

F.A.S. RTE. NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	1
FED. ROAD DIST. NO.		ILLINOIS	CONTRACT NO. 63536	
			FED AID PROJECT BRS-0034(107)	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

FOR INDEX OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PROJECT LOCATED IN SENECA TOWNSHIP

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PLANS FOR PROPOSED
FEDERAL AID HIGHWAY**

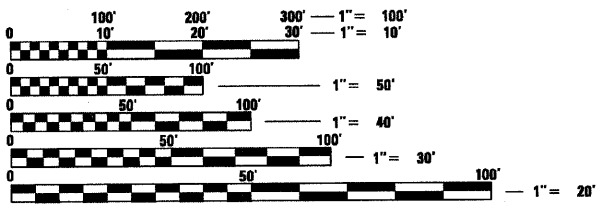
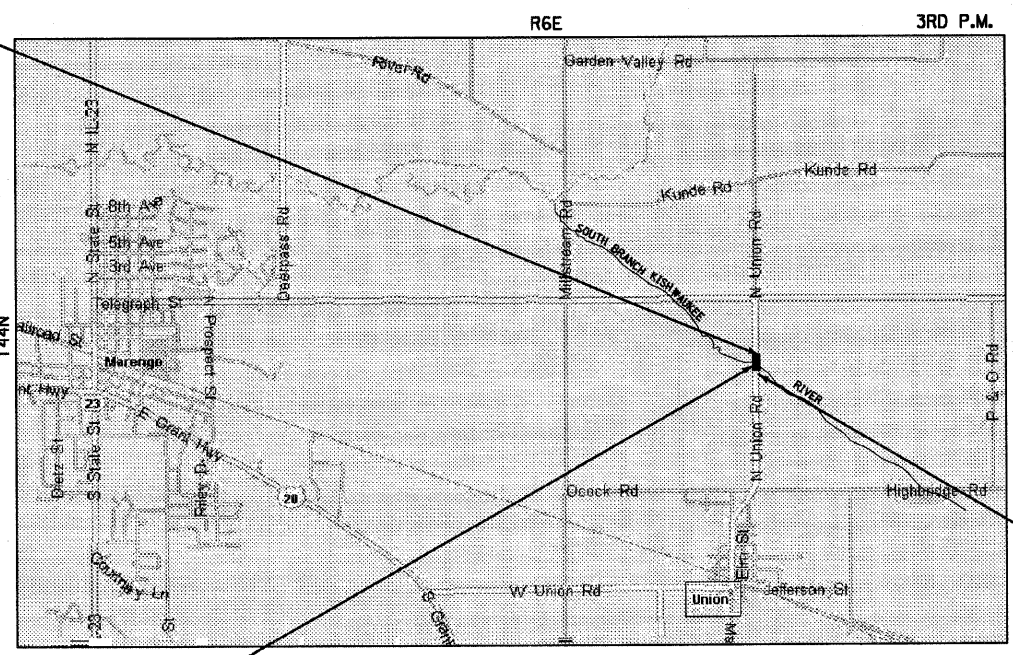
FAS 0034 UNION ROAD OVER
SOUTH BRANCH OF KISHWAUKEE RIVER
SECTION 06-00320-02-BR
PROJECT NO. : BRS-0034(107)
BRIDGE REPLACEMENT AND ROADWAY RECONSTRUCTION
MCHENRY COUNTY
JOB NO. : C-91-140-11

D-91-140-11



TRAFFIC DATA
UNION ROAD
POSTED SPEED LIMIT = 55 MPH
DESIGN SPEED LIMIT = 60 MPH
2008 ADT = 2,280 VPD
2030 ADT = 8,000 VPD
DESIGN DESIGNATION
UNION ROAD: 2066(21) COLLECTOR (0.83) (FD-10)

UNION ROAD
IMPROVEMENT ENDS
STA 211 + 92



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

REPLACEMENT OF SN: 056-0035
WITH SN: 056-3178 OVER
SOUTH BRANCH KISHWAUKEE RIVER

LOCATION MAP
(NOT TO SCALE)
SENECA TOWNSHIP
GROSS LENGTH OF IMPROVEMENT = 1,007 LF OR (0.191 MILES)
NET LENGTH OF IMPROVEMENT = 1,007 LF OR (0.191 MILES)

UNION ROAD
IMPROVEMENT BEGINS
STA 201 + 85

J.U.L.I.E. DESIGN STAGE REQUEST
DIG NO. A1761050
Call Before You Dig
JULIE
ILLINOIS ONE-CALL SYSTEM
CONTACT JULIE AT 811 OR 800-892-0123 WITH THE FOLLOWING:
COUNTY = MCHENRY
CITY-TOWNSHIP = SENECA
SEC. & 1/4 SEC. NO. = 33
48 HOURS (2 working days) BEFORE YOU DIG



MATTHEW D. WASHKOWAK
62-052835
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
PROJECT MANAGER
"LICENSE EXPIRES 11-30-11"

BRANDON L. BUZZELL
081-006358
LICENSED STRUCTURAL ENGINEER OF ILLINOIS
2/20/2010
STRUCTURAL ENGINEER
"LICENSE EXPIRES 11-30-12"

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
APPROVED DECEMBER 17, 2010
Joseph R. Karpuski, Jr.
COUNTY OF MCHENRY, COUNTY ENGINEER
PASSED JANUARY 7, 2011
Cheryl Marshall-Hart
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS
RELEASING FOR BID BASED ON LIMITED REVIEW JANUARY 7, 2011
Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

CONTRACT NO. 63536

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC. ALL RIGHTS RESERVED. PROFESSIONAL DESIGN FIRM LICENSE NO. 384-0022 EXPIRES 11/30/2011. ILLINOIS ONE-CALL SYSTEM PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. (847)705-4406 SCHAUMBURG, IL
Other: J.T.F. J029600.dwg
PLOT: S:\PROJECTS\06-00320-02-BR\06-00320-02-BR-001.dwg
DATE: 1/20/2011 10:03:00 AM
PLOTTER: HP DesignJet 2450

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE DETAILS IN THE PLANS, THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS, AND THE LATEST EDITION OF THE FOLLOWING STATE OF ILLINOIS SPECIFICATIONS: "THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" (REFERRED TO AS THE "STANDARD SPECIFICATIONS"), THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", THE "MANUAL OF TEST PROCEDURES FOR MATERIALS" AND THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS".
2. THE LOCATIONS OF PUBLIC OR PRIVATE UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND THE COUNTY DOES NOT GUARANTEE THEIR ACCURACY. THE CONTRACTOR SHALL CONTACT THE STATE-WIDE ONE CALL NOTICE SYSTEM (J.J.L.I.E., 800-892-0123 OR 811) AND HAVE THE RESPECTIVE UTILITY COMPANIES FIELD LOCATE ALL THEIR FACILITIES A MINIMUM OF 48 HOURS, BUT NOT MORE THAN 14 CALENDAR DAYS PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ALSO VERIFY THE DEPTHS OF THE EXISTING UTILITIES IF NECESSARY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE AND PRESERVATION OF THESE FACILITIES. ANY RELOCATION OR LOWERING OF UTILITIES SHALL BE COORDINATED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR SHALL NOTIFY THE COUNTY AT LEAST 48 HOURS IN ADVANCE OF BEGINNING WORK TO OBTAIN COUNTY UTILITY LOCATIONS AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER.
5. MATERIALS RESULTING FROM THE REMOVAL OF PAVEMENT, DRIVEWAYS, CURB AND GUTTER, HOT-MIX ASPHALT SURFACES, ETC. SHALL BE REMOVED AT THE END OF EACH DAY TO AN APPROVED SITE. IN THE JUDGMENT OF THE COUNTY, SHOULD IT BE NECESSARY TO REMOVE SUCH MATERIALS, THE COUNTY WILL HAVE THE MATERIAL REMOVED AND THE CONTRACTOR WILL BE BILLED (CHARGED) ACCORDINGLY.
6. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY RESIDENTS AND THE COUNTY WHEN ACCESS TO THEIR DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO RESURFACING, CURB AND GUTTER, AND/OR DRIVEWAY REPLACEMENT. THE CONTRACTOR SHALL DISTRIBUTE NOTICES PROVIDED BY THE COUNTY TO RESIDENTS.
7. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE OWNERS, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
8. ACCESS TO FIELD AND PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT TO THE DRIVEWAYS. TEMPORARY RAMPS SHALL BE CONSTRUCTED AS NEEDED TO PROVIDE SUCH ACCESS, UTILIZING CRUSHED STONE OR CRUSHED GRAVEL AND SHALL BE PAID FOR IN ACCORDANCE WITH THE SPECIAL PROVISION AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS.

9. ANY SIGNS OR MAILBOXES THAT ARE IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED AND REPLACED IN ACCORDANCE WITH THE MOST CURRENT VERSIONS OF IDOT HIGHWAY STANDARDS AND U.S.P.S. STANDARDS. MAIL SERVICE SHALL BE MAINTAINED AT ALL TIMES. THIS WORK IS INCLUDED IN THE COST FOR EARTH EXCAVATION.

10. THE FINISHED HOT-MIX ASPHALT SURFACE SHALL BE CONSTRUCTED 0.25 INCH ABOVE THE GUTTER FLAG.
11. STORM STRUCTURE OFFSET LOCATIONS ARE TO THE EDGE OF PAVEMENT IF THE STRUCTURE IS IN THE CURB LINE OR TO THE CENTER OF STRUCTURE IF THE STRUCTURE IS NOT IN THE CURBLINE.
12. CHANNEL EXCAVATION OPERATIONS SHALL BE DONE WHEN WATER LEVEL IS AT OR BELOW NORMAL WATER SURFACE ELEVATION (816.80) OR AS APPROVED BY THE ENGINEER.
13. DRAIN TILE SYSTEMS DISTURBED DURING DEVELOPMENT MUST BE RECONNECTED BY THOSE RESPONSIBLE FOR THEIR DISTURBANCE UNLESS THE APPROVED ENGINEERING PLANS INDICATE HOW THE DRAIN TILE SYSTEM IS TO BE CONNECTED TO THE PROPOSED STORM WATER MANAGEMENT SYSTEM. ALL ABANDONED DRAIN TILES SHALL BE REMOVED IN THEIR ENTIRETY.

14. FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL NEW STRUCTURES SHALL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF COST OF THE STRUCTURE.
15. STORM SEWER SHALL BE BACKFILLED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR THE ROAD AND BRIDGE CONSTRUCTION ARTICLE 550.07, METHOD 1 ONLY.

16. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE TO ALL EXISTING AND PROPOSED DRAINAGE STRUCTURES AT ALL TIMES DURING CONSTRUCTION. METHODS USED BY THE CONTRACTOR SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THIS WORK IS INCLUDED IN THE COST OF THE CONTRACT.

17. THE PRIME COAT APPLICATION RATE SHALL BE 0.1 GAL/SY. THE AGGREGATE PRIME COAT APPLICATION RATE SHALL BE 4 LB/SY.

18. ALL SAW CUTTING SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

19. ANY DAMAGE TO PAVEMENT, SIDEWALK, CURB OR ANY OTHER PORTION OF THE ROADWAY NOT SPECIFICALLY TO BE REMOVED AND REPLACED SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL CHARGE.
20. THE CONTRACTOR SHALL PROVIDE AND INSTALL TWO (2) WEIGHTED SANDBAGS ON EACH TYPE I OR TYPE II BARRICADE USED. ONE (1) WEIGHTED SANDBAG SHALL BE PLACED ACROSS EACH BOTTOM RAIL.
21. FOR STEEL BAR CERTIFICATION, PLEASE CONTACT IDOT BUREAU OF MATERIALS AT 847-705-4363.
22. CHANGEABLE MESSAGE SIGNS SHALL BE PLACED IN ADVANCE OF THE TEMPORARY DETOUR ROUTE AT LEAST ONE WEEK PRIOR TO THE CLOSURE OF UNION ROAD.
23. THE MCHENRY COUNTY SOIL AND WATER CONSERVATION DISTRICT (MCSWCD) MUST BE NOTIFIED ONE WEEK PRIOR TO THE PRE-CONSTRUCTION CONFERENCE; AND ONE WEEK PRIOR TO THE COMMENCEMENT OF LAND DISTURBING ACTIVITIES AND ONE WEEK PRIOR TO THE FINAL INSPECTION. PRIOR TO COMMENCING LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN INDICATED ON THESE PLANS (INCLUDING BUT NOT LIMITED TO: ADDITIONAL PHASES OF DEVELOPMENT AND OFF-SITE BORROW OR WASTE AREAS) A SUPPLEMENTARY EROSION CONTROL PLAN SHALL BE SUBMITTED TO THE OWNER FOR REVIEW BY THE MCSWCD.
24. THE CONTRACTOR IS RESPONSIBLE FOR INSTALLATION OF ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT EROSION AND SEDIMENTATION AS DETERMINED BY THE MCSWCD.
25. EXISTING NAME PLATE SHALL BE SALVAGED AND GIVEN TO MCDOT CONSTRUCTION MANAGER PRIOR TO DEMOLITION.
26. THE SURVEY DATUM USED FOR THIS PROJECT IS NAVD88.

IN-STREAM OR STREAM-SIDE SESC NOTES

THE CONTRACTOR SHALL CONTACT SOREN HALL AT 1-312-846-5532 WITH THE U.S. ARMY CORPS OF ENGINEERS, CHICAGO DISTRICT WITH A PROPOSED COFFERDAM PLAN MEETING THE STANDARDS LISTED BELOW. MEANS AND METHODS FOR COMPLETING WORK WITHIN A WATERWAY MUST BE APPROVED BY THE CORPS PRIOR TO THE COMMENCEMENT OF WORK. THE CORPS WILL APPROVE THE COFFERDAM PLAN TO ENSURE IT MEETS EROSION AND SEDIMENT CONTROL STANDARDS. HOWEVER, IT IS INCUMBENT UPON THE CONTRACTOR TO ENSURE THAT ALL COFFERDAMS ARE CONSTRUCTED TO WITHSTAND EXPECTED FLOWS. ALL WORK NEEDED TO SATISFY ACOE REQUIREMENTS SHALL BE INCLUDED IN THE COST FOR REMOVAL OF EXISTING STRUCTURES.

WORK WITHIN A WATERWAY MUST MEET THE FOLLOWING STANDARDS:

1. WORK IN THE WATERWAY SHALL BE TIMED TO TAKE PLACE DURING LOW OR NO-FLOW CONDITIONS.
2. WATER SHALL BE ISOLATED FROM THE IN-STREAM WORK AREA USING A NON-ERODIBLE COFFERDAM (STEEL SHEETS, AQUA BARRIERS, ETC.). EARTHEN COFFERDAMS ARE NOT PERMISSIBLE.
3. WORK MAY NOT BE PERFORMED IN THE WATER, EXCEPT FOR THE PLACEMENT OF THE MATERIALS NECESSARY FOR THE CONSTRUCTION OF THE COFFERDAM. THE COFFERDAM MUST BE CONSTRUCTED FROM THE UPLAND AREA AND NO EQUIPMENT MAY ENTER THE WATER AT ANY TIME. ONCE THE COFFERDAM IS IN PLACE AND THE ISOLATED AREA IS DEWATERED, EQUIPMENT MAY ENTER THE COFFERED AREA TO PERFORM THE REQUIRED WORK.
4. IF BYPASS PUMPING IS NECESSARY, THE PUMP SHALL BE PLACED ON A STABLE SURFACE OR FLOATED TO PREVENT SEDIMENT FROM BEING SUCKED INTO THE HOSE. THE BYPASS DISCHARGE SHALL BE PLACED ON A NON-ERODIBLE, ENERGY DISSIPATING SURFACE PRIOR TO REJOINING THE STREAM FLOW AND SHALL NOT CAUSE EROSION OF DOWNSTREAM AREAS. CLEANING OR FILTERING OF BYPASS WATER IS NOT NECESSARY UNLESS OTHERWISE REQUIRED.
5. DURING DEWATERING OF THE COFFERED AREA, ALL WATER MUST BE FILTERED TO REMOVE SEDIMENT. POSSIBLE OPTIONS FOR SEDIMENT REMOVAL INCLUDE BAFFLE SYSTEMS, ANIONIC POLYMERS, DEWATERING BAGS, OR OTHER APPROPRIATE METHODS. WATER SHALL HAVE SEDIMENT REMOVED PRIOR TO BEING RE-INTRODUCED TO THE DOWNSTREAM WATERWAY. DISCHARGE WATER IS CONSIDERED CLEAN IF IT DOES NOT RESULT IN A VISUALLY IDENTIFIABLE DEGRADATION OF WATER CLARITY.
6. THE SIDE SLOPES SHALL BE RESEED AND STABILIZED WITH AN APPROPRIATE EROSION CONTROL BLANKET PRIOR TO ACCEPTING FLOWS. THE SUBSTRATE SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS AND STABLE ENOUGH TO ACCEPT FLOWS.

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 ALL RIGHTS RESERVED. PROJECT NO. 070630-GEN-NOTES-SHT
 LICENSE NO. 184-00129 - EXPIRES 4/30/2011
 12/28/2010 10:51:55 PM



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED - PER IDOT 12-27-10
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-gen.notes.sht

**MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER**

GENERAL NOTES	
SCALE: NONE	STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	3
CONTRACT NO. 63536				
JOB NO. C-91-140-11 ILLINOIS FED. AID PROJECT BRS-0034107				

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC. ...
 LICENSE NO. 184-000217 - EXPRESS - 4/30/2010
 11/6/2011 AM



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED - PER IDOT 12-27-10
CHECKED - MDW	REVISED - PER IDOT 01-06-11
DATE - 08/20/10	FILE - 070630-S0002.shx

MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER

SUMMARY OF QUANTITIES
 SCALE: NONE STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	5
CONTRACT NO. 63536			JOB NO. C-91-140-11 ILLINOIS FED. AID PROJECT BRS-0034(107)	

CODE NUMBER	DESCRIPTION	UNITS	TOTAL QUANTITY	CONSTRUCTION TYPE CODE
			BRIDGE 0011	
67100100	MOBILIZATION	L SUM	1	1
70100450	TRAFFIC CONTROL AND PROTECTION, STANDARD 701201	L SUM	1	1
70100460	TRAFFIC CONTROL AND PROTECTION, STANDARD 701306	L SUM	1	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	16	16
70300100	SHORT-TERM PAVEMENT MARKING	FOOT	624	624
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	70	70
72400100	REMOVE SIGN PANEL ASSEMBLY - TYPE A	EACH	4	4
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1,848	1,848
78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	376	376
78200410	GUARDRAIL MARKERS, TYPE A	EACH	16	16
78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4
Z0001050	AGGREGATE SUBGRADE 12"	SO YD	2,528	2,528
Z0001900	ASBESTOS BEARING PAD REMOVAL	EACH	44	44
Z0004552	APPROACH SLAB REMOVAL	SO YD	174	174
Z0007124	STEEL RAILING (SPECIAL)	FOOT	359	359
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1
Z0019600	DUST CONTROL WATERING	UNIT	42	42
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	52	52
Z0042002	POROUS GRANULAR EMBANKMENT, SUBGRADE	CU YD	455	455
Z0046304	PIPE UNDERDRAINS FOR STRUCTURES 4"	FOOT	182	182
Z0066600	STABILIZED DRIVEWAYS 8"	SO YD	599	599
Δ Z0076600	TRAINEES	HOUR	1,000	1,000
XX001193	FENCE REMOVAL SPECIAL	L SUM	1	1
X2070304	POROUS GRANULAR EMBANKMENT, SPECIAL	CU YD	176	176
X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	1	1
X4023000	TEMPORARY ACCESS (ROAD)	EACH	2	2
X4024000	TEMPORARY ACCESS (FIELD ENTRANCE)	EACH	1	1
X5015225	PIPE CULVERT REMOVAL (SPECIAL)	FOOT	20	20
X5020501	UNDERWATER STRUCTURE EXCAVATION PROTECTION - LOCATION 1	EACH	1	1
X6310088	TRAFFIC BARRIER TERMINAL TYPE 6A (SPECIAL)	EACH	4	4
X7810300	RECESSED REFLECTIVE PAVMENT MARKER	EACH	40	40
X8900030	REMOVE EXISTING TEMPORARY TRAFFIC SIGNAL EQUIPMENT	EACH	1	1
XX006119	TRAFFIC CONTROL AND PROTECTION (DETOUR)	L SUM	1	1

• DENOTES SPECIAL ITEM Δ CONSTRUCTION CODE 0042

STATION	SQUARE FEET BY STATION				VOLUMES, IN CUBIC YARDS				REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (CU YD)	EARTH EXCAVATION TO BE USED IN EMBANKMENT (CU YD)	BALANCE WASTE (+) SHORTAGE (-) (CU YD)
	TOPSOIL EXCAVATION (SQ FT)	EARTH EXCAVATION (SQ FT)	UNDERCUT PGES (SQ FT)	EMBANKMENT (SQ FT)	TOPSOIL EXCAVATION (CU YD)	EARTH EXCAVATION (CU YD)	UNDERCUT PGES (CU YD)	EMBANKMENT (CU YD)			
202+00.00	19.2	30.5	0.0	31.0							
202+50.00	42.9	47.9	45.0	52.6	58.0	73.0	42.0	78.0	100.0	62.1	-16.0
203+00.00	67.6	36.8	45.0	104.6	103.0	79.0	84.0	146.0	187.0	67.2	-78.9
203+50.00	79.2	84.2	0.0	119.2	136.0	113.0	42.0	208.0	178.0	96.1	-112.0
204+00.00	85.2	85.6	0.0	140.1	153.0	158.0	0.0	241.0	153.0	134.3	-106.7
204+50.00	91.7	35.0	0.0	204.5	164.0	112.0	0.0	320.0	164.0	95.2	-224.8
205+00.00	96.4	52.6	0.0	228.3	175.0	82.0	0.0	401.0	175.0	69.7	-331.3
205+50.00	90.9	58.0	15.0	216.5	174.0	103.0	14.0	412.0	188.0	87.6	-324.5
206+00.00	59.7	50.4	15.0	266.6	140.0	101.0	28.0	448.0	168.0	85.9	-362.2
206+41.00	55.0	71.6	15.0	266.6	88.0	93.0	23.0	405.0	111.0	79.1	-326.0
BRIDGE PAVEMENT - STA 206+41 TO 207+80											
207+80.00	112.5	143.5	0.0	174.9							
208+00.00	112.5	143.5	10.0	174.9	84.0	107.0	4.0	130.0	88.0	91.0	-39.1
208+50.00	115.2	105.1	10.0	201.5	211.0	231.0	19.0	349.0	230.0	196.4	-152.7
209+00.00	111.9	104.4	10.0	191.3	211.0	194.0	19.0	364.0	230.0	164.9	-199.1
209+50.00	109.2	85.8	0.0	169.1	205.0	177.0	10.0	334.0	215.0	150.5	-183.6
210+00.00	100.5	69.4	35.0	147.7	195.0	144.0	33.0	294.0	228.0	122.4	-171.6
210+50.00	58.6	43.4	35.0	112.1	148.0	105.0	65.0	241.0	213.0	89.3	-151.8
211+00.00	51.4	39.0	20.0	103.6	102.0	77.0	51.0	200.0	153.0	65.5	-134.6
211+50.00	36.3	4.9	0.0	68.6	82.0	41.0	19.0	160.0	101.0	34.9	-125.2
211+92.00	36.3	4.9	0.0	68.6	57.0	8.0	0.0	107.0	57.0	6.8	-100.2
TOTALS:					2,486.0	1,998.0	453.0	4,838.0	2,939.0	1,698.3	-3,139.7
SHRINKAGE FACTOR = 15%											

NOTES:

- 15% SHRINKAGE FACTOR WAS USED
- ASSUMED 6-INCH TOPSOIL EXCAVATION WITHIN EXISTING ROW AND 12-INCH BETWEEN EXISTING ROW AND PROPOSED ROW
- ASSUMED 12-INCH DEPTH FOR UNDERCUT AREAS
- ANY PGES AND REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL FOR GROUND STABILIZATION NOT NEEDED AT THE TIME OF CONSTRUCTION SHOULD BE DELETED FROM THE CONTRACT.
- REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL = TOPSOIL EXCAVATION + UNDERCUT
- UNDERCUT PGES INCLUDES DITCH UNDERCUT + 6-INCH AVERAGE POTENTIAL ROADWAY UNDERCUT

SUMMARY			
20200100	20400800	20201200	Z0042002
EARTH EXCAVATION (CU YD)	FURNISHED EXCAVATION (CU YD)	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL (CU YD)	POROUS GRANULAR EMBANKMENT SUBGRADE (CU YD)
2,000	3,140	2,940	455

CONTROL: © 2010 BY BAXTER & WOODMAN, INC. \\PILOTS\NORTH\070630-EarthSch.sht
 LICENSE NO.: 184-00121 - EXPIRES 4/30/2011
 12/16/2010 3:33:58 PM



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-EarthSch.sht

MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER

EARTHWORK SCHEDULE

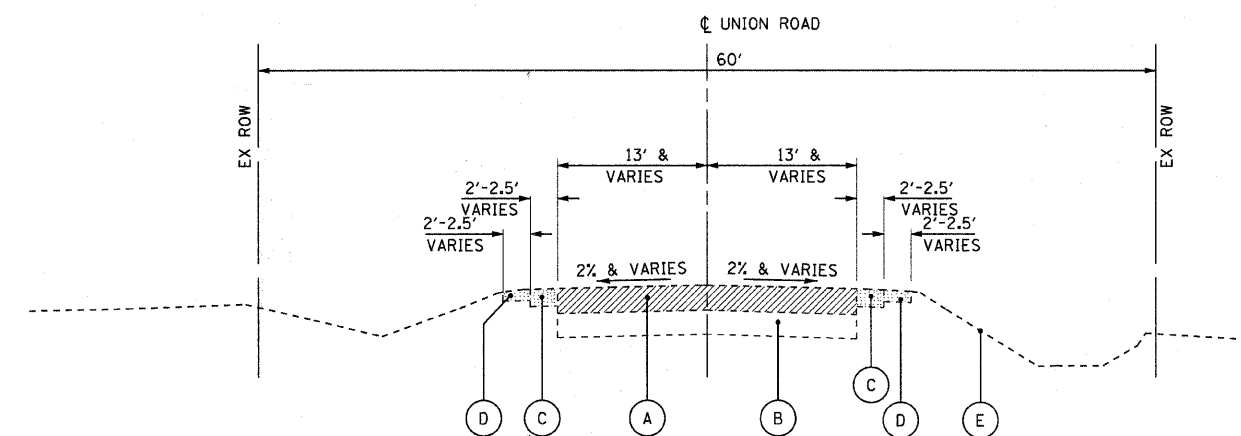
SCALE: NONE

STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	6
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63536	
			BRS-0034(107)	

EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE (VARIES 9" TO 9 3/4")
- (B) AGGREGATE SUBBASE (VARIES 10 1/2" TO 10 3/4")
- (C) HOT-MIX ASPHALT SHOULDERS
- (D) AGGREGATE SHOULDERS
- (E) EXISTING GROUND
- [Hatched Box] PAVEMENT REMOVAL
- [Dotted Box] SHOULDER REMOVAL



**EXISTING TYPICAL SECTION
UNION ROAD**

STA. 201+85 TO STA. 211+92

COPYRIGHT © 2010 BY BAKER & WOODMAN, INC. PROJECT: UNION ROAD BRIDGE OVER SOUTH BRANCH OF KISHWAUKEE RIVER
 LICENSE NO. 184-0023 - EXPIRES 7/30/2011
 12/13/2010 12:06:03 PM \\corps.bowood.com\proj\070630-ExTypSec-Union.sht

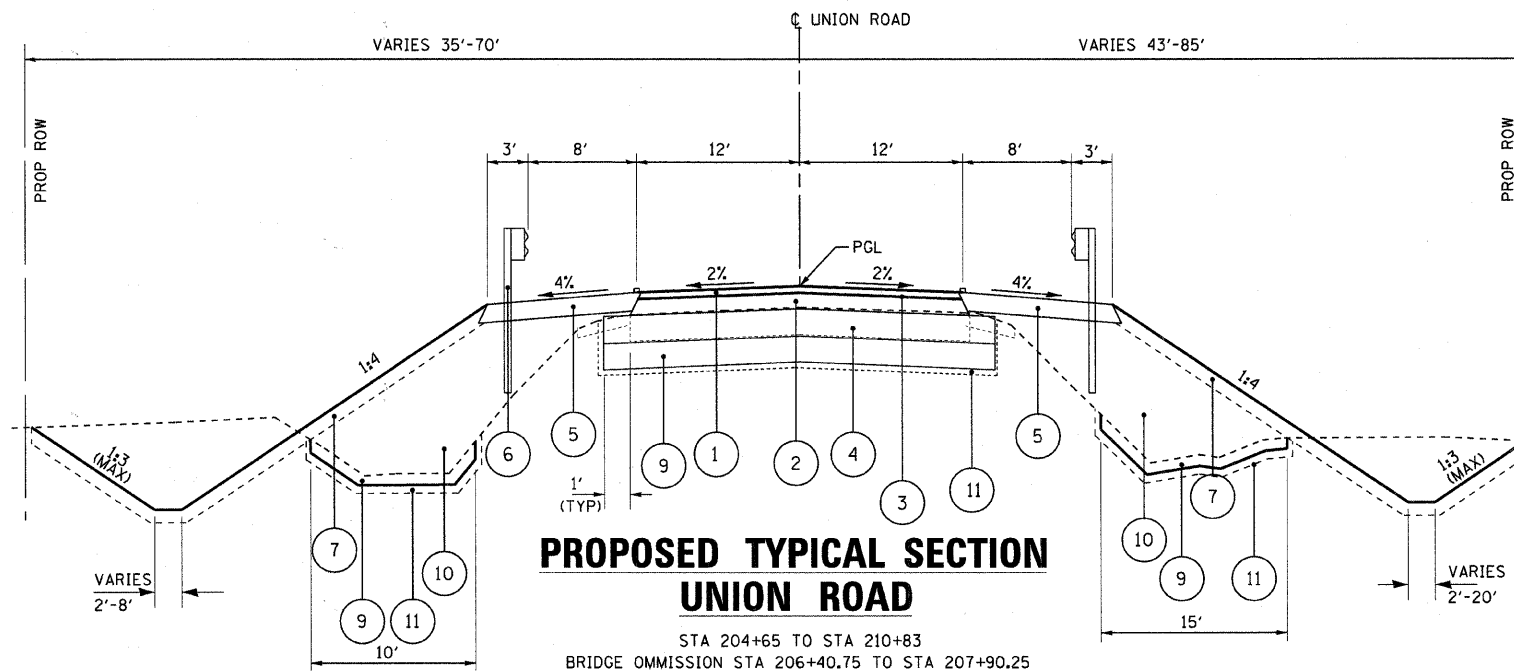
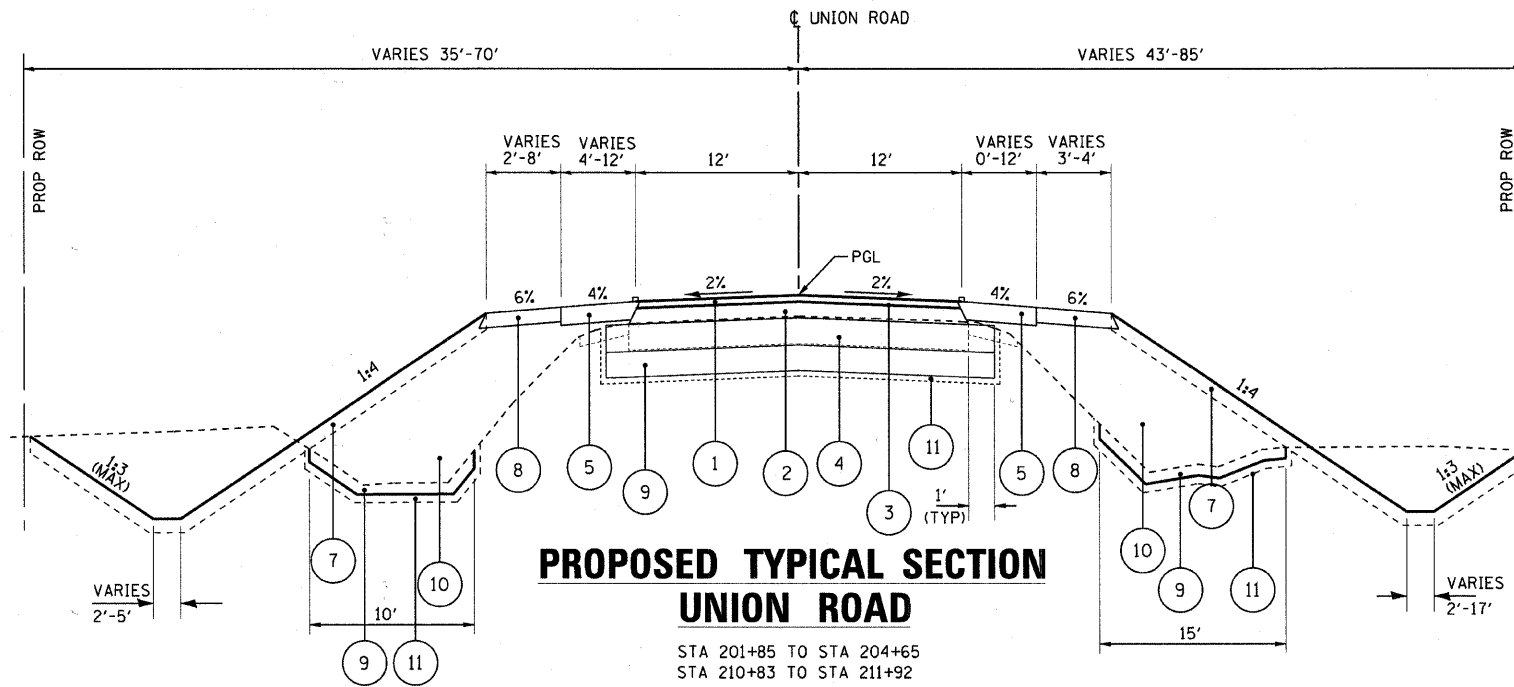


DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-ExTypSec-Union.sht

**MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER**

EXISTING TYPICAL SECTION	
SCALE: NONE	STA. TO STA.

F.A.S. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	7
JOB NO. C-91-140-11 [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63536	
			BRS-0034(107)	



EXISTING LEGEND

- (A) HOT-MIX ASPHALT SURFACE AND BINDER COURSE (VARIES 9" TO 9 3/4")
- (B) AGGREGATE SUBBASE (VARIES 10 1/2" TO 10 3/4")
- (C) HOT-MIX ASPHALT SHOULDERS
- (D) AGGREGATE SHOULDERS
- (E) EXISTING GROUND
- [Hatched Box] PAVEMENT REMOVAL
- [Dotted Box] SHOULDER REMOVAL

PROPOSED LEGEND

- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50-2"
- (2) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50-7"
- (3) BITUMINOUS MATERIALS (PRIME COAT)
- (4) AGGREGATE SUBGRADE 12"
- (5) HOT-MIX ASPHALT SHOULDERS, 6"
- (6) PROPOSED STEEL PLATE BEAM GUARDRAIL
- (7) TOPSOIL FURNISH AND PLACE, 6" SEEDING, CLASS 2A EROSION CONTROL BLANKET
- (8) AGGREGATE SHOULDERS, TYPE B-6"
- (9) REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL
•• POROUS GRANULAR EMBANKMENT, SUBGRADE
- (10) EMBANKMENT
- (11) GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

NOTES:

1. ••POROUS GRANULAR EMBANKMENT, SUBGRADE HAS BEEN PROVIDED TO REPLACE SOILS WHICH TEND TO BE UNSTABLE WHEN WET. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH PGES WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY USE OF CONE PENETROMETER OR PROOFROLLING. IF UNSTABLE SOILS ARE ENCOUNTERED THE SOILS SHALL BE REMOVED AND REPLACED WITH PGES. THE REMOVAL AND REPLACEMENT AREA SHALL EXTEND TO 12 INCHES BEYOND THE SHOULDER AND COME UP AT A 1:1 SLOPE TO EXISTING GROUND SURFACE. THESE LIMITS MAY BE ALTERED BY THE ENGINEER IF FIELD CONDITIONS SO WARRANT. REMOVAL OF THESE UNSUITABLE SOILS SHALL BE PAID FOR AS "REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL." ANY PGES AND REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL FOR GROUND STABILIZATION NOT NEEDED AT THE TIME OF CONSTRUCTION SHOULD BE DELETED FROM THE CONTRACT.

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC. ...
 LICENSE NO. 184-00023 - EXPIRES 4/30/2011
 12/16/2008 3:26:03 PM



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-PrTypSec-Union.sht

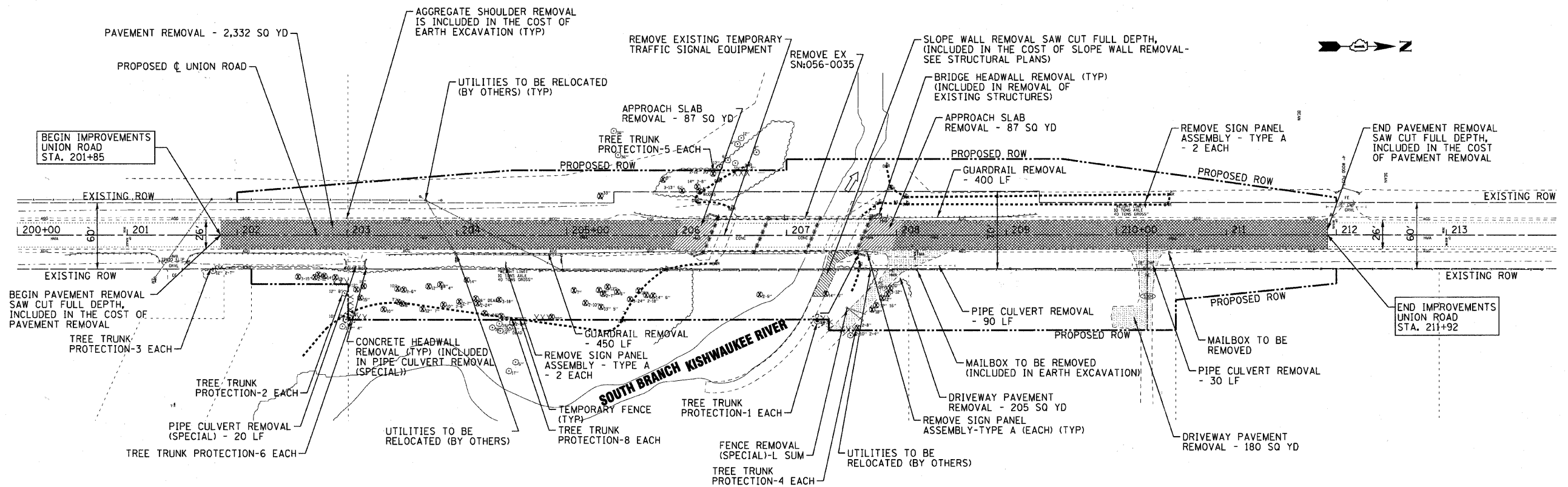
**MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER**

PROPOSED TYPICAL SECTION

SCALE: NONE

STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	8
CONTRACT NO. 63536				
JOB NO. C-91-140-11		ILLINOIS FED. AID PROJECT		BRS-0034(107)











UNION ROAD

**MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER**

**EXISTING CONDITIONS AND REMOVAL PLAN
UNION ROAD AT SOUTH BRANCH KISHWAUKEE RIVER**

LEGEND:

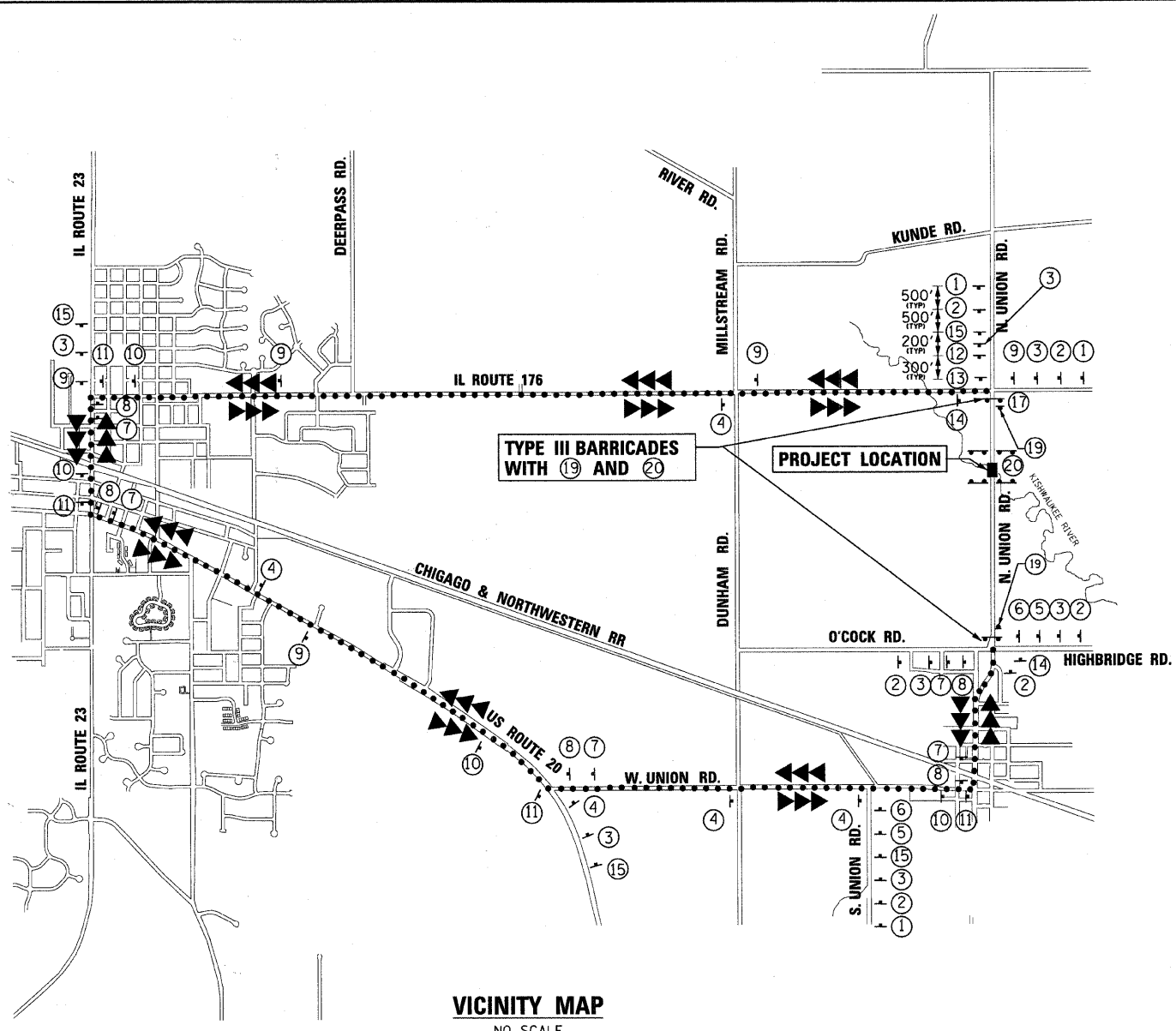
-  DRIVEWAY PAVEMENT REMOVAL
-  APPROACH SLAB REMOVAL
-  PAVEMENT REMOVAL
-  SLOPE WALL REMOVAL
-  TREE REMOVAL
-  WETLAND BOUNDARY
-  TREE TRUNK PROTECTION
-  TEMPORARY FENCE

