F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	1
FED. ROAD DIST. NO. 1	ILLINOIS	CONTRACT NO. 60A85		

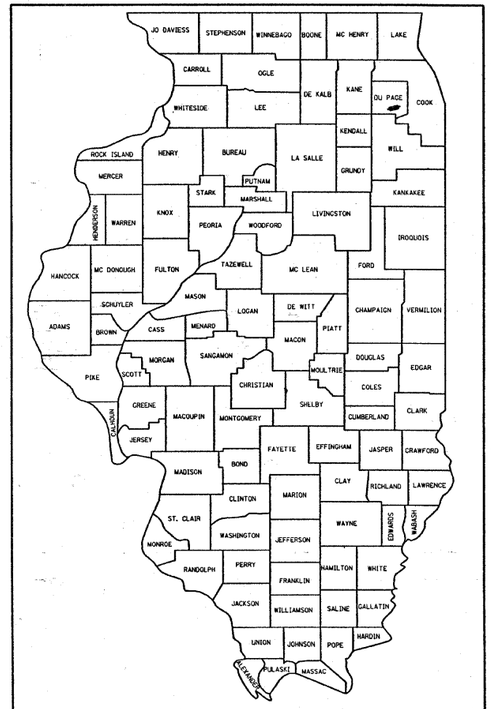
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS
F.A.P. 311 /US 34 (OGDEN AVE.)
I–355 TO WARWICK AVE.
RESURFACING (MAINTENANCE)
SECTION NO.: 9Y–RS–6
PROJECT: ESP-0311(037)
DuPAGE COUNTY
C–91–117–06

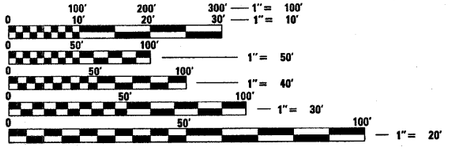
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN THE VILLAGE OF DOWNERS GROVE

D–91–117–06



LOCATION OF SECTION INDICATED THUS:– ▬



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1–800–892–0123
OR 811

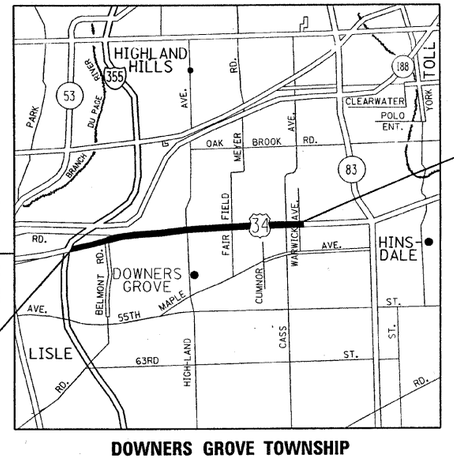
PROJECT ENGINEER: J. CHANG (847) 705–4432
PROJECT MANAGER: KEN ENG (847) 705–4247

CONTRACT NO. 60A85

R 10 E

T 36 N

T 35 N



PROJECT BEGINS
STA. 13+03

PROJECT ENDS
STA. 229+73

DOWNERS GROVE TOWNSHIP

TRAFFIC DATA

2005 ADT = 31200 – 34300
SPEED = 45 MPH

GROSS AND NET LENGTH OF IMPROVEMENT = 21670 LINEAL FEET = 4.10 MILES

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED January 12, 20 09
Diane M. O'Keefe /JD
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER
March 13, 20 09
Charles J. Ingersoll /JD
ENGINEER OF DESIGN AND ENVIRONMENT
March 13, 20 09
Christine M. Reed /JD
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

INDEX OF SHEETS:

STATE STANDARDS:

000001-*05* STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS

442201-*03* CLASS C AND D PATCHES

604001-*03* FRAMES AND LIDS, TYPE 1

604086-*02* FRAMES AND GRATES, TYPE 23

606001-*04* CONCRETE CURB AND COMBINATION CONRETE CURB AND GUTTER

701426-*03* LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS > 45 MPH

701602-*04* URBAN LANE CLOSURE, MULTILANE, 2W WITH BI-DIRECTIONAL LEFT TURN LANE

701606-*06* URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN

701701-*06* URBAN LANE CLOSURE, MULTILANE INTERSECTION

701801-*04* LANE CLOSURE MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE

701901-*01* TRAFFIC CONTROL DEVICES

886001-*01* DETECTOR LOOP INSTALLATION

886006-*01* TYPICAL LAYOUT FOR DETECTOR LOOP

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FORFIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED)

10 FEET (3METERS) TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEM OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE VILLAGE OF DOWNERS GROVE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR CORY JUCIUS AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO START OF WORK.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENT BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES (40 MM) WHERE THE SPEED LIMIT IS 45 MPH (80 KM/ H). WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H).

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE IN ACCORDANCE WITH THE DISTRICT ONE "TYPICAL APPLICATION RAISED REFLECTIVE PAVEMENT MARKERS" DETAIL.

THE RESIDENT ENGINEER SHOULD CONTACT MR. DON CHIARUGI, AREA TRAFFIC ENGINEER, AT (847) 741-9857 PRIOR TO PLACING ANY PAVEMENT MARKINGS.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	2
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A85

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DUPAGE	41	3
FED. ROAD DIST. NO. 1	ILLINOIS	HIGHWAY PROJECT		

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN 100% FED. TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: ROADWAY I000					
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	59	59					
40600300	AGGREGATE (PRIME COAT)	TON	292	292					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	44	44					
40600895	CONSTRUCTING TEST STRIP	EACH	1	1					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	999	999					
40600990	TEMPORARY RAMP	SQ YD	999	999					
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	14285	14285					
42001300	PROTECTIVE COAT	SQ YD	157	157					
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SQ YD	145757	145757					
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	805	805					
44201835	CLASS D PATCHES, TYPE I, 16 INCH	SQ YD	92	92					
44201839	CLASS D PATCHES, TYPE II, 16 INCH	SQ YD	1150	1150					
44201843	CLASS D PATCHES, TYPE III, 16 INCH	SQ YD	575	575					
44201845	CLASS D PATCHES, TYPE IV, 16 INCH	SQ YD	1150	1150					
□55039700	STORM SEWERS TO BE CLEANED	FOOT	2000	2000					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	10	10					
60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	20	20					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	3	3					
67100100	MOBILIZATION	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	30987	30987					
70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	2871	2871					

SUMMARY OF QUANTITIES

CODE NO	ITEM	UNIT	URBAN 100% FED. TOTAL QUANTITIES	CONSTRUCTION TYPE CODE: ROADWAY I000					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	55092	55092					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4108	4108					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1458	1458					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1445	1445					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	31080	31080					
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	2871	2871					
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	55092	55092					
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4108	4108					
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1458	1458					
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1445	1445					
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2164	2164					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1840	1840					
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	2584	2584					
X0322256	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4					
X4067107	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	5740	5740					
XX001306	SIDEWALK REMOVAL AND REPLACEMENT	SQ FT	8200	8200					
□Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	30	30					
⊙Z0076600	TRAINEES	HOUR	1500	1500					

⊙ Y080

* SPECIALTY ITEMS

□ NON-PARTICIPATING ITEMS

REVISIONS	
NAME	DATE
Rev.	

ILLINOIS DEPARTMENT OF TRANSPORTATION

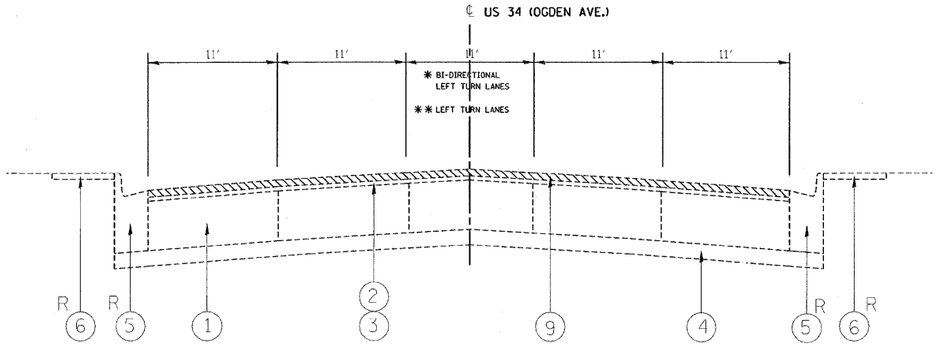
SUMMARY OF QUANTITIES

FAP 311/ US 34 (OGDEN AVE.)

I-355 TO WARWICK AVE.

PLOT DATE: 1/9/2009

1/9/2009

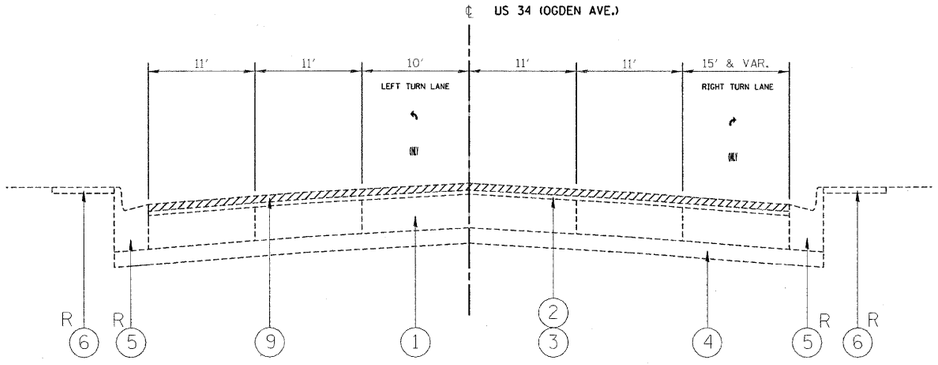


EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 13+03 TO STA. 42+64

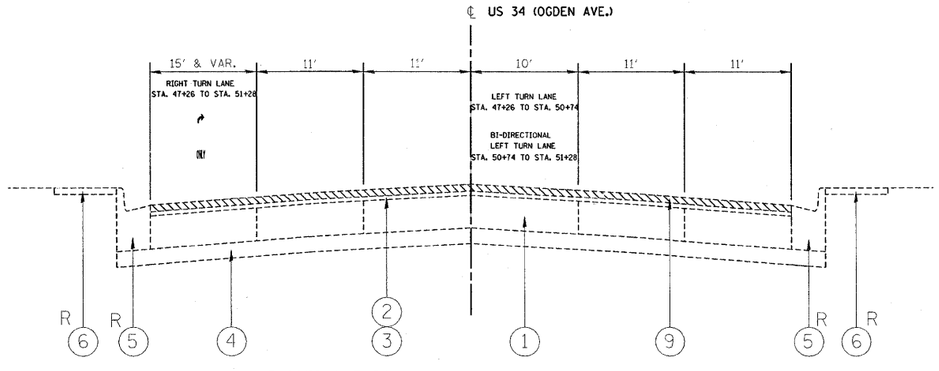
LEGEND:

* STA. 14+50 TO STA. 18+37
STA. 21+16 TO STA. 23+40
STA. 29+54 TO STA. 41+56

** STA. 13+03 TO STA. 13+35
STA. 19+07 TO STA. 21+16
STA. 23+40 TO STA. 25+88
STA. 27+00 TO STA. 29+54



EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 42+64 TO STA. 45+89

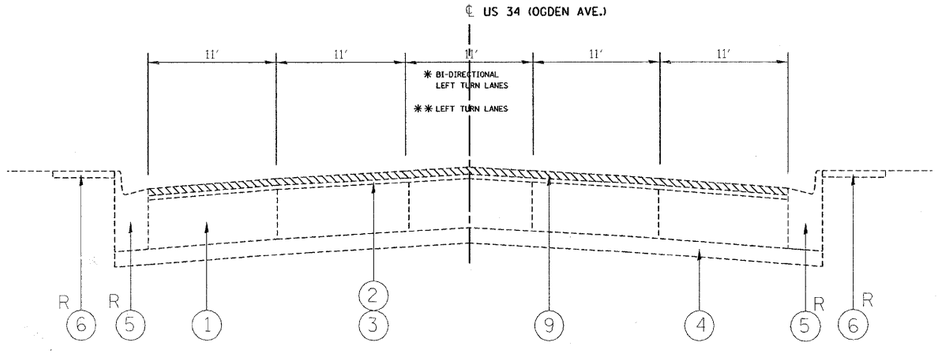


EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 45+89 TO STA. 51+28

LEGEND:

(1) EXISTING P.C.C. PAVEMENT, ±10"
(2) EXISTING HOT-MIX ASPHALT SURFACE, ±8"
(3) EXISTING HOT-MIX ASPHALT SURFACE (AFTER MILLING), ±6"
(4) EXISTING STABILIZED SUB-BASE
(5) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(6) EXISTING SIDEWALK
(7) EXISTING CORRUGATED MEDIAN
(8) EXISTING HOT-MIX ASPHALT SHOULDER
(9) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
(10) PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
(11) PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1¾"
R CURB AND GUTTER REMOVAL AND REPLACEMENT

NOTE:
THE MILLING SHALL BE DONE PRIOR TO PATCHING



EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 51+28 TO STA. 178+92

LEGEND:

* STA. 50+74 TO STA. 108+48
STA. 116+47 TO STA. 118+27
STA. 126+10 TO STA. 160+98
STA. 169+40 TO STA. 178+13

** STA. 108+48 TO STA. 111+57
STA. 112+52 TO STA. 116+47
STA. 118+27 TO STA. 121+64
STA. 122+69 TO STA. 126+10
STA. 160+98 TO STA. 164+77
STA. 165+81 TO STA. 169+40

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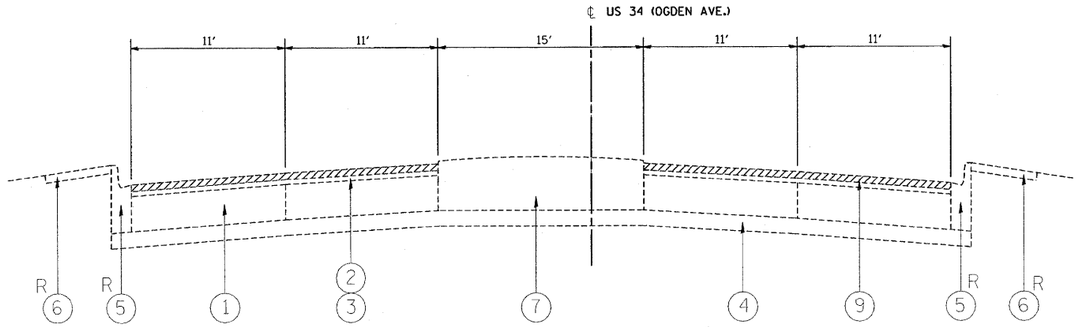
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING TYPICAL SECTIONS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	4
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



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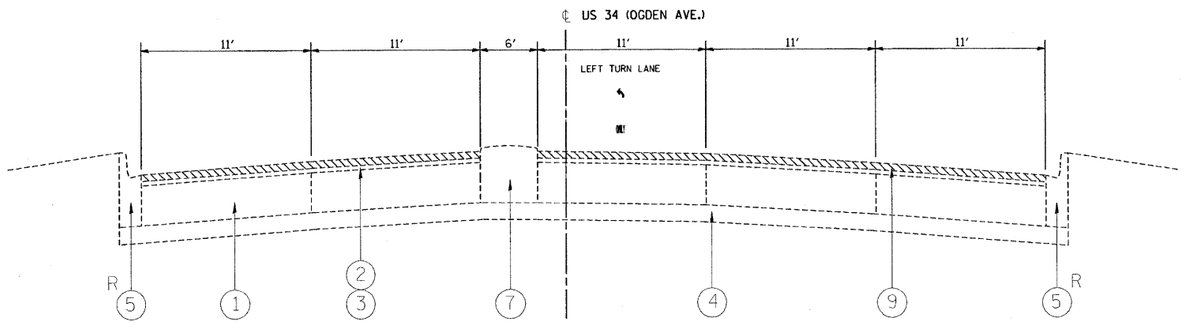
STA. 178+92 TO STA. 183+08



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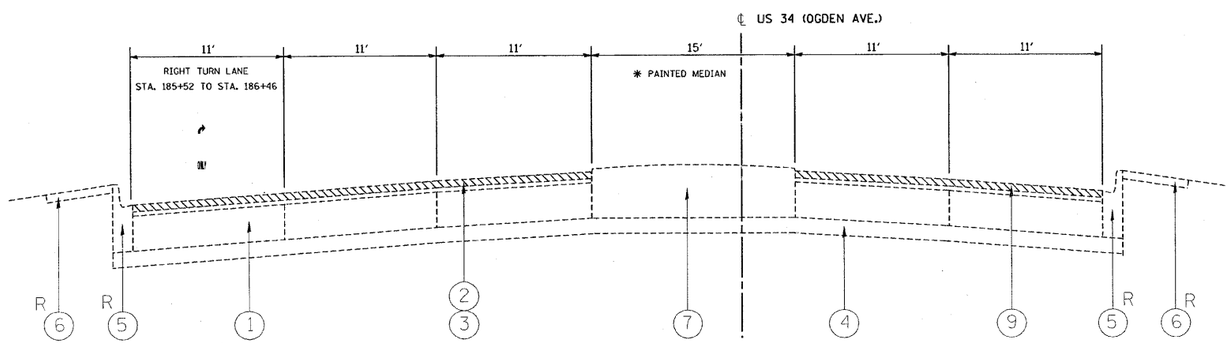
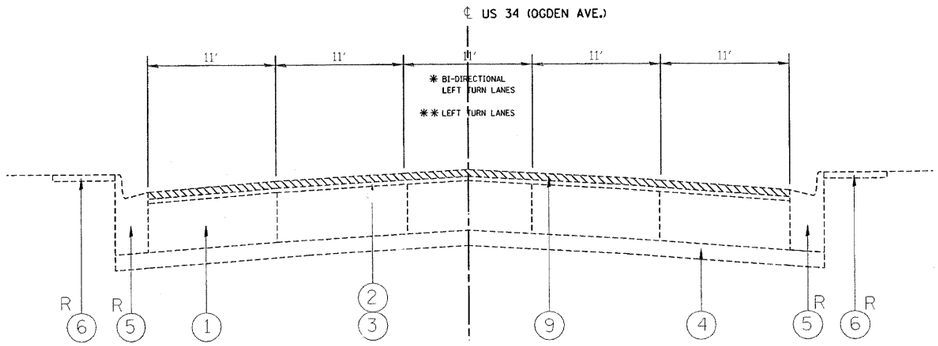
STA. 183+08 TO STA. 184+70



EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 185+52 TO STA. 189+26

LEGEND:
* STA. 187+98 TO STA. 189+26



EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 189+26 TO STA. 213+47

LEGEND:
* STA. 189+26 TO STA. 202+48
STA. 205+87 TO STA. 213+47
** STA. 203+33 TO STA. 205+87

LEGEND:

1. EXISTING P.C.C. PAVEMENT, ±10"
2. EXISTING HOT-MIX ASPHALT SURFACE, ±8"
3. EXISTING HOT-MIX ASPHALT SURFACE (AFTER MILLING), ±6"
4. EXISTING STABILIZED SUB-BASE
5. EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
6. EXISTING SIDEWALK
7. EXISTING CORRUGATED MEDIAN
8. EXISTING HOT-MIX ASPHALT SHOULDER
9. PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
10. PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
11. PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1¾"

R CURB AND GUTTER REMOVAL AND REPLACEMENT

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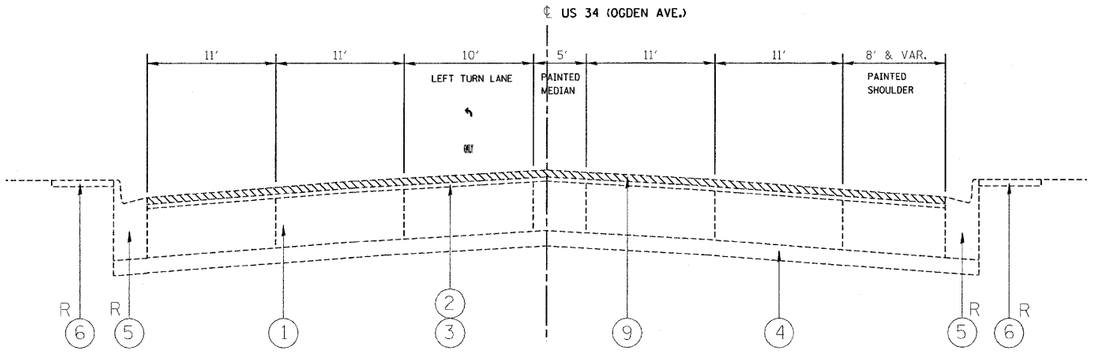
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING TYPICAL SECTIONS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	5
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



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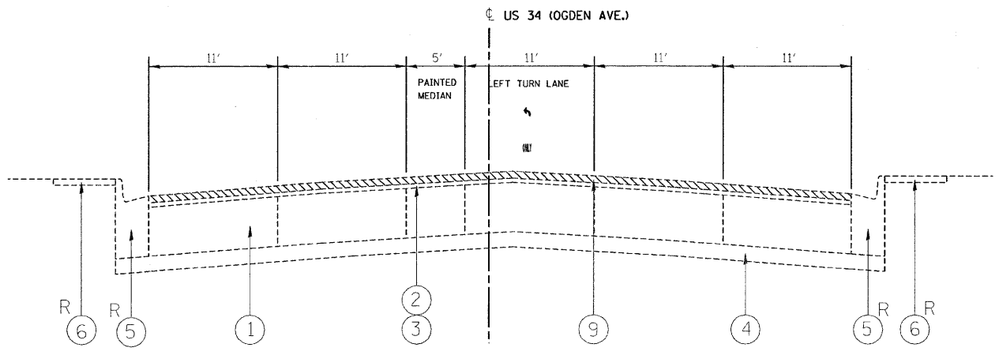
STA. 213+47 TO STA. 217+69



