- PAVEMENT MARKING LEGEND**
- A THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
  - B THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE
  - C THERMOPLASTIC PAVEMENT MARKING - LINE 8" WHITE EDGE LINE
  - D THERMOPLASTIC PAVEMENT MARKING - LINE 12" PAVEMENT DIAGNOLS SPACED @ 500' CENTERS
  - E THERMOPLASTIC PAVEMENT MARKING - LINE 12" WHITE GORE DIAGNOLS SPACED @ 30' CENTERS
  - F PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 4" WHITE LANE LINES - 2' DASH - 6' SKIP
  - G PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 5" WHITE LANE LINES - 10' DASH - 30' SKIP
  - H POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" WHITE EDGE LINE
  - I POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" YELLOW EDGE LINE
  - J POLYUREA PAVEMENT MARKING, TYPE I, LINE 5" WHITE LANE LINES - 10' DASH - 30' SKIP
  - K POLYUREA PAVEMENT MARKING, TYPE I, LINE 8" WHITE EDGE LINE
  - L POLYUREA PAVEMENT MARKING, TYPE I, LINE 12" WHITE GORE DIAGNOLS SPACED @ 30' CENTERS



**GUIDE SIGN SEQUENCE NUMBERING CODE**

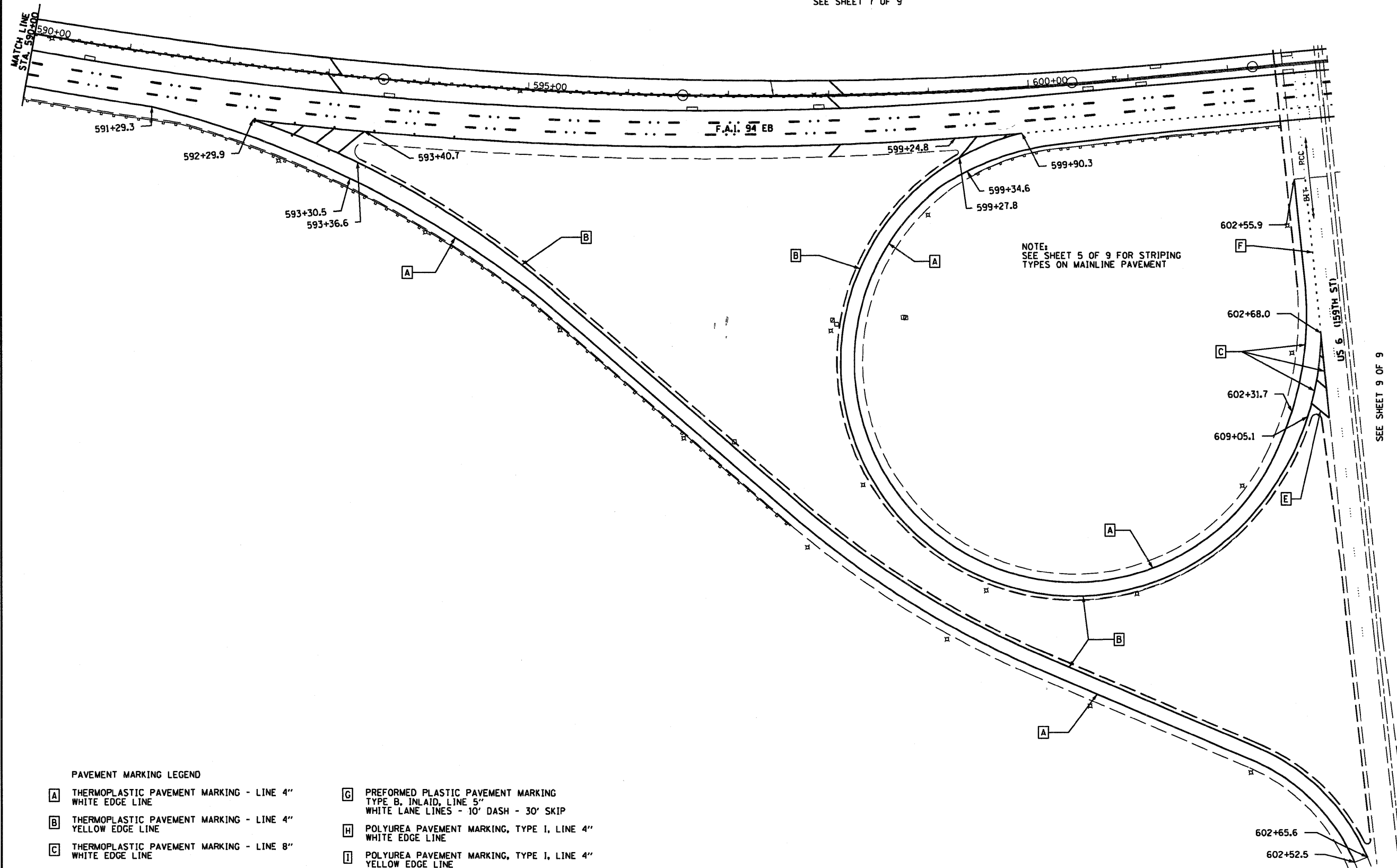
EXAMPLE: EB-TR-03

DIRECTION OF TRAFFIC	EXAMPLE	SIGN PANEL NUMBER	MOUNTING TYPE
EB - EASTBOUND I-94	EB-TR-03		TR - Truss
WB - WESTBOUND I-94			CL - Cantilever
SI - STONY ISLAND FEEDER RAMPS			BR - Bridge Mounted
			WP - Wood Post
			BS - Breakaway Steel
			LP - Light Pole

REMOVE AND REPLACE SIGN PANEL

EXISTING LUMINAIRE

NOTE:  
SEE TC-12 "MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT & TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS



SEE SHEET 9 OF 9

PAVEMENT MARKING LEGEND

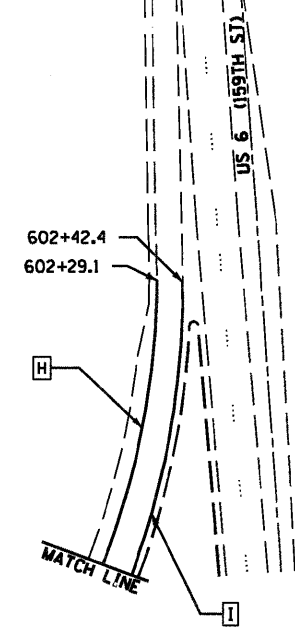
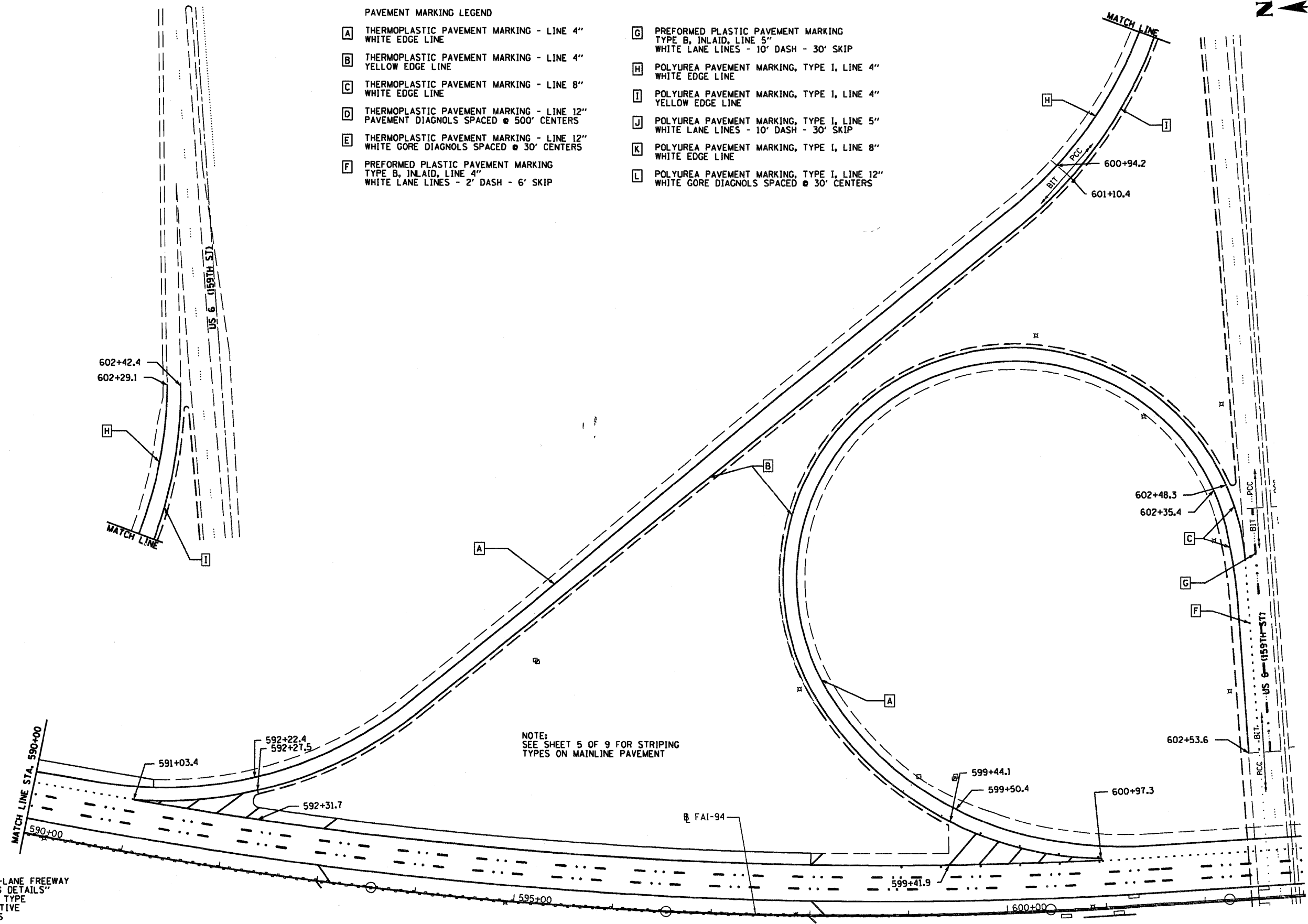
- [A] THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
- [B] THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE
- [C] THERMOPLASTIC PAVEMENT MARKING - LINE 8" WHITE EDGE LINE
- [D] THERMOPLASTIC PAVEMENT MARKING - LINE 12" PAVEMENT DIAGNOLS SPACED @ 500' CENTERS
- [E] THERMOPLASTIC PAVEMENT MARKING - LINE 12" WHITE GORE DIAGNOLS SPACED @ 30' CENTERS
- [F] PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 4" WHITE LANE LINES - 2' DASH - 6' SKIP
- [G] PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 5" WHITE LANE LINES - 10' DASH - 30' SKIP
- [H] POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" WHITE EDGE LINE
- [I] POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" YELLOW EDGE LINE
- [J] POLYUREA PAVEMENT MARKING, TYPE I, LINE 5" WHITE LANE LINES - 10' DASH - 30' SKIP
- [K] POLYUREA PAVEMENT MARKING, TYPE I, LINE 8" WHITE EDGE LINE
- [L] POLYUREA PAVEMENT MARKING, TYPE I, LINE 12" WHITE GORE DIAGNOLS SPACED @ 30' CENTERS

NOTE:  
SEE TC-12 "MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS" FOR PLACEMENT & TYPE OF RAISED REFLECTIVE PAVEMENT MARKERS

FILE NAME = D:\60021-060-PMK-6.dgn	USER NAME = dwoznarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) PAVEMENT MARKING AND SIGNING PLANS</b>	F.A.I. RTE. 94	SECTION (0405.ETC.42-7&0707RS-11)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 60
PLOT SCALE = 100.00' / IN.	PLOT DATE = 2/5/2009	DRAWN -	REVISED -	SCALE: 1" = 50' SHEET NO. 6 OF 9 SHEETS STA. 590+0 TO STA. 605+00		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT		CONTRACT NO. 60021		



- PAVEMENT MARKING LEGEND**
- [A] THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
  - [B] THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE
  - [C] THERMOPLASTIC PAVEMENT MARKING - LINE 8" WHITE EDGE LINE
  - [D] THERMOPLASTIC PAVEMENT MARKING - LINE 12" PAVEMENT DIAGNOLS SPACED @ 500' CENTERS
  - [E] THERMOPLASTIC PAVEMENT MARKING - LINE 12" WHITE GORE DIAGNOLS SPACED @ 30' CENTERS
  - [F] PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 4" WHITE LANE LINES - 2' DASH - 6' SKIP
  - [G] PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 5" WHITE LANE LINES - 10' DASH - 30' SKIP
  - [H] POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" WHITE EDGE LINE
  - [I] POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" YELLOW EDGE LINE
  - [J] POLYUREA PAVEMENT MARKING, TYPE I, LINE 5" WHITE LANE LINES - 10' DASH - 30' SKIP
  - [K] POLYUREA PAVEMENT MARKING, TYPE I, LINE 8" WHITE EDGE LINE
  - [L] POLYUREA PAVEMENT MARKING, TYPE I, LINE 12" WHITE GORE DIAGNOLS SPACED @ 30' CENTERS



NOTE:  
SEE SHEET 5 OF 9 FOR STRIPING  
TYPES ON MAINLINE PAVEMENT

NOTE:  
SEE TC-12 "MULTI-LANE FREEWAY  
PAVEMENT MARKING DETAILS"  
FOR PLACEMENT & TYPE  
OF RAISED REFLECTIVE  
PAVEMENT MARKERS

SEE SHEET 8 OF 9

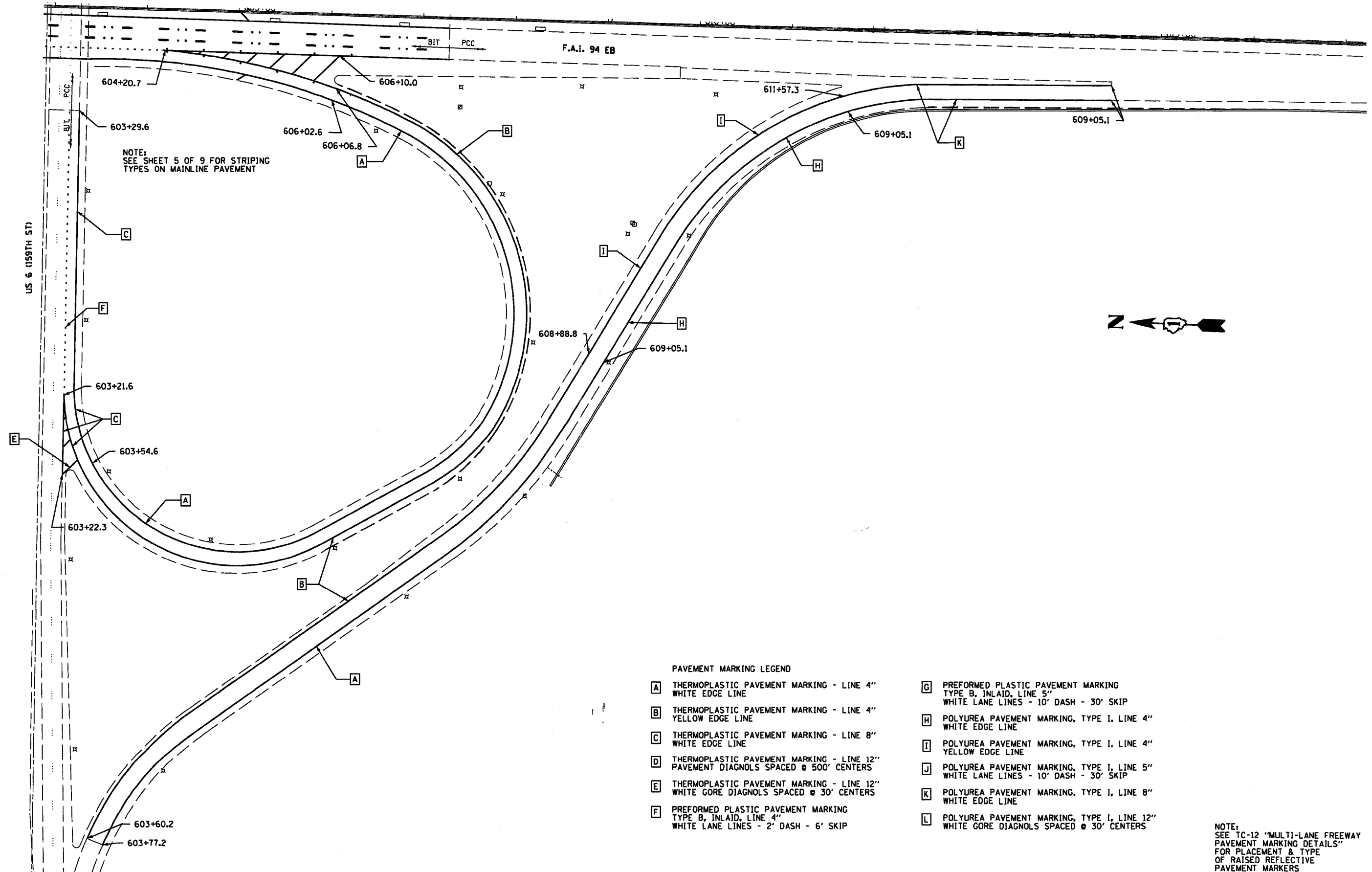
SEE SHEET 6 OF 9

FILE NAME = D:\60021-061-PMK-7.dgn	USER NAME = dvoznieraki	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) PAVEMENT MARKING AND SIGNING PLANS</b>		F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 61
	PLOT SCALE = 100.00' / IN.	DRAWN - DMW	REVISED -		SCALE: 1" = 50'	SHEET NO. 7 OF 9 SHEETS	STA. 590+00 TO STA. 605+00	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60021	
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -								
		DATE - 2/5/2009	REVISED -								



SEE SHEET 6 OF 9

US 6 (159TH ST)



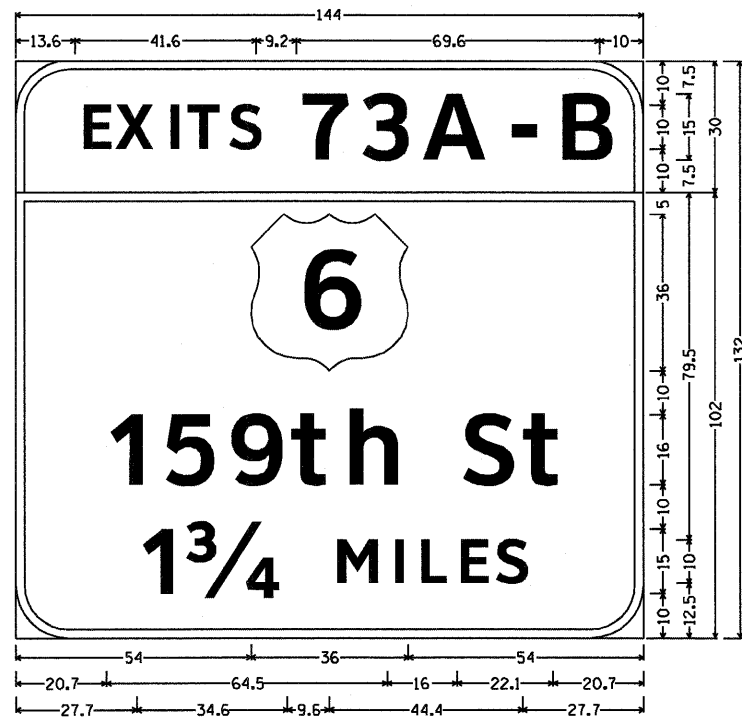
NOTE:  
SEE SHEET 5 OF 9 FOR STRIPING  
TYPES ON MAINLINE PAVEMENT

PAVEMENT MARKING LEGEND

- A THERMOPLASTIC PAVEMENT MARKING - LINE 4" WHITE EDGE LINE
- B THERMOPLASTIC PAVEMENT MARKING - LINE 4" YELLOW EDGE LINE
- C THERMOPLASTIC PAVEMENT MARKING - LINE 8" WHITE EDGE LINE
- D THERMOPLASTIC PAVEMENT MARKING - LINE 12" PAVEMENT DIAGNOLS SPACED @ 500' CENTERS
- E THERMOPLASTIC PAVEMENT MARKING - LINE 12" WHITE GORE DIAGNOLS SPACED @ 30' CENTERS
- F PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 4" WHITE LANE LINES - 2' DASH - 6' SKIP
- G PREFORMED PLASTIC PAVEMENT MARKING TYPE B, INLAID, LINE 5" WHITE LANE LINES - 10' DASH - 30' SKIP
- H POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" WHITE EDGE LINE
- I POLYUREA PAVEMENT MARKING, TYPE I, LINE 4" YELLOW EDGE LINE
- J POLYUREA PAVEMENT MARKING, TYPE I, LINE 5" WHITE LANE LINES - 10' DASH - 30' SKIP
- K POLYUREA PAVEMENT MARKING, TYPE I, LINE 8" WHITE EDGE LINE
- L POLYUREA PAVEMENT MARKING, TYPE I, LINE 12" WHITE GORE DIAGNOLS SPACED @ 30' CENTERS

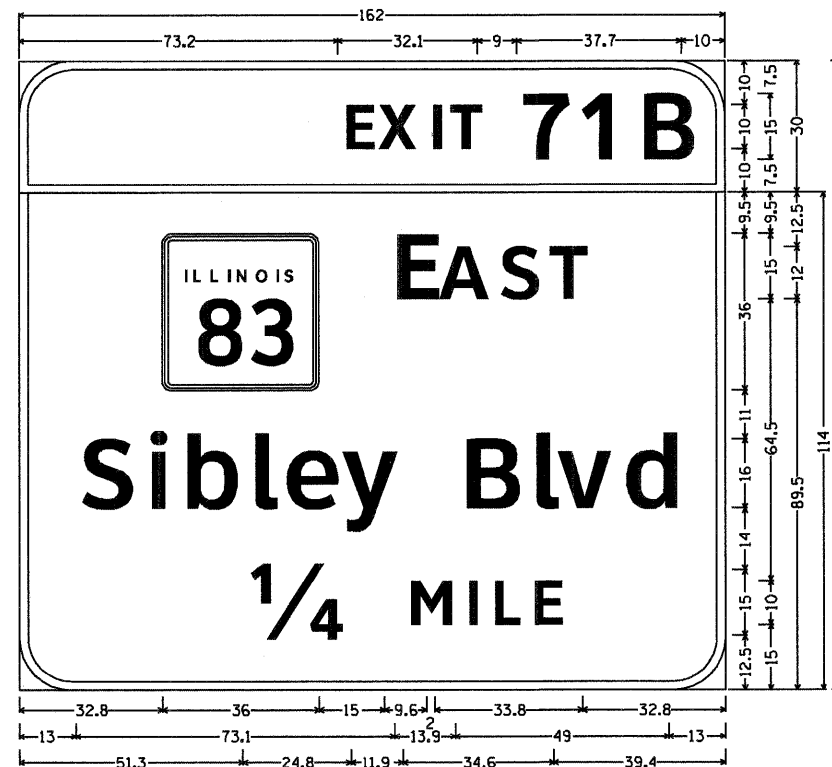
NOTE:  
SEE TC-12 "MULTI-LANE FREEWAY  
PAVEMENT MARKING DETAILS"  
FOR PLACEMENT & TYPE  
OF RAISED REFLECTIVE  
PAVEMENT MARKERS

FILE NAME = D168021-863-PMK-9.dgn	USER NAME = dvoznarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) PAVEMENT MARKING AND SIGNING</b>		F.A.I. RTE. 94	SECTION (0405,ETC,42-780707RS-11)	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 63
	PLOT SCALE = 100.00' / IN.	DRAWN -	DMN REVISED -		SCALE: 1" = 50'	SHEET NO. 9 OF 9 SHEETS	STA. 605+00 TO STA. 607+28	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 60021	
	PLOT DATE = 2/5/2009	CHECKED -	REVISED -								
		DATE -	2/5/2009 REVISED -								



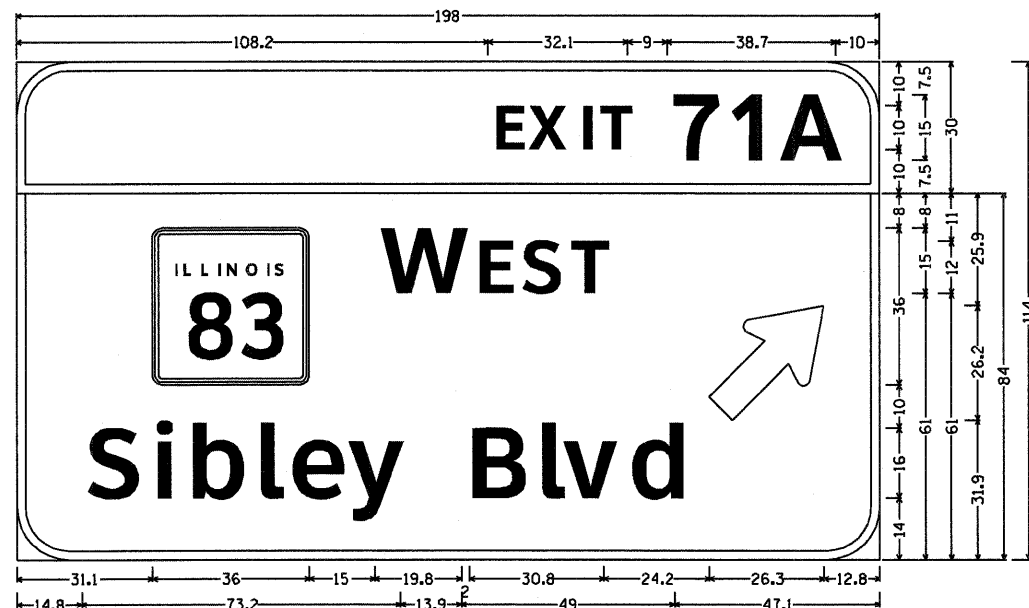
9.0" Radius, 2.0" Border, White on Green;  
 [EXITS 73A-B] ClearviewHwy-5-W;  
 9.0" Radius, 2.0" Border, White on Green;  
 [159th St] ClearviewHwy-5-W-R; [1 3/4 MILES] ClearviewHwy-5-W;

EB-TR-01



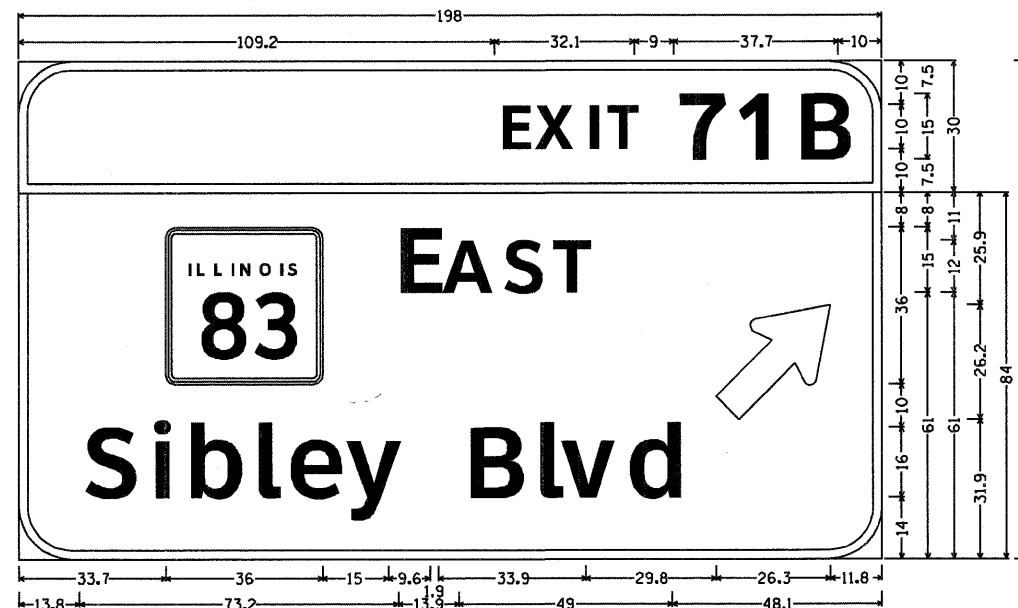
IL 83 Should Have no Border;  
 9.0" Radius, 2.0" Border, White on Green;  
 [EXIT 71B] ClearviewHwy-5-W;  
 9.0" Radius, 2.0" Border, White on Green;  
 [EAST] ClearviewHwy-5-W; [Sibley Blvd] ClearviewHwy-5-W-R 90° spacing;  
 [1/4 MILE] ClearviewHwy-5-W;

EB-TR-02



IL 83 Should Have no Border;  
 9.0" Radius, 2.0" Border, White on Green;  
 [EXIT 71A] ClearviewHwy-5-W;  
 9.0" Radius, 2.0" Border, White on Green;  
 [WEST] ClearviewHwy-5-W; [Sibley Blvd] ClearviewHwy-5-W-R 90° spacing; Standard Arrow Custom 33.4" X 20.3" 45L;

EB-TR-03  
 AND  
 WB-BR-06



IL 83 Should Have no Border;  
 9.0" Radius, 2.0" Border, White on Green;  
 [EXIT 71B] ClearviewHwy-5-W;  
 9.0" Radius, 2.0" Border, White on Green;  
 [EAST] ClearviewHwy-5-W; [Sibley Blvd] ClearviewHwy-5-W-R 90° spacing; Standard Arrow Custom 33.4" X 20.3" 45L;

