STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

D-91-569-20

TO DAYSS STEPHENDLY WANGEROOD DRONG MACH HOME WAS HORD WANGER AND CONTROLLED WAS HORD WANGER CHAYFORD WAS HORD WAS H

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN CITY OF LOCKPORT

TRAFFIC DATA

2019 ADT: 23,100 VPD

POSTED SPEED LIMIT: 40 MPH

0 100' 200' 300' — 1" = 100'
0 50' 100' — 1" = 50'
0 50' 100' — 1" = 40'
0 50' 100' — 1" = 30'
0 50' 100' — 1" = 20'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: LUKASZ POCIECHA PROJECT MANAGER: FAWAD AQUEEL

(847) 705–4255

PROPOSED HIGHWAY PLANS

F.A.P. ROUTE 351: IL. ROUTE 7
OVER EJ & E RAILROAD
SECTION: 2020–172–BR
PROJECT: NHPP–Z6S7(332)
BRIDGE DECK OVERLAY AND BRIDGE JOINT REPAIR
WILL COUNTY
C-91-368-20

PROJECT BEGINS:
STA. 03 + 14.35

PROJECT ENDS:
STA. 13 + 43.44

LOCATION MAP
(NOT TO SCALE)

RIDE

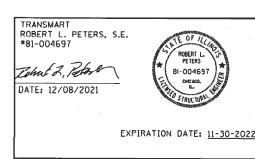
ROMEOVILLE

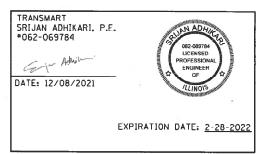
TO STATEVILLE ROAD

LOCKPORT TOWNSHIP

GROSS LENGTH = 1029 FT. = 0.19 MILE

NET LENGTH = 1029 FT. = 0.19 MILE



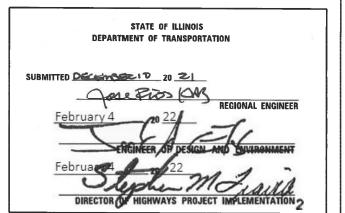


Transmart

100 South Wacker Drive Suite 400
Chicago, Illinois 60606

CONTACT: SRIJAN ADHIKARI

(312) 922-1700 EXT. 107



PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 62M55

INDEX OF SHEETS

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| 1 | COVER SHEET |
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| 5 | EXISTING TYPICAL SECTIONS |
| 6 | PROPOSED TYPICAL SECTIONS |
| 7 | ALIGNMENT PLAN |
| 8 | MOT AND CONSTRUCTION STAGING GENERAL NOTES |
| 9 | MAINTENANCE OF TRAFFIC TYPICAL SECTIONS |
| 10 | MAINTENANCE OF TRAFFIC STAGE 1 PLAN |
| 11 | MAINTENANCE OF TRAFFIC STAGE 2 PLAN |
| 12 | MAINTENANCE OF TRAFFIC STAGE 3 PLAN |
| 13 | REMOVAL PLAN |
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| 24 | DISTRICT ONE DETAIL: BUTT JOINTS AND HMA TAPER (BD-32) |
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| 27 | DISTRICT ONE DETAIL: TYPICAL PAVEMENT MARKINGS (TC-13) |
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| 29 | DISTRICT ONE DETAIL: ARTERIAL ROAD INFORMATION SIGN (TC-22) |

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE CITY OF LOCKPORT, LOCKPORT TOWNSHIP AND WILL COUNTY DIVISION OF TRANSPORTATION.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONTACT THE IDOT ARTERIAL DISTRICT ONE TRAFFIC CONTROL SUPERVISOR KALPANA KANNAN-HOSADURGA AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT ERIC CAMPOS, ARTERIAL TRAFFIC FIELD ENGINEER VIA EMAIL AT ERIC.CAMPOS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- 10. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- 11. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.
- 12. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER, REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- 13. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.
- MEADE ELECTRIC COMPANY. THE IDOT DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR. LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES. CALL 773-287-7672 FOR THE INITIAL LOCATE. REQUEST FOR LOCATES OF PREVIOUSLY MARKED FACILITIES SHALL BE AT THE CONTRACTOR'S EXPENSE.
- 15. IN ADDITION TO FIELD REVIEW AND AERIAL DATA, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. IT SHALL BE THE CONTACTOR'S RESPONSIBILITY TO VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS, SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE BID
- 16. THE CONTACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATEIRAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.

GENERAL NOTES (CONTINUED)

- 17. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS. IT THE CONTACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDU 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS. THE CONTACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SIDE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION 11.G.1. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPERATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTACT UNIT PRICES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 18. THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A USACE PERMIT. IF THE CONTACTOR CHOOSES TO USE ACTIVITIES REQUIRING A USACE PERMIT, IT IS THE CONTACTOR'S RESPONSIBILITY TO SECURE THE PROPER USAGE PERMITS. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO SECURE AND COMPLY WITH A USACE PERMIT FOR CONTRACTOR'S ACTIVITIES WILL NOT BE PAID FOR SEPERATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATIONS WILL BE ALLOWED.
- 19. THESE PLANS HAVE BEEN PREPARED FROM NOTES RECIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS.
- 20. THE CONTACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN, ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- 21. SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
- 22. THE CENTERLINE IS FOR INFORMATION ONLY.

LIST OF STATE STANDARDS

| STANDARD NO. | DESCRIPTION |
|--------------|---|
| 000001-08 | STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS |
| 001006 | DECIMAL OF AN INCH AND FOOT |
| 701501-06 | URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED |
| 701606-10 | URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIA |
| 701611-01 | URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN |
| 701701-10 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701901-08 | TRAFFIC CONTROL DEVICES |
| 780001-05 | TYPICAL PAVEMENT MARKINGS |
| 704001-08 | TEMPORARY CONCRETE BARRIER |
| | |

| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
|-----------------------------|------------------|-----------|
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

COUNTY

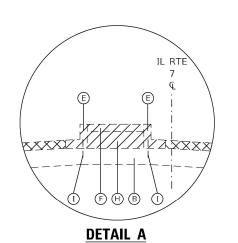
WILL

29

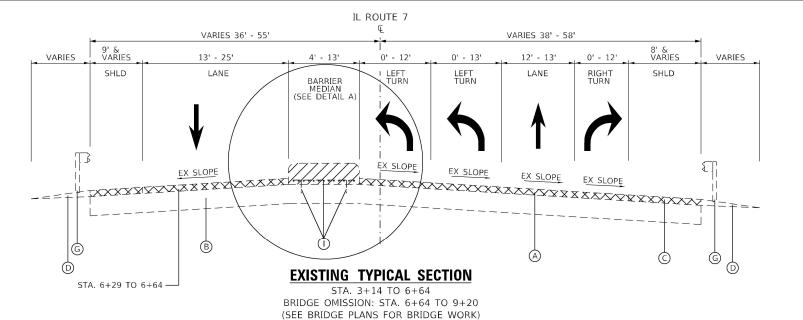
CONTRACT NO. 62M55

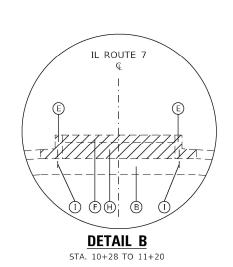
| | SUMMARY OF QU | 1 | 1 | CONSTR. CODE | ODE | | SUMMARY OF QUANTITIES | | | | |
|---|--|----------------|-------|---------------------|---|-----|-----------------------|---|----------------------|-----------------------------------|-------------------------------------|
| SP. CODE TEM NO | ITEM | | UNIT | TOTAL QUANT I TY | 80% FED / 20% STATE 0059 URBAN | SP. | | ITEM | UNIT | | 80% FED 20% STA 0059 URBAN |
| 20200100 | EARTH EXCAVATION | | CU YD | 45 | 45 | | 70307100 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS - TYPE IV TAPE | SQ FT | 110 | 110 |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT) | | POUND | 1010 | 1010 | | 70307120 | TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE | FOOT | 14026 | 14026 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT | JOINT | SQ YD | 180 | 180 | | 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 500 | 500 |
| 40603085 | 6 HOT-MIX ASPHALT BINDER COURSE, IL-19.0 | , N70 | TON | 101 | 101 | | 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 525 | 525 |
| 40604062 | HOT-MIX ASPHALT SURFACE COURSE, IL-9.5 | , MIX "D", N70 | TON | 188 | 188 | | 70600255 | IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2 | 2 EACH | 2 | 2 |
| 44003100 | MEDIAN REMOVAL | | SQ FT | 5900 | 5900 | | 70600322 | IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2 | EACH | 2 | 2 |
| 50102400 | CONCRETE REMOVAL | | CU YD | 100.3 | 100.3 | * | 72400100 | REMOVE SIGN PANEL ASSEMBLY - TYPE A | EACH | 4 | 4 |
| 50157300 | PROTECTIVE SHIELD | | SQ YD | 560 | 560 | * | 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 218 | 218 |
| 50300255 | CONCRETE SUPERSTRUCTURE | | CU YD | 24.4 | 24.4 | * | 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 5617 | 561 |
| 50300260 | BRIDGE DECK GROOVING | | SQ YD | 1159 | 1159 | * | 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 675 | 67 |
| 50300300 | PROTECTIVE COAT | | SQ YD | 1186 | 1186 | * | 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 460 | 46 |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | | POUND | 3000 | 3000 | * | 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 53 | 53 |
| 50800515 | 5 BAR SPLICERS | | EACH | 52 | 52 | * | 78009004 | MODIFIED URETHANE PAVEMENT MARKING - LINE 4" | FOOT | 933 | 93: |
| 52000110 | PREFORMED JOINT STRIP SEAL | | FOOT | 144 | 144 | * | 78009006 | MODIFIED URETHANE PAVEMENT MARKING - LINE 6" | FOOT | 69 | 69 |
| 67100100 |) MOBILIZATION | | L SUM | 1 | 1 | * | 78009012 | MODIFIED URETHANE PAVEMENT MARKING - LINE 12" | FOOT | 52 | 52 |
| 70300150 | SHORT TERM PAVEMENT MARKING REMOVAL | | SQ FT | 4785 | 4785 | * | 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 53 | 53 |
| FranSmo 100 South Wacker Dri Chicago, Illinois 6060 | ve Suite 400 PLOT SCALE = 2.0000 / in. CHECKED S | DU REVISED | | DEPAR | STATE OF I | | | LE INICE 7 (IE: INICE 33 TO OLD 3111 31.) | SECTION 20-172-BR | COUNTY WILL CONTRA D. AID PROJECT | TOTAL SHEETS 29 ACT NO. 62 |