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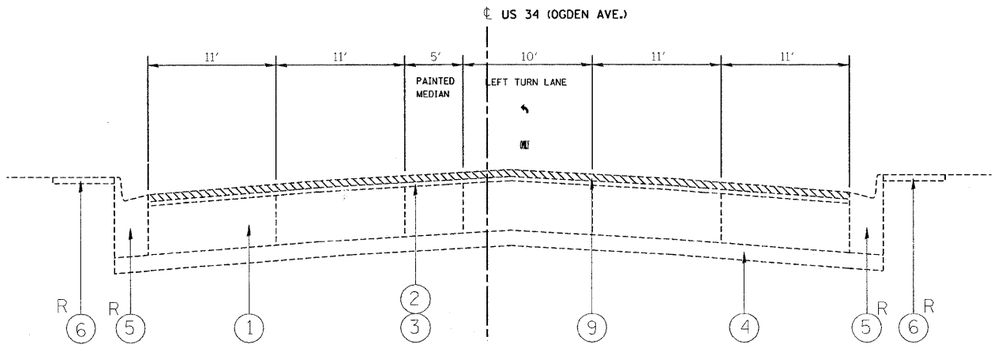
STA. 217+69 TO STA. 220+77



EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 220+77 TO STA. 224+29

LEGEND:

(1) EXISTING P.C.C. PAVEMENT, ±10"
(2) EXISTING HOT-MIX ASPHALT SURFACE, ±8"
(3) EXISTING HOT-MIX ASPHALT SURFACE (AFTER MILLING), ±6"
(4) EXISTING STABILIZED SUB-BASE
(5) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(6) EXISTING SIDEWALK
(7) EXISTING CORRUGATED MEDIAN
(8) EXISTING HOT-MIX ASPHALT SHOULDER
(9) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
(10) PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
(11) PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1¾"
R CURB AND GUTTER REMOVAL AND REPLACEMENT

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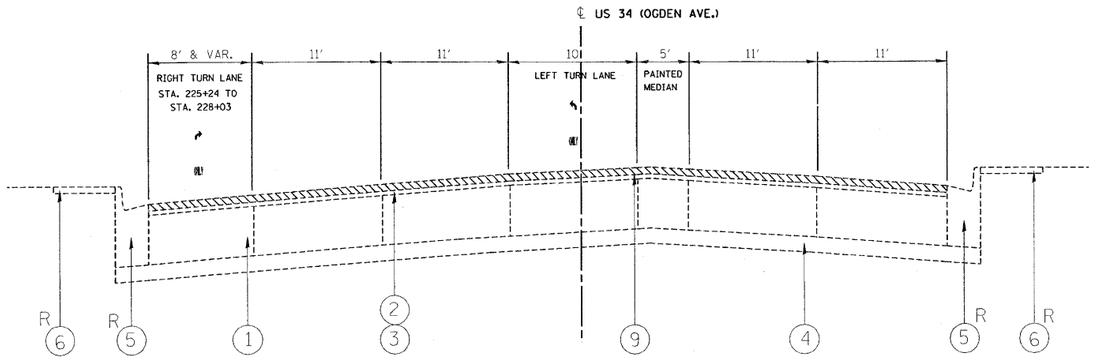
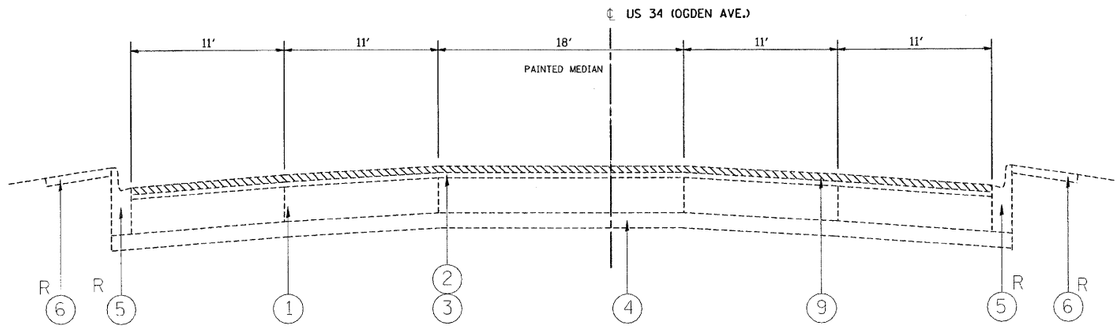
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING TYPICAL SECTIONS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.
SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	6
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 224+29 TO STA. 228+57



EXISTING TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 228+57 TO STA. 229+73

LEGEND:

(1)	EXISTING P.C.C. PAVEMENT, ±10"
(2)	EXISTING HOT-MIX ASPHALT SURFACE, ±8"
(3)	EXISTING HOT-MIX ASPHALT SURFACE (AFTER MILLING), ±6"
(4)	EXISTING STABILIZED SUB-BASE
(5)	EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(6)	EXISTING SIDEWALK
(7)	EXISTING CORRUGATED MEDIAN
(8)	EXISTING HOT-MIX ASPHALT SHOULDER
(9)	PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
(10)	PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
(11)	PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1¾"
R	CURB AND GUTTER REMOVAL AND REPLACEMENT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING TYPICAL SECTIONS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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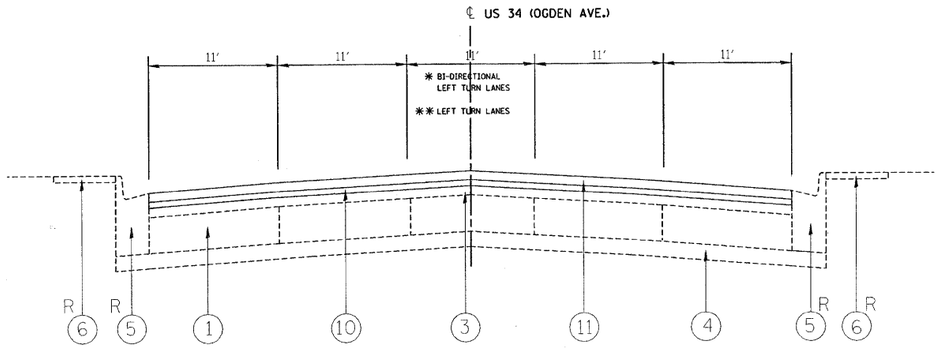
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

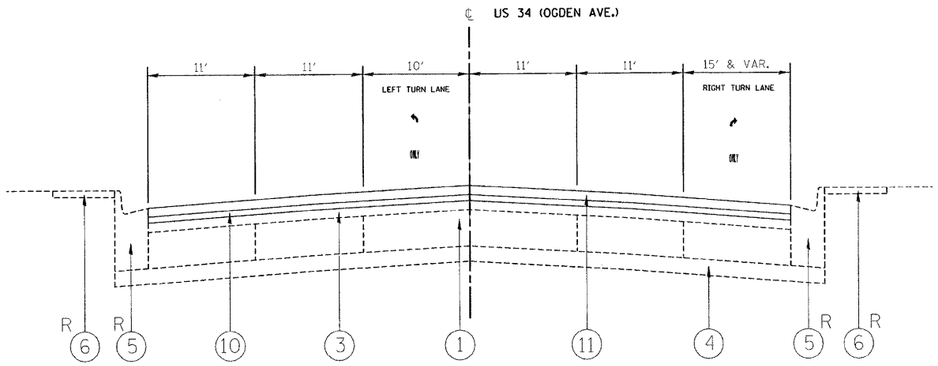


PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 13+03 TO STA. 42+64

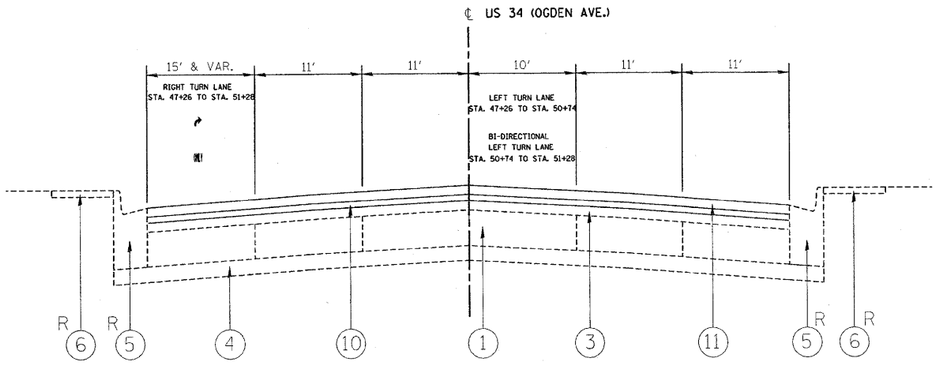
LEGEND:

✱ STA. 14+50 TO STA. 18+37
STA. 21+16 TO STA. 23+40
STA. 29+54 TO STA. 41+56

✱✱ STA. 13+03 TO STA. 13+35
STA. 19+07 TO STA. 21+16
STA. 23+40 TO STA. 25+88
STA. 27+00 TO STA. 29+54



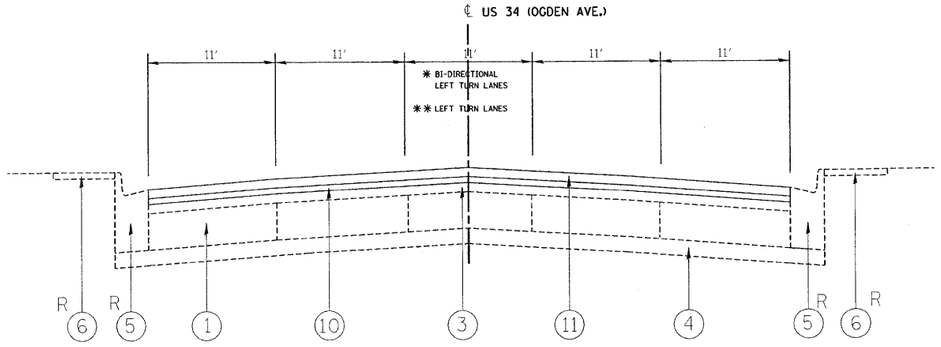
PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 42+64 TO STA. 45+89



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 45+89 TO STA. 51+28

LEGEND:

(1) EXISTING P.C.C. PAVEMENT, ±10″
(2) EXISTING HOT-MIX ASPHALT SURFACE, ±8″
(3) EXISTING HOT-MIX ASPHALT SURFACE (AFTER MILLING), ±6″
(4) EXISTING STABILIZED SUB-BASE
(5) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(6) EXISTING SIDEWALK
(7) EXISTING CORRUGATED MEDIAN
(8) EXISTING HOT-MIX ASPHALT SHOULDER
(9) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2½″
(10) PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾″
(11) PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX ″F″, N90, 1¾″
R CURB AND GUTTER REMOVAL AND REPLACEMENT



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 51+28 TO STA. 178+92

LEGEND:

✱ STA. 50+74 TO STA. 108+48
STA. 116+47 TO STA. 118+27
STA. 126+10 TO STA. 160+98
STA. 169+40 TO STA. 178+13

✱✱ STA. 108+48 TO STA. 111+57
STA. 112+52 TO STA. 116+47
STA. 118+27 TO STA. 121+64
STA. 122+69 TO STA. 126+10
STA. 160+98 TO STA. 164+77
STA. 165+81 TO STA. 169+40

HOT-MIX ASPHALT MIXTURE REQUIREMENT TABLE

MIXTURE USE	AC TYPE	DESIGN AIR VOIDS
CLASS ″D″ PATCHES, 9″ HMA BINDER COURSE, IL-19MM	✱PG 64 -22	4% @ 70
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES, HMA BINDER COURSE., IL-19MM	✱PG 64 -22	4% @ 70
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	SBS/SBR PG 76-28/ -22	4% @ 50
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX ″F″, N90, IL-9.5MM	SBS/SBR PG 70 -22	4% @ 90

NOTES:
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE COURSE QUANTITIES IS 112 LBS./SQ. YD./ IN.

THE MILLING SHALL BE DONE PRIOR TO PATCHING

✱ WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58 -22

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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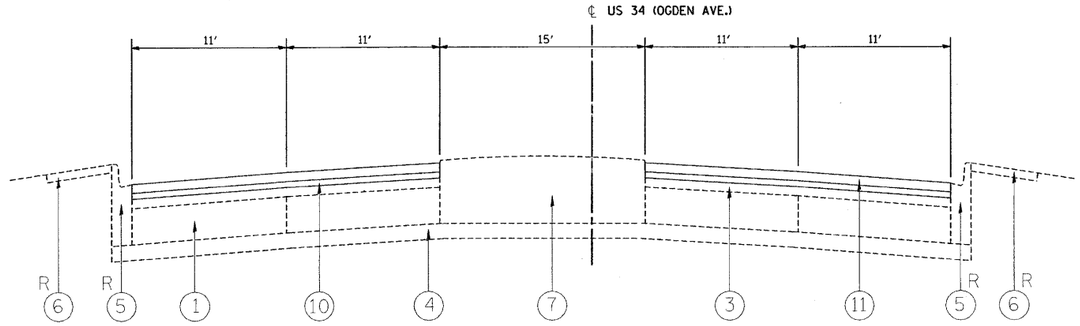
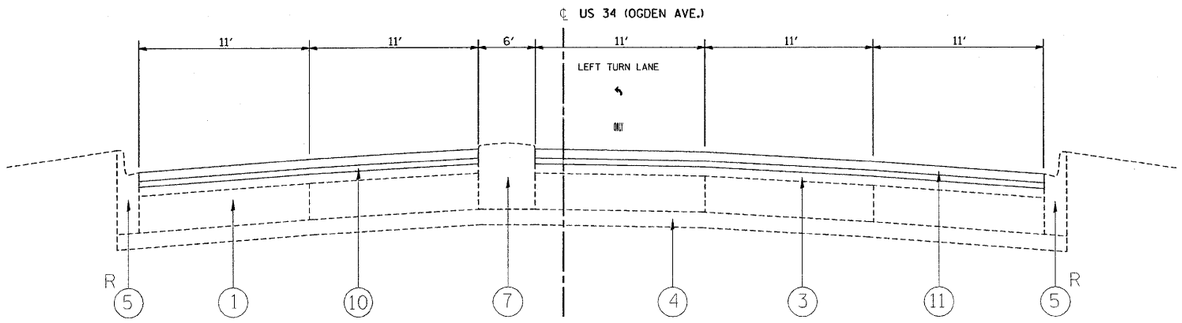
		CONTRACT NO. 60A85		

FED. ROAD DIST. NO. 1 | ILLINOIS | FED. AID PROJECT



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 178+92 TO STA. 183+08



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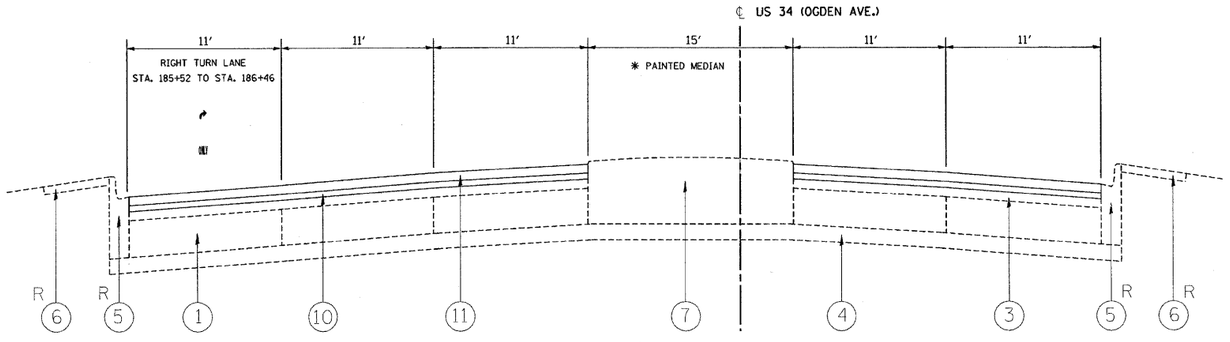
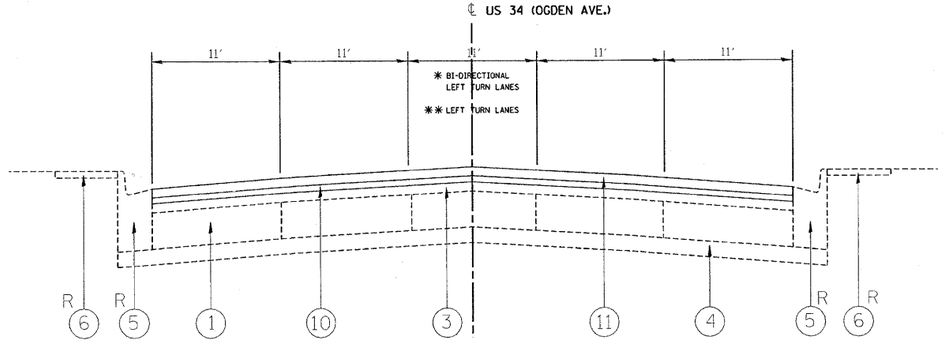
STA. 183+08 TO STA. 184+70



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 185+52 TO STA. 189+26

LEGEND:

* STA. 187+98 TO STA. 189+26



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 189+26 TO STA. 213+47

LEGEND:

* STA. 189+26 TO STA. 202+48
STA. 205+87 TO STA. 213+47

** STA. 203+33 TO STA. 205+87

LEGEND:

(1) EXISTING P.C.C. PAVEMENT, ±10"
(2) EXISTING HOT-MIX ASPHALT SURFACE, ±8"
(3) EXISTING HOT-MIX ASPHALT SURFACE (AFTER MILLING), ±6"
(4) EXISTING STABILIZED SUB-BASE
(5) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(6) EXISTING SIDEWALK
(7) EXISTING CORRUGATED MEDIAN
(8) EXISTING HOT-MIX ASPHALT SHOULDER
(9) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
(10) PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
(11) PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1¾"
R CURB AND GUTTER REMOVAL AND REPLACEMENT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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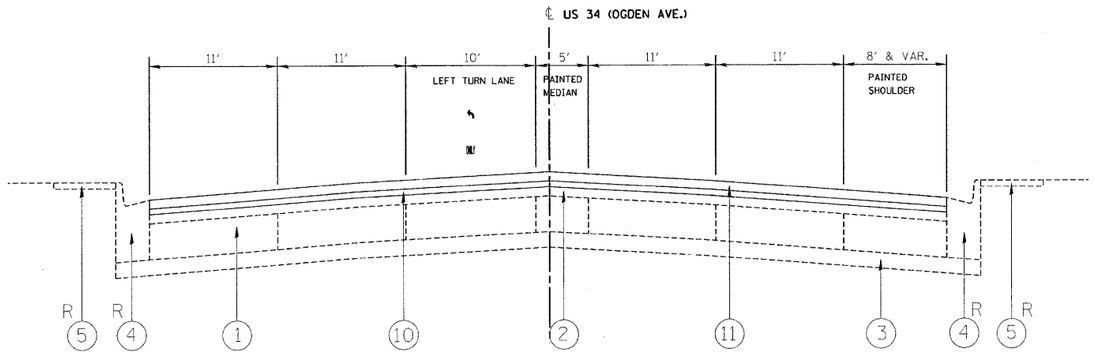

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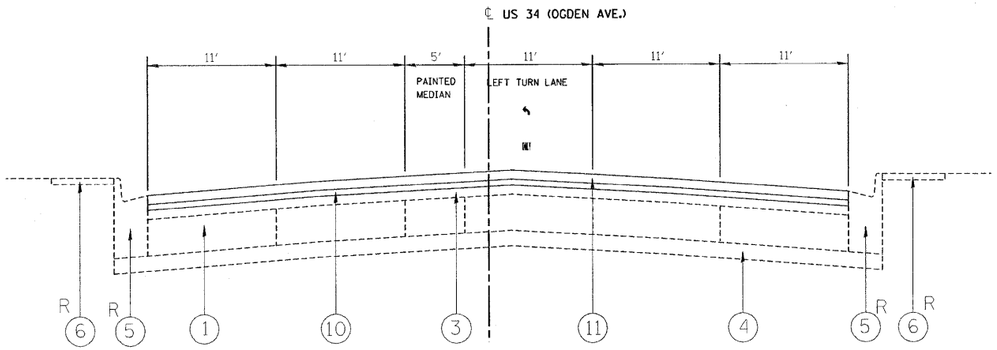
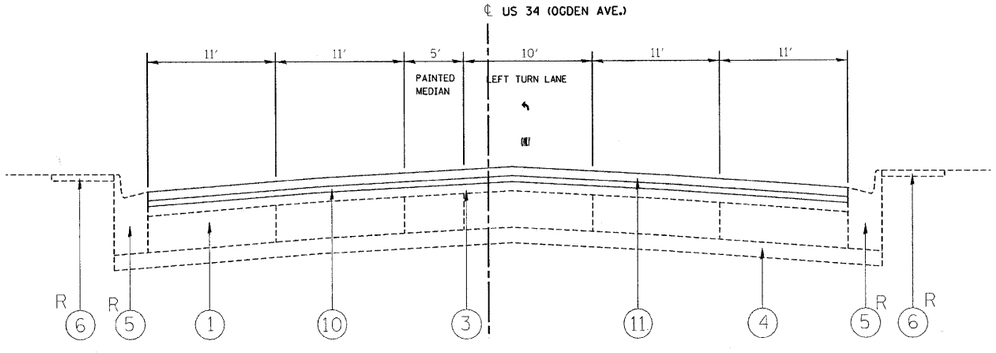
STA. 213+47 TO STA. 217+69



