

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-------------|----------|--------------------|-----------|
| 351 | 2020-172-BR | WILL | 29 | 1 |
| | | ILLINOIS | CONTRACT NO. 62M55 | |

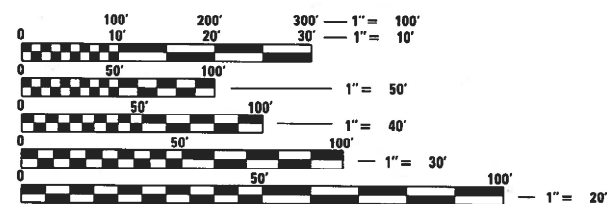
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN
CITY OF LOCKPORT

TRAFFIC DATA

2019 ADT: 23,100 VPD

POSTED SPEED LIMIT: 40 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-692-0123
OR 811

PROJECT ENGINEER: LUKASZ POCIECHA
PROJECT MANAGER: FAWAD AQUEEL

(847) 705-4255

CONTRACT NO. 62M55

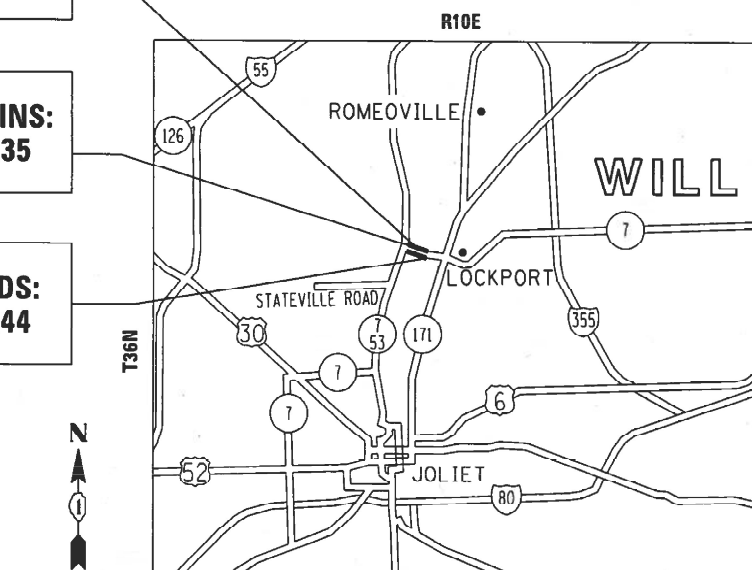
PROPOSED HIGHWAY PLANS

**F.A.P. ROUTE 351: IL. ROUTE 7
OVER EJ & E RAILROAD
SECTION: 2020-172-BR
PROJECT: NHPP-Z6S7(332)
BRIDGE DECK OVERLAY AND BRIDGE JOINT REPAIR
WILL COUNTY
C-91-368-20**

EX. STRUCTURE
SN 099-0274

PROJECT BEGINS:
STA. 03 + 14.35

PROJECT ENDS:
STA. 13 + 43.44



LOCATION MAP
(NOT TO SCALE)

LOCKPORT TOWNSHIP

GROSS LENGTH = 1029 FT. = 0.19 MILE
NET LENGTH = 1029 FT. = 0.19 MILE

TRANSMART
ROBERT L. PETERS, S.E.
#81-004697

Robert L. Peters
DATE: 12/08/2021

DATE: 12/08/2021

EXPIRATION DATE: 11-30-2022

TRANSMART
SRIJAN ADHIKARI, P.E.
#062-069784

Srijan Adhikari
DATE: 12/08/2021

DATE: 12/08/2021

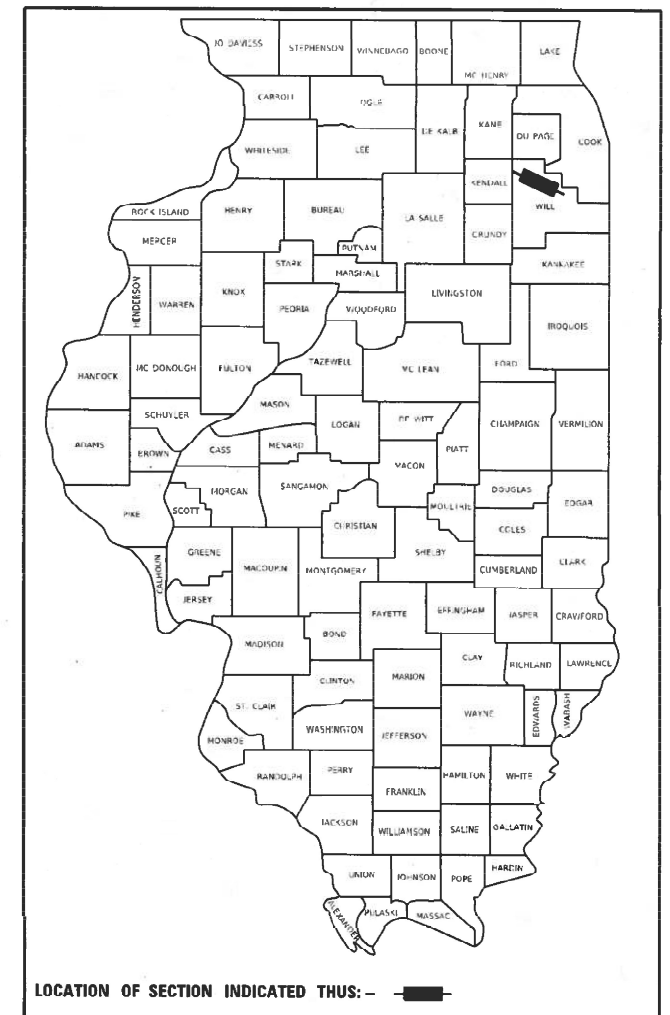
EXPIRATION DATE: 2-28-2022

TranSmart
100 South Wacker Drive Suite 400
Chicago, Illinois 60606

CONTACT: SRIJAN ADHIKARI

(312) 922-1700 EXT. 107

D-91-569-20



LOCATION OF SECTION INDICATED THUS: —

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED December 10, 2021
Jose Pinos REGIONAL ENGINEER
February 4, 2022
Stephen M. Fawad ENGINEER OF DESIGN AND ENVIRONMENT
February 4, 2022
Stephen M. Fawad DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

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MODEL: Default
FILE NAME: S:\Projects\2015 - ITD\PTB 196-016 - Infrastructure Engineer\Project Work\CADD\CADD Sheets\11 - 7 at EJE RRD 62MSS-shl-generate-01.dgn

| SUMMARY OF QUANTITIES | | | | | |
|-----------------------|------------|--|-------|-------------------|------------------------|
| SP. ITEM | CODE NO | ITEM | UNIT | TOTAL QUANTITY | CONSTR . CODE |
| | | | | | 80% FED / 20% STATE |
| | | | | | 0059 |
| | | | | | URBAN |
| | 20200100 | EARTH EXCAVATION | CU YD | 45 | 45 |
| | | | | | |
| | 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 1010 | 1010 |
| | | | | | |
| | 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 180 | 180 |
| | | | | | |
| | 40603085 | HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 | TON | 101 | 101 |
| | | | | | |
| | 40604062 | HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70 | TON | 188 | 188 |
| | | | | | |
| | 44003100 | MEDIAN REMOVAL | SQ FT | 5900 | 5900 |
| | | | | | |
| | 50102400 | CONCRETE REMOVAL | CU YD | 100.3 | 100.3 |
| | | | | | |
| | 50157300 | PROTECTIVE SHIELD | SQ YD | 560 | 560 |
| | | | | | |
| | 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 24.4 | 24.4 |
| | | | | | |
| | 50300260 | BRIDGE DECK GROOVING | SQ YD | 1159 | 1159 |
| | | | | | |
| | 50300300 | PROTECTIVE COAT | SQ YD | 1186 | 1186 |
| | | | | | |
| | 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 3000 | 3000 |
| | | | | | |
| | 50800515 | BAR SPLICERS | EACH | 52 | 52 |
| | | | | | |
| | 52000110 | PREFORMED JOINT STRIP SEAL | FOOT | 144 | 144 |
| | | | | | |
| | 67100100 | MOBILIZATION | L SUM | 1 | 1 |
| | | | | | |
| | 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | SQ FT | 4785 | 4785 |

| SUMMARY OF QUANTITIES | | | | | |
|-----------------------|------------|---|-------|-------------------|------------------------|
| SP. ITEM | CODE NO | ITEM | UNIT | TOTAL QUANTITY | CONSTR . CODE |
| | | | | | 80% FED / 20% STATE |
| | | | | | 0059 |
| | | | | | URBAN |
| | 70307100 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE | SQ FT | 110 | 110 |
| | | | | | |
| | 70307120 | TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE | FOOT | 14026 | 14026 |
| | | | | | |
| | 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 500 | 500 |
| | | | | | |
| | 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 525 | 525 |
| | | | | | |
| | 70600255 | IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2 | EACH | 2 | 2 |
| | | | | | |
| | 70600322 | IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2 | EACH | 2 | 2 |
| | | | | | |
| * | 72400100 | REMOVE SIGN PANEL ASSEMBLY - TYPE A | EACH | 4 | 4 |
| | | | | | |
| * | 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 218 | 218 |
| | | | | | |
| * | 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 5617 | 5617 |
| | | | | | |
| * | 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 675 | 675 |
| | | | | | |
| * | 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 460 | 460 |
| | | | | | |
| * | 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 53 | 53 |
| | | | | | |
| * | 78009004 | MODIFIED URETHANE PAVEMENT MARKING - LINE 4" | FOOT | 933 | 933 |
| | | | | | |
| * | 78009006 | MODIFIED URETHANE PAVEMENT MARKING - LINE 6" | FOOT | 69 | 69 |
| | | | | | |
| * | 78009012 | MODIFIED URETHANE PAVEMENT MARKING - LINE 12" | FOOT | 52 | 52 |
| | | | | | |
| * | 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 53 | 53 |

TranSmart

100 South Wacker Drive Suite 400 Chicago, Illinois 60606

USER NAME = kchamberlain

DESIGNED = HDU

DRAWN = HDU

CHECKED = SA

PLOT DATE = 12/14/2021

REVISED =

REVISED =

REVISED =

REVISED =

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

IL RTE. 7 (IL RTE. 53 TO OLD 9TH ST.)

SUMMARY OF QUANTITIES

F.A.P. RTE.

SECTION

COUNTY

TOTAL SHEETS

SHEET NO.

SCALE:

SHEET 1 OF 2 SHEETS

STA.

TO STA.

351

2020-172-BR

WILL

29

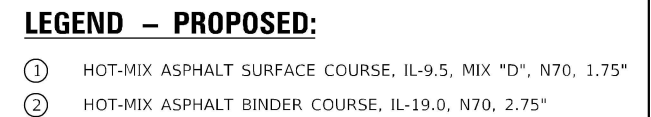
3

CONTRACT NO. 62M55

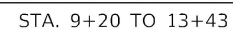
ILLINOIS

FED. AID PROJECT

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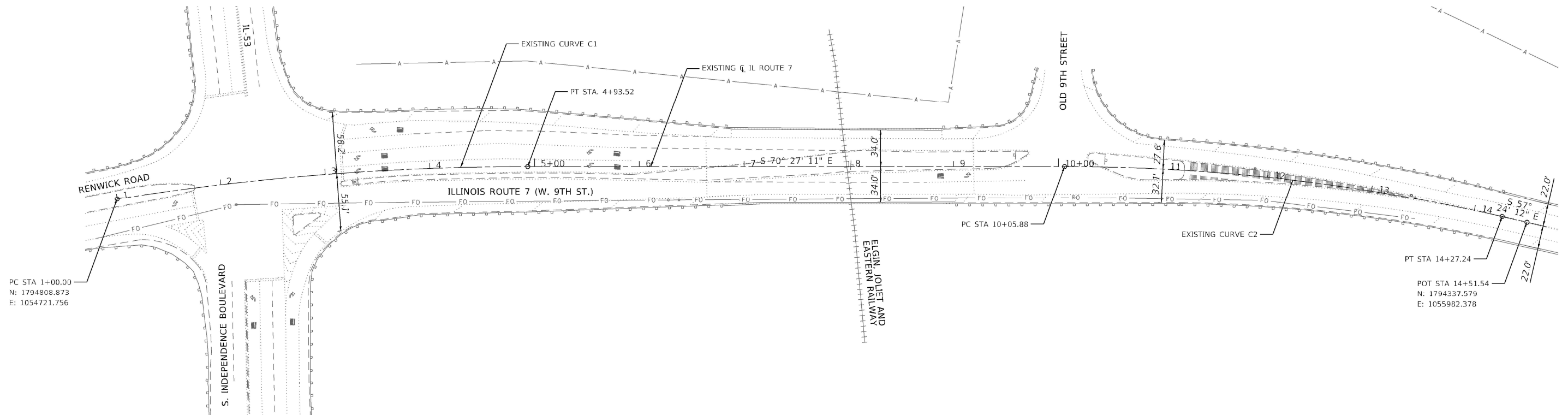
STA. 3+14 TO 6+64
BRIDGE OMISSION: STA. 6+64 TO 9+20
(SEE BRIDGE PLANS FOR BRIDGE WORK)



NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON -POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS

| | | | | | | | | | | | | |
|----------------------------|------------------|-----------|---|--|-------|----|--------|--------------------|-------------|--------|--------------|-----------|
| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL RTE. 7 (IL RTE. 53 TO OLD 9TH ST.) PROPOSED TYPICAL SECTIONS | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | DRAWN - HDU | REVISED - | | | | | | 351 | 2020-172-BR | WILL | 29 | 6 |
| PLOT SCALE = 1.0000' / in. | CHECKED - SA | REVISED - | | | | | | CONTRACT NO. 62M55 | | | | |
| PLOT DATE = 12/8/2021 | DATE = 12/8/2021 | REVISED - | | SCALE: | SHEET | OF | SHEETS | STA. | TO STA. | | | |

| | |
|----------------------------|-----------------------------|
| EXIST. CURVE C1 | EXIST. CURVE C2 |
| PI STA. = 2+97.17 | PI STA. = 12+17.47 |
| Δ = 9° 01' 08" (RT) | Δ = 13° 02' 59" (RT) |
| D = 2° 17' 31" | D = 3° 05' 49" |
| R = 2,500.00' | R = 1,850.00' |
| T = 197.17' | T = 211.60' |
| L = 393.52' | L = 421.36' |
| E = 7.76' | E = 12.06' |
| e = N/A | e = N/A |
| T.R. = N/A | T.R. = N/A |
| S.E. RUN = N/A | S.E. RUN = N/A |
| P.C. STA. = 1+00.00 | P.C. STA. = 10+05.88 |
| P.T. STA. = 4+93.52 | P.T. STA. = 14+27.24 |



| | | |
|------------------------------|------------------|-----------|
| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 100.0000' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

**IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.)
ALIGNMENT PLAN**

| | | | | | |
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| SCALE: | SHEET | OF | SHEETS | STA. | TO STA. |
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|----------------|-------------|--------------------|-----------------|--------------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 351 | 2020-172-BR | WILL | 29 | 7 |
| | | CONTRACT NO. 62M55 | | |
| ILLINOIS | | FED. AID PROJECT | | |

MAINTENANCE OF TRAFFIC AND
CONSTRUCTION STAGING GENERAL NOTES

1.
- ALL EXISTING SIGNAGE SHALL BE MAINTAINED AND VISIBLE TO TRAFFIC DURING CONSTRUCTION OR AS OTHERWISE SHOWN ON THE PLANS.
2.
- SHOULD CONSTRUCTION CONTRACTS BE ONGOING IMMEDIATELY ADJACENT TO THE WORK SITE, THE CONTRACTOR SHALL COORDINATE MAINTENANCE OF TRAFFIC EFFORTS ACCORDINGLY.