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC. PROJECT NO. 070630-REMOVAL
 LICENSE NO. 184-00121 - EXPIRES 4/30/2011
 69108 PW 12/13/2010



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-REMOVAL.shx

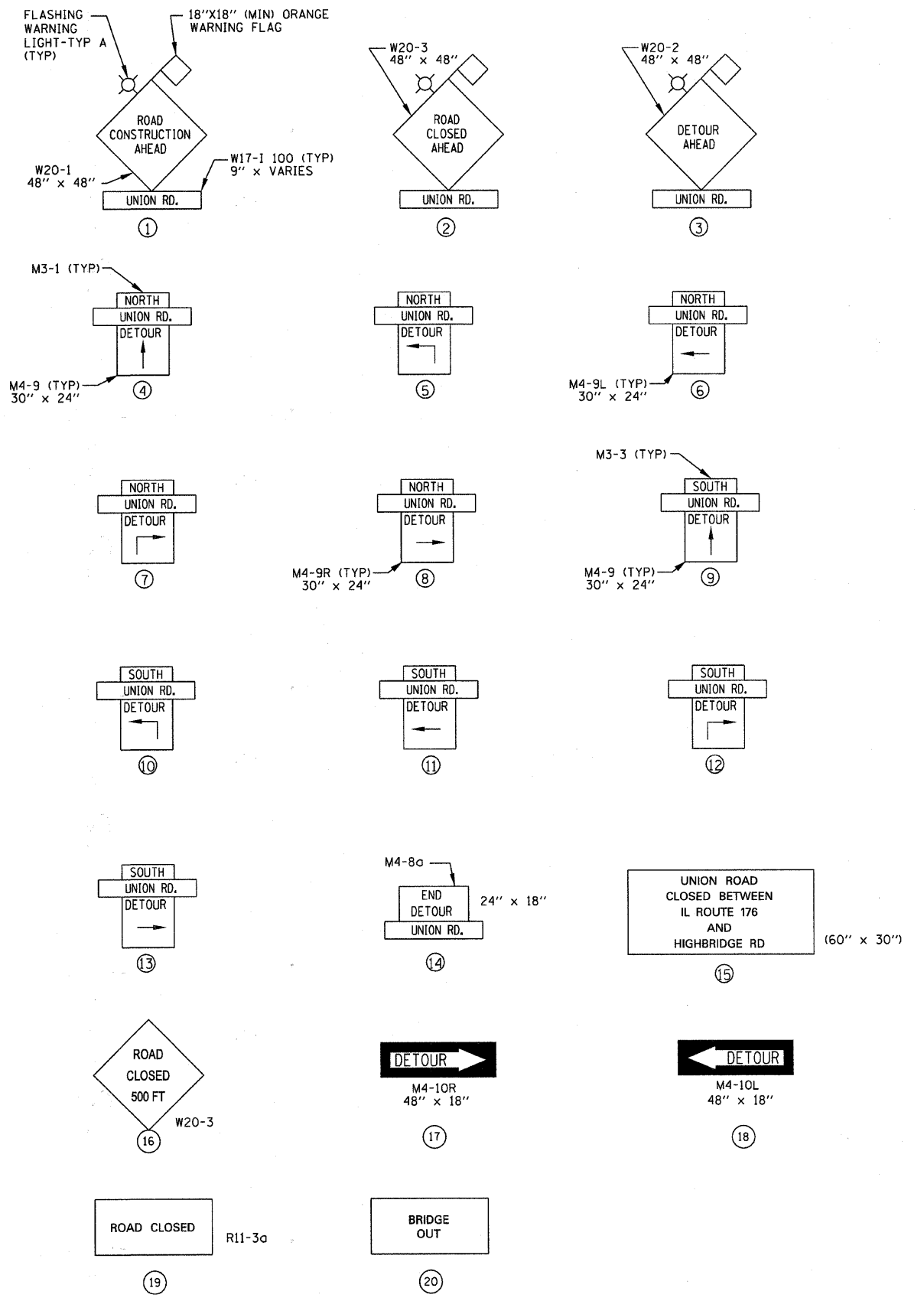
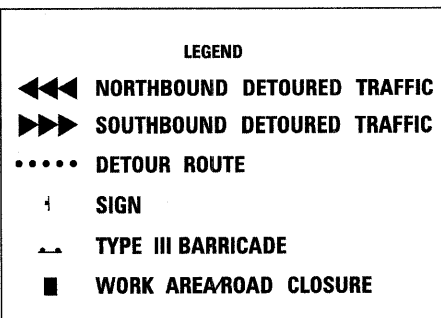
SCALE: H: 1"=50'	STA. 200+00 TO STA. 214+00	F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 11
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT		CONTRACT NO. 63536	



VICINITY MAP
NO SCALE

MAINTENANCE OF TRAFFIC NOTES:

1. SIGNING AND BARRICADE PLACEMENT SHALL BE IN ACCORDANCE WITH THE LATEST IDOT HIGHWAY STANDARDS AND IDOT DISTRICT 1 STANDARD DETAILS.
2. THE ENGINEER SHALL BE INFORMED 48 HOURS IN ADVANCE OF ANY CHANGE IN CONSTRUCTION STAGING.
3. THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR. ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR.
4. EROSION AND SEDIMENT CONTROLS SHALL BE INSTALLED PRIOR TO START OF CONSTRUCTION WITH CONTINUED MAINTENANCE FOR THE DURATION OF CONSTRUCTION, SEE EROSION CONTROL SHEETS FOR ADDITIONAL INFORMATION.
5. THE CONTRACTOR SHALL MAINTAIN EMERGENCY VEHICLE AND LOCAL TRAFFIC DURING THE CONSTRUCTION. THE COST FOR MAINTAINING ACCESS ON UNION ROAD SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION FOR TEMPORARY DETOUR.
6. THE CONTRACTOR SHALL PROVIDE TEMPORARY ACCESS TO ALL ENTRANCES DURING THE CONSTRUCTION OF UNION ROAD AND UNION BRIDGE OVER THE SOUTH BRANCH KISHAWAUKEE RIVER. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR TEMPORARY ACCESS (ROAD), TEMPORARY ACCESS (PRIVATE ENTRANCE) AND TEMPORARY ACCESS (FIELD ENTRANCE).



SIGN DETAILS

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC. PROJECT: 06-00320-02-BR. SHEET: 12. DATE: 08/20/10. DRAWN: BCD. CHECKED: MDW. DATE: 08/20/10. FILE: 070630-Union-Detour.sht.



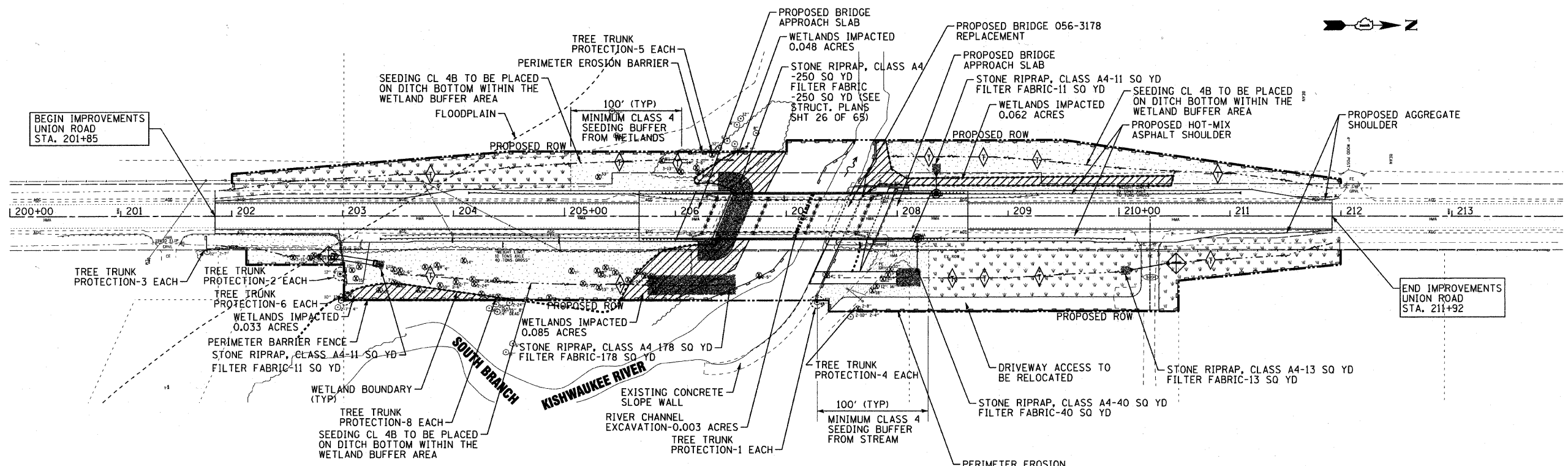
DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-Union-Detour.sht

**MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHAWAUKEE RIVER**

TEMPORARY DETOUR PLAN

SCALE: NONE STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	12
JOB NO. C-91-140-11 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63536	
			BRS-00341071	



SOIL EROSION AND SEDIMENT CONTROL NOTES:

- ALL AREAS OF BARE GROUND WILL BE TEMPORARILY SEEDED EVERY 7 DAYS UNTIL PERMANENT EROSION CONTROL IS IN PLACE.
- ALL AREAS WITHIN R.O.W. WILL BE RESTORED WITH 6" TOPSOIL, EROSION CONTROL BLANKET AND SEEDING, CLASS 2A.
- DITCH CHECKS SHALL BE EITHER ROLLED EXCELSIOR OR TRIANGULAR SILT DIKES.
- ENGINEER TO STAKE LOCATIONS FOR PROPOSED TREES, AND SHALL BE APPROVED BY McHENRY COUNTY PRIOR TO INSTALLATION.
- NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN OR NEAR THE CRITICAL AREAS SHOULD BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOW. ONCE WORK IS IN THIS AREA BEGINS, PRIORITY SHALL BE GIVEN TO THE COMPLETION OF THE WORK AND FINAL STABILIZATION OF ALL DISTURBED AREAS.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
- PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION.
- SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
- ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AND EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURES).
- ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
- SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF McHENRY COUNTY.
- THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S. OR ISOLATED WATERS OF McHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
- ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.
- HEAVY DUTY EROSION CONTROL BLANKET SHALL BE USED AND SHALL BE IN ACCORDANCE WITH THE "ILLINOIS URBAN MANUAL", LATEST EDITION, AND "IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION", LATEST EDITION SECTION 251.04.
- THE DITCH BOTTOMS SHALL BE SEEDED WITH SEEDING CLASS 4B WITHIN THE WETLAND BUFFER AREAS.

UNION ROAD

SOIL EROSION AND SEDIMENT CONTROL NOTES: (CONT.)

- ALL SEDIMENTATION AND EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE "ILLINOIS URBAN MANUAL", LATEST EDITION, WHERE NOT ALREADY PROVIDED FOR WITHIN THE PLANS, SPECIFICATIONS AND SPECIAL PROVISIONS.
- ESTABLISH PERMANENT STABILIZATION WITHIN 14 DAYS OF FINAL GRADING OR WHEN DISTURBED AREA IS LEFT IDLE FOR MORE THAN 14 DAYS.
- THE EROSION CONTROL QUANTITIES PROVIDED IN THE PLANS ARE APPROXIMATE. THE ACTUAL NEED FOR QUANTITIES WILL BE DETERMINED IN THE FIELD BY THE ENGINEER AT THE TIME OF CONSTRUCTION.

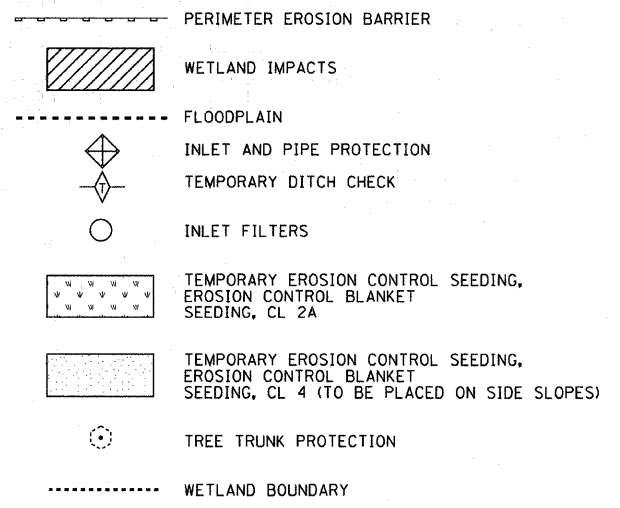
MAINTENANCE SCHEDULE

- PERIMETER EROSION BARRIER - AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL PERIMETER EROSION BARRIER WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP THE PERIMETER EROSION BARRIER FUNCTIONAL AS DESIGNED.
- HEAVY DUTY EROSION CONTROL BLANKET - AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL EROSION BLANKET WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP THE EROSION BLANKET FUNCTIONAL AS DESIGNED.
- INLET AND PROTECTION - AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL INLET AND PIPE PROTECTION WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP THE INLET AND PIPE PROTECTION FUNCTIONAL AS DESIGNED.
- TEMPORARY DITCH CHECKS - AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL TEMPORARY DITCH CHECKS WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP THE TEMPORARY DITCH CHECKS FUNCTIONAL AS DESIGNED.
- INLET FILTERS - AT A MINIMUM, THE CONTRACTOR SHALL INSPECT ALL INLET FILTERS WEEKLY OR AFTER EACH ONE-HALF INCH OR GREATER RAINFALL EVENT. ANY REQUIRED REPAIRS SHALL BE MADE BY THE CONTRACTOR TO KEEP THE INLET FILTERS FUNCTIONAL AS DESIGNED.

CONSTRUCTION SEQUENCING

- INSTALL SEDIMENT AND EROSION CONTROL SYSTEMS
- COMPLETE TREE REMOVAL, CLEARING, AND GRUBBING
- STRIP AND STOCKPILE TOPSOIL AND BEGIN MASS GRADING. TEMPORARY SEED AS REQUIRED.
- DEMOLISH EXISTING STRUCTURE WITHOUT IMPACT OR DEBRIS ENTERING THE EXISTING WATERWAY.
- CONSTRUCT UNDERWATER STRUCTURE EXCAVATION PROTECTION AND INSTALL PILES AND STRUCTURE.
- COMPLETE ROADWAY REPLACEMENT THRU BINDER AND GRADING.
- COMPLETE FINAL SURFACE, PAVEMENT MARKINGS, AND RESTORATION.
- REMOVE EROSION CONTROL MEASURES AND RESTORE.

LEGEND



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED - PER IDOT 12-27-10
CHECKED - MDW	REVISED - PER IDOT 01-06-11
DATE - 08/20/10	FILE - 070630-EROS-UNION-50.sh

**MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER**

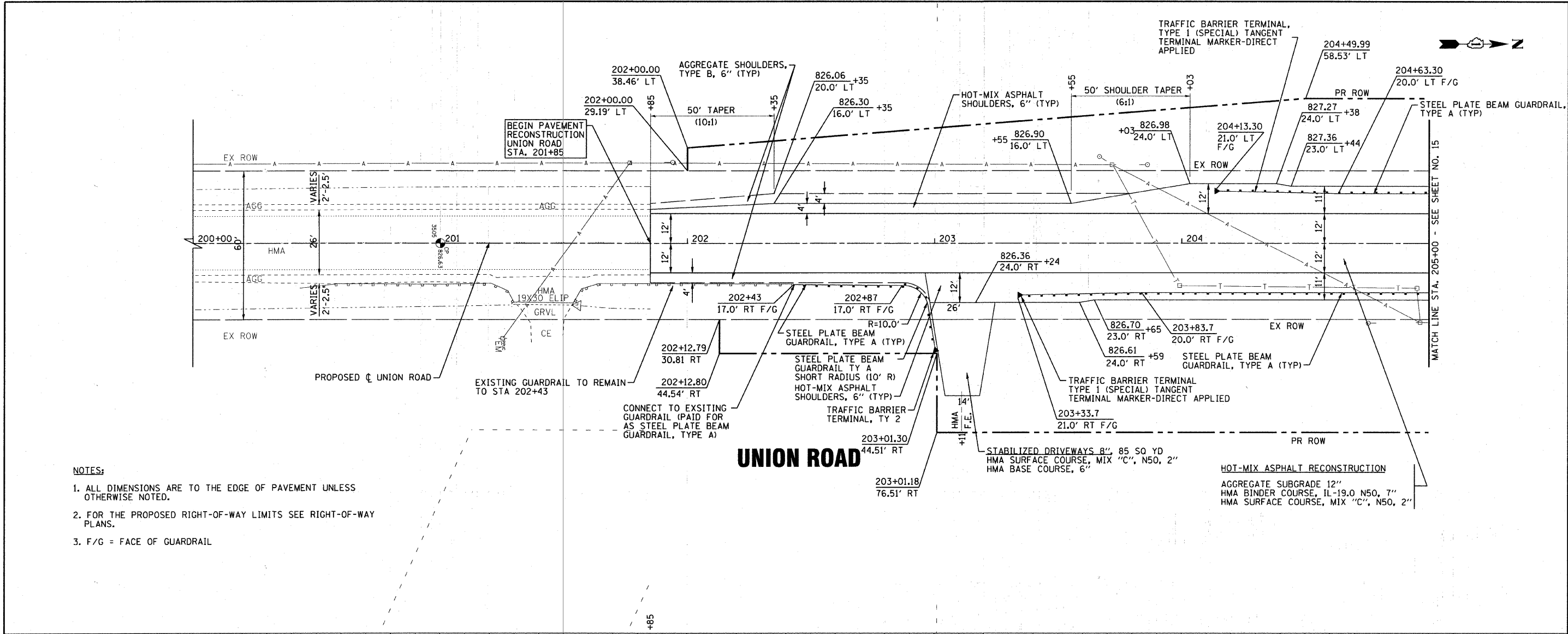
**EROSION CONTROL PLAN
UNION ROAD AT SOUTH BRANCH KISHWAUKEE RIVER**

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	13
CONTRACT NO. 63536				
JOB NO. C-91-140-11 ILLINOIS FED. AID PROJECT BRS-0034(107)				

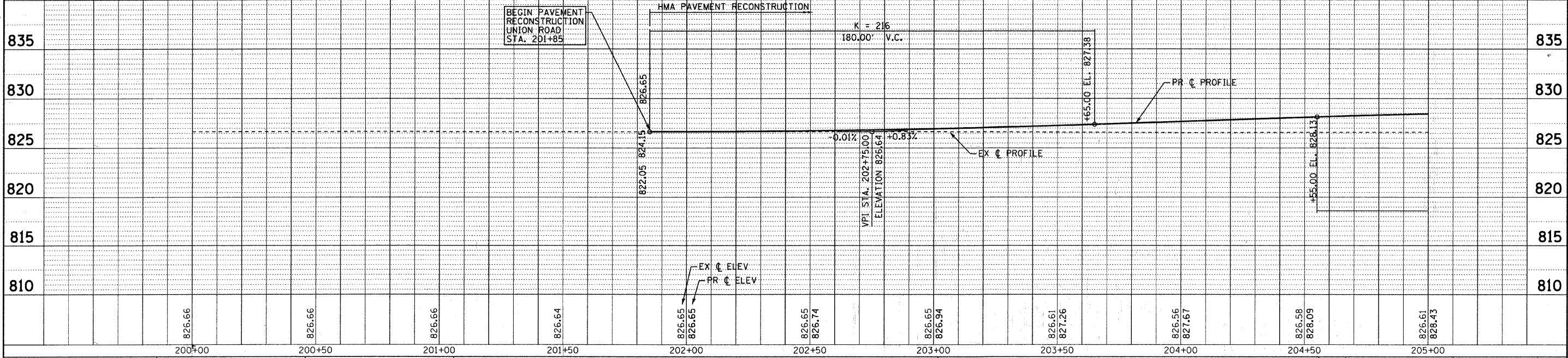
SCALE: H: 1"=50'

STA. 200+00 TO STA. 214+00

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 ALL RIGHTS RESERVED. NO PART OF THIS PUBLICATION
 MAY BE REPRODUCED OR TRANSMITTED IN ANY FORM
 OR BY ANY MEANS, ELECTRONIC OR MECHANICAL,
 INCLUDING PHOTOCOPYING, RECORDING, OR BY
 ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM,
 WITHOUT PERMISSION IN WRITING FROM BAXTER &
 WOODMAN, INC. PROJECT NO. 070630-EROS-UNION-50
 DATE: 8/20/10



- NOTES:**
1. ALL DIMENSIONS ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
 2. FOR THE PROPOSED RIGHT-OF-WAY LIMITS SEE RIGHT-OF-WAY PLANS.
 3. F/G = FACE OF GUARDRAIL



COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 505 S. WASHINGTON ST., SUITE 200, CHICAGO, IL 60607
 312.467.1200 FAX 312.467.1201
 www.baxterandwoodman.com

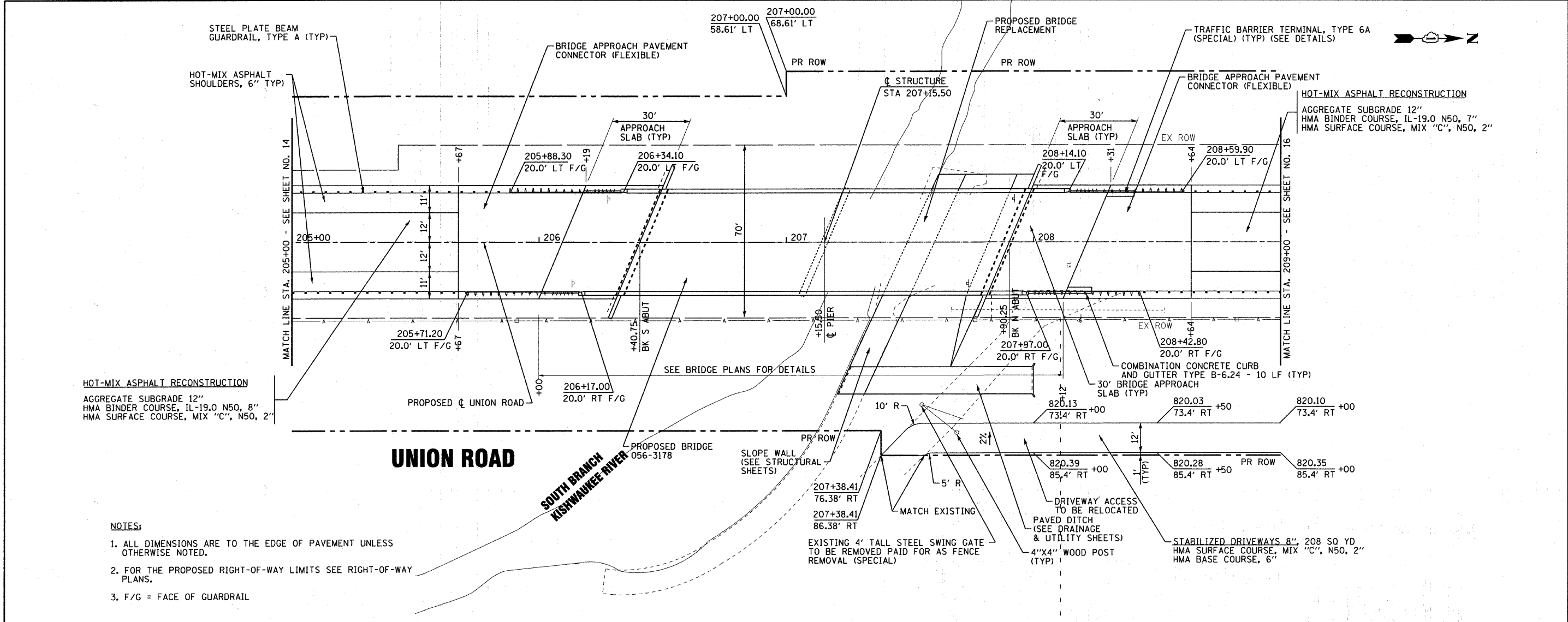


DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-PH2-UNION-PP1.shp

MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER

PLAN AND PROFILE
 SCALE: H: 1"=20' V: 1"=5'
 STA. 200+20 TO STA. 205+00

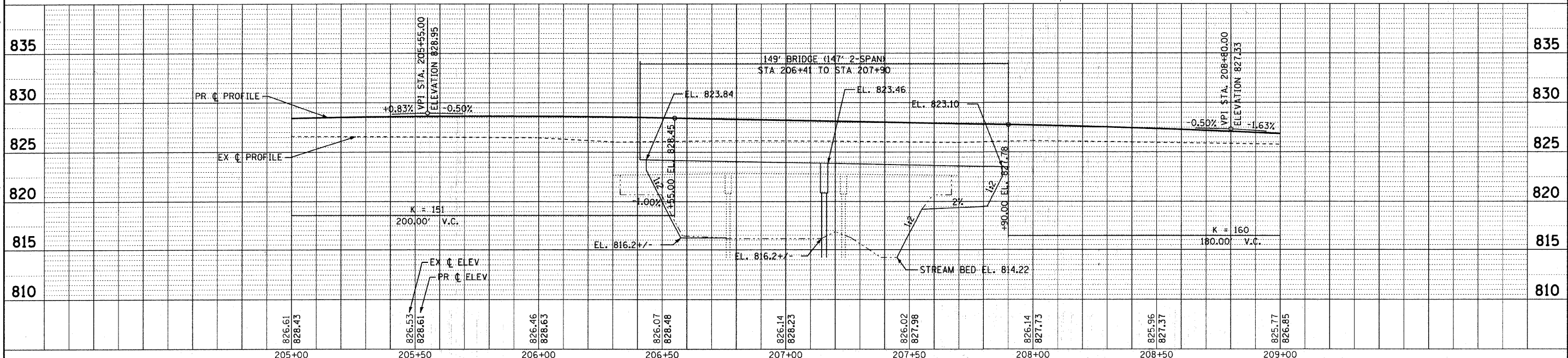
F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 14
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT BR5-0034(107)	



HOT-MIX ASPHALT RECONSTRUCTION
 AGGREGATE SUBGRADE 12"
 HMA BINDER COURSE, IL-19.0 N50, 8"
 HMA SURFACE COURSE, MIX "C", N50, 2"

NOTES:

1. ALL DIMENSIONS ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
2. FOR THE PROPOSED RIGHT-OF-WAY LIMITS SEE RIGHT-OF-WAY PLANS.
3. F/G = FACE OF GUARDRAIL



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-PH2-UNION-PP2.sht

MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER

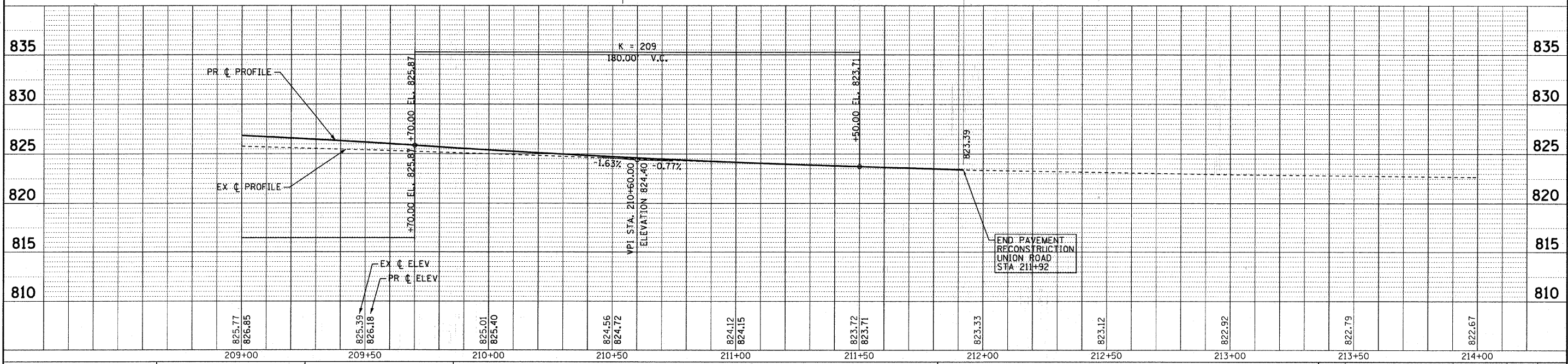
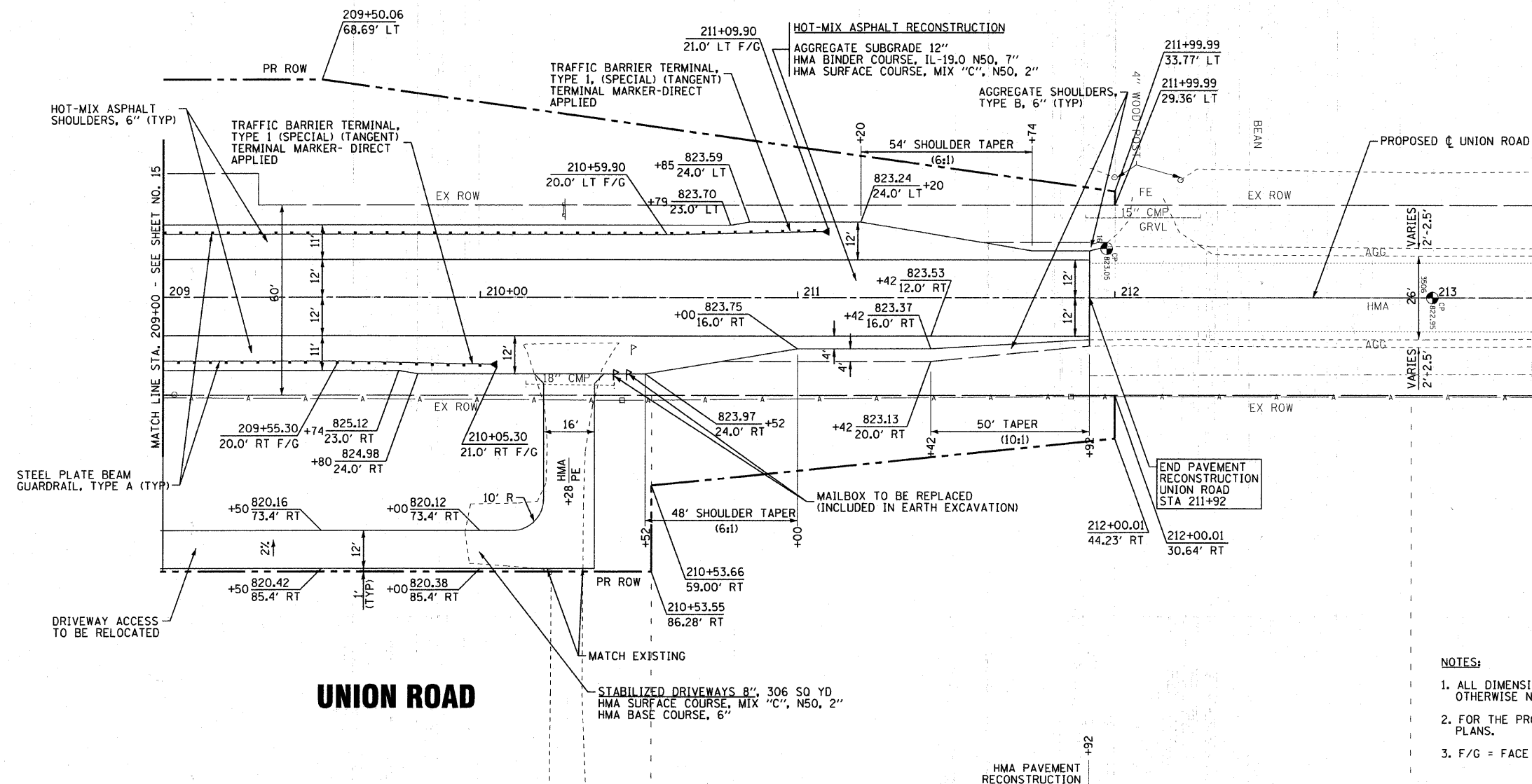
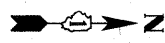
PLAN AND PROFILE

SCALE: H: 1"=20' V: 1"=5'

STA. 205+00 TO STA. 209+00

F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 15
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT BR5-0034(107)	

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-00221 - EXPIRES 6/30/2011
 12/2/2010

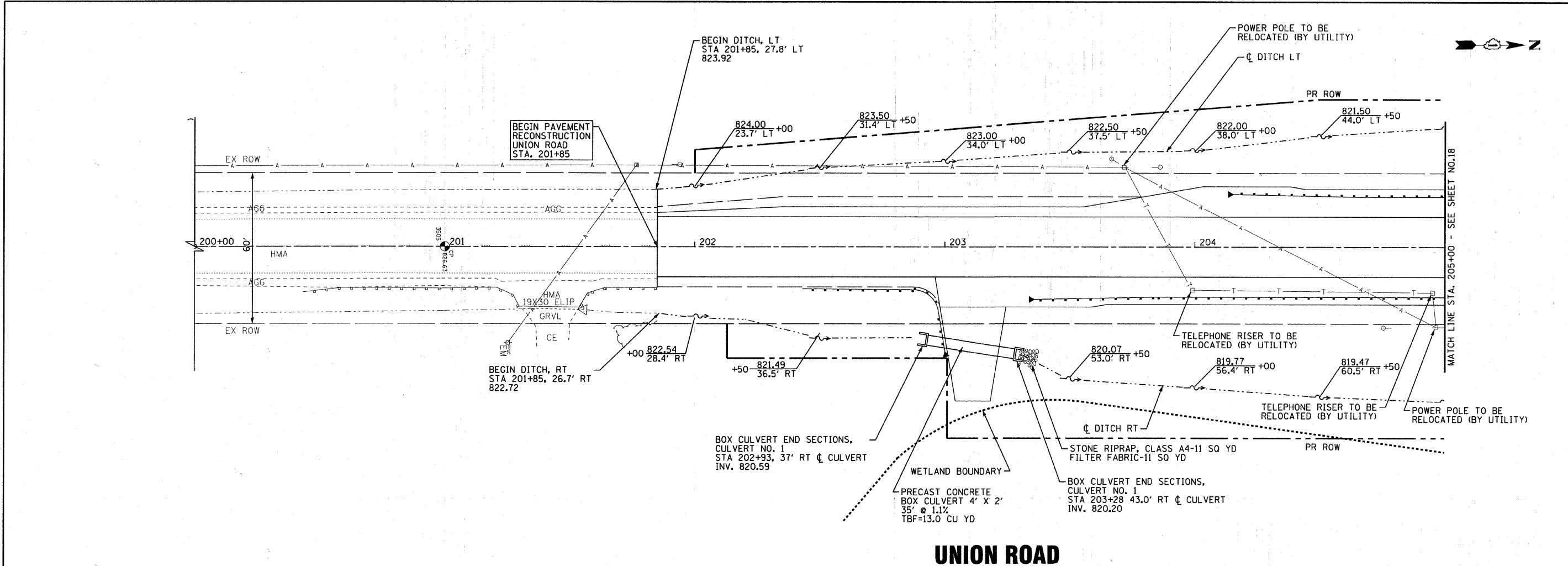


DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-PH2-UNION-PP3.sht

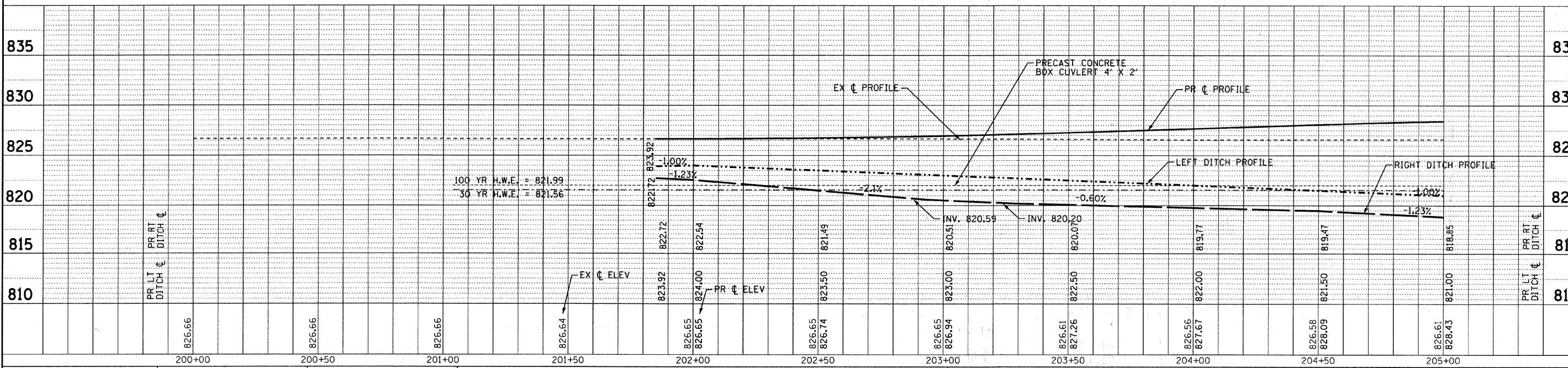
MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER

PLAN AND PROFILE
 SCALE: H: 1"=20' V: 1"=5'
 STA. 209+00 TO STA. 214+00

F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 16
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT BRS-0034(107)	



UNION ROAD



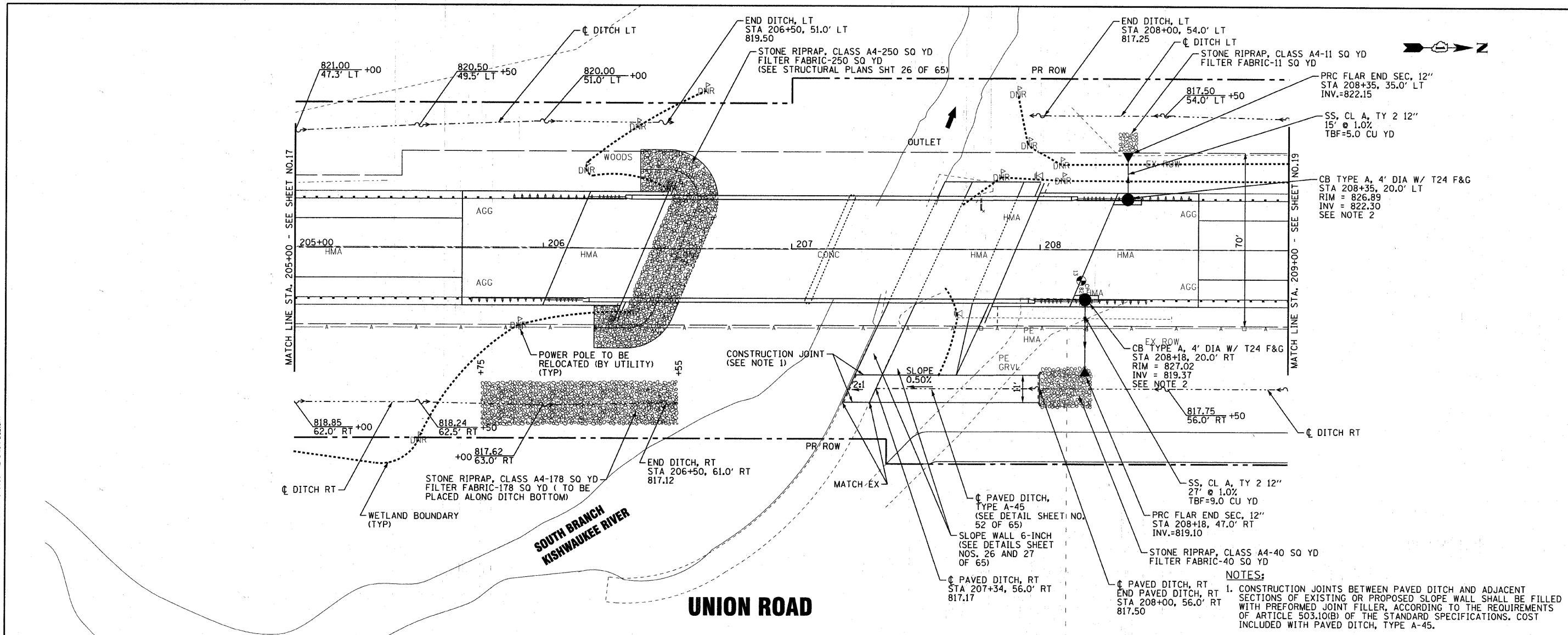
COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC. ...
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 EXP. NO. 061725 AM
 PROJECT: MCHENRY COUNTY DIVISION OF TRANSPORTATION - PHASE 2 - UNION ROAD - DRAINAGE & UTILITY PLAN

	DESIGNED - CAC	REVISED - PER IDOT 10-08-10
	DRAWN - BCD	REVISED -
	CHECKED - MDW	REVISED -
	DATE - 08/20/10	FILE - 070630-PH2-UNION-DUI.shp

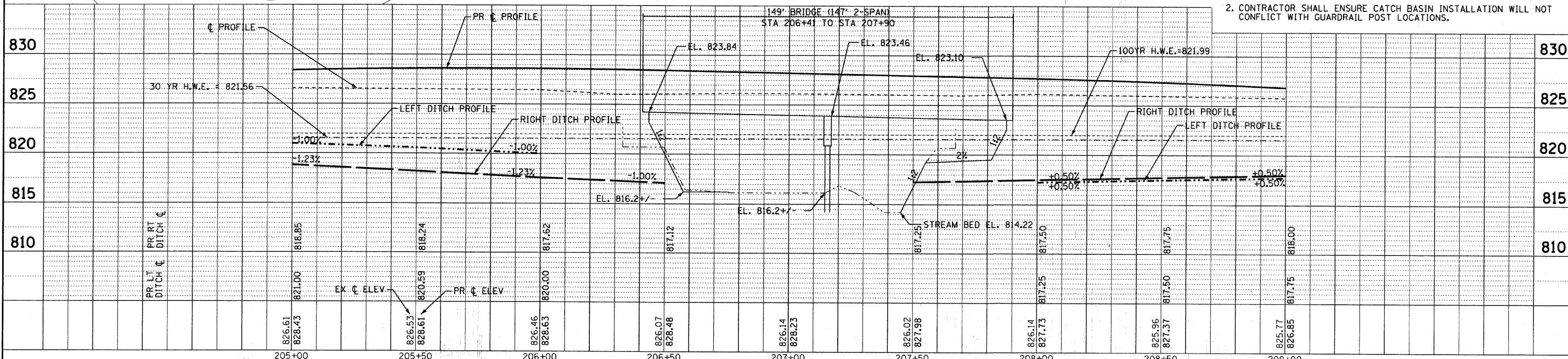
MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER

DRAINAGE & UTILITY PLAN
 SCALE: H: 1"=20' V: 1"=5'
 STA. 200+20 TO STA. 205+00

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	17
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63536	
			BRS-00341071	



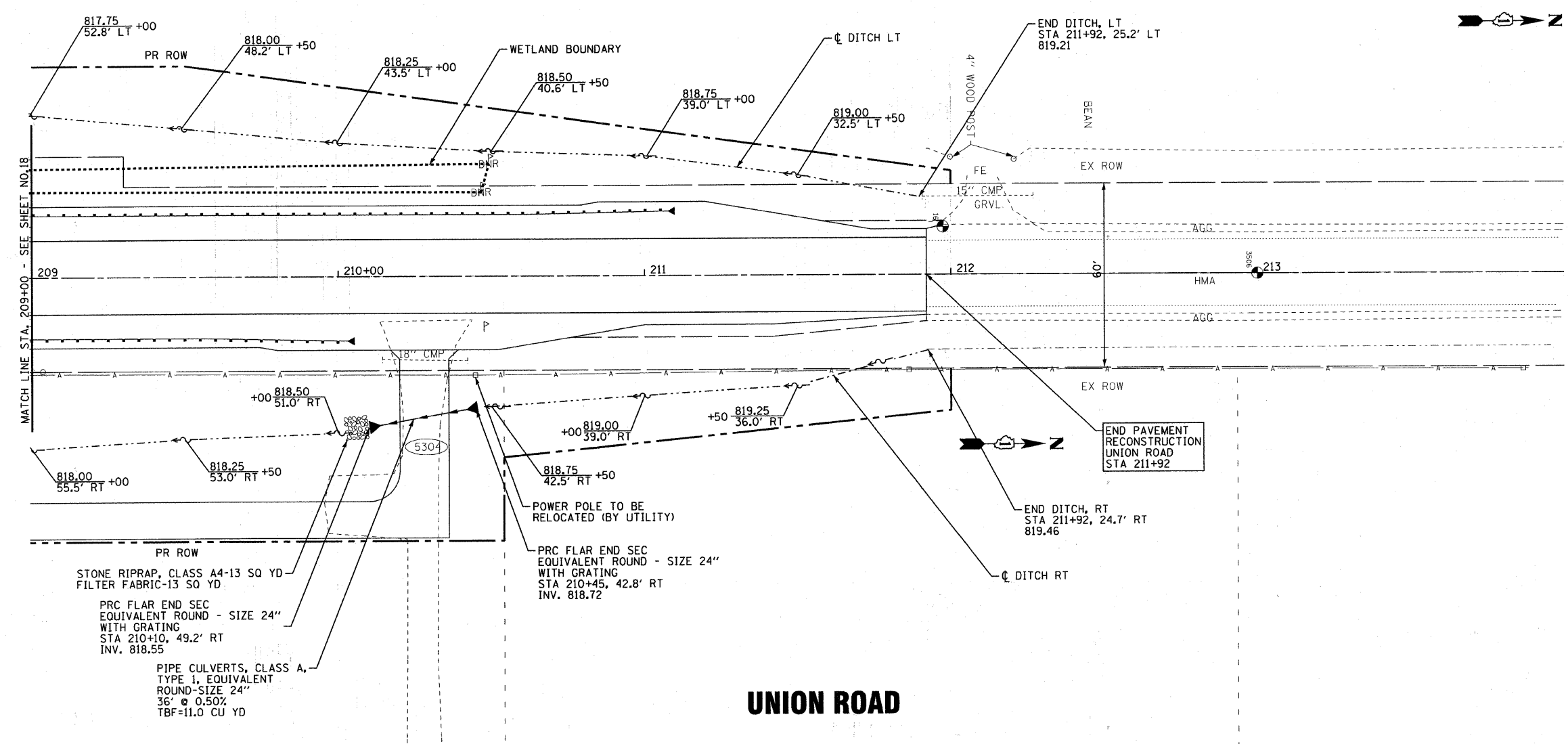
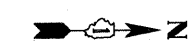
- NOTES:**
- CONSTRUCTION JOINTS BETWEEN PAVED DITCH AND ADJACENT SECTIONS OF EXISTING OR PROPOSED SLOPE WALL SHALL BE FILLED WITH PREFORMED JOINT FILLER, ACCORDING TO THE REQUIREMENTS OF ARTICLE 503.10(B) OF THE STANDARD SPECIFICATIONS. COST INCLUDED WITH PAVED DITCH, TYPE A-45.
 - CONTRACTOR SHALL ENSURE CATCH BASIN INSTALLATION WILL NOT CONFLICT WITH GUARDRAIL POST LOCATIONS.