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 217+69 TO STA. 220+77



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 220+77 TO STA. 224+29

LEGEND:

(1) EXISTING P.C.C. PAVEMENT, ±10"

(2) EXISTING HOT-MIX ASPHALT SURFACE, ±8"

(3) EXISTING HOT-MIX ASPHALT SURFACE (AFTER MILLING), ±6"

(4) EXISTING STABILIZED SUB-BASE

(5) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12

(6) EXISTING SIDEWALK

(7) EXISTING CORRUGATED MEDIAN

(8) EXISTING HOT-MIX ASPHALT SHOULDER

(9) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2½"

(10) PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"

(11) PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1¾"

R CURB AND GUTTER REMOVAL AND REPLACEMENT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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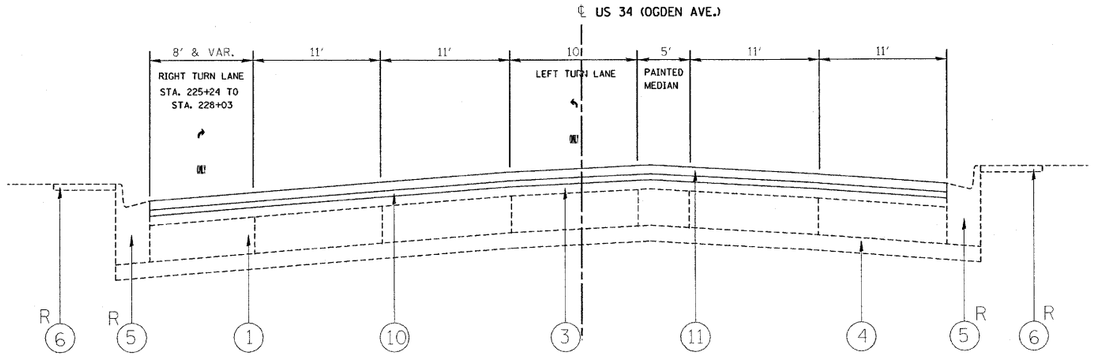
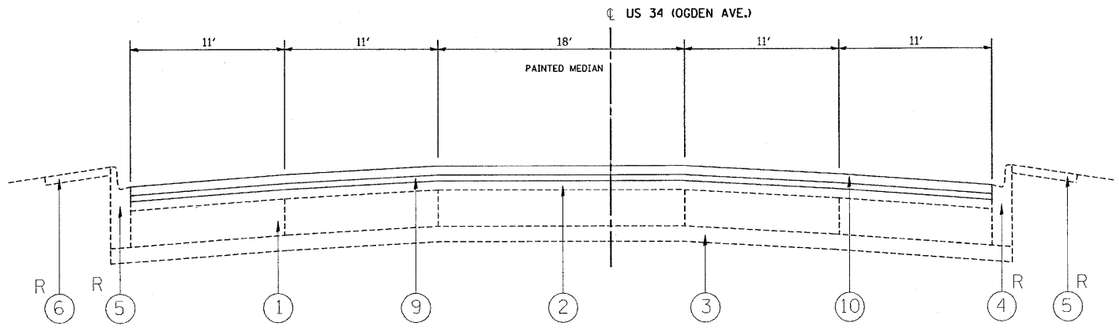
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 224+29 TO STA. 228+03



PROPOSED TYPICAL SECTION US 34 (OGDEN AVE.)
STA. 228+03 TO STA. 229+73

LEGEND:

(1) EXISTING P.C.C. PAVEMENT, ±10"
(2) EXISTING HOT-MIX ASPHALT SURFACE, ±8"
(3) EXISTING HOT-MIX ASPHALT SURFACE (AFTER MILLING), ±6"
(4) EXISTING STABILIZED SUB-BASE
(5) EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
(6) EXISTING SIDEWALK
(7) EXISTING CORRUGATED MEDIAN
(8) EXISTING HOT-MIX ASPHALT SHOULDER
(9) PROP. HOT-MIX ASPHALT SURFACE REMOVAL, 2½"
(10) PROP. POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, ¾"
(11) PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90, 1¾"
R CURB AND GUTTER REMOVAL AND REPLACEMENT

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED TYPICAL SECTIONS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.
SCALE: | SHEET NO. OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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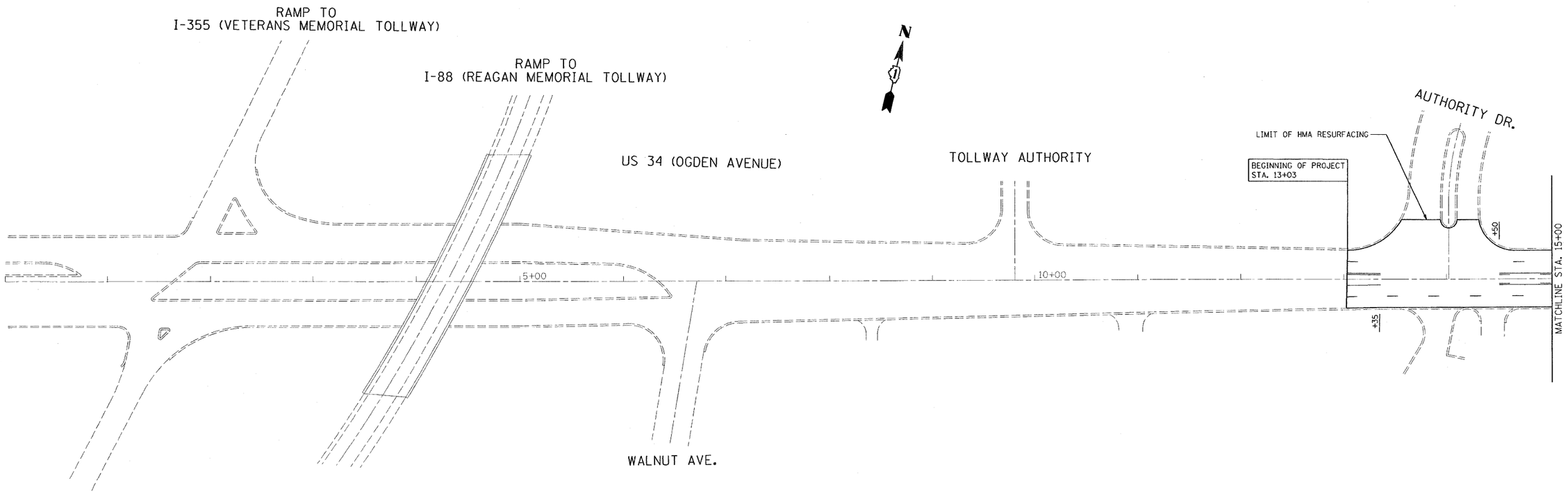
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. 13+03 TO STA. 30+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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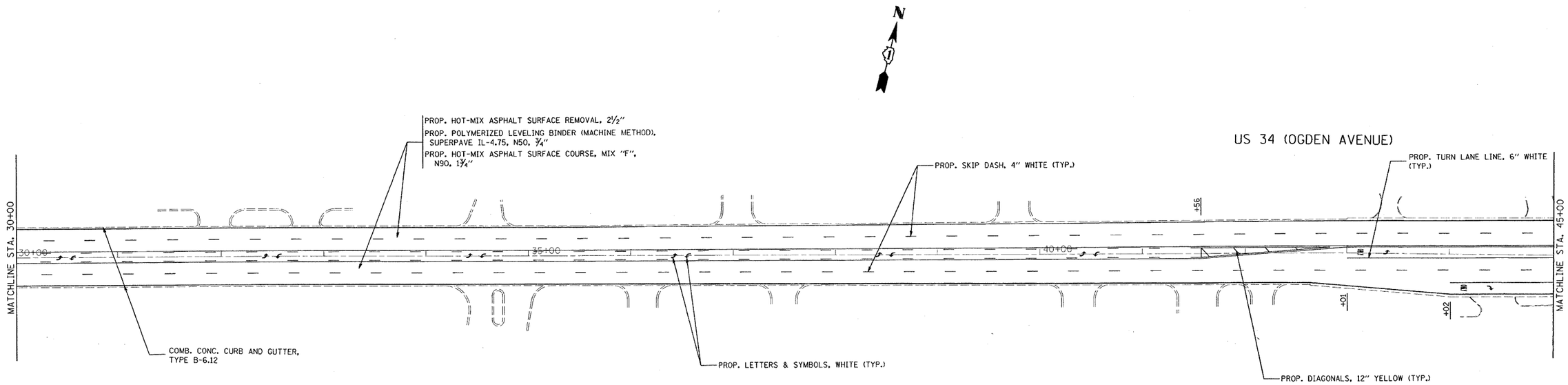

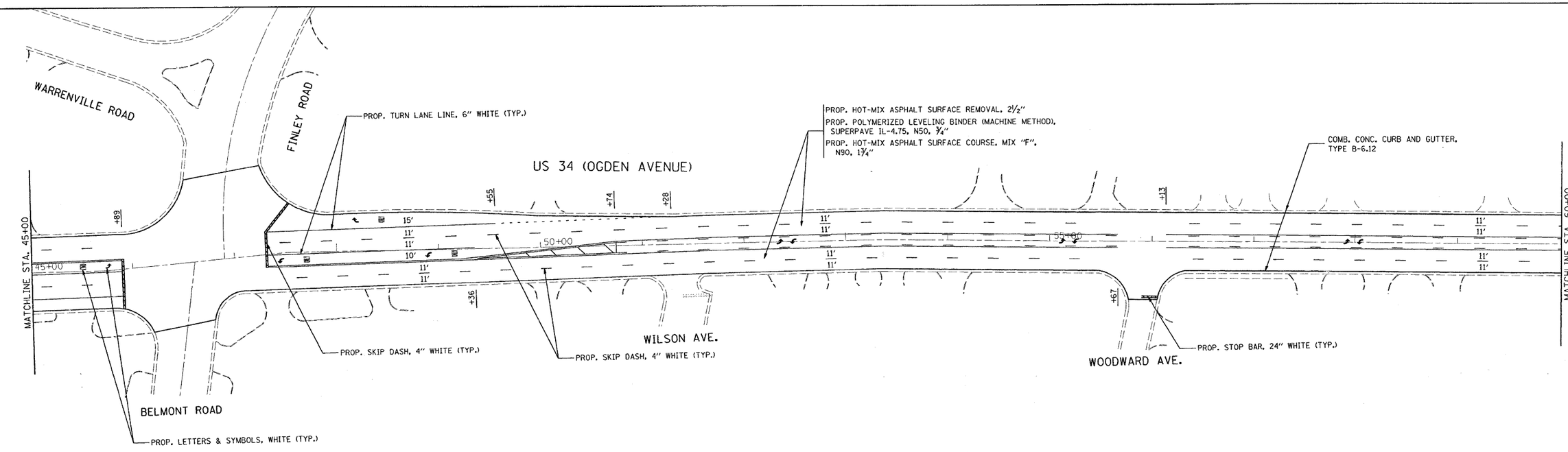

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. 30+00 | TO STA. 60+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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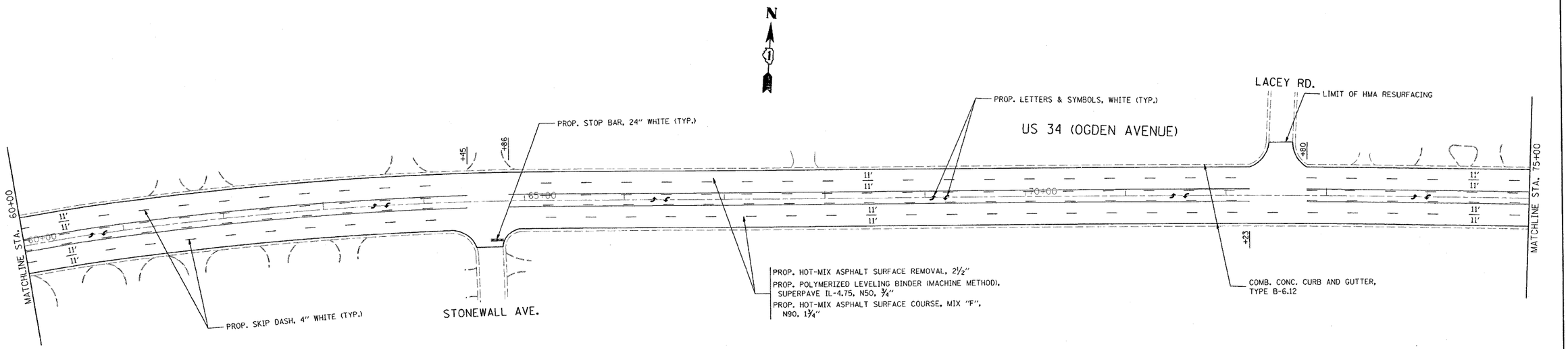
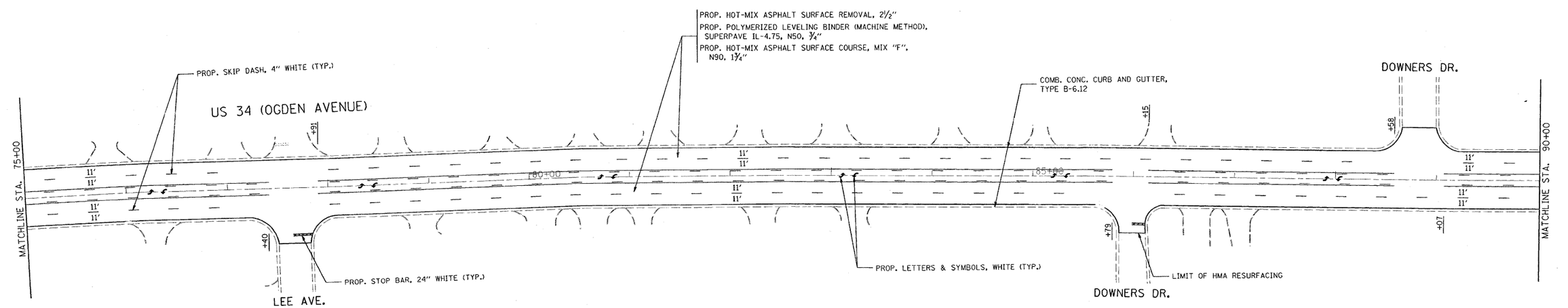
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. 60+00 TO STA. 90+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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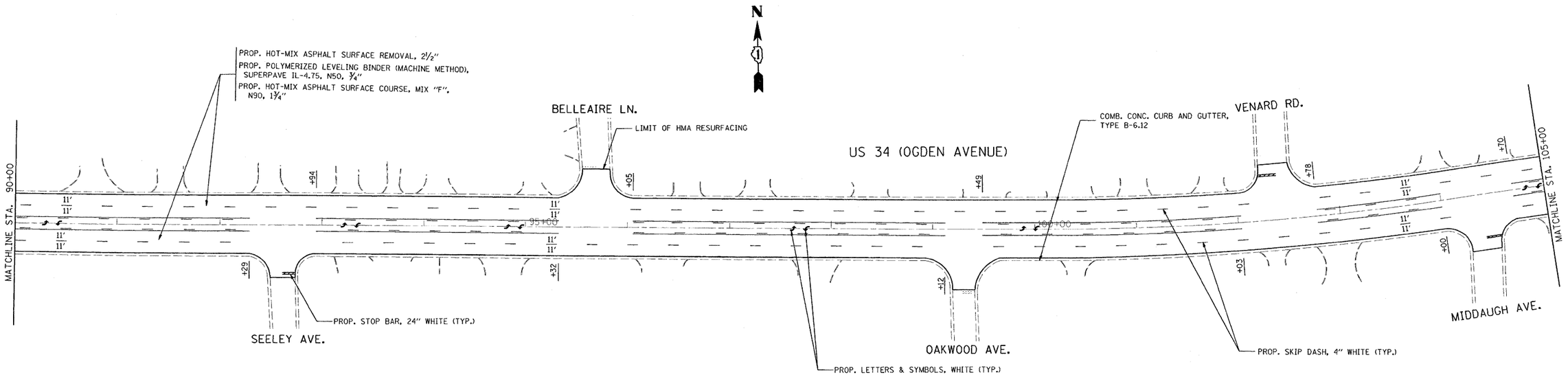
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. 90+00 TO STA. 120+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CONTRACT NO. 60A85		

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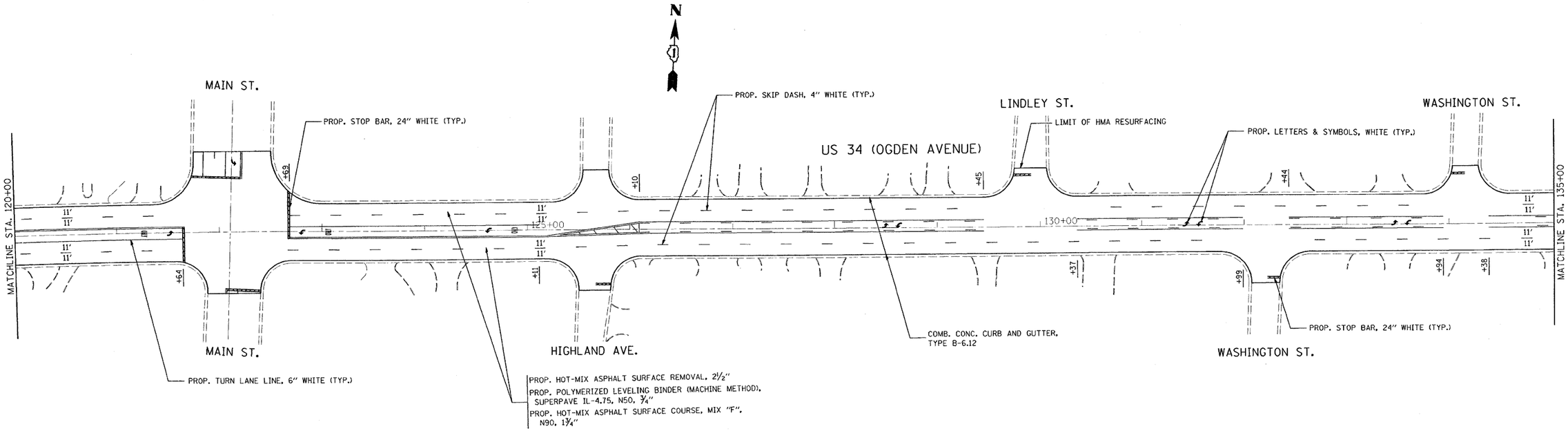

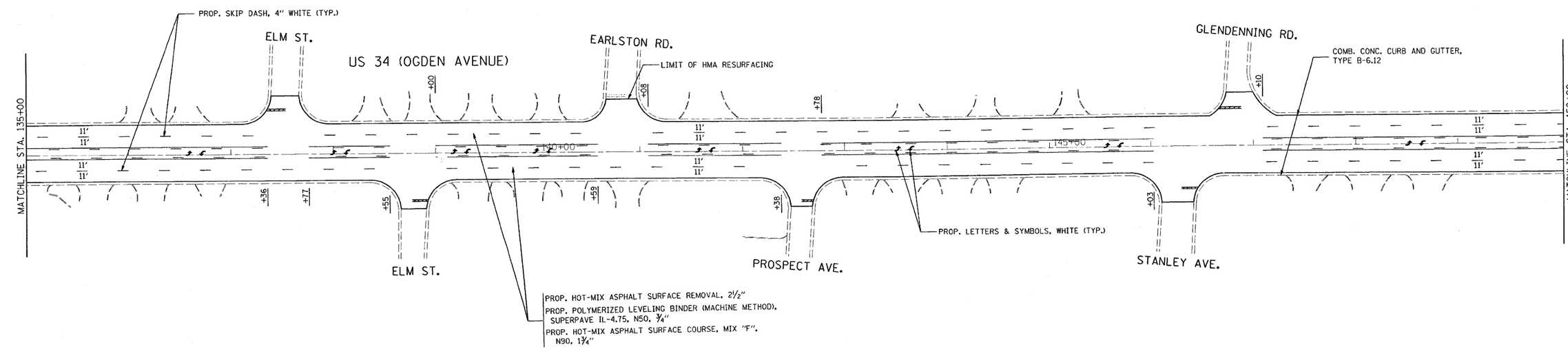

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. 120+00 TO STA. 150+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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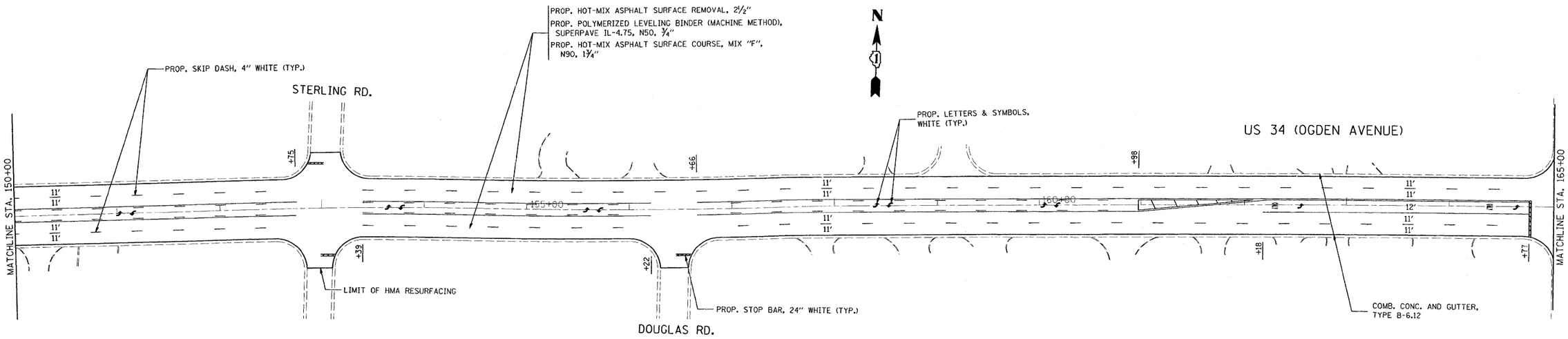