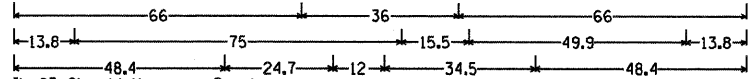
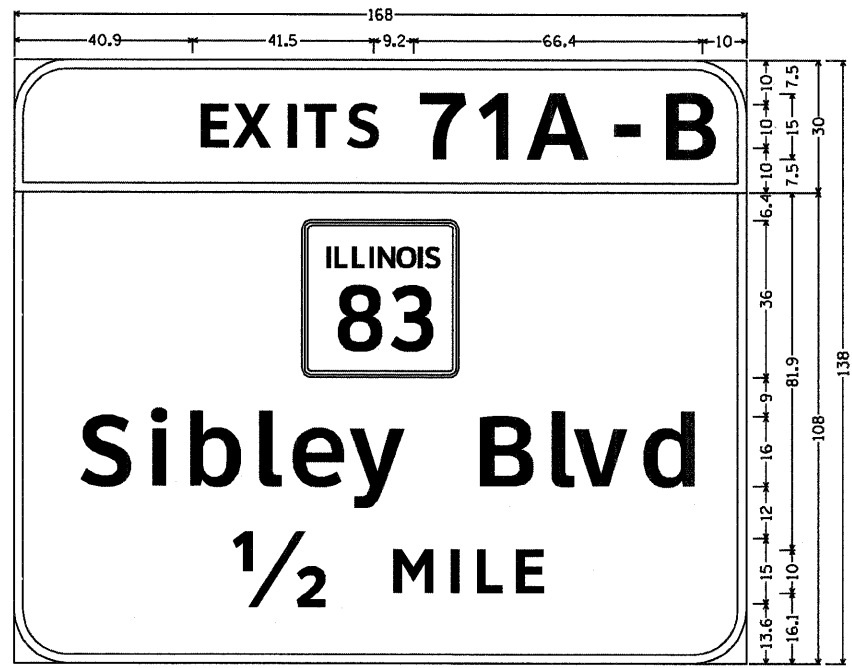
EB-BR-05  
 AND  
 WB-TR-10

ALL DIMENSIONS ARE INCHES

FILE NAME = DI60021-064-SIGN.1.dgn	USER NAME = dwozniarski	DESIGNED - DRAWN - DMW, GDS	REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SIGN DETAILS</b>			F.A.J. RTE. 94	SECTION I0405.ETC.42-7&0707RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 64
PLOT SCALE = N/A	CHECKED -	REVISED -	REVISED -		SCALE: N/A	SHEET NO. 1 OF 6 SHEETS	STA.	TO STA.	CONTRACT NO. 60021			
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -	REVISED -		FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT							

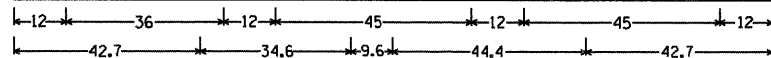
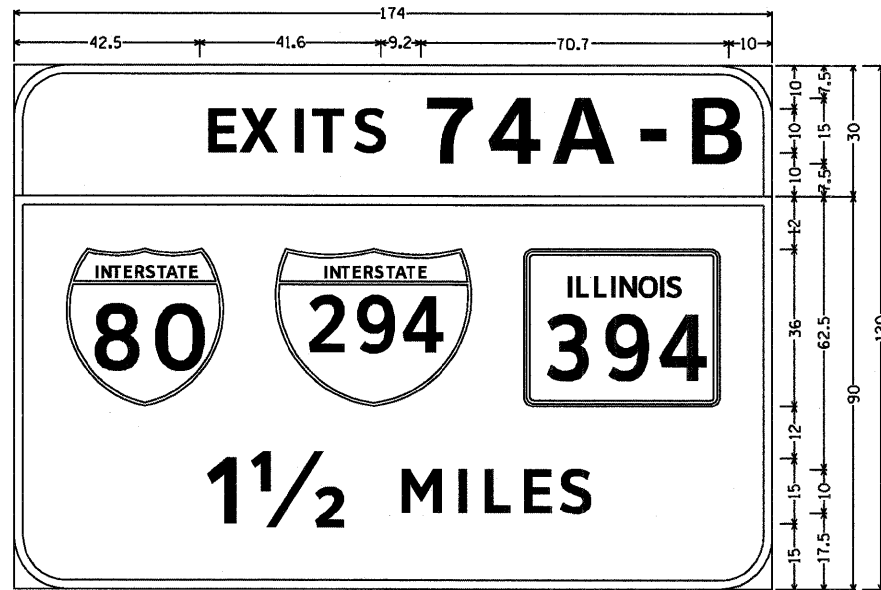






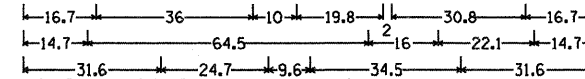
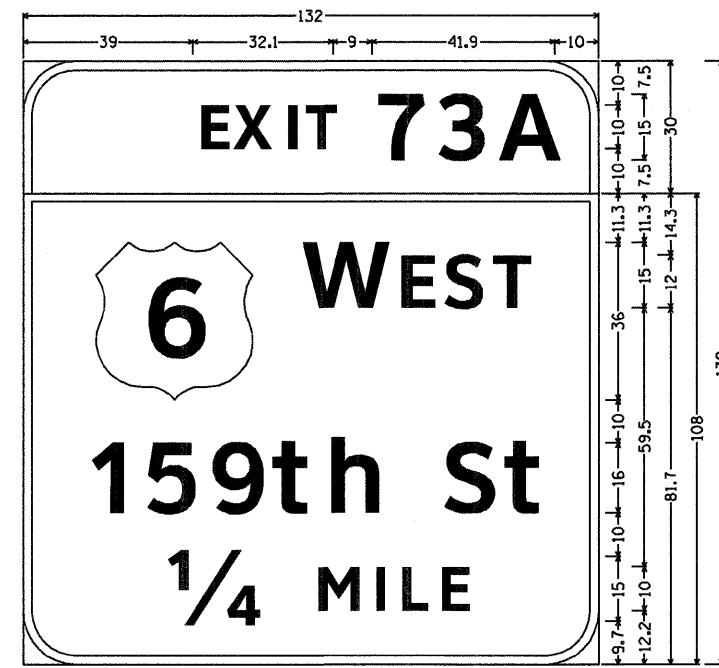
IL 83 Should Have no Border;  
 9.0" Radius, 2.0" Border, White on Green;  
 [EXITS 71A-B] ClearviewHwy-5-W;  
 9.0" Radius, 2.0" Border, White on Green;  
 [Sibley Blvd] ClearviewHwy-5-W-R; [1/2 MILE] ClearviewHwy-5-W;

WB-BR-14



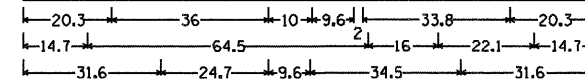
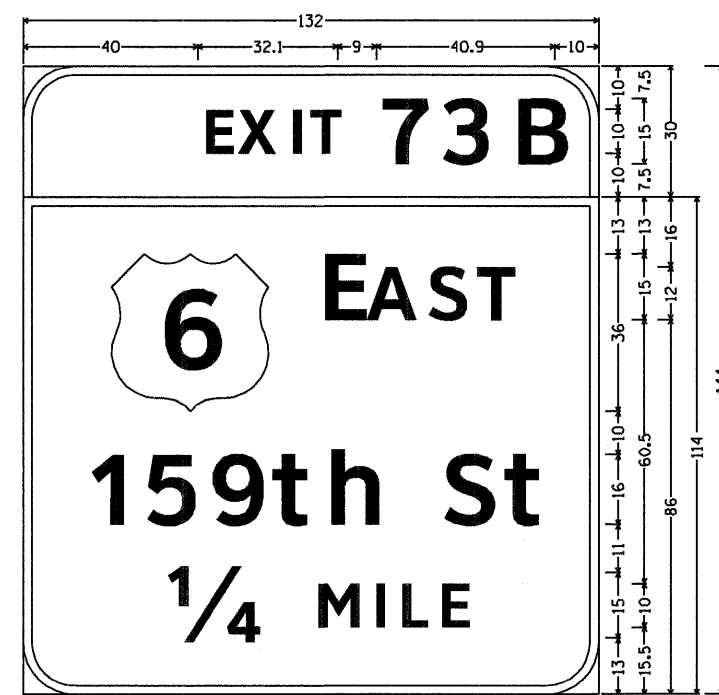
IL 394 Sign Does not have border.;  
 [EXITS 74A-B] ClearviewHwy-5-W;  
 9.0" Radius, 2.0" Border, White on Green;  
 Interstate 294 14.0" D; [1 1/2 MILES] ClearviewHwy-5-W;

EB-TR-17



9.0" Radius, 2.0" Border, White on Green;  
 [EXIT 73A] ClearviewHwy-5-W;  
 9.0" Radius, 2.0" Border, White on Green;  
 [WEST] ClearviewHwy-5-W; [159th St] ClearviewHwy-5-W-R;  
 [1/4 MILE] ClearviewHwy-5-W;

EB-BS-16



9.0" Radius, 2.0" Border, White on Green;  
 [EXIT 73B] ClearviewHwy-5-W;  
 9.0" Radius, 2.0" Border, White on Green;  
 [EAST] ClearviewHwy-5-W; [159th St] ClearviewHwy-5-W-R;  
 [1/4 MILE] ClearviewHwy-5-W;

EB-TR-18

ALL DIMENSIONS ARE INCHES

FILE NAME = D168021-866-SIGN_3.dgn	USER NAME = dwoznarski	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>BISHOP FORD EXPRESSWAY (I-94) SIGN DETAILS</b>		F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = N/A	CHECKED -	REVISED -				94	(0405,ETC,42-7&07)RS-11	COOK	133	66
PLOT DATE = 2/5/2009	DATE - 2/5/2009	REVISED -	REVISED -		SCALE: N/A	SHEET NO. 3 OF 6 SHEETS	STA.	TO STA.	CONTRACT NO. 60021		
							FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				







STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

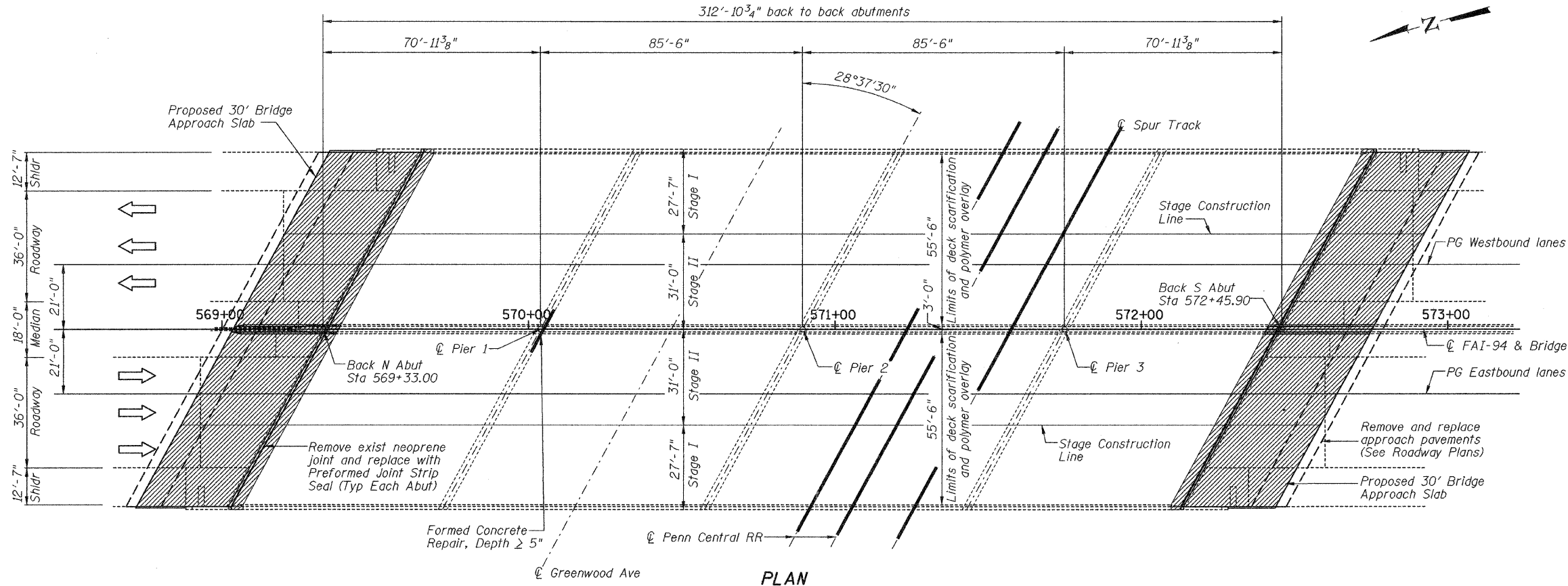
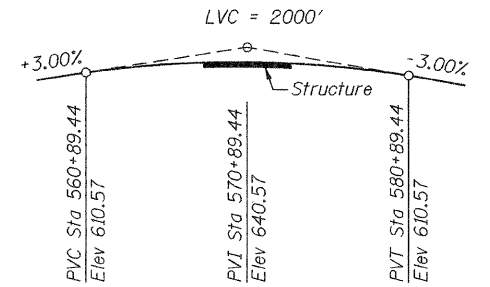
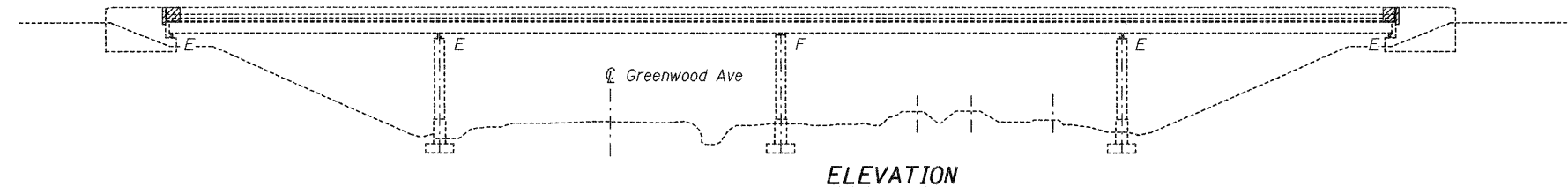
Existing structure:  
The original structure was built in 1947 as Route SA-66, Section 066-505.2-MFT, 1975-079-BR. In 1981, the bridge was widened and reconstructed with a new deck and new line of steel beams on each side and the superstructure was cleaned and painted.

Bench Mark:  
"X" scribed in chiseled box on top of concrete barrier wall at the southeast corner of bridge structure for Westbound FAI-94 over Greenwood Avenue  
Elev 627.12 (Assumed Local Datum)

Stage construction shall be utilized to maintain traffic during construction.  
No salvage

**DESIGN SPECIFICATIONS**  
2002 AASHTO 17th Edition

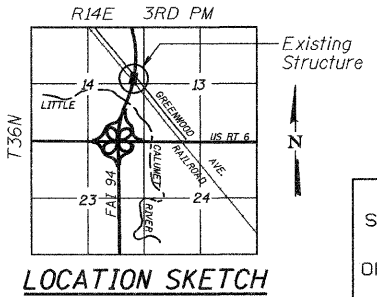
**DESIGN STRESSES**  
FIELD UNITS  
f'c = 3,500 psi  
fy = 60,000 psi (Reinforcement)  
fy = 36,000 psi (M270 Grade 36)



**EXISTING PROFILE GRADE FAI-94**

**SCOPE OF WORK**  
Shot blast the existing deck and apply a thin anti-icing overlay <sup>3</sup>/<sub>8</sub>" min thickness.  
Replace deck joints at each abutment with Preformed Joint Strip Seal.  
Replace bridge approach pavements.  
Jack and remove bearings and replace with elastomeric bearings at each abutment.  
Remove debris from abutment bearing seats.  
Repair north pier cap with formed concrete repair.  
Repair the west parapet in the south approach, westbound lanes, with formed concrete repair.  
Re-seal joint between median parapets

**GENERAL PLAN AND ELEVATION  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162**



DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

**HOMER L. CHASTAIN & ASSOCIATES, LLP**  
CONSULTING ENGINEERS  
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CHICAGO (773) 714-0050  
ROCKFORD (815) 489-0050  
184-001397

SHEET NO. 1	F.A.I. RTE. 94	SECTION (0405,ETC,42-7&070)RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 70
OF 17 SHEETS	BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**GENERAL NOTES**

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Stage construction shall be utilized to maintain traffic during construction.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with "Concrete Removal".

The Contractor shall exercise care during removal of existing joints to ensure that the slab, beams and diaphragms' integrity will not be detrimentally impacted. The Contractor shall repair any damage(s) to the slab, beams and diaphragms caused by his operation as directed by the Engineer at no additional cost to the Department.

Protective coat shall be applied only to the new concrete provided for the reconstruction of the joints (top of deck slab, top and traffic face of parapet).

The Engineer shall determine extent, location and type of substructure and deck slab repairs in the field.

Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose potentially detrimental foreign material shall be removed from the surfaces in contact with new concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that can be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

The Contractor shall make allowance for the deflection of forms, shrinkage and settlement of falsework, in addition to allowance for dead load deflection. Forms for deck slab shall be removed prior to placement of bridge approach pavement.

If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06 of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.

The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior and bottom flange of the fascia beams shall be Interstate Green, Munsell No. 7.5G 4/8. See Special Provision for "Cleaning and Painting New Metal Structures".

Exterior structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures".

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB.	TOTAL
CONCRETE REMOVAL	CU. YD.	40.0		40.0
CONCRETE STRUCTURES	CU. YD.		80.0	80.0
CONCRETE SUPERSTRUCTURE	CU. YD.	406.8		406.8
PROTECTIVE COAT	SQ. YD.	120.0		120.0
REINFORCEMENT BARS, EPOXY COATED	POUND	102580		102580
BAR SPLICERS	EACH	508		508
PREFORMED JOINT STRIP SEAL	FOOT	262.0		262.0
ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	36		36
CONCRETE SEALER	SQ. FT.	581.0		581.0
CLEANING BRIDGE SEATS	SQ. FT.	581.0		581.0
DECK SLAB REPAIR (PARTIAL)	SQ. YD.	193.0		193.0
SILICONE JOINT SEALER, 2"	FOOT	313		313
STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5")	SQ. FT.	4.0	10.0	14.0
ANTI-ICING SURFACE OVERLAY	SQ. YD.	3860.0		3860.0
JACK AND REMOVE EXISTING BEARINGS	EACH	36		36
ANCHOR BOLTS, 1"	EACH	72		72
FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	5210		5210

**INDEX OF SHEETS**

- 1 GENERAL PLAN AND ELEVATION
- 2 GENERAL NOTES, BILL OF MATERIAL AND INDEX OF SHEETS
- 3 TYPICAL SECTION THRU BRIDGE
- 4 CONSTRUCTION STAGING DETAILS
- 5 EXPANSION JOINT REMOVAL AND REPLACEMENT PLAN AT N. ABUTMENT
- 6 EXPANSION JOINT REMOVAL AND REPLACEMENT PLAN AT S. ABUTMENT
- 7 EXPANSION JOINT REMOVAL AND REPLACEMENT DETAILS 1
- 8 EXPANSION JOINT REMOVAL AND REPLACEMENT DETAILS 2
- 9 PREFORMED JOINT STRIP SEAL
- 10 BAR SPLICER ASSEMBLY DETAILS
- 11 TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
- 12 ELASTOMERIC BEARING ASSEMBLY - TYPE II
- 13 PIER 1 REPAIR DETAILS
- 14 TOP OF NORTH APPROACH SLAB ELEVATIONS
- 15 TOP OF SOUTH APPROACH SLAB ELEVATIONS
- 16 BRIDGE APPROACH SLAB DETAILS (SHEET 1 OF 2)
- 17 BRIDGE APPROACH SLAB DETAILS (SHEET 2 OF 2)

**STANDARD DRAWINGS**

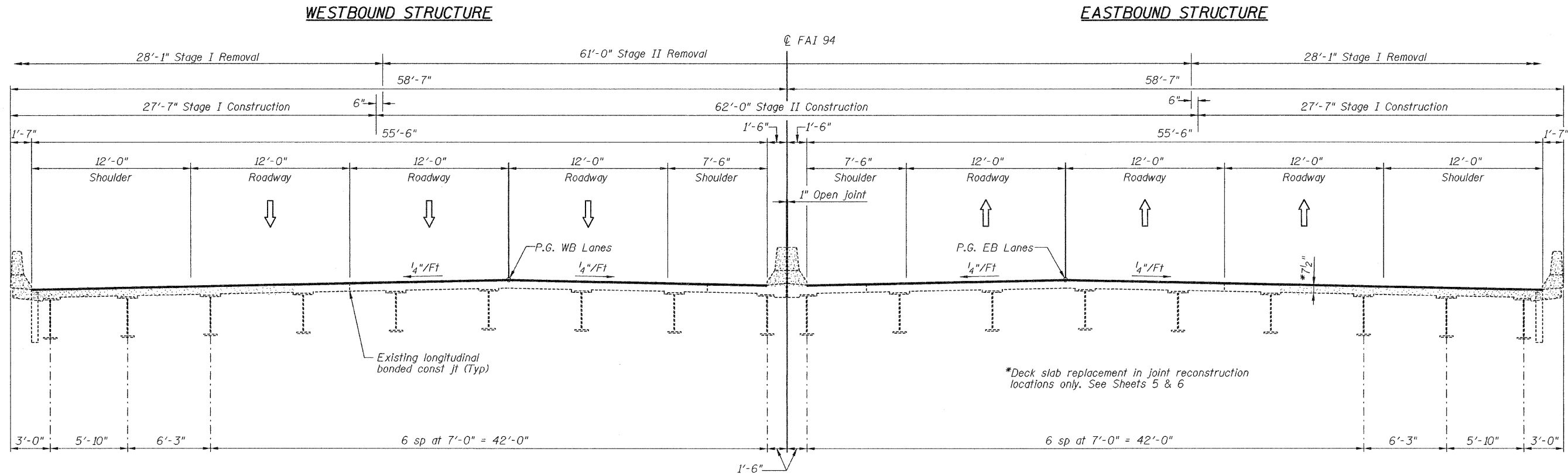
**GENERAL NOTES, BILL OF MATERIAL  
AND INDEX OF SHEETS  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162**

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

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CHASTAIN  
& ASSOCIATES, LLP  
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184-001397

SHEET NO. 2 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	71
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. -		ILLINOIS	FED. AID PROJECT		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**TYPICAL CROSS SECTION THRU BRIDGE**  
(Looking South)

**NOTES**

For joint removal limits see Sheets 5 thru 8

**TYPICAL SECTION THRU BRIDGE**  
**BISHOP FORD EXPRESSWAY (I-94)**  
**GREENWOOD AVENUE**  
**COOK COUNTY**  
**STATION**  
**STRUCTURE NUMBER 016-0162**

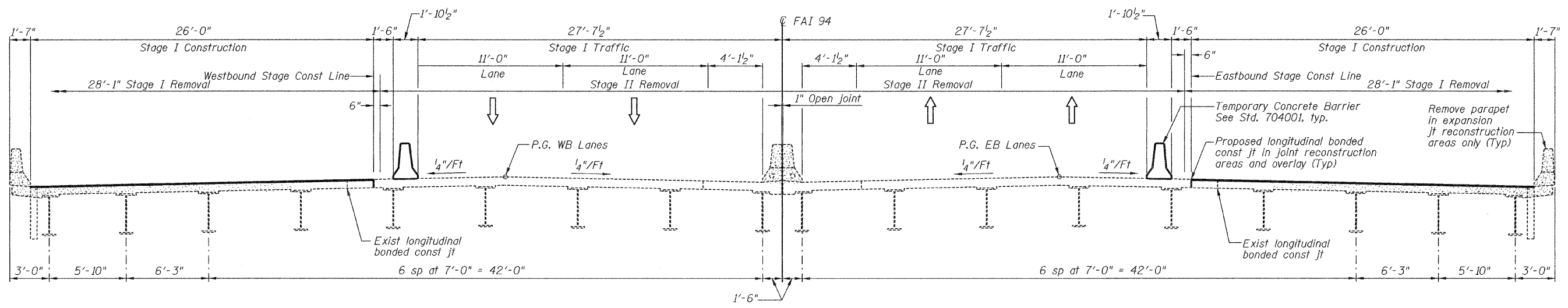
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DRAWN -	R KING
CHECKED -	

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ROCKFORD (815) 489-0050  
184-001397

SHEET NO. 3 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	72
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					



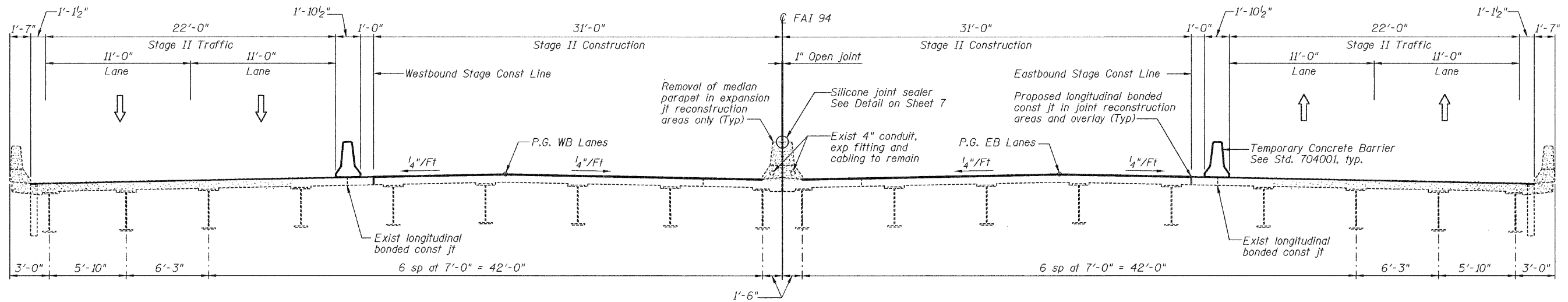
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



WESTBOUND STRUCTURE

STAGE I  
(Looking South)

EASTBOUND STRUCTURE



STAGE II  
(Looking South)

NOTES

For joint removal limits see Sheets 5 thru 8

The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable.

