

CONSTRUCTION PLANS FOR VERMILION REGIONAL AIRPORT AUTHORITY

VERMILION REGIONAL AIRPORT
DANVILLE, ILLINOIS

100% SUBMITTAL

IL. PROJ. NO. DNV-4588
FED PROJ. NO. 3-17-SBGP-139
REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

JANUARY 12, 2018

SUMMARY OF QUANTITIES			
ITEM	DESCRIPTION	QUANTITY	UNIT
AR150510	ENGINEER'S FIELD OFFICE	1	LS
AR152480	SHOULDER ADJUSTMENT	1,650	SY
AR152511	SUBGRADE REPAIR	200	SY
AR201660	BITUMINOUS CRACK REPAIR	966	LF
AR201670	CRACK CONTROL FABRIC	946	SY
AR401610	BITUMINOUS SURFACE COURSE	859	TON
AR401650	BITUMINOUS PAVEMENT MILLING	4,125	SY
AR401655	BUTT JOINT CONSTRUCTION	774	SY
AR603510	BITUMINOUS TACK COAT	1,698	GAL
AR620520	PAVEMENT MARKING - WATERBORNE	1,524	SF
AR620525	PAVEMENT MARKING - BLACK BORDER	2,101	SF
AR620900	PAVEMENT MARKING REMOVAL	85	SF
AR701900	REMOVE PIPE	16	LF
AR701920	REPLACE PIPE	16	LF
AR701980	REPAIR PIPE	1	EACH
AR901510	SEEDING	0.3	AC
AR908525	KNITTED STRAW MAT	1,320	SY

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No.	Sheet Number	Sheet Title
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03	GI501	CONSTRUCTION SAFETY PHASING NOTES
04	GI502	CONSTRUCTION ACTIVITY PLAN & DETAILS
05	GC100	CONSTRUCTION ACTIVITY PLAN
06	GC101	CONSTRUCTION ACTIVITY PLAN - CLOSURES
07	CD101	EXISTING CONDITIONS & MILLING PLAN
08	CP101	PROPOSED IMPROVEMENTS & TYPICAL SECTION
09	CP501	SINKHOLE & PAVING DETAILS
10	CS101	STAKING PLAN
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14	CG301	CROSS SECTIONS SHEET 1
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811 Know what's below. Call before you dig. **COMMON GROUND ALLIANCE** www.call811.com or Phone: 811

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL 911 IN THE EVENT IN WHICH DAMAGE RESULTS IN THE RELEASE OF NATURAL GAS.

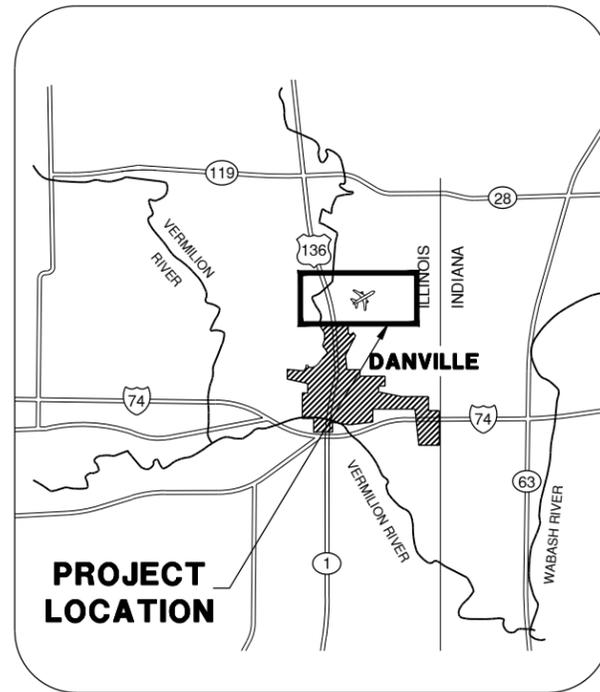
DESIGN INFORMATION

GEOMETRIC CRITERIA
TAXIWAY DESIGN GROUP 2

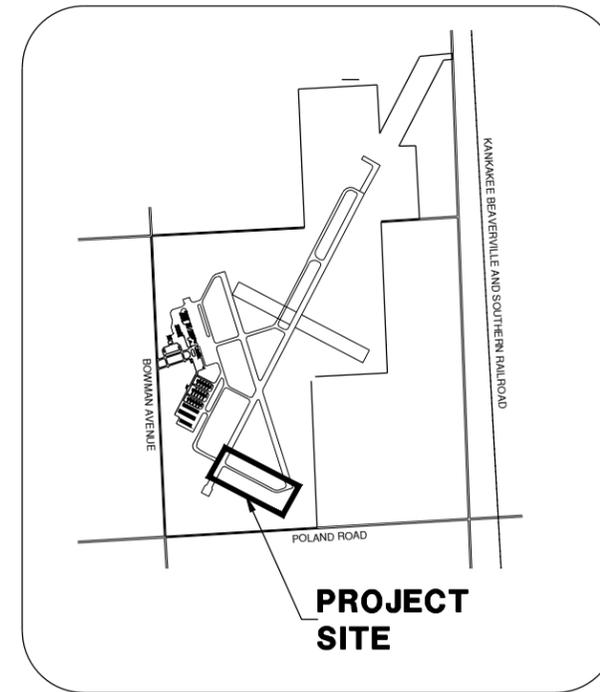
PAVEMENT DESIGN CRITERIA
DEPARTURE WEIGHT = 20,000 LBS.
SINGLE WHEEL GEAR

CALL J.U.L.I.E. BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 20 NORTH
RANGE: 11 WEST
SECTION: 15
COUNTY: VERMILION
CIVIL TOWNSHIP: NEWELL



LOCATION MAP



SITE PLAN

COMMON TRAFFIC ADVISORY FREQUENCY 122.7
APPROXIMATE MAXIMUM HEIGHT OF EQUIPMENT ABOVE GROUND IS 25 FT.

VERMILION REGIONAL AIRPORT AUTHORITY
VERMILION REGIONAL AIRPORT

APPROVED *Michael B. Her*
DATE 1/9/2018 2018

Jan. 9, 2018
CHRISTOPHER B. GROTH
062-056232
LICENSED PROFESSIONAL ENGINEER OF ILLINOIS
Chris B. Groth
Exp. 11-30-2019

CMT
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SUBMITTED BY *Christy B. Groth*
DATE Jan 9, 2018
CMT JOB NUMBER: 16042-01-00

GENERAL

- 1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. ~~PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.~~
6. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

NOT IN CONTRACT

1. COORDINATION

- 1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH THE AIRPORT STAFF AND RESIDENT ENGINEER. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

- 1. TOTAL CONTRACT TIME SHALL BE 36 CALENDAR DAYS.
2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN (CAP) SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

- 1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
4. THE AIRPORT WILL BE HOSTING EVENTS THROUGHOUT THE SUMMER AND RESERVES THE RIGHT TO HALT CONSTRUCTION UNTIL FURTHER NOTICE. KNOWN EVENTS AT THIS TIME ARE:
- JULY 14-15
- SEPTEMBER 5-9

5. CONTRACTOR ACCESS

- 1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION ACTIVITY PLAN SHEETS. ALL COSTS RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE GATE SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE CLOSED DURING WORK HOURS.
3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE VILLAGE, COUNTY, TOWNSHIP, OR I.D.O.T.
4. CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE MARKED AND FLAGGED PER SECTION 70-10 OF THE STANDARD SPECIFICATIONS. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT WILL BE 25'.
5. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, ASPHALT, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE. WHILE INSIDE THE AOA FENCE, THE TRUCK DRIVERS SHALL BE ESCORTED BY THE CONTRACTOR PERSONNEL THAT HAS OBTAINED PROPER DRIVING PRIVILEGES.
6. CONTRACTOR WORK CREWS MUST MAINTAIN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY (CTAF, 122.7) AT ALL TIMES WHEN INSIDE THE AIRPORT OPERATIONS AREA (AOA). THE CONTRACTOR SHALL SUPPLY ALL APPROPRIATE RADIOS NEEDED FOR COMMUNICATIONS AND ONLY HIS PERSONNEL WHO HAVE SUCCESSFULLY SATISFIED THE AIRPORT OF THEIR COMPETENCE MAY OPERATE THESE RADIOS.
7. THE CONTRACTORS STORAGE AND STAGING AREA WILL BE AS SHOWN ON THE SITE PLAN.
8. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
9. WHEN THE CONTRACTOR IS NOT WORKING, EQUIPMENT SHALL BE STORED AT THE STAGING AREA.
10. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.
11. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.
12. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE COMMON TRAFFIC ADVISORY FREQUENCY. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
13. THE CONTRACTOR SHALL THOROUGHLY CLEAN ALL CONSTRUCTION AREAS AND HAUL ROUTES WHICH WILL BE OPENED TO AIR TRAFFIC TO THE SATISFACTION OF AIRPORT OPERATIONS OR THE RESIDENT ENGINEER. A POWER BROOM AND OPERATOR SHALL BE ON SITE AT ALL TIMES WHEN ACTIVE PAVEMENTS ARE UTILIZED FOR CONSTRUCTION TRAFFIC.
14. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS OR STORAGE AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
15. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.
16. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

6. WILDLIFE MANAGEMENT

- 1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. NOTIFICATION OF CONSTRUCTION ACTIVITIES

- 1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

8. INSPECTION REQUIREMENTS

- 1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

9. UNDERGROUND UTILITIES

- 1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. ANY UTILITY, INCLUDING AIRFIELD ELECTRICAL CABLE AND LIGHTS, DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY HIM AT HIS OWN EXPENSE IN A MANNER WHICH IS SATISFACTORY TO THE ENGINEER AND TO THE OWNER OF THE UTILITY. ANY REPAIRS THAT MUST BE MADE BY THE OWNER OF THE UTILITY SHALL HAVE THE COST REIMBURSED TO THE UTILITY BY THE CONTRACTOR. AIRFIELD LIGHTING CABLES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY A QUALIFIED ELECTRICIAN WITH THE COSTS TO BE BORNE BY THE CONTRACTOR.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.

10. PENALTIES

- 1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

11. SPECIAL CONDITIONS

- 1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

12. RUNWAY AND TAXIWAY VISUAL AIDS

- 1. ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION ACTIVITY PLAN.
2. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

13. MARKING AND SIGNS FOR ACCESS ROUTES

- 1. BARRICADES AND SIGNS SHALL BE USED ALONG THE CONTRACTOR'S ACCESS ROUTE AS DETAILED ON THE CONSTRUCTION ACTIVITY PLAN SHEETS.

14. HAZARD MARKING AND LIGHTING

- 1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION ACTIVITY PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. THE AIRPORT WILL PROVIDE TWO PORTABLE CLOSED RUNWAY MARKERS FOR USE DURING THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTENANCE OF THE RUNWAY CLOSURE MARKERS INCLUDING FUEL, OIL CHANGES AND REPLACEMENT OF THE LIGHTS. UPON COMPLETION OF THE PROJECT, THE PORTABLE CLOSED RUNWAY MARKERS SHALL BE RETURNED TO THE AIRPORT. THE CONTRACTOR SHALL SUPPLY TWO PORTABLE CLOSED RUNWAY CLOSURE MARKERS FOR THE OTHER RUNWAY.

15. PROTECTION

- 1. ALL WORK REQUIRED INSIDE OF THE RUNWAY 3/21 OR 16/34 SAFETY AREAS, WHICH EXTEND 250' FROM THE RUNWAY CENTERLINE, WILL REQUIRE THE RUNWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 48 HOURS PRIOR TO THE REQUESTED CLOSURE TIME SO THE AIRPORT CAN ISSUE NOTAMS.
2. ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WHICH EXTENDS 66' FROM THE TAXIWAY CENTERLINE OF TAXIWAYS, WILL REQUIRE THE TAXIWAY TO BE CLOSED. THE CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 72 HOURS PRIOR TO THE REQUESTED CLOSURE TIME.

16. OTHER LIMITATIONS ON CONSTRUCTION

- 1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
3. THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.



License No. 184-000613 CONSULTANTS

100% SUBMITTAL JANUARY 12, 2018

REHABILITATE TAXIWAY D BETWEEN RUNWAY 3 AND 34

OWNER



VERMILION REGIONAL AIRPORT AUTHORITY DANVILLE, ILLINOIS

Table with 3 columns: MARK, DATE, DESCRIPTION

Table with 2 columns: FIELD, VALUE. Includes project details like FED PROJ. NO., IL PROJ. NO., CMT PROJECT NO., etc.

