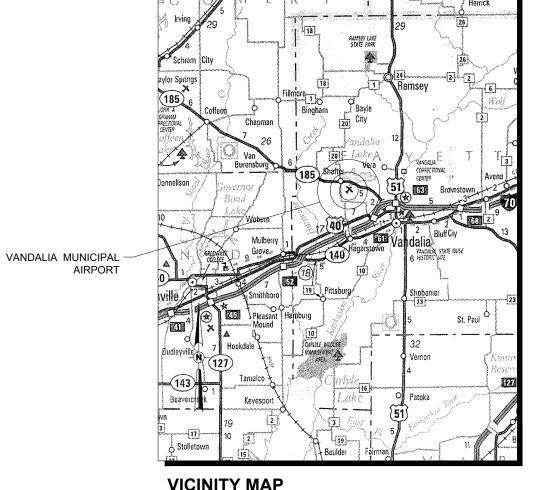
# **CONSTRUCTION PLANS**

# **CONSTRUCT T-HANGAR ACCESS TAXIWAYS**

VANDALIA PARK DISTRICT
VANDALIA MUNICIPAL AIRPORT (VLA)
VANDALIA, FAYETTE COUNTY, ILLINOIS

**IL. PROJECT NO. VLA-4628** 



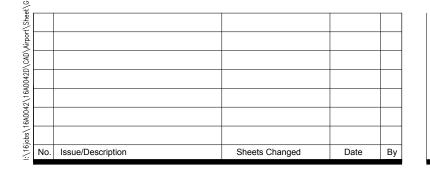
VICINITY MAP



**LOCATION MAP** 

NOTICE TO CONTRACTORS AND BIDDERS

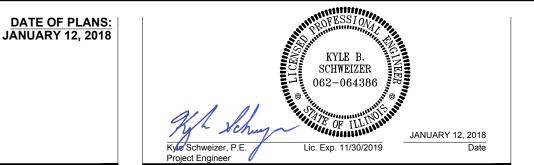
THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.



PREPARED BY:

**HANSON** 

HANSON PROFESSIONAL SERVICES INC. 1525 South Sixth Street Springfield, Illinois 62703-2886 Telephone: 217.788.2450 Fax: 217.788.2503



VANDALIA PARK DISTRICT
900 North Locust Street
Vandalia, Illinois 62471

Chad Feldpouch
Airport Manager

JANUARY 12, 2018
Date

SUMMARY OF QUANTITIES - ALTERNATE A						
ITEM NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	AS-BUILT QUANTITY		
AS401613	BIT. SURF. CSEMETHOD I, SUPERPAVE	TON	530			
AS403613	BIT. BASE CSEMETHOD I, SUPERPAVE	TON	630			
AS602510	BITUMINOUS PRIME COAT	GAL.	1,100			
AS603510	BITUMINOUS TACK COAT	GAL.	550			

TOTAL

QUANTITY

1

1 384

3,600

2

0.6

18

2,900

1,350

3.600

1,300

1.440

1.440

34

300

1

2

5

2

0.6

8

UNIT

L.S.

L.S

L.S.

CY

S.Y

FACH

ACRE

S.Y

S.Y

TON

S.Y

S.Y.

S.Y.

S.F

S.F

LE

EACH

EACH

**EACH** 

EACH

EACH

EACH

ACRE

AS-BUILT

QUANTITY

	SUMMARY OF QUANTITIES - ALTERNATE B						
ITEM NO.	ITEM NO. DESCRIPTION UNIT TOTAL AS-BUILT QUANTITY						
AT401613	BIT. SURF. CSEMETHOD I, SUPERPAVE	TON	120				
AT501501	TEST BATCH PCC CONCRETE	EACH	1				
AT501505	5" PCC PAVEMENT	S.Y.	3,525				
AT603510	BITUMINOUS TACK COAT	GAL.	200				

INDEX TO SHEETS			
SHEET NO.	SHEET TITLE		
1	COVER SHEET		
2	SUMMARY OF QUANTITIES, INDEX TO SHEETS, GENERAL NOTES		
3	EXISTING SITE AND SAFETY PLAN		
4	TYPICAL SECTION AND DETAILS		
5	PROPOSED SITE PLAN		
6	DRAINAGE AND STORMWATER POLLUTION PREVENTION PLAN		
7	DRAINAGE DETAILS		
8	STORM WATER POLLUTION PREVENTION PLAN DETAILS		
9	STAKING PLAN		
10	JOINTING PLAN (ALT. B ONLY)		
11	JOINTING DETAILS (ALT. B ONLY)		
12	CROSS SECTIONS SHEET 1		
13	CROSS SECTIONS SHEET 2		

#### UTILITY NOTE

THE LOCATION, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS ARE NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED. THAT THE LOCATIONS. SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER, CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED. WITH AND/OR LOCATED BY THE FAA. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND UTILITIES.

J.U.L.I.E.	INFORMATION
COUNTY	FAYETTE
CITY	VANDALIA
TOWNSHIP	BEAR GROVE
SECTION NO.	2
ADDRESS	VANDALIA MUNICIPAL AIRPORT
	457 E. 1700 AVE.
	VANDALIA ILLINOIS 62471