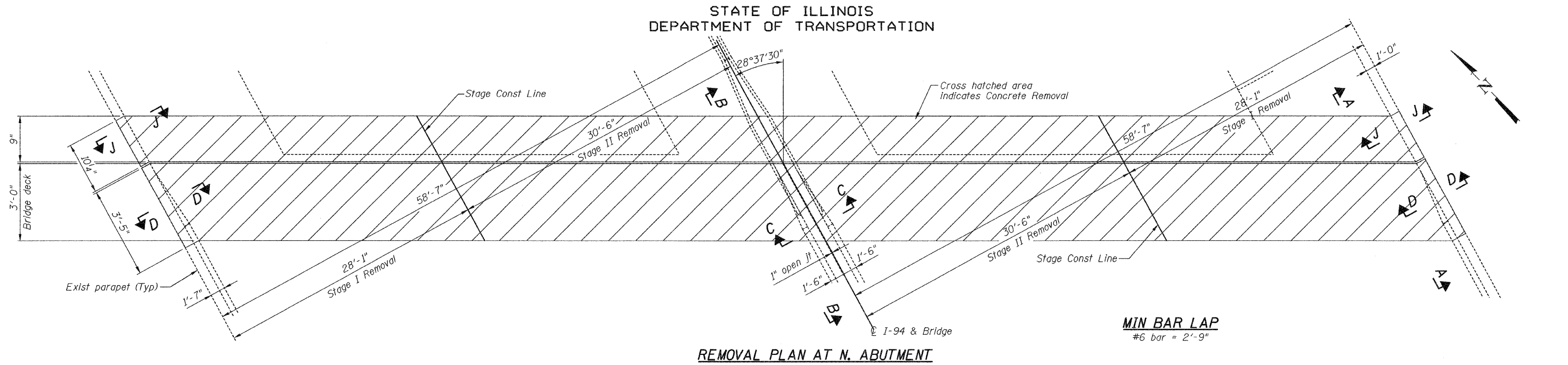
CONSTRUCTION STAGING DETAILS  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

**HOMER L. CHASTAIN & ASSOCIATES, LLP**  
CONSULTING ENGINEERS  
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CHICAGO (773) 714-0050  
ROCKFORD (815) 489-0050  
184-001397

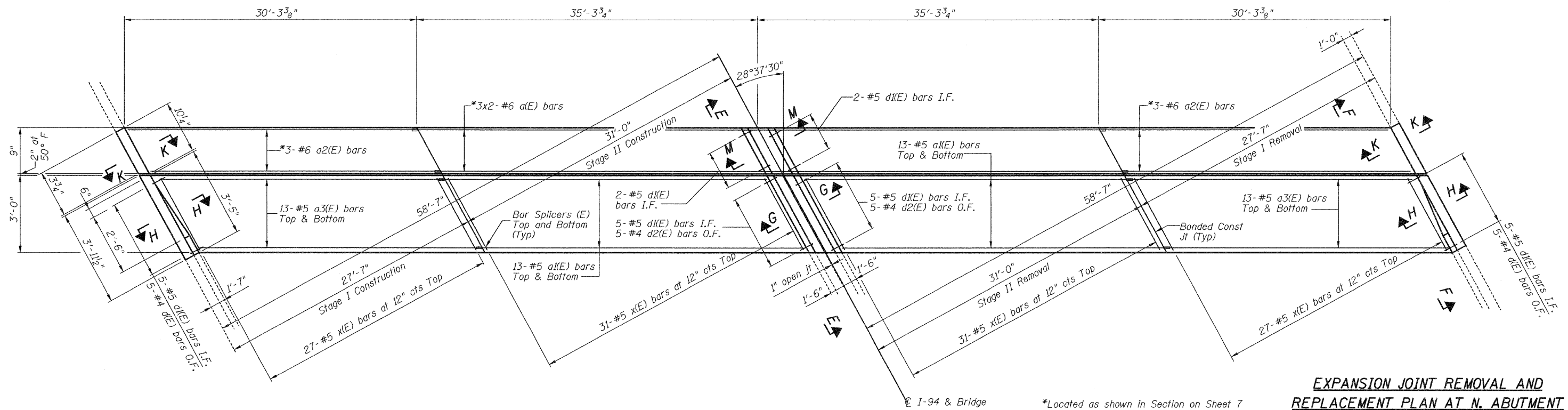
SHEET NO. 4 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	73
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



REMOVAL PLAN AT N. ABUTMENT

MIN BAR LAP  
#6 bar = 2'-9"



PROPOSED PLAN AT N. ABUTMENT

EXPANSION JOINT REMOVAL AND  
REPLACEMENT PLAN AT N. ABUTMENT  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

NOTES

- All reinforcement bars are to be evenly spaced unless otherwise noted.
- Reinforcement bars designated (E) shall be epoxy coated.
- O.F. denotes Outside Face, I.F. denotes Inside Face.
- For Bill of Material and bar details see Sheet 8
- For additional abutment backwall demolition and reconstruction see Sheet 7

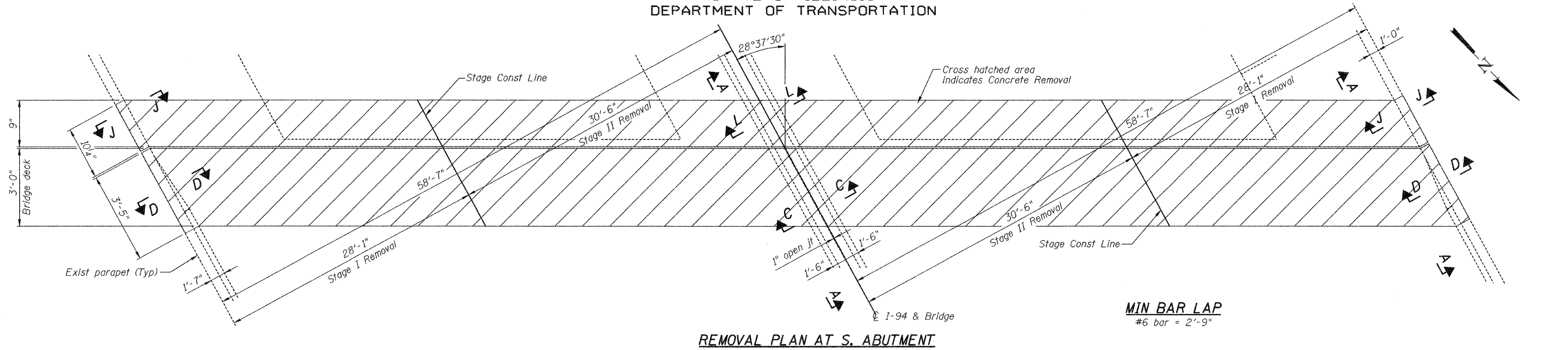
DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

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CHASTAIN  
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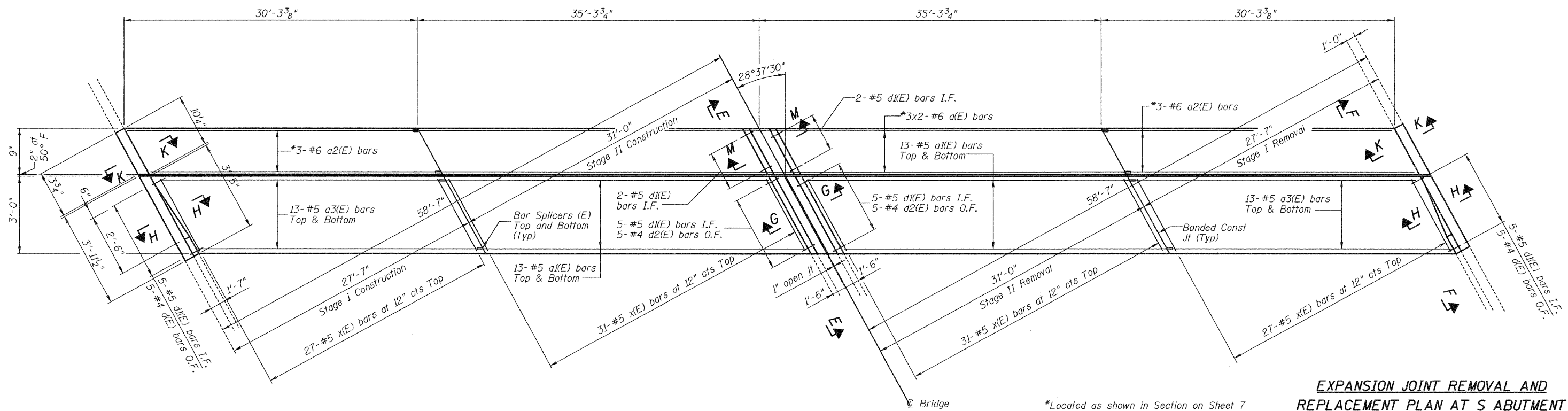
\*Located as shown in Section on Sheet 7

SHEET NO. 5	F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707)RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 74
OF 17 SHEETS	BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



REMOVAL PLAN AT S. ABUTMENT



PROPOSED PLAN AT S. ABUTMENT

EXPANSION JOINT REMOVAL AND  
REPLACEMENT PLAN AT S ABUTMENT  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

NOTES

- All reinforcement bars are to be evenly spaced unless otherwise noted.
- Reinforcement bars designated (E) shall be epoxy coated.
- O.F. denotes Outside Face, I.F. denotes Inside Face.
- For Bill of Material and bar details see Sheet 8
- For additional abutment backwall demolition and reconstruction see Sheet 7

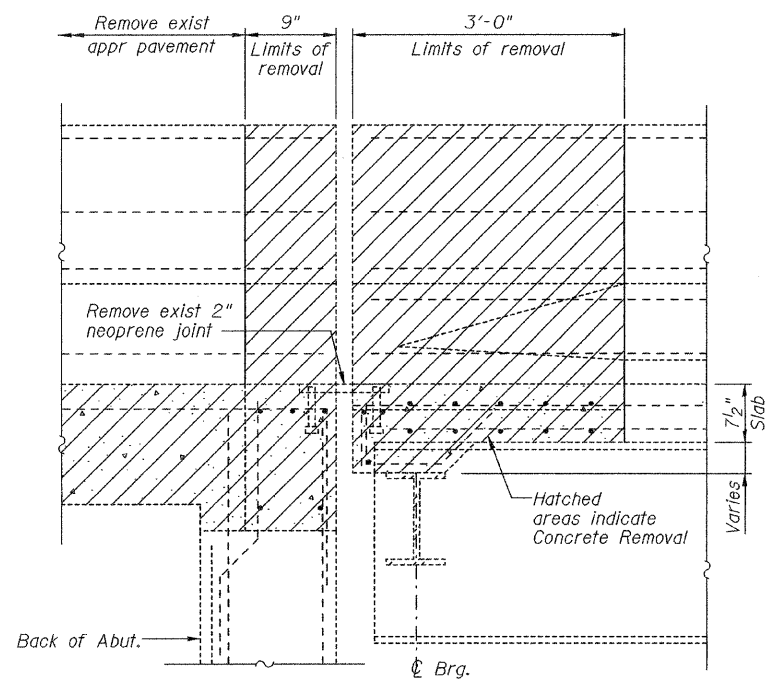
DESIGNED -	
CHECKED -	
DRAWN -	R KING
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184-001397

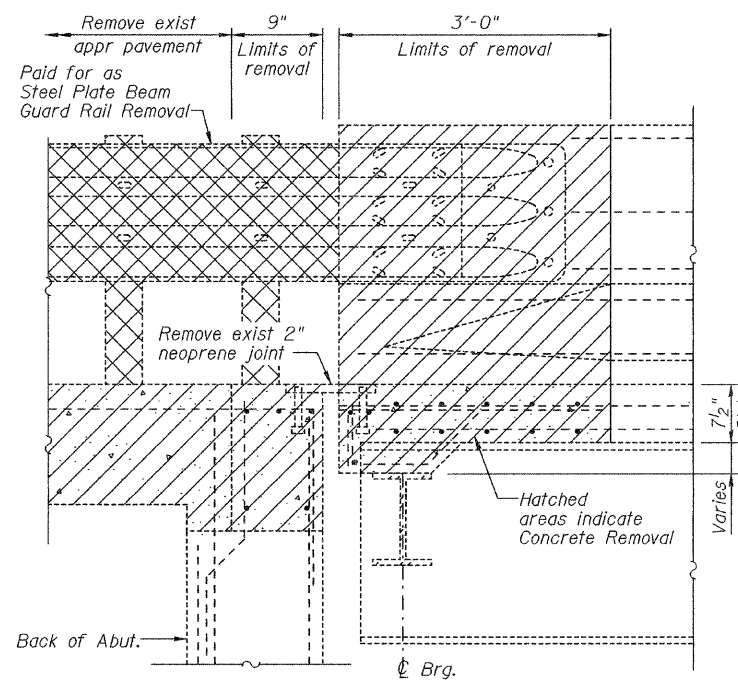
SHEET NO. 6	F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707)RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 75
OF 17 SHEETS	BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

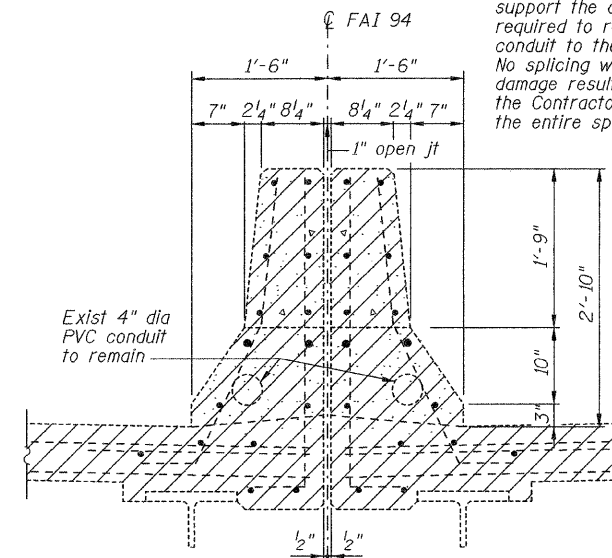
The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable.



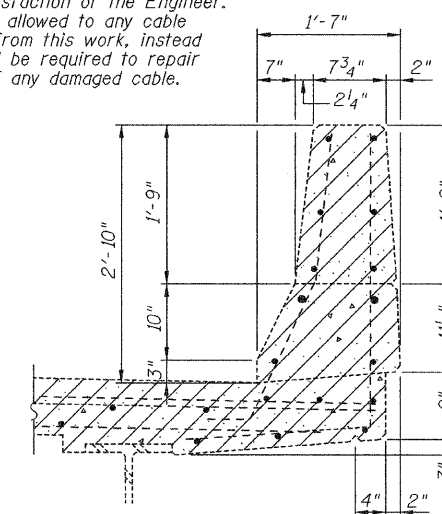
SECTION A-A



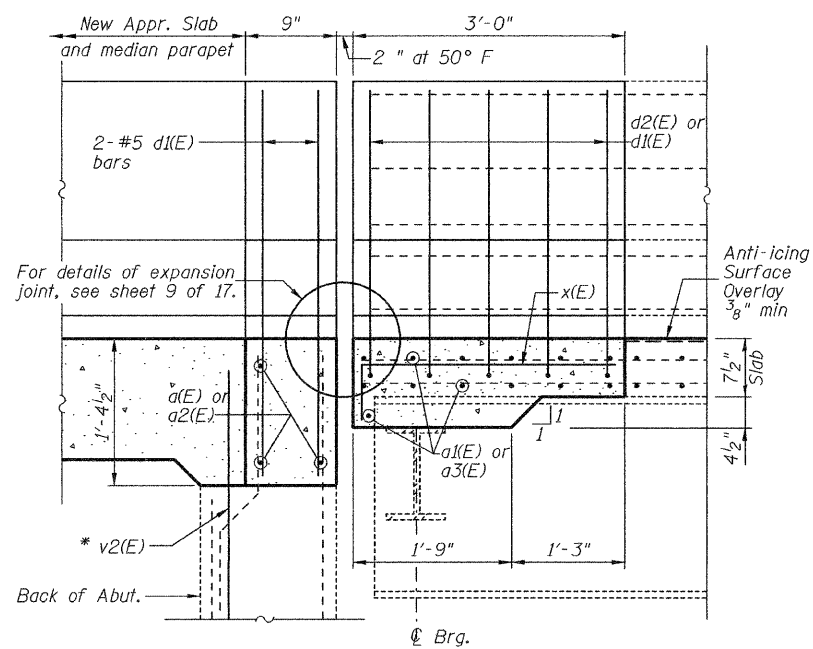
SECTION B-B



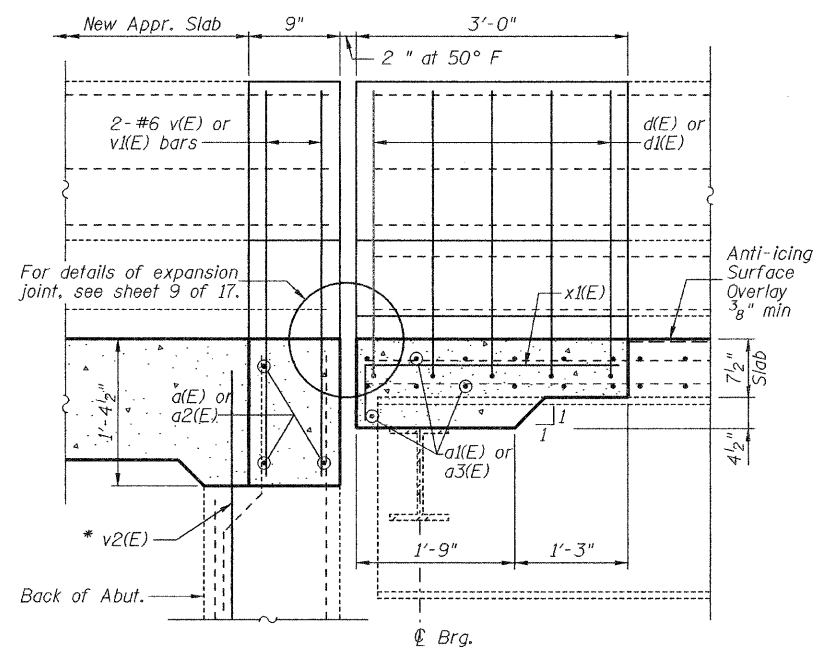
MEDIAN PARAPET  
SECTION C-C



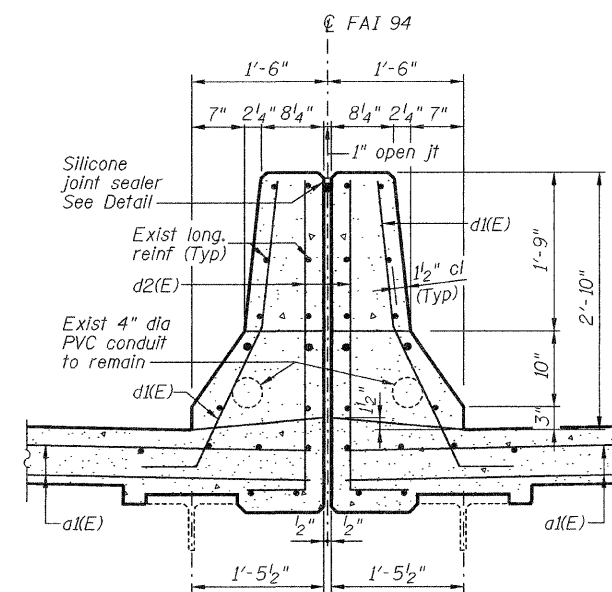
EB & WB PARAPET  
SECTION D-D



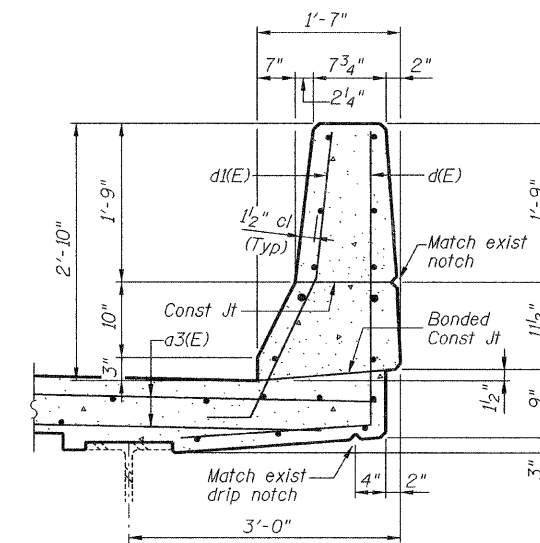
SECTION E-E



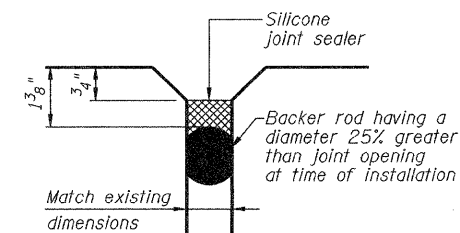
SECTION F-F



MEDIAN PARAPET  
SECTION G-G



EB & WB PARAPET  
SECTION H-H



SILICONE BRIDGE JOINT SEALER  
(Typical entire length of bridge)

\* See details on sheets 16 & 17 of 17.

EXPANSION JOINT REMOVAL AND  
REPLACEMENT DETAILS 1  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

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SHEET NO. 7 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&070)RS-11	COOK	133	76
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

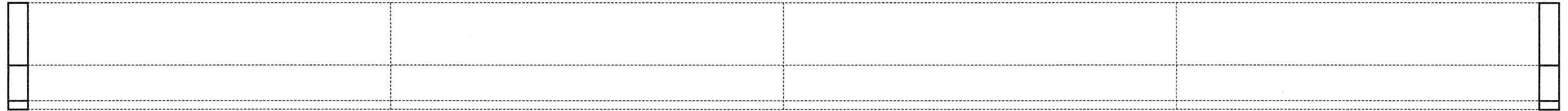
S Abut

⊙ Pier No. 3

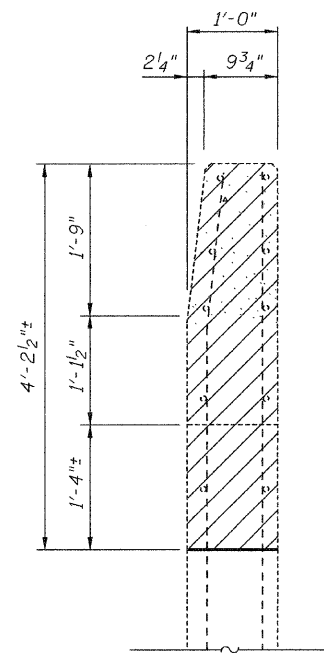
⊙ Pier No. 2

⊙ Pier No. 1

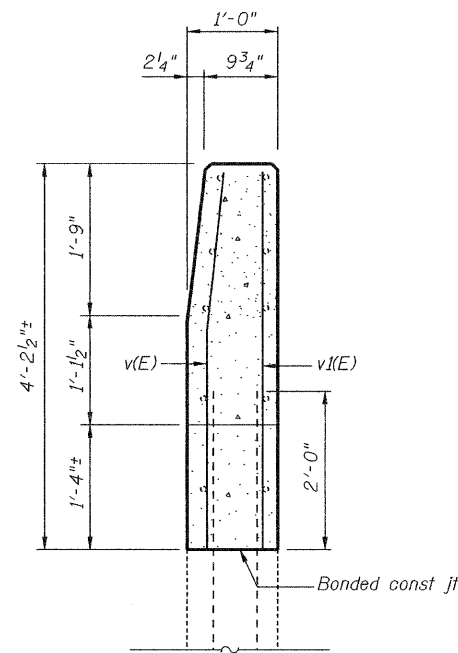
N Abut



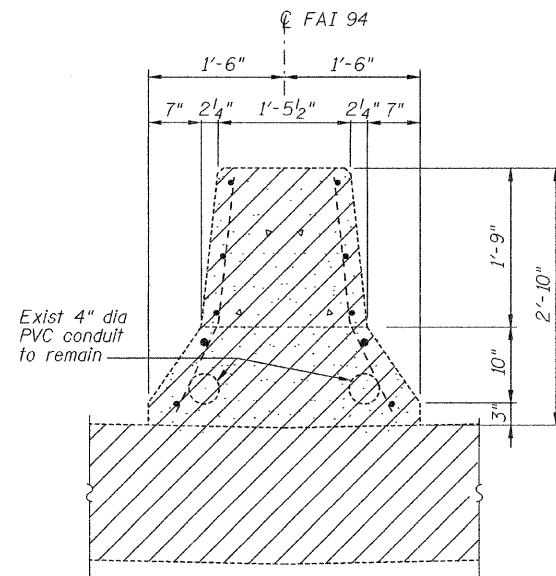
INSIDE ELEVATION OF WEST PARAPET



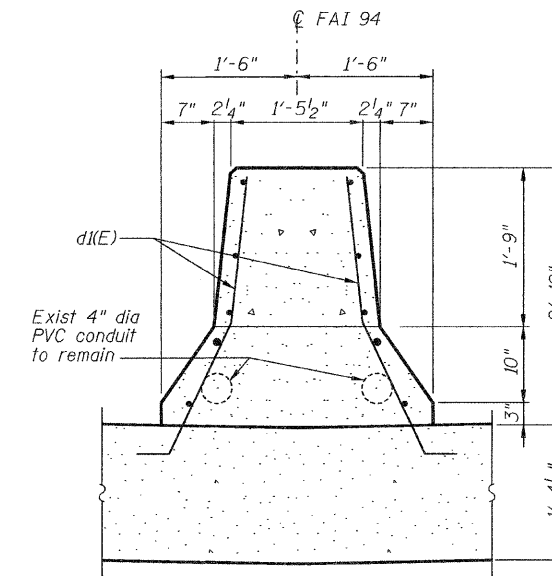
SECTION J-J



SECTION K-K



MEDIAN PARAPET  
SECTION L-L

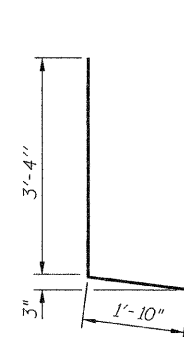


MEDIAN PARAPET  
SECTION M-M

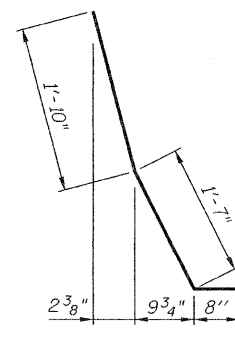
SUPERSTRUCTURE  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	12	#6	35'-0"	—
a1(E)	52	#5	35'-0"	—
a2(E)	12	#6	29'-10"	—
a3(E)	52	#5	29'-10"	—
d(E)	20	#4	5'-2"	L
d1(E)	48	#5	4'-1"	┘
d2(E)	20	#4	4'-0"	┘
v(E)	8	#6	4'-0"	I
v1(E)	8	#6	4'-1"	—
x(E)	232	#5	3'-3"	┘
Concrete Removal			Cu Yd	40.0
Concrete Superstructure			Cu Yd	40.0
Protective Coat			Sq Yd	120.0
Reinforcement Bars, Epoxy Coated			Pound	6750
Silicone Joint Sealer, 2"			Foot	313

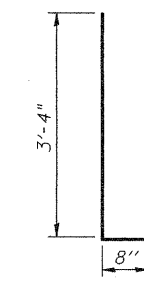
Reinforcement bars designated (E) shall be epoxy coated.



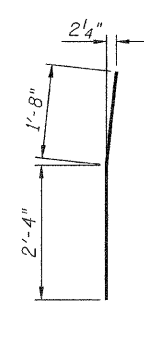
BAR d(E)



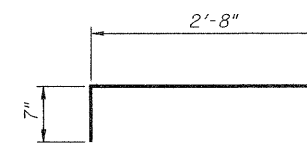
BAR d1(E)



BARS d2(E)



BAR v(E)



BAR x(E)

EXPANSION JOINT REMOVAL AND  
REPLACEMENT DETAILS 2  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

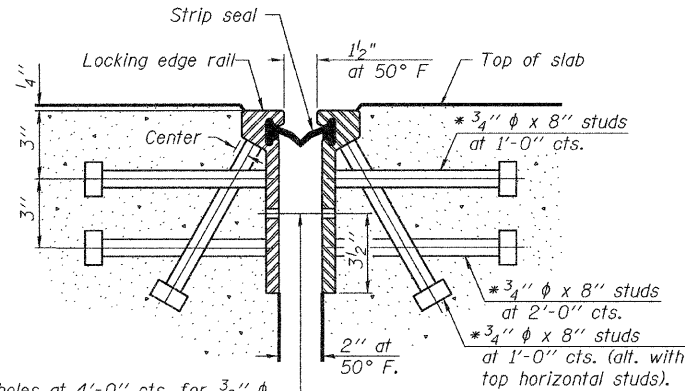
DESIGNED -	
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CHECKED -	

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SHEET NO. 8 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	77
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

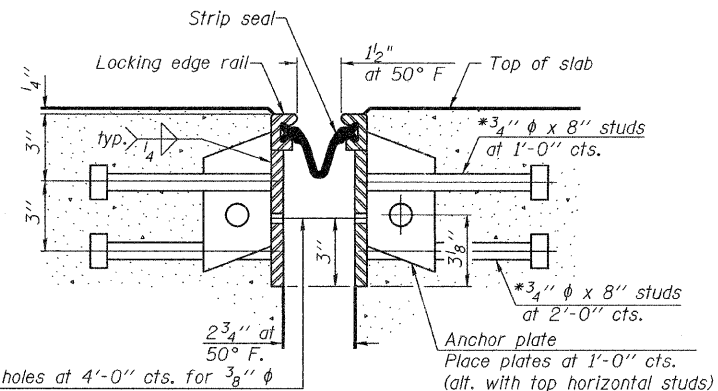
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU  
ROLLED RAIL JOINT

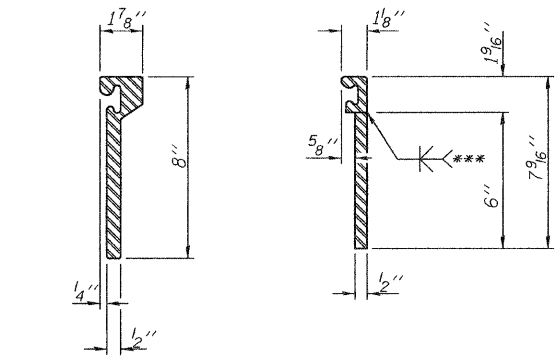


7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

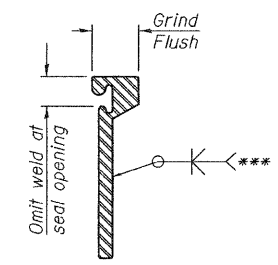
SECTION THRU  
WELDED RAIL JOINT

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches. The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints. The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State. All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

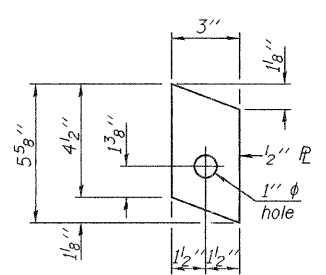


ROLLED  
EXTRUDED RAIL      WELDED RAIL

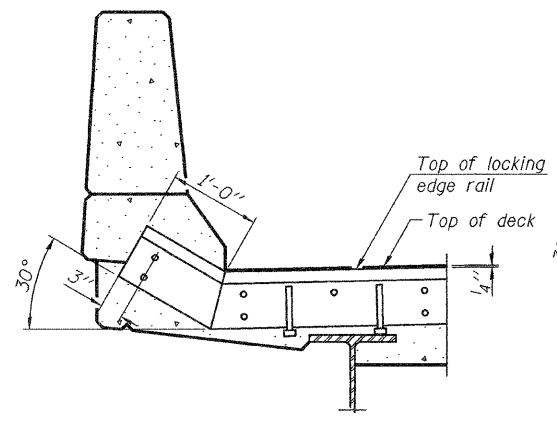


LOCKING EDGE  
RAIL SPLICE

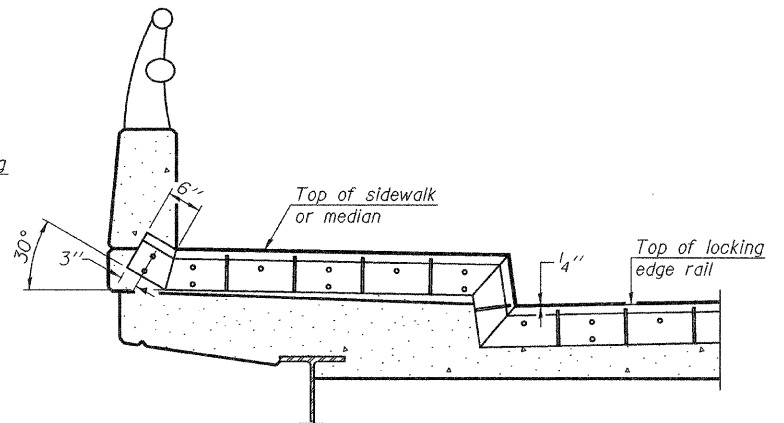
The inside of the locking edge rail groove shall be free of weld residue.



ANCHOR PLATE  
(for welded rail)



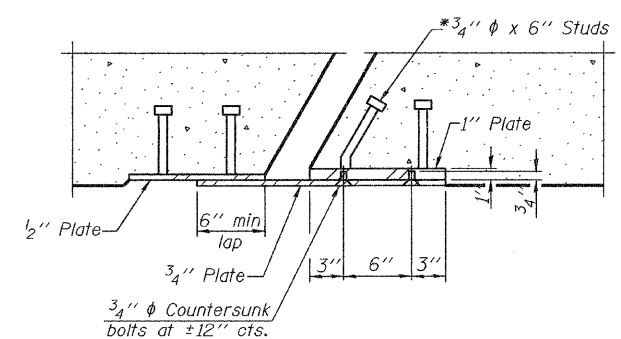
AT PARAPET



AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.