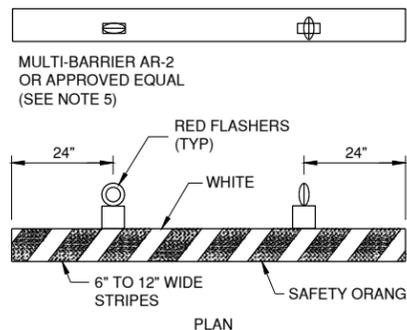
CONSTRUCTION SAFETY PHASING NOTES

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SEQUENCE OF CONSTRUCTION NOTES

THE GENERAL PROGRESSION OF THE WORK SHALL BE AS FOLLOWS:

- SUBMIT MATERIAL AND EQUIPMENT SHOP, PLAN AND WORKING DRAWINGS FOR REVIEW. INCLUDE WITH THE SUBMITTALS ALL BUY AMERICAN CERTIFICATIONS FOR ALL MATERIALS.
- SUBMIT NOTICE OF OBSTRUCTION EVALUATION- AIRPORT AIRSPACE ANALYSIS (OE/AAA) INFORMATION FOR ANTICIPATED EQUIPMENT HEIGHTS IF IN EXCESS OF 25'. NOTE THAT THIS PROCESS MAY REQUIRE UP TO 90 DAYS FOR FAA APPROVAL. EQUIPMENT ABOVE 25' HEIGHT SHALL NOT BE UTILIZED UNTIL FAA APPROVAL HAS BEEN PROVIDED.
- SUBMIT PROJECT SCHEDULE SHOWING RELATIONSHIP BETWEEN CONSTRUCTION DURATION FOR PAY ITEMS IN RELATION TO THE PHASES OF WORK WHERE THEY ARE BEING PERFORMED. CLEARLY IDENTIFY DATES OF RUNWAY CLOSURES AND WHAT PHASES WILL BE WORKED IN DURING THAT CLOSURE.
- SUBMIT PRELIMINARY MATERIALS CERTIFICATIONS INCLUDING BUY AMERICAN CERTIFICATIONS AND WAIVER REQUEST FOR MATERIALS THAT DO NOT MEET THE CONTRACT REQUIREMENTS.
- INITIATE CONSTRUCTION OF TAXIWAY/APRON REHABILITATION. INSTALL BARRICADES AS OUTLINED ON THE CONSTRUCTION ACTIVITY PLANS. FIELD VERIFY LOCATION OF EXISTING UTILITIES.
- INITIATE CONSTRUCTION WITHIN THE VARIOUS PHASES OF THE PROJECT. TAXIWAY C/C1 AND APRON REHABILITATION PHASES SHALL BE CONSIDERED THE PRIMARY WORK AREAS. WORK IN THESE AREAS SHALL INCLUDE REMOVAL OF PAVEMENT, PAVEMENT MILLING, BITUMINOUS PAVING, PAVEMENT MARKING AND TURFING.
- UPON COMPLETION OF ALL PHASES, THE CONTRACTOR SHALL REQUEST A FINAL INSPECTION OF THE PROJECT.

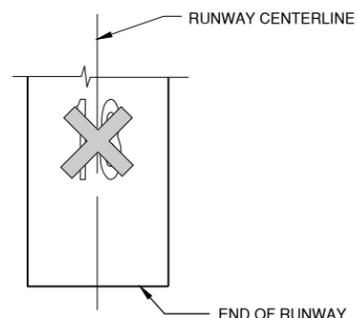


BEAM BARRICADE NOTES:

- FLASHERS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A 15' SPACING BETWEEN ENDS OF BARRICADES ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION OR AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- FLASHERS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR LOW AS POSSIBLE, BUT NOT TO EXCEED 3 INCHES ABOVE THE GROUND.

LOW PROFILE LIGHTED BARRICADE

N.T.S.



RUNWAY SAFETY AREAS

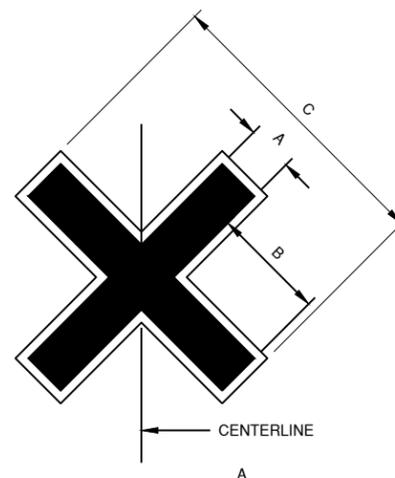
- WORK IN THE RUNWAY 3/21 AND 16/34 SAFETY AREA SHALL BE LIMITED TO THAT WORK NECESSARY TO REHABILITATE TAXIWAY D PAVEMENT. NO PAVEMENT IMPROVEMENTS WILL BE MADE TO THE SURFACE OF RUNWAY 3/21 AND 16/34.
- WORK WITHIN THE RUNWAY 3/21 AND 16/34 SAFETY AREA SHALL CONSIST OF PAVEMENT MILLING, PAVEMENT CONSTRUCTION AND PAVEMENT MARKING. THE MILLED PAVEMENT SURFACE WILL RESULT IN A DROP OFF WITHIN THE RSA OF LESS THAN 3". CONSTRUCTION WITHIN THE RSA SHALL REQUIRE THE CLOSURE OF RUNWAY 3/21 AND 16/34. RUNWAY SHALL BE REOPENED UPON COMPLETION OF CONSTRUCTION ACTIVITIES.
- WORK WITHIN THE RUNWAY SAFETY AREAS SHALL OCCUR DURING DAY TIME OPERATIONS ONLY AS NOTED IN PHASING TABLE. RUNWAYS SHALL BE REOPENED AT THE END OF EACH WORK DAY. BARRICADES AND RUNWAY CLOSURE X SHALL BE PLACED AT THE BEGINNING OF EACH WORK DAY CLOSURE AND REMOVED AT THE END OF THE WORK DAY.
- EQUIPMENT OR PERSONNEL SHALL REMAIN CLEAR OF THE RUNWAY PAVEMENTS AT ALL TIMES.
- NO EQUIPMENT, STOCKPILES OR EXCAVATIONS SHALL REMAIN INSIDE THE RUNWAY SAFETY AREAS AFTER WORKING HOURS.

CLOSED RUNWAY MARKER DETAIL

N.T.S.

NOTES

- MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE SELF-CONTAINED MARKERS PROVIDED BY THE CONTRACTOR THROUGH THE CONTRACT.
- COST OF INSTALLING, MAINTAINING, RELOCATING AND REMOVING MARKERS SHALL BE INCIDENTAL TO THE CONTRACT.
- MARKERS SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS.
- NON PAINTED MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT.



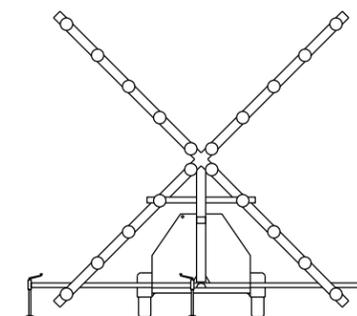
CLOSED RUNWAY/ TAXIWAY MARKER DETAIL

N.T.S.

SYMBOL TYPE	DIMENSION	A	B	C
CLOSED RUNWAY		10'-0"	25'-0"	60'-0"
CLOSED TAXIWAY		5'-0"	12'-6"	30'-0"

NOTES

- CLOSURE MARKERS SHALL BE SOLID YELLOW.
- MARKERS SHALL BE PLACED ON TAXIWAYS AT THE RUNWAY INTERSECTIONS INSIDE THE RUNWAY SAFETY AREA.
- MARKERS MAY BE PAINTED ON THE TAXIWAY USING TEMPORARY PAINT OR CONSTRUCTED OF FABRIC, COLORED PLASTIC, PAINTED SHEETS OF PLYWOOD OR SIMILAR MATERIALS.
- NON PAINTED MARKERS SHALL BE SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST OR OTHER WIND CURRENTS. METHODS OF SECURING THE MARKERS SHALL NOT PROTRUDE MORE THAN 3" ABOVE THE PAVEMENT.



LIGHTED RUNWAY CLOSURE MARKER

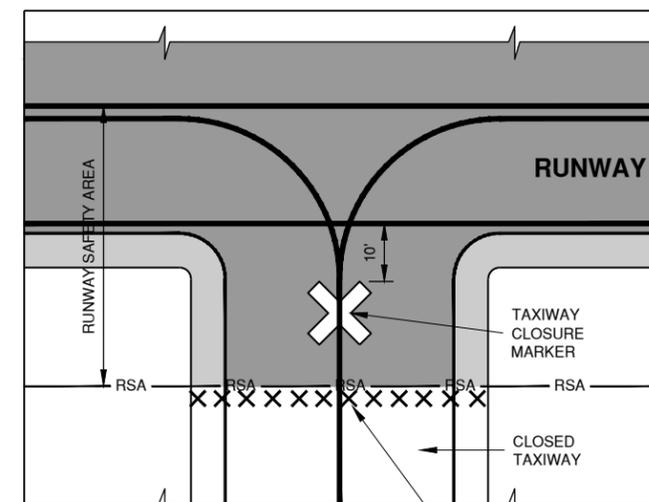
N.T.S.

NOTES

- TO BE PLACED ON PAVEMENT AT THE RUNWAY NUMERALS.
- PAIR OF LIGHTED 'X'S TO BE PROVIDED BY THE AIRPORT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE (FUEL, OIL, LIGHT BULBS) WHEN USED DURING CONSTRUCTION CLOSURES. A SECOND PAIR OF LIGHTED X'S SHALL BE PROVIDED BY THE CONTRACTOR.

TAXIWAY OBJECT FREE AREAS

- WORK IN THE TAXIWAY OBJECT FREE AREAS SHALL REQUIRE THAT TAXIWAY TO BE CLOSED.
- NO EQUIPMENT, OPEN TRENCHES OR EXCAVATIONS SHALL REMAIN INSIDE THE TAXIWAY OBJECT FREE AREAS AFTER WORKING HOURS.
- THE TAXIWAY SHALL BE CLOSED WITH BARRICADES AT 15' CLEAR SPACING PRIOR TO WORKING IN THE CLOSED WORK AREAS.



CLOSED TAXIWAY/ACTIVE RUNWAY BARRICADE DETAIL

N.T.S.

100% SUBMITTAL
JANUARY 12, 2018

REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

OWNER



VERMILION REGIONAL AIRPORT
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AUTHORITY
DANVILLE, ILLINOIS

MARK | DATE | DESCRIPTION

FED PROJ. NO. 3-17-SBGP-139

IL PROJ. NO. DNV-4588

CMT PROJECT NO: 16042-01-00

CAD DWG FILE: DNV4588-1604201-GI502.DWG

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DRAWN BY: DPA

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CONSTRUCTION
ACTIVITY PLAN &
DETAILS

GI502

SHEET 4 OF 16

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CMT PROJECT NO: 16042-01-00

CAD DWG FILE: DNV4588-1604201-GC100.DWG

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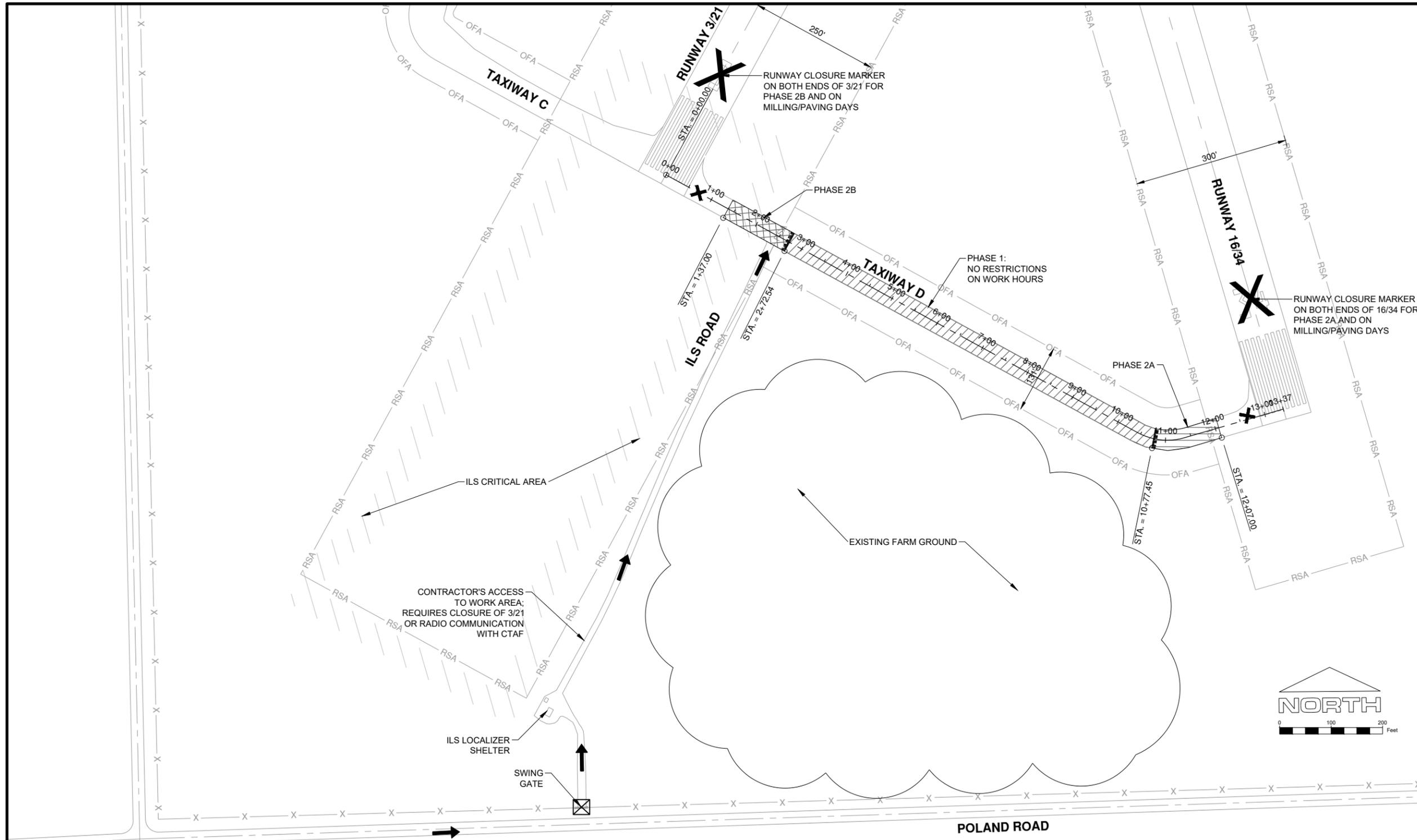
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CONSTRUCTION
ACTIVITY PLAN