DRAWN BY: BAXTER & WOODMAN, INC. PROJECT: MCHENRY COUNTY, ILLINOIS, UNION ROAD BRIDGE OVER SOUTH BRANCH OF KISHAWKEE RIVER. DATE: 08/20/10.

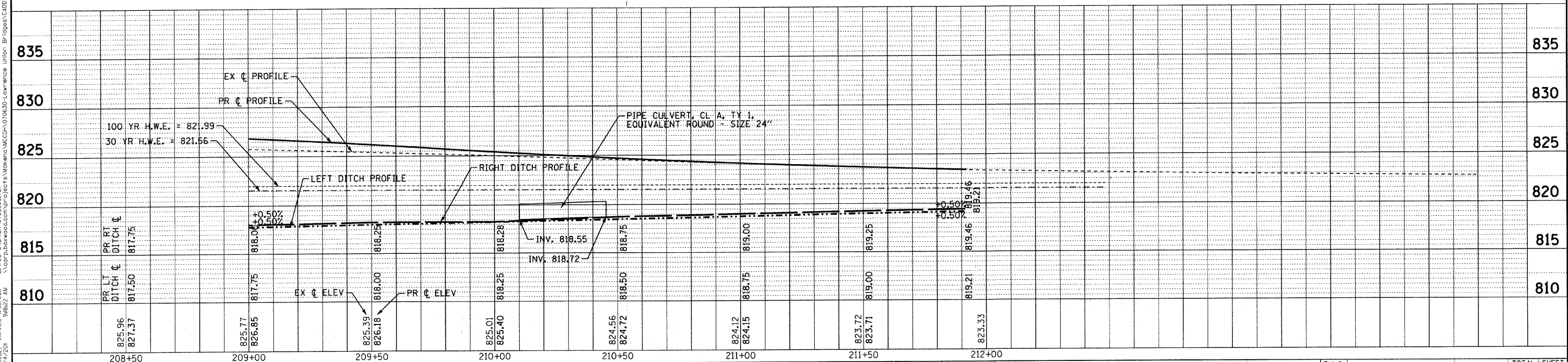
	DESIGNED - CAC	REVISED - PER IDOT 10-08-10	MCHENRY COUNTY DIVISION OF TRANSPORTATION UNION ROAD BRIDGE OVER SOUTH BRANCH OF KISHAWKEE RIVER	DRAINAGE & UTILITY PLAN		F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	DRAWN - BCD	REVISED -				0034	06-00320-02-BR	MCHENRY	65	18
	CHECKED - MDW	REVISED -				CONTRACT NO. 63536				
	DATE - 08/20/10	FILE - 070630-PH2-UNION-DU2.sh1				JOB NO. C-91-140-11 ILLINOIS FED. AID PROJECT BRS-00341071				

SCALE: H: 1"=20' V: 1"=5' STA. 205+00 TO STA. 209+00



UNION ROAD

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. - 06-00221 - EXPIRES 8/20/2011
 PROJECT: 06-00320-02-BR - SOUTH BRANCH OF KISHWAUKEE RIVER
 DRAWING: 070630-PH2-UNION-DU3.sht



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630-PH2-UNION-DU3.sht

MCHENRY COUNTY DIVISION OF TRANSPORTATION
UNION ROAD BRIDGE OVER
SOUTH BRANCH OF KISHWAUKEE RIVER

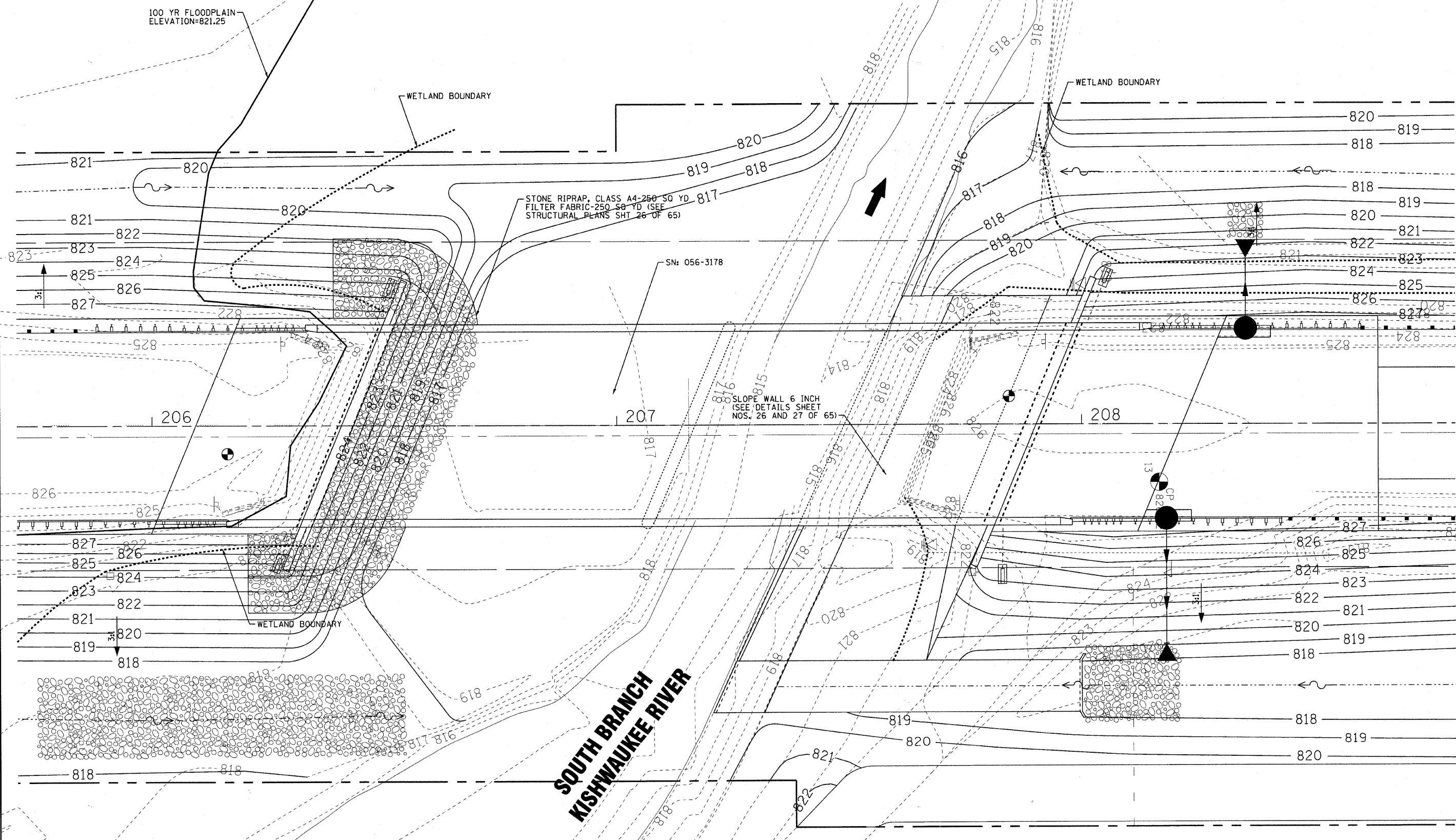
DRAINAGE & UTILITY PLAN

SCALE: H: 1"=20' V: 1"=5'	STA. 209+00 TO STA. 214+00	JOB NO. C-91-140-11	ILLINOIS FED. AID PROJECT	BR5-00341077
---------------------------	----------------------------	---------------------	---------------------------	--------------

F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 19
CONTRACT NO. 63536				

NOTES:

1. ACTUAL GRADING LIMITS TO BE DETERMINED BY RESIDENT ENGINEER.
2. SEE STRUCTURAL PLANS FOR WINGWALL, ABUTMENT, PIER AND RIPRAP DETAILS.



COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 555 S. WOODSTOCK AVE., SUITE 200, WOODSTOCK, IL 60098
 815/486-1100 FAX 815/486-1101
 12/13/2010
 I:\Projects\070630U\070630U-Lawrence Union Br\0630U-GradingDet.sht



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - MDW	REVISED -
DATE - 08/20/10	FILE - 070630U-GradingDet.sht

**MCHENRY COUNTY DIVISION OF TRANSPORTATION
 UNION ROAD BRIDGE OVER
 SOUTH BRANCH OF KISHWAUKEE RIVER**

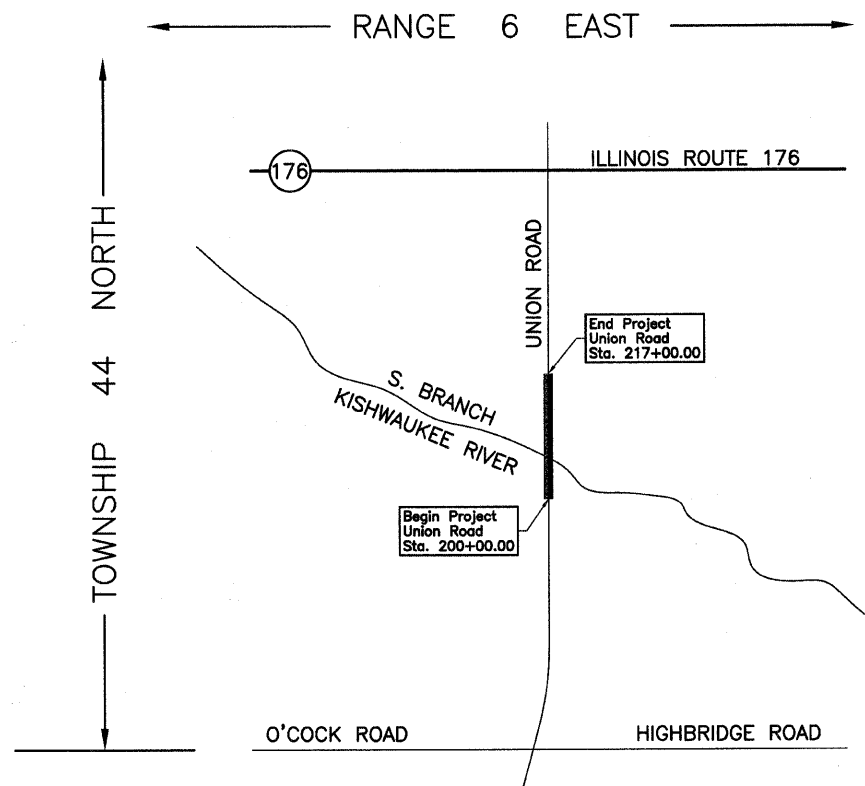
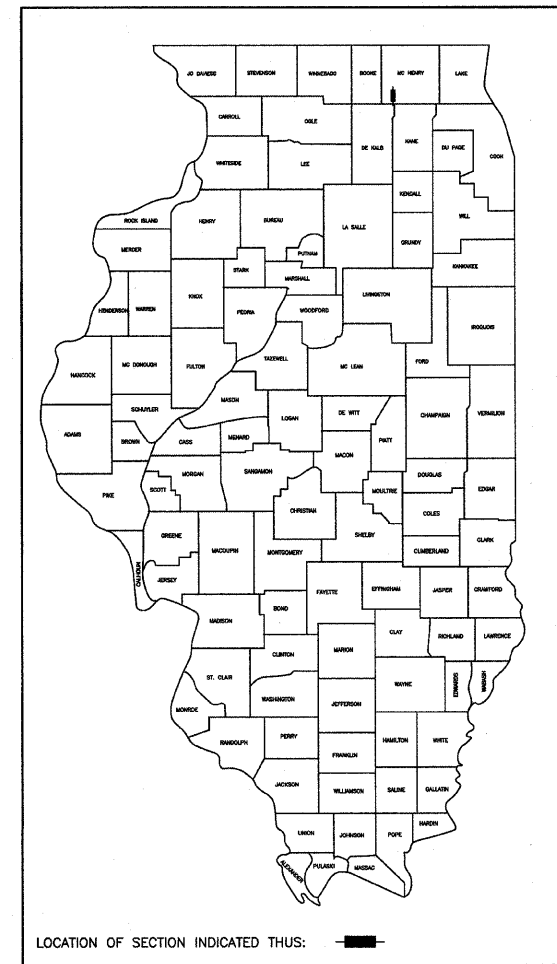
GRADING DETAIL AT BRIDGE ABUTMENTS
 SCALE: 1"=10'
 STA. TO STA.

F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 20
JOB NO. C-91-140-11		ILLINOIS FED. AID PROJECT		CONTRACT NO. 63536
BRS-0034(107)				

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 RIGHT OF WAY PLANS
 FOR PROPOSED
 FEDERAL AID HIGHWAY

ROUTE: UNION ROAD
 SECTION: 06-00320-00-BR
 PROJECT NO.:
 JOB NO.: R-55-001-97
 COUNTY: McHENRY
 LIMITS: @ S. BRANCH KISWAUKEE RIVER

F.A.S.RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0028	06-00320-01-BR	McHENRY	65	21
CALSA. SEC.		ILLINOIS PROJECT		



PROJECT LENGTH = 1700.00 LIN. FT. = 0.322 MILE, UNION ROAD

APPROVED _____ 20 _____
 LOCAL AGENCY OFFICIAL

APPROVED _____ 20 _____
 ENGINEER OF LAND ACQUISITION

APPROVED _____ 20 _____
 ENGINEER OF LOCAL ROADS & STREETS

APPROVED _____ 20 _____
 DISTRICT ENGINEER

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PART OF THE EAST 1/2 AND WEST 1/2 OF SEC. 33, TWP. 44 N., R. 6 E. OF THE 3RD. P.M., IN McHENRY COUNTY, ILLINOIS.

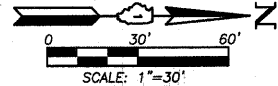
F.A.S.R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0028	06-00320-00-BR	McHENRY	65	22

PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
0002	Frederick L. Rader	100.455	0.085	0.057	100.370	N/A	N/A	12-33-400-007 12-33-400-009	
0003	Abelardo Castillo	15.447	0.884	0.333	14.563	N/A	N/A	12-33-200-016	

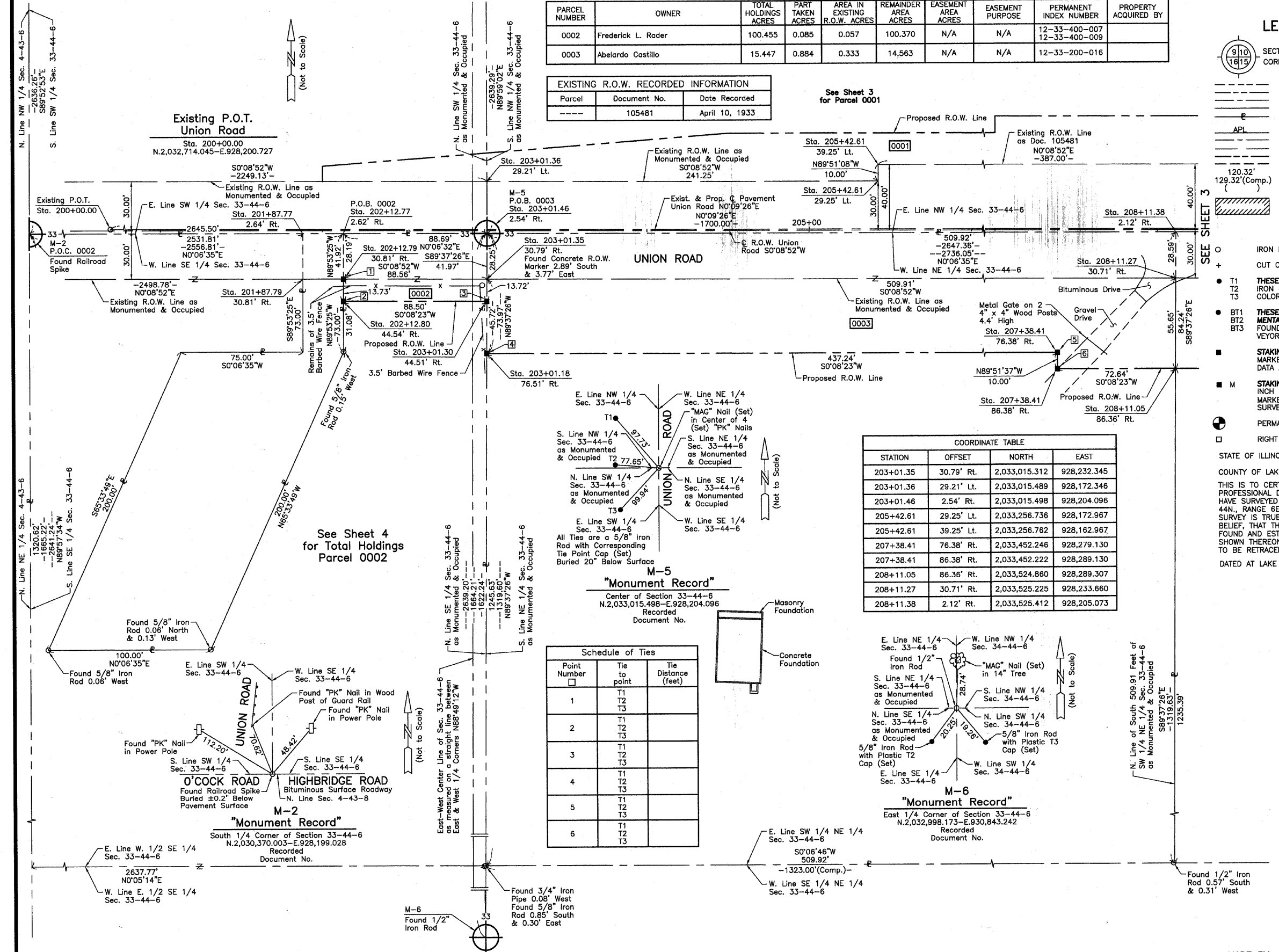
Parcel	Document No.	Date Recorded
---	105481	April 10, 1933

LEGEND

- SECTION CORNER
- QUARTER SECTION CORNER
- SECTION LINE
- QUARTER SECTION LINE
- PLATTED LOT LINE
- PROPERTY (DEED) LINE
- APPARENT PROPERTY LINE
- CENTER LINE
- EXISTING RIGHT OF WAY LINE
- PROPOSED RIGHT OF WAY LINE
- PROPOSED EASEMENT
- MEASURED DIMENSION
- COMPUTED DIMENSION
- RECORD DATA
- EXISTING BUILDING
- IRON PIPE OR ROD FOUND
- "MAG" NAIL SET
- CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1, T2, T3: THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO TIE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT1, BT2, BT3: THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.



Bearing are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.



COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
203+01.35	30.79' Rt.	2,033,015.312	928,232.345
203+01.36	29.21' Lt.	2,033,015.489	928,172.346
203+01.46	2.54' Rt.	2,033,015.498	928,204.096
205+42.61	29.25' Lt.	2,033,256.736	928,172.967
205+42.61	39.25' Lt.	2,033,256.762	928,162.967
207+38.41	76.38' Rt.	2,033,452.246	928,279.130
207+38.41	86.38' Rt.	2,033,452.222	928,289.130
208+11.05	86.36' Rt.	2,033,524.860	928,289.307
208+11.27	30.71' Rt.	2,033,525.225	928,233.660
208+11.38	2.12' Rt.	2,033,525.412	928,205.073

Schedule of Ties

Point Number	Tie to point	Tie Distance (feet)
1	T1 T2 T3	
2	T1 T2 T3	
3	T1 T2 T3	
4	T1 T2 T3	
5	T1 T2 T3	
6	T1 T2 T3	

PRESIDENT
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
LICENSE EXPIRATION DATE: NOVEMBER 30, 2010
THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
Note: Surface Coordinates are Shown.

COORDINATE TABLE

STATION	OFFSET	NORTH	EAST
201+87.77	2.64' Rt.	2,032,901.808	928,203.878
201+87.79	30.81' Rt.	2,032,901.754	928,232.053
202+12.77	2.62' Rt.	2,032,926.808	928,203.926
202+12.79	30.81' Rt.	2,032,926.754	928,232.117
202+12.80	44.54' Rt.	2,032,926.728	928,245.849
203+01.18	76.51' Rt.	2,033,015.012	928,278.064
203+01.30	44.51' Rt.	2,033,015.222	928,246.065

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371
SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

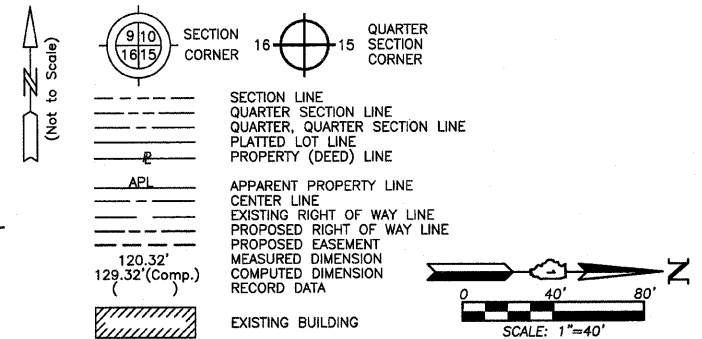
PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
UNION ROAD
SECTION 06-00320-00-BR McHENRY COUNTY
PROJECT JOB NO. R-55-001-97
STATION 200+00.00 TO STATION 209+00
SCALE: 1"=30' SHEET 2 OF 4

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PART OF THE EAST 1/2 AND PART OF THE WEST 1/2 OF SEC. 33, TWP. 44 N., R. 6 E. OF THE 3RD. P.M., IN McHENRY COUNTY, ILLINOIS.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0028	06-00320-00-BR	McHENRY	65	23

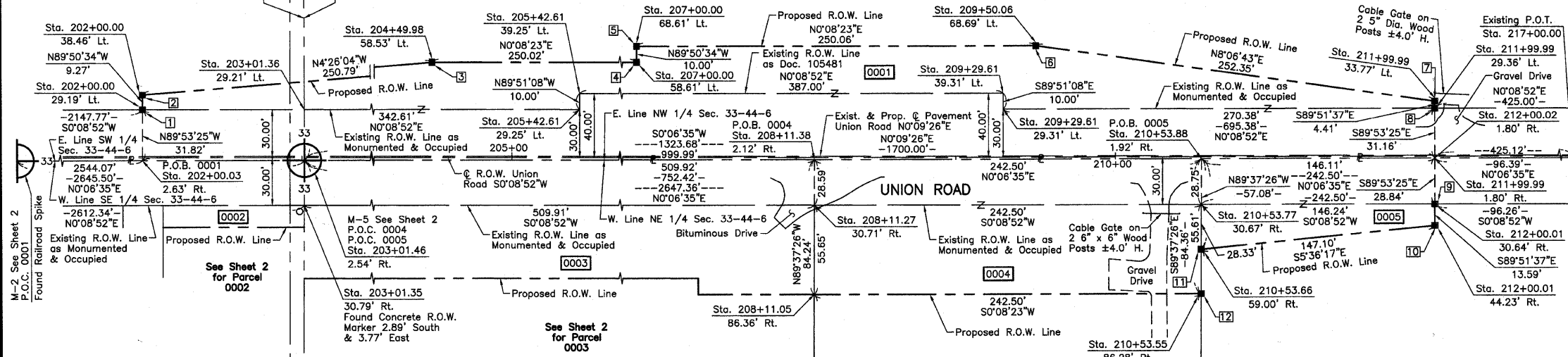
LEGEND



PARCEL NUMBER	OWNER	TOTAL HOLDINGS ACRES	PART TAKEN ACRES	AREA IN EXISTING R.O.W. ACRES	REMAINDER AREA ACRES	EASEMENT AREA ACRES	EASEMENT PURPOSE	PERMANENT INDEX NUMBER	PROPERTY ACQUIRED BY
0001	American National Bank and Trust Company of Chicago, as Trustee under Trust Agreement dated the 5th day of July, 1988 and known as Trust Number 105919-7	120.136	1.353	0.812	118.783	N/A	N/A	12-33-100-002 12-33-300-002	
0004	David P. Nickels and Linda E. Nickels, husband and wife, as Tenants by the Entirety	7.346	0.469	0.160	6.877	N/A	N/A	12-33-200-015	
0005	Robert D. Thompson and Patricia A. Thompson, his wife, as joint tenants	7.347	0.167	0.097	7.180	N/A	N/A	12-33-200-014	

See Sheet 4 for Total Holdings Parcel 0001

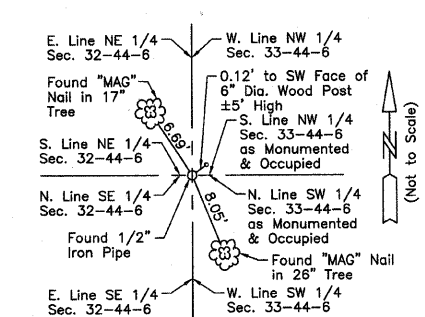
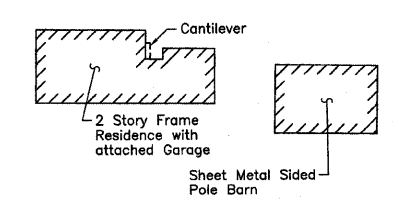
Existing P.O.T. Union Road
Sta. 217+00.00
N.2,034,414.039-E.928,205.391



Parcel	Document No.	Date Recorded
0001	105481	April 10, 1933

Point Number	Tie to point	Tie Distance (feet)
1	T1 T2 T3	
2	T1 T2 T3	
3	T1 T2 T3	
4	T1 T2 T3	
5	T1 T2 T3	
6	T1 T2 T3	
7	T1 T2 T3	
8	T1 T2 T3	
9	T1 T2 T3	
10	T1 T2 T3	
11	T1 T2 T3	
12	T1 T2 T3	

STATION	OFFSET	NORTH	EAST
205+42.61	29.25' Lt.	2,033,256.736	928,172.967
205+42.61	39.25' Lt.	2,033,256.762	928,162.967
207+00.00	58.61' Lt.	2,033,414.204	928,144.037
207+00.00	68.61' Lt.	2,033,414.231	928,134.037
208+11.05	86.36' Rt.	2,033,524.860	928,289.307
208+11.27	30.71' Rt.	2,033,525.225	928,233.660
208+11.38	2.12' Rt.	2,033,525.412	928,205.073
209+29.61	29.31' Lt.	2,033,643.735	928,173.965
209+29.61	39.31' Lt.	2,033,643.761	928,163.965
209+50.06	68.69' Lt.	2,033,664.294	928,134.646
210+53.55	86.28' Rt.	2,033,767.361	928,289.898
210+53.66	59.00' Rt.	2,033,767.540	928,262.621
210+53.77	30.67' Rt.	2,033,767.726	928,234.284
210+53.88	1.92' Rt.	2,033,767.915	928,205.538
211+99.99	1.80' Rt.	2,033,914.022	928,205.818
211+99.99	29.36' Lt.	2,033,914.112	928,174.662
211+99.99	33.77' Lt.	2,033,914.123	928,170.255
212+00.01	30.64' Rt.	2,033,913.966	928,234.661
212+00.01	44.23' Rt.	2,033,913.933	928,248.255
212+00.02	1.80' Rt.	2,033,914.053	928,205.818



THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM AND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 33, TOWNSHIP 44N., RANGE 6E., OF THE THIRD PRINCIPAL MERIDIAN, McHENRY COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.

STATION	OFFSET	NORTH	EAST
202+00.00	29.19' Lt.	2,032,914.125	928,172.084
202+00.00	38.46' Lt.	2,032,914.150	928,162.818
202+00.03	2.63' Rt.	2,032,914.064	928,203.902
203+01.35	30.79' Rt.	2,033,015.312	928,232.345
203+01.36	29.21' Lt.	2,033,015.489	928,172.346
203+01.46	2.54' Rt.	2,033,015.498	928,204.096
204+49.98	58.53' Lt.	2,033,164.187	928,143.428

JORGENSEN & ASSOCIATES, INC.
120 PARK AVENUE
LAKE VILLA, ILLINOIS 60046
(847) 356-3371

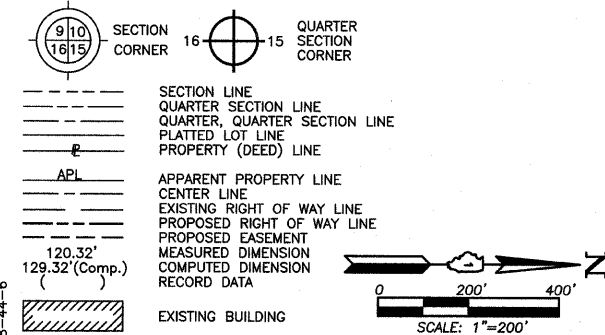
PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
UNION ROAD
SECTION 06-0320-00-BR McHENRY COUNTY
PROJECT TO STATION 217+00.00
JOB NO. R-55-001-97
SCALE: 1"=40' SHEET 3 OF 4

BUREAU OF LAND ACQUISITION
201 WEST CENTER COURT
SCHAUMBURG, ILLINOIS 60196

PART OF THE EAST 1/2 AND PART OF THE WEST 1/2 OF SEC. 33, TWP. 44 N., R. 6 E. OF THE 3RD. P.M., IN McHENRY COUNTY, ILLINOIS.

F.A.S.R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0028	06-00320-00-BR	McHENRY	65	24

LEGEND



Bearing are referenced to the Illinois Coordinate System NAD 83(2007) East Zone.

- IRON PIPE OR ROD FOUND
- ⊕ "MAG" NAIL SET
- + CUT CROSS FOUND OR SET
- 5/8" REBAR SET
- T1 THESE STAKES REFERENCE FOUND OR SET MONUMENTATION. SET 5/8 INCH IRON ROD FLUSH WITH GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- T2
- T3
- BT1 THESE STAKES, IN CULTIVATED AREAS, REFERENCE FOUND OR SET MONUMENTATION. BURIED 5/8 INCH IRON ROD 20 INCHES BELOW GROUND TO THE FOUND IRON STAKE. IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- BT2
- BT3
- STAKING OF PROPOSED RIGHT OF WAY. SET DIVISION OF HIGHWAYS SURVEY MARKER TO MONUMENT THE POSITION SHOWN. IDENTIFIED BY INSCRIPTION DATA AND SURVEYORS REGISTRATION NUMBER.
- M STAKING OF PROPOSED RIGHT OF WAY IN CULTIVATED AREAS. BURIED 5/8 INCH METAL ROD 20 INCHES BELOW GROUND TO MARK FUTURE SURVEY MARKER POSITION IDENTIFIED BY COLORED PLASTIC CAP BEARING SURVEYORS REGISTRATION NUMBER.
- ⊙ PERMANENT SURVEY MARKER, I.D.O.T STD 2135 (TO BE SET BY OTHERS)
- RIGHT OF WAY STAKING PROPOSED TO BE SET.

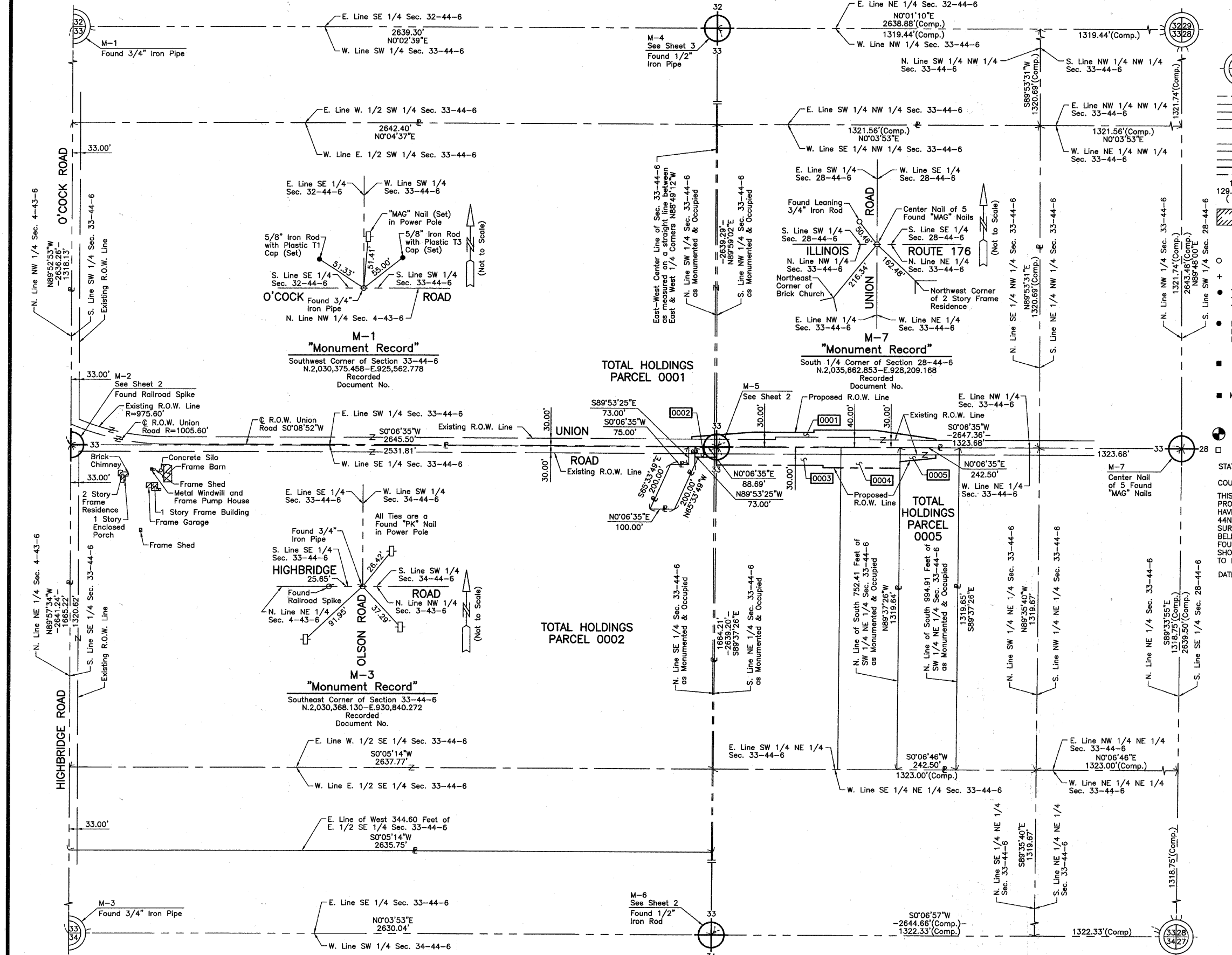
STATE OF ILLINOIS }
 COUNTY OF LAKE }SS
 THIS IS TO CERTIFY THAT WE, JORGENSEN & ASSOCIATES, INC., AN ILLINOIS PROFESSIONAL DESIGN FIRM LAND SURVEYING CORPORATION, NUMBER 184-2771, HAVE SURVEYED THE PLAT OF HIGHWAYS SHOWN HEREON IN SECTION 33, TOWNSHIP 44N., RANGE 6E., OF THE THIRD PRINCIPAL MERIDIAN, McHENRY COUNTY, THAT THE SURVEY IS TRUE AND COMPLETE AS SHOWN TO THE BEST OF MY KNOWLEDGE AND BELIEF, THAT THE PLAT CORRECTLY REPRESENTS SAID SURVEY, THAT ALL MONUMENTS FOUND AND ESTABLISHED ARE OF PERMANENT QUALITY AND OCCUPY THE POSITIONS SHOWN THEREON AND THAT THE MONUMENTS ARE SUFFICIENT TO ENABLE THE SURVEY TO BE RETRACED. MADE FOR THE DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS.
 DATED AT LAKE VILLA, ILLINOIS THIS _____ DAY OF _____ 20____ A.D.

JORGENSEN & ASSOCIATES, INC.
 120 PARK AVENUE
 LAKE VILLA, ILLINOIS 60046
 (847) 356-3371
 PRESIDENT
 ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 35-2797
 LICENSE EXPIRATION DATE: NOVEMBER 30, 2010
 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
 Note: Surface Coordinates are Shown.

JORGENSEN & ASSOCIATES, INC.
 120 PARK AVENUE
 LAKE VILLA, ILLINOIS 60046
 (847) 356-3371
 SHEET 1 IS A COVER SHEET AND IS NOT RECORDED.

PLAT OF HIGHWAYS
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
 UNION ROAD
 SECTION 06-00320-00-BR McHENRY COUNTY
 PROJECT JOB NO. R-55-001-97
 STATION NONE TO STATION
 SCALE: 1"=200' SHEET 4 OF 4

BUREAU OF LAND ACQUISITION
 201 WEST CENTER COURT
 SCHAUMBURG, ILLINOIS 60196

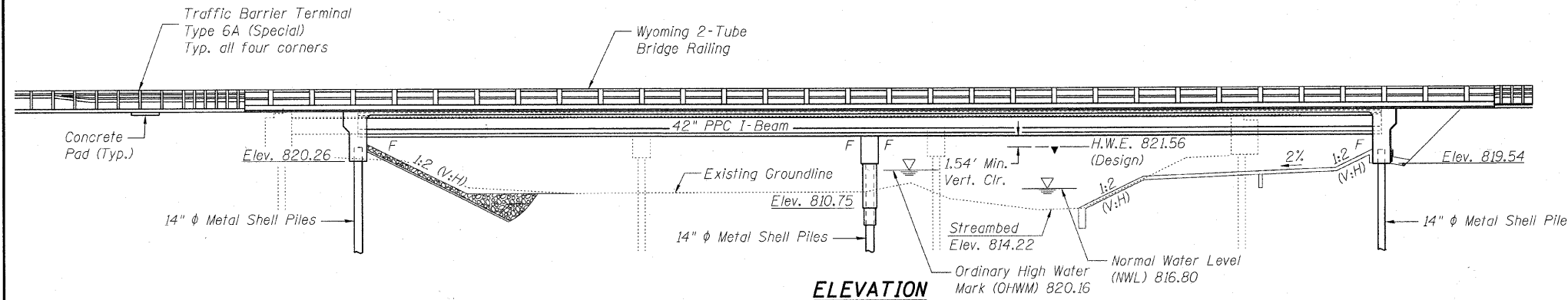


MCHENRY COUNTY
DIVISION OF TRANSPORTATION

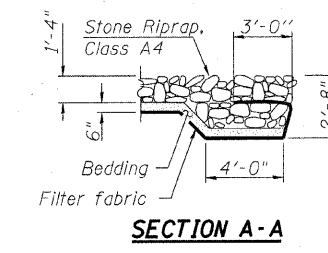
Benchmark: Chiseled square at northwest corner of Union Road bridge over south branch Kishwaukee River. Elev. 826.79

Existing Structure: S.N. 056-0035 was built in 1963 as C.H. Route 14 - Sec. 3B. 33'-8" out to out width with 2'-10" raised curbs and steel railings. 140'-6" back-to-back of abutments. The existing superstructure is composed of nine 27" PPC deck beams, with outside modified deck beams, and a bituminous overlay. The substructure consists of reinforced concrete wall piers and open abutments with precast concrete piles. The existing structure is to be removed and replaced with proposed structure. Traffic to be detoured during construction.