TYPICAL END TREATMENTS



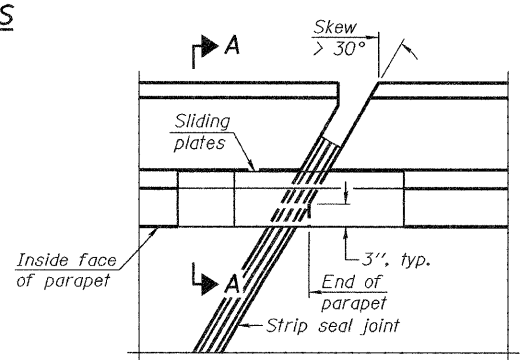
SECTION B-B

BILL OF MATERIAL

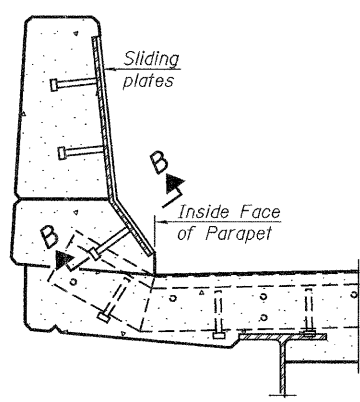
Item	Unit	Total
Preformed Joint Strip Seal	Foot	262.0

PREFORMED JOINT STRIP SEAL  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

LOCKING EDGE RAILS



PLAN



SECTION A-A

POINT BLOCK DETAILS  
(for skews > 30°)

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

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SHEET NO. 9 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-T&0707)RS-11	COOK	133	78
		BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21	
		FED. ROAD DIST. NO. - ILLINOIS		FED. AID PROJECT	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

The diameter of this part is equal or larger than the diameter of bar spliced.

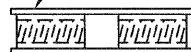
The diameter of this part is the same as the diameter of the bar spliced.

ROLLED THREAD DOWEL BAR



\*\* ONE PIECE

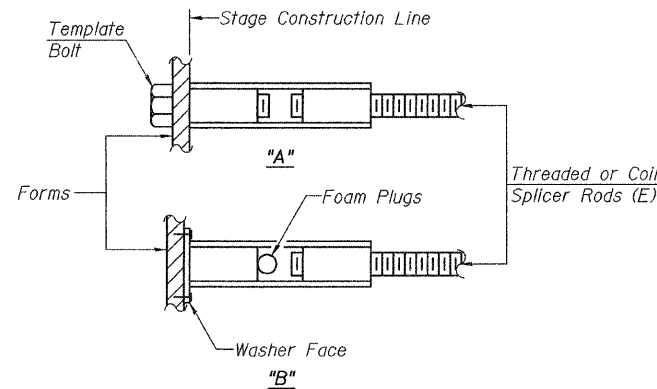
Wire Connector



WELDED SECTIONS

**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\*Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



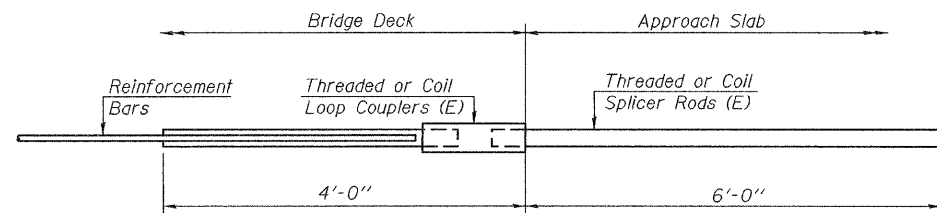
**INSTALLATION AND SETTING METHODS**

"A": Set bar splicer assembly by means of a template bolt.  
"B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
(E) : Indicates epoxy coating.

**NOTES**  
Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

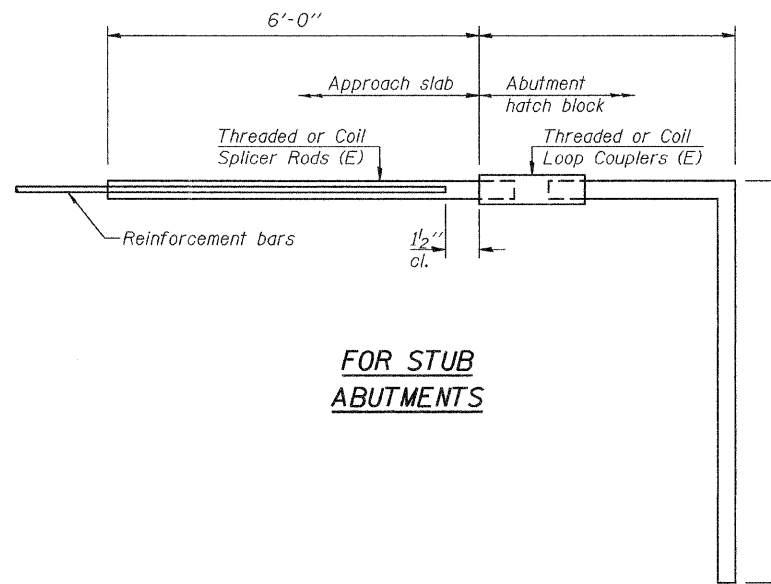
- ① Minimum Capacity (Tension in kips) =  $1.25 \times f_y \times A_t$
  - ② Minimum \*Pull-out Strength (Tension in kips) =  $0.66 \times f_y \times A_t$
- Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
\* = 28 day concrete

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



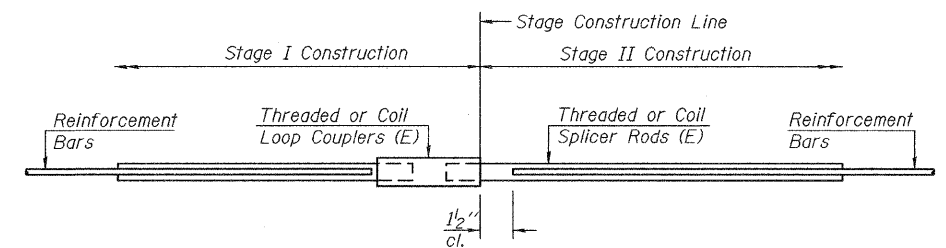
**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



**FOR STUB ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



**STANDARD**

Bar Size	No. Assemblies Required	Location
#4	100	Approach Slab
#5	184	Approach Slab
#5	160	Approach Footing
#5	52	Deck
#6	12	Abutment

**BAR SPLICER ASSEMBLY DETAILS  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162**

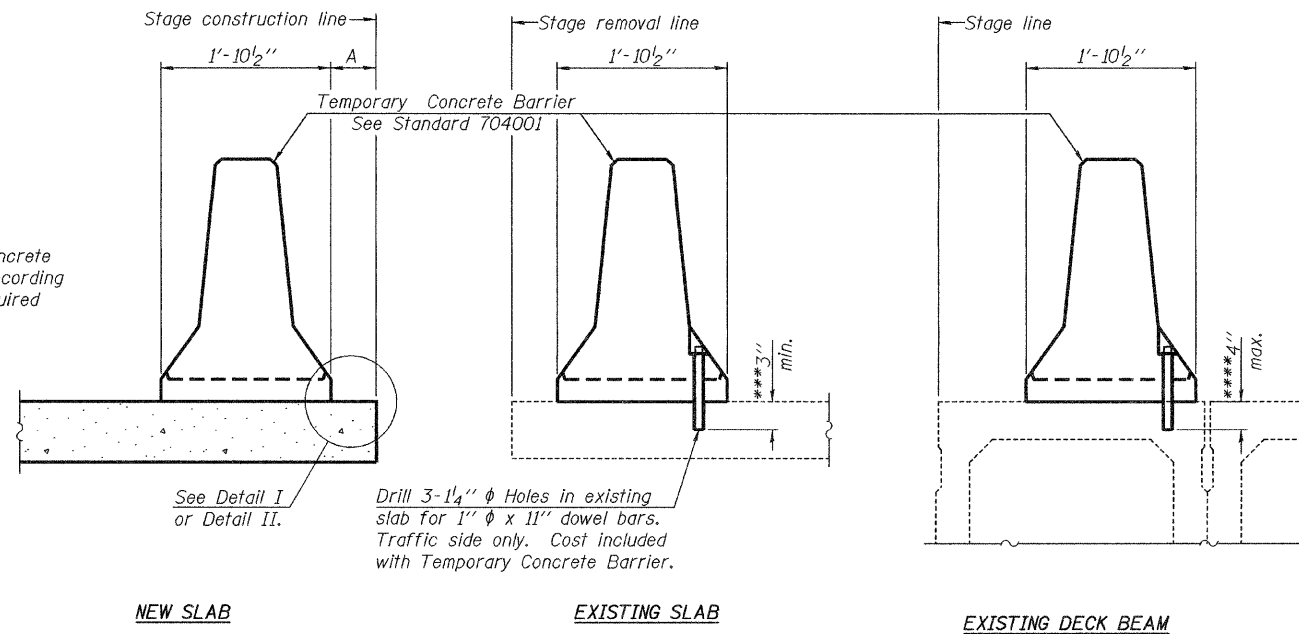
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DRAWN -	R KING
CHECKED -	

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SHEET NO. 10	F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707)RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 79
OF 17 SHEETS	BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21		
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".

NEW SLAB

EXISTING SLAB

EXISTING DECK BEAM

See Detail I or Detail II.

Drill 3-1/4"  $\phi$  Holes in existing slab for 1"  $\phi$  x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

NOTES

Detail I - With Bar Splicer or Couplers:  
Connect one (1) 1"x7"x10" steel  $\bar{P}$  to the top layer of couplers with 2-5/8"  $\phi$  bolts screwed to coupler at approximate  $\bar{C}$  of each barrier panel.

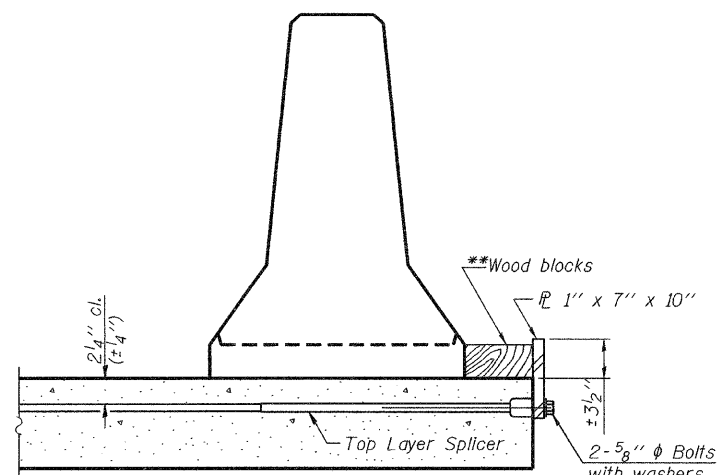
Detail II - With Extended Reinforcement Bars:  
Connect one (1) 1"x7"x10" steel  $\bar{P}$  to the concrete slab or concrete wearing surface with 2-5/8"  $\phi$  Expansion Anchors or cast in place Inserts spaced between the top layer of reinforcement at approximate  $\bar{C}$  of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

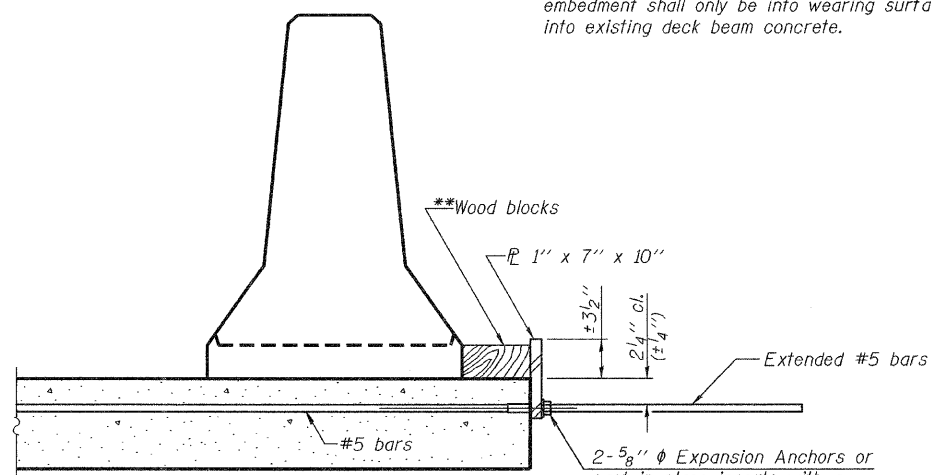
SECTIONS THRU SLAB OR DECK BEAM

\*\*\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

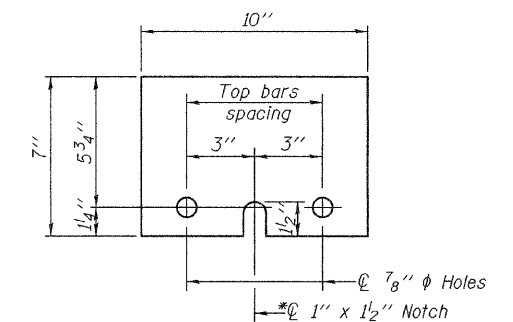
\*\*\*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.



DETAIL I



DETAIL II



STEEL RETAINER  $\bar{P}$  1" x 7" x 10"

\* Required only with Detail II

\*\*Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.

TEMPORARY CONCRETE BARRIER  
FOR STAGE CONSTRUCTION  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

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R-27 10-1-08

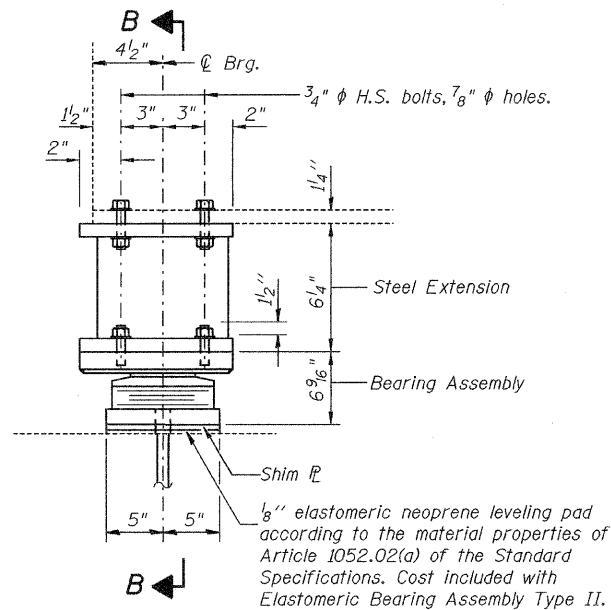
SHEET NO. 11 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	80
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. -			ILLINOIS FED. AID PROJECT		



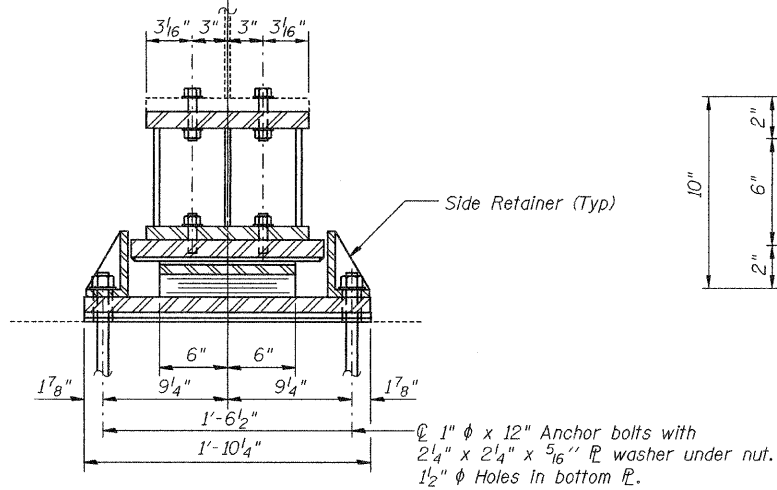
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

GIRDER REACTIONS

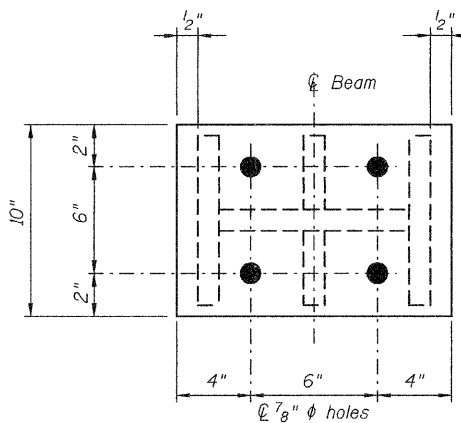
R $\bar{D}$	(k)	25.0
R $\bar{L}$	(k)	36.4
IMP	(k)	9.4
R (TOTAL)	(k)	70.8



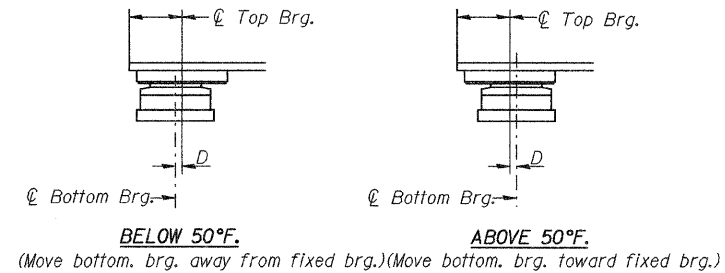
ELEVATION AT ABUTMENTS



SECTION A-A



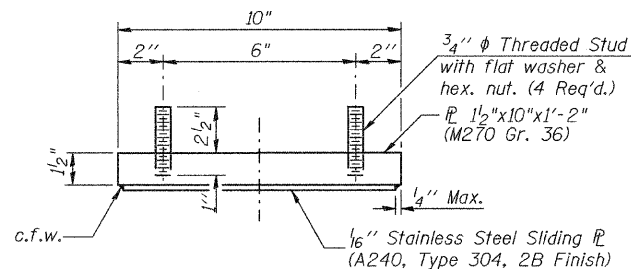
PLAN STEEL EXTENSION



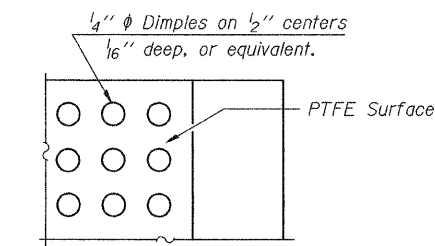
BELOW 50°F.  
ABOVE 50°F.  
**SETTING ANCHOR BOLTS AT EXP. BRG.**  
D=1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

Note:  
Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.

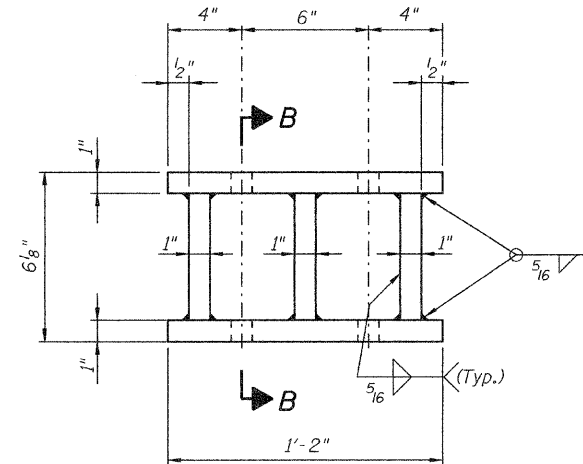
Notes:  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Anchor bolts at fixed bearings may be either cast in place or installed in holes drilled after the supported member is in place.  
Anchor bolts for Type II bearings shall be placed in holes drilled in the concrete through holes in the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.  
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.  
Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type II.  
The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.  
Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.



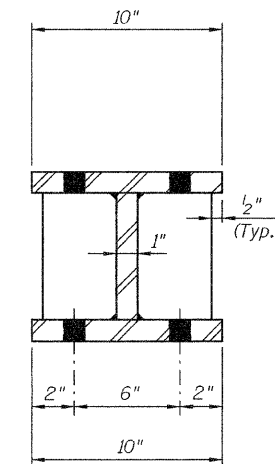
TOP BEARING ASSEMBLY



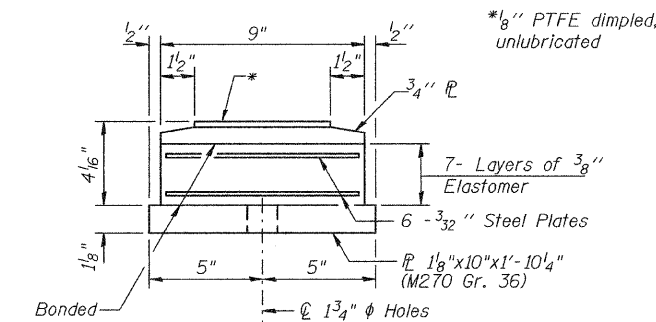
PLAN-PTFE SURFACE



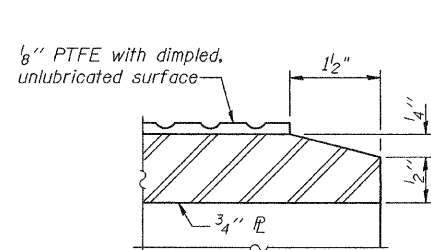
ELEVATION STEEL EXTENSION



SECTION B-B



BOTTOM BEARING ASSEMBLY



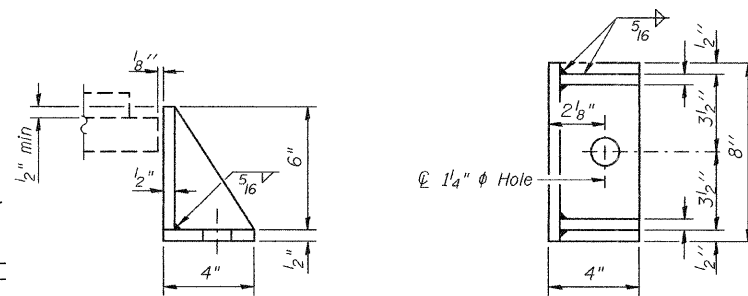
SECTION THRU PTFE

FABRICATED STEEL EXTENSION

BILL OF MATERIAL

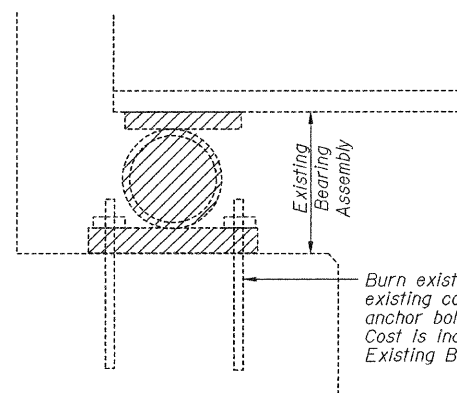
Item	Unit	Total
Elastomeric Bearing Assembly Type II	Each	36
Anchor Bolts, 1" phi	Each	72
Furnishing and Erecting Structural Steel	lbs	5210

TYPE II ELASTOMERIC EXPANSION BEARING



SIDE RETAINER

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



EXISTING BEARING REMOVAL DETAIL

(N. Abut, S. Abut)

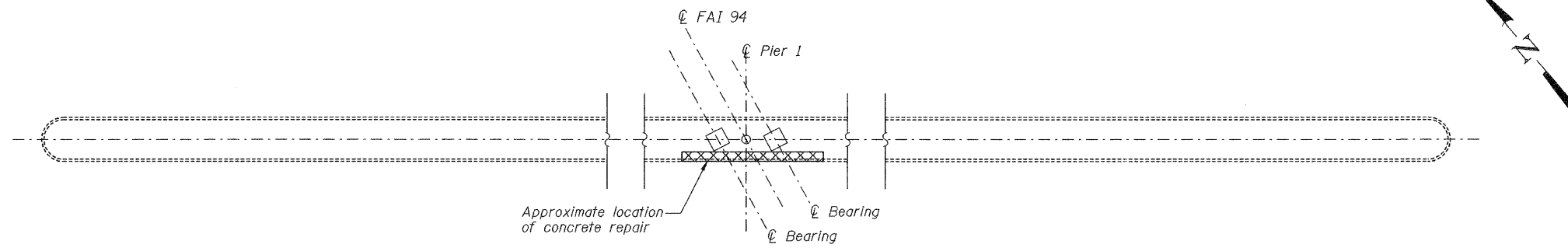
ELASTOMERIC BEARING ASSEMBLY, TYPE II  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

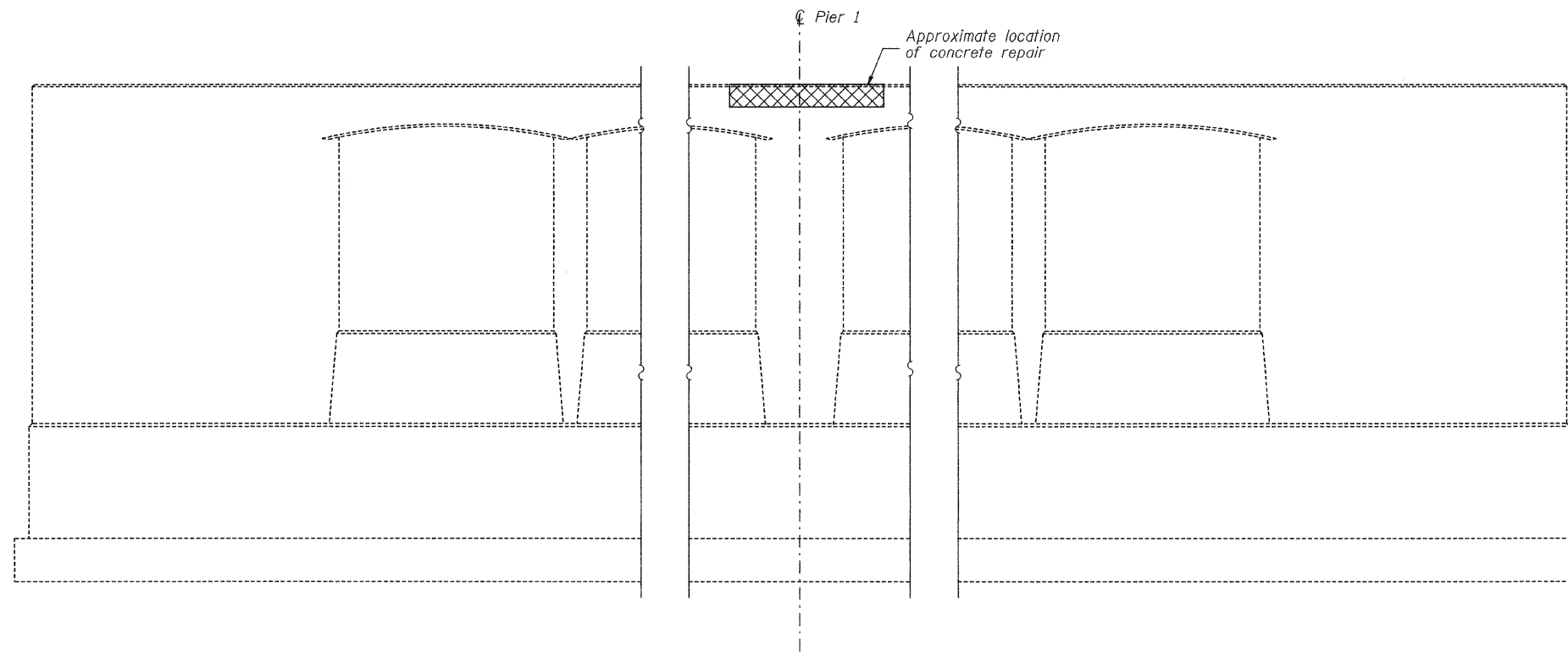
**HOMER L. CHASTAIN & ASSOCIATES, LLP**  
CONSULTING ENGINEERS  
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CHICAGO (773) 714-0050  
ROCKFORD (815) 489-0050  
184-001397

SHEET NO. 12	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
OF 17 SHEETS	94	(0405,ETC,42-7&0707)RS-11	COOK	133	81
		BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21	
		FED. ROAD DIST. NO. - ILLINOIS		FED. AID PROJECT	

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**PIER 1 REPAIR PLAN**



**PIER 1 REPAIR ELEVATION**

**BILL OF MATERIAL**

Item	Unit	Total
Structural Repair of Concrete (Depth Greater Than 5")	Sq. Ft.	10.0

**PIER 1 REPAIR DETAILS**  
**BISHOP FORD EXPRESSWAY (I-94)**  
**GREENWOOD AVENUE**  
**COOK COUNTY**  
**STATION**  
**STRUCTURE NUMBER 016-0162**

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

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184-001597

 Formed Concrete Repair Depth ≥ 5"

SHEET NO. 13 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	82
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. _ ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	569+65.17	57.58	624.58
A	569+55.17	57.58	624.54
B	569+45.17	57.58	624.50
End of North appr pavement	569+35.17	57.58	624.45

EAST MEDIAN CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	569+34.56	1.50	624.81
A	569+24.56	1.50	624.76
B	569+14.56	1.50	624.71
End of North appr pavement	569+04.56	1.50	624.65

PG EASTBOUND LANES

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	569+22.28	-21.00	625.15
A	569+12.28	-21.00	625.10
B	569+02.28	-21.00	625.05
End of North appr pavement	568+92.28	-21.00	624.99

