

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS

CONSTRUCTION PLANS FOR CHICAGO EXECUTIVE AIRPORT REHABILITATE ACCESS ROAD AT NW QUADRANT HANGARS

ILLINOIS PROJECT: PWK-4794

JANUARY 30, 2023

PROJECT INFORMATION
 CONTRACTOR:
 RESIDENT ENGINEER:
 ASSISTANT RESIDENT ENGINEER:
 ORIGINAL CONTRACT AMOUNT:
 FINAL CONSTRUCTION COST:
 IDOT LETTING DATE:
 IDOT AWARD DATE:
 NOTICE TO PROCEED:
 START OF CONSTRUCTION:
 SUBSTANTIAL COMPLETION:

LOCAL AGENCY CONTACT INFORMATION
 VILLAGE OF WHEELING - 847.459.2600
 CITY OF PROSPECT HEIGHTS - 847.398.6070

ENGINEER'S PROJECT PERMIT LOG
 NPDES # N/A
 FAA AIRSPACE #
 CCDD LPC-663 DATED CONTRACTOR TO SUPPLY
 MWRDGC PERMIT # 03-246 & RL 09-063 N/A
 VILLAGE APP FOR CONSTRUCTION PERMIT #
 VILLAGE FLOODPLAIN PERMIT #
 CONTRACTOR'S REGISTRATION WITH VILLAGE
 VILLAGE SITE ALTERATION PERMIT #
 CITY APPLICATION FOR PERMIT # N/A
 CITY FLOODPLAIN PERMIT # N/A
 CITY SITE GRADING PERMIT # N/A
 CONTRACTOR'S REGISTRATION WITH CITY N/A

CHICAGO EXECUTIVE AIRPORT

TOWNSHIP: 42 NORTH WHEELING TOWNSHIP
 RANGE: 11 EAST (SECTION: 13)
 COOK COUNTY

CMT 21002030.00
 CRAWFORD, MURPHY & TILLY, INC.
 CONSULTING ENGINEERS
 License No. 062-069052

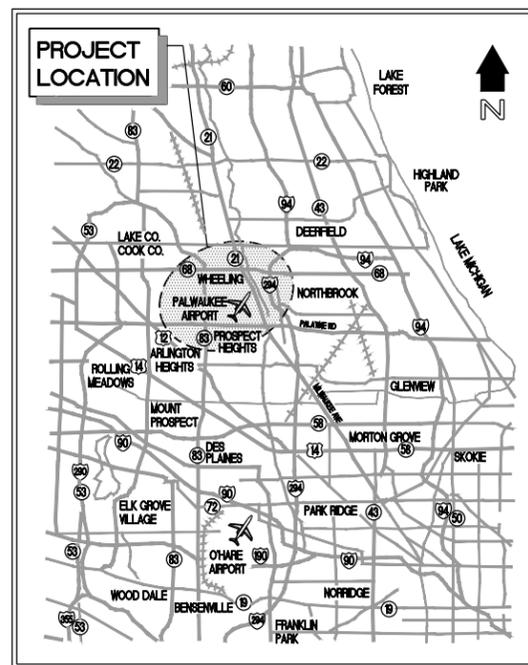
SUBMITTED BY *D. Kyle Peabody*
 D. KYLE PEABODY, P.E.