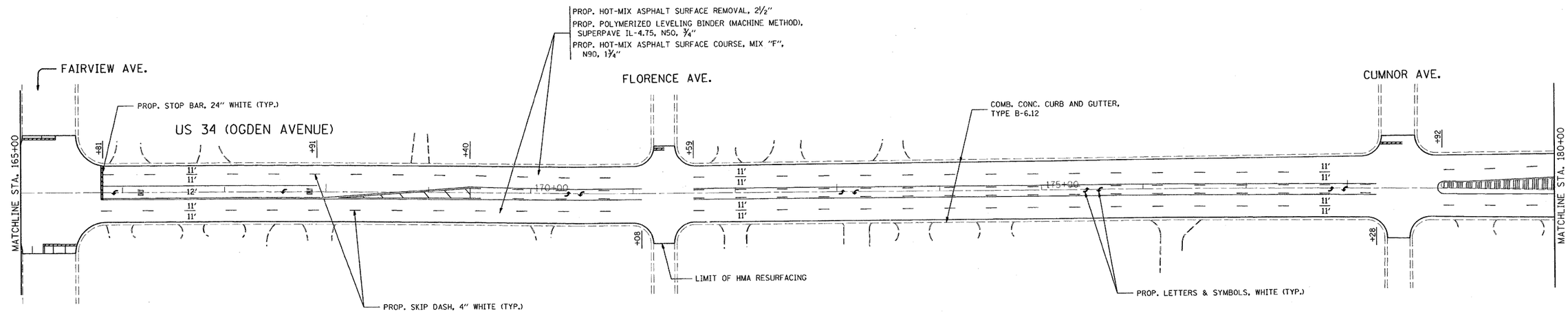

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. 150+00 | TO STA. 180+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CONTRACT NO. 60A85		
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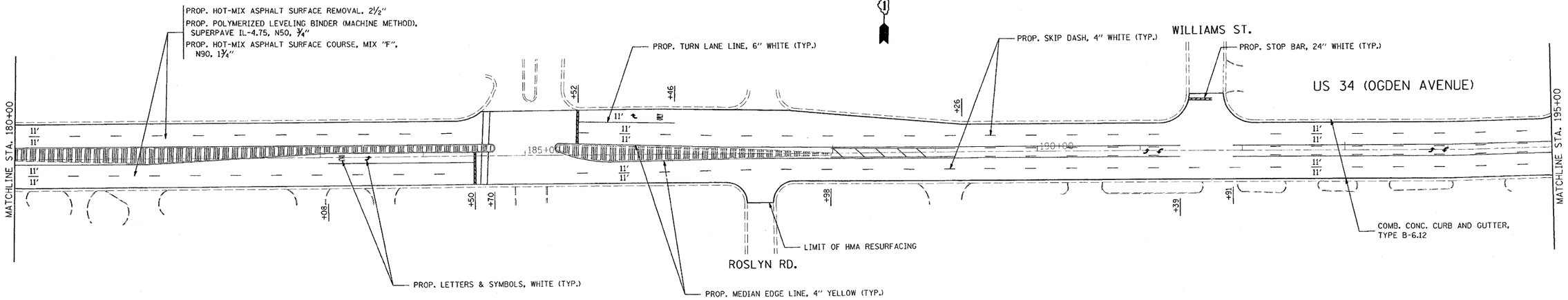

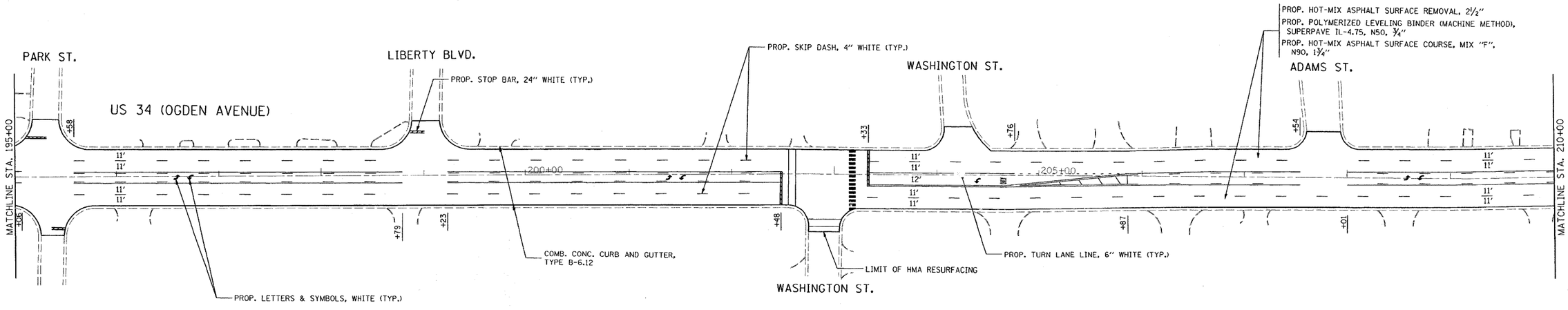

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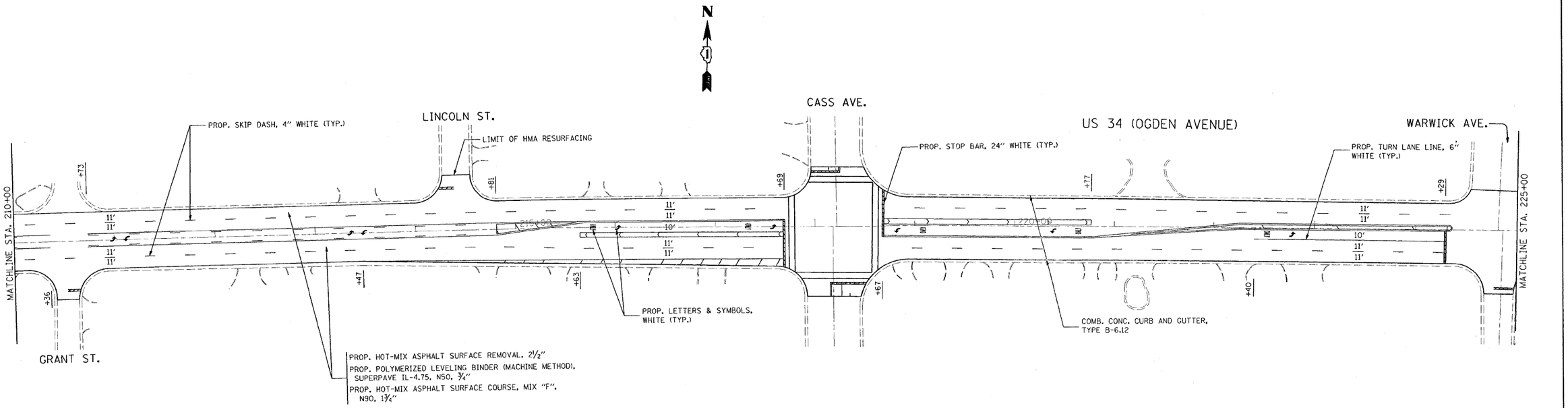
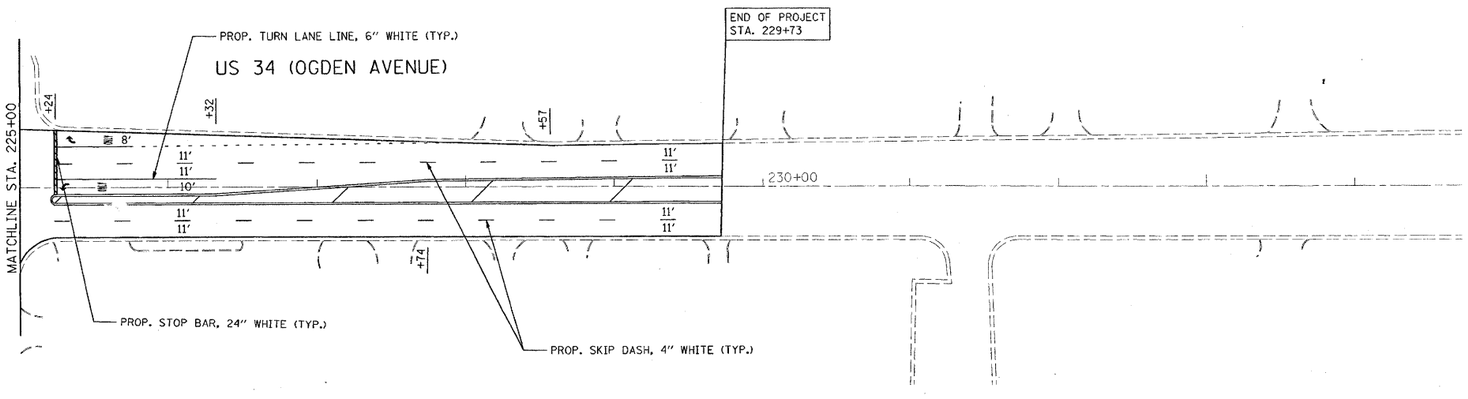
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. 180+00 | TO STA. 210+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	18
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED -	REVISED -
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -
	PLOT DATE = 1/7/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

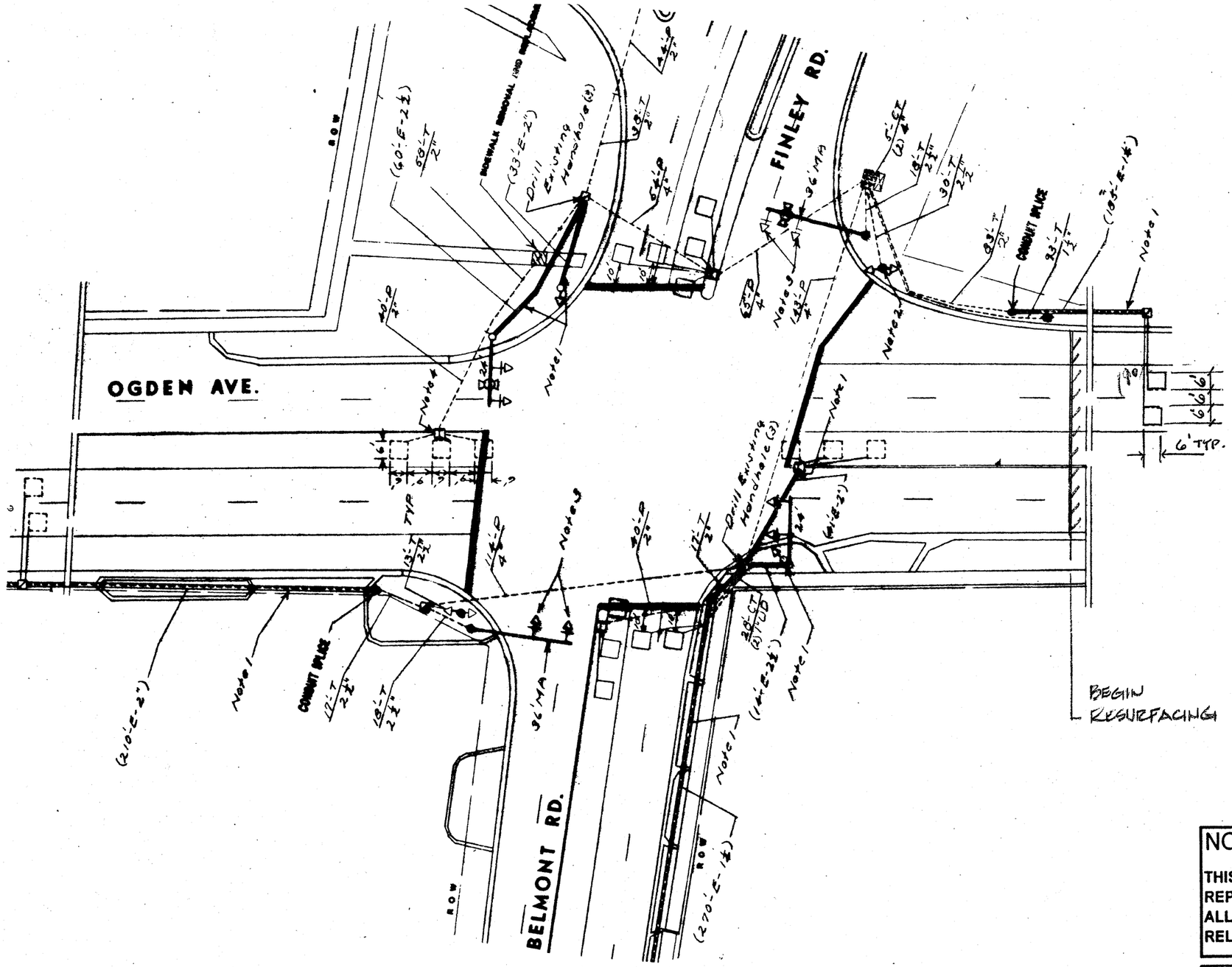
ROADWAY AND PAVEMENT MARKING PLANS
US 34 (OGDEN AVE.)--I-355 TO WARWICK AVE.

SCALE: | SHEET NO. OF SHEETS | STA. 210+00 TO STA. 229+73

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	19
		CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DUPAGE	41	20
STA.		TO STA.		
FED. ROAD DIST NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT No. : 60A85



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
86600600	90	Foot	Detector Loop Replacement

86600600

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT

U.S. RTE. 34 @ FINLEY / BELMONT RDS

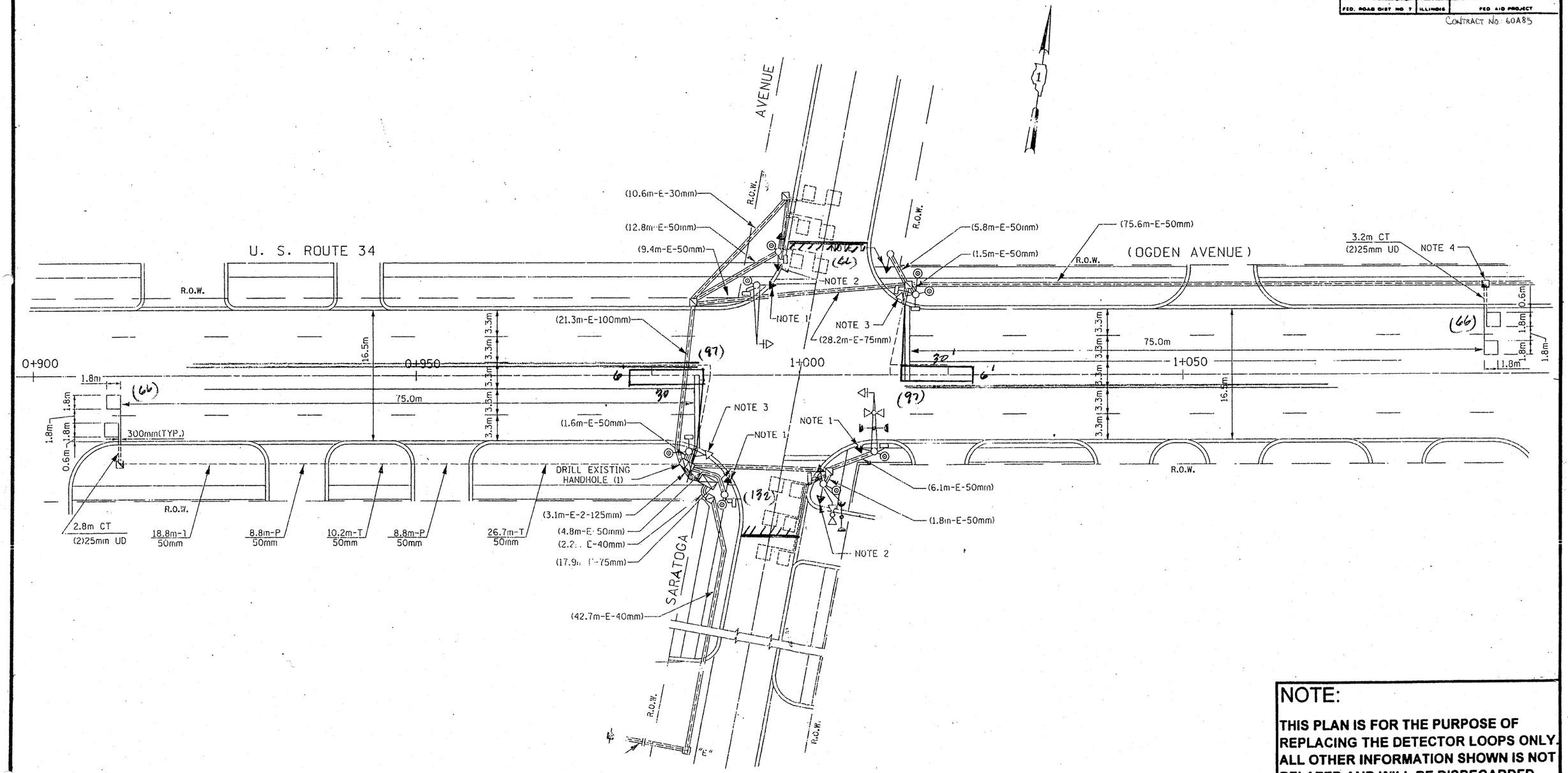
SCALE: NONE.

DATE FEB. 07

DRAWN BY JHE
DESIGNED BY: JHE
CHECKED BY DAD

F. A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DuPAGE	41	21
STA.		TO STA.		
FED. ROAD DIST NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 60A85



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
~~86600600~~ 88600605	524	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT

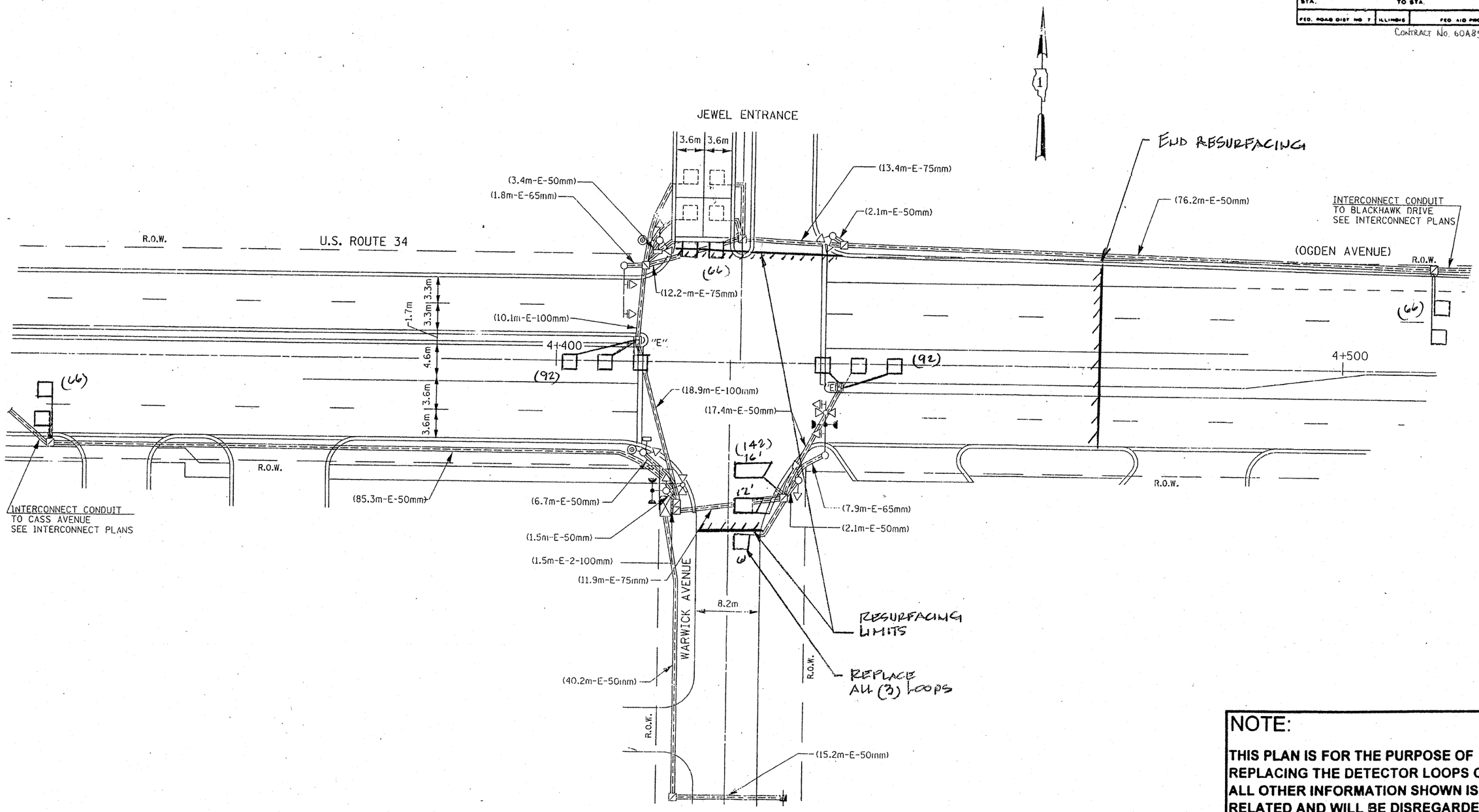
U.S. RTE. 34 @ SARATOGA AV.

