## DETAIL OF P.C. CONCRETE DRIVEWAYS

## WHERE NARROW BOULEVARDS OR RADII GREATER THAN 15' (4.6 m) ARE CONSTRUCTED. PROP. P.C. CONCRETE DRIVEWAY PAVEMENT BACK CURB RAMPS AT PRIVATE ENTRANCES MAY BE ELIMINATED IF GRADING CAN BE LONGITUDINAL CURB ACCOMPLISHED WITHOUT THE CURB. EXPANSION JOINT 12'-0" (3.6 m) MIN. & VAR. FOR ALLEYS AND PRIVATE ENTRANCES NOTE: THE ENTRANCE WIDTHS SHOWN ON THE PLANS SHALL BE INTERPRETED TO BE THE WIDTHS (10.7 m) MAX. & VAR. FOR RADTHS AT THE COMPLETED RADIUS, WHICH MAY COMMERCIAL ENTRANCES O POINT BE LOCATED BEHIND THE R.O.W. LINE. (300) R.O.W. LINE -VARIES PROP. P.C.C. SIDEWALK, 5" (125) OR EXIST. SIDEWALK RAMP P.C.C. SIDEWALK, TO REMAIN IN PLACE 18% MAX. LONG. SLOPE 15' (4.6 m) RADIUS (TO FACE) UNLESS OTHERWISE SHOWN IN PLANS -12" (300) 12" (300) ± CENTÉRS GROOVES TOTAL MOTTHER PARTY INTO THE TOTAL TOTAL

<u>PLAN</u>

IN THE OPINION OF THE ENGINEER.

TO HIGH CURBS).

12" (300 & VAR.

SECTION A-A WITH POSITIVE GRADE

12" (300) & VAR.

CONSTRUCTION WILL CREATE A MOWING

PROBLEM (I.E. POINTED AREAS ADJACENT

THICKNESS OF PROP. P.C. CONC. SIDEWALK SHALL BE THE SAME

THICKNESS AS THE P.C. CONC.

DRIVEWAY PAVEMENT

SEE GENERAL NOTE #1

SEE GENERAL NOTE #1

SEE GENERAL NOTES

SEE GENERAL NOTE #1

THICKNESS OF PROP. P.C. CONC. SIDEWALK SHALL BE THE SAME THICKNESS AS THE P.C. CONC.

ALL DIMENSIONS ARE IN

UNLESS OTHERWISE SHOWN.

INCHES (MILLIMETERS)

DRIVEWAY PAVEMENT

C. CONCRETE SIDEWALK, 5" (125) SHALL
POURED BETWEEN THE CURB OF THE GENERAL NOTES DRIVEWAY AND SIDEWALK RAMP, WHERE

1. THIS LONGITUDINAL CURB EXPANSION JOINT SHALL BE PLACED ONLY WHERE THE PROPOSED P.C. CONCRETE SIDEWALK OR DRIVEWAY PAVEMENT MEETS AN EXISTING CONCRETE ENTRANCE. THE P.C.C. DRIVEWAY PAVEMENT WILL BE POURED MONOLITHIC WITH THE P.C.C. SIDEWALK WHEN THE DISTANCE BETWEEN THE SIDEWALK AND RIGHT-OF-WAY IS LESS THAN OR EQUAL TO 2' (0.6 m). THE LONGITUDINAL EXPANSION JOINT BETWEEN THE DRIVEWAY PAVEMENT AND SIDEWALK WILL NOT BE NEEDED UNLESS THE DISTANCE EXCEEDS 2' (0.6 m).

NOTE: SIDEWALK RAMPS SHALL BE CONSTRUCTED AT COMMERCIAL ENTRANCES. AT PRIVATE ENTRANCES RAMPS SHALL USUALLY BE NEEDED

2. THE LONGITUDINAL CURB EXPANSION JOINT SHALL CONFORM TO SECTION 1051 OF THE STANDARD SPECS.

EDGE OF PROP. PAVEMENT

- 3. DRIVEWAYS THAT WILL MEET EXISTING OR PROPOSED CURB NEAR THE R.O.W. LINE SHALL HAVE THE CURB CONSTRUCTED FULL HEIGHT TO THE R.O.W. LINE.
- 4. DRIVEWAYS THAT WILL BE CONSTRUCTED WITH FULL HEIGHT CURBS AND NOT MEETING EXISTING OR PROPOSED CURBS NEAR THE R.O. W. LINE SHALL HAVE THE CURB SLOPED AS SHOWN ABOVE.
- 5. DEPRESSED CURB SHALL BE BUILT ONLY AT PRIVATE DRIVES WITH NO SIDEWALK RAMPS.
- 6. THE ENTRANCE GRADES WILL BE AS SHOWN ON THE STATION CROSS SECTIONS AND AS DIRECTED BY THE ENGINEER.
- 7. THE P.C. CONCRETE DRIVEWAY PAVEMENT SHALL BE CONSTRUCTED WITH SCORED GROOVES. AS SPECIFIED IN ARTICLE 424.06 OF THE STANDARD SPECIFICATIONS, AT APPROXIMATELY EVERY OTHER JOINT, FITHER IN THE EXISTING SIDEWALK OR THE PROPOSED SIDEWALK. THE PROPOSED P.C. CONCRETE SIDEWALK SHALL BE CONSTRUCTED PRIOR TO CONSTRUCTING THE P.C. CONC. DRIVEWAY PAVEMENT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 8. THE COMBINATION CONCRETE CURB AND GUTTER SHALL BE CONSTRUCTED AS SHOWN AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER LIN.FT. (METER) FOR COMBINATION CONCRETE CURB AND GUTTER OF THE TYPE SPECIFIED IN THE PLANS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- 9. THE P.C. CONCRETE SIDEWALK SHALL BE CONSTRUCTED AS SHOWN ABOVE AND WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQ.FT. (m2) FOR P.C. CONCRETE SIDEWALK OF THE THICKNESS SPECIFIED IN THE PLANS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE EXTRA THICKNESS AS SHOWN ABOVE OR THE SEQUENCE OF CONSTRUCTION AS SPECIFIED.
- 10. BEFORE A CHANGE IN THE METHOD OF CONSTRUCTION IS ALLOWED, THE REQUEST SHALL BE SUBMITTED IN WRITING AND MUST BE APPROVED BY THE ENGINEER.
- 11. THE VARIABLE HEIGHT CURB ADJACENT TO THE P.C. CONCRETE DRIVEWAY PAVEMENT SHALL BE CONSTRUCTED MONOLITHIC WITH AND INCLUDED IN THE AREA OF THE P.C. CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED IN THE PLANS. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQ. YD. (m  $^2$ ) FOR P.C. CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED IN THE PLANS, INCLUDING THE MONOLITHIC CURBS AS SPECIFIED, THE SCORED GROOVES, THE LONGITUDINAL CURB EXPANSION JOINTS, AND THE ADDITIONAL THICKNESS REQUIRED ADJACENT TO THE DEPRESSED COMBINATION CONCRETE CURB AND GUTTER. AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

## TYPICAL DETAIL OF MAILBOX TURNOUT IN CURB AND GUTTER SECTION

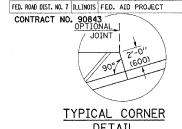
1.5% TO 4% SLOPE (OR SLOPE TO MATCH ADJACENT DRIVE)

TYPICAL CROSS SECTION

THICKNESS AS SHOWN

6'-0" (1.8 m) MIN

8'-0" (2.4 m) MAX.



711 115(BY,BY-1)BR VERMILION 93 68

RTE. SECTION

COUNTY TOTAL SHEE

GENERAL NOTES

LONGITUDINAL CURB

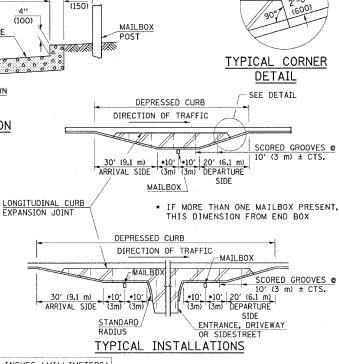
EDGE OF PAVEMENT

1. THE LONGITUDINAL CURB EXPANSION JOINT SHALL CONFORM TO SECTION 1051 OF THE STANDARD SPECIFICATIONS.

DEPRESSED

(900)

- 2. THE MAILBOX TURNOUT CROSS SLOPE WILL BE AS SHOWN ABOVE, AS SHOWN ON THE STATION CROSS SECTIONS, OR AS DIRECTED BY THE
- 3. THE MAILBOX TURNOUT SHALL BE CONSTRUCTED WITH SCORED GROOVES AT APPROXIMATELY 10' (3 m) CENTERS. IN THE EVENT THERE IS EXISTING OR PROPOSED SIDEWALK PRESENT, THESE SCORED GROOVES SHALL BE PLACED IN LINE WITH EVERY OTHER JOINT IN THE ADJACENT SIDEWALK.
- 4. THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (m<sup>2</sup>) FOR P.C. CONCRETE DRIVEWAY PAVEMENT OF THE THICKNESS SPECIFIED IN THE PLANS WHICH PRICE SHALL INCLUDE THE LONGITUDINAL CURB EXPANSION JOINT, MONOLITHIC CURB AS SHOWN, SCORED GROOVES, AND THE ADDITIONAL THICKNESS REQUIRED ADJACENT TO THE DEPRESSED COMBINATION CONCRETE CURB AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



(600)

	NAME DATE		REVISIONS		]
DESIGNED	J.H.M.	6/88	NAME	DATE	Г
CHECKED	F.M.S.	6/88	D.L.P.	10/96	Πí
CADD NO.	A-1.11		K.A.G.	06/03	L.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

## ILLINOIS DEPARTMENT OF TRANSPORTATION ROADWAY DETAILS DRAWN BY DIST. 5 DATE 5/03 CHECKED BY BWC

D.L.P CADD NO. K.A.G 06/03 BLANK, WESSELINK, COOK & ASSOCIATES

NAME

**LEGEND** 

PROP. COMBINATION CONCRETE CURB AND GUTTER

CROSS SLOPE FOR SIDEWALK:

THICKNESS AS

THICKNESS AS SPECIFIED

IN THE PLANS

SECTION A-A WITH NEGATIVE GRADE

-VAR. [-6" (150)

SECTION A-A WITHOUT SIDEWALK

DATE

11/97

IN THE PLANS

SPECIFIED

2% (1/4" PER FT.) (MAX.)

PROPOSED COMBINATION CONCRETE
CURB AND GUTTER (DEPRESSED)

CROSS SLOPE FOR SIDEWALK: 1% (1/8" PER FT.) (MIN.)

PROPOSED COMBINATION CONCRETE

VAR. HEIGHT CURB PRIVATE ENT. FULL HEIGHT CURB COMMERCIAL ENT.

OR AS OTHERWISE SHOWN ON PLANS

CURB AND GUTTER (DEPRESSED)

PROPOSED COMBINATION CONCRETE

NAME DATE

J.M.H. 04/83

CHECKED F.M.S. 04/83

DESIGNED

2% (1/4" PER FT.) (MAX.) SEE NOTE #5

4'-0'' (1.2 m) & VAR.

4'-0" (1.2 m)

SEE NOTE #5

3'-0" (0.9 m)

\* WIDTH OF PROPOSED

GUTTER FLAG

(30 mm)

ENGINEERS - CONSULTANTS

DECATUR, ILLINOIS

FAP ROUTE 711 (US ROUTE 136) SECTION 115(BY, BY-1)BR

VERMILION COUNTY