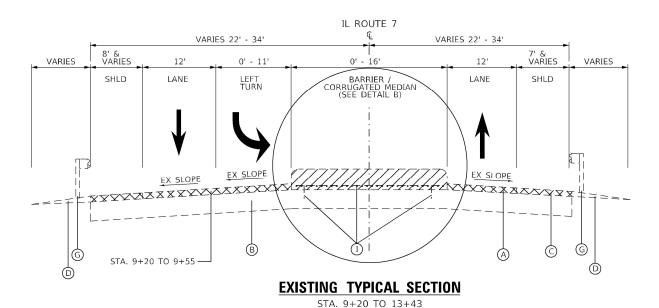
| | | | SUMMARY OF QUANTITIES | -1 | | CONSTR. CODE | | | SUMMARY OF QUANTITIES | | | CONSTR. |
|-----------------------|-----|------------|--|--------|---------------------|---|-----|------------|---|------------------------|--------------------|--------------------------------------|
| SP I TE | | CODE NO | ITEM | UNIT | TOTAL QUANT I TY | 80% FED / 20% STATE 0059 URBAN | SP. | CODE NO | ITEM | UNIT | TOTAL QUANT I T | 80% FED 20% STAT 0059 URBAN |
| | 7 | 8300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 3 | 3 | ø | 20076600 | TRAINEES | HOURS | 500 | 10 |
| | | | | | | | Ø | Z0076604 | TRAINEES - TRAINING PROGRAM GRADUATE | HOURS | 500 | 500 |
| | 7 | 78300202 | PAVEMENT MARKING REMOVAL - WATER BLASTING | SQ FT | 2430 | 2430 | - | | | | | 5 |
| * | 8 | 35000200 | MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION | EACH | 1 | 1 | | | | | | |
| * | 8 | 39502378 | REBUILD EXISTING HANDHOLE TO HEAVY-DUTY HANDHOLE | EACH | 1 | 1 | | | | | | |
| - | × | (0322215 | CLEANING BRIDGE SCUPPERS AND DOWNSPOUTS | EACH | 4 | 4 | | | | | | |
| | | | | | | | | | | | | |
| | х | (6700407 | ENGINEER'S FIELD OFFICE, TYPE A (D1) | CAL MO | 12 | 12 | | | | | | |
| | X | (7010216 | TRAFFIC CONTROL AND PROTECTION, (SPECIAL) | L SUM | 1 | 1 | | | | | | |
| 6 | z | 20001700 | APPROACH SLAB REPAIR (FULL DEPTH) | SQ YD | 3 | 3 | | | | | | |
| 62M55-sht-S00-02.d | Z | 20006014 | BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES | SQ YD | 1159 | 1159 | | | | | | |
| 5 - IL 7 at EJE RRIDI | Z | 20012130 | BRIDGE DECK SCARIFICATION, 3/4" | SQ YD | 1159 | 1159 | | | | | | |
| ADD Sheets/D162M5 | z | 20012754 | STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES) | SQ FT | 10 | 10 | | | | | | |
| roject Work/CADD/C | z | 20030850 | TEMPORARY INFORMATION SIGNING | SQ FT | 51.4 | 51.4 | | | | | | |
| structure Engineerly | z | 20048665 | RAILROAD PROTECTIVE LIABILITY INSURANCE | L SUM | 1 | 1 | | | | | | |
| * * * | z | 20049100 | RAISED PAVEMENT MARKER REFLECTOR REPLACEMENT | EACH | 26 | 26 | | | | | | |
| 2015 - IDOT | | | | | | | | | | | | ø 004 |
| I 100 | rai | nSmo | Suite 400 PLOT SCALE = 2.0000 / in. CHECKED SA REVISED | EACH | DEPAR | STATE OF | | | IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.) F.A.P. RTE. | SECTION 2020-172-BR | COUN | ACT NO. 62M55 |



STA. 5+92 TO 6+64







LEGEND – EXISTING:

- HOT-MIX ASPHALT SURFACE AND BINDER COURSE, 4.5" +/-
- PCC BASE COURSE, 10" +/-
- 0 HOT MIX ASPHALT SHOULDER, 6" +/-
- D AGGREGATE SHOULDER, 6" +/-
- E CONCRETE CURB AND GUTTER
- F CONCRETE MEDIAN SURFACE, 4" +/-
- G
- (H)SAND FILL, 6.5" +/-
- EXISTING DOWEL BARS
- EXISTING MEDIAN REMOVAL

HMA SURFACE REMOVAL - BUTT JOINT

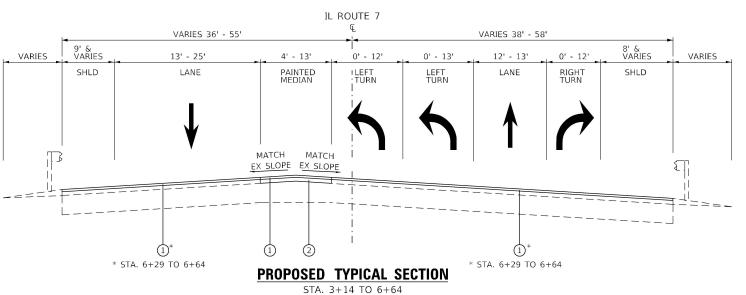
- 1. CONTRACTOR TO CUT EXISTING DOWEL BARS AT CURB AND GUTTER AND MEDIAN TO BE FLUSH WITH EXISTING PCC BASE COURSE. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN MEDIAN REMOVAL.
- 2. THE SAND FILL PRESENT BETWEEN THE EXISTING CONCRETE MEDIAN SURFACE AND PCC BASE COURSE SHALL BE REMOVED AND DISPOSED OF AS EARTH EXCAVATION.
- 3. HMA SURFACE REMOVAL DEPTH SHALL BE 1.75" AT THE END OF THE BUTT JOINT RATHER THAN 1.5" AS SHOWN ON BD-32 "BUTT JOINT AND HMA TAPER DETAILS."



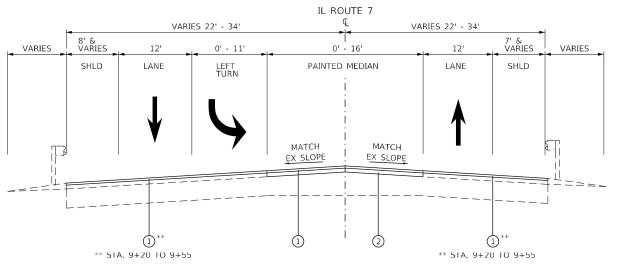
| JSER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
|-----------------------------|------------------|-----------|
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

SCALE:

| | SECT | ION | | COUNTY | SHEETS | NO. |
|---|--------|----------|--------|------------|--------|------|
| 1 | 2020-1 | 72-BR | | WILL | 29 | 5 |
| _ | | | | CONTRACT | NO. 62 | 2M55 |
| | | ILLINOIS | FED. A | ID PROJECT | | |



STA. 3+14 TO 6+64
BRIDGE OMISSION: STA. 6+64 TO 9+20
(SEE BRIDGE PLANS FOR BRIDGE WORK)



PROPOSED TYPICAL SECTION

STA. 9+20 TO 13+43

| HOT-MIX ASPHALT MIXTURE REQUIREMENTS | QUALITY MANAGEMENT | |
|---|-----------------------|-------------------|
| MIXTURE TYPE | AIR VOIDS @ Ndes | PROGRAM (QMP) |
| MEDIAN SURFACE | | |
| HMA SURFACE COURSE, MIX "D", IL-9.5, N70, 1.75" | 4% AT 70 GYR | QC/QA |
| HMA BINDER COURSE, IL-19.0, N70, 2.75" | 4% AT 70 GYR | QC/QA |
| PAVEMENT RESTORATION | | |
| HMA SURFACE COURSE, MIX "D", IL-9.5, N70, 1.75" | 4% AT 70 GYR | QC/QA |
| QMP DESIGNATION: QUALITY CONTROL/ QUALITY ASSURANCE (QA/QC) | : QUALITY CONTROL FOR | PERFORMANCE (QCP) |

NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON -POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS

| TracCoorF | USE |
|----------------------------------|-----|
| Iranəmart | |
| 100 South Wacker Drive Suite 400 | PLO |
| Chicago, Illinois 60606 | PLO |

| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - | |
|-----------------------------|------------------|-----------|--|
| | DRAWN - HDU | REVISED - | |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - | |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - | |

SCALE:

| IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.) PROPOSED TYPICAL SECTIONS | | | | | | F.A.P. RTE. SECTION | | COUNTY | TOTAL SHEETS | SHEET NO. | | |
|---|---------------------------|----|--------|------|---------|------------------------|----------------|----------|-----------------|--------------|------|--|
| | | | | | | 351 | 51 2020-172-BR | | WILL | 29 | 6 | |
| | FROFUSED TIFICAL SECTIONS | | | | | CONTRACT NO. 62 | | | | | 2M55 | |
| | SHEET | OF | SHEETS | STA. | TO STA. | | | ILLINOIS | FED. A | ID PROJECT | | |

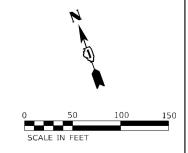
LEGEND - PROPOSED:

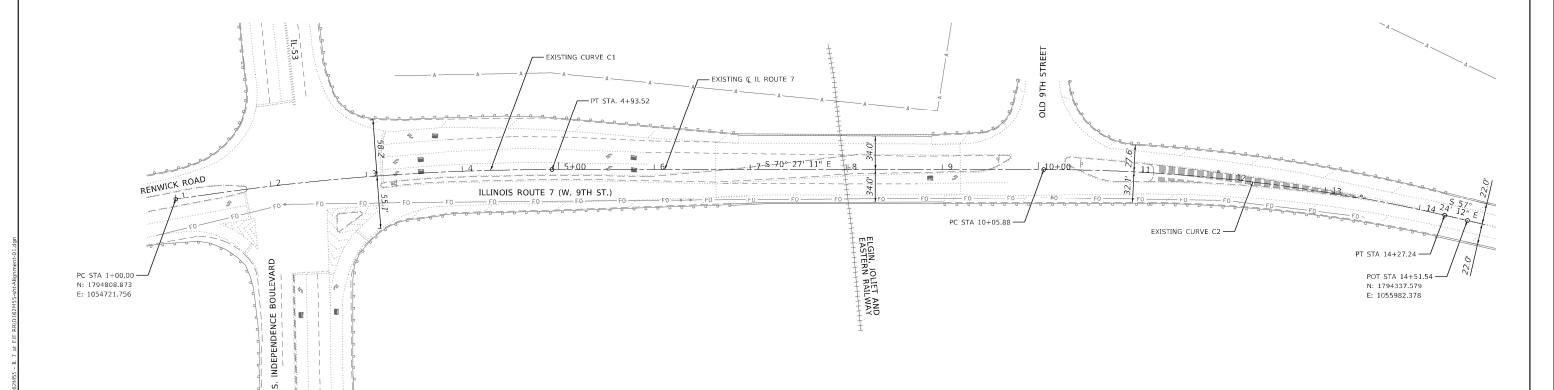
(1) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.75"

2 HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, 2.75"

EXISTING CURVE DATA

EXIST. CURVE C1 PI STA. = 2+97.17 $\Delta = 9^{\circ} 01' 08" (RT)$ EXIST. CURVE C2 PI STA. = 12+17.47 $\Delta = 13^{\circ} 02' 59'' (RT)$ D = 3° 05' 49" R = 1,850.00' T = 211.60' D = 2° 17' 31" R = 2,500.00'T = 197.17'L = 393.52'L = 421.36'E = 7.76E = 12.06'e = N/A T.R. = N/A e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 10+05.88 P.T. STA. = 14+27.24 S.E. RUN = N/AP.C. STA. = 1+00.00 P.T. STA. = 4+93.52





| 3.5 | Tue of contract | U | | | | | |
|-----|----------------------------------|---|--|--|--|--|--|
| 1 | Iransmart | | | | | | |
| | 100 South Wacker Drive Suite 400 | | | | | | |
| | Chicago, Illinois 60606 | Ρ | | | | | |

| | USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
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| STATE OF ILLINOIS | | | |
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| DEPARTMENT | OF | TRANSPORTATION | |

| IL. RTE. 7 | (IL. RT | E. 53 TO | OLD 9 | TH ST.) | F.A.P. RTE. | SECTION |
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MAINTENANCE OF TRAFFIC AND CONSTRUCTION STAGING GENERAL NOTES

- ALL EXISTING SIGNAGE SHALL BE MAINTAINED AND VISIBLE TO TRAFFIC DURING CONSTRUCTION OR AS OTHERWISE SHOWN ON THE PLANS.
- 2. SHOULD CONSTRUCTION CONTRACTS BE ONGOING IMMEDIATELY ADJACENT TO THE WORK SITE, THE CONTRACTOR SHALL COORDINATE MAINTENANCE OF TRAFFIC EFFORTS ACCORDINGLY.

CONSTRUCTION STAGING DESCRIPTIONS

STAGE 1

- 1. REMOVE EXISTING CONFLICTING PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLAN STAGE 1.
- 2. SHIFT IL ROUTE 7 WESTBOUND TRAFFIC TO THE NORTH ADJACENT TO THE EXISTING BRIDGE PARAPET. SHIFT IL ROUTE 7 EASTBOUND TRAFFIC TO THE SOUTH ADJACENT TO THE EXISTING BRIDGE PARAPET.
- 3. REMOVE EXISTING SIGNAGE, BARRIER AND CORRUGATED MEDIAN THROUGHOUT THE PROJECT LIMITS.
- 4. HYDROSCARIFY AND OVERLAY EXISTING BRIDGE WITHIN LIMITS OF WORK ZONE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS AND TYPICAL SECTIONS.
- 5. CONSTRUCT NEW FLUSH MEDIAN THROUGHOUT THE PROJECT LIMITS.