#### GENERAL NOTES

- PROJECT DESCRIPTION: THIS PROJECT SHALL CONSIST OF CONSTRUCTING PAVED ACCESS TAXIWAYS TO A RECENTLY CONSTRUCTED T-HANGAR BUILDING. ASSOCIATED WORK ITEMS INCLUDE GRADING, DRAINAGE, AGGREGATE BASE COURSE, CONCRETE OR ASPHALT PAVING, PAVEMENT MARKING, TURFING AND EROSION
- THE RULES, REGULATIONS, AND SPECIFICATIONS ENUMERATED HEREIN SHALL BE CONSIDERED AS MINIMUM REQUIREMENTS. THEY SHALL NOT PROHIBIT THE CONTRACTOR FROM FURNISHING AND INSTALLING HIGHER GRADES OF MATERIAL THAN ARE SPECIFIED HEREIN IF APPROVED BY THE ENGINEER
- ACCESS TO THE SITE SHALL BE RESTRICTED EXCLUSIVELY TO THE DESIGNATED CONSTRUCTION ENTRANCE, STAGING AREA, AND HAUL ROUTE. NO EQUIPMENT OR PERSONNEL SHALL BE PERMITTED OUTSIDE THE GENERAL PROJECT AREA
- 4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT AND KEEP CLEAN OF DEBRIS ALL EXISTING AIRFIELD AND ROADWAY PAVEMENTS AT ALL TIMES. ANY DAMAGE TO EXISTING ELECTRICAL, DRAINAGE, AND PAVEMENT STRUCTURES SHALL BE IMMEDIATELY REPAIRED AT NO ADDITIONAL COST TO
- 5. CONTRACTOR IS REQUIRED TO PROVIDE THEIR OWN RESTROOM FACILITIES.
- 6. THE OWNER SHALL HAVE THE RIGHT OF FIRST REFUSAL FOR ALL SALVAGEABLE MATERIAL REMOVED ON
- 7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING PERMITS FOR HAULING ON PUBLIC ROADS, AS APPLICABLE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CORRECTING ANY DAMAGES TO ANY PAVEMENTS (PUBLIC OR PRIVATE) CAUSED BY HIS/HER CONSTRUCTION EQUIPMENT OR PERSONNEL
- THE CONTRACTOR SHALL PROVIDE ONE SET OF PRELIMINARY REDLINED RECORD DRAWINGS TO THE RESIDENT ENGINEER AT THE COMPLETION OF THE PROJECT FOR INCORPORATION INTO THE OFFICIAL RECORD DRAWINGS HE WILL PREPARE.
- 9. CONTRACTOR SHALL NOTE THAT SOME AREAS WITHIN THE AIRPORT PROPERTY LINE AND OUTSIDE THE CONSTRUCTION LIMITS MAY BE USED FOR AGRICULTURAL PURPOSES. THE CONSTRUCTION LIMITS SHALL BE RESTRICTED TO AREAS THAT ARE ABSOLUTELY NECESSARY TO DISTURB TO COMPLETE THE REQUIRED WORK ITEMS. LIMITS SHALL BE COORDINATED WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING ANY WORK. ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITY WHICH HAVE BEEN PREVIOUSLY FARMED OR ARE DESIGNATED TO BE FARMED AFTER THE PROJECT COMPLETION SHALL BE CHISEL PLOWED OR OTHERWISE SCARIFIED TO RETURN THE AREA TO A REASONABLE TILLABLE CONDITION (IF SO PERMITTED BY THE AIRPORT MANAGER) AT THE CONTRACTOR'S EXPENSE
- 10. APPROXIMATE LOCATIONS OF UNDERGROUND UTILITIES ARE SHOWN THROUGHOUT THESE PLANS. THE CONTRACTOR SHALL DETERMINE EXACT LOCATIONS AND PROTECT THESE UTILITIES DURING CONSTRUCTION. ANY UTILITIES DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL COORDINATE WITH THE PROPER AUTHORITIES FOR THE PURPOSE OF LOCATING AND PROTECTING EXISTING UNDERGROUND UTILITIES.
- 11. ALL REQUESTS FOR INFORMATION SHALL BE PROVIDED IN WRITING AND ALL DIRECTIVES SHALL BE RECEIVED IN WRITING.
- 12. CHANGES TO THE PROJECT SHALL NOT BE INCORPORATED INTO THE PROJECT UNTIL EXPRESSLY APPROVED BY THE ENGINEER.
- 13. ANY BUY AMERICAN MATERIAL WAIVERS, IF REQUIRED FOR PROJECT, SHALL BE APPLIED FOR AND APPROVED PRIOR TO NOTICE TO PROCEED BEING ISSUED. NO DELAY MAY BE CLAIMED BY THE CONTRACTOR RELATED TO DURATION TO OBTAIN APPROVAL OF THE WAIVER.
- 14. NPDES PERMIT THIS PROJECT WILL DISTURB MORE THAN 1 ACRE, THEREFORE A NPDES PERMIT IS REQUIRED. THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING THE EROSION CONTROL MEASURES AS SHOWN IN THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) AND IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE
- 15. MATERIAL CERTIFICATIONS MATERIALS CANNOT BE INSTALLED UNTIL ALL THE MATERIAL CERTIFICATIONS FOR THAT ITEM HAVE BEEN RECEIVED, REVIEWED AND ACCEPTED BY THE RESIDENT ENGINEER. MATERIALS INSTALLED WITHOUT APPROVAL ARE SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S
- 16. <u>CERTIFIED PAYROLLS</u> THE RESIDENT ENGINEER <u>CANNOT</u> FORWARD A CONSTRUCTION REPORT FOR PAYMENT FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THAT PERIOD HAVE BEEN RECEIVED.

Hanson Professional Services Inc. 1525 S. 6th Street Springfield, IL 62703 phone: 217-788-2450 fax: 217-788-2503