PG WESTBOUND LANES

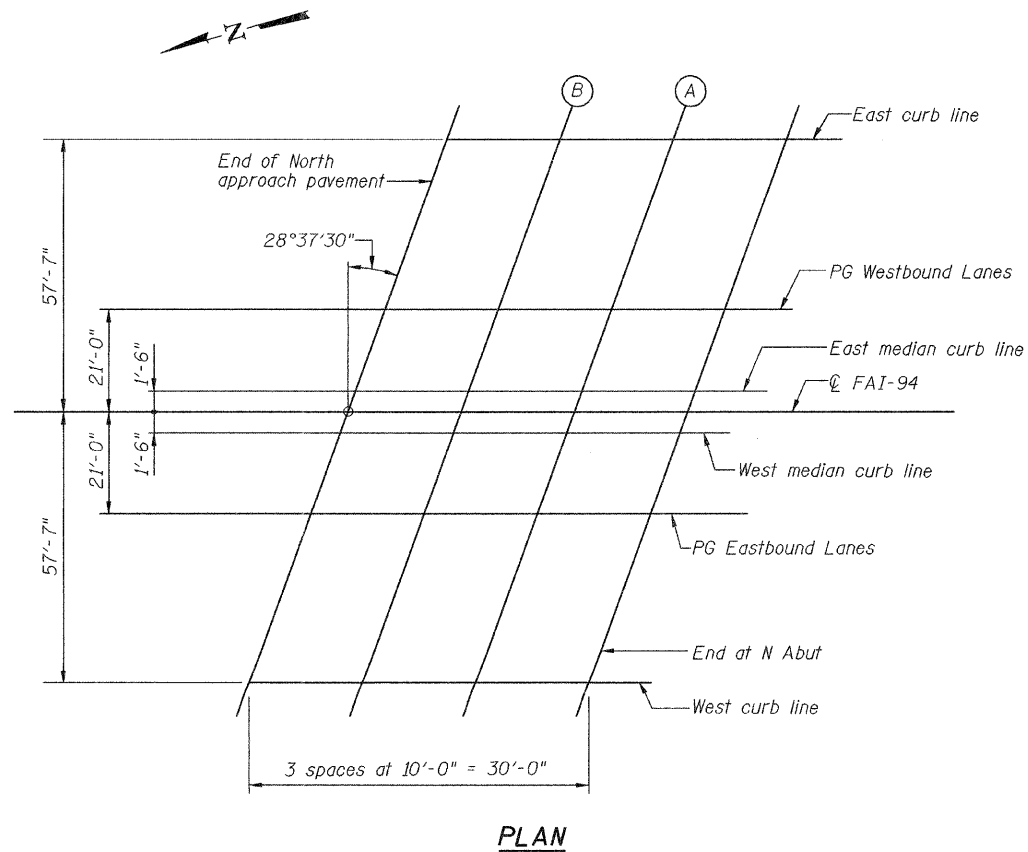
Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	569+45.20	21.00	625.26
A	569+35.20	21.00	625.21
B	569+25.20	21.00	625.17
End of North appr pavement	569+15.20	21.00	625.12

WEST MEDIAN CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	569+32.92	-1.50	624.80
A	569+22.92	-1.50	624.75
B	569+12.92	-1.50	624.70
End of North appr pavement	569+02.92	-1.50	624.64

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	569+02.31	-57.58	624.28
A	568+92.31	-57.58	624.23
B	568+82.31	-57.58	624.17
End of North appr pavement	568+72.31	-57.58	624.10



TOP OF NORTH APPROACH  
SLAB ELEVATIONS  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	
<b>E-AS</b>	

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5-16-08

SHEET NO. 14 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	83
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS			FED. AID PROJECT		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at S Abut	572+76.52	57.58	624.28
A	572+86.52	57.58	624.23
B	572+96.52	57.58	624.17
End of South appr pavement	573+06.52	57.58	624.10

EAST MEDIAN CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at S Abut	572+45.91	1.50	624.80
A	572+55.91	1.50	624.75
B	572+65.91	1.50	624.70
End of South appr pavement	572+75.91	1.50	624.64

PG EASTBOUND LANES

Location	Station	Offset	Theoretical Grade Elevations
End at S Abut	572+33.63	-21.00	625.26
A	572+43.63	-21.00	625.21
B	572+53.63	-21.00	625.17
End of South appr pavement	572+63.63	-21.00	625.12

PG WESTBOUND LANES

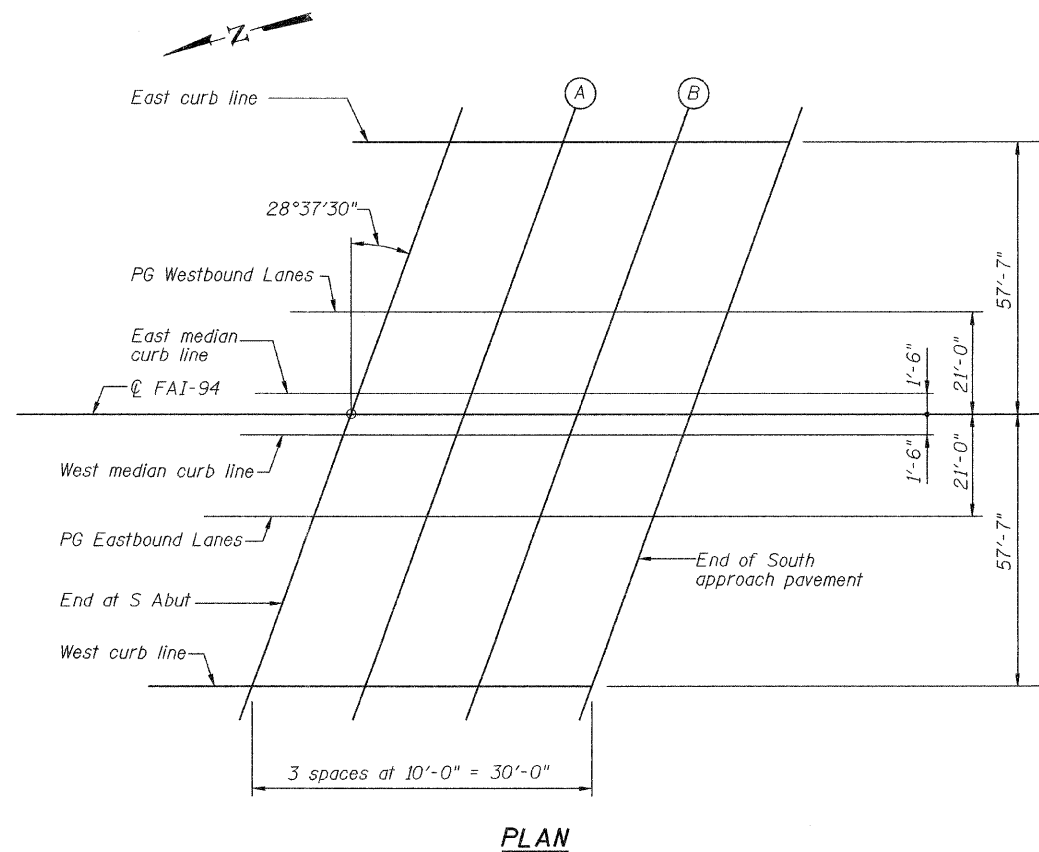
Location	Station	Offset	Theoretical Grade Elevations
End at S Abut	572+56.55	21.00	625.15
A	572+66.55	21.00	625.10
B	572+76.55	21.00	625.05
End of South appr pavement	572+86.55	21.00	624.99

WEST MEDIAN CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at S Abut	572+44.27	-1.50	624.81
A	572+54.27	-1.50	624.76
B	572+64.27	-1.50	624.71
End of South appr pavement	572+74.27	-1.50	624.65

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at S Abut	572+13.66	-57.58	624.58
A	572+23.66	-57.58	624.54
B	572+33.66	-57.58	624.50
End of South appr pavement	572+43.66	-57.58	624.45



**TOP OF SOUTH APPROACH  
SLAB ELEVATIONS  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162**

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	
<b>E-AS</b>	

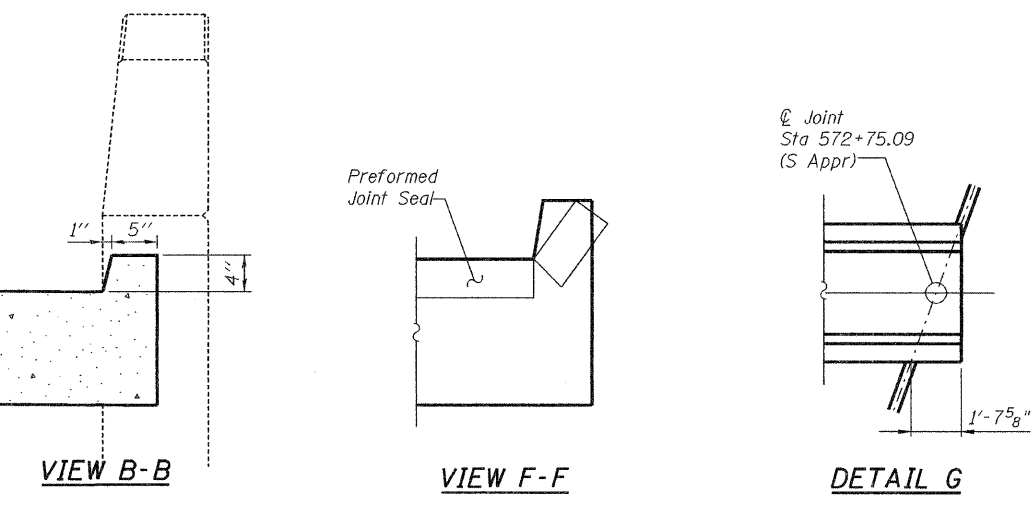
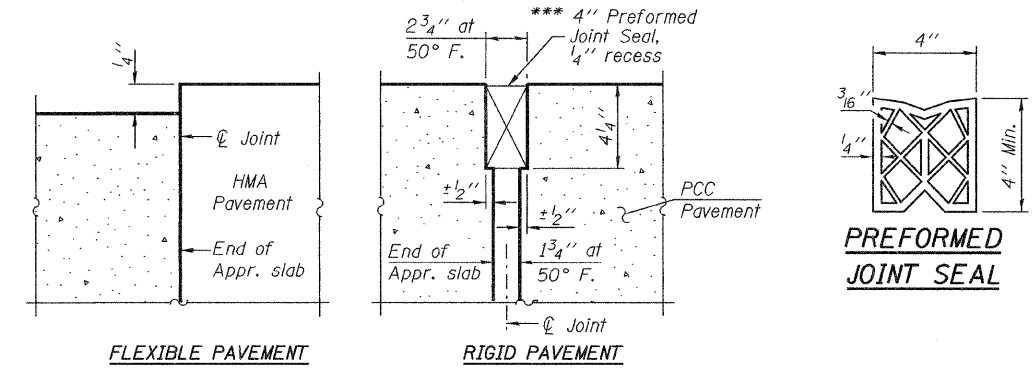
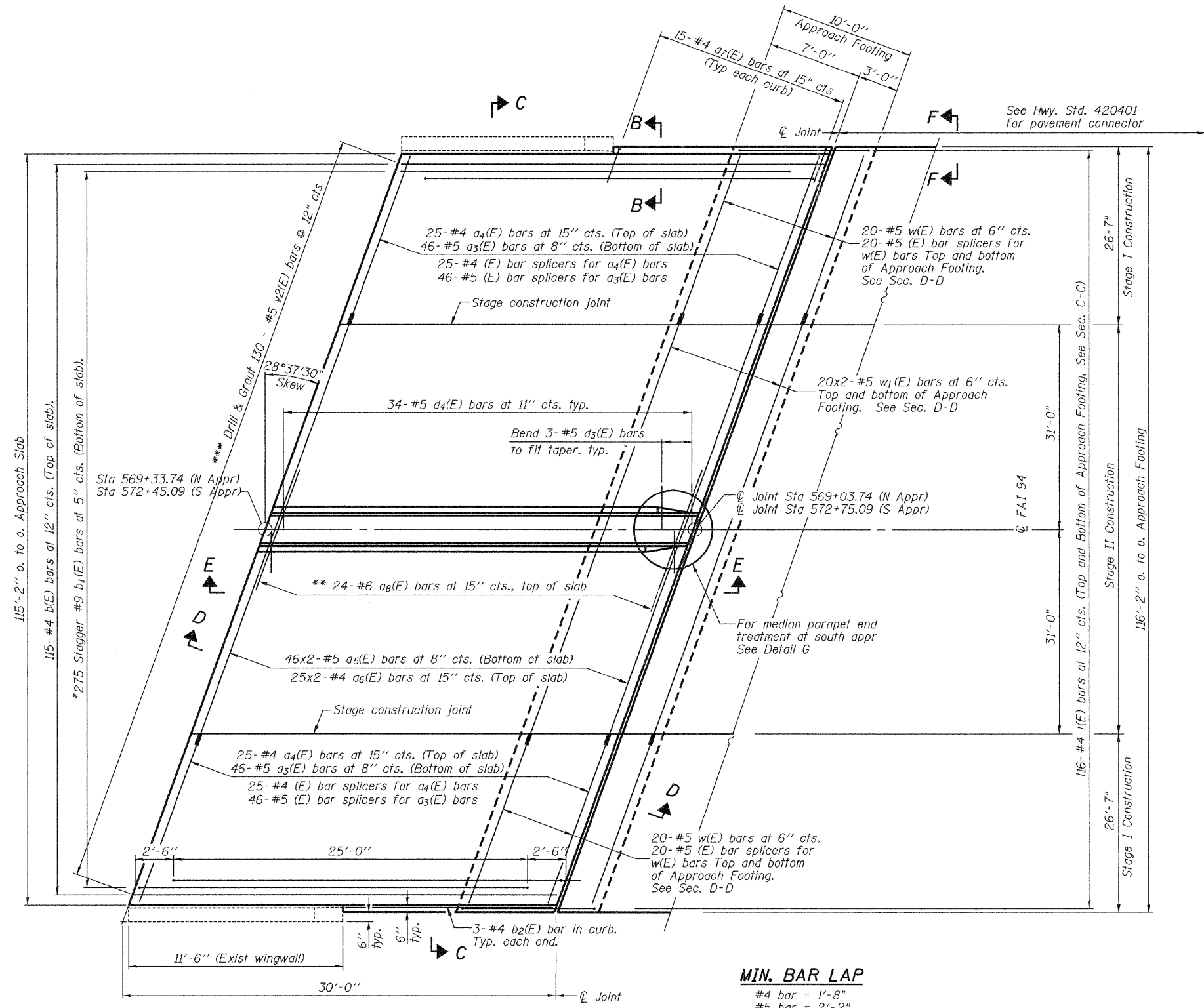
5-16-08

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SHEET NO. 15	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
OF 17 SHEETS	94	(0405,ETC,42-7&0707)RS-11	COOK	133	84
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - [ILLINOIS] FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

\*\*\* Cost included with Concrete Superstructure.



PLAN

**MIN. BAR LAP**  
#4 bar = 1'-8"  
#5 bar = 2'-2"

DESIGNED -	
CHECKED -	
DRAWN -	R KING
CHECKED -	

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184-001397

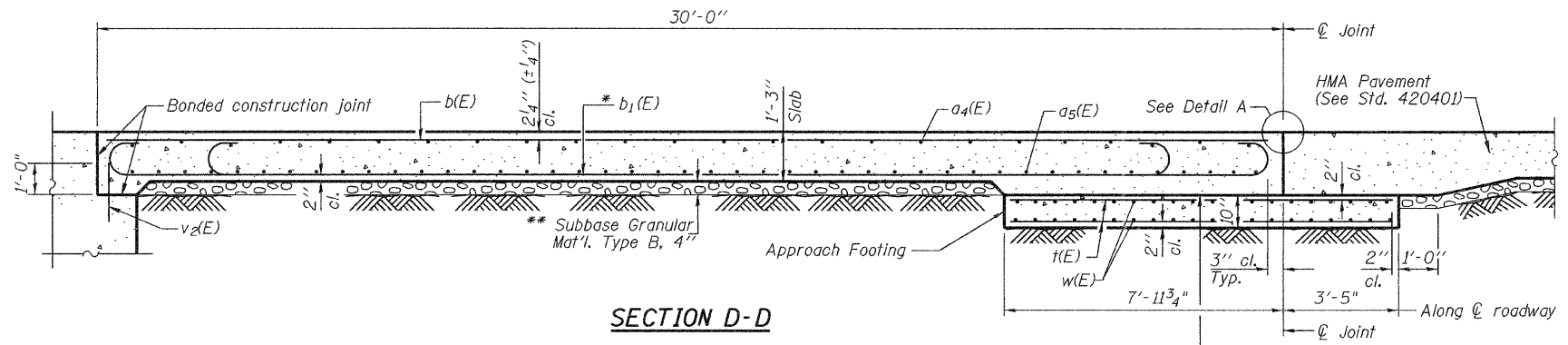
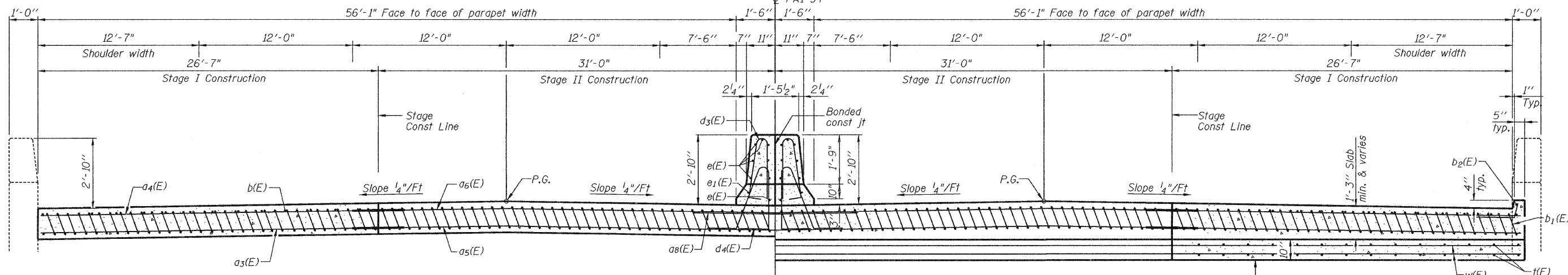
- \* Tilt #9<sub>1</sub>b (E) bars as required to maintain clearance.
- \*\* Alternate with a(E) bars, typ. median parapet.
- \*\*\* Bars shall be drilled and epoxy grouted according to Article 584 of the Standard Specifications.

Notes:  
See sheet 17 of 17 for Sections C-C & D-D and View E-E.  
a(E) thru a8(E), and w(E) bar spacings measured perpendicular to C Rdwy.

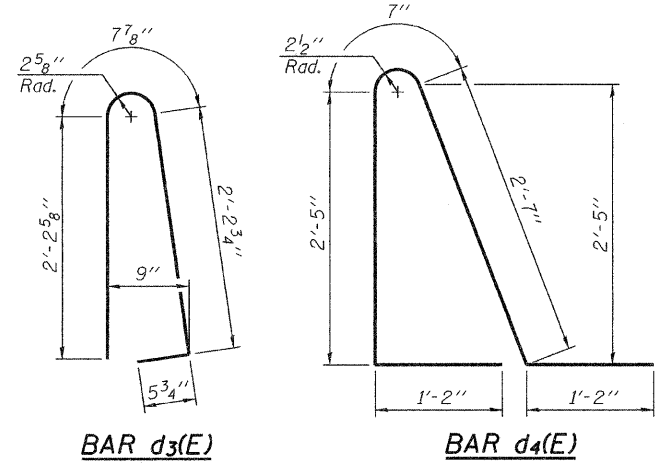
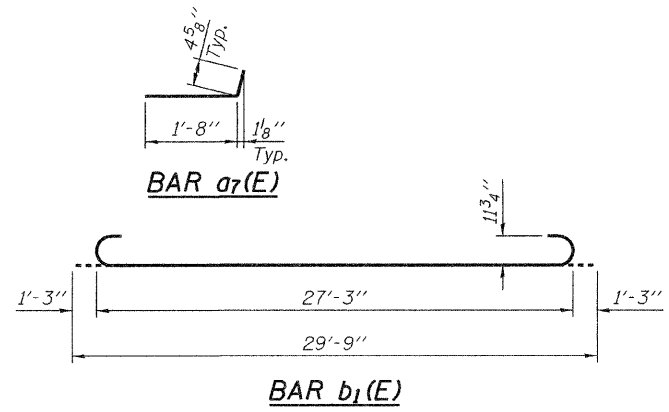
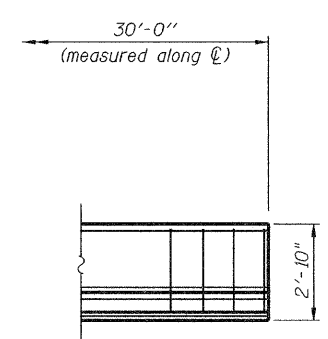
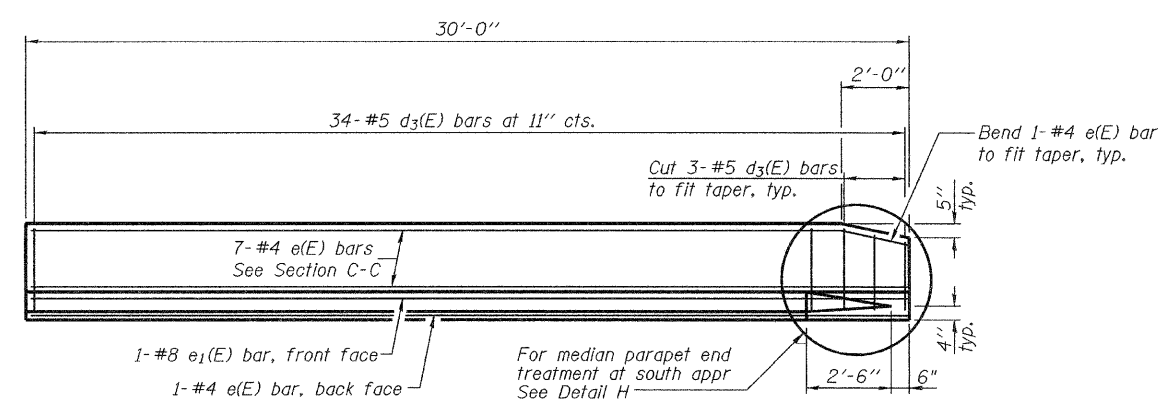
(Sheet 1 of 2)  
**BRIDGE APPROACH SLAB DETAILS**  
**BISHOP FORD EXPRESSWAY (I-94)**  
**GREENWOOD AVENUE**  
**COOK COUNTY**  
**STATION**  
**STRUCTURE NUMBER 016-0162**

SHEET NO. 16 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	85
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



\* Tilt #9 b<sub>1</sub>(E) bars as required to maintain clearance.  
\*\* Cost included with Concrete Superstructure.



TWO APPROACHES  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape	
a <sub>3</sub> (E)	184	#5	29'-10"	—	
a <sub>4</sub> (E)	100	#4	29'-10"	—	
a <sub>5</sub> (E)	184	#5	36'-4"	—	
a <sub>6</sub> (E)	100	#4	36'-4"	—	
a <sub>7</sub> (E)	60	#4	2'-1"	—	
a <sub>8</sub> (E)	48	#6	6'-0"	—	
b(E)	230	#4	29'-8"	—	
b <sub>1</sub> (E)	550	#9	29'-9"	—	
b <sub>2</sub> (E)	12	#4	18'-0"	—	
d <sub>3</sub> (E)	136	#5	5'-7"	—	
d <sub>4</sub> (E)	136	#5	7'-11"	—	
e(E)	32	#4	29'-8"	—	
e <sub>1</sub> (E)	4	#8	29'-8"	—	
f(E)	464	#4	11'-0"	—	
v <sub>2</sub> (E)	260	#5	2'-0"	—	
w(E)	160	#5	29'-10"	—	
w <sub>1</sub> (E)	160	#5	36'-4"	—	
Concrete Superstructure				Cu. Yd.	366.8
Concrete Structures				Cu. Yd.	80.0
Reinforcement Bars, Epoxy Coated				Pound	95,830
Structural Repair of Concrete (Depth Greater Than 5")				Sq. Ft.	4.0

(Sheet 2 of 2)  
BRIDGE APPROACH SLAB DETAILS  
BISHOP FORD EXPRESSWAY (I-94)  
GREENWOOD AVENUE  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0162

DESIGNED -
CHECKED -
DRAWN - R KING
CHECKED -

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Notes:  
See sheet 16 of 17 for Detail A and View B-B.  
Approach slab and parapet concrete shall be paid for as Concrete Superstructure.  
Approach footing concrete shall be paid for as Concrete Structures.  
Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.  
The approach footing maximum applied service bearing pressure (Q<sub>max</sub>) = 2.0 ksf.  
For bar splicer details, see sheet 10 of 17.  
Cost of excavation for approach footing included with Concrete Structures.

SHEET NO. 17 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	86
		BISHOP FORD EXPRESSWAY (I-94)	CONTRACT NO. 60D21		
		FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

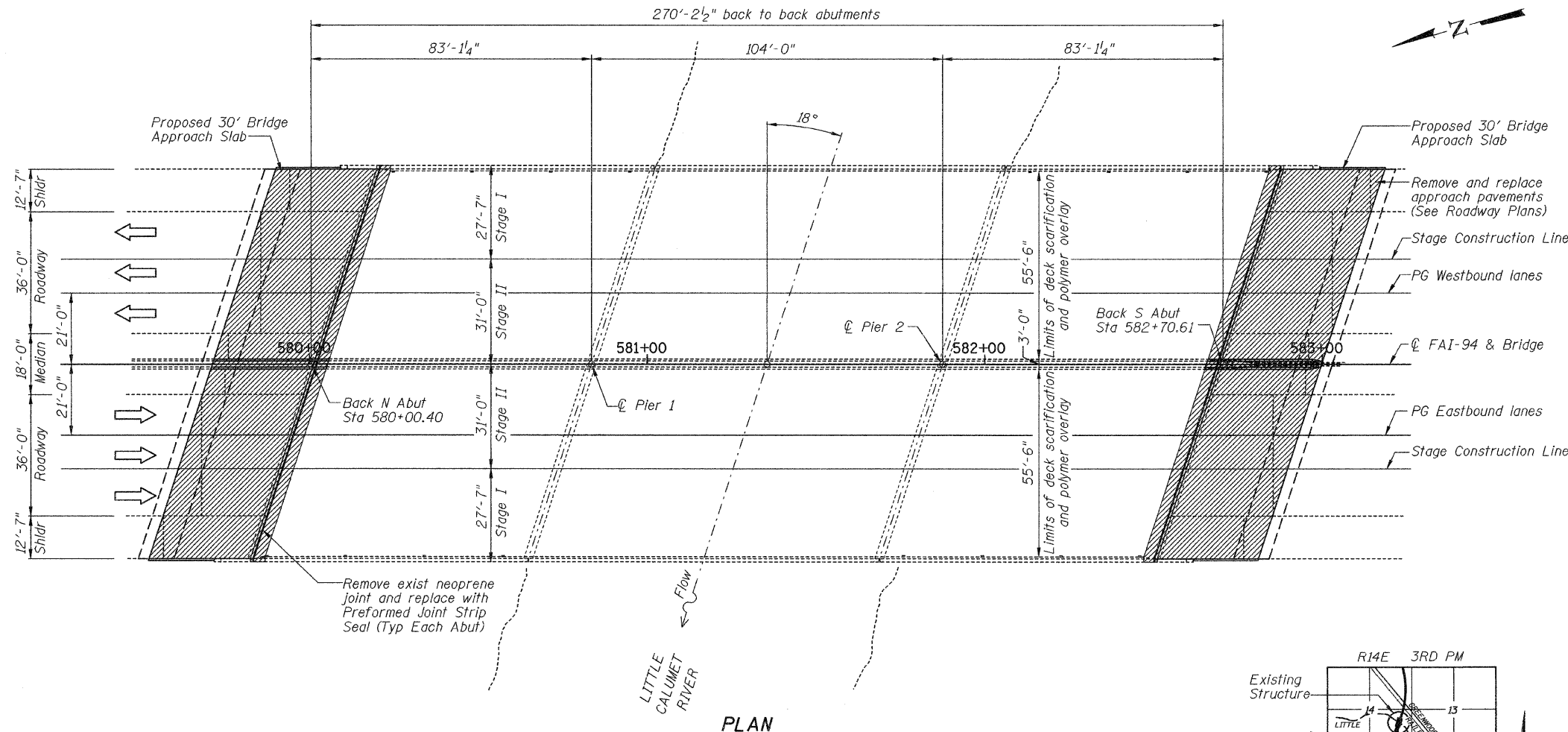
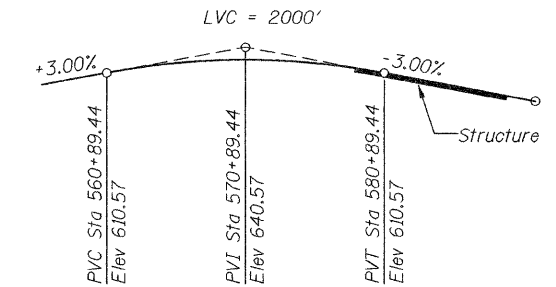
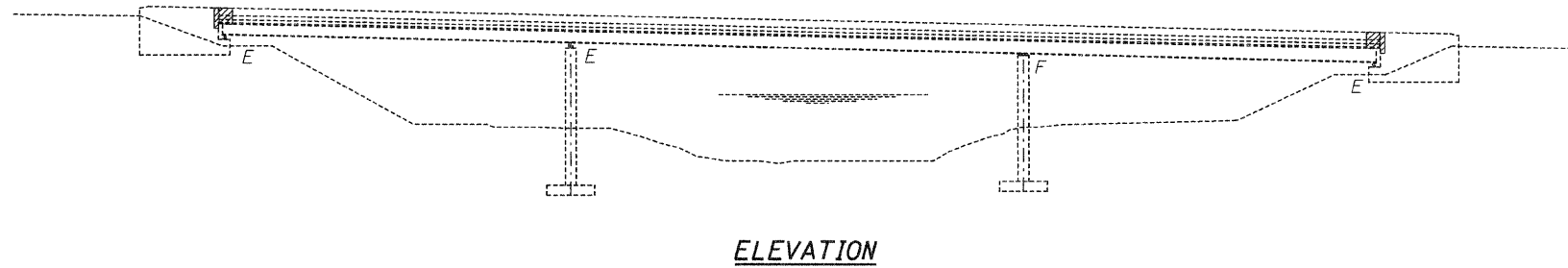
Existing structure:  
The original structure was built in 1947 as Route SA-66, Section 066-505.2-MFT, 1975-079-BR. In 1981, the bridge was widened and reconstructed with a new deck and new line of steel beams on each side and the superstructure was cleaned and painted.