STAGE 2

- 1. PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLAN STAGE 2
- 2. SHIFT IL ROUTE 7 WESTBOUND AND EASTBOUND TRAFFIC TO THE SOUTH.
- 3. HYDROSCARIFY AND OVERLAY NORTHERN SECTION OF THE EXISTING BRIDGE WITHIN LIMITS OF WORK ZONE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.

STAGE 3

- 1. PLACE TEMPORARY PAVEMENT MARKINGS AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLAN STAGE 3.
- 2. SHIFT IL ROUTE 7 WESTBOUND AND EASTBOUND TRAFFIC TO THE NORTH.
- 3. HYDROSCARIFY AND OVERLAY REMAINING SOUTHERN SECTION OF THE EXISTING BRIDGE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS.
- 4. INSTALL PERMANENT PAVEMENT MARKINGS.

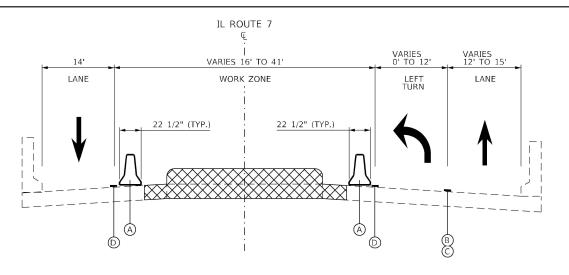
| J. 6. | Tue Concrete* | ι |
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| MANIE. | Iransmart | |
| 4 | 100 South Wacker Drive Suite 400 | F |
| _ | Chicago, Illinois 60606 | F |

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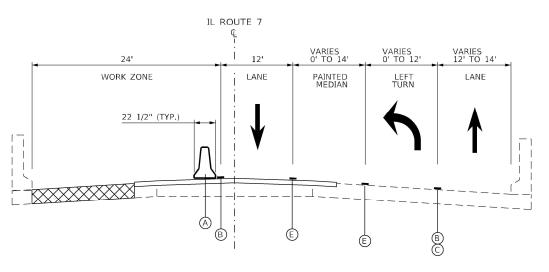
| 10T / | | • | 53 TO OLD N STAGING | 9TH ST.) GENERAL NOTES |
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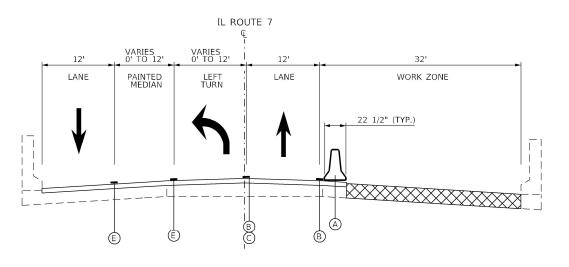
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STAGE 1 ALONG EX IL 7 BRIDGE



STAGE 2 ALONG EX IL 7 BRIDGE



STAGE 3 ALONG EX IL 7 BRIDGE

TranSmart 100 South Wacker Drive Suite 400 Chicago, Illinois 60606

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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

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LEGEND

WORK ZONE

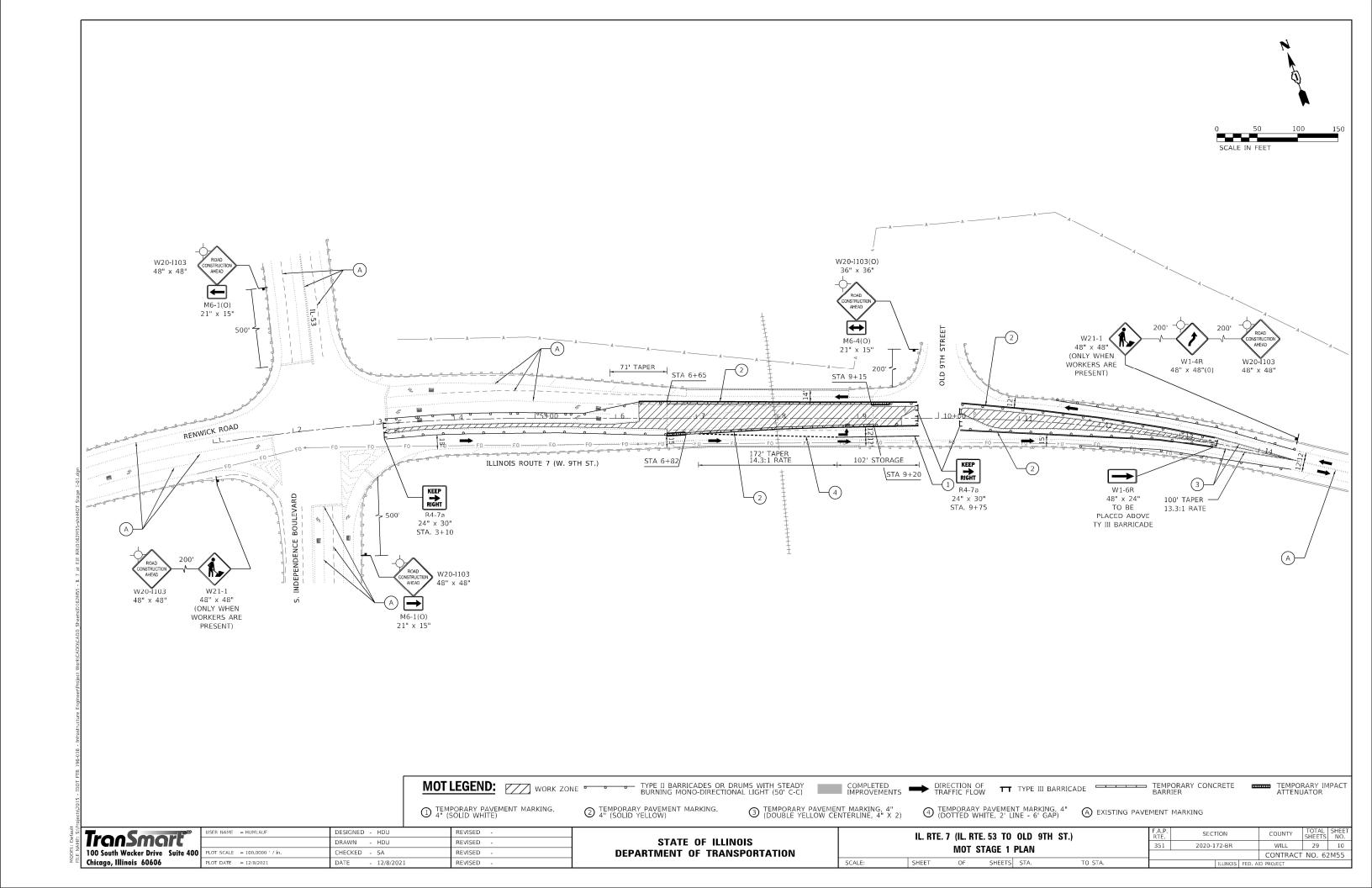
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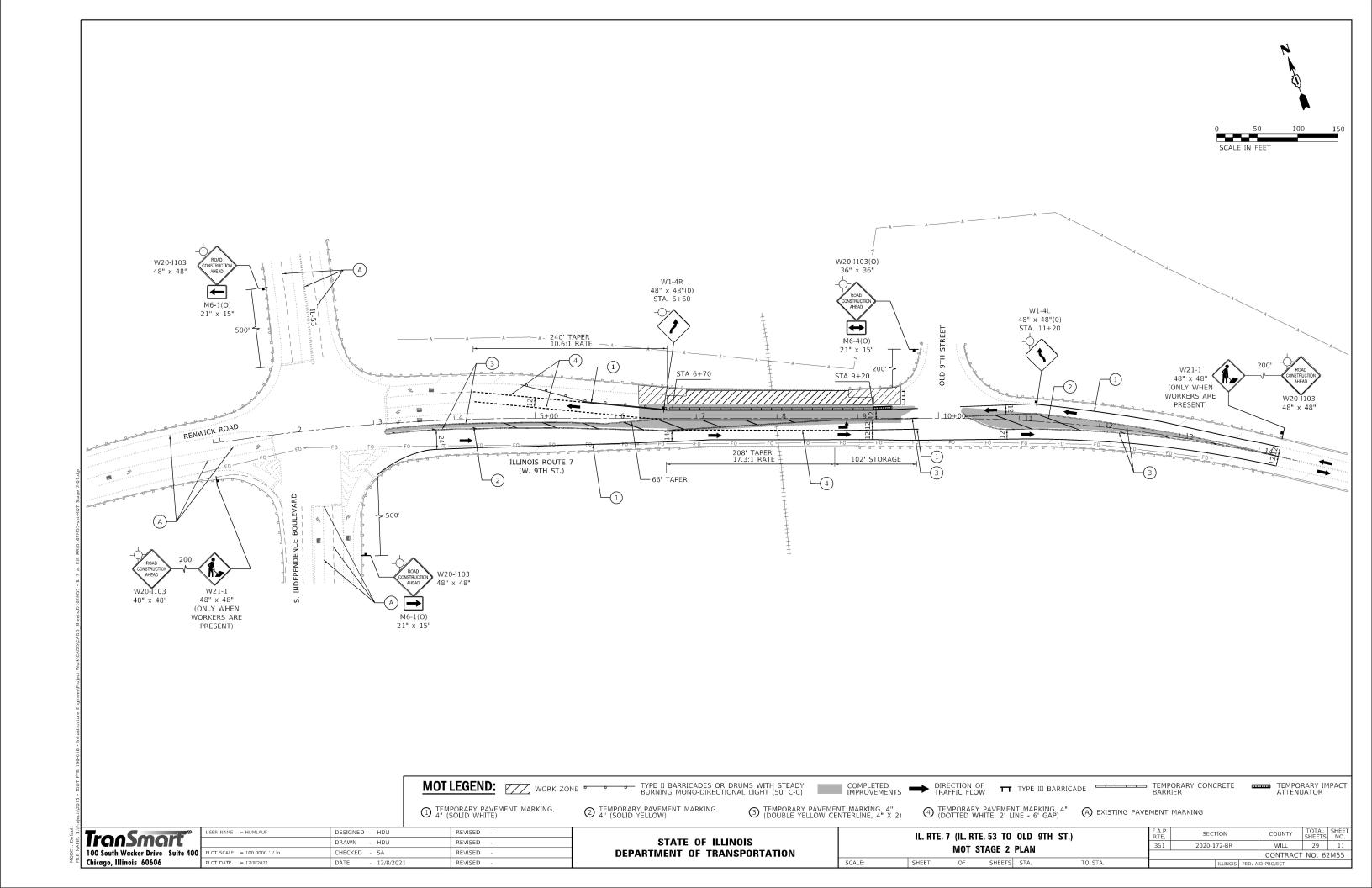
(D) Œ TEMPORARY CONCRETE BARRIER