CONSTRUCTION STAGING DESCRIPTIONS

STAGE 1

1. REMOVE EXISTING CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLAN - STAGE 1.
2. SHIFT IL ROUTE 7 WESTBOUND TRAFFIC TO THE NORTH ADJACENT TO THE EXISTING BRIDGE PARAPET. SHIFT IL ROUTE 7 EASTBOUND TRAFFIC TO THE SOUTH ADJACENT TO THE EXISTING BRIDGE PARAPET.
3. REMOVE EXISTING SIGNAGE, BARRIER AND CORRUGATED MEDIAN THROUGHOUT THE PROJECT LIMITS.
4. HYDROSCARIFY AND OVERLAY EXISTING BRIDGE WITHIN LIMITS OF WORK ZONE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS.
5. CONSTRUCT NEW FLUSH MEDIAN THROUGHOUT THE PROJECT LIMITS.

STAGE 2

1. PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLAN - STAGE 2.
2. SHIFT IL ROUTE 7 WESTBOUND AND EASTBOUND TRAFFIC TO THE SOUTH.
3. HYDROSCARIFY AND OVERLAY NORTHERN SECTION OF THE EXISTING BRIDGE WITHIN LIMITS OF WORK ZONE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.

STAGE 3

1. PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLAN - STAGE 3.
2. SHIFT IL ROUTE 7 WESTBOUND AND EASTBOUND TRAFFIC TO THE NORTH.
3. HYDROSCARIFY AND OVERLAY REMAINING SOUTHERN SECTION OF THE EXISTING BRIDGE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.
4. INSTALL PERMANENT PAVEMENT MARKINGS.

MODEL: Default
FILE NAME: 330-Projects2015 - IDOT PTB 196-016 - Infrastructure Engineer\Project Work\CADD\CADD Sheets\ID162M55 - IL 7 at E/E RRD 162M55-shr\MOT-Notes-01.dgn



100 South Wacker Drive Suite 400
Chicago, Illinois 60606

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|-----------------------------|------------------|-----------|
| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

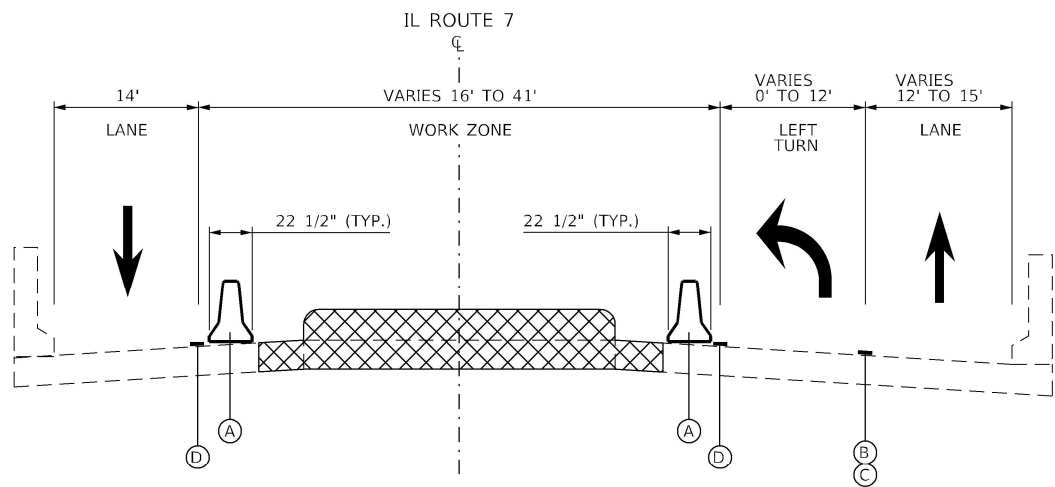
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.)
MOT AND CONSTRUCTION STAGING GENERAL NOTES

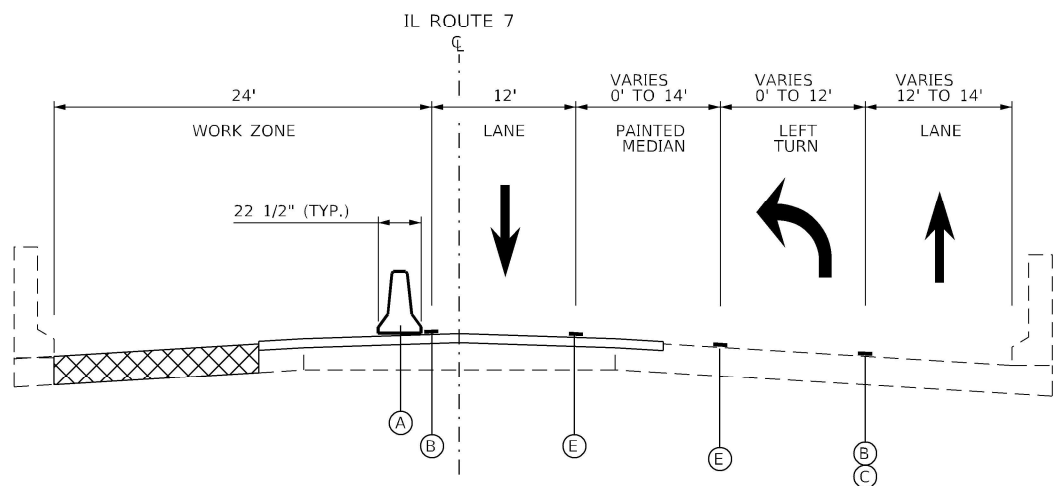
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| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| CONTRACT NO. 62M55 | | | | |
| | | ILLINOIS | FED. AID PROJECT | |

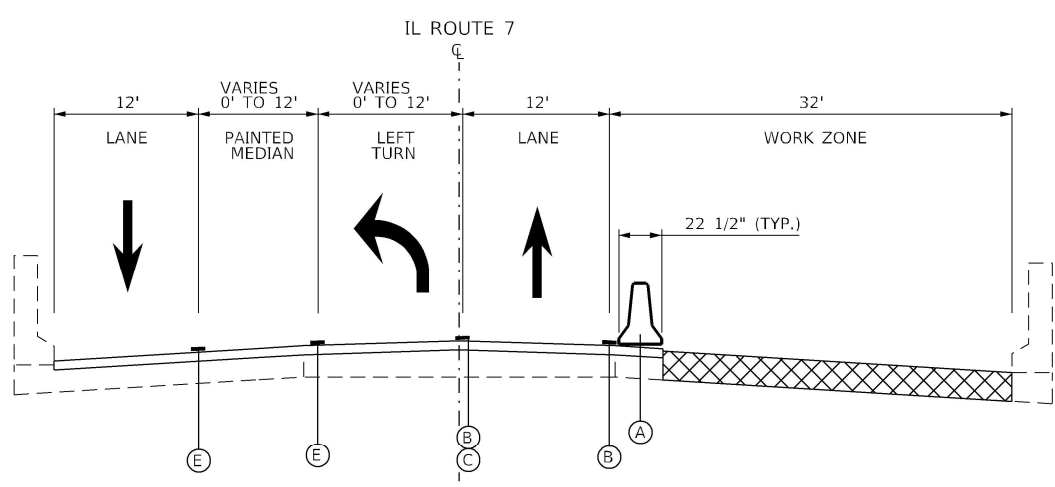
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STAGE 1
ALONG EX IL 7 BRIDGE



STAGE 2
ALONG EX IL 7 BRIDGE



STAGE 3
ALONG EX IL 7 BRIDGE

LEGEND

- (A) TEMPORARY CONCRETE BARRIER
- (B) TEMPORARY PAVEMENT MARKING, 4" SOLID WHITE
- (C) TEMPORARY PAVEMENT MARKING, 4" DOTTED WHITE
- (D) TEMPORARY PAVEMENT MARKING, 4" SOLID YELLOW
- (E) TEMPORARY PAVEMENT MARKING, 4" DOUBLE YELLOW
- WORK ZONE

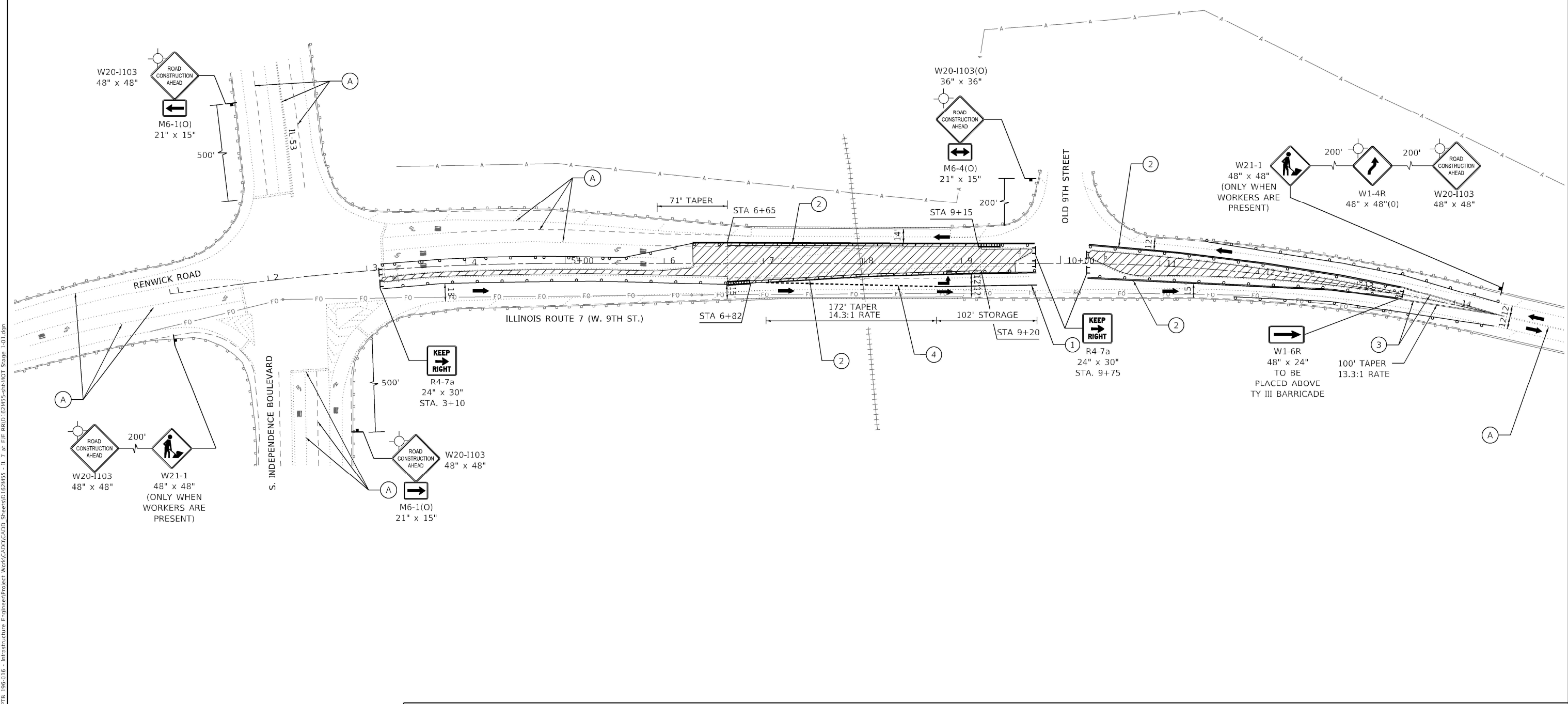
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100 South Wacker Drive Suite 400
Chicago, Illinois 60606

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| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
| | DRAWN - HDU | REVISED - |
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| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

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|--|-------|----|--------|
| IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.) | | | |
| MAINTENANCE OF TRAFFIC TYPICAL SECTIONS | | | |
| SCALE: | SHEET | OF | SHEETS |
| | STA. | TO | STA. |

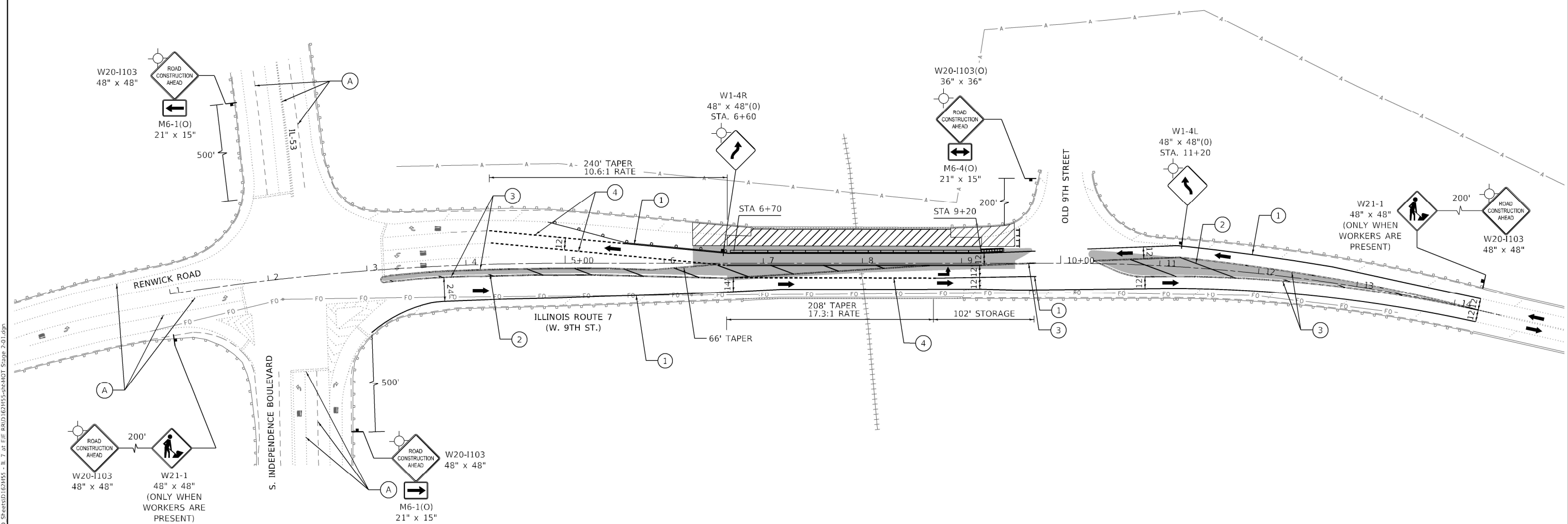
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|--------------------|-------------|----------|------------------|-----------|
| 351 | 2020-172-BR | WILL | 29 | 9 |
| CONTRACT NO. 62M55 | | | | |
| | | ILLINOIS | FED. AID PROJECT | |



