

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
066	2018-114-I	MCHENRY	41	1
ILLINOIS			CONTRACT NO. 62R50	

FOR INDEX OF SHEETS, SEE SHEET NO. 2

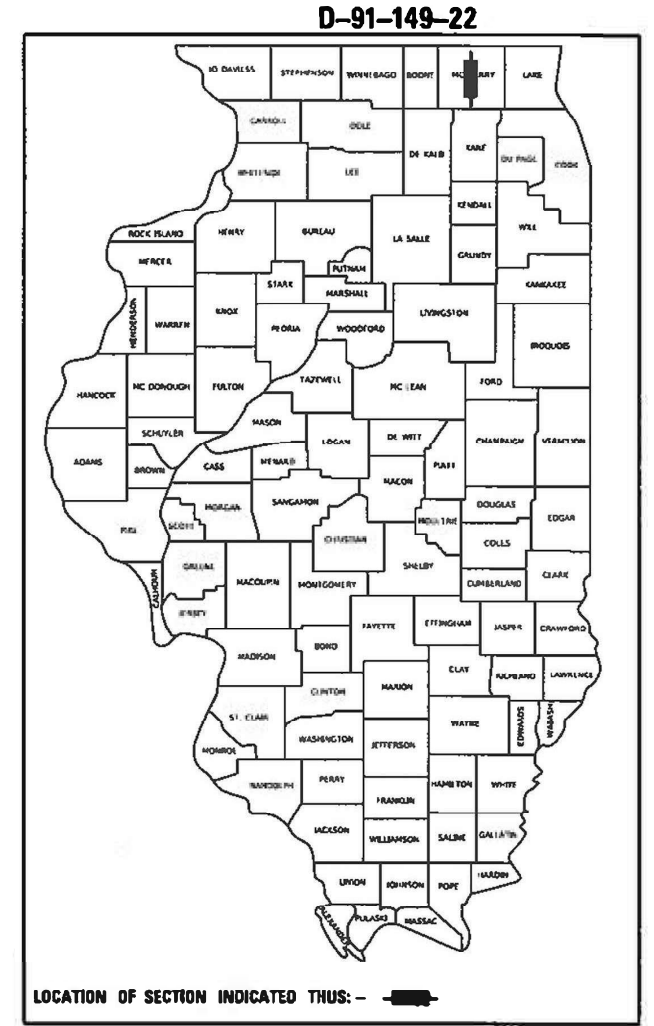
TRAFFIC DATA

EXISTING ADT = 6100 (2019)
POSTED SPEED LIMIT = 55 MPH
FUNCTIONAL CLASS: PRINCIPAL ARTERIAL

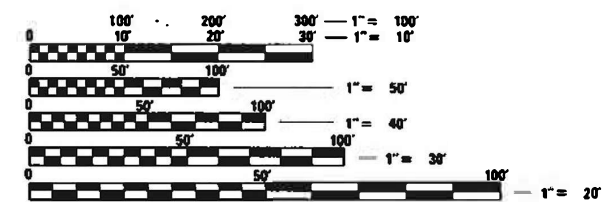
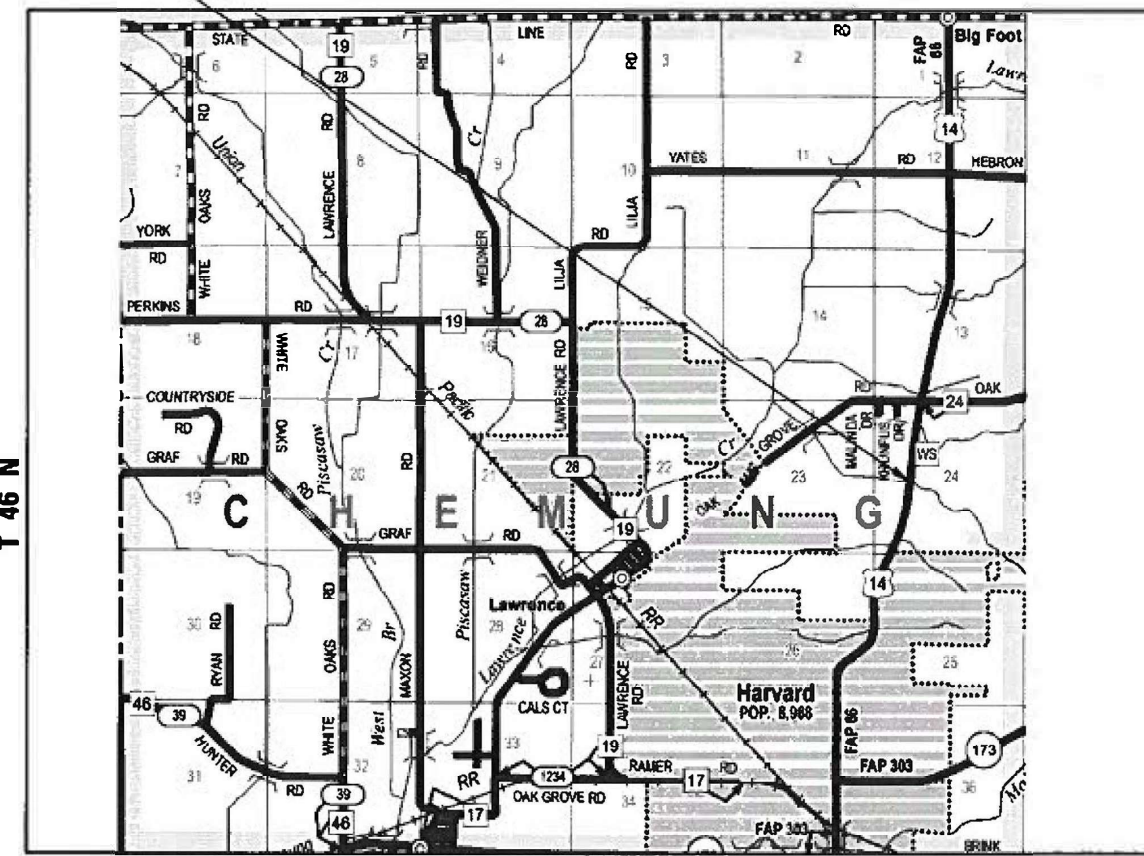
PROJECT LOCATED IN THE UNINCORPORATED CHEMUNG TOWNSHIP

PROPOSED HIGHWAY PLANS
F.A.P. ROUTE 066: US 14 (DIVISION ST)
OVER DITCH (3 MILES SOUTH OF STATE LINE)
SECTION: 2018-114-I
PROJECT NO. NHPP HJVH(690)
CULVERT REPLACEMENT
MCHENRY COUNTY

LOCATION OF IMPROVEMENT
EX. SN 056-0263
PROP. SN 056-8300



 Thomas P. Paolicchi, P.E. Lic. No. 062.056747 Expires: 11/30/2023 ABNA ENGINEERING (SHTS 2-16, 29-41)	SEAL
 Stephen E. Alsbury, S.E. Lic. No. 081-5261 Expires: 11/30/2024 ABNA ENGINEERING (SHTS 17-28)	SEAL



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-992-0123
OR 811

PROJECT ENGINEER: PRAVEEN KAINI, PE 847-705-4237
PROJECT MANAGER: J. ALAIN MIDY, PE 847-221-3056

GROSS LENGTH = 345 FT. = 0.065 MILE
NET LENGTH = 345 FT. = 0.065 MILE

CONTRACT NO. 62R50



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED DECEMBER 15 20 22
Steve Zin [Signature]
REGIONAL ENGINEER

February 3, 2023
Steve Zin [Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

February 3, 2023
Stephen M. Alsbury [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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HIGHWAY STANDARDS

280001-07	TEMPORARY EROSION CONTROL SYSTEMS
630001-12	STEEL PLATE BEAM GUARDRAIL
630101-10	STRONG POST GUARDRAIL ATTACHED TO CULVERT
630301-09	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701201-05	LANE CLOSURE 2L, 2W, DAY ONLY, FOR SPEEDS ≥ 45 MPH
701206-05	LANE CLOSURE, 2L, 2W, NIGHT ONLY, FOR SPEEDS ≥ 45 MPH
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701321-18	LANE CLOSURE, 2L, 2W, BRIDGE REPAIR WITH BARRIER
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL J.U.L.I.E. AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF HARVARD.
- PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN SCOPE OF THE WORK, HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED BASED AT THE UNIT PRICE BID FOR THE WORK.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE RESIDENT ENGINEER SHALL CONTACT WALTER CZARNY, IDOT'S AREA TRAFFIC FIELD ENGINEER VIA EMAIL AT WALTER.CZARNY@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE AGGREGATE GRADATION FOR THE AGGREGATE SUBGRADE IMPROVEMENT 12" LOWER LIFT SHALL BE CS 1 OR RR 1.

COMMITMENTS

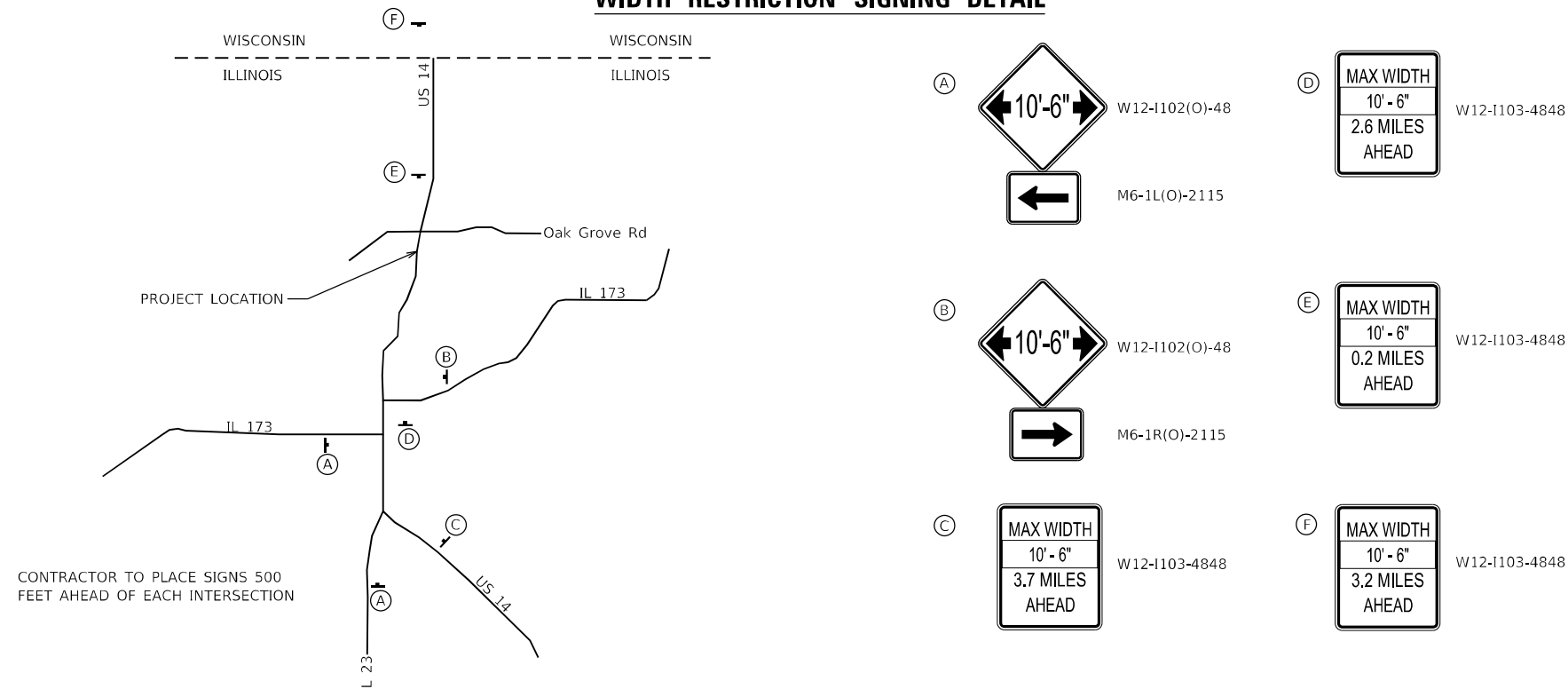
NO COMMITMENTS FOR THIS CONTRACT.

1.

404 PERMITTING AND WORK IN THE WATERWAYS

- DURING WORK ON THE BANKS OF THE STREAM/WETLAND, WORK MUST BE TIMED TO TAKE PLACE DURING LOW OR NO FLOW CONDITIONS.
- IF BYPASS PUMPING IS NECESSARY, THE INLET OF THE PUMP SHALL BE PLACED IN A SUMP PIT AND THE OUTLET PLACED ON A NON-ERODIBLE ENERGY DISSIPATING SURFACE PRIOR TO REJOINING THE WATERWAY FLOW OR WETLAND. FILTERING OF BY-PASS WATER IS NOT REQUIRED UNLESS THE BYPASS WATER HAS BECOME SEDIMENT-LADEN AS A RESULT OF CONSTRUCTION ACTIVITIES. DISCHARGED WATER SHALL BE NO MORE TURBID THAN THE RECEIVING WATER. DISCHARGE SHALL BE IMMEDIATELY STOPPED IF RECEIVING WATERS SHOW EVIDENCE OF CLOUDY WATER, EROSION, OR SEDIMENT ACCUMULATION.
- IF DEWATERING THE CONSTRUCTION AREA IS NECESSARY, ALL WATER REMOVED FROM THE WORK AREA SHALL BE FILTERED USING FILTER BAGS OR AN ALTERNATE APPROVED MEASURE. WATER MUST HAVE SEDIMENT REMOVED BEFORE BEING ALLOWED TO RETURN TO THE SOURCE CREEK/STREAM/RIVER/WETLAND. DISCHARGE FROM DEWATERING SHALL BE TO A STABLE SURFACE THAT EXTENDS TO THE POINT WHERE WATER RE-ENTERS THE WATERWAY.
- THE SIDE SLOPES MUST BE RE-SEEDING AND STABILIZED WITH APPROPRIATE EROSION CONTROL BLANKET PRIOR TO ACCEPTING FLOWS. THE BOTTOM OF THE SWALE MUST BE BROUGHT BACK TO ITS ORIGINAL GRADE AND STABLE ENOUGH TO ACCEPT FLOWS.
- THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT, AS A CONDITION OF THIS PERMIT THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED WITH THE EXCEPTION OF COFFERDAMS WHICH WILL BE PAID FOR AS COFFERDAM (TYPE 1) (IN-STREAM/WETLAND WORK) WITH A BASIS OF PAYMENT OF EACH.
- CONCENTRATED FLOW MUST BE ISOLATED FROM THE WORK AREA USING A NON-ERODIBLE COFFERDAM, STEEL SHEETS, AQUA BARRIERS, JERSEY BARRIERS, ETC. THE EXACT MEANS AND METHODS SHALL BE DISCUSSED DURING A SCHEDULED PRE-CONSTRUCTION IN-STREAM WORK MEETING. EARTHEN COFFERDAMS ARE NOT PERMISSIBLE.
- THE IN-STREAM WORK PLAN WILL BE DESIGNED TO ALLOW FOR THE CONVEYANCE OF THE 2-YEAR PEAK FLOW PAST THE WORK AREA WITHOUT OVERTOPPING THE COFFERDAM. THE USACE HAS THE DISCRETION TO REDUCE THIS REQUIREMENT IF DOCUMENTED TO BE INFEASIBLE OR UNNECESSARY.
- COFFERDAMS MUST BE CONSTRUCTED FROM SHORE AND NO EQUIPMENT MAY ENTER FLOWING WATER ANY TIME. IF THE INSTALLATION OF THE COFFERDAM CANNOT BE COMPLETED FROM SHORE, CONSTRUCTION OF A CAUSEWAY WILL BE NECESSARY TO ENSURE THAT EQUIPMENT DOES NOT ENTER FLOWING WATER. EQUIPMENT MAY ENTER THE COFFERED AREA ONCE THE COFFERDAM IS IN PLACE AND THE ISOLATED AREA IS DEWATERED.

WIDTH RESTRICTION SIGNING DETAIL



MODEL SHEET
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745 McClintock Drive
Suite 210
Burr Ridge, IL 60527
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Fax: 773-239-3728

USER NAME = tpaolilcchi	DESIGNED - TPP	REVISED -
	DRAWN - OI/JKR	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - JO	REVISED -
PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	2
			CONTRACT NO. 62R50	
ILLINOIS / FED. AID PROJECT				

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE		CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE	
				80% FED	20% STATE					80% FED	20% STATE
				ROADWAY						ROADWAY	
				0004	URBAN					0004	URBAN
20101200	TREE ROOT PRUNING	EACH	5	5		28001100	TEMPORARY EROSION CONTROL BLANKET	SQ YD	2277	2277	
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	5	5		28100107	STONE RIPRAP, CLASS A4	SQ YD	115	115	
20200100	EARTH EXCAVATION	CU YD	487	487		28200200	FILTER FABRIC	SQ YD	274	274	
20300100	CHANNEL EXCAVATION	CU YD	97	97	*	30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	237	237	
20400800	FURNISHED EXCAVATION	CU YD	151	151		35102400	AGGREGATE BASE COURSE, TYPE B 12"	SQ YD	735	735	
20700220	POROUS GRANULAR EMBANKMENT	CU YD	18	18		35501332	HOT-MIX ASPHALT BASE COURSE, 12"	SQ YD	38	38	
21101505	TOPSOIL EXCAVATION AND PLACEMENT	CU YD	373	373		40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	120	120	
25000300	SEEDING, CLASS 3	ACRE	0.47	0.47		40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	26	26	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	42	42		40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	113	113	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	42	42		40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	30	30	
25100630	EROSION CONTROL BLANKET	SQ YD	2277	2277		44000100	PAVEMENT REMOVAL	SQ YD	411	411	
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	100	100		44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	266	266	
28000305	TEMPORARY DITCH CHECKS	FOOT	40	40		44004250	PAVED SHOULDER REMOVAL	SQ YD	225	225	
28000400	PERIMETER EROSION BARRIER	FOOT	882	882		48101500	AGGREGATE SHOULDERS, TYPE B 6"	SQ YD	304	304	

* SPECIALTY ITEM

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USER NAME = tpaolicchi
PLOT SCALE = 40.0000' / in.
PLOT DATE = 12/15/2022

DESIGNED - TPP
DRAWN - OJ/JKR
CHECKED - JO
DATE - 07/08/2022

REVISED -
REVISED -
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REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)

SCALE: NTS SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-1	MCHENRY	41	3
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R50	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE		CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE	
				80% FED ROADWAY 0004 URBAN	20% STATE					80% FED ROADWAY 0004 URBAN	20% STATE
48203037	HOT-MIX ASPHALT SHOULDERS, 10"	SQ YD	735		735	* 66900530	SOIL DISPOSAL ANALYSIS	EACH			
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1		1	* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1		1
50200100	STRUCTURE EXCAVATION	CU YD	194		194	* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1		1
50200450	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL FOR STRUCTURES	CU YD	109		109	* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	35		35
50800105	REINFORCEMENT BARS	POUND	19920		19920	67100100	MOBILIZATION	L SUM	1		1
50800515	BAR SPLICERS	EACH	76		76	70106500	TEMPORARY BRIDGE TRAFFIC SIGNALS	EACH	1		1
51500100	NAME PLATES	EACH	1		1	70106700	TEMPORARY RUMBLE STRIPS	EACH	6		6
52200020	TEMPORARY SOIL RETENTION SYSTEM	SQ FT	335		335	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	120		120
52200600	GEOTEXTILE RETAINING WALL	SQ FT	54		54	70300100	SHORT TERM PAVEMENT MARKING	FOOT	100		100
54003000	CONCRETE BOX CULVERTS	CU YD	102.5		102.5	70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	1380		1380
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	275		275	70306120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE III TAPE	FOOT	102		102
* 63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	4		4	70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	3938		3938
63200310	GUARDRAIL REMOVAL	FOOT	360		360	70307210	TEMPORARY PAVEMENT MARKING - LINE 24" - TYPE IV TAPE	FOOT	52		52
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD				70400100	TEMPORARY CONCRETE BARRIER	FOOT	875		875

* SPECIALTY ITEM



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USER NAME = tpaoltechi	DESIGNED - TPP	REVISED -
DRAWN - OJ/JKR	CHECKED - JO	REVISED -
PLOT SCALE = 40.0000' / in.	DATE - 07/08/2022	REVISED -
PLOT DATE = 12/15/2022		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)			
SCALE: NTS	SHEET 2 OF 2 SHEETS	STA.	TO STA.

F.A.P. RTE. 66	SECTION 2018-114-I	COUNTY MCHENRY	TOTAL SHEETS 41	SHEET NO. 4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R50	

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE		
				80% FED 20% STATE		
				ROADWAY		
				0004	URBAN	
70400125	PINNING TEMPORARY CONCRETE BARRIER	EACH	200	200		
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	875	875		
70600270	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	EACH	2	2		
70600330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	2	2		
* 72501000	TERMINAL MARKER - DIRECT APPLIED	EACH	4	4		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3175	3175		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	192	192		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2	2		
* 78200005	GUARDRAIL REFLECTORS, TYPE A	EACH	22	22		
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	2	2		
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	1516	1516		
89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	2	2		
* X0326276	TEMPORARY LIGHTING FOR SINGLE LANE STAGING	L SUM	1	1		
X0900064	MEMBRANE WATERPROOFING SYSTEM FOR BURIED STRUCTURES	SQ YD	97	97		
X0900075	COFFERDAM (TYPE 1) (IN-STREAM WETLAND WORK)	EACH	2	2		

* SPECIALTY ITEM

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	CONSTR. CODE		
				80% FED 20% STATE		
				ROADWAY		
				0004	URBAN	
X2010400	STUMP REMOVAL ONLY	UNIT	67	67		
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	12		
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1		
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	10	10		
* X8620250	UNINTERRUPTABLE POWER SUPPLY AND CABINET, SPECIAL	EACH	1	1		
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1		
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	51.4	51.4		
Z0054400	ROCK FILL	CU YD	87	87		
Z0062456	TEMPORARY PAVEMENT	SQ YD	174	174		
Ø Z0076600	TRAINEES	HOURS	500	500		
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500		
* Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1	1		

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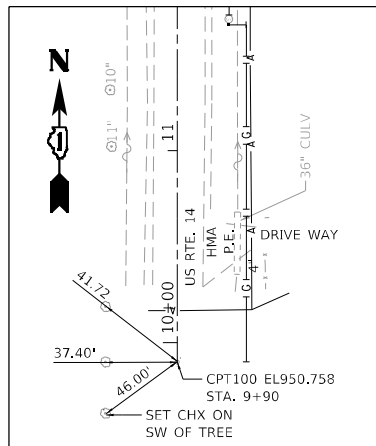
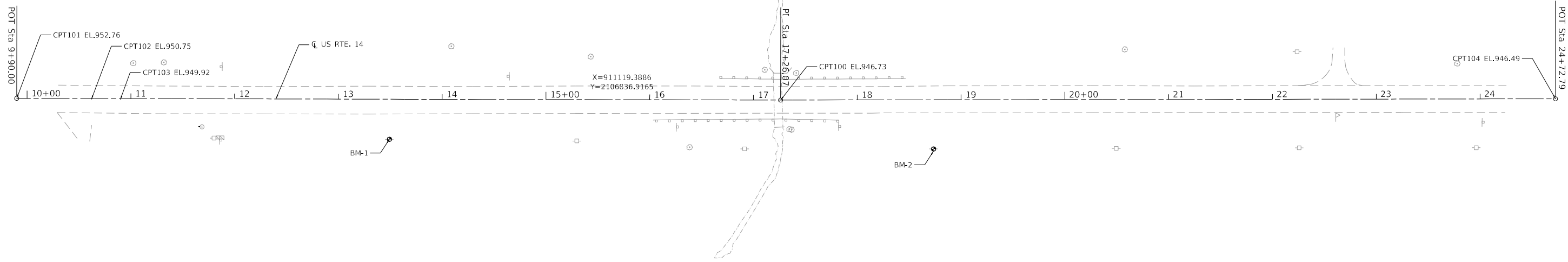
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SUMMARY OF QUANTITIES			
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SCALE: NTS	SHEET 2 OF 2 SHEETS	STA.	TO STA.

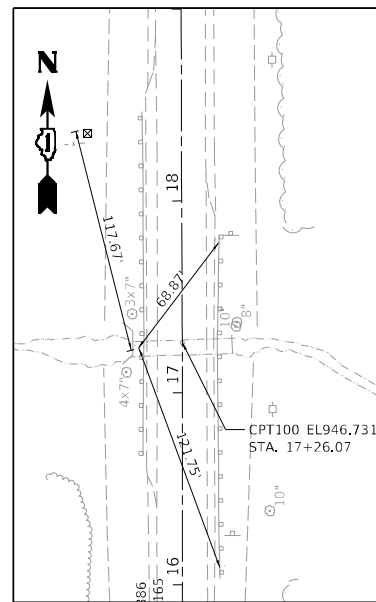
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			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				

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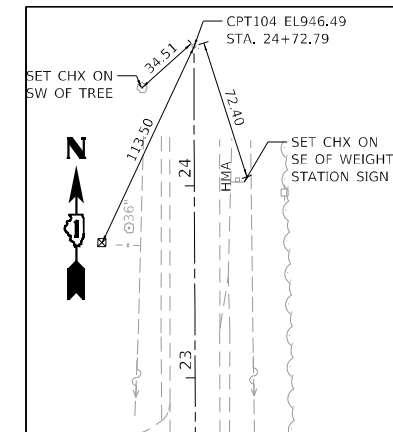
REV-SEP



CONTROL POINT #101
AT STA. 9+90.00



CONTROL POINT #100
AT STA. 17+26.07



CONTROL POINT #104
AT STA. 24+72.79

BENCHMARK 1

BENCHMARK 1 SPIKE AT POWER POLE NORTH OF CPT#101
@ US RTE. 14 (3RD POWER POLE SOUTH OF CREEK)
AT STA. 13+49.13, OFFSET RT - 38.55', ELEV. 945.99

BENCHMARK 2

BENCHMARK 2 SPIKE AT POWER POLE NORTH OF CREEK
@ US RTE. 14 (1ST POWER POLE NORTH OF CREEK)
AT STA. 18+73.48, OFFSET RT - 47.30', ELEV. 945.58

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745 McClintock Drive
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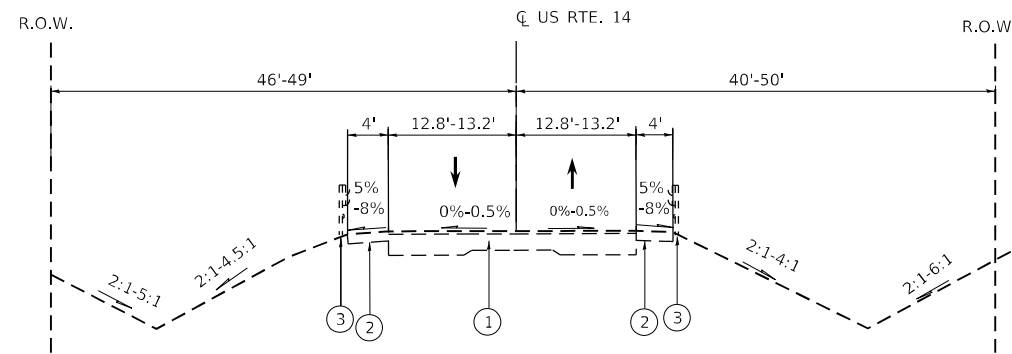
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ALIGNMENTS, TIES AND BENCHMARKS
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	6
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				

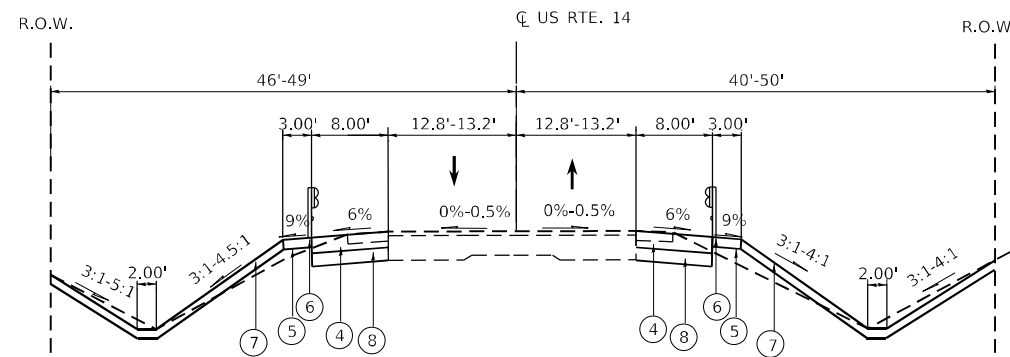


EXISTING TYPICAL SECTION

STA. 14+38 TO STA. 16+85
STA. 17+68 TO STA. 20+15

LEGEND

- ① EXISTING PAVEMENT (3/4" BONDED WEARING COURSE ON 3" HMA SURF ON 9" PCC BASE COURSE)
- ② EXISTING HMA SHOULDER
- ③ EXISTING GUARDRAIL
- ④ PROPOSED 8" HMA SHOULDERS, 10" (LIMITS VARY)
- ⑤ PROPOSED 3' AGG SHOULDER, TYPE B, 6" (LIMITS VARY)
- ⑥ PROPOSED STEEL PLATE BEAM GUARDRAIL (LIMITS VARY)
- ⑦ PROPOSED TOPSOIL, 4" & SEEDING CLASS 3 (LIMITS VARY)
- ⑧ PROPOSED AGGREGATE BASE COURSE, TYPE B, 12" (LIMITS VARY)



PROPOSED TYPICAL SECTION

STA. 14+38 TO STA. 16+85
STA. 17+68 TO STA. 20+15

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS (%) AT N _{DES}	QUALITY MANAGEMENT PROGRAM (QMP)
MAINLINE PAVEMENT (RESURFACING)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", 1L-9.5, N70; 2"	4.0% AT 70 GYR.	QC/QA
TEMPORARY PAVEMENT		
HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N70; 10"	4.0% AT 70 GYR.	QC/QA
MAINLINE PAVEMENT (RECONSTRUCTION)		
HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N70; 10"	4.0% AT 70 GYR.	QC/QA
HOT-MIX SURFACE COURSE, MIX "D", 1L-9.5, N70; 2"	4.0% AT 70 GYR.	QC/QA
MAINLINE PAVEMENT (RECONSTRUCTION) OVER CULVERT		
HOT-MIX ASPHALT BASE COURSE (HMA BINDER 1L-19.0); 12"	4.0% AT 70 GYR.	QC/QA
HOT-MIX SURFACE COURSE, MIX "D", 1L-9.5, N70; 2"	4.0% AT 70 GYR.	QC/QA
HMA SHOULDERS, 10"		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", 1L-9.5, N70; 2"	4.0% AT 70 GYR.	QC/QA
HOT-MIX ASPHALT BINDER COURSE, 1L-19.0, N70; 8"	4.0% AT 70 GYR.	QC/QA
QMP DESIGNATIONS: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

NOTES:
1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ. YD./IN.