Bench Mark:  
"X" scribed in chiseled box on top of concrete barrier wall at the southeast corner of bridge structure for Westbound FAI-94 over the Little Calumet River  
Elev 606.96 (Assumed Local Datum)

Stage construction shall be utilized to maintain traffic during construction.  
No salvage

**DESIGN SPECIFICATIONS**  
2002 AASHTO 17th Edition

**DESIGN STRESSES**  
FIELD UNITS  
f'c = 3,500 psi  
fy = 60,000 psi (Reinforcement)  
fy = 36,000 psi (M270 Grade 36)



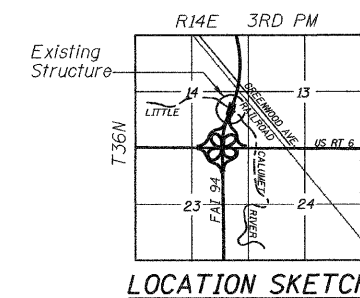
**EXISTING PROFILE GRADE FAI-94**

**SCOPE OF WORK**

- Shot blast the existing deck and apply a thin anti-icing overlay 3/8" min thickness.
- Replace deck joints at each abutment with Preformed Joint Strip Seal.
- Replace bridge approach pavements.
- Jack and remove bearings and replace with elastomeric bearings at each abutment.
- Remove debris from abutment bearing seats.
- Clean and paint steel beam ends and end diaphragms at abutments.
- Repair spall in the east parapet in the south approach, westbound lanes with formed concrete repair.
- Re-seal joint between center parapets.

**GENERAL PLAN AND ELEVATION  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163**

DESIGNED -	<b>HOMER L. CHASTAIN &amp; ASSOCIATES, LLP</b> CONSULTING ENGINEERS DECATUR (217) 422-8544 CHICAGO (773) 714-0050 ROCKFORD (815) 489-0050 184-001597
CHECKED -	
DRAWN -	
CHECKED -	



SHEET NO. 1	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
OF 17 SHEETS	94	(0405,ETC,42-7&0707)RS-11	COOK	133	87
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

**GENERAL NOTES**

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Stage construction shall be utilized to maintain traffic during construction.

Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.

Reinforcement bars designated (E) shall be epoxy coated.

The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.

Structural steel shall only be painted for a distance of 5 ft. each way from the deck joints. All structural steel shall be cleaned as specified in the Special Provision for "Cleaning and Painting Existing Steel".

Two 1/8 in. adjusting shims shall be provided for each bearing in addition to all other plates or shims and placed as shown on bearing details.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with "Concrete Removal".

The Contractor shall exercise care during removal of existing joints to ensure that the slab, beams and diaphragms' integrity will not be detrimentally impacted. The Contractor shall repair any damage(s) to the slab, beams and diaphragms caused by his operation as directed by the Engineer at no additional cost to the Department.

Protective coat shall be applied only to the new concrete provided for the reconstruction of the joints (top of deck slab, top and traffic face of parapet).

The Engineer shall determine extent, location and type of substructure and deck slab repairs in the field.

All surfaces designated to be cleaned and painted shall be cleaned per near white metal blast cleaning SSPC-SP10. All surfaces designated to be cleaned and painted shall be painted according to the requirements of Paint System 1-OZ/E/U. See Special Provision for Cleaning and painting Existing Steel Structure.

The color of the final coat on exterior faces and bottom flanges of the fascia beams, exterior bearings and deck drains shall be Green, Munsell No. 7.5G 4/8, see Special Provision for Cleaning and Painting Existing Steel Structures.

The color of the final coat of all interior structural steel (beams, diaphragms, bearings, etc.) shall be Grey, Munsell No. 5B 7/1, see Special Provision for Cleaning and Painting Existing Steel Structures".

The SSPC QP-1 and QP-2 Contractor certifications will not be required for this contract. A minimum of 4 air monitors will be required to monitor abrasive blasting operations at this site, see special provision for "Containment and Disposal of Lead Paint Cleaning Residues".

The Inorganic Zinc Rich Primer / Acrylic / Acrylic Paint System shall be used for shop and field painting of new structural steel except where otherwise noted. The color of the final finish coat for all interior steel surfaces shall be gray, Munsell No. 5B 7/1. The color of the final finish coat for the exterior steel surfaces shall be Interstate Green, Munsell No. 7.5G 4/8. See Special Provision for "Cleaning and Painting New Metal Structures".

As directed by the Engineer, existing construction accessories welded to the top of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by an individual acceptable to the Engineer. Any cracks that can be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

The Contractor shall make allowance for the deflection of forms, shrinkage and settlement of falsework, in addition to allowance for dead load deflection. Forms for deck slab shall be removed prior to placement of bridge approach pavement.

If the Contractor elects to use cantilever forming brackets on the exterior beams or girders, the brackets shall be placed at the same locations as required for the hardwood blocks in Article 503.06 of the Standard Specifications. If additional cantilever forming brackets are required, hardwood blocking shall be wedged between the exterior and first interior beam at each of these additional bracket locations.

**TOTAL BILL OF MATERIAL**

ITEM	UNIT	SUPER	SUB.	TOTAL
CONCRETE REMOVAL	CU. YD.	40.0		40.0
CONCRETE STRUCTURES	CU. YD.		74.2	74.2
CONCRETE SUPERSTRUCTURE	CU. YD.	405.0		405.0
PROTECTIVE COAT	SQ. YD.	110.0		110.0
REINFORCEMENT BARS, EPOXY COATED	POUND	99860		99860
BAR SPLICERS	EACH	508		508
PERFORMED JOINT STRIP SEAL	FOOT	242.0		242.0
ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	18		18
ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	18		18
CONCRETE SEALER	SQ. FT.	610.0		610.0
CLEANING BRIDGE SEATS	SQ. FT.	610.0		610.0
DECK SLAB REPAIR (PARTIAL)	SQ. YD.	168.0		168.0
SILICONE JOINT SEALER, 2"	FOOT	275.0		275.0
STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5")	SQ. FT.	4.0		4.0
ANTI-ICING SURFACE OVERLAY	SQ. YD.	3350.0		3350.0
JACK AND REMOVE EXISTING BEARINGS	EACH	36		36
CLEANING AND PAINTING EXISTING STEEL STRUCTURES	L. SUM	1		1
ANCHOR BOLTS, 1"	EACH	72		72
CONTAINMENT AND DISPOSAL OF LEAD PAINT CLEANING RESIDUES, NO. 1	L. SUM	1		1
FURNISHING AND ERECTING STRUCTURAL STEEL	LBS	6960		6960

**INDEX OF SHEETS**

- 1 GENERAL PLAN AND ELEVATION
- 2 STRUCTURAL NOTES, BILL OF MATERIAL AND INDEX OF SHEETS
- 3 TYPICAL SECTION THRU BRIDGE
- 4 CONSTRUCTION STAGING DETAILS
- 5 EXPANSION JOINT REMOVAL AND REPLACEMENT PLAN AT N ABUTMENT
- 6 EXPANSION JOINT REMOVAL AND REPLACEMENT PLAN AT S ABUTMENT
- 7 EXPANSION JOINT REMOVAL AND REPLACEMENT DETAILS 1
- 8 EXPANSION JOINT REMOVAL AND REPLACEMENT DETAILS 2
- 9 PREFORMED JOINT STRIP SEAL
- 10 BAR SPLICER ASSEMBLY DETAILS
- 11 TEMPORARY CONCRETE BARRIER
- 12 ELASTOMERIC BEARING DETAILS - TYPE I
- 13 ELASTOMERIC BEARING DETAILS - TYPE II
- 14 TOP OF NORTH APPROACH SLAB ELEVATIONS
- 15 TOP OF SOUTH APPROACH SLAB ELEVATIONS
- 16 BRIDGE APPROACH SLAB DETAILS (SHEET 1 OF 2)
- 17 BRIDGE APPROACH SLAB DETAILS (SHEET 2 OF 2)

**STRUCTURAL NOTES, BILL OF MATERIAL  
AND INDEX OF SHEETS  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163**

**STANDARD DRAWINGS**

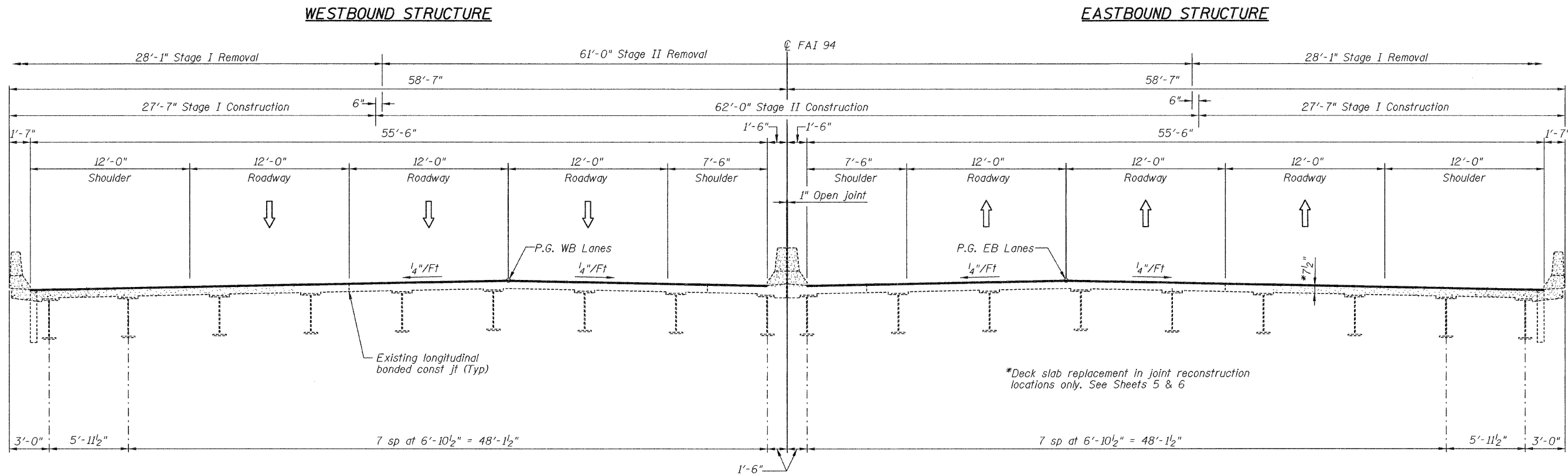
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SHEET NO. 2 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	88
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT					



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



TYPICAL CROSS SECTION THRU BRIDGE  
(Looking South)

**NOTES**

For joint removal limits see Sheets 5 thru 8

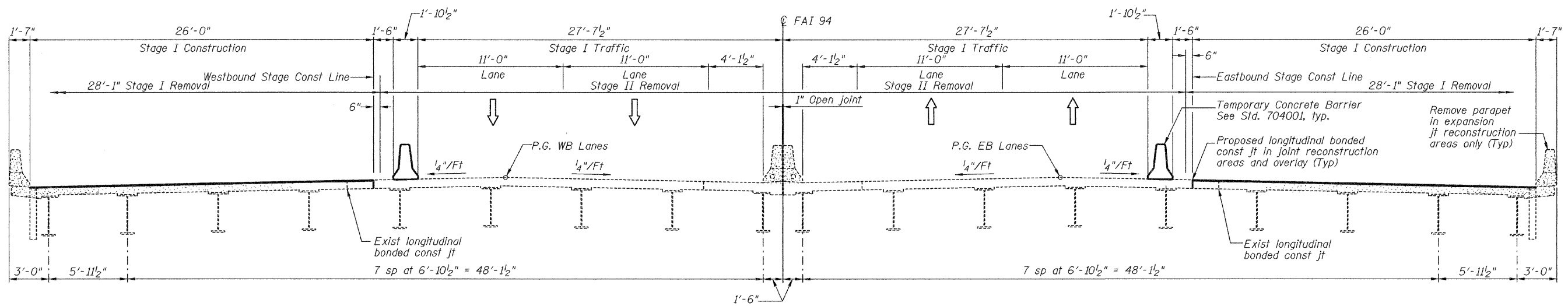
TYPICAL SECTION THRU BRIDGE  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163

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SHEET NO. 3 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

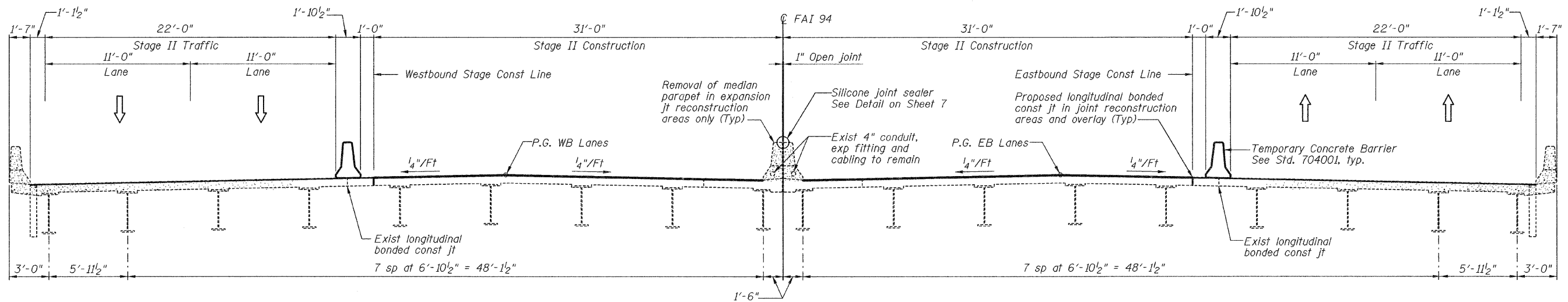
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DEPARTMENT OF TRANSPORTATION



**WESTBOUND STRUCTURE**

**STAGE I**  
(Looking South)

**EASTBOUND STRUCTURE**



**STAGE II**  
(Looking South)

**NOTES**

For joint removal limits see Sheets 5 thru 8

The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable.

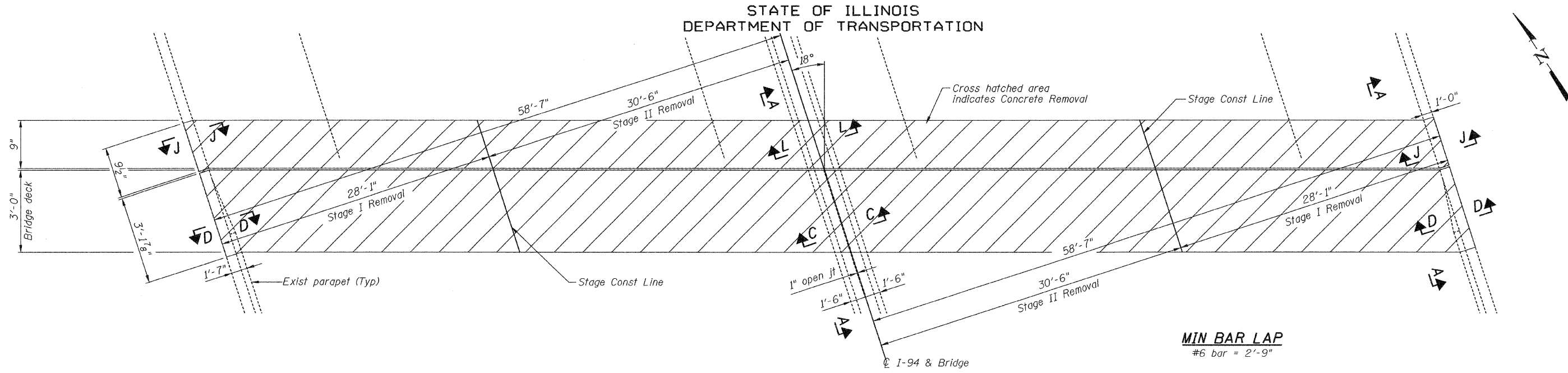
**CONSTRUCTION STAGING DETAILS**  
**BISHOP FORD EXPRESSWAY (I-94)**  
**LITTLE CALUMET RIVER**  
**COOK COUNTY**  
**STATION**  
**STRUCTURE NUMBER 016-0163**

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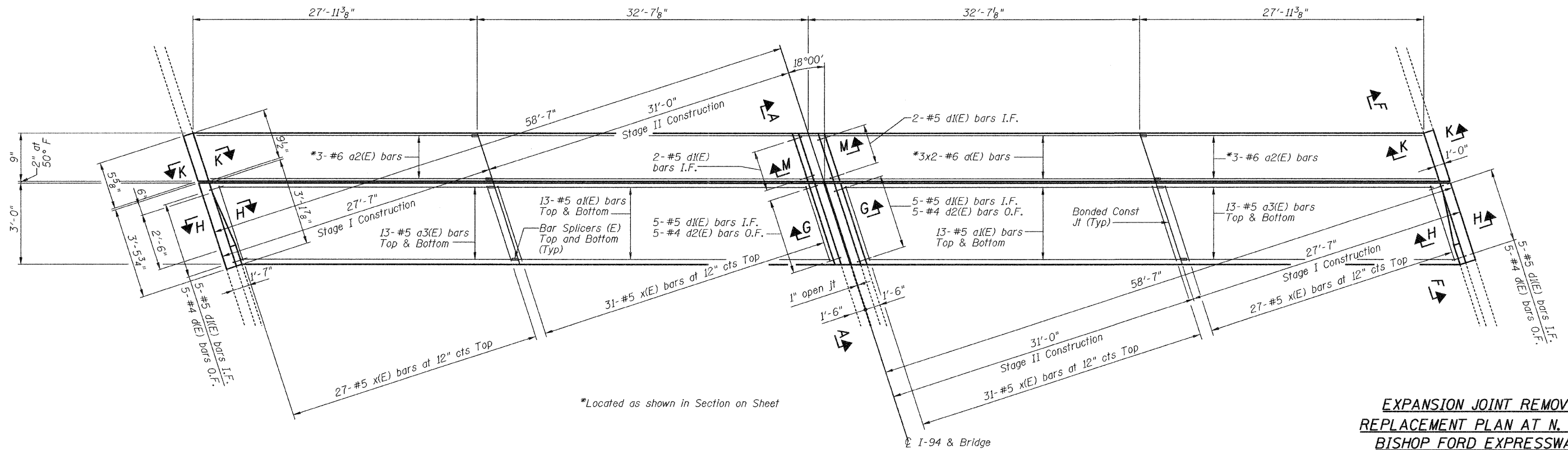
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SHEET NO. 4	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	90
OF 17 SHEETS	BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21		
FED. ROAD DIST. NO. -		ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**REMOVAL PLAN AT N. ABUTMENT**



**PROPOSED PLAN AT N. ABUTMENT**

**EXPANSION JOINT REMOVAL AND  
REPLACEMENT PLAN AT N. ABUTMENT  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163**

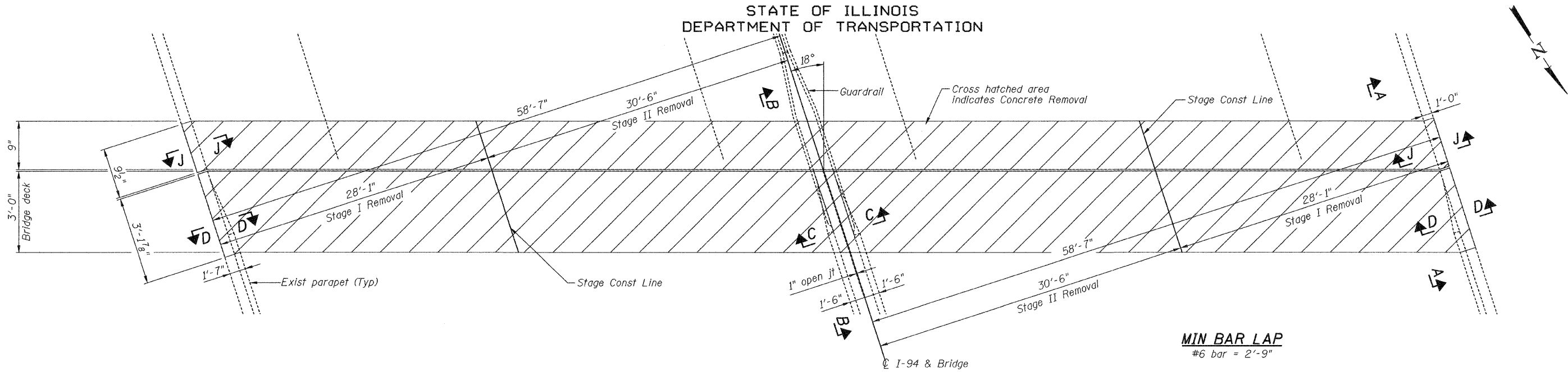
- NOTES**
- All reinforcement bars are to be evenly spaced unless otherwise noted.
  - Reinforcement bars designated (E) shall be epoxy coated.
  - O.F. denotes Outside Face, I.F. denotes Inside Face.
  - For Bill of Material and bar details see Sheet 8
  - For additional abutment backwall demolition and reconstruction see Sheet 7

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DRAWN -	
CHECKED -	

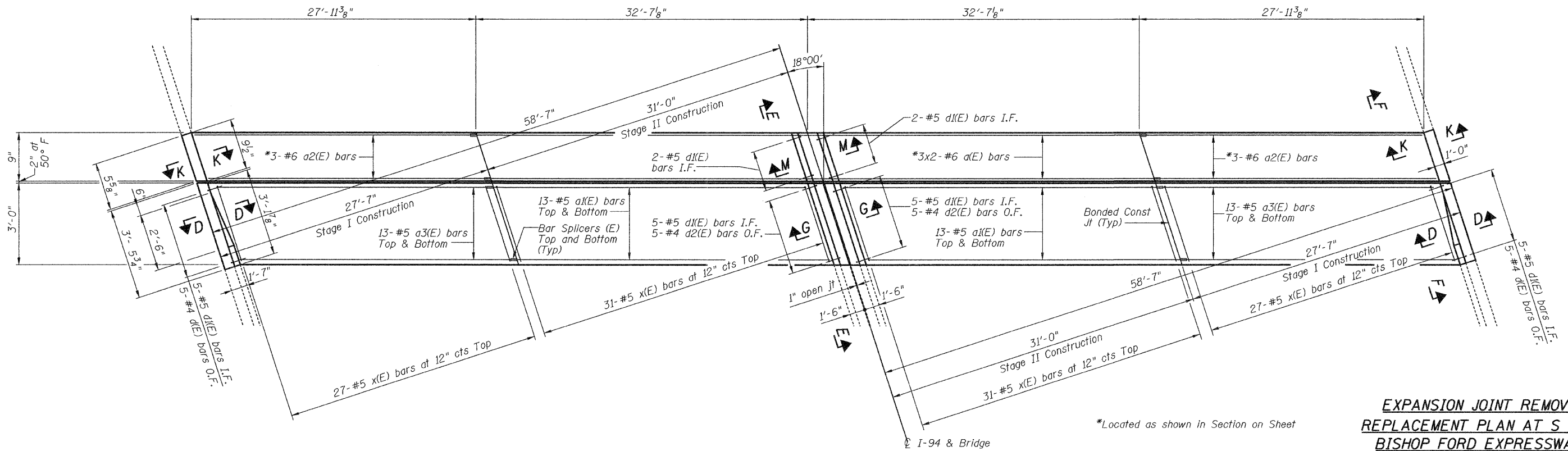
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SHEET NO. 5 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	91
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



**REMOVAL PLAN AT S. ABUTMENT**



**PROPOSED PLAN AT S. ABUTMENT**

**EXPANSION JOINT REMOVAL AND  
REPLACEMENT PLAN AT S ABUTMENT  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163**

- NOTES**
- All reinforcement bars are to be evenly spaced unless otherwise noted.
  - Reinforcement bars designated (E) shall be epoxy coated.
  - O.F. denotes Outside Face, I.F. denotes Inside Face.
  - For Bill of Material and bar details see Sheet 8
  - For additional abutment backwall demolition and reconstruction see Sheet 7

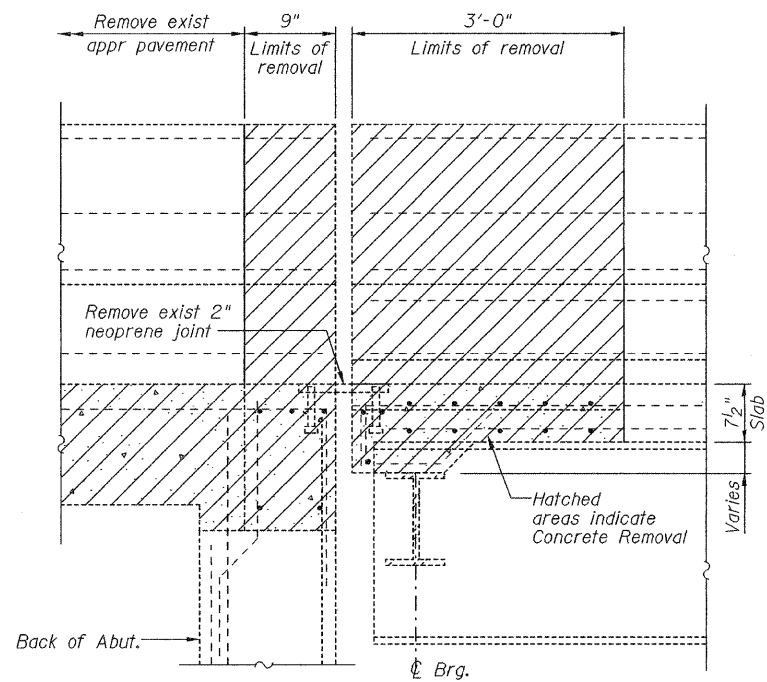
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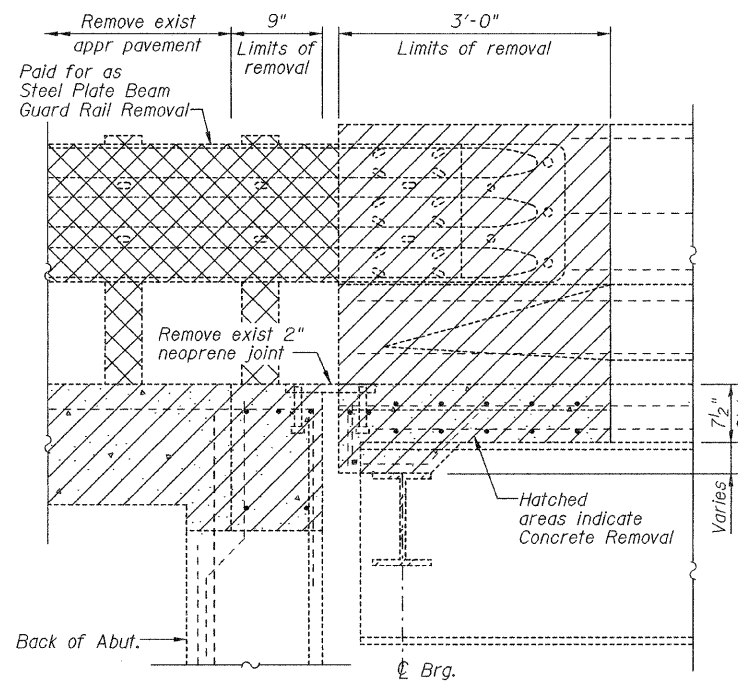
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BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. -			ILLINOIS FED. AID PROJECT		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

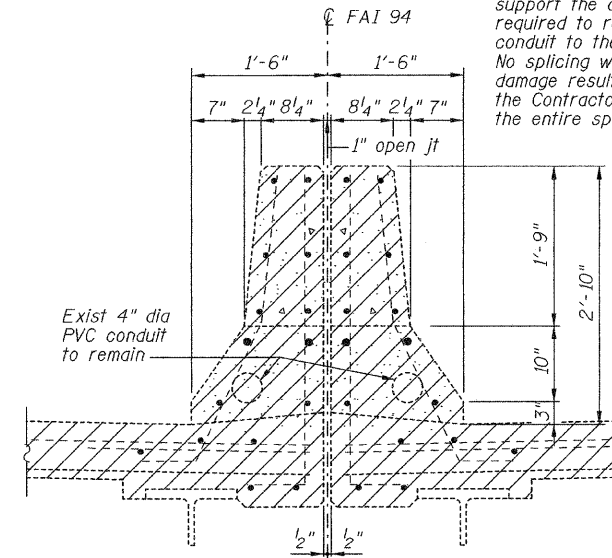
The Contractor shall exercise extreme care with the existing conduits in sections of the parapet to be removed and to protect and support the conduit. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damage resulting from this work, instead the Contractor will be required to repair the entire span of any damaged cable.