GC100
SHEET 5 OF 16



PHASING SCHEDULE			
PHASE	WORK AREA	PAVEMENT STATUS	WORK HOURS
1	TXY D BETWEEN HOLD LINES	*TXY D CLOSED, ALL RUNWAYS OPEN	UNRESTRICTED
2A	TXY D INSIDE RWY 34 RSA	*TXY D CLOSED, RWY 16/34 CLOSED, TXY B CLOSED	8:00 AM - 9:00 PM
2B	TXY D INSIDE RWY 3 RSA	*TXY D CLOSED, 3/21 CLOSED, TXY C CLOSED, TXY A CLOSED EAST OF 16	8:00 AM - 9:00 PM

1. ACCESS TO TAXIWAY D IS THROUGH THE RUNWAY SAFETY AREA OF 3/21. ON MILLING AND PAVING DAYS, BOTH 3/21 AND 16/34 SHALL BE CLOSED TO AIR TRAFFIC PER GC101. ON OTHER WORKING DAYS, THE CONTRACTOR SHALL MONITOR RADIO AND YIELD THE RIGHT-OF-WAY TO AIRCRAFT USING 3/21 AS THE CONTRACTOR ACCESSES THE WORK AREA.

- NOTES**
- CONTRACTOR SHALL NOTIFY THE AIRPORT 48 HOURS PRIOR TO INITIATING THE PHASE.
 - CONTRACTOR SHALL PLACE BARRICADES AND TAXIWAY CLOSURE MARKINGS AT THE LOCATIONS SHOWN ON THE PLANS.
 - CONSTRUCTION ACCESS WILL CROSS ACTIVE RUNWAY SPACE. THE CONTRACTOR SHALL YIELD THE RIGHT OF WAY TO ALL AIRCRAFT WHEN ACCESSING THE WORK AREA.
 - PHASES 1, 2A & 2B MAY BE CONCURRENT. IF THE CONTRACTOR ELECTS TO CONSTRUCT THEM CONCURRENTLY, HE SHALL CLOSE 3/21 WITH THE AIRPORT'S CLOSURE MARKERS AND PLACE BARRICADES PER SHEET GC101. THE CONTRACTOR SHALL CLOSE 16/34 USING HIS OWN RUNWAY CLOSURE MARKERS.
 - BETWEEN THE HOURS OF 9:00 PM TO 8:00 AM, AT LEAST ONE PAVED RUNWAY MUST BE OPEN TO AIR TRAFFIC. CONTRACTOR SHALL REMOVE ALL BARRICADES & CLOSURE MARKERS BY 9:00 PM.

LEGEND

	FARM LIMITS		CONTRACTOR ACCESS
	PHASE 1		RUNWAY SAFETY AREA
	PHASE 2A		TAXIWAY OBJECT FREE AREA
	PHASE 2B		SWING GATE
			RUNWAY CLOSURE MARKER
			TAXIWAY CLOSURE MARKER

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REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

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FAA AIRSPACE CRITICAL POINT TABLE						
POINT NUMBER	LATITUDE	LONGITUDE	GROUND ELEVATION	OBSTRUCTION HEIGHT	ABOVE GROUND ELEVATION	DESC.
11	N40° 12' 06.10"	W87° 36' 02.07"	677'	25'	691'	BARRICADE
12	N40° 12' 01.03"	W87° 35' 44.72"	674'	25'	686'	BARRICADE
13	N40° 12' 00.35"	W87° 35' 53.10"	679'	25'	685'	BARRICADE
14	N40° 12' 05.13"	W87° 35' 42.62"	660'	25'	691'	BARRICADE
15	N40° 11' 59.05"	W87° 35' 52.65"	658'	25'	691'	BARRICADE
16	N40° 11' 52.80"	W87° 35' 57.37"	667'	25'	691'	BARRICADE
17	N40° 11' 46.65"	W87° 36' 01.57"	663'	25'	691'	BARRICADE
18	N40° 11' 36.75"	W87° 35' 55.58"	649'	25'	691'	BARRICADE
19	N40° 11' 32.97"	W87° 35' 45.03"	653'	25'	678'	BARRICADE
20	N40° 12' 03.80"	W87° 35' 39.52"	680'	25'	705'	BARRICADE
21	N40° 12' 01.73"	W87° 35' 41.05"	677'	25'	702'	BARRICADE

NOTES

- CONTRACTOR SHALL NOTIFY THE AIRPORT 48 HOURS PRIOR TO INITIATING THE PHASE.
- CONTRACTOR SHALL PLACE BARRICADES AND TAXIWAY CLOSURE MARKINGS AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO INITIATING ANY WORK IN THIS PHASE.
- CONSTRUCTION ACCESS WILL CROSS ACTIVE RUNWAY SPACE. THE CONTRACTOR SHALL YIELD THE RIGHT OF WAY TO ALL AIRCRAFT WHEN ACCESSING THE WORK AREA AND MONITOR RADIO.
- PHASES 1A & 1B MAY BE CONCURRENT. IF THE CONTRACTOR ELECTS TO CONSTRUCT 1A & 1B CONCURRENTLY, HE SHALL CLOSE 3/21 WITH THE AIRPORT'S CLOSURE MARKERS AND PLACE BARRICADES PER THIS SHEET.
- BETWEEN THE HOURS OF 9:00 PM TO 8:00 AM, AT LEAST ONE PAVED RUNWAY MUST BE OPEN TO AIR TRAFFIC.

LEGEND

- PHASE 1
- PHASE 2A
- PHASE 2B
- BEAM BARRICADES
- FAA AIRSPACE CRITICAL POINT
- CONTRACTOR ACCESS
- RUNWAY SAFETY AREA
- TAXIWAY OBJECT FREE AREA
- SWING GATE
- RUNWAY CLOSURE MARKER
- TAXIWAY CLOSURE MARKER

MARK | DATE | DESCRIPTION

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REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

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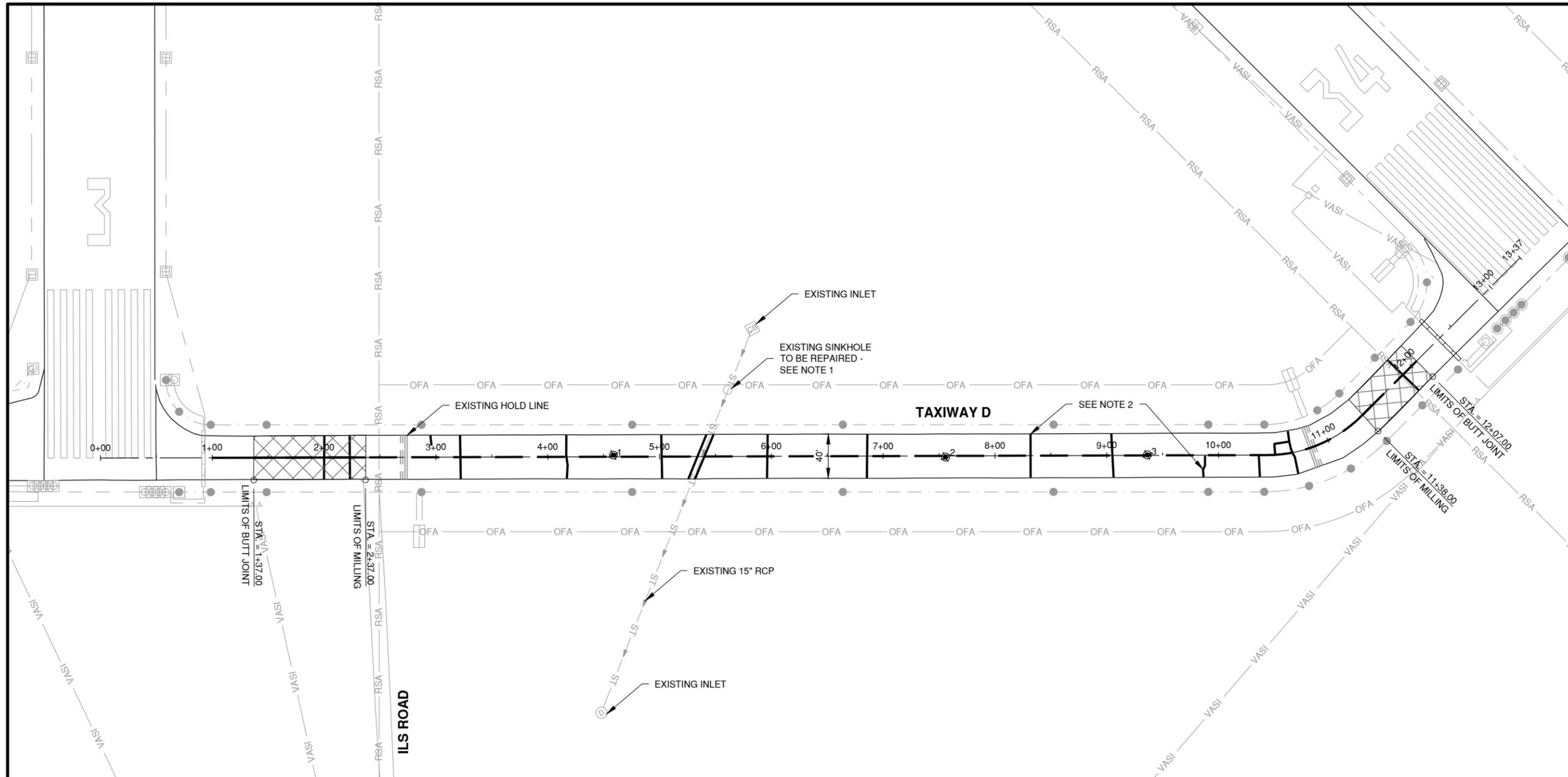
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**EXISTING
CONDITIONS &
MILLING PLAN**

CD101
SHEET 7 OF 16



EXISTING PAVEMENT CORE DATA

	NORTHING	EASTING	ELEV.	SUBGRADE CBR	SAND DEPTH	CR. STONE DEPTH	HMA DEPTH
CORE #1	1285146.067	1189636.003	651.32	N/A	N/A	15"	5.75"
CORE #2	1285001.659	1189895.606	652.02	11.5	N/A	14"	6"
CORE #3	1284916.520	1190054.736	652.98	N/A	4"	10"	6"

NOTES

- REFER TO SHEET CP501 FOR DETAILS ON FRACTURED PIPE REPAIR OR REMOVAL & REPLACEMENT.
- REFER TO CRACK REPAIR DETAIL ON SHEET CP501.
- A DISCRETIONARY QUANTITY OF 90 SY OF CRACK CONTROL FABRIC MAY BE USED AT THE R.E.'S DIRECTION FOLLOWING THE MILLING OPERATION.