2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22", AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

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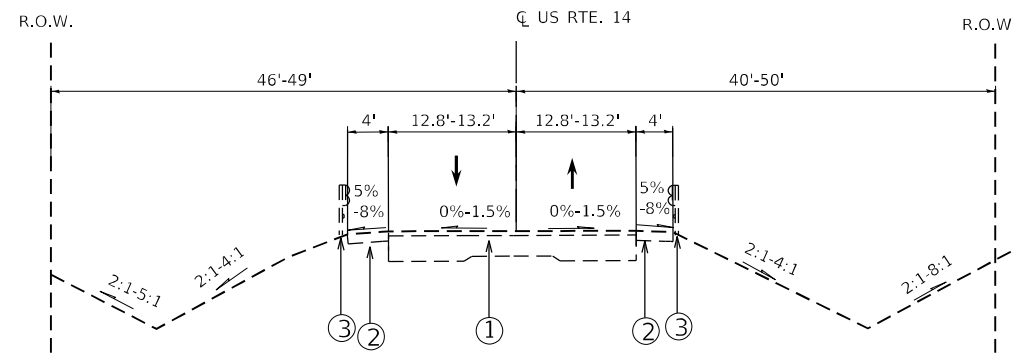
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)**

SCALE: SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	7
			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				

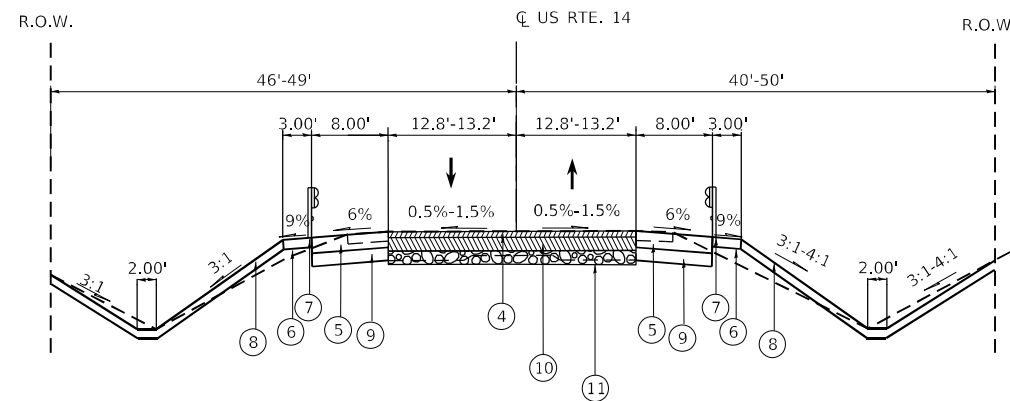


EXISTING TYPICAL SECTION

STA. 16+85 TO STA. 17+20
 STA. 17+33 TO STA. 17+68

LEGEND

- ① EXISTING HMA PAVEMENT (3/4" BONDED WEARING COURSE ON 3" HMA SURF ON 9" PCC BASE COURSE)
- ② EXISTING HMA SHOULDER
- ③ EXISTING GUARDRAIL
- ④ PROPOSED HMA SC, MIX "D", N70, 2"
- ⑤ PROPOSED 8" HMA SHOULDERS, 10"
- ⑥ PROPOSED 3' AGG SHOULDER, TYPE B, 6"
- ⑦ PROPOSED STEEL PLATE BEAM GUARDRAIL
- ⑧ PROPOSED TOPSOIL, 4" & SEEDING CLASS 3
- ⑨ PROPOSED AGGREGATE BASE COURSE, TYPE B, 12"
- ⑩ PROPOSED RECONSTRUCTION
- ⑪ AGGREGATE SUBGRADE IMPROVEMENT, 12"



PROPOSED TYPICAL SECTION

STA. 16+85 TO STA. 17+20
 STA. 17+33 TO STA. 17+68

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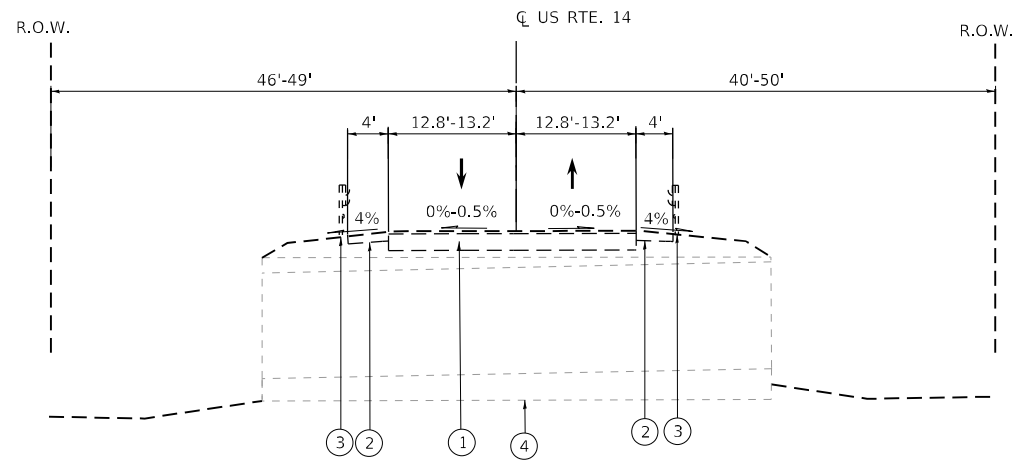
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
 U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)**

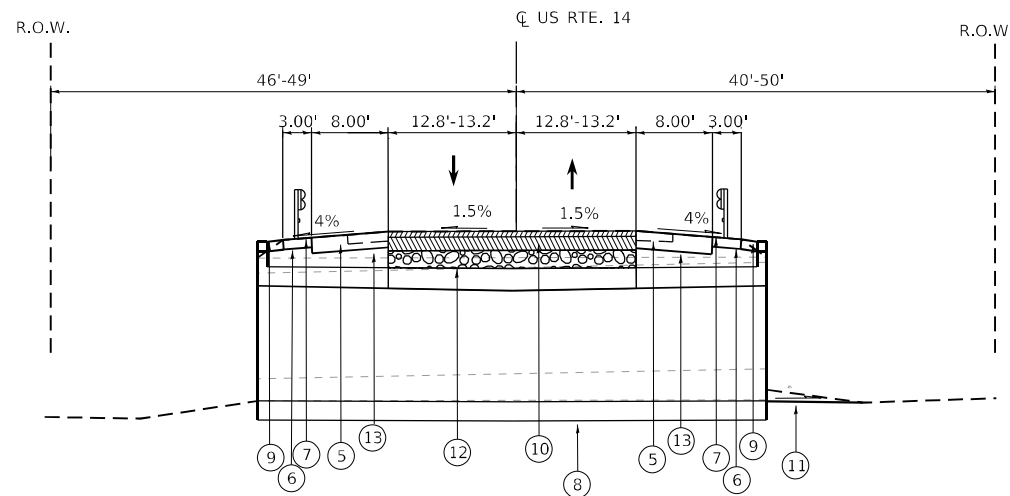
SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	8
CONTRACT NO. 62R50			ILLINOIS FED. AID PROJECT	



EXISTING TYPICAL SECTION

STA. 17+20 TO STA. 17+33



PROPOSED TYPICAL SECTION

STA. 17+20 TO STA. 17+33

LEGEND

- ① EXISTING PAVEMENT (3/4" BONDED WEARING COURSE ON 9" CONC SLAB 15' X 24')
- ② EXISTING HMA SHOULDER
- ③ EXISTING GUARDRAIL
- ④ EXISTING CULVERT
- ⑤ PROPOSED 8' HMA SHOULDERS, 10"
- ⑥ PROPOSED 3' AGG SHOULDER, TYPE B, 6"
- ⑦ PROPOSED STEEL PLATE BEAM GUARDRAIL
- ⑧ PROPOSED CULVERT (SEE CULVERT PLANS)
- ⑨ PROPOSED TOPSOIL, 4" & SEEDING CLASS 3
- ⑩ PROPOSED RECONSTRUCTION (SEE NOTE 1)
- ⑪ PROPOSED STREAM GRADING
- ⑫ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑬ PROPOSED AGGREGATE BASE COURSE, TYPE B, 12"

NOTE:

1. FOR THE PROPOSED FLEXIBLE PAVEMENT SECTION ABOVE THE PROPOSED CULVERT (STA. 17+20 TO 17+33) THE CONTRACTOR SHALL PLACE HMA BC 12" AND THEN DURING THE RESURFACING OPERATION THE CONTRACTOR SHALL MILL 2" AND OVERLAY 2" OF HMA. FOR THE PROPOSED FLEXIBLE PAVEMENT SECTION OUTSIDE THE CULVERT ALONG THE MAINLINE (STA. 16+85 TO 17+20, STA. 17+33 TO 17+68) THE CONTRACTOR SHALL PLACE HMA BINDER COURSE, IL-19.0, N70, 10" AND THEN DURING THE RESURFACING OPERATION THE CONTRACTOR SHALL MILL 2" AND OVERLAY 2" OF HMA.

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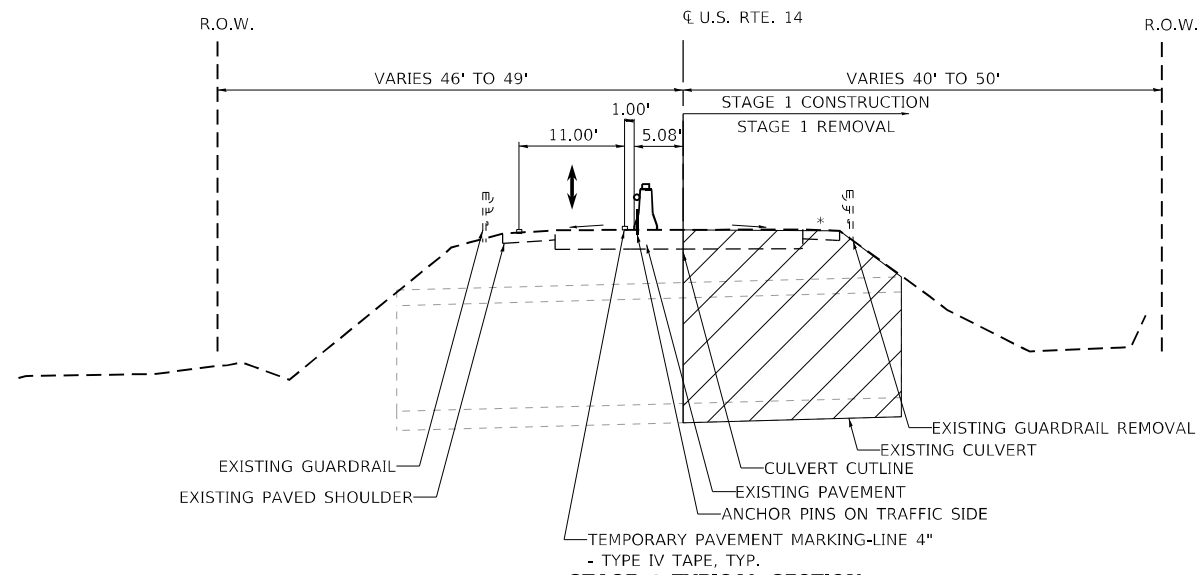
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

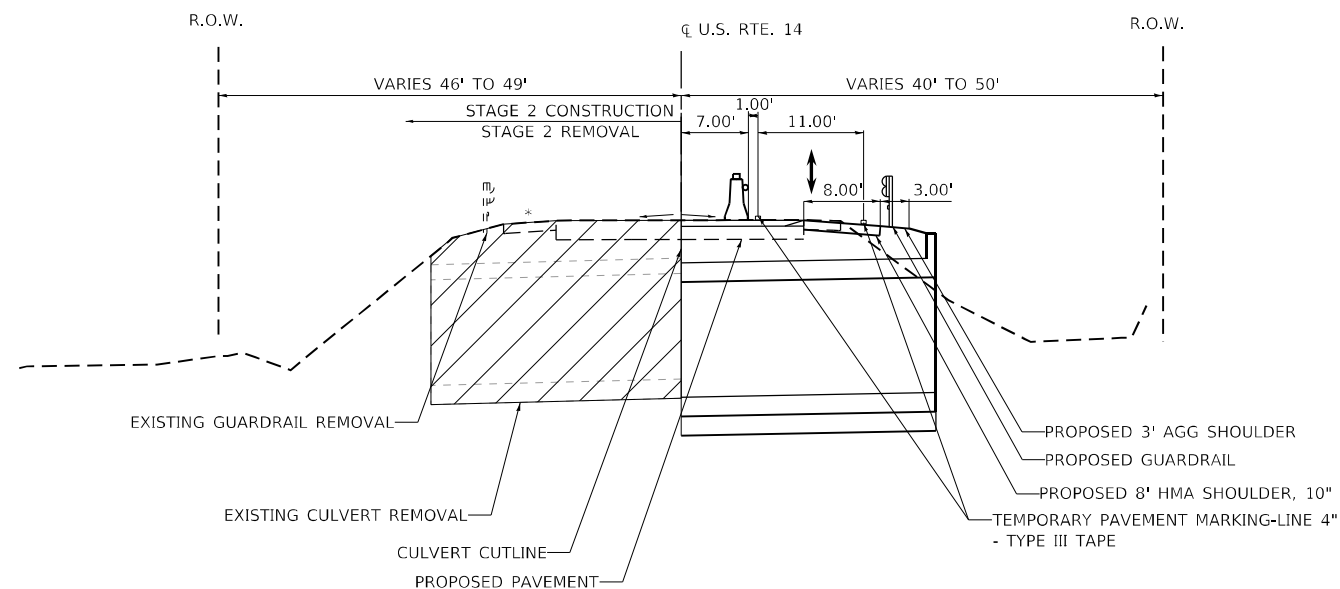
**TYPICAL SECTIONS
U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)**

SCALE: SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	9
CONTRACT NO. 62R50			ILLINOIS FED. AID PROJECT	



STAGE 1 TYPICAL SECTION
U.S. RTE. 14 STA. 14+57 TO STA. 19+65
 * EXISTING SHOULDER REMOVAL (STA. 15+80 TO 18+14)



STAGE 2 TYPICAL SECTION
U.S. RTE. 14 STA. 14+89 TO STA. 19+95
 * EXISTING SHOULDER REMOVAL (STA. 16+41 TO 18+67)

LEGEND

- DIRECTION OF TRAFFIC
- TEMPORARY CONCRETE BARRIER WALL WITH REFLECTORS (PER IDOT STANDARDS 704001 AND 782006)
- WORK AREA

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE PERMANENT TRAFFIC CONTROL DEPICTED HERE IN IS THE MINIMUM REQUIREMENT. ADDITIONAL TRAFFIC CONTROL DEVICES AS SPECIFIED IN THE HIGHWAY STANDARDS AS SHOWN IN THE INDEX OF SHEETS AND THE SPECIAL PROVISIONS SHALL BE PLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL TRAFFIC CONTROL DEVICES SHALL BE CONSIDERED INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (ARTERIALS) (D1) UNLESS OTHERWISE INDICATED WITHIN THESE GENERAL NOTES, PLANS OR SPECIAL PROVISIONS.
2. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING LABOR, SIGNS AND TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC UNLESS NOTED OTHERWISE IN THE SPECIAL PROVISIONS.
4. THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING THE WORK.
5. WORK ZONE SPEED LIMIT SHALL BE 45 MPH.
6. ALL EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL PLAN SHALL BE COVERED OR REMOVED IN ACCORDANCE WITH ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.
7. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE ALL EXISTING PAVEMENT MARKINGS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLAN.
8. THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN TRAFFIC IN ACCORDANCE WITH THE TRAFFIC CONTROL PLANS, SPECIAL PROVISIONS, APPLICABLE STATE STANDARDS, AND AS DIRECTED BY THE ENGINEER. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO IMPLEMENTING ANY CHANGES.
9. TRAFFIC CONDITIONS, ACCIDENTS, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN IN THE PLANS. THE CONTRACTOR SHALL PROMPTLY RESPOND AT THE TIME OF NOTIFICATION BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC CONTROL DEVICES.
10. THE ENGINEER SHALL BE INFORMED A MINIMUM OF 48 HOURS IN ADVANCE OF ANY PROPOSED CHANGE TO THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN.
11. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETERIORATION AFTER SEVEN (7) DAYS OF SERVICE SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. ALL MARKINGS THAT REQUIRE REPLACEMENT PRIOR TO SEVEN (7) DAYS OF SERVICE OR REPLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE.
12. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM THE TRAFFIC IMMEDIATELY WHEN THEY ARE NO LONGER NECESSARY.
13. TEMPORARY CONCRETE BARRIERS AND TEMPORARY IMPACT ATTENUATORS SHALL BE PLACED AS SHOWN IN THE PLANS. FURNISHING, INSTALLING AND RELOCATING TEMPORARY CONCRETE BARRIER AND TEMPORARY IMPACT ATTENUATORS SHALL BE IN ACCORDANCE WITH IDOT SPECIAL PROVISIONS, IDOT HIGHWAY STANDARDS, STANDARD SPECIFICATIONS, AND AS DIRECTED BY THE ENGINEER.
14. TEMPORARY CONCRETE BARRIER WALL SHALL BE CONTINUOUSLY PINNED TO THE PAVEMENT IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION WHERE A 3.5 FOOT CLEAR ZONE FREE FROM DROP-OFFS, FIXED OBJECTS, OR OTHER OBSTACLES CANNOT BE PROVIDED FOR BEHIND THE WALL. ANY HOLES LEFT IN THE EXISTING PAVEMENT AREAS THAT ARE NOT TO BE RESURFACED SHALL BE FILLED WITH A NON SHRINK GROUT TO THE SURFACE OF THE EXISTING PAVEMENT. THE FILLING OF ANY HOLES SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF TEMPORARY CONCRETE BARRIER.
15. IMMEDIATELY AFTER THE COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL RESTORE ALL PERMANENT PAVEMENT MARKINGS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES THAT WERE COVERED, REMOVED, DAMAGED OR OTHERWISE AFFECTED BY CONSTRUCTION.
16. THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE AND EROSION CONTROL PROTECTION DURING ALL PHASES OF CONSTRUCTION.

SEQUENCING NOTES:

PRE-STAGE:

TEMPORARY TRAFFIC SIGNALS WILL BE INSTALLED AT EACH END OF THE WORK ZONE. TEMPORARY ROADWAY LIGHTING WILL ALSO BE INSTALLED (SEE IDOT DISTRICT 1 STD. BE-805). TEMPORARY PAVEMENT WORK WILL BE INSTALLED ALONG THE SOUTHBOUND SHOULDER (SEE IDOT STANDARD 701101-04). TEMPORARY CULVERT RETENTION SYSTEM WILL BE PLACED FROM BELOW TO ENSURE THE STRUCTURAL INTEGRITY OF THE EXISTING STRUCTURE.

STAGE-1:

STAGE 1 WILL CONSIST OF SHIFTING NORTHBOUND AND SOUTHBOUND TRAFFIC TO THE SOUTHBOUND LANE. AN 11- FOOT LANE WILL BE PROVIDED BY UTILIZING 5.5- FEET OF THE EXISTING SOUTHBOUND LANE AND 5.5- FEET OF THE EXISTING PAVED SHOULDER IN ORDER TO SHIFT TRAFFIC AS FAR AWAY FROM THE CUT LINE AS POSSIBLE. TEMPORARY CONCRETE BARRIERS WILL BE INSTALLED TO PROTECT THE WORK ZONE TO THE EAST. TRAFFIC WILL BE SIGNALIZED WITH STOP BARS TO THE NORTH AND SOUTH OF THE PROJECT LIMITS. THE EAST SECTION OF THE CULVERT WILL THEN BE REMOVED AND REPLACED WITH A CAST-IN-PLACE REINFORCED CONCRETE BOX CULVERT. PAVEMENT RECONSTRUCTION WILL BE PROVIDED OVER THE NEW CULVERT SECTION AND THE SHOULDERS WILL BE WIDENED TO 8-FOOT. THE GUARDRAIL WILL ALSO BE REMOVED AND REPLACED.

STAGE-2:

STAGE 2 WILL CONSIST OF SHIFTING NORTHBOUND AND SOUTHBOUND TRAFFIC TO THE NORTHBOUND LANE OVER THE NEWLY CONSTRUCTED SECTION OF THE CULVERT. AN 11-FOOT LANE WILL BE PROVIDED BY UTILIZING 6- FEET OF THE EXISTING NORTHBOUND LANE WITH THE NEW PROPOSED PAVEMENT AND 5- FEET OF THE NEWLY WIDENED PAVED SHOULDER. TEMPORARY CONCRETE BARRIERS WILL BE SHIFTED TO PROTECT THE NEW WORK ZONE TO THE WEST. TRAFFIC WILL BE SIGNALIZED WITH STOP BARS TO THE NORTH AND SOUTH OF PROJECT LIMITS SIMILAR TO STAGE 1. THE WEST SECTION OF THE CULVERT WILL THEN BE CONSTRUCTED. PAVEMENT RECONSTRUCTION WILL BE PROVIDED OVER THE NEW CULVERT SECTION AND THE SHOULDERS WILL BE WIDENED TO 8-FOOT. THE GUARDRAIL ON THE WEST SIDE OF THE ROADWAY WILL BE REMOVED AND REPLACED. UPON COMPLETION OF THE WESTERN SECTION, THE TEMPORARY CONCRETE BARRIERS WILL BE REMOVED.

STAGE-3:

PAVEMENT RESURFACING WILL OCCUR. THE ROADWAY WILL BE RESTRIPEDED ACCORDING TO THE FINAL STRIPING PLAN UTILIZING IDOT STANDARD 701311-03. REMOVE TEMPORARY PAVEMENT INSTALLED DURING PRE-STAGE. REMOVE TEMPORARY LIGHTING AND SIGNAL ITEMS.

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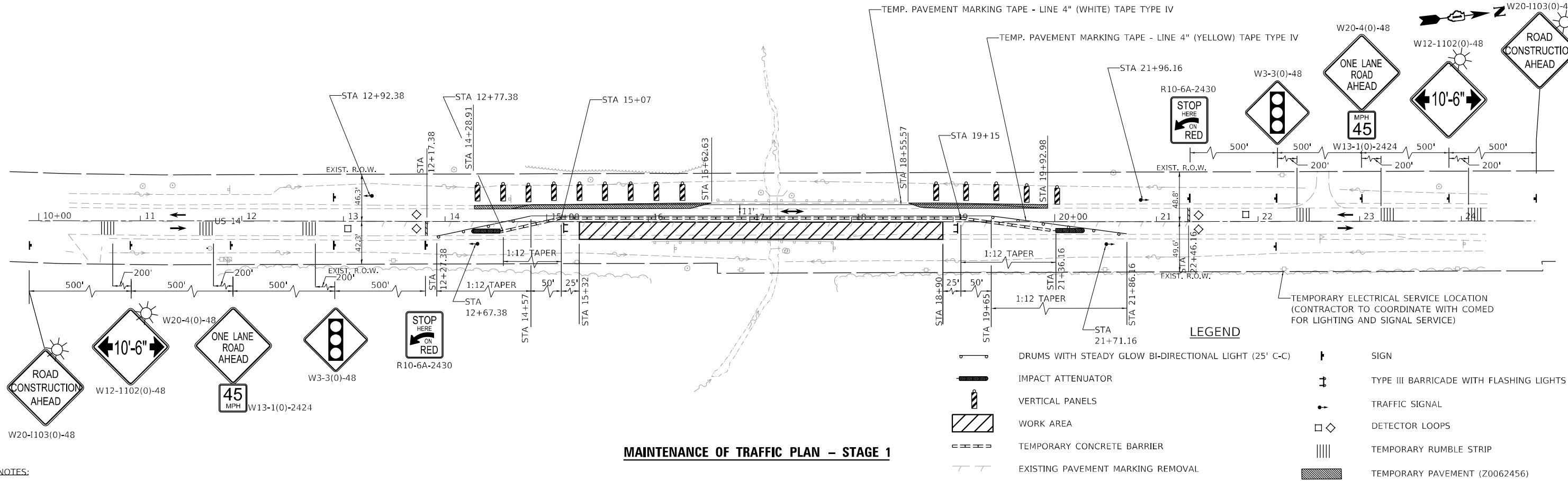
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 DEPARTMENT OF TRANSPORTATION**

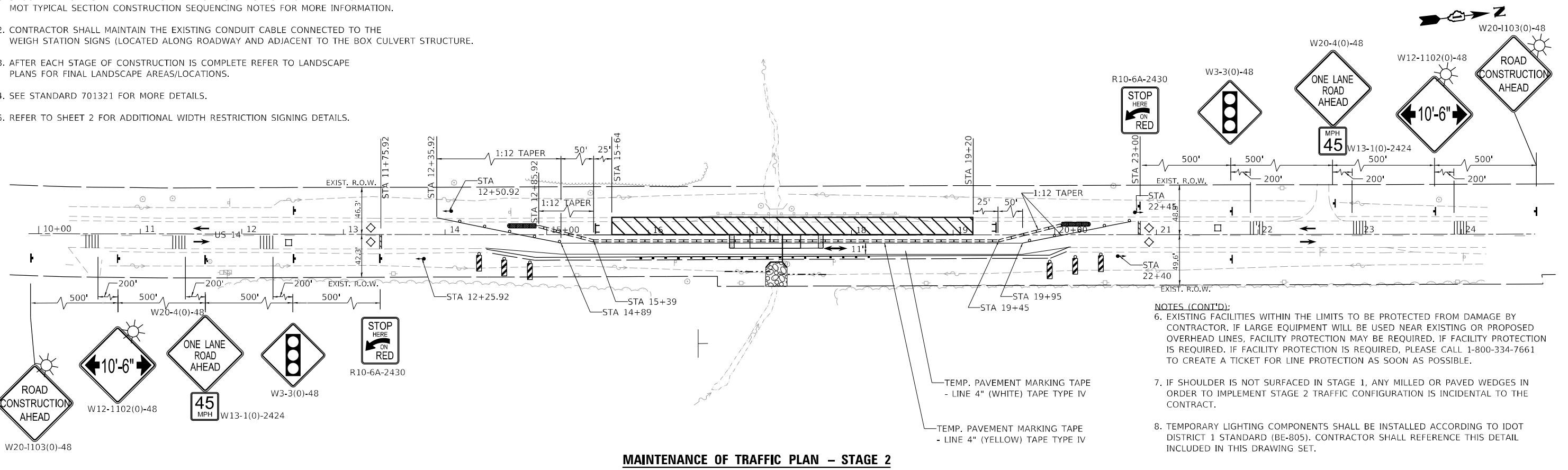
**MAINTENANCE OF TRAFFIC - TYPICAL SECTIONS
 U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. 15+63 TO STA. 18+86

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	10
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				



- NOTES:**
1. SHOULDER WORK SHALL BE PERFORMED DURING PRE-STAGE MOT. REFER TO MOT TYPICAL SECTION CONSTRUCTION SEQUENCING NOTES FOR MORE INFORMATION.
 2. CONTRACTOR SHALL MAINTAIN THE EXISTING CONDUIT CABLE CONNECTED TO THE WEIGH STATION SIGNS (LOCATED ALONG ROADWAY AND ADJACENT TO THE BOX CULVERT STRUCTURE).
 3. AFTER EACH STAGE OF CONSTRUCTION IS COMPLETE REFER TO LANDSCAPE PLANS FOR FINAL LANDSCAPE AREAS/LOCATIONS.
 4. SEE STANDARD 701321 FOR MORE DETAILS.
 5. REFER TO SHEET 2 FOR ADDITIONAL WIDTH RESTRICTION SIGNING DETAILS.



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC PLAN - STAGE 1 & 2
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)**

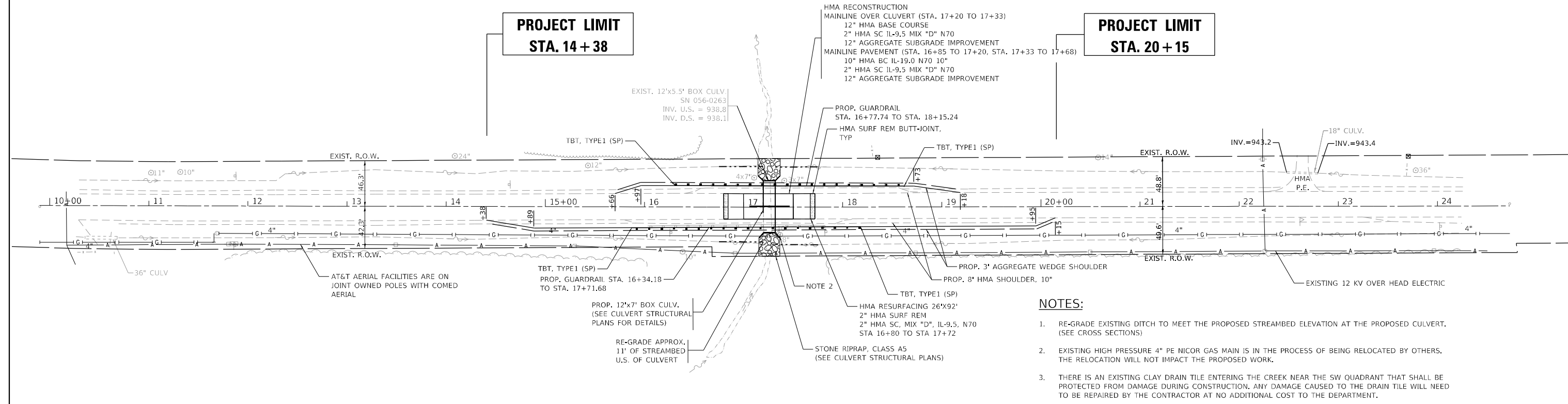
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F.A.P. RTE. 66	SECTION 2018-114-I	COUNTY MCHENRY	TOTAL SHEETS 41	SHEET NO. 11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R50	

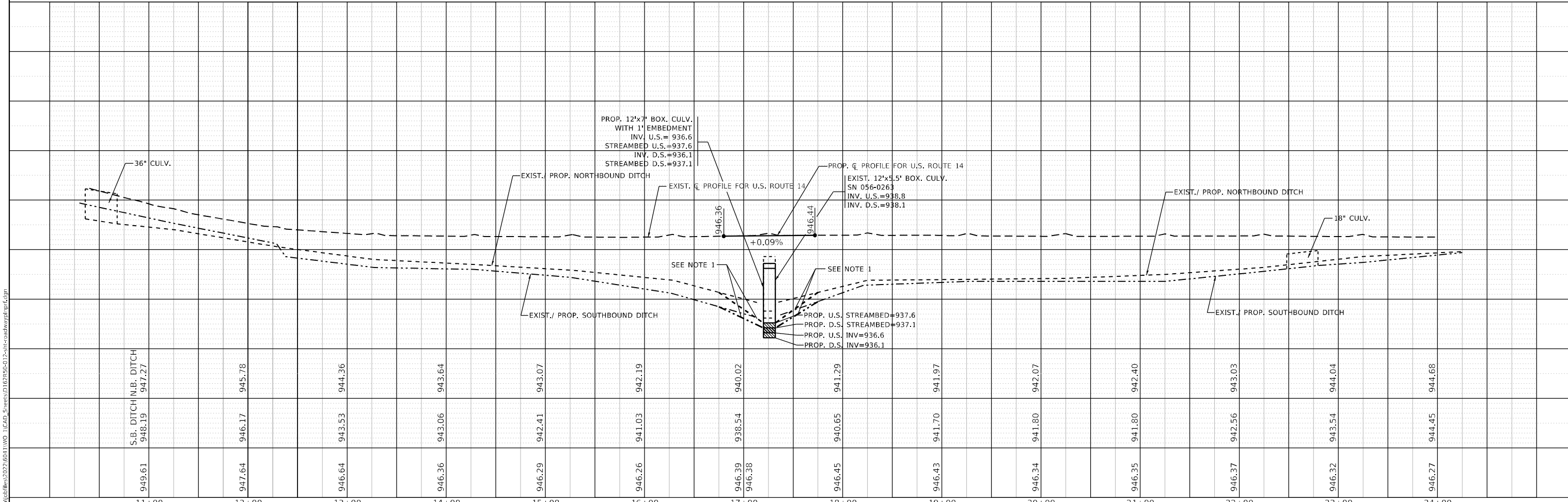


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- NOTES:**
- RE-GRADE EXISTING DITCH TO MEET THE PROPOSED STREAMBED ELEVATION AT THE PROPOSED CULVERT. (SEE CROSS SECTIONS)
 - EXISTING HIGH PRESSURE 4" PE NICOR GAS MAIN IS IN THE PROCESS OF BEING RELOCATED BY OTHERS. THE RELOCATION WILL NOT IMPACT THE PROPOSED WORK.
 - THERE IS AN EXISTING CLAY DRAIN TILE ENTERING THE CREEK NEAR THE SW QUADRANT THAT SHALL BE PROTECTED FROM DAMAGE DURING CONSTRUCTION. ANY DAMAGE CAUSED TO THE DRAIN TILE WILL NEED TO BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE DEPARTMENT.