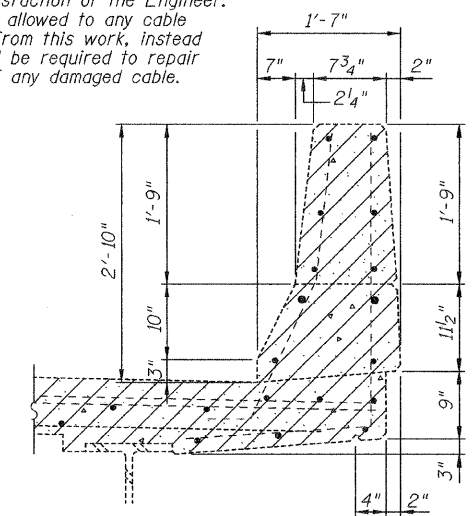
SECTION A-A



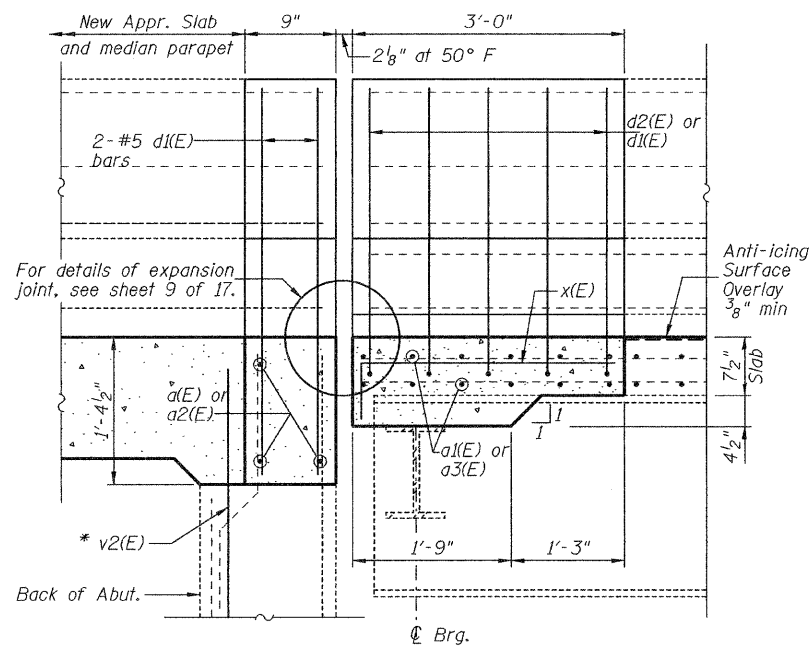
SECTION B-B



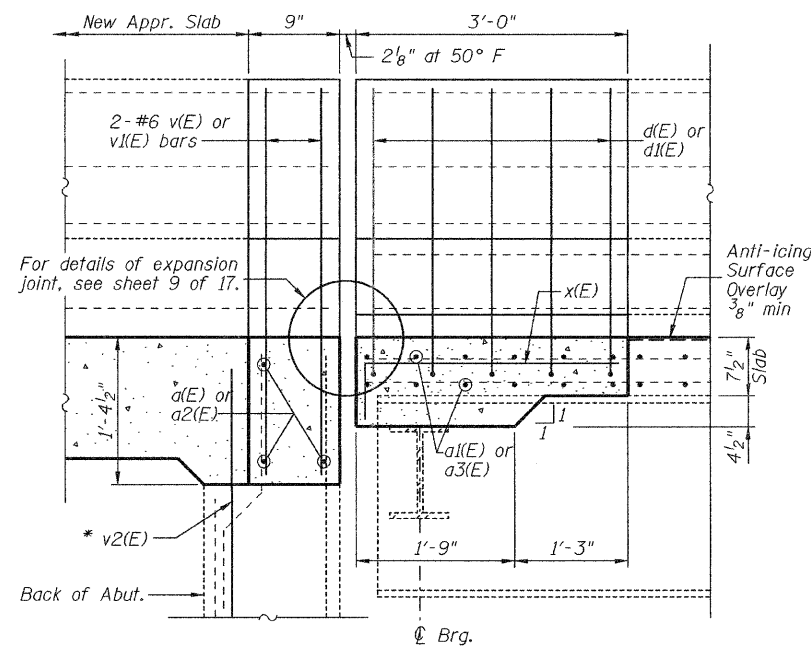
MEDIAN PARAPET  
SECTION C-C



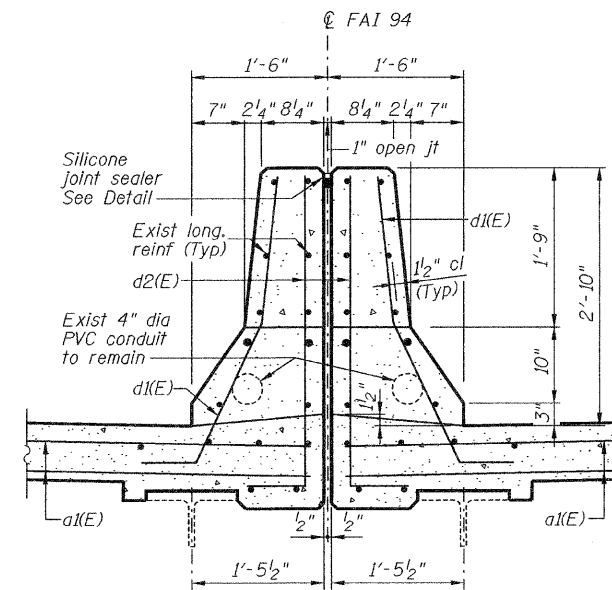
EB & WB PARAPET  
SECTION D-D



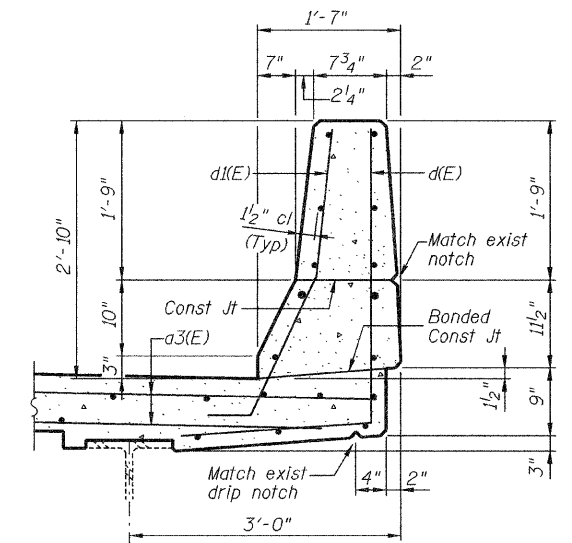
SECTION E-E



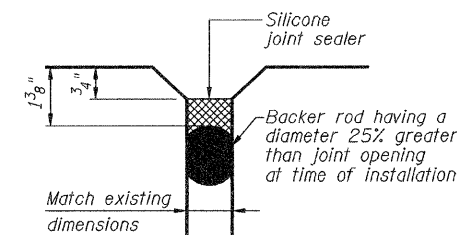
SECTION F-F



MEDIAN PARAPET  
SECTION G-G



EB & WB PARAPET  
SECTION H-H



SILICONE BRIDGE JOINT SEALER  
(Typical entire length of bridge)

\* See details on sheets 16 & 17 of 17.

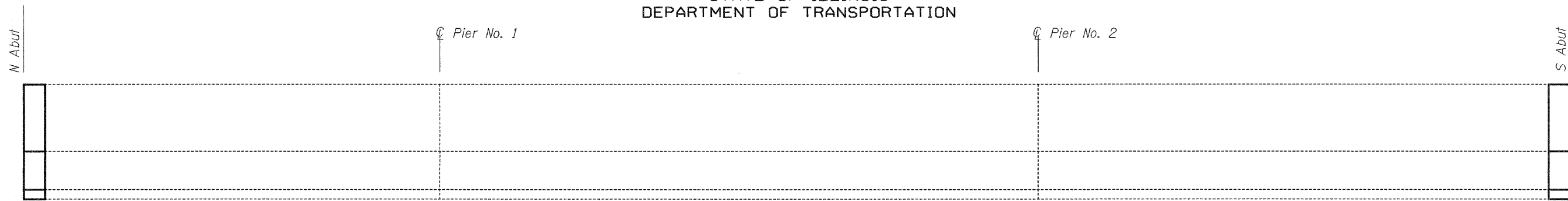
EXPANSION JOINT REMOVAL AND REPLACEMENT DETAILS 1  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163

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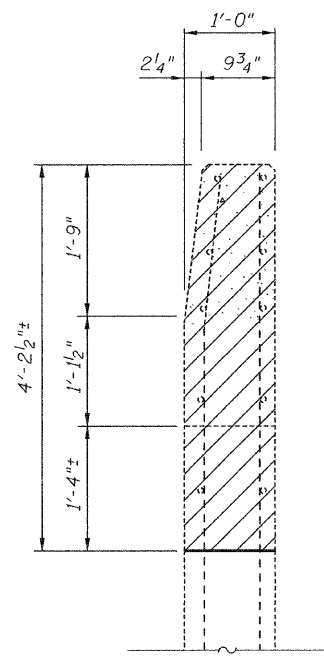
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SHEET NO. 7 OF 17 SHEETS	F.A.I. RTE. 94	SECTION (0405,ETC,42-7&070)RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 93
	BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

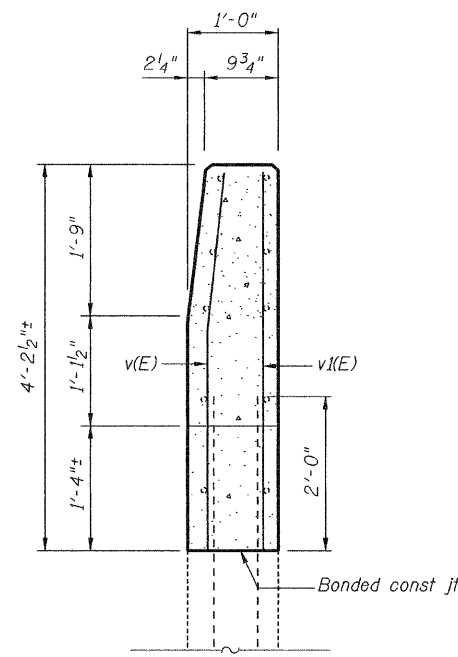
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



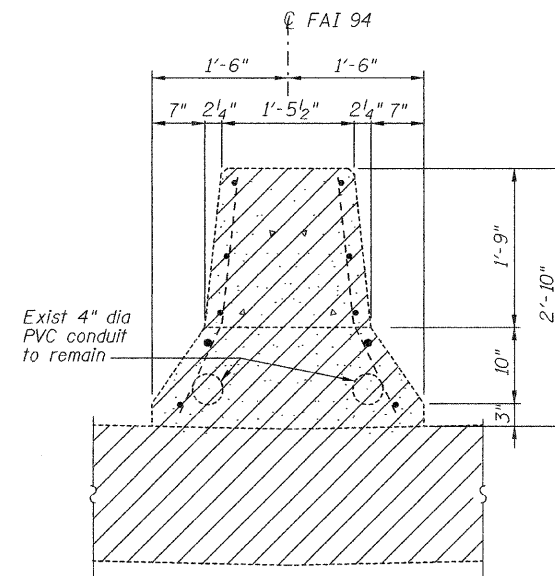
INSIDE ELEVATION OF EAST PARAPET



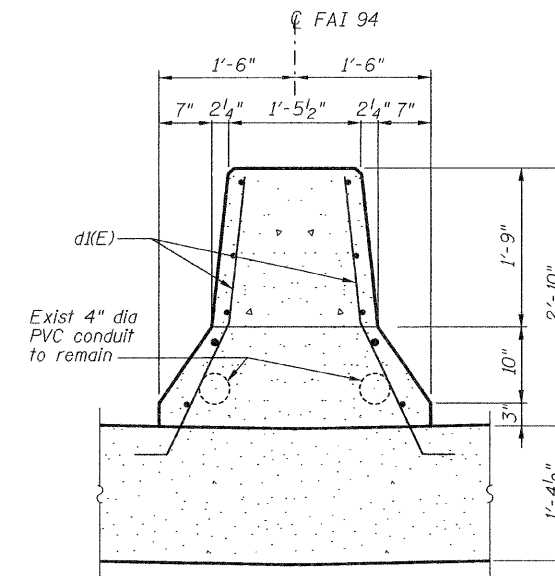
SECTION J-J



SECTION K-K



MEDIAN PARAPET  
SECTION L-L

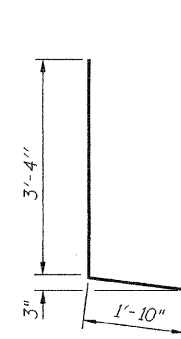


MEDIAN PARAPET  
SECTION M-M

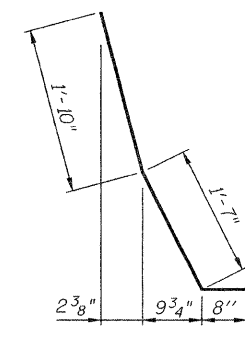
SUPERSTRUCTURE  
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	12	#6	32'-4"	—
a1(E)	52	#5	32'-4"	—
a2(E)	12	#6	27'-7"	—
a3(E)	52	#5	27'-7"	—
d(E)	20	#4	5'-2"	L
d1(E)	48	#5	4'-1"	—
d2(E)	20	#4	4'-0"	—
v(E)	8	#6	4'-0"	I
v1(E)	8	#6	4'-1"	—
x(E)	232	#5	3'-3"	—
Concrete Removal			Cu Yd	40.0
Concrete Superstructure			Cu Yd	40.0
Protective Coat			Sq Yd	110
Reinforcement Bars, Epoxy Coated			Pound	6390
Silicone Joint Sealer, 2"			Foot	275

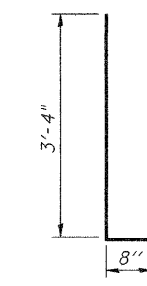
Reinforcement bars designated (E) shall be epoxy coated.



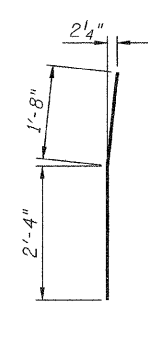
BAR d(E)



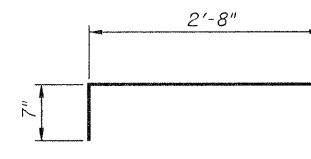
BAR d1(E)



BARS d2(E)



BAR v(E)



BAR x(E)

EXPANSION JOINT REMOVAL AND  
REPLACEMENT DETAILS 2  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163

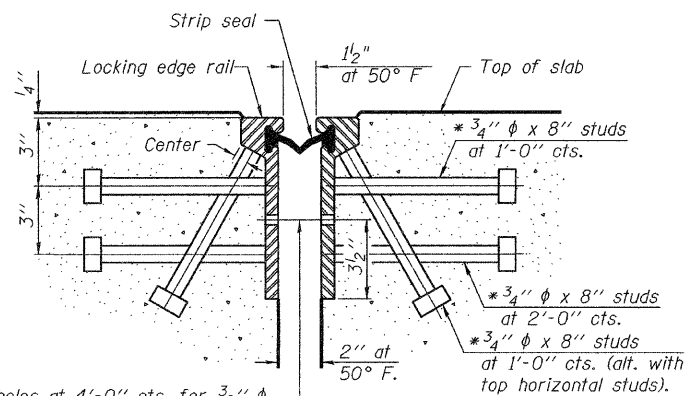
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SHEET NO. 8 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	94
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT			

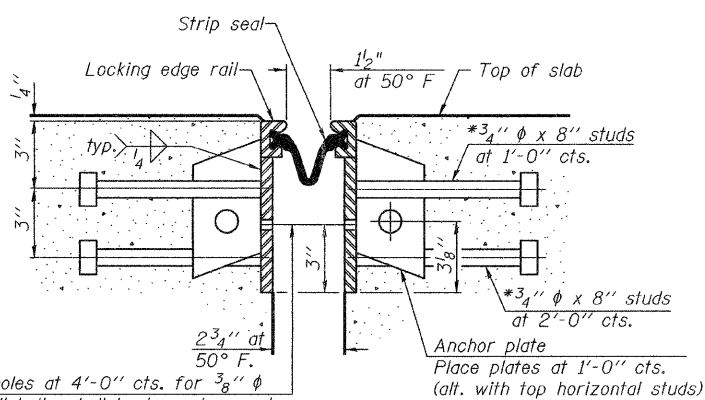
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

\* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU  
ROLLED RAIL JOINT



7/16"  $\phi$  holes at 4'-0" cts. for 3/8"  $\phi$  bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

SECTION THRU  
WELDED RAIL JOINT

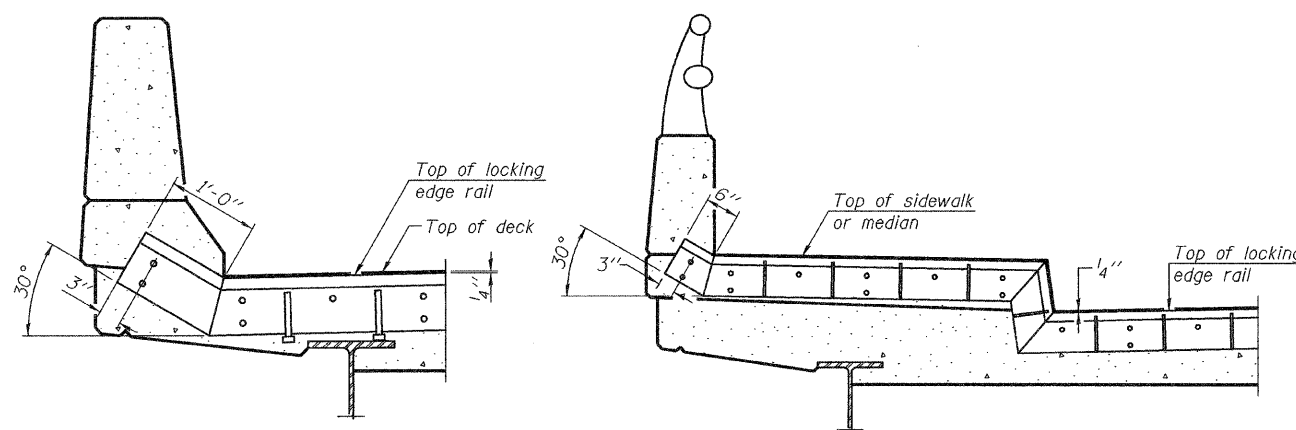
Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The height and thickness of the Locking Edge Rails shown are minimum dimensions. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities and stage construction joints.

The manufacturer's recommended installation methods shall be followed. The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

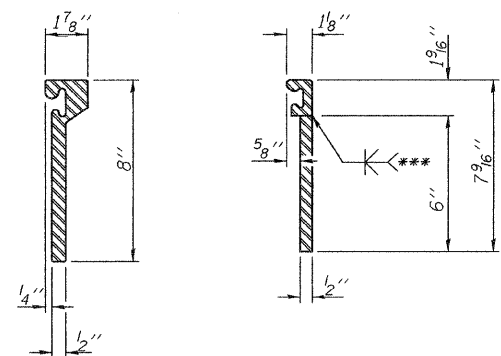
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.



AT PARAPET

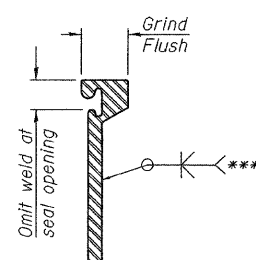
AT SIDEWALK OR MEDIAN

Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



ROLLED  
EXTRUDED RAIL

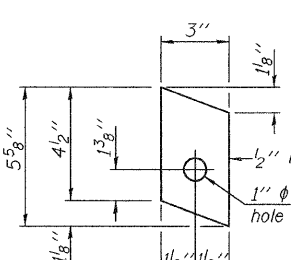
WELDED RAIL



\*\*\*Back gouge not required if complete joint penetration is verified by mock-up.

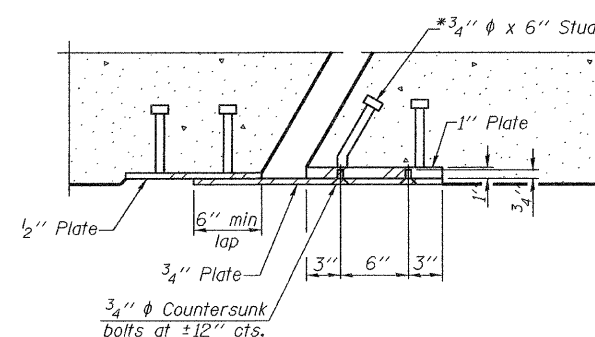
LOCKING EDGE  
RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.



ANCHOR PLATE  
(for welded rail)

TYPICAL END TREATMENTS

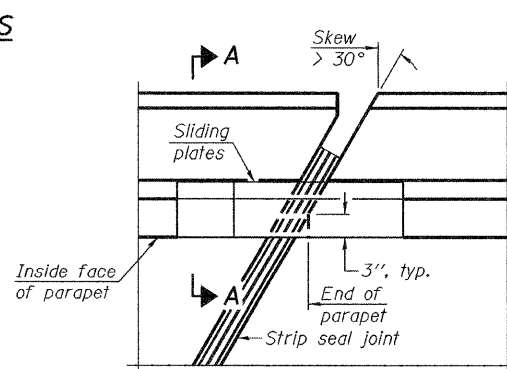


SECTION B-B

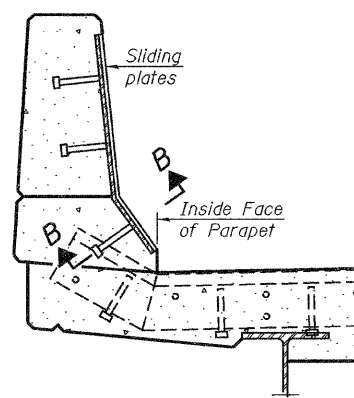
BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	242.0

PREFORMED JOINT STRIP SEAL  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163



PLAN



SECTION A-A

POINT BLOCK DETAILS  
(for skews > 30°)

DESIGNED -
CHECKED -
DRAWN -
CHECKED -

EJ-SSJ

HOMER L.  
CHASTAIN  
& ASSOCIATES, LLP  
CONSULTING ENGINEERS

DECATUR (217) 422-8544  
CHICAGO (773) 714-0050  
ROCKFORD (815) 489-0050

10-1-08

184-001397

SHEET NO.	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
9	94	(0405,ETC,42-7&0707)RS-11	COOK	133	95
OF 17 SHEETS			BISHOP FORD EXPRESSWAY (I-94) CONTRACT NO. 60D21		
			FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

The diameter of this part is equal or larger than the diameter of bar spliced.

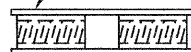
The diameter of this part is the same as the diameter of the bar spliced.

ROLLED THREAD DOWEL BAR



\*\* ONE PIECE

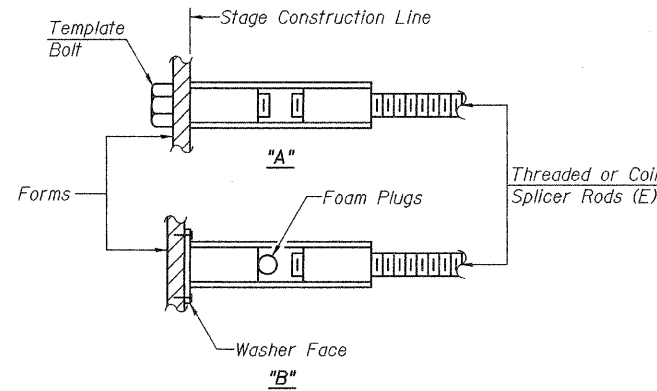
Wire Connector



WELDED SECTIONS

**BAR SPLICER ASSEMBLY ALTERNATIVES**

\*\*Heavy Hex Nuts conforming to ASTM A 563, Grade C, D or DH may be used.



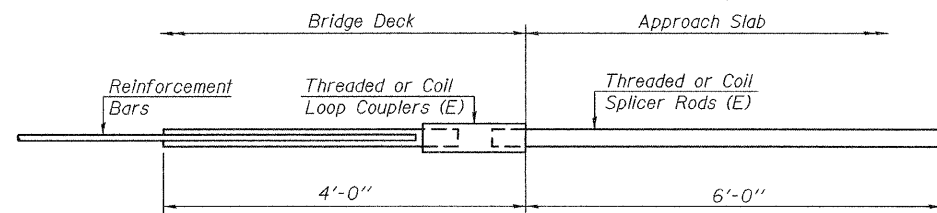
**INSTALLATION AND SETTING METHODS**

"A" : Set bar splicer assembly by means of a template bolt.  
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.  
(E) : Indicates epoxy coating.

**NOTES**  
Bar splicer assemblies shall be of an approved type and shall develop in tension at least 125 percent of the yield strength of the lapped reinforcement bars.  
Splicer rods shall be of minimum 60 ksi yield strength, threaded or coiled full length.  
All reinforcement bars shall be lapped and tied to the splicer rods or dowel bars.  
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars.  
Other systems of similar design may be submitted to the Engineer for approval. Approval shall be based on certified test results from an approved testing laboratory that the proposed bar splicer assembly satisfies the following requirements:

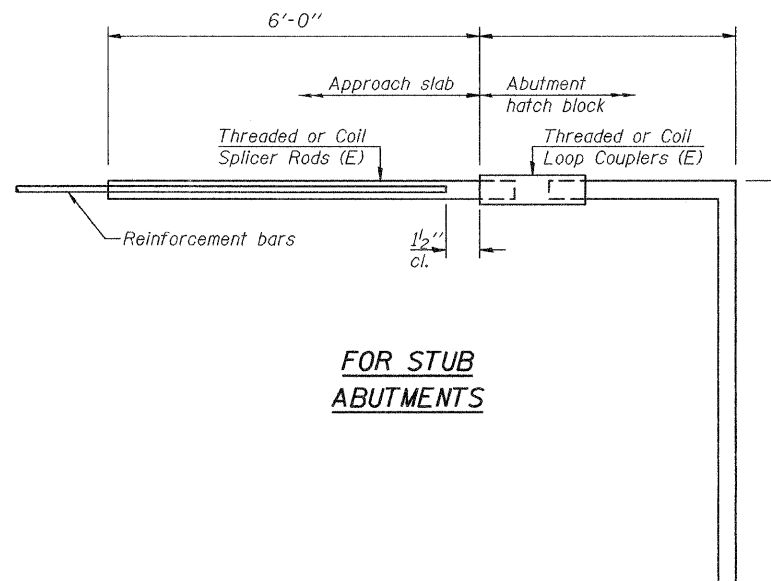
- ① Minimum Capacity (Tension in kips) =  $1.25 \times f_y \times A_t$
  - ② Minimum \*Pull-out Strength (Tension in kips) =  $0.66 \times f_y \times A_t$
- Where  $f_y$  = Yield strength of lapped reinforcement bars in ksi.  
 $A_t$  = Tensile stress area of lapped reinforcement bars.  
\* = 28 day concrete

Bar Size to be Spliced	Splicer Rod or Dowel Bar Length	Strength Requirements	
		Min. Capacity kips - tension	Min. Pull-Out Strength kips - tension
#4	1'-8"	14.7	7.9
#5	2'-2"	23.0	12.3
#6	2'-7"	33.1	17.4
#7	3'-5"	45.1	23.8
#8	4'-6"	58.9	31.3
#9	5'-9"	75.0	39.6
#10	7'-3"	95.0	50.3
#11	9'-0"	117.4	61.8



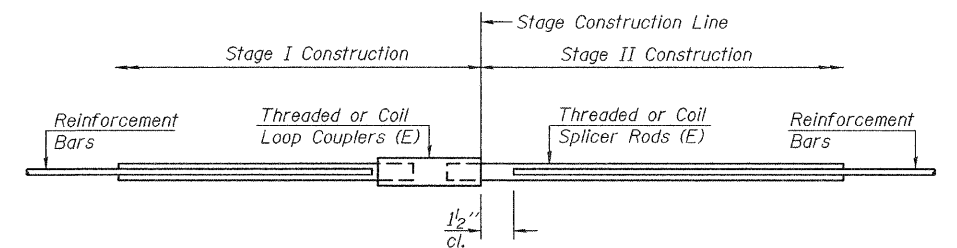
**FOR INTEGRAL OR SEMI-INTEGRAL ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



**FOR STUB ABUTMENTS**

Bar Splicer for #5 bar
Min. Capacity = 23.0 kips - tension
Min. Pull-out Strength = 12.3 kips - tension
No. Required =



**STANDARD**

Bar Size	No. Assemblies Required	Location
#4	100	Approach Slab
#5	184	Approach Slab
#5	160	Approach Footing
#5	52	Deck
#6	12	Abutment

**BAR SPLICER ASSEMBLY DETAILS  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163**

DESIGNED -
CHECKED -
DRAWN -
CHECKED -

**HOMER L. CHASTAIN & ASSOCIATES, LLP**  
CONSULTING ENGINEERS  
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184-001397

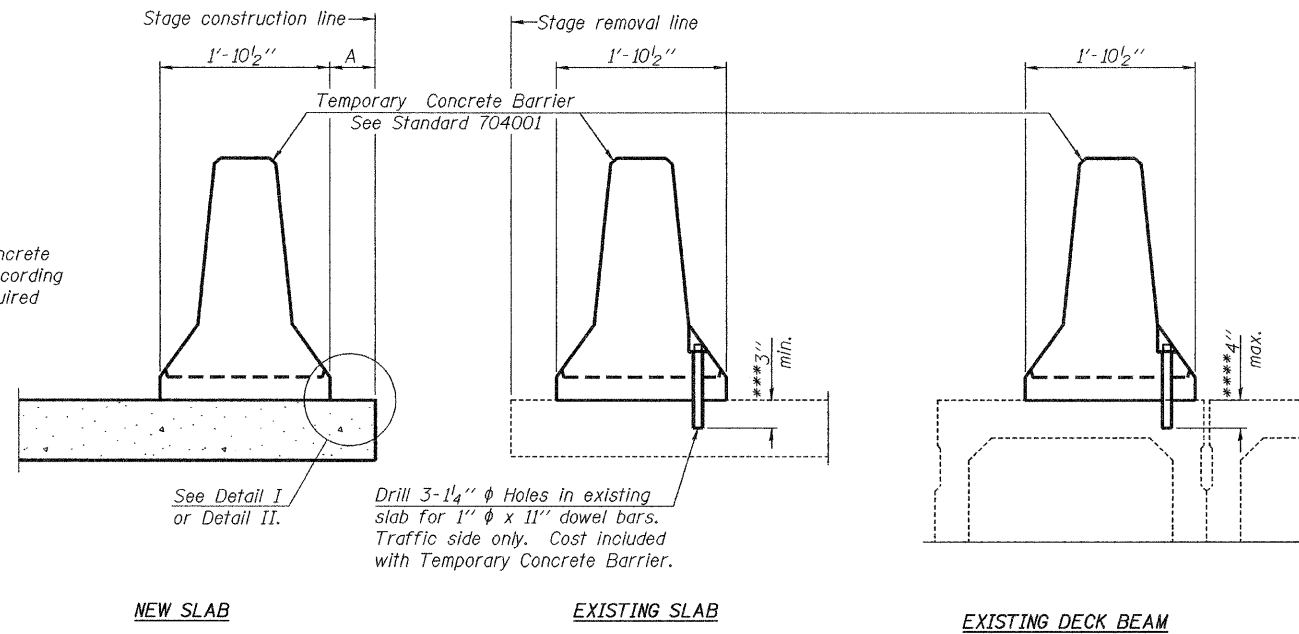
BSD-1 10-1-08

SHEET NO. 10 OF 17 SHEETS	F.A.I. RTE. 94	SECTION (0405,ETC,42-7&0707)RS-11	COUNTY COOK	TOTAL SHEETS 133	SHEET NO. 96
	BISHOP FORD EXPRESSWAY (I-94) CONTRACT NO. 60D21			FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT	



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

When "A" is 3'-6" or less, the temporary concrete barrier shall be anchored to the new slab according to Detail I or Detail II. No anchorage is required when "A" is greater than 3'-6".