SCALE: NONE
DATE: FEB. 07

DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	97-RS-6	DUPAGE	41	22
STA.		TO STA.		
FED. ROAD DIST NO. 7	ILLINOIS	FED AID PROJECT		

CONTRACT NO. 60A85



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
~~86600600~~ 88600600	524	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT

U.S. RTE. 34 @ WARWICK AVENUE

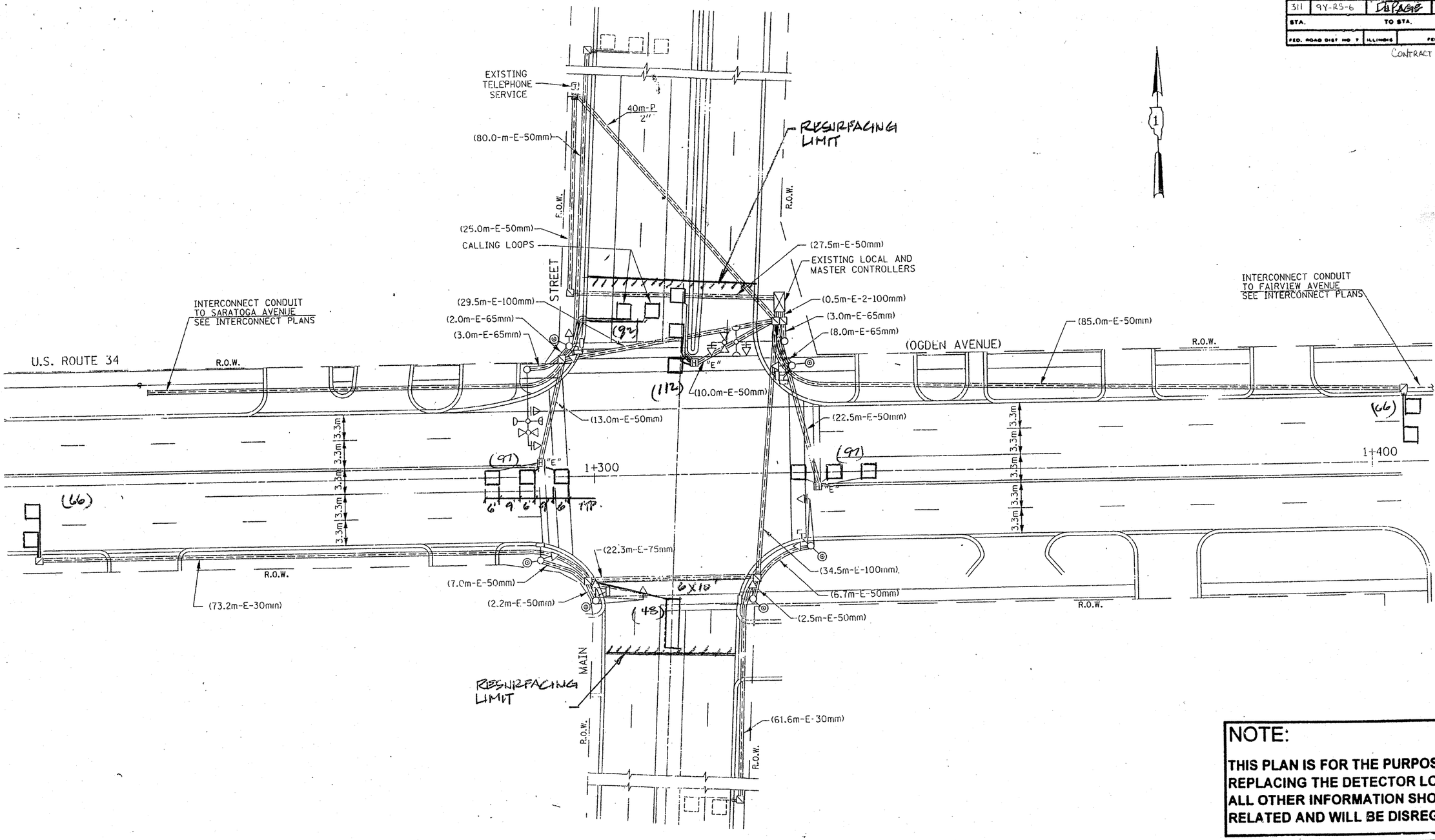
SCALE: NONE

DATE: FEB, 07

DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
311	9Y-RS-6	DUPAGE	41	23
STA.		TO STA.		
FED. ROAD DIST NO 7	ILLINOIS	FED AID PROJECT		

CONTRACT NO.: 60A85



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
~~88600600~~ 88600100	578	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT

U.S. RTE. 34 @ MAIN ST. (D. GROVE)

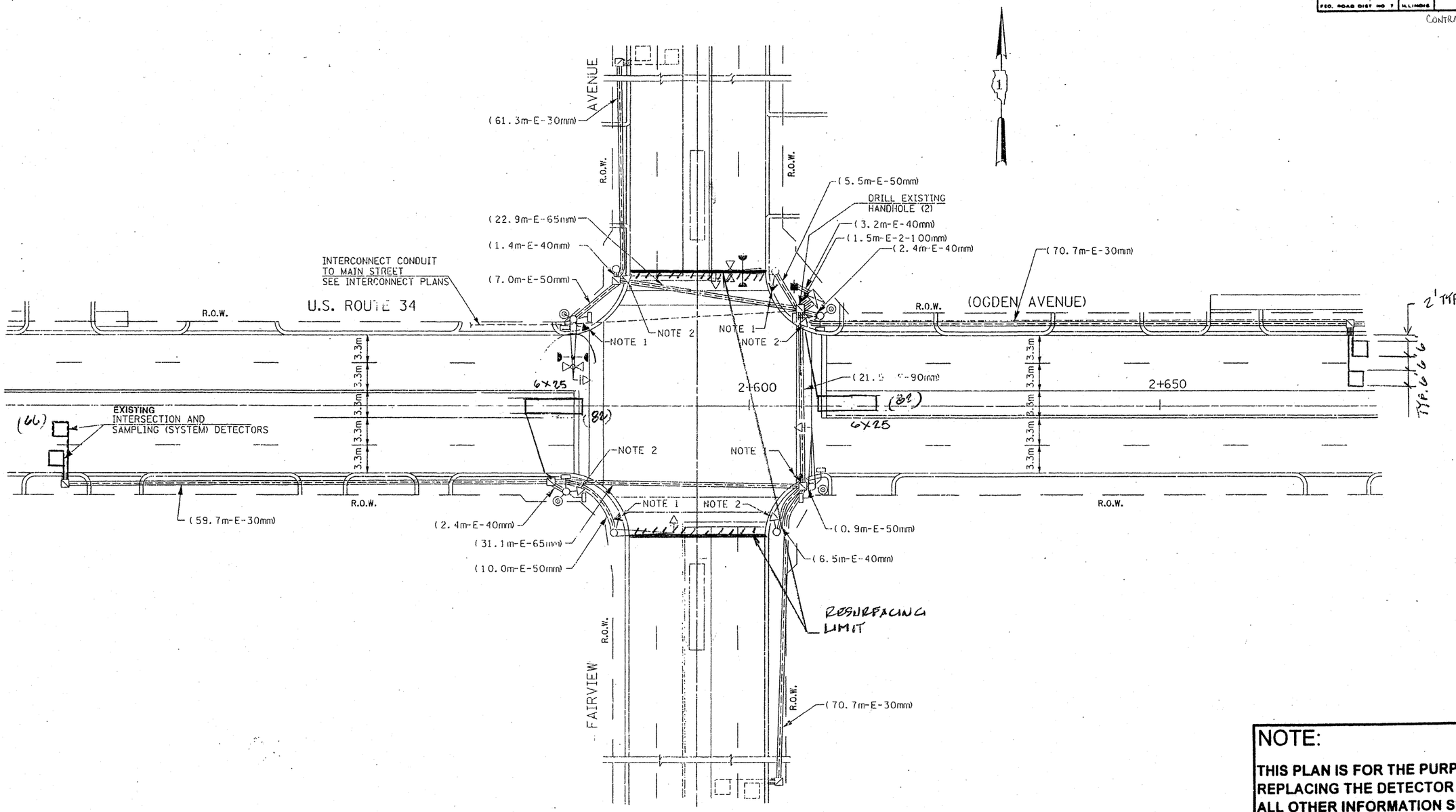
SCALE: NONE

DATE: FEB. 2007

DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DUPAGE	41	24
STA.		TO STA.		
FED. ROAD DIST. NO. 7	ILLINOIS	FED. AID PROJECT		

CONTRACT No.: 60A85



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
~~86600600~~ 88600650	296	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT

U.S. RTE. 34 @ FAIRVIEW AVENUE

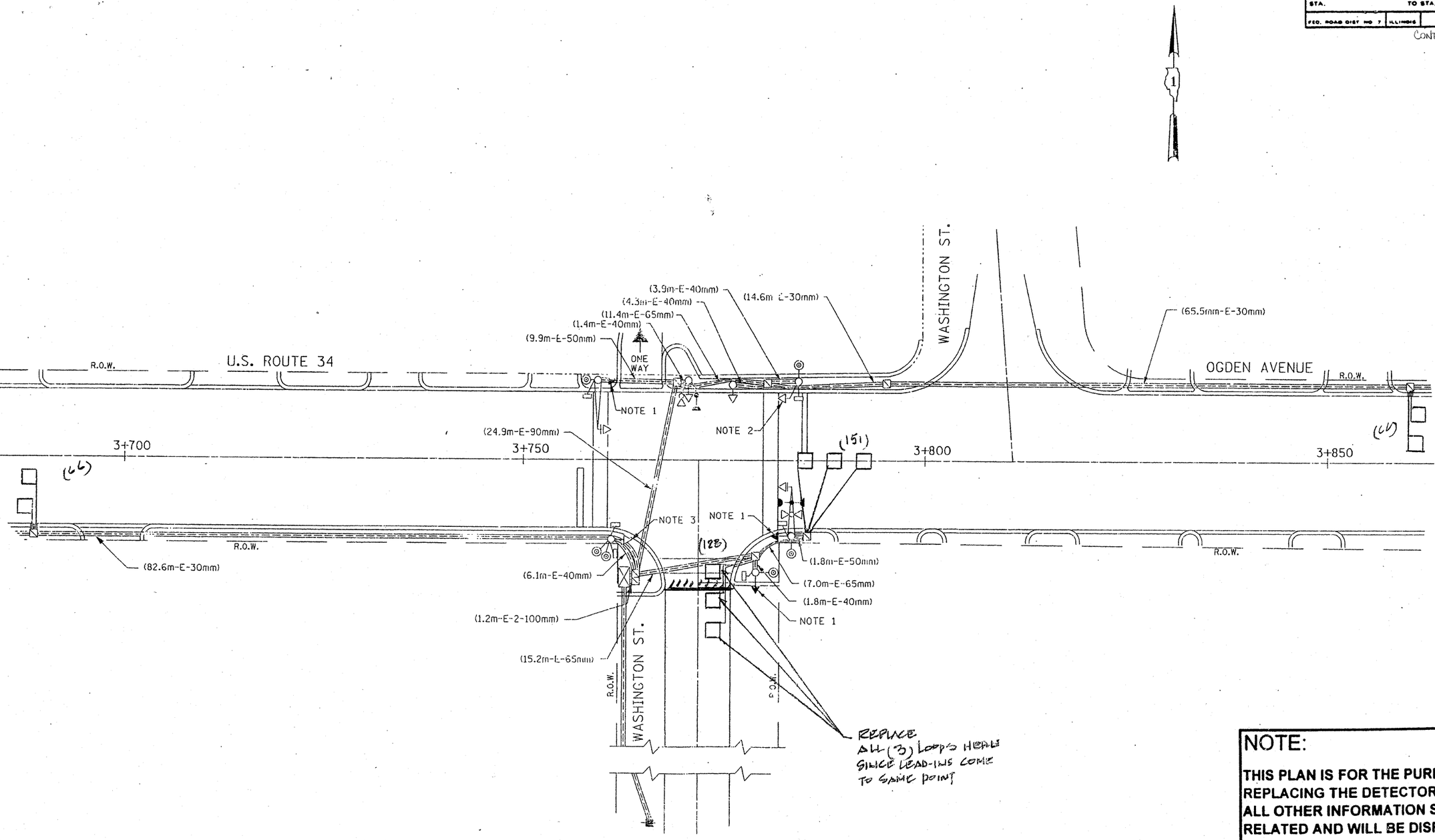
SCALE: NONE

DATE: FEB. '07

DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

F. A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
311	9Y-R5-6	Cook	41	25
STA.		TO STA.		
FED. ROAD DIST NO 7	ILLINOIS	FED AID PROJECT		

CONTRACT No.: 60A85



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
~~88600600~~ 88600100	411	Foot	Detector Loop Replacement

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP REPLACEMENT

U.S. RTE 34 @ WASHINGTON STREET

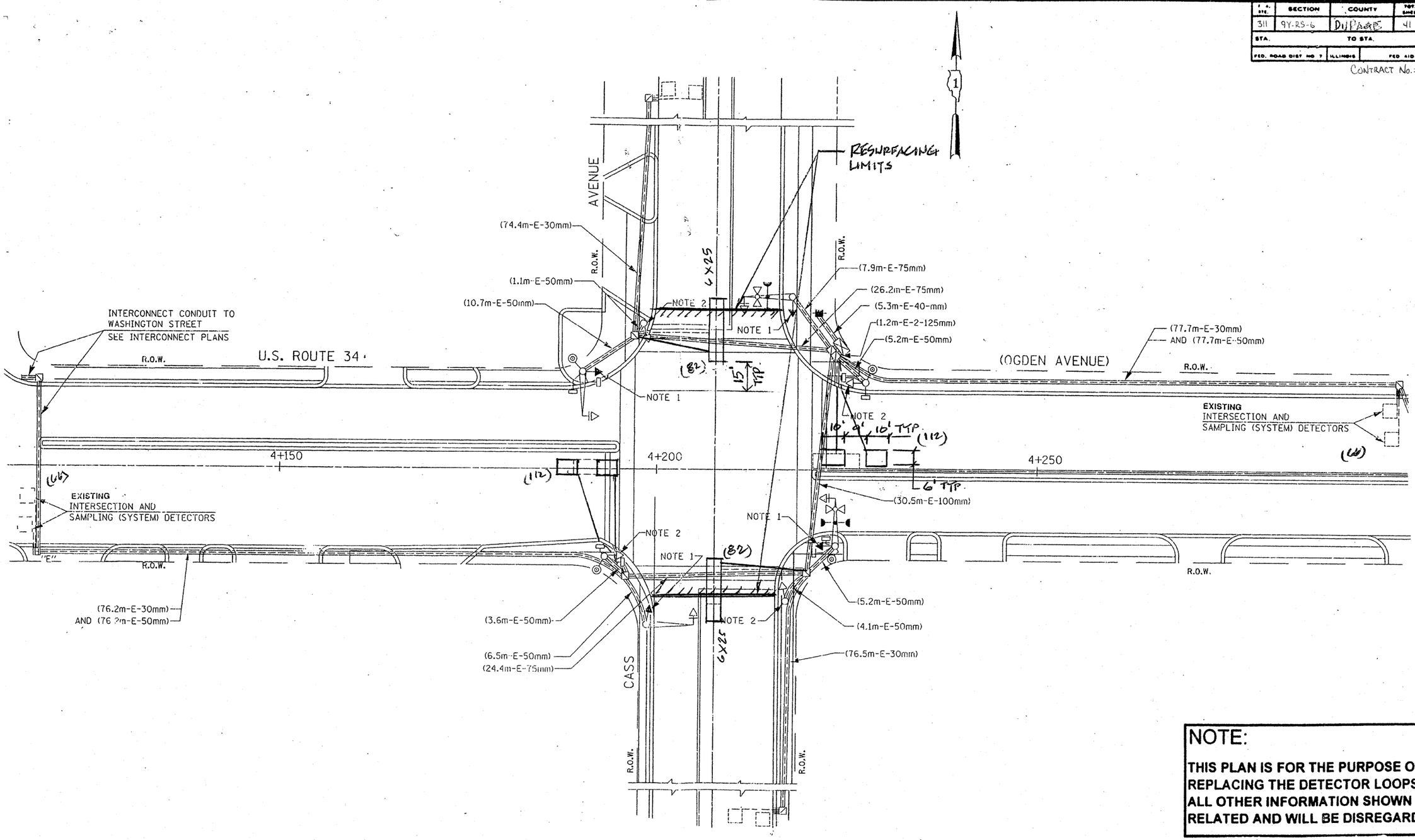
SCALE: NONE

DATE: FEB. 07

DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD

F. A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO
311	97-RS-6	DUPAGE	41	26
STA.		TO STA.		
FED. ROAD DIST NO 7	ILLINOIS	FED AID PROJECT		

CONTRACT No.: 60A85



REPLACE ALL DETECTOR LOOPS AS SHOWN

(WITHIN THE RESURFACING LIMITS)

CODE NO.	QUANTITY	UNIT	ITEM
88600600	520	Foot	Detector Loop Replacement

88600600

NOTE:

THIS PLAN IS FOR THE PURPOSE OF REPLACING THE DETECTOR LOOPS ONLY. ALL OTHER INFORMATION SHOWN IS NOT RELATED AND WILL BE DISREGARDED.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

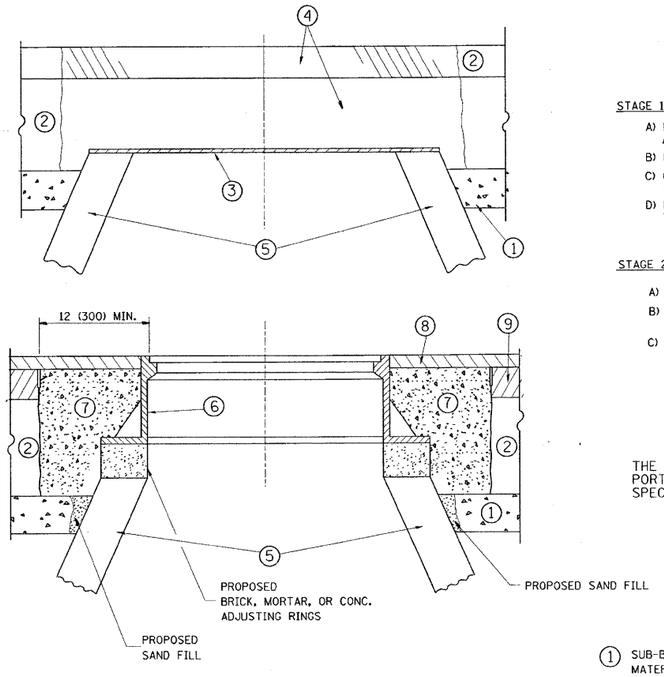
DETECTOR LOOP REPLACEMENT

U.S. RTE. 34 @ CASS AVENUE

SCALE: NONE

DATE: FEB. 07

DRAWN BY: JHE
DESIGNED BY: JHE
CHECKED BY: DAD



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.

B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.

C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.

D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.

B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.