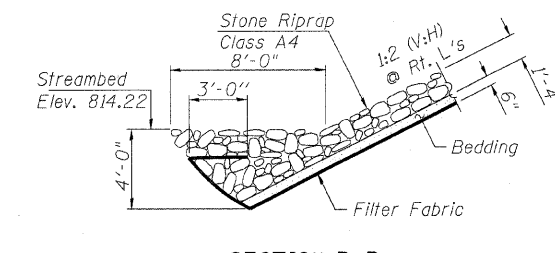
Existing Name Plate to be salvaged. See Roadway General Notes.



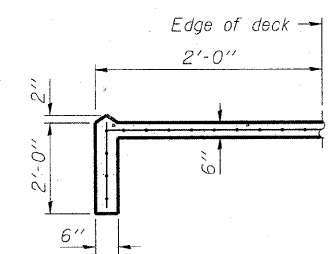
ELEVATION



SECTION A-A



SECTION B-B



SECTION C-C

S. BRANCH KISHWAUKEE RIVER
BUILT 20__ BY
MCHENRY COUNTY
SEC. 06-00320-02-BR
F.A.S. RT. 34 STA. 207+15.50
STR. NO. 056-3178 LOADING HL-93

NAME PLATE
See Std. 515001

DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (ft.)	S. Abut.	Pier	N. Abut.
	820.26	810.75	819.54

LOADING HL-93

Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

2007 AASHTO LRFD Bridge Design Specifications with 2008 and 2009 Interims

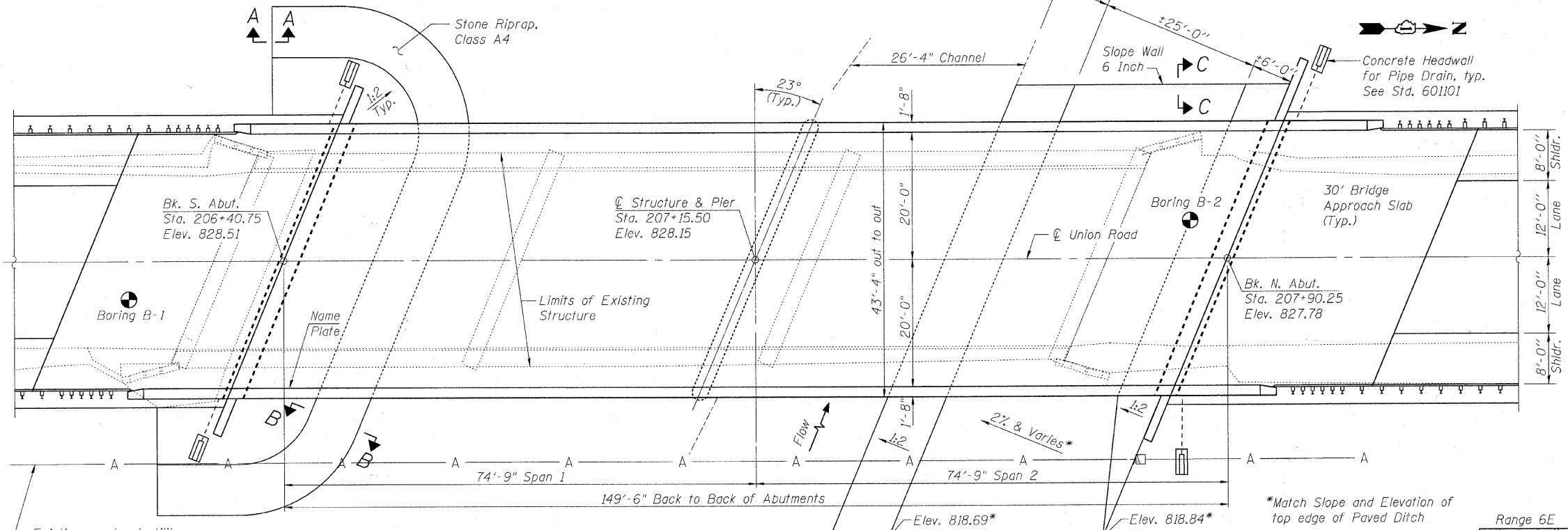
DESIGN STRESSES

FIELD UNITS

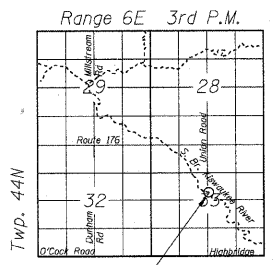
- $f'_c = 3,500$ psi
- $f_y = 60,000$ psi (Reinforcement)
- $f'_c = 7,000$ psi
- $f'_ci = 6,000$ psi
- $f_{pu} = 270,000$ psi (1/2" dia. low lax strands)
- $f_{pb} = 201,960$ psi (1/2" dia. low lax strands)

SEISMIC DATA

LRFD Seismic Performance Zone (LRFD SPZ) = 1
Design Spectral Acceleration @ 1.0 sec. (SD1) = 0.15g
Design Spectral Acceleration @ 2.0 sec. (SDS) = 0.05g
Soil Site Class - D

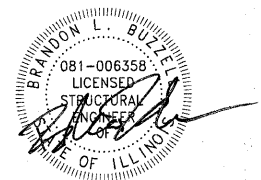


PLAN



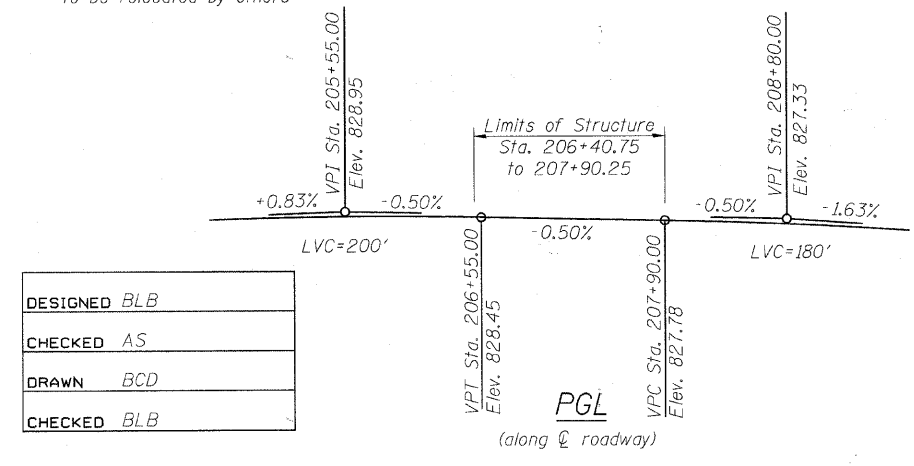
PROPOSED BRIDGE
LOCATION SKETCH

GENERAL PLAN
UNION ROAD OVER THE SOUTH
BRANCH OF THE KISHWAUKEE RIVER
FAS 34 - SEC. 06-00320-02-BR
MCHENRY COUNTY
STATION 207+15.50
STRUCTURE NO. 056-3178



DATE: 1/03/2011
LICENSE EXPIRES 11/30/12

I certify that to the best of my knowledge, information and belief, this bridge design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the current AASHTO LRFD Bridge Design Specifications.



DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 1 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	26
CONTRACT NO. 63536					
JOB NO. C-91-140-11		ILLINOIS FED. AID PROJECT		BRS-0034(107)	

COPYRIGHT © 2010 BY BAXTER & MOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 081-006358
 EXPIRES 11/30/12

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

GENERAL NOTES

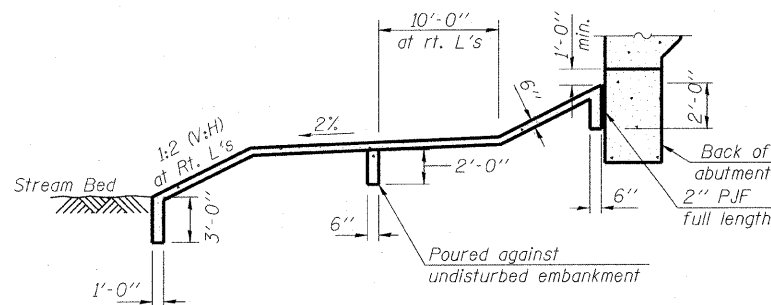
Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Special Provisions.
Reinforcement bars designated (E) shall be epoxy coated.
Layout of slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
Sloped wall shall be reinforced with welded wire fabric, 6" x 6" - W4.0 x W4.0, weighing 58 lbs. per 100 sq. ft.
The Contractor is advised that the existing structure contains members that are in deteriorated condition with reduced load carrying capacity. It is the Contractor's responsibility to account for the condition of the existing structure when developing construction procedures for removal.

TOTAL BILL OF MATERIAL

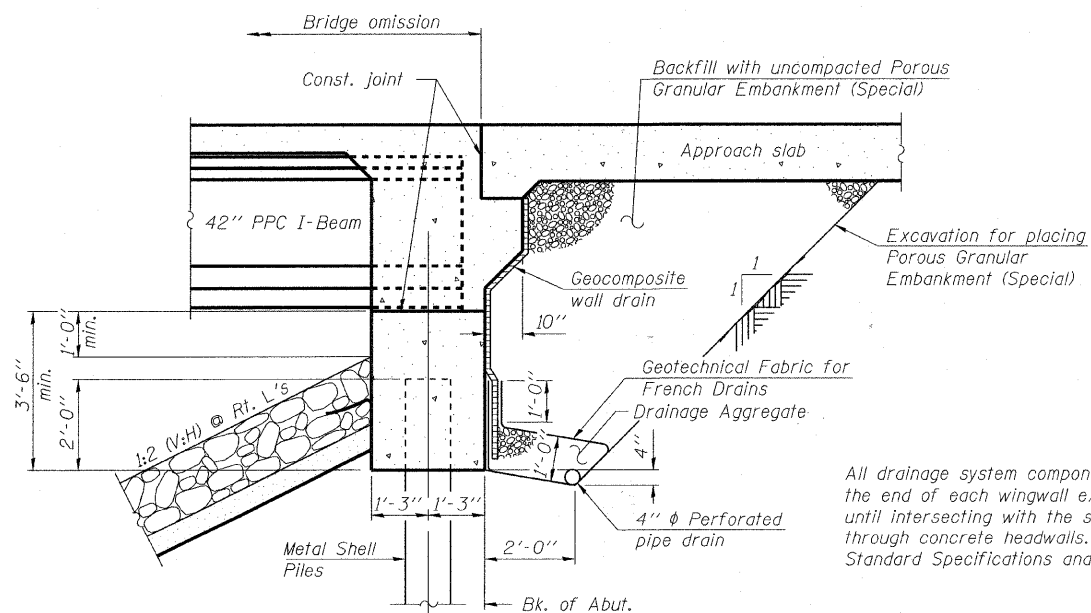
ITEM	UNIT	SUPER	SUB	TOTAL
Porous Granular Embankment, Special	Cu. Yd.		176	176
Stone Riprap, Class A4	Sq. Yd.		250	250
Filter Fabric	Sq. Yd.		250	250
Removal of Existing Structures	Each		1	1
Structure Excavation	Cu. Yd.		402.0	402.0
Concrete Structures	Cu. Yd.		125.2	125.2
Concrete Superstructure	Cu. Yd.	353.3		353.3
Bridge Deck Grooving	Sq. Yd.		876	876
Protective Coat	Sq. Yd.		1023	1023
Furnishing and Erecting Precast Prestressed Concrete I Beams, 42 In.	Foot	883		883
Reinforcement Bars, Epoxy Coated	Pound	89330	10940	100270
Bar Splicers	Each	88		88
Furnishing Metal Shell Piles 14" x 0.312"	Foot		1629	1629
Driving Piles	Foot		1629	1629
Test Pile Metal Shells	Each		2	2
Name Plates	Each	1		1
Geocomposite Wall Drain	Sq. Yd.		94	94
Pipe Underdrains for Structures 4"	Foot		182	182
Slope Wall Removal	Sq. Yd.		190	190
Slope Wall 6 Inch	Sq. Yd.		362	362
Concrete Encasement	Cu. Yd.		7.1	7.1
Underwater Structure Excavation Protection - Location 1	Each		1	1
Steel Railing (Special)	Foot	359		359
Asbestos Bearing Pad Removal	Each	44		44
Pile Shoes	Each		37	37

INDEX OF SHEETS

- 1 General Plan
- 2 General Data
- 3-5 Top of Slab Elevations
- 6 Top of South Approach Slab Elevations
- 7 Top of North Approach Slab Elevations
- 8 Superstructure
- 9 Superstructure Details
- 10 Diaphragm Details
- 11-12 Bridge Approach Slab Details
- 13-14 Wyoming 2-Tube Bridge Railing Details
- 15 Framing Plan
- 16 42" PPC I-Beam
- 17 42" PPC I-Beam Details
- 18 Abutments
- 19 Pier
- 20 Metal Shell Pile Details
- 21 Bar Splicer Assembly and Mechanical Splicer Details
- 22-23 Soil Borings



**SECTION THRU
CONCRETE SLOPEWALL**



SECTION THRU INTEGRAL ABUTMENT

South Abutment Shown
North Abutment similar
(Horiz. dim. @ Rt. L's)

All drainage system components shall extend to 2'-0" from the end of each wingwall except an outlet pipe shall extend until intersecting with the side slopes. The pipes shall drain through concrete headwalls. (See Article 601.05 of the Standard Specifications and Highway Standard 601101.)

WATERWAY INFORMATION

Drainage Area = 71.5 sq. mi. Ex. Low Grade Elev. = 822.57 @ Sta. 216+83
Pr. Low Grade Elev. = 822.57 @ Sta. 216+83

Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.		Nat. H.W.E.	Head - Ft.		Headwater El.	
			Exist.	Prop.		Exist.	Prop.	Exist.	Prop.
Design	10	2095	465	499	821.29	0.48	0.46	821.77	821.75
Base	30	2719	495	533	821.56	0.67	0.66	822.23	822.22
Max. Calc.	50	3099	509	550	821.69	0.81	0.69	822.50	822.38
	100	3983	545	589	821.99	0.85	0.77	822.84	822.76
	500	5817	597	668	822.60	0.94	0.85	823.54	823.45

10 year velocity through existing bridge = 4.7 fps
10 year velocity through proposed bridge = 4.4 fps

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

GENERAL DATA
STRUCTURE NO. 056-3178

SHEET NO. 2 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	27
CONTRACT NO. 63536					
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

BEAM 1

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back S. Abut.	206+48.53	-18.33	828.10	828.10
☉ S. Abut.	206+49.89	-18.33	828.09	828.09
A	206+59.89	-18.33	828.05	828.08
B	206+69.89	-18.33	828.00	828.06
C	206+79.89	-18.33	827.95	828.03
D	206+89.89	-18.33	827.90	827.98
E	206+99.89	-18.33	827.85	827.92
F	207+09.89	-18.33	827.80	827.84
☉ Pier	207+23.28	-18.33	827.73	827.73
G	207+33.28	-18.33	827.68	827.71
H	207+43.28	-18.33	827.63	827.70
I	207+53.28	-18.33	827.58	827.66
J	207+63.28	-18.33	827.53	827.61
K	207+73.28	-18.33	827.48	827.55
L	207+83.28	-18.33	827.43	827.48
☉ N. Abut.	207+96.67	-18.33	827.36	827.36
Back N. Abut.	207+98.03	-18.33	827.36	827.36

BEAM 2

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back S. Abut.	206+45.42	-11.00	828.27	828.27
☉ S. Abut.	206+46.78	-11.00	828.26	828.26
A	206+56.78	-11.00	828.21	828.25
B	206+66.78	-11.00	828.16	828.23
C	206+76.78	-11.00	828.11	828.19
D	206+86.78	-11.00	828.06	828.15
E	206+96.78	-11.00	828.01	828.09
F	207+06.78	-11.00	827.96	828.01
☉ Pier	207+20.17	-11.00	827.90	827.90
G	207+30.17	-11.00	827.85	827.88
H	207+40.17	-11.00	827.80	827.86
I	207+50.17	-11.00	827.75	827.83
J	207+60.17	-11.00	827.70	827.78
K	207+70.17	-11.00	827.65	827.72
L	207+80.17	-11.00	827.60	827.64
☉ N. Abut.	207+93.56	-11.00	827.53	827.53
Back N. Abut.	207+94.92	-11.00	827.53	827.53

BEAM 3

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back S. Abut.	206+42.31	-3.67	828.43	828.43
☉ S. Abut.	206+43.67	-3.67	828.43	828.43
A	206+53.67	-3.67	828.38	828.41
B	206+63.67	-3.67	828.33	828.40
C	206+73.67	-3.67	828.28	828.36
D	206+83.67	-3.67	828.23	828.32
E	206+93.67	-3.67	828.18	828.25
F	207+03.67	-3.67	828.13	828.18
☉ Pier	207+17.06	-3.67	828.07	828.07
G	207+27.06	-3.67	828.02	828.05
H	207+37.06	-3.67	827.97	828.03
I	207+47.06	-3.67	827.92	828.00
J	207+57.06	-3.67	827.87	827.95
K	207+67.06	-3.67	827.82	827.89
L	207+77.06	-3.67	827.77	827.81
☉ N. Abut.	207+90.45	-3.67	827.70	827.70
Back N. Abut.	207+91.81	-3.67	827.69	827.69

☉ & PGL

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back S. Abut.	206+40.75	0.00	828.51	828.51
☉ S. Abut.	206+42.11	0.00	828.51	828.51
A	206+52.11	0.00	828.46	828.50
B	206+62.11	0.00	828.42	828.48
C	206+72.11	0.00	828.37	828.45
D	206+82.11	0.00	828.32	828.40
E	206+92.11	0.00	828.27	828.34
F	207+02.11	0.00	828.22	828.26
☉ Pier	207+15.50	0.00	828.15	828.15
G	207+25.50	0.00	828.10	828.13
H	207+35.50	0.00	828.05	828.12
I	207+45.50	0.00	828.00	828.08
J	207+55.50	0.00	827.95	828.03
K	207+65.50	0.00	827.90	827.97
L	207+75.50	0.00	827.85	827.90
☉ N. Abut.	207+88.89	0.00	827.78	827.78
Back N. Abut.	207+90.25	0.00	827.78	827.78

**TOP OF SLAB ELEVATIONS
STRUCTURE NO. 056-3178**

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 4 23 SHEETS	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	29
JOB NO. C-91-140-11			ILLINOIS	FED. AID PROJECT	BRS-0034(107)
CONTRACT NO. 63536					

COPYRIGHT © 2010 BY TAYLOR & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL ENGINEER
 LICENSE NO. 12122000
 PROJECT NO. 06-00320-02-BR
 SHEET NO. 4 OF 29
 DATE: 11/11/10

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

BEAM 4

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back S. Abut.	206+39.19	3.67	828.44	828.44
☉ S. Abut.	206+40.55	3.67	828.44	828.44
A	206+50.55	3.67	828.40	828.43
B	206+60.55	3.67	828.35	828.41
C	206+70.55	3.67	828.30	828.38
D	206+80.55	3.67	828.25	828.33
E	206+90.55	3.67	828.20	828.27
F	207+00.55	3.67	828.15	828.19
☉ Pier	207+13.94	3.67	828.08	828.08
G	207+23.94	3.67	828.03	828.07
H	207+33.94	3.67	827.98	828.05
I	207+43.94	3.67	827.93	828.01
J	207+53.94	3.67	827.88	827.97
K	207+63.94	3.67	827.83	827.90
L	207+73.94	3.67	827.78	827.83
☉ N. Abut.	207+87.33	3.67	827.71	827.71
Back N. Abut.	207+88.69	3.67	827.71	827.71

BEAM 5

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back S. Abut.	206+36.08	11.00	828.30	828.30
☉ S. Abut.	206+37.44	11.00	828.30	828.30
A	206+47.44	11.00	828.26	828.29
B	206+57.44	11.00	828.21	828.28
C	206+67.44	11.00	828.16	828.24
D	206+77.44	11.00	828.11	828.20
E	206+87.44	11.00	828.06	828.13
F	206+97.44	11.00	828.01	828.06
☉ Pier	207+10.83	11.00	827.94	827.94
G	207+20.83	11.00	827.89	827.93
H	207+30.83	11.00	827.84	827.91
I	207+40.83	11.00	827.79	827.87
J	207+50.83	11.00	827.74	827.83
K	207+60.83	11.00	827.69	827.76
L	207+70.83	11.00	827.64	827.69
☉ N. Abut.	207+84.22	11.00	827.58	827.58
Back N. Abut.	207+85.58	11.00	827.57	827.57

BEAM 6

Location	Station	Offset	Theoretical Grade Elevations	Theoretical Grade Elevations Adjusted For Dead Load Deflection
Back S. Abut.	206+32.97	18.33	828.16	828.16
☉ S. Abut.	206+34.33	18.33	828.16	828.16
A	206+44.33	18.33	828.12	828.15
B	206+54.33	18.33	828.07	828.14
C	206+64.33	18.33	828.02	828.10
D	206+74.33	18.33	827.97	828.06
E	206+84.33	18.33	827.92	827.99
F	206+94.33	18.33	827.87	827.92
☉ Pier	207+07.72	18.33	827.81	827.81
G	207+17.72	18.33	827.76	827.79
H	207+27.72	18.33	827.71	827.77
I	207+37.72	18.33	827.66	827.74
J	207+47.72	18.33	827.61	827.69
K	207+57.72	18.33	827.56	827.63
L	207+67.72	18.33	827.51	827.55
☉ N. Abut.	207+81.11	18.33	827.44	827.44
Back N. Abut.	207+82.47	18.33	827.43	827.43

**TOP OF SLAB ELEVATIONS
STRUCTURE NO. 056-3178**

DESIGNED <i>BLB</i>
CHECKED <i>AS</i>
DRAWN <i>BCD</i>
CHECKED <i>BLB</i>

SHEET NO. 5 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	30
CONTRACT NO. 63536					
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

COPYRIGHT © 2010 BY BAYLER & WOODMAN, INC.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
15582000
1/2/25/2010

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

WEST EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
S. End S. Appr. Slab	206+19.24	-20.00	828.17
A1	206+29.24	-20.00	828.14
A2	206+39.24	-20.00	828.10
N. End S. Appr. Slab	206+49.24	-20.00	828.06

WEST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
S. End S. Appr. Slab	206+15.84	-12.00	828.34
A1	206+25.84	-12.00	828.32
A2	206+35.84	-12.00	828.28
N. End S. Appr. Slab	206+45.84	-12.00	828.24

☉ ROADWAY & PROFILE GRADE

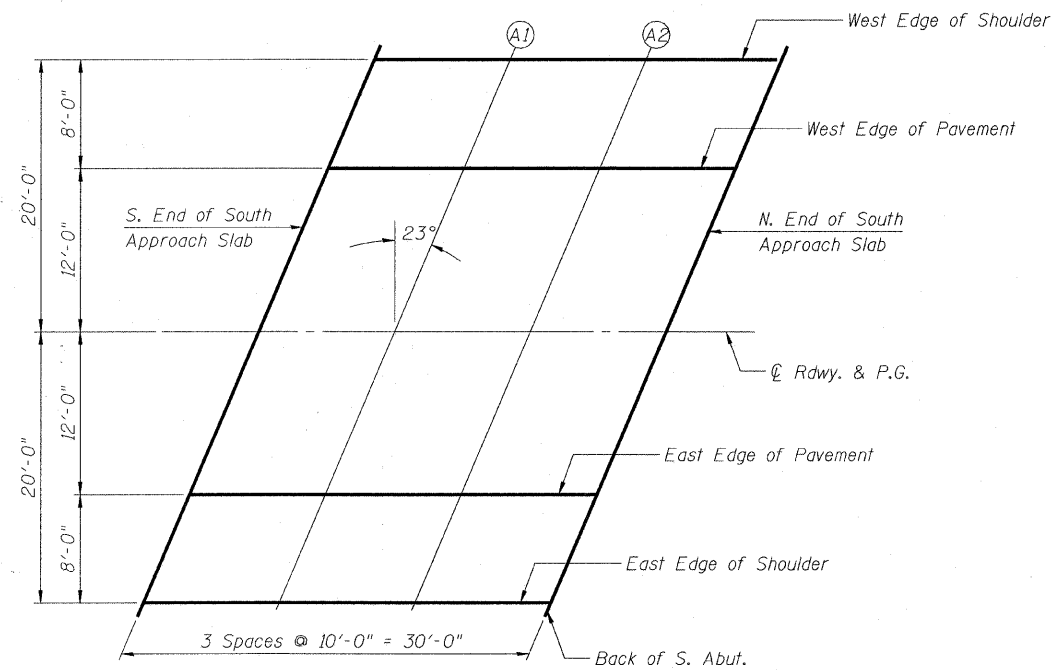
Location	Station	Offset	Theoretical Grade Elevations
S. End S. Appr. Slab	206+10.75	0.00	828.61
A1	206+20.75	0.00	828.58
A2	206+30.75	0.00	828.55
N. End S. Appr. Slab	206+40.75	0.00	828.51

EAST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
S. End S. Appr. Slab	206+05.66	12.00	828.37
A1	206+15.66	12.00	828.35
A2	206+25.66	12.00	828.32
N. End S. Appr. Slab	206+35.66	12.00	828.28

EAST EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
S. End S. Appr. Slab	206+02.26	20.00	828.20
A1	206+12.26	20.00	828.19
A2	206+22.26	20.00	828.16
N. End S. Appr. Slab	206+32.26	20.00	828.13



PLAN
South Approach

**TOP OF SOUTH APPROACH SLAB ELEVATIONS
STRUCTURE NO. 056-3178**

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 6	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	31
23 SHEETS	CONTRACT NO. 63536				
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

WEST EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
S. End N. Appr. Slab	207+98.74	-20.00	827.32
A3	208+08.74	-20.00	827.26
A4	208+18.74	-20.00	827.19
N. End N. Appr. Slab	208+28.74	-20.00	827.12

WEST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
S. End N. Appr. Slab	207+95.34	-12.00	827.50
A3	208+05.34	-12.00	827.45
A4	208+15.34	-12.00	827.38
N. End N. Appr. Slab	208+25.34	-12.00	827.31

☉ ROADWAY & PROFILE GRADE

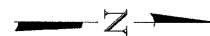
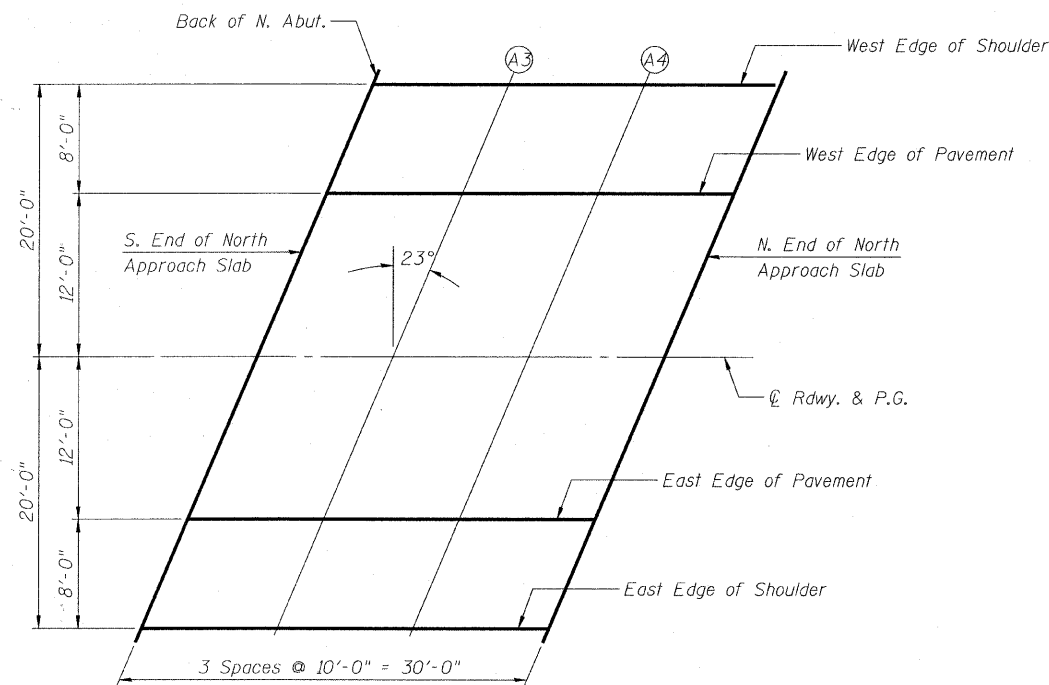
Location	Station	Offset	Theoretical Grade Elevations
S. End N. Appr. Slab	207+90.25	0.00	827.78
A3	208+00.25	0.00	827.73
A4	208+10.25	0.00	827.67
N. End N. Appr. Slab	208+20.25	0.00	827.60

EAST EDGE OF PAVEMENT

Location	Station	Offset	Theoretical Grade Elevations
S. End N. Appr. Slab	207+85.16	12.00	827.55
A3	207+95.16	12.00	827.50
A4	208+05.16	12.00	827.45
N. End N. Appr. Slab	208+15.16	12.00	827.38

EAST EDGE OF SHOULDER

Location	Station	Offset	Theoretical Grade Elevations
S. End N. Appr. Slab	207+81.76	20.00	827.40
A3	207+91.76	20.00	827.35
A4	208+01.76	20.00	827.30
N. End N. Appr. Slab	208+11.76	20.00	827.24



PLAN
North Approach

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

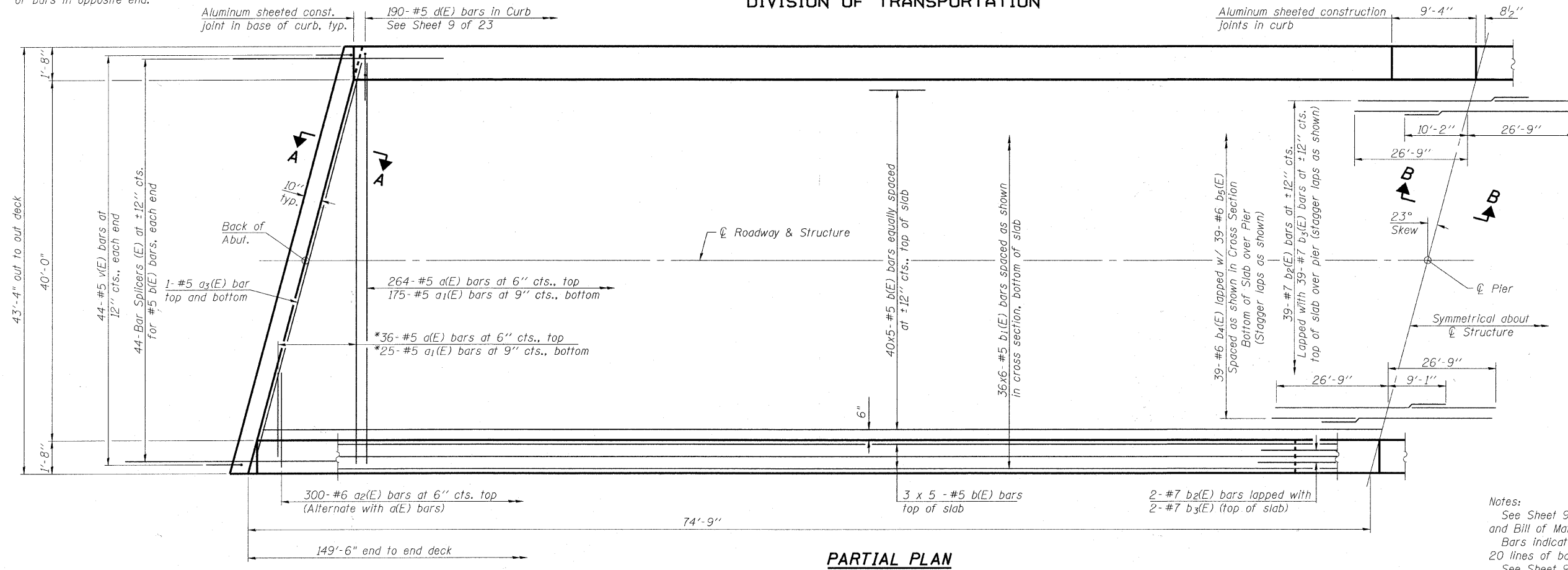
**TOP OF NORTH APPROACH SLAB ELEVATIONS
STRUCTURE NO. 056-3178**

SHEET NO. 7 23 SHEETS	F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 32
	CONTRACT NO. 63536				
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

COPYRIGHT © 2010 BY BAYTER & WOODMAN, INC.
 ALL RIGHTS RESERVED. THIS DOCUMENT IS THE PROPERTY OF BAYTER & WOODMAN, INC. AND IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN.
 ANY REUSE OR DISTRIBUTION OF THIS DOCUMENT WITHOUT THE WRITTEN PERMISSION OF BAYTER & WOODMAN, INC. IS STRICTLY PROHIBITED.
 DATE: 12/29/2009
 FILE: 06-00320-02-BR.dwg
 PLOT: 12/29/2009 10:30 AM

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

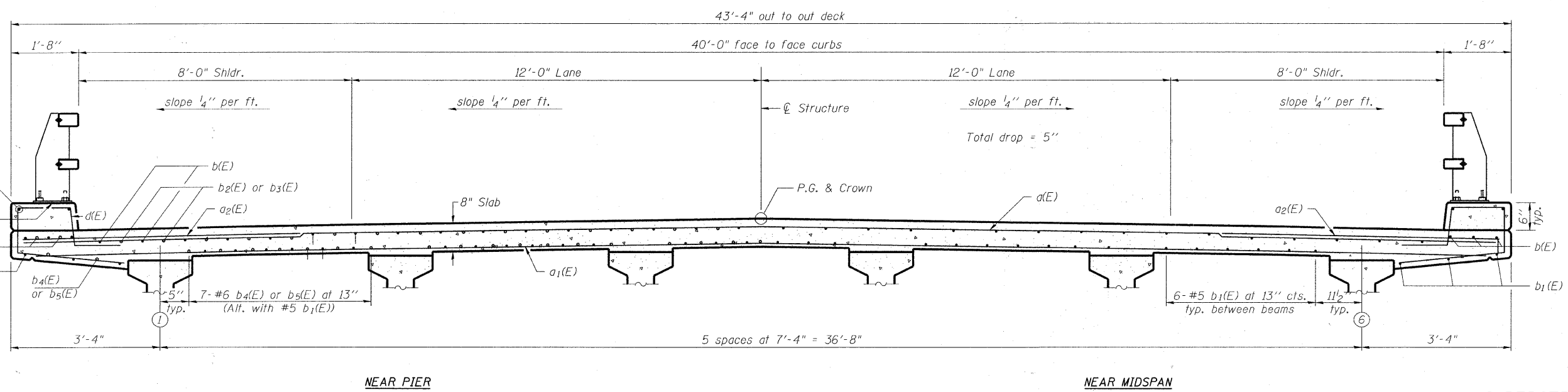
* Order a(E) and a₁(E) bars full length.
Cut to fit skew and use remainder
of bars in opposite end.



MIN. LAP LENGTHS

#4 bar	2'-1"
#5 bar	2'-7"
#6 bar	3'-1"
#7 bar	4'-2"

Notes:
See Sheet 9 of 23 for superstructure details and Bill of Material.
Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.
See Sheet 9 of 23 for curb reinforcement.
See Sheet 10 of 23 for Sections A-A and B-B.



CROSS SECTION
(Looking North)

**SUPERSTRUCTURE
STRUCTURE NO. 056-3178**

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

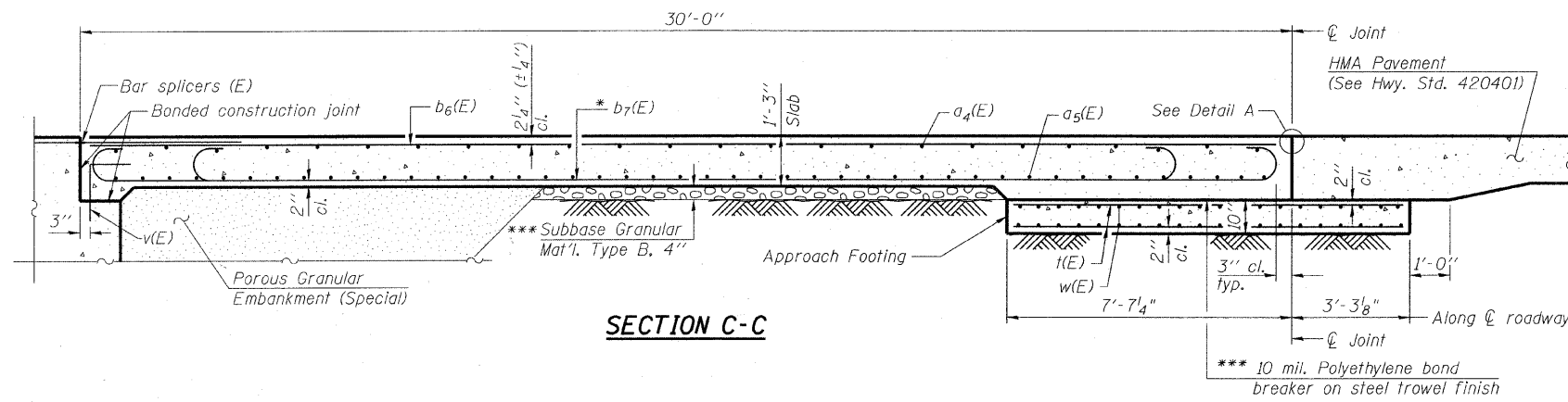
SHEET NO. 8 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	33
			CONTRACT NO. 63536		
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

COPYRIGHT © 2000 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 061-000000-000000-000000
 12/22/2000

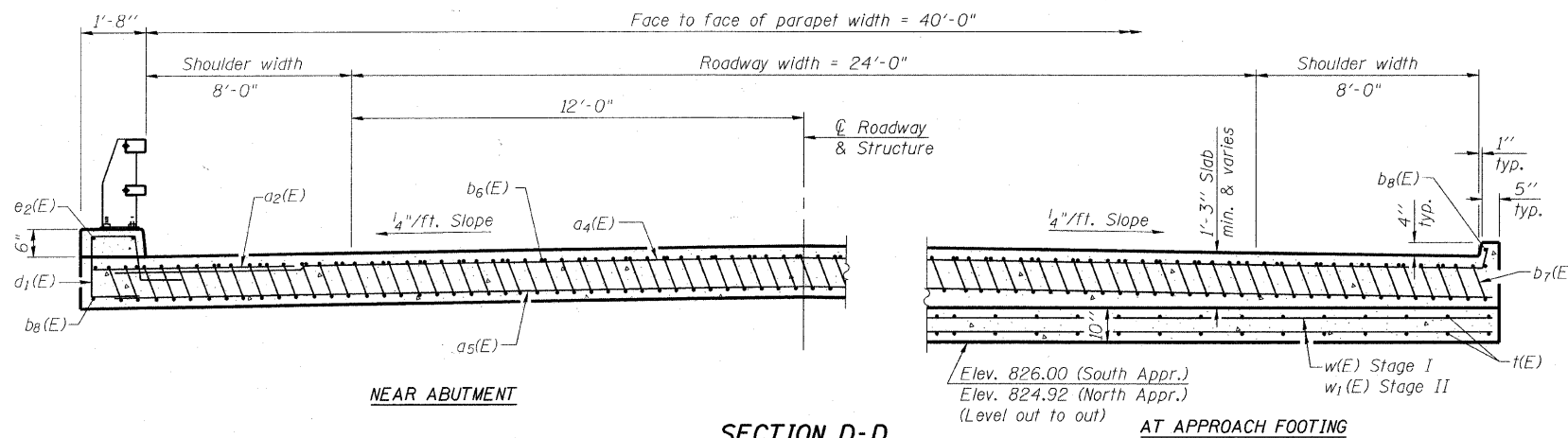
MCHENRY COUNTY
DIVISION OF TRANSPORTATION

Notes:

See sheet 11 of 23 for Detail A and View B-B.
Approach slab and curb concrete shall be paid for as Concrete Superstructure.
Approach footing concrete shall be paid for as Concrete Structures.
Reinforcement shall be paid for as Reinforcement Bars, Epoxy Coated.
For v(E) bar details, see sheet 10 of 23.
The approach footing maximum applied service bearing pressure (Qmax) = 2.0 ksf.
For bar splicer details, see sheet 21 of 23.
Cost of excavation for approach footing included with Concrete Structures.
For Porous Granular Embankment (Special) and drainage treatment details, see sheet 2 of 23.
For additional curb details, see sheet 9 of 23.



SECTION C-C



NEAR ABUTMENT

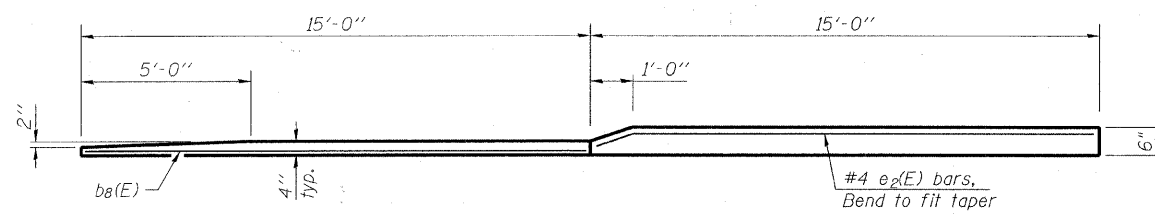
SECTION D-D

(See Plan for dimensions not shown)

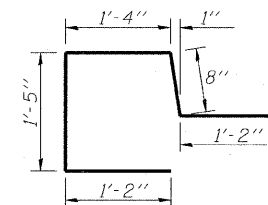
AT APPROACH FOOTING

* Tilt #9 b7(E) bars as required to maintain clearance.

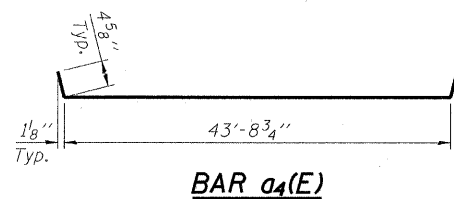
*** Cost included with Concrete Superstructure.