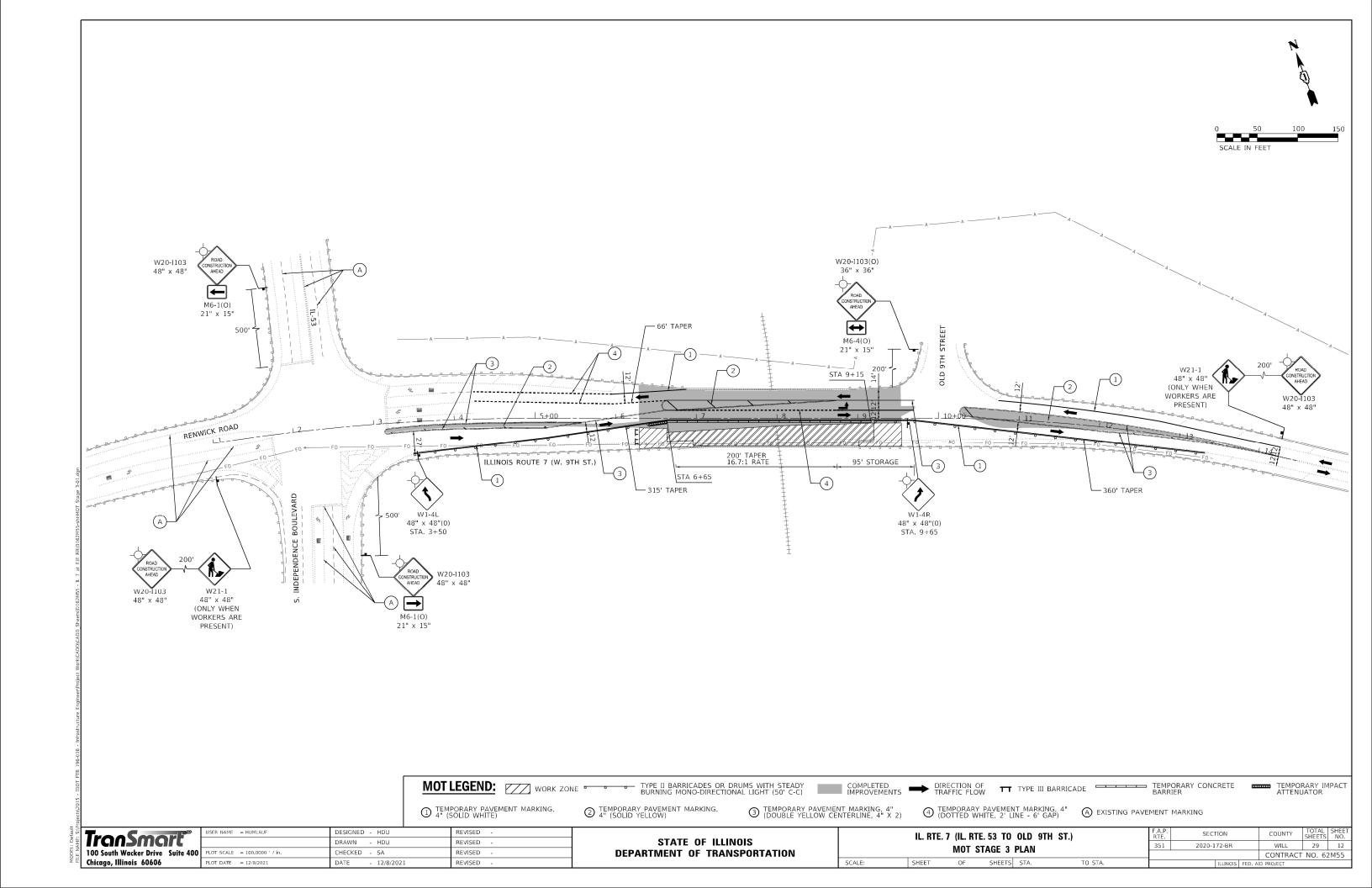
TEMPORARY PAVEMENT MARKING, 4" SOLID WHITE TEMPORARY PAVEMENT MARKING, 4" DOTTED WHITE

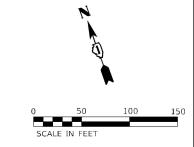
TEMPORARY PAVEMENT MARKING, 4" SOLID YELLOW

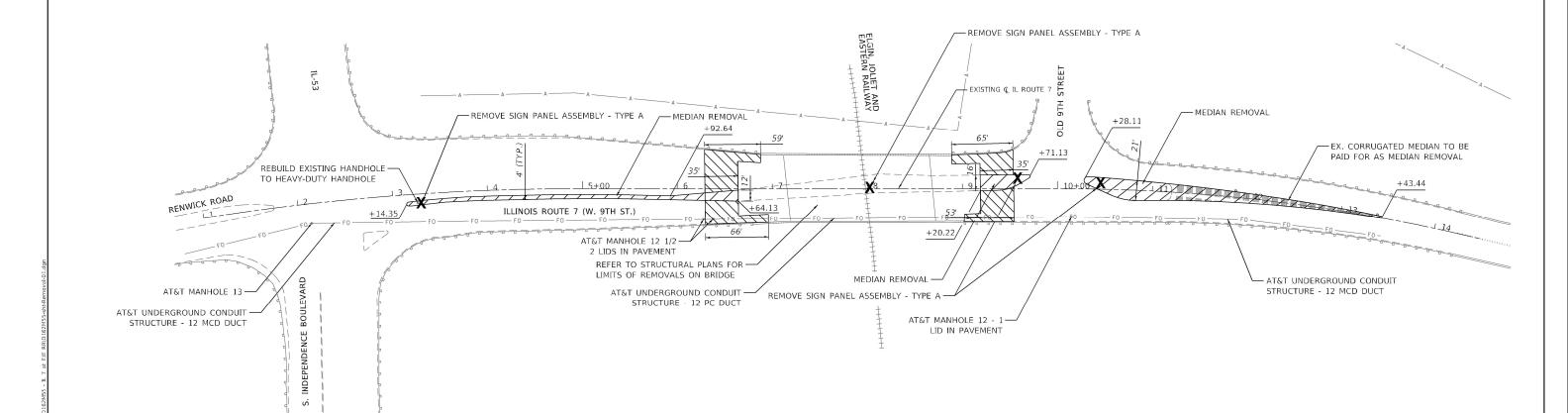
TEMPORARY PAVEMENT MARKING, 4" DOUBLE YELLOW











NOTES:

- 1. MEDIAN REMOVAL SHALL INCLUDE THE REMOVAL AND DISPOSAL OF ANY MATERIALS NECESSARY TO INSTALL THE PROPOSED MEDIAN PAVEMENT, SUCII AS, BUT NOT LIMITED TO, EXISTING CURB AND GUTTER, CONCRETE MEDIAN SURFACE AND DOWEL BARS.
- 2. THE SAND FILL PRESENT BETWEEN THE EXISTING CONCRETE MEDIAN SURFACE AND PCC BASE COURSE SHALL BE REMOVED AND DISPOSED OF AS EARTH EXCAVATION.
- 3. HMA SURFACE REMOVAL DEPTH SHALL BE 1.75" AT THE END OF THE BUTT JOINT RATHER THAN 1.5" AS SHOWN ON BD-32 "BUTT JOINT AND HMA TAPER DETAILS."

LEGEND

SIGN REMOVAL



TO STA.

MEDIAN REMOVAL



HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

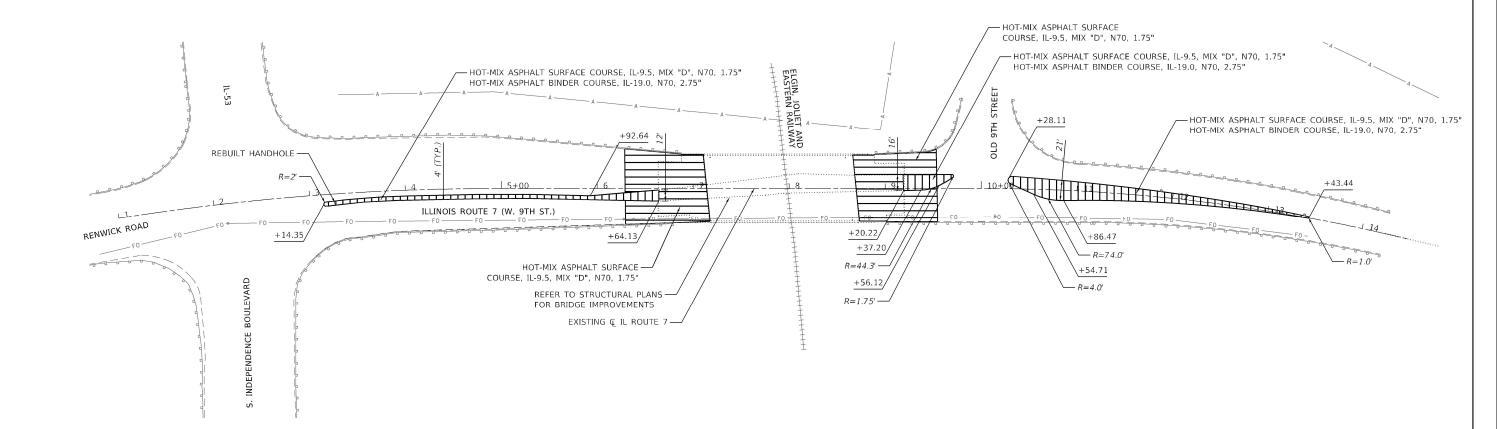


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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

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LEGEND

HMA SURF CSE, IL-9.5, MIX "D", N70, 1.75"

HMA SURF CSE, IL-9.5, MIX "D", N70, 1.75" HMA BINDER CSE, IL-19.0, N70, 2.75"

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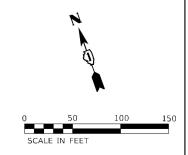
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

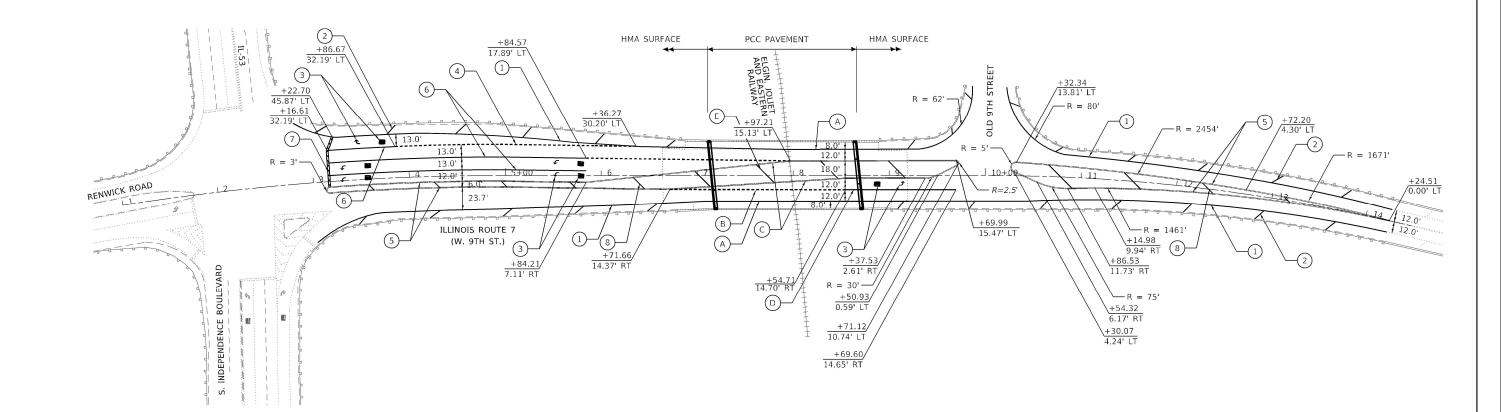
IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.)

ROADWAY PLAN

SHEET OF SHEETS STA.

| CONTRACT NO. 62M55 | ILLINOIS | FED. AID PROJECT |





LEGENI

- 1) PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", WHITE SOLID LINE
- 2 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", WHITE SOLID LINE
- 3) PROPOSED THERMOPLASTIC PAVEMENT MARKING LETTERS AND SYMBOLS
- (4) PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", WHITE 6' SKIP 2' DASH
- (5) PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4", DOUBLE YELLOW SOLID LINE
- (6) PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 6", WHITE SOLID LINE
- 7 PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 24", WHITE SOLID LINE
- (8) PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 12", YELLOW SOLID LINE @ 75' SPACING
- (A) PROPOSED MODIFIED URETHANE PAVEMENT MARKING LINE 4", WHITE SOLID LINE
- B PROPOSED MODIFIED URETHANE PAVEMENT MARKING LINE 6", WHITE 6' SKIP 2' DASH
- © PROPOSED MODIFIED URETHANE PAVEMENT MARKING LINE 4", DOUBLE YELLOW SOLID LINE
- (D) PROPOSED MODIFIED URETHANE PAVEMENT MARKING LINE 6", WHITE SOLID LINE
- E PROPOSED MODIFIED URETHANE PAVEMENT MARKING LINE 12", YELLOW SOLID LINE @ 75' SPACING

NOTES:

SHEETS STA.