11+00	12+00	13+00	14+00	15+00	16+00	17+00	18+00	19+00	20+00	21+00	22+00	23+00	24+00
949.61	947.64	946.64	946.36	946.29	946.26	946.39	946.45	946.43	946.34	946.35	946.37	946.32	946.27
948.19	946.17	943.53	943.06	942.41	941.03	938.54	940.65	941.70	941.80	941.80	942.56	943.54	944.45
947.27	945.78	944.36	943.64	943.07	942.19	940.02	941.29	941.97	942.07	942.40	943.03	944.04	944.68



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN & PROFILE
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)**

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	12
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				

EROSION AND SEDIMENT CONTROL NOTES

1. ALL CONTROL MEASURES NECESSARY MUST MEET THE MINIMUM REQUIREMENTS AS DESCRIBED IN THE LATEST EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION. ADDITIONAL DETAILS AND BMPs ARE ALSO AVAILABLE AND CAN BE UTILIZED AS SHOWN IN THE ILLINOIS URBAN MANUAL, REVISED TO LATEST VERSION AS AMENDED. ALL ESC MEASURES WILL BE MAINTAINED IN ACCORDANCE WITH THE IDOT EROSION AND SEDIMENT CONTROL FIELD GUIDE FOR CONSTRUCTION INSPECTION AND IDOT'S BEST MANAGEMENT PRACTICES - MAINTENANCE GUIDE: (HTTP://WWW.IDOT.ILLINOIS.GOV/TRANSPORTATION-SYSTEM/ENVIRONMENT/EROSION-AND-SEDIMENT-CONTROL).
2. ALL THE SOIL EROSION AND SEDIMENT CONTROL FEATURES MUST BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE. SOIL DISTURBANCE MUST BE PHASED OR ENACTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES MUST CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY AND/OR PERMANENT MEASURES.
3. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE THAT SEDIMENT TRANSPORT OFF THE SITE IS REDUCED BY A COMBINATION OF MINIMIZATION OF EROSION AT THE SOURCE AND THE INSTALLATION OF SPECIFIC MEASURES TO CONTROL OR REDUCE THE TRANSPORT OF SEDIMENT. A COPY OF THE EROSION AND SEDIMENT CONTROL SCHEDULE BEING IMPLEMENTED BY THE CONTRACTOR MUST BE APPROVED BY THE ENGINEER, WILL BE ON THE CONSTRUCTION SITE AT ALL TIMES.
4. ALL RUNOFF ORIGINATING ON DISTURBED AREAS ASSOCIATED WITH THIS PROJECT WILL PASS THROUGH ONE OR MORE MEASURES THAT WILL MINIMIZE THE OFF-SITE SEDIMENT IMPACTS OF THE CONSTRUCTION ACTIVITIES.
5. DISTURBED AREAS ARE TO BE PROTECTED FROM EROSION IN A TIMELY MANNER. UPON COMPLETION OF GRADING OR CONSTRUCTION ACTIVITY, THE AREA WILL BE STABILIZED (USING PERMANENT MEASURES WHEN POSSIBLE) WITHIN ONE (1) CALENDAR DAY.
6. THE CONTRACTOR MUST CLEAN UP, GRADE THE WORK AREA AS THE PROJECT PROGRESSES AND INSTALL EROSION PROTECTION TO ELIMINATE THE CONCENTRATION OF RUNOFF, OR MUST INSTALL APPROPRIATE SEDIMENT CONTROL DEVICES TO TRAP SEDIMENT. PAVEMENT MUST BE CLEANED DAILY OR AS NECESSARY TO REMOVE EARTHEN MATERIAL TO THE SATISFACTION OF THE ENGINEER OR AUTHORIZED IDOT PERSONNEL.
7. STABILIZATION OF CUT OR FILL SLOPES WITH TEMPORARY OR PERMANENT EROSION CONTROL MEASURES IS REQUIRED WHENEVER THE CUT OR FILL ACTIVITY REACHES 10-FT VERTICALLY OR THE FINISHED SLOPE EQUALS 30-FT, WHICHEVER IS MORE RESTRICTIVE. ONCE THE STABILIZATION MEASURES ARE INSTALLED, THE PLACEMENT OF FILL EXCAVATION ACTIVITIES ARE ALLOWED TO PROCEED.
8. THE CONTRACTOR WILL ASSUME RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DURING CONSTRUCTION. THE CONTRACTOR SHALL DESIGNATE ONE OF HIS EMPLOYEES TO BE RESPONSIBLE FOR IMPLEMENTATION OF THE EROSION AND SEDIMENT CONTROL PLAN ON ALL DISTURBED AREAS THROUGHOUT THE PROJECT.
9. THE CONTRACTOR'S REPRESENTATIVE HAS TO BE KNOWLEDGEABLE ABOUT INSTALLATION AND MAINTENANCE OF THE REQUIRED MEASURES AND HAVE TAKEN AN ILLINOIS DEPARTMENT OF TRANSPORTATION OR APPROVED EQUAL EROSION AND SEDIMENT CONTROL COURSE. THIS PERSON SHALL HAVE THE AUTHORITY TO CARRY OUT THE IMPLEMENTATION OF ANY INSTRUCTION CONCERNING THE EROSION AND SEDIMENT CONTROL PLAN PROVIDED BY THE ENGINEER. THIS INDIVIDUAL AND THE ENGINEER MUST MAKE INSPECTIONS A MINIMUM OF ONCE EVERY SEVEN DAYS OF THE FOLLOWING:
 - A. DISTURBED AREAS OF THE PROJECT SITE THAT HAVE NOT BEEN FULLY STABILIZED.
 - B. STRUCTURAL CONTROL MEASURES (SUCH AS PERIMETER EROSION BARRIER, ETC.)
 - C. LOCATIONS WHERE VEHICLES ENTER OR EXIT THE PROJECT SITE.
 - D. AN ADDITIONAL INSPECTION OF THE ITEMS LISTED ABOVE MUST BE MADE WITHIN 24-HOURS AFTER A 24-HOUR RAINFALL OR EQUIVALENT SNOWFALL EVENT GREATER THAN 0.5-INCH. DURING WINTER MONTHS, ALL MEASURES MUST BE CHECKED BY THE CONTRACTOR AFTER EACH SIGNIFICANT SNOWMELT.

14. TOPSOIL PLACEMENT:
TOPSOIL WILL BE PLACED ON FINAL SLOPES WHICH WILL NOT BE DISTURBED BY FUTURE CONSTRUCTION. TOPSOIL WILL NOT BE PLACED ON SURFACES WHICH WILL BE PAVED IN THE FUTURE NOR ON TEMPORARY STEEP SLOPES.
15. IN AREAS WHERE A PERMANENT VEGETATIVE COVER IS PRACTICABLE AND INCLUDED IN THE CONTRACT DOCUMENTS, A SPECIAL EFFORT SHOULD BE MADE TO ESTABLISH A COVER AS SOON AS A DISTURBED AREA IS BROUGHT TO FINAL GRADE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG FINAL GRADING AND SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
16. THE CONTRACTOR'S REPRESENTATIVE AND THE ENGINEER MUST KEEP A WRITTEN REPORT SUMMARIZING THE REQUIRED INSPECTIONS. THE REPORTS MUST BE KEPT AT THE SITE DURING CONSTRUCTION. THE REPORT MUST ALSO BE RETAINED FOR THREE YEARS FROM THE DATE THE SITE IS FINALLY STABILIZED.
17. ANY SEDIMENT LADEN DEWATERING DISCHARGE MUST BE DIRECTED TO AN APPROVED SEDIMENT TRAPPING CONTROL MEASURE PRIOR TO RELEASE FROM THE PROJECT SITE.
19. SEEDING USAGE

CLASS 3:
USED ON FINAL DISTURBED CONSTRUCTION AREAS INDICATED ON THE PLANS.

TEMPORARY EROSION CONTROL SEEDING:
USED IN AREAS REQUIRING SHORT TERM TEMPORARY SEEDING DURING CONSTRUCTION.

20. THE CONTRACTOR MUST COOPERATE WITH THE ENGINEER AND HIS/HER REPRESENTATIVE WHO WILL MAKE SITE VISITS TO REVIEW THE COMPLIANCE OF THE PLANS IN THE FIELD AND AUDIT IF NECESSARY. THE CONTRACTOR MUST PREPARE THE LOGS AND RECORDS WHEN REQUIRED AND SUBMIT TO IDOT AND/OR APPROPRIATE AGENCIES.

21. THE INSTALLATION, MAINTENANCE, REMOVAL AND RESTORATION OF THE AREA DISTURBED BY THE PLACEMENT OF THE PERIMETER EROSION BARRIER ARE INCLUDED IN THE CONTRACT UNIT PRICE FOR PERIMETER EROSION BARRIER. AFTER ALL PERIMETER EROSION BARRIER IS REMOVED, THE AREAS DAMAGED BY THE PERIMETER EROSION CONTROL BARRIER MUST BE RESTORED TO THEIR ORIGINAL CONDITION.

22. STABILIZATION MEASURES SHALL BE INITIATED IMMEDIATELY WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN ONE (1) DAY AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED ON ALL DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION WILL NOT OCCUR FOR A PERIOD OF FOURTEEN (14) OR MORE CALENDER DAYS.

EROSION AND SEDIMENT CONTROL STRATEGY:

1. ERECT PERIMETER EROSION BARRIERS AND TEMPORARY FENCES AS SHOWN ON PLANS.
2. CLEAR AND GRUB, REMOVE EXISTING TREES AND BUSHES AS NECESSARY.
3. INSTALL EROSION AND SEDIMENT CONTROL MEASURES PER THE PLANS.
4. INSPECT AND MAINTAIN ALL EROSION AND SEDIMENT CONTROL MEASURES FOR THE DURATION OF CONSTRUCTION.
5. TEMPORARY STABILIZATION OF EACH STAGE SHOULD BE COMPLETED BEFORE WORK IS MOVED TO SUBSEQUENT STAGES.
6. STABILIZE DISTURBED AREAS WITH TEMPORARY EROSION CONTROL MEASURES. USE THE PERMANENT SEEDING WITH EROSION CONTROL BLANKET FOR PERMANENT STABILIZATION AS SHOWN ON THE PLANS.
7. WHEN THE PERMANENT STABILIZATION IS ESTABLISHED, REMOVE ALL REMAINING TEMPORARY EROSION CONTROL MEASURES.

SOIL PROTECTION SCHEDULE:

STABILIZATION TYPE	JAN.	FEB.	MAR.	APR.	MAY.	JUN.	JUL.	AUG.	SEPT.	OCT.	NOV.	DEC.
PERMANENT SEEDING						→					→	
DORMANT SEEDING		→								→		→
TEMPORARY SEEDING									→			
EROSION CONTROL BLANKET											→	

HIGHWAY STANDARDS:

STD. NO.	TITLE
280001	TEMPORARY EROSION CONTROL SYSTEMS

MODEL: Default
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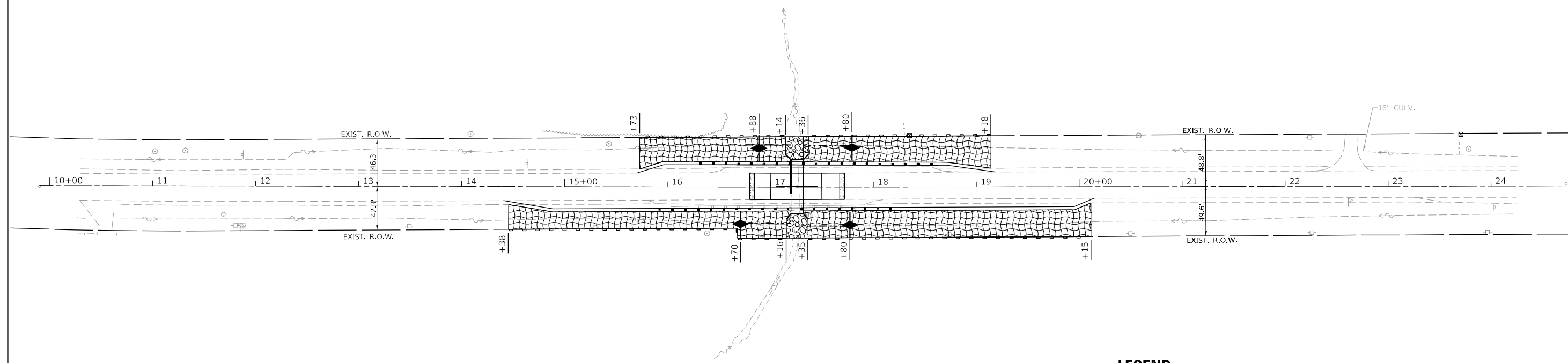
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PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**




**EROSION AND SEDIMENT CONTROL NOTES
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	13
CONTRACT NO. 62R50			ILLINOIS FED. AID PROJECT	



LEGEND

-  TEMPORARY EROSION CONTROL BLANKET & TEMPORARY EROSION CONTROL SEEDING
-  TEMPORARY DITCH CHECK
-  PERIMETER EROSION BARRIER

NOTE

1. EROSION CONTROL MEASURES SHALL BE PERFORMED AS FOLLOWS:
 - A. STAGE 1 EROSION CONTROL MEASURES LOCATION DURING STAGE 2. SEE MOT PLAN.
 - B. STAGE 2 EROSION CONTROL MEASURES LOCATION DURING STAGE 1. SEE MOT PLAN.

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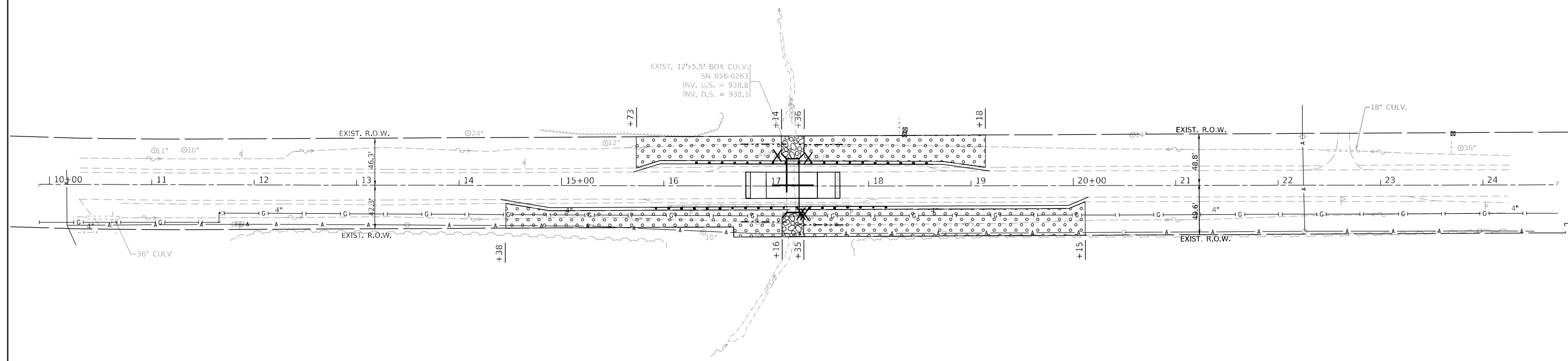
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PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**



**EROSION CONTROL PLAN
U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	14
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				



LEGEND

-  TOP SOIL 4" & SEEDING, CLASS 3 WITH EROSION CONTROL BLANKET
-  STUMP REMOVAL ONLY

STUMP REMOVAL SCHEDULE

POINT	STATION	NORTHING	EASTING	UNITS
31	17+10.7	2106975.1784	911123.7385	28
32	17+41.2	2107004.9857	911130.3458	21
33	17+34.8	2106992.1380	911183.1687	10
34	17+36.7	2106994.1781	911184.0130	8
TOTAL				67

NOTE

1. LANDSCAPING SHALL BE PERFORMED AS FOLLOWS:
 - A. EAST SIDE OF US RTE 14 DURING STAGE 1. SEE MOT PLAN.
 - B. WEST SIDE OF US RTE 14 DURING STAGE 2. SEE MOT PLAN.

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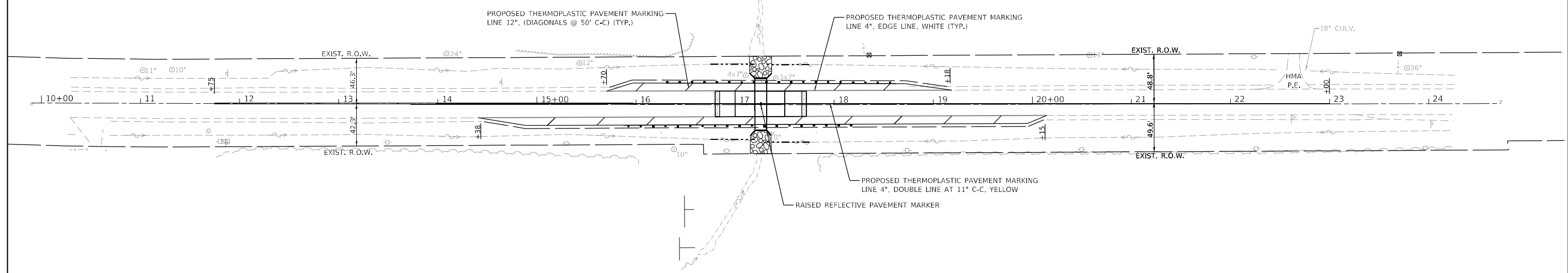
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PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**LANDSCAPING PLAN
U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	15
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				



NOTE

1. SHORT TERM PAVEMENT MARKINGS ON FINAL WEARING SURFACE SHALL BE TYPE III TEMPORARY TAPE.

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PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)	
SCALE: 1"=50'	SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	16
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				

Bench Mark: Chiseled "□" on the Northwest Corner of the North Headwall on S.N. 056-0263. Elev. = 944.07

Existing Structure: S.N. 056-0263 was built in 1931 as S.B.I Rte. 23, Section 21-G, and reconstructed in 1976 under F.A. Rte. 82, Section 21RS-1. The existing structure consists of one 5'-6" x 12'-0" box culvert 53'-0" long. The culvert has no skew with 45° flared wingwalls on the west and 0° flared wingwalls on the east.

No Salvage.

DESIGN STRESSES

FIELD UNITS

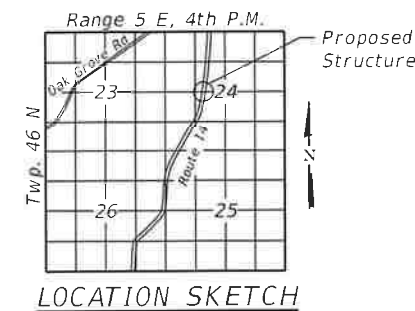
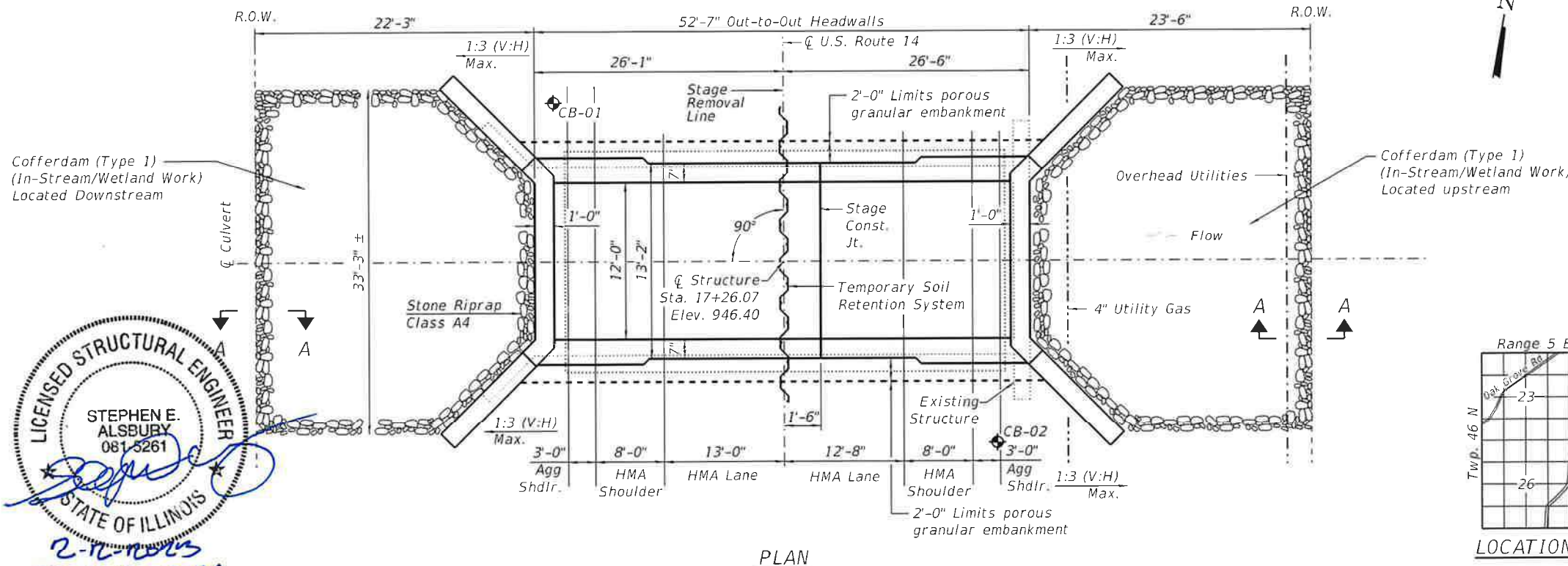
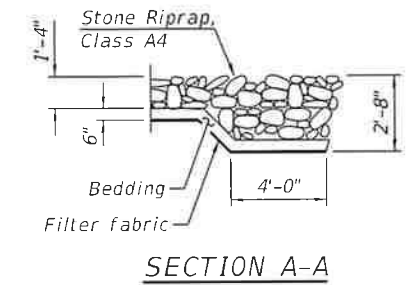
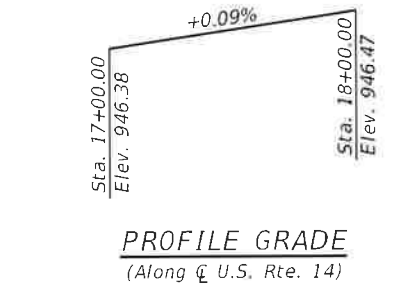
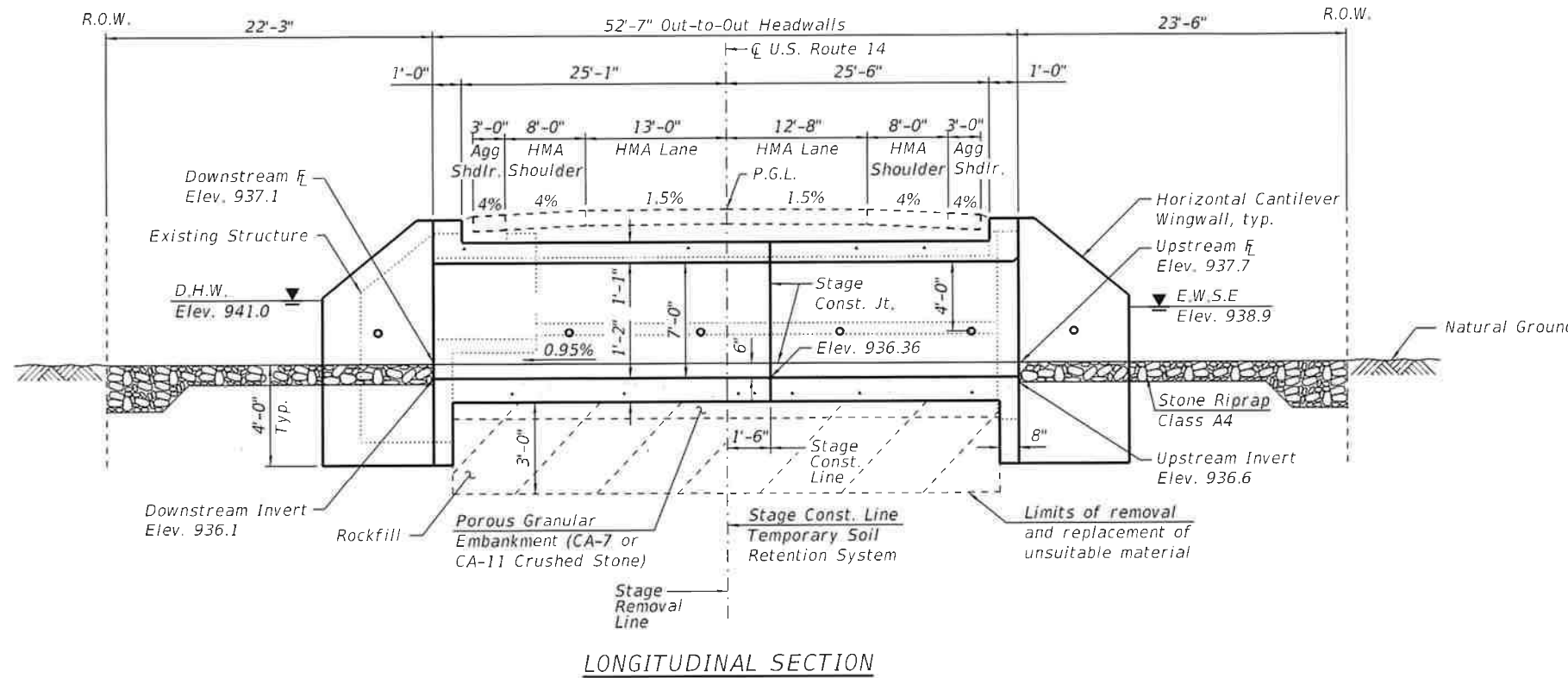
$f'_c = 3,500$ psi
 $f_y = 60,000$ psi (Reinforcement)

LOADING HL-93 AND DESIGN TANDEM

Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS

2020 AASHTO LRFD Bridge Design Specifications, 9th Edition



INDEX OF SHEETS

1. General Plan & Elevation
2. General Data
3. Staging Plan
4. Culvert Details - Top Slab
5. Culvert Details - Bottom Slab
6. Culvert Details 3
7. Culvert Details 4
8. Bar Splicer Assembly and Mechanical Splicer Details
9. Boring Logs
10. Reference 1 - 1999 Culvert Extension
11. Reference 2 - 1999 Culvert Extension
12. Reference 3 - 2019 Slab on Grade Retrofit

GENERAL PLAN & ELEVATION
U.S. 14 OVER DITCH
3 MILES SOUTH OF STATE LINE
F.A.P. 66 - SECTION 2018-114-I
MCHENRY COUNTY
STA. 17+26.07
STRUCTURE NO. 056-8300

LICENSED STRUCTURAL ENGINEER
STEPHEN E. ALSBURY
 081-5261
 STATE OF ILLINOIS
 2-11-2023
 Exp 11-30-2024

FILE NAME: \\labna\labfiles\2022\6041\W0_1\CADD_Sheets\0568300-62R50-001-GRP&E.dgn

ABNA
 DESIGN FIRM REG. 184.002117
 745 McClintock Drive
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 Fax: 773-239-3728

USER'S NAME	DESIGNED	REVISIONS
M. Winner	M. Winner	-
S. Alsbury	S. Alsbury	-
M. Winner	M. Winner	-
09.02.2022	09.02.2022	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION
STRUCTURE NO. 056-8300
 SHEET 1 OF 12 SHEETS

F.A.P. KIL	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	17

CONTRACT NO. 62R50
 ILLINOIS FED AID PROJECT

GENERAL NOTES

- Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
- Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.
- The Contractor shall make allowance for the deflection of forms, shrinkage and settlement of falsework, in addition to allowance for dead load deflection.
- Reinforcement bars shall conform to the requirements of ASTM A 706 Gr. 60.
- Precast concrete alternate will not be allowed.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Porous Granular Embankment	Cu. Yd.	18
Stone Riprap, Class A4	Sq. Yd.	115
Filter Fabric	Sq. Yd.	274
Removal of Existing Structures	Each	1
Structure Excavation	Cu. Yd.	194
Removal and Disposal of Unsuitable Material for Structures	Cu. Yd.	109
Reinforcement Bars	Pound	19,920
Bar Splicers	Each	76
Name Plates	Each	1
Temporary Soil Retention System	Sq. Ft.	335
Concrete Box Culverts	Cu. Yd.	102.5
Membrane Waterproofing System for Buried Structures	Sq. Yd.	97
Rock Fill	Cu. Yd.	87
GeoTextile Retaining Wall	Sq. Ft.	54
Cofferdam (Type 1)(In-Stream/Wetland Work)	Each	2

STATION 17+26.07
 BUILT 20__ BY
 STATE OF ILLINOIS
 LOADING HL-93
 STRUCTURE NO. 056-8300

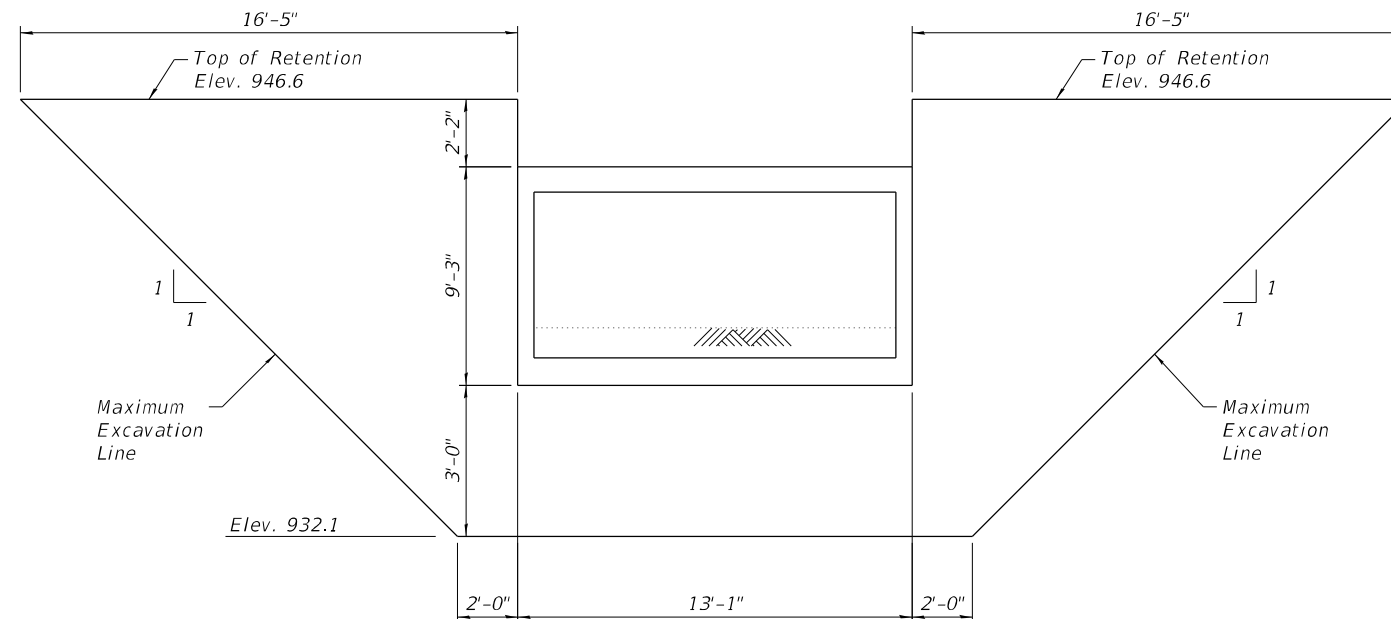
NAME PLATE
 See Std. 515001

DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (ft.)	D.S. Invert	U.S. Invert
	933.1	933.6

WATERWAY INFORMATION

Drainage Area = 0.9781 Square Miles					Existing Overtopping Elev. = 946.3 @ Sta. 16+00 Proposed Overtopping Elev. = 946.3 @ Sta 16+00				
Flood Event	Freq. Yr.	Discharge cfs	Waterway Opening - sf		Natural H.W.E - ft	Head - ft		Headwater Elevation - ft	
			Existing	Proposed		Existing	Proposed	Existing	Proposed
	2	69	12.0	14.2	939.8	0.7	0.0	940.5	939.8
	10	179	18.0	32.4	940.3	1.8	0.0	942.1	940.3
Design	50	306	26.4	40.8	941.0	2.5	1.1	943.5	942.1
Base	100	368	28.8	43.2	941.2	2.9	1.5	944.1	942.7



TEMPORARY SOIL RETENTION DETAIL

Note to Contractor:
 Culvert bottom slab to be buried 1'-0" below existing grade.
 Salvage soil to place in culvert.
 Cost included with Pay Item Structure Excavation.

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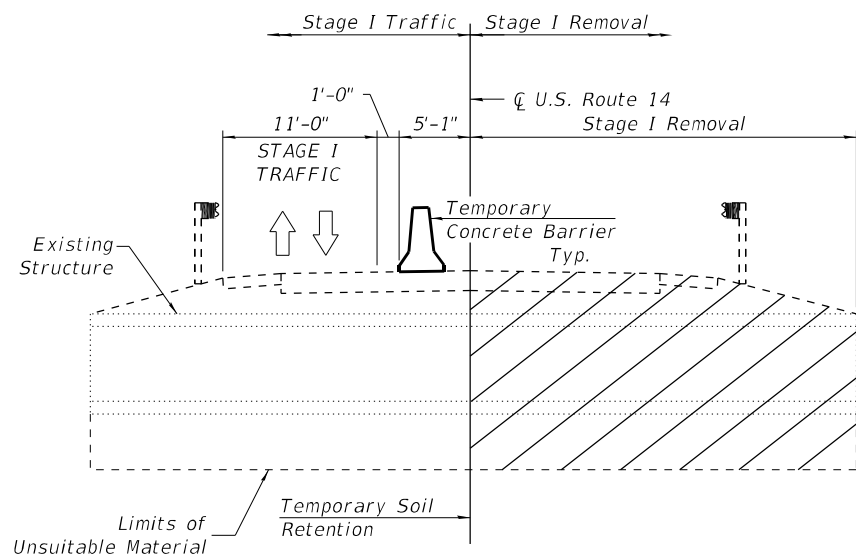
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	CHECKED - S. Alsbury	REVISED -
PLOT SCALE: N.T.S.	DRAWN - M. Winner	REVISED -
PLOT DATE: 2/2/2023	CHECKED - 09.02.2022	REVISED -

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 DEPARTMENT OF TRANSPORTATION

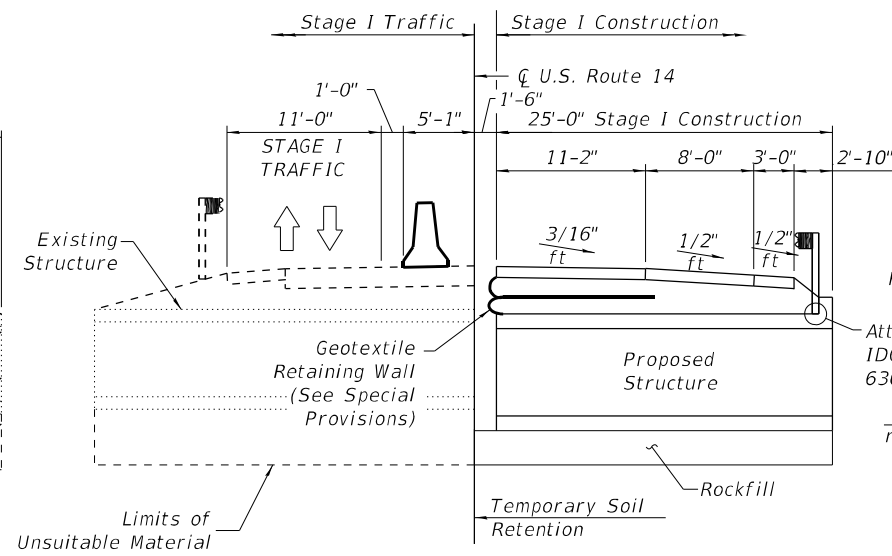
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 STRUCTURE NO. 056-8300

SHEET 2 OF 12 SHEETS

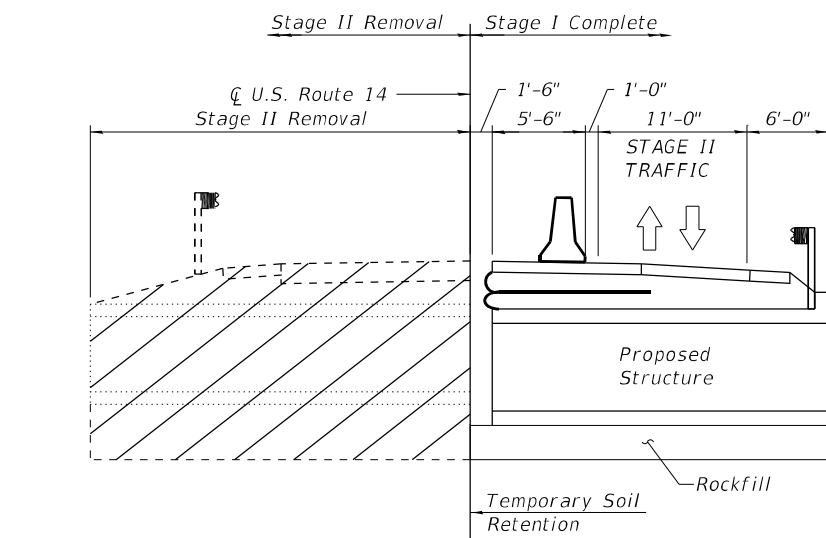
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	18
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				



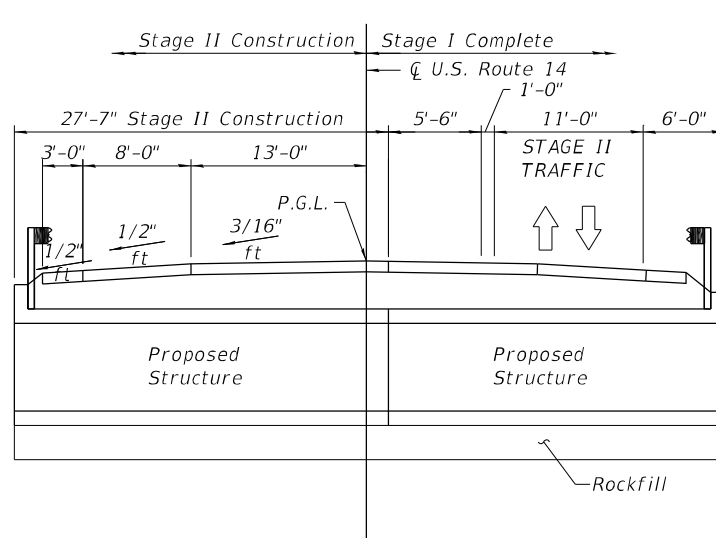
STAGE I TRAFFIC



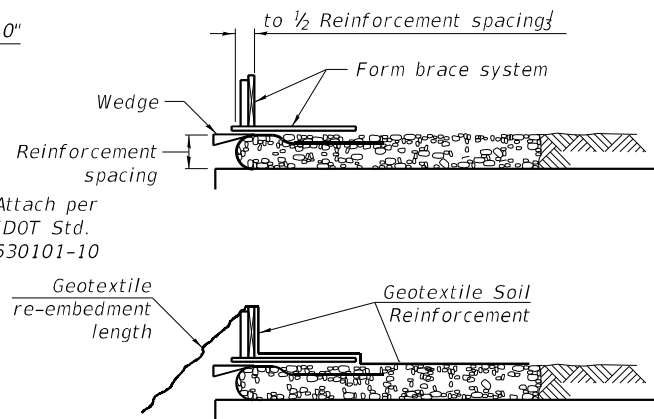
STAGE I CONSTRUCTION



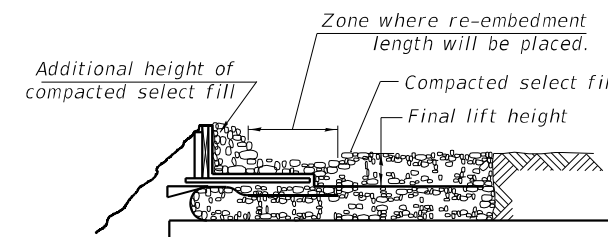
STAGE II TRAFFIC



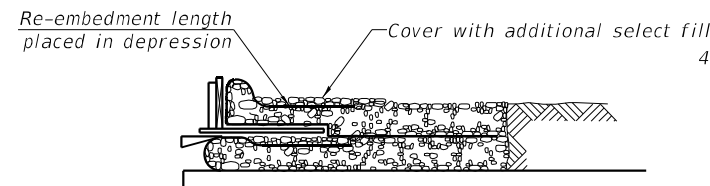
STAGE II CONSTRUCTION



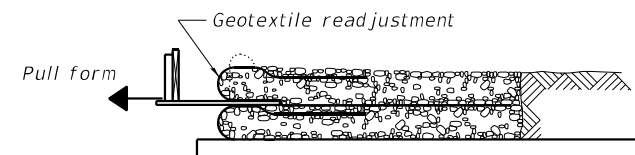
1. Place form brace system on completed reinforcement level; back from the finished fabric face a distance of $\frac{1}{3}$ to $\frac{1}{2}$ the geotextile reinforcement spacing.
2. Position fabric so that the required geotextile re-embedment length extends over the top of the form brace and the design reinforcement width is placed with no slack against the previous level.



3. Compact select fill material in lifts to final lift height, create (± 3 ") depression in zone where re-embedment length will be located and place additional height of compacted select fill against form brace.



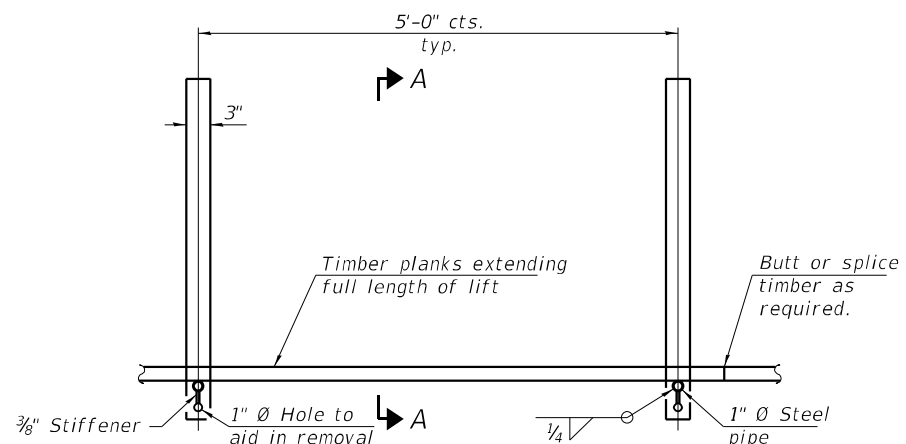
4. Fold geotextile re-embedment length back over form brace into zone where depression was made in select fill and place additional select fill (± 3 ") to embed geotextile and bring to final lift height.



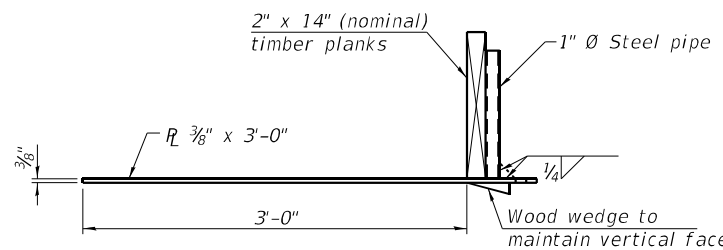
5. Pull form brace outward allowing geotextile face to slightly readjust to form tight round face level with plan reinforcement spacing.

GEOTEXTILE WALL CONSTRUCTION SEQUENCE

Note:
The geotextile soil reinforcement shall have a minimum allowable tensile strength (T min.) of 40 lb./in. as determined by the procedure described in the Special Provision. The computations supporting the determination of (T min.) shall be submitted to the engineer for approval.



PLAN



SECTION A-A

Note:
This is a suggested detail, the Contractor is responsible for the design of the form brace system to be used.

GEOTEXTILE FORM BRACE DETAIL

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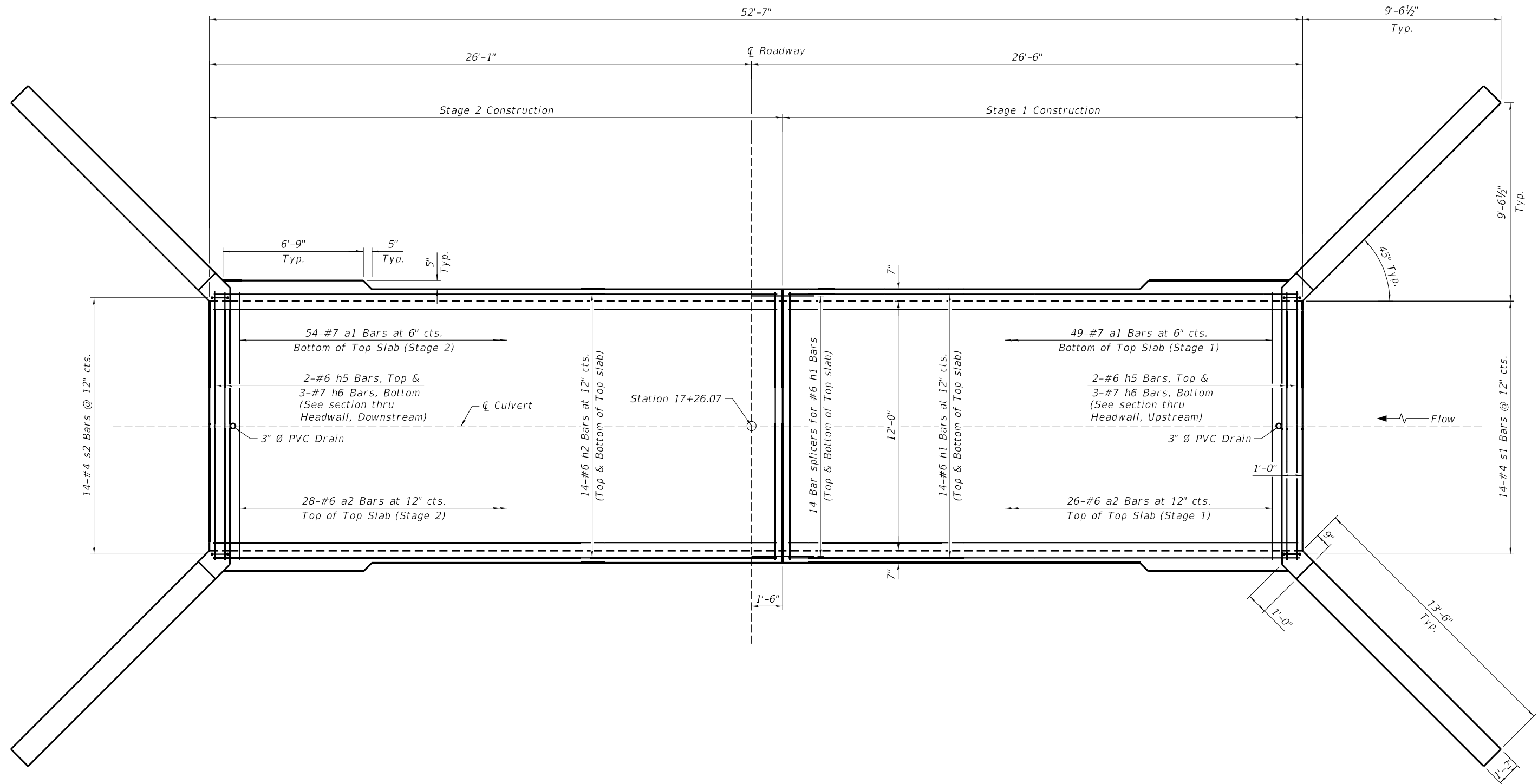
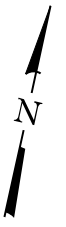


USER NAME:	DESIGNED - M. Winner	REVISED -
DESIGNED - M. Winner	CHECKED - S. Alsbury	REVISED -
PLOT SCALE: N.T.S.	DRAWN - M. Winner	REVISED -
PLOT DATE: 2/2/2023	CHECKED - 09.02.2022	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

STAGING PLAN STRUCTURE NO. 056-8300

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	19
CONTRACT NO. 62R50				



PLAN

Note:
See sheet 6 for drain detail & sections
Thru head walls.

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745 McClintock Drive
Suite 210
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Ph. 773-881-4786
Fax: 773-239-3728

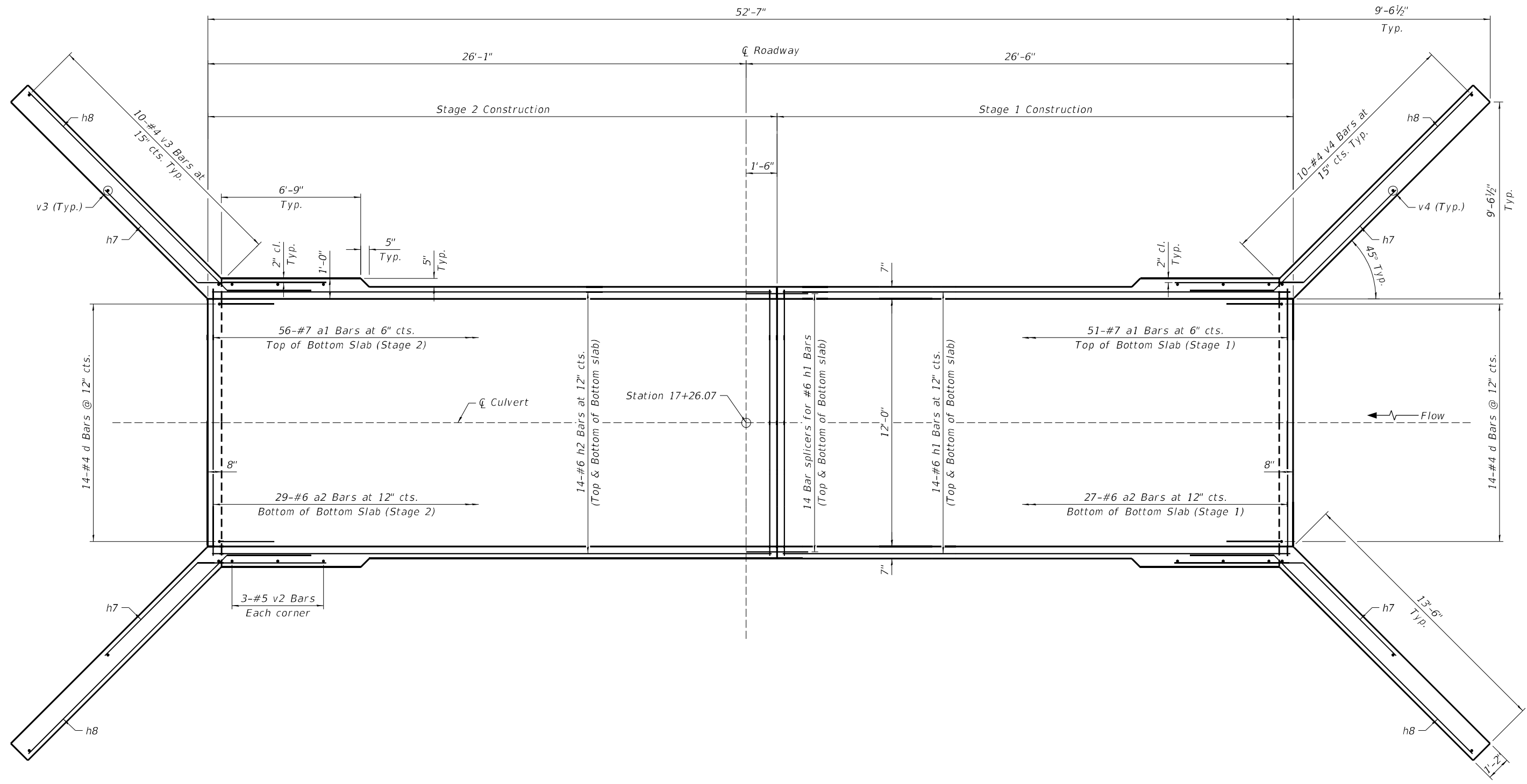
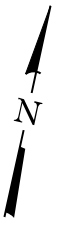
USER NAME	DESIGNED - M. Winner	REVISED -
	CHECKED - S. Alsbury	REVISED -
PLOT SCALE N.T.S.	DRAWN - M. Winner	REVISED -
PLOT DATE 2/2/2023	CHECKED - 09.02.2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CULVERT DETAILS - TOP SLAB
STRUCTURE NO. 056-8300

SHEET 4 OF 12 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	20
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				



PLAN

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745 McClintock Drive
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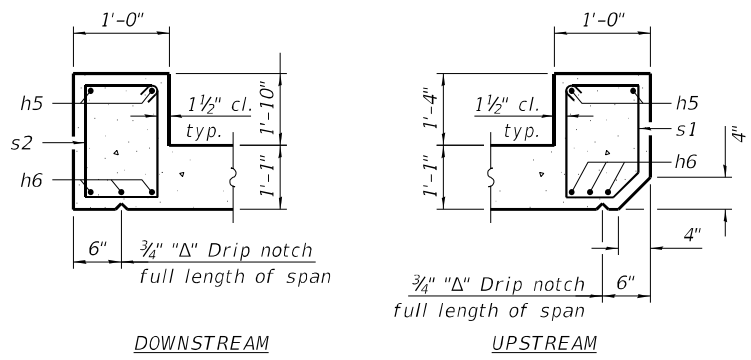
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	CHECKED - S. Alsbury	REVISED -
PLOT SCALE N.T.S.	DRAWN - M. Winner	REVISED -
PLOT DATE 2/2/2023	CHECKED - 09.02.2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CULVERT DETAILS - BOTTOM SLAB
STRUCTURE NO. 056-8300

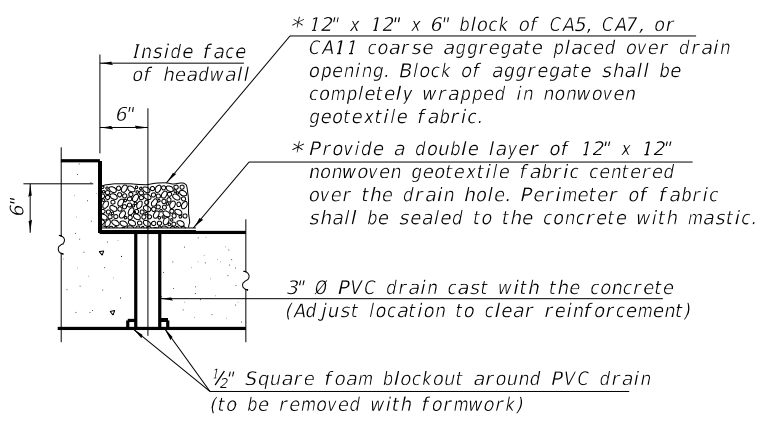
SHEET 5 OF 12 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	21
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				



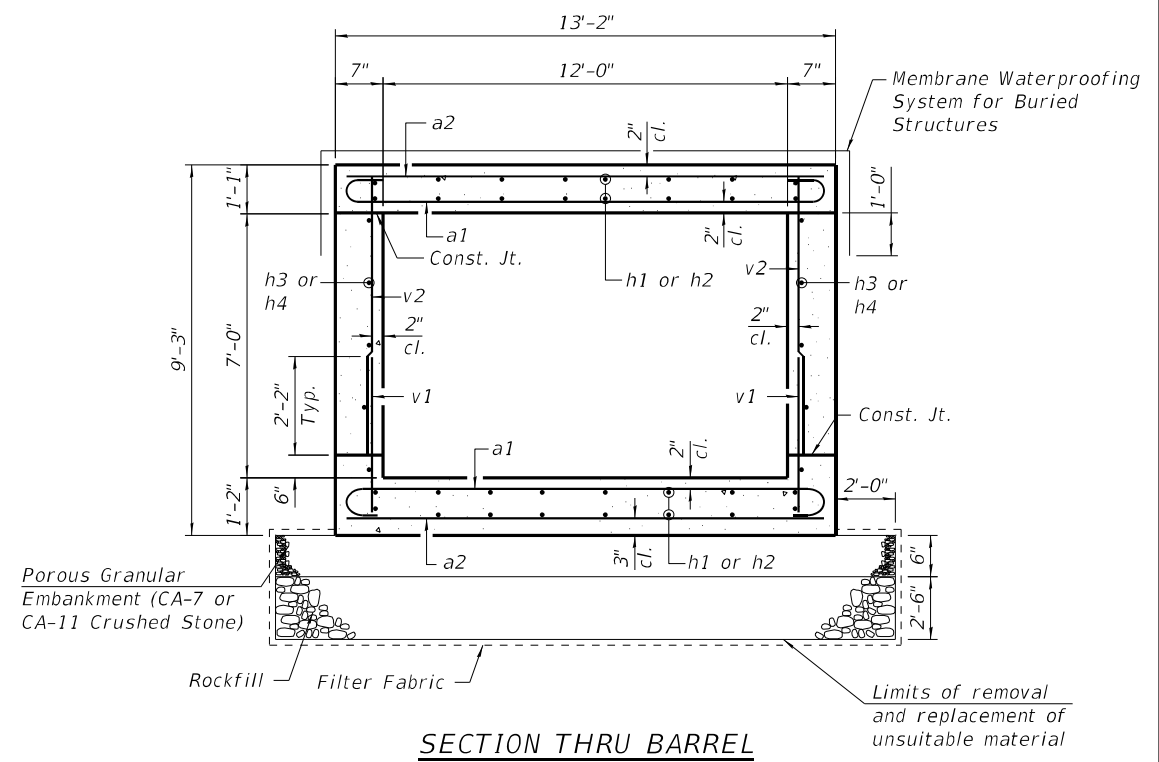
SECTION THRU HEADWALL

* Nonwoven geotextile fabric shall conform to the requirements of Article 1080.01 of the Standard Specifications. The minimum weight of the fabric shall be 6 ounces per square yard.



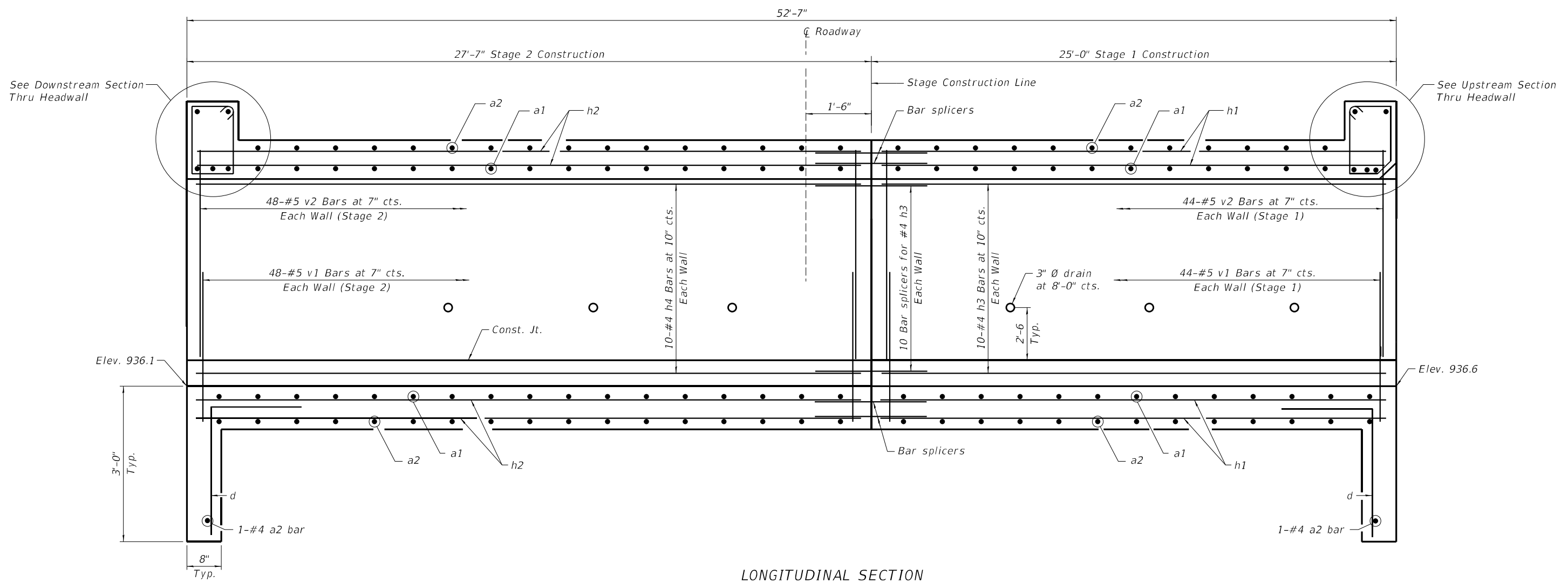
DRAIN DETAIL

(All costs associated with furnishing and constructing the above drain detail will not be measured for payment but shall be included in the contract unit price for the associated work.)



SECTION THRU BARREL

Notes:
 A distance not less than 6'-9" of the barrel shall be poured monolithically with the wingwalls.
 At the Contractor's option, a longer v1 bar may be ordered to replace the v2 bar. No reduction in quantities shall be made for this substitution.