C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

1. SUB-BASE GRANULAR MATERIAL
2. EXISTING PAVEMENT
3. 36 (900) DIAMETER METAL PLATE
4. PROPOSED CRUSHED STONE AND HMA SURFACE MIX
5. EXISTING STRUCTURE
6. FRAME AND LID (SEE NOTES)
7. CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
8. PROPOSED HMA SURFACE COURSE
9. PROPOSED HMA BINDER COURSE

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

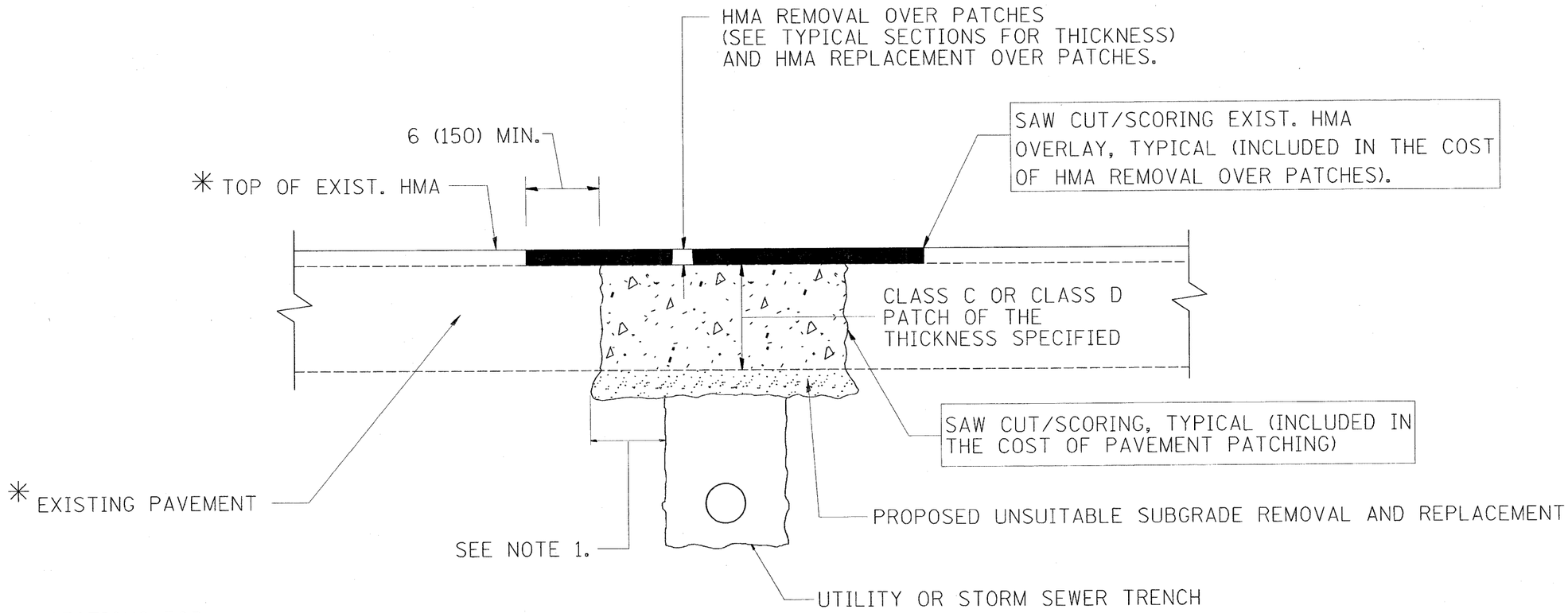
FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpo	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 1/7/2009	DATE - 10-25-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	27
	BD600-03 (BD-8)	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

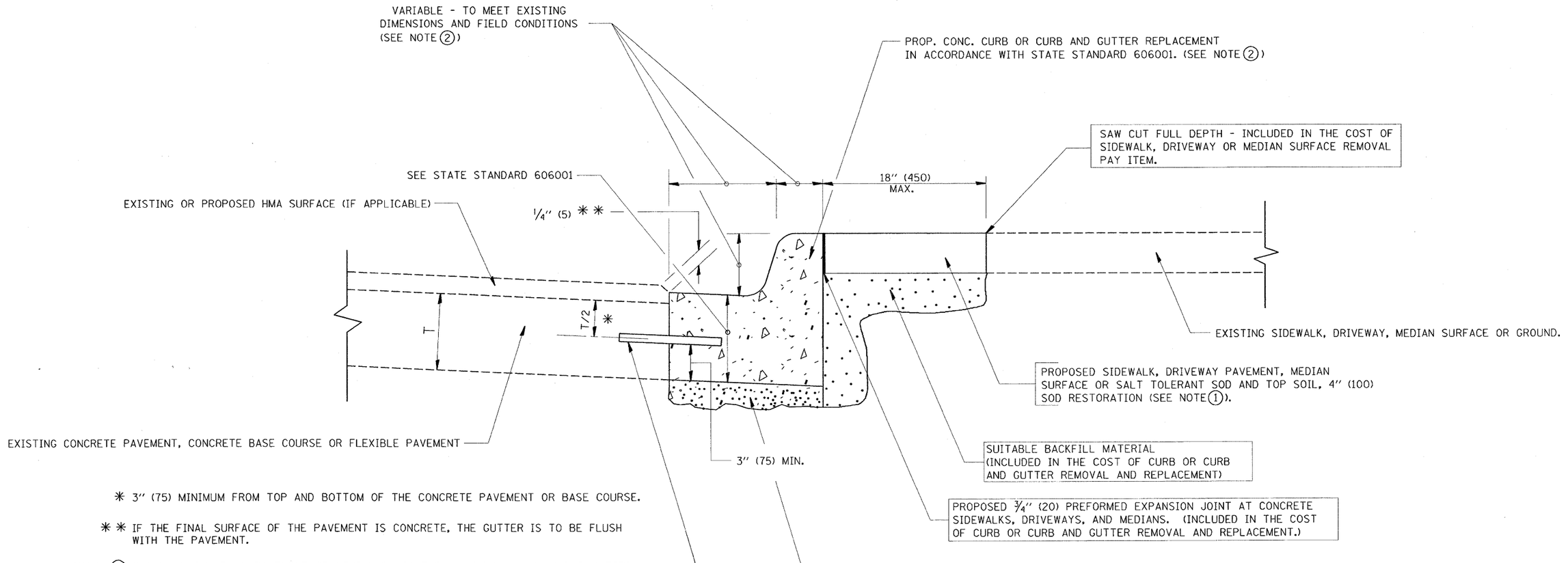
1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE FULL DEPTH PATCHES
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED - R. SHAH	REVISED - A. ABBAS 01-20-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - A. ABBAS 04-27-98			311	9Y-RS-6	DU PAGE	41	28
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - R. BORO 01-01-07				BD400-04 (BD-22)	CONTRACT NO. 60A85		
	PLOT DATE = 1/7/2009	DATE - 10-25-94	REVISED - R. BORO 09-04-07		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.	FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 4" (100) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(2) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(3) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(4) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(5) THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(7) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

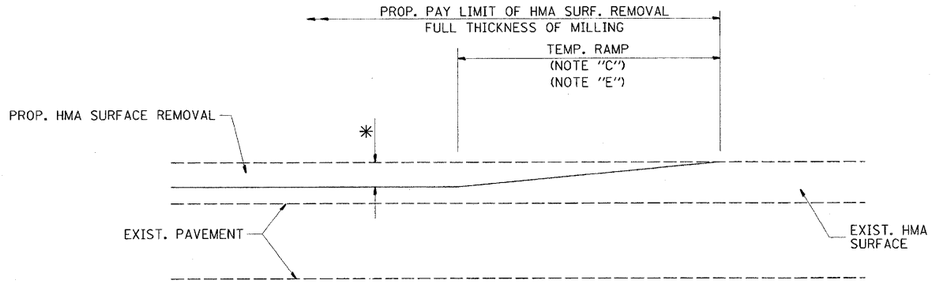
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design.dgn	USER NAME = steedpa	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
	PLOT SCALE = 50.0000 ' / IN.	DRAWN -	REVISED - A. ABBAS 03-21-97		
	PLOT DATE = 1/7/2009	CHECKED -	REVISED - M. GOMEZ 01-22-01		
		DATE - 03-11-94	REVISED - R. BORO 01-01-07		SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

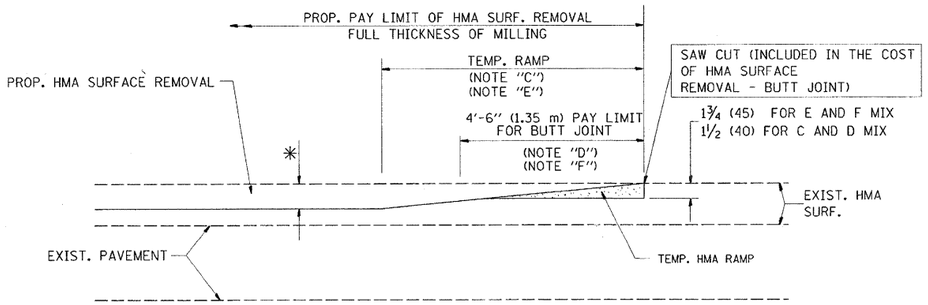
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	29
	BD600-06 (BD-24)	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

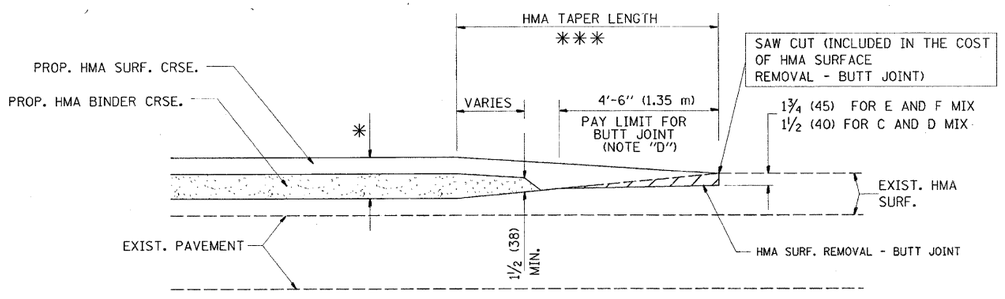


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

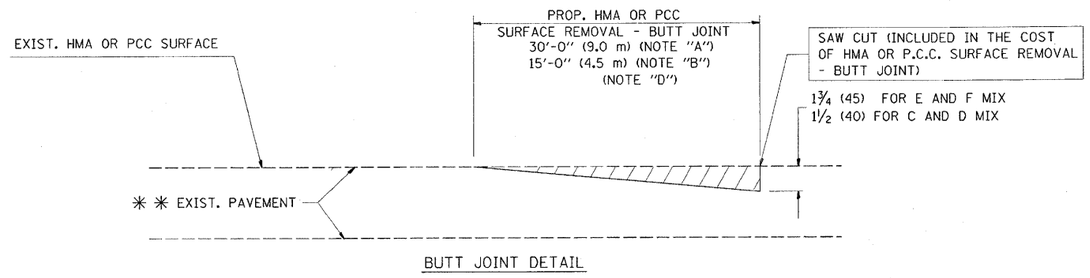
OPTION 2

TYPICAL TEMPORARY RAMP

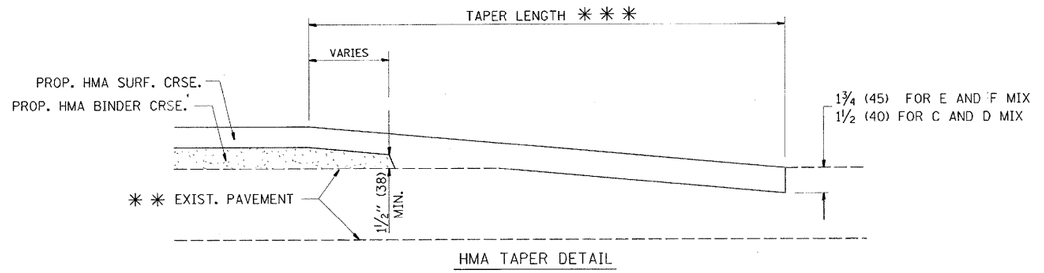


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

* * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

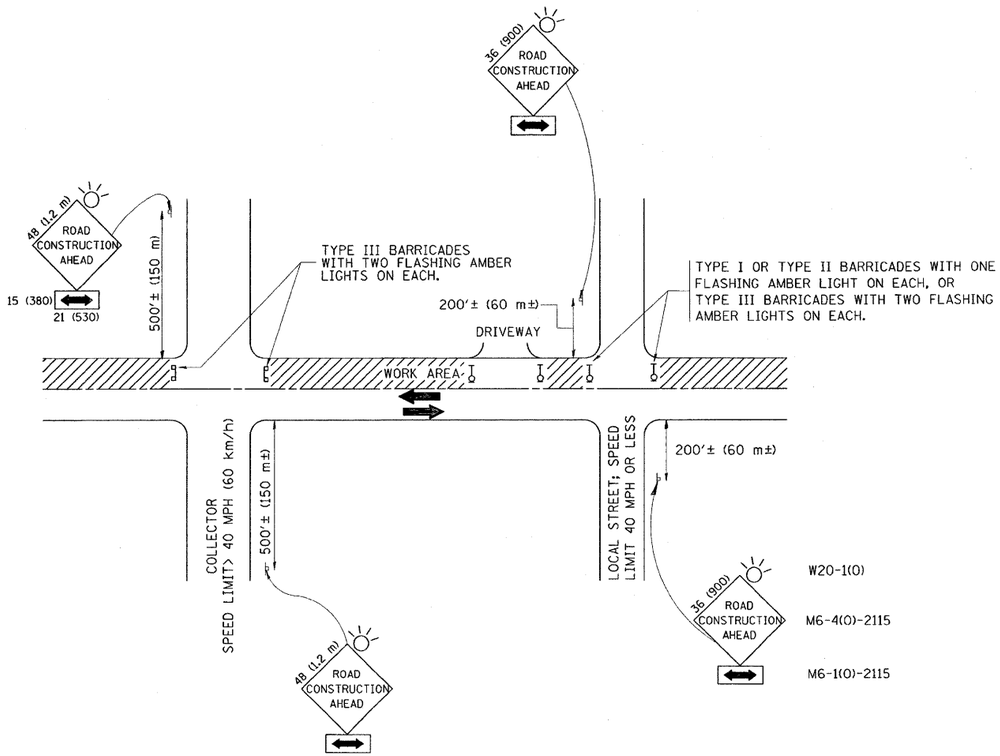
FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/7/2009	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	30
	BD400-05 BD32	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millmeters (inches) unless otherwise shown.

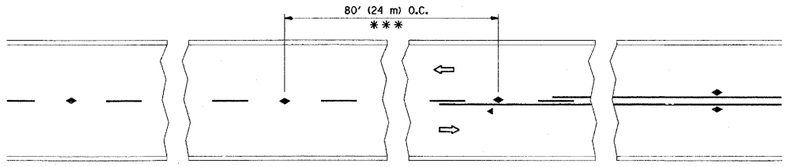
FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/7/2009	DATE - 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

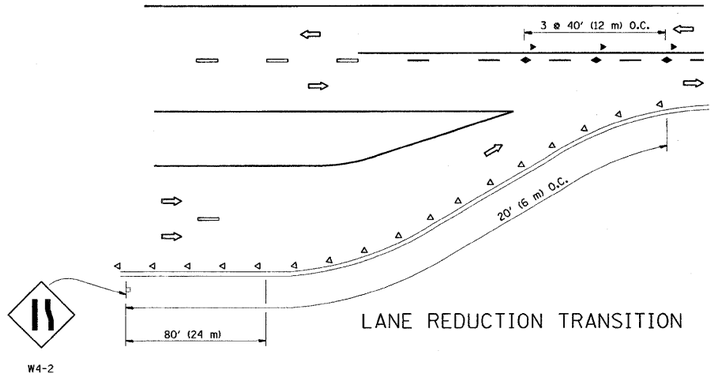
SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	31
	TC-10	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

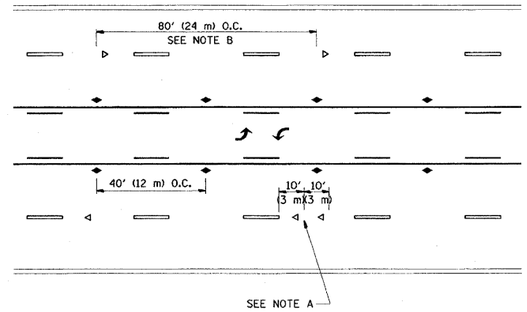


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

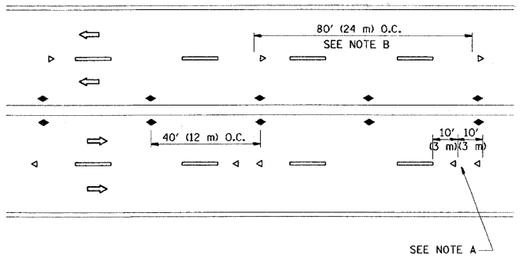
TWO-LANE/TWO-WAY



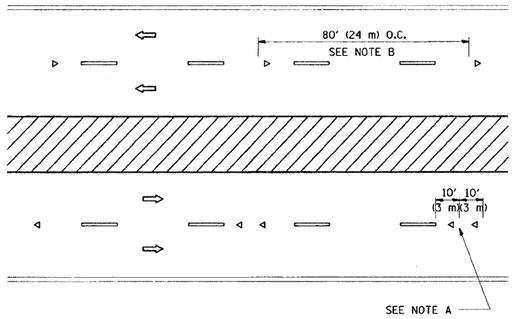
LANE REDUCTION TRANSITION



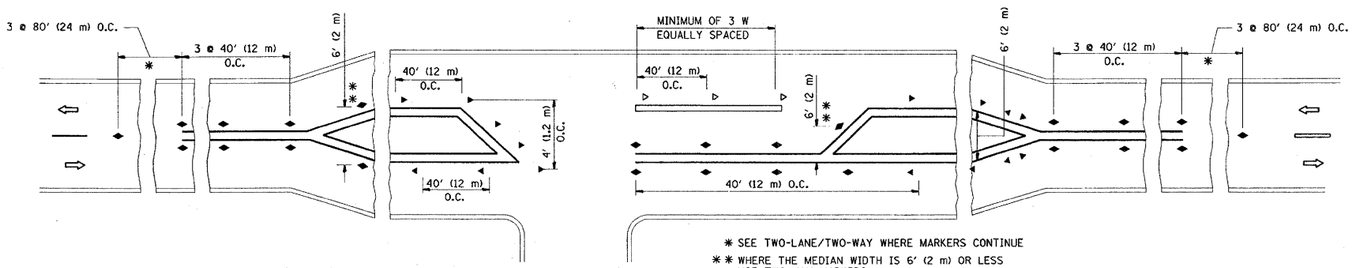
TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

LEFT TURN

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

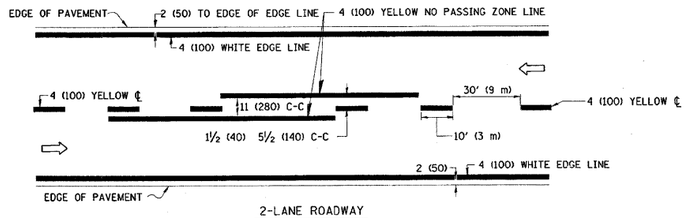
FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design.dgn	USER NAME = steedpa	DESIGNED -	REVISED - T. RAMMACHER 09-19-94
		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 1/7/2009	DATE	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

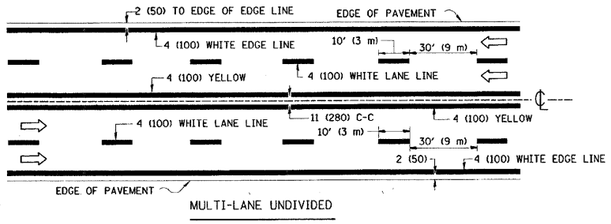
TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

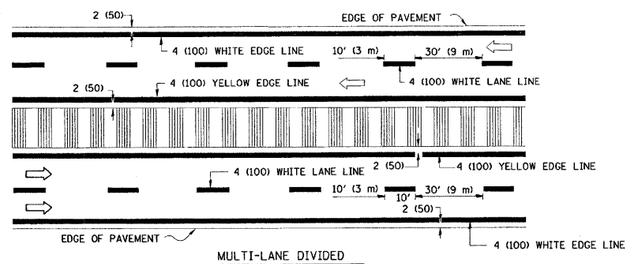
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	32
	TC-11	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



2-LANE ROADWAY



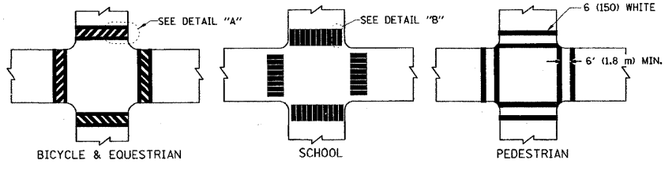
MULTI-LANE UNDIVIDED

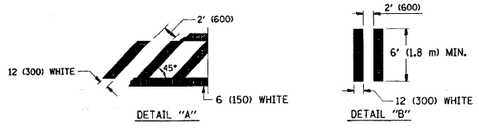


MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

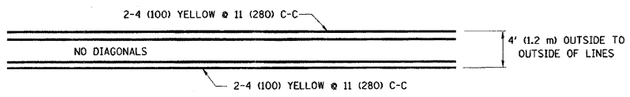
NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

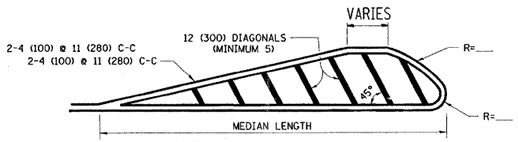




TYPICAL CROSSWALK MARKING



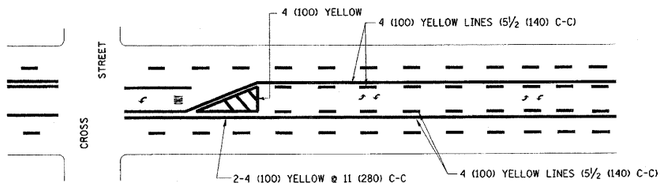
4' (1.2 m) WIDE MEDIANS ONLY



FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

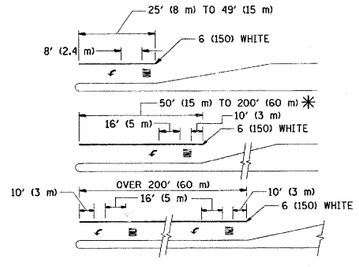
MEDIANS OVER 4' (1.2 m) WIDE



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

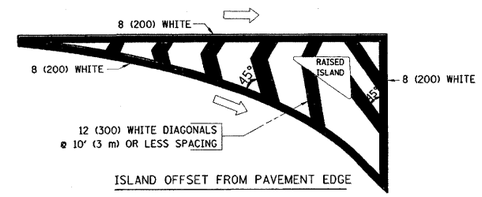


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) ONLY AREA = 20.8 SQ. FT. (1.9 m^2)

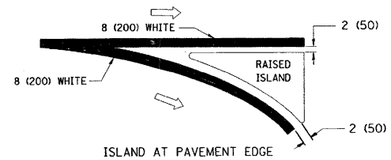
✳ TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVEDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

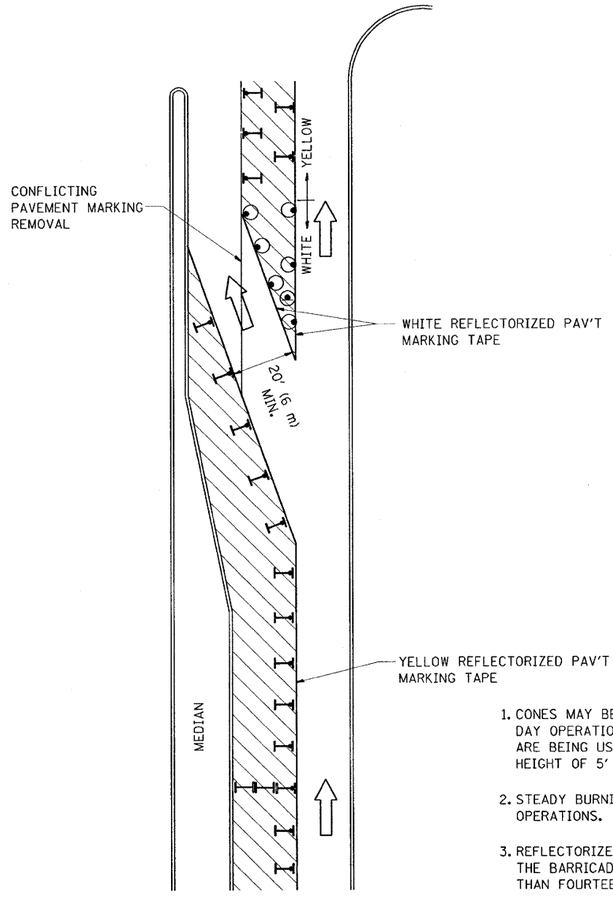
FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED - EVERS	REVISED -T. RAMMACHER 10-27-94
		DRAWN -	REVISED -A. HOUSEH 10-09-96
	PLOT SCALE = 50.0200 ' / IN.	CHECKED -	REVISED -A. HOUSEH 10-17-96
	PLOT DATE = 1/7/2009	DATE - 03-19-90	REVISED -T. RAMMACHER 01-06-00

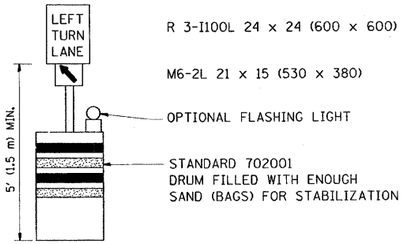
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	33
	TC-13	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		





GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

All dimensions are in inches (millimeters) unless otherwise shown.