SHEET

- 1. MODIFIED URETHANE PAVEMENT MARKINGS TO BE USED ON PCC SURFACES.
- 2. THERMOPLASTIC PAVEMENT MARKINGS TO BE USED ON HMA SURFACES.
- 3. ALL PAVEMENT MARKINGS TO BE INSTALLED ACCORDING TO IDOT D1 STANDARD TC-13.
- 4. ALL RAISED REFLECTIVE PAVEMENT MARKERS TO BE INSTALLED ACCORDING TO IDOT D1 STANDARD TC-11

| : S:\Pr | TracCoort | |
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| FILE NAME: | Iransmart | |
| Щ | 100 South Wacker Drive Suite 400 | |
| Ē | Chicago, Illinois 60606 | |

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| IL. RTE. 7 (IL. RTE. 53 TO OLD 9TH ST.) | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
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| PAVEMENT MARKING PLAN | 351 | 2020-172-BR | WILL | 29 | 15 |
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BENCHMARK

None.

EXISTING STRUCTURE

Built in 1990 Structure Number 099-0274. Existing bridge is three spans, 156'-7½" back to back of abutments, out to out width is 71'-2". Superstructure is W27x146 steel girders, composite in positive regions, supporting a cast in place concrete deck. Substructure is stub abutments on steel H piles and two multi column piers supported on spread footing.

Traffic will be maintained utilizing staged construction.

No salvage.

SCOPE OF WORK

- Remove and reconstruct bridge deck expansion joints at abutments and install new preformed joint strip seals.
- Perform deck slab and approach slab repairs as required.
- 3. Perform ¾" hydroscarification and apply 2½" bridge deck latex overlay on bridge deck.
- l. Clean bridge deck scuppers.
- 5. Perform bridge deck sawcut grooving to deck
- Apply Protective Coat to new deck slab, new overlay, and top and inside face of new parapets.

INDEX OF SHEETS

- . General Plan and Elevation
- General Notes and Total Bill of Material
- 3. Stage Construction
- 4. Deck Plan and Section
- 5. Repair Details
- . Temporary Concrete Barrier
- Preformed Joint Strip Seal
- 8. Bar Splicer Assembly and Mechanical Splicer

<u>DESIGN STRESSES</u>

FIELD UNITS (EXISTING)

Bridges, 17th Edition

f'c = 3,500 psi

fy = 60,000 psi (Reinforcement)

fy = 36,000 psi (Struct.) (M183)

FIELD UNITS (NEW CONSTRUCTION)

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway

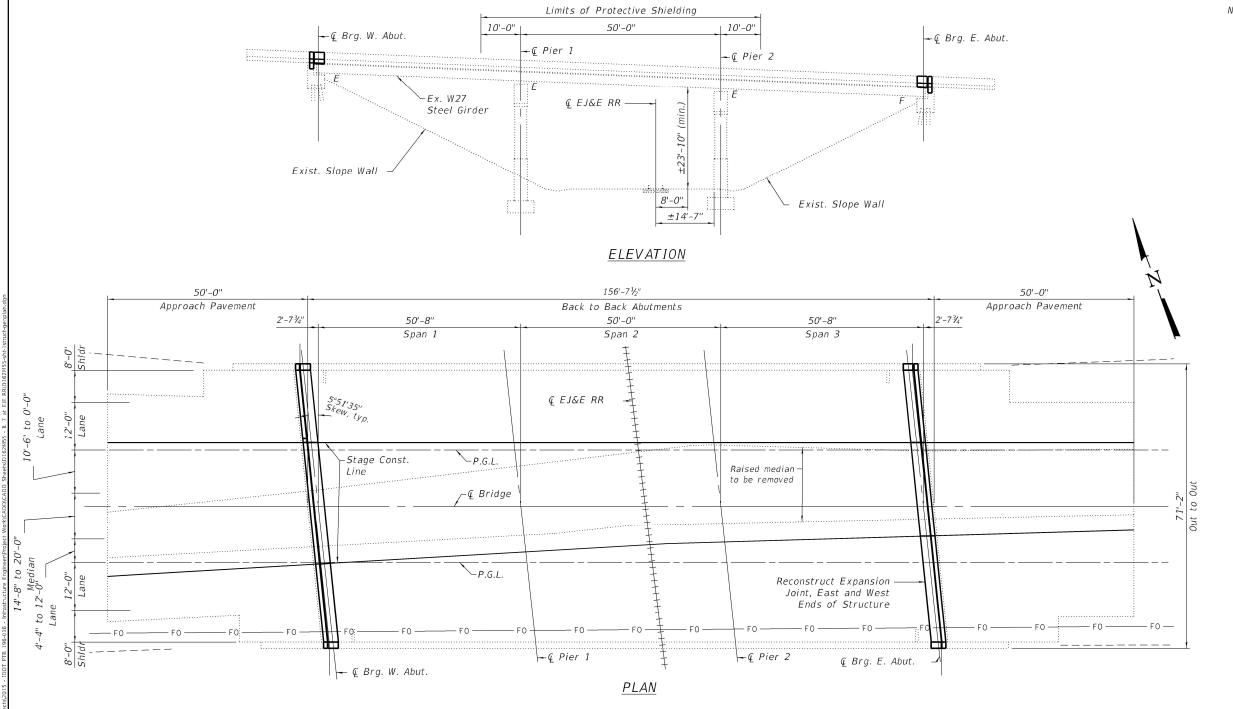
f'c = 3,500 psi

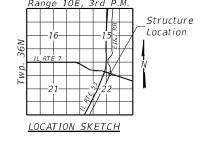
f'c = 4,000 psi (Superstructure)

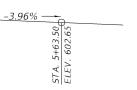
fy = 60,000 psi (Reinforcement)

LOADING HS 20-44 (EXIST.)

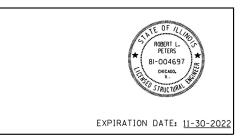
No allowance for future wearing surface.







PROFILE GRADE LINE IL RTE. 7 Note: Profile grade line based on as-built plans



GENERAL PLAN & ELEVATION
ILLINOIS RTE. 7 (W 9TH ST)
OVER EJ&E RAILROAD

F.A.P. RTE. 351 SECTION 2020-172-BR

WILL COUNTY
STA 5+53.84

STRUCTURE NO. 099-0274

Transmart

100 South Wacker Drive Chicago, Illinois 60606

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 099 - 0274

SHEET 1 OF 8 SHEETS STA.

TO STA.

 F.A.P. RTE.
 SECTION
 COUNTY
 TOTAL SHEETS
 SHEET NO.

 351
 2020-172-BR
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 CONTRACT NO. 62M55

GENERAL NOTES

- 1. Reinforcement bars designated (E) shall be epoxy coated.
- 2. Plan dimensions and details relative to existing plans are subject to normal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for work.
- 3. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- 4. All exposed concrete edges shall have a $\frac{3}{4}$ "x45° chamfer except where shown otherwise.
- 5. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete. As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer. Any cracks that cannot be removed by grinding \(^1\)4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.
- 6. Protective Coat shall be applied to the designated areas of the deck and parapets.
- 7. The concrete for bridge decks finished according to Article 503.16(a) of the Standard Specifications shall be placed and compacted parallel to the skew in uniform increments along centerline of bridge. The machine used for finishing shall be set parallel to the skew for striking off and screeding the concrete.
- 8. Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50°F.

TOTAL BILL OF MATERIAL

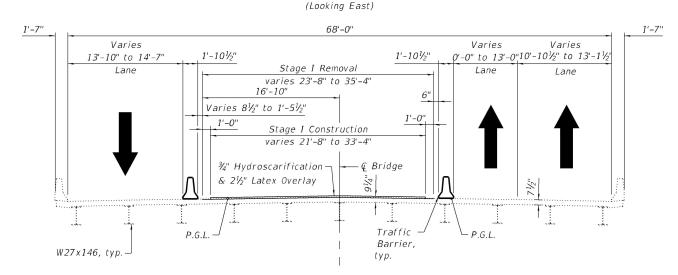
| ITEM | UNIT | SUPER | SUB | _ TOTAL |
|--|-------|-------|-----|---------|
| Concrete Removal | Cu Yd | 100.3 | | 100.3 |
| Protective Shield | Sq Yd | 560 | | 560 |
| Concrete Superstructure | Cu Yd | 24.4 | | 24.4 |
| Bridge Deck Grooving | Sq Yd | 1,159 | | 1,159 |
| Reinforcement Bars, Epoxy Coated | Pound | 3,000 | | 3,000 |
| Protective Coat | Sq Yd | 1,186 | | 1,186 |
| Bar Splicers | Each | 52 | | 52 |
| Preformed Joint Strip Seal | Foot | 144 | | 144 |
| Clean Bridge Scuppers and Downspouts | Each | 4 | | 4 |
| Approach Slab Repair (Full Depth) | Sq Yd | 3 | | 3 |
| Bridge Deck Latex Concrete Overlay, 2 1/2 Inches | Sq Yd | 1,159 | | 1,159 |
| Bridge Deck Scarification, 3/4" | Sq Yd | 1,159 | | 1,159 |
| Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches) | Sq Ft | 10 | | 10 |
| | | | | |
| | | | | |
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| USER NAME = kchamberlain | DESIGNED - PAF | REVISED - |
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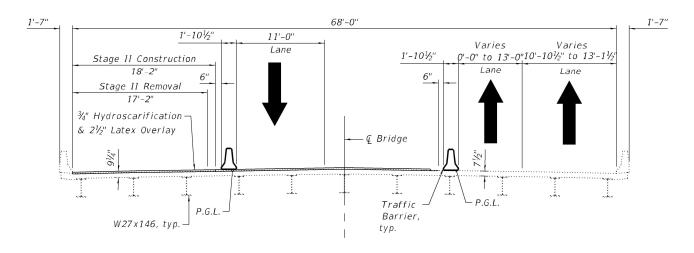
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| GENERAL NOTES AND TOTAL BILL OF MATERIALS | | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | | | |
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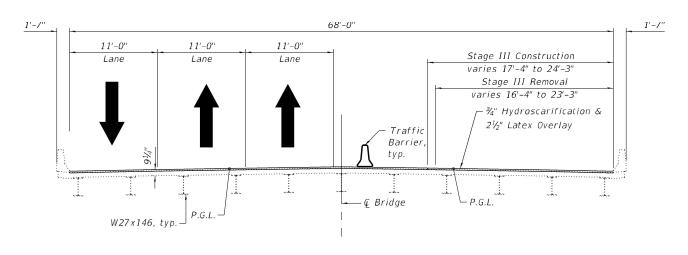
STAGE I MEDIAN REMOVAL



$\frac{STAGE\ I\ DECK\ REMOVAL\ AND\ CONSTRUCTION}{(Looking\ East)}$



$\frac{\textit{STAGE II DECK REMOVAL AND CONSTRUCTION}}{\textit{(Looking East)}}$



STAGE III DECK REMOVAL AND CONSTRUCTION (Looking East)

