

FOR INDEX OF SHEETS, SEE SHEET 2
FOR LIST OF STATE STANDARDS, SEE SHEET 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

**PLANS FOR PROPOSED
FEDERAL-AID HIGHWAY**

MUN ROUTE 0500 (163RD ST /MEADOWVIEW DRIVE)
94TH ST TO 88TH ST
FAU ROUTE 1037 (HAVEN AVENUE)
167TH ST TO 163RD ST /MEADOWVIEW DRIVE
RESURFACING, SIDEWALKS, PAVEMENT MARKING
SECTION 14-00025-00-RS
PROJECT 3ZL7(104)
VILLAGE OF ORLAND HILLS
COOK COUNTY
C-91-267-14

MUN RTE 0500 1037	SECTION 14-00025-00-RS	COUNTY ILLINOIS COOK	TOTAL SHEETS 17	SHEET 1
CONTRACT NO. 61E50				

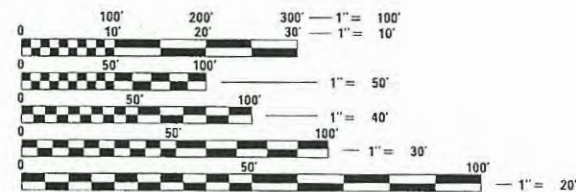


PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLER, P.E., 847-705-4406, SCHAUMBURG, IL

TRAFFIC DATA

163RD ST./MEADOWVIEW DR. (FAU 0500)
MINOR COLLECTOR
TRAFFIC = 1,900 ADT (2014)
2% TRUCKS
POSTED SPEED = 20 MPH
DESIGN SPEED = 20 MPH

HAVEN AVE. (FAU 1037)
MINOR COLLECTOR
TRAFFIC = 950 ADT (2014)
2% TRUCKS
POSTED SPEED = 20 MPH
DESIGN SPEED = 20 MPH



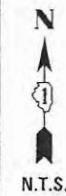
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CALL JULIE 811
WITH THE FOLLOWING:
COUNTY COOK
CITY-TOWNSHIP ORLAND HILLS/ORLAND
SECTION & RANGE T41N-R10E Sec E10, S2, N14, W12
48 HOURS BEFORE YOU DIG.
EXCLUDING SAT., SUN., & HOLIDAYS

MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR
LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-287-7672.



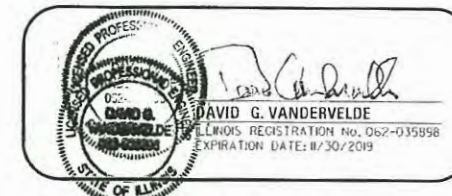
ORLAND TOWNSHIP
LOCATION MAP
GROSS LENGTH = 7,132 FT. = 1.35 MILE
NET LENGTH = 7,132 FT. = 1.35 MILE



END IMPROVEMENTS
163RD ST./MEADOWVIEW DR.
STA. 51+52.00

END IMPROVEMENTS
HAVEN AVENUE
STA. 132+36.00

BEGIN IMPROVEMENTS
HAVEN AVENUE
STA. 101+51.00



CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500
PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/19

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED *Ryan K. Hastings*
NOVEMBER 27, 2017
VILLAGE OF ORLAND HILLS
PRESIDENT

PASSED *CHRISTOPHER HOLT*
DECEMBER 12, 2017
DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW *Anthony J. Quinly (PES)*
DECEMBER 12, 2017
REGIONAL ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

PAGE	TITLE
1	COVER SHEET
2	GENERAL NOTES, INDEX OF SHEETS, INDEX OF STANDARDS
3	SUMMARY OF QUANTITIES
4	TYPICAL SECTIONS
5-11	ROADWAY PLANS
12	CONSTRUCTION DETAILS
13-17	DISTRICT 1 DETAILS

HIGHWAY STANDARDS

000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-10	PERPENDICULAR CURB RAMPS
424016-04	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424026-02	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
604001-04	FRAME AND LIDS TYPE 1
604036-03	GRATE TYPE 8
604056-04	FRAME AND GRATE TYPE 11V
606001-07	CONC. CURB TYPE B AND COMB. CONC CURB AND GUTTER
701001-02	OFF-ROAD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 M) AWAY
701006-05	OFF-ROAD OPERATIONS, 2L, 2W, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-07	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A AND B METAL POSTS (FOR SIGNS & MARKERS)
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2018; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JUNE 2014 SEVENTH EDITION; THE "ILLINOIS URBAN MANUAL " AND THE "ILLINOIS URBAN MANUAL FIELD MANUAL FOR INSPECTION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES"; THE "AMERICANS WITH DISABILITIES ACT OF 1990 ACCESSIBILITY GUIDELINES"; THE "DRAFT" REHABILITATION ACT OF 1973 (SECTION 504); THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES; THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.

CODES OF THE IEPA TITLE 35, AND O.S.H.A. SHALL BE ADHERED TO FOR THE CONSTRUCTION OF THIS PROJECT. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE ADHERENCE TO THESE (NOT THE VILLAGE OF ORLAND HILLS OR CHRISTOPHER B. BURKE ENGINEERING).

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHEETING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS BEFORE CONSTRUCTION BEGINS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE AGENCY, AT THE CONTRACTOR'S OWN EXPENSE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF ADEQUATE SIGNS AND WARNING DEVICES TO INFORM AND PROTECT THE PUBLIC.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATION IS REQUIRED.)

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT WATER DEPARTMENT FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT.

MISCELLANEOUS

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

AGGREGATE BASE REPAIR, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLAN QUANTITY.

THE THICKNESS OF EXISTING PAVEMENT TO BE REMOVED HAS BEEN ESTIMATED FROM PAVEMENT CORES. THE PAVEMENT CORING GEOTECHNICAL REPORT IS ATTACHED TO THE PROJECT SPECIFICATIONS. THE VILLAGE DOES NOT GUARANTEE ITS ACCURACY. NO ADJUSTMENTS TO PLAN QUANTITIES SHALL BE MADE FOR VARIATIONS IN PAVEMENT THICKNESS.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO RESIDENTIAL DRIVEWAYS AT ALL TIMES. THIS WORK SHALL BE COMPLETED AND PAID FOR IN ACCORDANCE WITH THE SPECIAL PROVISION FOR "AGGREGATE SURFACE COURSE FOR TEMPORARY ACCESS". AGGREGATE RAMPS SHALL BE CONSTRUCTED FROM THE EXISTING ROADWAY AGGREGATE BASE TO THE EXISTING CURB AND GUTTER. THE GRADE OF THE AGGREGATE RAMPS SHALL NOT EXCEED 12%.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS AS DIRECTED BY THE ENGINEER.

CURB AND GUTTER TO BE REMOVED AT DRIVEWAYS SHALL BE REPLACED WITH DEPRESSED CURB AND GUTTER.

TEMPERATURE CONTROL FOR CONCRETE PLACEMENT; DELETE THE SECOND AND THIRD SENTENCES OF THE SECOND PARAGRAPH OF ARTICLE 1020.14 (a) OF THE STANDARD SPECIFICATIONS.

PAVING

HOT-MIX ASPHALT BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED. THE THICKNESSES SHOWN ON THE PLANS ARE THE MINIMUM ACCEPTABLE THICKNESSES.

LANDSCAPING

CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

WHEN DIRECTED BY THE ENGINEER, SUPPLEMENTAL WATERING SHALL BE APPLIED TO ALL SODDED AREAS PRIOR TO FINAL ACCEPTANCE AT A RATE SPECIFIED BY THE ENGINEER.

THE CONTRACTOR SHALL ADHERE TO LIMITS OF RESTORATION SHOWN. AREAS OUTSIDE THESE LIMITS THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE RESTORED BY THE CONTRACTOR.

CONTRACTOR SHALL BE CAUTIOUS NOT TO DISTURB ANY PARKWAY TREES DURING CONSTRUCTION. ANY TREES THAT ARE DAMAGED OR DISTURBED DURING THE CONSTRUCTION SHALL BE REPLACED IN KINDS BY THE CONTRACTOR AT HIS OWN EXPENSE. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ORLAND HILLS 163RD ST/MEADOWVIEW DRIVE GENERAL NOTES	MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\0rlandHills\170439\Civ1\GEN.1.170439.sht		DRAWN -	REVISED -			0500	14-00025-00-R5	COOK	17	2	
		CHECKED -	REVISED -			1037					CONTRACT NO. 61E50
Default	PLOT DATE = 12/14/2017	DATE -	REVISED -			SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT	

CONSTRUCTION CODE 0005

S.P.	SI	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
		20101700	SUPPLEMENTAL WATERING	UNIT	1
		20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	250
		28000510	INLET FILTERS	EACH	55
		31101180	SUBBASE GRANULAR MATERIAL, TYPE B 2"	SQ YD	1209
		35800100	PREPARATION OF BASE	SQ YD	26600
		35800200	AGGREGATE BASE REPAIR	TON	750
		40600990	TEMPORARY RAMP	SQ YD	100
		40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	4000
		40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	3000
		42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD	82
		42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	10880
*		42400800	DETECTABLE WARNINGS	SQ FT	1100
		44000100	PAVEMENT REMOVAL	SQ YD	26500
		44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	225
		44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	80
		44000600	SIDEWALK REMOVAL	SQ FT	10400
		60250200	CATCH BASINS TO BE ADJUSTED	EACH	38
		60255500	MANHOLES TO BE ADJUSTED	EACH	2

S.P. = SPECIAL PROVISION
SI = SPECIALTY ITEM

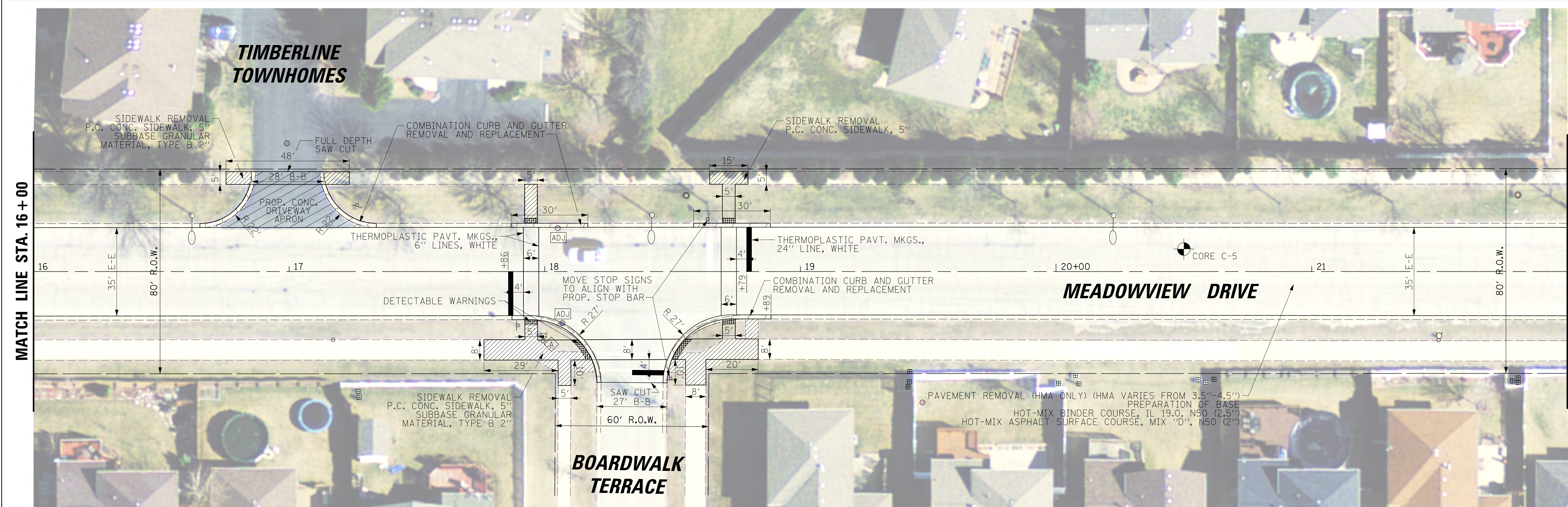
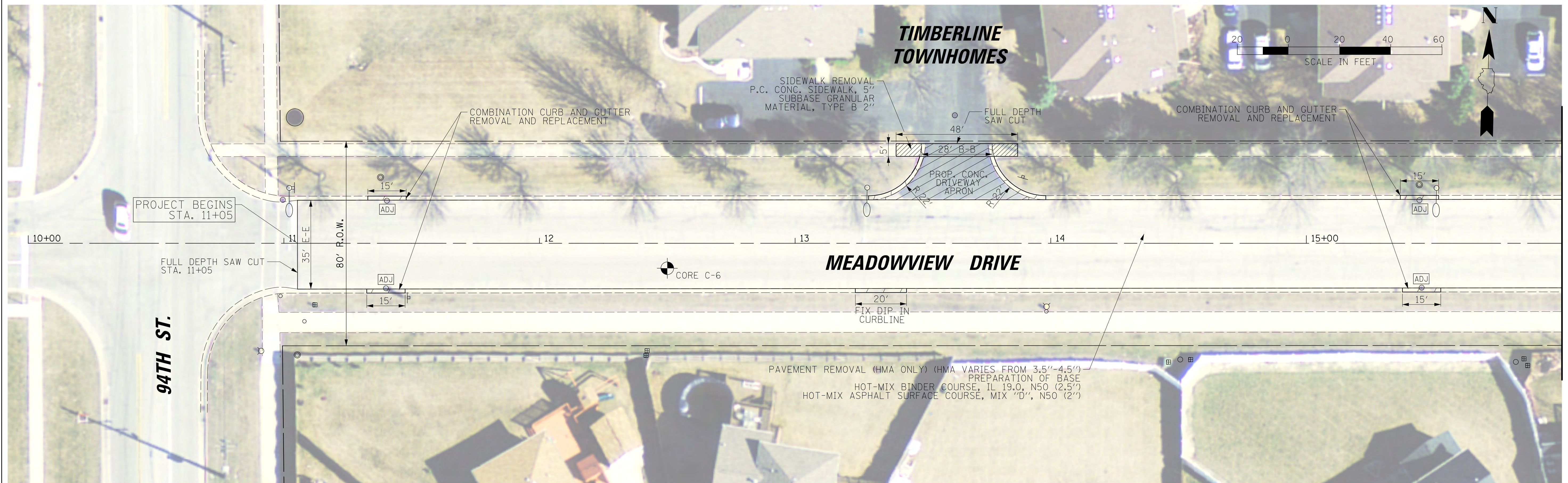
S.P.	SI	CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
		60260100	INLETS TO BE ADJUSTED	EACH	11
		60266600	VALVE BOXES TO BE ADJUSTED	EACH	6
		67100100	MOBILIZATION	LSUM	1
		70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	LSUM	1
		70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	LSUM	1
		72000100	SIGN PANEL - TYPE 1	SQ FT	6
		72900100	METAL POST - TYPE A	FOOT	10
	*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3200
	*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	515
	*	Z0004518	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 5"	SQ YD	40
	*	Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2745
	*	Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	48
	*	X0326806	WASHOUT BASIN	LSUM	1
	*	X2520700	SODDING, SPECIAL	SQ YD	650
	*	X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH	56
	*	X6026050	SANITARY MANHOLES TO BE ADJUSTED	EACH	3
	*	XX007278	PARKWAY RESTORATION	SQ YD	13

FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -
N:\D:\landhills\178439\Civil\500.1\178439.dwt		DRAWN -	REVISED -
Default	PLOT SCALE = 20'	CHECKED -	REVISED -
	PLOT DATE = 12/14/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

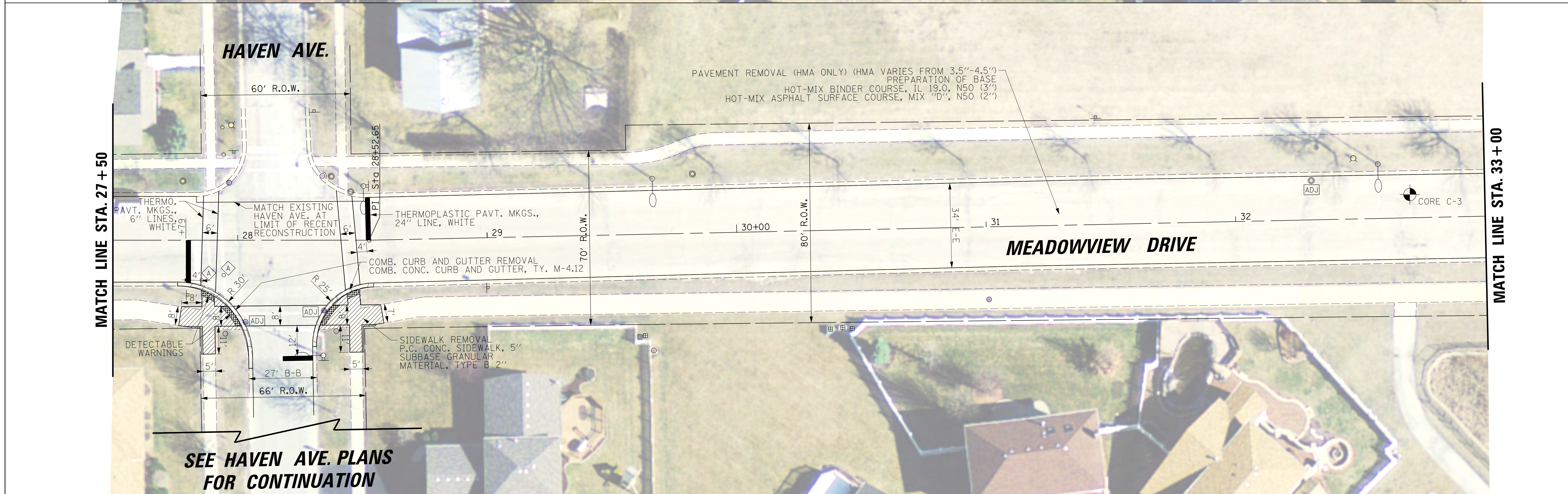
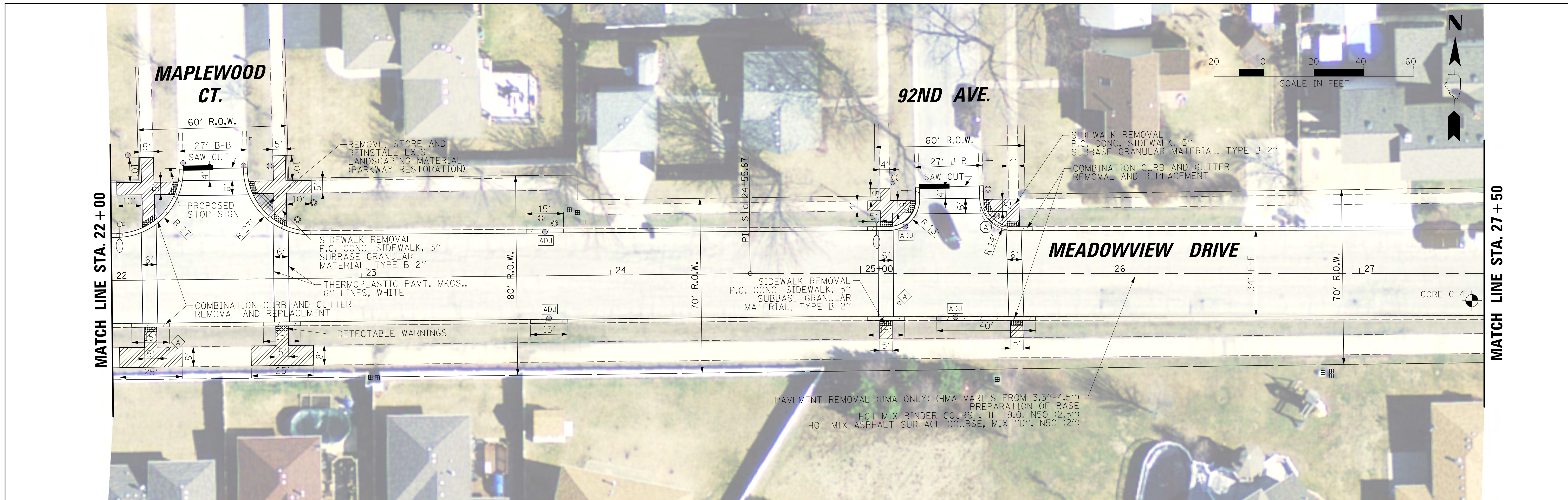
ORLAND HILLS 163RD ST/MEADOWVIEW DRIVE		MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SUMMARY OF QUANTITIES		0500	14-00025-00-RS	COOK	17	3
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 61E50		

ILLINOIS FED. AID PROJECT	
---------------------------	--



FILE NAME =	USER NAME = jpatrick	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF ORLAND HILLS 163RD ST./MEADOWVIEW DRIVE AND HAVEN AVENUE ROADWAY PLAN	MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\0rlandHills\170439\Civil\MEAD01.170439.dwg	PLOT SCALE = 20'	DRAWN -	REVISED -			0500	14-00025-00-RS	COOK	17	5	
Default	PLOT DATE = 12/14/2017	CHECKED -	REVISED -			CONTRACT NO. 61E50					
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

SCALE: 1" = 20' SHEET 1 OF 7 SHEETS STA. 10+00.00 TO STA. 22+00.00



**SEE HAVEN AVE. PLANS
FOR CONTINUATION**

FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -
N:\0-landHills\170439\Civi1\MEAD2.170439.dwg		DRAWN -	REVISED -
Default	PLOT SCALE = 20'	CHECKED -	REVISED -
	PLOT DATE = 12/14/2017	DATE -	REVISED -

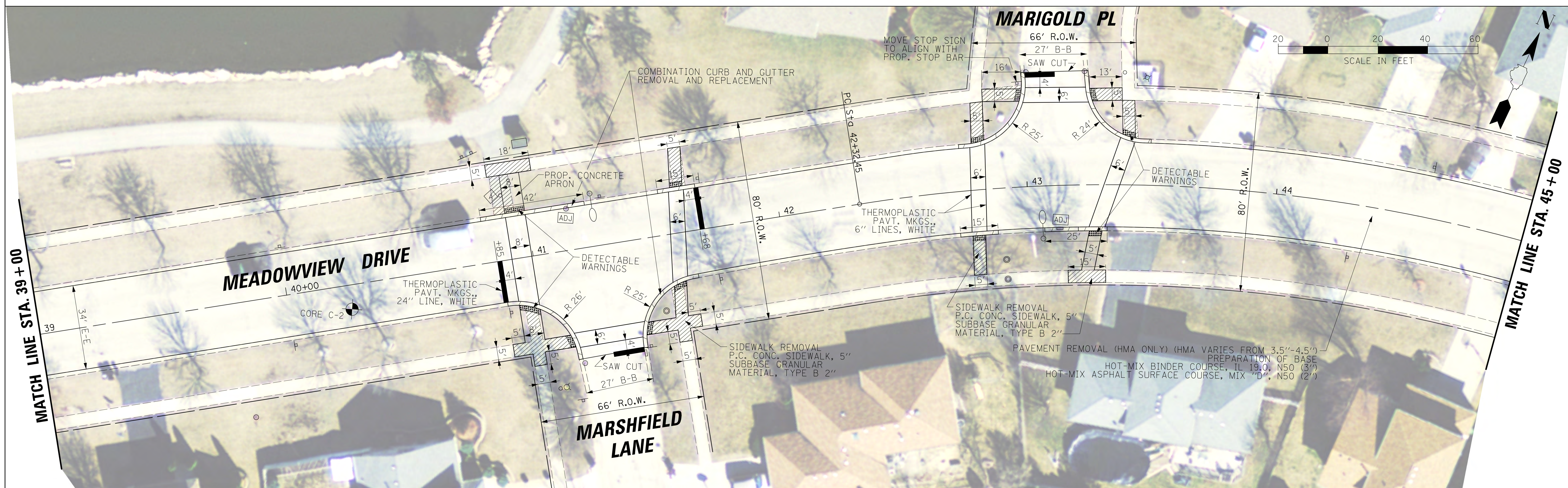
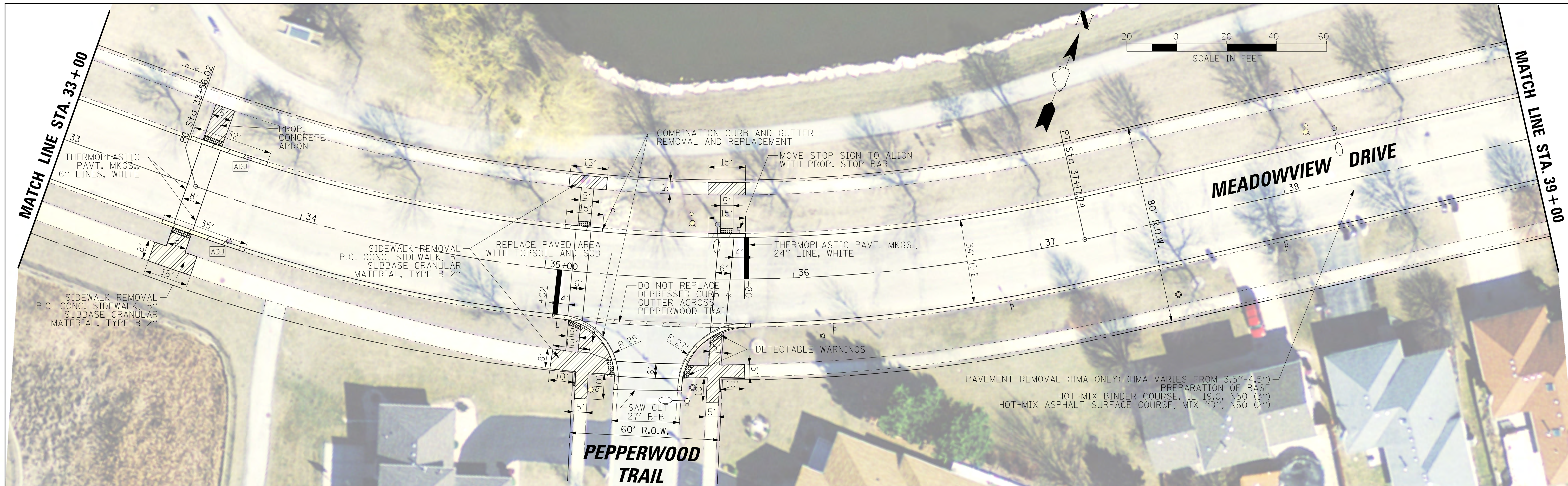
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF ORLAND HILLS
163RD ST./MEADOWVIEW DRIVE AND HAVEN AVENUE
ROADWAY PLAN**

MUN. RTE. 0500 1037	SECTION 14-00025-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 6
CONTRACT NO. 61E50				

SCALE: 1" = 20' SHEET 2 OF 7 SHEETS STA. 22+00.00 TO STA. 33+00.00

ILLINOIS FED. AID PROJECT



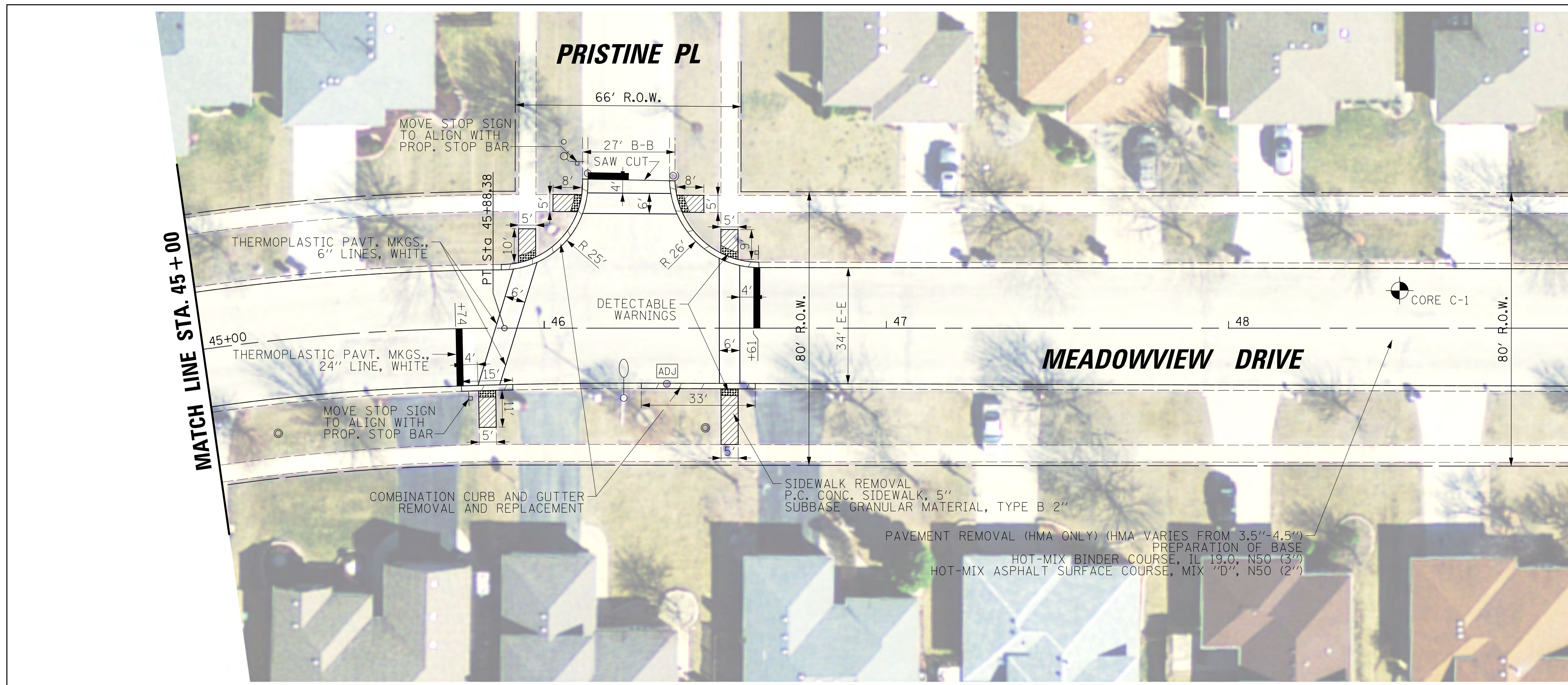
FILE NAME =	USER NAME = jpatrick	DESIGNED -	REVISED -
N:\0rlandHills\170439\Civil\MEAD03.170439.dwg		DRAWN -	REVISED -
Default	PLOT SCALE = 20'	CHECKED -	REVISED -
	PLOT DATE = 12/14/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

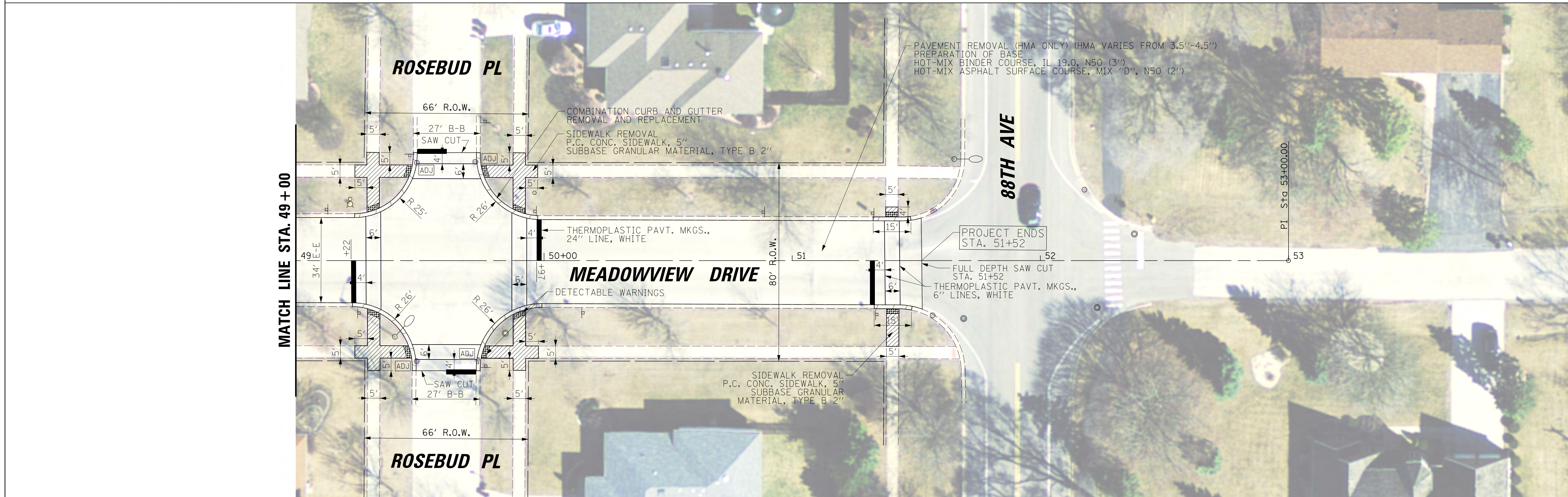
**VILLAGE OF ORLAND HILLS
163RD ST./MEADOWVIEW DRIVE AND HAVEN AVENUE
ROADWAY PLAN**

SCALE: 1" = 20' SHEET 3 OF 7 SHEETS STA. 33+00.00 TO STA. 45+00.00

MUN. R.T.E. 0500 1037	SECTION 14-00025-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 7
CONTRACT NO. 61E50			ILLINOIS FED. AID PROJECT	

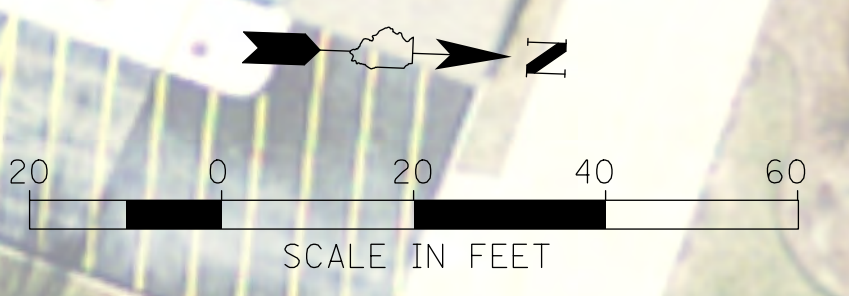
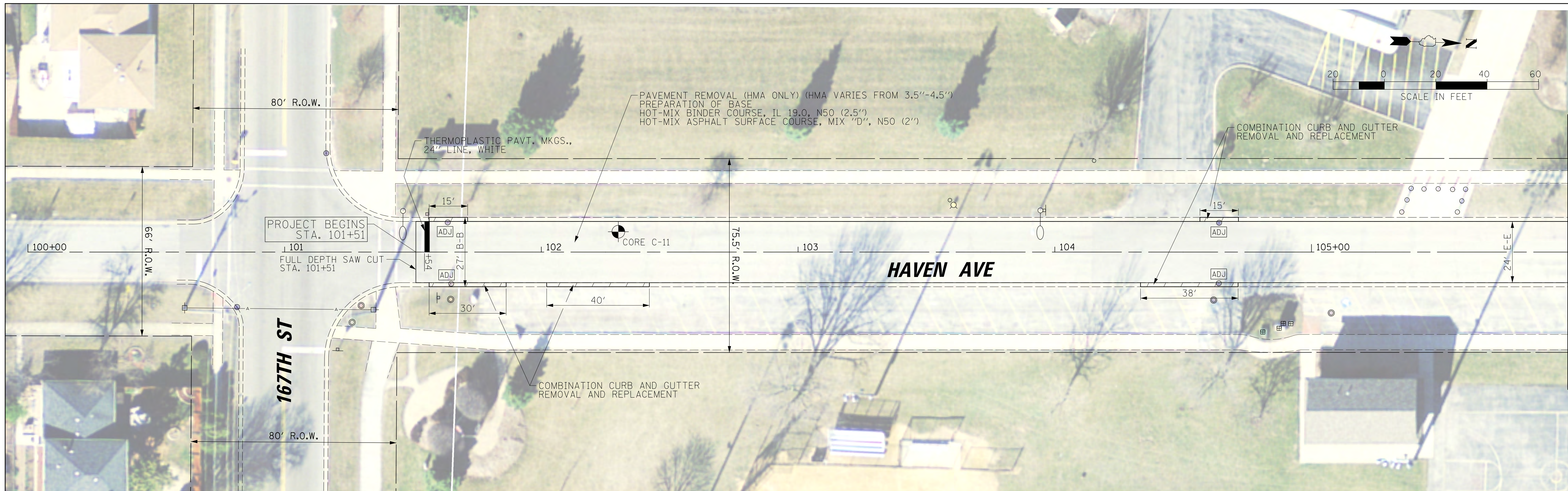


MATCH LINE STA. 49 + 00

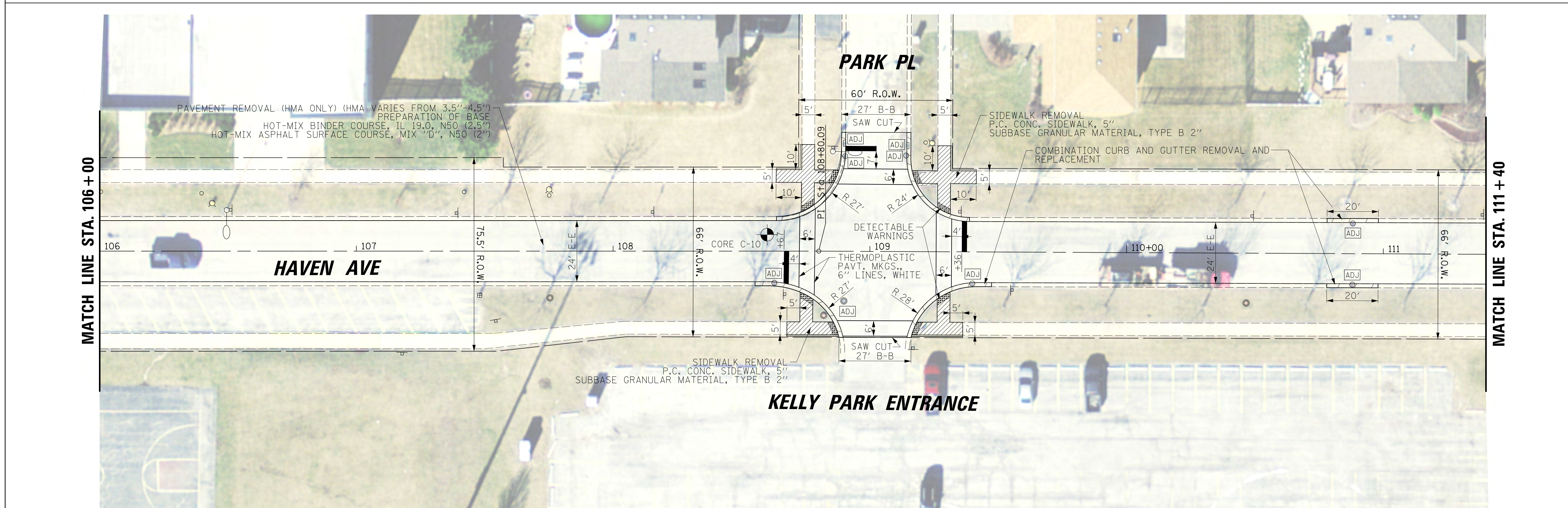


MATCH LINE STA. 49 + 00

FILE NAME = N:\0-landHills\170439\Civi1\MEAD4.170439.plt	USER NAME = jpatrick	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF ORLAND HILLS 163RD ST./MEADOWVIEW DRIVE AND HAVEN AVENUE ROADWAY PLAN	MUN. RTE. 0500 1037	SECTION 14-00025-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 8	
Default	PLOT SCALE = 20'	CHECKED -	REVISED -			SCALE: 1" = 20'	SHEET 4 OF 7 SHEETS	CONTRACT NO. 61E50		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 12/14/2017	DATE -	REVISED -			STA. 45+00.00 TO STA. 53+00.00					



MATCH LINE STA. 106+00



MATCH LINE STA. 106+00

MATCH LINE STA. 111+40

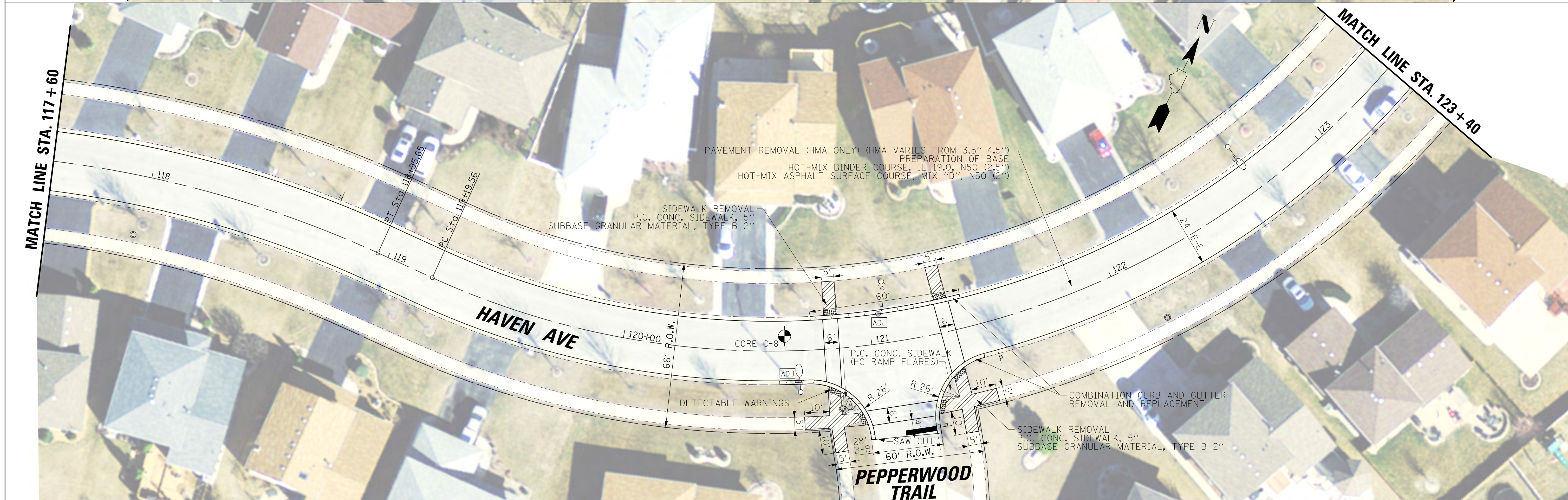
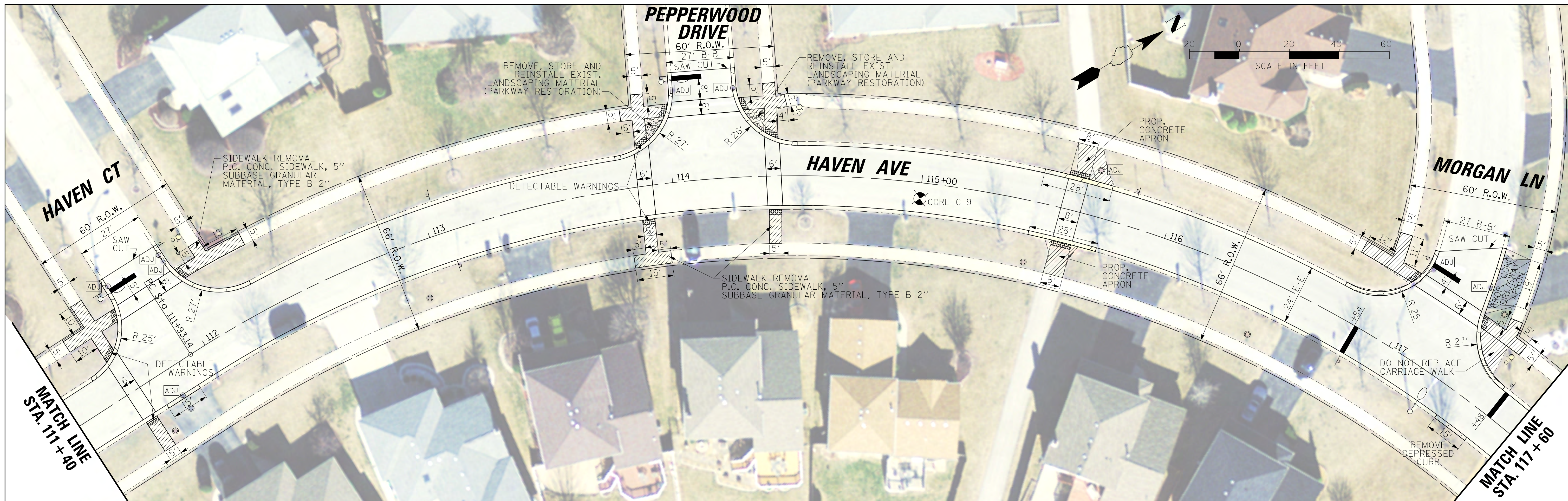
FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -
N:\0-LandHills\170439\Civi1\Haven1.170439.dwt		DRAWN -	REVISED -
Default	PLOT SCALE = 20'	CHECKED -	REVISED -
	PLOT DATE = 12/14/2017	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

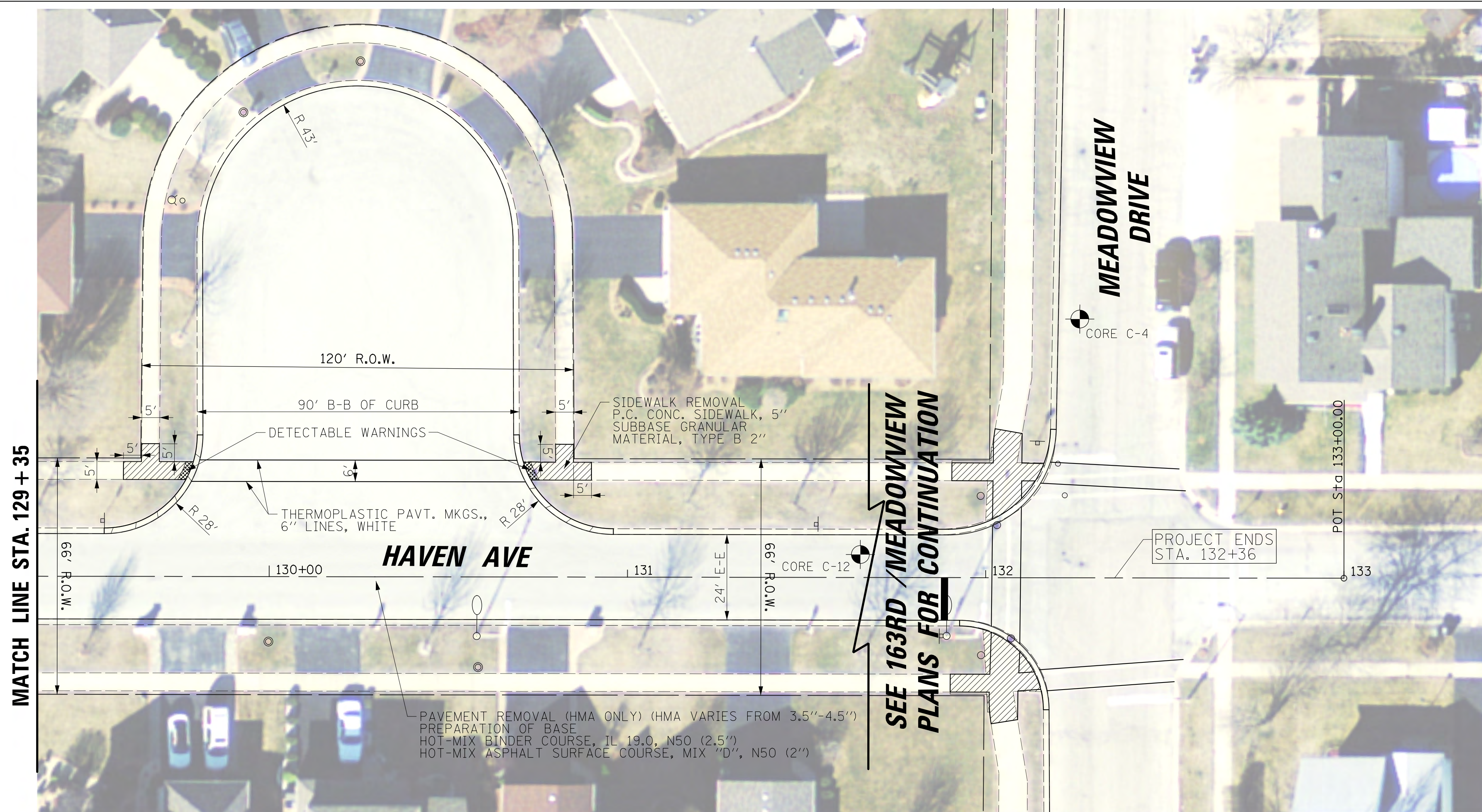
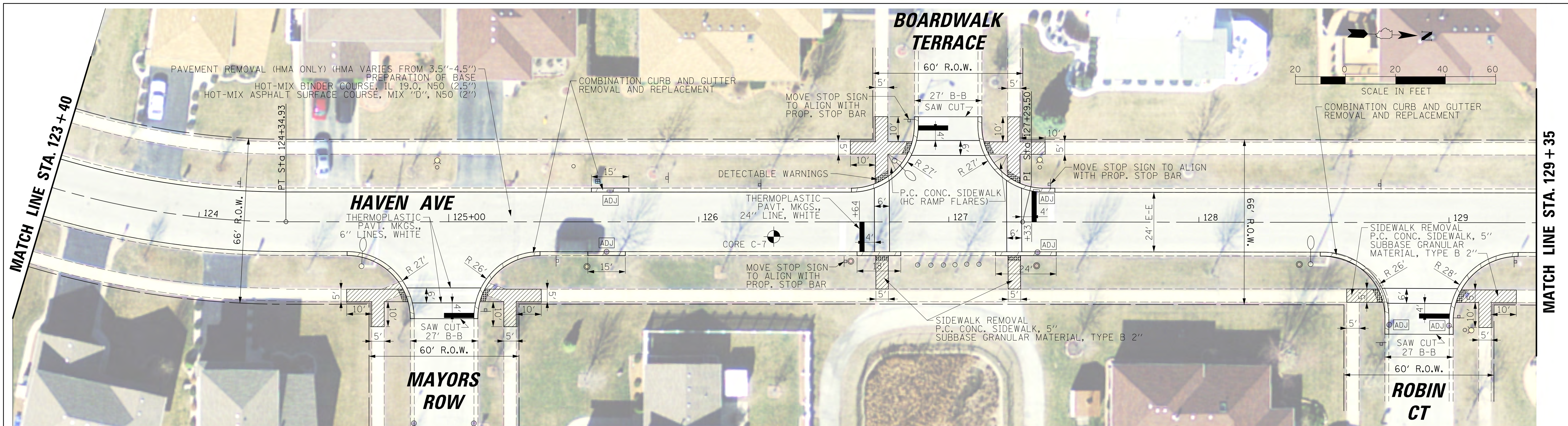
**VILLAGE OF ORLAND HILLS
163RD ST./MEADOWVIEW DRIVE AND HAVEN AVENUE
ROADWAY PLAN**

SCALE: 1" = 20' SHEET 5 OF 7 SHEETS STA. 100+00.00 TO STA. 111+40.00

MUN. RTE. 0500 1037	SECTION 14-00025-00-R5	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 9
CONTRACT NO. 61E50			ILLINOIS FED. AID PROJECT	



FILE NAME = N:\0rLandHills\170439\Civi1\Haven2.170439.sht	USER NAME = jstrick	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF ORLAND HILLS 163RD ST./MEADOWVIEW DRIVE AND HAVEN AVENUE ROADWAY PLAN	MUN. RTE. 0500 1037	SECTION 14-00025-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 10		
PLOT SCALE = 20'	CHECKED -	REVISED -	SCALE: 1" = 20'			SHEET 6 OF 7 SHEETS	STA. 111+40.00 TO STA. 123+40.00	CONTRACT NO. 61E50		ILLINOIS FED. AID PROJECT		
Default	PLOT DATE = 12/14/2017	DATE -	REVISED -									



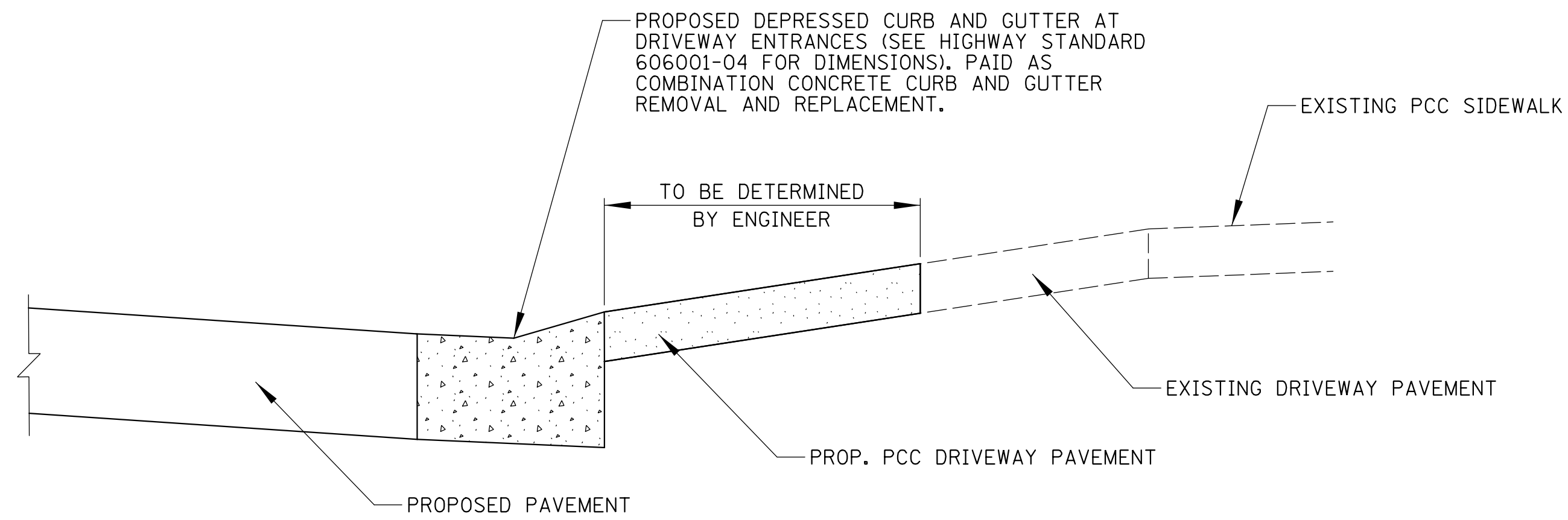
FILE NAME =	USER NAME = jstrick	DESIGNED -	REVISED -
N:\0r\landHills\170439\Civi1\Haven3.170439.sht		DRAWN -	REVISED -
Default	PLOT SCALE = 20'	CHECKED -	REVISED -
	PLOT DATE = 12/14/2017	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

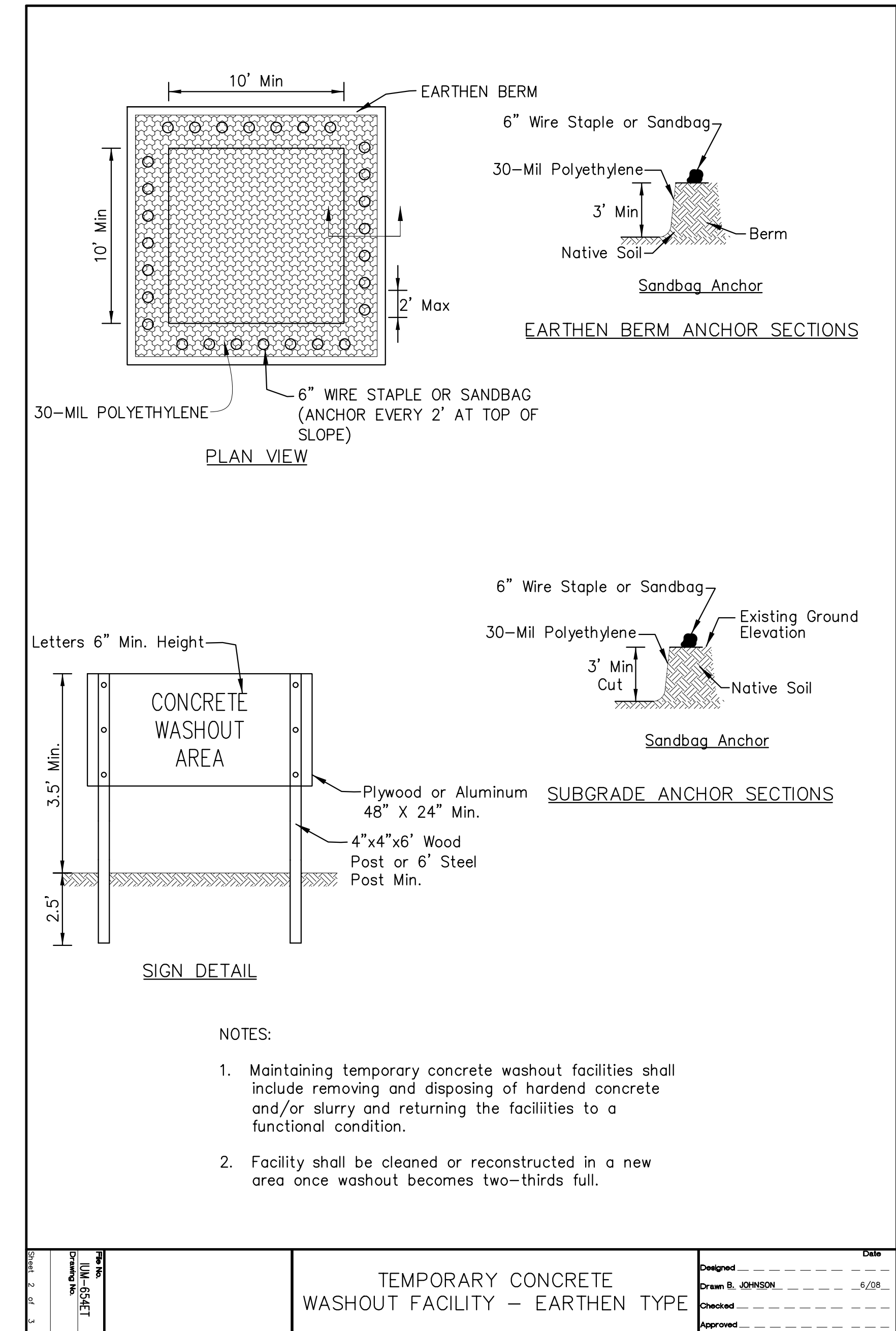
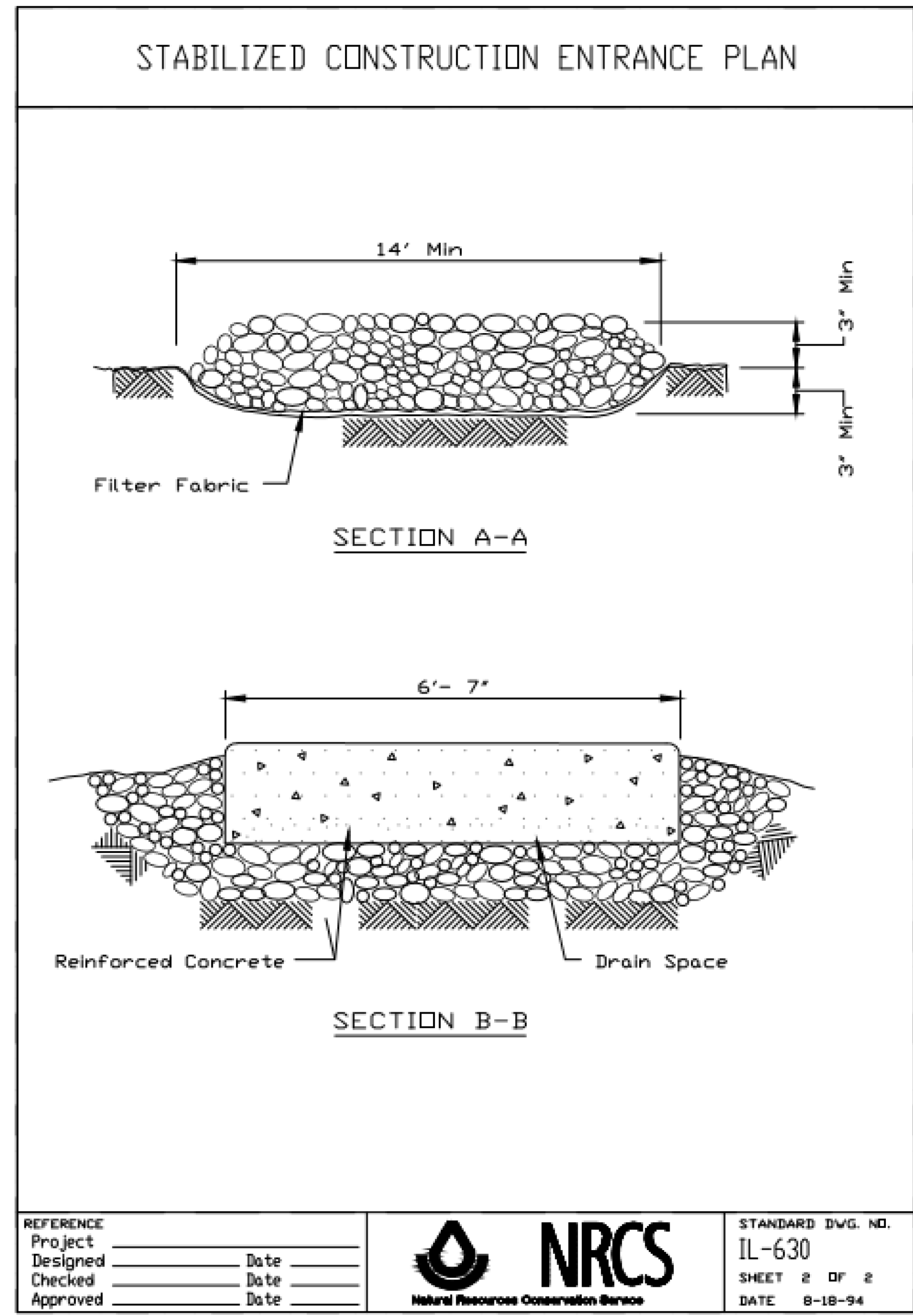
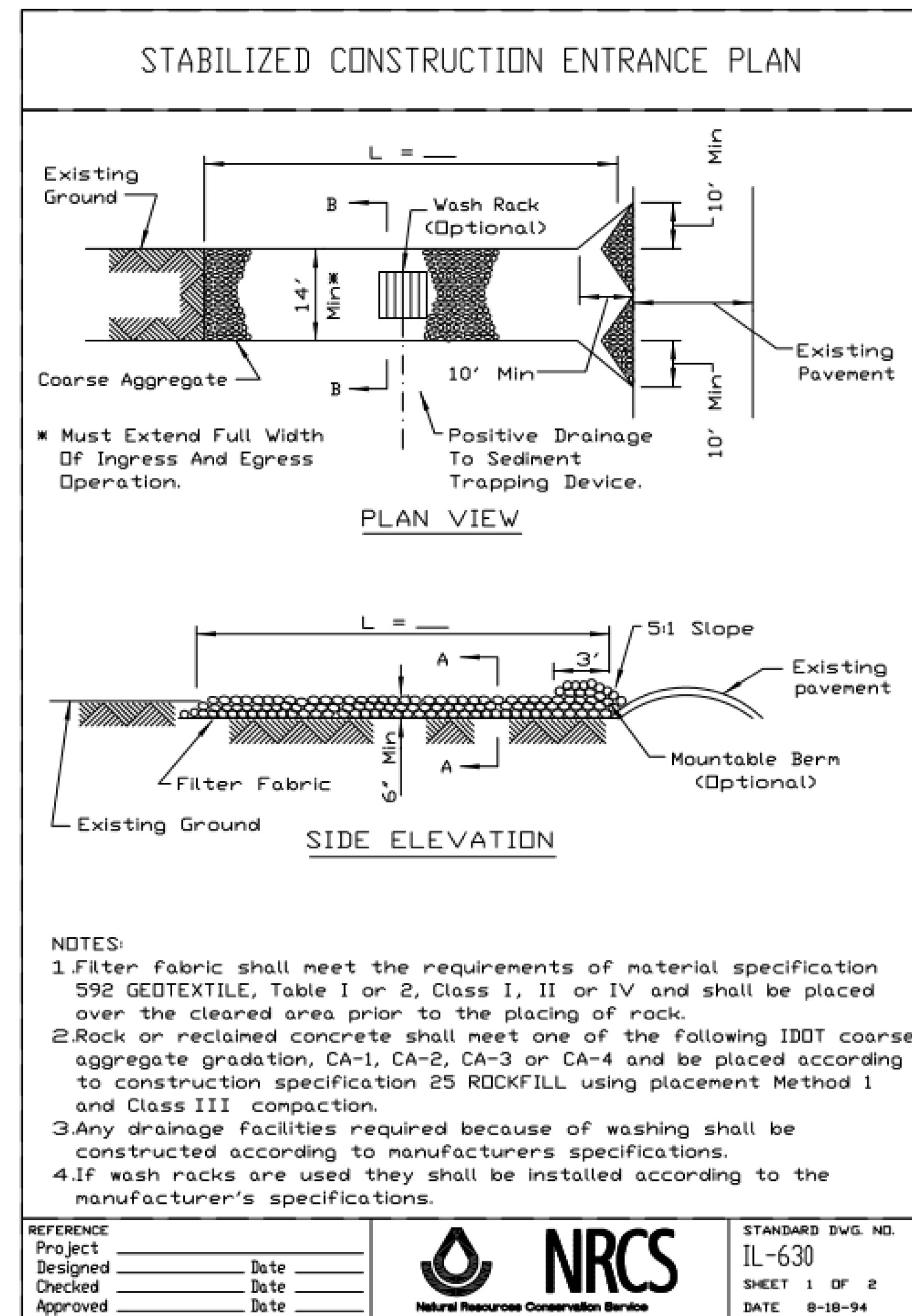
VILLAGE OF ORLAND HILLS
163RD ST./MEADOWVIEW DRIVE AND HAVEN AVENUE
ROADWAY PLAN

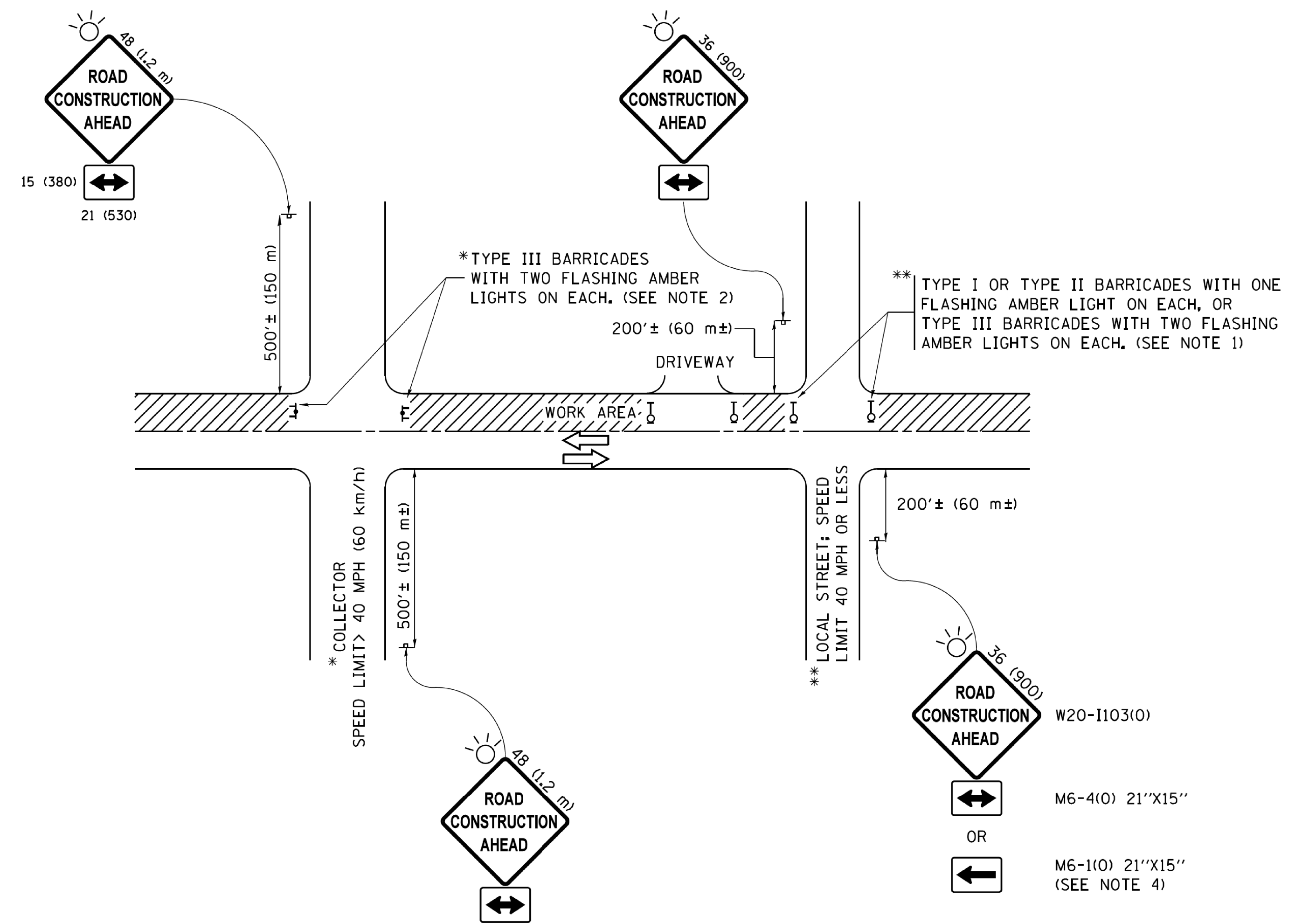
SCALE: 1" = 20' SHEET 7 OF 7 SHEETS STA. 123+40.00 TO STA. 133+00.00

MUN. RTE. 0500 1037	SECTION 14-00025-00-R5	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 11
CONTRACT NO. 61E50			ILLINOIS FED. AID PROJECT	



CURB AND GUTTER DETAIL AT DRIVEWAYS
N.T.S.





NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

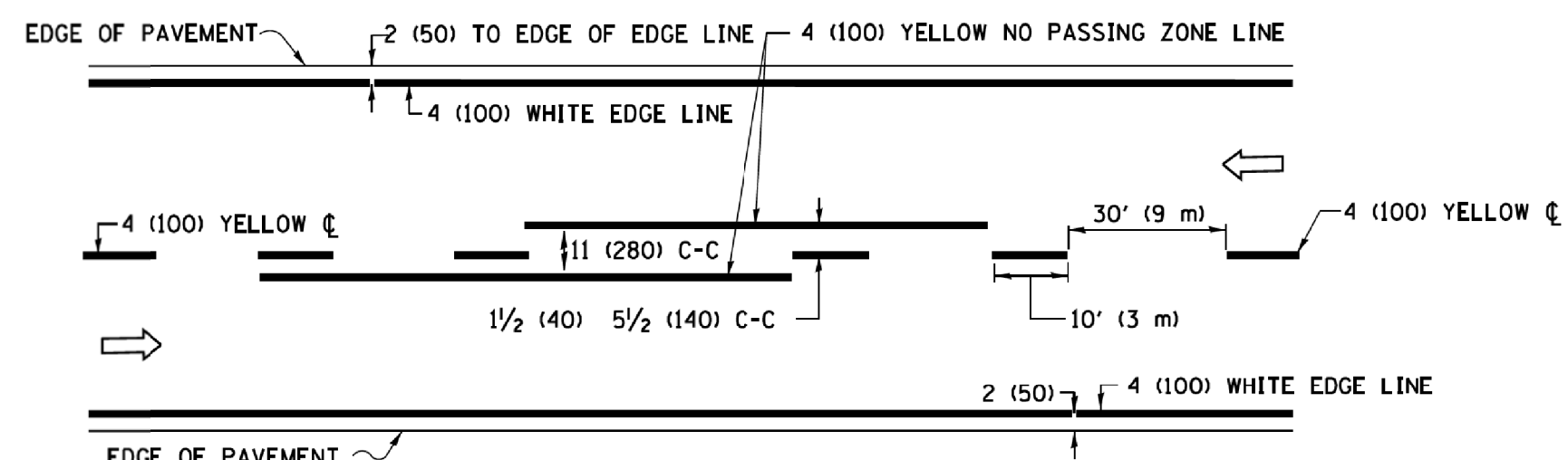
FILE NAME =	USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
p:\work\084EBIDINTEG\illinois.gov\PIWIDT\Documents\IDOT Offices\District 1\Projects\Dist 1\CADD\cadd\cadsheets\10.dgn		DRAWN	REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

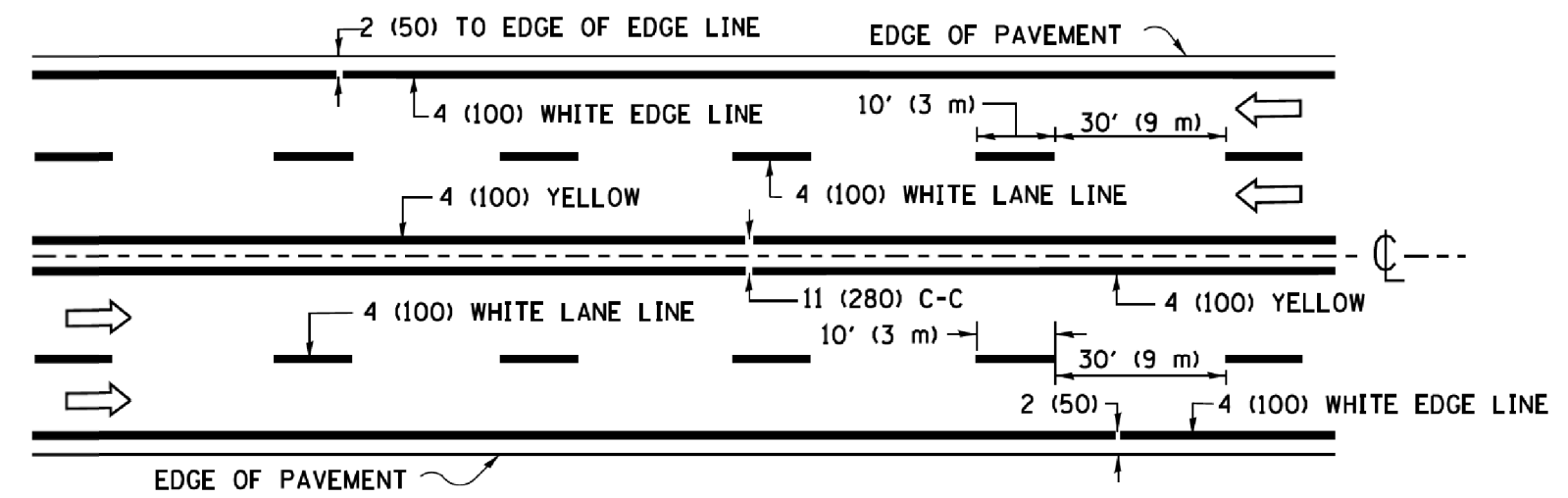
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

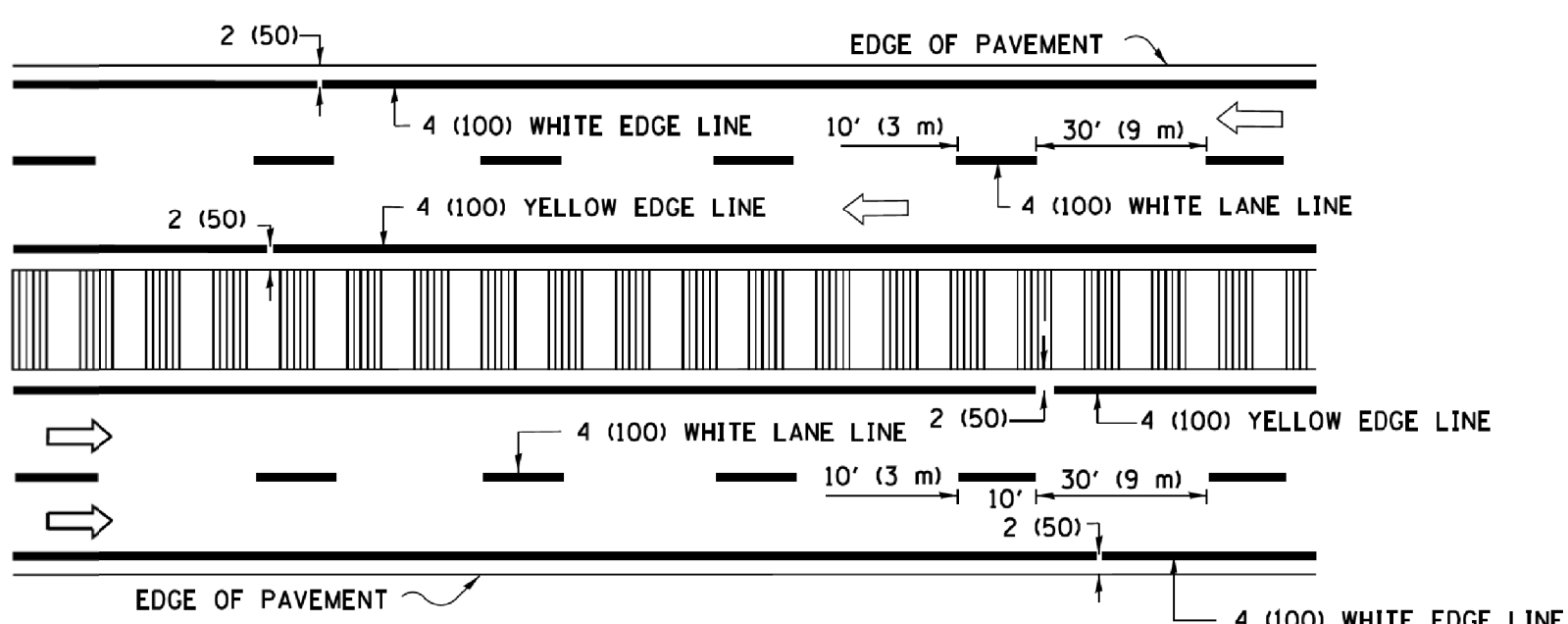
MUN. RTE. 0500 1037	SECTION 14-00025-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 13
TC-10		CONTRACT NO. 61E50		
ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

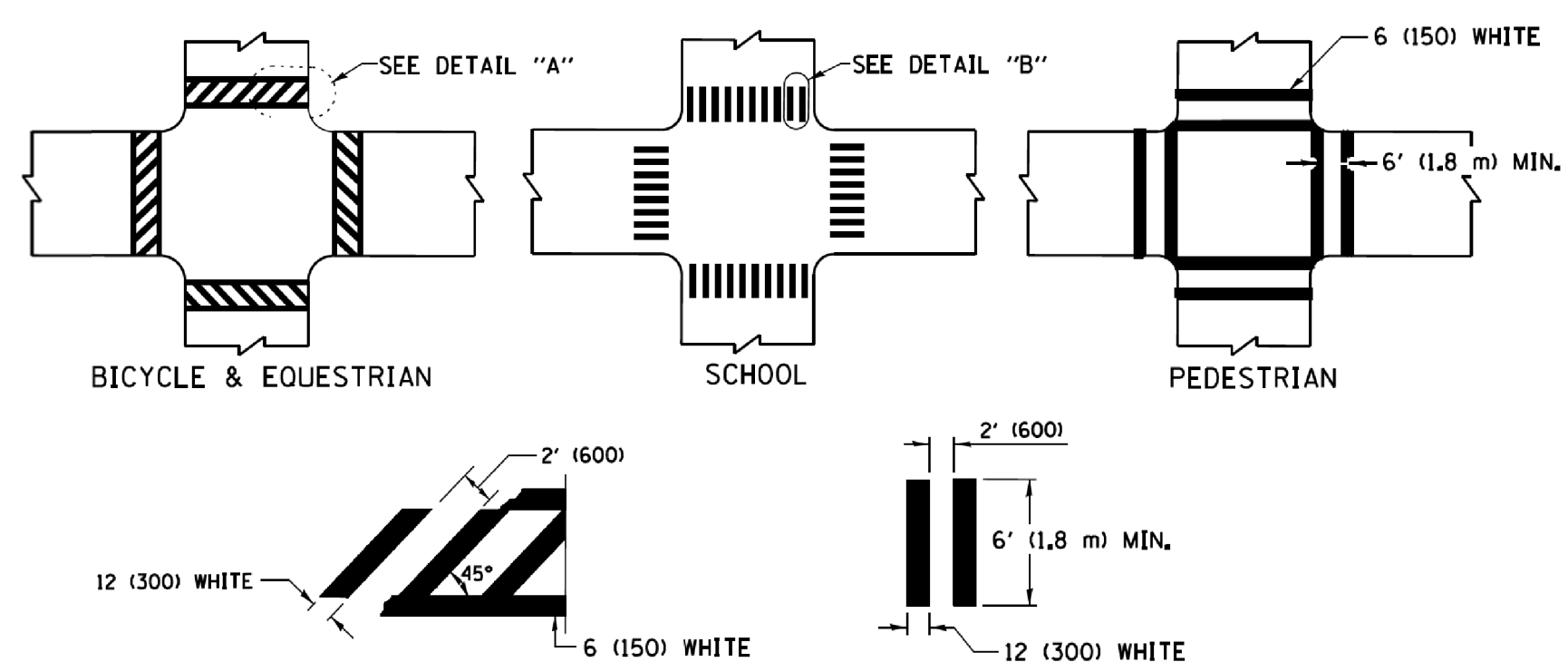


MULTI-LANE UNDIVIDED



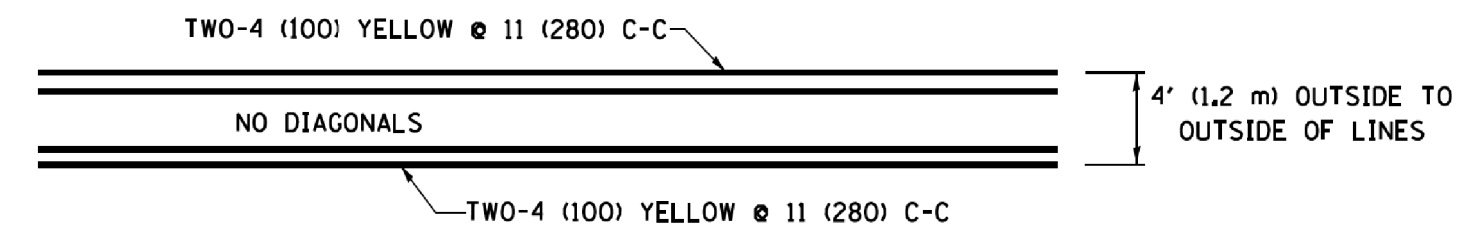
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

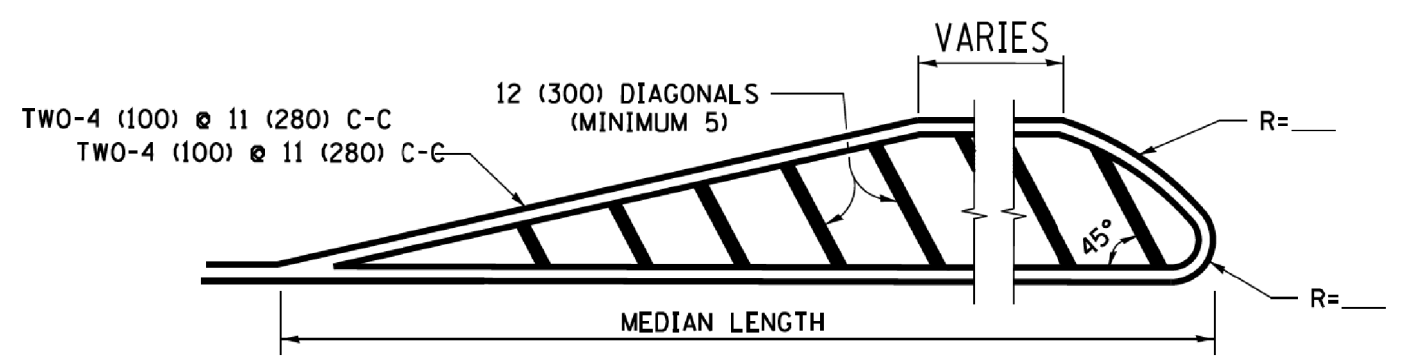


TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

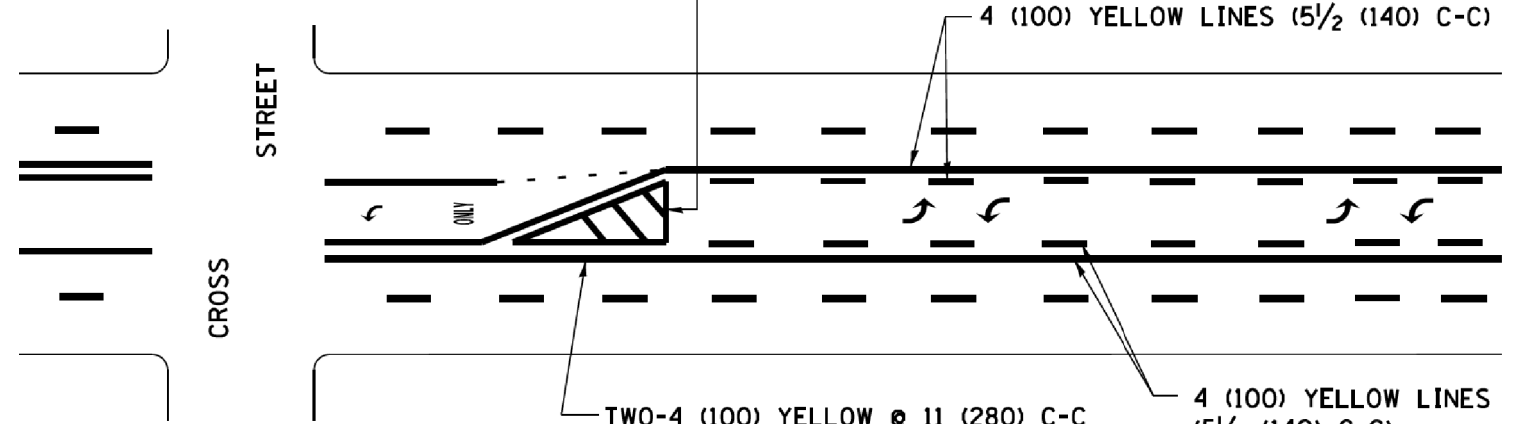


4' (1.2 m) WIDE MEDIANS ONLY



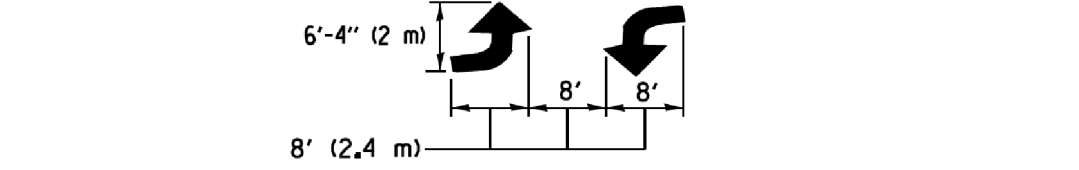
MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



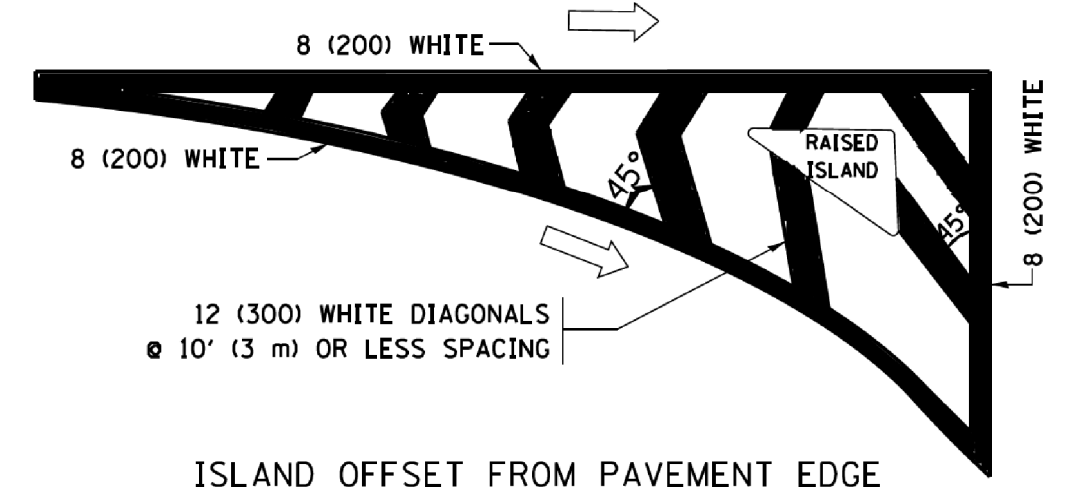
**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

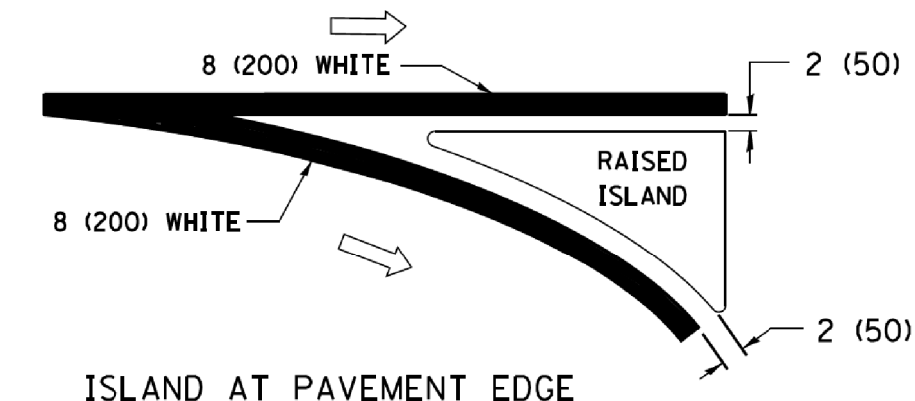


**TYPICAL LEFT (OR RIGHT) TURN LANE
TYPICAL TURN LANE MARKING**

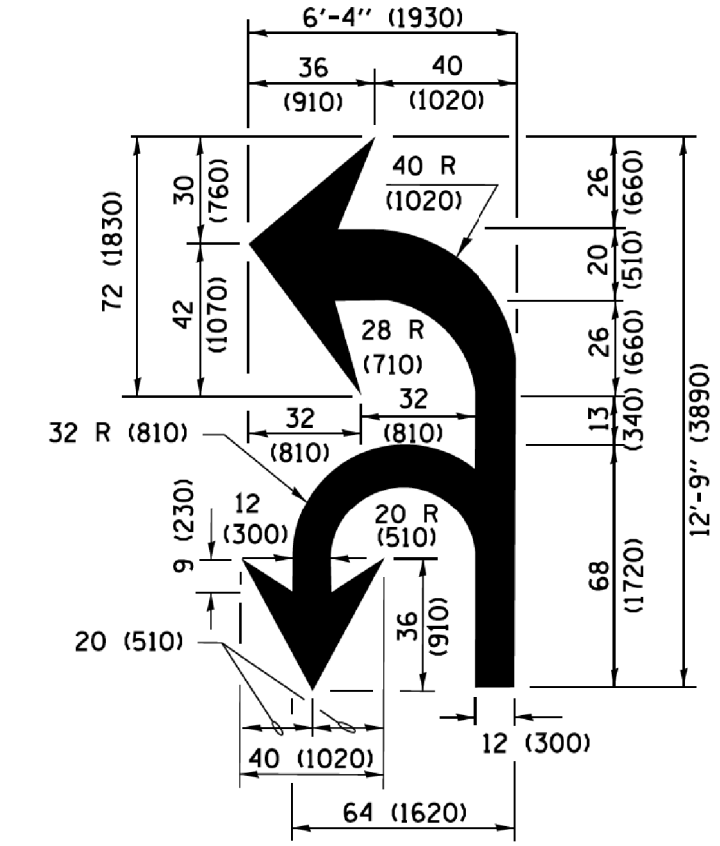
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