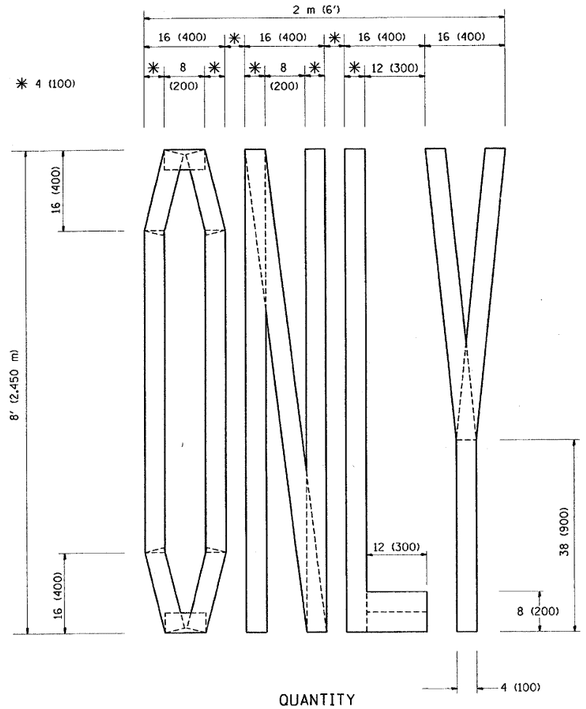
FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED -	REVISED -T. RAMMACHER 09-08-94
		DRAWN -	REVISED - A. HOUSEH 11-07-95
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - A. HOUSEH 10-12-96
	PLOT DATE = 1/7/2009	DATE -	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

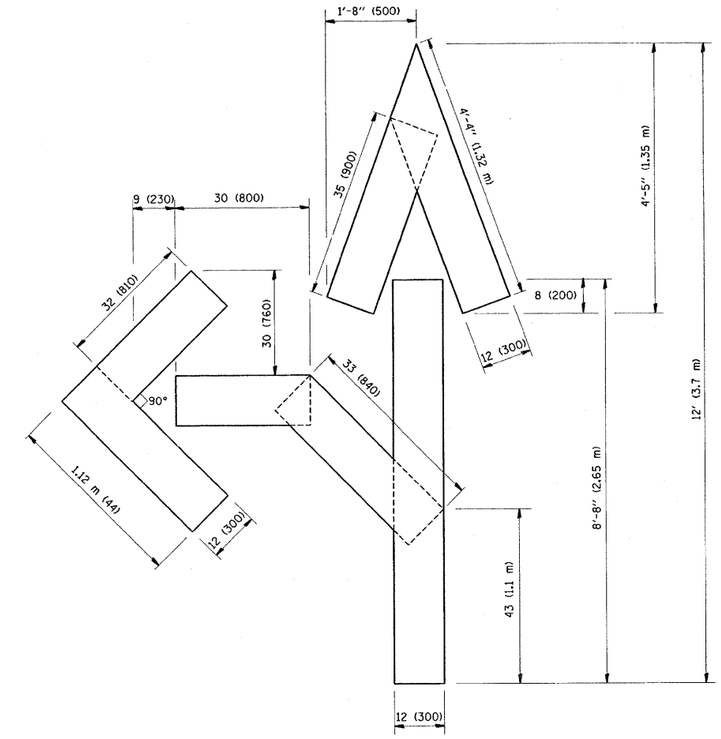
TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

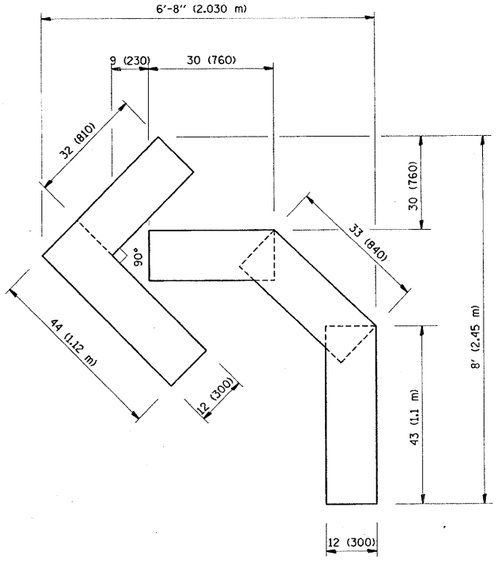
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	34
	TC-14	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
		DRAWN -	REVISED -T. RAMMACHER 11-04-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98
	PLOT DATE = 1/7/2009	DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	35
	TC-16	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.

2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.

3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.

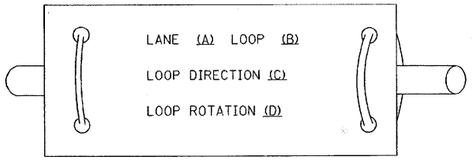
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.

5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.

6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.

7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

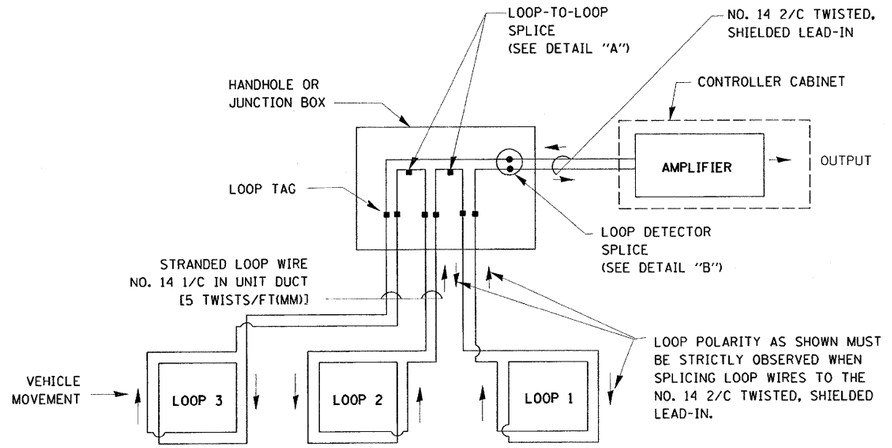


A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY

B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.

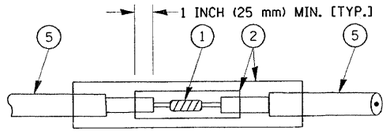
C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".

D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

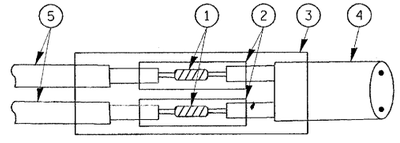


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.



DETAIL "A"
LOOP-TO-LOOP SPLICE



DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

(1) WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.

(2) WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.

(3) WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGHT 6" (150 mm), UNDERWATER GRADE.

(4) NO. 14 2/C TWISTED, SHIELDED CABLE.

(5) LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

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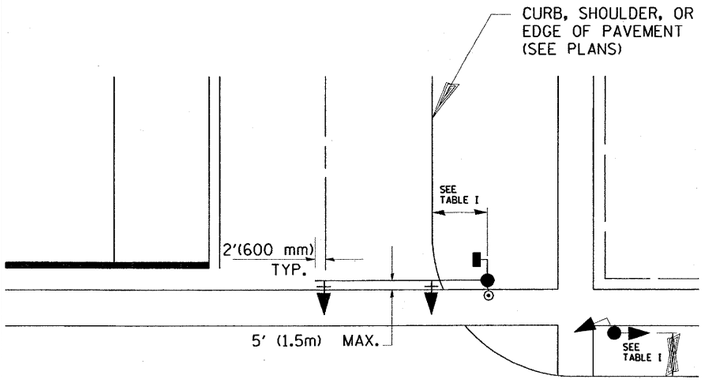
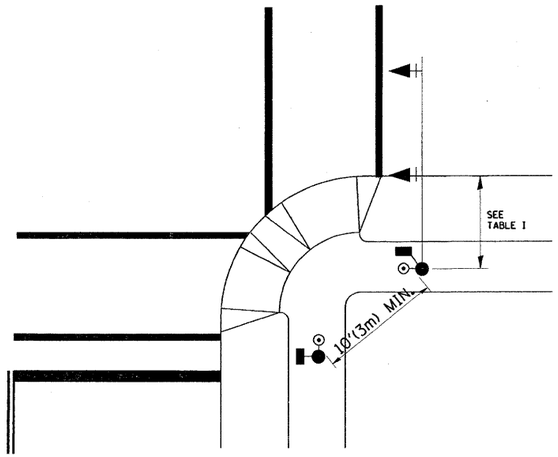
	PLOT SCALE = 50.0000 ' / IN.	DRAWN - R.W.P.	REVISED - BUR. TRAFFIC 01-01-02				COUNTY DU PAGE
	PLOT DATE = 1/7/2009	CHECKED - D.A.Z.	REVISED -				TOTAL SHEETS 41
		DATE - 05-30-00	REVISED -		SCALE: NONE SHEET NO. 1 OF 4 SHEETS STA. TO STA.		SHEET NO. 36
						TS-05	CONTRACT NO. 60A85
						FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT

TRAFFIC SIGNAL MAST ARM AND POST

MAST ARM MOUNTED SIGNAL IN PROPOSED & FUTURE SIDEWALK AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNAL AND PUSHBUTTON DETECTOR



PEDESTRIAN SIGNAL PUSHBUTTON



RECOMMENDED PUSHBUTTON LOCATIONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHALL BE IN ACCORDANCE WITH THE CURRENT MUTCD (SEE NOTE 1). TO MEET MUTCD REQUIREMENTS, PEDESTRIAN SIGNAL PUSHBUTTONS MAY HAVE TO BE MOUNTED ON A SEPARATE POST.

NOTES:

1. AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS WITH PEDESTRIAN ACTUATION, EACH PUSHBUTTON SHALL ACTIVATE BOTH THE WALK INTERVAL AND THE ACCESSIBLE PEDESTRIAN SIGNALS.

   AT ACCESSIBLE PEDESTRIAN SIGNAL LOCATIONS, PUSHBUTTONS SHOULD CLEARLY INDICATE WHICH CROSSWALK SIGNAL IS ACTUATED BY EACH PUSHBUTTON. PUSHBUTTONS AND TACTILE ARROWS SHOULD HAVE HIGH VISUAL CONTRAST (SEE THE DEPARTMENT OF JUSTICE'S AMERICANS WITH DISABILITIES ACT STANDARDS FOR ACCESSIBLE DESIGN, 1991). TACTILE ARROWS SHOULD POINT IN THE SAME DIRECTION AS THE ASSOCIATED CROSSWALK. AT CORNERS OF SIGNALIZED LOCATIONS WITH ACCESSIBLE PEDESTRIAN SIGNALS WHERE PEDESTRIAN PUSHBUTTONS ARE PROVIDED, THE PUSHBUTTONS SHOULD BE SEPARATED BY THE DISTANCE OF AT LEAST 10 FT (3m). THIS ENABLES PEDESTRIANS WHO HAVE VISUAL DISABILITIES TO DISTINGUISH AND LOCATE THE APPROPRIATE PUSHBUTTON.

   PUSHBUTTONS FOR ACCESSIBLE PEDESTRIAN SIGNALS SHOULD BE LOCATED AS FOLLOWS:

   A: ADJACENT TO A LEVEL ALL-WEATHER SURFACE TO PROVIDE ACCESS FROM A WHEELCHAIR, AND WHERE THERE IS AN ALL WEATHER SURFACE, WHEELCHAIR ACCESSIBLE ROUTE TO THE RAMP.

   B: WITHIN 5 FT (1.5m) OF THE CROSSWALK EXTENDED.

   C: WITHIN 10 FT (3m) OF THE EDGE OF CURB, SHOULDER, OR PAVEMENT.

   D: PARALLEL TO THE CROSSWALK TO BE USED (SEE MUTCD FIGURE 4E-2).

   E: NORMAL PEDESTRIAN PUSHBUTTON MOUNTING HEIGHT SHOULD BE 3.5 FT (1.05m) ABOVE ADJACENT SIDEWALK

2. PEDESTRIAN SIGNAL FACES SHALL BE MOUNTED WITH THE BOTTOM OF THE HOUSING NOT LESS THAN 8 FT (2.4m) NOR MORE THAN 10 FT (3.0m) ABOVE THE SIDEWALK LEVEL AND SO THERE IS A PEDESTRIAN INDICATION IN THE LINE OF PEDESTRIANS' VISION WHICH PERTAINS TO THE CROSSWALK BEING USED.

3. THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, NOT MOUNTED OVER A ROADWAY, SHALL BE AT LEAST 10 FT (3.0m) BUT NOT MORE THAN 15 FT (4.5m) ABOVE THE SIDEWALK OR, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE HIGHWAY IF NO SIDEWALKS EXIST.

4. THE BOTTOM OF THE HOUSING OF A VEHICLE SIGNAL FACE, MOUNTED OVER A ROADWAY, SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001 AND 877006. (16 FT (5m) MIN., 18 FT (5.5m) MAX., FROM HIGHEST POINT OF PAVEMENT)

PEDESTRIAN SIGNAL POST

PEDESTRIAN SIGNAL HEAD AND PEDESTRIAN PUSHBUTTON DETECTOR LOCATION

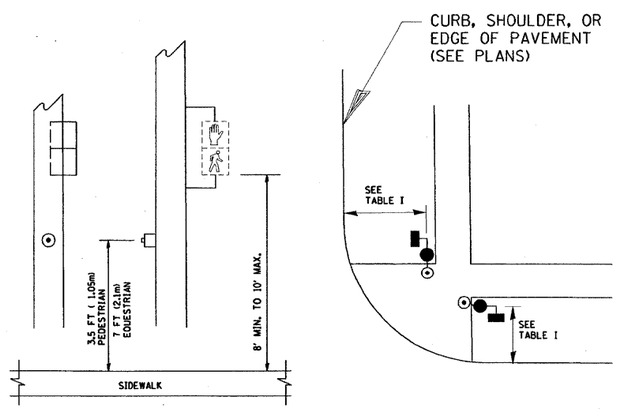


TABLE I

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MIN. DIST. FROM BACK OF CURB)	SHOULDER/NON-CURBED AREA (MIN. DIST. FROM EDGE OF PAVEMENT)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2FT(0.6m), MINIMUM 10FT(3.0m)
PEDESTRIAN PUSHBUTTON	SEE NOTE 1	SEE NOTE 1

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 2 OF 4 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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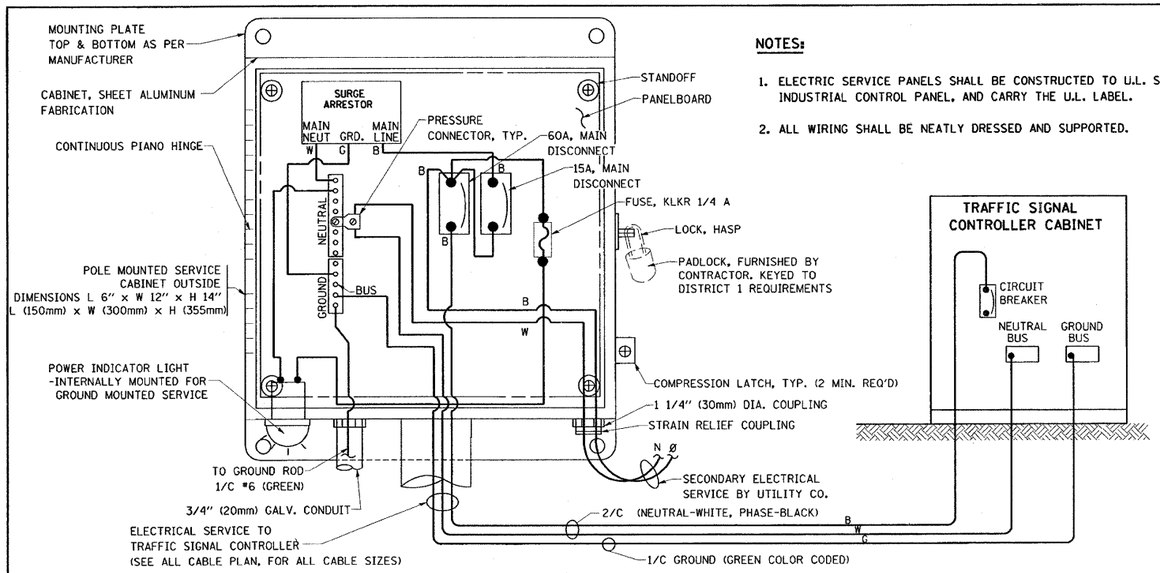
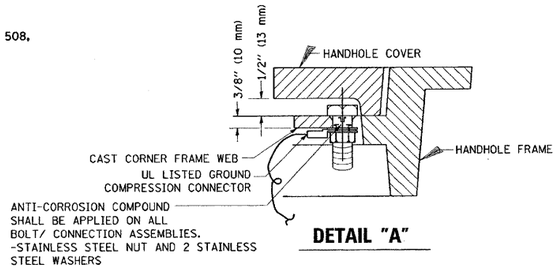
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



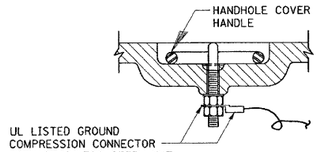
ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)

NOTES:

1. ELECTRIC SERVICE PANELS SHALL BE CONSTRUCTED TO U.L. STD 508, INDUSTRIAL CONTROL PANEL, AND CARRY THE U.L. LABEL.
2. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.