MOT LEGEND:

- ① TEMPORARY PAVEMENT MARKING, 4" (SOLID WHITE) ② TEMPORARY PAVEMENT MARKING, 4" (SOLID YELLOW) ③ TEMPORARY PAVEMENT MARKING, 4" (DOUBLE YELLOW CENTERLINE, 4" X 2") ④ TEMPORARY PAVEMENT MARKING, 4" (DOTTED WHITE, 2' LINE - 6' GAP) A EXISTING PAVEMENT MARKING
- WORK ZONE TYPE II BARRICADES OR DRUMS WITH STEADY BURNING MONO-DIRECTIONAL LIGHT (50' C-C) COMPLETED IMPROVEMENTS DIRECTION OF TRAFFIC FLOW TYPE III BARRICADE TEMPORARY CONCRETE BARRIER TEMPORARY IMPACT ATTENUATOR

| | | | | | | | | | | | | |
|---|------------------------------|-------|------------------|-----------|---|---|--|---------------------------|-------------|--------|--------------|-----------|
| TranSmart 100 South Wacker Drive Suite 400 Chicago, Illinois 60606 | USER NAME = HUMLAUF | | DESIGNED - HDU | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.) MOT STAGE 1 PLAN | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | DRAWN - HDU | | REVIS | REVISED - | | | | 351 | 2020-172-BR | WILL | 29 | 10 |
| | PLOT SCALE = 100,0000' / in. | | CHECKED - SA | REVISED - | | | | CONTRACT NO. 62M55 | | | | |
| | PLOT DATE = 12/8/2021 | | DATE - 12/8/2021 | REVISED - | | | | ILLINOIS FED. AID PROJECT | | | | |
| SCALE: | | SHEET | OF | SHEETS | STA. | TO STA. | | | | | | |



MOT LEGEND:

- WORK ZONE
- TYPE II BARRICADES OR DRUMS WITH STEADY BURNING MONO-DIRECTIONAL LIGHT (50' C-C)
- COMPLETED IMPROVEMENTS
- DIRECTION OF TRAFFIC FLOW
- TYPE III BARRICADE
- TEMPORARY CONCRETE BARRIER
- TEMPORARY IMPACT ATTENUATOR
- ① TEMPORARY PAVEMENT MARKING, 4" (SOLID WHITE)
- ② TEMPORARY PAVEMENT MARKING, 4" (SOLID YELLOW)
- ③ TEMPORARY PAVEMENT MARKING, 4" (DOUBLE YELLOW CENTERLINE, 4" X 2)
- ④ TEMPORARY PAVEMENT MARKING, 4" (DOTTED WHITE, 2' LINE - 6' GAP)
- ⑤ EXISTING PAVEMENT MARKING

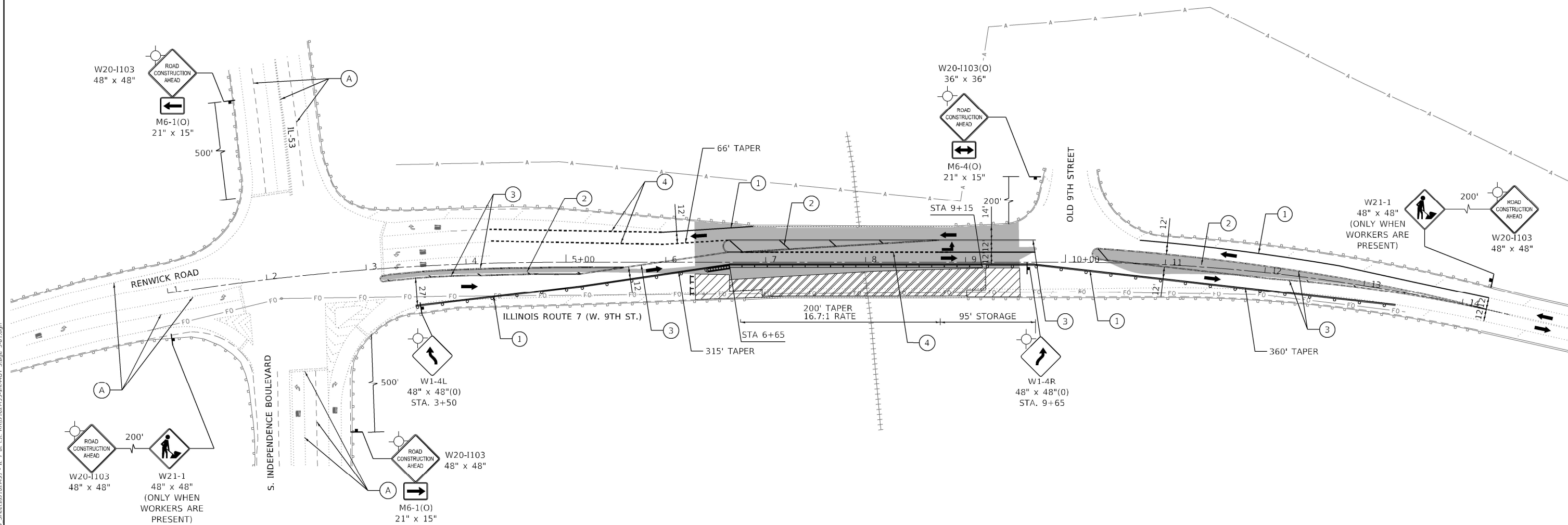
TranSmart
100 South Wacker Drive Suite 400
Chicago, Illinois 60606

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| | DRAWN - HDU | REVISED - |
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| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | |
|---|-------|----|--------|
| IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.) | | | |
| MOT STAGE 2 PLAN | | | |
| SCALE: | SHEET | OF | SHEETS |
| | STA. | TO | STA. |

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 351 | 2020-172-BR | WILL | 29 | 11 |
| CONTRACT NO. 62M55 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



MOT LEGEND:

- ① TEMPORARY PAVEMENT MARKING, 4" (SOLID WHITE) ② TEMPORARY PAVEMENT MARKING, 4" (SOLID YELLOW) ③ TEMPORARY PAVEMENT MARKING, 4" (DOUBLE YELLOW CENTERLINE, 4" X 2") ④ TEMPORARY PAVEMENT MARKING, 4" (DOTTED WHITE, 2' LINE - 6' GAP) A EXISTING PAVEMENT MARKING
- WORK ZONE TYPE II BARRICADES OR DRUMS WITH STEADY BURNING MONO-DIRECTIONAL LIGHT (50' C-C) COMPLETED IMPROVEMENTS DIRECTION OF TRAFFIC FLOW TYPE III BARRICADE TEMPORARY CONCRETE BARRIER TEMPORARY IMPACT ATTENUATOR

TranSmart
100 South Wacker Drive Suite 400
Chicago, Illinois 60606

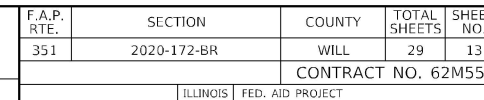
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| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

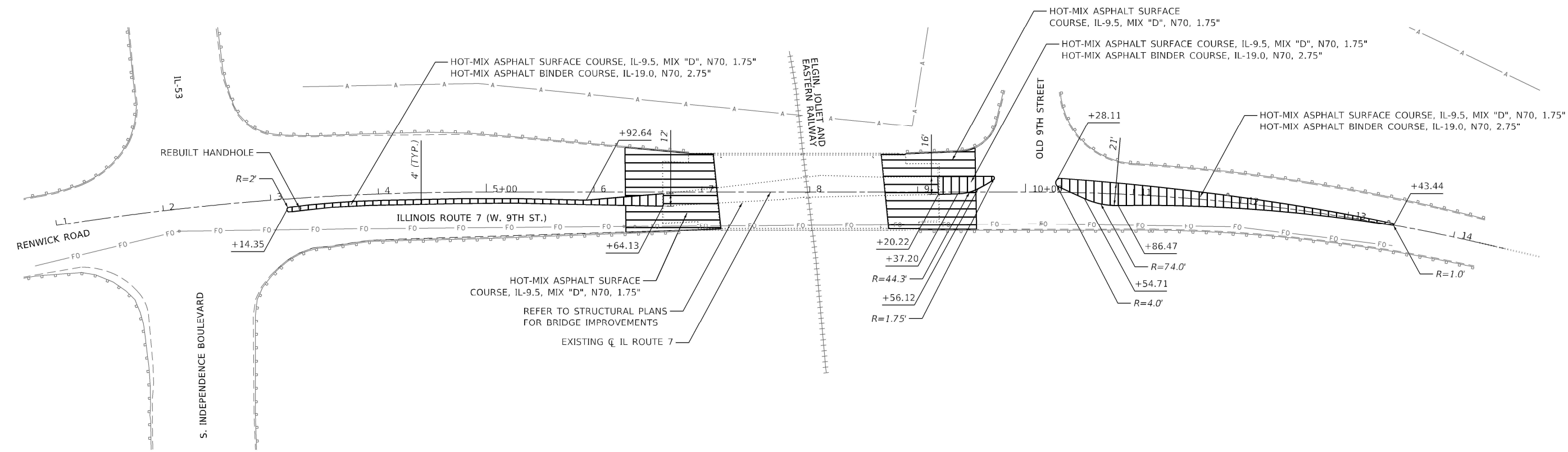
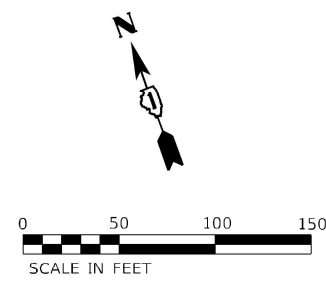
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.)
MOT STAGE 3 PLAN

SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 351 | 2020-172-BR | WILL | 29 | 12 |
| CONTRACT NO. 62M55 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |





| LEGEND | | | | |
|--------|--|--|--|--|
| | HMA SURF CSE, IL-9.5, MIX "D", N70, 1.75" | | | |
| | HMA SURF CSE, IL-9.5, MIX "D", N70, 1.75" HMA BINDER CSE, IL-19.0, N70, 2.75" | | | |

MODEL: Default
FILE NAME: 311-Projects\2015 - IDOT PTB 196-016 - Infrastructure Engineer\Project Work\CADD\CADD Sheets\ID16\M55 - IL 7 at EJE RRD\62M55-shtPlan-01.dgn

100 South Wacker Drive Suite 400

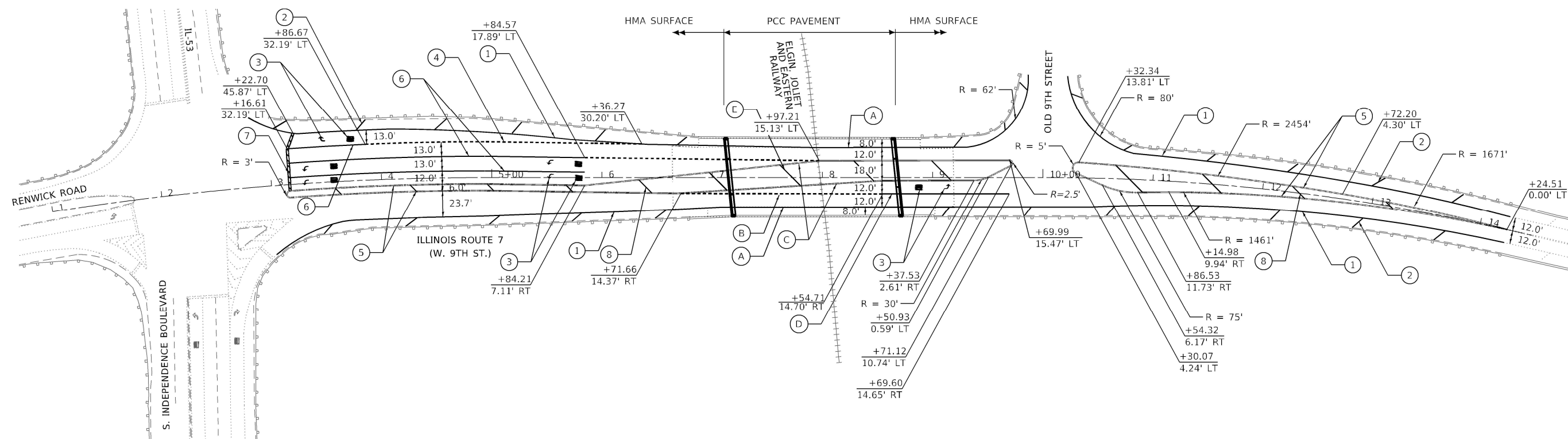
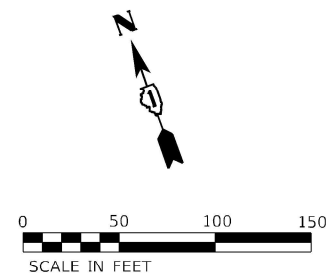
Chicago, Illinois 60606

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| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
| DRAWN - HDU | REVIS | REVISED - |
| PLOT SCALE = 100,0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.) ROADWAY PLAN | | | |
|---|-------|-----------|--------------|
| SCALE: | SHEET | OF SHEETS | STA. TO STA. |

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 351 | 2020-172-BR | WILL | 29 | 14 |
| CONTRACT NO. 62M55 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



BENCHMARK

None.

EXISTING STRUCTURE

Built in 1990 Structure Number 099-0274. Existing bridge is three spans, 156'-7½" back to back of abutments, out to out width is 71'-2". Superstructure is W27x146 steel girders, composite in positive regions, supporting a cast in place concrete deck. Substructure is stub abutments on steel H piles and two multi column piers supported on spread footing.

Traffic will be maintained utilizing staged construction.

No salvage.