LONGITUDINAL SECTION

FILE NAME: J:\2022\6041\WO_1\CAD_Sheets\0568300-62R50-006-Culvert_Det_3.dgn



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USER NAME	DESIGNED - M. Winner	REVISED -
	CHECKED - S. Alsbury	REVISED -
PLOT SCALE N.T.S.	DRAWN - M. Winner	REVISED -
PLOT DATE 2/2/2023	CHECKED - 09.02.2022	REVISED -

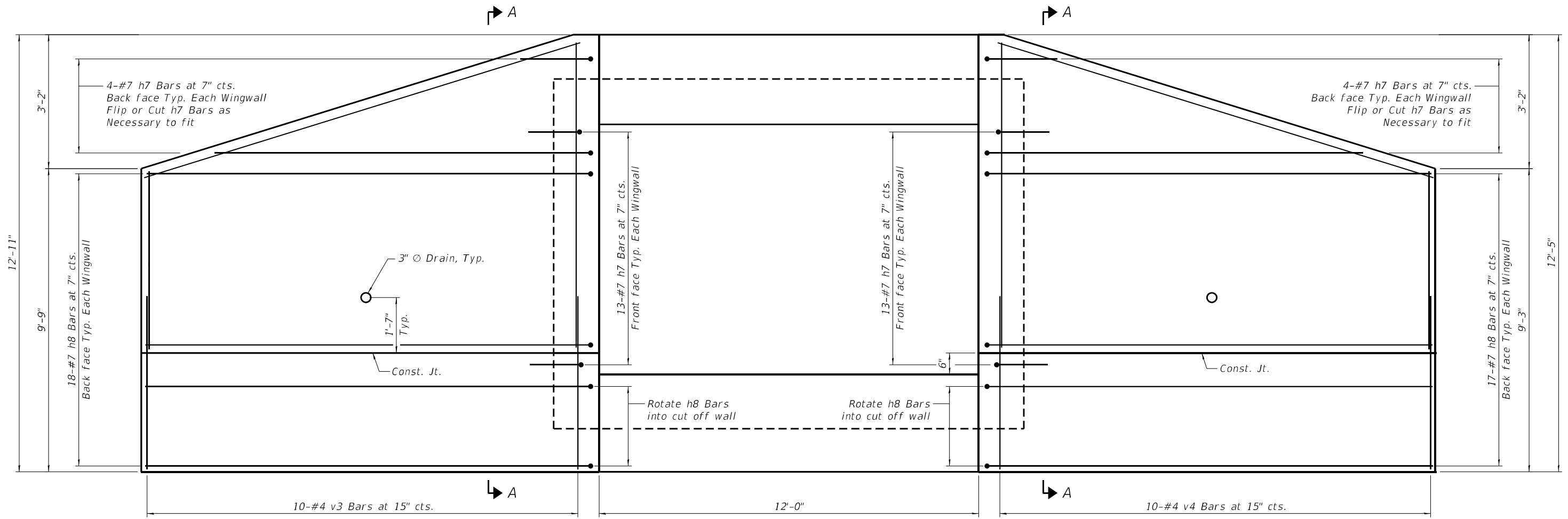
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CULVERT DETAILS 3
 STRUCTURE NO. 056-8300**

SHEET 6 OF 12 SHEETS

F.A.P. R.T.L.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	22
CONTRACT NO. 62R50				

ILLINOIS FED. AID PROJECT



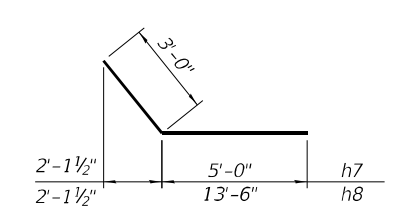
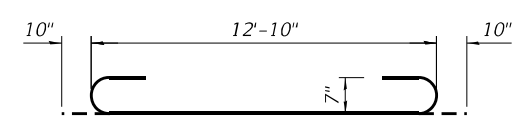
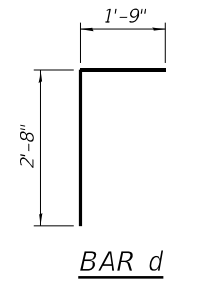
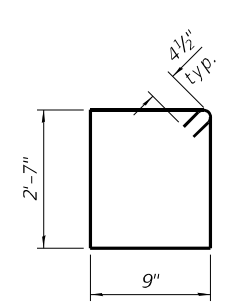
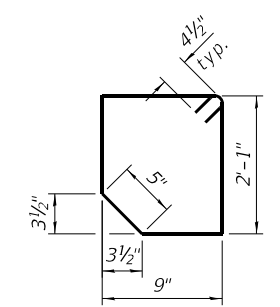
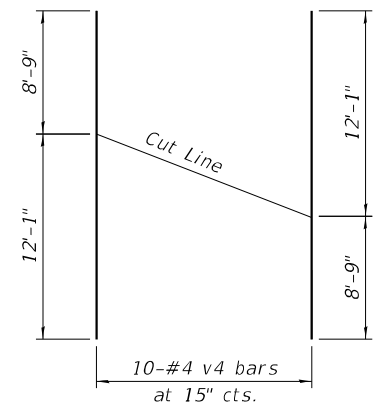
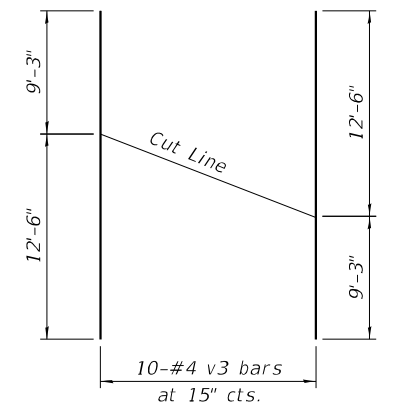
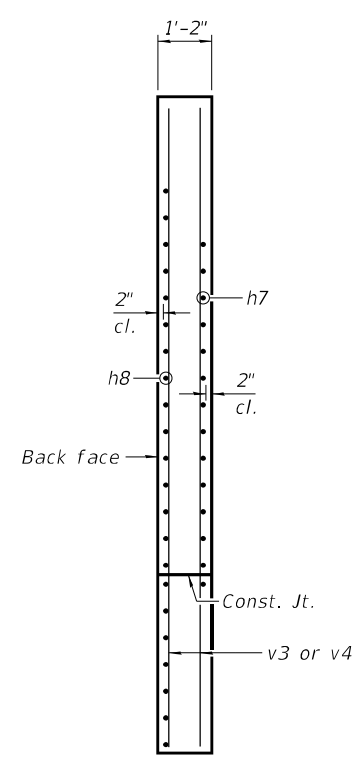
Downstream

END VIEW

Upstream

BILL OF MATERIAL

Bar	No.		Size	Length	Shape
	Stage 1	Stage 2			
a1	100	110	#7	14'-6"	
a2	54	58	#6	12'-10"	
d	14	14	#4	4'-5"	
h1	56	0	#6	24'-8"	
h2	0	56	#6	27'-3"	
h3	20	0	#4	24'-8"	
h4	0	20	#4	27'-3"	
h5	2	2	#6	12'-10"	
h6	3	3	#7	12'-10"	
h7	34	34	#7	8'-0"	
h8	36	36	#7	16'-6"	
s1	14	0	#4	6'-3"	
s2	0	14	#4	7'-5"	
v1	88	96	#5	3'-8"	
v2	88	96	#5	7'-10"	
v3	10	0	#4	21'-9"	
v4	0	10	#4	20'-10"	
Bar Splicers				Each	76
Concrete Box Culverts				Cu. Yd.	102.5
Reinforcement Bars				Pound	19,920



SECTION A-A

BAR v3 Cutting Diagram

BAR v4 Cutting Diagram

BAR s1

BAR s2

BAR d

BAR a1

BARS h7 & h8

FILE NAME: J:\2022\6041\WO_1\CAD_Sheets\0568300-62R50-007-Culvert Det 4.dgn

ABNA
DESIGN FIRM REG. 184.002117

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USER NAME	DESIGNED - M. Winner	REVISED -
	CHECKED - S. Alsbury	REVISED -
PLOT SCALE N.T.S.	DRAWN - M. Winner	REVISED -
PLOT DATE 2/2/2023	CHECKED - 09.02.2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CULVERT DETAILS 4
STRUCTURE NO. 056-8300

SHEET 7 OF 12 SHEETS

F.A.P. RTL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	23
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				

SOIL BORING LOG

Date 5/31/22

ROUTE 14 DESCRIPTION Proposed Culvert Replacement LOGGED BY DD

SECTION US Route 14 over Drainage Ditch LOCATION SEC. TWP. RNG. Latitude Longitude

COUNTY McHenry DRILLING RIG HSA HAMMER TYPE Auto 98

STRUCT. NO. Station	D E P T H S	B L O W S	U C S Qu	M O I S T T	Surface Water Elev.		Stream Bed Elev.		D E P T H S	B L O W S	U C S Qu	M O I S T T
					(ft)	(/6")	(tsf)	(%)				

8 inches of Asphalt	945.42											
Dark Brown, Dry to Moist FILL: SILTY CLAY, with sand and gravel		4						925.09		3		
		3	1.0	15						3	1.3	21
		3	P							4	B	
		3						922.59		2		
		2	0.3	11						3	0.6	15
		3	P							4	B	
		-5								-25		
1 inch of concrete at 6.0 feet, low recovery		5								9		
		2		12						9	0.4	12
		2								7	B	
		5								5		
Brown and Gray, Moist FILL: SAND, trace gravel	937.59	3		13						5	1.5	14
		3								7	B	
		-10								-30		
		4								4		
Soft to Medium Stiff Gray, Moist SILTY CLAY, trace sand and gravel (CL/ML) Low recovery at 11.0 feet	935.59	5		14						6		15
		4								6		
		5								6		
		4								6		
Sand seam at 13.5 feet		10								6		
		12		16						6	1.7	13
		10								5	B	
		-15								-35		
		2								3		
Stiff Gray, Moist to Very Moist SILTY CLAY LOAM, trace gravel (CL)	930.09	3	1.0	23						5	1.0	13
		4	B							5	B	
		3								6		
		2	1.8	17						7	2.1	15
		3	B							8	B	
		-20								-40		

End of Boring
The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, form 137 (Rev. 8-99)

SOIL BORING LOG

Date 5/31/22

ROUTE 14 DESCRIPTION Proposed Culvert Replacement LOGGED BY DD

SECTION US Route 14 over Drainage Ditch LOCATION SEC. TWP. RNG. Latitude Longitude

COUNTY McHenry DRILLING RIG HSA HAMMER TYPE Auto 98

STRUCT. NO. Station	D E P T H S	B L O W S	U C S Qu	M O I S T T	Surface Water Elev.		Stream Bed Elev.		D E P T H S	B L O W S	U C S Qu	M O I S T T
					(ft)	(/6")	(tsf)	(%)				

8 inches of Asphalt	945.64											
Dark Brown, Moist FILL: SILTY CLAY, with sand and gravel		5								3		
		6	0.3	9						6	2.3	26
		7	P							4	B	
		4								2		
		3	NR	NR						3	1.7	24
		2								2	B	
		-5								-25		
		3								5		
Light Brown, Moist FILL: SILTY CLAY, with sand and gravel	939.81	2	0.4	16						3	1.9	13
		2	B							5	B	
		3								3		
		5		12						6	1.5	12
		8	P							6	P	
		-10								-30		
		2								4		
Soft to Medium Stiff Gray, Moist SILTY CLAY, trace sand (CL-ML)	935.81	3	0.4	12						5	1.7	14
		3	B							4	B	
		3								3		
		6	0.8	13						6	0.5	14
		10	B							5	P	
		-15								-35		
		2								4		
Soft to Very Stiff Gray, Moist SILTY CLAY LOAM, trace gravel (ML/CL)	930.31	2	0.4	29						5	2.1	14
		2	B							6	B	
		2								6		
		6								6		
		2	0.8	18						6	2.3	14
		3	B							7	B	
		-20								-40		

End of Boring
The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, form 137 (Rev. 8-99)

FILE NAME: \\abna\jobfiles\2022\6041\WO_1\CAD_Sheets\0568300-62R50-009-Boring_Logs.dgn



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M. Winner	M. Winner	
S. Alsbury	S. Alsbury	
M. Winner	M. Winner	
09.02.2022	09.02.2022	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BORING LOGS
STRUCTURE NO. 056-8300

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	25
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				

FOR INFORMATION ONLY

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
303		McHENRY	41
STA.		TO STA.	

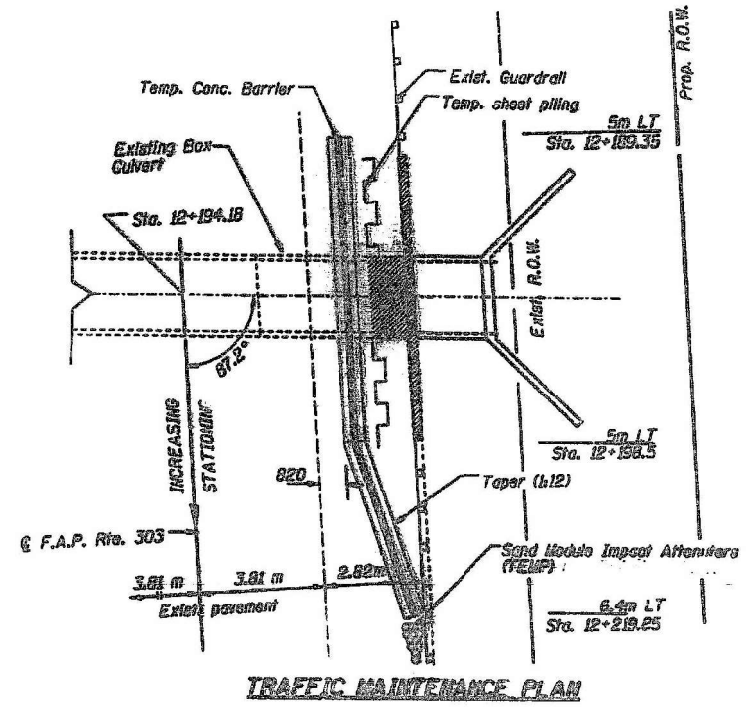
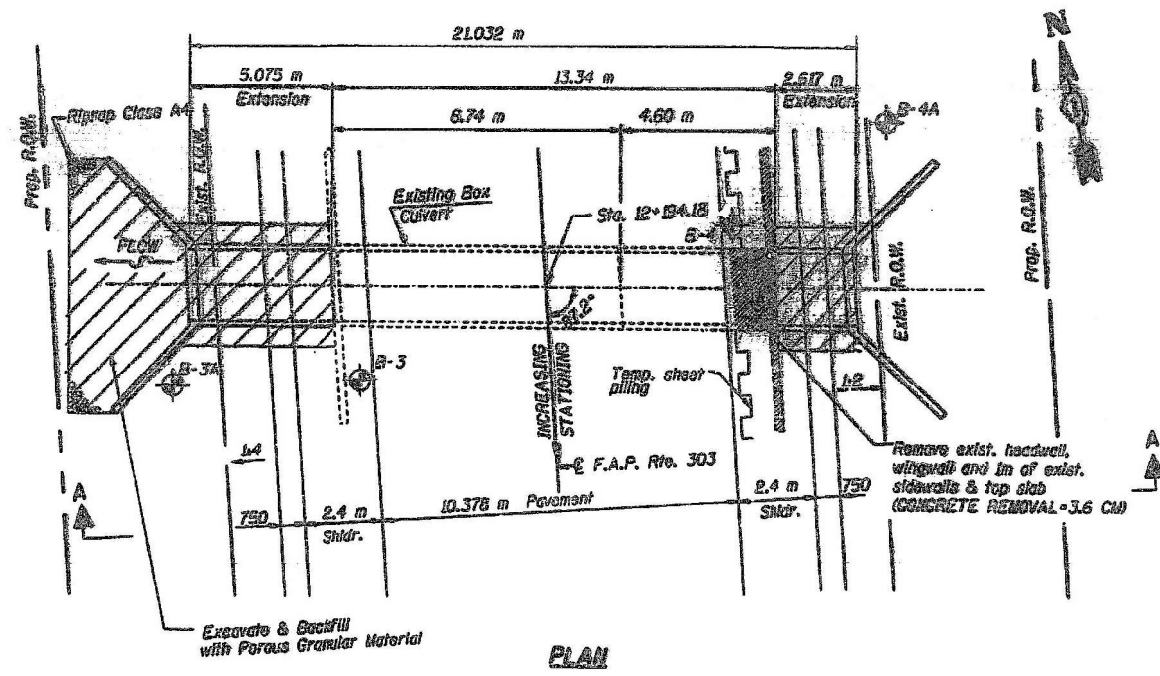
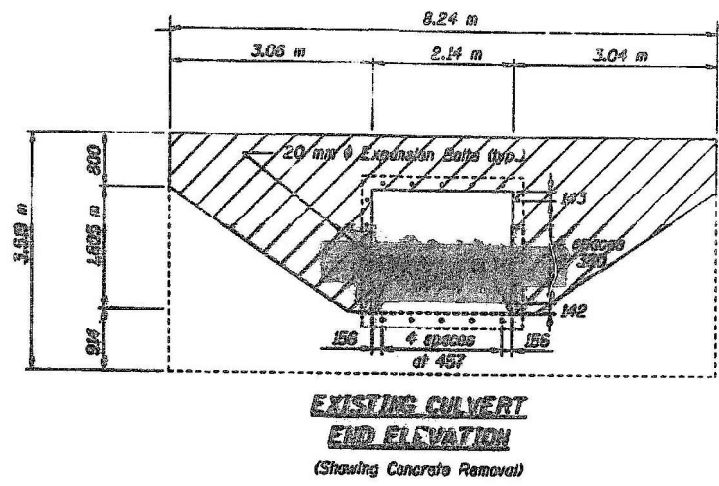
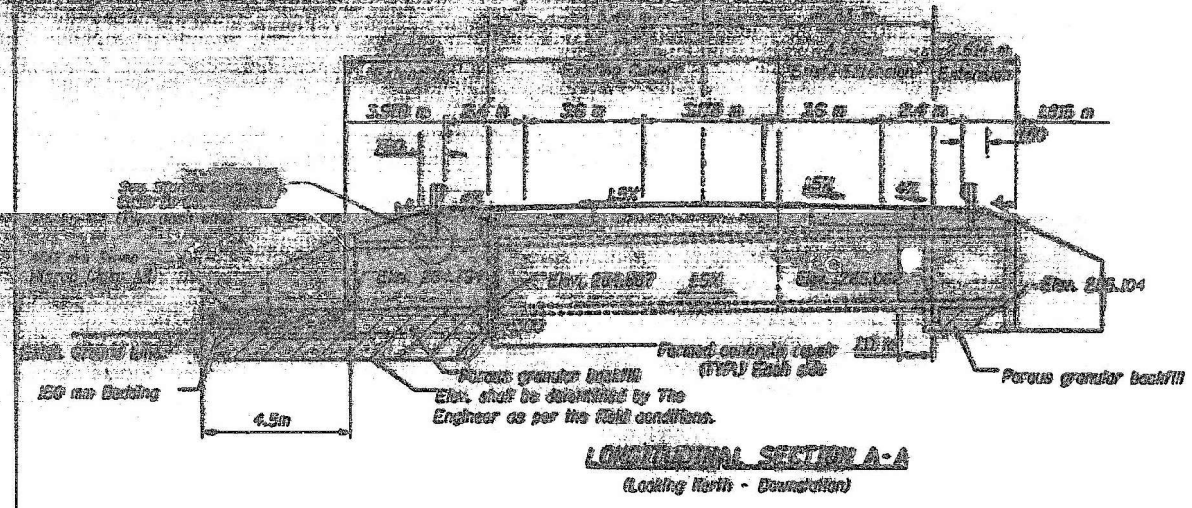
LEGEND

CONCRETE REMOVAL
 Reinforcement to culvert barrel to be cleaned, bent on site its length and incorporated into the new construction.

BILL OF MATERIAL

Bar	No.	Size	Length (m)	Shape
a	64	#20	3.04	
a2	14	#15	2.21	
a	16	#15	1.36	
n	8	#25	5.00	
h1	14	#15	5.00	
h2	12	#15	5.00	
h3	22	#25	2.40	
h4	22	#25	3.90	
h5	30	#20	2.40	
h6	22	#25	2.40	
h7	22	#25	4.00	
h8	8	#25	1.50	
h9	14	#15	2.52	
h10	12	#15	1.93	
v	68	#15	2.16	
vi	16	#15	3.03	
ve	10	#15	1.93	

Item	Unit	Qty.
Stone Riprap Class A4	Sq. m.	26
Filter Fabric	Sq. m.	34
Concrete Removal	Cu. m.	3.0
Structure Excavation	Cu. m.	51
Concrete Box Culverts	Cu. m.	22.7
Reinforcement Bar	kg	2770
Formed Concrete Repair	Sq. m.	14
Temp. Sheet Piling	Sq. m.	64
Porous Granular Backfill	Cu. m.	77
Expansion Bolts	kg	25
Temp. Concrete Barrier	Water	3000
Sand Mod. Imp. Atterberg	kg	12



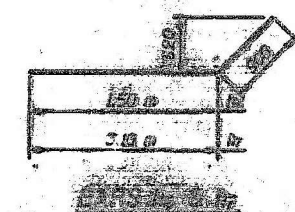
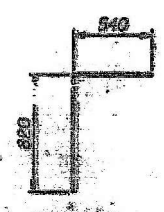
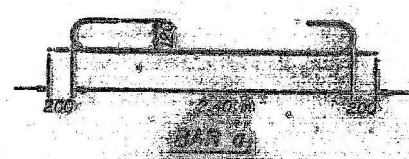
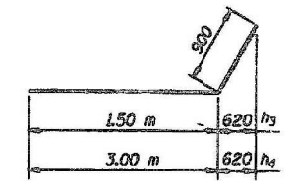
WATERWAY INFORMATION

Drainage Area = 1.86 km² @ Sta. 12+150
 C&G Grade Elev. 207.71 m @ Sta. 12+150

Flood	Freq. Yr.	Q cms	Opening Sq. m		Bot. H.W.E.	Head - m		Headwater Elev. - m	
			Exist.	Prop.		Exist.	Prop.	Exist.	Prop.
Design	50	5.69	3.21	2.94	205.55	1.01	0.87	206.56	206.42
Base	100	6.63	3.54	3.24	203.63	1.11	1.05	204.74	204.68
Overlapping Max. Calc.	500								

NOTES:

- If the contractor chooses to alter the temporary barrier or sheet piling design requirements shown on the plans for their design requirements, then full design submittals with all required details will be expected by the Department for review and approval.
- The minimum required section modulus for the sheet piling is 490,000 mm³/m.
- All dimensions are in millimeters (mm) unless noted.
- For backfilling and embankment use the Department Specifications.
- Exposed edges shall be finished.



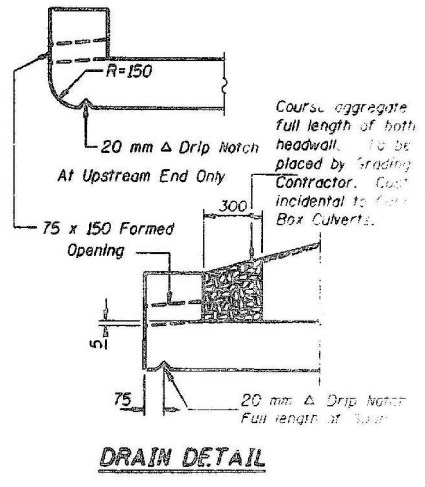
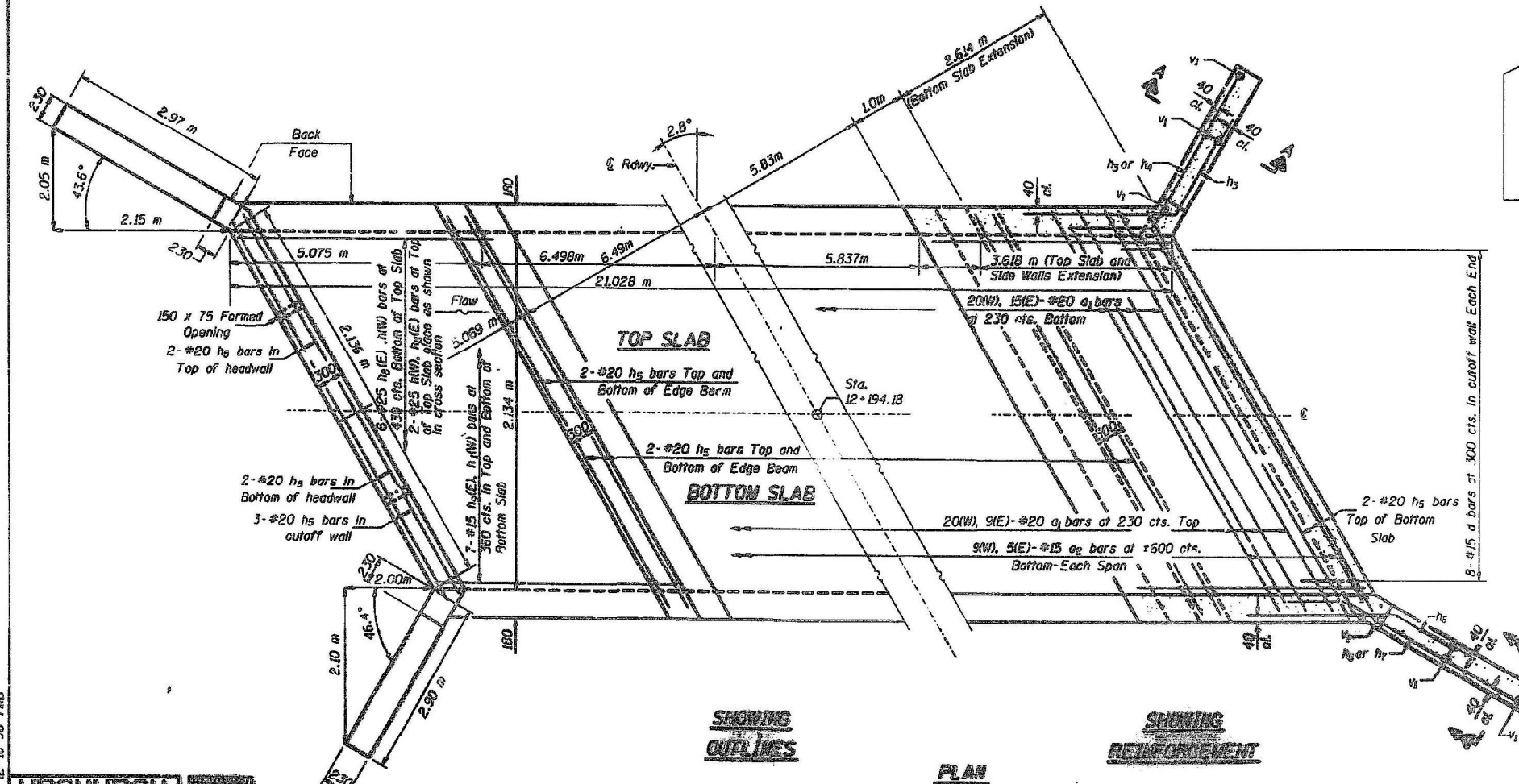
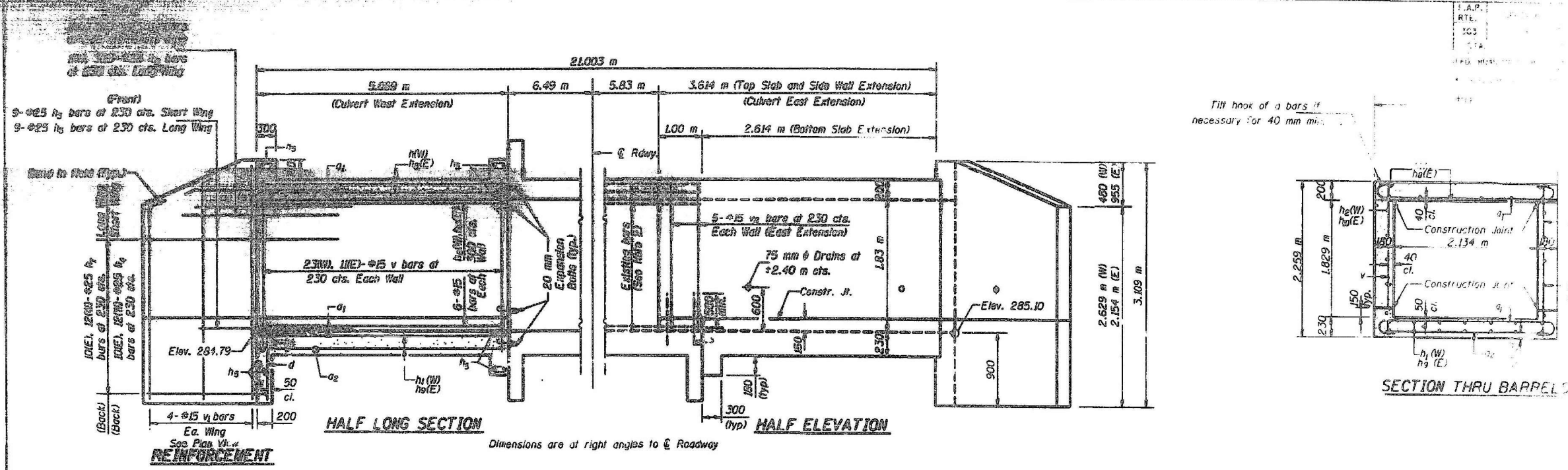
ILLINOIS DEPARTMENT OF TRANSPORTATION

2.1 m x 1.8 m
CULVERT EXTENSION
 STA. 12+194.16

DATE 12-20-22
 DRAWN BY A...
 CHECKED BY J...

FILE NAME: \\abna\jobfiles\2022\60411\WO_1\CAD_Sheets\0568300-62R50-01-0-REF-1.dgn

FOR INFORMATION ONLY



- NOTES**
1. All dimensions in meters.
 2. Existing reinforcement to be removed and replaced with new reinforcement as shown.
 3. W = WEST Extension
E = EAST Extension

REVISIONS	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION
REINFORCEMENT DETAILS
CULVERT EXTENSION
STA. 12+194.18
 DATE 12/20/22

FILE NAME: \\abna\jobfiles\2022\16041\WO_1\CAD_Sheets\0568300-62R50-011-REF2.dgn

J.P. CHURCH AND ASSOCIATES
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 Fax: 773-239-3728

USER NAME	DESIGNED	REVISIONS
	- M. Winner	
	CHECKED - S. Alsbery	
	DRAWN - M. Winner	
	CHECKED - 09.02.2022	

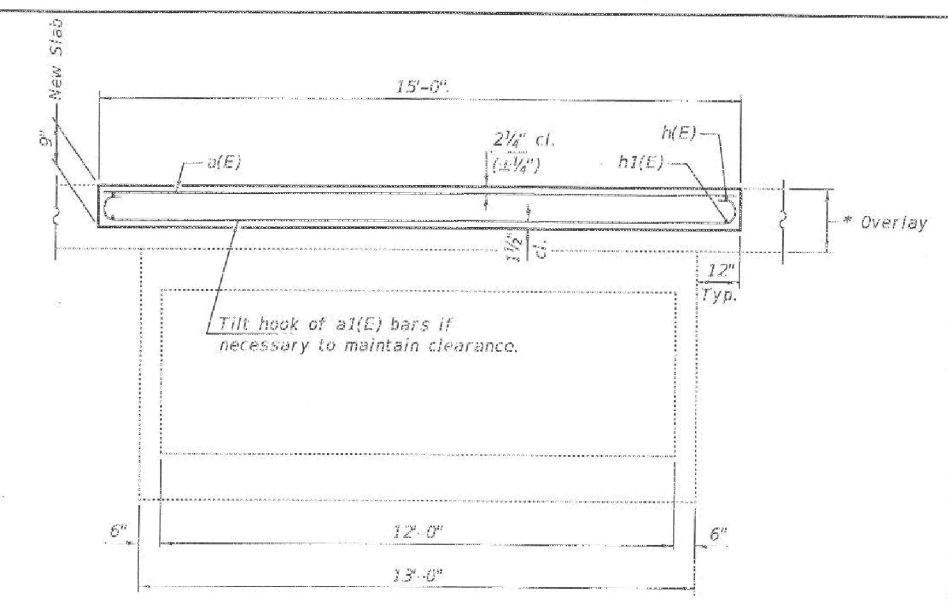
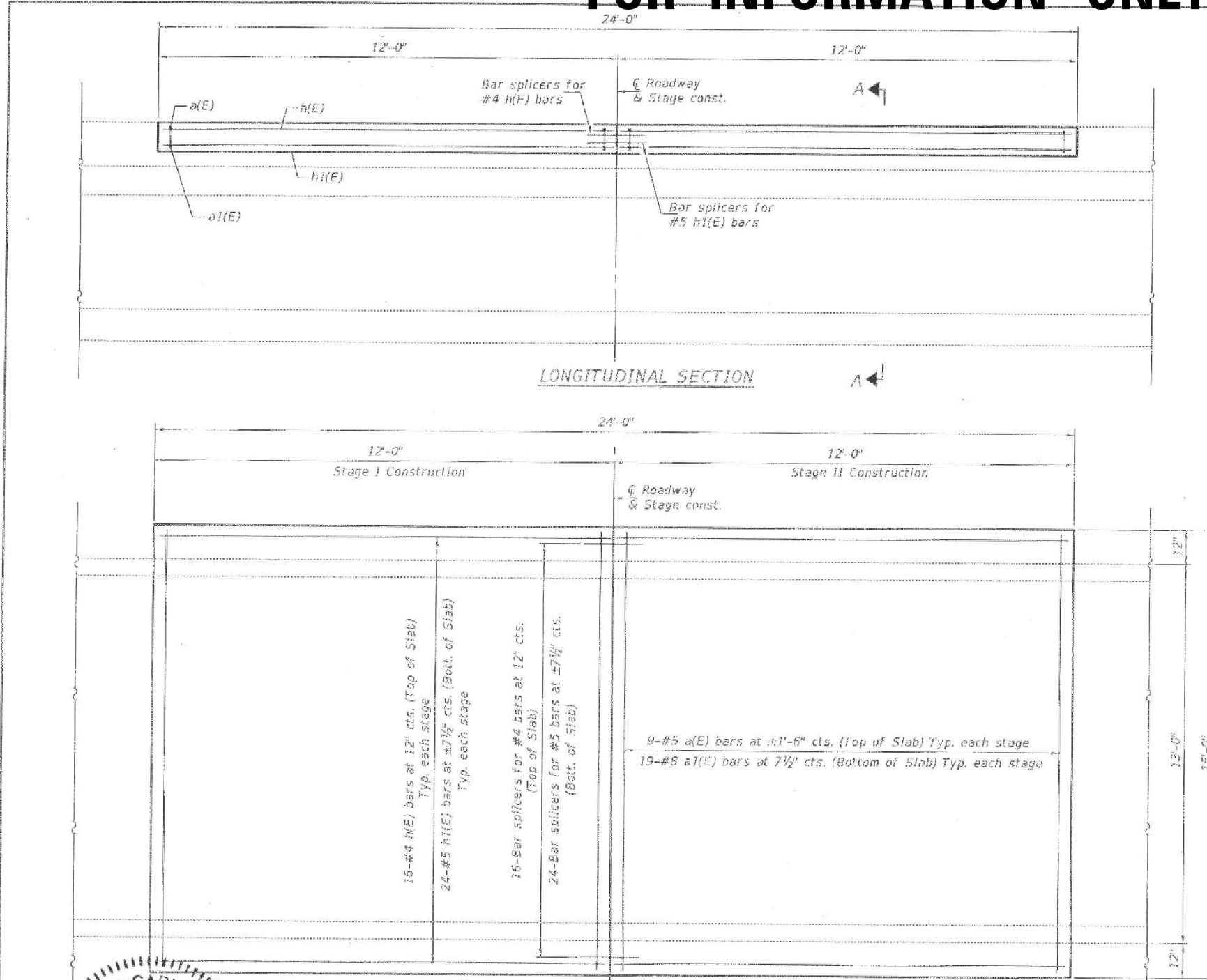
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

REFERENCE 2 - 1999 CULVERT EXTENSION
 STRUCTURE NO. 056-8300

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	27

CONTRACT NO. 62R50

FOR INFORMATION ONLY



SECTION A-A

* Remove 9" of existing overlay and replace with 9" concrete slab as shown. Slope to match roadway. Cost of removal included with Hot-Mix Asphalt Surface Removal, 9".

GENERAL NOTES

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

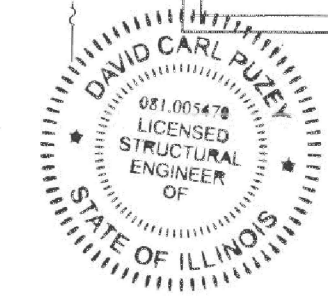
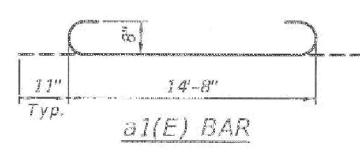
Reinforcement bars designated (E) shall be epoxy coated.
The slab surface shall have its final finish lined according to Article 420.09(e)(1) of the Standard Specifications. Cost included with Concrete

DESIGN STRESSES

FIELD UNITS
 $f_c = 4,000$ psi
 $f_y = 60,000$ psi (Reinf.)

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	18	#5	14'-8"	—
a1(E)	36	#8	16'-5"	—
h(E)	32	#4	11'-8"	—
h1(E)	48	#5	11'-8"	—
Bar Splicers			Each	40
Hot-Mix Asphalt Surface Removal, 9"			Sq. Yd.	40.0
Reinforcement Bars, Epoxy Coated			Pound	2790
Concrete Superstructure			Cu. Yd.	10.0



DESIGNED: *Victor H. Vela*
 CHECKED: *Tommy A. Dost*
 DRAWN: *VENKAT REDDY*
 DATE: OCTOBER 1, 2019

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

REPAIR DETAILS
 FAP 066 (US 14) OVER DIICH
 SN 056-0263
 SHEET NO. 1 OF 2 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
066	2019-090-I	McHENRY	18	9

CONTRACT NO. 62R57

FILE NAME: \\abna\jobfiles\2022\160411\WO_1\CAD_Sheets\0568300-62R50-01-2-REF3.dgn



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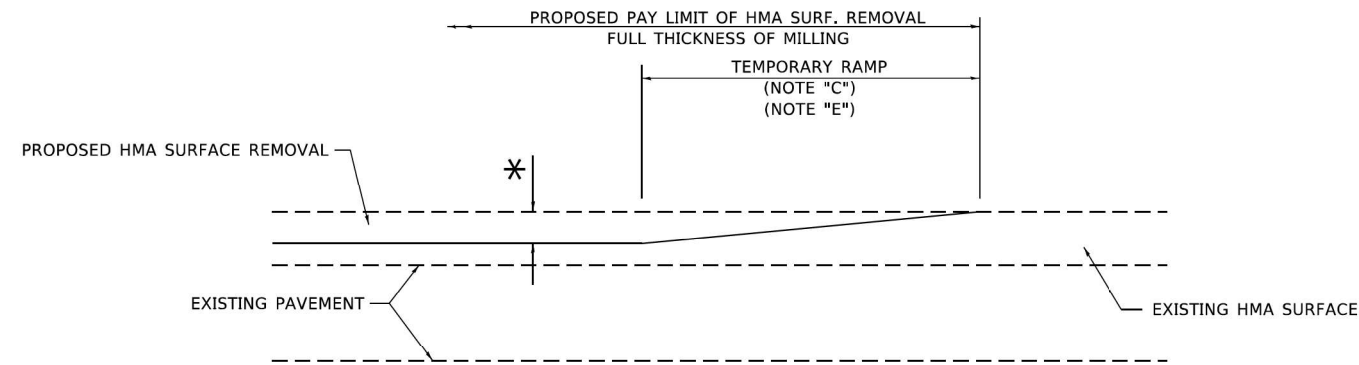
USER NAME	DESIGNED	CHECKED	DRAWN	CHECKED
	- M. Winner	- S. Alsbery	- M. Winner	- 09.02.2022

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

REFERENCE 3 - 2019 SLAB ON GRADE RETROFIT
 STRUCTURE NO. 056-8300
 SHEET 12 OF 12 SHEETS

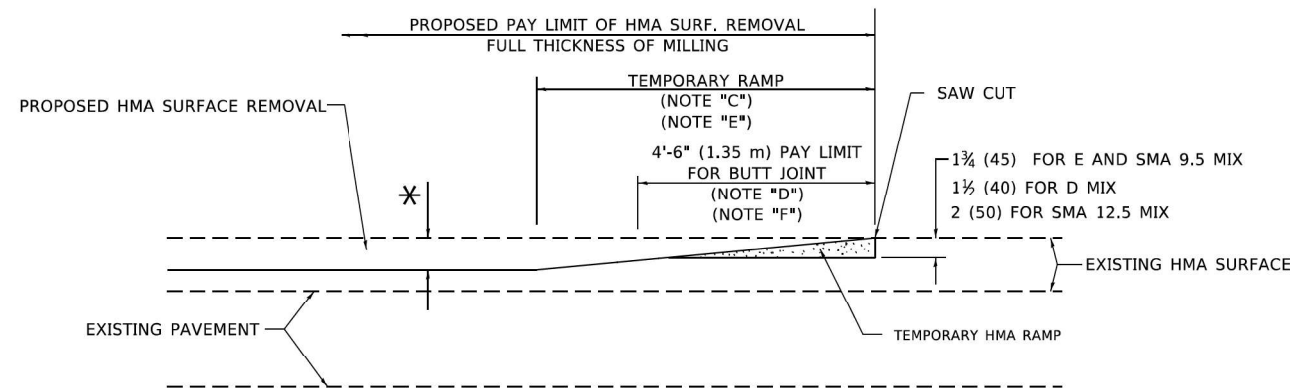
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	McHENRY	41	28

CONTRACT NO. 62R50



MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

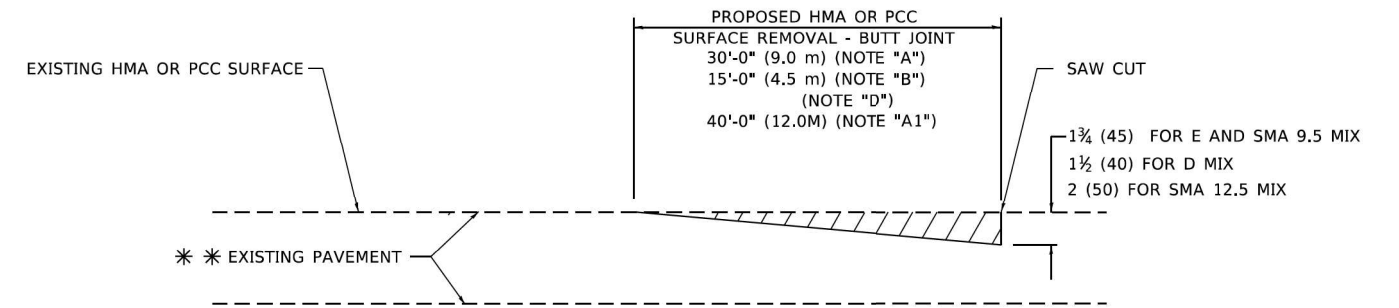
OPTION 1



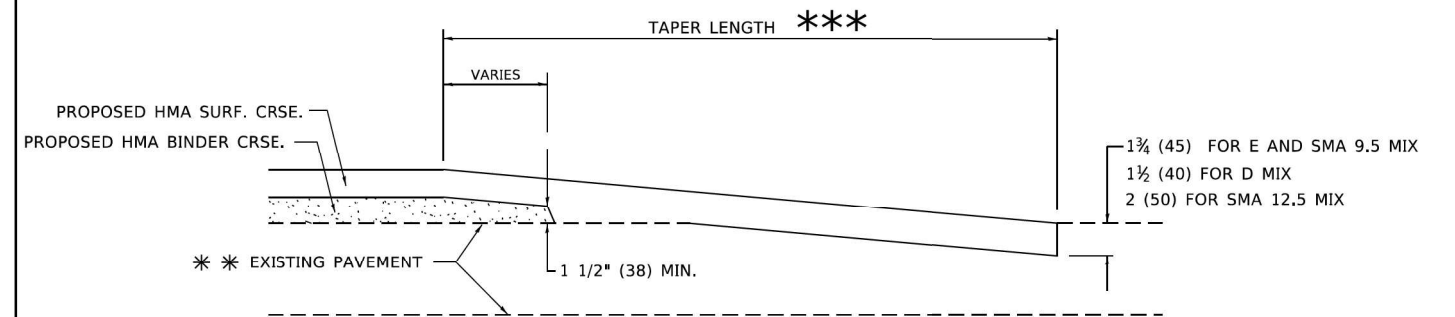
HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

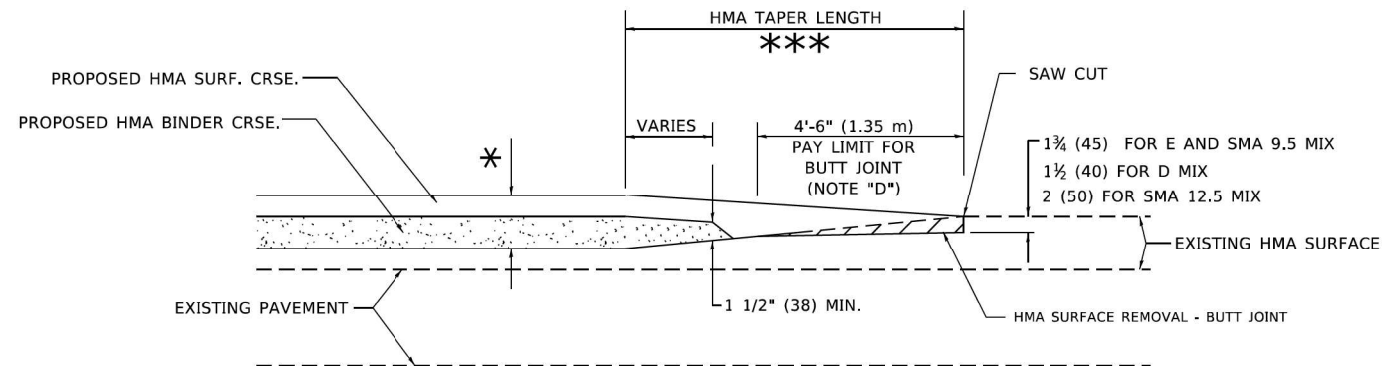
GENERAL NOTES

- A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- A1. INTERSTATES
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

- 1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- 2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL: Default
FILE NAME: \\na1\geob\2022\60411\100_1\CAD_Sheets\162856-ct-detail-BD-32-Butt Joint and HMA Details.dgn



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DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS (BD-32)
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)

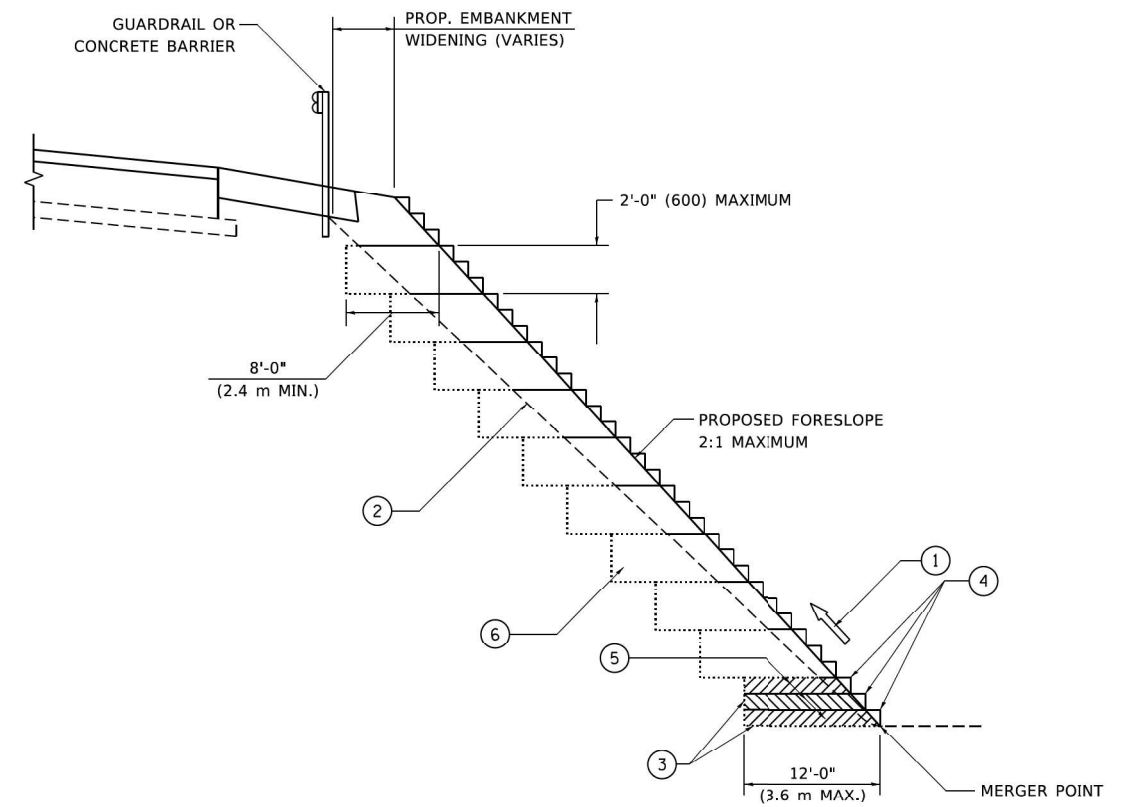
SCALE: 1"=50'

SHEET 1 OF 1 SHEETS

STA.

TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	29
			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE NAME: \\na1\ed\ba2022\6041\W0_1\CAD_Series\162856\ch-detail\BD-51-Butt.dwg, and HMA_Detail.dgn



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PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BENCHING DETAIL FOR EMBANKMENT WIDENING (BD-51)
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)**

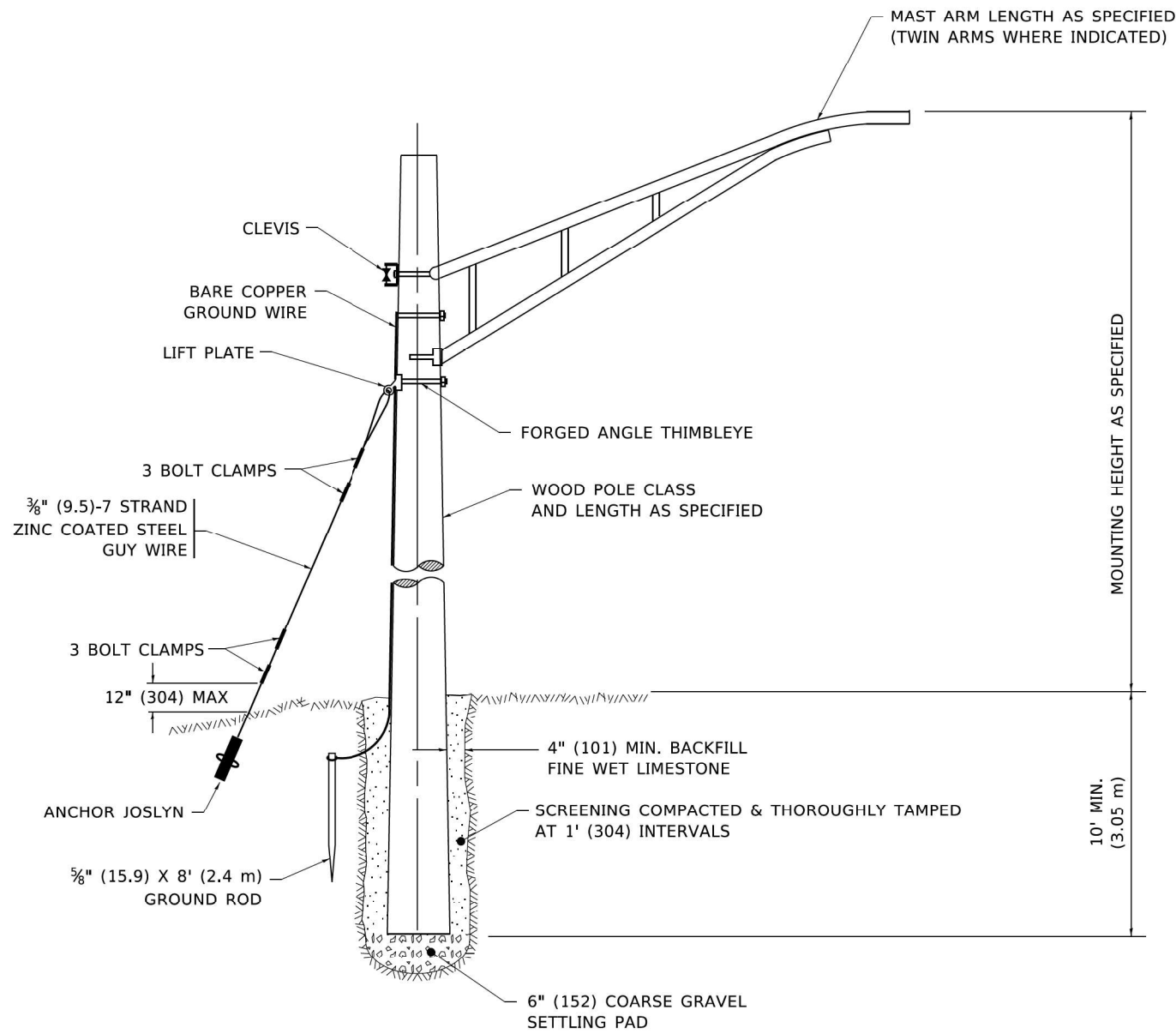
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SHEET 1 OF 1 SHEETS

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TO STA.

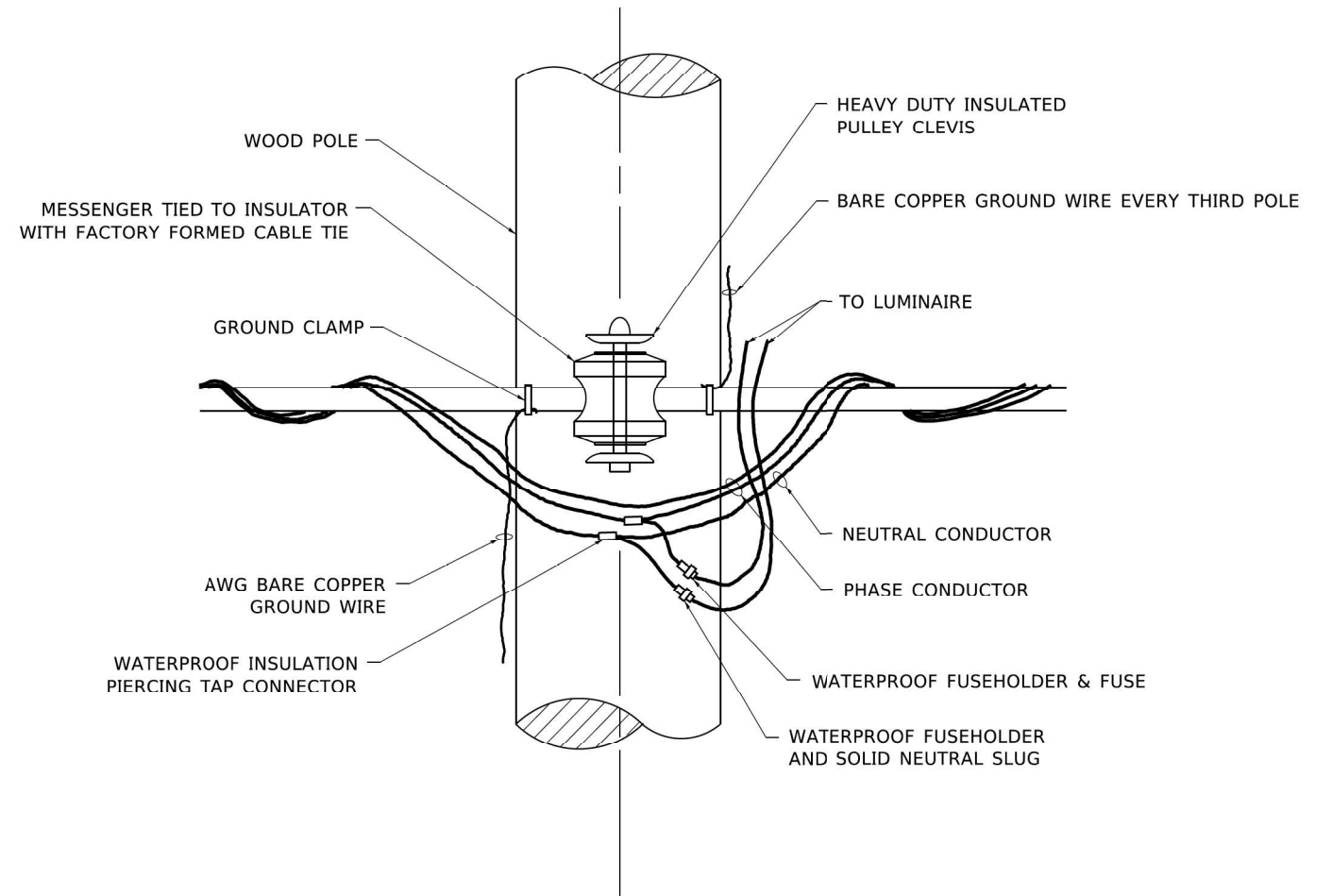
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	30
			CONTRACT NO. 62R50	
		ILLINOIS	FED. AID PROJECT	



TEMPORARY LIGHT POLE DETAIL

NOTE:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. MAST ARM SHALL BE RATED FOR THE SPECIFIED MOUNTING HEIGHT.



TEMPORARY LIGHT POLE ATTACHMENT DETAIL

MODEL: Default
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 PLOT DATE: 12/13/2022



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHT POLE DETAILS (BE-800)
U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)

SCALE: 1"=50'

SHEET 1 OF 1 SHEETS

STA. TO STA.

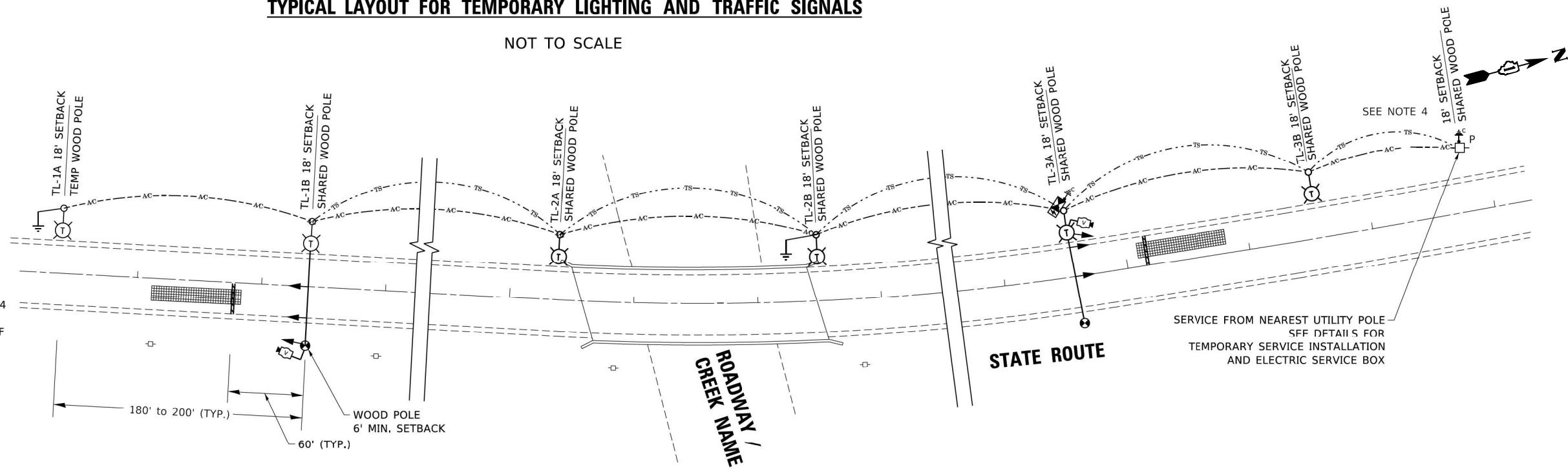
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	31
			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				

TYPICAL LAYOUT FOR TEMPORARY LIGHTING AND TRAFFIC SIGNALS

NOT TO SCALE

LEGEND

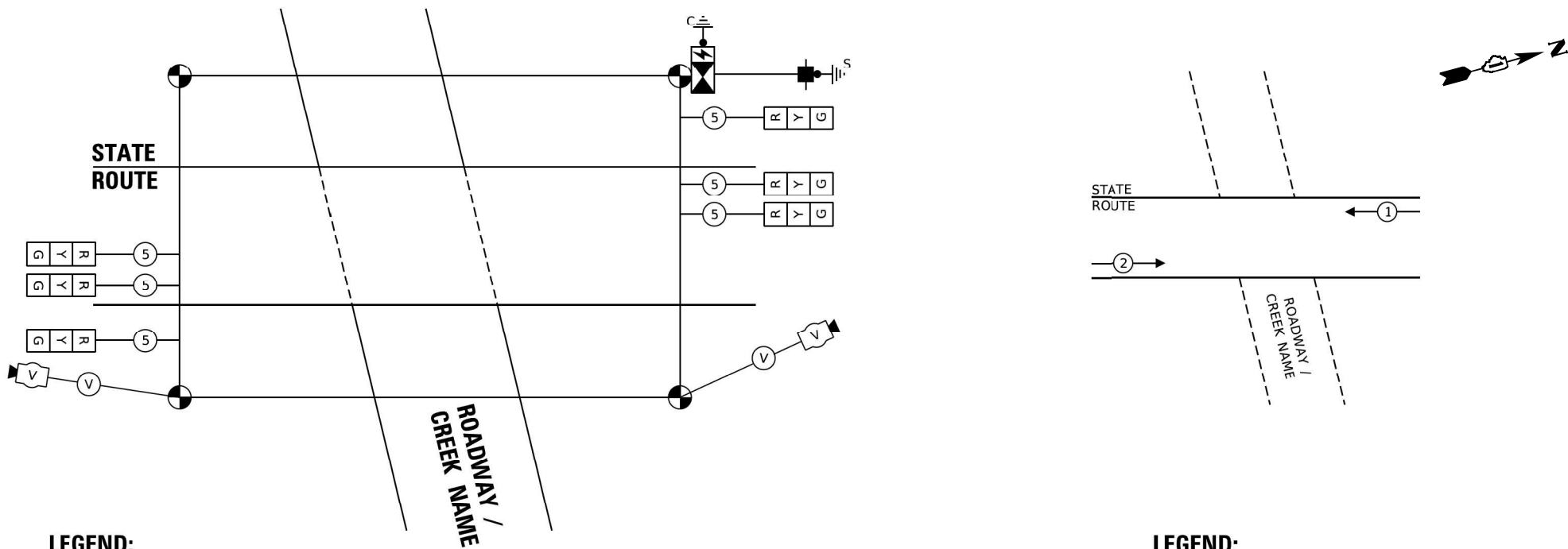
- 26,000-30,000 LUMENS, 120V, MCIII LED WITH PHOTO CELL 15' MA, 50' MH ON WOOD POLE, CLASS 4
- 3-1/C#2, AERIAL CABLE WITH MESSENGER WIRE UNLESS OTHERWISE NOTED
- TL-1A** TEMPORARY LIGHTING UNIT NUMBER - ONE CIRCUIT A
- GROUND ROD 5/8" DIA. x 10'
- COMBINATION LIGHTING AND TRAFFIC POLE MOUNTED ELECTRICAL SERVICE BOX
- TEMPORARY WOOD POLE - NOMINAL 60 FT., CLASS 4
- TEMPORARY LED TRAFFIC SIGNAL HEAD, NUMBER OF SECTION AND DISPLAY AS REQUIRED.
- TEMPORARY TRAFFIC SIGNAL SPAN WIRE, NUMBER OF CONDUCTORS AS REQUIRED.
- TEMPORARY TRAFFIC CONTROLLER WITH UPS AND BOTTOM PLATE MOUNTED TO WOOD POLE
- TEMPORARY VIDEO DETECTOR



GENERAL NOTES:

1. CONTACT TO THE ELECTRIC UTILITY SHALL BE INITIATED BEFORE THE PRECONSTRUCTION MEETING, AND DOCUMENTATION OF CONTACT SHALL BE PRESENTED AT THAT MEETING. NO PLACEMENT OF POLES WILL BE ALLOWED WITHOUT EVIDENCE OF A SIGNED AGREEMENT WITH THE ELECTRIC UTILITY, FURNISHED TO THE ENGINEER.
2. UNLESS OTHERWISE INDICATED, AND EXCEPT AS OTHERWISE NOTED, THIS STANDARDIZED LAYOUT SHALL APPLY FOR BRIDGES NOT EXCEEDING A 250-FOOT SPAN. FOR BRIDGE SPANS IN EXCESS OF 250 FEET, THE POLES IMMEDIATELY ADJACENT TO THE BRIDGE SHALL BE 100-FOOT POLES (90-FOOT MOUNTING HEIGHT), WITH 56,000-60,000 LUMENS TYPE III LED HIGH-MAST LUMINAIRES AS APPROVED BY THE ENGINEER.
3. THE LAYOUT OF THE TEMPORARY EQUIPMENT WILL VARY BASED ON FIELD CONDITIONS, STAGING, UTILITY IMPACTS, AND THE ELECTRIC SERVICE LOCATION AS COORDINATED WITH THE ELECTRIC UTILITY. THE CONTRACTOR SHALL SUBMIT A PLAN INDICATING THE SETTING OF POLES, TRAFFIC SIGNALS, AND COMBINED SERVICE. THIS PLAN MUST BE APPROVED BY THE ENGINEER BEFORE ANY POLES ARE PLACED
4. THE ELECTRIC SERVICE SHALL BE 240/120V. WHERE 240V SERVICE IS NOT AVAILABLE, THE CONTRACTOR MAY SUBMIT A PROPOSAL FOR 120V SERVICE. DROP CABLE, MAIN BREAKER, AND ALL OTHER SERVICE APPURTENANCES SHALL BE APPROPRIATELY RATED AND INCLUDED REGARDLESS OF THE SERVICE VOLTAGE APPLIED.
5. THE TEMPORARY LIGHTING AND TRAFFIC SIGNAL INSTALLATION SHALL SHARE ANY COMMON ELEMENTS SUCH AS WOOD POLES, ELECTRICAL SERVICE, ELECTRICAL SERVICE BOX, CABLE, ETC. THE CONTRACTOR SHALL COORDINATE TEMPORARY LIGHTING AND TRAFFIC SIGNAL INSTALLATIONS.
6. THE LIGHT POLE SETBACK FROM THE EDGE OF TRAVEL PAVEMENT SHALL BE 18 FT. UNLESS THE LIGHT POLE IS BEHIND GUARDRAIL. THE LIGHT POLES INSTALLED BEHIND THE GUARDRAIL OR BARRIER WALL SHOULD HAVE AT LEAST 8 FT. SETBACK FROM THE BACK OF THE SHOULDER AND OR AS DIRECTED BY THE ENGINEER.
7. EACH LIGHTING UNIT SHALL BE CONTROLLED BY A PHOTO CELL MOUNTED ON EACH LUMINAIRE WITH THE LIGHTING CIRCUIT FED FROM THE TEMPORARY SERVICE DISCONNECT BOX. OTHER MEANS OF LUMINAIRE CONTROL CAN BE CONSIDERED IF APPROVED BY THE ENGINEER.
8. THE CONTRACTOR SHALL SPLICE AERIAL CABLE AT THE LIGHT POLE USING HEAT SHRINKABLE CAPS WITH THE FACTORY APPLIED WATERPROOF SEALANT OR AN APPROVED UL LISTED AERIAL TAP DEVICE.
9. ALL AREAS DISTURBED UNDER THIS CONTRACT SHALL BE RESTORED TO THE ORIGINAL CONDITION OR BETTER, TO THE SATISFACTION OF THE ENGINEER.
10. THIS LAYOUT IS ONLY FOR SINGLE LANE STAGING WITHOUT PRIVATE DRIVEWAYS WITHIN SINGLE LANE WORK ZONE.

CABLE PLAN AND PHASE DESIGNATION DIAGRAM (TYPICAL)



LEGEND:

- TEMPORARY VIDEO / RADAR DETECTOR
- TEMPORARY SIGNAL HEAD 12" SECTION.
- CABLE INDICATES NUMBER OF CONDUCTORS IN CABLE ALL CONDUCTORS TO BE NUMBER 14 AWG UNLESS OTHERWISE NOTED.

LEGEND:

- NUMBER ASSOCIATED WITH PROTECTED PHASE

MODEL: Default FILE NAME: \\na01ab08\2022\60411\10_1\CAD Sheets\162850\ch-draw\BE-805-Temporary Lighting And Traffic.dgn for circle base station-1.dgn



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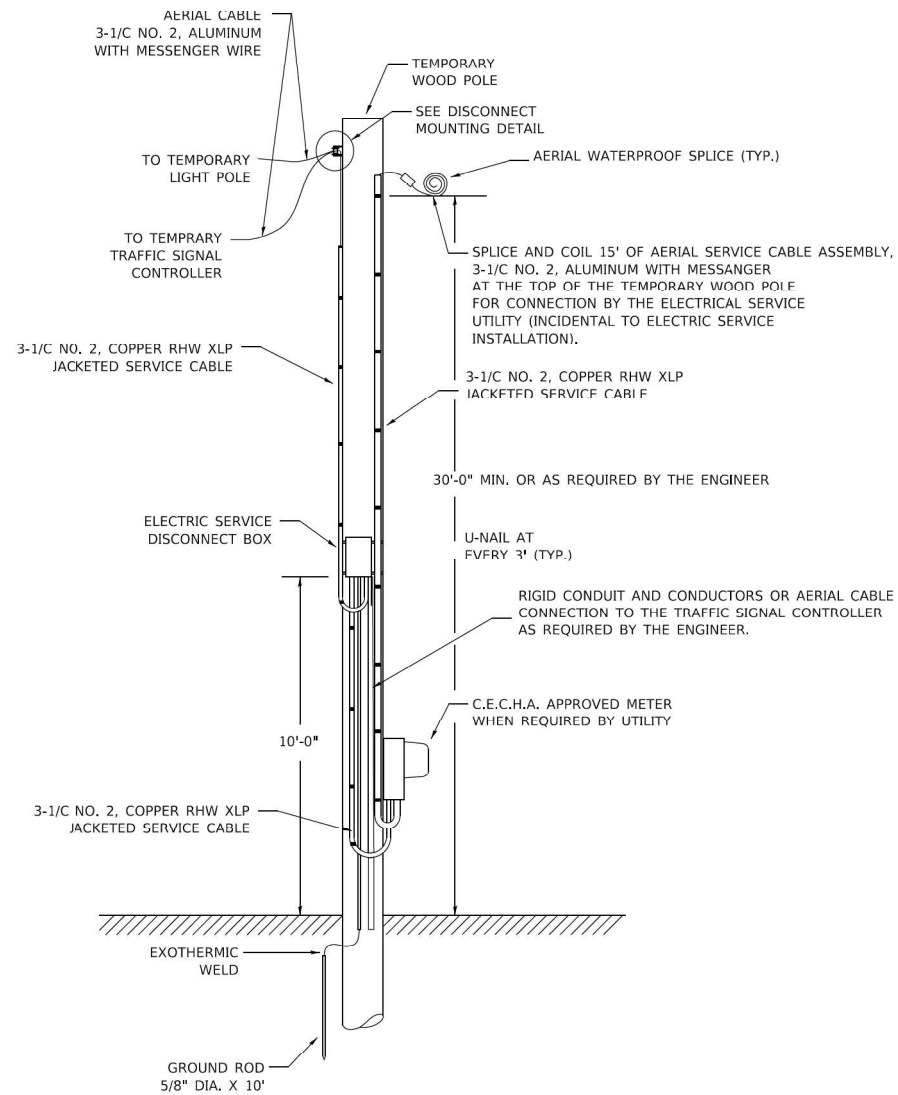
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	DATE = 07/08/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY LIGHTING AND TRAFFIC SIGNALS (BE-805) FOR SINGLE
LANE STAGING U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)

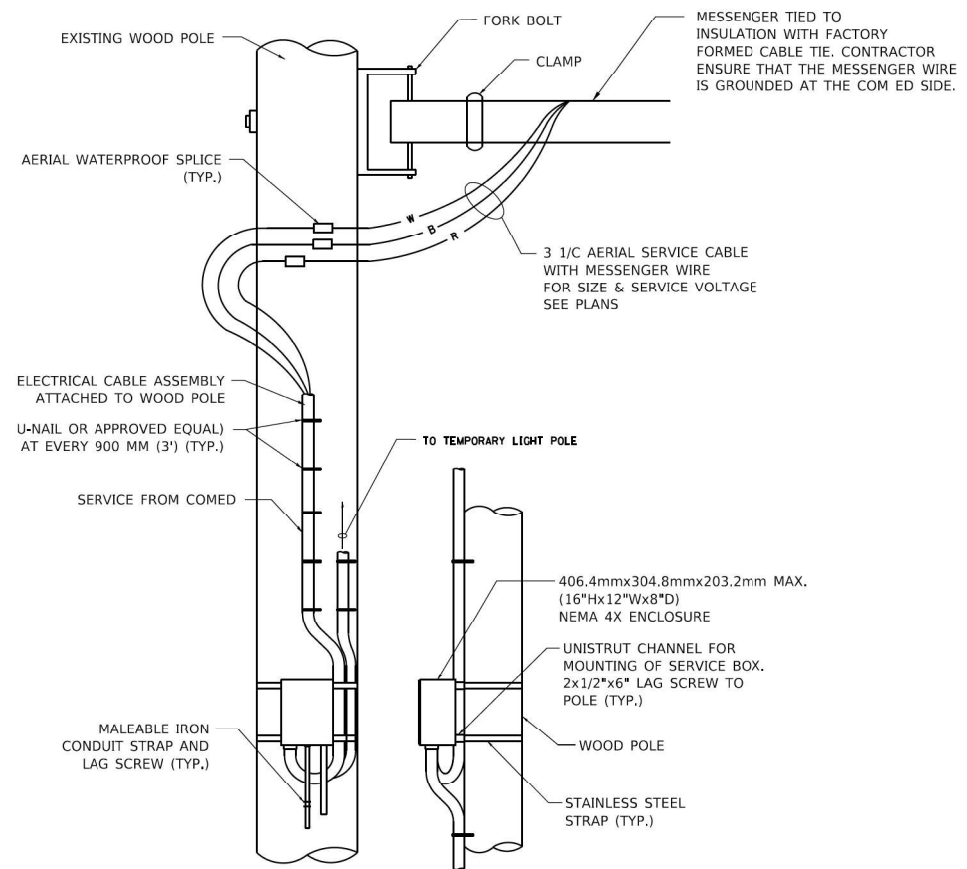
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F.A.P. RTE. 66	SECTION 2018-114-I	COUNTY MCHENRY	TOTAL SHEETS 41	SHEET NO. 32
			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				



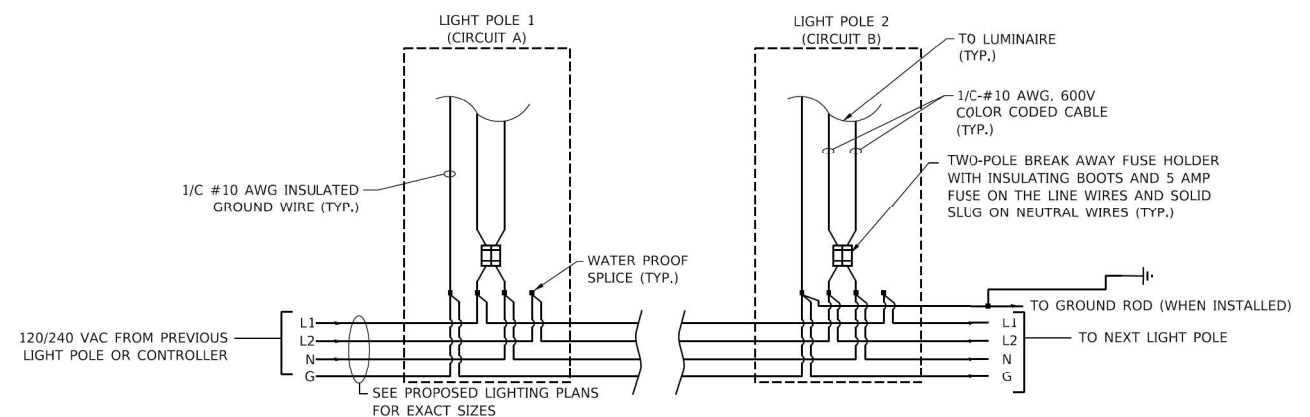
TEMPORARY SERVICE INSTALLATION DETAIL

NOT TO SCALE



DISCONNECT MOUNTING DETAIL

NOT TO SCALE



LIGHT POLE WIRING DETAIL

NOT TO SCALE

MODEL: Default; FILE NAME: \\na106b16a2022\6041\W0_1\CAD Sheets\16285\ch-detail-BE-805-Temporary Lighting and Traffic Signal.dwg; for circle line station2.dwg



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PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY LIGHTING AND TRAFFIC SIGNALS (BE-805) FOR SINGLE
LANE STAGING U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)**

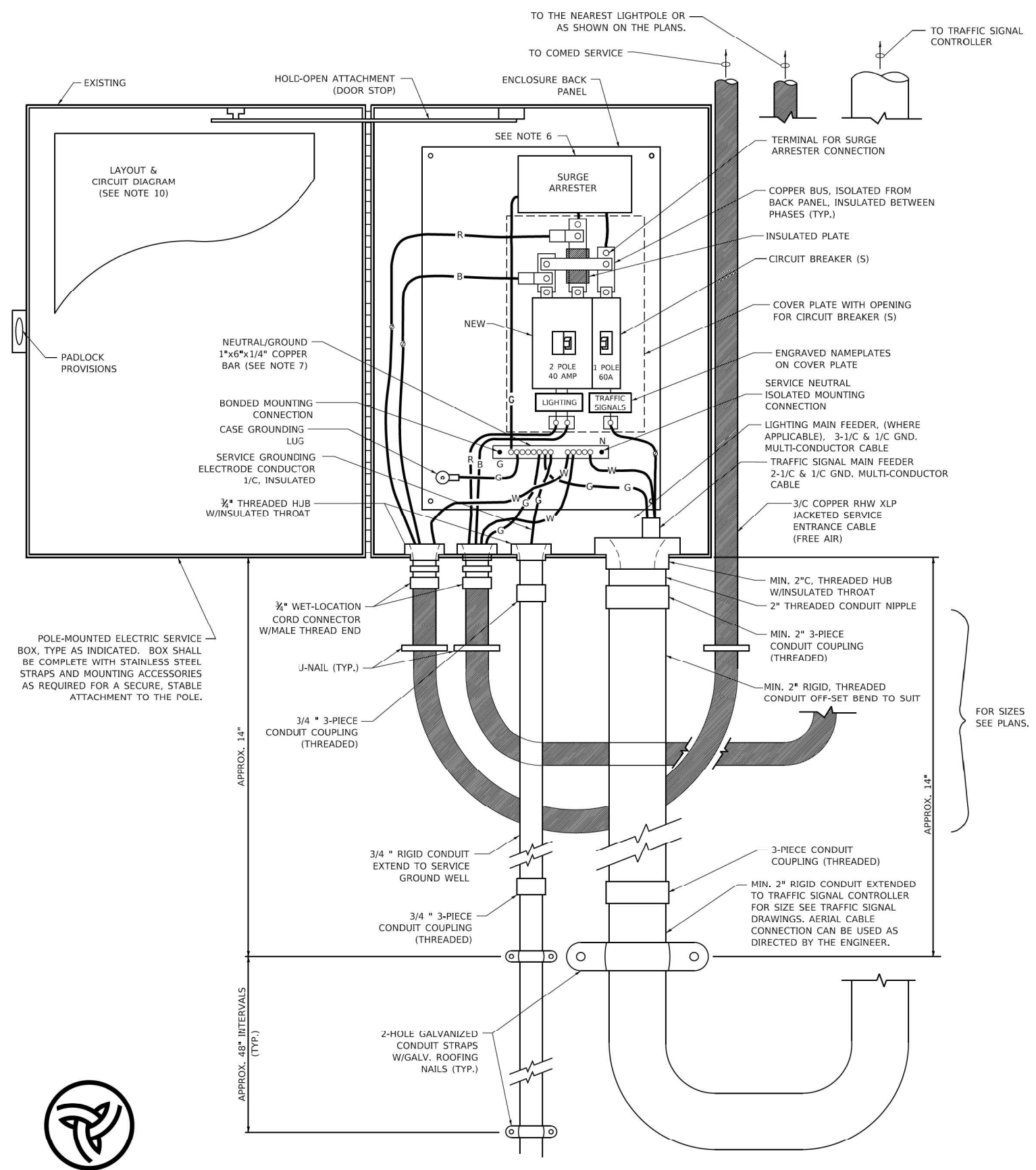
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SHEET 2 OF 3 SHEETS

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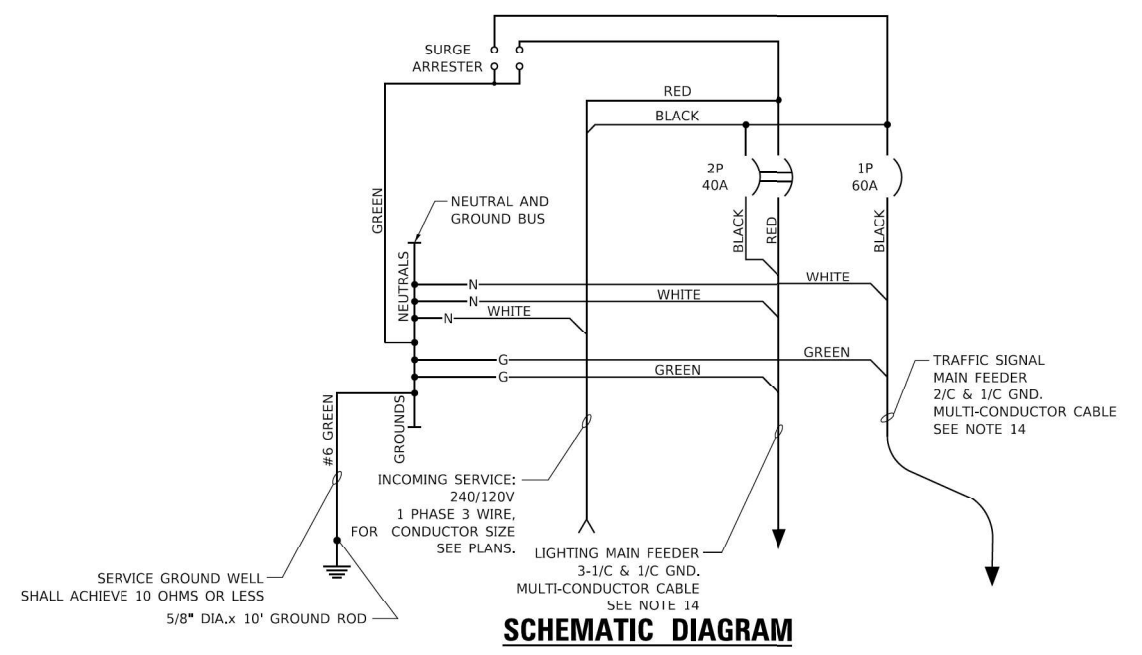
F.A.P. RTE. 66	SECTION 2018-114-I	COUNTY MCHENRY	TOTAL SHEETS 41	SHEET NO. 33
			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				



**POLE-MOUNTED ELECTRIC SERVICE ENTRANCE
GENERAL LAYOUT DIAGRAM**
NOT TO SCALE

NOTES:

- ELECTRIC SERVICE SHALL BE OF THE VOLTAGE INDICATED OR DESIGNATED BY THE ENGINEER, AND SERVICE DROP CABLE SHALL BE COMPATIBLE WITH THE SERVICE ACCORDINGLY. SOME INSTALLATIONS MAY CALL FOR SERVICE ENTRANCE EQUIPMENT SUITABLE FOR 3-WIRE SERVICE EVEN THOUGH INITIALLY WIRED FOR 2-WIRE SERVICE.
- THE POLE-MOUNTED ELECTRIC SERVICE BOX SHALL BE CONFIGURED AND FULLY EQUIPPED FOR 240/120V 3W SERVICE, COMPLETE WITH LIGHTING MAIN BREAKER AND TRAFFIC SIGNALS MAIN BREAKER AS REQUIRED.
- THE ELECTRIC SERVICE EQUIPMENT ASSEMBLY SHALL BE UL LISTED AS SUITABLE FOR USE AS SERVICE ENTRANCE EQUIPMENT.
- THE ELECTRIC SERVICE EQUIPMENT ENCLOSURE SHALL BE NEMA 4X STAINLESS STEEL, NOMINALLY 12"W X 16"H X 8"D, WITH A PIANO-HINGED DOOR, STEEL BACK PANEL, FAST-ACTING STAINLESS STEEL ENCLOSURE CLAMPS, PADLOCK PROVISIONS AND DOOR STOP, HOFFMAN CATALOG NO. A-16H120856LP/A-16 P12/A-DSTOPK/C-PMK12, OR APPROVED EQUAL.
- CIRCUIT BREAKERS SHALL BE THERMAL MAGNETIC BOLT-ON TYPE WITH A MINIMUM INTERRUPTING CAPACITY OF 25,000 SYMMETRICAL AMPERES AT 240 VOLTS. THEY SHALL BE LOCKABLE IN THE "OFF" POSITION FOR COMPLIANCE WITH OSHA LOCK-OUT/TAG-OUT REQUIREMENTS. HANDLES SHALL BE TRIP FREE.
- THE SURGE PROTECTOR SHALL BE SUITABLE FOR THE SERVICE VOLTAGE SINGLE PHASE 60HZ AC, WITH A SURGE ENERGY CAPABILITY OF 2160 JOULES OR BETTER AT 8/20 MICRO-SECONDS, RATED -40 TO 60 DEGREES C., WITH LED OPERATING INDICATORS, AND SHALL BE UL LISTED PER UL 1449, CUTLER-HAMMER CM0V230L065XST OR APPROVED EQUAL.
- BUS BARS, CONNECTORS, AND LUGS SHALL BE COPPER, INSULATED AND ISOLATED, AND CONFIGURED TO PREVENT SHORTED CONDITIONS FROM TIGHTENING TERMINATIONS, ETC. THE OVERALL BUS SECTION SHALL BE CONFIGURED BEHIND AN INSULATING BARRIER SHIELD WHICH IS REMOVABLE FOR ACCESS TO CONNECTIONS, OR THE ASSEMBLY SHALL BE A MANUFACTURED SPECIALTY PANELBOARD, CUTLER-HAMMER PRL2A OR APPROVED EQUAL.
- THE COMBINATION GROUND AND NEUTRAL BAR SHALL BE CONFIGURED WITH SEPARATE GROUND AND NEUTRAL SECTIONS AND SPARE TERMINALS AS INDICATED. THE HEADS OF GROUND SCREWS SHALL BE PAINTED GREEN. THE HEADS OF NEUTRAL SCREWS SHALL BE PAINTED WHITE. THE SERVICE NEUTRAL AND SERVICE GROUNDING ELECTRODE CONDUCTOR SHALL BE TERMINATED ADJACENT TO EACH OTHER AT THE DIVIDE BETWEEN THE SECTIONS AND WIRING SHALL BE TERMINATED ONLY UPON THE APPROPRIATE SECTION.
- THE WIRING TERMINALS, INCLUDING THE GROUND/NEUTRAL BAR SHALL BE ARRANGED TO PROVIDE ADEQUATE ROOM FOR PERFORMING FIELD TERMINATIONS.
- A PLASTIC LAMINATED LAYOUT AND CIRCUIT DIAGRAM SHALL BE MECHANICALLY SECURED TO THE INTERIOR SIDE OF THE ENCLOSURE DOOR.
- A 2-COLOR ENGRAVED PLASTIC NAMEPLATE, ATTACHED WITH SCREWS, AND ENGRAVED AS INDICATED, SHALL BE PROVIDED FOR EACH MAIN BREAKER.
- LUGS AND CONNECTORS SHALL BE RATED FOR 75 C CONDUCTOR.
- THE EXACT MOUNTING HEIGHT OF THE BOX SHALL BE FIELD DETERMINED TO AVOID OBSTRUCTIONS AND PUBLIC ACCESS. TYPICAL HEIGHT SHALL BE APPROXIMATELY 10 FEET ABOVE GRADE.



SCHEMATIC DIAGRAM

MODEL: Default FILE NAME: \\na\al\cib\2022\60411\10_1\CAD Sheets\162856ch-detailed-BE-805-Temporary Lighting And Traffic Signal for single lane station-3.dgn



6" DECAL ON FRONT COVER

ABNA
DESIGN FIRM REG. 184.002117

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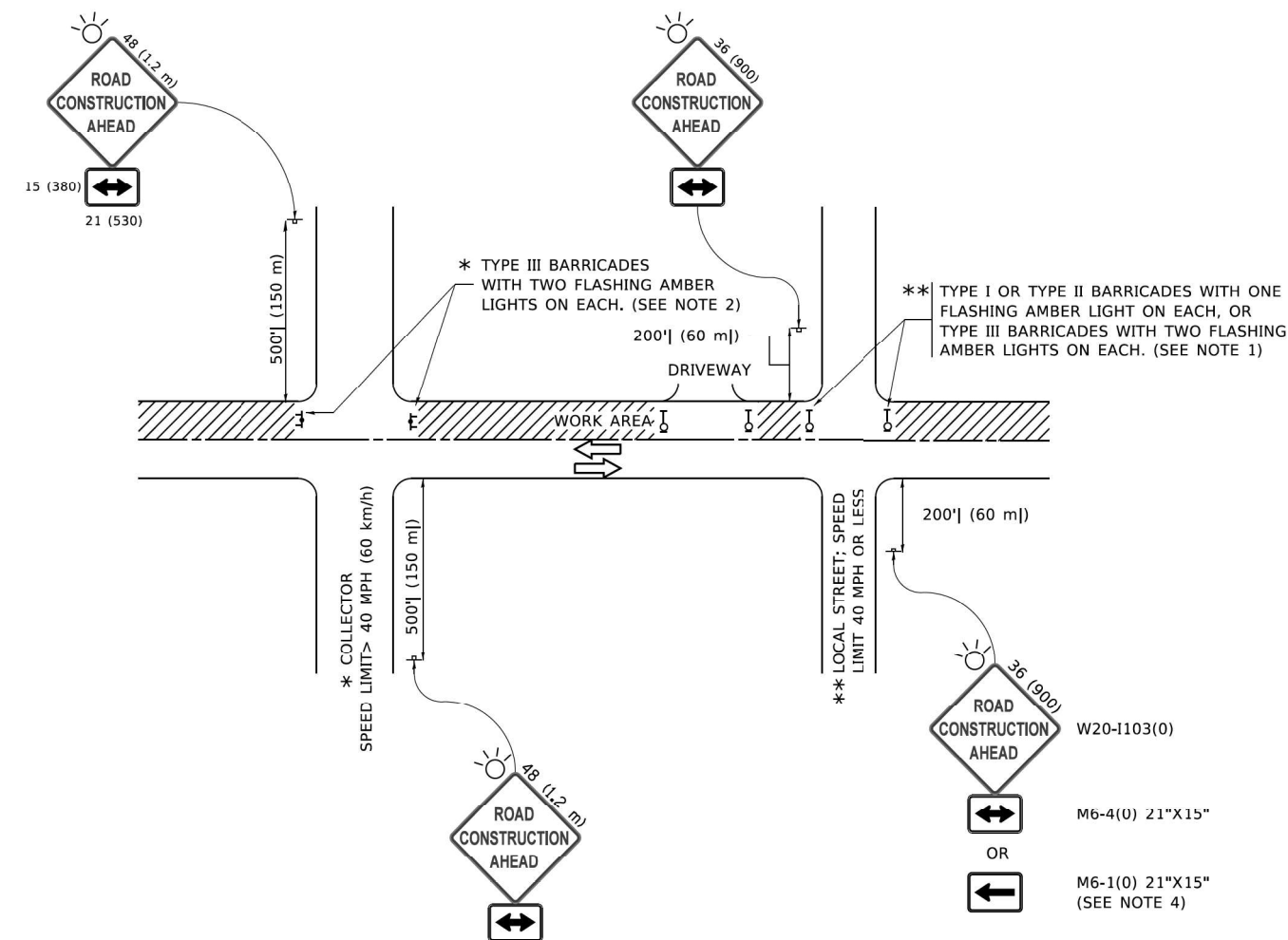
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	DATE = 07/08/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY LIGHTING AND TRAFFIC SIGNALS (BE-805) FOR SINGLE
LANE STAGING U.S. RTE. 14 (OVER DITCH 3 MILES S/O STATE LINE)**

F.A.P. RTE. 66	SECTION 2018-114-I	COUNTY MCHENRY	TOTAL SHEETS 41	SHEET NO. 34
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R50	

SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. TO STA.



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default; FILE NAME: \\na\ah\eb\ba\2022\60411\WO_1\CAD_Sheets\162856\ch-decal\TC-10-TrafficControl and driveways.dgn



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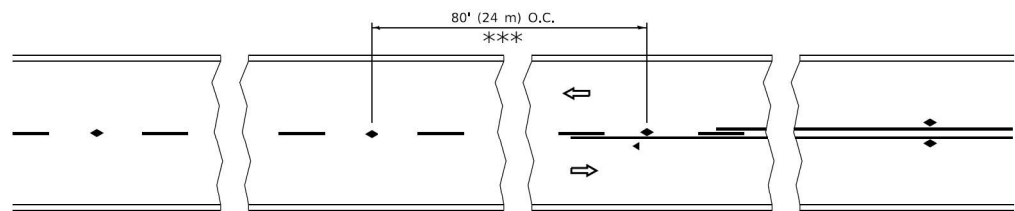
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PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS,
AND DRIVEWAYS (TC-10) U.S. 14 (OVER DITCH 3 MILES SO STATE LINE)**

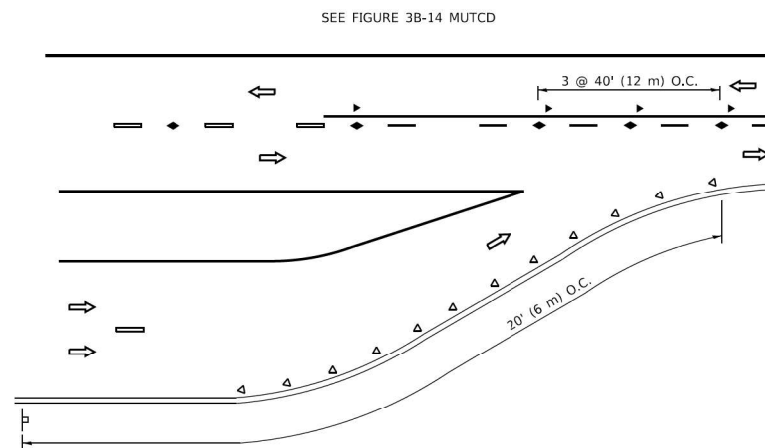
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	35
			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				

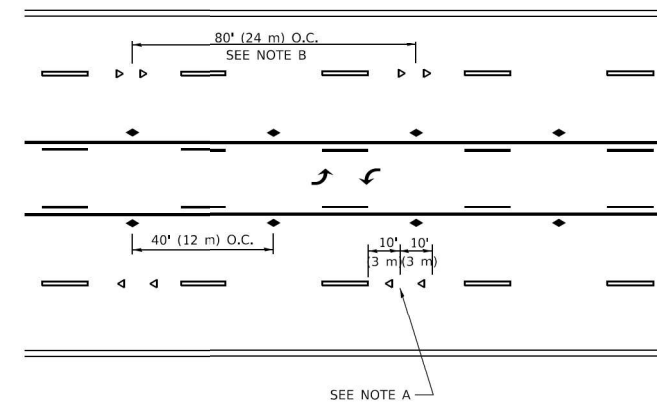


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

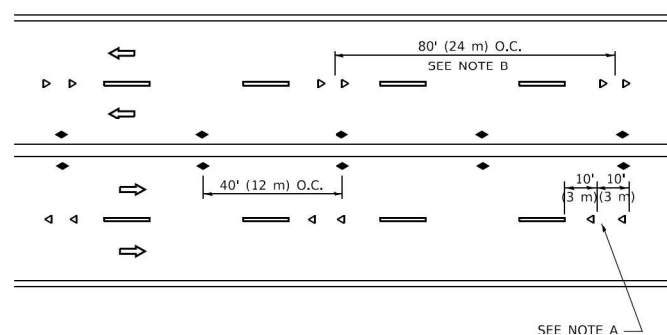
TWO-LANE/TWO-WAY



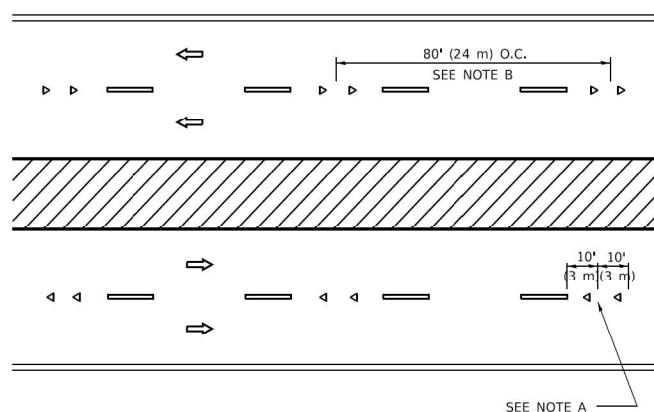
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

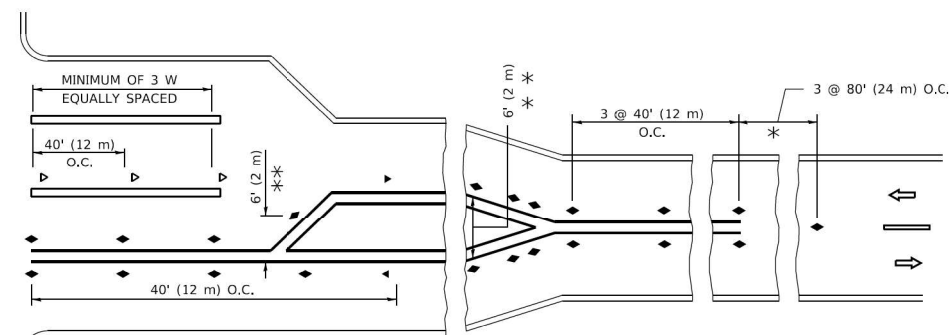
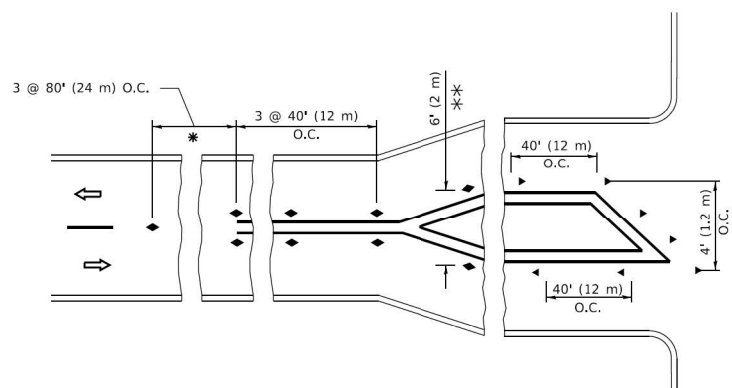
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default; FILE NAME: \\na1a1ebf8a2022.60411\WO_1\CAD_Sheets\1628565ch-draw\TC-11-Raised_Reflective_Markings.dgn



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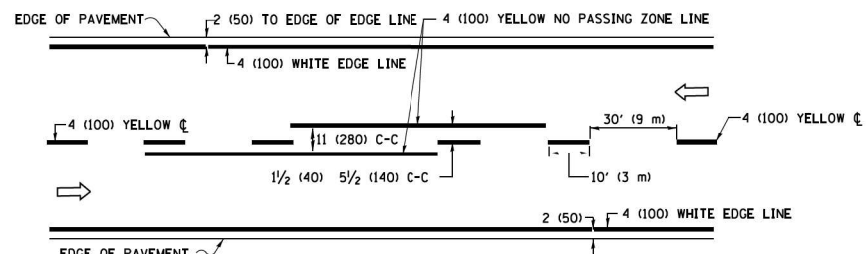
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	DRAWN - OJ/JKR	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - JO	REVISED -
PLOT DATE = 12/13/2022	DATE = 07/08/2022	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

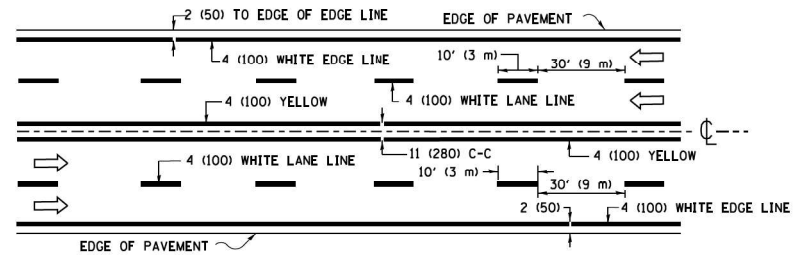
**TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
 (TC-11) U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

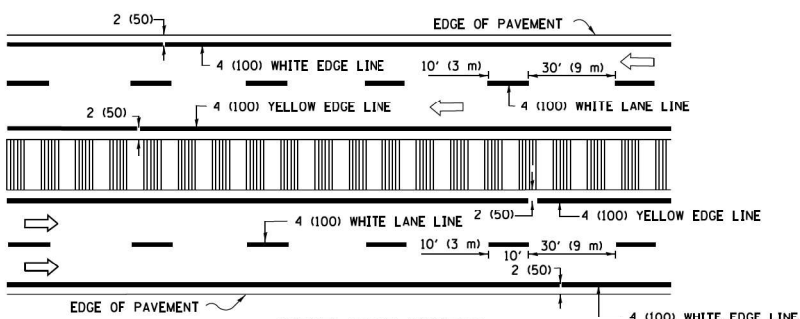
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	36
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

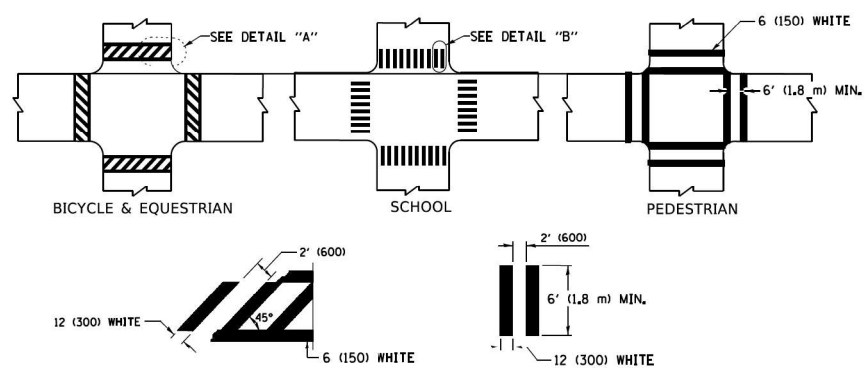


MULTI-LANE UNDIVIDED



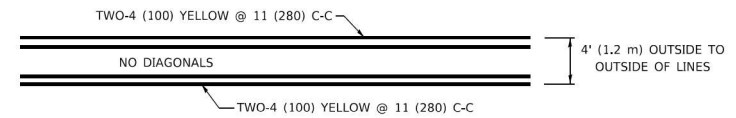
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

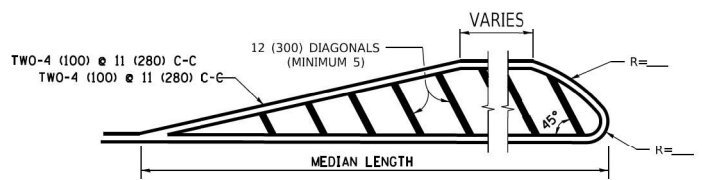


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

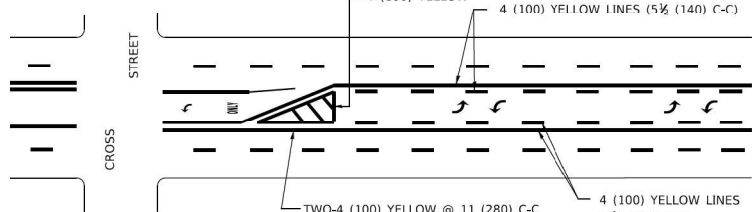


4' (1.2 m) WIDE MEDIANS ONLY



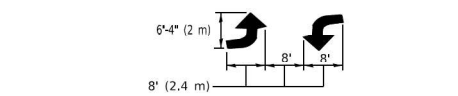
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



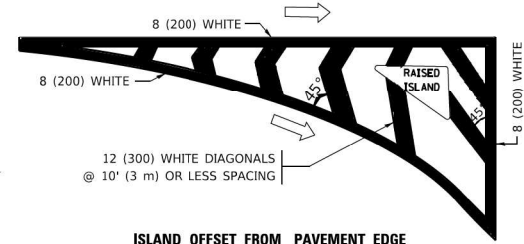
**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

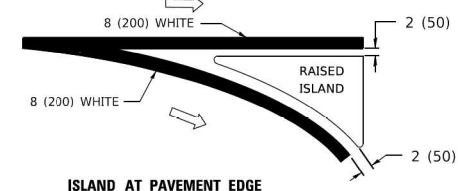


**TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING**

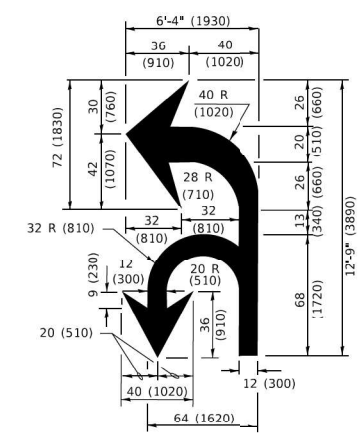
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



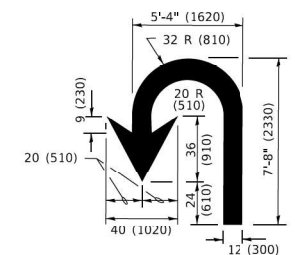
ISLAND OFFSET FROM PAVEMENT EDGE



**ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING**



COMBINATION LEFT AND U-TURN



U-TURN

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	7 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15' 6" (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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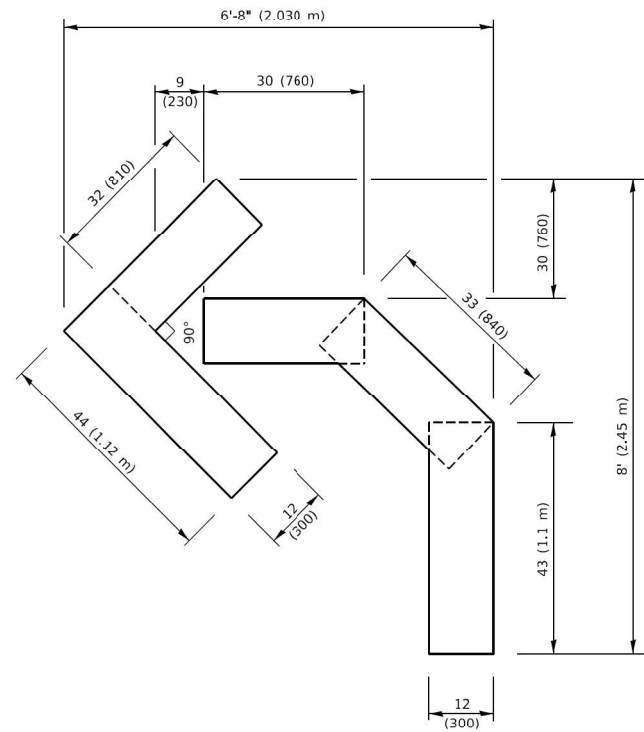
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PLOT DATE = 12/13/2022	CHECKED - JO	REVISED -
	DATE = 07/08/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)

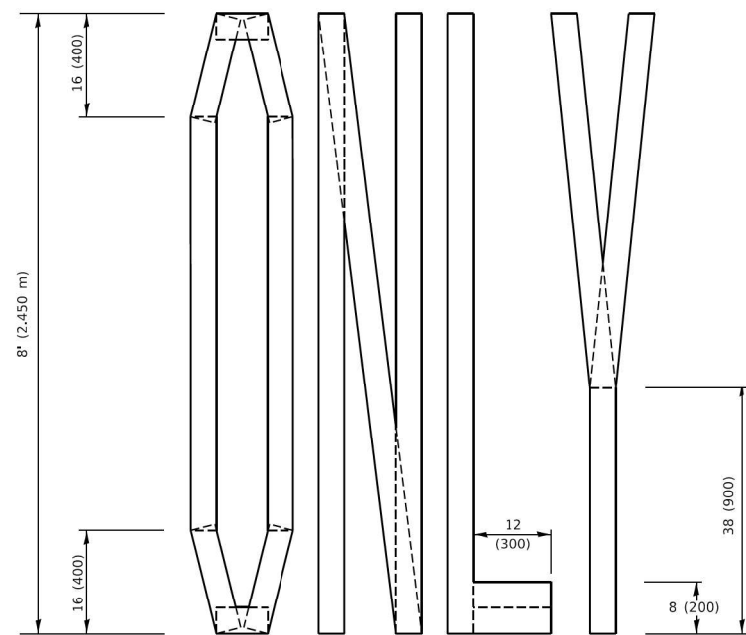
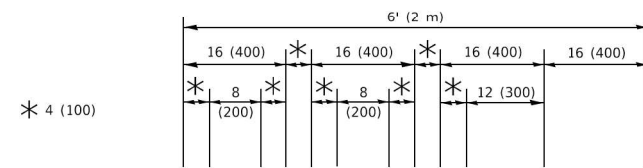
SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 66	SECTION 2018-114-I	COUNTY MCHENRY	TOTAL SHEETS 41	SHEET NO. 37
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62R50	



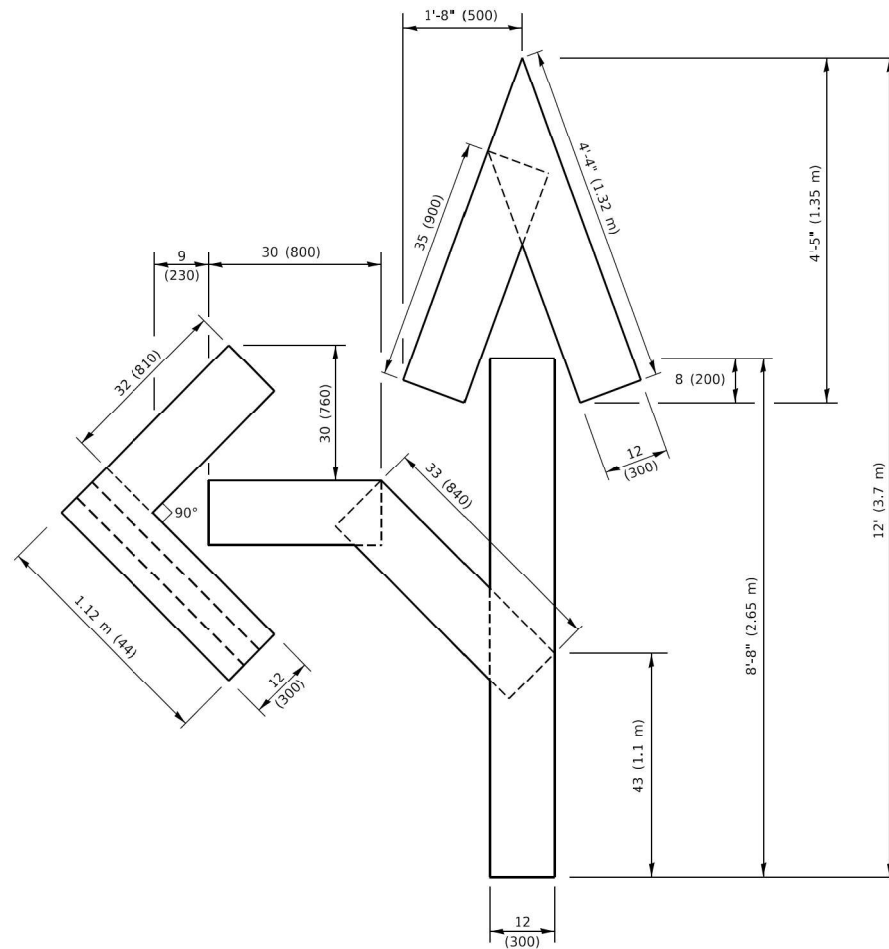
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

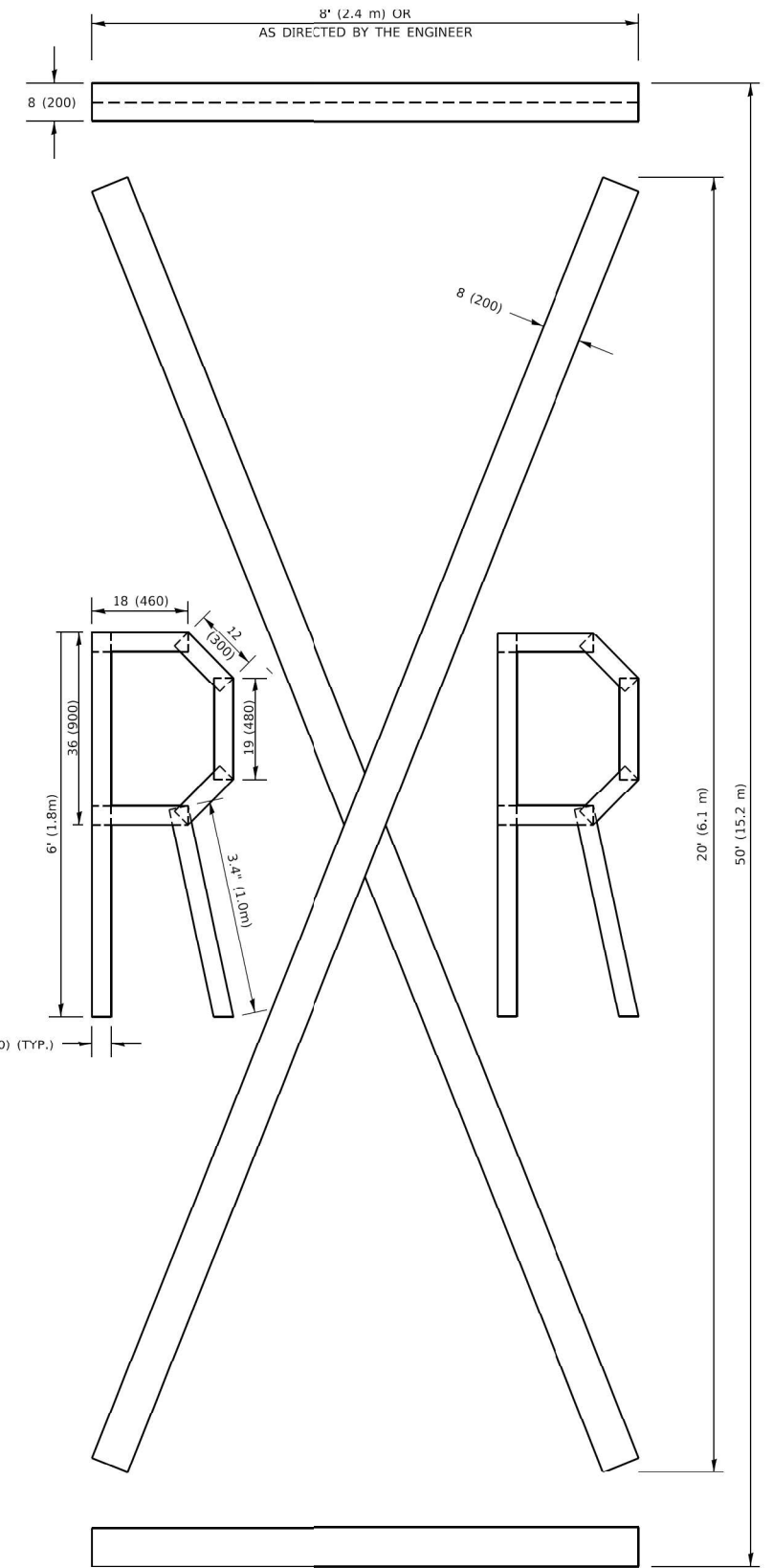


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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745 McClintock Drive
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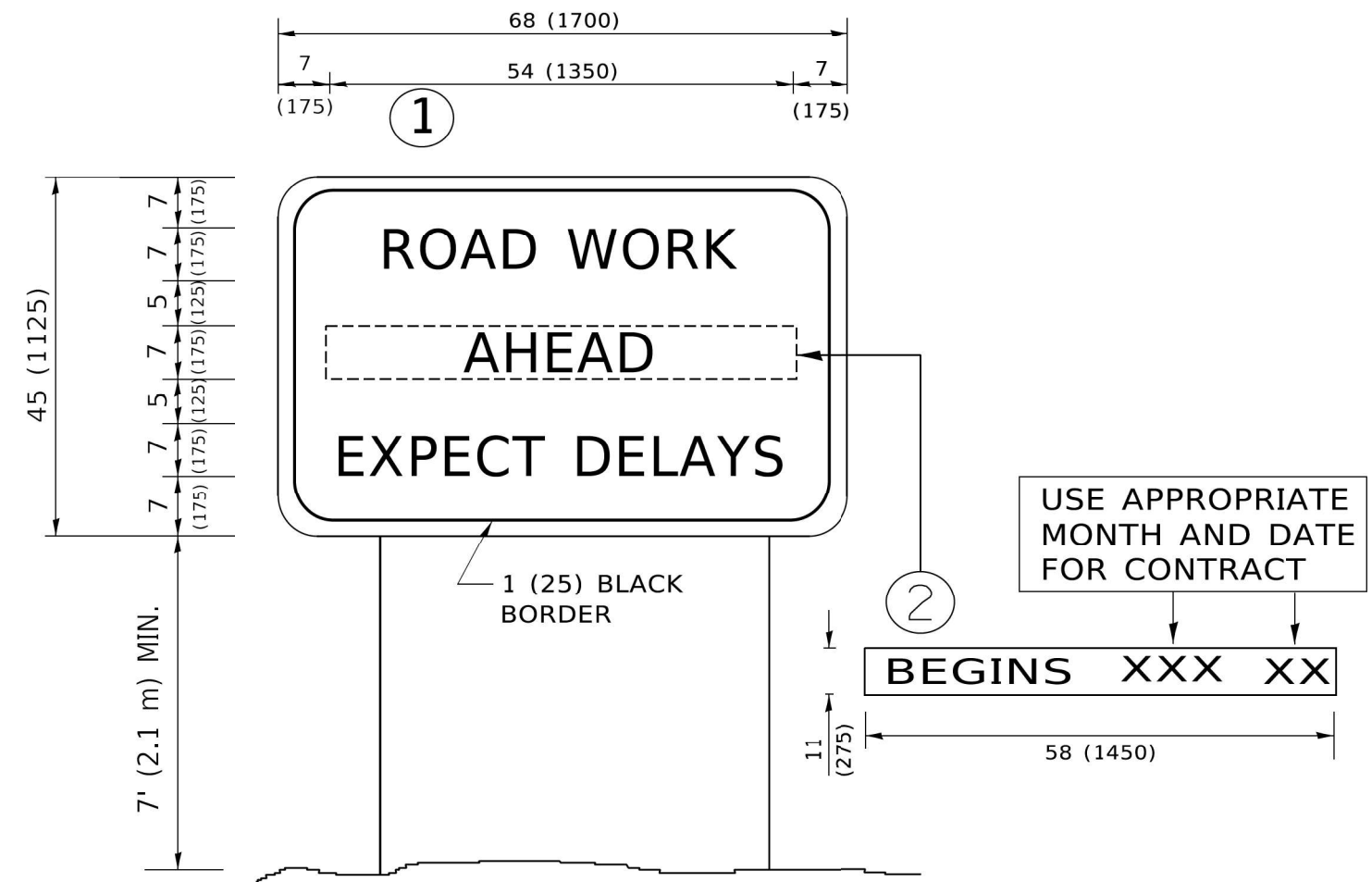
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PLOT DATE = 12/13/2022	DATE = 07/08/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKINGS LETTERS AND SYMBOLS (TC-16)
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	38
			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT DATE = 12/13/2022	DATE - 07/08/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN (TC-22)
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)

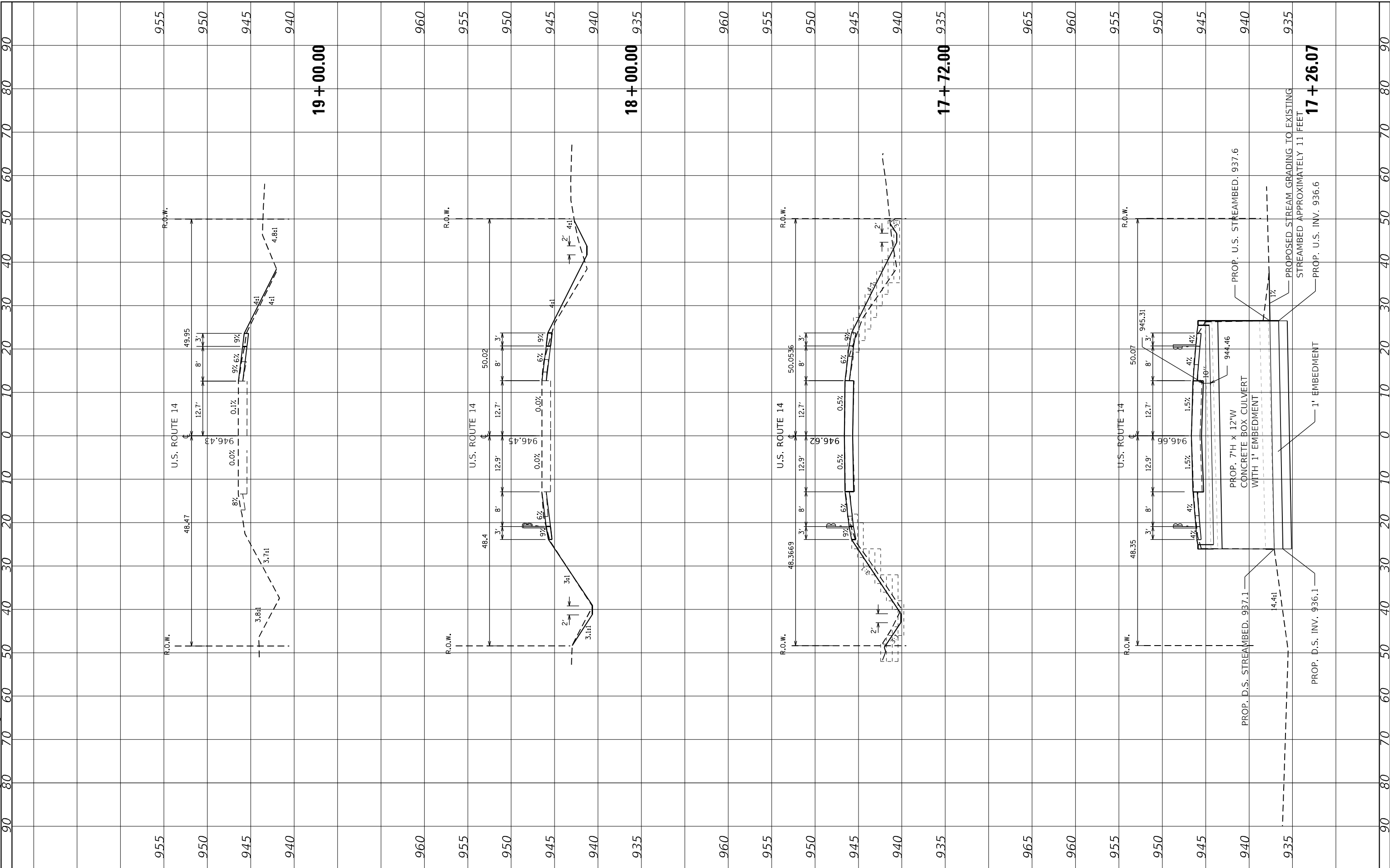
SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-114-I	MCHENRY	41	39
			CONTRACT NO. 62R50	
ILLINOIS FED. AID PROJECT				

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
	PLOTTED		
	TEMPLATE		
	AREAS CHECKED		
	AREAS CHECKED		

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ABNA
 745 McClintock Drive
 Suite 210
 Burr Ridge, IL 60527
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USER NAME = tpaolktchi	DESIGNED - TPP	REVISED -
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PLOT DATE = 12/13/2022	CHECKED - JO	REVISED -
	DATE - 12/13/2022	REVISED -

DESIGNED - TPP	REVISED -
DRAWN - OJJ/KR	REVISED -
CHECKED - JO	REVISED -
DATE - 12/13/2022	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)

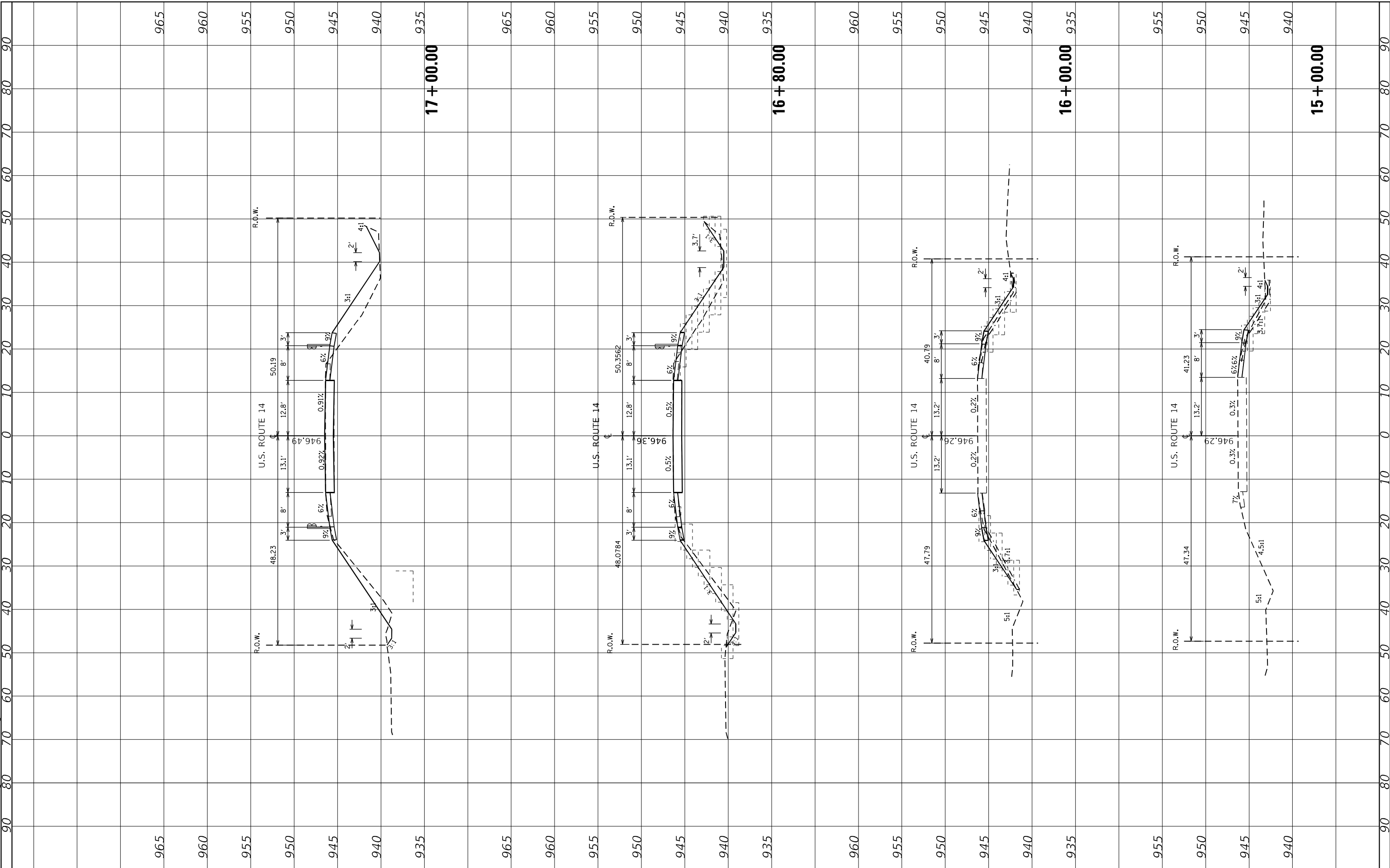
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F.A.P. RTE. 66	SECTION 2018-118-I	COUNTY 2018-118-I	TOTAL SHEETS 41	SHEET NO. 41
CONTRACT NO. 62R50			ILLINOIS FED. AID PROJECT	

FINAL SURVEY NO.	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
AREAS CHECKED	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY NO.	SURVEYED	BY	DATE
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USER NAME	= tpaolcchi
DESIGNED	- TPP
DRAWN	- OJ/JKR
CHECKED	- JO
DATE	- 12/13/2022

REVISIONS	
REVISIONS	
REVISIONS	
REVISIONS	
REVISIONS	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
 U.S. RTE. 14 (OVER DITCH 3 MILES SO STATE LINE)**

SCALE: SHEET 1 OF 2 SHEETS STA. 15+00.00 TO STA. 17+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
66	2018-118-I	2018-118-I	41	40
CONTRACT NO. 62R50				
ILLINOIS FED. AID PROJECT				