DETAIL "A"



DETAIL "B"

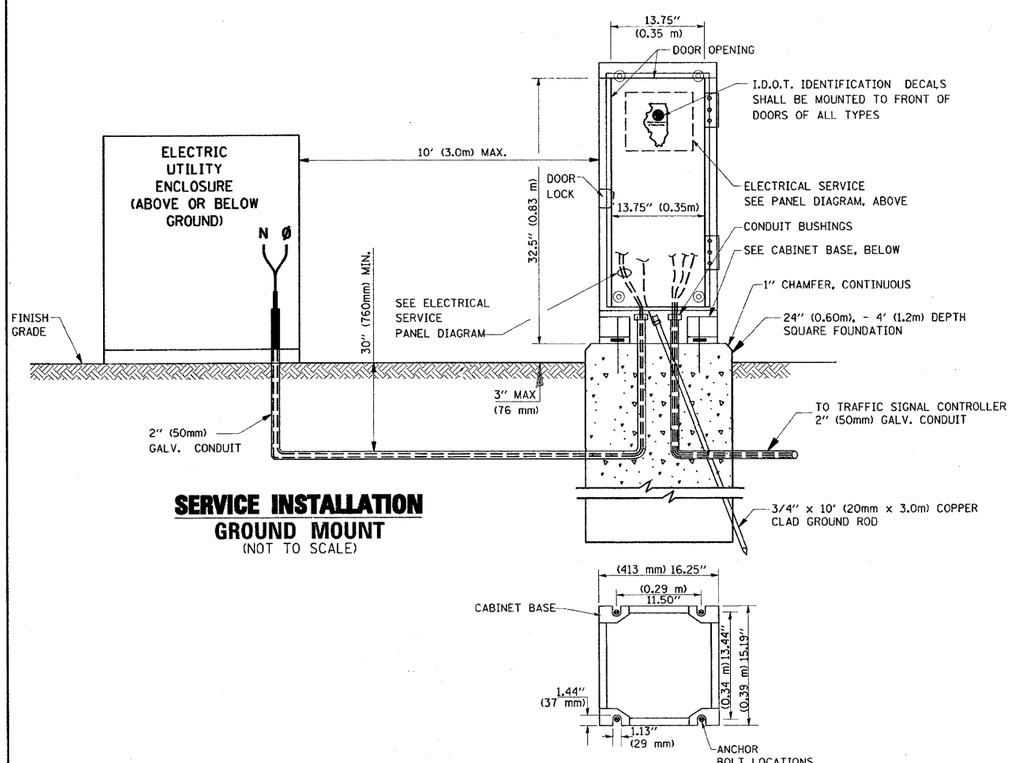
NOTES:

GROUNDING SYSTEM

1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.

SERVICE INSTALLATION POLE MOUNT (SHOWN)

(NOT TO SCALE)

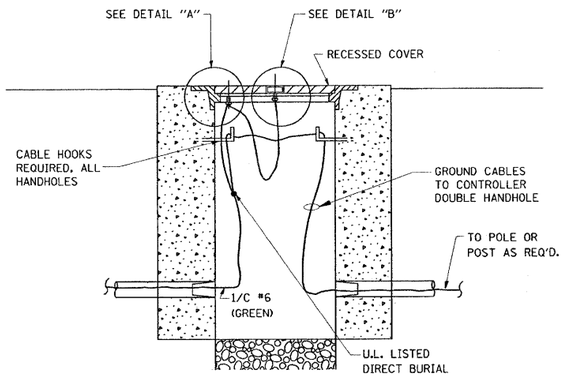


SERVICE INSTALLATION GROUND MOUNT

(NOT TO SCALE)

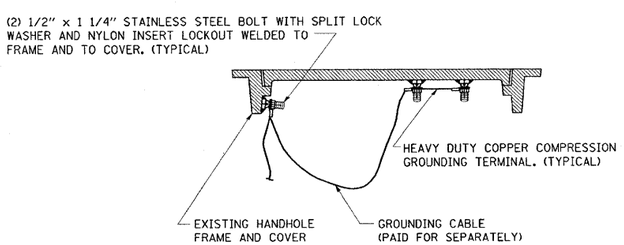
CABINET – BASE BOLT PATTERN

(NOT TO SCALE)



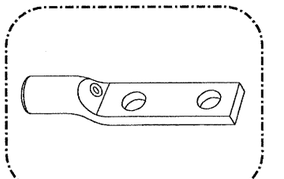
HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)

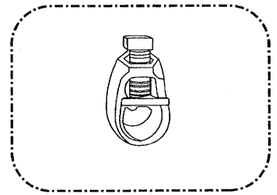


EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL

(NOT TO SCALE)



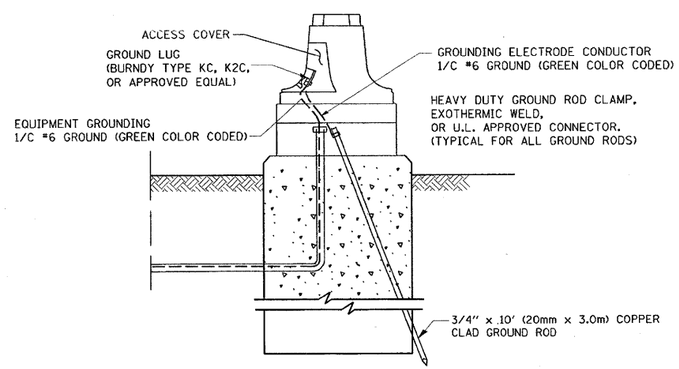
HEAVY-DUTY COMPRESSION TERMINAL (BURNDY TYPE YGHA OR APPROVED EQUAL)



3/4" (20mm) HEAVY-DUTY GROUND ROD CLAMP (BURNDY TYPE GRC OR APPROVED EUAL)

NOTES:

- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
- GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES
  6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES
  13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES.
  5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



MAST ARM POLE / POST–GROUNDING DETAIL

(NOT TO SCALE)

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	PLOT DATE = 1/7/2009	DATE - 05-30-00	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 3 OF 4 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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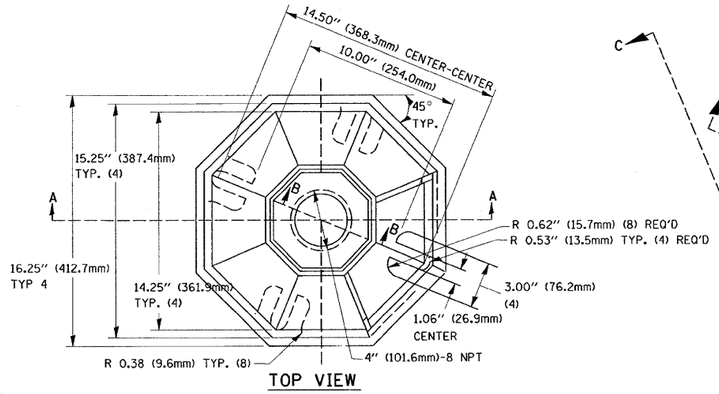
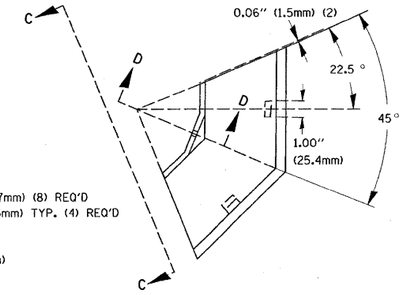
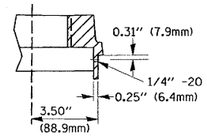
	TS-05	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



TOP VIEW





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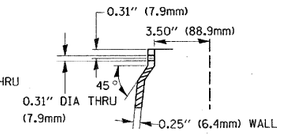

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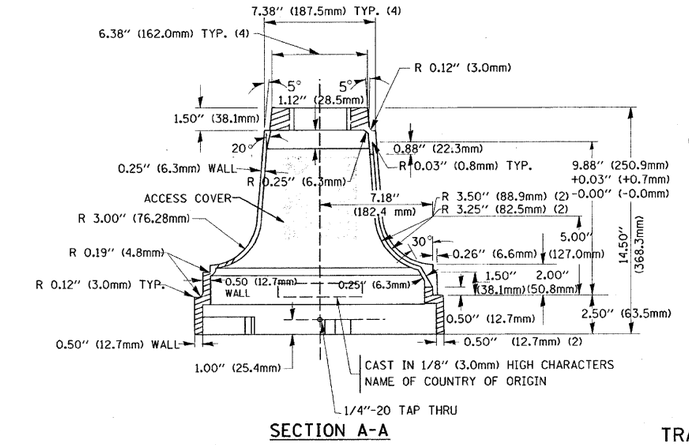

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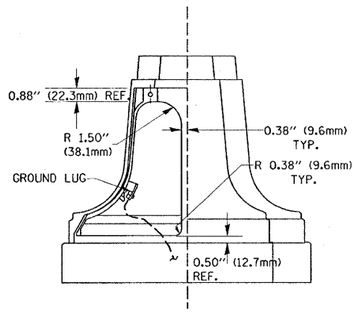
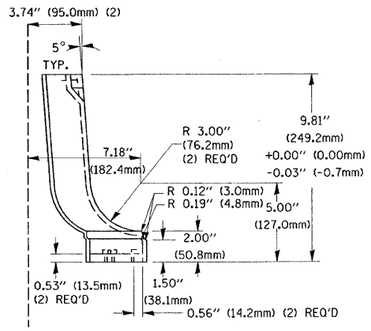
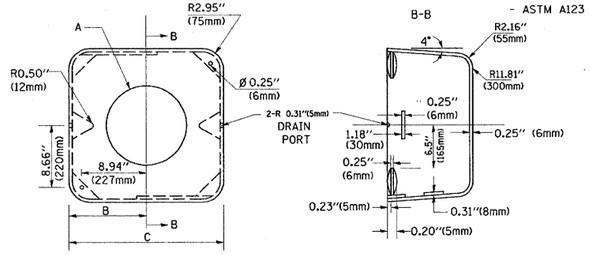


VIEW C-C



TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

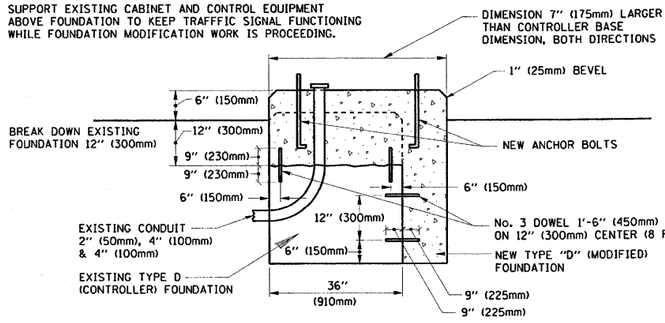
MATERIAL:
- ASTM A48 CLASS 30 GREY IRON
- ASTM A123 HOT DIPPED GALVANIZED



TYPE	A	B	C	HEIGHT	WEIGHT
I	Ø 10.125"(257mm)	9.5"(241mm)	19"(483mm)	12" (300mm)	24kg
II	Ø 11.125"(283mm)	10.75"(273mm)	21.5"(546mm)	12" (300mm)	26kg

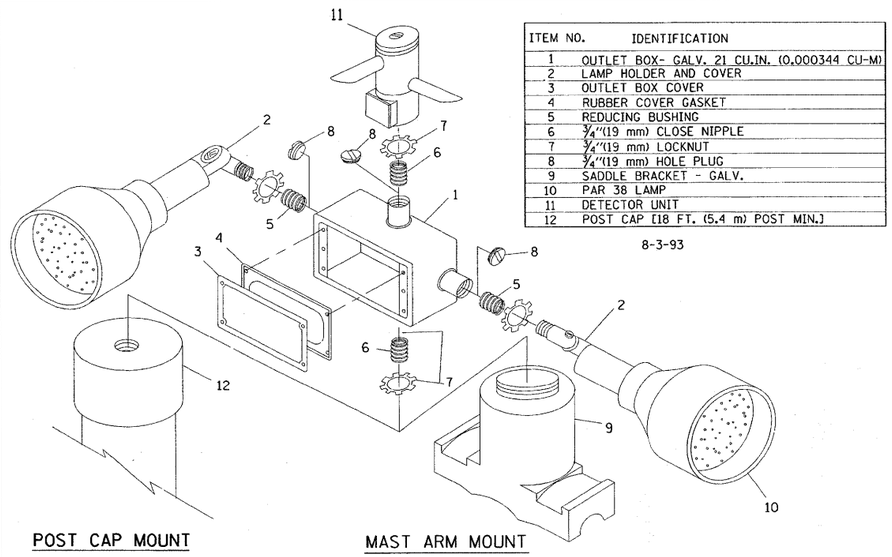
SHROUD DETAIL

NOTE:
SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION

(NOT TO SCALE)



ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	¾"(19 mm) CLOSE NIPPLE
7	¾"(19 mm) LOCKNUT
8	¾"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	PAR 38 LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

8-3-93

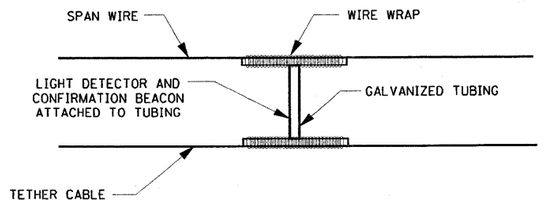
POST CAP MOUNT

MAST ARM MOUNT

EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL

NOTES:

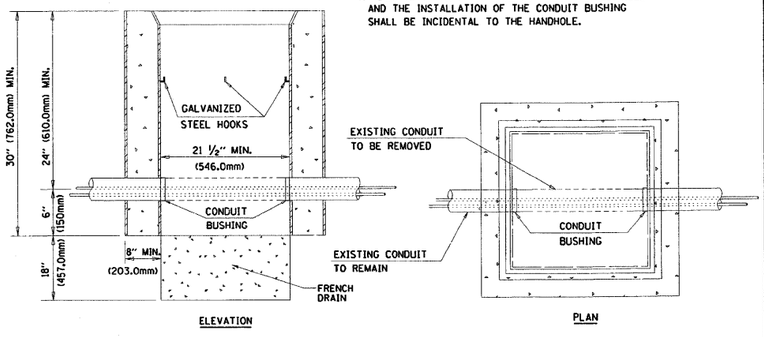
1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
   ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
   ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A ¾"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



LIGHT DETECTOR AND CONFIRMATION BEACON MOUNTING FOR TEMPORARY TRAFFIC SIGNALS

(NOT TO SCALE)

NOTES:
1. REMOVAL OF EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHING SHALL BE INCIDENTAL TO THE HANDHOLE.



DETAIL
HANDHOLE TO INTERCEPT EXISTING CONDUIT
N.T.S.

FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED - D.A.D.	REVISED - BUR.TRAFFIC 03-15-01	
		DRAWN - R.W.P.	REVISED - BUR.TRAFFIC 11-12-01	
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - D.A.Z.	REVISED - BUR.TRAFFIC 01-01-02	
	PLOT DATE = 1/7/2009	DATE - 05-30-00	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

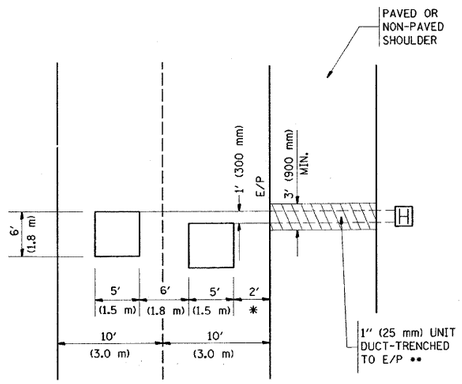
DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

SCALE: NONE | SHEET NO. 4 OF 4 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	39
	TS-05	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



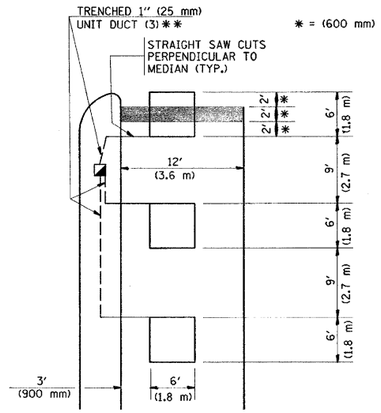
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

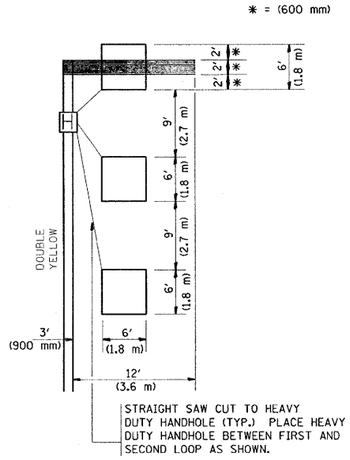


** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

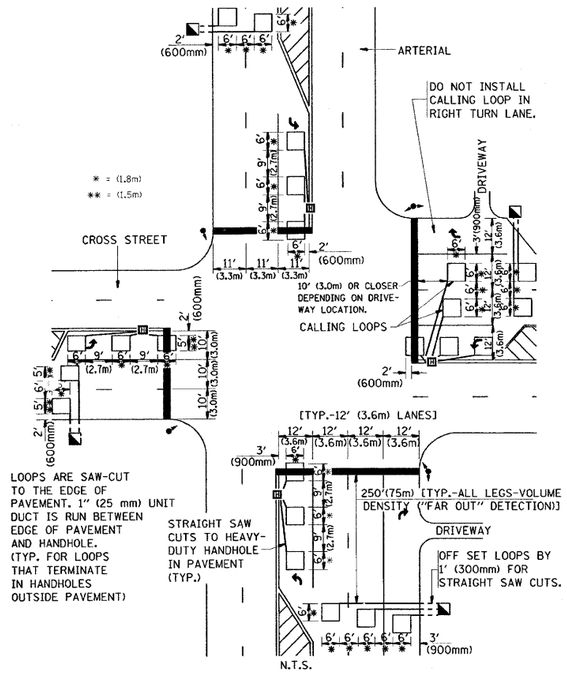
LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH

(PROTECTED / PERMITTED LEFT TURN PHASING)



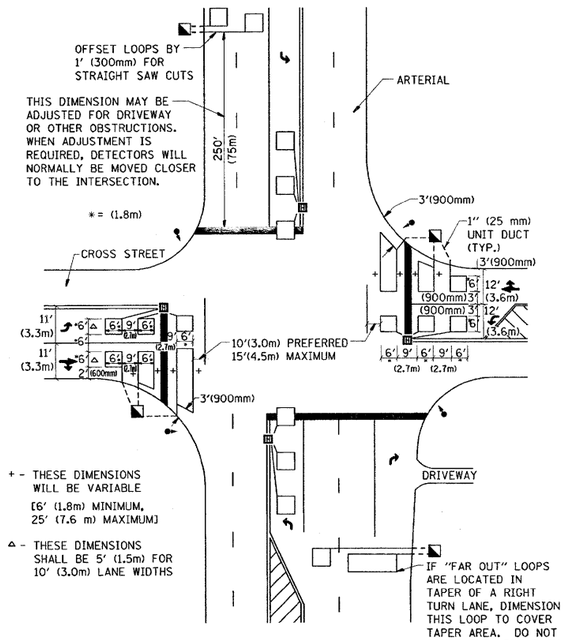
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



DETAIL 1
N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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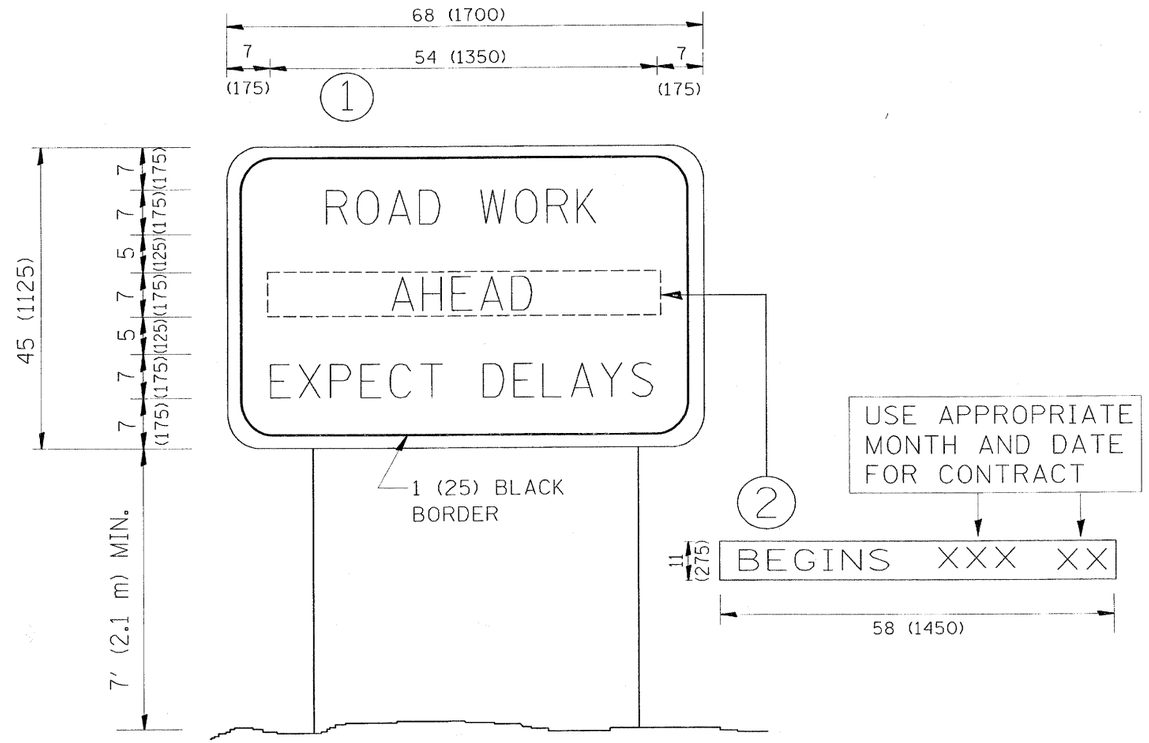
		DRAWN -	REVISED -
	PLOT SCALE = 50.0000 ' / IN.	CHECKED - R.K.F.	REVISED -
	PLOT DATE = 1/7/2009	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 – DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	40
	TS-07	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw_work\PWIDOT\STEEDPA\dms89735\design_aa.dgn	USER NAME = steedpa	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.0000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/7/2009	DATE -	REVISED - C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
311	9Y-RS-6	DU PAGE	41	41
	TC-22	CONTRACT NO. 60A85		
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		