Illinois Licensed Professional Service Corporation #184-001084

Vandalia Park District 900 North Locust Street Vandalia, Illinois 62471

**CONSTRUCT** T-HANGAR ACCESS **TAXIWAYS** 

IL. PROJ. No: VLA-4628

Contract No. VA020

NO.	DATE	DES	CRIPT	ION	
NO.	DATE	DES	DWN	REV	
ISSUE: JAN. 12, 2018					
PROJECT NO: 16A0042					

CAD FILE: G-002-FLP.DWG

DESIGN BY: KBS DRAWN BY: KBS REVIEWED BY: RAW

SHEET TITLE

SUMMARY OF QUANTITIES, INDEX TO SHEETS. **GENERAL NOTES** 

AR901510

SEEDING

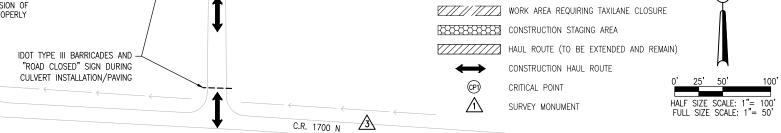
- CONSTRUCTION SAFETY PLAN THE VANDALIA MUNICPAL AIRPORT IS COMPRISED OF TWO PAVED RUNWAYS. THE PROPOSED T-HANGAR PAVING PROJECT WILL NOT REQUIRE RUNWAY CLOSURE, HOWEVER TEMPORARY CLOSURES OF THE TAXILANE ON THE EAST SIDE OF THE EXISTING T-HANGAR, AS WELL AS THE AIRPORT ENTRANCE ROAD WILL BE NECESSARY. THE NEW EAST T-HANGAR WILL ALSO BE CLOSED FOR THE DURATION OF THE PROJECT. SPECIFIC ITEMS REGARDING CLOSURES INCLUDE:
  - . THE TAXILANE SHALL BE CLOSED ANYTIME THE CONTRACTOR IS WORKING WITHIN 40 FEET OF THE TAXILANE CENTERLINE, OR HAS SURFACE DROP-OFFS WITHIN 25 FEET OF THE CENTERLINE THAT ARE GREATER THAN 3 INCHES. THE TAXILANE IS ANTICIPATED TO BE CLOSED FROM THE TIME TRENCH DRAIN EXCAVATION BEGINS UNTIL THE FIRST LIFT OF ASPHALT OR CONCRETE IS PLACED.
  - THE AIRPORT MANAGER AND RESIDENT ENGINEER/TECHNICIAN SHALL BE NOTIFIED A MINIMUM OF 72 HOURS PRIOR TO THE DESIRED TAXILANE CLOSING TIME.
  - . THE EXISTING TAXILANE SHALL BE PAVED WITHIN THREE DAYS OF MILLING.
- · BARRICADES SHALL BE PLACED AT THE LOCATION(S) SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN.
- THE CONTRACTOR SHALL MAKE EVERY EFFORT TO MINIMIZE CLOSURES AND ACCOMMODATE TENANTS OF THE EXISTING HANGAR. REGARDLESS WHETHER THE TAXILANE IS OPEN, THE CONTRACTOR SHALL SWEEP THE TAXILANE DAILY AND KEEP THE AREA WITHIN 40 FEET OF THE TAXILANE CENTERLINE FREE OF EQUIPMENT AND MATERIALS WHEN NOT WORKING.
- . THE CONTRACTOR WILL BE ALLOWED TO CLOSE THE AIRPORT ENTRANCE ROAD FOR 1 FULL DAY TO INSTALL AND BACKFILL THE CULVERT ACROSS THE ROAD, AND 1 DAY TO PAVE. COMPACTED AGGREGATE MAY BE USED AS A SHORT-TERM BACKFILL UNTIL PAVING CAN BE COMPLETED. AIRPORT SHALL BE NOTIFIED A MINIMUM OF 3 DAYS BEFORE DESIRED CLOSURE DATE(S).
- 2. ALL PROVISIONS OF FAA ADVISORY CIRCULAR AC 150/5370-2 (CURRENT EDITION), "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" APPLY TO THIS CONTRACT. EXCEPT AS MODIFIED BY THIS SAFETY PLAN, BY THE AIRPORT MANAGER, OR THROUGH THE RESIDENT ENGINEER/TECHNICIAN AT THE PRECONSTRUCTION CONFERENCE OR DURING THE COURSE OF THE CONTRACT
- 3. ALL COSTS ASSOCIATED WITH THE WORK AND MATERIALS INVOLVING CONSTRUCTION SAFETY & PHASING AS SHOWN OR DESCRIBED IN THE CONTRACT DOCUMENTS, OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER/TECHNICIAN OR AIRPORT MANAGER THROUGHOUT THE PROJECT, SHALL BE INCIDENTAL TO THE CONSTRUCTION CONTRACT.
- AIRFIELD SAFETY ASSURANCE AIRFIELD SAFETY SHALL BE HELD PARAMOUNT AT ALL TIMES. ANY INDIVIDUALS RESPONSIBLE FOR INCURSIONS OR POTENTIAL INCURSIONS WITH AIR TRAFFIC DUE TO NON-COMPLIANCE WITH REQUIREMENTS SET FOR IN THESE PLANS, SPECIFICATIONS, SPECIAL PROVISIONS, AND FAA ADVISORY CIRCULAR 150/5370-2F WILL BE SUBJECT TO AN IMMEDIATE SUSPENSION OF DRIVING PRIVILÉGES ON THE AIRPORT OR A COMPLETE RESTRICTION FROM ENTERING THE AIR OPERATIONS AREA ALTOGETHER. THE AIRPORT MANAGER OR RESIDENT ENGINEER/TECHNICIAN MAY STOP THE WORK AT ANY TIME THEY BELIEVE AIRFIELD SAFETY IS BEING COMPROMISED.
- 5. ALL CONSTRUCTION EQUIPMENT OPERATING IN THE PRESCRIBED CONSTRUCTION AREA IS REQUIRED TO DISPLAY A CHECKERBOARD FLAG PROPERLY LOCATED OR A ROTATING BEACON (STROBE) AS SPECIFIED IN AC 150/5210-5, "PAINTING, MARKING, AND LIGHTING OF VEHICLES USED ON AN AIRPORT" (CURRENT EDITION).
- THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT IS 25 FEET OR AS SHOWN FOR A PARTICULAR LOCATION. EQUIPMENT WITH HEIGHTS GREATER THAN THAT SHOWN WILL NOT BE ALLOWED ONSITE UNLESS ADDITIONAL AIR SPACE APPROVAL IS
- 7. CLOSED AIRFIELD PHASING AREAS, OPEN TRENCHES, AND OTHER HAZARDOUS AREAS AT THE CONSTRUCTION SITE SHALL BE PROMINENTLY MARKED WITH LIGHTED BARRICADES WITH STEADY BURNING OR FLASHING RED LIGHTS AS SPECIFIED IN 150/5370-2, "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION", LATEST EDITION. LIGHTED BARRICADES MUST BE NO TALLER THAN 18" (EXCLUSIVE OF SUPPLEMENTARY — LIGHTS AND FLAGS) ON THE TAXIWAYS. CONTRACTOR SHALL NIGHT CHECK BARRICADES DAILY FOR PROPER OPERATION
- 8. NO PAVEMENT EDGE DROP-OFFS GREATER THAN 3" ARE ALLOWED TO REMAIN ON ANY ACTIVE AIRFIELD PAVEMENT AREA. TEMPORARY EARTH OR SHOULDERS SHALL BE PLACED IF AN INCOMPLETE AREA IS TO BE REOPENED TO AIRCRAFT TRAFFIC.
- SOIL, DEBRIS, AND LOOSE MATERIAL DROPPED OR TRUCKED ONTO AIRPORT ROADS, TAXIWAYS, AND SOD SURFACES, OR WHICH CAN BE BLOWN ONTO SUCH SURFACES, SHALL BE IMMEDIATELY SWEPT, PICKED UP AND RÉMOVED, OR PLACED INTO CLOSED CONTAINERS. ANY DAMAGE TO AIRPORT PROPERTY SHALL BE REPAIRED IMMEDIATELY AT NO COST TO THE OWNER.
- 10. EACH CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND MAINTAINING AIRPORT LIGHTING AND NAVIGATIONAL ELECTRICAL SYSTEMS DURING CONSTRUCTION. A CONTACT PERSON AND TELEPHONE NUMBER FOR 24 HOUR EMERGENCY IMMEDIATE REPAIR SHALL BE SUBMITTED TO THE AIRPORT MANAGER AND ENGINEER, HAUL ROUTES CROSSING PAVEMENT, DRAINAGE, MISCELLANEOUS STRUCTURES AND/OR AIRFIELD CABLES SHALL BE PROTECTED FROM DAMAGE.
- 11. ALL AIRCRAFT AND AIRPORT OPERATIONS HAVE THE RIGHT-OF-WAY. CONTRACTOR TO YIELD TO VEHICLES AND REMAIN CLEAR AT ALL TIMES.
- 12. THE CONTRACTOR SHALL UTILIZE WATER AND/OR CHEMICALS APPROVED BY THE RESIDENT ENGINEER AS NECESSARY TO CONTROL DUST.
- 13. RADIO CONTROL THE CONTRACTOR WILL BE REQUIRED TO PROVIDE AN AIRBAND RADIO AND MAINTAIN TWO-WAY RADIO CONTACT WITH THE VANDALIA MUNICIPAL AIRPORT UNICOM (122.8 MHz) WHILE HE HAS PERSONNEL OR EQUIPMENT ON THE AIRFIELD.

	SURVEY CONTROL POINTS						
NO.	DESCRIPTION	NORTHING	EASTING	ELEV.			
$\triangle$	MAG NAIL IN PAVEMENT	846,318.11	748,210.15				
<b>1</b>	MAG NAIL IN PAVEMENT	845,935.61	748,208.94				
⅓	NGS "Y 256" DISK IN CULVERT HEADWALL	845,571	748,030	531.89			

	CRITICAL POINTS						
POINT #	DESCRIPTION	LATITUDE	LONGITUDE	GRND. ELEV. (MSL)	HEIGHT (AGL)		
1	EQUIPMENT	N038* 59' 16.549"	W089* 09' 49.610"	532.0	25'		
2	EQUIPMENT	N038* 59' 16.541"	W089* 09' 47.331"	531.6	25'		
3	EQUIPMENT	N038* 59' 12.053"	W089' 09' 54.002"	532.0	25'		
4	EQUIPMENT	N038* 59' 14.892"	W089' 09' 53.299"	534.0	25'		
5	BARRICADE	N038° 59' 16.185"	W089° 09' 50.454"	533.0	4'		

## SURVEY NOTES

- 1. ALL COORDINATE VALUES SHOWN IN TABLE ARE BASED ON ILLINOIS STATE PLANE COORDINATE SYSTEM, EAST ZONE, NAD-83 (2011). ALL ELEVATIONS ARE REFERENCED TO NAVD 88.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION LAYOUT AND ANY EXTENSION OF THE CONTROL NETWORK NEEDED TO PROPERLY COMPLETE THE WORK



CORPORATE

HANGAR

CORPORATE

HANGAR

- SITE ACCESS ROAD

(OIL AND CHIP)

AIRPORT ENTRANCE ROAD (OIL AND CHIP)

ACCESS TAXIWAY

TERMINAL

**RUILDING** 

MATERIAL

STORAGE AND STAGING AREA

MAIN APRON



TAXILANE MUST BE CLOSED WHEN

BARRICADE PLACEMENT

DURING TAXILANE

-CP5

NEW DITCH/CULVERT

UNDER ENTRANCE ROAD

LEGEND

---- BARRICADES DURING TAXILANE CLOSURE

— — — PROJECT LIMITS

WORKERS OR EQUIPMENT ARE IN THIS

AREA, OR WHEN SURFACE DROP-OFFS

GREATER THAN 3" ARE PRESENT WITHIN

**EXISTING** SWALE

GRADING

- PAVING LIMITS

EXISTING TAXILANE

TO BE MILLED

AND OVERLAID

CONCRETE

WASHOUT

HAUL ROUTE TO BE

LEFT IN PLACE

(SEE DETAIL)

TILLED FIELD

LIMITS

25' OF THE CENTERLINE, MATERIALS

MUST BE KEPT OUTSIDE THIS AREA.