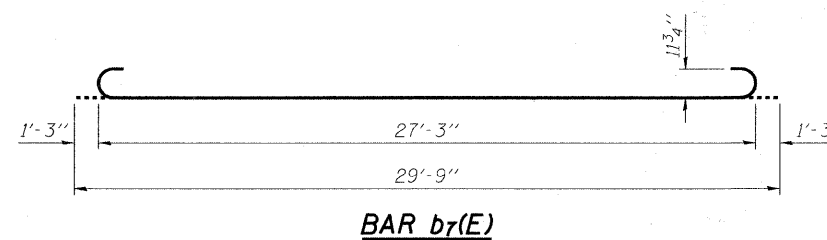
VIEW E-E



BAR d1(E)



BAR a4(E)



BAR b7(E)

TWO APPROACHES
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a2(E)	48	#6	6'-6"	—
a4(E)	50	#4	44'-6"	—
a5(E)	92	#5	44'-2"	—
b6(E)	66	#4	29'-8"	—
b7(E)	196	#9	29'-9"	—
b8(E)	8	#4	14'-8"	—
d1(E)	76	#5	5'-9"	□
e2(E)	8	#4	14'-8"	—
t(E)	168	#4	10'-6"	—
w(E)	80	#5	44'-2"	—
Concrete Superstructure			Cu. Yd.	127.7
Concrete Structures			Cu. Yd.	27.5
Reinforcement Bars, Epoxy Coated			Pound	32810
Bridge Deck Grooving			Sq. Yd.	245
Protective Coat			Sq. Yd.	287

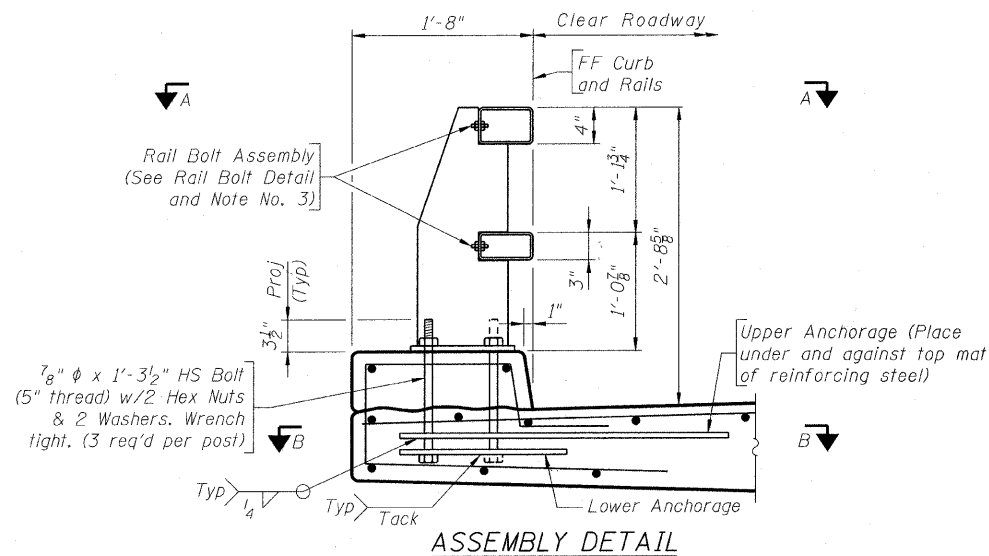
(Sheet 2 of 2)

BRIDGE APPROACH SLAB DETAILS
STRUCTURE NO. 056-3178

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 12 23 SHEETS	F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 37
	CONTRACT NO. 63536				
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

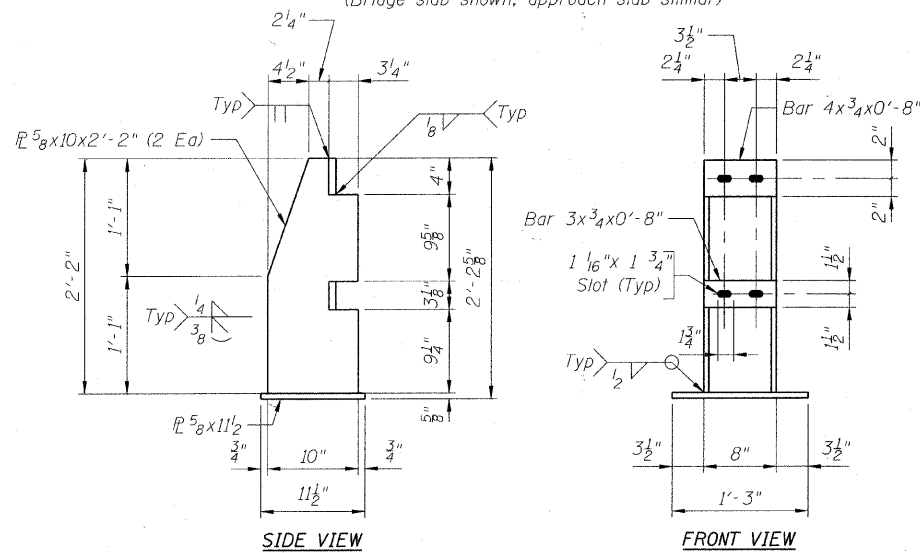
MCHENRY COUNTY
DIVISION OF TRANSPORTATION



ASSEMBLY DETAIL

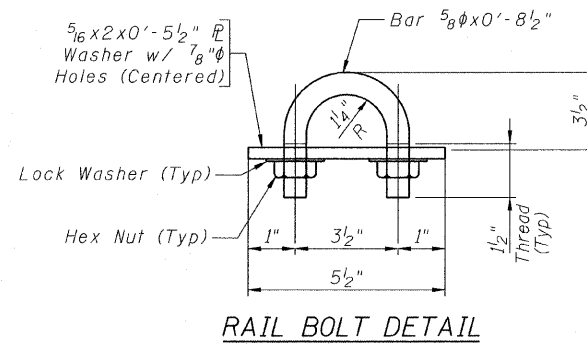
(Shown near Post)

(Bridge slab shown, approach slab similar)



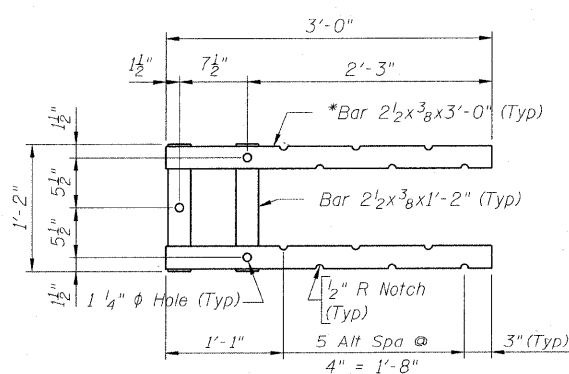
POST DETAILS

(See View A-A for anchor bolt hole spacing)



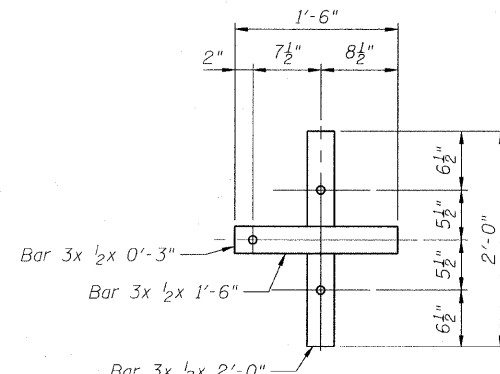
RAIL BOLT DETAIL

- Note:
- 1) Ensure the expansion splice is located in the railing panel which passes over the bridge expansion joint if indicated on the plan
 - 2) Anchor bolts may be tack welded to lower anchorage (Shop or field).
 - 3) At post locations, drill two 1 1/8" φ holes in the rails to receive rails bolts (Shop or field). See Post Details for hole spacing
 - 4) Before installing rails, paint all cut, drilled or otherwise damaged surface areas of the railing components with two coats of zinc rich paint conforming to the requirements of ASTM A 780
 - 5) After installing the rails, paint all exposed bolt threads with two coats of zinc rich paint conforming to the requirements of ASTM A 780.
 - 6) All steel components shall be galvanized according to AASHTO M III unless noted otherwise.
 - 7) Shim Plates shall be provided in accordance with Article 509.05(a) of the Standard Specifications.



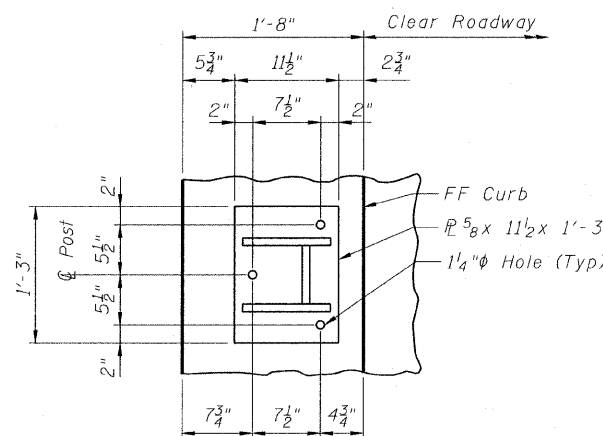
SECTION B-B

(Showing upper anchorage)
(Anchor Bolts and slab not shown)



SECTION B-B

(Showing lower anchorage)
(Anchor Bolts and slab not shown)



VIEW A-A

(anchor bolts, rails and rail bolts not shown)

BILL OF MATERIAL

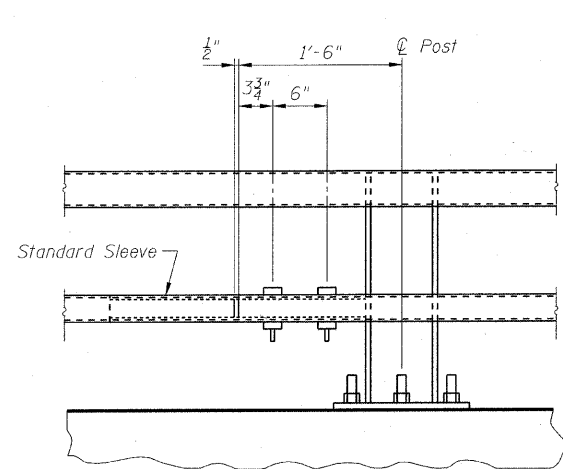
Item	Unit	Total
Steel Railing (Special)	Foot	359

**WYOMING 2-TUBE
BRIDGE RAILING DETAILS
STRUCTURE NO. 056-3178**

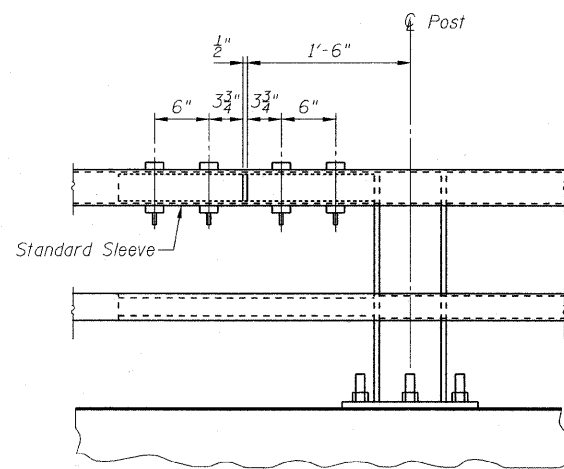
DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 13 23 SHEETS	F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 38
	CONTRACT NO. 63536				
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

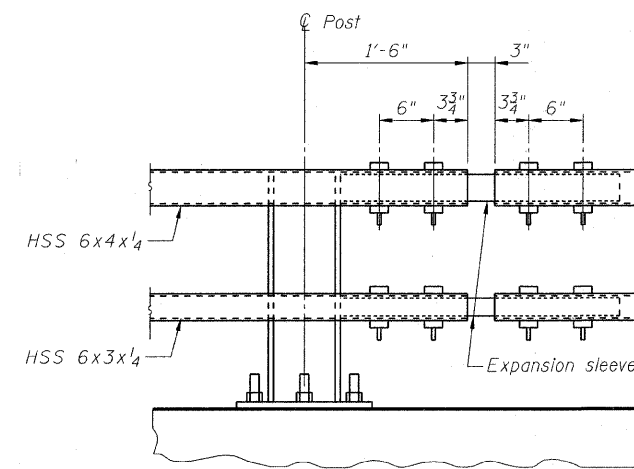
MCHENRY COUNTY
DIVISION OF TRANSPORTATION



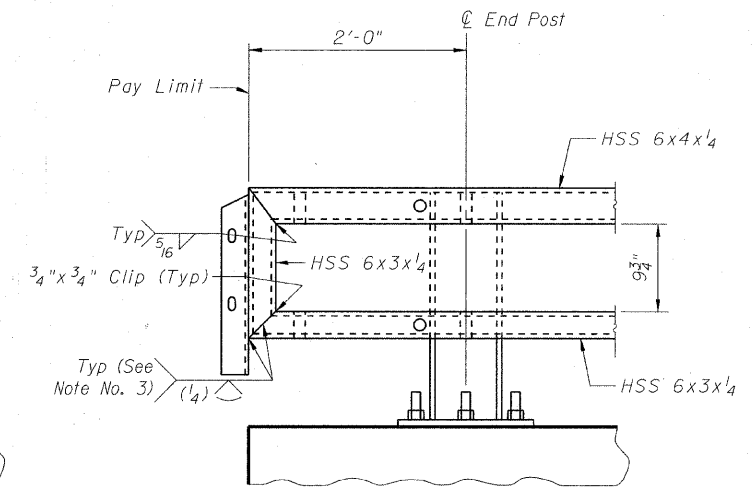
STANDARD SPLICE
(Top or bottom rail)



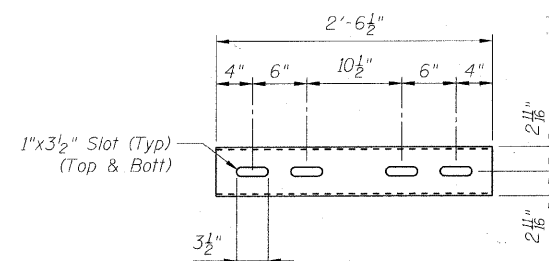
DOUBLE-BOLTED SPLICE
(Top or bottom rail)



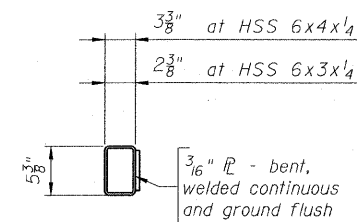
EXPANSION SPLICE
(Top and bottom rail)



ELEVATION AT TERMINAL TYPE 6A (SPECIAL)

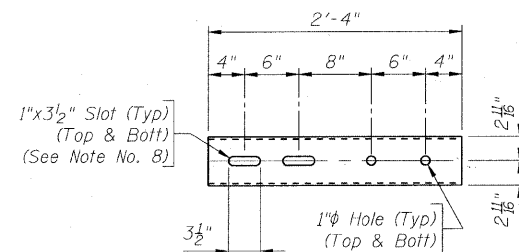


TOP VIEW

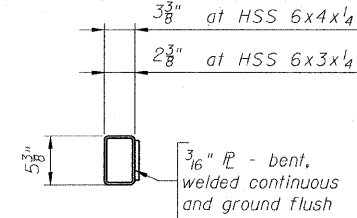


END VIEW

EXPANSION SLEEVE DETAILS

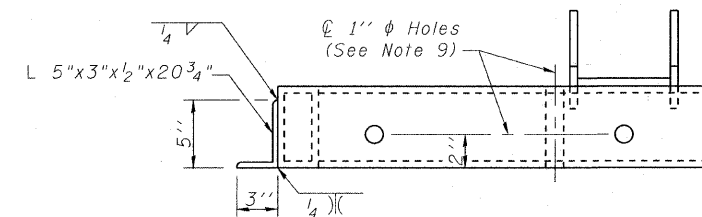


TOP VIEW

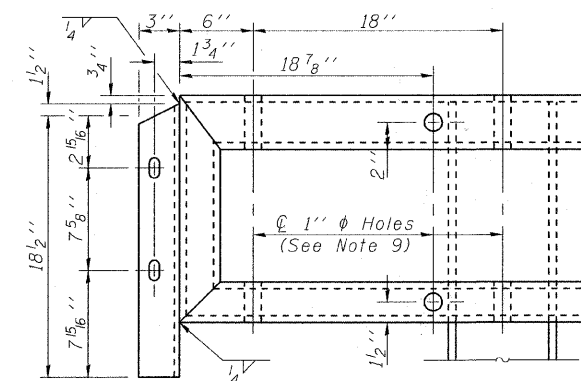


END VIEW

STANDARD SLEEVE DETAILS



TOP VIEW



FRONT VIEW

END OF RAIL

NOTES

- 1) Either top or bottom rail in terminal section may be the longer rail.
- 2) Ensure each rail length is continuous over a minimum of two posts. Railing that is part of a type 6A terminal is continuous if either the top or bottom rail in the terminal is continuous over a minimum of two posts.
- 3) Ensure the fabricator prepares a sample of the indicated joint and it is macroetched to demonstrate that the required effective throat is achieved.
- 4) In rehabilitation work, ensure railing that cannot feasibly be made continuous over a minimum of two posts has a double-bolted splice.
- 5) Splices may be located on either side of post.
- 6) Not more than one splice is permitted per side of post, except at expansion splices.
- 7) Do not shop splice rails.
- 8) Slots may be omitted in standard sleeves where bolts are required on one side of splice only.
- 9) Holes in Steel Railings may be field drilled for connection to Traffic Barrier Terminal. Any galvanized steel exposed or damaged by drilling operation shall be painted with two coats of a zinc-rich paint conforming to ASTM A 780.

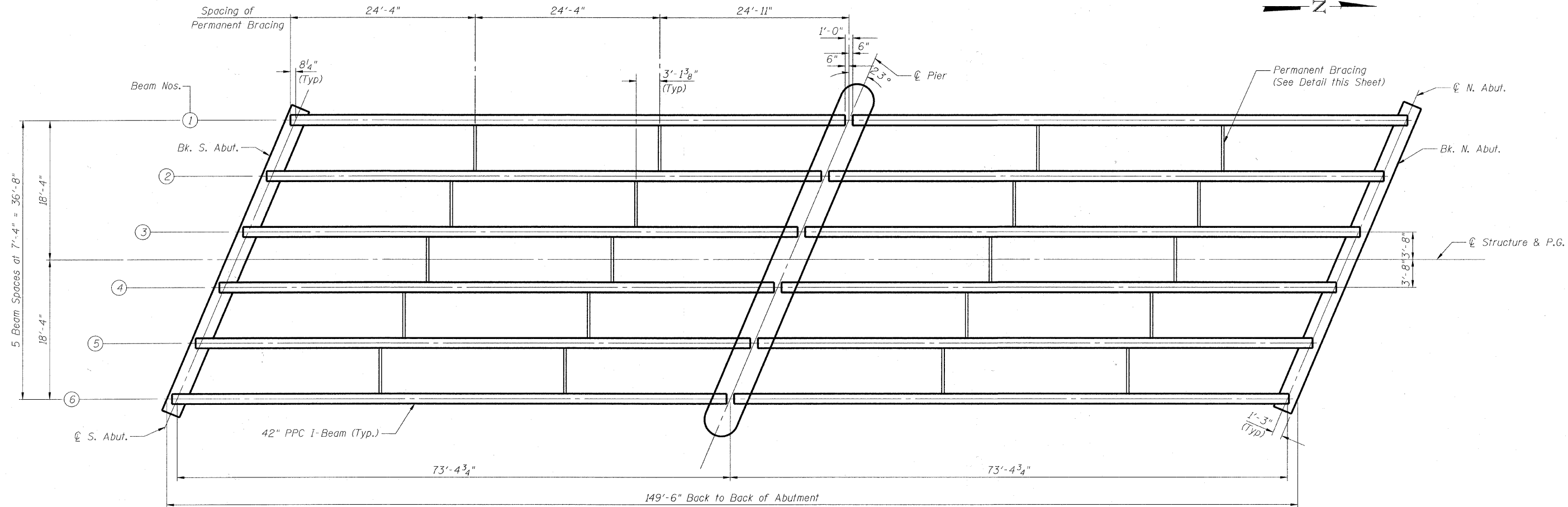
**WYOMING 2-TUBE
BRIDGE RAILING DETAILS
STRUCTURE NO. 056-3178**

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

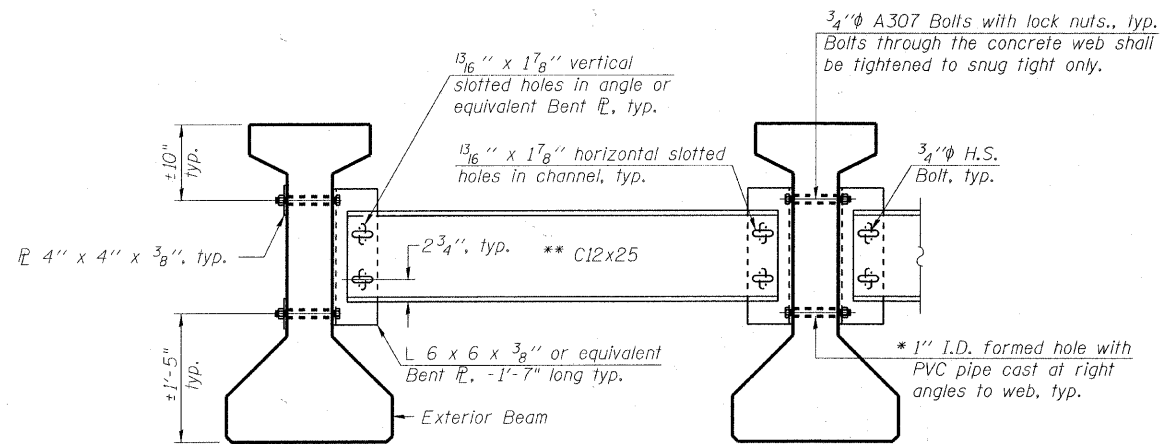
SHEET NO. 14 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	39
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT	CONTRACT NO. 63536 BRS-0034(107)	

COPYRIGHT © 2010 BY BAYLOR & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 5660 S. WOODLAND AVE., SUITE 100, CHICAGO, IL 60632
 TEL: (773) 440-1200 FAX: (773) 440-1201
 WWW.BAYLORANDWOODMAN.COM

MCHENRY COUNTY
DIVISION OF TRANSPORTATION



FRAMING PLAN



**PERMANENT BRACING DETAILS FOR
42" PPC I-BEAMS**

- * Fabricator shall locate to miss strands within permissible tolerances.
- ** Alternate C12x30 channels are permitted to facilitate material acquisition.

Notes:

All material for bracing shall be hot dip galvanized according to AASHTO M111 unless otherwise noted. Two hardened washers are required for each set of oversized holes. All holes shall be 15/16" unless otherwise noted. 5/16" x 3" x 3" plate washers are required over all slotted holes.

All bolts shall be galvanized according to AASHTO M232. Bracing shall be installed as beams are erected and tightened as soon as possible during erection. Permanent bracing shall not be paid for separately, but shall be included in the cost of Furnishing and Erecting Precast Prestressed Concrete I-Beams.

	INTERIOR BEAM MOMENT TABLE		
		0.4 Sp. 1	Pier
I	(in ⁴)	90956	90956
I'	(in ⁴)	287120	287120
S_b	(in ³)	5153	5153
S_b'	(in ³)	8878	8878
S_t	(in ³)	3736	3736
S_t'	(in ³)	29723	29723
DC1	(k/ft)	1.243	1.243
M_{DC1}	(k)	803.6	0
DC2	(k/ft)	0.075	0.075
M_{DC2}	(k)	28.5	50.1
DW	(k)	0.367	0.367
M_{DW}	(k)	139.3	244.5
M_{LL+IM}	(k)	968.2	978.3

	INTERIOR BEAM REACTION TABLE		
		Abut.	Pier
R_{DC1}	(k)	45.6	91.2
R_{DC2}	(k)	2.1	6.9
R_{DW}	(k)	10.1	33.5
R_{LL+IM}	(k)	61.3	113.6
R_{Total}	(k)	119.1	245.2

* The total R_{DC2} , R_{DW} , R_{LL+IM} , and impact reactions are assumed to be distributed evenly to each bearing line at a pier regardless of the span ratios. The bearing design at a pier is based on the maximum reactions of either span.

- I : Non-composite moment of inertia of beam section (in⁴).
- I' : Composite moment of inertia of beam section (in⁴).
- S_b : Non-composite section modulus for the bottom fiber of the prestressed beam (in³).
- S_b' : Composite section modulus for the bottom fiber of the prestressed beam (in³).
- S_t : Non-composite section modulus for the top fiber of the prestressed beam (in³).
- S_t' : Composite section modulus for the top fiber of the prestressed beam (in³).
- DC1: Un-factored non-composite dead load (kips/ft.).
- M_{DC1} : Un-factored moment due to non-composite dead load (kip-ft.).
- DC2: Un-factored long-term composite (superimposed excluding future wearing surface) dead load (kips/ft.).
- M_{DC2} : Un-factored moment due to long-term composite (superimposed excluding future wearing surface) dead load (kip-ft.).
- DW: Un-factored long-term composite (superimposed future wearing surface only) dead load (kips/ft.).
- M_{DW} : Un-factored moment due to long-term composite (superimposed future wearing surface only) dead load (kip-ft.).
- M_{LL+IM} : Un-factored live load moment plus dynamic load allowance (impact) (kip-ft.).

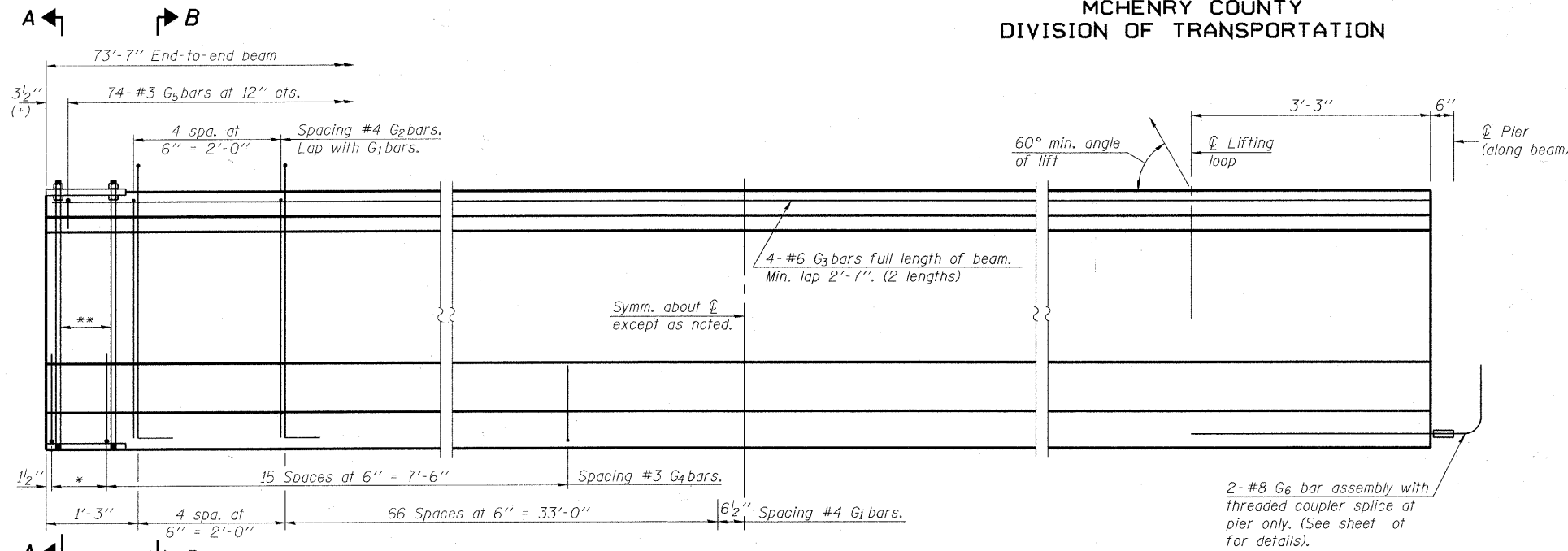
**FRAMING PLAN
STRUCTURE NO. 056-3178**

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 15 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	40
CONTRACT NO. 63536					
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
No. 12/28/2010
12/28/2010

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

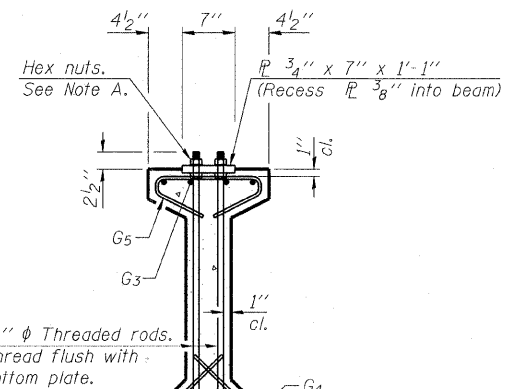


ELEVATION OF BEAM
(Showing reinforcement & dimensions)

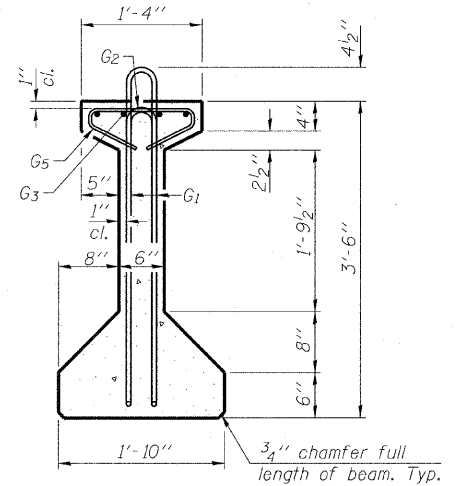
*3 spaces at 3" = 9".
**4-3/4" φ threaded dowel rods at 3" cts., Each Face.

2-#8 G6 bar assembly with threaded coupler splice at pier only. (See sheet of for details).

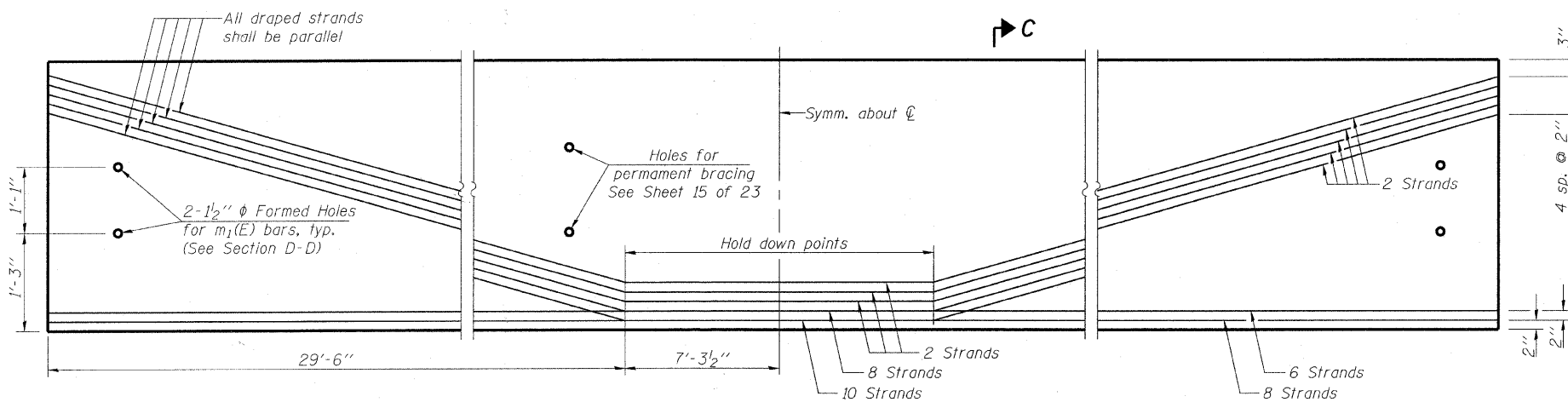
Note A:
Hex nuts (top and bottom) with lock washers (top). Only tighten sufficiently to compress lock washers.



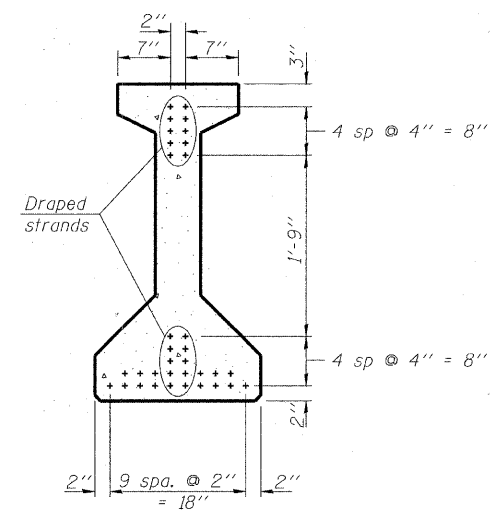
SECTION A-A



SECTION B-B



ELEVATION OF BEAM
(Showing prestressing steel)



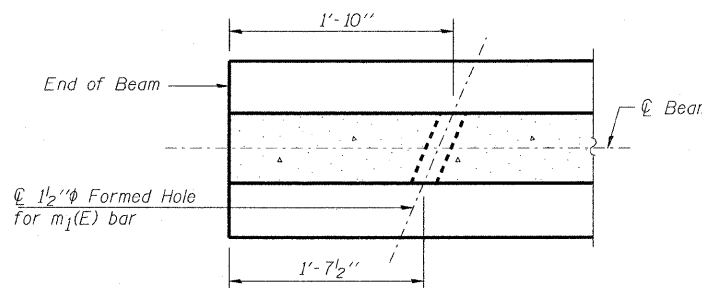
SECTION C-C

*****BAR LIST
ONE BEAM ONLY**

Bar	No.	Size	Length	Shape
G1	143	#4	8'-5"	∩
G2	10	#4	6'-8"	∩
G3	8	#6	38'-1"	∩
G4	38	#3	4'-11"	∩
G5	74	#3	2'-6"	∩
G6	2	#8	6'-6"	∩

***For information only

Notes:
See sheet 17 of 23 for additional details and Bill of Material.
Required release strength, f'ci, shall be 6,000 psi.



SECTION D-D

Other end similar by rotation

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

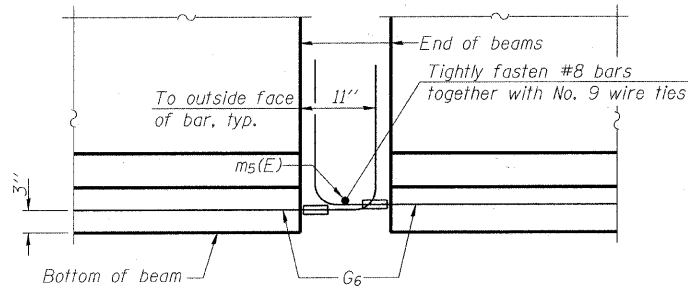
**42" PPC I-BEAM
STRUCTURE NO. 056-3178**

SHEET NO. 16 23 SHEETS	F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 41
	CONTRACT NO. 63536				
JOB NO. C-191-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

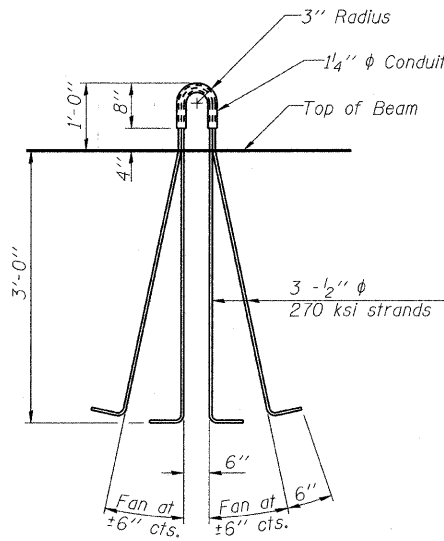
MCHENRY COUNTY
DIVISION OF TRANSPORTATION

NOTES

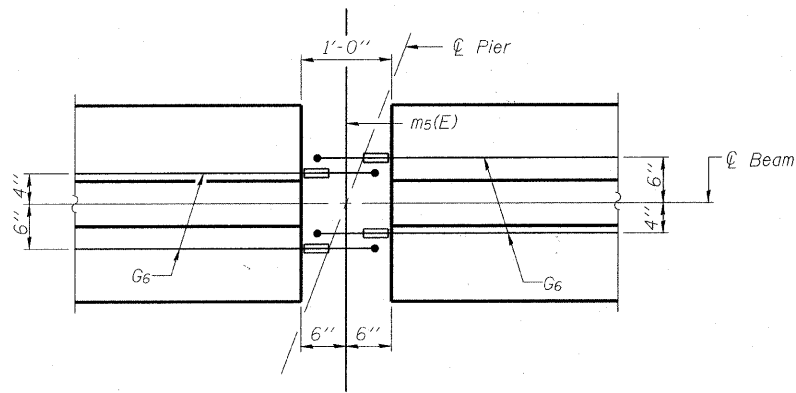
Inserts for $\frac{3}{4}$ " ϕ threaded dowel rods, when specified, are to be two strut, ferrule type for interior beams and single ferrule, flared loop type for exterior beams. Prestressing steel shall be uncoated high strength, low relaxation 7-wire strand, Grade 270. The nominal diameter shall be $\frac{1}{2}$ " and the nominal cross-sectional area shall be 0.153 sq. in. Reinforcement bars shall conform to ASTM A 706, Grade 60. (See Special Provisions). A minimum $2\frac{1}{2}$ " ϕ lifting pin shall be used to engage the lifting loops during handling. Till G_6 bars when necessary to maintain $1\frac{1}{2}$ " clearance. The top and bottom plates shall be AASHTO M270 Grade 50. The bottom plates and studs shall be galvanized according to AASHTO M111. Top plates and threaded rods need not be galvanized. Threaded rods shall be ASTM F 1554 Grade 55. The G_6 bar assembly shall have the threaded ends oversized to ensure no reduction in cross sectional area after threading. The coupler splice shall be capable of developing 125 percent of the yield strength of the reinforcement bar.



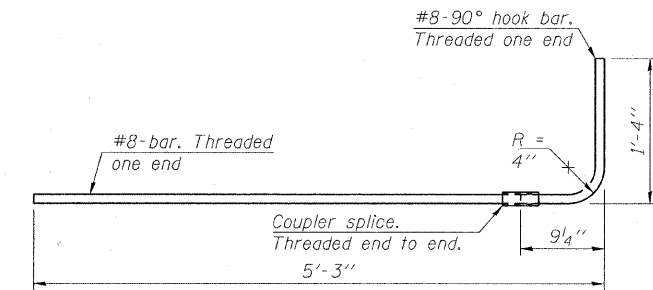
ELEVATION OF BEAM AT PIER



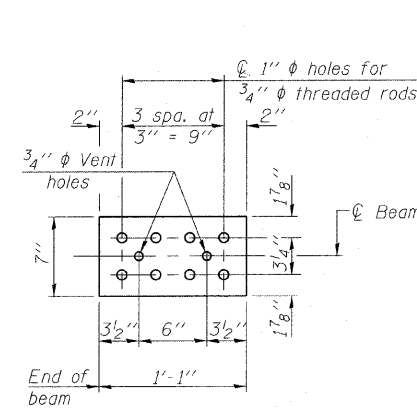
LIFTING LOOP DETAIL



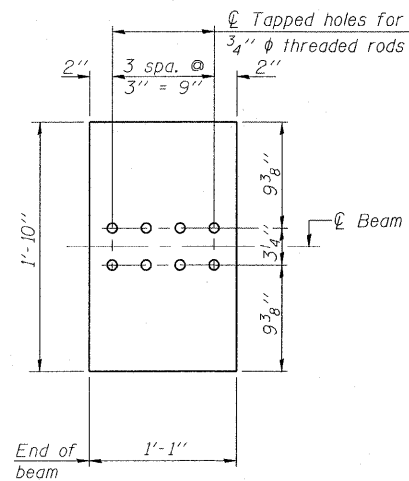
PLAN OF BEAM AT PIER



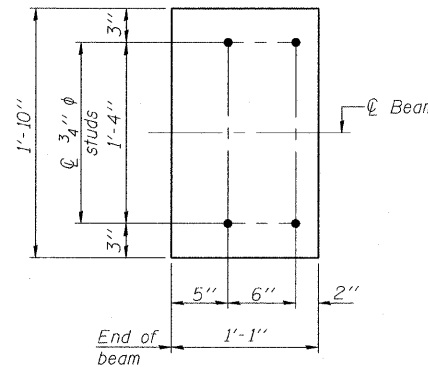
G6 BAR ASSEMBLY



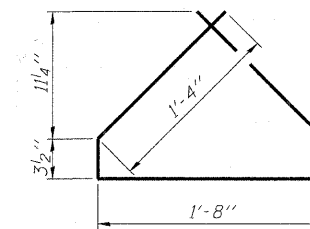
TOP PLATE



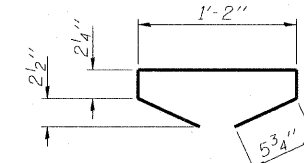
BOTTOM PLATE
(Showing threaded rods)



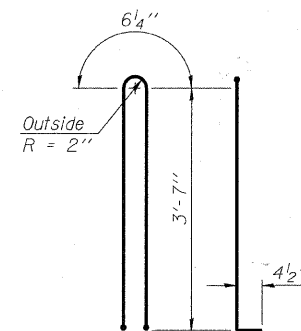
BOTTOM PLATE
(Showing studs)



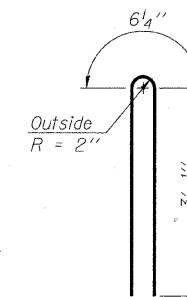
BAR G4



BAR G5



BAR G1



BAR G2

BILL OF MATERIAL

Item	Unit	Total
Furnishing and Erecting Precast Prestressed Concrete I-Beams, 42"	Foot	883

42" PPC I-BEAM DETAILS
STRUCTURE NO. 056-3178

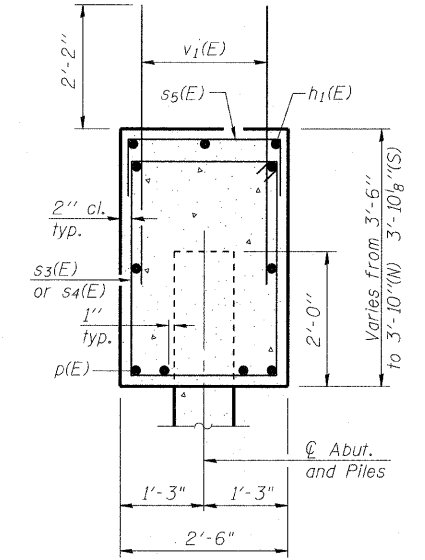
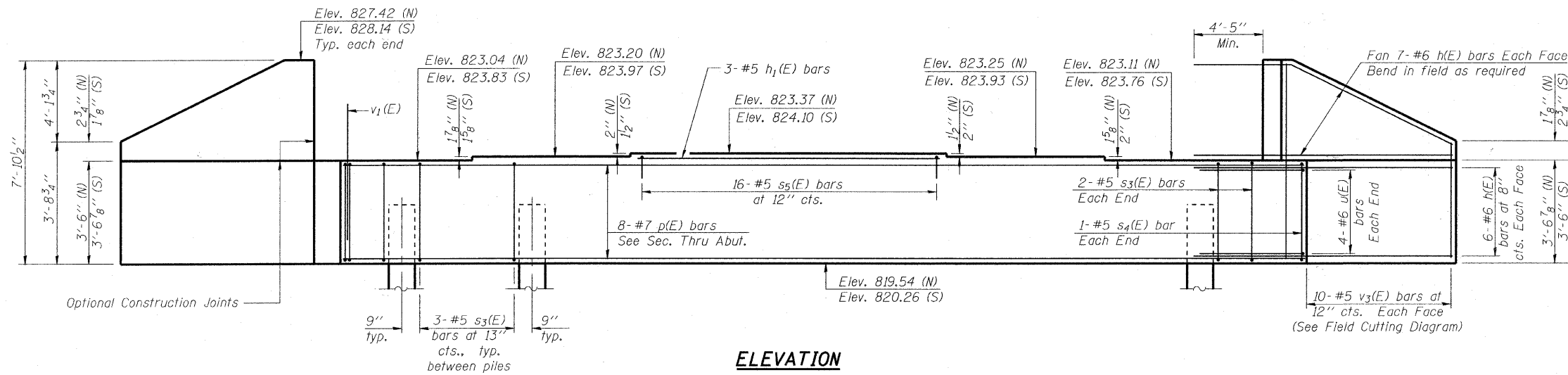
DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

See bearing details for pintle hole locations when required.