DATE 1/30/2023

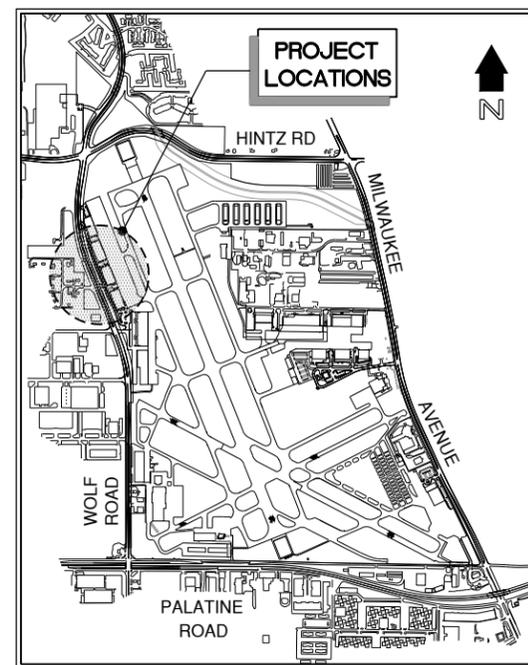
CHICAGO EXECUTIVE AIRPORT

APPROVED *Jeffrey J. Miller* EXECUTIVE DIRECTOR
 JEFFREY J. MILLER A.A.E., ACE

DATE 1/10/2023



LOCATION MAP



SITE PLAN



D. Kyle Peabody

LICENSE EXPIRATION
 DATE: 11/30/2023
 DATE SIGNED: 01/30/2023

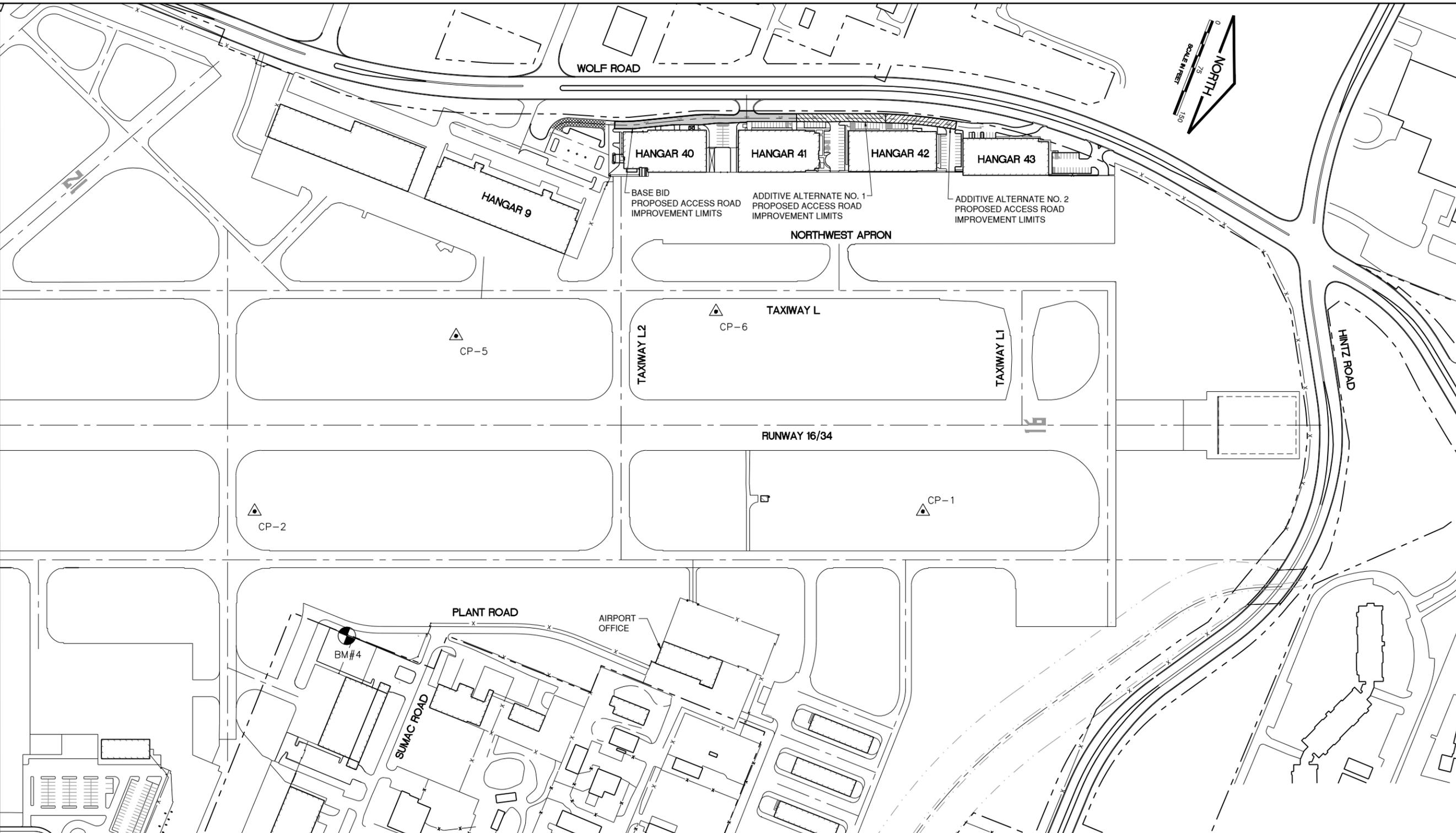
811 Know what's below. Call before you dig.

J.U.L.I.E. JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS
 www.illinois1call.com

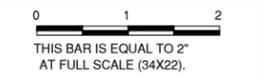
THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

DATE: Tuesday, January 31, 2023, 1:54:06 PM
 FILE: K:\Chicago\ExecAir\02020300_00_RehabNWAccessRd\Draw\Sheets\Draw_SitePlan.dwg
 UPDATE BY: Sheue Tong Lee
 LAYOUT: Layout1
 IMAGE FILES: p:\khw\LOGO_GEA+Color-Small (2).jpg
 XREF DWG: Rehab NW Access Road - Site Plan.dwg
 DON'T EDIT THIS ACCESS ROAD.DWG



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NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE ACCESS ROAD AT NW QUADRANT HANGARS
SITE PLAN AND PROJECT CONTROL

NOTES

- SUBTRACT 0.24 FEET FROM ELEVATIONS SHOWN IN PLANS (1929 DATUM) TO OBTAIN 1988 NAVD.
- BM-4 IS A CHISELED SQUARE ON THE WEST SIDE OF A LIGHT POLE BASE AT THE SW CORNER OF THE PARKING LOT.
- AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
- THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO THE CONTRACT.
- THE CONTRACTOR WILL BE REQUIRED TO FURNISH AND PLACE CONSTRUCTION LAYOUT STAKES FOR THIS PROJECT. CONSTRUCTION LAYOUT SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM FOR WHICH THE LAYOUT IS REQUIRED.

HORIZONTAL CONTROL			
POINT	DESCRIPTION	NORTHING	EASTING
CP-1	IRON ROD	1987333.73	616096.81
CP-2	IRON ROD	1985484.43	616831.72
CP-5	IRON ROD	1985849.27	616125.64
CP-6	IRON ROD	1986541.34	615772.44

HORIZONTAL CONTROL COORDINATES EXPRESSED IN NAD27.

VERTICAL CONTROL		
POINT	DESCRIPTION	ELEVATION
CP-1	IRON ROD	638.32
CP-2	IRON ROD	637.22
CP-5	IRON ROD	637.27
CP-6	IRON ROD	639.18
BM-4	CHISELED SQUARE	641.59

VERTICAL CONTROL ELEVATIONS EXPRESSED IN NAVD29.

- LEGEND**
- PROPOSED 1-1/2" BITUMINOUS MILL AND OVERLAY (BASE BID)
 - PROPOSED 1-1/2" OR 2" BITUMINOUS MILL AND OVERLAY (ADDITIVE ALTERNATE NO. 1)
 - PROPOSED 2" BITUMINOUS MILL AND OVERLAY (ADDITIVE ALTERNATE NO. 2)
 - EXISTING BUILDING
 - EXISTING FENCE
 - EXISTING AIRPORT PROPERTY LINE
 - CONTRACTOR'S STAGING AND STORAGE AREA

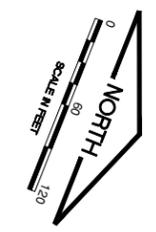
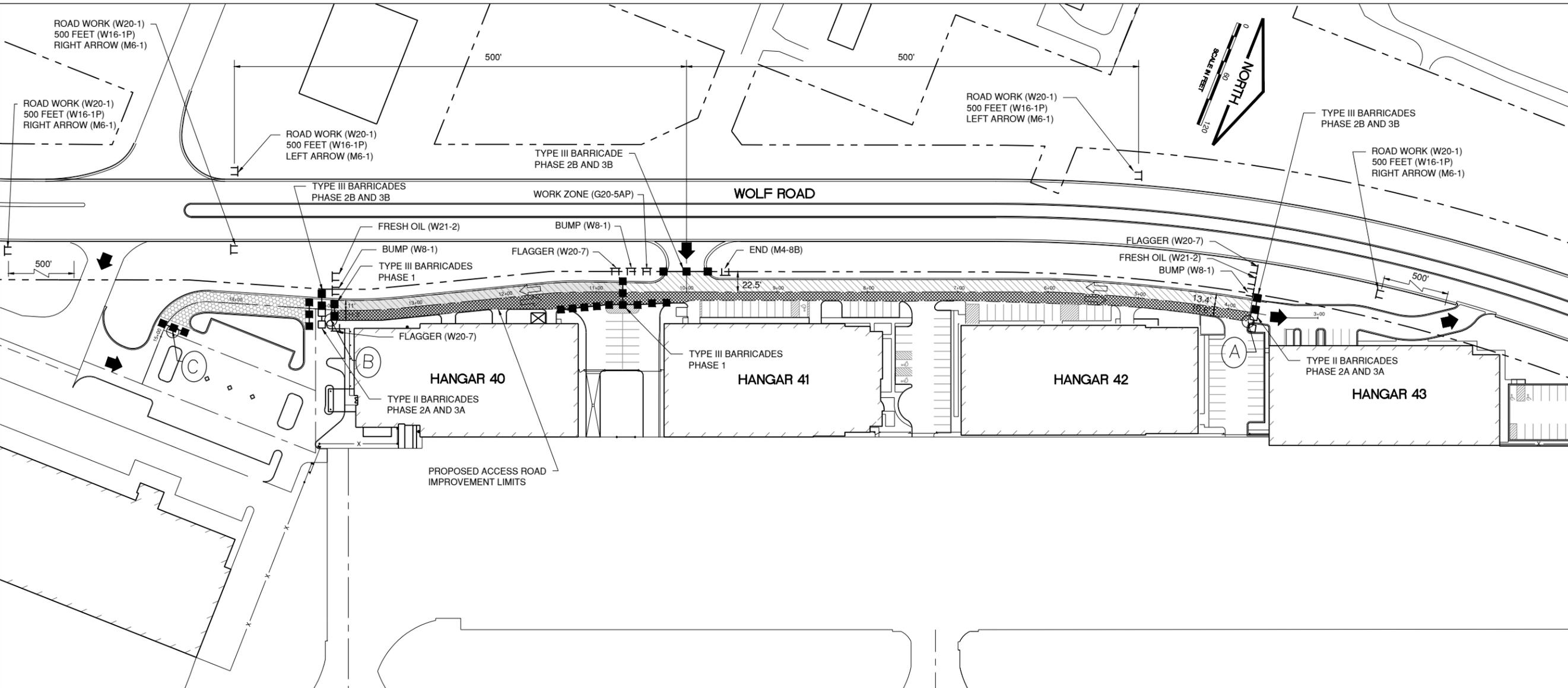
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DESIGN BY:	STL
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	DKP
DATE:	01/30/2023
JOB No:	21002030.00

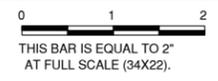
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SHEET 3 OF 9 SHEETS

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 UPDATE BY: Sheue Tong Lee
 LAYOUT: Layout1
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 REF DWG: Rehab NW Access Road - Site Plan.dwg
 DOWNTOWN: Rehab NW Access Road



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SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 1 (MAXIMUM 4 CALENDAR DAYS)

1. PLACE BARRICADES FOR LANE CLOSURE.
2. INSTALL INLET PROTECTION INCLUDING AWARDED ADDITIVE ALTERNATES.
3. PERFORM FULL DEPTH PATCHES AS DIRECTED BY THE RESIDENT ENGINEER.
4. PERFORM PAVEMENT REMOVAL AND PLACE 2-1/2" BITUMINOUS BASE COURSE.
5. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES.

PHASE 2A

1. PLACE BARRICADES FOR LANE CLOSURE.
2. MILL 1-1/2" OR 2" OF EXISTING PAVEMENT. MILL DEPTH SHALL NOT EXCEED 2-INCHES.
3. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

PHASE 2B

1. PLACE BARRICADES FOR LANE CLOSURE.
2. PERFORM FULL DEPTH PATCHES AS DIRECTED BY THE RESIDENT ENGINEER.
3. MILL 1-1/2" OR 2" OF EXISTING PAVEMENT. MILL DEPTH SHALL NOT EXCEED 2-INCHES.
4. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

PHASE 3A

1. PLACE BARRICADES FOR LANE CLOSURE.
2. TACK COAT EXISTING PAVEMENT AND CONSTRUCT 1-1/2" OR 2" BITUMINOUS SURFACE OVERLAY.
3. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

PHASE 3B

1. PLACE BARRICADES FOR LANE CLOSURE.
2. TACK COAT EXISTING PAVEMENT AND CONSTRUCT 1-1/2" OR 2" BITUMINOUS SURFACE OVERLAY.
3. CLEAN PAVEMENT, REMOVE BARRICADES, AND OPEN ALL TRAFFIC LANES AT THE END OF EACH WORKING DAY.

STAGING NOTES (ALL PHASES)

1. NO WORK SHALL OCCUR BETWEEN JUNE 19, 2023 AND JUNE 25, 2023, BOTH DAYS INCLUSIVE.
2. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING VEHICULAR ACCESS TO ALL AREAS AT ALL TIMES UNLESS OTHERWISE SHOWN. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED, HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN VEHICULAR ACCESS AND MUST BE APPROVED BY THE AIRPORT EXECUTIVE DIRECTOR AND RESIDENT ENGINEER.
3. STAGE 2A, 2B, 3A AND 3B SHALL NOT BE COMPLETED AT THE SAME TIME.
4. CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL (BARRICADES, CONES, FLAGGERS, ETC.) AND PERSONNEL TO SAFELY DIRECT TRAFFIC FOR THE DURATION OF THIS WORK. COST OF PROVIDING TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.
5. CONTRACTOR SHALL PROVIDE ACCESS TO HANGAR 9, HANGAR 40, HANGAR 41, HANGAR 42, AND HANGAR 43 PARKING LOTS AND FBO PARKING LOT AT ALL TIMES DURING CONSTRUCTION.
6. ACCESS TO AND FROM WOLF ROAD SHALL BE AVAILABLE AT ALL TIMES. FLAGGERS SHALL DIRECT TRAFFIC DURING CONSTRUCTION OPERATION WHILE ONE-WAY VEHICULAR TRAFFIC IS BEING UTILIZED.
7. CONTRACTOR SHALL RE-OPEN ALL LANES AT THE END OF EACH WORKING DAY. THE PAVEMENT SHALL MAINTAIN AN ELEVATION DIFFERENCE NO MORE THAN 2 INCHES BETWEEN TRAVEL LANES.
8. TEMPORARY TRAFFIC CONTROL SIGNS SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".

LEGEND

- PHASE 1 - PROPOSED REM & REP BIT PAVEMENT - TYPE B
- PHASE 1 - PROPOSED REMOVE BITUMINOUS PAVEMENT
- PHASE 2A - PROPOSED 1-1/2" OR 2" BITUMINOUS PAVEMENT MILLING
- PHASE 3A - PROPOSED BITUMINOUS SURFACE COURSE
- PHASE 2B - PROPOSED 1-1/2" OR 2" BITUMINOUS PAVEMENT MILLING
- PHASE 3B - PROPOSED BITUMINOUS SURFACE COURSE
- CONTRACTOR'S STAGING AND STORAGE AREA
- IDOT TYPE II BARRICADES
- IDOT TYPE III BARRICADES
- CONTRACTOR'S ACCESS ROUTE
- EXISTING BUILDING
- EXISTING FENCE
- EXISTING AIRPORT PROPERTY LINE
- VEHICLE TRAVEL DIRECTION
- CRITICAL POINT

CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE ACCESS ROAD AT NW QUADRANT HANGARS
SEQUENCE OF CONSTRUCTION

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JOB No:	21002030.00

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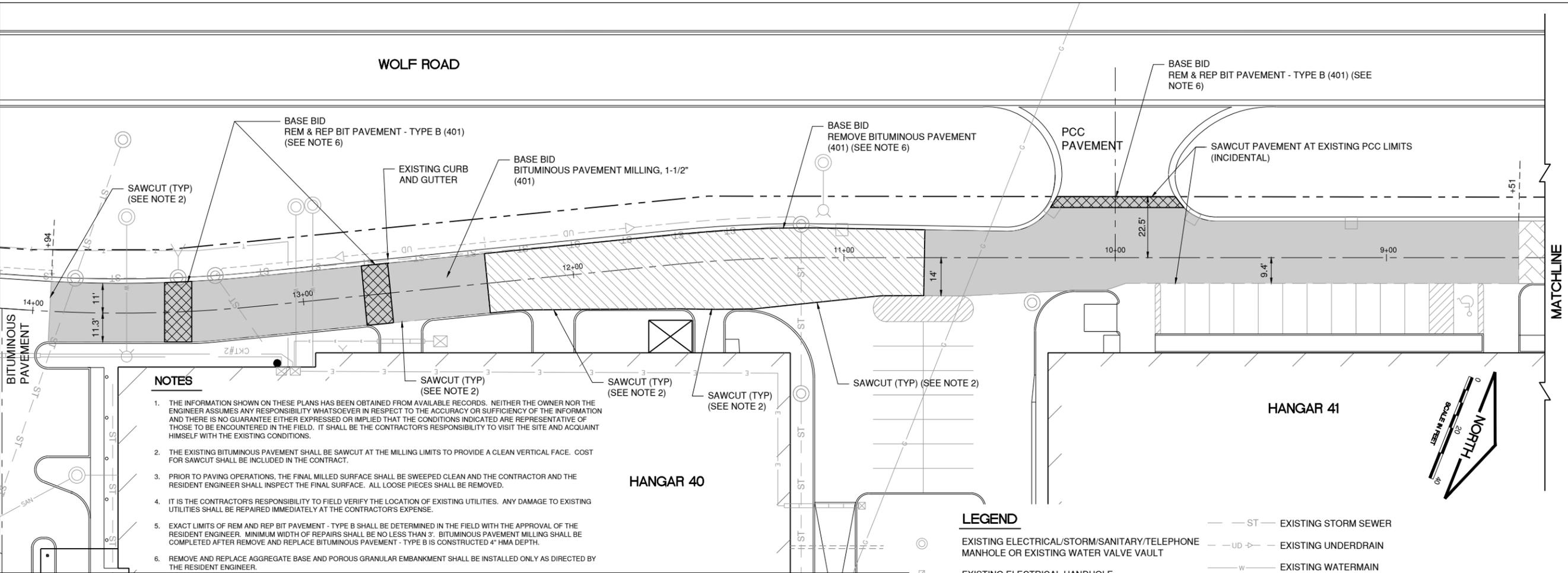
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UPDATE BY: Sheue Tong Lee
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IMAGE FILES: pavement\ODD_CEA\4-Codes-Small (2).jpg

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DATE: Tuesday, January 31, 2023, 1:54:29 PM
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NOTES

1. THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.
2. THE EXISTING BITUMINOUS PAVEMENT SHALL BE SAWCUT AT THE MILLING LIMITS TO PROVIDE A CLEAN VERTICAL FACE. COST FOR SAWCUT SHALL BE INCLUDED IN THE CONTRACT.
3. PRIOR TO PAVING OPERATIONS, THE FINAL MILLED SURFACE SHALL BE SWEEPED CLEAN AND THE CONTRACTOR AND THE RESIDENT ENGINEER SHALL INSPECT THE FINAL SURFACE. ALL LOOSE PIECES SHALL BE REMOVED.
4. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF EXISTING UTILITIES. ANY DAMAGE TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
5. EXACT LIMITS OF REM AND REP BIT PAVEMENT - TYPE B SHALL BE DETERMINED IN THE FIELD WITH THE APPROVAL OF THE RESIDENT ENGINEER. MINIMUM WIDTH OF REPAIRS SHALL BE NO LESS THAN 3'. BITUMINOUS PAVEMENT MILLING SHALL BE COMPLETED AFTER REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B IS CONSTRUCTED 4" HMA DEPTH.
6. REMOVE AND REPLACE AGGREGATE BASE AND POROUS GRANULAR EMBANKMENT SHALL BE INSTALLED ONLY AS DIRECTED BY THE RESIDENT ENGINEER.

LEGEND

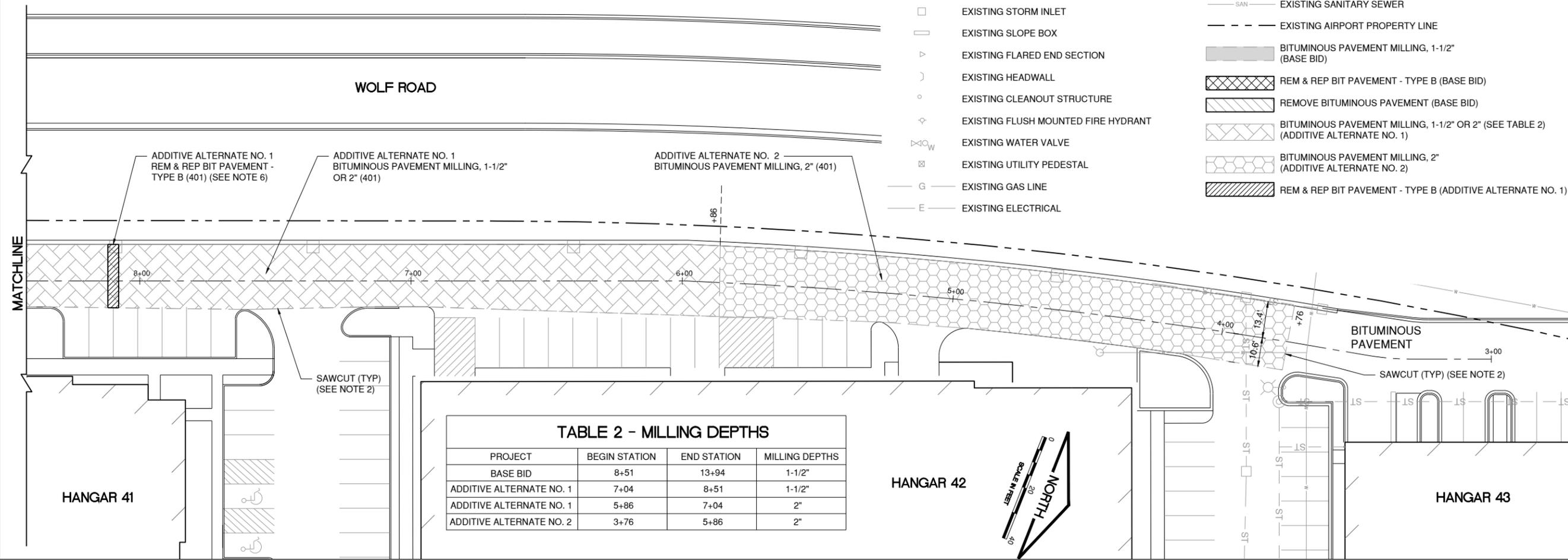
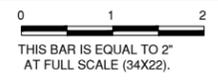
- ST — EXISTING STORM SEWER
- UD — EXISTING UNDERDRAIN
- W — EXISTING WATERMAIN
- SAN — EXISTING SANITARY SEWER
- — — — — EXISTING AIRPORT PROPERTY LINE
- BITUMINOUS PAVEMENT MILLING, 1-1/2" (BASE BID)
- ▨ REM & REP BIT PAVEMENT - TYPE B (BASE BID)
- ▩ REMOVE BITUMINOUS PAVEMENT (BASE BID)
- ▧ BITUMINOUS PAVEMENT MILLING, 1-1/2" OR 2" (SEE TABLE 2) (ADDITIVE ALTERNATE NO. 1)
- ▦ BITUMINOUS PAVEMENT MILLING, 2" (ADDITIVE ALTERNATE NO. 2)
- ▤ REM & REP BIT PAVEMENT - TYPE B (ADDITIVE ALTERNATE NO. 1)
- EXISTING ELECTRICAL/STORM/SANITARY/TELEPHONE MANHOLE OR EXISTING WATER VALVE VAULT
- EXISTING ELECTRICAL HANDHOLE
- EXISTING STORM INLET
- EXISTING SLOPE BOX
- ▽ EXISTING FLARED END SECTION
- ⌋ EXISTING HEADWALL
- EXISTING CLEANOUT STRUCTURE
- ⊕ EXISTING FLUSH MOUNTED FIRE HYDRANT
- ⊕_W EXISTING WATER VALVE
- ⊕ EXISTING UTILITY PEDESTAL
- G — EXISTING GAS LINE
- E — EXISTING ELECTRICAL

TABLE 2 - MILLING DEPTHS

PROJECT	BEGIN STATION	END STATION	MILLING DEPTHS
BASE BID	8+51	13+94	1-1/2"
ADDITIVE ALTERNATE NO. 1	7+04	8+51	1-1/2"
ADDITIVE ALTERNATE NO. 1	5+86	7+04	2"
ADDITIVE ALTERNATE NO. 2	3+76	5+86	2"

REVISIONS

NUMBER	BY	DATE



**CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE ACCESS ROAD AT NW QUADRANT HANGARS**

EXISTING CONDITIONS/PROPOSED REMOVALS

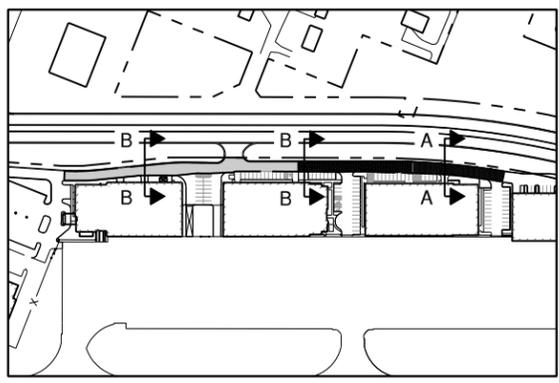
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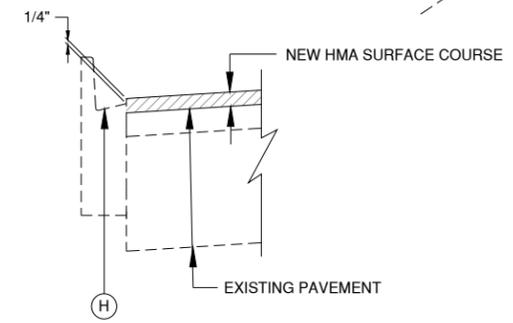
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SHEET 6 OF 9 SHEETS

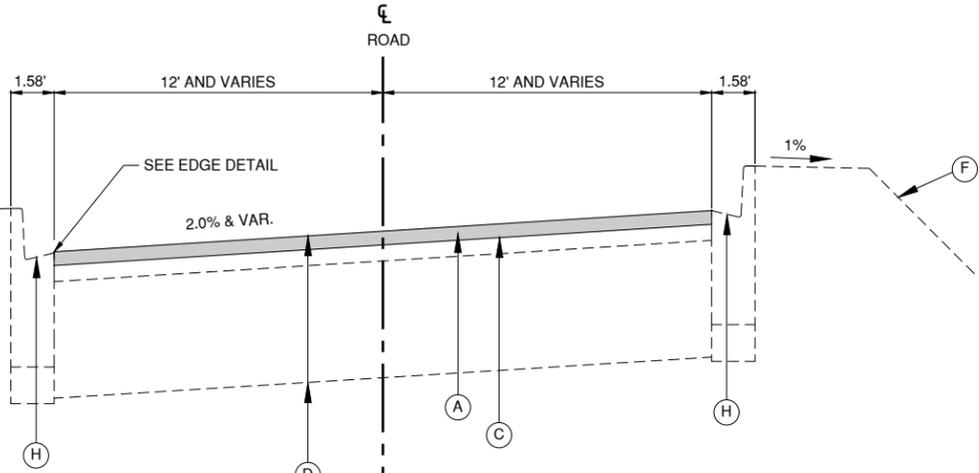
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 REHAB NW Access Road - Site Plan.dwg



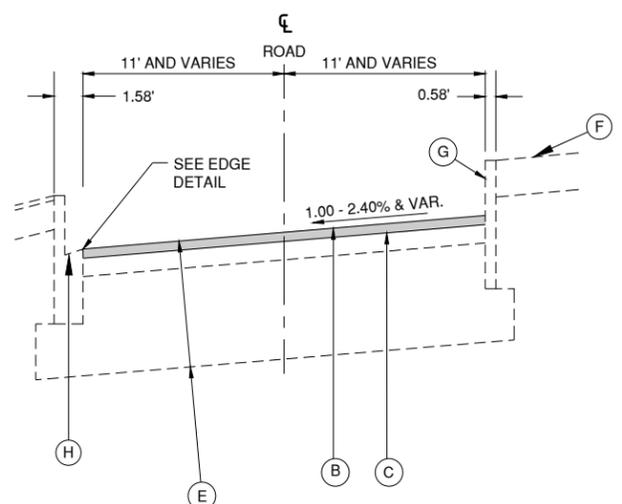
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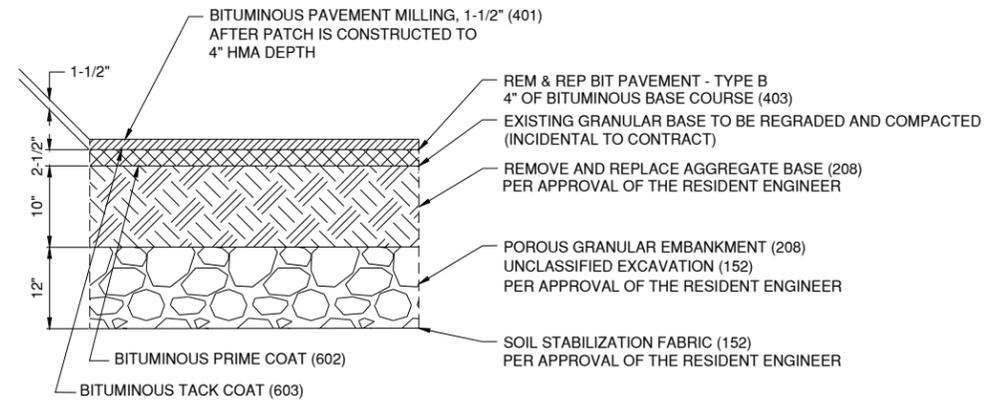
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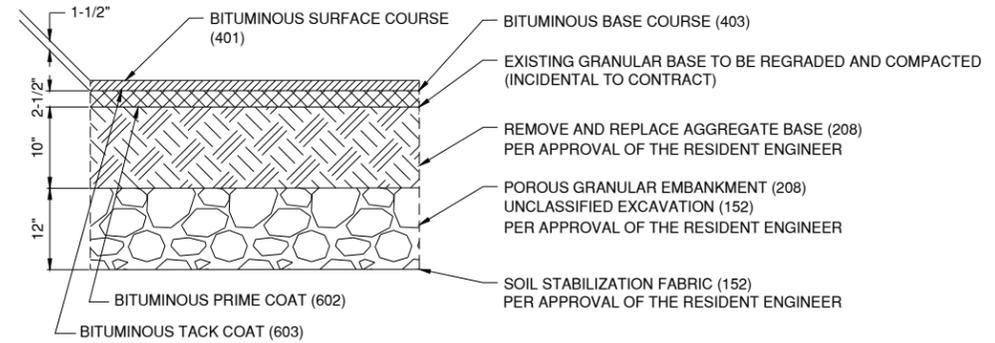
STA 3+76 TO STA 7+04
TYPICAL SECTION A-A
NOT TO SCALE



STA 7+04 TO STA 13+94
TYPICAL SECTION B-B
NOT TO SCALE



REM AND REP BIT PAVEMENT
- TYPE B DETAIL
NOT TO SCALE



REMOVE BITUMINOUS PAVEMENT DETAIL
NOT TO SCALE

NOTES

1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. REM & REP BIT PAVEMENT - TYPE B QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT REM & REP BIT PAVEMENT - TYPE B AREAS IN THE FIELD DURING CONSTRUCTION.
3. AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE SHALL BE DONE AT SECTIONS WHERE THE REM & REP BIT PAVEMENT - TYPE B IS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES.

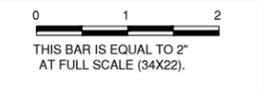
NOTES

1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORDS. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AREA IN THE FIELD DURING CONSTRUCTION.
3. AS FIELD CONDITIONS WARRANT AT THE TIME OF CONSTRUCTION, FURNISHING AND PLACING OF POROUS GRANULAR EMBANKMENT AND/OR REMOVAL AND REPLACEMENT OF AGGREGATE BASE SHALL BE DONE AT SECTIONS WHERE THE REMOVE BITUMINOUS PAVEMENT IS UTILIZED. THIS SHALL BE DONE TO REPAIR SOFT SUBGRADE AS DETERMINED BY THE RESIDENT ENGINEER BASED ON RESULTS OF THE GEOTECHNICAL TESTING OR PROOF ROLL. NO ADJUSTMENT IN UNIT PRICE WILL BE ALLOWED FOR AN INCREASE OR DECREASE IN QUANTITIES.

LEGEND

- (A) PROPOSED BITUMINOUS OVERLAY
BITUMINOUS PAVEMENT MILLING, 2" (401)
2" BITUMINOUS SURFACE COURSE (401)
- (B) PROPOSED BITUMINOUS OVERLAY
BITUMINOUS PAVEMENT MILLING, 1-1/2" (401)
1-1/2" BITUMINOUS SURFACE COURSE (401)
- (C) PROPOSED BITUMINOUS TACK COAT (603)
- (D) EXISTING PAVEMENT
2" HMA SURFACE COURSE, MIX C, N50
2" HMA BINDER COURSE, IL-19.0, N50
10" AGGREGATE BASE COURSE
- (E) EXISTING PAVEMENT
1-1/2" HMA SURFACE COURSE
2" HMA BINDER COURSE
10" CRUSHED AGGREGATE BASE COURSE
- (F) EXISTING GROUND LINE
- (G) CONCRETE BARRIER CURB
- (H) COMBINATION CONCRETE CURB AND GUTTER OR BARRIER CURB

REVISIONS		
NUMBER	BY	DATE



CHICAGO EXECUTIVE AIRPORT
WHEELING/PROSPECT HEIGHTS, ILLINOIS
REHABILITATE ACCESS ROAD AT NW QUADRANT HANGARS

TYPICAL SECTIONS

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