SCOPE OF WORK

1. Remove and reconstruct bridge deck expansion joints at abutments and install new preformed joint strip seals.
2. Perform deck slab and approach slab repairs as required.
3. Perform ¾" hydroscarification and apply 2½" bridge deck latex overlay on bridge deck.
4. Clean bridge deck scuppers.
5. Perform bridge deck sawcut grooving to deck slab.
6. Apply Protective Coat to new deck slab, new overlay, and top and inside face of new parapets.

INDEX OF SHEETS

1. General Plan and Elevation
2. General Notes and Total Bill of Material
3. Stage Construction
4. Deck Plan and Section
5. Repair Details
6. Temporary Concrete Barrier
7. Preformed Joint Strip Seal
8. Bar Splicer Assembly and Mechanical Splicer Details

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway Bridges, 17th Edition

DESIGN STRESSES

FIELD UNITS (EXISTING)

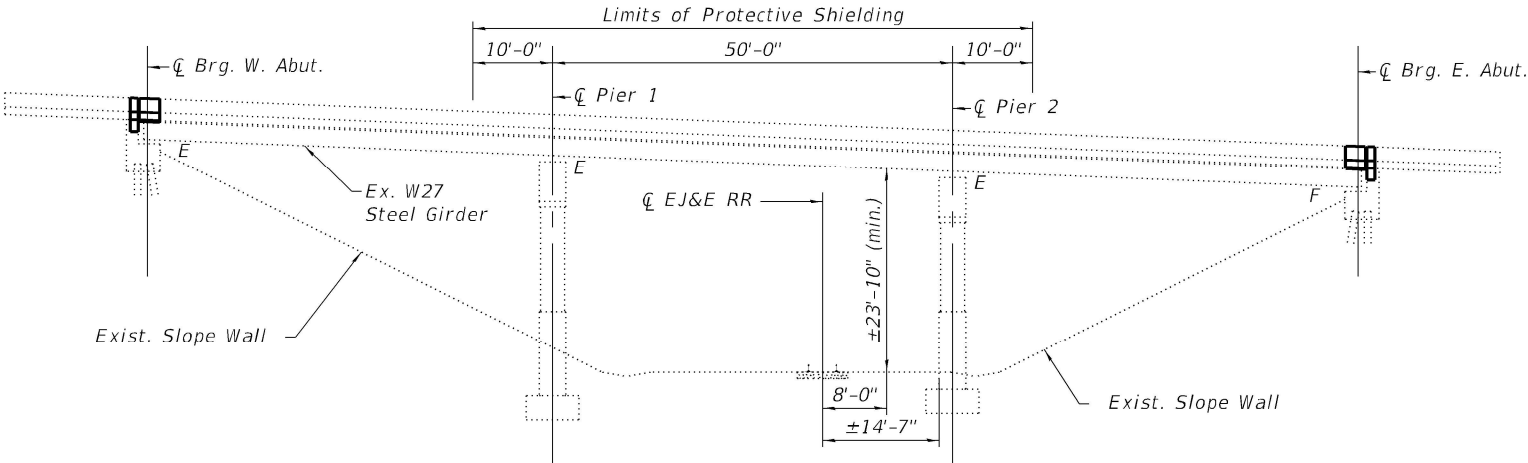
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fy = 60,000 psi (Reinforcement)
fy = 36,000 psi (Struct.) (M183)

FIELD UNITS (NEW CONSTRUCTION)

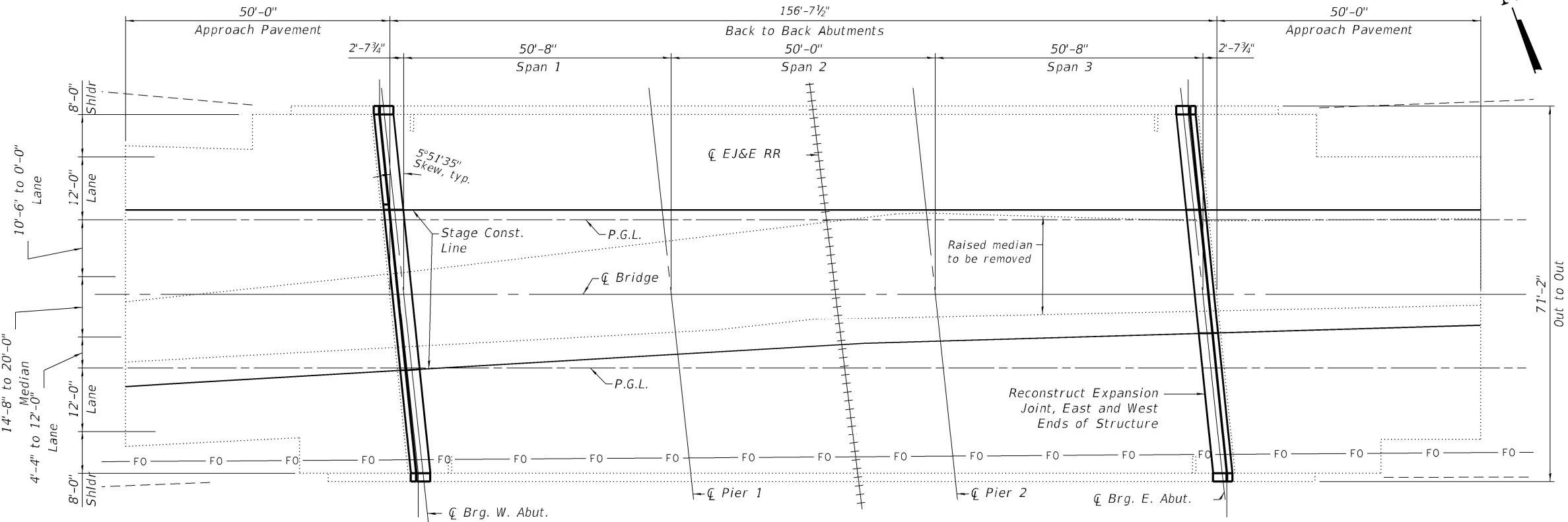
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f'c = 4,000 psi (Superstructure)
fy = 60,000 psi (Reinforcement)

LOADING HS 20-44 (EXIST.)

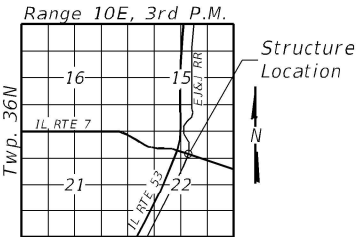
No allowance for future wearing surface.



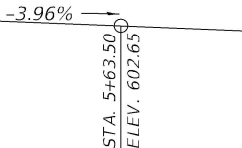
ELEVATION



PLAN

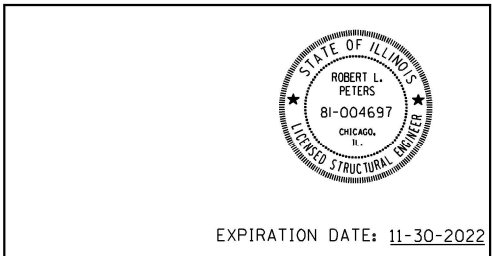


LOCATION SKETCH



PROFILE GRADE LINE IL RTE. 7

Note: Profile grade line based on as-built plans



GENERAL PLAN & ELEVATION

ILLINOIS RTE. 7 (W 9TH ST)

OVER EJ&E RAILROAD

F.A.P. RTE. 351 SECTION 2020-172-BR

WILL COUNTY

STA 5+53.84

STRUCTURE NO. 099-0274



| | | |
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| USER NAME = kchamberlain | DESIGNED - PAF | REVISED - |
| DRAWN - PAF | REVISED - | |
| PLOT SCALE = / in. | CHECKED - CK | REVISED - |
| PLOT DATE = 1/5/2022 | DATE - 1/5/2022 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 099 - 0274

SCALE: SHEET 1 OF 8 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 351 | 2020-172-BR | WILL | 29 | 16 |
| CONTRACT NO. 62M55 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to existing plans are subject to normal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for work.
3. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
4. All exposed concrete edges shall have a $\frac{3}{4}$ "x45° chamfer except where shown otherwise.
5. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding $\frac{1}{4}$ inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
6. Protective Coat shall be applied to the designated areas of the deck and parapets.
7. The concrete for bridge decks finished according to Article 503.16(a) of the Standard Specifications shall be placed and compacted parallel to the skew in uniform increments along centerline of bridge. The machine used for finishing shall be set parallel to the skew for striking off and screeding the concrete.
8. Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

TOTAL BILL OF MATERIAL

| ITEM | UNIT | SUPER | SUB | TOTAL |
|--|-------|-------|-----|-------|
| Concrete Removal | Cu Yd | 100.3 | | 100.3 |
| Protective Shield | Sq Yd | 560 | | 560 |
| Concrete Superstructure | Cu Yd | 24.4 | | 24.4 |
| Bridge Deck Grooving | Sq Yd | 1,159 | | 1,159 |
| Reinforcement Bars, Epoxy Coated | Pound | 3,000 | | 3,000 |
| Protective Coat | Sq Yd | 1,186 | | 1,186 |
| Bar Splicers | Each | 52 | | 52 |
| Preformed Joint Strip Seal | Foot | 144 | | 144 |
| Clean Bridge Scuppers and Downspouts | Each | 4 | | 4 |
| Approach Slab Repair (Full Depth) | Sq Yd | 3 | | 3 |
| Bridge Deck Latex Concrete Overlay, 2 1/2 Inches | Sq Yd | 1,159 | | 1,159 |
| Bridge Deck Scarification, 3/4" | Sq Yd | 1,159 | | 1,159 |
| Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches) | Sq Ft | 10 | | 10 |
| | | | | |
| | | | | |
| | | | | |
| | | | | |



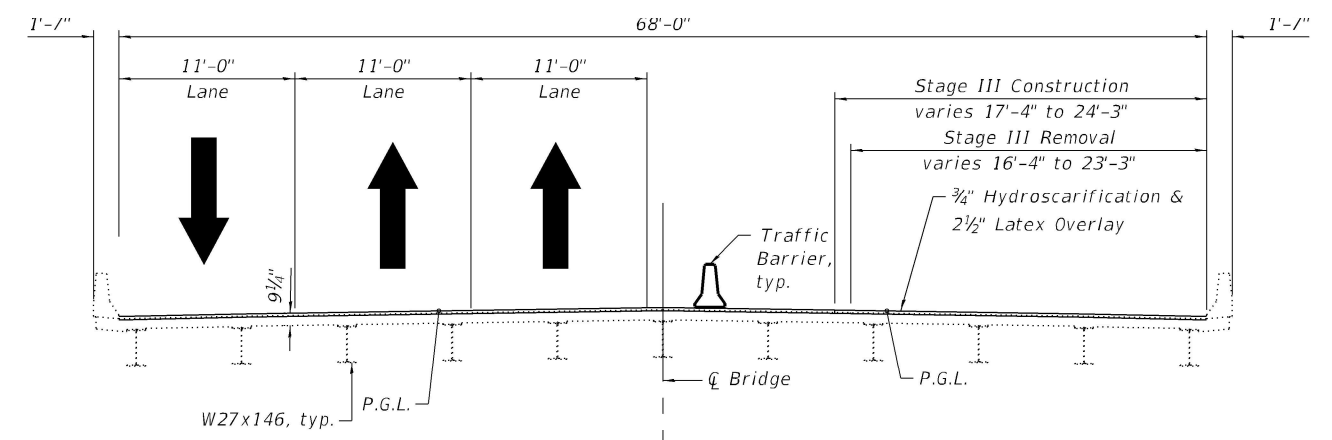
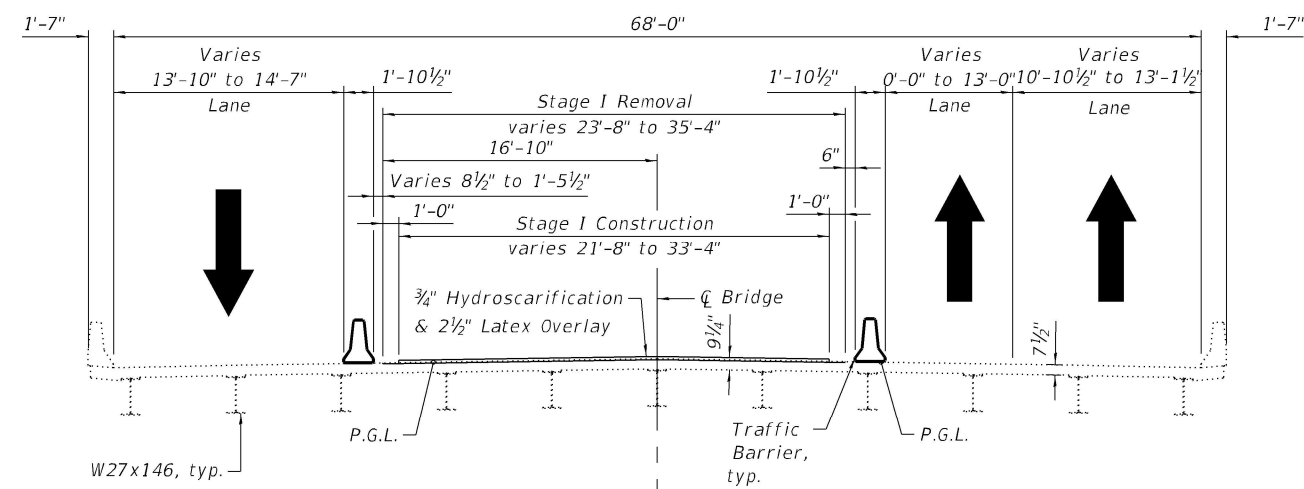
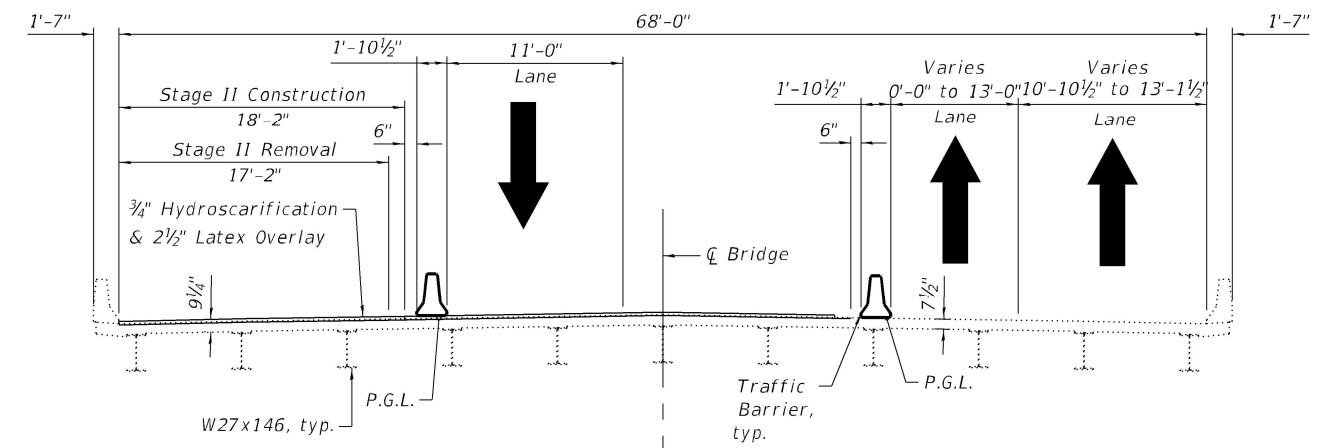
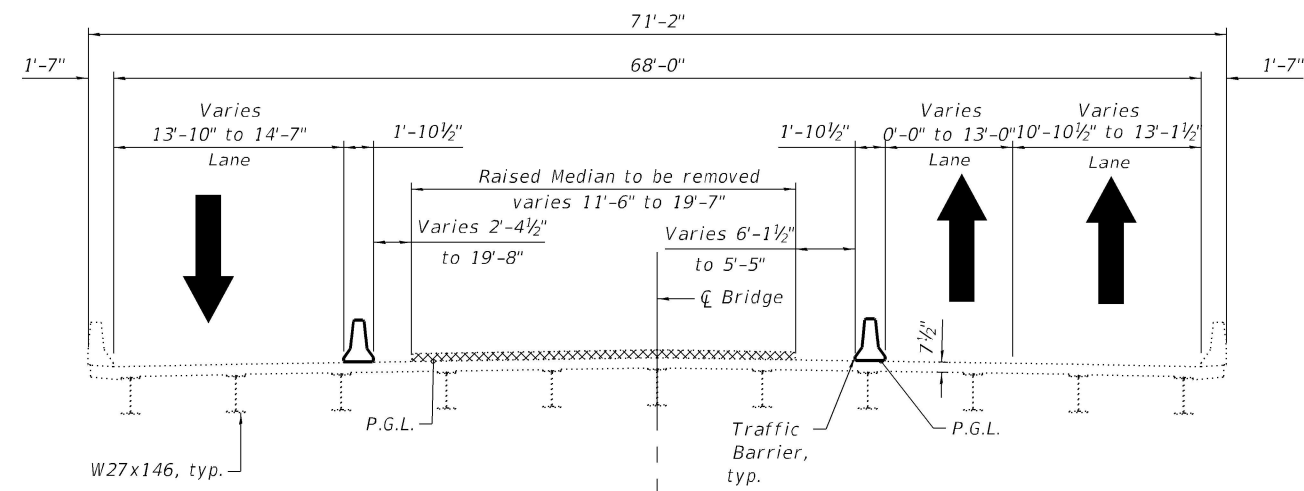
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| | DRAWN - PAF | REVISED - |
| PLOT SCALE = | CHECKED - CK | REVISED - |
| PLOT DATE = 1/4/2022 | DATE - 1/4/2022 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES AND TOTAL BILL OF MATERIALS
STRUCTURE NO. 099 - 0274