NOTES

1. See MOT plans for additional details.

Transmart**
100 South Wacker Drive Suite 400
Chicago, Illinois 60606

 USER NAME
 = kchamberlain
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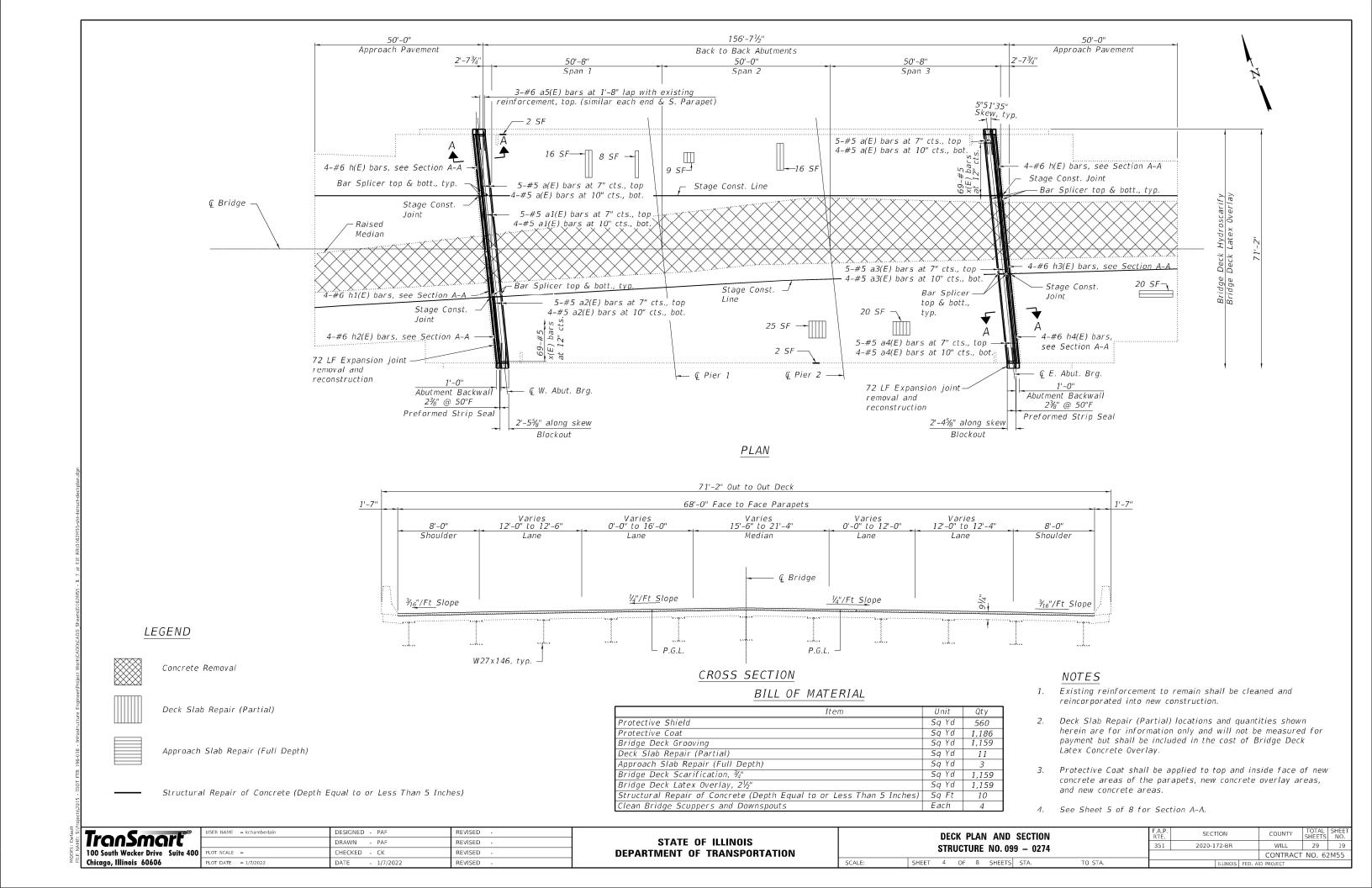
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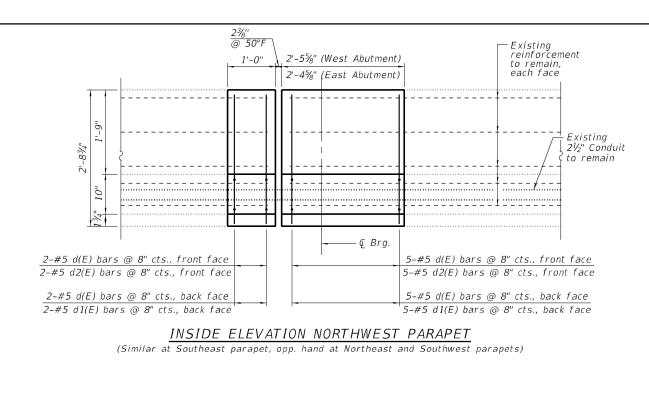
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION
STRUCTURE NO. 099 — 0274

SHEET 3 OF 8 SHEETS STA. TO STA.

A.P. SECTION COUNTY TOTAL SHEETS NO. 351 2020-172-BR WILL 29 18 CONTRACT NO. 62M55

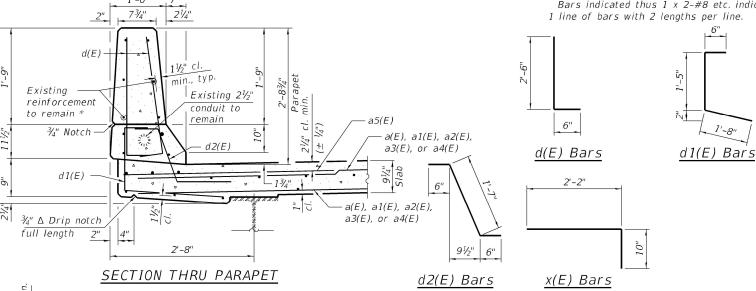


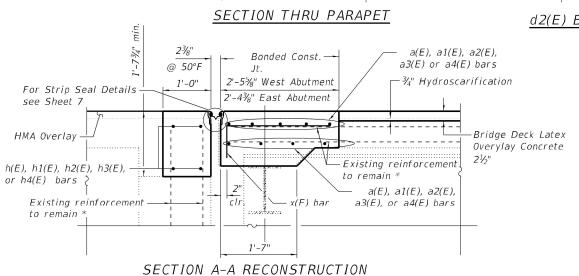


SUPERSTRUCTURE BILL OF MATERIAL Bar No. Size Length Shape

| a(E) | 18 | #5 | 18'-7" | |
|----------|----------|---------|---------|-------|
| a1(E) | 9 | #5 | 32'-1" | |
| a2(E) | 9 | #5 | 20'-1" | |
| a3(E) | 9 | #5 | 25'-1" | |
| a4(E) | 9 | #5 | 27'-1" | |
| a5(E) | 12 | #6 | 4'-0'' | |
| | | | | |
| d(E) | 56 | #5 | 3'-0" | ٦ |
| d1(E) | 28 | #5 | 3'-7" | ١ |
| d2(E) | 28 | #5 | 2'-7" | \ |
| | | | | |
| h(E) | 8 | #6 | 17'-0" | |
| h1(E) | 4 | #6 | 32'-2" | |
| h2(E) | 4 | #6 | 18'-5" | |
| h3(E) | 4 | #6 | 25'-1" | |
| h4(E) | 4 | #6 | 25'-6" | |
| | | | | |
| x(E) | 138 | #5 | 3'-0" | ٦ |
| | | | | |
| | | | | |
| Reinfo | rcemen | t Bars, | Pound | 2 000 |
| Ероху | Coated | 1 | Pouna | 3,000 |
| Concrete | | | Cu. Yd. | 24.4 |
| Super: | structui | re | cu. ru. | 24.4 |
| Concre | ete | | Cu. Yd. | 100.3 |
| Remov | al | | cu. Tu. | 100.5 |
| | | | · | |
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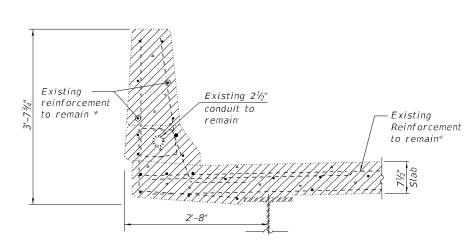
Bars indicated thus 1 x 2-#8 etc. indicates 1 line of bars with 2 lengths per line.



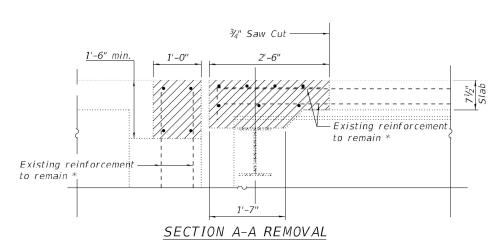


LEGEND

Concrete Removal



SECTION THRU PARAPET



* If existing reinforcement is fully exposed or has greater than 25% section loss shall be replaced in kind with minimum lap or mechanical splicer.

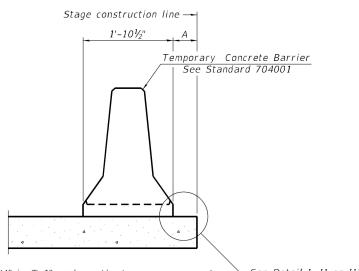
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| Iransmart | |
| 100 South Wacker Drive Suite 400 | PL |
| Chicago, Illinois 60606 | PL |

| USER NAME = kchamberlain | DESIGNED - PAF | REVISED - |
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| | DRAWN - PAF | REVISED - |
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| PLOT DATE = 1/4/2022 | DATE - 1/4/2022 | REVISED - |

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

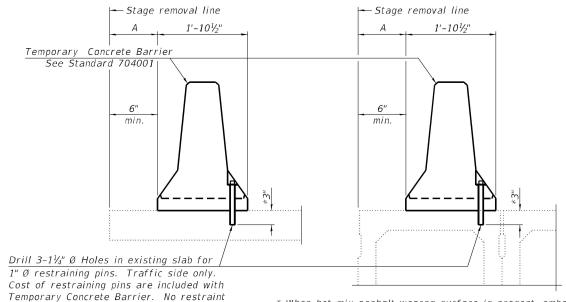
| REPAIR DETAILS | | | | | | | F.A.P. RTE. SEC | | |
|--------------------------|---|----|------|--------|-------|---------|--------------------|--|--|
| STRUCTURE NO. 099 - 0274 | | | | | | 351 | 2020-17 | | |
| | • | | J.,_ | 110.00 | 0 02, | • | | | |
| SHEET | 5 | OF | 8 | SHEETS | STA. | TO STA. | | | |

172-BR WILL 29 20 CONTRACT NO. 62M55



∽ See Detail I, II or III When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



* When hot-mix asphalt wearng surface is present, embedment shall be 3" plus the wearing surface depth.

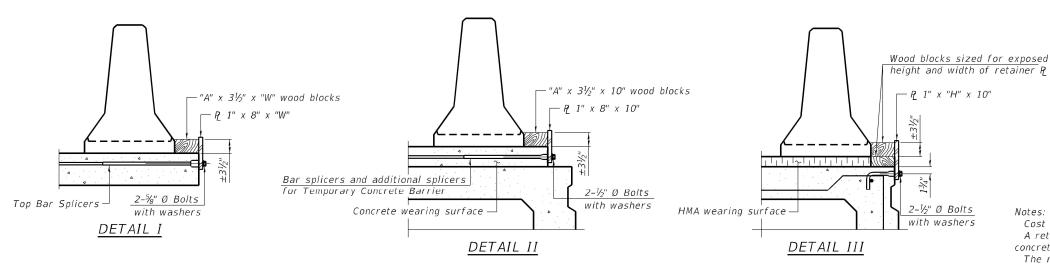
EXISTING DECK BEAM

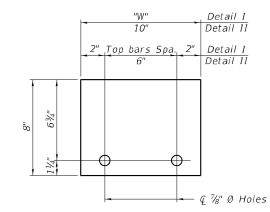
1x8 UNC US Std. 1½6" I.D. x 2½" O.D. x approx. 8 guage thick washer RESTRAINING PIN

SECTIONS THRU SLAB OR DECK BEAM

is required when "A" is greater than 3'-1".