SHEET NO. 17 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	42
			CONTRACT NO. 63536		
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

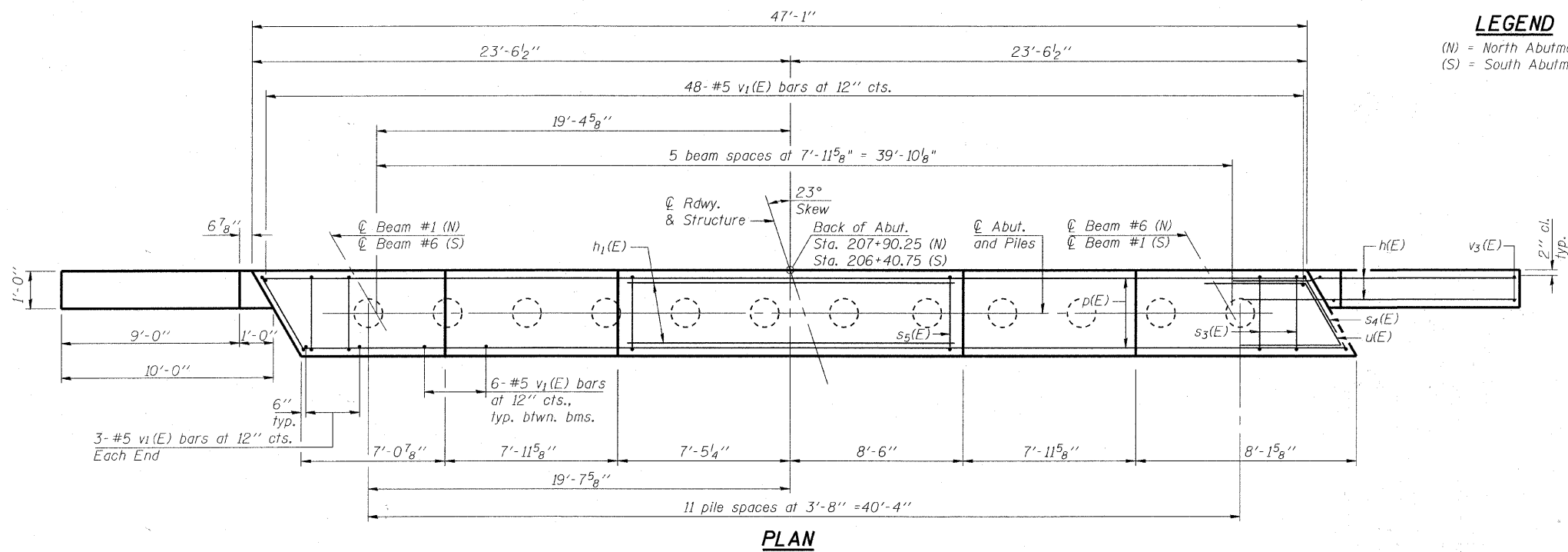
Notes:
Pour steps monolithically with cap.



LEGEND
(N) = North Abutment
(S) = South Abutment

**BILL OF MATERIAL
TWO ABUTMENTS**

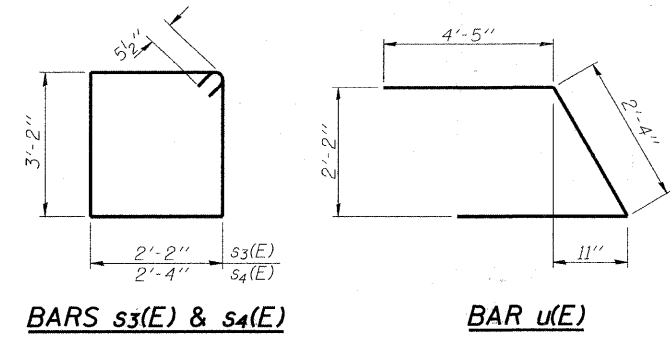
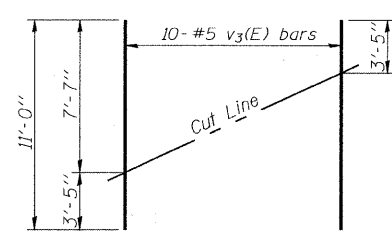
Bar	No.	Size	Length	Shape
h(E)	104	#6	15'-3"	—
h ₁ (E)	6	#5	15'-8"	—
p(E)	16	#7	46'-9"	—
s ₃ (E)	74	#5	11'-7"	□
s ₄ (E)	4	#5	11'-11"	□
s ₅ (E)	32	#5	4'-2"	□
u(E)	16	#6	11'-2"	∟
v ₁ (E)	168	#5	4'-4"	—
v ₃ (E)	40	#5	11'-0"	—
Structure Excavation			Cu. Yd.	350
Concrete Structures			Cu. Yd.	40.6
Reinforcement Bars, Epoxy Coated			Pound	6580
Furnishing Metal Shell Piles 14" x 0.312"			Foot	836
Driving Piles			Foot	836
Test Pile Metal Shells			Each	2
Pile Shoes			Each	24



PILE DATA

NORTH ABUTMENT
 Type: 14" ϕ Metal Shells w/ 0.312" walls with Pile Shoes
 Nominal Required Bearing: 260 Kips
 Factored Resistance Available: 130 Kips
 Est. Length: 40'
 No. Production Piles: 11
 No. Test Piles: 1

SOUTH ABUTMENT
 Type: 14" ϕ Metal Shells w/ 0.312" walls with Pile Shoes
 Nominal Required Bearing: 260 Kips
 Factored Resistance Available: 130 Kips
 Est. Length: 36'
 No. Production Piles: 11
 No. Test Piles: 1



DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

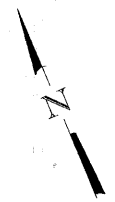
SHEET NO. 18 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	43
CONTRACT NO. 63536					
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

**ABUTMENTS
STRUCTURE NO. 056-3178**

For details of Bar Splicers, see sheet 21 of 23.
For details of piles see sheet 20 of 23.

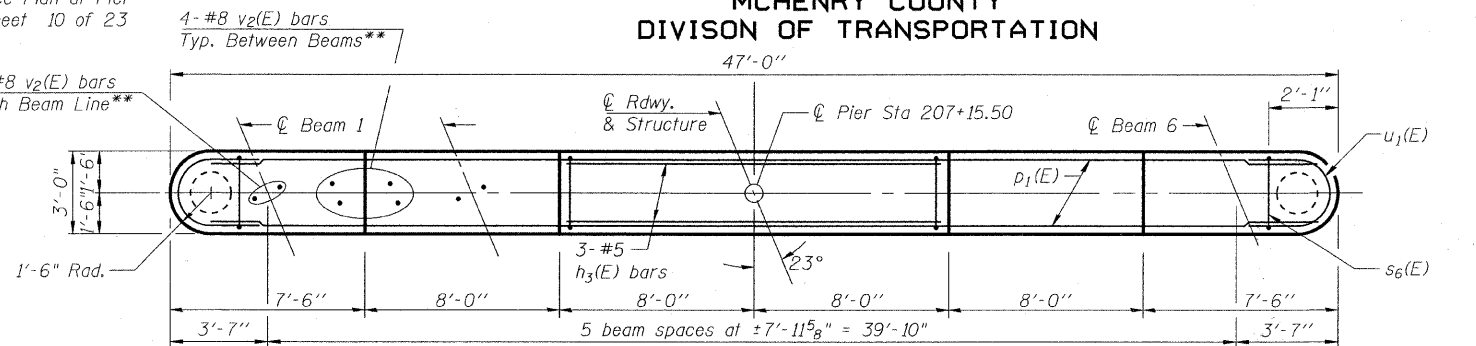
COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-000002-0000-0000
 PROJECT NO. 06-00320-02-BR
 DATE: 06/20/10

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

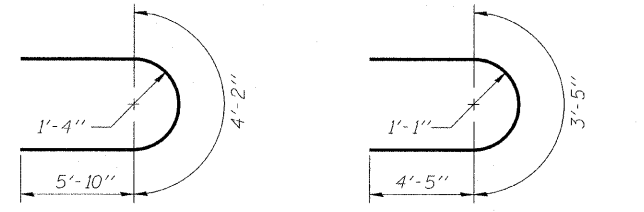


Notes:
Space reinforcement in cap to miss anchor bolts.
Pour steps monolithically with cap.
For details of piles, see sheet 20 of 23.
If a portion of the pier wall or concrete encasement is under water, reinforcement may be placed underwater into forms. Concrete shall be tremied according to Article 503.08 of the Standard Specifications to an elevation of 1'-0" above the water line at the time of construction.

** See Plan at Pier Sheet 10 of 23

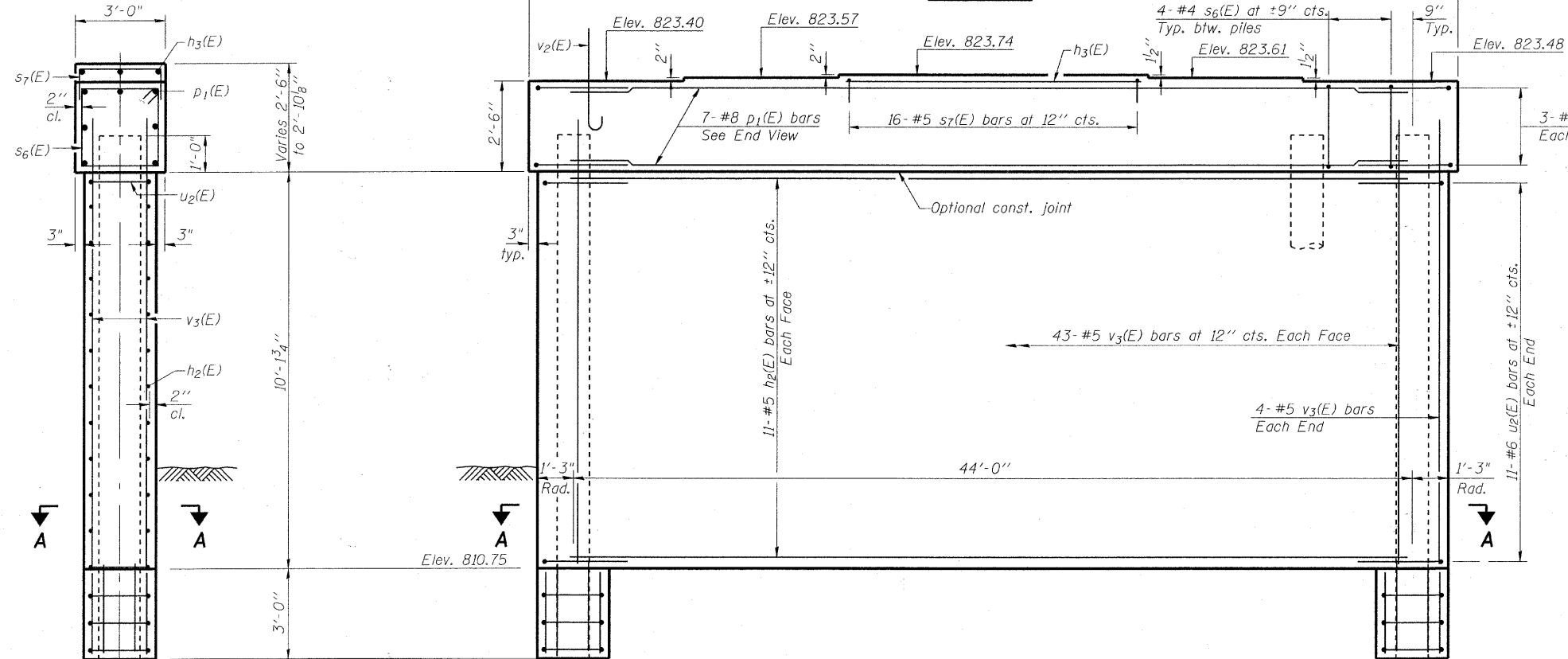


TOP PLAN



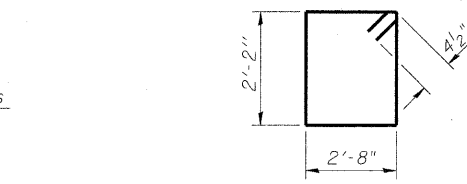
BAR u1(E)

BAR u2(E)

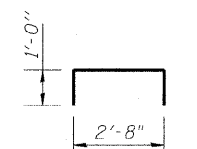


ELEVATION

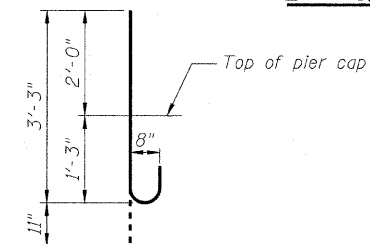
(Looking perpendicular to pier from south)



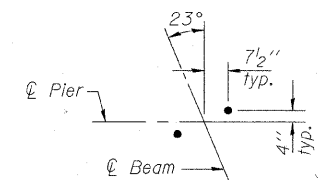
BAR s6(E)



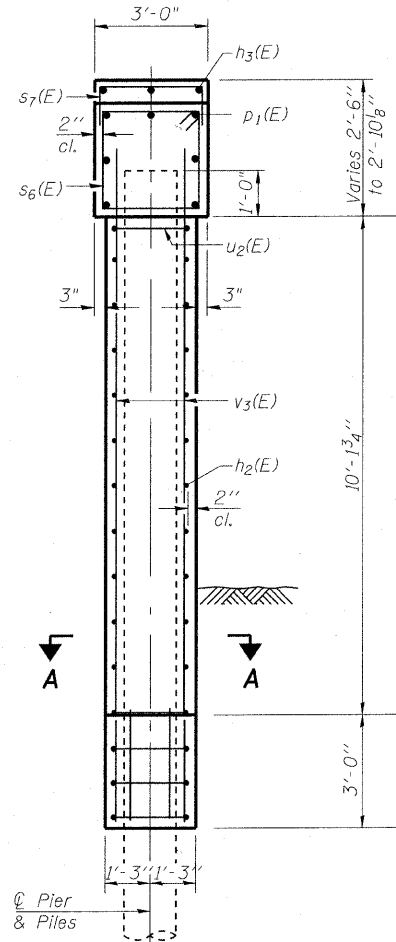
BAR s7(E)



BAR v2(E)



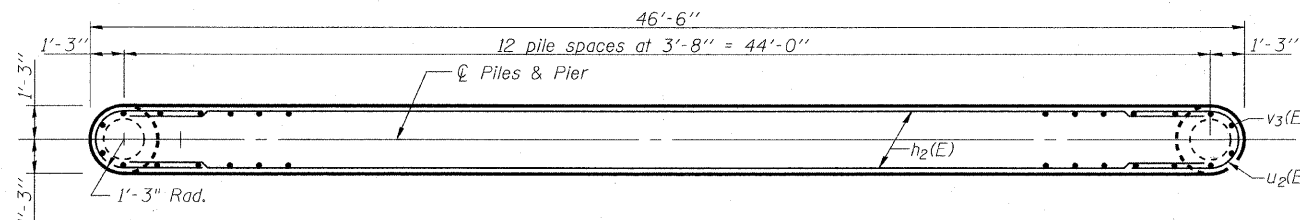
BAR v2(E) LAYOUT



END VIEW

PILE DATA

Type: 14" ϕ Metal Shell w/ 0.312" walls with Pile Shoes
Nominal Required Bearing: 384 Kips
Factored Resistance Available: 192 Kips
Est. Length: 61'
No. Production Piles: 13
No. Test Piles: 0



SECTION A-A

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h2(E)	22	#5	44'-0"	—
h3(E)	3	#5	15'-8"	—
p1(E)	7	#8	44'-0"	—
s6(E)	48	#4	10'-5"	□
s7(E)	16	#5	4'-8"	┌
u1(E)	6	#7	15'-10"	U
u2(E)	22	#6	12'-3"	U
v2(E)	32	#8	4'-2"	U
v3(E)	94	#5	11'-3"	—
Structure Excavation			Cu. Yd.	52
Concrete Structures			Cu. Yd.	57.1
Reinforcement Bars, Epoxy Coated			Pound	4360
Furnishing Metal Shell Piles 14" x 0.312"			Foot	793
Driving Piles			Foot	793
Underwater Structure Excavation Protection, Location 1			Each	1
Concrete Encasement			Cu. Yd.	7.1
Pile Shoes			Each	13

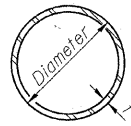
PIER
STRUCTURE NO. 056-3178

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 19 23 SHEETS	F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 44
	CONTRACT NO. 63536			JOB NO. C-191-140-11 ILLINOIS FED. AID PROJECT BRS-00341017	

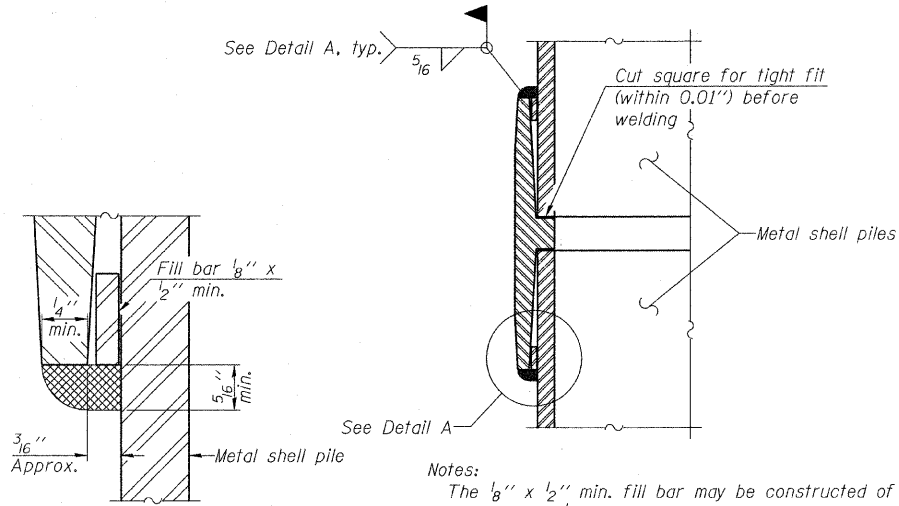
COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 5660 S. W. 111th Ave., Suite 100, Lincoln, IL 62550
 618-292-2000 FAX 618-292-2001
 www.baxterandwoodman.com

MCHENRY COUNTY
DIVISION OF TRANSPORTATION



METAL SHELL PILE TABLE

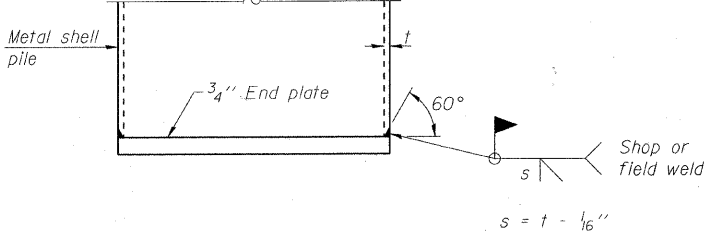
Designation and outside diameter	Wall thickness <i>t</i>	Weight per foot (Lbs./ft.)	Inside volume (yd. ³ /ft.)
PP12	0.179"	22.60	0.0274
PP12	0.250"	31.37	0.0267
PP14	0.250"	36.71	0.0368
PP14	0.312"	45.61	0.0361



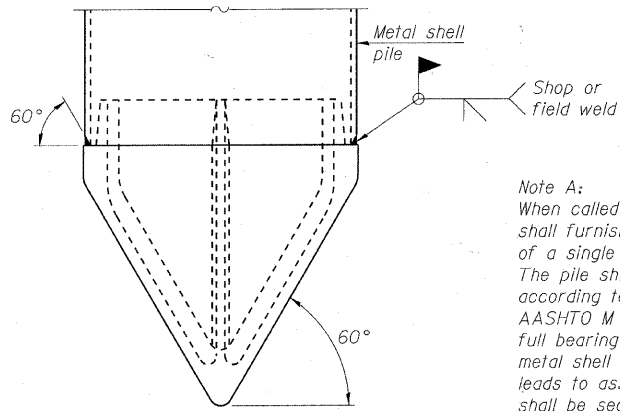
DETAIL A

WELDED COMMERCIAL SPLICE

Notes:
The $\frac{1}{8}''$ x $\frac{1}{2}''$ min. fill bar may be constructed of 2 bars with a $\frac{1}{8}''$ max. gap between them.
Pile segments shall be driven to solid contact with splicer before welding.



END PLATE ATTACHMENT

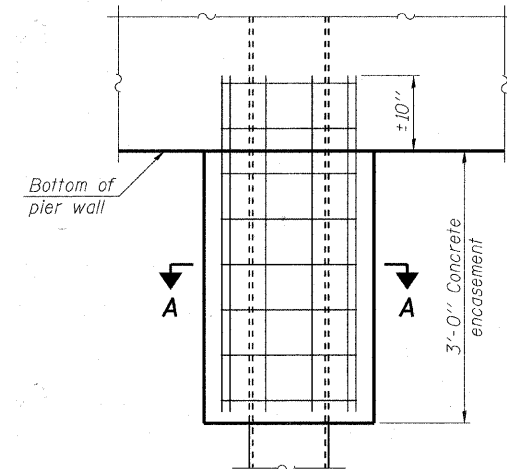


Note A:
When called for on the plans, the Contractor shall furnish metal shell pile shoes consisting of a single piece conical pile point as shown. The pile shoes shall be cast in one piece steel according to either ASTM A 148 Grade 90-60 or AASHTO M 103 Grade 65-35 and shall provide full bearing over the full circumference of the metal shell pile. The pile shoe shall have tapered leads to assure proper alignment and fitting and shall be secured to the pile with a circumferential weld.

METAL SHELL PILE SHOE ATTACHMENT

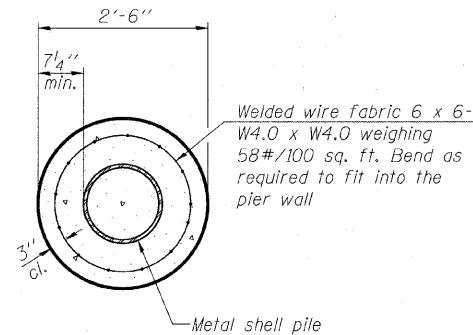
(See Note A)

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB



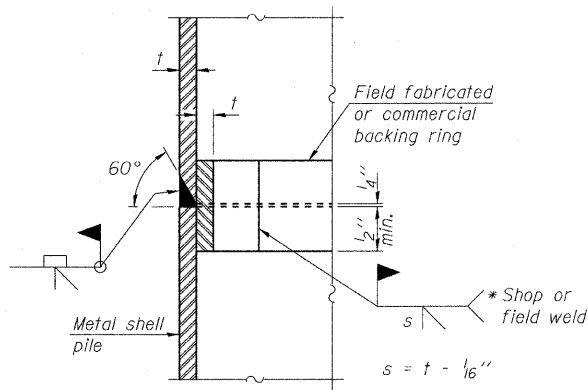
ELEVATION

CONCRETE ENCASEMENT AT PIERS



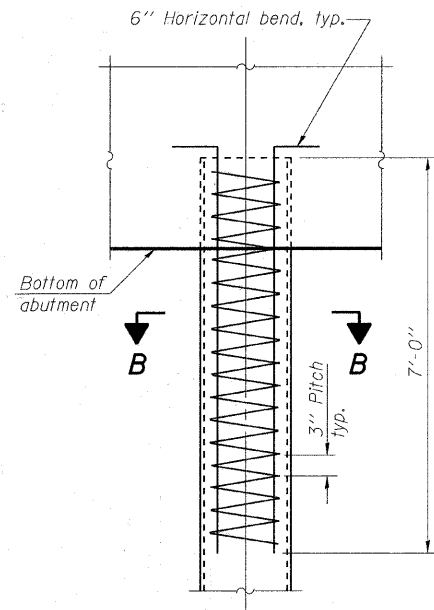
SECTION A-A

Note:
Forms for encasement may be omitted when soil conditions permit.



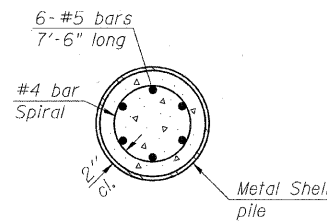
COMPLETE PENETRATION WELD SPLICE

* Field fabricated backing ring may be made from pile shell by removing segment to allow reducing circumference and vertically rejoin with partial joint penetration weld.



ELEVATION

METAL SHELL REINFORCEMENT AT ABUTMENTS



SECTION B-B

Welded Wire Fabric included with the cost of Concrete Encasement.
Metal Shell Reinforcement at Abutments included with Furnishing Metal Shell Piles.

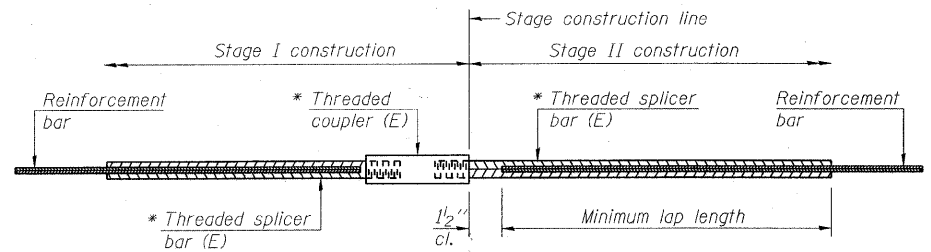
Note:
The metal shell piles shall be according to ASTM A 252 Grade 3.

METAL SHELL PILE DETAILS
STRUCTURE NO. 056-3178

SHEET NO. 20 23 SHEETS	F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 45
	CONTRACT NO. 63536				
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT BRS-0034(107)		

COPYRIGHT © 2000 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 1001 S. WOODSTOCK AVENUE, SUITE 100
 DEERFIELD, ILLINOIS 60015-5500

MCHENRY COUNTY
DIVISION OF TRANSPORTATION



STANDARD BAR SPLICER ASSEMBLY

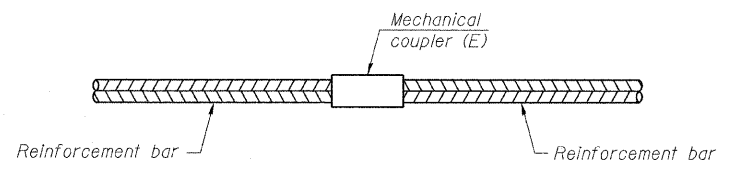
Bar size to be spliced	Minimum Lap Lengths			
	Table 1	Table 2	Table 3	Table 4
3, 4	1'-5"	1'-11"	2'-1"	2'-4"
5	1'-9"	2'-5"	2'-7"	2'-11"
6	2'-1"	2'-11"	3'-1"	3'-6"
7	2'-9"	3'-10"	4'-2"	4'-8"
8	3'-8"	5'-1"	5'-5"	6'-2"
9	4'-7"	6'-5"	6'-10"	7'-9"

Table 1: Black bar, 0.8 Class C
Table 2: Black bar, Top bar lap, 0.8 Class C
Table 3: Epoxy bar, 0.8 Class C
Table 4: Epoxy bar, Top bar lap, 0.8 Class C

Threaded splicer bar length = min. lap length + 1/2" + thread length

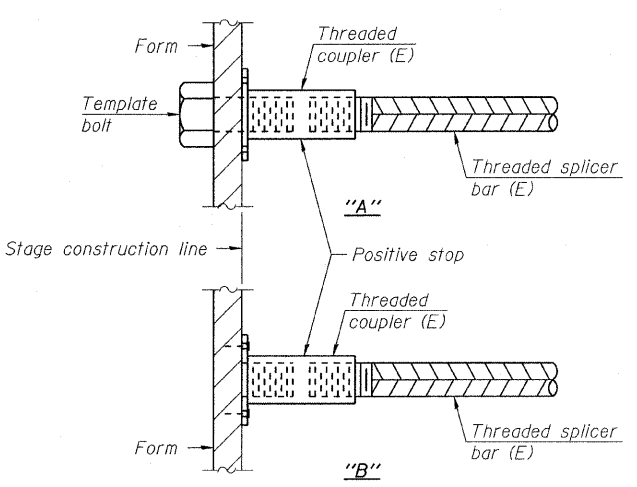
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Table for minimum lap length



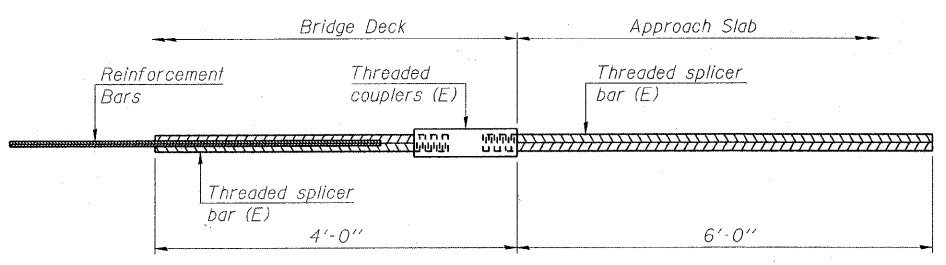
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



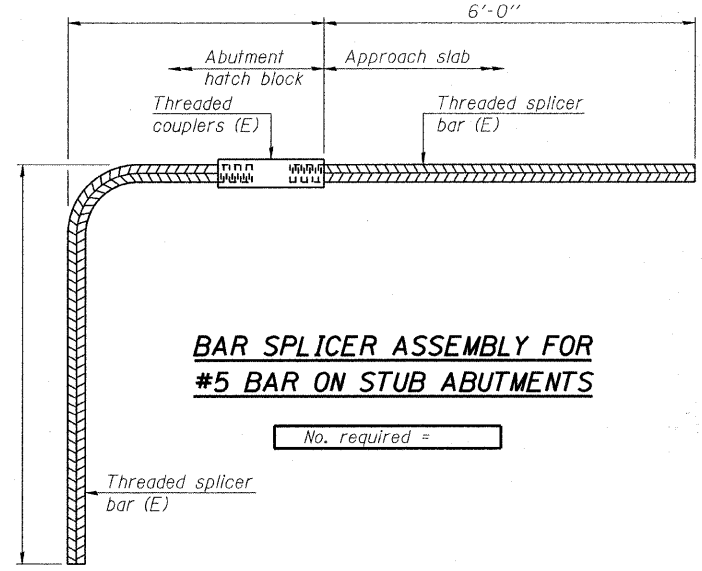
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required = 88



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

**BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 056-3178**

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 21 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	46
CONTRACT NO. 63536					
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

COPYRIGHT © 2006 BY BAXTER & WOODMAN, INC. ... STATE OF ILLINOIS - PROFESSIONAL ENGINEER'S FIRM ...

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

SOIL AND MATERIAL CONSULTANTS, INC. File No. 19458 **BORING LOG 2**
 Client **Baxter & Woodman, Inc.** Sheet **1** of **4**
 Comments _____ Project **Union Rd. over south branch of** Date **10/9/08**
Kishwaukee River Structural #056-0035
 Location **McHenry County, IL** Drilled By **AC**
 Equipment CME 45B H.A. Other Logged By **DA**

Elev., ft.	826.1	Description	Depth, ft.	1.3	S	T	R	B	N	Pen.	W	Uw	Qu
		Dark brown sand & silt, trace clay, gravel & organic matter, damp-very damp, very loose to loose - Fill											
			1		SS		13"	1	2				19.0
			5		SS		8"	8	7				17.0
			3		SS		15"	3	5				73.9
		Brown fine-medium sand, some coarse sand & gravel, very damp-saturated, medium dense	10		SS		12"	10	17				8.6
		Gray fine sand, trace medium-coarse sand & gravel, very damp-saturated, medium dense	5		SS		15"	6	11				15.6
			15		SS		12"	5	10				17.2
			7		SS		15"	6	11				23.2
		Gray fine sand, trace medium-coarse sand & gravel, very damp-saturated, loose	20		SS		11"	3	7				14.2

Water Level— depth, ft. elev., ft. S - sample T - type: J (jar), SS (split-spoon), ST (shallow tube) R - recovery length, in.
 - while drilling: 8.5 B - Standard Penetration Test (SPT), blows/6" interval. W - water content, %
 - after drilling: 8.5 N - SPT, blows/foot to drive 2" O.D. split-spoon sampler with 140 lb. hammer falling 30".
 - hrs. after drilling: Pen. - pocket penetrometer reading, tons/sq. ft. Uw - dry unit weight of soil, lbs./cu.ft.
 Qu - unconfined compressive strength, tons/sq. ft.

F-111b

SOIL AND MATERIAL CONSULTANTS, INC. File No. 19458 **BORING LOG 2**
 Client **Baxter & Woodman, Inc.** Sheet **2** of **4**
 Comments _____ Project **Union Rd. over south branch of** Date **10/9/08**
Kishwaukee River Structural #056-0035
 Location **McHenry County, IL** Drilled By **AC**
 Equipment CME 45B H.A. Other Logged By **DA**

Elev., ft.	826.1	Description	Depth, ft.	20	S	T	R	B	N	Pen.	W	Uw	Qu
		Gray fine sand, trace medium-coarse sand & gravel, very damp-saturated, loose to medium dense	9		SS		13"	4	8				13.8
			26		SS		14"	8	13				13.1
			11		SS		18"	9	17				12.2
			30		SS		18"	10	18				12.4
		Gray fine sand, very damp-saturated, medium dense	13		SS		10"	10	19				17.1
		Gray fine sand, trace medium-coarse sand & gravel, very damp-saturated, medium dense	35		SS		18"	11	19				9.1
			18		SS		18"	10	18				10.5
			40		SS		18"	8	15				14.1

Water Level— depth, ft. elev., ft. S - sample T - type: J (jar), SS (split-spoon), ST (shallow tube) R - recovery length, in.
 - while drilling: B - Standard Penetration Test (SPT), blows/6" interval. W - water content, %
 - after drilling: N - SPT, blows/foot to drive 2" O.D. split-spoon sampler with 140 lb. hammer falling 30".
 - hrs. after drilling: Pen. - pocket penetrometer reading, tons/sq. ft. Uw - dry unit weight of soil, lbs./cu.ft.
 Qu - unconfined compressive strength, tons/sq. ft.

F-111b

SOIL AND MATERIAL CONSULTANTS, INC. File No. 19458 **BORING LOG 2**
 Client **Baxter & Woodman, Inc.** Sheet **3** of **4**
 Comments _____ Project **Union Rd. over south branch of** Date **10/9/08**
Kishwaukee River Structural #056-0035
 Location **McHenry County, IL** Drilled By **AC**
 Equipment CME 45B H.A. Other Logged By **DA**

Elev., ft.	826.1	Description	Depth, ft.	40	S	T	R	B	N	Pen.	W	Uw	Qu
		Gray fine sand, trace medium-coarse sand & gravel, very damp-saturated, medium dense	17		SS		18"	9	17				11.9
			46		SS		18"	9	16				10.2
		Gray fine-medium sand, some coarse sand & gravel, very damp-saturated, medium dense to dense	19		SS		18"	10	19				9.4
			50		SS		16"	7	14				12.9
			55		SS		18"	10	31				5.9
		Gray fine-medium sand, some coarse sand & gravel, very damp-saturated, medium dense	60		SS		18"	13	28				10.7

Water Level— depth, ft. elev., ft. S - sample T - type: J (jar), SS (split-spoon), ST (shallow tube) R - recovery length, in.
 - while drilling: B - Standard Penetration Test (SPT), blows/6" interval. W - water content, %
 - after drilling: N - SPT, blows/foot to drive 2" O.D. split-spoon sampler with 140 lb. hammer falling 30".
 - hrs. after drilling: Pen. - pocket penetrometer reading, tons/sq. ft. Uw - dry unit weight of soil, lbs./cu.ft.
 Qu - unconfined compressive strength, tons/sq. ft.

F-111b

SOIL AND MATERIAL CONSULTANTS, INC. File No. 19458 **BORING LOG 2**
 Client **Baxter & Woodman, Inc.** Sheet **4** of **4**
 Comments _____ Project **Union Rd. over south branch of** Date **10/9/08**
Kishwaukee River Structural #056-0035
 Location **McHenry County, IL** Drilled By **AC**
 Equipment CME 45B H.A. Other Logged By **DA**

Elev., ft.	826.1	Description	Depth, ft.	60	S	T	R	B	N	Pen.	W	Uw	Qu
		Gray fine-medium sand, some coarse sand & gravel, very damp-saturated, dense	65		SS		18"	21	36				6.7
			70		SS		18"	18	32				6.6
		Gray medium-coarse sand & gravel, some fine sand, very damp-saturated, very dense	75		SS		8"	50+	50+				8.0
			80		SS		3"	50+	50+				5.2

Water Level— depth, ft. elev., ft. S - sample T - type: J (jar), SS (split-spoon), ST (shallow tube) R - recovery length, in.
 - while drilling: B - Standard Penetration Test (SPT), blows/6" interval. W - water content, %
 - after drilling: N - SPT, blows/foot to drive 2" O.D. split-spoon sampler with 140 lb. hammer falling 30".
 - hrs. after drilling: Pen. - pocket penetrometer reading, tons/sq. ft. Uw - dry unit weight of soil, lbs./cu.ft.
 Qu - unconfined compressive strength, tons/sq. ft.

F-111b

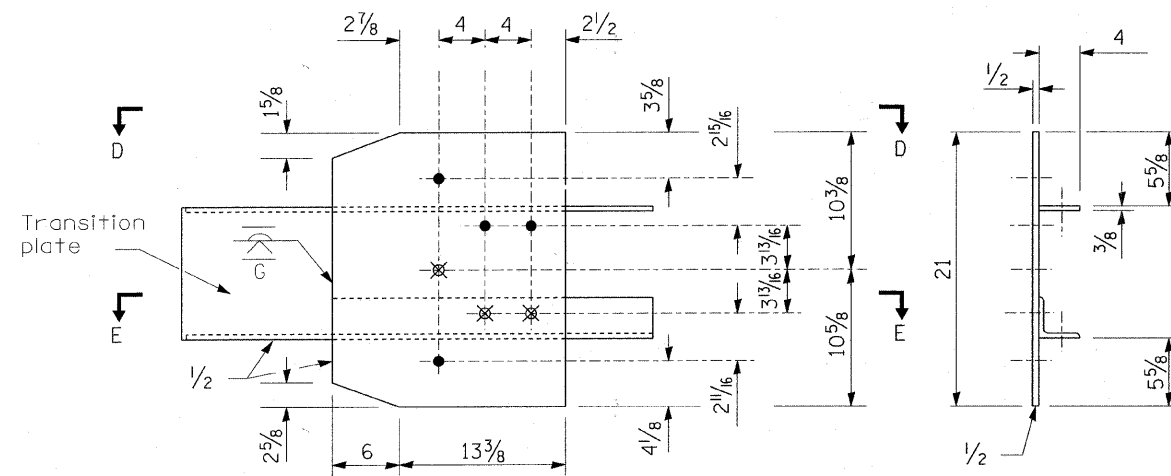
SOIL BORINGS
STRUCTURE NO. 056-3178

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

SHEET NO. 23 23 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	48
CONTRACT NO. 63536					
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

COPYRIGHT © 2008 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL ENGINEERING FIRM
 LICENSE NO. 184-002121
 EXP. 12/31/11
 PROJECT: UNION RD. OVER SOUTH BRANCH OF KISHAWA
 DRAWING: SOIL BORINGS
 DATE: 10/9/08
 SHEET: 23 OF 48

MCHENRY COUNTY
DIVISION OF TRANSPORTATION

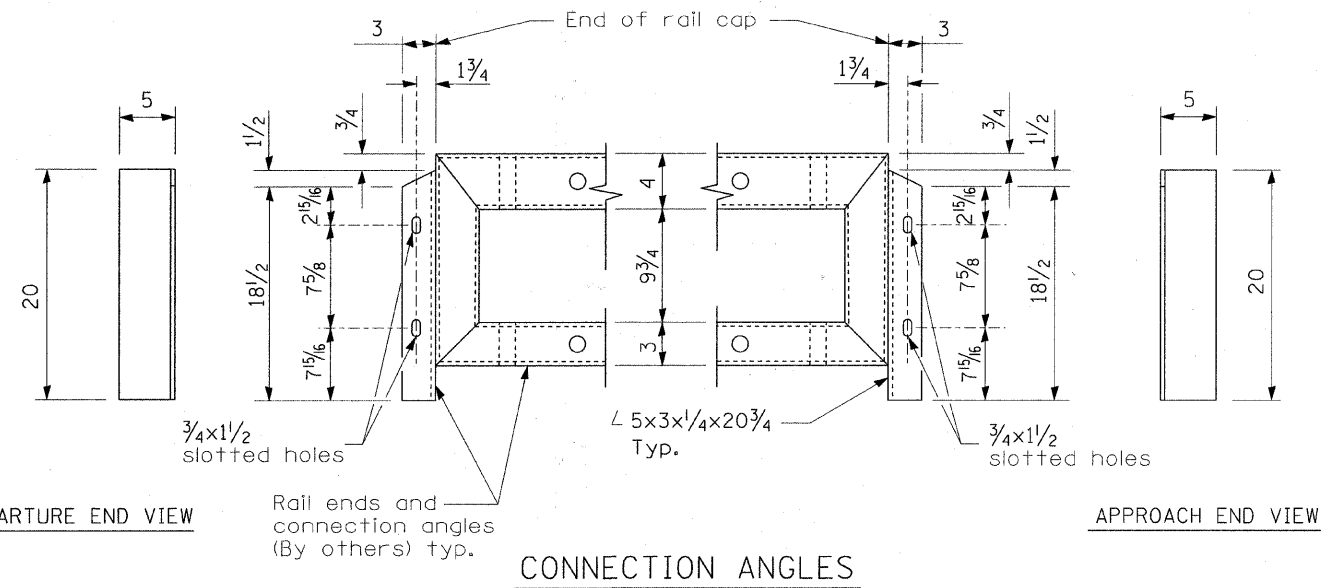
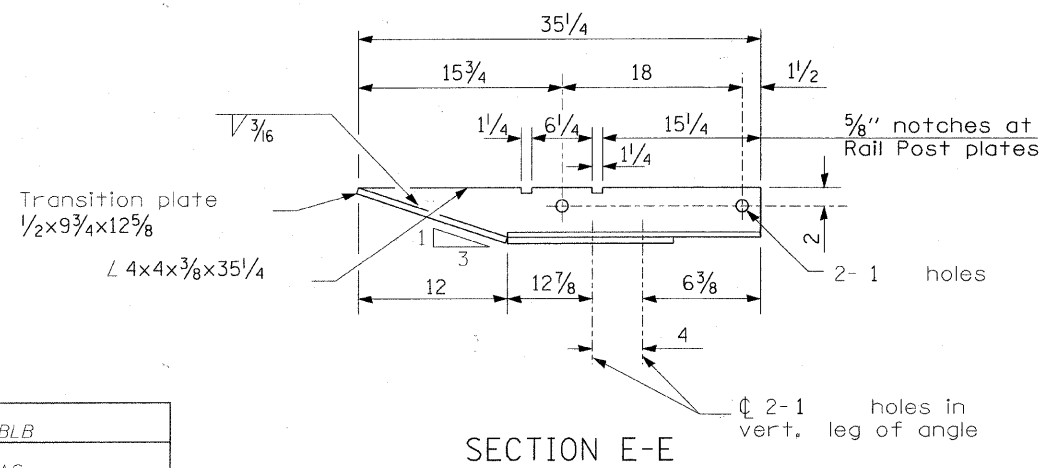
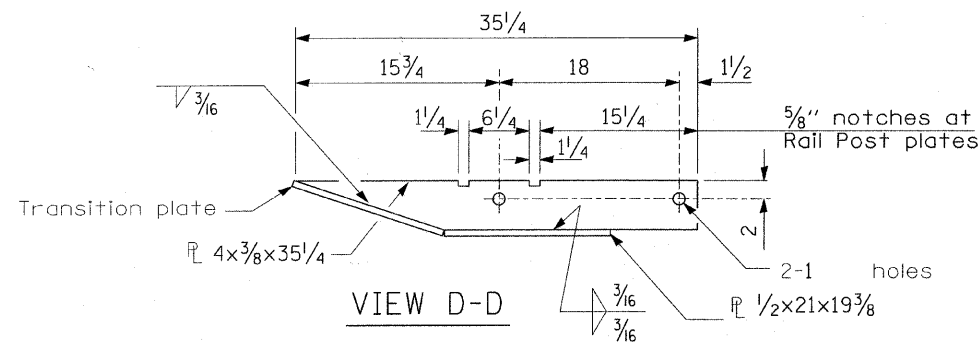


- \varnothing 4-1 holes for 7/8 H.S. bolts and nuts
- ⊗ Drill and tap 3 holes for 7/8 H.S. bolts.