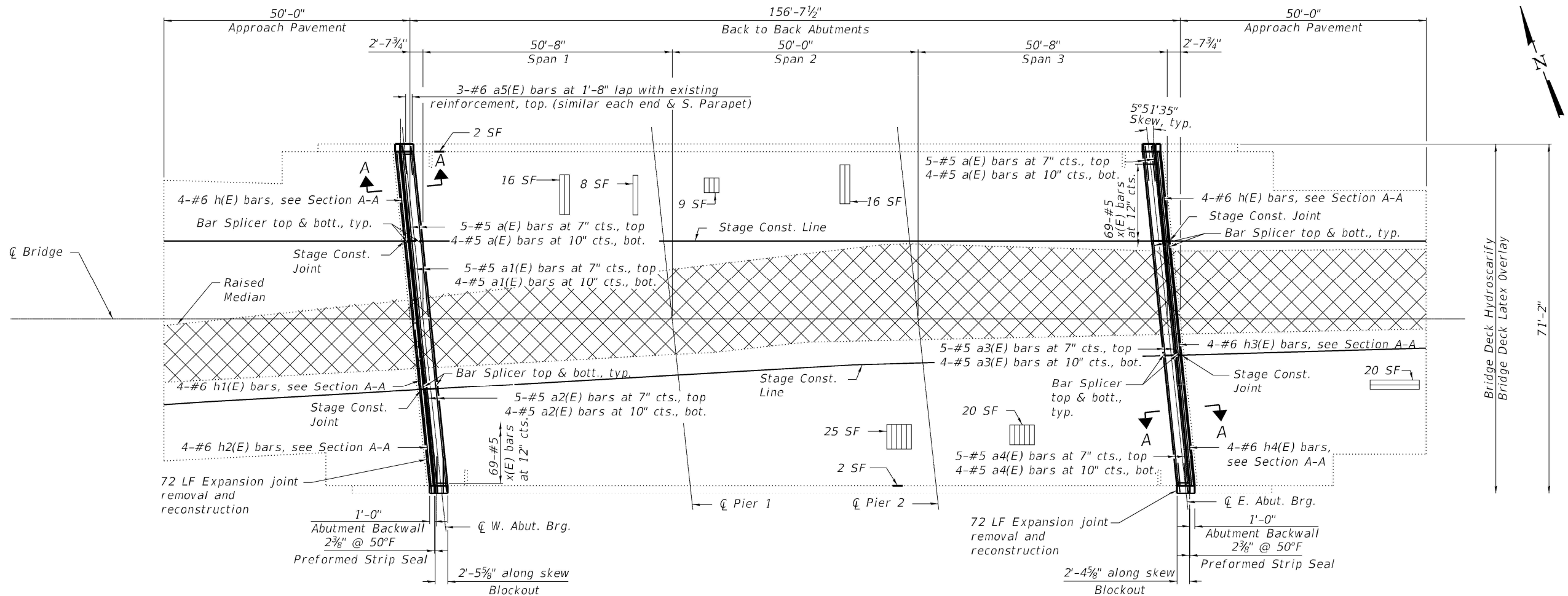
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| SCALE: | SHEET 2 OF 8 SHEETS | STA. | TO STA. |
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|----------------|-------------|--------------------|-----------------|--------------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 351 | 2020-172-BR | WILL | 29 | 17 |
| | | CONTRACT NO. 62M55 | | |
| ILLINOIS | | FED. AID PROJECT | | |

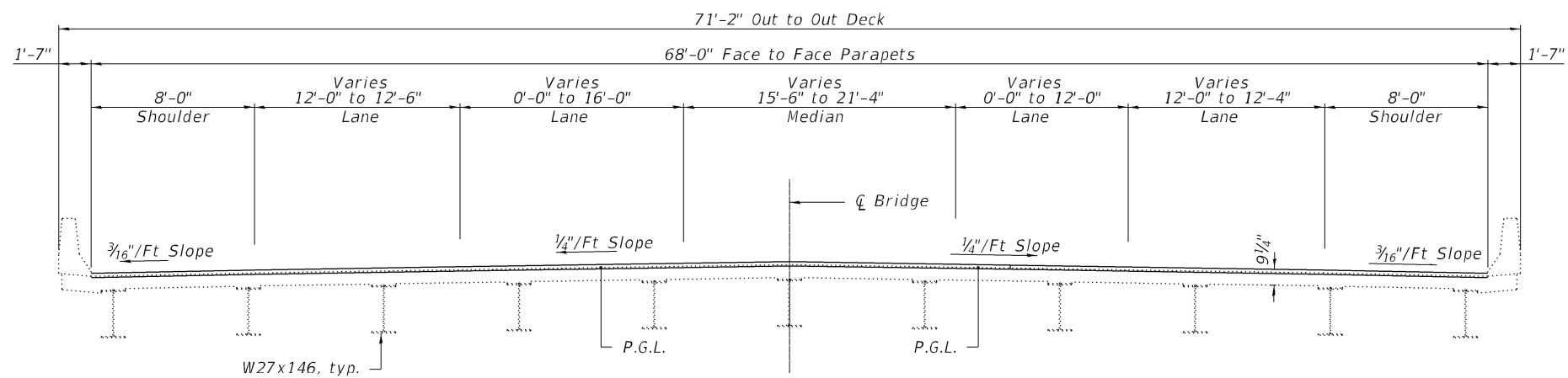


NOTES

1. See MOT plans for additional details.



PLAN



CROSS SECTION

BILL OF MATERIAL

| Item | Unit | Qty |
|--|-------|-------|
| Protective Shield | Sq Yd | 560 |
| Protective Coat | Sq Yd | 1,186 |
| Bridge Deck Grooving | Sq Yd | 1,159 |
| Deck Slab Repair (Partial) | Sq Yd | 11 |
| Approach Slab Repair (Full Depth) | Sq Yd | 3 |
| Bridge Deck Scarification, 3/4" | Sq Yd | 1,159 |
| Bridge Deck Latex Overlay, 2 1/2" | Sq Yd | 1,159 |
| Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches) | Sq Ft | 10 |
| Clean Bridge Scuppers and Downspouts | Each | 4 |

NOTES

- Existing reinforcement to remain shall be cleaned and reincorporated into new construction.
- Deck Slab Repair (Partial) locations and quantities shown herein are for information only and will not be measured for payment but shall be included in the cost of Bridge Deck Latex Concrete Overlay.
- Protective Coat shall be applied to top and inside face of new concrete areas of the parapets, new concrete overlay areas, and new concrete areas.
- See Sheet 5 of 8 for Section A-A.

LEGEND



Concrete Removal



Deck Slab Repair (Partial)



Approach Slab Repair (Full Depth)



Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK PLAN AND SECTION
STRUCTURE NO. 099 - 0274

SCALE: SHEET 4 OF 8 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 351 | 2020-172-BR | WILL | 29 | 19 |
| CONTRACT NO. 62M55 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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| PLOT SCALE = | DRAWN - PAF | REVISED - |
| PLOT DATE = 1/7/2022 | CHECKED - CK | REVISED - |
| | DATE - 1/7/2022 | REVISED - |

MODEL: Default
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DATE: 1/4/2022

TranSmart
100 South Wacker Drive Suite 400
Chicago, Illinois 60606

| | | |
|--------------------------|-----------------|-----------|
| USER NAME = kchamberlain | DESIGNED - PAF | REVISED - |
| | DRAWN - PAF | REVISED - |
| PLOT SCALE = | CHECKED - CK | REVISED - |
| PLOT DATE = 1/4/2022 | DATE - 1/4/2022 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

REPAIR DETAILS
STRUCTURE NO. 099 - 0274

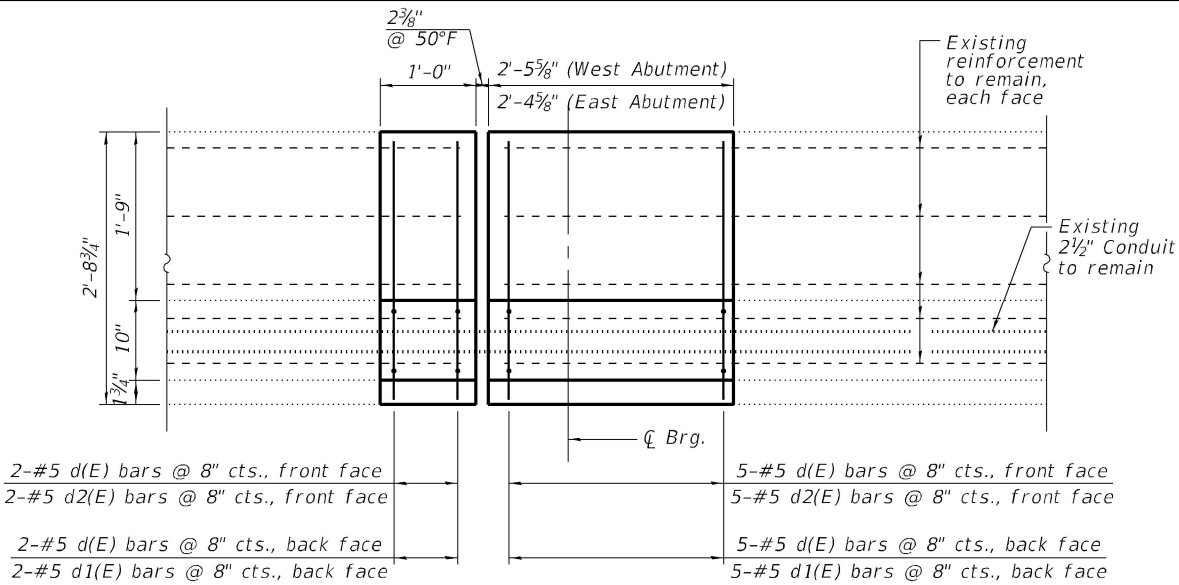
SCALE: SHEET 5 OF 8 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-------------|--------|--------------|-----------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 351 | 2020-172-BR | WILL | 29 | 20 |
| CONTRACT NO. 62M55 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

**SUPERSTRUCTURE
BILL OF MATERIAL**

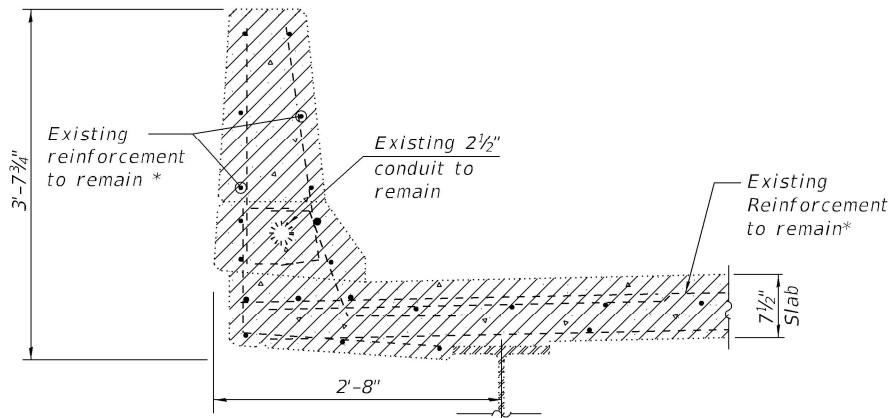
| Bar | No. | Size | Length | Shape |
|----------------------------------|-----|------|---------|-------|
| a(E) | 18 | #5 | 18'-7" | ———— |
| a1(E) | 9 | #5 | 32'-1" | ———— |
| a2(E) | 9 | #5 | 20'-1" | ———— |
| a3(E) | 9 | #5 | 25'-1" | ———— |
| a4(E) | 9 | #5 | 27'-1" | ———— |
| a5(E) | 12 | #6 | 4'-0" | ———— |
| | | | | |
| d(E) | 56 | #5 | 3'-0" | ┐ |
| d1(E) | 28 | #5 | 3'-7" | ┐ |
| d2(E) | 28 | #5 | 2'-7" | Λ |
| | | | | |
| h(E) | 8 | #6 | 17'-0" | ———— |
| h1(E) | 4 | #6 | 32'-2" | ———— |
| h2(E) | 4 | #6 | 18'-5" | ———— |
| h3(E) | 4 | #6 | 25'-1" | ———— |
| h4(E) | 4 | #6 | 25'-6" | ———— |
| | | | | |
| x(E) | 138 | #5 | 3'-0" | ┐ |
| | | | | |
| | | | | |
| Reinforcement Bars, Epoxy Coated | | | Pound | 3,000 |
| Concrete Superstructure | | | Cu. Yd. | 24.4 |
| Concrete Removal | | | Cu. Yd. | 100.3 |
| | | | | |

Bars indicated thus 1 x 2-#8 etc. indicates 1 line of bars with 2 lengths per line.