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Illinois Licensed Professional Service Corporation #184-001084

Vandalia Park District 900 North Locust Street Vandalia, Illinois 62471

**CONSTRUCT** T-HANGAR ACCESS **TAXIWAYS** 

IL. PROJ. No: VLA-4628

Contract No. VA020

П						
		NO.	DATE	DES	CRIPT	ION
		NO.	בואל	DES	DWN	REV
	į	SSUE:	JAN. 12	, 2018		
	Ì	PROJEC	CT NO: 1	6A004	2	
		CAD FIL	E: G-00	3-SFY	DWG	
ı	li	DESIGN	BY: KB	s —		

DRAWN BY: KBS

REVIEWED BY: RAW

SHEET TITLE

**EXISTING SITE AND** SAFETY PLAN

HAUL ROUTE DETAIL

NOT TO SCALE

www.hanson-inc.com

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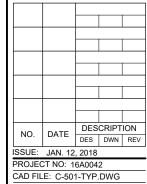
Illinois Licensed Professional Service Corporation #184-001084

Vandalia Park District 900 North Locust Street Vandalia, Illinois 62471

CONSTRUCT T-HANGAR ACCESS **TAXIWAYS** 

IL. PROJ. No: VLA-4628

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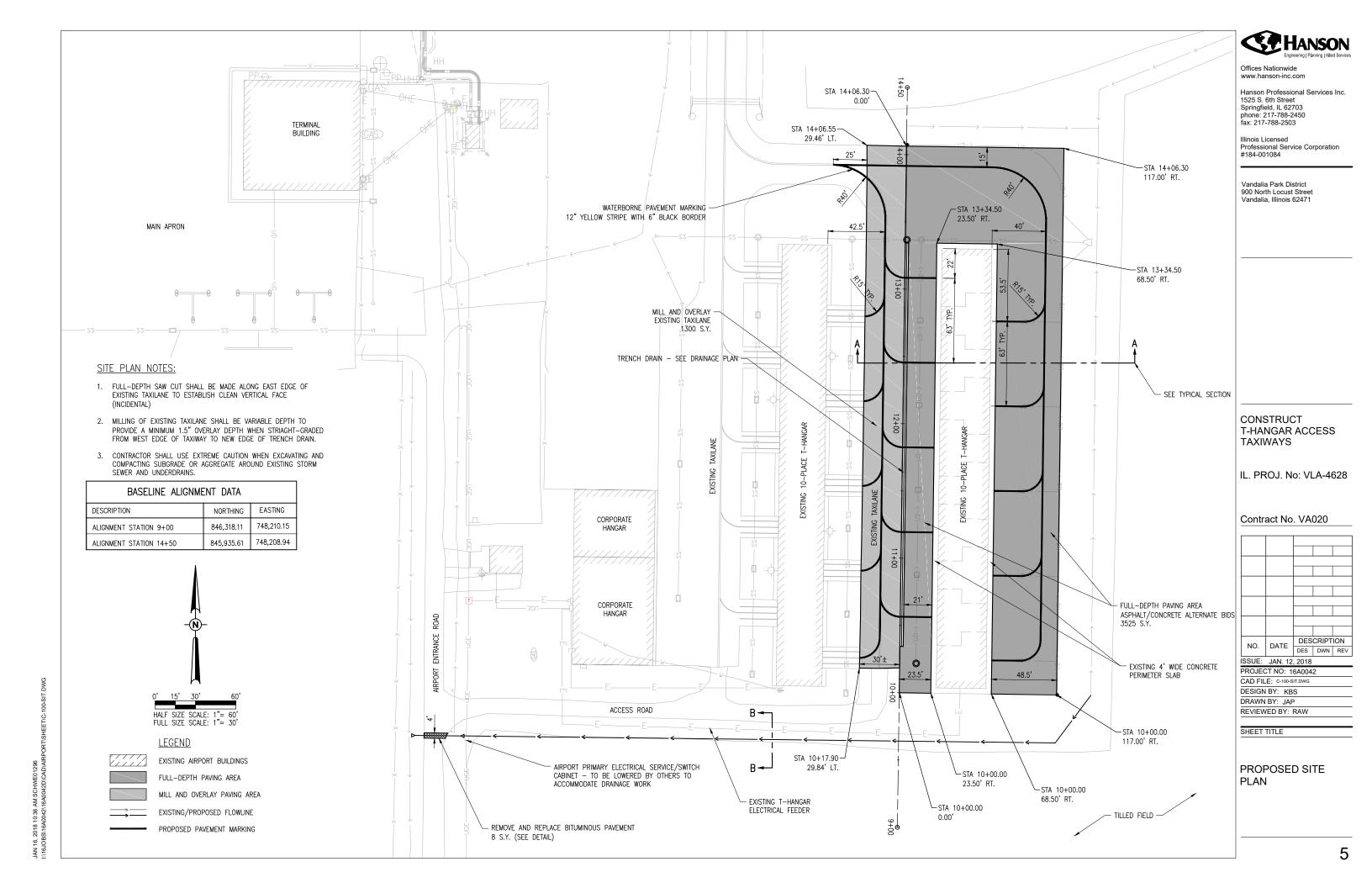
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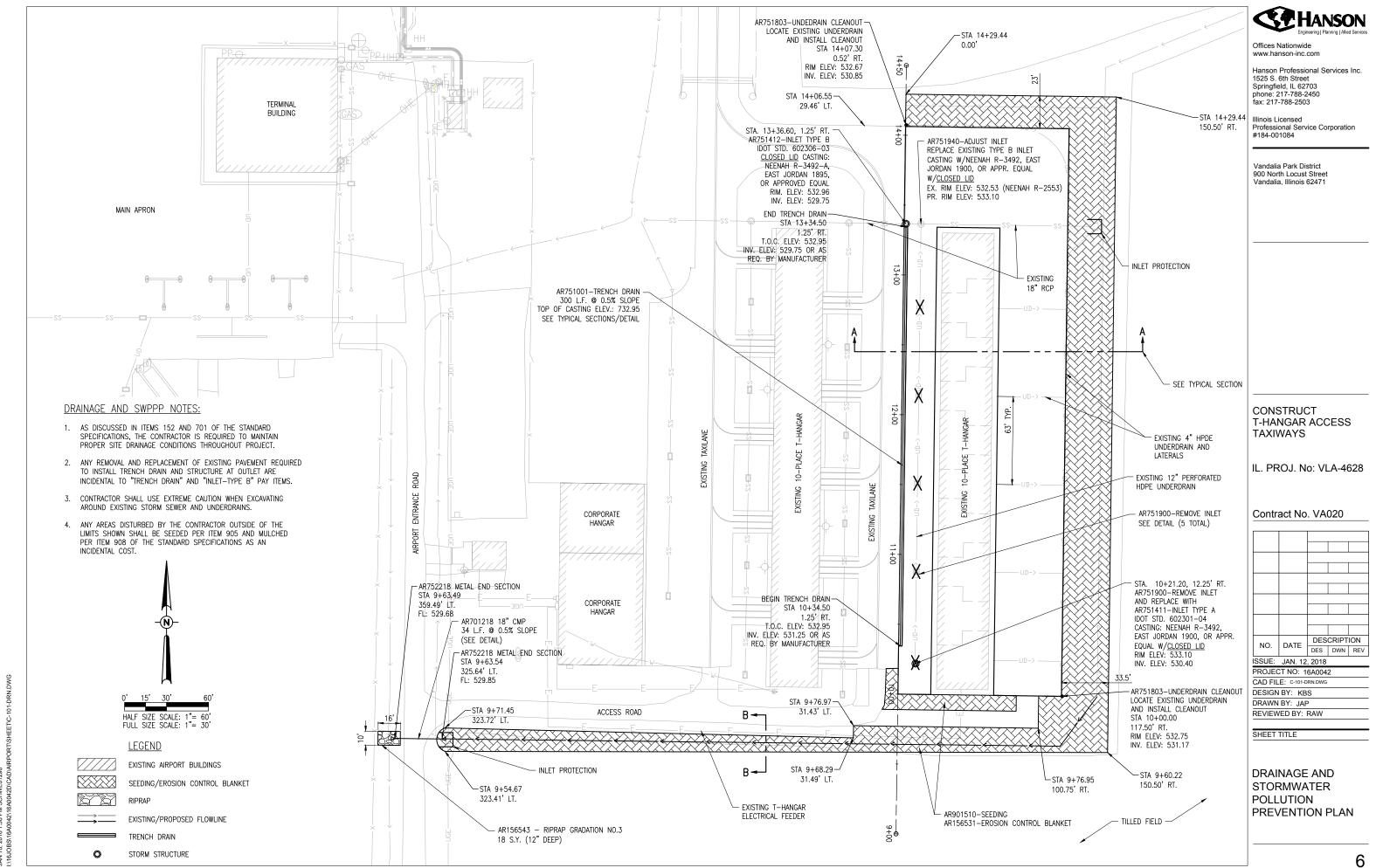
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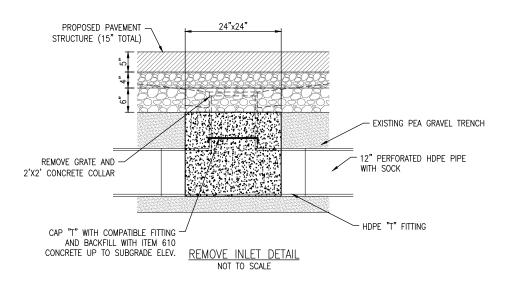
TYPICAL SECTION AND DETAILS

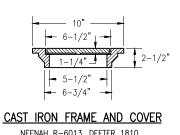
DITCH TYPICAL SECTION B-B

NOT TO SCALE









NEENAH R-6013, DEETER 1810, EAST JORDAN 2790-6 OR APPROVED EQUAL

### **CLEANOUT NOTES**

- 1. DIAMETER OF PIPE AS SPECIFIED.
- 2. TOP OF CLEANOUTS SHALL BE 2" ABOVE FINISH GROUND LINE AT LOCATION SHOWN ON PLANS.
- 3. 1/2" CHAMFER TO BE USED ON ALL EXPOSED EDGES OF CLEANOUTS.
- 4. THE CONCRETE SHALL BE STRUCTURAL PORTLAND CEMENT CONCRETE (NON- REINFORCED) IN ACCORDANCE WITH ITEM 610.

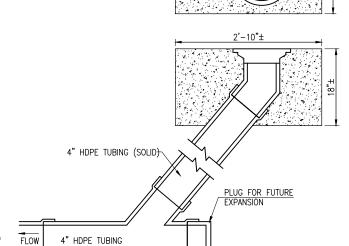
8" PVC CONNECTION

TO NEW INLET

1.0' (ALL SIDES)

<u>Plan</u>

(INCIDENTAL)



**UNDERDRAIN CLEANOUT** 

NO SCALE

CORE DRILL

OPENING



www.hanson-inc.com

1525 S. 6th Street Springfield, IL 62703

Illinois Licensed

phone: 217-788-2450 fax: 217-788-2503

Vandalia Park District

900 North Locust Street

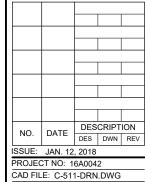
Vandalia, Illinois 62471

Hanson Professional Services Inc.

Professional Service Corporation #184-001084

IL. PROJ. No: VLA-4628

# Contract No. VA020



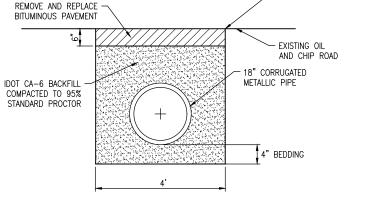
DESIGN BY: KBS

DRAWN BY: KBS REVIEWED BY: RAW

SHEET TITLE

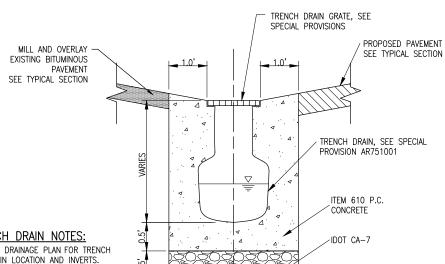
DRAINAGE DETAILS

SAW CUT (BOTH SIDES)



CULVERT DETAIL

NOT TO SCALE



# TRENCH DRAIN NOTES:

# 1. SEE DRAINAGE PLAN FOR TRENCH DRAIN LOCATION AND INVERTS.

EXISTING 18" RCP -

INLET TYPE B

 TRENCH DRAIN ACCESSORIES, TO INCLUDE BUT NOT LIMITED TO, CHANNEL FRAMES, COUPLERS, AND ADAPTERS ARE THE RESPONSIBILITY OF THE CONTRACTOR AND INCIDENTAL TO TRENCH DRAIN.

# © TRENCH DRAIN SECTION A-A TRENCH DRAIN

∠ CONCRETE INLET 1. HOLE FOR TRENCH DRAIN PIPING TO BE CORE DRILLED INTO INLET AT ELEVATION SPECIFIED. 2.CONNECTION IS INCIDENTAL TO PIPE.