LEGEND

GUARDRAIL CONNECTION PLATE ASSEMBLY DETAILS

(Mirror for opposite end)



COPYRIGHT © 2010 BY BAYTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 5555 S. W. 111th Street, Suite 100, Mokena, IL 60449-1000
 815-221-2000

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

TRAFFIC BARRIER TERMINAL
TYPE 6A (SPECIAL)
STRUCTURE NO. 056-3178

SHEET NO. 3 3 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	51
CONTRACT NO. 63536					
JOB NO. C-91-140-11		ILLINOIS	FED. AID PROJECT	BRS-0034(107)	

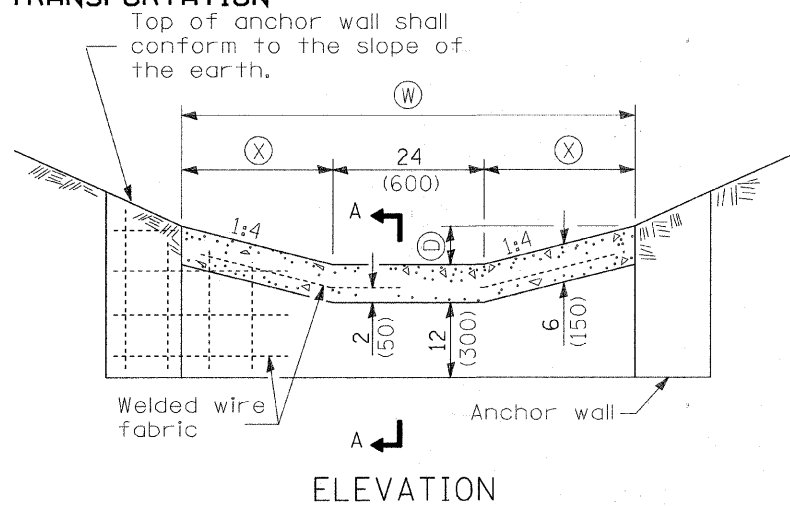
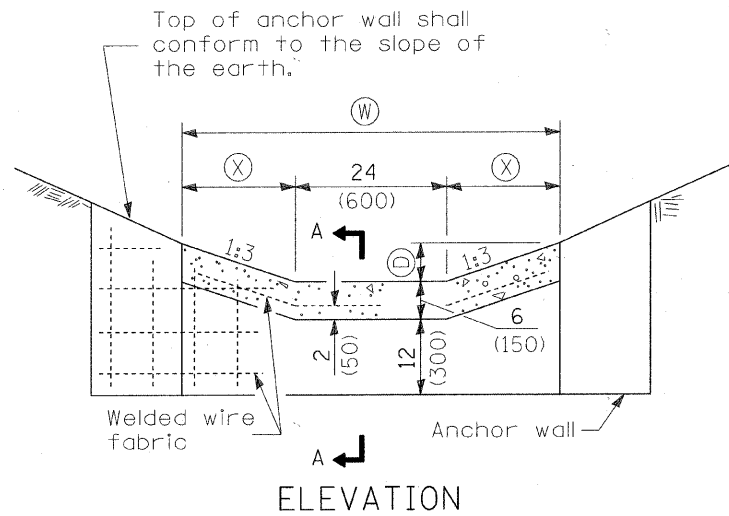
MCHENRY COUNTY

TABLE FOR PAVED DITCH TYPE A DIVISION OF TRANSPORTATION

TYPE	(D)	(W)	(X)	Flow Area sq. ft. (m ²)	Conc. Area sq. yd. (m ²)
A-15	6 (150)	5'-0" (1.5 m)	18 (450)	1.75 (0.175)	0.278 (0.225)
A-22	9 (225)	6'-6" (1.95 m)	27 (675)	3.19 (0.287)	0.361 (0.293)
A-30	12 (300)	8'-0" (2.4 m)	36 (900)	5.00 (0.450)	0.444 (0.360)
A-37	15 (375)	9'-6" (2.85 m)	3'-9" (1.12 m)	7.19 (0.645)	0.528 (0.426)
A-45	18 (450)	11'-0" (3.3 m)	4'-6" (1.35 m)	9.75 (0.877)	0.611 (0.495)
A-52	21 (525)	12'-6" (3.75 m)	5'-3" (1.58 m)	12.69 (1.144)	0.694 (0.564)
A-60	24 (600)	14'-0" (4.2 m)	6'-0" (1.8 m)	16.00 (1.440)	0.778 (0.630)

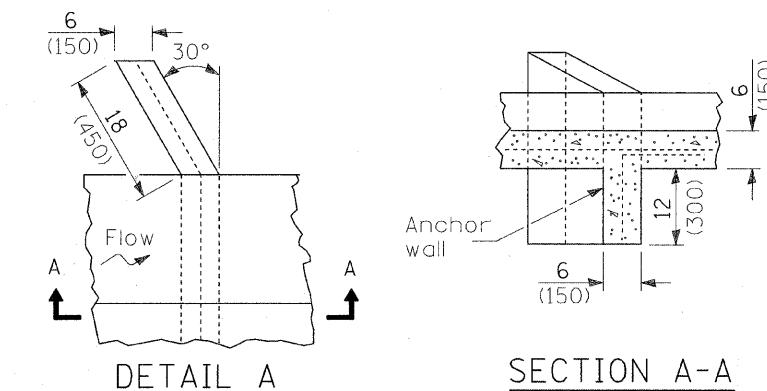
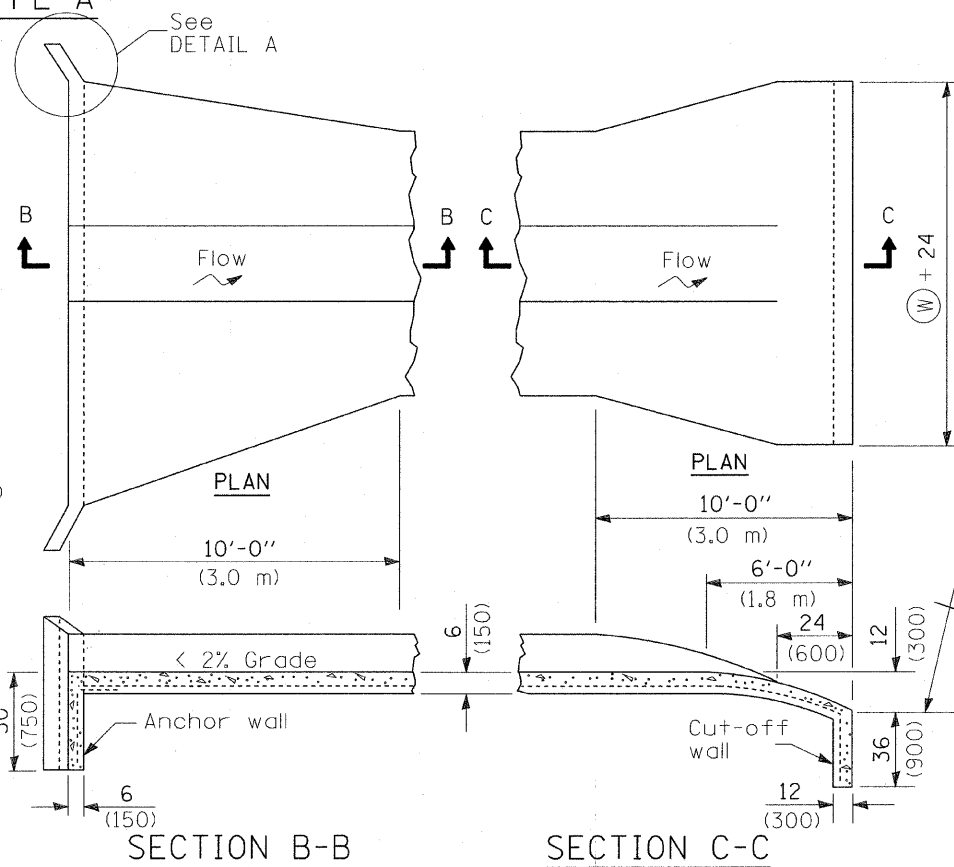
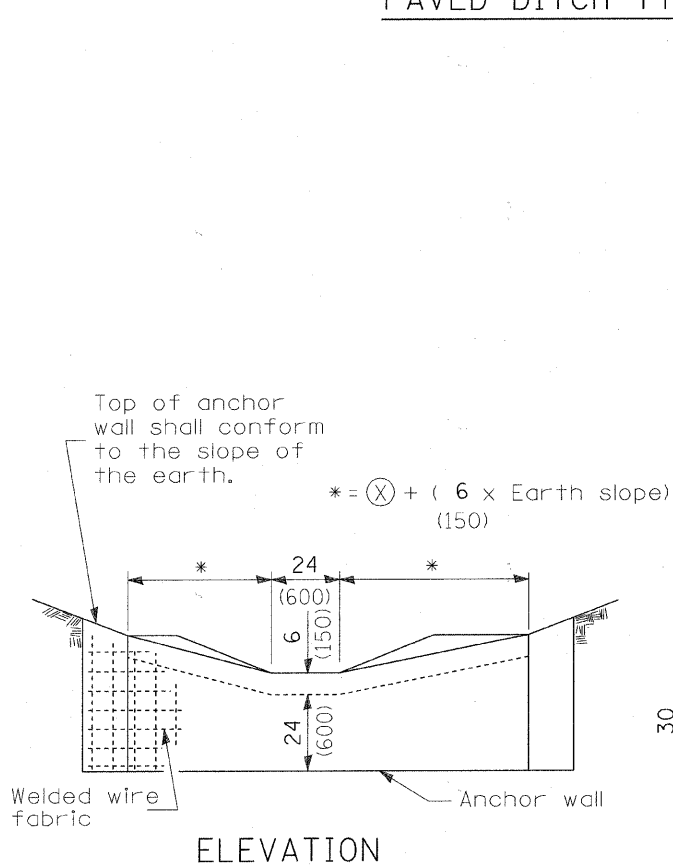
TABLE FOR PAVED DITCH TYPE B

TYPE	(D)	(W)	(X)	Flow Area sq. ft. (m ²)	Conc. Area sq. yd. (m ²)
B-15	6 (150)	6'-0" (1.8 m)	24 (600)	2.00 (0.180)	0.333 (0.270)
B-22	9 (225)	8'-0" (2.4 m)	36 (900)	3.75 (0.337)	0.444 (0.360)
B-30	12 (300)	10'-0" (3.0 m)	4'-0" (1.2 m)	6.00 (0.540)	0.555 (0.450)
B-37	15 (375)	12'-0" (3.6 m)	5'-0" (1.5 m)	8.75 (0.787)	0.667 (0.540)
B-45	18 (450)	14'-0" (4.2 m)	6'-0" (1.8 m)	12.00 (1.080)	0.778 (0.630)
B-52	21 (525)	16'-0" (4.8 m)	7'-0" (2.1 m)	15.75 (1.417)	0.889 (0.720)
B-60	24 (600)	18'-0" (5.4 m)	8'-0" (2.4 m)	20.00 (1.800)	1.000 (0.810)



PAVED DITCH TYPE A

PAVED DITCH TYPE B



DETAIL OF ANCHOR WALL

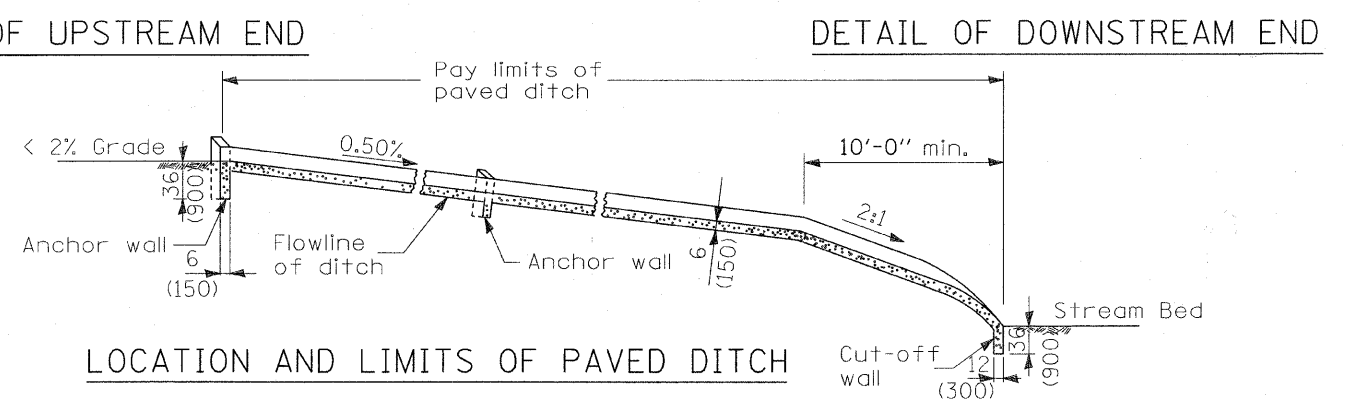
GENERAL NOTES

Construction joints between paved ditch and adjacent sections of existing or proposed slope wall shall be filled with preformed joint filler, according to the requirements of article 503.10(b) of the standard specifications. Cost included with paved ditch, type a-45 (special).

All slopes are expressed as of vertical displacement to units of horizontal displacement (V:H).

All dimensions are in inches (millimeters) unless otherwise shown.

PAVED DITCH, TYPE A-45 (SPECIAL)

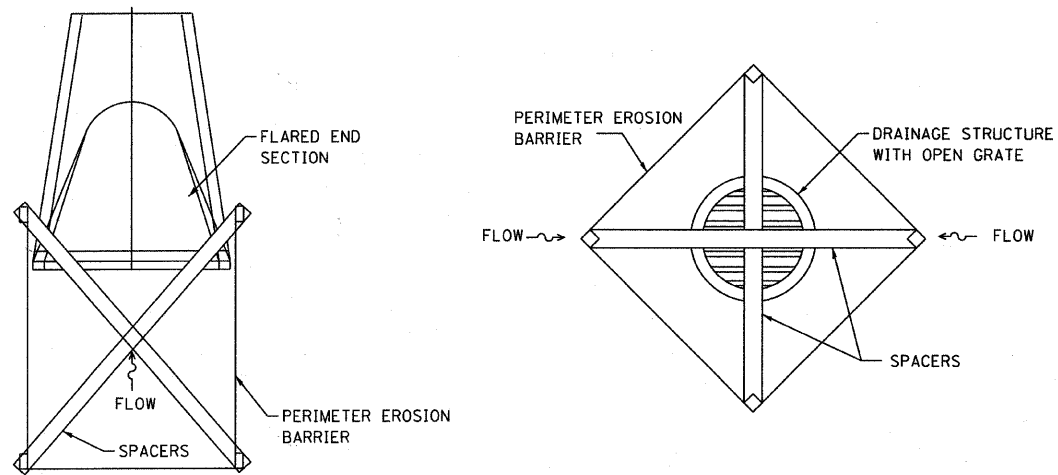


LOCATION AND LIMITS OF PAVED DITCH

DESIGNED	BLB
CHECKED	AS
DRAWN	BCD
CHECKED	BLB

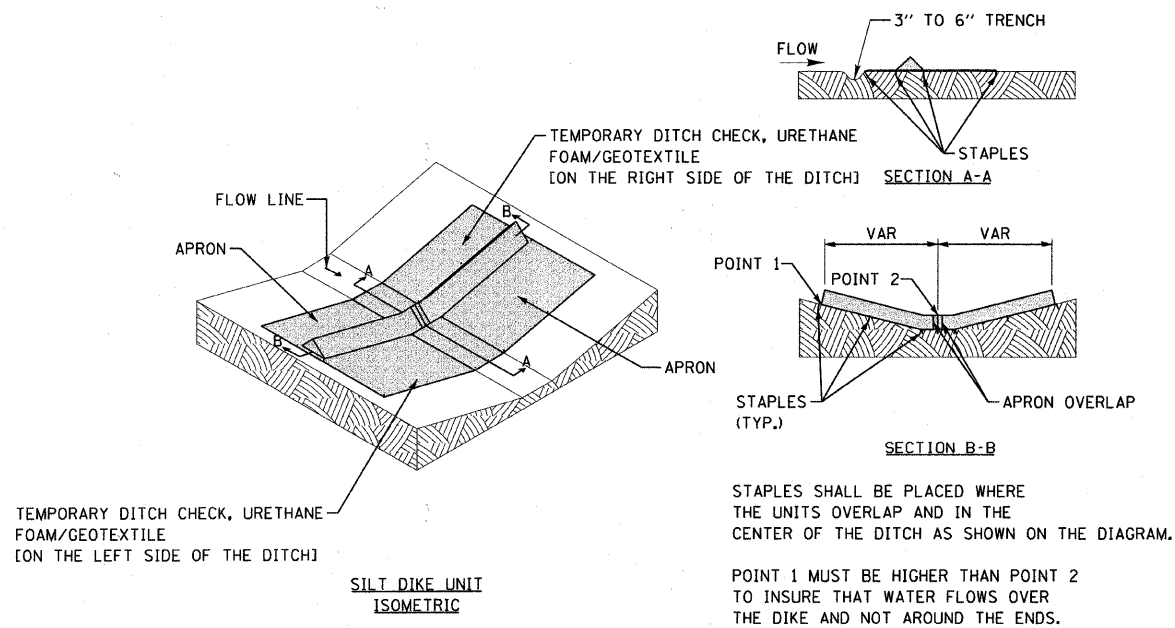
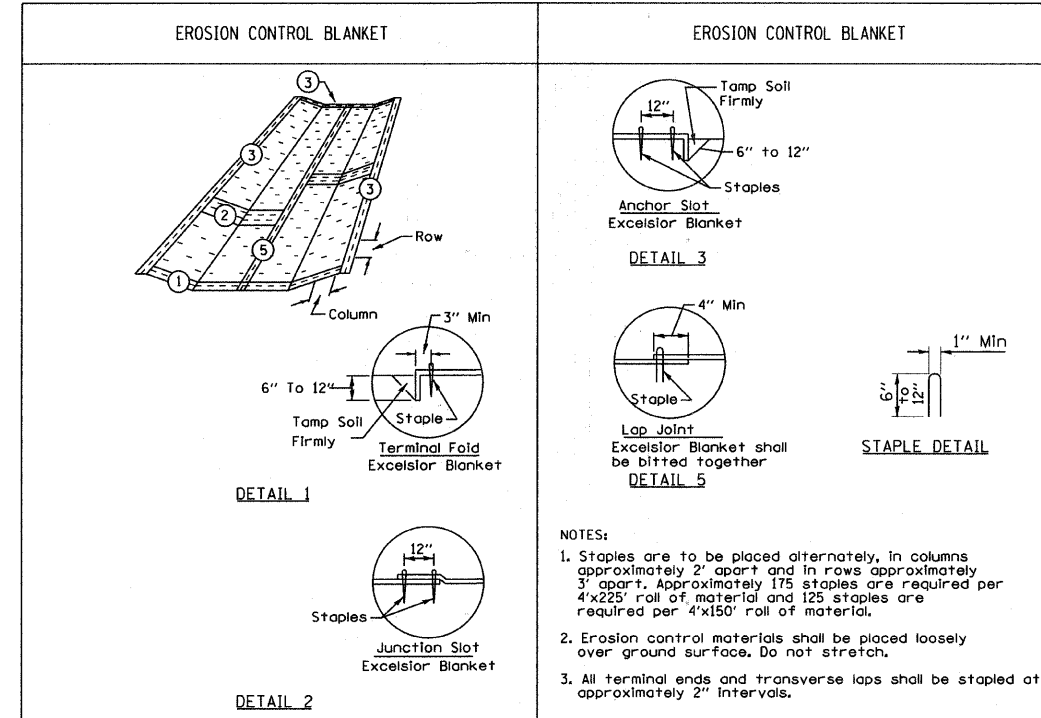
SHEET NO. 1 1 SHEETS	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	0034	06-00320-02-BR	MCHENRY	65	52
JOB NO. C-91-140-11			ILLINOIS	FED. AID PROJECT	BRS-0034(107)

COPYRIGHT © 2000 BY BAYTER & WOODMAN, INC.
 STATE OF ILLINOIS - PROFESSIONAL DESIGN FIRM
 15600 S. WOODHURST AVE., SUITE 100, CHICAGO, IL 60640
 TEL: (773) 225-2000 FAX: (773) 225-2001
 WWW.BAYTERWOODMAN.COM



•INSTALL PER IDOT STANDARD 280001-05
 •• PAID FOR AS PERIMETER EROSION BARRIER

DRAINAGE PROTECTION
 NOT TO SCALE

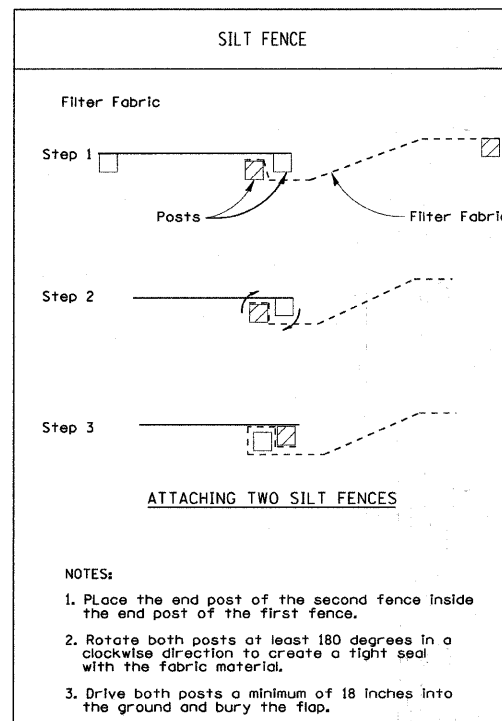


TEMPORARY DITCH CHECK, URETHANE
 FOAM/GEOTEXTILE
 (ON THE LEFT SIDE OF THE DITCH)

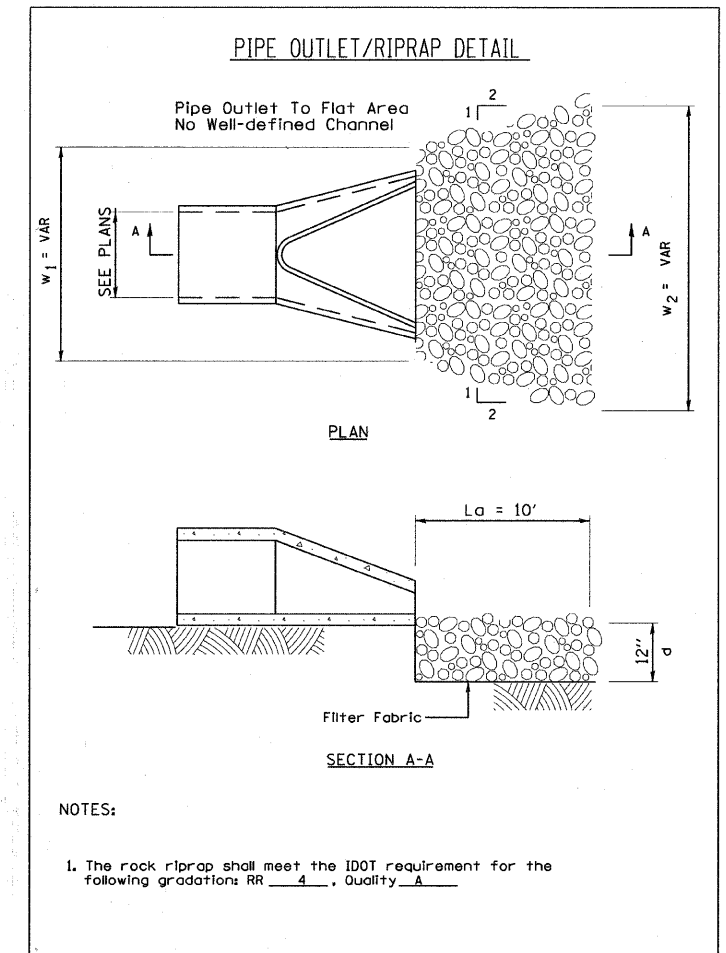
**SILT DIKE UNIT
 ISOMETRIC**

NOTES:
 1. THE INSTALLATION SHOWN ABOVE WILL BE MEASURED FOR
 PAYMENT IN FEET PERPENDICULAR TO THE FLOW LINE.

TEMPORARY DITCH CHECK
 NO SCALE



NOTES:
 1. Place the end post of the second fence inside
 the end post of the first fence.
 2. Rotate both posts at least 180 degrees in a
 clockwise direction to create a tight seal
 with the fabric material.
 3. Drive both posts a minimum of 18 inches into
 the ground and bury the flap.



NOTES:
 1. The rock riprap shall meet the IDOT requirement for the
 following gradation: RR 4, Quality A

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 ALL RIGHTS RESERVED. 070630-Details.sht
 DRAWN BY: BCD
 CHECKED BY: MDW
 DATE: 08/20/10
 FILE: 070630-Details.sht
 PROJECT: MCHENRY COUNTY DIVISION OF TRANSPORTATION
 UNION ROAD BRIDGE REPLACEMENT AND ROADWAY RECONSTRUCTION
 SHEET NO. 65 OF 65
 CONTRACT NO. 63536
 JOB NO. C-91-140-11
 ILLINOIS FED. AID PROJECT
 BRS-0034(107)



DESIGNED	- CAC	REVISED	-
DRAWN	- BCD	REVISED	-
CHECKED	- MDW	REVISED	-
DATE	- 08/20/10	FILE	- 070630-Details.sht

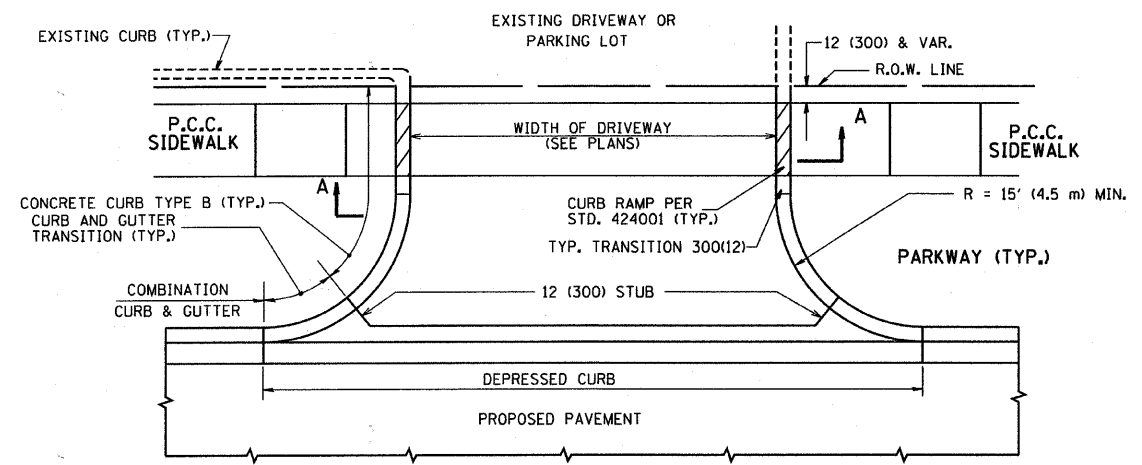
**MCHENRY COUNTY DIVISION OF TRANSPORTATION
 UNION ROAD BRIDGE REPLACEMENT
 AND ROADWAY RECONSTRUCTION**

MISCELLANEOUS DETAILS

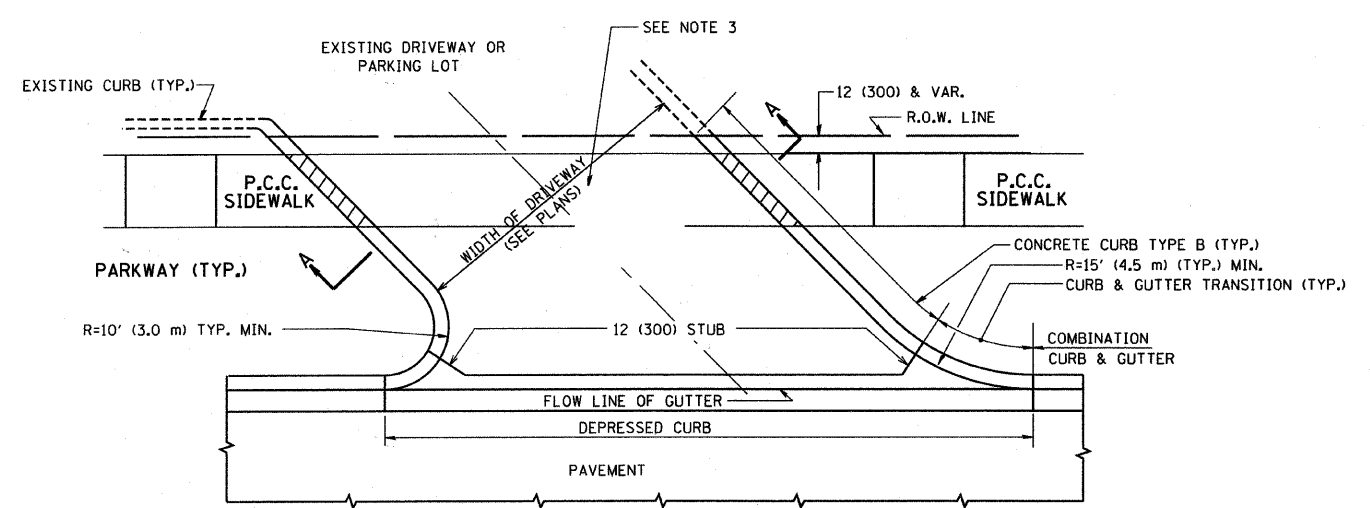
SCALE: NONE

STA. TO STA.

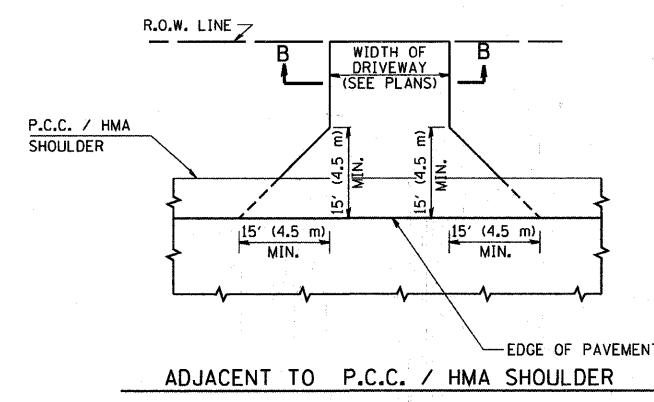
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	53
CONTRACT NO. 63536				
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT BRS-0034(107)	



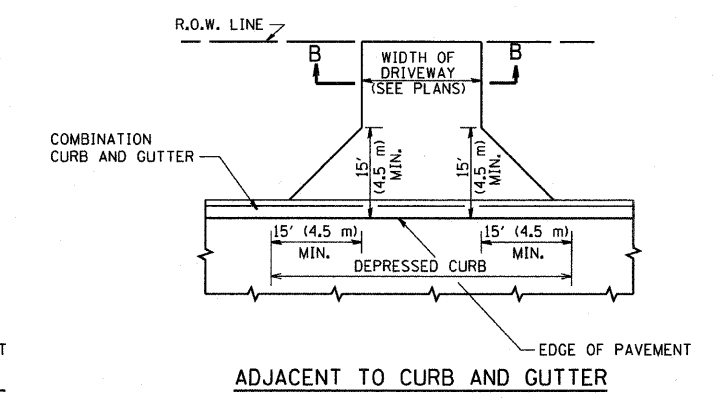
WITH CONCRETE CURB, TYPE B



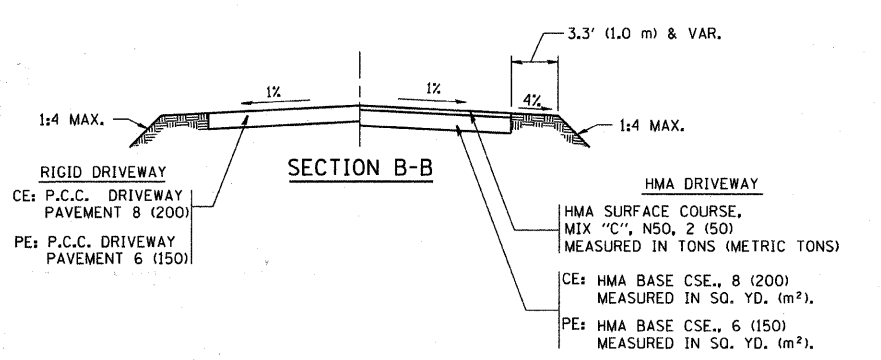
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "C", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

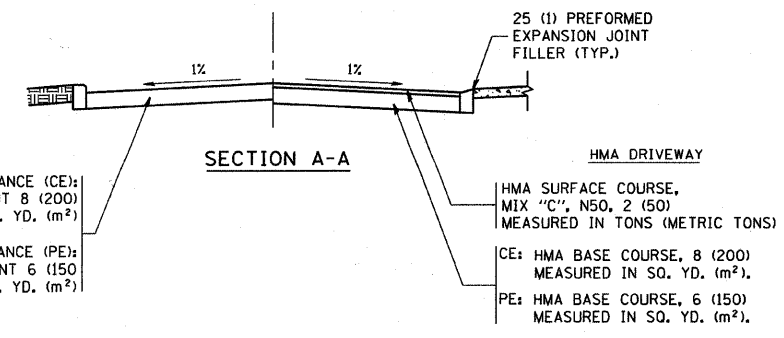
COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.



FILE NAME =
 c:\projects\diststd22x34\bd01.dgn

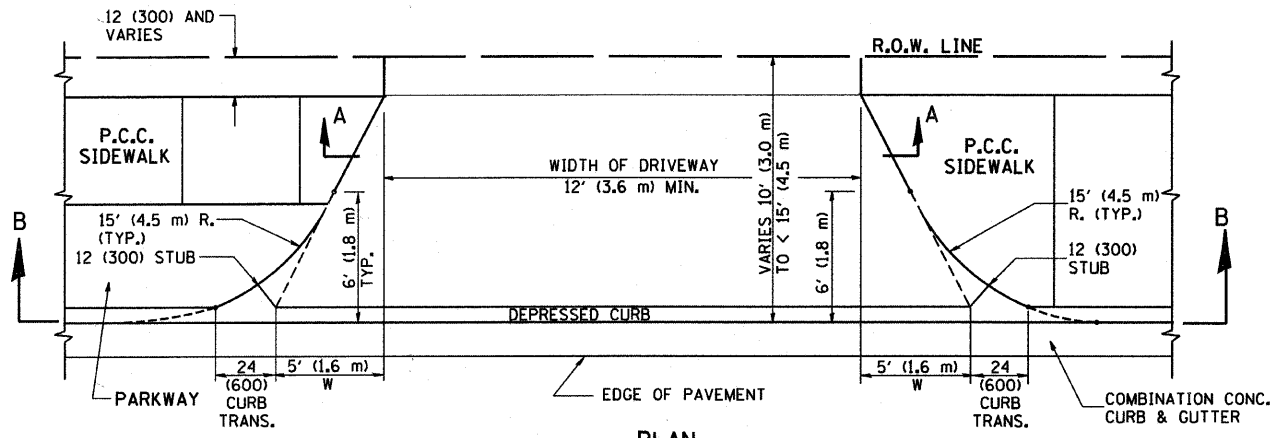
USER NAME = bauerdl
 PLOT SCALE = 49.9999' / IN.
 PLOT DATE = 6/12/2008

DESIGNED - R. SHAH
 DRAWN -
 CHECKED -
 DATE - 11-04-95
 REVISED - M. GOMEZ 04-06-01
 REVISED - P. LOFLUER 04-15-03
 REVISED - R. BORO 01-01-07
 REVISED - R. BORO 06-11-08

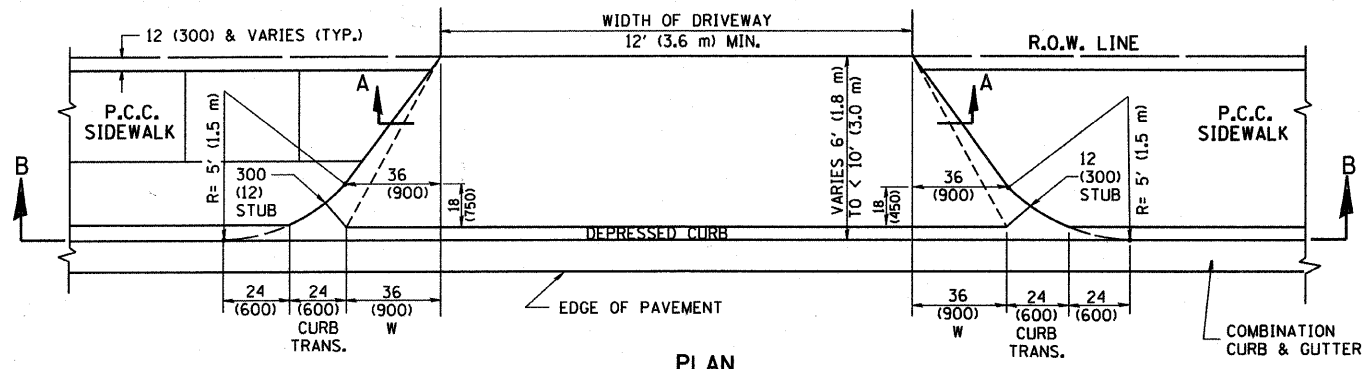
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
 AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)
 SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

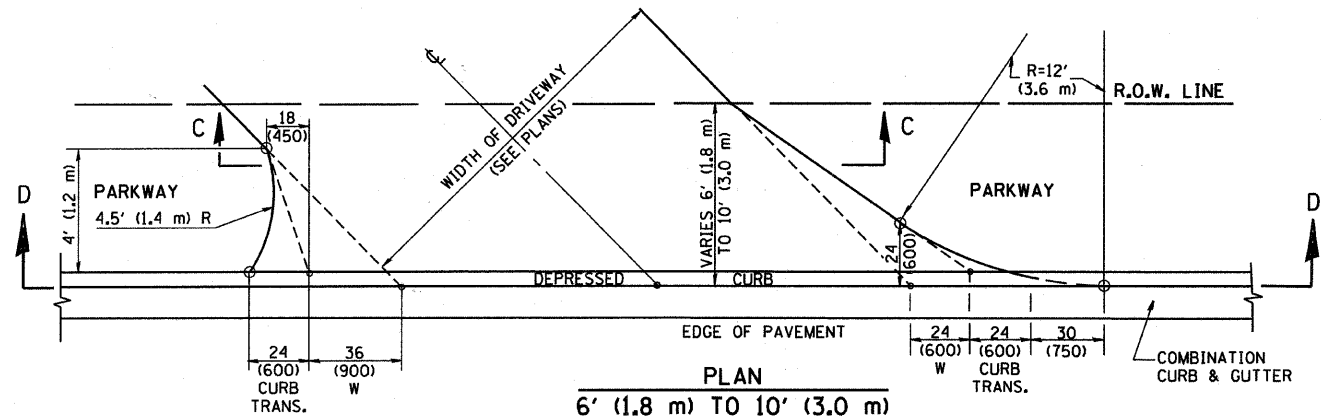
F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	54
BDD156-07 (BD-01)			CONTRACT NO. 63536	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			BRS-0034(107)	



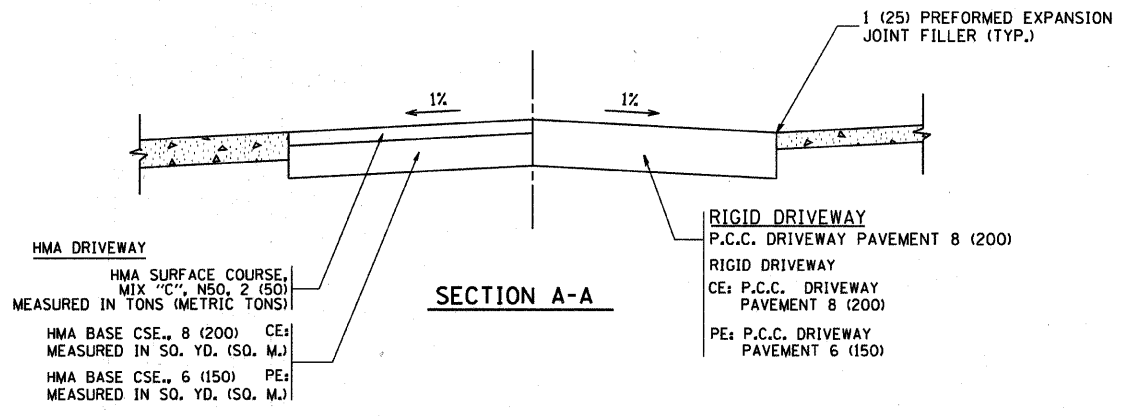
PLAN
10' (3.0 m) TO < 15' (4.5 m)



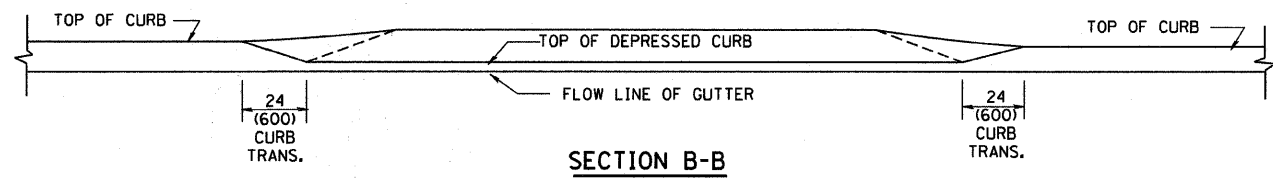
PLAN
6' (1.8 m) TO < 10' (3.0 m)



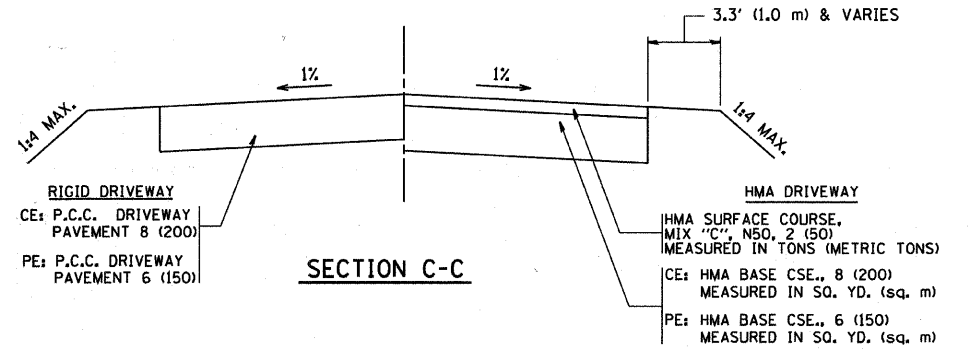
PLAN
6' (1.8 m) TO 10' (3.0 m)



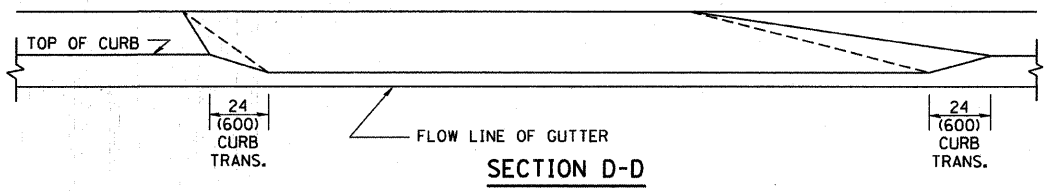
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

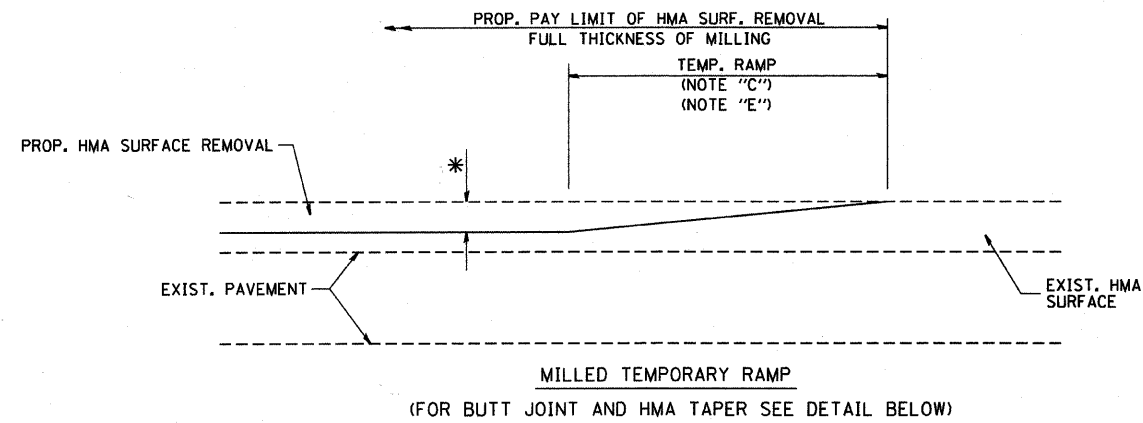
COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

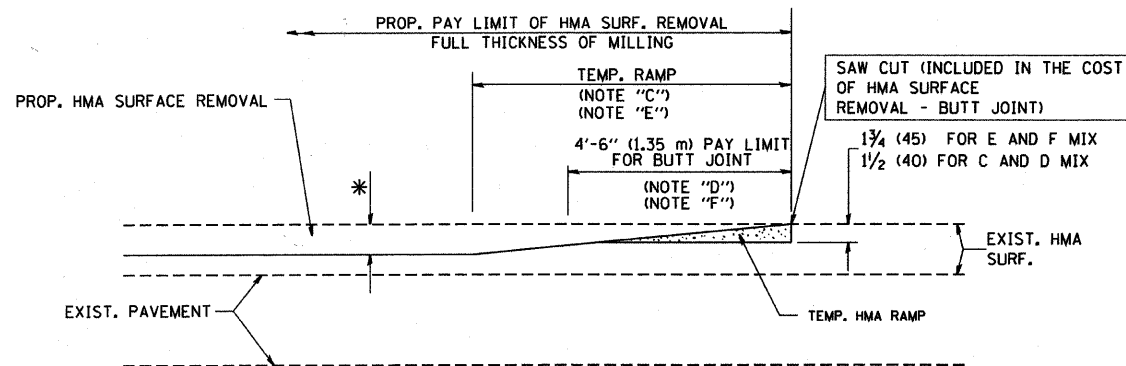
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME = W:\diststd\22x34\bd02.dgn	USER NAME = gaglienobt	DESIGNED - R. SHAH	REVISED - T. HOLTZ 04-08-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY DETAILS			F.A. R.T.E. = 0034	SECTION = 06-00320-02-BR	COUNTY = MCHENRY	TOTAL SHEETS = 65	SHEET NO. = 55
PLOT SCALE = 50.0000' / IN.	CHECKED -	DRAWN -	REVISED - M. GOMEZ 04-06-01		DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)			BD400-02 (BD-02)		CONTRACT NO. 63536		
PLOT DATE = 1/4/2008	DATE = 11-06-95	CHECKED -	REVISED - P. LoFLEUR 04-15-03		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT BRS-0034(107)				
		DATE =	REVISED - R. BORO 01-01-07									



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

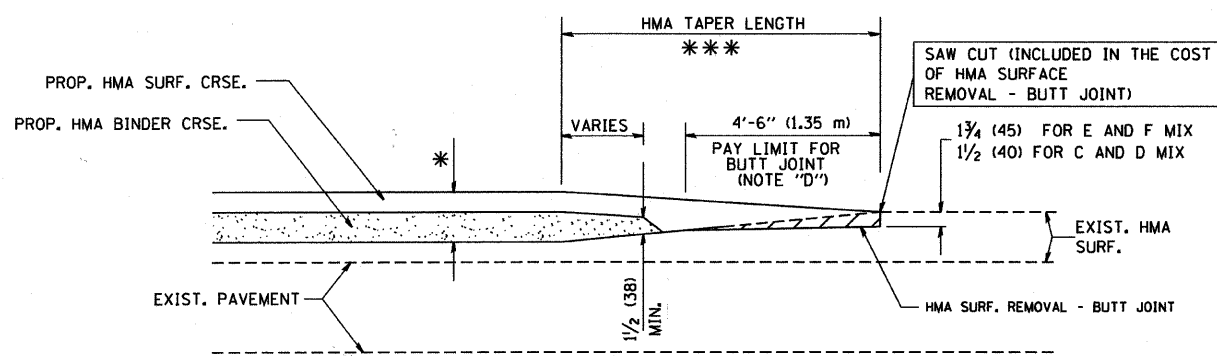
OPTION 1



HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

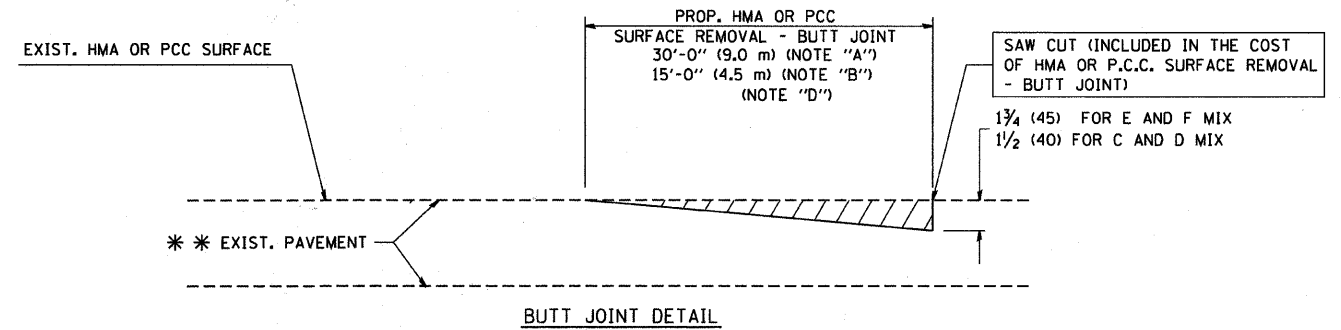
OPTION 2

TYPICAL TEMPORARY RAMP

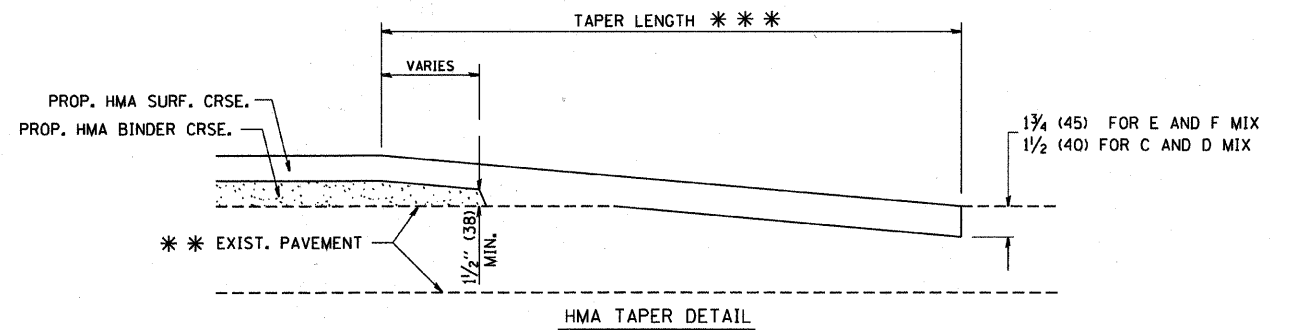


BUTT JOINT AND
HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =
W:\dst\std\22x34\bd32.dgn

USER NAME = gegl1n0bt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

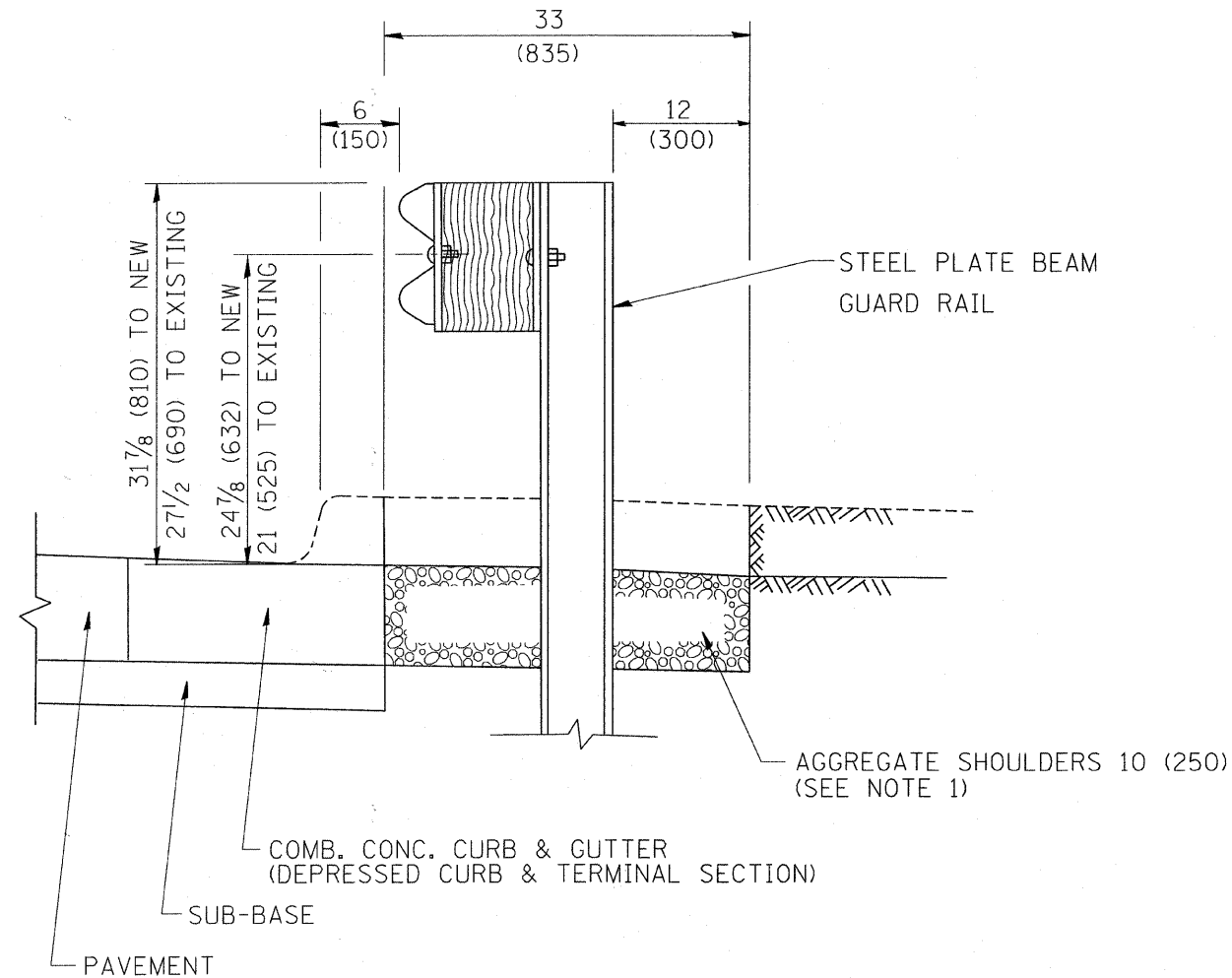
REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

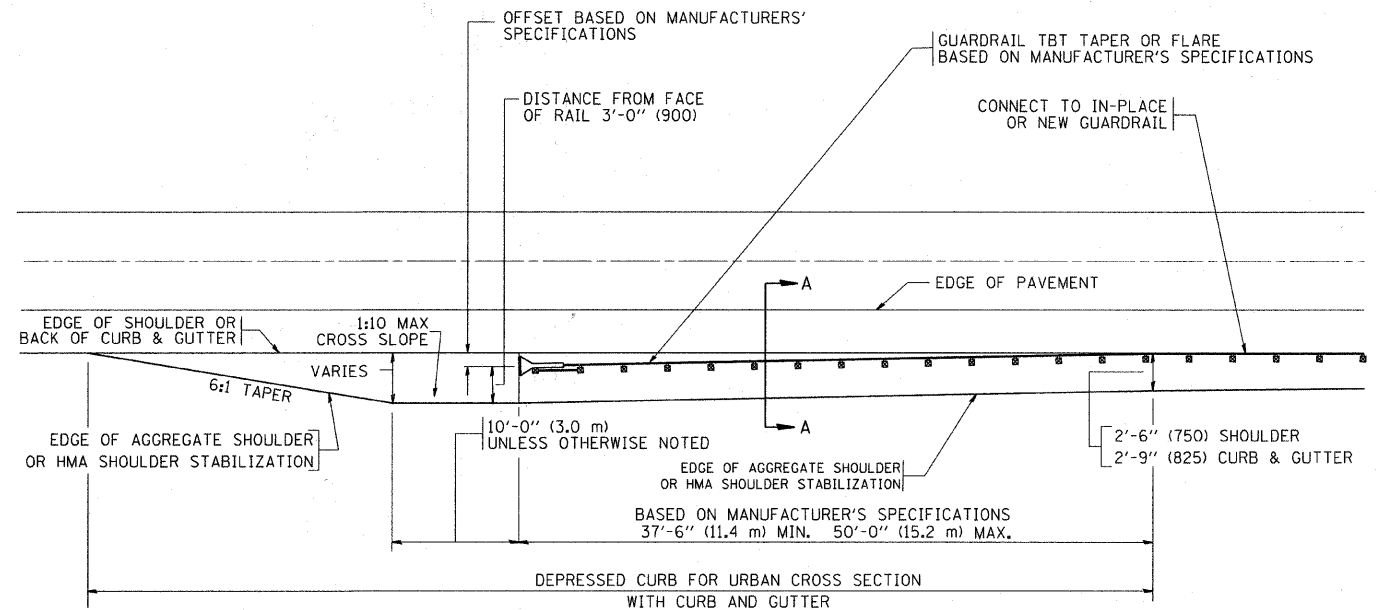
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	56
BD400-05 BD32			CONTRACT NO. 63536	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			BRS-0034(107)	



SECTION A-A

- NOTES:
1. THE AGGREGATE SHOULDER, 10" OR HMA SHOULDER, 6" (IF REQUIRED) SHALL EXTEND UNDER THE TRAFFIC BARRIER TERMINAL.
 2. "EXISTING" GUARDRAIL REFERS TO CONNECTING TERMINAL SECTION TO GUARD RAILING PRIOR TO THE MIDWEST GUARDRAIL SYSTEM.
 3. THE CONTRACTOR SHALL VERIFY THE TYPE/HEIGHT OF GUARDRAIL IN-PLACE BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL. THE TERMINAL SECTION HEIGHT TO BE PLACED MUST MATCH THE HEIGHT OF THE IN-PLACE GUARDRAIL.

**DETAILS FOR STEEL PLATE BEAM
GUARD RAIL ADJACENT TO CURB AND GUTTER
[FOR ROADWAY SPEED 35 MPH (60 kmh) TO 45 MPH (70 kmh)]**



**DEPRESSED CURB AND GUTTER AND
SHOULDER TREATMENT AT TBT TY. 1 SPL.**

BASIS OF PAYMENT: HMA SHOULDERS 6 (150) (IF REQUIRED) WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SHOULDERS 6" (150 mm)".

STEEL PLATE BEAM GUARD RAIL AND TRAFFIC BARRIER TERMINAL, OF THE TYPE SPECIFIED WILL BE PAID FOR SEPARATELY.

TBT = TRAFFIC BARRIER TERMINAL
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

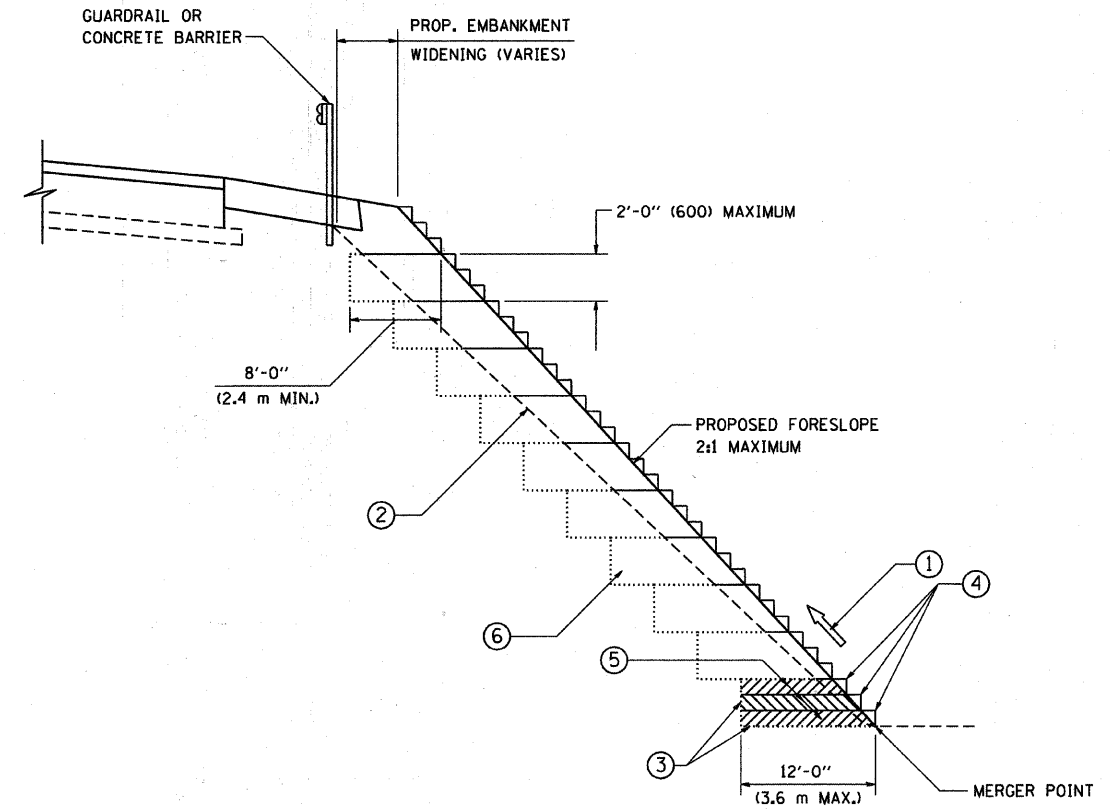
FILE NAME =	USER NAME = drivakosgn	DESIGNED - M. DE YONG	REVISED - E. GOMEZ 08-28-00
ca:\pwork\PWIDOT\DRIVAKOSGN\d0108315\634.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 49.9999' / IN.	CHECKED -	REVISED - R. BORO 12-08-2008
	PLOT DATE = 9/21/2009	DATE - 09-22-90	REVISED - R. BORO 09-14-2009

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR DEPRESSED CURB & GUTTER AND
SHOULDER TREATMENT AT TBT TY 1 SPL.**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 57
BD600-10 (BD 34)		CONTRACT NO. 63536		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT BRS-0034(107)				



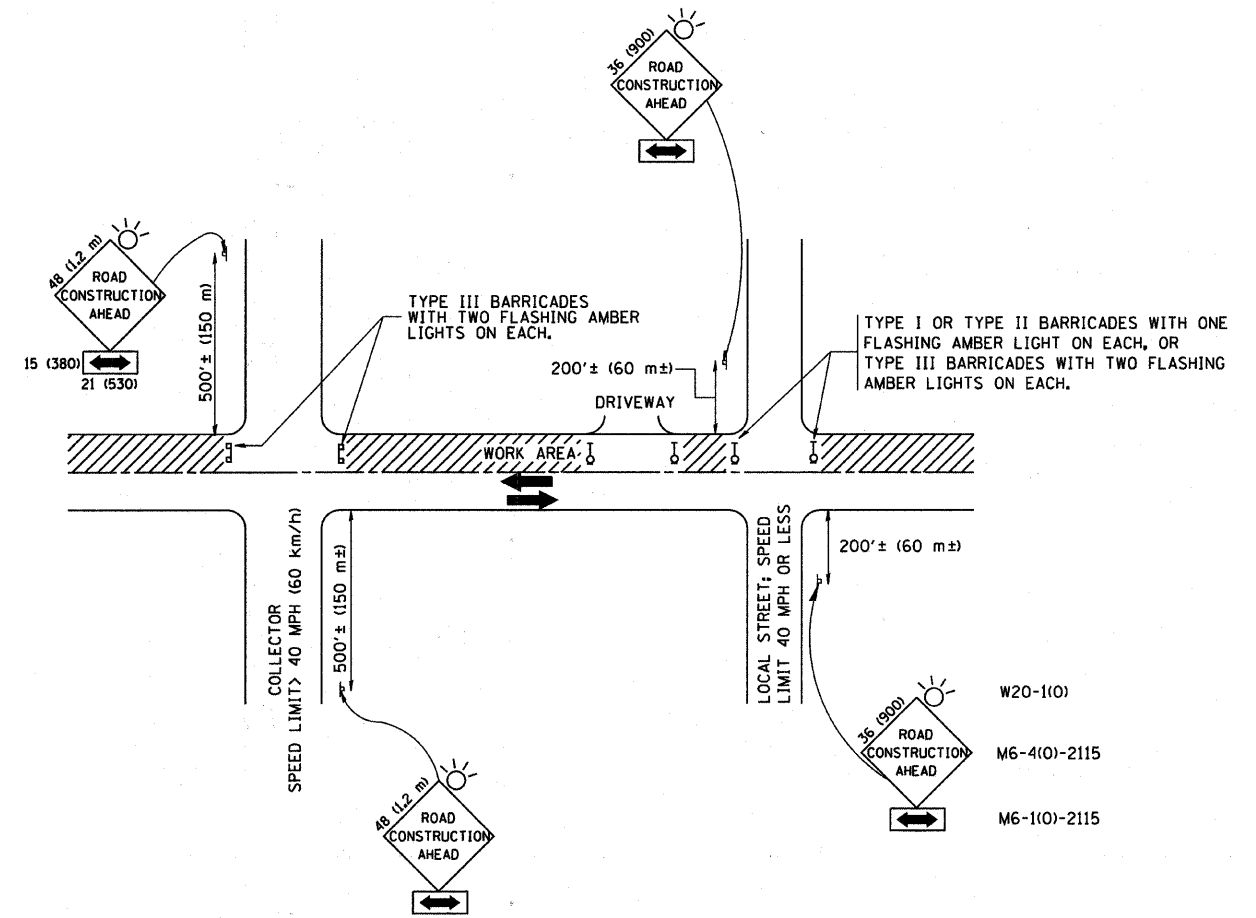
**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd51.dgn	USER NAME = gegltenobt	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BENCHING DETAIL FOR EMBANKMENT WIDENING			F.A. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 58
	PLOT SCALE = 50.0000' / IN.	DRAWN - CADD	REVISED -		SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. BD-51	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	
	PLOT DATE = 1/4/2008	CHECKED - S.E.B.	REVISED -								CONTRACT NO. 63536	
		DATE - 06-16-04	REVISED -								BRS-0034(107)	



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

S

All dimensions are in millimeters (inches) unless otherwise shown.

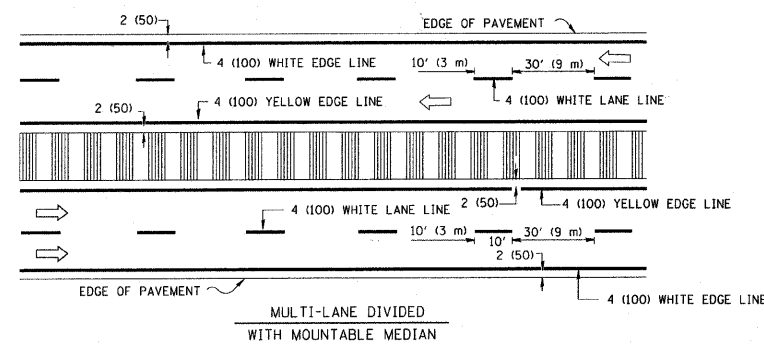
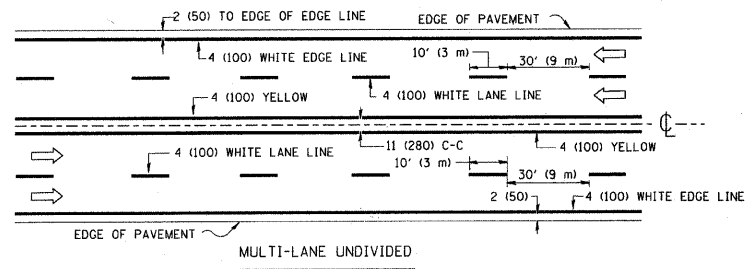
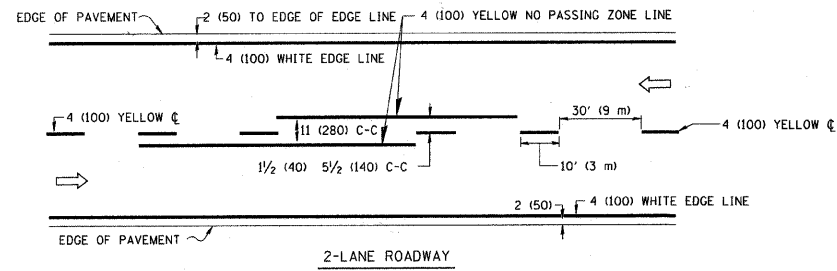
FILE NAME =	USER NAME = geglionob1	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
W:\diststd\22x34\tbl8.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

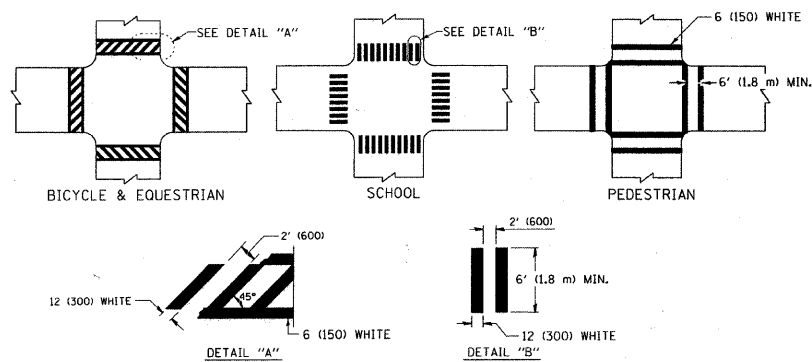
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	59
TC-10			CONTRACT NO. 63536	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT BRS-0034(107)				

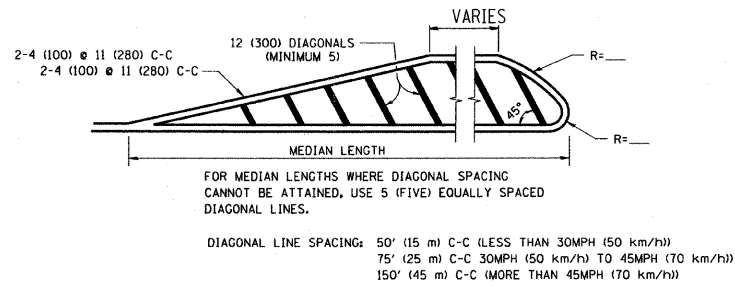
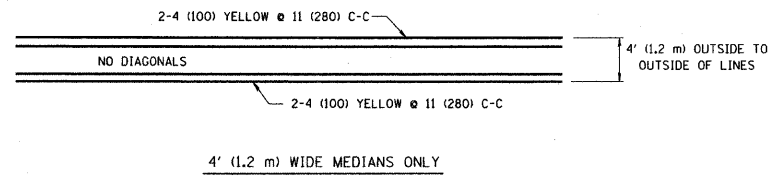


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

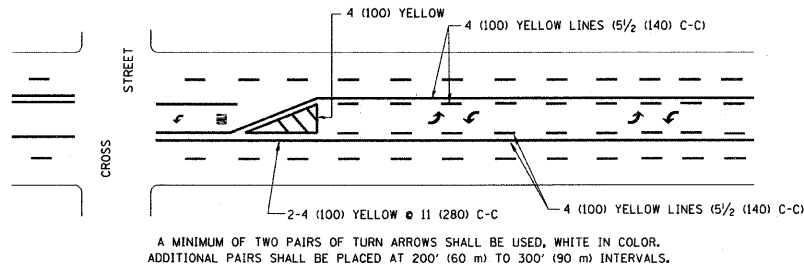
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING

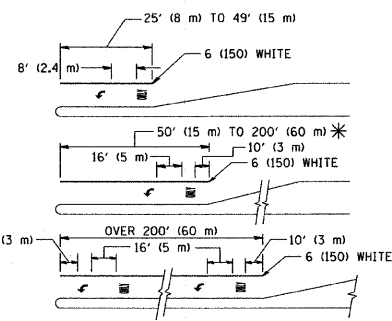


MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

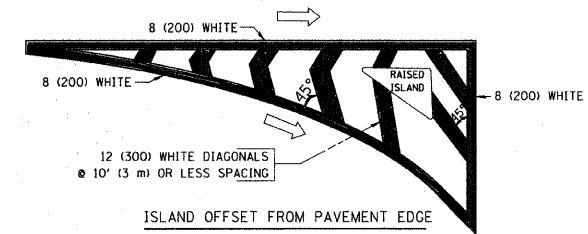


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 * AREA = 15.6 SQ. FT. (1.5 m²) □ AREA = 20.8 SQ. FT. (1.9 m²)

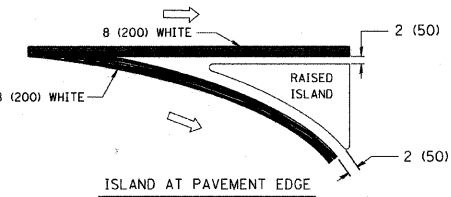
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION	4 (100)	SOLID	YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE
NO PASSING ZONE LINES: FOR BOTH DIRECTIONS	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 78000! AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

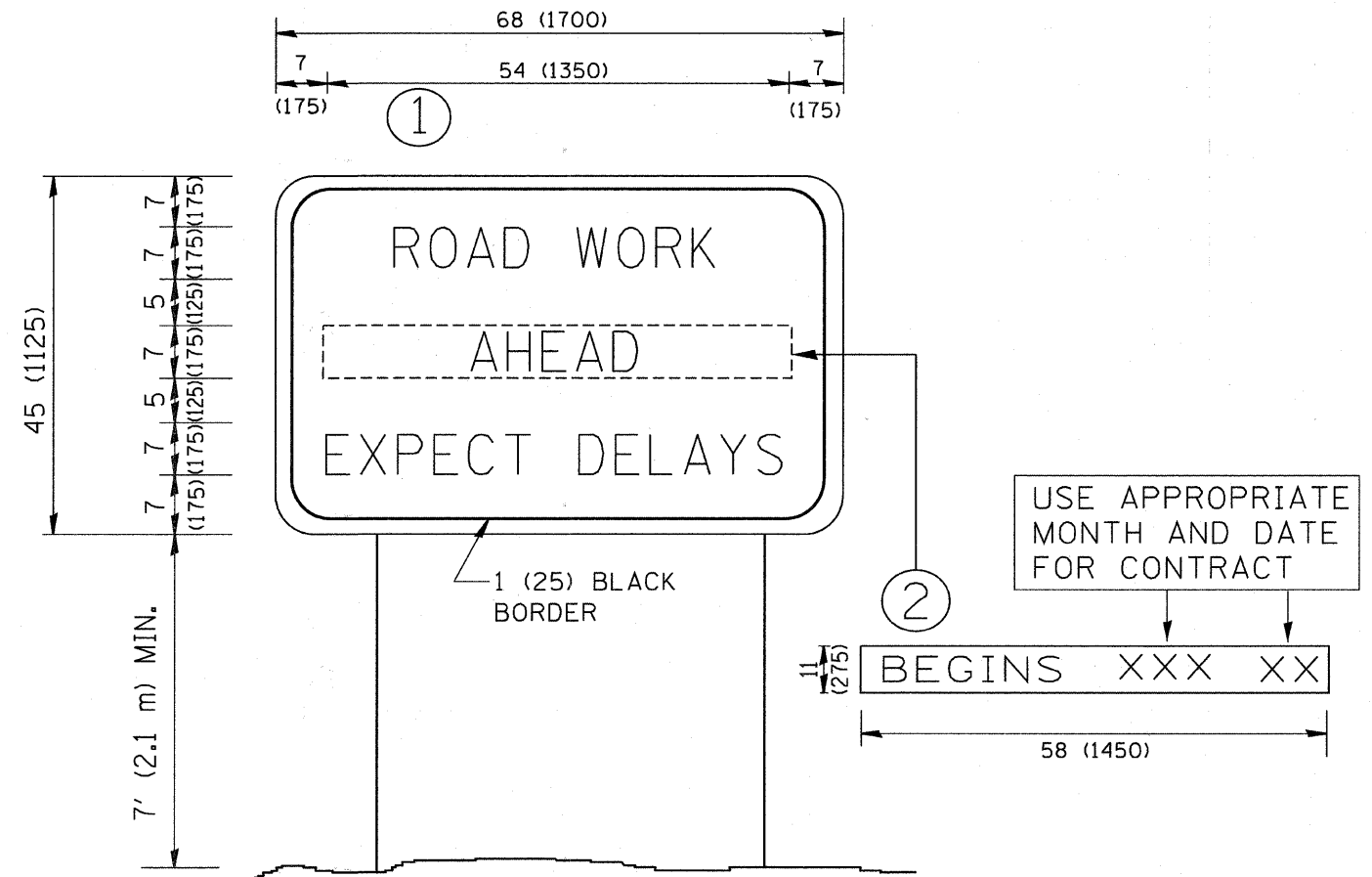
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
ca:\pw_work\pwsdot\drivakosgn\d8108315\td	3.dgn	DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50,000 / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0034	06-00320-02-BR	MCHENRY	65	60
TC-13			CONTRACT NO. 63536	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT BRS-0034(107)				



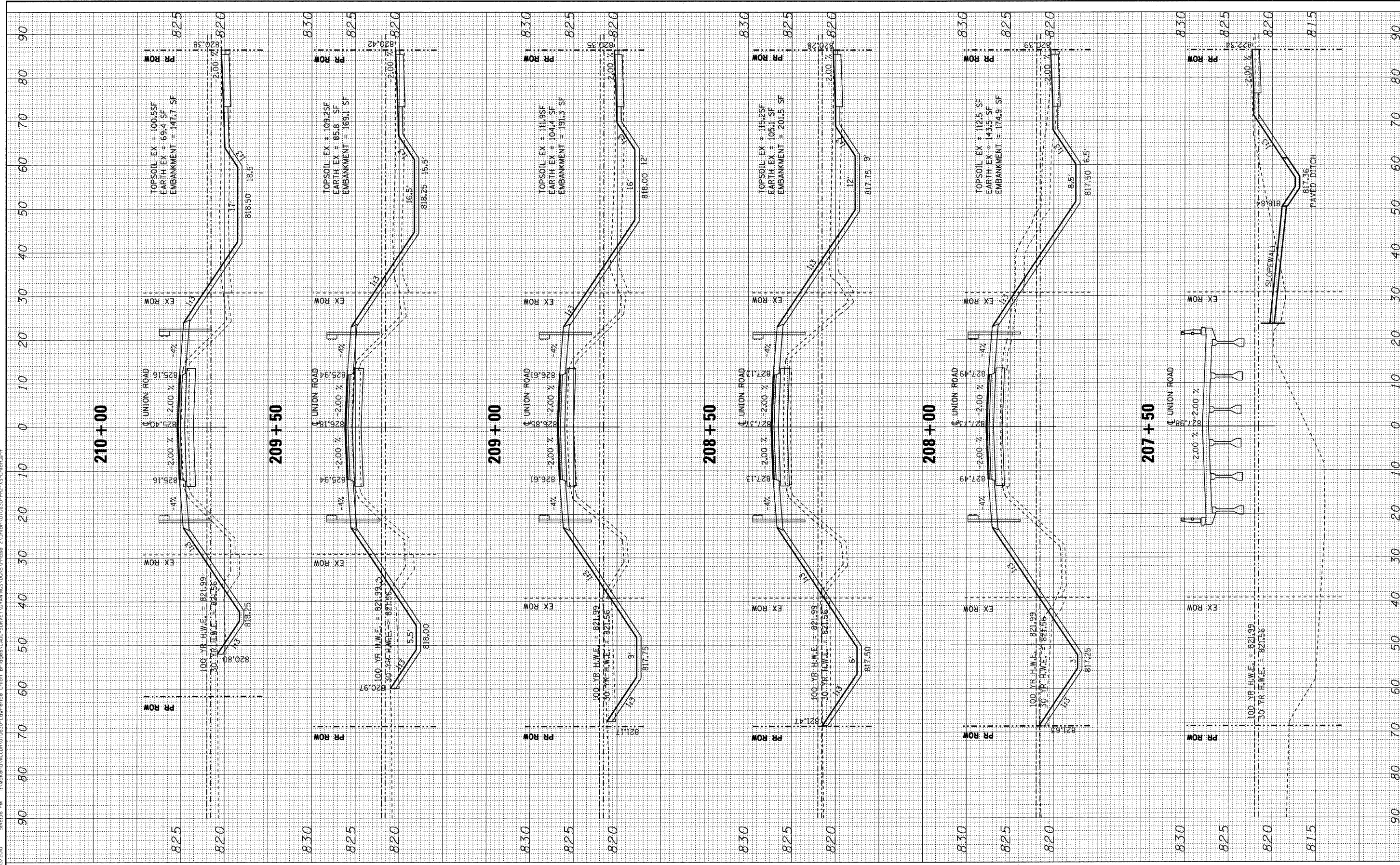
NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = w:\diststd\22x34\to22.dgn	USER NAME = geglianobt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN	F.A.S. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 50.000 / IN.	CHECKED -	REVISED - R. MIRS 12-11-97	0034			06-00320-02-BR	MCHENRY	65	61	
PLOT DATE = 1/4/2008	DATE -	REVISED - T. RAMMACHER 02-02-99	TC-22			CONTRACT NO. 63536				
		REVISED - C. JUCLIS 01-31-07	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			BRS-0034(107)				
						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	

COPYRIGHT © 2010 BY BAXTER & WOODMAN, INC.
 STATE OF ILLINOIS PROFESSIONAL DESIGN FIRM
 No. - BR-12/13/2010
 1: Mokena\VCC\0109530-Lawrence Union Bridge\Drawings\00AS\Phase 2\Union\0109530-Ph2-XS-Union.rvt
 5/8/16 BW



DESIGNED - CAC	REVISED - PER IDOT 10-08-10
DRAWN - BCD	REVISED -
CHECKED - TMS	REVISED -
DATE - 08-20-10	REVISED -

**MCHENRY COUNTY DIVISION OF TRANSPORTATION
 UNION ROAD BRIDGE OVER
 SOUTH BRANCH OF KISHWAUKEE RIVER**

**CROSS SECTIONS - UNION ROAD AT
 SOUTH BRANCH KISHWAUKEE RIVER**

SCALE: H_v 1"=10' V_v 1"=5'
 STA. TO STA.

F.A.S. RTE. 0034	SECTION 06-00320-02-BR	COUNTY MCHENRY	TOTAL SHEETS 65	SHEET NO. 64
JOB NO. C-91-140-11			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63536	
			BRS-00341071	