Drill 3-1/4"  $\phi$  Holes in existing slab for 1"  $\phi$  x 11" dowel bars. Traffic side only. Cost included with Temporary Concrete Barrier.

**NOTES**

**Detail I - With Bar Splicer or Couplers:**  
Connect one (1) 1"x7"x10" steel  $\square$  to the top layer of couplers with 2-5/8"  $\phi$  bolts screwed to coupler at approximate  $\phi$  of each barrier panel.

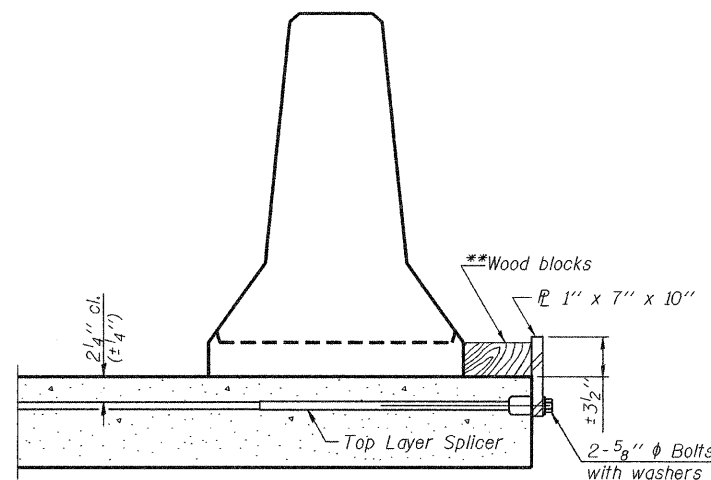
**Detail II - With Extended Reinforcement Bars:**  
Connect one (1) 1"x7"x10" steel  $\square$  to the concrete slab or concrete wearing surface with 2-5/8"  $\phi$  Expansion Anchors or cast in place inserts spaced between the top layer of reinforcement at approximate  $\phi$  of each barrier panel.

Cost of anchorage is included with Temporary Concrete Barrier. The 1" x 7" x 10" plate shall not be removed until stage II construction forms and all reinforcement bars are in place and the concrete is ready to be placed.

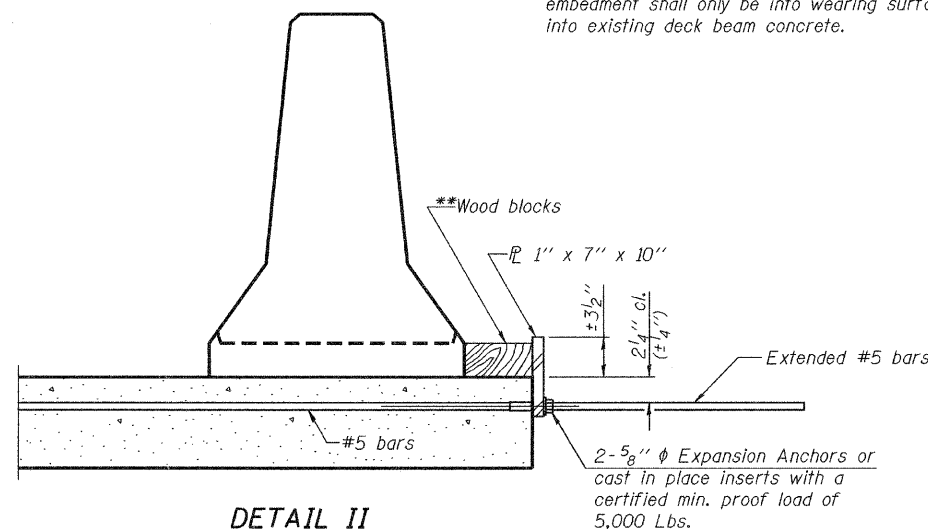
**SECTIONS THRU SLAB OR DECK BEAM**

\*\*\* Dimension shown is minimum required embedment into concrete. If hot-mix asphalt wearing surface is present, minimum embedment shall be in addition to wearing surface depth.

\*\*\*\* If existing deck beam is to remain in place after stage construction, embedment shall only be into wearing surface and not into existing deck beam concrete.

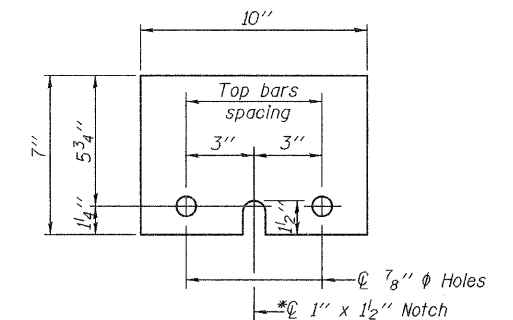


**DETAIL I**



**DETAIL II**

\*\*Wood blocks may be omitted when required to provide minimum stage traffic lane width. When the wood blocks are omitted, the concrete barrier shall be in direct contact with the steel retainer plate.



**STEEL RETAINER  $\square$  1" x 7" x 10"**

\* Required only with Detail II

**TEMPORARY CONCRETE BARRIER**  
**BISHOP FORD EXPRESSWAY (I-94)**  
**LITTLE CALUMET RIVER**  
**COOK COUNTY**  
**STATION**  
**STRUCTURE NUMBER 016-0163**

DESIGNED -	
CHECKED -	
DRAWN -	
CHECKED -	

R-27

**HOMER L. CHASTAIN & ASSOCIATES, LLP**  
CONSULTING ENGINEERS

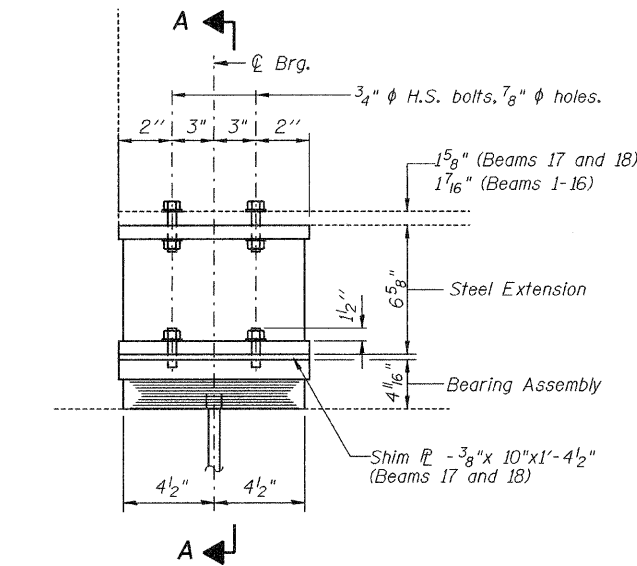
DECATUR (217) 422-8544  
CHICAGO (773) 714-0050  
ROCKFORD (815) 489-0050

10-1-08

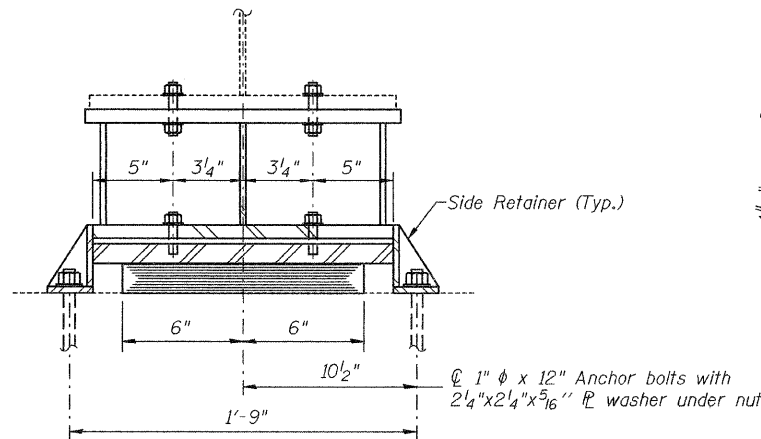
184-001397

SHEET NO. 11 OF 17 SHEETS	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	97
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. -		ILLINOIS FED. AID PROJECT			

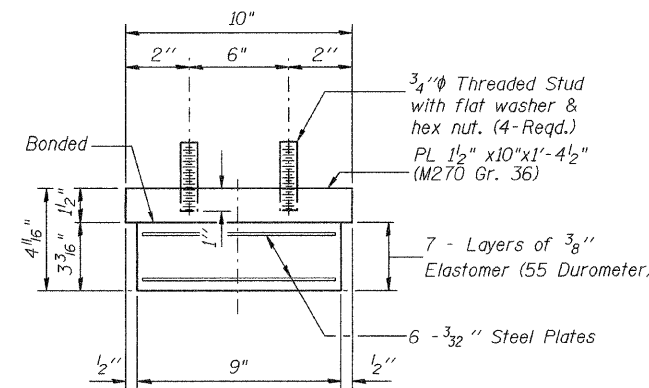
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION



ELEVATION AT SOUTH ABUTMENT

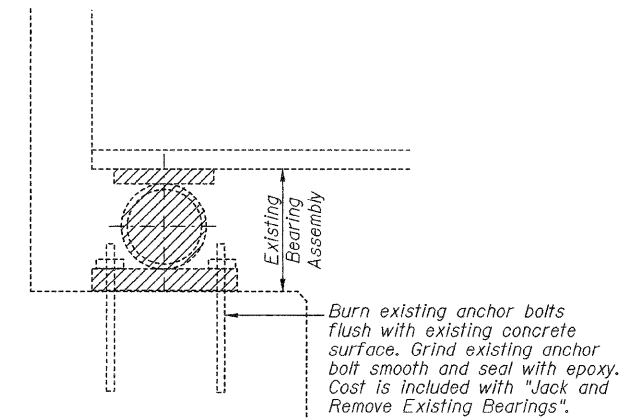


SECTION A-A



BEARING ASSEMBLY

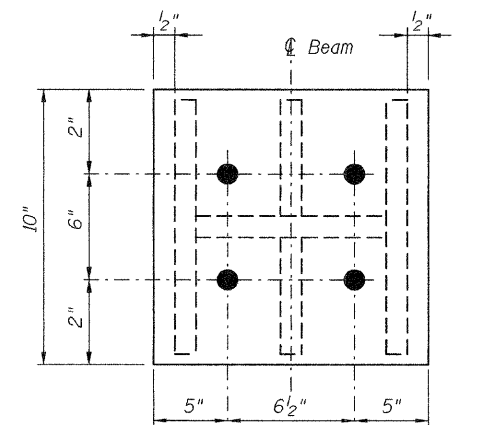
Note: Shim plates shall not be placed under Bearing Assembly.



EXISTING BEARING REMOVAL DETAIL

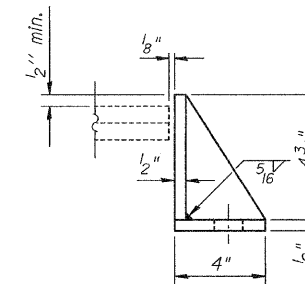
(At Abutments)

TYPE I ELASTOMERIC EXPANSION BEARING



PLAN STEEL EXTENSION

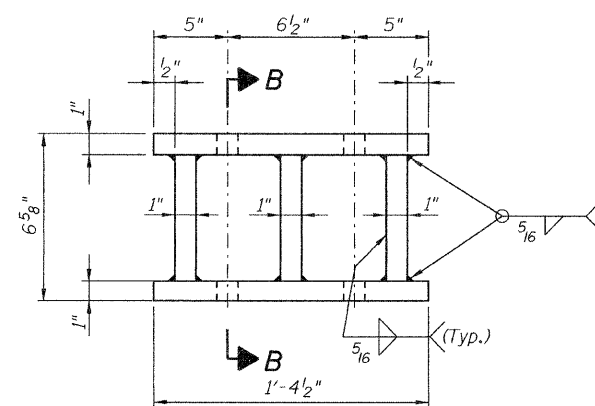
Note: Prior to ordering any material, the Contractor shall verify in the field all bearing height and shim thickness dimensions.



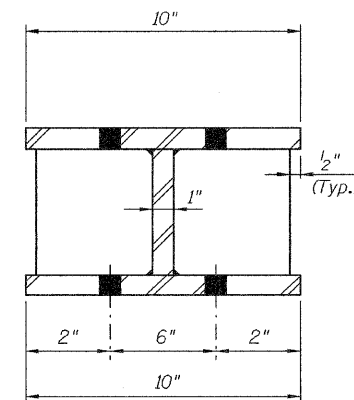
SIDE RETAINER

(36 Req'd)

Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.



ELEVATION STEEL EXTENSION



SECTION B-B

GIRDER REACTIONS

R $\varnothing$	(k)	31.3
R $\perp$	(k)	41.1
IMP	(k)	10.0
R (TOTAL)	(k)	82.4

BILL OF MATERIAL

Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	18
Anchor Bolts, 1"	Each	36
Furnishing and Erecting Structural Steel	lbs	2940

ELASTOMERIC BEARING  
DETAILS - TYPE I  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163

DESIGNED -
CHECKED -
DRAWN -
CHECKED -

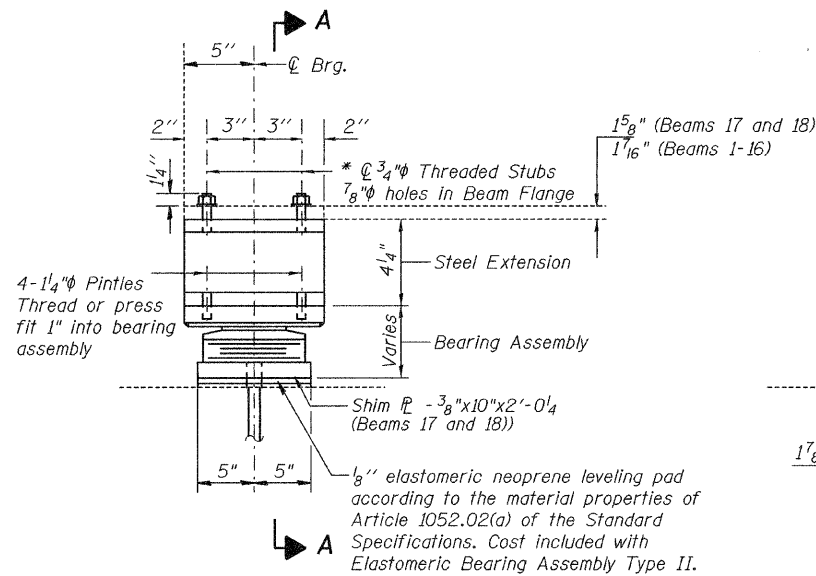
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 CONSULTING ENGINEERS  
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 CHICAGO (773) 714-0050  
 ROCKFORD (815) 489-0050  
 184-001397

FABRICATED STEEL EXTENSION

(18 required)

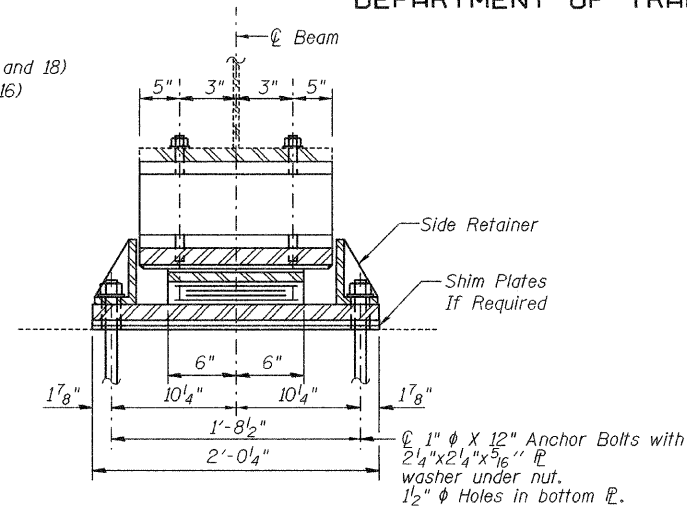
SHEET NO. 12 OF 17 SHEETS	F.A.I.-RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&0707)RS-11	COOK	133	98
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT					

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

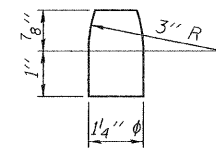


**ELEVATION AT ABUT.**

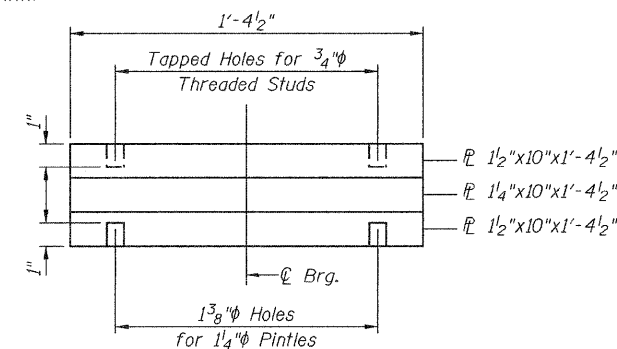
\*  $\phi$  3/4" Threaded Studs, shall be placed in the field.



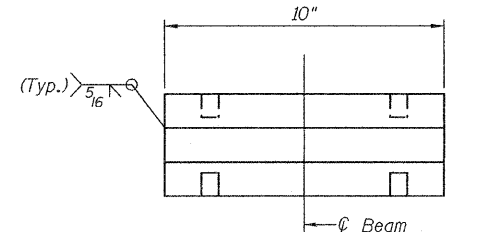
**SECTION A-A**



**PINTLE**

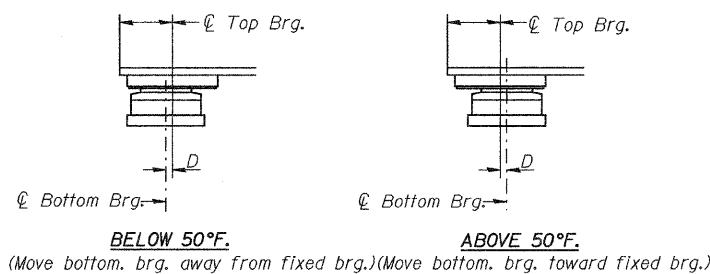


**ELEVATION STEEL EXTENSION**



**END VIEW STEEL EXTENSION**

Note:  
Prior to ordering any material, the Contractor shall verify in the field all bearing heights and shim thickness dimensions.



**SETTING ANCHOR BOLTS AT EXP. BRG.**

D=1/8" per each 100' of expansion for every 15° temp. change from the normal temp. of 50°F.

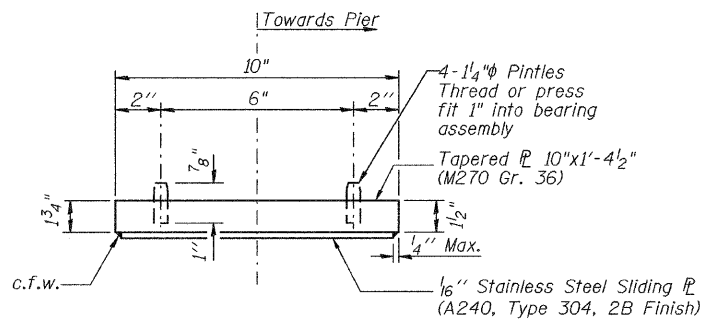
Notes:  
Anchor bolts shall be ASTM F1554 all-thread (or an Engineer-approved alternate material) of the grade(s) and diameter(s) specified. ASTM A307 Grade C anchor bolts may be used in lieu of ASTM F1554 Grade 36 (Fy=36ksi). The corresponding specified grade of AASHTO M314 anchor bolts may be used in lieu of ASTM F1554.  
Anchor bolts at fixed bearings may be either cast in place or installed in holes drilled after the supported member is in place.  
Anchor bolts for Type II bearings shall be placed in holes drilled in the concrete through holes in the bottom bearing plate after members are in place. Side retainers shall be placed after bolts are installed.  
Drilled and set anchor bolts shall be installed according to Article 521.06 of the Standard Specifications.  
Side retainers and other steel members required for the bearing assembly shall be included in the cost of Elastomeric Bearing Assembly, Type II.  
The 1/8" PTFE sheet shall be bonded directly to the top steel plate with a two-component, medium viscosity epoxy resin, conforming to the requirements of the Federal Specification MMM-A-134, Type I. The bond agent shall be applied on the full area of the contact surfaces.  
Bonding of 1/8" PTFE sheet during vulcanizing process will be permitted provided the process and method of adjusting assembly height is approved by the Engineer.

**GIRDER REACTIONS**

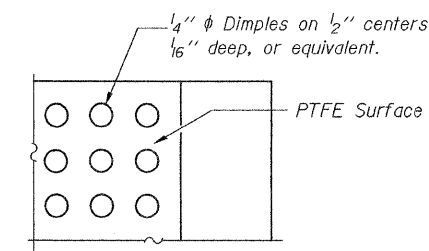
R $\phi$	(k)	31.3
R $\frac{1}{2}$	(k)	41.1
IMP	(k)	10.0
R (TOTAL)	(k)	82.4

**BILL OF MATERIAL**

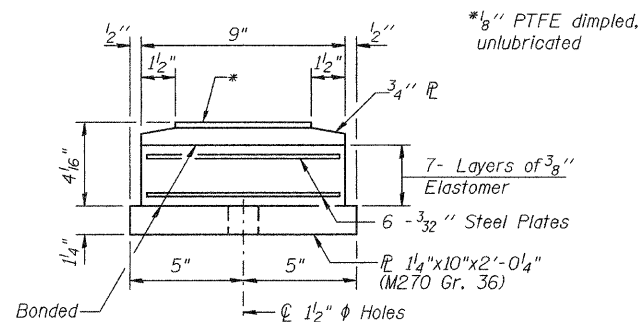
Item	Unit	Total
Elastomeric Bearing Assembly Type I	Each	18
Anchor Bolts, 1"	Each	36
Furnishing and Erecting Structural Steel	lbs	4020



**TOP BEARING ASSEMBLY**

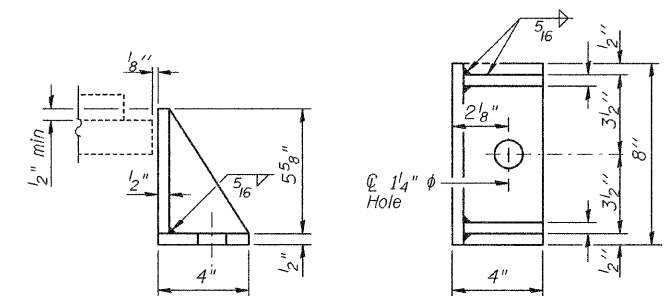


**PLAN-PTFE SURFACE**



**BOTTOM BEARING ASSEMBLY**

**TYPE II ELASTOMERIC EXPANSION BEARING**



**SIDE RETAINER**

(18 Req'd)  
Equivalent rolled angle with stiffeners will be allowed in lieu of welded plates.

**ELASTOMERIC BEARING  
DETAILS - TYPE II  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163**

DESIGNED -	
CHECKED -	
DRAWN -	
CHECKED -	

**HOMER L. CHASTAIN & ASSOCIATES, LLP**  
CONSULTING ENGINEERS  
DECATUR (217) 422-8544  
CHICAGO (773) 714-0050  
ROCKFORD (815) 489-0050  
184-001397

SHEET NO. 13 OF 17 SHEETS	F.A.I.-RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	94	(0405,ETC,42-7&070)RS-11	COOK	133	99
		BISHOP FORD EXPRESSWAY (I-94)		CONTRACT NO. 60D21	
		FED. ROAD DIST. NO. - ILLINOIS FED. AID PROJECT			

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

EAST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	580+19.52	57.58	611.83
A	580+09.52	57.58	612.11
B	579+99.52	57.58	612.39
End of North appr pavement	579+89.52	57.58	612.66

EAST MEDIAN CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	580+01.30	1.50	612.69
A	579+91.30	1.50	612.96
B	579+81.30	1.50	613.23
End of North appr pavement	579+71.30	1.50	613.50

PG EASTBOUND LANES

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	579+93.99	-21.00	613.30
A	579+83.99	-21.00	613.57
B	579+73.99	-21.00	613.83
End of North appr pavement	579+63.99	-21.00	614.10

PG WESTBOUND LANES

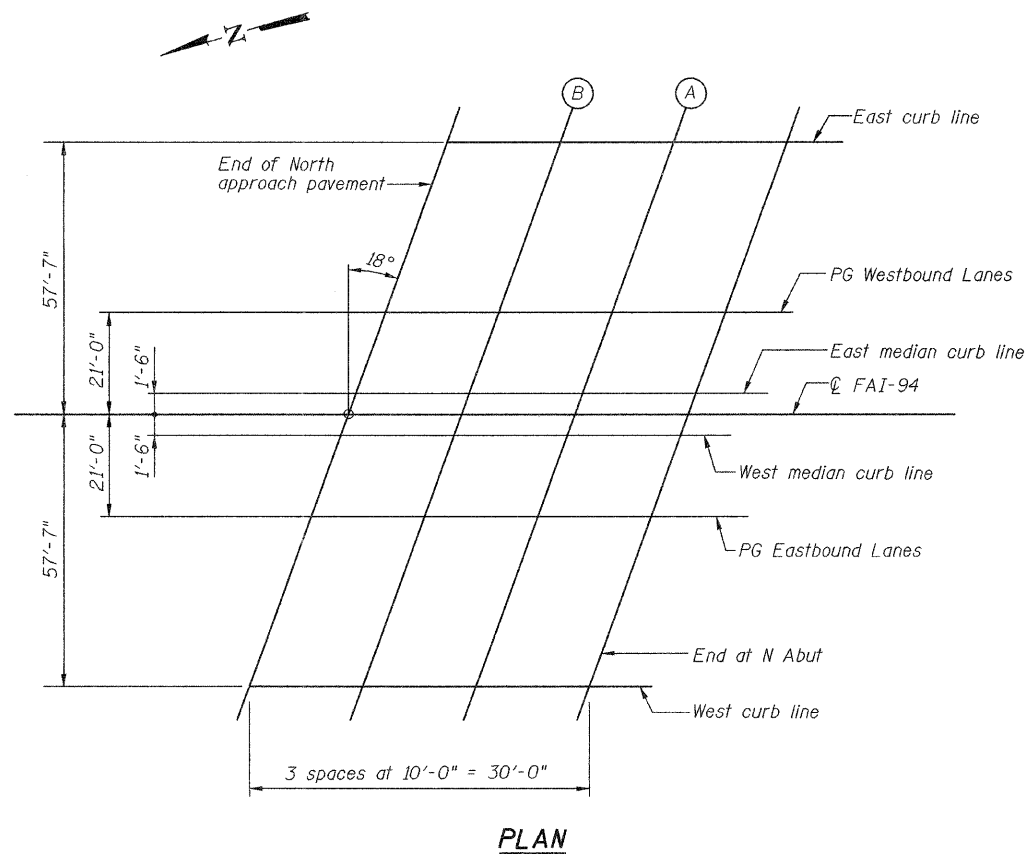
Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	580+07.63	21.00	612.92
A	579+97.63	21.00	613.20
B	579+87.63	21.00	613.47
End of North appr pavement	579+77.63	21.00	613.74

WEST MEDIAN CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	580+00.32	-1.50	612.72
A	579+90.32	-1.50	612.99
B	579+80.32	-1.50	613.26
End of North appr pavement	579+70.32	-1.50	613.53

WEST CURB LINE

Location	Station	Offset	Theoretical Grade Elevations
End at N Abut	579+82.10	-57.58	612.86
A	579+72.10	-57.58	613.12
B	579+62.10	-57.58	613.39
End of North appr pavement	579+52.10	-57.58	613.65



**TOP OF NORTH APPROACH  
SLAB ELEVATIONS  
BISHOP FORD EXPRESSWAY (I-94)  
LITTLE CALUMET RIVER  
COOK COUNTY  
STATION  
STRUCTURE NUMBER 016-0163**

DESIGNED -
CHECKED -
DRAWN -
CHECKED -

**HOMER L.  
CHASTAIN  
& ASSOCIATES, LLP**  
CONSULTING ENGINEERS  
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CHICAGO (773) 714-0050  
ROCKFORD (815) 489-0050  
184-001397

SHEET NO. 14	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
OF 17 SHEETS	94	(0405,ETC,42-7&0707)RS-11	COOK	133	100
BISHOP FORD EXPRESSWAY (I-94)			CONTRACT NO. 60D21		
FED. ROAD DIST. NO. -			ILLINOIS FED. AID PROJECT		