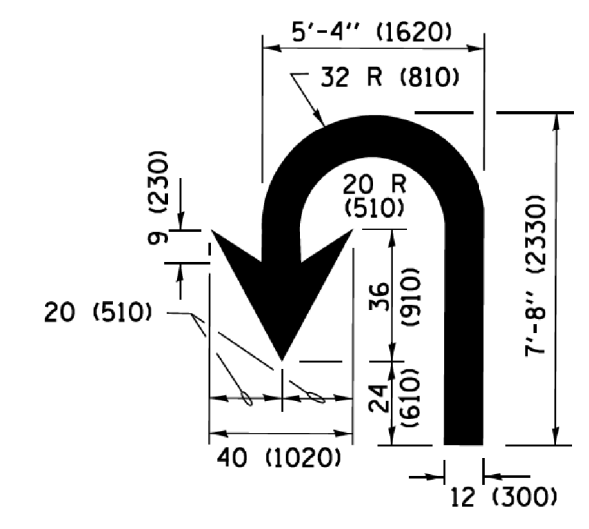
ISLAND OFFSET FROM PAVEMENT EDGE



**ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING**



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

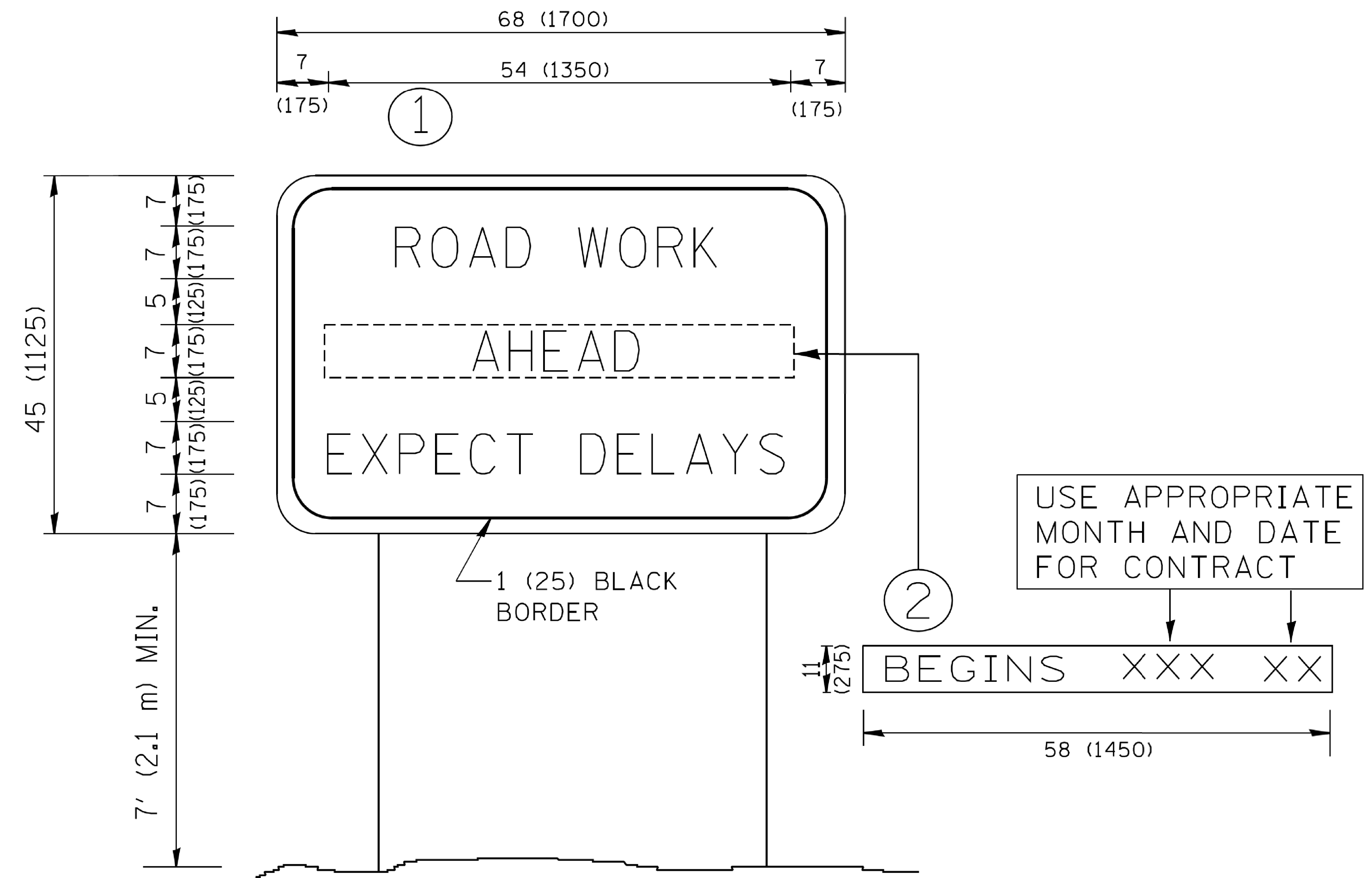
FILE NAME = W:\dststd\22x34\to13.dgn	USER NAME = l1eyss	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
		DRAWN -	REVISED - C. JUCIUS 07-01-13
		CHECKED -	REVISED - C. JUCIUS 12-21-15
		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
TYPICAL PAVEMENT MARKINGS**

SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
-------------	---------	------	--------	------	---------

MUN. RTE. 0500 1037	SECTION 14-00025-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 14
TC-13		CONTRACT NO. 61E50	ILLINOIS FED. AID PROJECT	

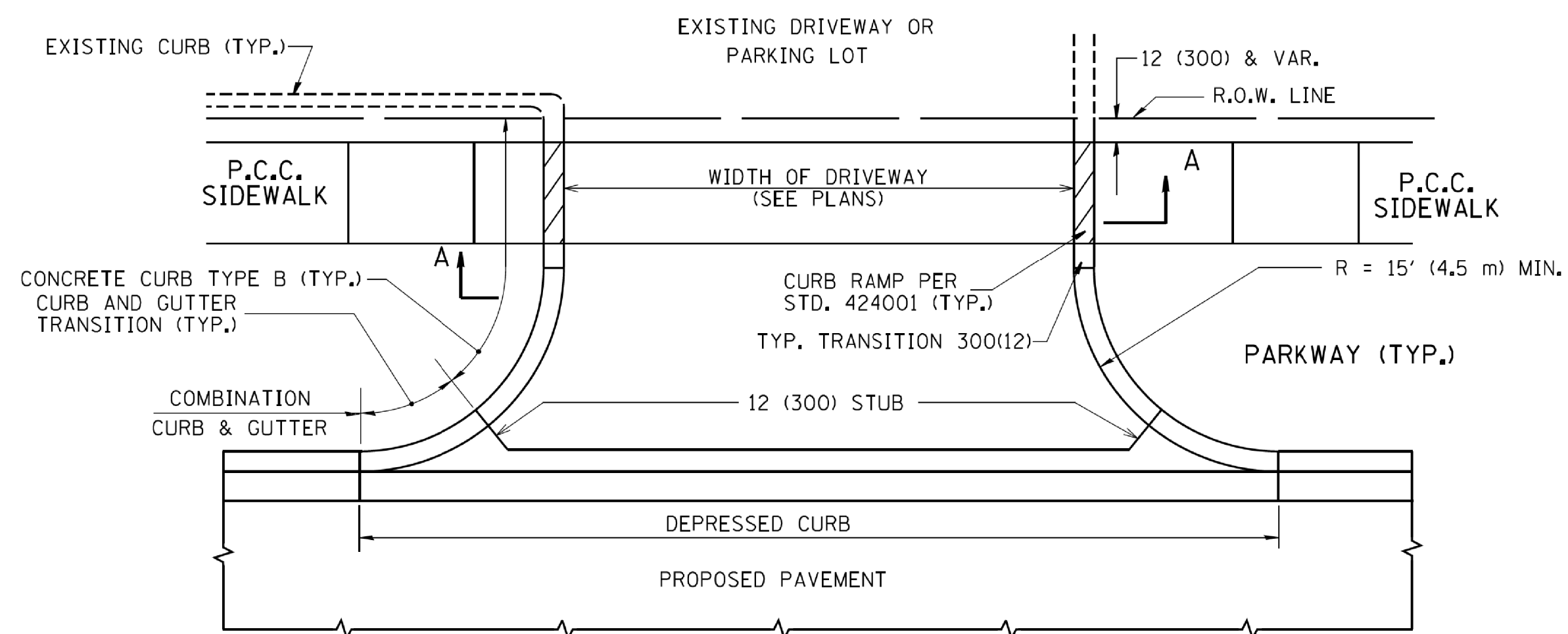


NOTES:

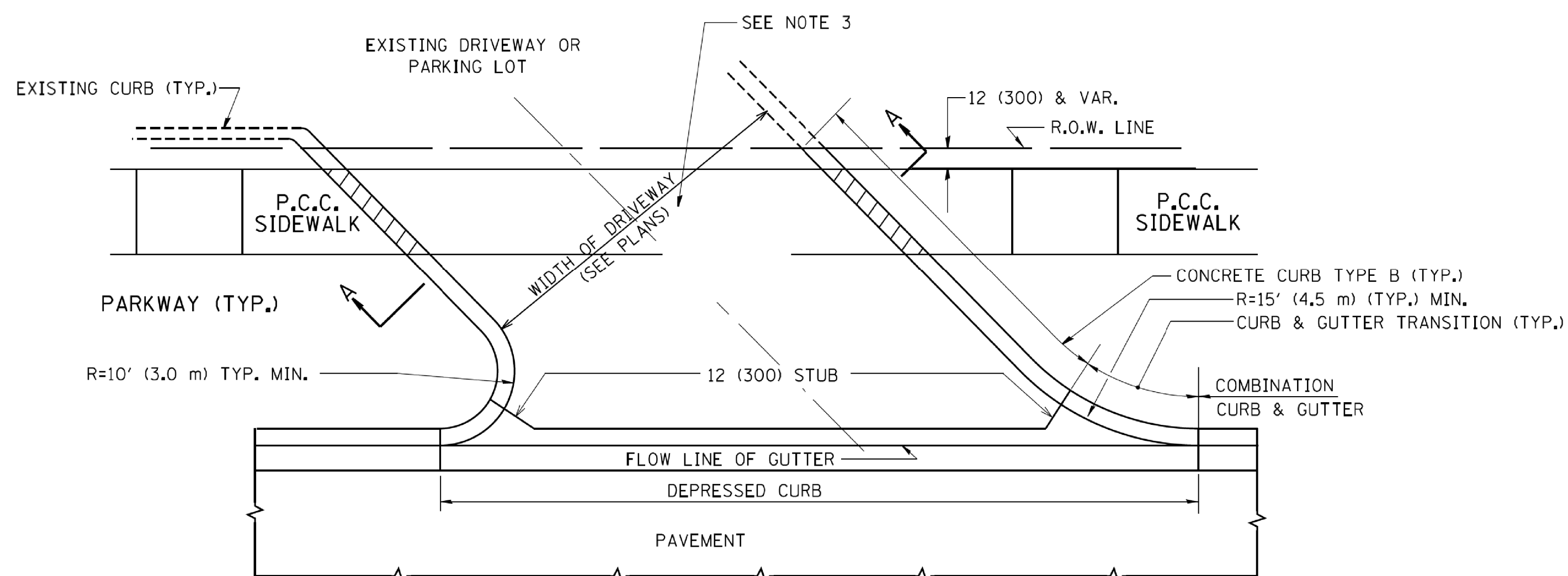
1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

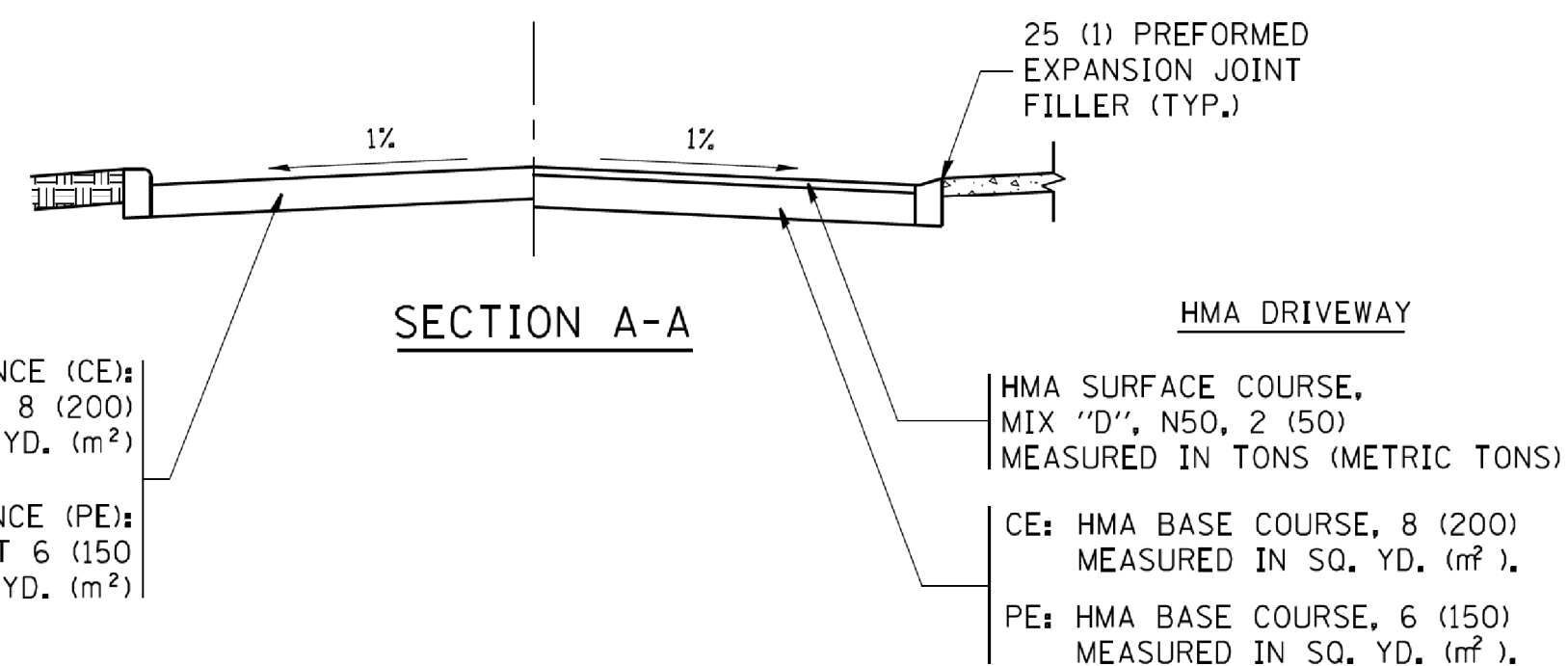
FILE NAME = W:\distsd\22x34\to22.dgn	USER NAME = geglonebt	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			MUN RTE. 0500 1037	SECTION 14-00025-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 15
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			TC-22		CONTRACT NO. 61E50		
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							
		DATE -	REVISED - C. JUCIUS 01-31-07									