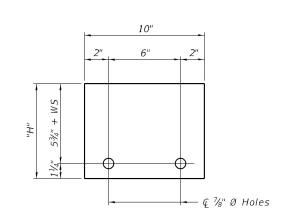
EXISTING SLAB





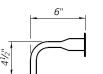
STEEL RETAINER P 1" x 8" x "W"

(Detail I and II)



STEEL RETAINER P 1" x "H" x 10" (Detail III)

SCALE:



BAR SPLICER FOR #4 BAR - DETAIL III

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier. A retainer assembly shall be located at the approximate (of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than $1\frac{1}{2}$ ", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

- Detail I Installation for a new bridge deck or bridge slab.
- Detail II Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27

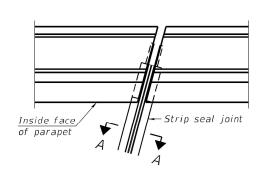
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STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

| TEMPORA | RY CO | NCRI | ETE I | BAR | RIER FO | OR STAG | E CONSTRUCTION | F.A.P. RTE. | SECT | TON | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------|-----------------------------|------|-------|------|---------|----------|----------------|----------------|--------|----------------|------------|-----------------|--------------|
| | | STR | пст | IIRF | NO OO | 9 - 0274 | | 351 | 2020-1 | 72-BR | WILL | 29 | 21 |
| | 0111001011L 110: 033 - 02/4 | | | | | | | | | CONTRAC | T NO. 63 | 2M55 | |
| : | SHEET | 6 | OF | 8 | SHEETS | STA. | TO STA. | | | TILINOIS FED A | ID PROJECT | | |

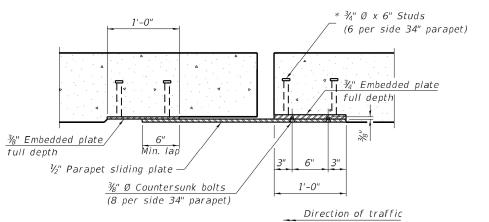


2" Max.

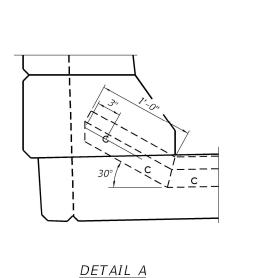
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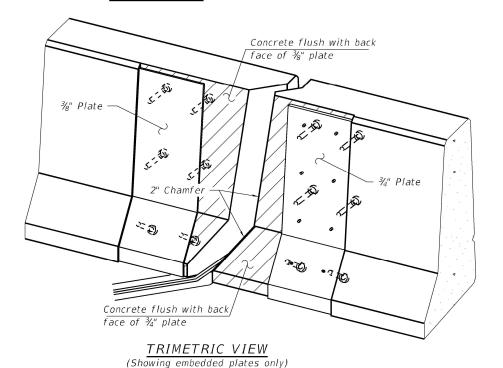
%" Ø x 6" Studs

Detail A



SECTION B-B





The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4½" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be $\frac{3}{6}$ and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal. 34" F-shape barrier shown, 42" F-shape similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

ELEVATION AT PARAPET

6" cts., typ.

Parapet sliding

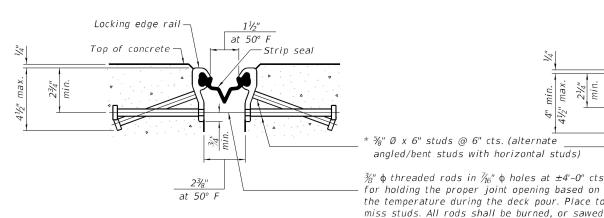
Inside Face

Top of locking

Top of deck

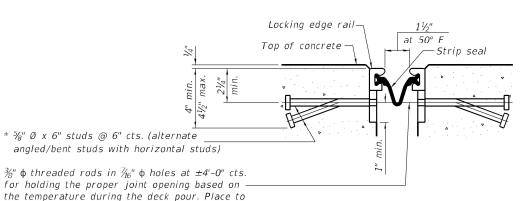
edge rail

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

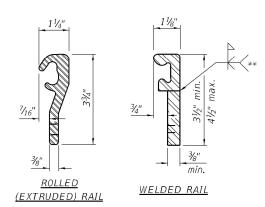


8-11-17

SHOWING ROLLED RAIL JOINT

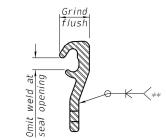


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

| Item | Unit | Total |
|----------------------------|------|-------|
| Preformed Joint Strip Seal | Foot | 144 |
| | | |

SECTION A-A

off flush with the plates after concrete is set.

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

TranSmart 100 South Wacker Drive Suite 400 Chicago, Illinois 60606

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STATE OF ILLINOIS

| PREFORMED JOINT STRIP | SEAL |
|------------------------|------|
| | |
| STRUCTURE NO. 099 - 02 | .74 |
| | |

| F.A.P. RTE. | SECTION | | COUNTY | TOTAL SHEETS | SHE |
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| | | | CONTRACT | NO. 62 | 2M55 |
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DEPARTMENT OF TRANSPORTATION

SCALE:

SHEET 7 OF 8 SHEETS STA.

TO STA.

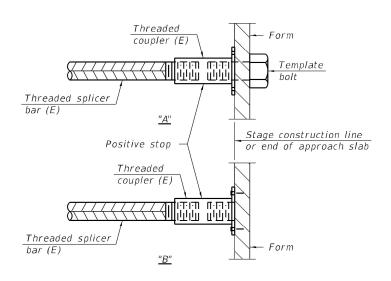
STANDARD BAR SPLICER ASSEMBLY PLAN

(All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + $1\frac{1}{2}$ " + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

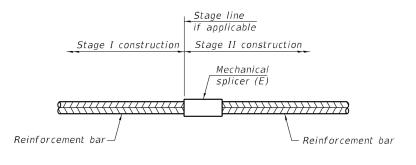
| Location | Bar size | No. assemblies required | Minimum Iap length |
|---------------|-------------|----------------------------|-----------------------|
| East Abutment | #6 | 8 | 4'-0'' |
| West Abutment | #6 | 8 | 4'-0'' |
| East Deck | #5 | 18 | 3'-6" |
| West Deck | #5 | 18 | 3'-6" |
| | | | |



INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
"B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

| size | required |
|------|----------|
| | |
| | |
| | |

Notes:

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

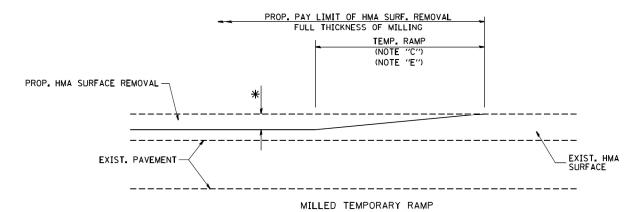
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| FILE NAME: | Iran>mart | |
| E N | 100 South Wacker Drive Suite 400 | PL |
| Ē | Chicago, Illinois 60606 | PL |

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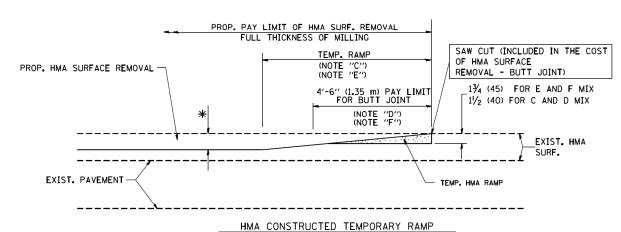
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(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

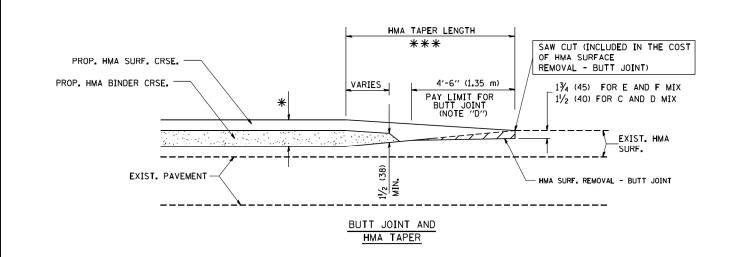
OPTION 1



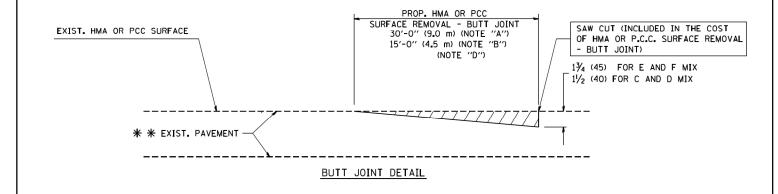
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

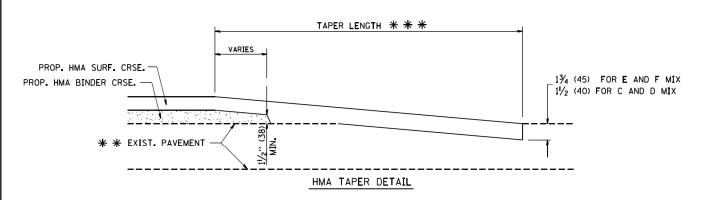
OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING





TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- ** ** * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A") 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SOUARE YARD (SOUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

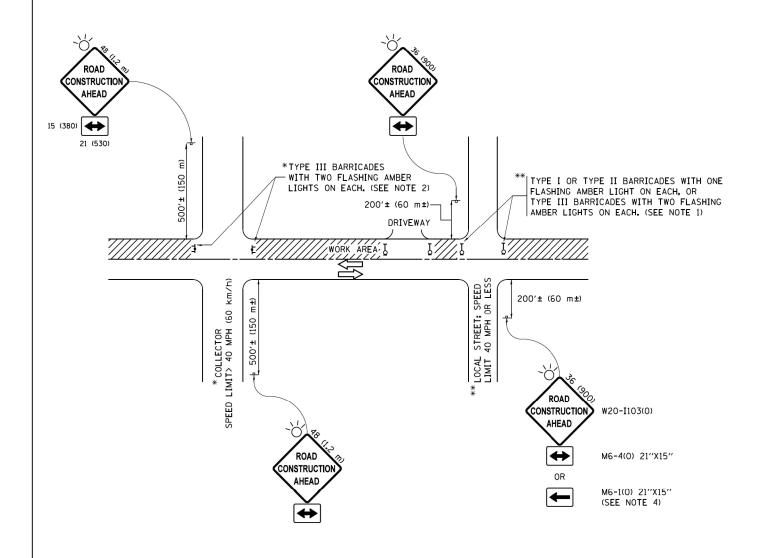
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<u>TranSmart</u> 100 South Wacker Drive Suite 400 Chicago, Illinois 60606

DESIGNED - HDU REVISED DRAWN - HDU REVISED CHECKED - SA REVISED PLOT DATE = 12/8/2021

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

SECTION **BUTT JOINT AND** 2020-172-BR WILL **HMA TAPER DETIALS** CONTRACT NO. 62M55 BD-32 TO STA.



NOTES:

- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200" (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 \times 48 (1.2 m \times 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500" (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEICHT
- 4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- 5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- 6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER
- 7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

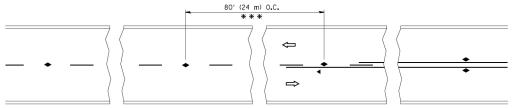


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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

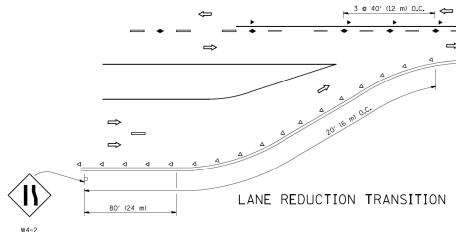
| | | | | TION FOR DRIVEWAYS |
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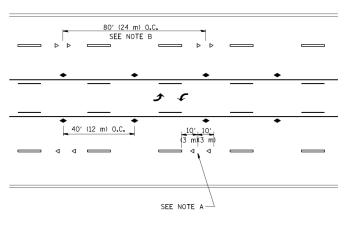
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| | TC-1 | CONTRACT | NO. 62 | 2M5 | | |
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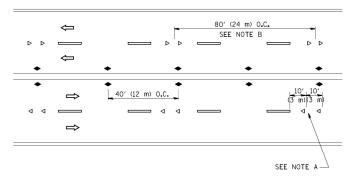
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO-LANE/TWO-WAY

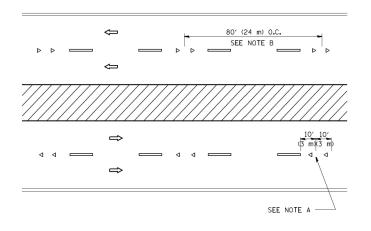




TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

- 1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
- 2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
- 3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

