

GENERAL NOTES

PROJECT DESCRIPTION

THIS PROJECT IS TO ADD DRAINAGE INFRASTRUCTURE TO ELIMINATE PONDING AND FLOODING CONDITIONS AROUND HANGAR AREAS AT TRI-TOWNSHIP AIRPORT. ITEMS INCLUDED IN THIS PROJECT, AMONG OTHER INCIDENTAL WORK, ARE GRADING, DRAINAGE, AND PAVING.

PROTECTION OF EXISTING AIRPORT FACILITIES

THE CONTRACTOR IS TO BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UNDERGROUND AND OVERHEAD UTILITIES AND LIGHTING EQUIPMENT; DRIVEWAY AND ROAD PAVEMENT AND SHOULDERS; RUNWAY, TAXIWAY AND APRON PAVEMENTS AND SHOULDERS; RUNWAY, TAXIWAY AND AIRPORT LIGHTING EQUIPMENT; AND SEEDED AND TURFED AREAS THAT ARE UTILIZED IN OR AFFECTED BY THE CONTRACTOR'S ACTIVITIES. ITEMS DAMAGED BY THE CONTRACTOR ARE TO BE REPAIRED AT CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE.

IN ADDITION, WHEN CONDITIONS DICTATE OR AS DETERMINED BY THE AIRPORT MANAGER OR THE OWNER'S REPRESENTATIVE, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICK-UP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AS PRESCRIBED BY THE SPECIAL PROVISIONS. THE COST OF SWEEPING SHALL BE PAID UNDER AR150530.

CONTRACTOR'S ACCESS AND TEMPORARY FACILITIES

CONTRACTOR'S ACCESS TO THE PROJECT WHEN ON AIRPORT PROPERTY IS SHOWN ON THIS SHEET. CONTRACTOR'S ACCESS TO THE AIRPORT ITSELF IS TO BE PROVIDED BY PUBLIC RIGHTS-OF-WAY. THE CONTRACTOR IS TO SECURE ALL NECESSARY PERMITS FOR THE USE OF ANY PUBLIC RIGHTS-OF-WAY AND IS TO MAINTAIN TRAFFIC ON THESE PUBLIC ROADS AT ALL TIMES, WITH THE COSTS OF PERMITTING, CLEANING AND REPAIRING OF PAVEMENT DAMAGED BY CONTRACTOR'S ACTIVITIES INCIDENTAL TO THE CONTRACT. USE OF AND REPAIRS TO ANY PUBLIC FACILITIES ARE TO BE COMPLETED TO THE SATISFACTION OF THE FACILITY'S OWNER.

THE CONTRACTOR IS TO PROVIDE TEMPORARY CONSTRUCTION ROADS WITHIN THE CONSTRUCTION LIMIT LINES AS MAY BE REQUIRED BY HIS ACTIVITIES. HEAVY VEHICLES SHALL NOT CROSS EXISTING PAVEMENT SURFACES EXCEPT AS APPROVED BY THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. ANY DAMAGE TO PAVEMENTS THAT MAY OCCUR BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. FOR HAUL ROUTES MADE BY THE CONTRACTOR THROUGH GRASSED AREAS, CONTRACTOR SHALL GRADE, LEVEL, TOPSOIL, SEED AND MULCH AT THE END OF THE PROJECT, COST INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR IS TO PROVIDE AN EQUIPMENT STORAGE AND PARKING AREA AT THE LOCATIONS AS SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE ACCESS ROADS AND THE STORAGE AREA DURING CONSTRUCTION AND TO RESTORE THE AREAS AT PROJECT COMPLETION TO CONDITIONS SUITABLE TO THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. AT THE AIRPORT MANAGER'S DISCRETION, THE TEMPORARY FACILITIES MAY REMAIN, BUT THEY MUST BE LEFT IN CONDITIONS SUITABLE TO THE AIRPORT MANAGER. THE COST OF PROVIDING, MAINTAINING AND RESTORING THE TEMPORARY FACILITIES IS INCIDENTAL TO THE CONTRACT.

RESPONSIBILITY FOR EXISTING UTILITIES

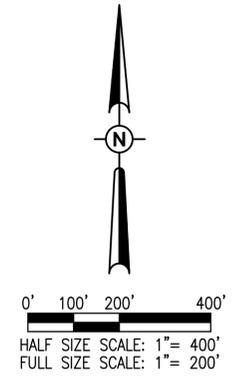
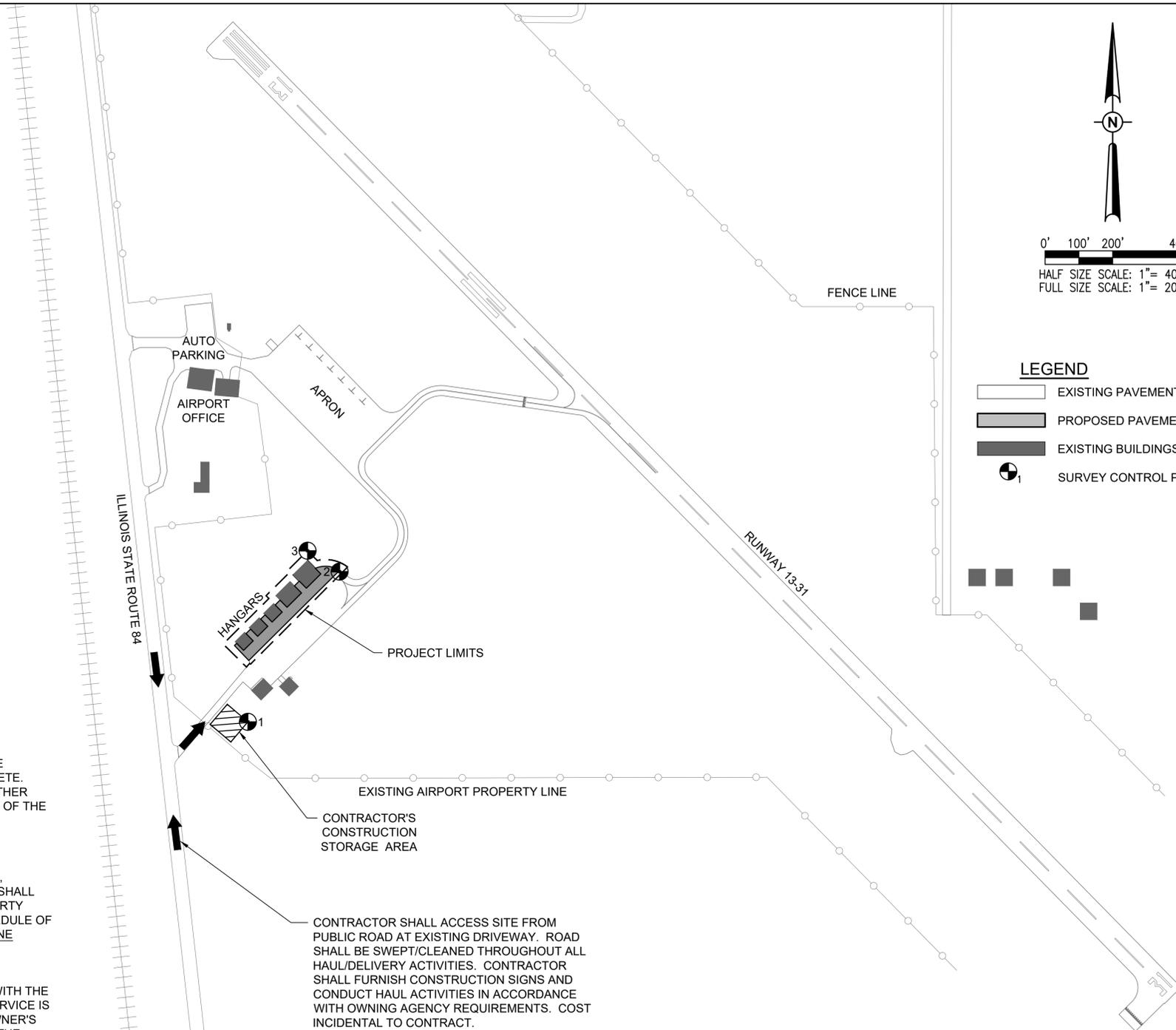
THE LOCATION, SIZE AND/OR TYPE OF MATERIAL OF EXISTING UNDERGROUND OR OVERHEAD UTILITIES AS MAY BE INDICATED ON THESE CONSTRUCTION PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE PROJECT ENGINEER HAVE INDEPENDENTLY VERIFIED THIS INFORMATION AND NEITHER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, SUFFICIENCY OR COMPLETENESS OF THE INFORMATION AND GIVE NO EXPRESSED OR IMPLIED GUARANTEE THAT ANY CONDITIONS INDICATED ARE REPRESENTATIVE OF ACTUAL CONDITIONS TO BE ENCOUNTERED.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AND AGENCIES OF HIS CONSTRUCTION PLANS AND SHALL OBTAIN FROM EACH PARTY DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF ALL UTILITIES AND THE WORKING SCHEDULE OF ANY REMOVALS OR ADJUSTMENTS REQUIRED OF THE UTILITY. THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (PHONE 800-892-0123) TO ASSIST IN THE ABOVE.

THE CONTRACTOR SHALL PROTECT ANY FACILITIES TO THE SATISFACTION OF THE UTILITY OR OWNING-AGENCY WITH THE COST OF ANY REQUIRED PROTECTION TO BE INCIDENTAL TO THE CONTRACT. IN THE EVENT A UTILITY LINE OR SERVICE IS UNEXPECTEDLY ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE AND THE UTILITY COMPANY OR AGENCY OF JURISDICTION. ANY SUCH UTILITIES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO SERVICE AT ONCE.

NOTES

- VERTICAL COORDINATES ARE IN NGVD 29. HORIZONTAL COORDINATES ARE IN STATE PLANE NAD 83 ILLINOIS WEST.
- STATIONS, OFFSETS AND ELEVATIONS SHOWN ARE IN FEET.
- THE AIRPORT REFERENCE CODE FOR RUNWAY 13-31 IS B-II. RUNWAY 13 HAS A NON-PRECISION APPROACH WITH VISIBILITY MINIMUM OF GREATER THAN 3/4 MILE WHILE RUNWAY 31 HAS A VISUAL APPROACH.



LEGEND

- EXISTING PAVEMENT
- PROPOSED PAVEMENT
- EXISTING BUILDINGS
- SURVEY CONTROL POINT

CONTROL POINTS					
POINT #	DESCRIPTION	GROUND ELEVATION	OBJECT ELEVATION	LATITUDE	LONGITUDE
1	STAGING AREA	609.09	634.09	042° 02' 38.94"	-090° 06' 46.25"
2	EQUIPMENT	613.79	638.79	042° 02' 43.32"	-090° 06' 42.65"
3	EQUIPMENT	614.09	638.10	042° 02' 43.93"	-090° 06' 43.91"

NOTES

- MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT IS 25 FEET FOR ALL WORK, EXCEPT BY PRIOR APPROVAL OF THE ENGINEER (SEE SPECIAL PROVISIONS).
- TRAFFIC TO BE MAINTAINED ON ALL AIRPORT ROADWAYS AT ALL TIMES.
- FOR SAFETY DIMENSIONS, SEE PHASING PLANS.
- FOR STAGING INFORMATION, SEE PHASING PLAN.



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TRI-TOWNSHIP AIRPORT AUTHORITY
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Savanna, Illinois 61074

MODIFY AIRCRAFT HANGAR AREA DRAINAGE

SBGP No:
3-17-SBGP-162/171/184
IDA No: SFY-4966

Contract No. TR013

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JANUARY 13, 2023

PROJECT NO: 21A0142

CAD FILE: C-101-SOW.DWG

DESIGN BY: HLE 10/07/2022

DRAWN BY: LDH 10/07/2022

REVIEWED BY: LDH 1/13/23

SHEET TITLE

SCOPE OF WORK



PROJECT IS LOCATED IN NORTHWEST 1/4 OF SECTION 25, SAVANNA TOWNSHIP, CARROLL COUNTY

JAN 16, 2023 10:56 AM ENGEL01909 1:21:00S21A0142\CAD\AIRPORT\1\SHEETC-101-SOW.DWG

CONSTRUCTION AND SAFETY NOTES

SAFETY IS REQUIRED

CONSTRUCTION OF THE PROJECT SHALL BE PERFORMED BY THE CONTRACTOR IN ACCORDANCE WITH THE GUIDELINES SPECIFIED IN FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT ISSUE) AND THE AIRPORT RULES AND REGULATIONS. BEFORE ANY NOTICE TO PROCEED WILL BE ISSUED, THE CONTRACTOR IS REQUIRED TO SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) TO THE AIRPORT OPERATOR FOR APPROVAL. ANY CONTRACTOR ACTIVITIES REQUIRED FOR PROJECT SAFETY SHALL BE PROVIDED BY THE CONTRACTOR AND BE INCIDENTAL TO THE CONTRACT.

SEQUENCE OF CONSTRUCTION

TO MINIMIZE DISRUPTIONS OF AIRPORT OPERATIONS, CONSTRUCTION OPERATIONS MUST BE CONTROLLED THROUGHOUT THE PROJECT'S DURATION, AND WORK MUST BE COMPLETED EXPEDITIOUSLY. A CONSTRUCTION PHASING PLAN DETAILING THE SEQUENCING OF THE CONTRACTOR'S WORK THROUGHOUT THE PROJECT IS INCLUDED IN THE PLANS. THE CONTRACTOR SHALL PROVIDE HIS WRITTEN ACCEPTANCE OF THE PROJECT CONSTRUCTION PHASING PLAN AT THE PRE-CONSTRUCTION CONFERENCE. ANY AND ALL CHANGES TO THE CONSTRUCTION PHASING PLAN THAT MAY BE REQUESTED BY THE CONTRACTOR MUST BE APPROVED BY THE PROJECT ENGINEER AND THE AIRPORT OWNER AND REVIEWED BY THE FAA. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE SUFFICIENT ADVANCE NOTICE OF ANY PROPOSED PHASING CHANGE TO PERMIT CONSIDERATION AND APPROVAL BY THE PROJECT ENGINEER, AIRPORT OWNER AND THE FAA. THE CONTRACTOR SHALL NOT BE ENTITLED TO ANY EXTRA COMPENSATION, NOR EXTENSION TO THE CONTRACT TIME, BECAUSE OF A PHASING CHANGE REQUEST NOR FOR ANY TIME NECESSARY IN RECEIVING THE REQUIRED APPROVALS. THE CONTRACTOR SHALL EXPEDITE WORK AT THOSE STAGES WHERE ACTIVE TAXIWAYS, HANGAR ACCESS, APRONS, ROADWAYS OR PARKING LOTS MUST BE CLOSED, TO MINIMIZE THE LENGTH OF TIME THAT AIRPORT OPERATIONS ARE RESTRICTED.

AT THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL PROVIDE A CONTRACTOR COORDINATION PLAN THAT COORDINATES HIS WORK WITH THE WORK OF HIS SUBCONTRACTORS AND THE WORK OF OTHER CONTRACTORS OF OTHER ON-GOING AIRPORT PROJECTS.

RUNWAY CLOSURE

NO RUNWAY CLOSURES WILL BE PERMITTED AS PART OF THIS PROJECT.

TEMPORARY BARRICADES

THE CONTRACTOR SHALL FURNISH BARRICADES FOR ANY AIRFIELD OR ROADWAY PAVEMENT TO BE CLOSED BY HIS WORK. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO FURNISH, PLACE AND MAINTAIN BARRICADES AS SHOWN IN DETAIL B, THIS SHEET, AND AS DIRECTED BY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR. THE COST OF THESE ITEMS, AND THEIR MAINTENANCE, IS TO BE PAID FOR UNDER AR150530 TRAFFIC MAINTENANCE. ANY WORK THAT REQUIRES PORTIONS OF AN ACTIVE RUNWAY, TAXIWAY OR APRON TO BE CLOSED MUST BE COMPLETED EXPEDITIOUSLY TO MINIMIZE DISRUPTION TO AIRCRAFT OPERATIONS.

VEHICULAR TRAFFIC CONTROL

THE CONTRACTOR SHALL ERECT AND MAINTAIN, AT NO COST TO THE CONTRACT, DIRECTIONAL AND INFORMATIONAL SIGNS FOR THE CONTRACTOR'S ACCESS ROUTES AT THE EXISTING CONSTRUCTION ENTRANCES AND FOR THE CONTRACTOR'S ROUTE WITHIN THE AIRPORT OPERATIONS AREA, AS NOTED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. WHERE CONTRACTOR EQUIPMENT IS OPERATING WITHIN ACTIVE AIRCRAFT OPERATIONS AREAS, RADIO-EQUIPPED FLAGGERS SHALL BE FURNISHED BY THE CONTRACTOR. CONTINUOUS PAVEMENT SWEEPING SHALL BE FURNISHED TO REMOVE DEBRIS FROM ACTIVE AIRCRAFT MOVEMENT PATHS. THE COST OF TRAFFIC CONTROL/FLAGGERS AND PAVEMENT SWEEPING SHALL BE PAID UNDER AR150530 TRAFFIC MAINTENANCE.

AIRFIELD OPERATIONAL SAFETY DURING CONSTRUCTION

THE CONTRACTOR SHALL NOT HAVE ACCESS TO ANY PART OF THE ACTIVE AIRFIELD (RUNWAYS, TAXIWAYS OR APRONS) FOR ANY EQUIPMENT OR PERSONNEL WITHOUT THE APPROVAL OF THE RESIDENT ENGINEER AND THE AIRPORT OWNER. ACTIVITIES WITHIN THE AIRPORT OPERATIONS AREA (AOA) ARE SUBJECT TO FEDERAL ACCESS CONTROL. BECAUSE OF THE HIGH REQUIREMENTS FOR AIRPORT SECURITY AND SAFETY, THE FOLLOWING REQUIREMENTS MUST BE ADHERED TO:

- ALL EMPLOYEES OF THE CONTRACTOR SHALL PARK THEIR PERSONAL VEHICLES IN THE DESIGNATED EQUIPMENT PARKING AND STORAGE AREA. EACH PERSON OR VEHICLE ENTERING THE CONTRACTOR AREA SHALL DO SO IN ACCORDANCE WITH THE POLICIES AND PROCEDURES OF THE AIRPORT OWNER. THE CONTRACTOR WILL TRANSPORT THE WORKERS FROM THE PARKING AREAS TO THE WORK AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE OF THE PROPOSED EQUIPMENT STORAGE AND PARKING AREAS.
- SHOULD ANY CONTRACTOR PERSONNEL BE IDENTIFIED AS NONCOMPLIANT WITH ANY VEHICLE DRIVING SAFETY REQUIREMENTS IN THIS PROJECT SAFETY PLAN OR IN THE AIRPORT VEHICLE OPERATIONS REGULATIONS, SUCH DRIVERS SHALL BE PENALIZED BY RESCISSION OF THEIR ON-AIRPORT DRIVING PRIVILEGES, AND THEIR ACCESS TO THE CONSTRUCTION LIMIT AREA WHEN OPERATING VEHICLES SHALL BE REVOKED.
- THE CONTRACTOR WILL BE REQUIRED TO BE IN CONTACT WITH AIRPORT OPERATIONS. THIS WILL KEEP THE CONTRACTOR IN CONTACT WITH AIRPORT PERSONNEL AND ENABLE THE AIRPORT PERSONNEL TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTICAL EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

THE CONTRACTOR SHALL REMAIN WITHIN THE CONSTRUCTION LIMITS LINE SHOWN IN THE PLANS. WHEN OUTSIDE THESE LIMITS, ALL CONTRACTOR ACTIVITIES SHALL REMAIN MORE THAN 250 FEET FROM THE CENTERLINE AND 300 FEET FROM THE END OF ACTIVE RUNWAY 13-31. FOR WORK NEAR TAXIWAYS AND APRONS, THE CONTRACTOR'S PERSONNEL AND EQUIPMENT MUST REMAIN AT LEAST 44.5 FEET FROM CENTERLINE OF ACTIVE CATEGORY I TAXIWAYS, 65.5 FEET FROM ACTIVE CATEGORY II TAXIWAYS, AND TEN (10) FEET FROM ACTIVE APRONS. WHEN CONSTRUCTION OPERATIONS MUST BE CONDUCTED WITHIN THESE SEPARATIONS, THE PAVEMENT MUST BE CLOSED TO AIRCRAFT ACTIVITY BY THE CONTRACTOR BY PROVIDING TEMPORARY BARRICADES AS SHOWN IN THE PLANS, AND IN THE CASE OF RUNWAY PAVEMENTS, CLOSED RUNWAY MARKERS. WHEN HAUL VEHICLES ARE PERMITTED TO CROSS ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL PROVIDE POSITIVE CONTROL OF CONSTRUCTION VEHICLES USING RADIO-EQUIPPED FLAGGERS. CONTRACTOR SHALL ESTABLISH AND MAINTAIN RADIO CONTACT WITH TRI-TOWNSHIP AIRPORT CTAU/UNICOM (122.7 MHz). ALL CONTRACTOR'S EQUIPMENT USED IN ACTIVE AIRPORT OPERATIONS AREAS SHALL BE EQUIPPED WITH A FAA-STANDARD FLAG, AS REFERENCED IN FAA AC 150/5370-2, CURRENT ISSUE. AIRCRAFT SHALL HAVE THE RIGHT-OF-WAY. ALL TAXIWAY WITHIN THE PROJECT LIMITS ARE CATEGORY I.

THE CONTRACTOR SHALL KEEP ALL OF HIS EQUIPMENT AND PERSONNEL AT LEAST 15 FEET FROM THE EDGE OF ANY ACTIVE ROADWAY OR AUTO PARKING PAVEMENT. WHEN HIS ACTIVITIES REQUIRE WORKING WITHIN 15 FEET OF THE ROAD/PAVEMENT EDGE, THE CONTRACTOR SHALL PROVIDE FOR TRAFFIC CONTROL IN ACCORDANCE WITH IDOT SPECIFICATIONS (HIGHWAY STANDARDS).

OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL AT THE CONSTRUCTION SITE SHALL BE DELINEATED WITH THE USE OF BARRICADES DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS. NO OPEN TRENCHES SHALL BE ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR THE TAXIWAY SAFETY AREA (TSA) WHEN THE RUNWAY OR TAXIWAY IS OPEN TO AIR TRAFFIC (INCLUDING OVERNIGHT). THE RSA IS DEFINED AS 75 FEET FROM THE CENTERLINE AND 300 FEET FROM THE END OF RUNWAY 13-31. THE TSA IS MEASURED AT 24.5 FEET FROM THE CATEGORY I TAXIWAY CENTERLINE AND 39.5 FEET FROM THE CATEGORY II TAXIWAY CENTERLINE. NO VERTICAL DROP OF GREATER THAN 3-INCHES IN HEIGHT FROM PAVEMENT EDGE TO EARTH GRADE OR EARTH GRADE TO EARTH GRADE WITHIN THE RSA OR TSA WILL BE PERMITTED WHEN THE RUNWAY OR TAXIWAY IS OPEN TO AIR TRAFFIC. THE CONTRACTOR WILL HAVE STEEL PLATES ON-SITE TO ALLOW FOR THE RAPID COVERING OF TRENCHES OR EARTH DROPS IN THE EVENT OF UNEXPECTED WORK STOPPAGES FOR WEATHER OR AIRPORT EMERGENCIES. ALL TAXIWAYS WITHIN THE PROJECT LIMITS ARE CATEGORY I.

WHEN NOT IN USE AND DURING NONWORKING HOURS, CONTRACTOR'S EQUIPMENT SHALL BE PARKED WITHIN THE CONTRACTOR'S EQUIPMENT STORAGE AND PARKING AREAS. THE EQUIPMENT STORAGE AND PARKING AREAS ARE TO BE LOCATED AS SHOWN ON THE PHASING PLAN. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING THE CONSTRUCTION ENTRANCES IN GOOD CONDITION. THE COST OF MAINTAINING THE CONSTRUCTION ENTRANCE AND CONTRACTOR AREAS IS TO BE INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL PROTECT ALL EXISTING PAVEMENT EDGES FROM DAMAGE FROM CONSTRUCTION EQUIPMENT AND HAUL VEHICLES.

AT NO TIME SHALL THE CONTRACTOR CONDUCT ANY ACTIVITIES OR OPERATE OR PARK EQUIPMENT SO AS TO OBSTRUCT ACTIVE PART 77 AIRPORT IMAGINARY SURFACES OR THE RUNWAY PROTECTION ZONES (RPZ) AS DELINEATED IN THE PLANS. CONTRACTOR'S EQUIPMENT SHALL EXTEND NO HIGHER THAN 25 FEET. CRANES SHALL NOT BE USED DURING INSTRUMENT WEATHER CONDITIONS OR AT NIGHT. CRANES SHALL BE LOWERED WHEN NOT IN USE.

BEFORE REOPENING TEMPORARILY CLOSED PAVEMENTS, THE CONTRACTOR SHALL INSPECT AND CLEAN, AS NECESSARY, THE PAVEMENT TO ASSURE THAT NO MATERIALS OR OBJECTS THAT MAY DAMAGE AIRCRAFT OR VEHICLES REMAIN. ANY REQUIRED CLEANING SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT OWNER AND IS INCIDENTAL TO THE CONTRACT.

ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE APPROVED PROJECT SAFETY PLAN, ISSUED BY THE ILLINOIS DIVISION OF AERONAUTICS.

FAILURE TO USE THESE PRESCRIBED PROCEDURES OR ADHERE TO THE SAFETY REQUIREMENTS WILL RESULT IN THE SUSPENSION OF WORK.

NOTIFICATIONS BY CONTRACTOR

THE CONTRACTOR MUST NOTIFY THE RESIDENT ENGINEER AND THE AIRPORT OWNER 3 DAYS IN ADVANCE OF ANY REQUIRED PARTIAL OR COMPLETE CLOSING OF ANY RUNWAY, TAXIWAY OR APRON. THE DATE, TIME AND SCHEDULED DURATION OF THE CLOSING MUST BE APPROVED BY THE RESIDENT ENGINEER AND THE AIRPORT OWNER. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT OWNER 3 DAYS IN ADVANCE OF THE CONTRACTOR'S CLOSING OF OTHER ACTIVE ROADWAYS, AIRFIELD OR ROADWAY LIGHTING CIRCUITS, OR OTHER AIRPORT FACILITIES.

CONTRACTOR'S USE OF SITE

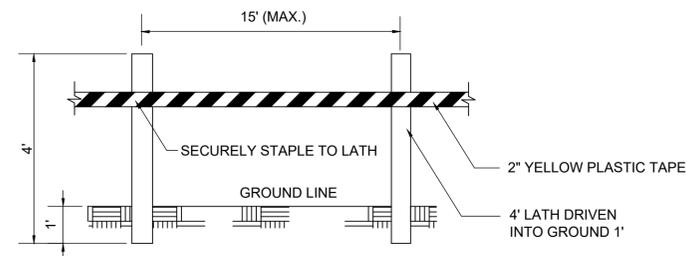
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THE CONTRACTOR IS TO PROVIDE TEMPORARY CONSTRUCTION ROADS WITHIN THE CONSTRUCTION LIMIT LINES AS MAY BE REQUIRED BY HIS ACTIVITIES. HEAVY VEHICLES SHALL NOT CROSS EXISTING PAVEMENT SURFACES EXCEPT AS APPROVED BY THE AIRPORT OWNER AND THE RESIDENT ENGINEER. ANY DAMAGE TO PAVEMENTS THAT MAY OCCUR BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT OWNER AND THE RESIDENT ENGINEER. FOR HAUL ROUTES MADE BY CONTRACTOR THROUGH GRASSED AREAS, CONTRACTOR SHALL GRADE, LEVEL, TOPSOIL, SEED AND MULCH AT THE END OF THE PROJECT, COST INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR IS TO PROVIDE AN EQUIPMENT STORAGE AND PARKING AREA AT THE LOCATIONS SHOWN IN THE PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE ACCESS ROADS AND THE STORAGE AREA DURING CONSTRUCTION AND TO RESTORE THE AREAS AT PROJECT COMPLETION TO CONDITIONS SUITABLE TO THE AIRPORT OWNER AND THE RESIDENT ENGINEER. AT THE AIRPORT OWNER'S DISCRETION, THE TEMPORARY FACILITIES MAY REMAIN, BUT THEY MUST BE LEFT IN CONDITIONS SUITABLE TO THE AIRPORT OWNER. THE COST OF PROVIDING, MAINTAINING AND RESTORING THE TEMPORARY FACILITIES IS INCIDENTAL TO THE CONTRACT.

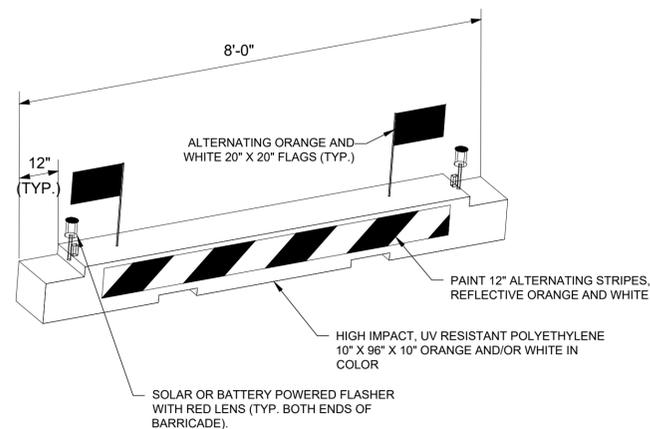
UTILITY OUTAGES AND SHUTDOWNS

THE CONTRACTOR SHALL PROVIDE 3 DAYS PRIOR NOTICE OF ANY OUTAGES OR SHUTDOWNS TO THE OWNER AND THE AGENCY OWNING THE AFFECTED UTILITY. THE CONTRACTOR SHALL PROVIDE ANY TEMPORARY CONNECTIONS OR OTHER MEASURES AS MAY BE REQUIRED TO MAINTAIN SERVICE AS MAY BE REQUIRED BY THE OWNING AGENCY AT NO COST TO THE OWNER.



MATERIALS ARE TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION. COST OF MATERIALS, INSTALLATION, RELOCATION AND MAINTENANCE OF LATHING AND WARNING TAPE IS TO BE INCIDENTAL TO THE CONTRACT.

**DETAIL A
LATHING AND WARNING TAPE**



**DETAIL B
LOW PROFILE AIRCRAFT BARRICADE**

BARRICADE NOTES

- ALL CONSTRUCTION SIGNS AND TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES INCLUDING THE ILLINOIS SUPPLEMENT (LATEST EDITION) AND THE FAA ADVISORY CIRCULARS (LATEST EDITION) UNLESS NOTED OTHERWISE. THE FAA OR MORE STRINGENT SPECIFICATIONS SHALL GOVERN.
- BARRICADES SHALL BE INTERLOCKED END TO END OVER THE LENGTH OF THE PAVEMENT WHERE PROTECTING OPEN RUNWAYS, AND SPACED END TO END A MAXIMUM OF 4 FEET IN OTHER ALL OTHER AREAS. BARRICADES ARE TO BE SET BACK FROM THE ACTIVE RUNWAY OR TAXIWAY CENTERLINE THE DISTANCE AS SHOWN ON THE PLANS.
- CONSTRUCTION RED WARNING LIGHT: THESE ARE PORTABLE, LENS DIRECTED, ENCLOSED LIGHTS. THE COLOR OF THE LIGHT EMITTED SHALL BE RED. THEY MAY BE USED IN EITHER A STEADY BURN (TYPE C) OR LOW INTENSITY FLASHING MODE (TYPE A) UNLESS NOTED OTHERWISE.
- THE LIGHTING SHALL BE MAINTAINED IN OPERATION DURING THE HOURS OF DARKNESS BETWEEN 1/2 HOUR AFTER SUNSET AND 1/2 HOUR BEFORE SUNRISE AND WHEN CONDITIONS EXIST WHICH TEND TO OBSCURE VISION.
- BARRICADES SHALL BE SECURED TO THE GROUND BY APPROVED METHODS TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS.
- THE ONLY COLOR COMBINATION ON BARRICADES IS ORANGE AND WHITE. THE ORANGE STRIPES SHALL BE ENCAPSULATED LENS REFLECTIVE SHEETING. THE WHITE STRIPES SHALL BE EITHER ENCAPSULATED OR ENCLOSED LENS REFLECTIVE SHEETING AND MUST BE IN ACCEPTABLE CONDITION.
- COST FOR PROVIDING, PLACING, MAINTAINING, RELOCATING AND REMOVING BARRICADES SHALL BE INCLUDED IN ITEM AR150530 TRAFFIC MAINTENANCE.

MODIFY AIRCRAFT HANGAR AREA DRAINAGE

SBGP No:
3-17-SBGP-162/171/184
IDA No: SFY-4966

Contract No. TR013

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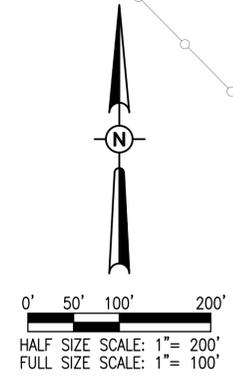
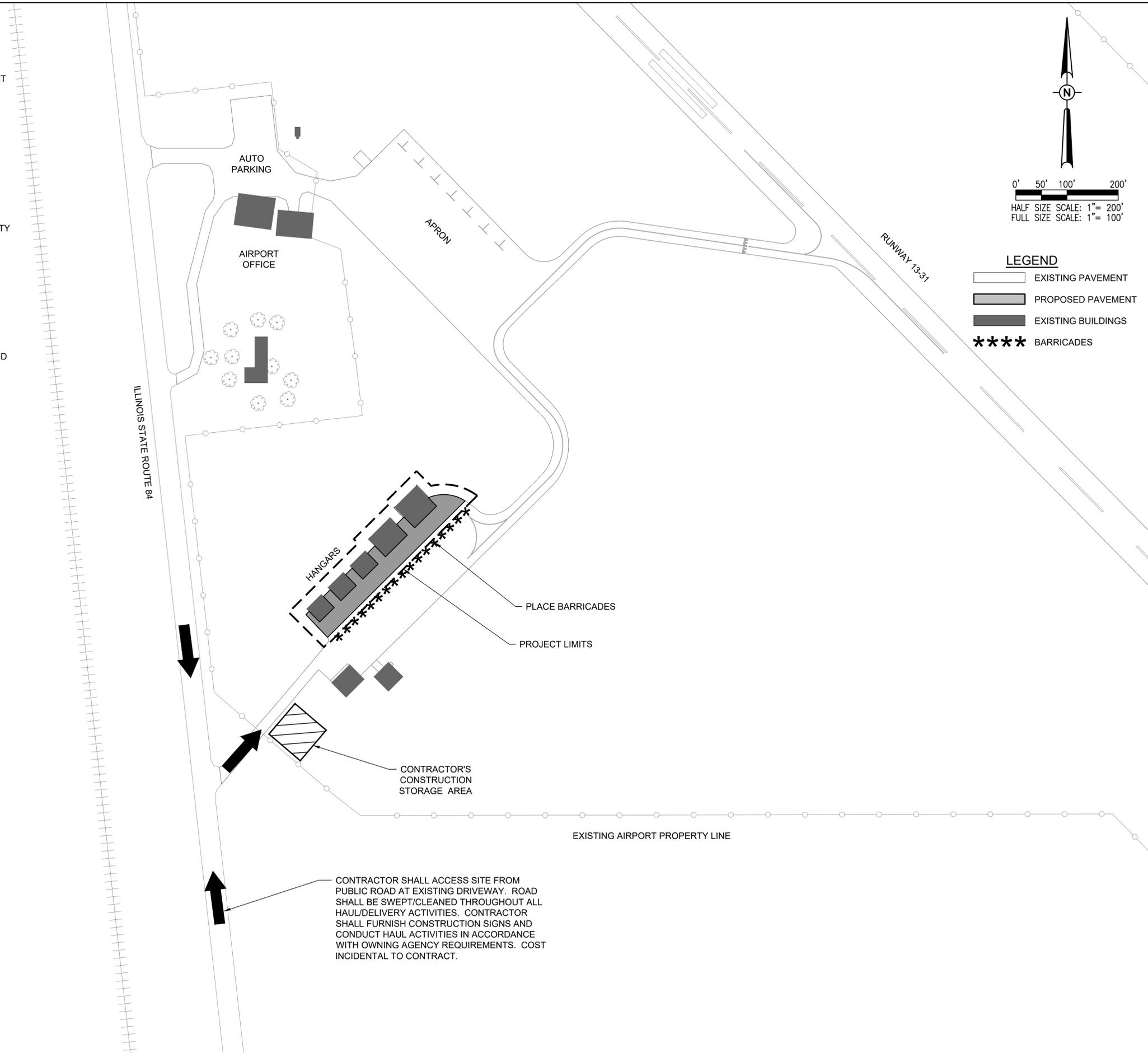
CONSTRUCTION SAFETY NOTES

NOTES

1. ALL CONTRACTOR ACTIVITIES SHALL TAKE PLACE WITHIN CONSTRUCTION LIMIT LINES AS SHOWN.
2. ALL CONSTRUCTION EQUIPMENT WILL BE LIMITED TO A HEIGHT OF 25 FEET UNLESS PRIOR APPROVAL IS GIVEN BY THE ENGINEER (SEE SPECIAL PROVISIONS).
3. CONTRACTOR'S EQUIPMENT MAY NOT DISRUPT FLIGHT OPERATIONS ON RUNWAY 13-31 AT ANY TIME.
4. TRAFFIC TO BE MAINTAINED ON AIRPORT ROADWAYS AT ALL TIMES.
5. CONTRACTOR VEHICLES SHALL OPERATE IN ACCORDANCE WITH SPECIAL PROVISIONS SECTION 40 WHEN WITHIN THE AIRPORT OPERATIONS AREA.
6. SEE SCOPE OF WORK ON SHEET 3 AND CONSTRUCTION SAFETY NOTES ON SHEET 4.

THIS PROJECT IS TO BE COMPLETED IN ONE PHASE, WORK ITEMS INCLUDE:

1. INSTALLATION OF PROVISIONS OF TRAFFIC MAINTENANCE AND EROSION CONTROL
2. FULL-DEPTH PAVEMENT REMOVAL
3. GRADING
4. INSTALLATION OF DRAINAGE INFRASTRUCTURE (TRENCH DRAIN, STORM SEWER, AND INFILTRATION TRENCH)
5. PLACE AGGREGATES AND HMA PAVEMENT
6. SEEDING, MULCHING AND TOPSOILING



LEGEND

	EXISTING PAVEMENT
	PROPOSED PAVEMENT
	EXISTING BUILDINGS
	BARRICADES

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SHEET TITLE

CONSTRUCTION SAFETY AND PHASING PLAN

**MODIFY AIRCRAFT
HANGAR AREA
DRAINAGE**

SBGP No:
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IDA No: SFY-4966

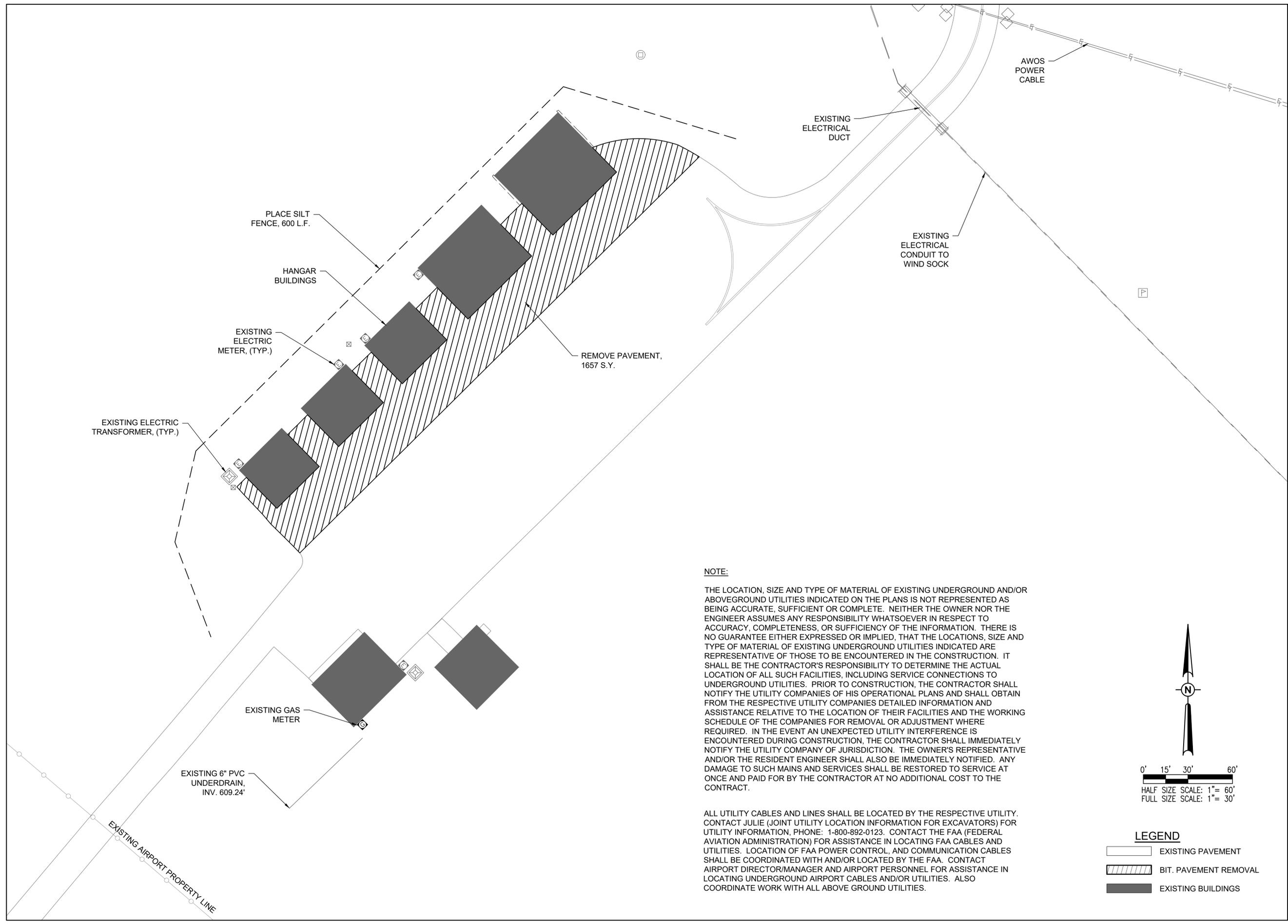
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DRAWN BY: LDH 10/10/2022
REVIEWED BY: LDH 1/13/23

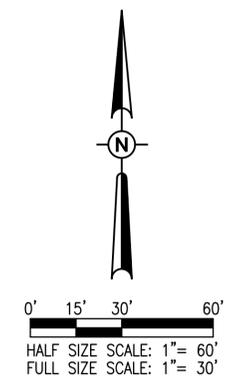
SHEET TITLE

REMOVAL PLAN



NOTE:
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

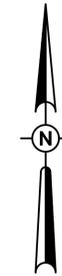
ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVE GROUND UTILITIES.



LEGEND

	EXISTING PAVEMENT
	BIT. PAVEMENT REMOVAL
	EXISTING BUILDINGS

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I:\21\085\21\A0142\CAD\AIRPORT\1\SHEETC-105-DEM.DWG



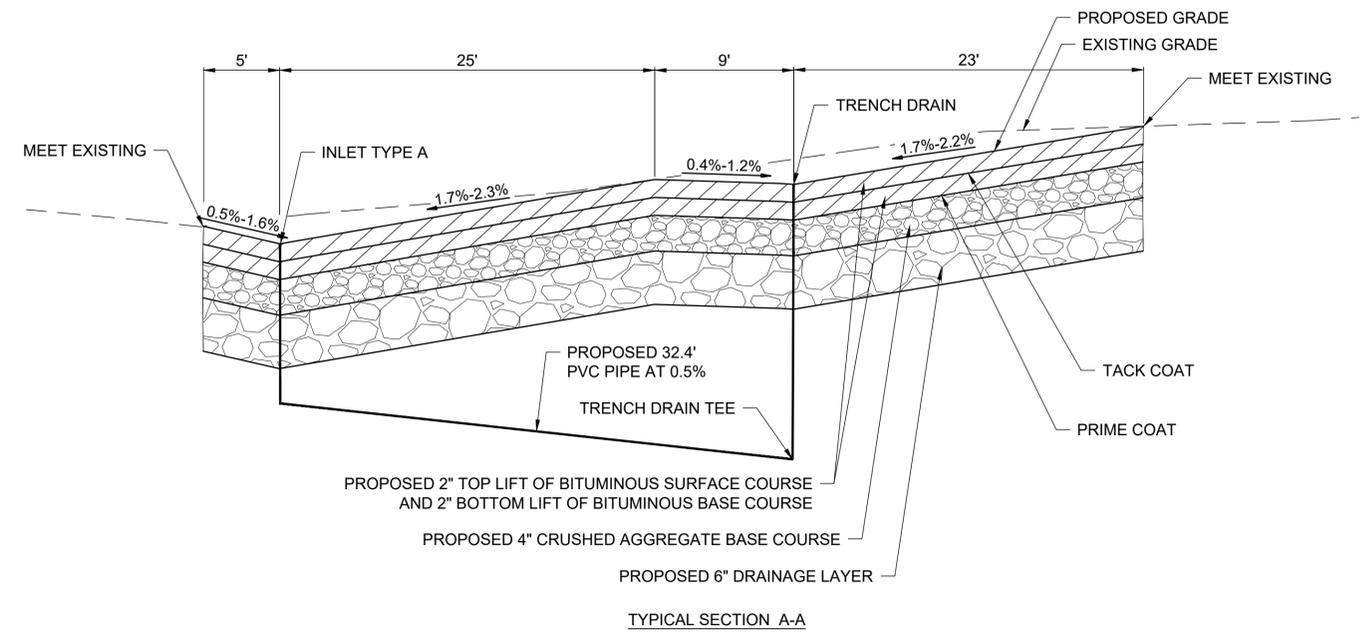
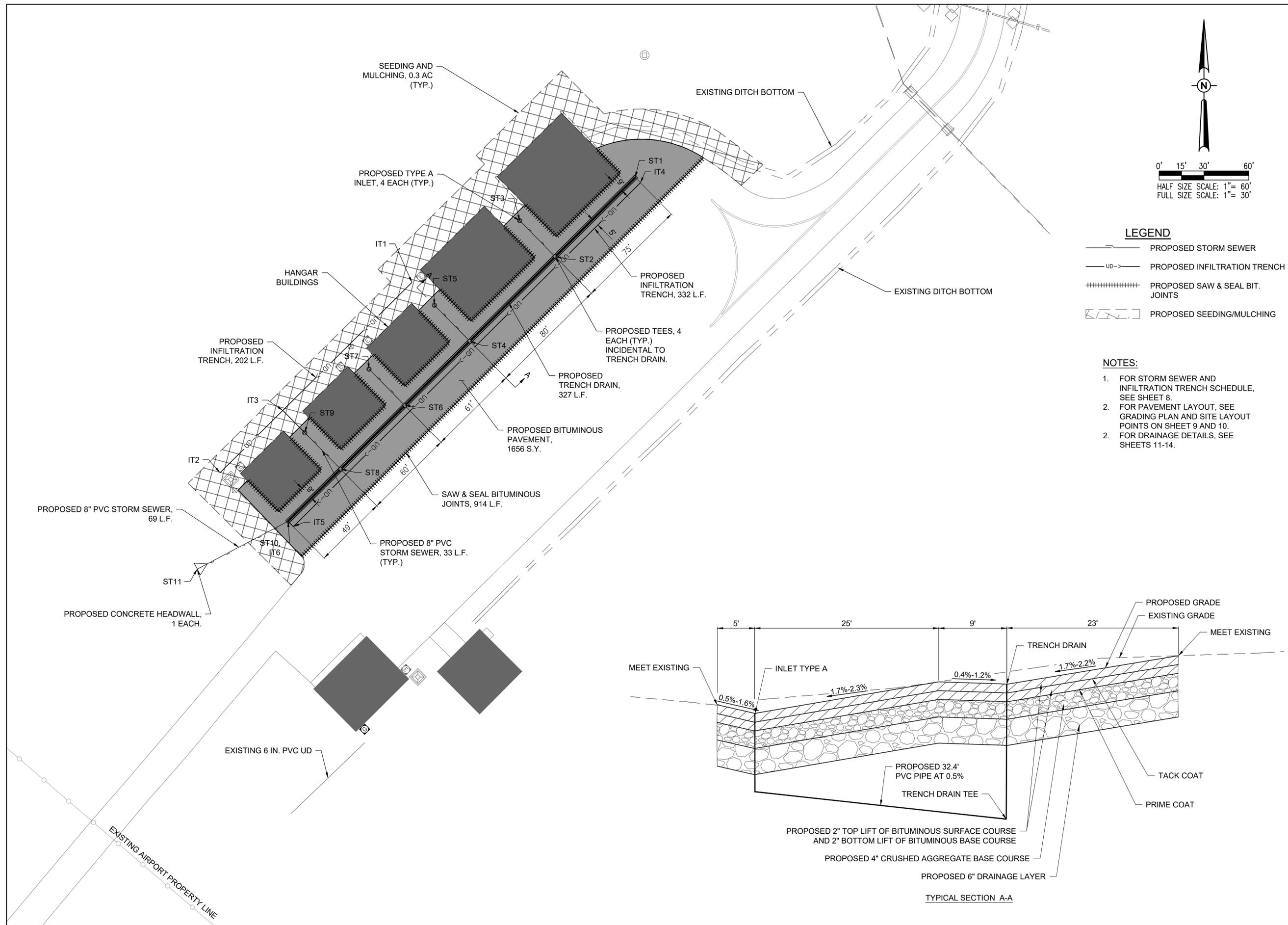
0' 15' 30' 60'
HALF SIZE SCALE: 1" = 60'
FULL SIZE SCALE: 1" = 30'

LEGEND

- PROPOSED STORM SEWER
- PROPOSED INFILTRATION TRENCH
- PROPOSED SAW & SEAL BIT JOINTS
- PROPOSED SEEDING/MULCHING

NOTES:

1. FOR STORM SEWER AND INFILTRATION TRENCH SCHEDULE, SEE SHEET 8.
2. FOR PAVEMENT LAYOUT, SEE GRADING PLAN AND SITE LAYOUT POINTS ON SHEET 9 AND 10.
2. FOR DRAINAGE DETAILS, SEE SHEETS 11-14.



MODIFY AIRCRAFT HANGAR AREA DRAINAGE

SBGP No:
3-17-SBGP-162/171/184
IDA No: SFY-4966

Contract No. TR013

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: JANUARY 13, 2023
PROJECT NO: 21A0142
CAD FILE: C-106-DRN.DWG
DESIGN BY: HLE 10/07/2022
DRAWN BY: LDH 10/07/2022
REVIEWED BY: LDH 1/13/23

SHEET TITLE

PROPOSED PLAN

STORM SEWER SCHEDULE

STRUCTURE	NORTHING	EASTING	DESCRIPTION	RIM EL.	INVERT EL.	PIPE LENGTH	SIZE	TYPE	INV. SLOPE
ST-1	1959051.08	2311427.32	TRENCH DRAIN - BEGIN	613.98	611.44				
						74.2	8.0 *	TRENCH DRAIN	0.50%
ST-2	1958998.22	2311373.79	TEE	613.62	611.07				
ST-3	1959022.27	2311350.04	INLET - TYPE A	613.15	611.23				
						32.5	8.0	PVC	0.50%
ST-2	1958998.22	2311373.79	TEE	613.62	611.07				
						78.3	8.0 *	TRENCH DRAIN	0.50%
ST-4	1958941.81	2311316.66	TEE	613.39	610.68				
ST-5	1958965.86	2311292.92	INLET - TYPE A	613.00	610.84				
						32.4	8.0	PVC	0.50%
ST-4	1958941.81	2311316.66	TEE	613.39	610.68				
						59.1	8.0 *	TRENCH DRAIN	0.50%
ST-6	1958898.90	2311273.20	TEE	613.13	610.38				
ST-7	1958923.20	2311249.20	INLET - TYPE A	612.72	610.55				
						32.8	8.0	PVC	0.50%
ST-6	1958898.90	2311273.20	TEE	613.13	610.38				
						58.4	8.0 *	TRENCH DRAIN	0.50%
ST-8	1958856.47	2311230.23	TEE	612.91	610.09				
ST-9	1958880.82	2311206.19	INLET - TYPE A	612.52	610.25				
						32.9	8.0	PVC	0.50%
ST-8	1958856.47	2311230.23	TEE	612.91	610.09				
						48.5	8.0 *	TRENCH DRAIN	0.50%
ST-10	1958821.71	2311195.02	TRENCH DRAIN - END	612.81	609.85				
						68.6	8.0	PVC	1.00%
ST-11	1958789.81	2311134.30	CONCRETE HEADWALL		609.16				

* 8" EQUIVALENT

INFILTRATION TRENCH SCHEDULE

STRUCTURE	NORTHING	EASTING	DESCRIPTION	RIM EL.	INVERT EL.	PIPE LENGTH	SIZE	TYPE	INV. SLOPE
IT-1	1958981.01	2311277.93	BEGIN INFILTRATION TRENCH	613.19	610.97				
						121.5	6.0	PERFORATED	0.50%
IT-3	1958938.09	2311234.49	TEE	612.69	610.36				
IT-2	1958854.15	2311149.53	BEGIN INFILTRATION TRENCH	611.95	610.54				
						59.0	6.0	PERFORATED	0.30%
IT-3	1958938.09	2311234.49	TEE	612.69	610.36				
						20.9	6.0	PERFORATED	0.50%
ST-9	1958880.82	2311206.19	INLET - TYPE A	612.52	610.25				
IT-4	1959047.52	2311430.84	BEGIN INFILTRATION TRENCH	614.00	611.50				
						326.5	6.0	PERFORATED	0.50%
IT-5	1958818.12	2311198.56	PIPE BEND	612.87	609.88				
						5.0	6.0	PERFORATED	0.50%
IT-6	1958821.71	2311195.02	END INFILTRATION TRENCH	612.81	609.85				

**MODIFY AIRCRAFT
HANGAR AREA
DRAINAGE**

SBGP No:
3-17-SBGP-162/171/184
IDA No: SFY-4966

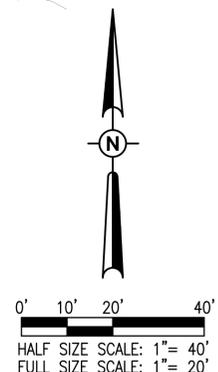
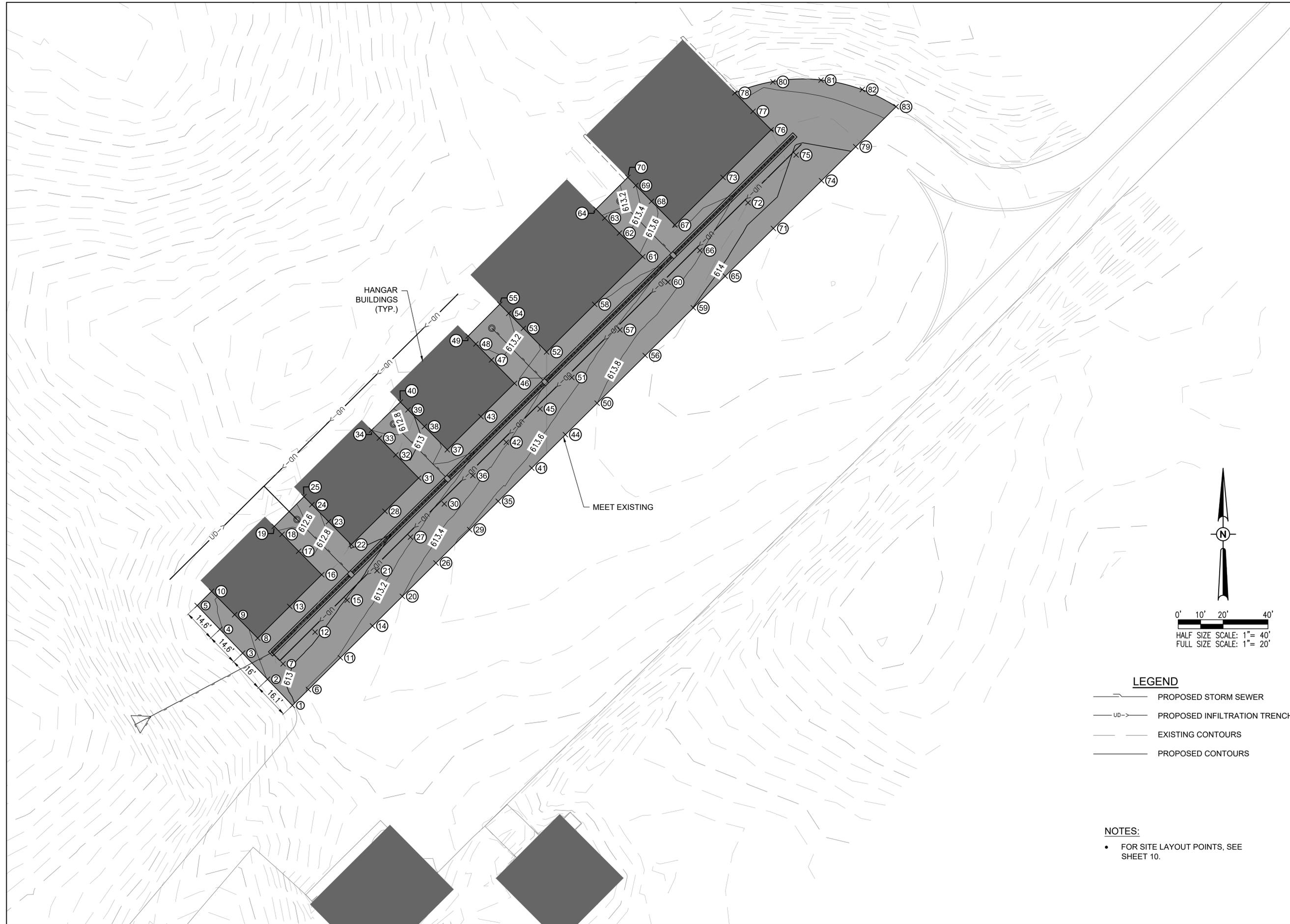
Contract No. TR013

NO.	DATE	DESCRIPTION		
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DRAWN BY: LDH 10/10/2022
REVIEWED BY: LDH 1/13/23

SHEET TITLE

**STORM SEWER AND
INFILTRATION
TRENCH SCHEDULE**



LEGEND

	PROPOSED STORM SEWER
	PROPOSED INFILTRATION TRENCH
	EXISTING CONTOURS
	PROPOSED CONTOURS

- NOTES:**
- FOR SITE LAYOUT POINTS, SEE SHEET 10.

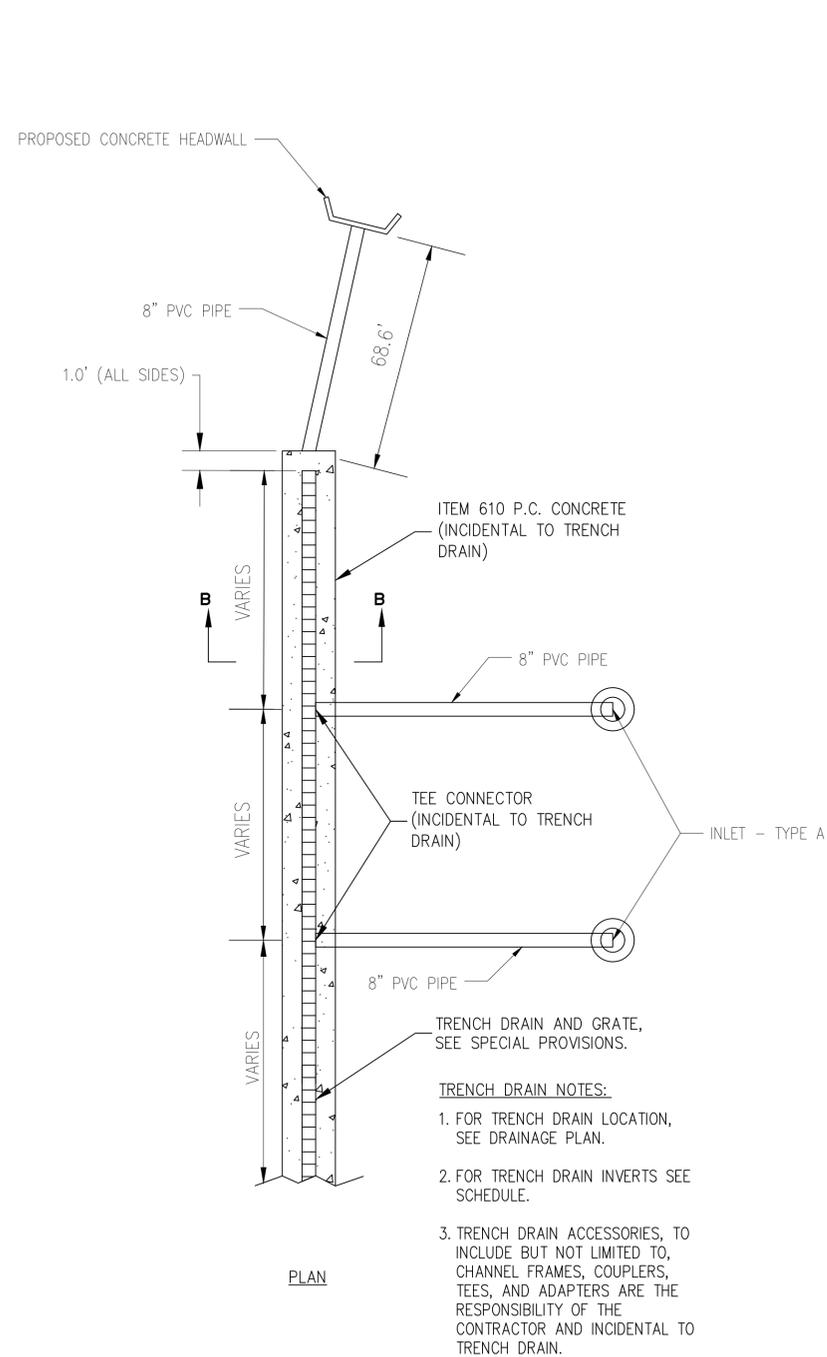
MODIFY AIRCRAFT HANGAR AREA DRAINAGE

SBGP No:
3-17-SBGP-162/171/184
IDA No: SFY-4966
Contract No. TR013

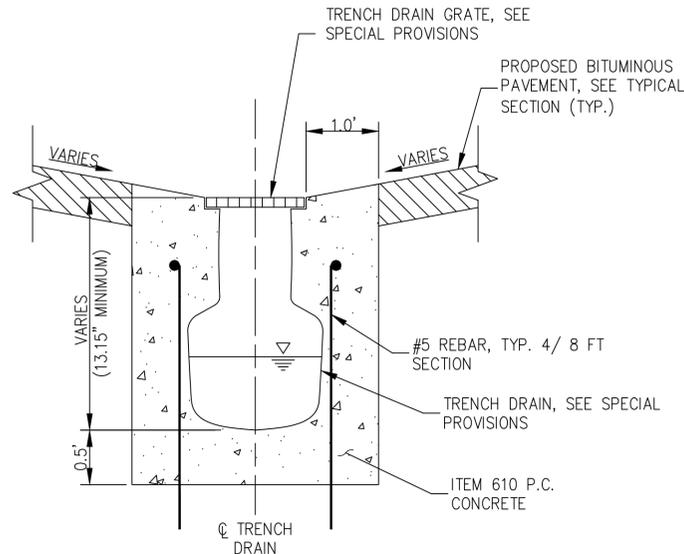
NO.	DATE	DESCRIPTION		
		DES	DWN	REV

SHEET TITLE

GRADING PLAN

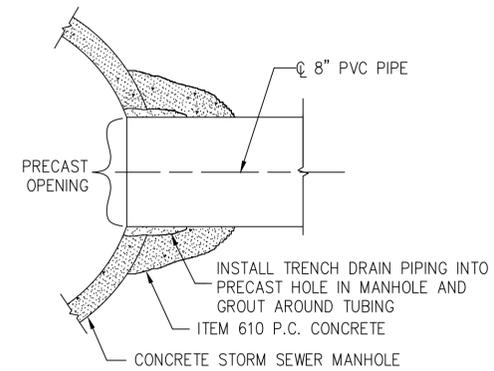


TRENCH DRAIN DETAIL



NOTE:
12 INCHES P.C. CONCRETE AROUND ALL SIDES OF TRENCH DRAIN, SEE PLAN.

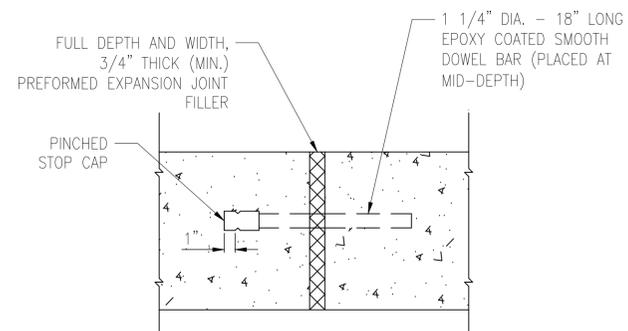
**SECTION B-B
TRENCH DRAIN**



NOTES

1. HOLE FOR TRENCH DRAIN PIPING TO BE PRECAST INTO MANHOLE AT ELEVATION SPECIFIED IN TRENCH DRAIN SCHEDULE.
2. CONNECTIONS INCIDENTAL TO UNDERDRAIN.

TRENCH DRAIN CONCRETE COLLAR AND GROUT CONNECTION



NOTES:

1. CONTRACTION JOINTS TO BE LOCATED AT NO GREATER THAN 12.5' ON CENTER. CONTRACTION JOINTS ARE TO BE EITHER 3/4" THICK PREFORMED JOINT FILLER, SAWED 2" DEEP AT 4 TO 24 HOURS OR FORMED WITH A 1/8" THICK STEEL TEMPLATE 2" DEEP.
2. EXPANSION JOINTS TO BE LOCATED AT 50' MAX SPACING.
3. ALL EXPANSION AND SAWED CONTRACTION JOINTS SHALL BE SEALED WITH HOT-POURED SEALANT, ASTM D6690, TYPE II, COST INCIDENTAL TO TRENCH DRAIN.

EXPANSION JOINT DETAIL

MODIFY AIRCRAFT HANGAR AREA DRAINAGE

SBGP No:
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IDA No: SFY-4966

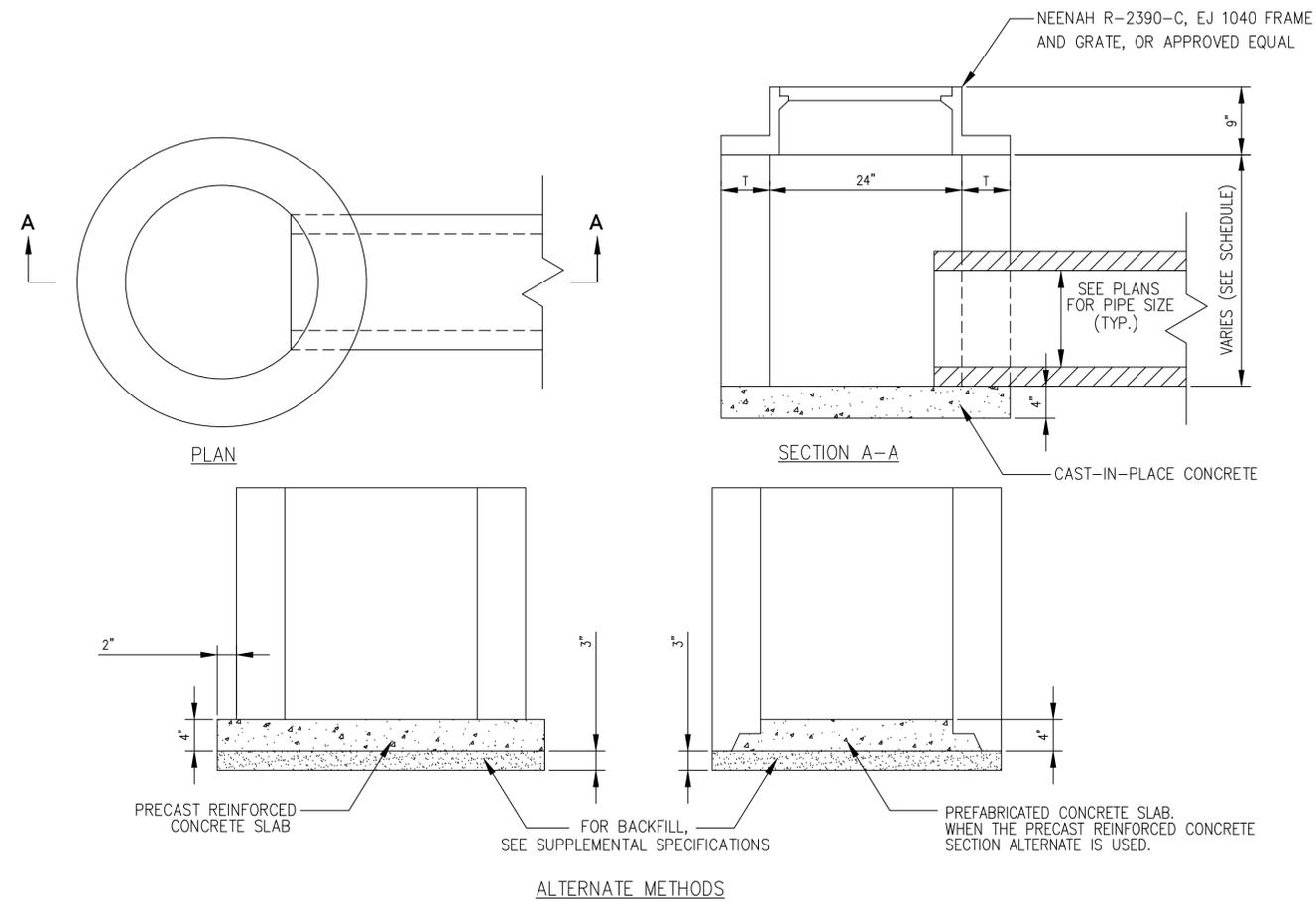
Contract No. TR013

NO.	DATE	DESCRIPTION		
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SHEET TITLE

DRAINAGE DETAILS - SHEET 1

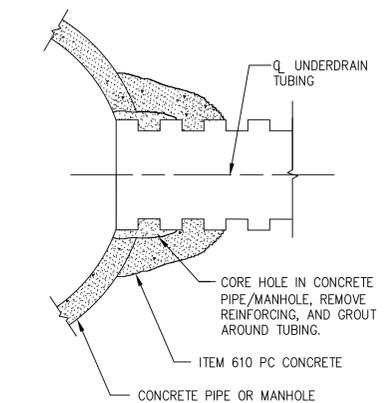


NOTES

1. SEE DRAINAGE SCHEDULES FOR LOCATION, SIZE AND NUMBER OF PIPE CONNECTIONS.
2. INLETS TO BE PRECAST REINFORCED CONCRETE SECTIONS (T = 5").

INLET TYPE A

(DOT STANDARD 602301)



STORM SEWER CONCRETE COLLAR AND GROUT CONNECTION

MODIFY AIRCRAFT HANGAR AREA DRAINAGE

SBGP No:
3-17-SBGP-162/171/184
IDA No: SFY-4966

Contract No. TR013

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SHEET TITLE

DRAINAGE DETAILS - SHEET 2

**MODIFY AIRCRAFT
HANGAR AREA
DRAINAGE**

SBGP No:
3-17-SBGP-162/171/184
IDA No: SFY-4966

Contract No. TR013

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PROJECT NO: 21A0142
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SHEET TITLE

**DRAINAGE DETAILS -
SHEET 4**

SEDIMENTATION AND EROSION CONTROL NOTES:

- A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- B. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:
 - UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.
 - AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- D. A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- E. TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.
- F. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
- G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.
- I. APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- J. STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- K. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- L. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- N. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- O. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.

STORM WATER POLLUTION PREVENTION NOTES

GENERAL
THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

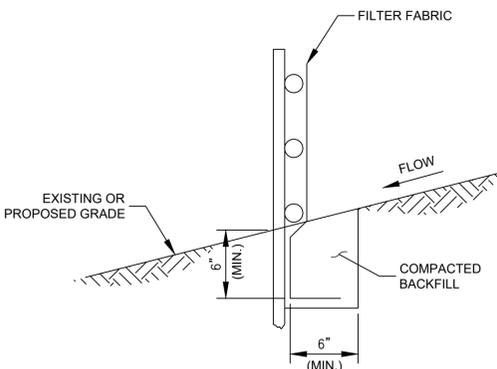
THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

POLLUTION PREVENTION MEASURES
THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

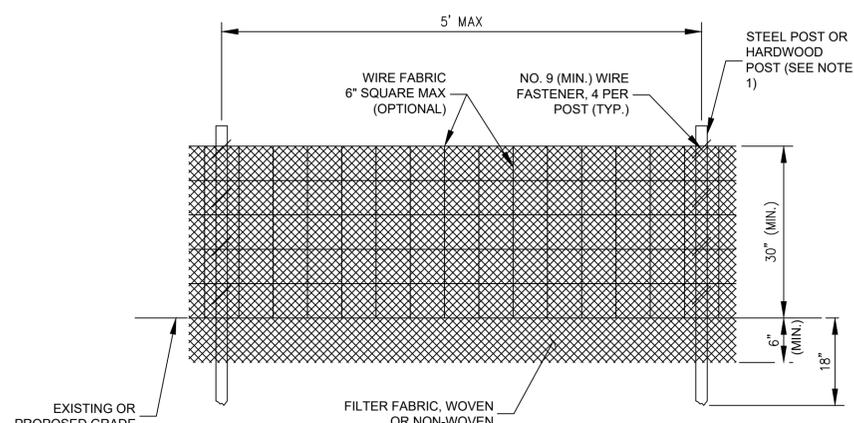
POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.

THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.

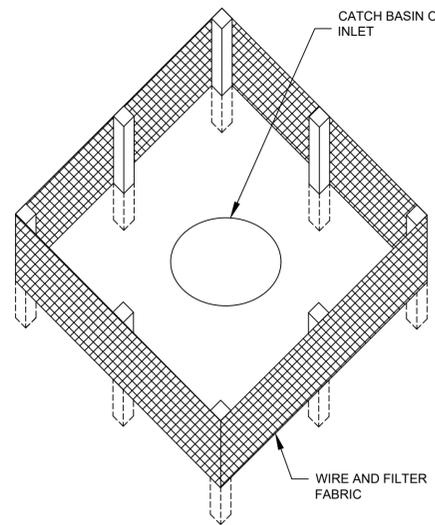


FABRIC ANCHOR DETAIL



ELEVATION

- NOTES:**
1. FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS.
 2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
 3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
 4. FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
 5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
 6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1.j.1.f.i. AS AMENDED), OR EQUIVALENT.
 7. A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
 8. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
 9. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
 10. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
 11. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
 12. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
 13. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
 14. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.



- NOTES:**
1. FILTER FABRIC SHALL BE EMBEDDED 8" INTO THE SOIL.
 2. INSPECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
 3. SILT FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS AT THE DIRECTION OF THE AIRPORT REPRESENTATIVE OR OWNER SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. CONTRACTOR SHALL PLACE SEED AND MULCH PER LANDSCAPING PLAN. COST OF REMOVAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SILT FENCE.
 4. AREAS DISTURBED OUTSIDE OF CONSTRUCTION LIMITS DURING PLACEMENT OF INLET PROTECTION TO BE RE-GRADED, SEEDED AND MULCHED, COST INCIDENTAL TO SILT FENCE.
 5. FENCE AND POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
 6. PAID UNDER AR156510 SILT FENCE.