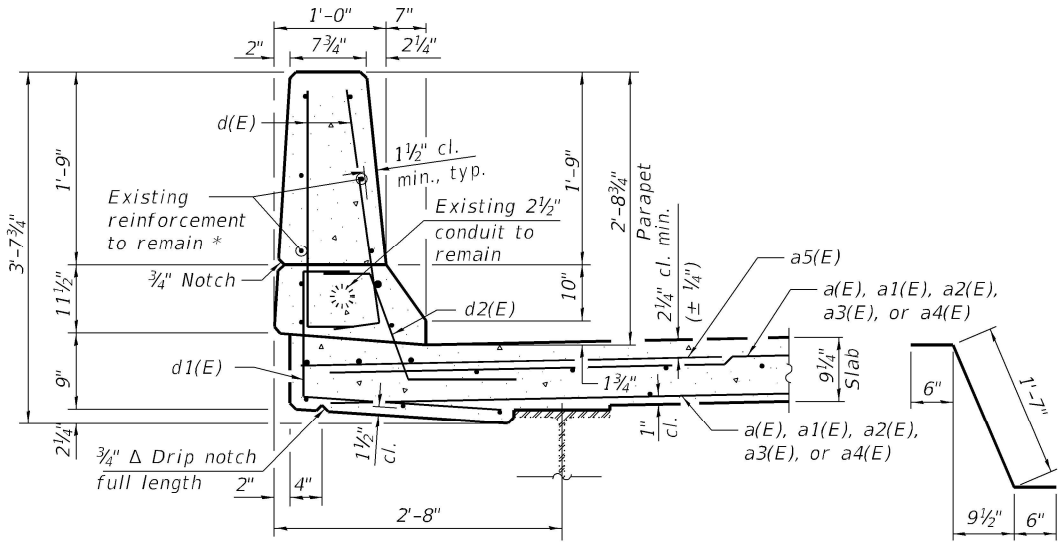


INSIDE ELEVATION NORTHWEST PARAPET

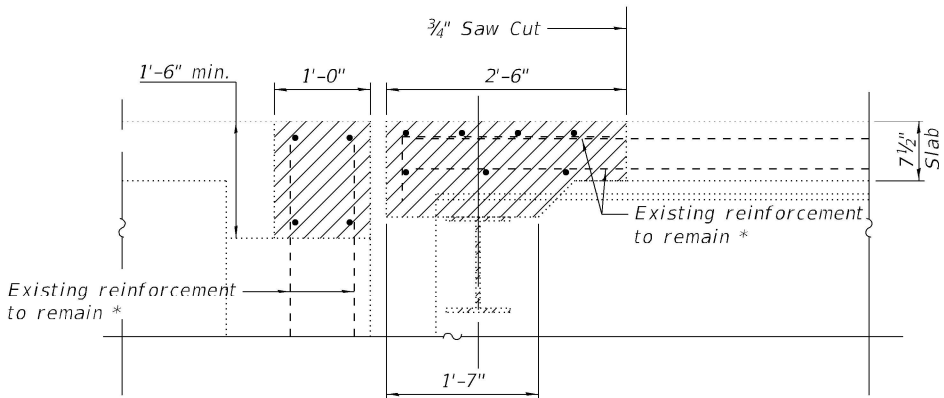
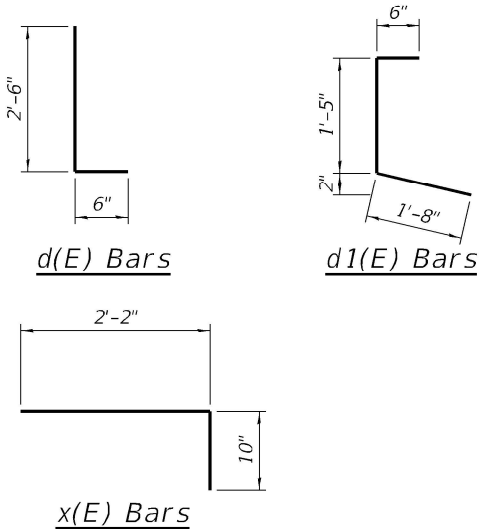
(Similar at Southeast parapet, opp. hand at Northeast and Southwest parapets)



SECTION THRU PARAPET

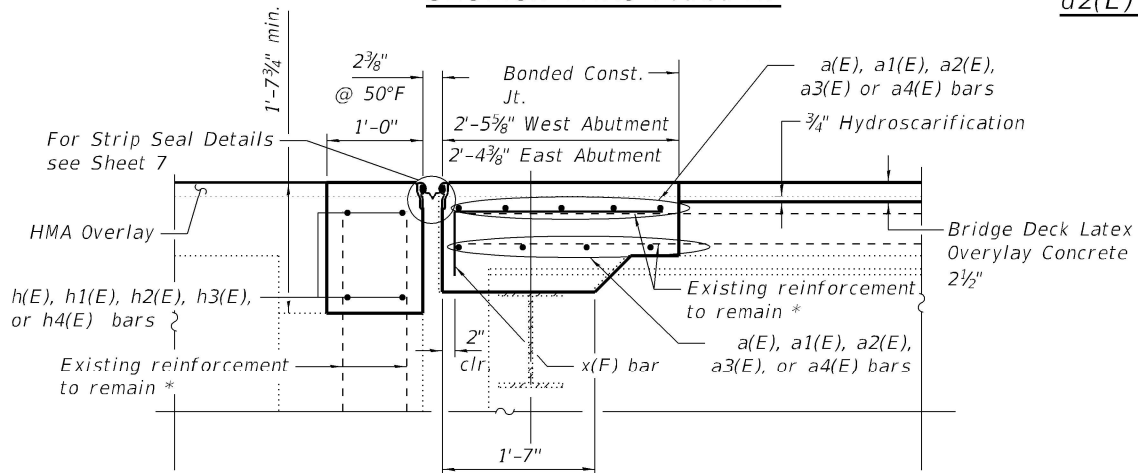


SECTION THRU PARAPET



SECTION A-A REMOVAL

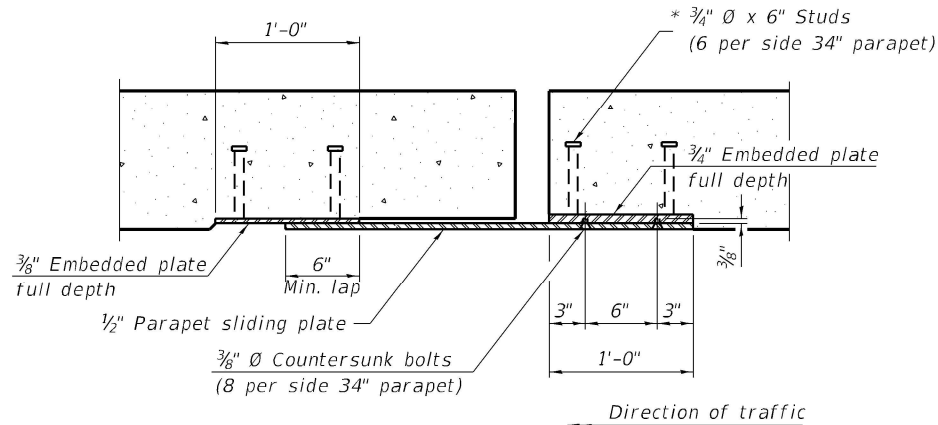
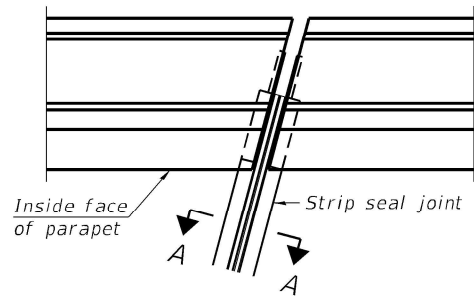
* If existing reinforcement is fully exposed or has greater than 25% section loss shall be replaced in kind with minimum lap or mechanical splicer.



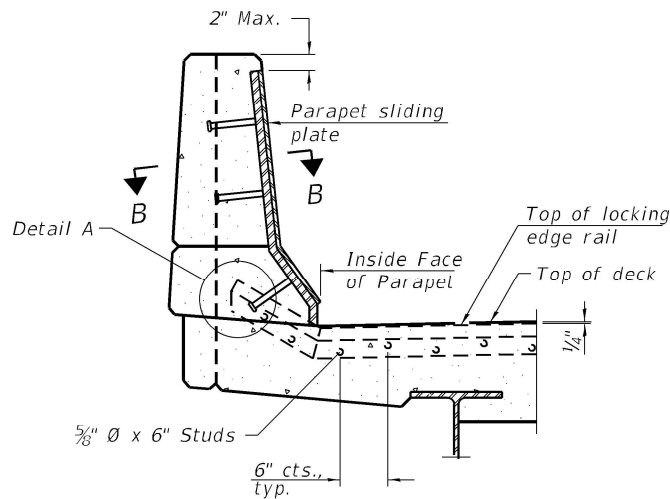
SECTION A-A RECONSTRUCTION

LEGEND



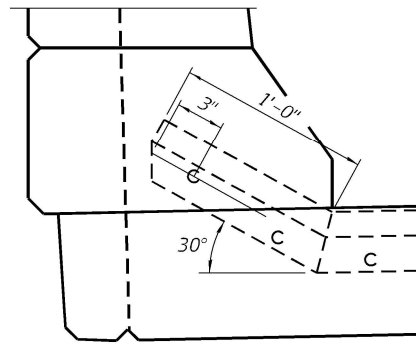


SECTION B-B

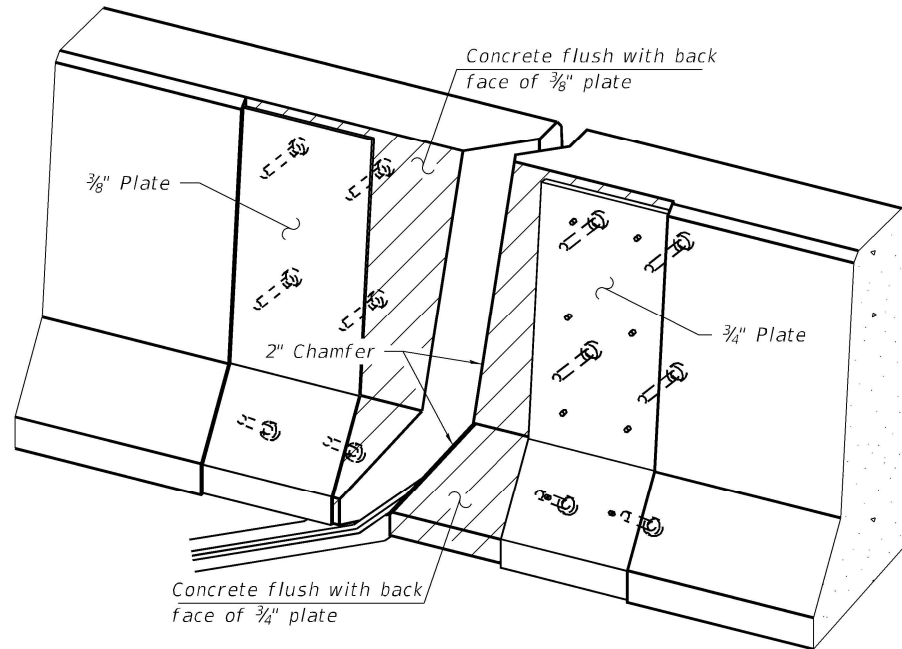


ELEVATION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

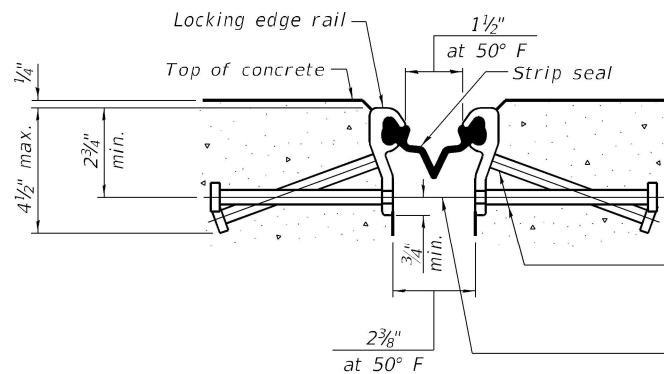


DETAIL A



TRIMETRIC VIEW

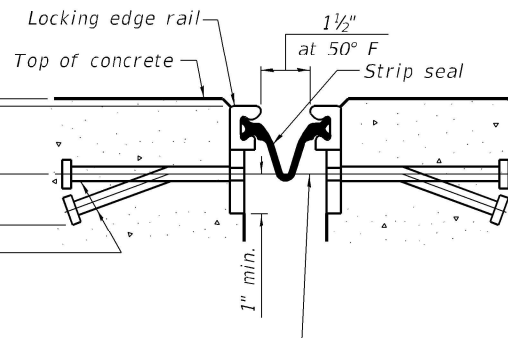
(Showing embedded plates only)



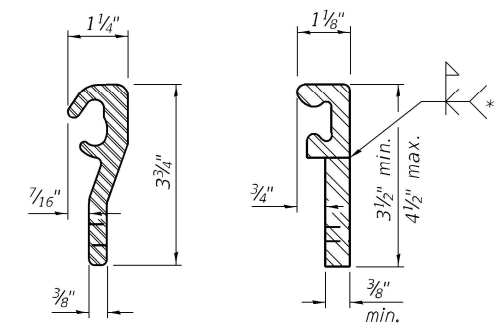
SHOWING ROLLED RAIL JOINT

* $\frac{5}{8}$ " \emptyset x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

$\frac{3}{8}$ " ϕ threaded rods in $\frac{7}{16}$ " ϕ holes at ± 4 '-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

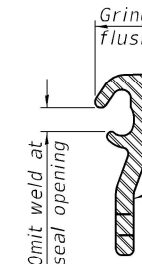


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

| Item | Unit | Total |
|----------------------------|------|-------|
| Preformed Joint Strip Seal | Foot | 144 |

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of $\frac{1}{4}$ ". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the $4\frac{1}{2}$ " maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

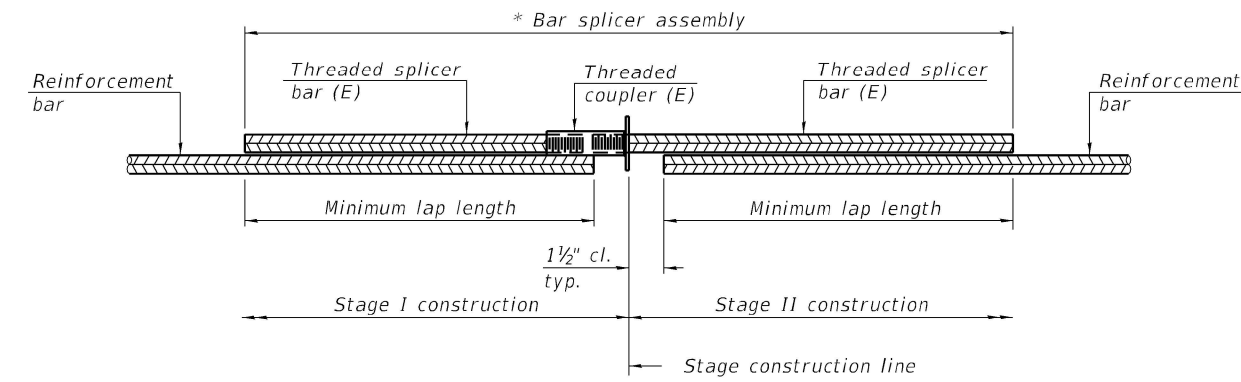
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be $\frac{3}{16}$ " and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

34" F-shape barrier shown, 42" F-shape similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

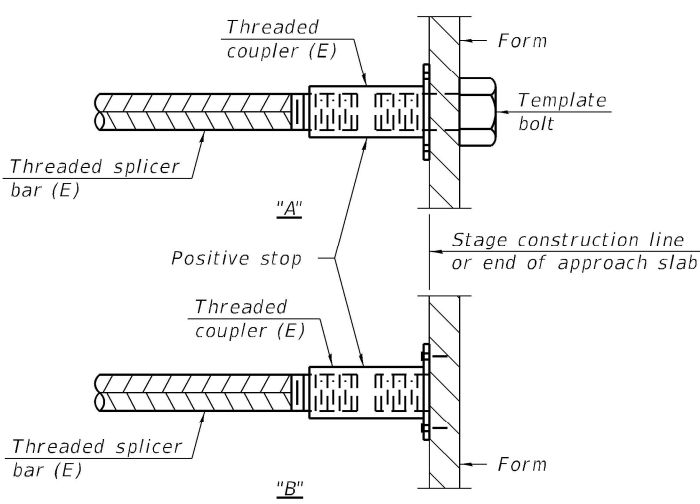


STANDARD BAR SPLICER ASSEMBLY PLAN
(All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Minimum lap length |
|---------------|----------|-------------------------|--------------------|
| East Abutment | #6 | 8 | 4'-0" |
| West Abutment | #6 | 8 | 4'-0" |
| East Deck | #5 | 18 | 3'-6" |
| West Deck | #5 | 18 | 3'-6" |
| | | | |

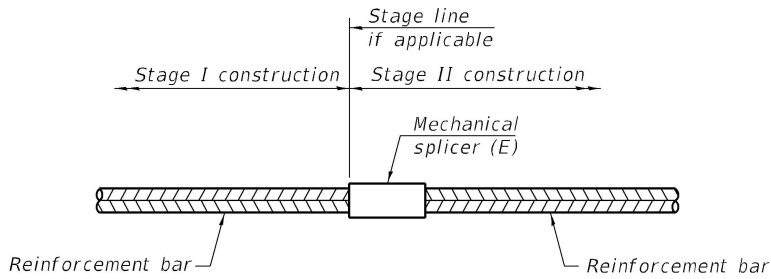


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
| | | |
| | | |
| | | |
| | | |

Notes:
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

1-1-2020

MODEL Default
FILE Name: 330Projects2015 - IDOT PTB 196-g16 - Infrastructure Engineer\Project Work\CADD\CADD Sheets\ID163M55 - IL 7 at EJE RRD 162M55-shrStruct-65D-1.dgn

TranSmart
100 South Wacker Drive Suite 400
Chicago, Illinois 60606

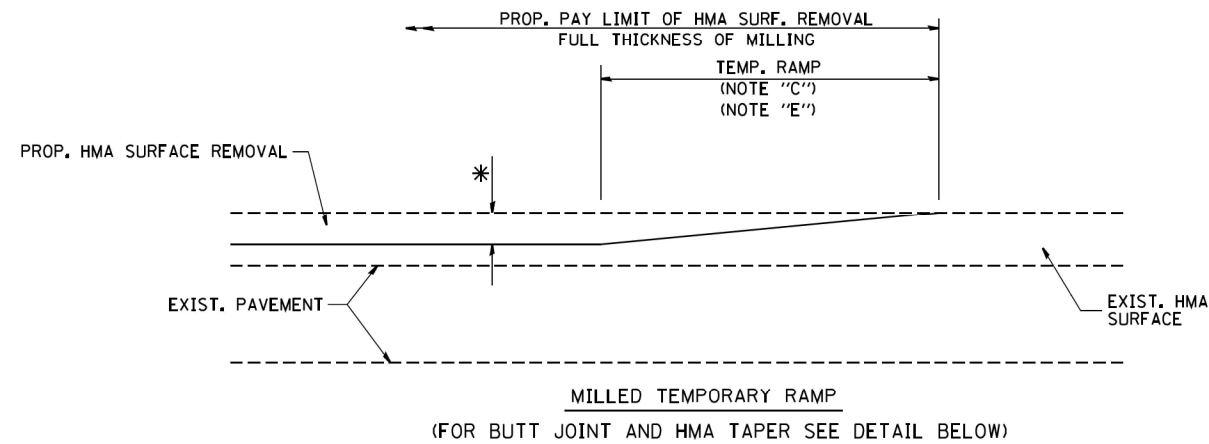
| | | |
|--------------------------|-----------------|-----------|
| USER NAME = kchamberlain | DESIGNED - PAF | REVISED - |
| | DRAWN - PAF | REVISED - |
| PLOT SCALE = | CHECKED - CK | REVISED - |
| PLOT DATE = 1/4/2022 | DATE - 1/4/2022 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

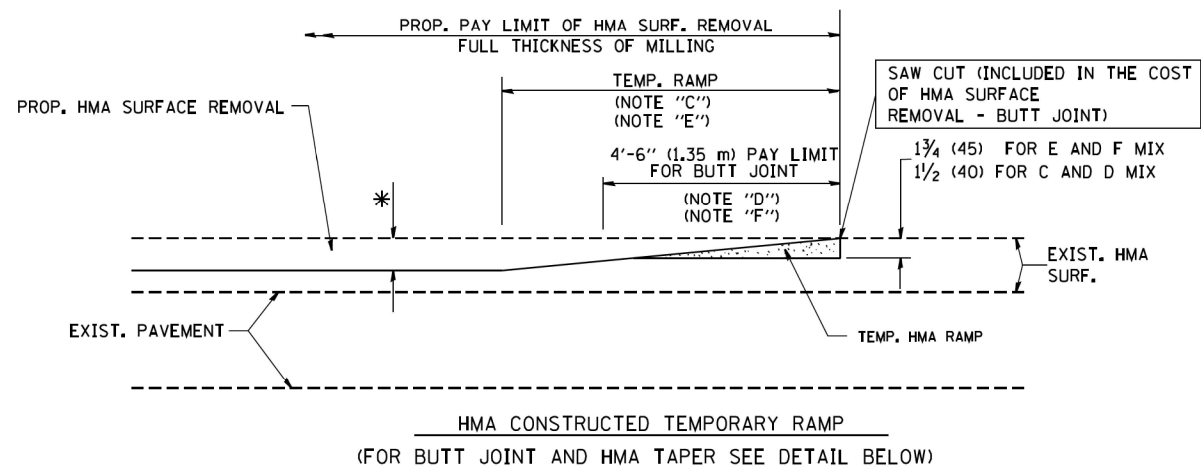
BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 099 – 0274

SCALE: SHEET 8 OF 8 SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|--------|--------------|-----------|
| 351 | 2020-172-BR | WILL | 29 | 23 |
| CONTRACT NO. 62M55 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

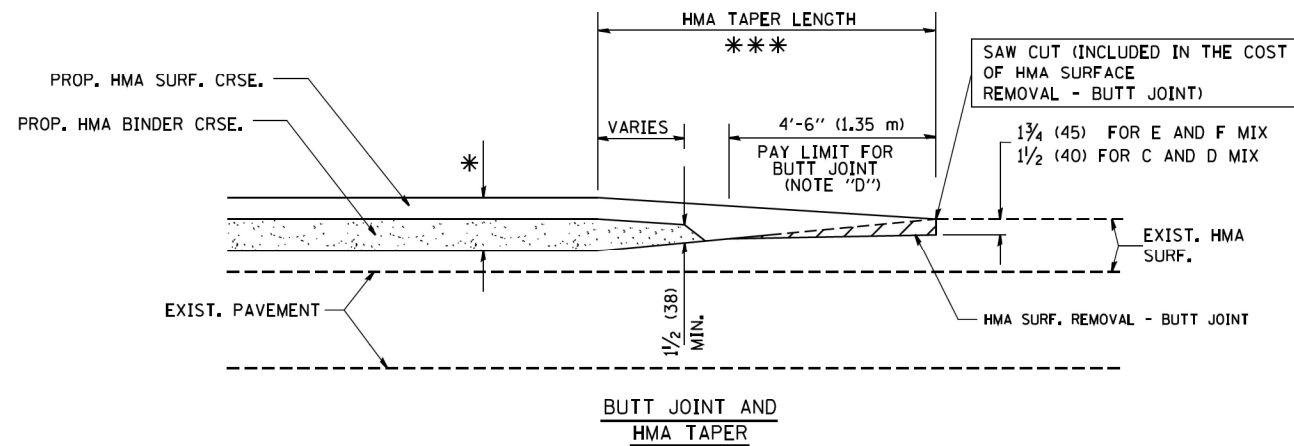


OPTION 1

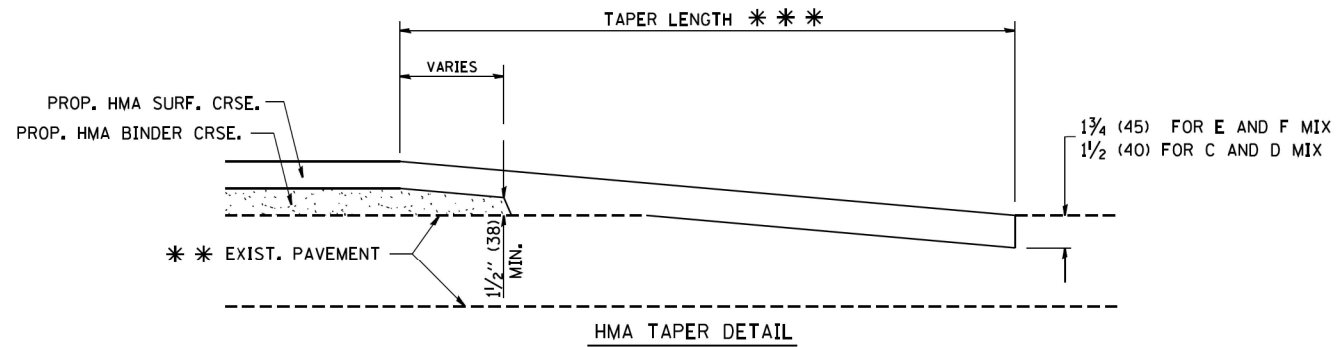
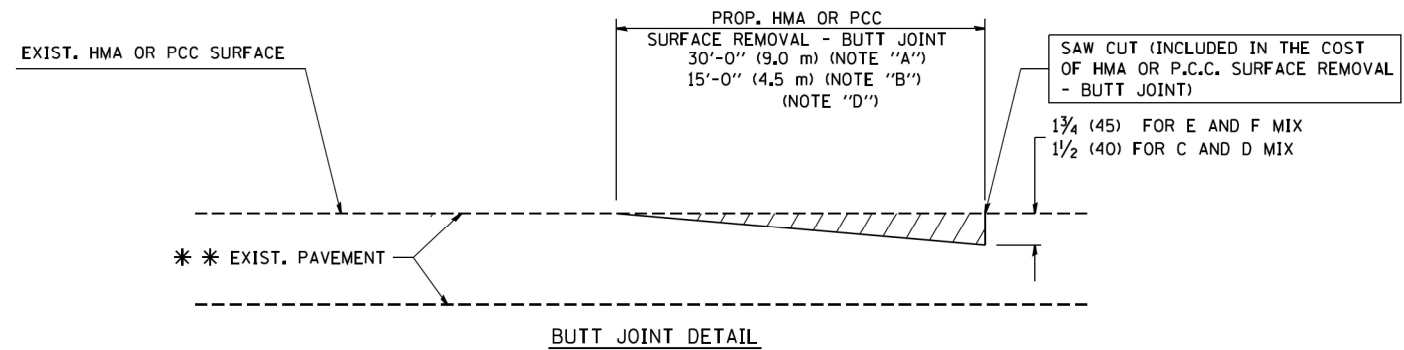


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- *** SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE NAME: 3015 - IDOT PTB 196-016 - Infrastructure Engineer\Project Work\CADD\CADD Sheets\163M55 - IL 7 at EJE RRD 162M55-sht-detail-BD-32.dgn

TranSmart
100 South Wacker Drive Suite 400
Chicago, Illinois 60606

| | | |
|----------------------------|------------------|-----------|
| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
| PLOT SCALE = 1.0000' / in. | DRAWN - HDU | REVISED - |
| PLOT DATE = 12/8/2021 | CHECKED - SA | REVISED - |
| | DATE - 12/8/2021 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: SHEET OF SHEETS STA. TO STA.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------|-------------|--------------------|-----------------|--------------|
| 351 | 2020-172-BR | WILL | 29 | 24 |
| BD-32 | | CONTRACT NO. 62M55 | | |
| ILLINOIS | | FED. AID PROJECT | | |



The diagram illustrates a lane reduction transition. It shows a 2-lane road on the left, indicated by two horizontal lines and two right-pointing arrows. This road narrows into a 1-lane road on the right, indicated by a single horizontal line and a single right-pointing arrow. The transition zone is marked with a series of triangles and labeled "20' (6 m) O.C.". A diamond-shaped warning sign with a black border and a white background, featuring two black vertical bars, is positioned at the start of the transition. A dimension line below the road indicates a distance of "80' (24 m)" from the sign to the start of the transition. Another dimension line above the road indicates a distance of "3 @ 40' (12 m) O.C." between three points on the 1-lane road. The text "LANE REDUCTION TRANSITION" is written in large, bold, black capital letters at the bottom right of the diagram.



1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

- YELLOW STRIPE
- == WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS
 USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

| | | |
|-----------------------------|------------------|-----------|
| USER NAME = HJMLAUF | DESIGNED - HDU | REVISED - |
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

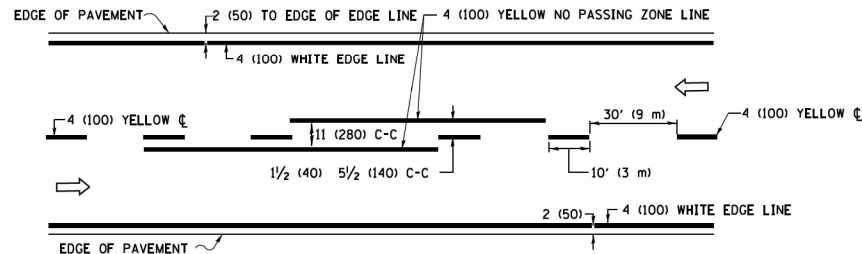
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| |
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| TRAFFIC CONTROL AND PROTECTION FOR RAISED REFLECTIVE PAVEMENT MARKINGS (SNOW-PLOW RESISTANCE) |
|--|

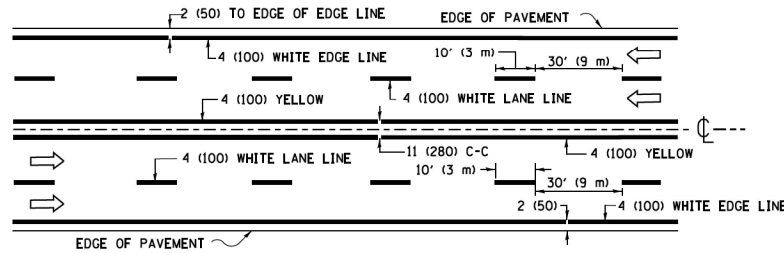
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|--------|-------|----|--------|------|---------|
| SCALE: | SHEET | OF | SHEETS | STA. | TO STA. |
|--------|-------|----|--------|------|---------|

| | | | | |
|----------------|-------------|--------------------|-----------------|--------------|
| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 351 | 2020-172-BR | WILL | 29 | 26 |
| TC-11 | | CONTRACT NO. 62M55 | | |
| ILLINOIS | | FED. AID PROJECT | | |

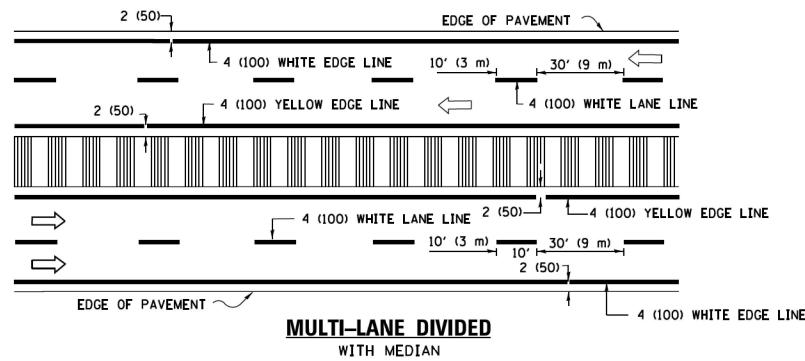
MODEL: Default
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PROJECT: 2015 - IDOT PBT 198-016 - Infrastructure Engineer/Project Work/CADD/CADD Sheets/D163M55 - IL 7 at E.E. RRD 162M55-CH-DETAIL-TC-13.dgn



2-LANE ROADWAY

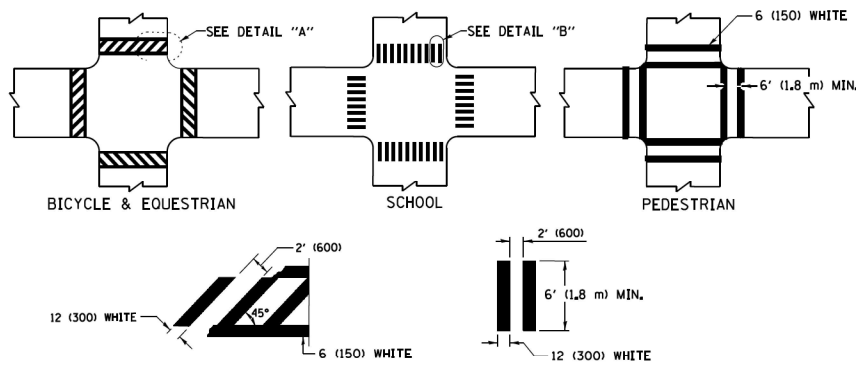


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

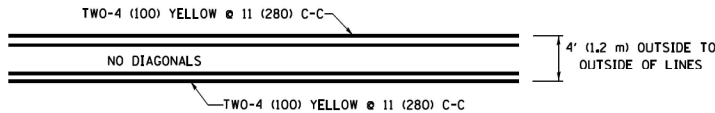


DETAIL "A"

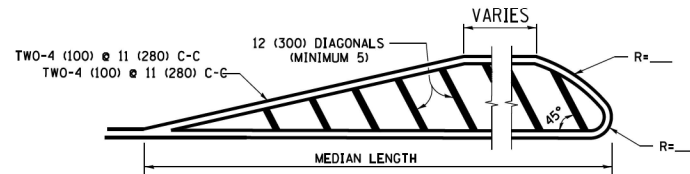
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

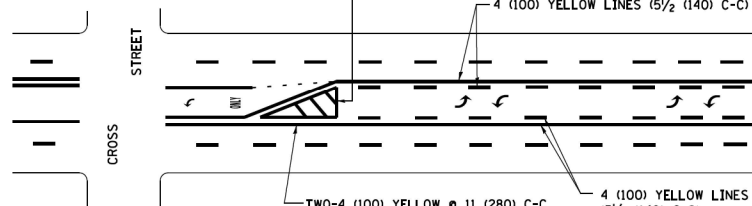


4' (1.2 m) WIDE MEDIANS ONLY

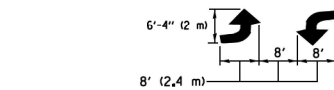


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

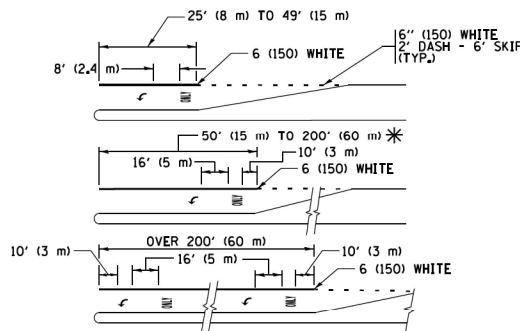


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

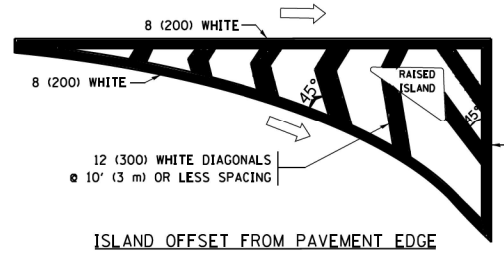


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

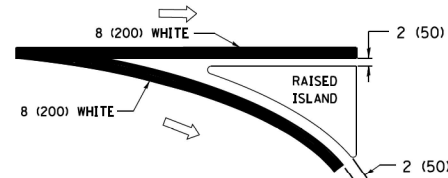
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

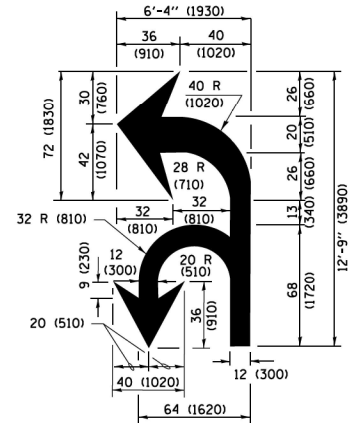


ISLAND OFFSET FROM PAVEMENT EDGE

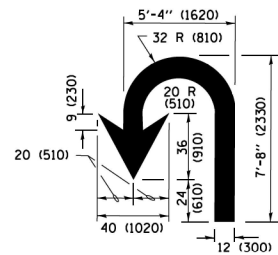


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

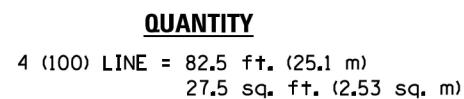
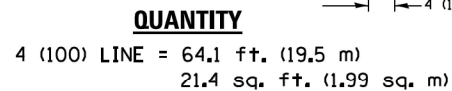
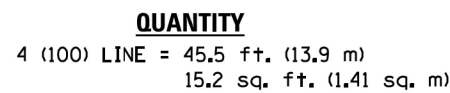
LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

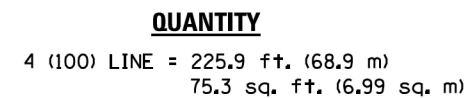
| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING /REMARKS |
|---|--|------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MEDIANS IN YELLOW |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| CORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8') | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |
| U TURN ARROW | SEE DETAIL | SOLID | WHITE | 16.3 SF |
| 2 ARROW COMBINATION LEFT AND U TURN | SEE DETAIL | SOLID | WHITE | 30.4 SF |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.



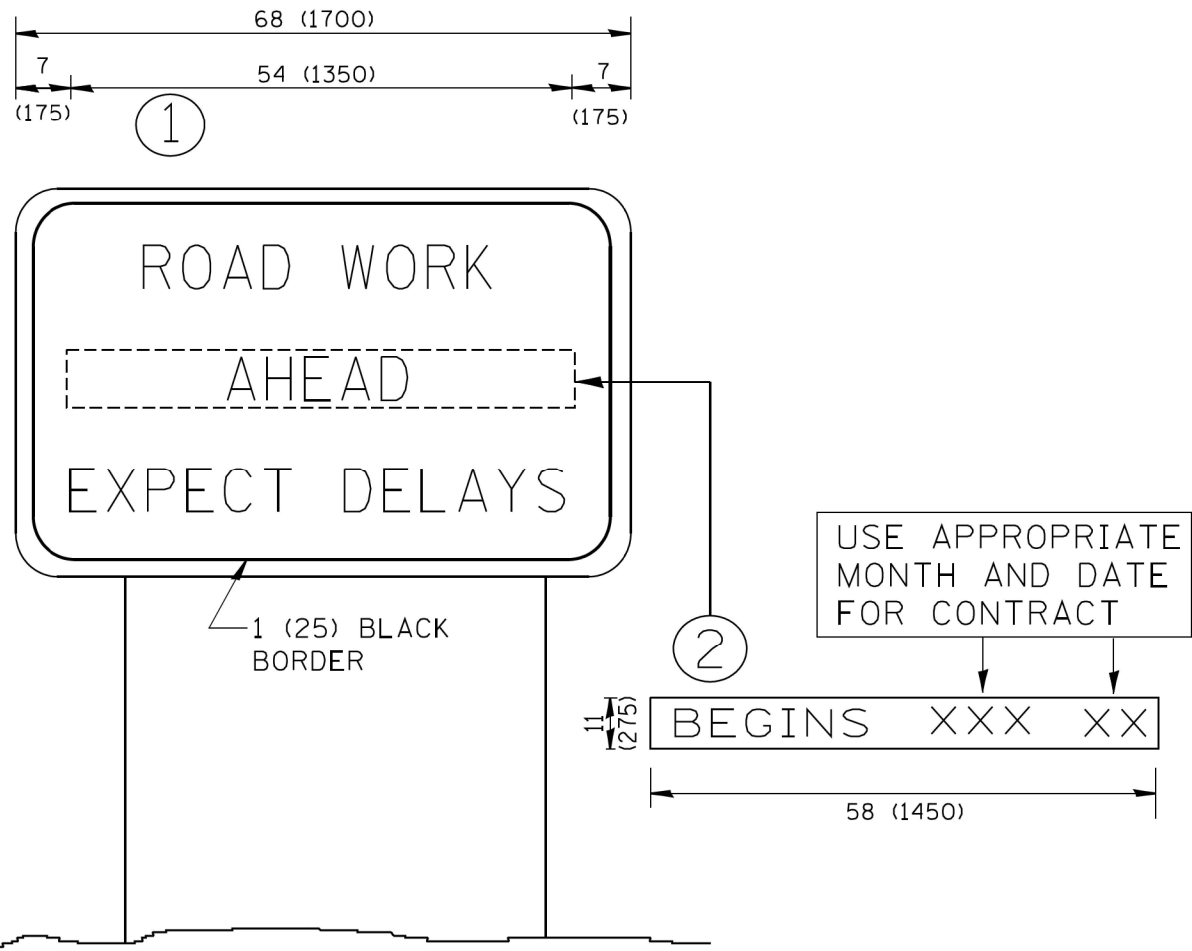
ALL QUANTITIES OF PLACEMENT ARE REPRESENTED
IN LINEAR FEET OF 4" LINES TO MATCH THE
4" TEMPORARY TAPE PAY ITEM AND REPRESENTS
THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



MODEL: Default
FILE NAME: S:\Projects\2015 - IDOT PTB - 09-016 - Infrastructure Engineer\Project Work\CADD\CADD Sheets\162M55 - IL 7 at E/E RRD162M55-sht-detail-TC-16.ign

MODEL: Default
FILE NAME: 331-projects2015 - IDOT PTH 196-016 - Infrastructure Engineer\Project Work\CADD\CADD Sheets\ID163M55 - IL 7 at EJE RRD 162M55-sht-detail-TC-22.dgn

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|-----------------------------|------------------|-----------|
| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

| F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|----------------|-------------|--------------------|-----------------|--------------|
| 351 | 2020-172-BR | WILL | 29 | 29 |
| TC-22 | | CONTRACT NO. 62M55 | | |
| ILLINOIS | | FED. AID PROJECT | | |

| ARTERIAL ROAD INFORMATION SIGN | | | | |
|--------------------------------|-------|----|--------|--------------|
| SCALE: | SHEET | OF | SHEETS | STA. TO STA. |