PROPOSED & 8" PVC PIPE

INSTALL TRENCH DRAIN PIPING INTO

└─ INLET AND GROUT AROUND TUBING

ITEM 610 P.C. CONCRETE

8" TRENCH DRAIN

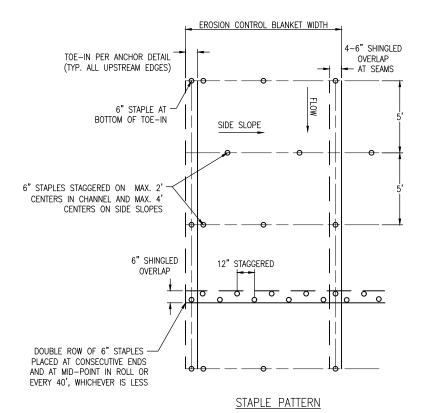
AND GRATE

(AR751001)

ITEM 610 P.C. CONCRETE

(INCIDENTAL TO TRENCH DRAIN)

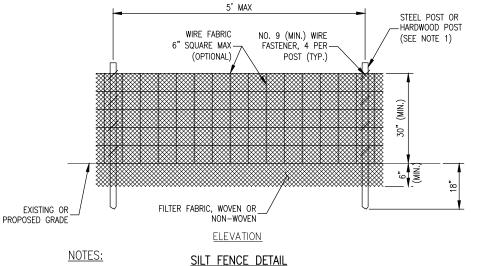
TRENCH DRAIN CONCRETE COLLAR AND GROUT CONNECTION

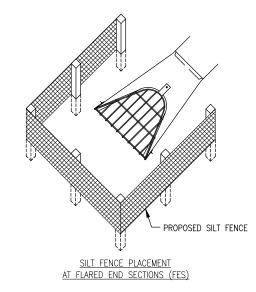


# -FILTER FABRIC EXISTING OR PROPOSED GRADE COMPACTED BACKFILL FABRIC ANCHOR DETAIL

### STORM WATER POLLUTION PREVENTION NOTES

- THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE, PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.
- 2. THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE
- THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLILITION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES
- POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.
- THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.
- ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.





# NOTES:

- FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET
- 2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
- 3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
- FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
- 5. WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
- 6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN (OR MAXIMUM OF 0.60mm).
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
- 8. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE
- SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
- 10. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
- 11. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
- 12. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
- 13. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.



EROSION CONTROL BLANKET DETAILS

INTERIOR

OF PIT

EXTERIOR

OF PIT

-SANDBAG

-30-MIL POLYETHYLENE

WALL

NOT TO SCALE

# CONCRETE WASHOUT DETAIL

### NOTES:

BACKFILL AND

COMPACT

IMPERMEABLE SHEETING MUST EXTEND OVER ENTIRE BASIN AND BERM TO 9. PREVENT ESCAPE OF DISCHARGE.

-BARRIER

WALL

- PROTECT AREA AROUND UNIT FOR 10 FEET WITH PLASTIC UNDER AND AROUND UNIT TO CONTAIN SPILLS OR OVERFLOW.
- FACILITY LINED WITH 30-MIL POLYETHYLENE LINER AND SECURED USING SAND BAGS, OR OTHER ANCHORS, AND SHALL BE FREE OF HOLES OR
- 4. FACILITY IS TO BE LOCATED ON LEVEL GROUND.

EROSION CONTROL BLANKET

6" STAPLES (TYP.

FLOW

ANCHOR DETAIL

10' MIN.

o o o olo o o o

- IMPENDING STORMS TO PREVENT OVERFLOW.
- PLACE AND SECURE A NON-COLLAPSING, NON WATER COLLECTING COVER OVER THE WASHOUT FACILITY TO PREVENT ACCUMULATION AND PRECIPITATION OVERFLOW
- REMOVE WASHOUT WATER FROM HIGH VOLUME FACILITIES WITH A VACUUM TRUCK AND DISPOSE OF PROPERLY. DO NOT DISCHARGE WASTEWATER INTO THE ENVIRONMENT. (NOTE: ACIDITY, NOT PARTICULATES, IS
- DO NOT DISCHARGE WASHOUT WATER INTO THE ENVIRONMENT; FACILITATE EVAPORATION OF LOW VOLUME WASHOUT WATER.

- INSPECT LINE FOR TEARS. AN INTACT LINER WILL ENSURE THAT CONCRETE WASTEWATER WILL NOT ESCAPE THE WASHOUT
- 10. REPLACE DAMAGED LINER IMMEDIATELY.
- 11. CHECK AREA SURROUNDING FACILITY FOR SIGNS OF EFFLUENT ESCAPING CONTAINMENT.
- 12. INSPECT WASHOUT AREA FOLLOWING POUR TO EVALUATE EFFECTIVENESS.
- WASHOUT NEEDS TO BE COVERED OR LIQUIDS TO BE REMOVED PRIOR TO 13. CHECK DEPTH OF SOLIDS TO ENSURE VOLUME IS SUFFICIENT FOR NEXT POUR.
- IF EFFLUENT CANNOT BE REMOVED PRIOR TO ANTICIPATED RAINFALL EVENT, 14. INSPECT WASHOUTS PRIOR TO POUR TO ENSURE SUFFICIENT VOLUME IS AVAILABLE TO CONTAIN WASHOUT.
  - REMOVE TEMPORARY CONCRETE WASHOUT FACILITIES WHEN NO LONGER NEEDED AND RESTORE DISTURBED AREAS TO ORIGINAL
  - 16. DISPOSE OF SOLIDIFIED CONCRETE WASTE, CONSIDERED CLEAN CONSTRUCTION OR DEMOLITION DEBRIS (CCDD) AS PER THE IEPA ACT (415 ILCS5).
  - 17. COST OF TEMPORARY WASHOUT AREA IS INCIDENTAL TO PCC

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llinois Licensed Professional Service Corporation #184-001084

Vandalia Park District 900 North Locust Stree Vandalia, Illinois 62471

CONSTRUCT T-HANGAR ACCESS **TAXIWAYS** 

L. PROJ. No: VLA-4628

Contract No. VA020

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PROJECT NO: 16A0042

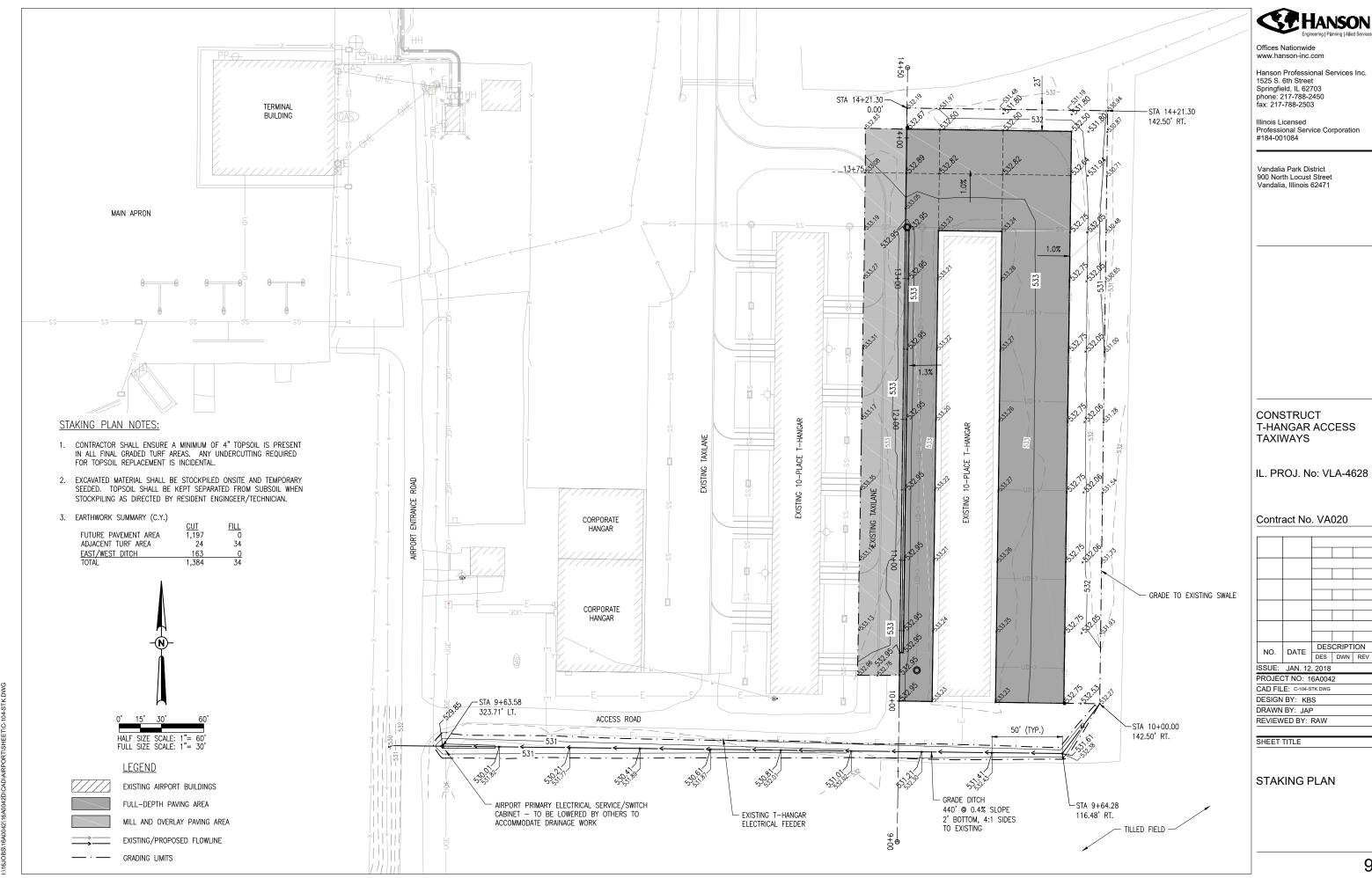
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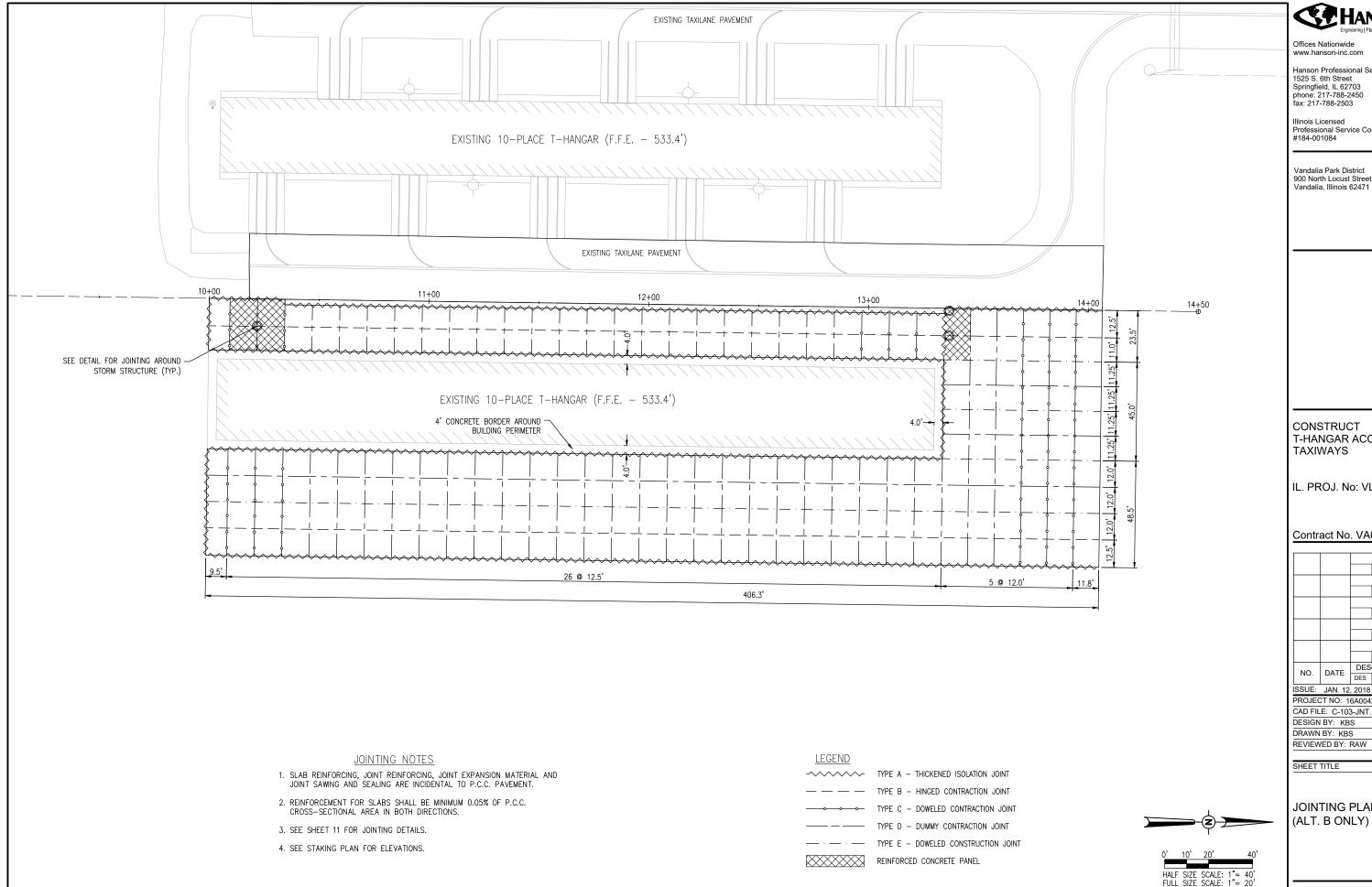
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SHEET TITLE

**DETAILS** 

STORM WATER **POLLUTION** PREVENTION PLAN





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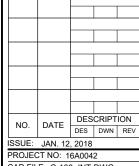
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T-HANGAR ACCESS

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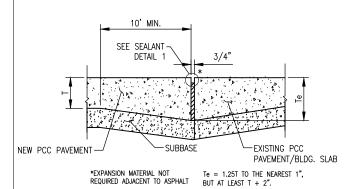
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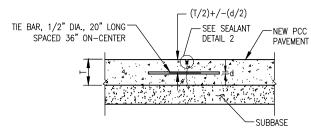


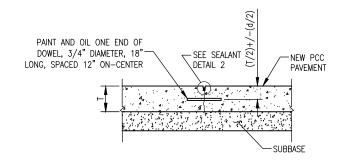
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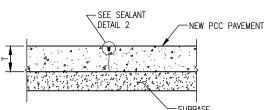
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JOINTING PLAN











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CONSTRUCT

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IL. PROJ. No: VLA-4628

Contract No. VA020

NO. DATE

DESIGN BY: KBS

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SHEET TITLE

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(ALT. B ONLY)

ISSUE: JAN. 12, 2018

PROJECT NO: 16A0042

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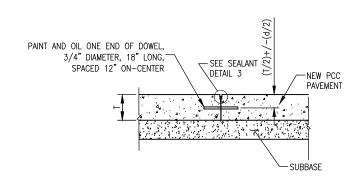
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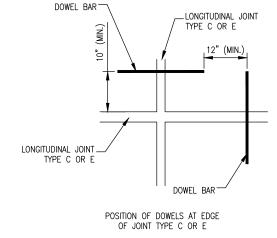
Hanson Professional Services Inc.

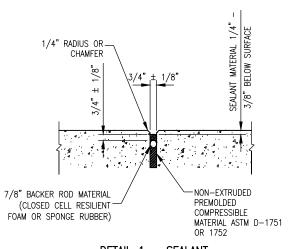
TYPE A - THICKENED EDGE ISOLATION JOINT

TYPE B - HINGED **CONTRACTION JOINT**  TYPE C - DOWELED **CONTRACTION JOINT** 

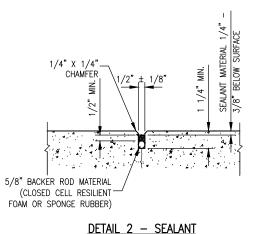
TYPE D - DUMMY **CONTRACTION JOINT** 





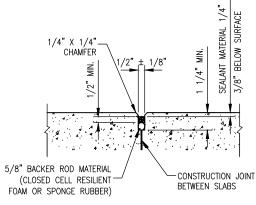


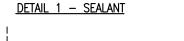
### TYPE E - DOWELED **CONSTRUCTION JOINT**

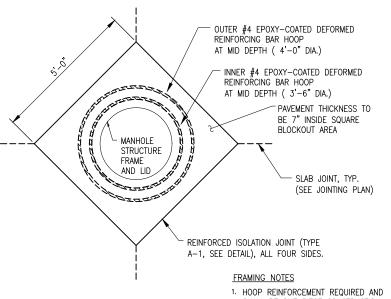


DETAIL 3 - SEALANT

**DOWEL PLAN VIEW** 







SHALL BE ONE PIECE CONSTRUCTION HAVING A MINIMUM LAP LENGTH OF

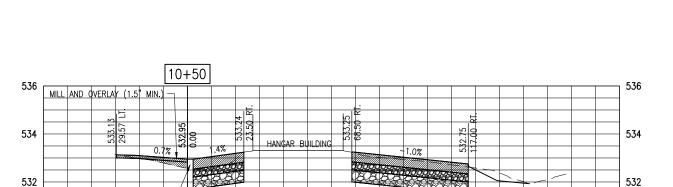
1. ALL JOINT SEALING TO BE INCIDENTAL TO ITEM AR501505 - 5" PCC PAVEMENT. 2. ALL JOINT SEALANT TO BE SILICONE.

JOINTING AND REINFORCING AT IN PAVEMENT MANHOLES

11

DESCRIPTION

DES DWN REV



60

100

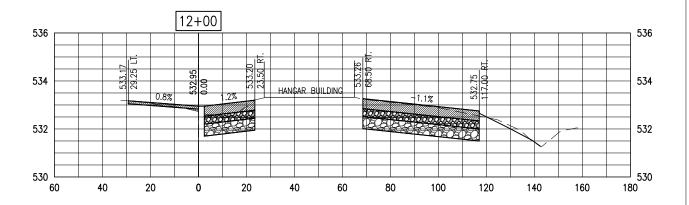
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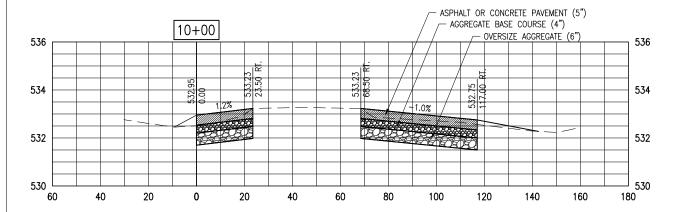
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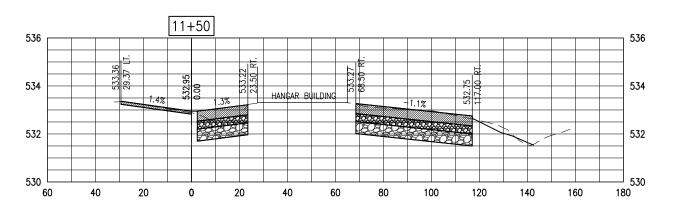
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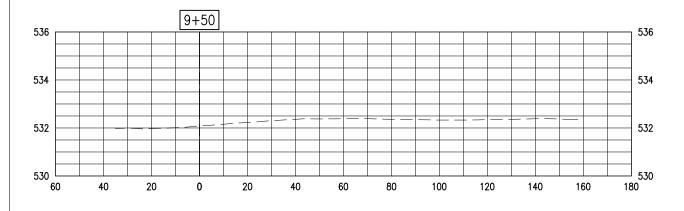
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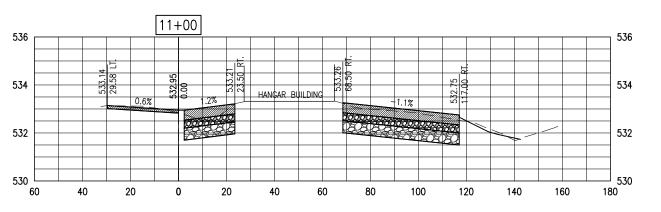
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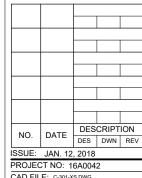
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IL. PROJ. No: VLA-4628

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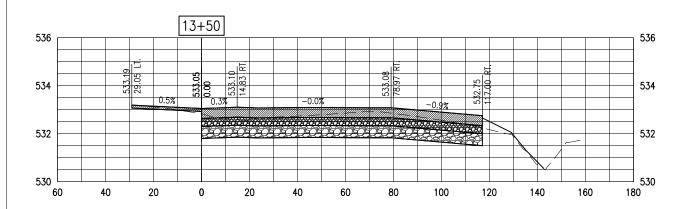
**CROSS SECTIONS** SHEET 1

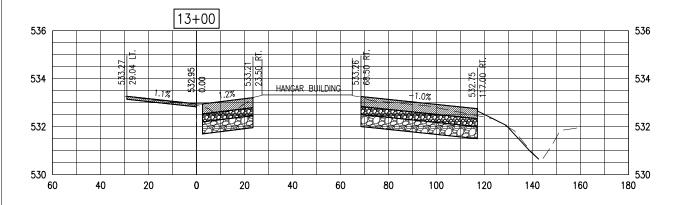
TRENCH DRAIN (SEE DETAIL)

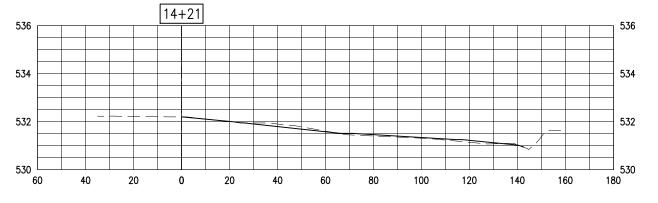
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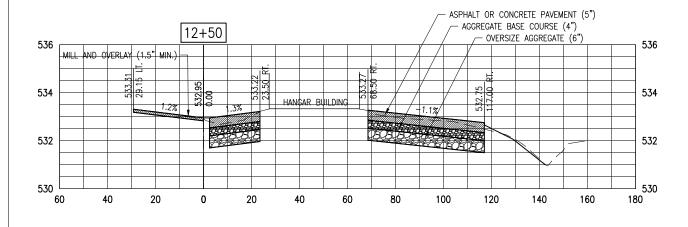
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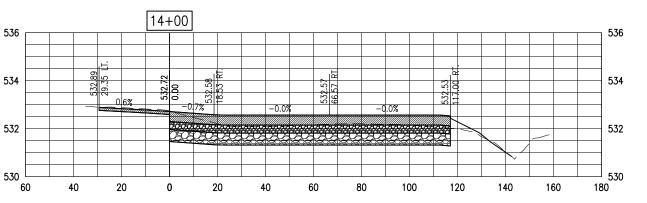
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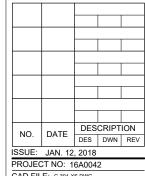
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**CROSS SECTIONS** SHEET 2