WITH CONCRETE CURB, TYPE B

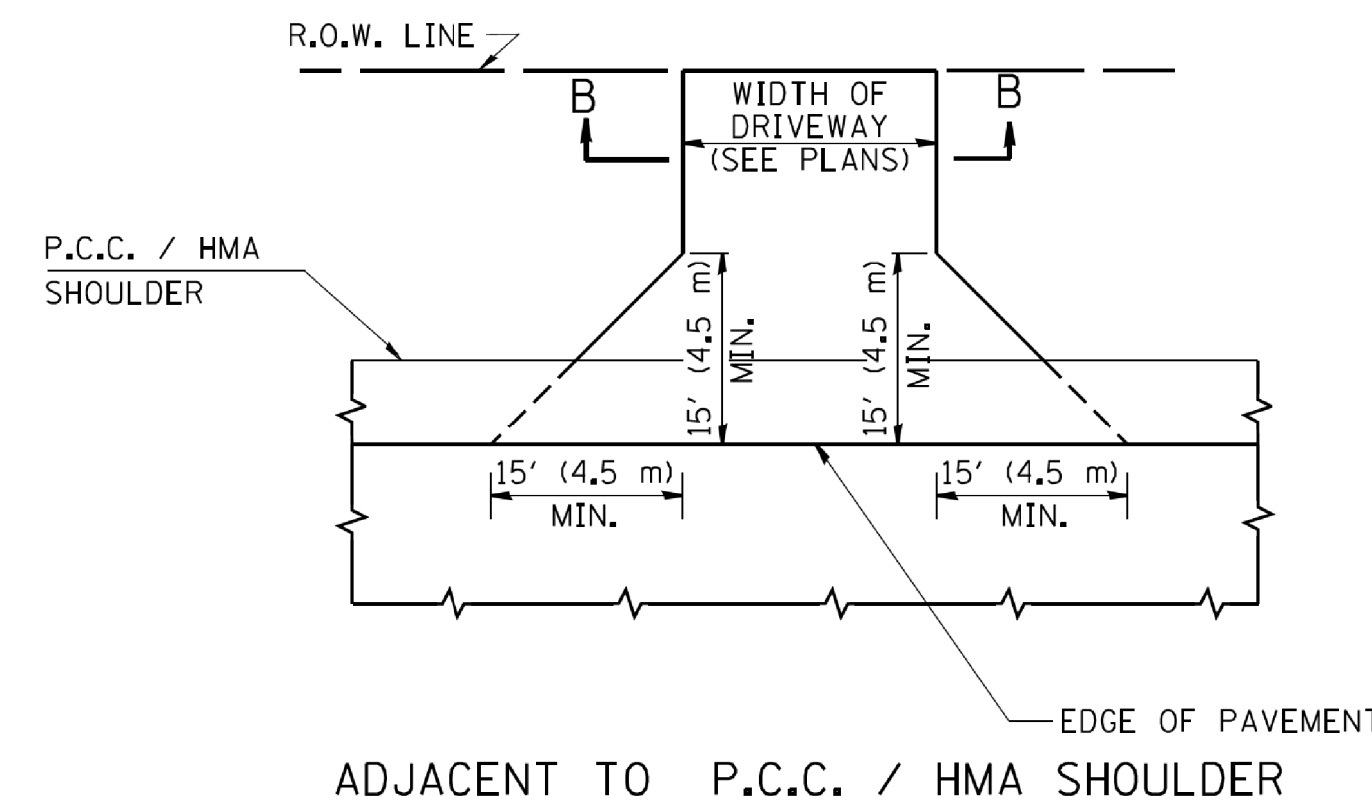


WITH CONCRETE CURB, TYPE B

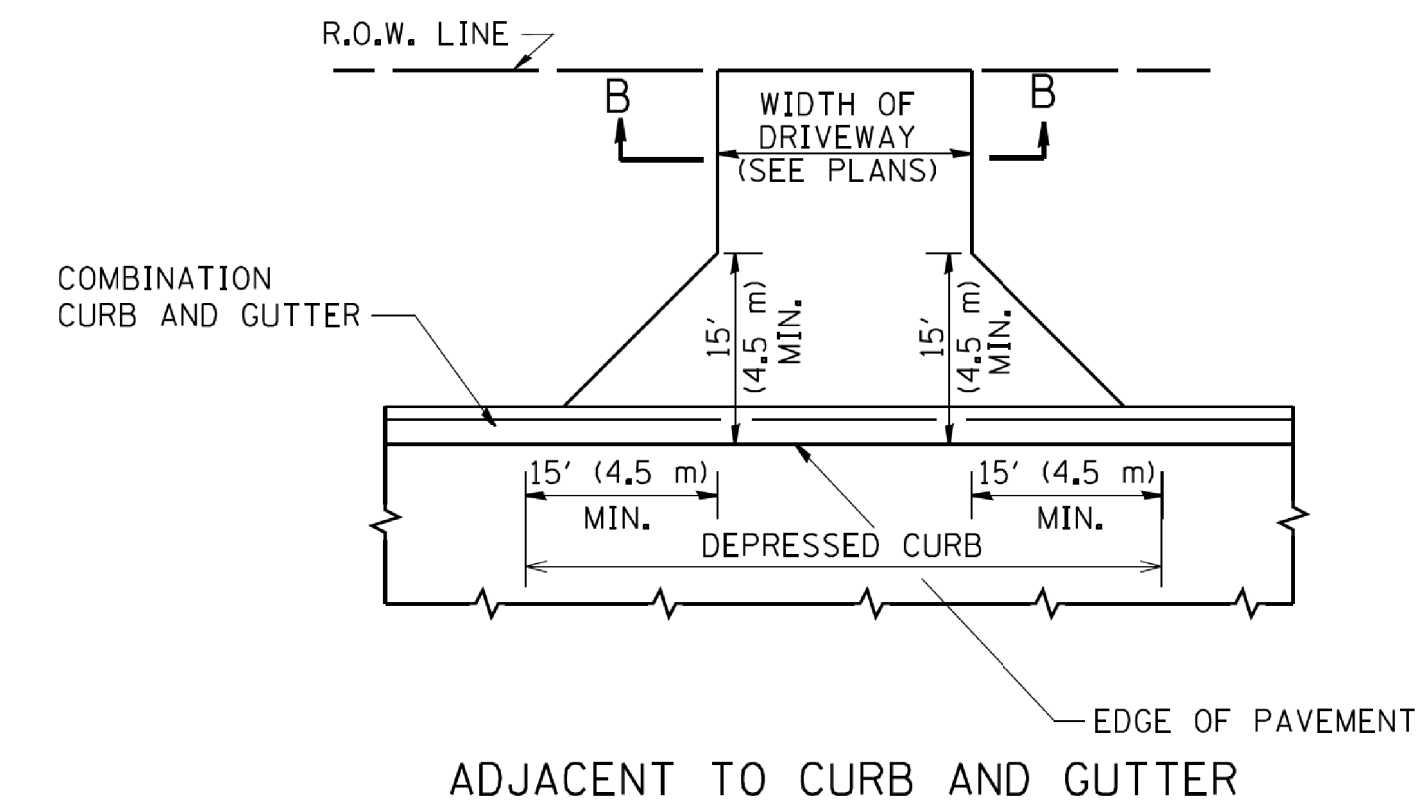


RIGID DRIVEWAY
 COMMERCIAL ENTRANCE (CE):
 P.C.C. DRIVEWAY PAVEMENT 8 (200)
 MEASURED IN SQ. YD. (m²)
 NON-COMMERCIAL ENTRANCE (PE):
 P.C.C. DRIVEWAY PAVEMENT 6 (150)
 MEASURED IN SQ. YD. (m²)

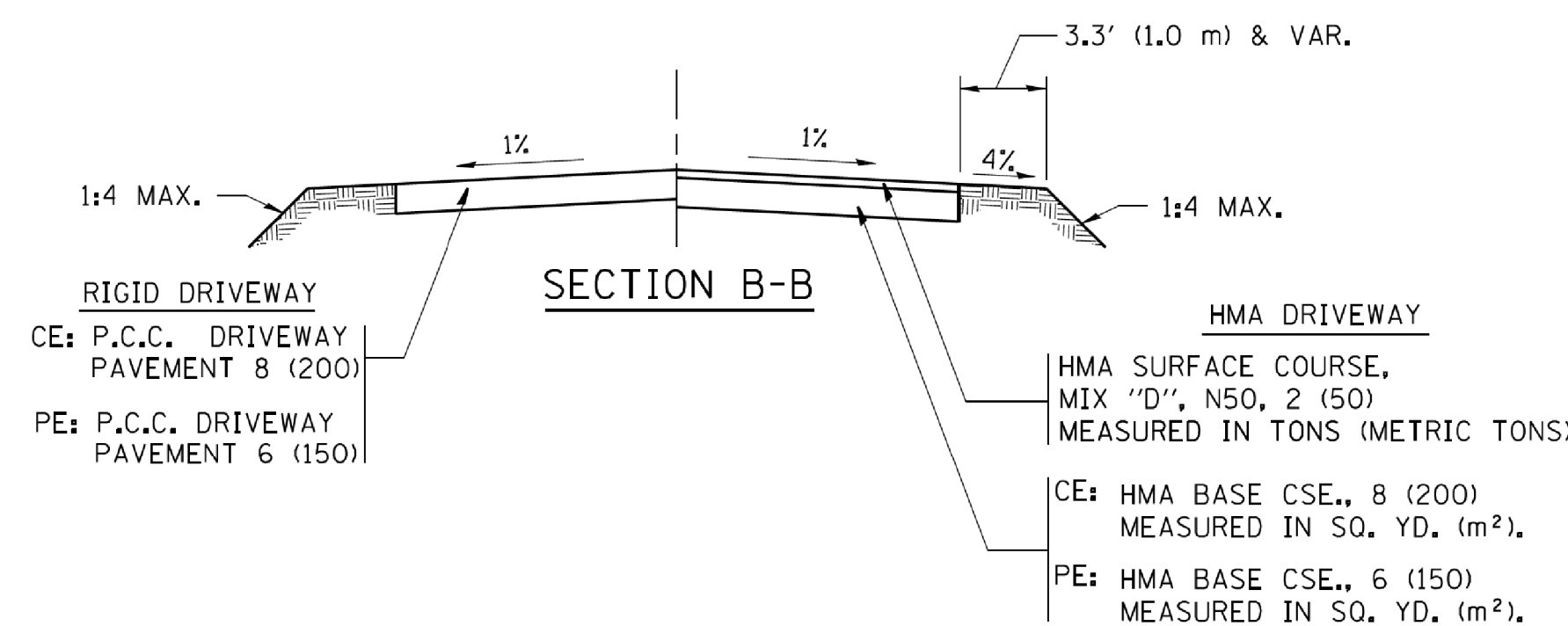
HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE COURSE, 8 (200)
 MEASURED IN SQ. YD. (m²),
 PE: HMA BASE COURSE, 6 (150)
 MEASURED IN SQ. YD. (m²).



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RIGID DRIVEWAY
 CE: P.C.C. DRIVEWAY PAVEMENT 8 (200)
 PE: P.C.C. DRIVEWAY PAVEMENT 6 (150)

HMA DRIVEWAY
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)
 CE: HMA BASE CSE., 8 (200)
 MEASURED IN SQ. YD. (m²),
 PE: HMA BASE CSE., 6 (150)
 MEASURED IN SQ. YD. (m²).

RURAL FIELD ENTRANCE (FE)
 HMA SURFACE COURSE,
 MIX "D", N50, 2 (50)
 MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200)
 MEASURED IN SQ. YD. (m²).

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = lgyaa	DESIGNED - R. SHAH	REVISED - P. LoFLUER 04-15-03
et\pw_work\pwsdot\lgyaa\d0108315\bd01.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.0000' / in.	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 9/6/2011	DATE - 11-04-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W.
 AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)**

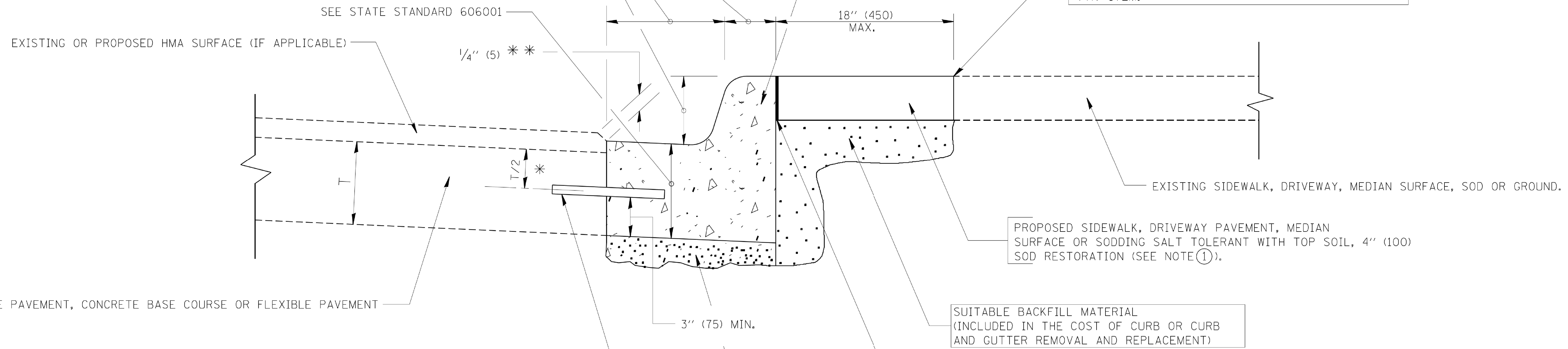
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0500 1037	14-00025-00-R5	COOK	17	16
BD0156-07 (BD-01)		CONTRACT NO. 61E50		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drvakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			MUN. RTE. 0500 1037	SECTION 14-00025-00-RS	COUNTY COOK	TOTAL SHEETS 17	SHEET NO. 17
PLOT SCALE = 50.000 ' / IN.								DRAWN -	REVISED - A. ABBAS 03-21-97	BD600-06 (BD-24)		
PLOT DATE = 12/15/2009					CHECKED -	REVISED - M. GOMEZ 01-22-01	CONTRACT NO. 61E50					
DATE = 03-11-94					DATE -	REVISED - R. BORO 12-15-09	SCALE: NONE	SHEET NO. 1 OF 1	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT