

LETTING #04A

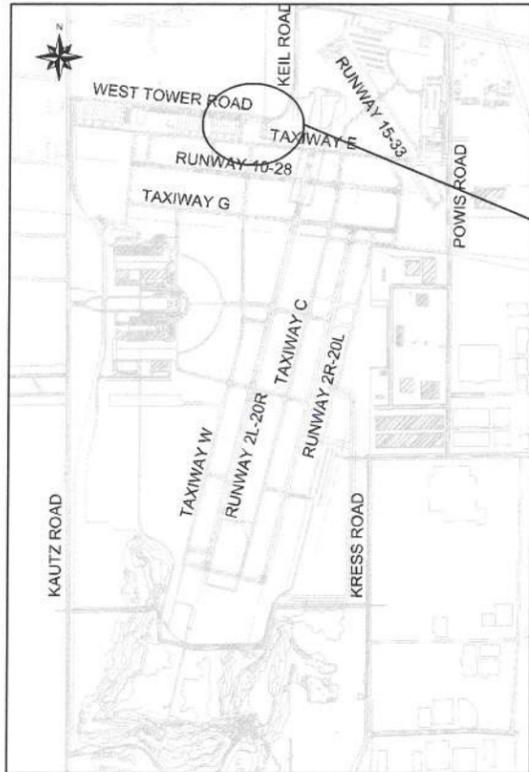
# DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

DU083

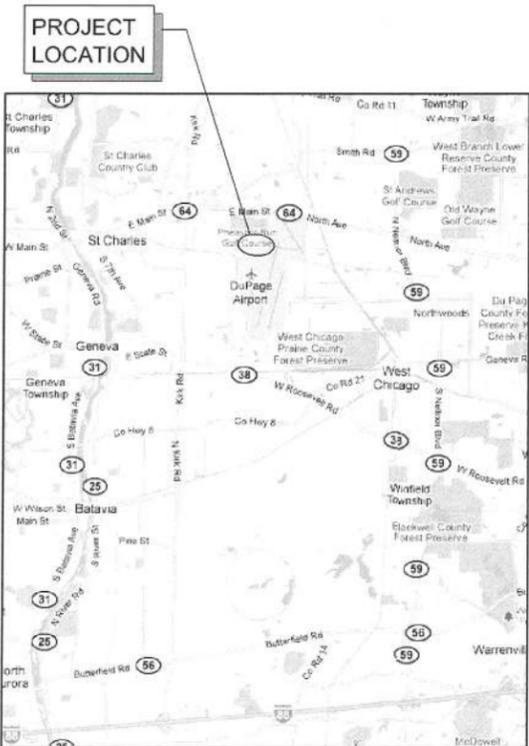
## CONSTRUCTION PLANS FOR DUPAGE AIRPORT

### PHASE 1 PAVEMENT REHABILITATION OF APRON AREAS AT ECHO T-HANGAR

ILLINOIS PROJECT: DPA-4557  
A.I.P. PROJECT: 3-17-SBGP-139  
IDOT LETTING MARCH 9, 2018  
JANUARY 12, 2018



SITE PLAN



LOCATION MAP

DUPAGE AIRPORT AUTHORITY  
2700 INTERNATIONAL DRIVE SUITE 200 WEST CHICAGO, IL 60145  
APPROVED BY: *[Signature]*  
EXECUTIVE DIRECTOR - DAVID BIRD  
DATE: Jan 10, 2018

ch2m:  
8735 WEST HIGGINS RD SUITE 400 CHICAGO, IL 60631  
SUBMITTED BY: *[Signature]*  
MICHAEL VONIC, P.E.  
DATE: 1/11/2018



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CH2M HILL  
MICHAEL VONIC, P.E.  
DATE SIGNED: 1/11/2018  
THIS SEAL APPLIES TO ALL SHEETS  
EXPIRATION DATE: 11/30/2019  
*[Professional Engineer Seal: Michael Vonic, License No. 062062420, State of Illinois]*

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1-800-892-0123

**SUMMARY OF QUANTITIES**

ITEM NO.	PAY ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY - LOCAL ONLY	ESTIMATED QUANTITY - FEDERAL/STATE/LOCAL SPLIT	TOTAL
1	AR150520	MOBILIZATION	LS	0	1	1
2	AR150530	TRAFFIC MAINTENANCE	LS	0	1	1
3	AR152410	UNCLASSIFIED EXCAVATION	CY	31	0	31
4	AR152511	SUBGRADE REPAIR	SY	150	250	400
5	AR156500	TEMPORARY EROSION CONTROL	LS	0	1	1
6	AR156520	INLET PROTECTION	EA	0	13	13
7	AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	83	83	166
8	AR209606	CRUSHED AGG. BASE COURSE - 6"	SY	93	0	93
9	AR401610	BITUMINOUS SURFACE COURSE	TON	472	653	1,125
10	AR401650	BITUMINOUS PAVEMENT MILLING	SY	4,242	6,000	10,242
11	AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	188	188	376
12	AR403610	BITUMINOUS BASE COURSE	TON	16	0	16
13	AR603510	BITUMINOUS TACK COAT	GAL	651	900	1551
14	AR620520	PAVEMENT MARKING - WATERBORNE	SF	0	111	111

**ABBREVIATIONS:**

AB	AGGREGATE BASE
ABV	ABOVE
A/C	ACCESS CONTROL
AC	ACRES
ADJ	ADJUST
AGG	AGGREGATE
AGS	AUXILIARY GAS VALVE
ALD	AIRFIELD LIGHTING DUCT
ALSF	APPROACH LIGHTING SEQUENCE FLASHERS
ARFF	AIRPORT RESCUE AND FIRE FIGHTING
AS	AERIAL SURVEYS
ASPH	ASPHALT
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
AUX	AUXILIARY
AVE.	AVENUE
ATCT	AIR TRAFFIC CONTROL TOWER
AX	AXIS OF ROTATION
B-B	BACK TO BACK
BLVD	BOULEVARD
BM	BENCHMARK
BV	BALL VALVE
CL	CENTERLINE
CB	CATCH BASIN
CC	CENTER TO CENTER
CED	COMBINED / COMMON ELECTRICAL DUCTBANK
CI	CAST IRON
CMH	COMMUNICATION MANHOLE
CMP	CORRUGATED METAL PIPE
COMED	COMMONWEALTH EDISON
COMM	COMMUNICATIONS
CONC	CONCRETE
C.Y.	CUBIC YARD
CSPN	CARGO SITE PREP NORTH
DAA	DUPAGE AIRPORT AUTHORITY
DET	DETAIL
DIA.	DIAMETER
DIP	DUCTILE IRON PIPE
DME	DISTANCE MEASURING EQUIPMENT
DOA	DEPARTMENT OF AVIATION
E	EAST
ED or E.D.	EDGE DRAIN
EMH	ELECTRIC MANHOLE
EL or ELEV	ELEVATION
ESVCP	EXTRA STRONG VITRIFIED CLAY PIPE
EX	EXISTING

**ABBREVIATIONS:**

FAA	FEDERAL AVIATION ADMINISTRATION
FFM	FAR FIELD MONITOR
FH	FIRE HYDRANT
FT	FEET
G	GRADE
GRND	GROUND
GS	GLIDE SLOPE
HR	HANGAR ROAD
HV	HIGH VOLTAGE
IDOT	ILLINOIS DEPARTMENT OF TRANSPORTATION
ILS	INSTRUMENT LANDING SYSTEM
IE or I.E.	INVERT ELEVATION
ID	IDENTIFICATION
IH	INSPECTION HOLE
INV.	INVERT
JC	JUNCTION CHAMBER
JV	JOINT VALVE
KV	KILOVOLT
K=L/A	LENGTH OF VERTICAL CURVE/ALGEBRAIC DIFFERENCE IN GRADE
L	LENGTH
L.S.	LUMP SUM
LOC	LOCALIZER
MAX	MAXIMUM
MH	MANHOLE
MIN	MINIMUM
MJ	MECHANICAL JOINT
MPR	MOUNT PROSPECT ROAD
N	NORTH
NAVD	NORTH AMERICAN VERTICAL DATUM
N.C.	NORMALLY CLOSED
N.O.	NORMALLY OPEN
N.I.C.	NOT IN CONTRACT
NTS	NOT TO SCALE
NO	NUMBER
NPR	NORTH PERIMETER ROAD
O.D.	OUTSIDE DIAMETER
OFA	OBJECT FREE AREA
OZ	OUNCE

**ABBREVIATIONS:**

PAPI	PRECISION APPROACH PATH INDICATOR LIGHTS
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
PCCP	PORTLAND CEMENT CONCRETE PAVEMENT
P.I.	POINT OF INTERSECTION
PL	PROPERTY LINE
PT	POINT OF TANGENCY
PVC	POINT OF VERTICAL CURVATURE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
PVC PIPE	POLYVINYL CHLORIDE PIPE
PSI	POUNDS PER SQUARE INCH
R	REMOVE
RAD	RADIUS
RC	REINFORCED CONCRETE
RCP	REINFORCED CONCRETE PIPE
RPP	REINFORCED POLYPROPYLENE
RPU	REMOTE PROCESSING UNIT
REV.	REVISION
RGS	RIGID STEEL
RD.	ROAD
RSA	RUNWAY SAFETY AREA
R/W	RUNWAY
RPZ	RUNWAY PROTECTION ZONE
SA	SANITARY
SCH	SCHEDULE
SD	STORM DRAIN
SHT	SHEET
S	SOUTH
SQR	SQUARE
S.F.	SQUARE FEET
S.Y.	SQUARE YARD
STA	STATION
ST.	STREET
ST	STORM SEWER
T	TANGENT
T.S.	TANGENT SPIRAL
TSA	TAXIWAY SAFETY AREA
T/W	TAXIWAY
TDZ	TOUCHDOWN ZONE
TYP	TYPICAL
UD or U.D.	UNDERDRAIN
UMH	UNDERDRAIN MANHOLE
U.N.O.	UNLESS NOTED OTHERWISE
VERT.	VERTICAL
VPI	VERTICAL POINT OF INTERSECTION
W	WEST
W/	WITH
WMG	WEST MASS GRADING
YD	YARD



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				MX
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				BY
				APVD

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PHASE 1 PAVEMENT REHABILITATION OF  
APRON AREAS AT ECHO-T-HANGAR  
CONTRACT #DU083



**SUMMARY OF QUANTITIES  
AND ABBREVIATIONS**

AS SHOWN
VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
DATE 10/06/2017
PROJ DPA-4557
DWG G-101
SHEET 2 of 11

**GENERAL NOTES**

- THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS AND ANY RULES, REGULATIONS, STANDARDS AND SPECIFICATIONS REFERENCED THEREIN. THE PROJECT IS SUBJECT TO INSPECTION BY REPRESENTATIVES OF THE DUPAGE AIRPORT AUTHORITY (DAA).
- THE PROJECT IS TO BE COMPLETED IN A TIMELY MANNER IN ACCORDANCE WITH THE CONTRACTOR'S APPROVED PROJECT SCHEDULE. THE SCHEDULE SHALL BE PROVIDED FOR COMPLETION OF THE WORK AS DESCRIBED IN THE CONTRACT SPECIFICATIONS. THE CONTRACTOR SHALL KEEP DAA INFORMED OF UPDATES TO THE PROJECT SCHEDULE AT ALL TIMES.
- THE CONTRACTOR SHALL COMPLETE THE ENTIRE PROJECT WITHIN THE TIME STATED IN THE CONTRACT.
- CONSTRUCTION AND MAINTENANCE OPERATIONS BY OTHERS MAY OCCUR CONCURRENTLY AND AT TIMES IN THE VICINITY OF CONSTRUCTION ASSOCIATED WITH THIS PROJECT. THE CONTRACTOR SHALL COORDINATE HIS OPERATIONS AND COOPERATE WITH MAINTENANCE CREWS AND OTHER CONTRACTORS. CONTRACTOR COORDINATION WITH APPROPRIATE GOVERNMENT AND UTILITY AGENCIES IS ALSO REQUIRED PRIOR TO AND DURING CONSTRUCTION.
- ACCESS TO THE SITE - THE CONTRACTOR'S MAY BE ASKED TO ACCESS THROUGH DESIGNATED SECURITY GATES AS SHOWN ON THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL VEHICLES AND PERSONNEL THAT ENTER THROUGH THESE DESIGNATED AIRPORT SECURITY GATES. AIRPORT SECURITY GATES SHALL BE SECURED AT ALL TIMES WHEN NOT IN USE.
- HAUL ROUTES - THE CONTRACTOR'S HAUL ROUTES AND CONSTRUCTION ACCESS SHALL BE EXISTING HAUL ROADS WHERE AVAILABLE. ANY DEBRIS (WHETHER CAUSED BY THE CONTRACTOR OR NOT) SHALL BE REMOVED IMMEDIATELY BY THE CONTRACTOR. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS, OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES/CONSTRUCTION ACCESS SHALL BE MAINTAINED AND STABILIZED BY THE CONTRACTOR AS REQUIRED TO SUPPORT CONSTRUCTION EQUIPMENT THROUGHOUT THE DURATION OF THIS PROJECT AND SHALL BE RESTORED, AT THE CONTRACTOR'S EXPENSE, TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE/CONSTRUCTION ACCESS UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES/CONSTRUCTION ACCESS SHALL BE JOINTLY INSPECTED AND AGREED UPON BY THE CONTRACTOR AND THE RESIDENT ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT/REMOVE TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO COMMENCING THE WORK.
- CONTRACTOR'S STAGING AREAS - AN AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES, FENCE, TREES, ETC. WITHIN THE STAGING AREA AND THE SITE SHALL BE RESTORED TO ITS ORIGINAL CONDITION WITHIN THIRTY (30) DAYS OF THE COMPLETION OF USE. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
- DISPOSAL AREAS - EXCESS SOILS, MILLED ASPHALT, CONCRETE RUBBLE AND UNSUITABLE EXCAVATION MUST BE DISPOSED OF OFFSITE. OFFSITE DISPOSAL AREAS SHALL BE IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL LAWS.
- SAFETY - THE CONTRACTOR SHALL CONDUCT ALL ACTIVITIES IN A SAFE MANNER AS SPECIFIED BY LOCAL, STATE AND FEDERAL LAWS.
- PROTECTION AND REPAIR OF EXISTING UTILITIES - THE CONTRACTOR SHALL VERIFY ALL UTILITIES, BY VISUAL, ELECTRICAL AND BY HAND EXCAVATION OR OTHER METHODS IN COORDINATION WITH ALL UTILITY COMPANIES AND DAA FACILITIES, PRIOR TO BEGINNING ANY CONSTRUCTION OPERATIONS. ANY AND ALL CONFLICTS OF EXISTING UTILITIES BY THE PROPOSED IMPROVEMENTS SHALL BE RESOLVED WITH THE RESIDENT ENGINEER PRIOR TO BEGINNING THE CONSTRUCTION ACTIVITIES. ANY REPAIRS TO EXISTING UTILITIES DAMAGED BY CONTRACTOR ACTIVITIES SHALL BE COMPLETED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF AN FAA REPRESENTATIVE. THE FAA MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS, IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS AT NO ADDITIONAL EXPENSE TO THE OWNER.

- CONSTRUCTION LIMITS - ALL CONTRACTOR VEHICLES AND TRAFFIC (UNLESS OTHERWISE AUTHORIZED) SHALL REMAIN WITHIN THE DESIGNATED STAGING AREA, CONSTRUCTION LIMITS AND HAUL ROUTES.
- THE CONTRACTOR SHALL OBTAIN ALL PERMITS AND LICENSES REQUIRED FOR THE CONSTRUCTION WORK. REFER TO THE SPECIAL CONDITIONS AND GENERAL CONDITIONS SECTIONS OF THE CONTRACT DOCUMENTS.
- EXISTING TOPOGRAPHIC, UNDERGROUND AND ABOVE GROUND UTILITY DATA FOR THIS PROJECT WERE OBTAINED FROM HISTORICAL DATA, DESIGN RECORDS, DAA REFERENCES AND PREVIOUS PROJECT "AS-BUILTS". CONTRACT IS RESPONSIBLE FOR VERIFYING ALL UTILITY INFORMATION.
- IF ANY UNUSUAL ODORS, SOIL STAINS OR BURIED WASTES ARE ENCOUNTERED, STOP WORK IMMEDIATELY AND NOTIFY THE OWNER, AND/OR RESIDENT ENGINEER.
- WORK PERFORMED UNDER THIS CONTRACT WILL NOT BE CONSIDERED COMPLETE UNTIL FINAL ACCEPTANCE OF THE PROJECT BY THE OWNER AND RECEIPT AND APPROVAL OF THE FOLLOWING DOCUMENTS:
  - CONTRACTOR'S WAIVER AND RELEASE OF LIEN.
  - ABSOLUTE BILL OF SALE.
  - CONTRACTOR'S LETTER OF WARRANTY (I.E., LETTER AGREEMENT).
  - CONTRACTOR AS-BUILTS.
  - FINAL PUNCH LIST COMPLETED AND ACCEPTED BY THE RESIDENT ENGINEER.
  - TRANSFER OF OWNER WARRANTY.
- IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, PREVAILING WAGE RATES SHALL BE POSTED AT THE CONTRACTOR STAGING AREA IN A WEATHERPROOF ENCLOSURE AND COPIES SHALL BE SUBMITTED TO THE RESIDENT ENGINEER AND THE OWNER.

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- IF BITUMINOUS PAVEMENT LOCATIONS, AFTER BEING MILLED OR DURING THE MILLING PROCESS BEGIN TO FAIL UNDER CONSTRUCTION TRAFFIC, THE RESIDENT ENGINEER WILL DESIGNATE AREAS OF COMPLETE PAVEMENT STRUCTURE UNDERCUT AND FULL DEPTH PAVEMENT RECONSTRUCTION REPLACEMENT WITH A NEW PAVEMENT SECTION AS SHOWN IN THE PLANS. IF ANY UNSUITABLE MATERIALS ARE DISCOVERED DURING THE MILLING PROCESS PER THE RESIDENT ENGINEER, IT SHALL BE UNDERCUT AND REMOVED BY THE CONTRACTOR AND REPLACED IN ACCORDANCE WITH THE PLAN.
- CONTRACTOR SHALL GRADE AND COMPACT ALL EXISTING AGGREGATE BASES TO THE SATISFACTION OF THE RESIDENT ENGINEER PRIOR TO PAVING. (INCIDENTAL TO THE CONTRACT).
- CONTRACTOR SHALL CONSTRUCT ALL IMPROVEMENTS TO DRAIN OFF OF NEWLY CONSTRUCTED PAVEMENTS. ANY AREAS OF NEW PAVEMENT THAT ARE FOUND TO NOT DRAIN PROPERLY BY THE RESIDENT ENGINEER SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
- THE AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE DAA, FAA, IDOT AND LOCAL ATCT REPRESENTATIVES IS MANDATORY TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.

**GENERAL PROJECT DESCRIPTION**

- THE WORK GENERALLY CONSISTS OF:
  - MILLING OF EXISTING PAVEMENT AREA AS SHOWN IN THE PROJECT DOCUMENTS
  - DISPOSE OF EXISTING ASPHALT MILLINGS
  - PLACEMENT OF BITUMINOUS SURFACE COURSE
  - OTHER WORK ITEMS AS SHOWN IN THE PROJECT DOCUMENTS.

**SECURITY NOTES**

- THE CONTRACTOR SHALL DESIGNATE TO THE RESIDENT ENGINEER IN WRITING THE NAME OF ITS 'CONTRACTOR SECURITY OFFICER' (C.S.O.). THE C.S.O. SHALL REPRESENT THE CONTRACTOR ON THE SECURITY REQUIREMENTS FOR THE CONTRACT. IT IS THE CONTRACTORS RESPONSIBILITY TO FAMILIARIZE HIMSELF/HERSELF WITH THE VARIOUS ASPECTS OF FAR 49 CFR PART 1542: "AIRPORT SECURITY", ADMINISTERED BY THE TRANSPORTATION SECURITY ADMINISTRATION (TSA). ANY VIOLATION OF CFR PART 1542 BY THE CONTRACTOR AND ANY SUBSEQUENT FINES IMPOSED DUE TO THE VIOLATION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR PERSONNEL SECURITY ORIENTATION: THE C.S.O. SHALL BE RESPONSIBLE FOR BRIEFING ALL CONTRACTOR PERSONNEL ON THESE REQUIREMENTS AND PROVISIONS ADOPTED BY THE DUPAGE AIRPORT AUTHORITY (DAA). ALL NEW CONTRACTOR EMPLOYEES SHALL BE BRIEFED ON THESE REQUIREMENTS PRIOR TO WORKING IN THE CONSTRUCTION AREA.
- ACCESS TO SITE: THE AIRPORT WILL NOT PROVIDE AIRFIELD OPERATIONS AREA ESCORTS. CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. NO OTHER ACCESS POINT SHALL BE ALLOWED UNLESS APPROVED BY THE RESIDENT ENGINEER. IF GATE IS TO BE LEFT UNLOCKED, CONTRACTOR SHALL PROVIDE GATE GUARDS FOR THE DURATION OF THE UNLOCKED CONDITION. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR ESCORTED BY THE CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE ACCESS GATE TO THE SITE. A CONTRACTOR'S FLAGMAN OR TRAFFIC CONTROL PERSON SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT THE ACCESS GATE. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE AIRPORT OPERATIONS AREA (AOA). DIRECTIONAL SIGNING ALONG THE DELIVERY ROUTE TO THE STORAGE AREA OR WORK SITE SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- CONSTRUCTION AREA LIMITS: THE LIMITS OF CONSTRUCTION, MATERIAL STORAGE AREAS, PARKING AREA AND OTHER AREAS DEFINED FOR THE CONTRACTOR'S EXCLUSIVE USE DURING CONSTRUCTION SHALL BE MARKED AND LIGHTED. THE CONTRACTOR SHALL ERECT AND MAINTAIN FENCING AROUND THE PERIMETER OF THESE AREAS AND VISIBLE DEVICES FOR DAY/NIGHT USE. TEMPORARY BARRICADES, FLAGGING AND FLASHING WARNING LIGHTS WILL BE REQUIRED AT CRITICAL ACCESS POINTS. THE TYPE OF MARKING AND WARNING DEVICES SHALL BE APPROVED BY THE RESIDENT ENGINEER AND AIRPORT OPERATIONS.
- THE C.S.O. SHALL PROVIDE OPERATIONS AND THE RESIDENT ENGINEER A CURRENT LIST OF COMPANIES AUTHORIZED TO CONDUCT WORK ON THE AIRPORT. CONTRACTOR EMPLOYEE PERSONAL VEHICLES SHALL BE RESTRICTED TO THE CONTRACTORS EMPLOYEE PARKING AREA AND ARE NOT ALLOWED ON THE AIRFIELD AT ANY TIME.



ISSUED FOR BID	REVISION	CHK	APVD
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SUITE 400  
CHICAGO, IL 60631

PHASE 1 PAVEMENT REHABILITATION OF  
APRON AREAS AT ECHO T-HANGAR  
CONTRACT #DU083

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**GENERAL NOTES**

AS SHOWN
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BAR IS ONE INCH ON ORIGINAL DRAWING.
DATE 10/06/2017
PROJ DPA-4557
DWG G-102
SHEET 3 of 11

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**CONSTRUCTION NOTES**

1. CONTRACTOR IS RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO AND FROM THE JOB SITE.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGES CAUSED BY DELIVERIES OR CONTRACTOR VEHICLES. PAVEMENT REPAIR OF THE HAUL ROUTES SHALL BE AT THE SOLE DISCRETION OF THE RESIDENT ENGINEER. NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR REPAIR OF DAMAGE CAUSED BY THE CONTRACTOR'S ACTIVITIES.
3. HAUL ROUTES ON THIS PLAN SHALL BE STRICTLY ADHERED TO AND REMAIN FREE AND CLEAN OF FOREIGN OBJECT DEBRIS (FOD). CONSTRUCTION ACCESS ROUTES MAY BE ADJUSTED IN FIELD TO THE SATISFACTION OF THE RESIDENT ENGINEER IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. ADJUSTMENTS TO THE CONSTRUCTION ACCESS/HAUL ROUTE SHALL BE AGREED UPON BY THE CONTRACTOR AND RESIDENT ENGINEER, AND SHALL BE MADE AT NO ADDITIONAL COST TO THE OWNER.
4. CONTRACTOR ACCESS ROADS AND STAGING AREAS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE COMPLETION OF THE PROJECT.
5. REFER TO DRAWING G-102, GENERAL NOTE 6 FOR DAILY INSPECTION AND CLEANING REQUIREMENTS OF ON- AND OFF-AIRPORT HAUL ROADS.
6. CONTRACTOR TO COORDINATE WORK WITH ADJACENT PROJECTS. SEE GENERAL NOTES, SHEET G-102.
7. THE CONTRACTORS STAGING AREA IS WITHIN THE AOA, AND SHALL BE SECURED AT ALL TIMES. ALL MATERIALS AND EQUIPMENT, WHEN NOT IN USE, SHALL BE PLACED IN APPROVED AREAS WHERE THEY WILL NOT CONSTITUTE A HAZARD TO AIRCRAFT OPERATIONS. ALL EQUIPMENT SHALL BE STORED IN A LOWERED CONFIGURATION WHEN NOT IN USE. EQUIPMENT AND STOCKPILED MATERIAL SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT JET BLASTS OR WIND CONDITIONS. THE CONTRACTOR'S STAGING AREA MUST BE LOCATED OUTSIDE OF THE TAXIWAY E OBJECT FREE AREA.

8. THE CONTRACTOR IS RESPONSIBLE FOR SECURING OBJECTS OR WRAPPING DEBRIS THAT MAY BE BLOWN BY WIND OR JET BLAST FROM THE SITE. CONTRACTOR TO NOTIFY THE TOWER IMMEDIATELY IF FOD GETS OUTSIDE THEIR WORK LIMITS.
9. WORK ASSOCIATED WITH THIS CONTRACT SHALL BE COMPLETED WITHIN THE DURATION OF 10 CALENDAR DAYS.
10. MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT WITHIN PROJECT LIMITS SHOWN IS 25', EQUIVALENT TO A FULL SIZE DUMP TRUCK WITH EXTENDED BED.
11. CONTRACTOR IS REQUIRED TO SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) TO DAA AND THE RESIDENT ENGINEER FOR APPROVAL PRIOR TO THE ISSUANCE OF NOTICE TO PROCEED. SEE FAA AC 150/5370-2G FOR ADDITIONAL DETAILS.
12. AIRCRAFT WITHIN HANGARS E1, E2, E3, E4, and E9 TO BE RELOCATED AS ASSIGNED BY DAA DURING DURATION OF PROPOSED WORK.
13. CONTRACTOR WILL NOT BE PERMITTED TO CLOSE THE AFFECTED TAXIWAY PAVEMENT FOR LONGER THAN 72 HOURS.
14. OVERNIGHT CLOSURES WILL NOT BE PERMITTED.

**CRITICAL POINTS**

Point Number	Latitude	Longitude	Ground Elevation	Object Height
CP 1	41° 54' 53.72"	88° 15' 20.54"	752.75	25
CP 2	41° 54' 52.95"	88° 15' 10.63"	752	25
CP 3	41° 54' 52.84"	88° 15' 9.00"	752	25
CP 4	41° 54' 52.64"	88° 15' 7.20"	752	25
CP 5	41° 54' 52.47"	88° 15' 5.61"	752	25
CP 6	41° 54' 53.59"	88° 15' 24.93"	752	0
CP 7	41° 54' 49.53"	88° 15' 11.81"	754	0
CP 8	41° 54' 51.93"	88° 15' 3.57"	752	0
CP 9	41° 54' 57.19"	88° 15' 18.13"	751	25
CP 10	41° 54' 56.32"	88° 15' 7.10"	751	25

**ACCESS NOTES**

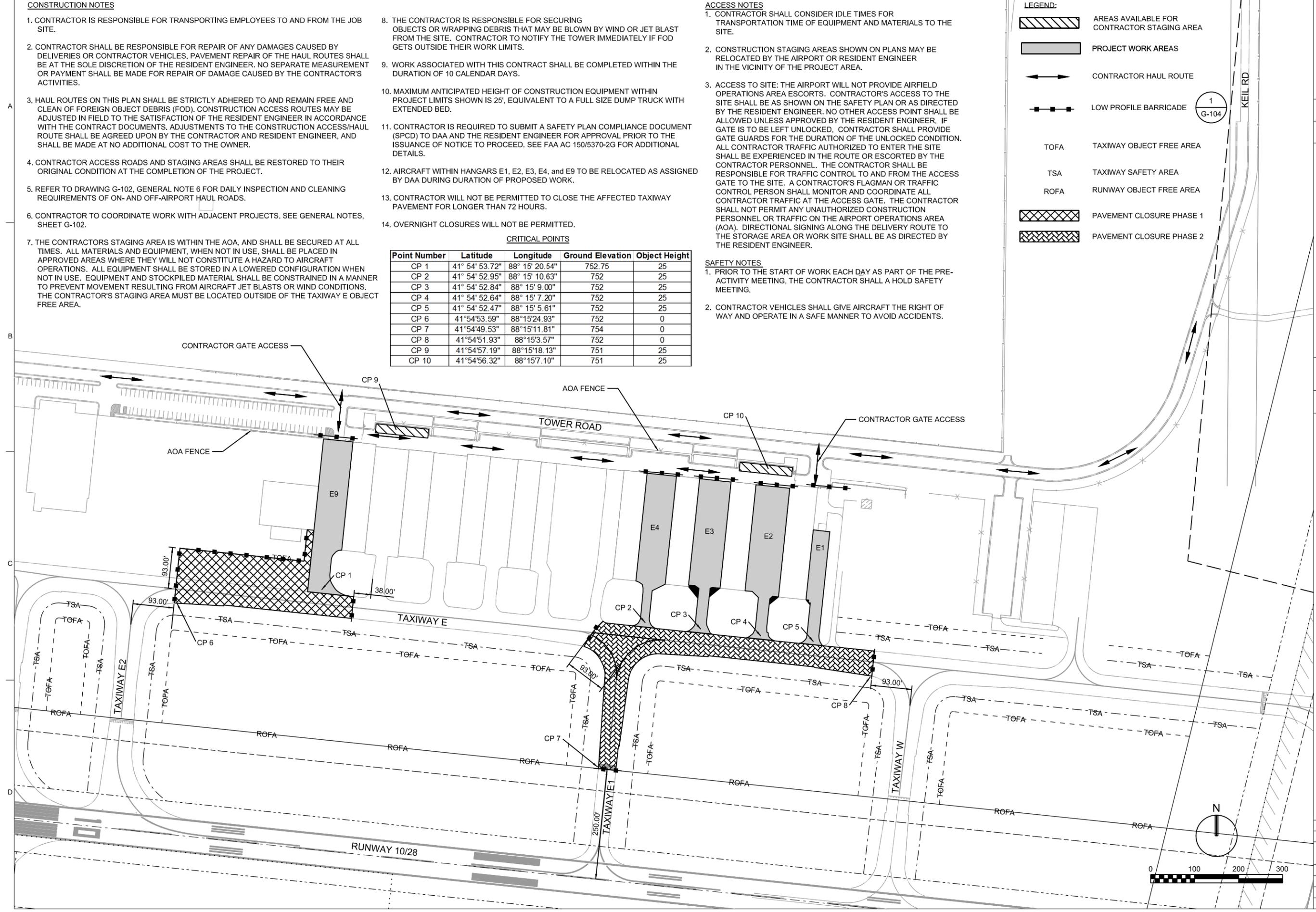
1. CONTRACTOR SHALL CONSIDER IDLE TIMES FOR TRANSPORTATION TIME OF EQUIPMENT AND MATERIALS TO THE SITE.
2. CONSTRUCTION STAGING AREAS SHOWN ON PLANS MAY BE RELOCATED BY THE AIRPORT OR RESIDENT ENGINEER IN THE VICINITY OF THE PROJECT AREA.
3. ACCESS TO SITE: THE AIRPORT WILL NOT PROVIDE AIRFIELD OPERATIONS AREA ESCORTS. CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. NO OTHER ACCESS POINT SHALL BE ALLOWED UNLESS APPROVED BY THE RESIDENT ENGINEER. IF GATE IS TO BE LEFT UNLOCKED, CONTRACTOR SHALL PROVIDE GATE GUARDS FOR THE DURATION OF THE UNLOCKED CONDITION. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR ESCORTED BY THE CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE ACCESS GATE TO THE SITE. A CONTRACTOR'S FLAGMAN OR TRAFFIC CONTROL PERSON SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT THE ACCESS GATE. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE AIRPORT OPERATIONS AREA (AOA). DIRECTIONAL SIGNING ALONG THE DELIVERY ROUTE TO THE STORAGE AREA OR WORK SITE SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.

**SAFETY NOTES**

1. PRIOR TO THE START OF WORK EACH DAY AS PART OF THE PRE-ACTIVITY MEETING, THE CONTRACTOR SHALL A HOLD SAFETY MEETING.
2. CONTRACTOR VEHICLES SHALL GIVE AIRCRAFT THE RIGHT OF WAY AND OPERATE IN A SAFE MANNER TO AVOID ACCIDENTS.

**LEGEND:**

-  AREAS AVAILABLE FOR CONTRACTOR STAGING AREA
-  PROJECT WORK AREAS
-  CONTRACTOR HAUL ROUTE
-  LOW PROFILE BARRICADE
-  TOFA TAXIWAY OBJECT FREE AREA
-  TSA TAXIWAY SAFETY AREA
-  ROFA RUNWAY OBJECT FREE AREA
-  PAVEMENT CLOSURE PHASE 1
-  PAVEMENT CLOSURE PHASE 2





8735 W. HIGGINS ROAD  
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CHICAGO, IL 60631

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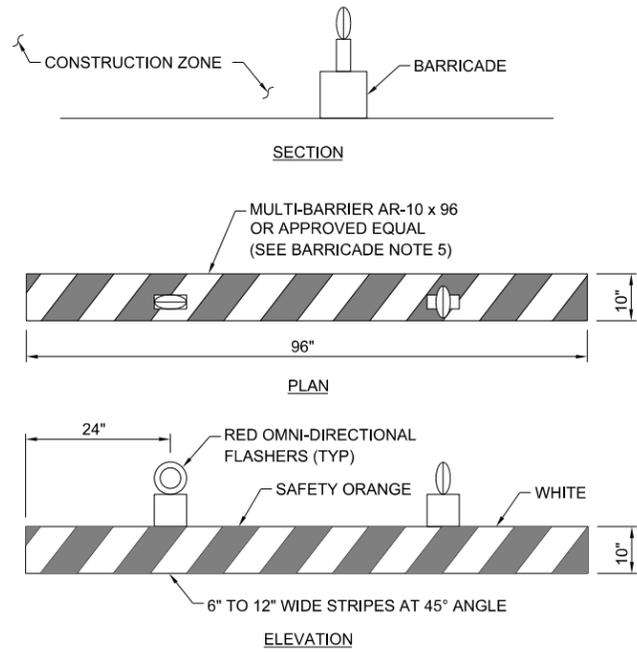
CONTRACTOR STAGING AND ACCESS ROUTE PLAN

AS SHOWN  
VERIFY SCALE  
BAR IS ONE INCH ON ORIGINAL DRAWING.

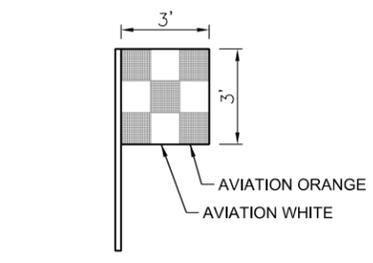
DATE 10/06/2017  
PROJ DPA-4557  
DWG G-103  
SHEET 4 of 12

FILENAME: PLOT DATE: 2018/01/23 PLOT TIME: 5:47:55 PM

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**1 LOW-PROFILE BARRICADE DETAIL**  
NTS



**2 VEHICLE FLAG**  
NTS

**BARRICADE NOTES:**

1. ALL AIRFIELD PAVEMENT CLOSURES SHALL BE TEMPORARY CLOSURES AS DESCRIBED IN FAA AC 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION." THE CONTRACTOR SHALL COMPLY WITH ALL THE REQUIREMENTS SET FORTH IN THE CONTRACT DOCUMENTS REGARDING AIRFIELD CLOSURES, SAFETY, AND SECURITY.
2. PRIOR TO REOPENING CLOSED AIRFIELD PAVEMENTS, THE ENTIRE SAFETY AREA MUST MEET FAA CRITERIA UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
3. FLASHERS TO BE BATTERY OPERATED AND/OR SOLAR POWERED. LENS TO BE RED AND BE ABLE TO ROTATE 90°.
4. FACING OF LOW-PROFILE BARRICADE TO BE COVERED WITH REFLECTIVE MATERIAL.
5. LOW-PROFILE BARRICADES TO BE PLACED END TO END ALONG OPERATIONAL PAVEMENT, ADJACENT TO CONSTRUCTION, AS DIRECTED BY THE RESIDENT ENGINEER.
6. FLASHERS SHALL BE SECURED TO THE BARRICADES, PER MANUFACTURER'S INSTRUCTIONS. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
7. LOW-PROFILE BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT'S COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW AS POSSIBLE, NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
8. THE CONTRACTOR SHALL PROVIDE BARRICADES WITH RED FLASHING BATTERY OPERATED LIGHTS AS SHOWN IN GOOD WORKING ORDER. CONTRACTOR SHALL MAINTAIN ALL BARRICADES AND ENSURE THEY ARE IN WORKING ORDER TWENTY-FOUR (24) HOURS A DAY, FOR THE DURATION OF THE PROJECT. THE BARRICADE LIGHTING SHALL BE CHECKED NIGHTLY BY THE CONTRACTOR.
9. THE LIGHTS SHALL BE OPERATIONAL AT EACH BARRICADE LOCATION AT ALL TIMES. ANY LIGHTS FOUND NON-OPERATIONAL SHALL BE REPAIRED IMMEDIATELY WITH EMERGENCY CONTACT NUMBER 24/7.
10. ALL BARRICADES SHALL BE CHECKED VISUALLY FOR SIGNS OF WEAR AND TEAR ON A DAILY BASIS AND SHALL BE REPAINTED OR REPLACED WHEN DEEMED APPROPRIATE BY THE RESIDENT ENGINEER. THE CONDITIONS OF LIGHTING UNITS SHALL BE CHECKED DAILY. ALL LIGHT FIXTURES SHALL BE VERIFIED IN OPERATING CONDITION AND GOOD WORKING ORDER BY THE CONTRACTOR ON A DAILY BASIS, BEFORE THE CONTRACTOR CEASES OPERATIONS FOR THE DAY.



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PHASE 1 PAVEMENT REHABILITATION OF  
APRON AREAS AT ECHO-T-HANGAR  
CONTRACT #DU083

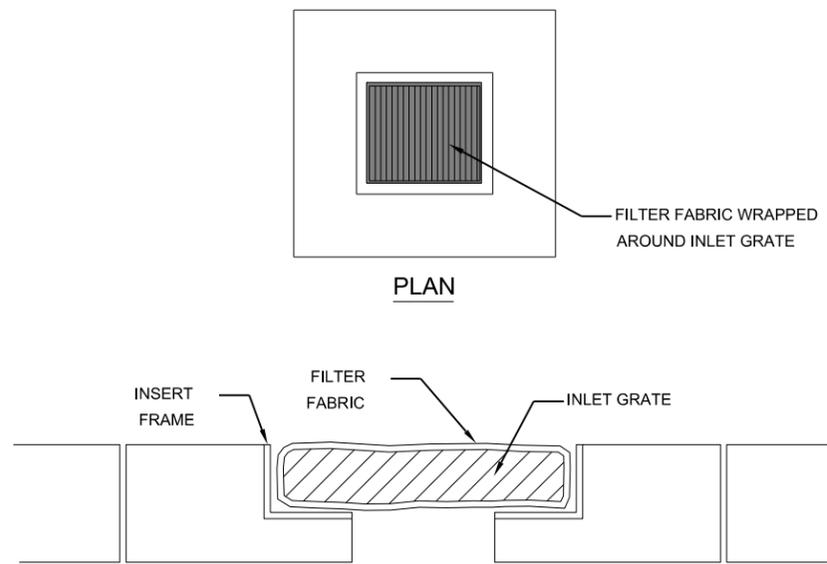
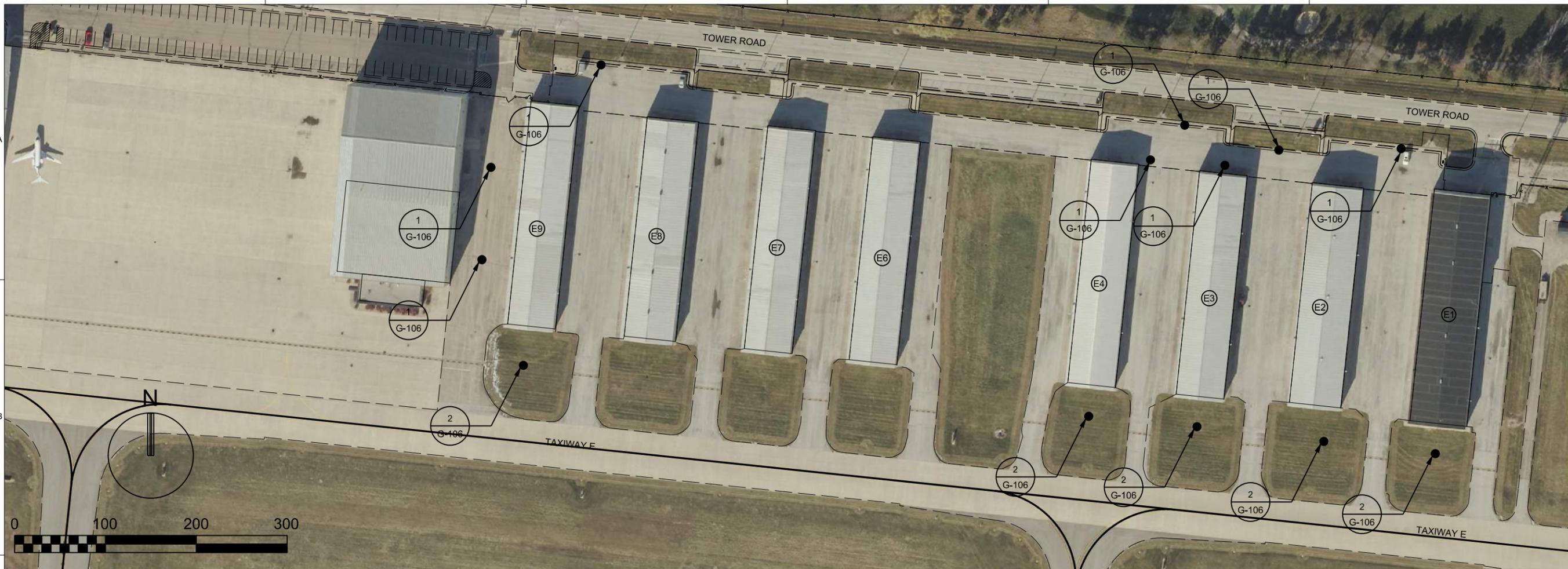
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**CONSTRUCTION NOTES  
AND DETAILS**

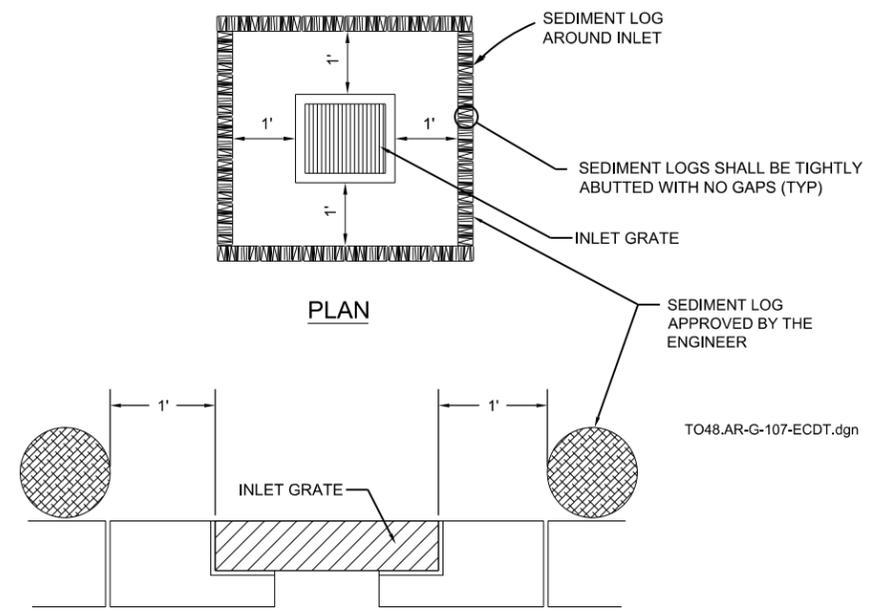
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**1** INLET PROTECTION FILTER FABRIC DETAIL  
NTS



**2** INLET PROTECTION SEDIMENT LOG DETAIL  
NTS

**DUPAGE AIRPORT AUTHORITY**

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PHASE 1 PAVEMENT REHABILITATION OF  
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SHEET	7 OF 12



1 2 3 4 5 6



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MATCHLINE - SEE SHEET C-100

N: 1,911,889.17  
E: 1,006,167.95

N: 1,911,876.01  
E: 1,006,293.13

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E: 1,006,429.41

N: 1,911,849.22  
E: 1,006,547.96

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E: 1,006,163.04

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E: 1,006,288.22

END WORK  
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E: 1,006,424.49

END WORK  
N: 1,911,714.14  
E: 1,006,533.85

1225 SY

519 SY

1282 SY

403 SY

378 SY

1265 SY

747 SY

872 SY

(E4)

(E3)

(E2)

(E1)

20 LF

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N: 1,911,584.66  
E: 1,006,297.89

N: 1,911,564.84  
E: 1,006,430.70

5  
C-102

4  
C-102

1  
C-103

(TYP)

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N: 1,911,564.78  
E: 1,006,273.17

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E: 1,006,493.26

N: 1,911,547.81  
E: 1,006,409.57

5  
C-102

5  
C-102

5  
C-102

BEGIN WORK  
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E: 1,006,126.94

BEGIN WORK  
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E: 1,006,252.13

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E: 1,006,388.42

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E: 1,006,506.98

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E: 1,006,249.53

N: 1,911,444.49  
E: 1,006,385.82

N: 1,911,432.06  
E: 1,006,504.38

81.00'

81.00'

81.00'

81.00'

13.66'

17.50'

17.50'

18.51'

19.49'

17.50'

17.50'

17.54'

27.59'

17.50'

17.50'

14.83'

6.62'

17.50'

13.80'

96.39'

LEGEND

(E6)

T-HANGAR  
NUMBER



LOCAL ONLY  
FUNDING LIMIT



NEW PAVEMENT  
SECTION  
(LOCAL FUNDING)



FEDERAL/STATE  
/LOCAL  
FUNDING SPLIT  
LIMIT



PAVEMENT  
SHEET FLOW

2  
C-102

4  
C-102

2  
C-102

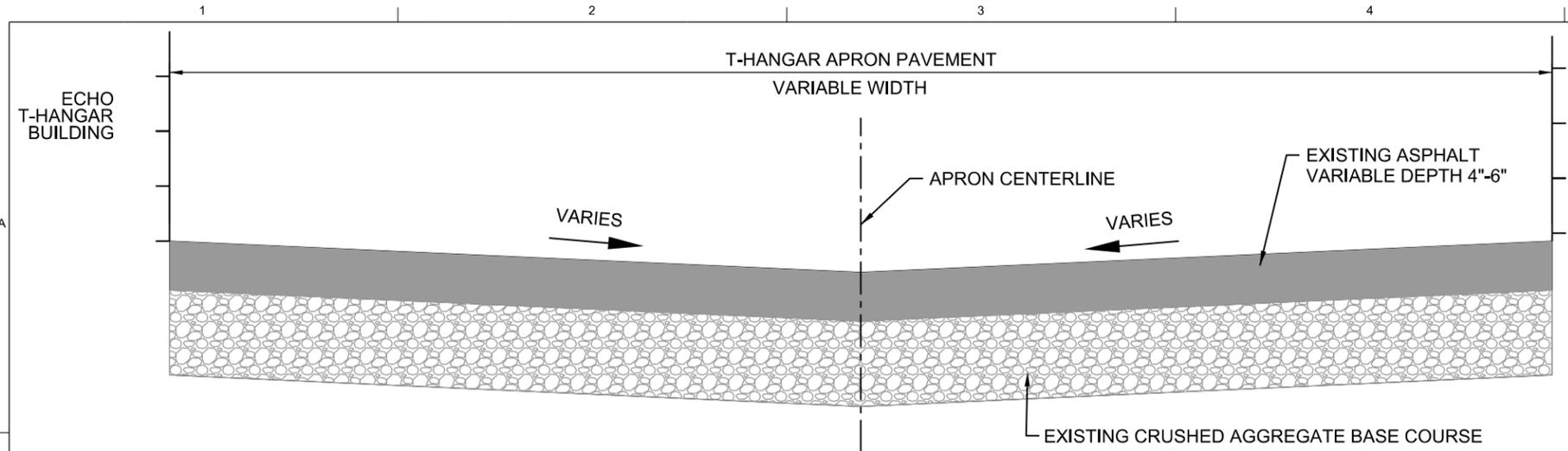
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PHASE 1 PAVEMENT REHABILITATION OF  
APRON AREAS AT ECHOT-HANGAR  
CONTRACT #DU083

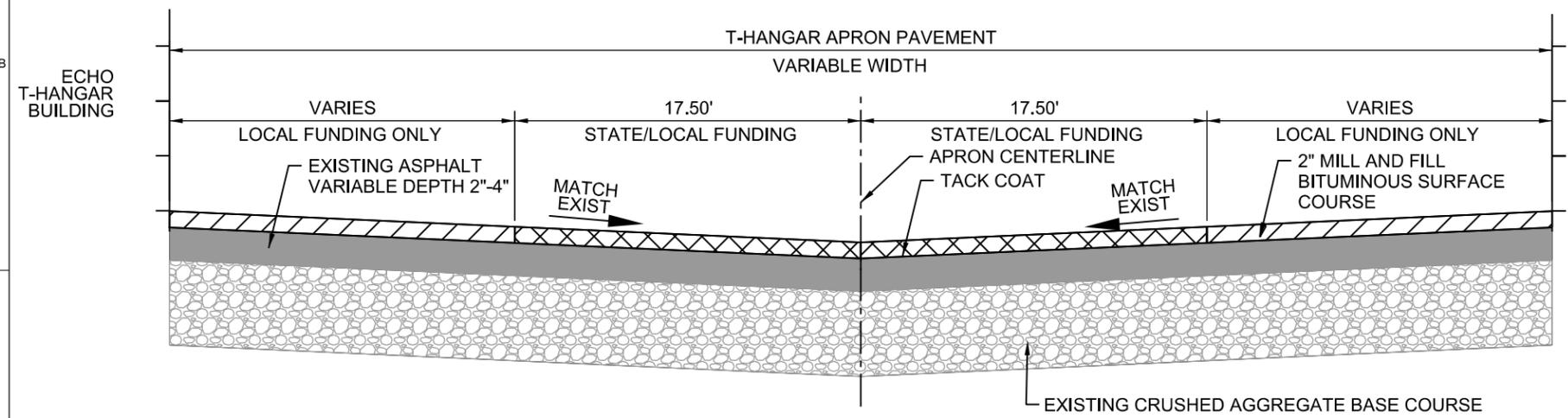
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PROJECT LOCATION PLAN

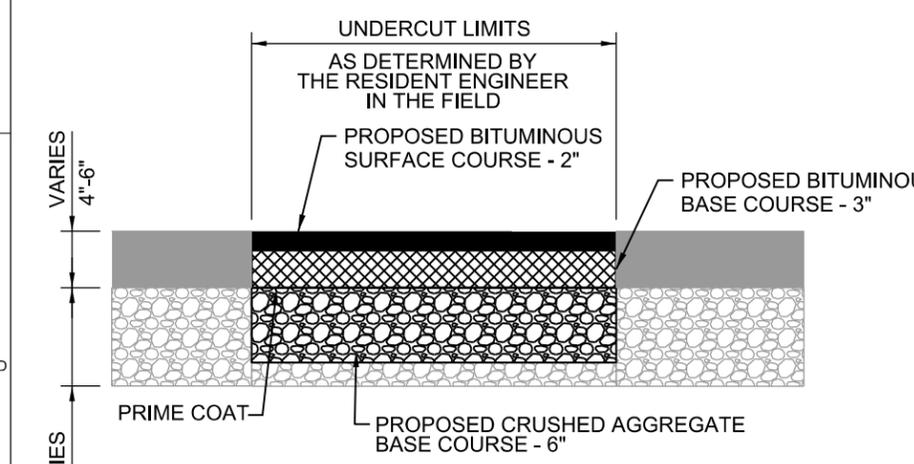
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DATE 10/06/2017
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DWG C-101
SHEET 9 OF 12



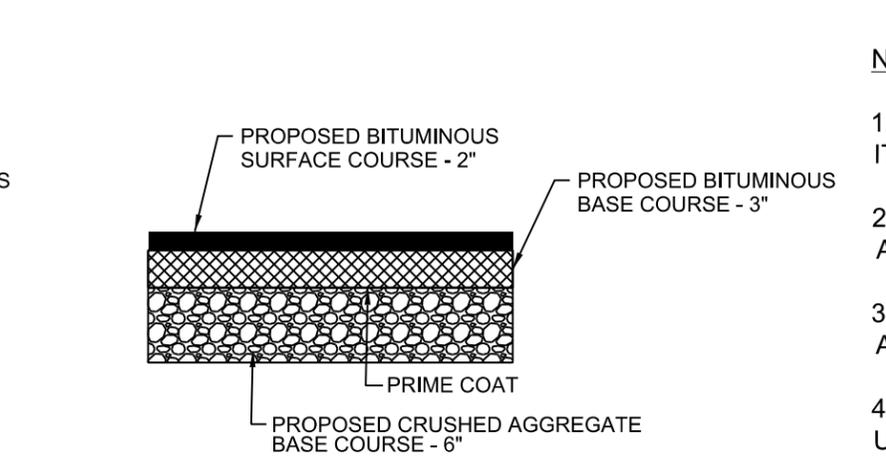
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NTS



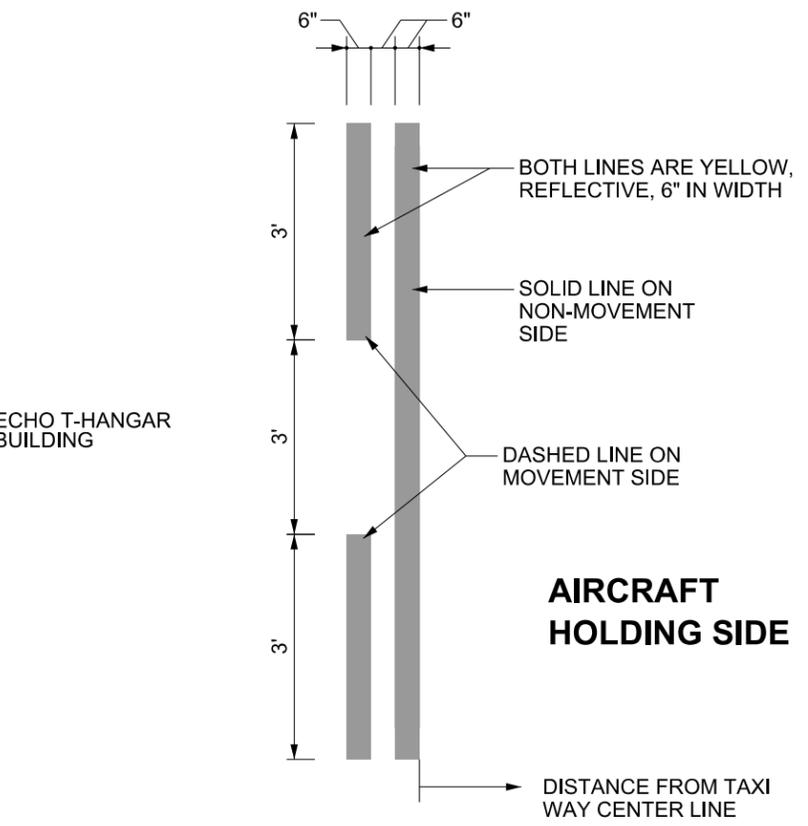
**2** MILL AND FILL SECTION  
NTS



**3** UNDERCUT SECTION  
NTS



**4** NEW PAVEMENT SECTION  
NTS



**5** NON-MOVEMENT AREA MARKINGS  
NTS

**NOTES:**

1. BITUMINOUS PAVEMENT MILLING SHALL BE PAID FOR UNDER ITEM AR401650.
2. BITUMINOUS SURFACE COURSE SHALL BE PAID FOR UNDER ITEM AR401610.
3. BITUMINOUS TACK COAT SHALL BE PAID FOR UNDER ITEM AR603510.
4. RESIDENT ENGINEER DIRECTED UNDERCUT SHALL BE PAID FOR UNDER AR152511 SUBGRADE REPAIR.
5. PRIME COAT AS SHOWN IN SECTIONS 3 AND 4 IS INCIDENTAL TO THE COST OF THE CONTRACT.

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PHASE 1 PAVEMENT REHABILITATION OF APRON AREAS AT ECHO T-HANGAR  
CONTRACT #DU083

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PLAN DETAILS

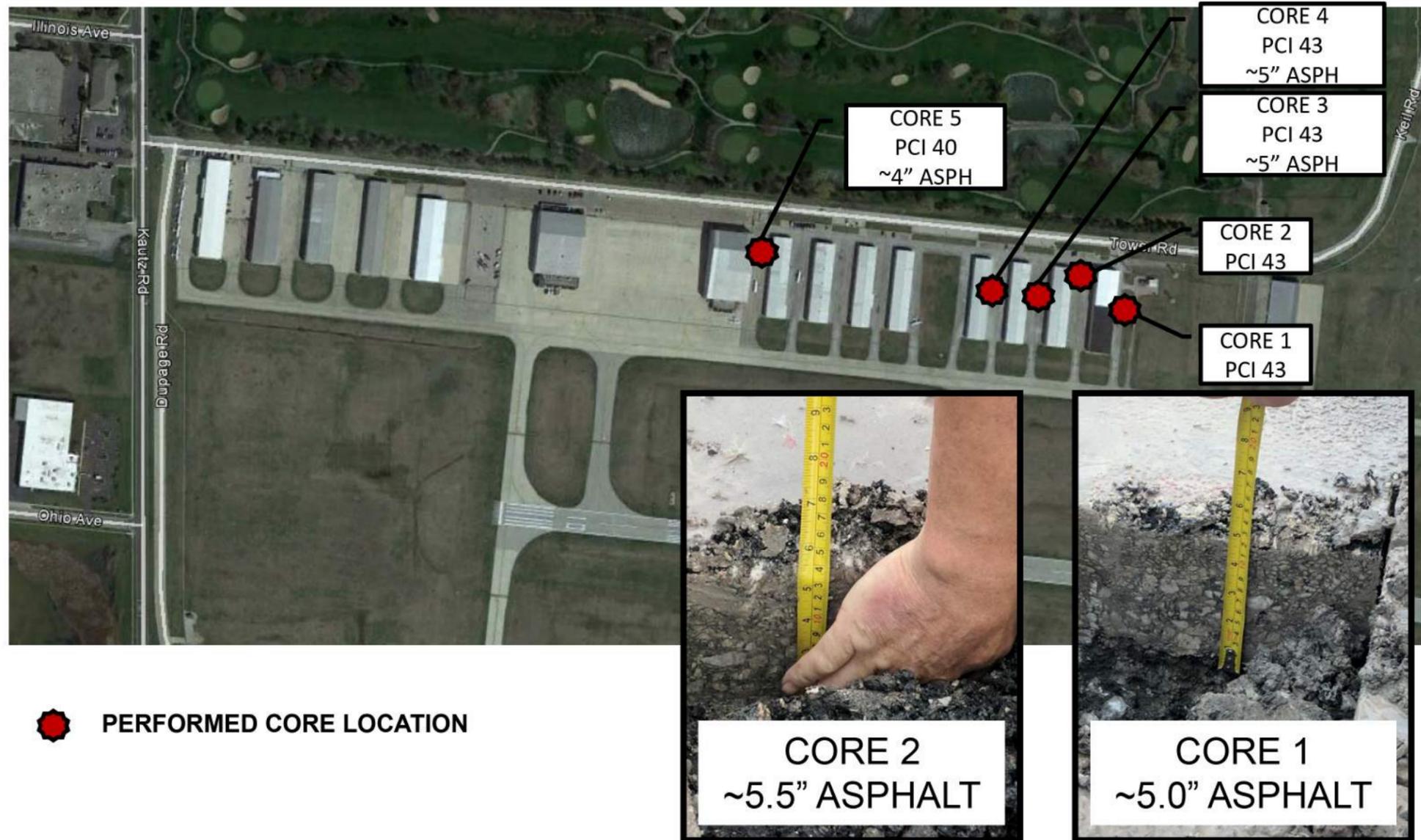
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# Existing Pavement Cores



**1** EXISTING PAVEMENT CORE DATA - FOR INFORMATION ONLY  
NTS

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PHASE 1 PAVEMENT REHABILITATION OF  
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PLAN DETAILS

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SHEET	12 of 12