LEGEND

- EXISTING CRACKS TO BE REPAIRED - AR201660
- EXISTING STAKE- MOUNTED LIGHT
- EXISTING BASE-MOUNTED LIGHT
- EXISTING SIGN
- EXISTING EDGE OF PAVEMENT
- EXISTING AIRFIELD CABLING
- PROPOSED BUTT JOINT
- PAVEMENT CORE
- VASI - VISUAL APPROACH SLOPE INDICATOR CONTROL CABLE
- ST - EXISTING DRAINAGE PIPE
- PROPOSED CRACK CONTROL FABRIC ON CENTERLINE - AR201670

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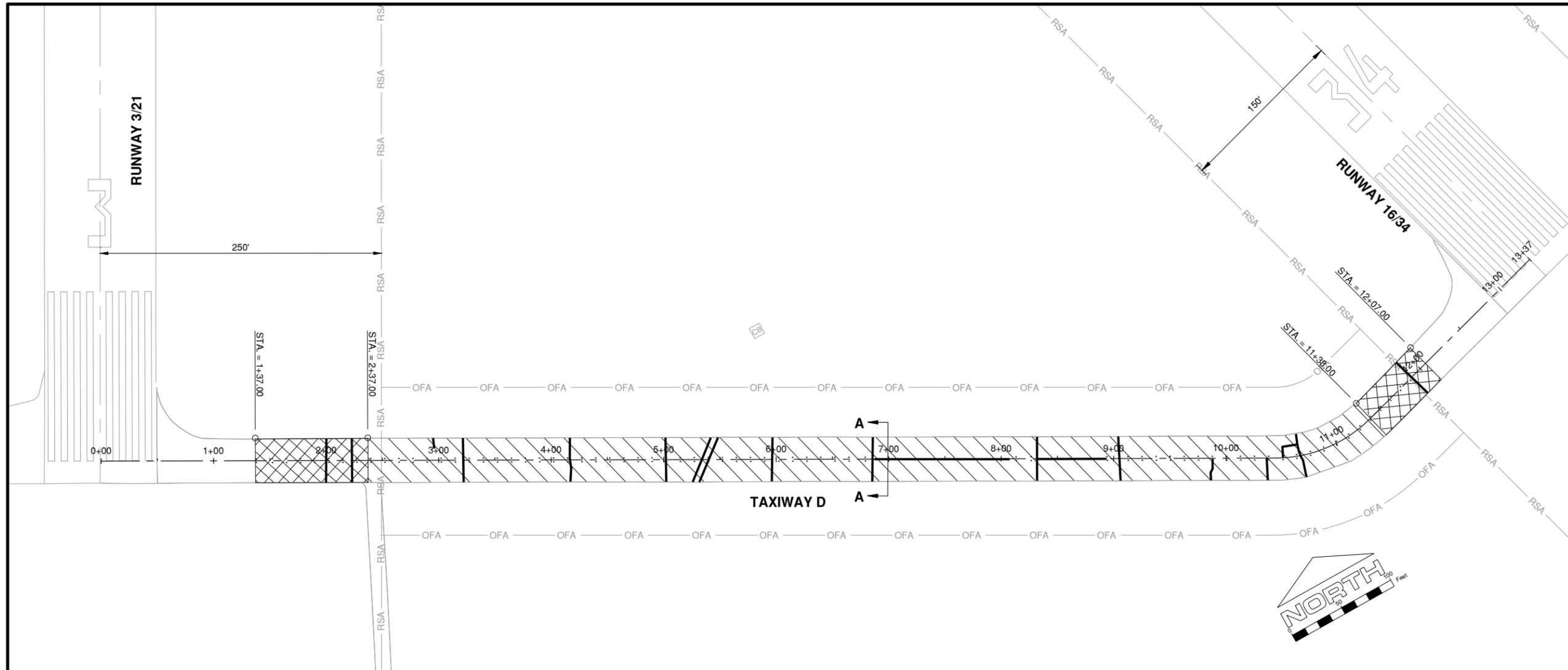
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REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

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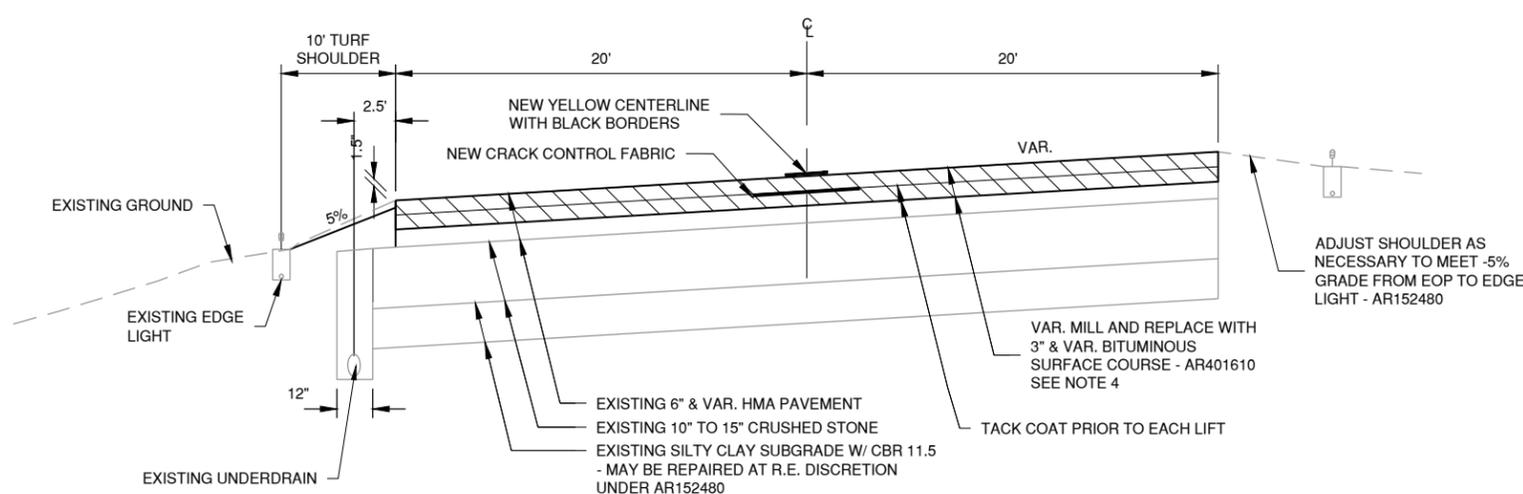


NOTES

- REFER TO STAKING PLAN FOR MILLING DEPTHS.
- BITUMINOUS CRACKS ARE TO BE WIDENED AND FILLED WITH ASPHALT PRIOR TO PAVING FIRST LIFT. CRACK REPAIR FABRIC SHALL NOT BE APPLIED UNTIL AFTER THE FIRST LIFT OF BITUMINOUS IS PLACED. REFER TO SHEET CP501 FOR CRACK REPAIR DETAIL.
- REFER TO PLAN & PROFILE SHEET FOR ALIGNMENT DATA TABLE.
- PAVEMENT SHALL BE CONSTRUCTED IN TWO LIFTS. THE FIRST LIFT SHALL BE THE MINIMUM THICKNESS ALLOWED SO THAT THE CRACK CONTROL FABRIC CAN HAVE MORE COVER THICKNESS ON THE FINAL LIFT.

LEGEND

- REPAIR CRACK - AR201660
- PROPOSED 3" & VAR. DEPTH BITUMINOUS SURFACE COURSE - AR401610
- PROPOSED 3" NOMINAL DEPTH BITUMINOUS SURFACE COURSE - AR401610
- CRACK CONTROL FABRIC ON CENTERLINE - AR201670



A TYPICAL SECTION **A**
N.T.S.

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SHEET TITLE
PROPOSED IMPROVEMENTS & TYPICAL SECTION
CP101
SHEET 8 OF 16

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REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

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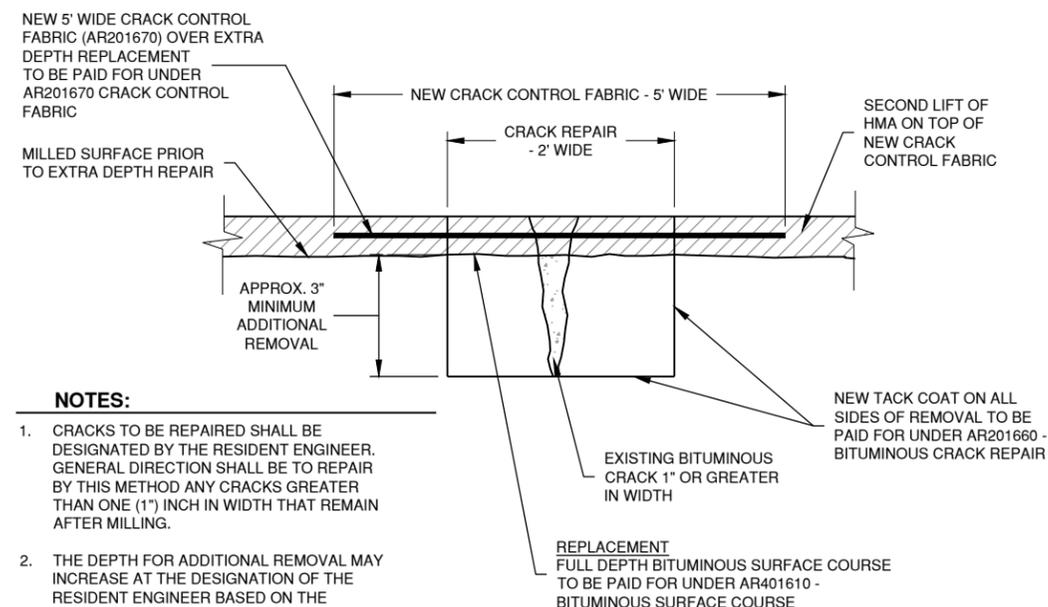
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SHEET TITLE

**SINKHOLE & PAVING
DETAILS**

SHEET **9** OF **16**

CP501

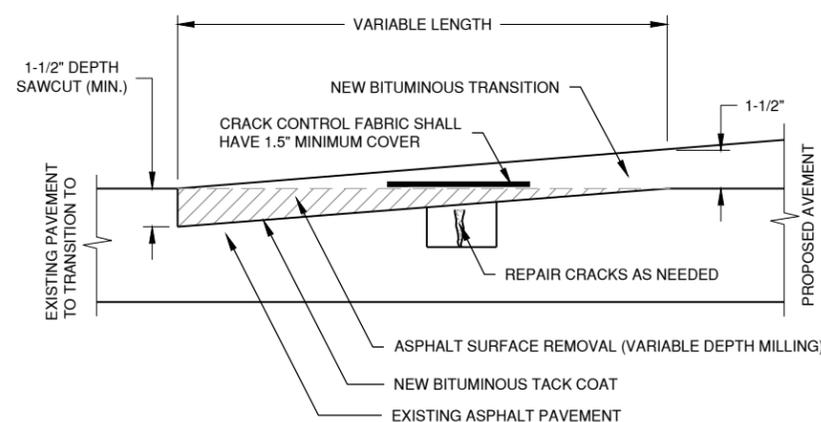


NOTES:

1. CRACKS TO BE REPAIRED SHALL BE DESIGNATED BY THE RESIDENT ENGINEER. GENERAL DIRECTION SHALL BE TO REPAIR BY THIS METHOD ANY CRACKS GREATER THAN ONE (1") INCH IN WIDTH THAT REMAIN AFTER MILLING.
2. THE DEPTH FOR ADDITIONAL REMOVAL MAY INCREASE AT THE DESIGNATION OF THE RESIDENT ENGINEER BASED ON THE SEVERITY OF THE CRACK. MINIMUM DEPTH OF THIS ADDITIONAL REMOVAL SHALL BE THREE (3") INCHES.
3. THE CONTRACTOR SHALL REMOVE ALL LOOSE DEBRIS FROM THE EXTRA DEPTH REMOVAL TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR TO APPLYING TACK COAT.
4. PARALLEL CRACKS LOCATED LESS THAN THREE (3) FEET APART SHALL BE CONSIDERED ONE (1) CRACK FOR MEASUREMENT.

BITUMINOUS CRACK REPAIR (AR201660)

N.T.S.

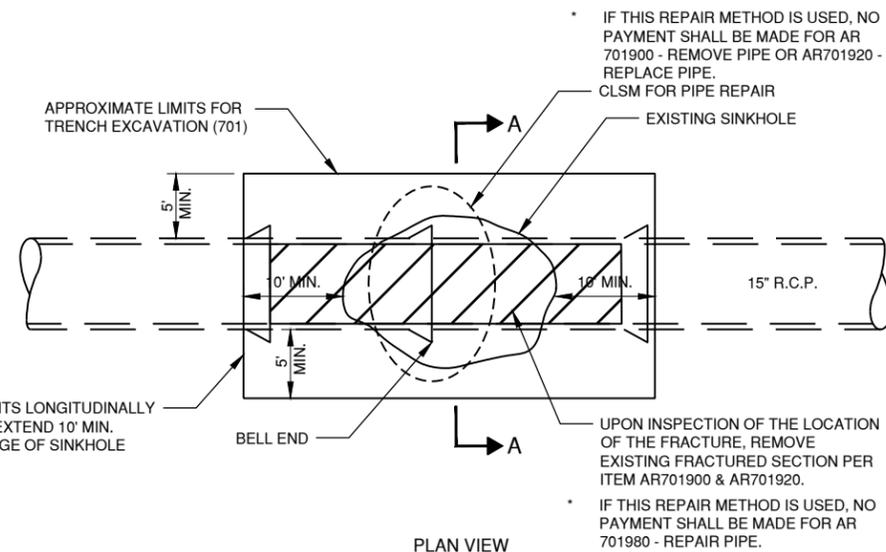


BUTT JOINT DETAIL

N.T.S.

NOTES:

1. VARIABLE DEPTH MILLING FOR BUTT JOINT SHALL BE PAID FOR UNDER AR401655. TACK COAT, BITUMINOUS SURFACE COURSE, AND CRACK REPAIR SHALL BE PAID FOR SEPARATELY UNDER THEIR RESPECTIVE PAY ITEMS.



PLAN VIEW

* IF THIS REPAIR METHOD IS USED, NO PAYMENT SHALL BE MADE FOR AR 701900 - REMOVE PIPE OR AR701920 - REPLACE PIPE. CLSM FOR PIPE REPAIR

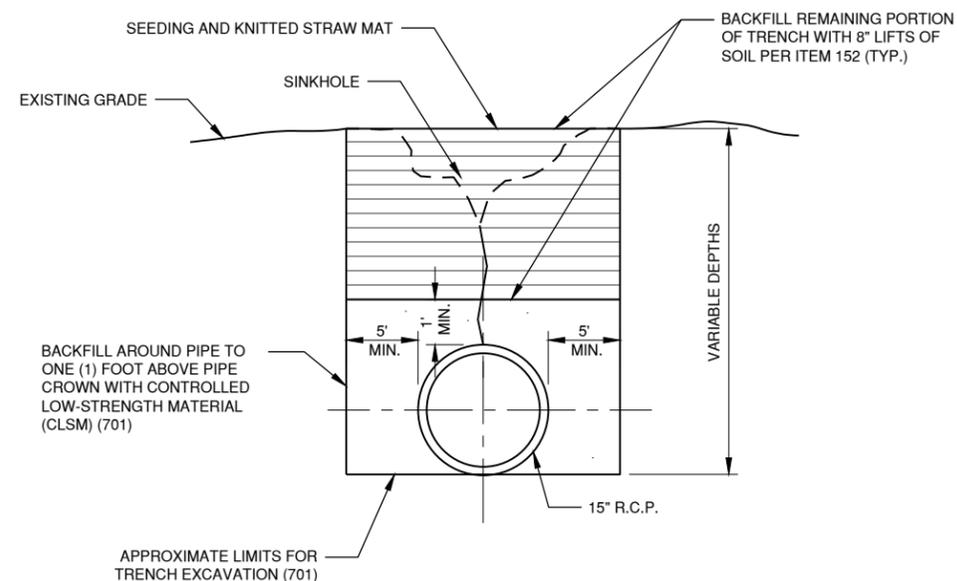
* IF THIS REPAIR METHOD IS USED, NO PAYMENT SHALL BE MADE FOR AR 701980 - REPAIR PIPE.

NOTES:
AR701980 - REPAIR PIPE

1. PAY ITEM FOR PIPE REPAIR SHALL INCLUDE TRENCH EXCAVATION, CLSM BACKFILL/REPAIR AROUND BROKEN SECTION, COMPACTED BACKFILL, & TOPSOIL, SEEDING AND KNITTED STRAW MAT SHALL BE PAID UNDER THEIR OWN SEPARATE PAY ITEMS.
2. CONTRACTOR SHALL VERIFY THAT NO ADDITIONAL DEFECTS EXIST BETWEEN PIPE BELLS.
3. IF PIPE HAS BEEN DAMAGED, REMOVAL & REPLACEMENT OF THAT SECTION MAY BE REQUIRED UNDER ITEMS AR701900 & AR701920.

NOTES:
AR701900 - REMOVE PIPE & AR701920 - REPLACE PIPE

1. PAY ITEM FOR PIPE REMOVAL & REPLACEMENT SHALL INCLUDE TRENCH EXCAVATION, CLSM BACKFILL/REPAIR AROUND BROKEN SECTION, COMPACTED BACKFILL, & TOPSOIL. SEEDING AND KNITTED STRAW MAT SHALL BE PAID UNDER THEIR OWN SEPARATE PAY ITEMS.



SECTION A-A

SINKHOLE REPAIR DETAIL

N.T.S.

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REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

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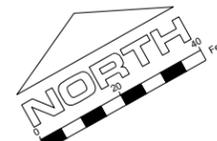
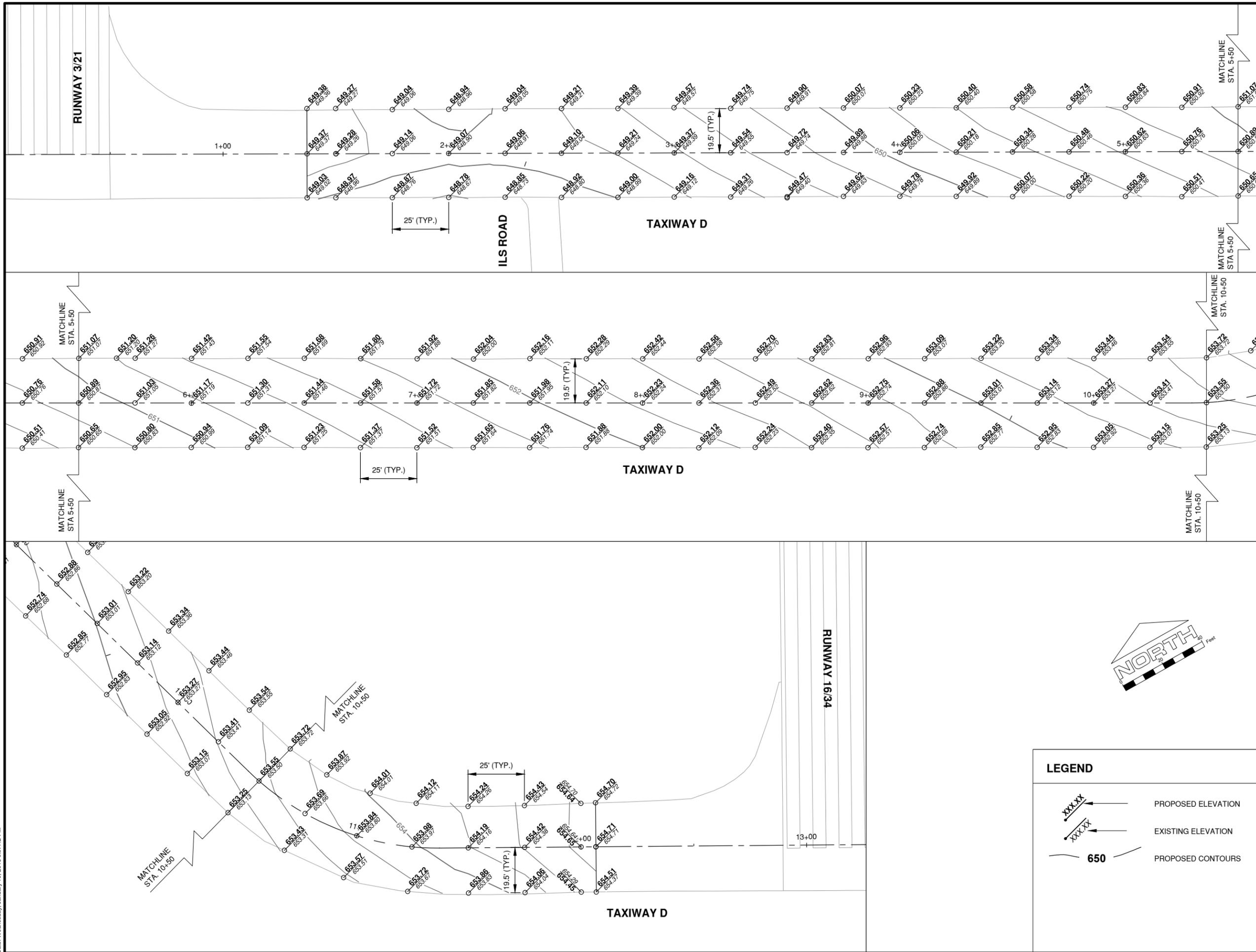


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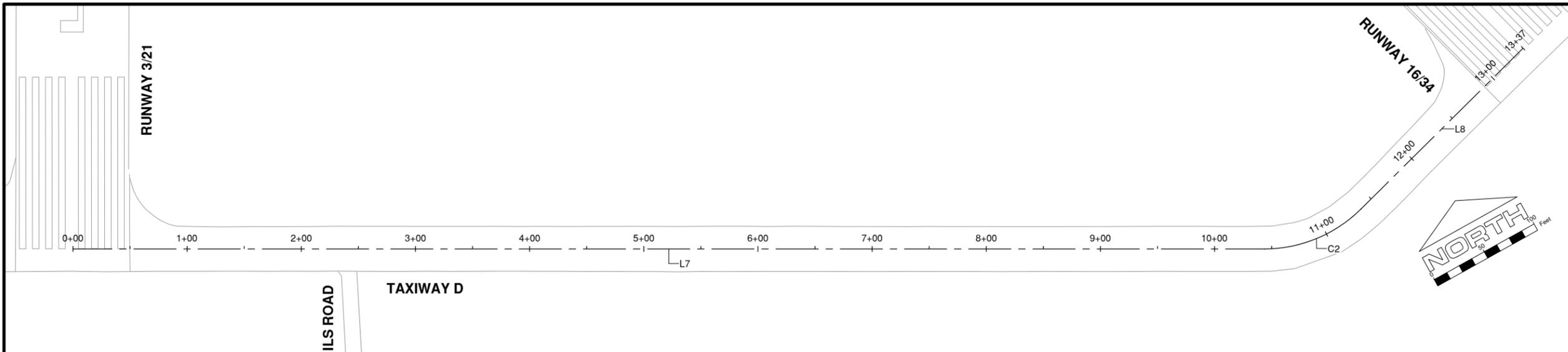
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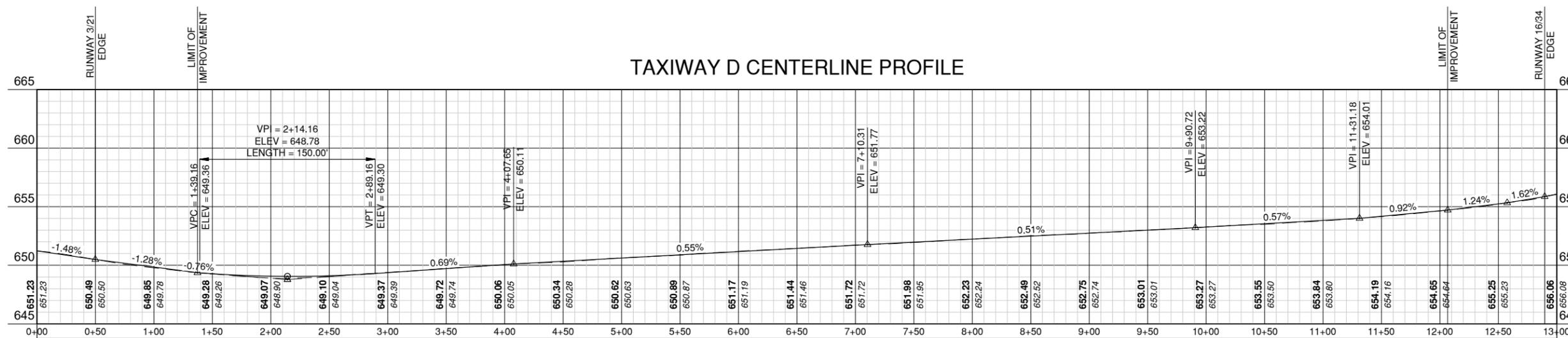
LEGEND

- PROPOSED ELEVATION
- EXISTING ELEVATION
- PROPOSED CONTOURS

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TAXIWAY D CENTERLINE PROFILE



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REHABILITATE TAXIWAY D BETWEEN RUNWAY 3 AND 34

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ALIGNMENT DATA					
ALIGNMENT SEGMENT	ALIGNMENT	BEGIN STATION/ PI STATION	NORTHING/ EASTING	END STATION/ CURVE RADIUS	NORTHING/ EASTING
L7	1604201-TXY D - CL ALIGNMENT	STA. 0+00.00	N:1285364.3043 E:1189232.5318	STA. 10+43.59	N: 1284864.8060 E: 1190148.8145
C2	1604201-TXY D - CL ALIGNMENT	CENTER STA. 10+92.93	PI N:1284841.1870 PI E:1190192.1414	R = 119.983	CENTER N: 1284970.1531 E: 1190206.2429
L8	1604201-TXY D - CL ALIGNMENT	STA. 11+37.22	N:1284854.8849 E:1190239.5486	STA. 13+37.25	N: 1284910.4004 E: 1190431.7234

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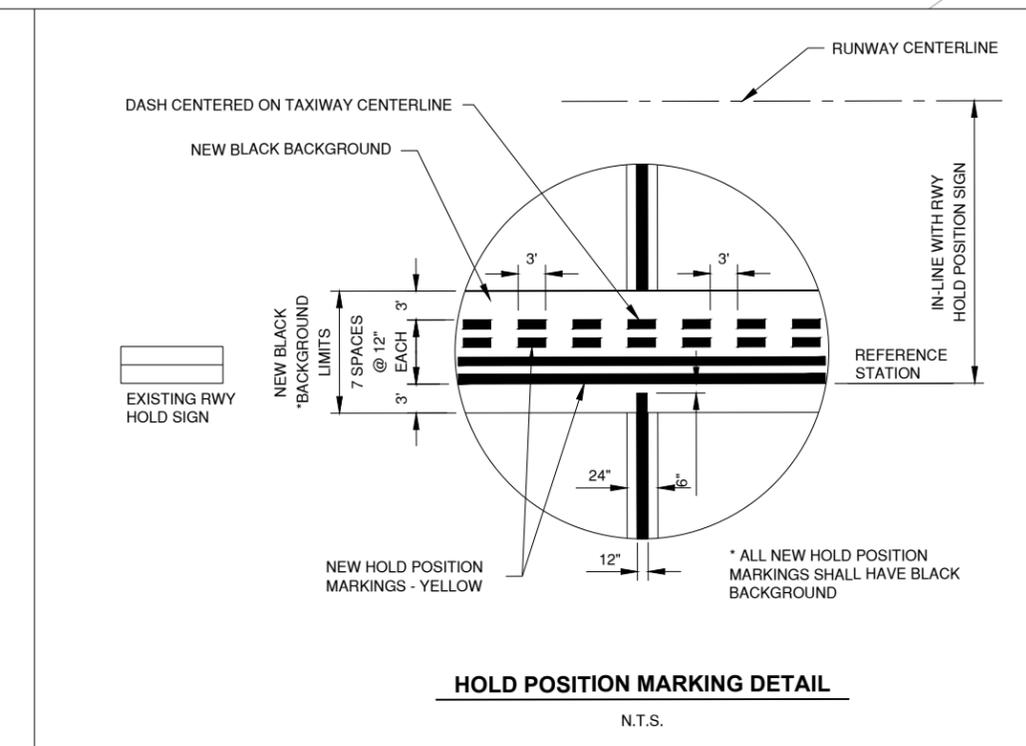
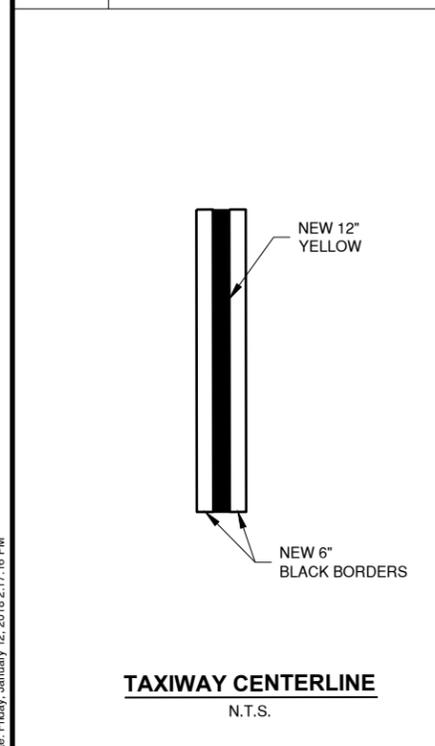
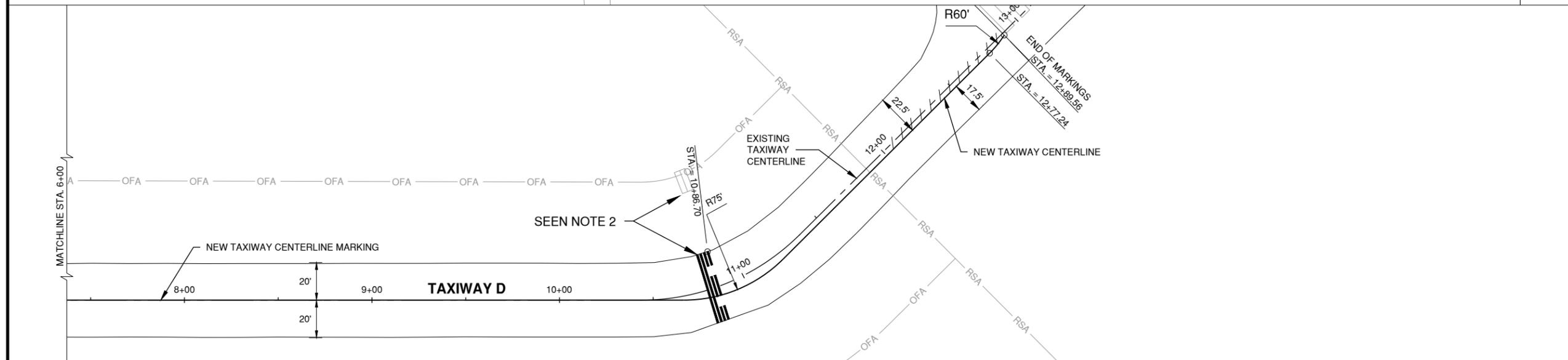
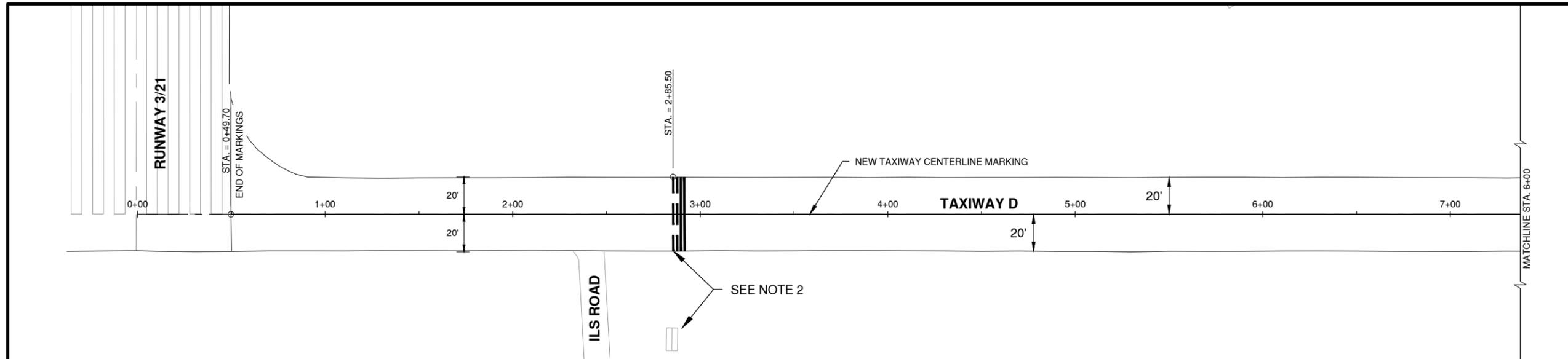
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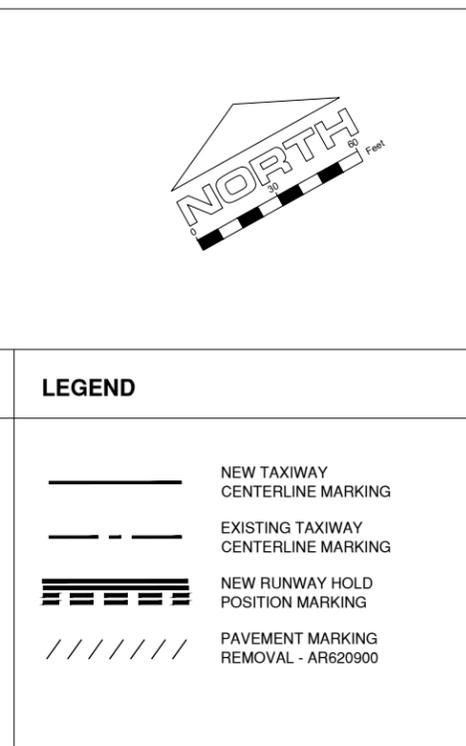
SHEET TITLE

MARKING PLAN &
DETAILS

CM101
SHEET 12 OF 16



- NOTES**
- ALL NEW AIRFIELD MARKING SHALL HAVE REFLECTIVE BEADS & 6" BLACK BORDER. BLACK BORDER DOES NOT RECEIVE REFLECTIVE BEADS.
 - STATIONING SHOWN FOR HOLD LINE IS FOR REFERENCE ONLY. ACTUAL MARKING SHOULD BE APPLIED PERPENDICULAR TO THE EXISTING HOLD SIGN PER THE DETAIL.



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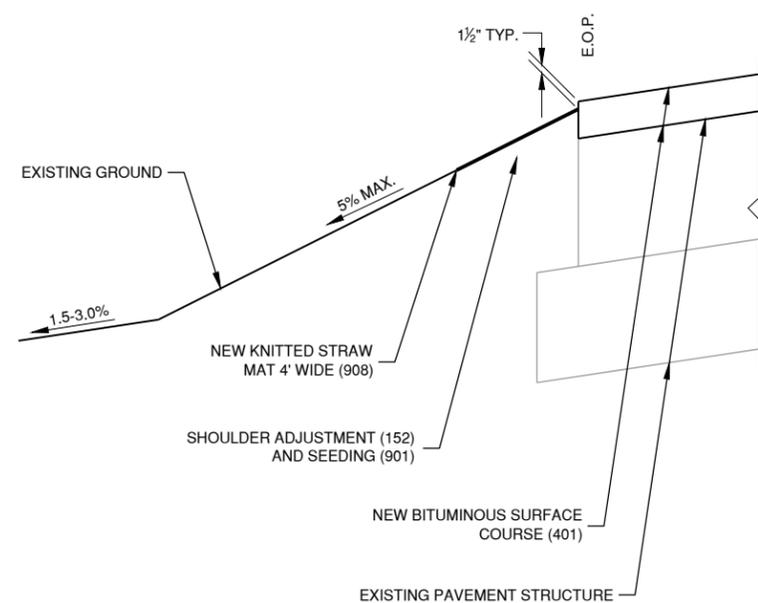
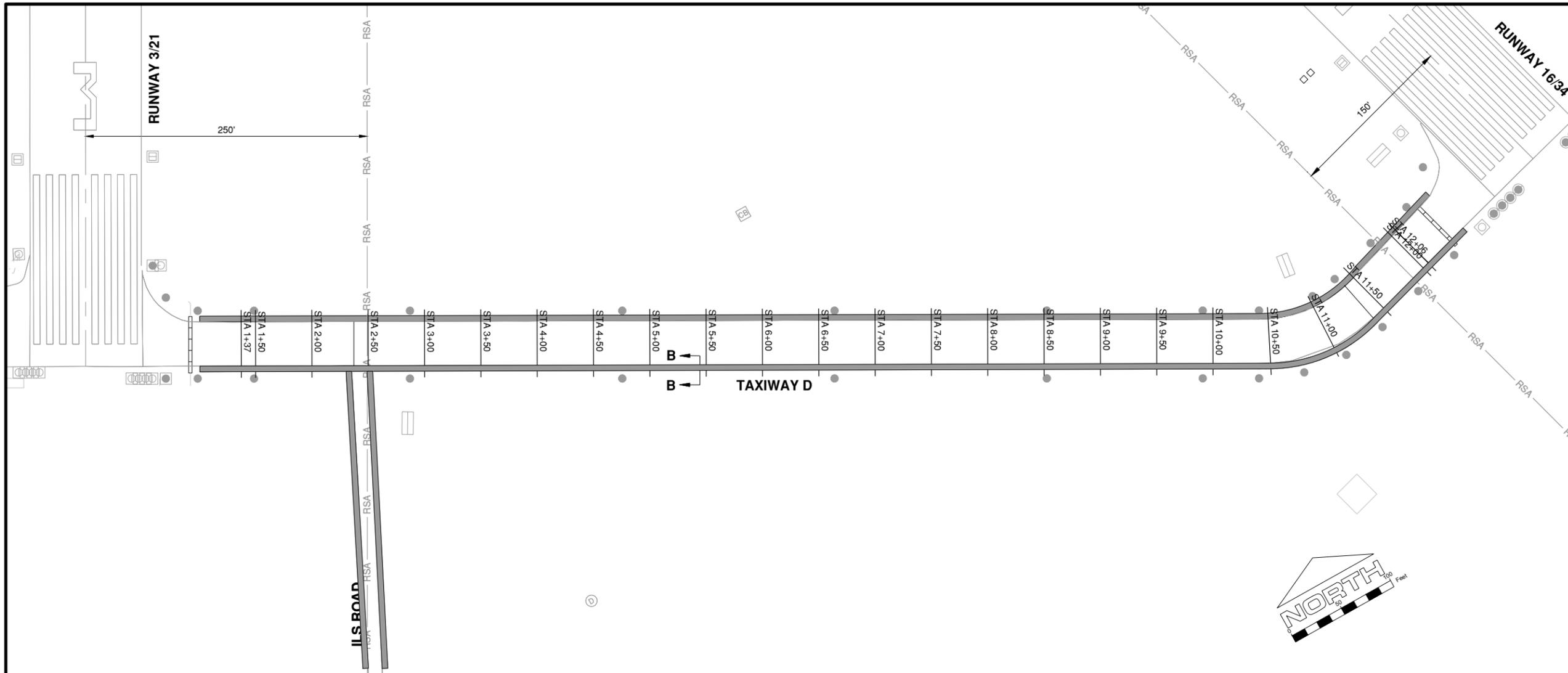
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**SHOULDER
ADJUSTMENT PLAN &
DETAIL**

LE101
SHEET 13 OF 16



SHOULDER DETAIL "B"
N.T.S.

NOTES

- SHOULDER ADJUSTMENT SHALL BE AS SHOWN IN SHOULDER DETAIL "B"
- A NOMINAL 5' WIDTH WILL BE REQUIRED TO MEET THE 5% SLOPE.
- ACTUAL LOCATION MAY VARY AT THE R.E.'S DISCRETION.

LEGEND

- 5' SHOULDER ADJUSTMENT - AR152480
- 4' KNITTED STRAW MAT - AR908525
- SEEDING - AR901510

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JANUARY 12, 2018

REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

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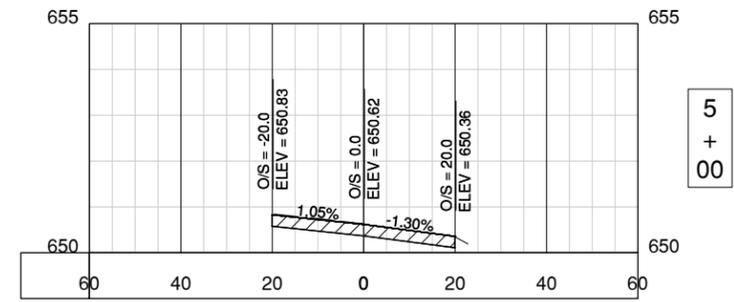


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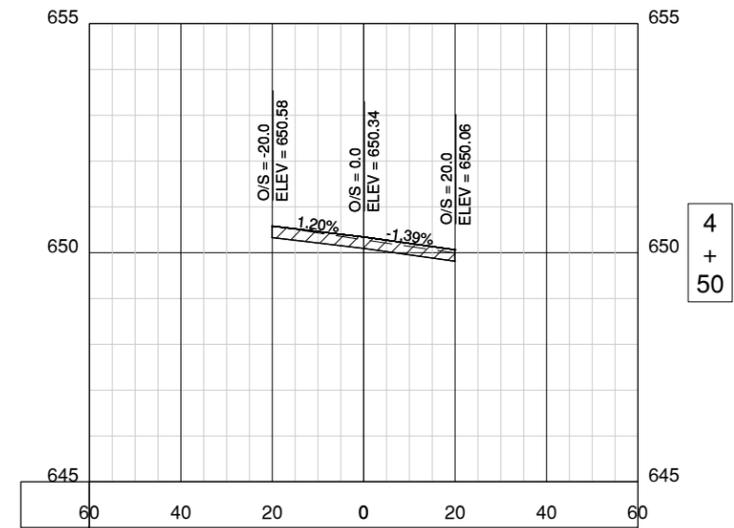
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DRAWN BY:	DPA
CHECKED BY:	CBG
APPROVED BY:	MJD
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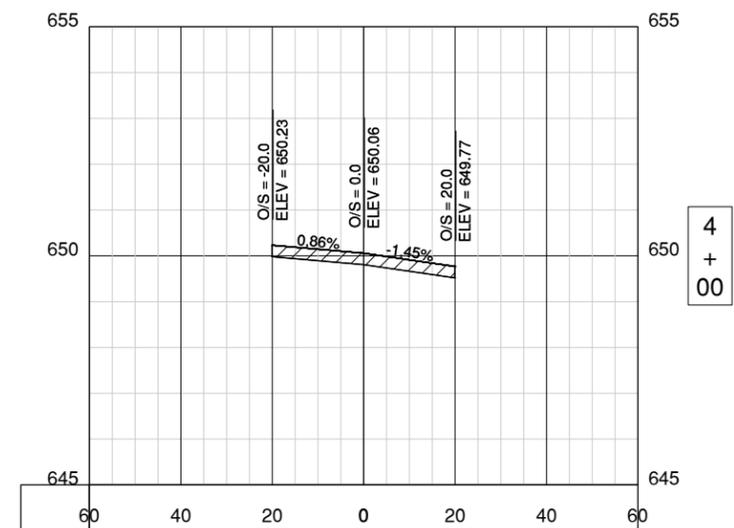
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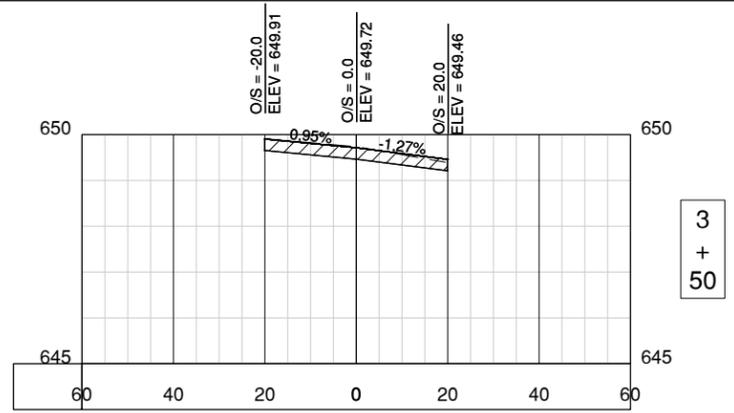
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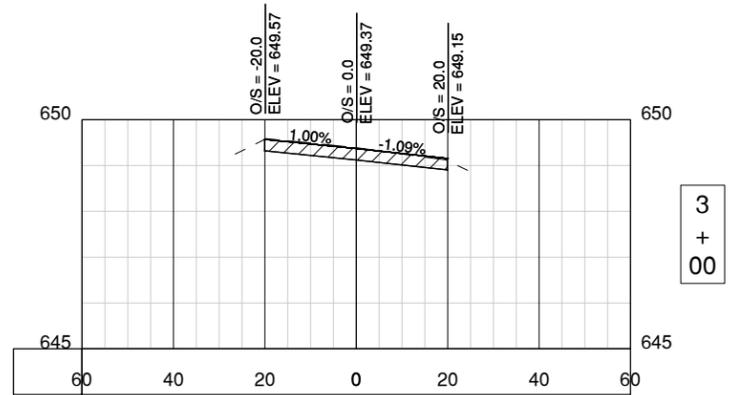
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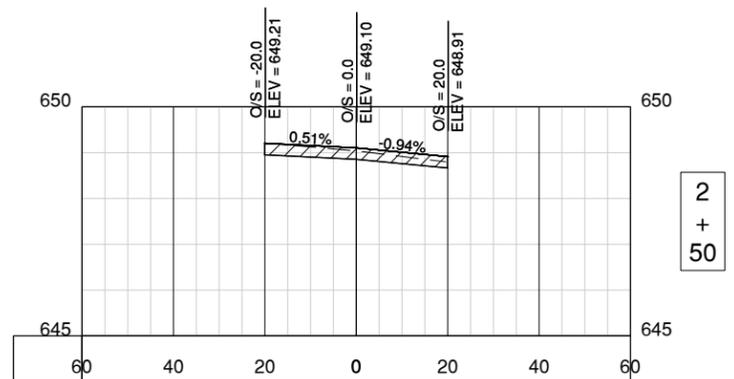
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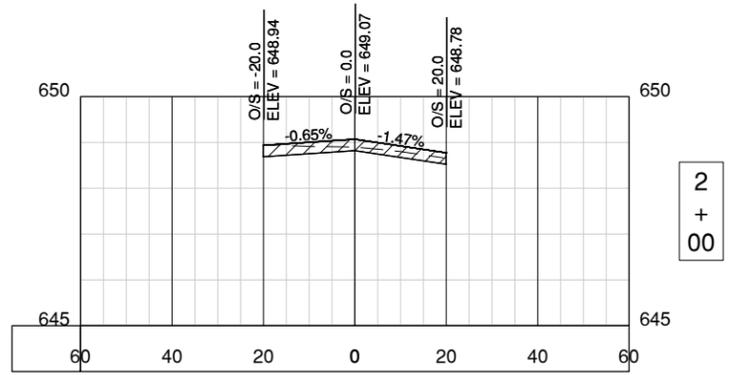
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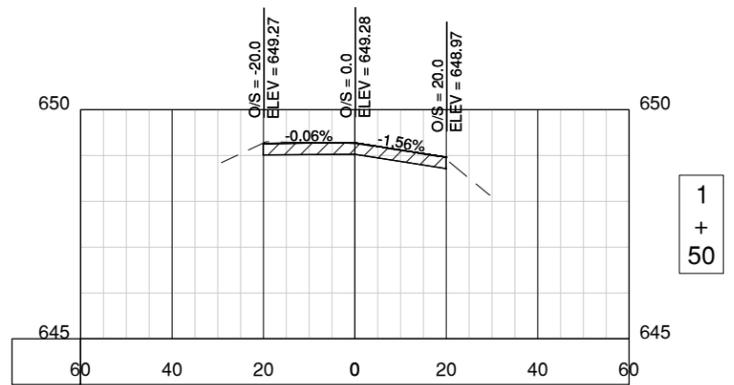
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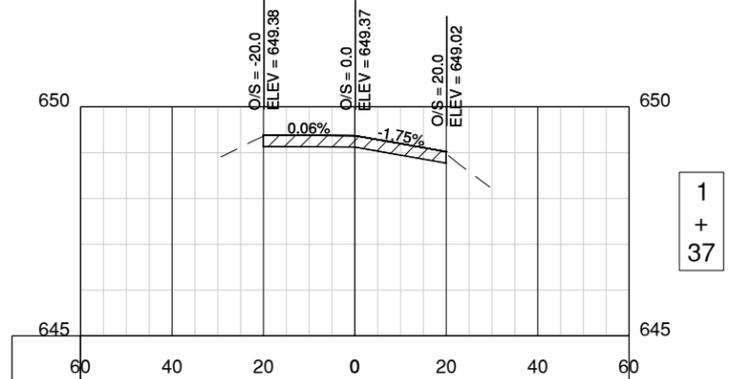
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REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

OWNER



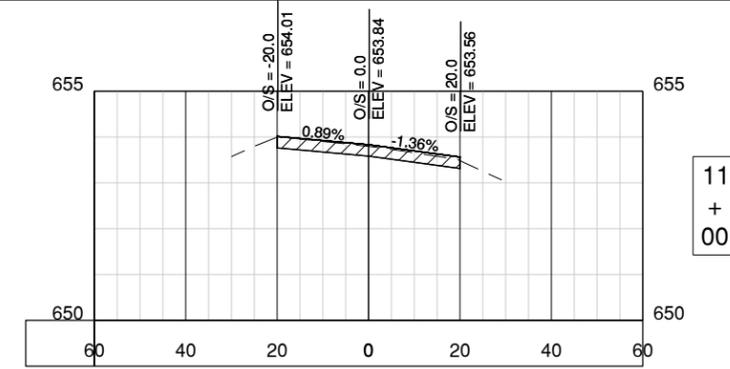
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DANVILLE, ILLINOIS

MARK | DATE | DESCRIPTION

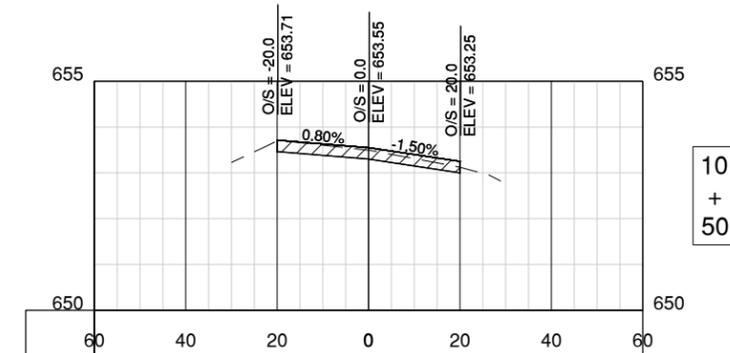
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IL PROJ. NO.	DNV-4588
CMT PROJECT NO.	16042-01-00
CAD DWG FILE:	16042-01-00-C-SPCR.DWG
DESIGNED BY:	EMH
DRAWN BY:	DPA
CHECKED BY:	CBG
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SHEET TITLE
CROSS SECTIONS
SHEET 2

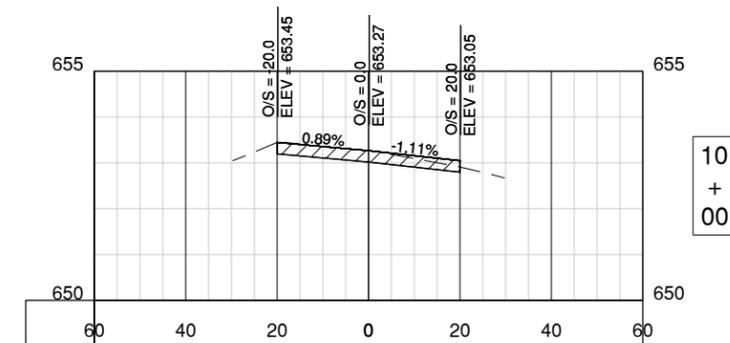
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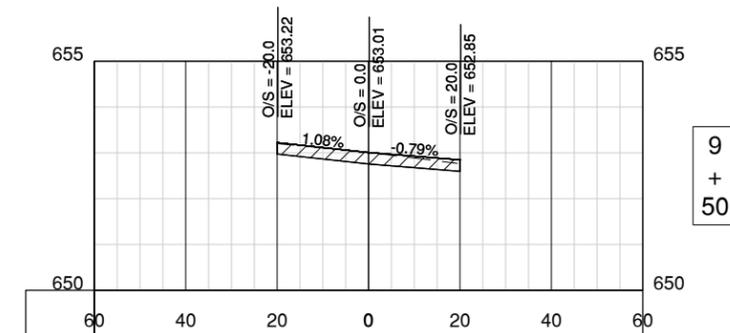
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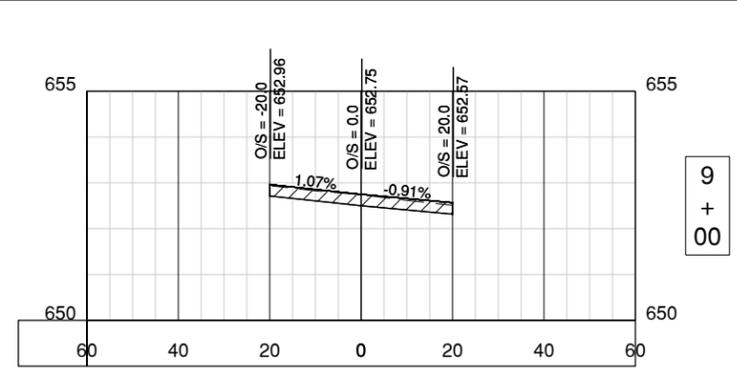
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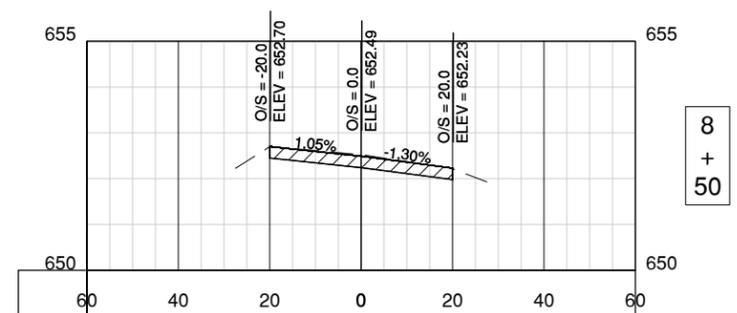
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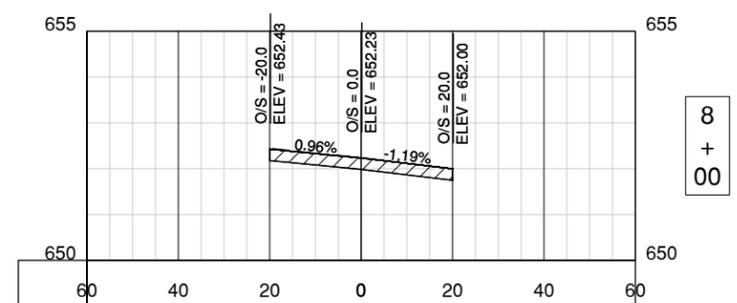
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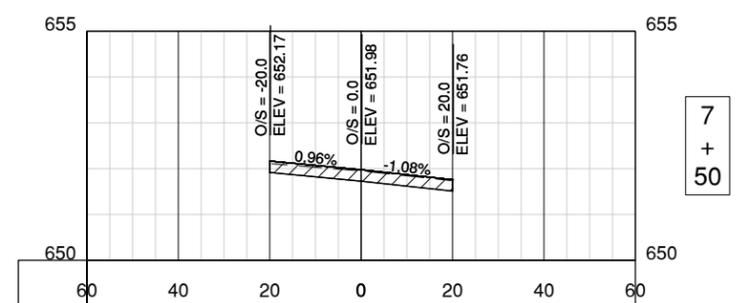
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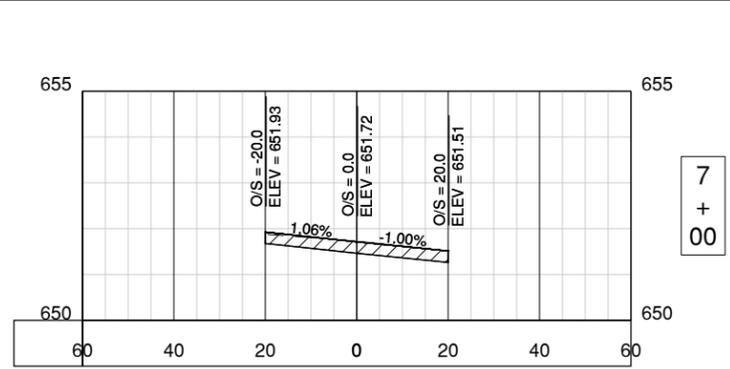
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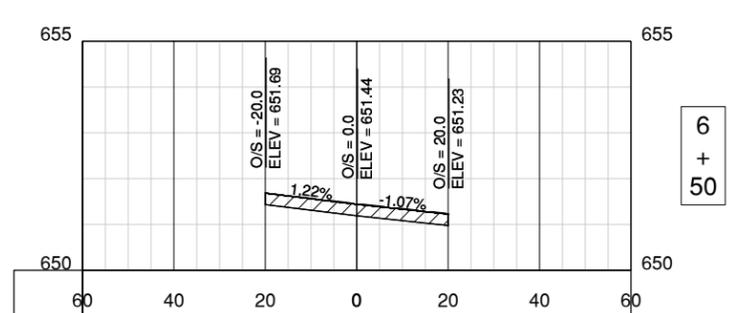
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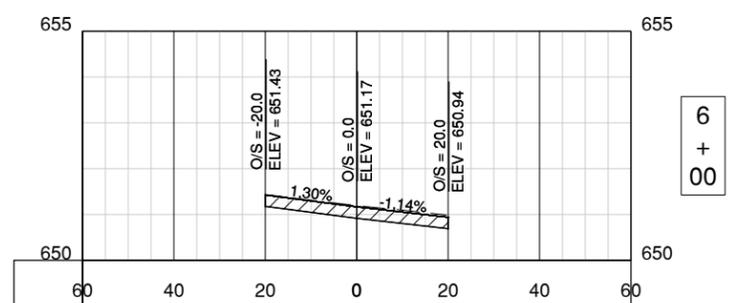
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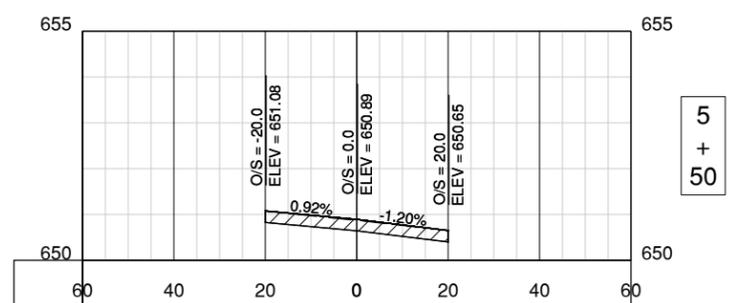
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JANUARY 12, 2018

REHABILITATE TAXIWAY D
BETWEEN RUNWAY 3 AND 34

OWNER



VERMILION REGIONAL AIRPORT
VERMILION REGIONAL AIRPORT
AUTHORITY
DANVILLE, ILLINOIS

MARK	DATE	DESCRIPTION

FED PROJ. NO. 3-17-SBGP-139

IL PROJ. NO. DNV-4588

CMT PROJECT NO: 16042-01-00

CAD DWG FILE: 16042-01-00-C-SPCR.DWG

DESIGNED BY: EMH

DRAWN BY: DPA

CHECKED BY: CBG

APPROVED BY: MJD

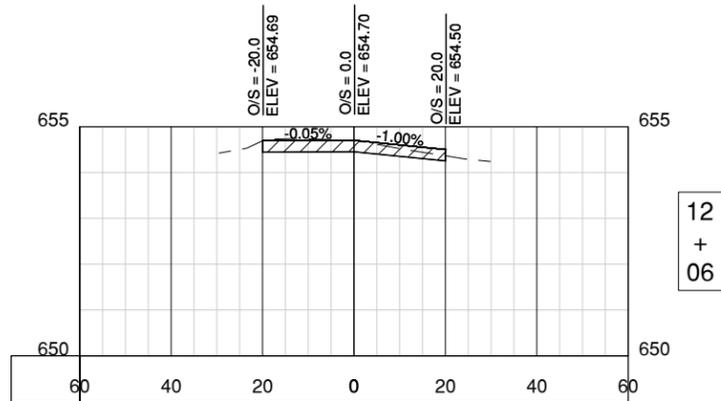
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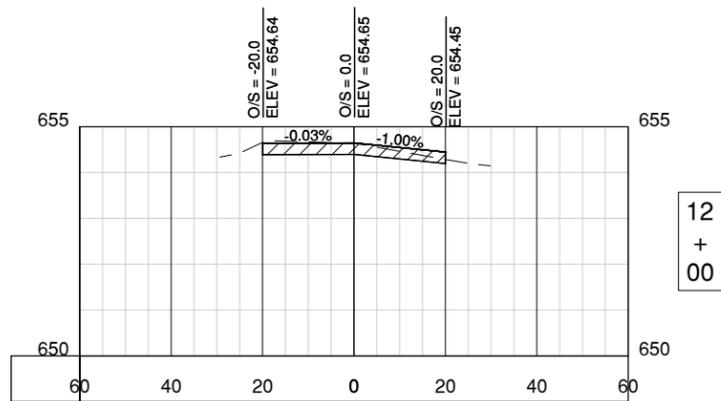
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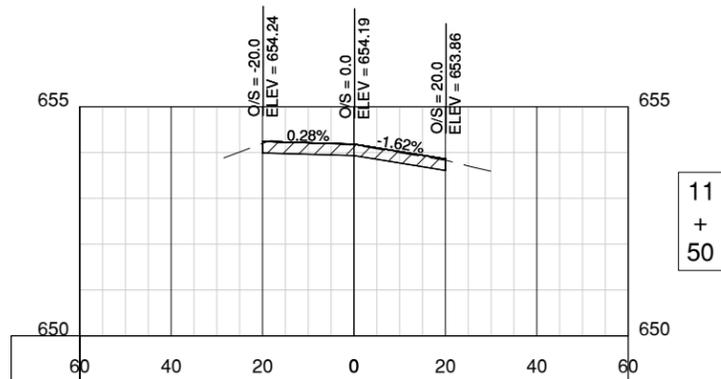
SHEET 16 OF 16



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