SYMBOLS

- ---- YELLOW STRIPE
- ── WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

DESIGN NOTES

- 1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
- 2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RICHT EDGE LINES.
- 3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
- MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

Transmart*

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Chicago, Illinois 60606

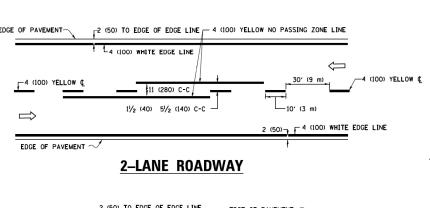
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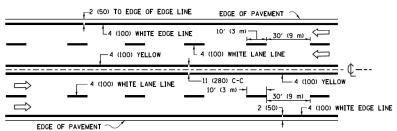
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR RAISED REFLECTIVE PAVEMENT MARKINGS (SNOW-PLOW RESISTANCE)

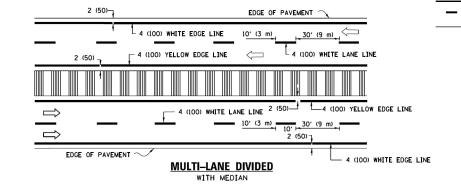
F.A.P. SECTION COUNTY TOTAL SHEETS NO.
351 2020-172-BR WILL 29 26

TC-11 CONTRACT NO. 62M55

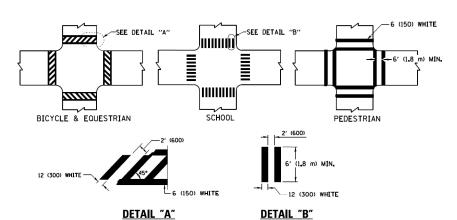




MULTI-LANE UNDIVIDED



TYPICAL LANE AND EDGE LINE MARKING

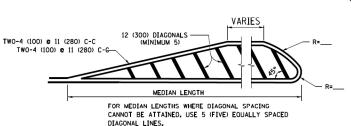


TYPICAL CROSSWALK MARKING

 $\mbox{\#}$ markings shall be installed parallel to the centerline of the road which it crosses

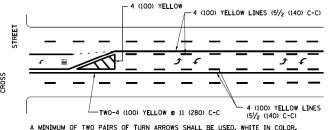
TWO-4 (100) YELLOW • 11 (280) C-C NO DIAGONALS 4' (1,2 m) OUTSIDE TO OUTSIDE OF LINES TWO-4 (100) YELLOW • 11 (280) C-C

4' (1.2 m) WIDE MEDIANS ONLY

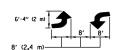


DIAGONAL LINE SPACING 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

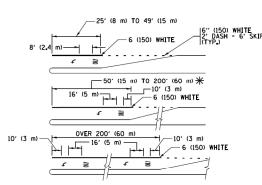


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR, ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

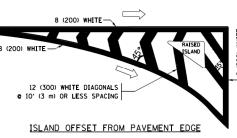
TYPICAL PAINTED MEDIAN MARKING

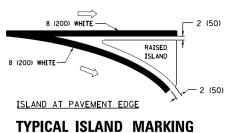


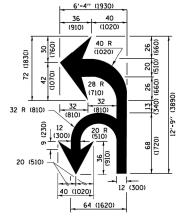
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

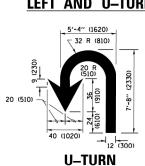
TYPICAL TURN LANE MARKING







COMBINATION LEFT AND U-TURN



|| || || || ||
LANE REDUCTION TRANSITION

D(FT)

345

425

500

580

665

750

SPEED LIMIT

30

35

50

55

 \divideontimes Lane reduction arrows required at speeds of 45 mph or greater or when specified in plans.

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING /REMARKS |
|---|---|------------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 a 4 (100) | SOLID SOLID | YELLOW YELLOW | 5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) ON FREEWAYS | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MEDIANS IN YELLOW |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½; (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EOUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 e 6 (150) 12 (300) e 45° 12 (300) e 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART 5EE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4,5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) T0 45MPH (70 km/h)) 30' (9 m) C-C (0VER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF. "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²) |
| SHOULDER DIAGONALS (REOUIRED FOR SHOULDERS ≥ 8') | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (0VER 45MPH (70 km/h)) |
| U TURN ARROW | SEE DETAIL | SOLID | WHITE | 16.3 SF |
| 2 ARROW COMBINATION LEFT AND U TURN | SEE DETAIL | SOLID | WHITE | 30.4 SF |

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimete

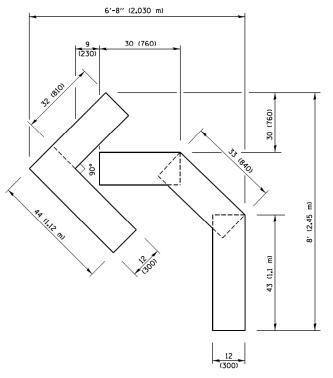
Transmart

100 South Wacker Drive Suite 400
Chicago, Illinois 60606

| JSER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
|-----------------------------|------------------|-----------|
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

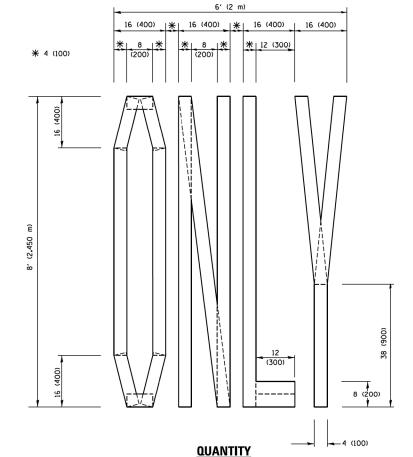
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| TYPICAL PAVEMENT MARKINGS | | | | | F.A.P. RTE. | SECT | ION | | COUNTY | TOTAL SHEETS | SHEET NO. | |
|---------------------------|-------|----|--------|------|----------------|--------|-------|----------|--------|-----------------|--------------|------|
| | | | | | 351 | 2020-1 | 72-BR | | WILL | 29 | 27 | |
| | | | | | | | TC-13 | 3 | | CONTRACT | NO. 62 | 2M55 |
| SCALE: | SHEET | OF | SHEETS | STA. | TO STA. | | | ILLINOIS | FED. A | ID PROJECT | | |

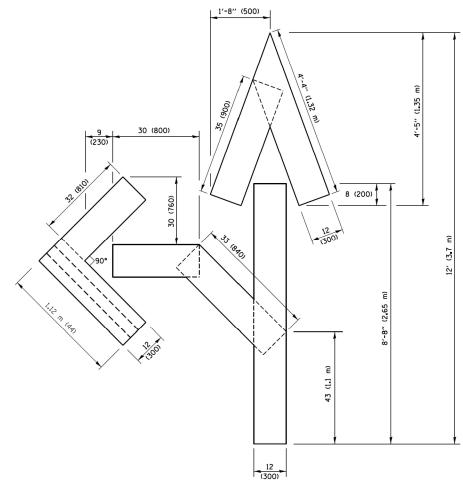


QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m) 15.2 sq. ft. (1.41 sq. m)



4 (100) LINE = 64.1 ft. (19.5 m) 21.4 sq. ft. (1.99 sq. m)

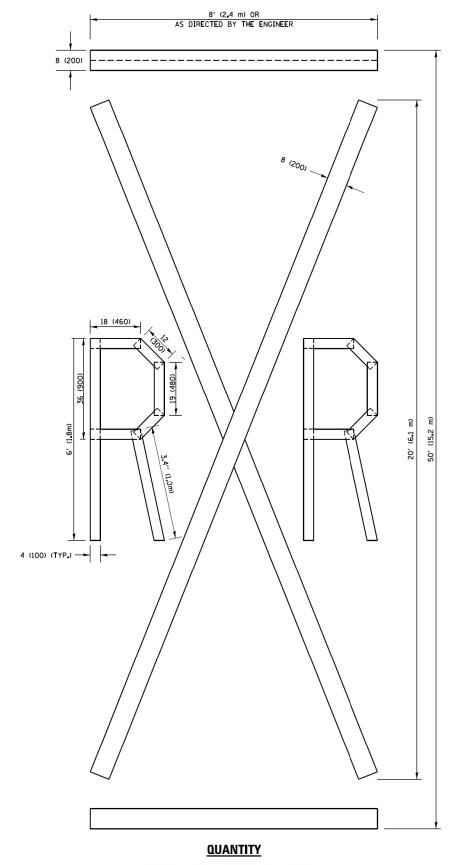


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m) 27.5 sq. ft. (2.53 sq. m)

NOTE

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



4 (100) LINE = 225.9 ft. (68.9 m) 75.3 sq. ft. (6.99 sq. m)

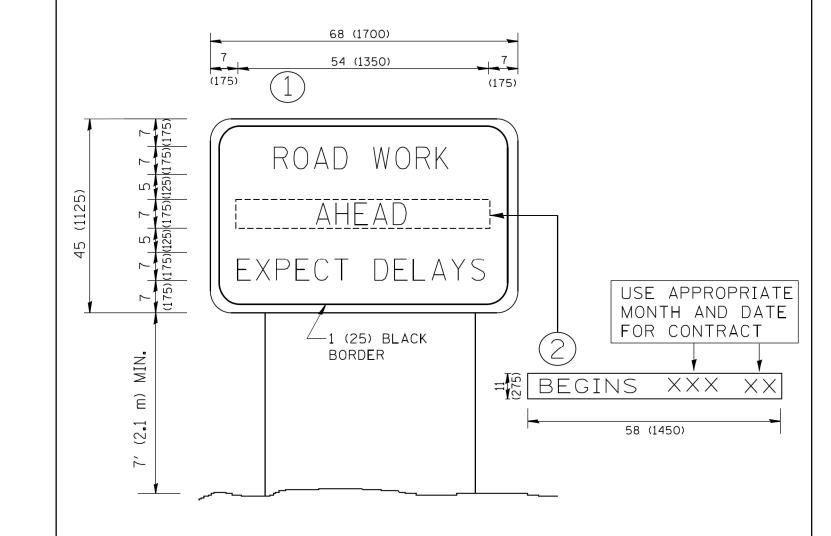
All dimensions are in inches (millimeters) unless otherwise shown.



| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
|-----------------------------|------------------|-----------|
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |
| | | |

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

| | SHORT TERM PAVEMENT MARKING | | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--|-----------------------------|----|--------|-----|--------|----------------|---------------|-----------|-----------------|--------------|
| | LETTER AND SYMBOLS | | | | 351 | 2020-172-BR | WILL | 29 | 28 | |
| | | | | | | TC-16 | CONTRACT | NO. 62 | 2M55 | |
| | SHEET | OF | SHEETS | STA | TO STA | | TUTMOTE FED A | D DDOLECT | | |



NOTES:

SCALE:

- 1. USE BLACK LETTERING ON ORANGE BACKGROUND.
- 2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
- 3. ERECT SIGN () WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
- 4. REMOVE PANEL 2) SOON AFTER THE START OF CONSTRUCTION.
- 5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
- 6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
- 7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | Tuo o Coo o e F | USE |
|---|----------------------------------|-----|
| | TranSmart" | |
| | 100 South Wacker Drive Suite 400 | PLC |
| 1 | Chicago, Illinois 60606 | PLC |

| USER NAME = HUMLAUF | DESIGNED - HDU | REVISED - |
|-----------------------------|------------------|-----------|
| | DRAWN - HDU | REVISED - |
| PLOT SCALE = 1.0000 ' / in. | CHECKED - SA | REVISED - |
| PLOT DATE = 12/8/2021 | DATE - 12/8/2021 | REVISED - |

| STAT | E 01 | F ILLINOIS |
|------------|------|----------------|
| DEPARTMENT | 0F | TRANSPORTATION |

| | | | | | F.A.P. RTE. | SECTION | COUNTY | TOTAL SHEETS | | |
|--|-------|----|--------|------|----------------|---------------------------|----------|-----------------|---|--|
| | | | | | 351 | 2020-172-BR | WILL | 29 | | |
| | | | | | | TC-22 | CONTRACT | NO. 62 |) | |
| | SHEET | OF | SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | | | | |