

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-RS	COOK	19	1
		ILLINOIS	CONTRACT NO. 61E41	

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LISTING OF HIGHWAY STANDARDS, SEE SHEET NO. 2

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

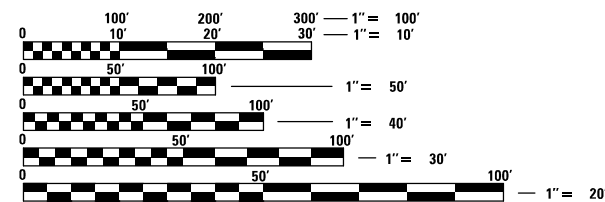
MUN ROUTE 1070 (AUSTIN AVENUE)
111TH STREET TO 107TH STREET
RESURFACING
SECTION 16-00055-00-RS
PROJECT NO. 1P5V(081)
VILLAGE OF CHICAGO RIDGE
COOK COUNTY
JOB NO. C-91-401-16



DESIGN DESIGNATION
MINOR COLLECTOR

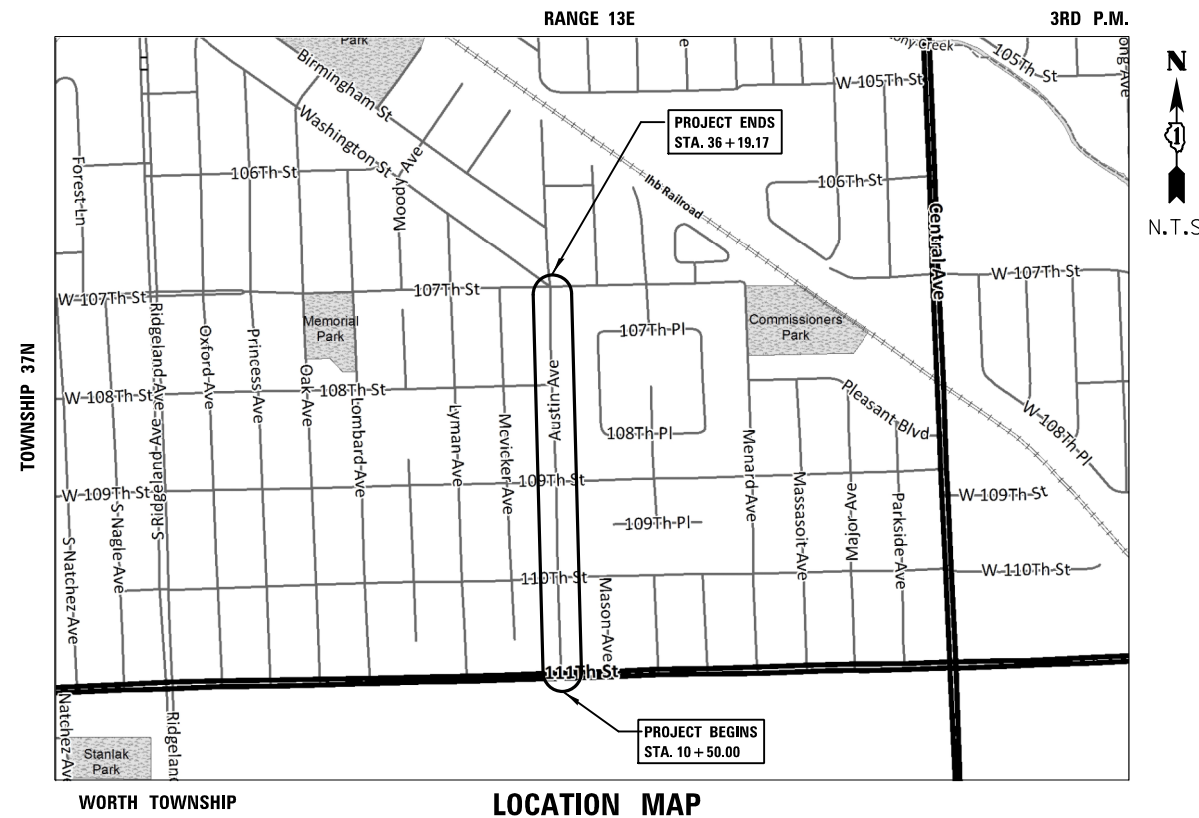
POSTED SPEED
20 M.P.H.

TRAFFIC DATA
ADT (YEAR) = 725 (2014)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



PROGRAM AND OFFICE ENGINEER: CHARLES RIDDLE, P.E. (847) 705-4406 SCHAUMBURG, IL

CB CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PROFESSIONAL DESIGN FIRM NO.: 184-00175
EXPIRATION DATE: APRIL 30, 2019

CONTRACT NO. 61E41

GROSS LENGTH = 2,569.17 FT. = 0.49 MILE
NET LENGTH = 2,569.17 FT. = 0.49 MILE



Andrew M. Pufundt
ENGINEER
11/17/2017
DATE
ANDREW M. PUFUNDT
ILLINOIS REGISTRATION No. 062-061729
EXPIRATION DATE: 11/30/2019

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED: *Colin E. T...*
November 21, 2017
VILLAGE PRESIDENT, VILLAGE OF CHICAGO RIDGE

PASSED: *CHRISTOPHER HOLT*
DECEMBER 12, 2017
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW: *Anthony J. Quigley*
DECEMBER 17, 2017
REGIONAL ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

GENERAL NOTES

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SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2018; THE LATEST EDITIONS OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD) AND "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS"; THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT STANDARD.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE AND ENGINEER DO NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 8-1-1 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATIONS IS REQUIRED).

THE CONTRACTOR SHALL CONTACT IDOT'S BUREAU OF MATERIALS (PHONE 847-705-4337) AT LEAST 24 HOURS BEFORE PLACING HOT MIX ASPHALT OR PORTLAND CEMENT CONCRETE.

WATER, STORM SEWER AND SANITARY SEWER

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT THE VILLAGE OF CHICAGO RIDGE PUBLIC WORKS DEPARTMENT (TEL. NO. 708-857-4457) FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE SHALL BE COMPLETED PER ARTICLE 107.15 OF THE STANDARD SPECIFICATIONS.

ALL EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN TEMPORARY OUTLETS AND CONNECTIONS FOR ALL PRIVATE OR PUBLIC DRAINS, SEWERS OR CATCH BASINS. HE/SHE SHALL PROVIDE FACILITIES TO TAKE IN ALL STORM WATER WHICH WILL BE RECEIVED BY THESE DRAINS AND SEWERS AND DISCHARGE THE SAME. HE/SHE SHALL PROVIDE AND MAINTAIN AN EFFICIENT PUMPING PLAN, IF NECESSARY, AND A TEMPORARY OUTLET AND BE PREPARED AT ALL TIMES TO DISPOSE OF THE WATER RECEIVED FROM THESE TEMPORARY CONNECTIONS UNTIL SUCH TIME AS THE PERMANENT CONNECTIONS WITH SEWERS ARE BUILT AND IN SERVICE.

THE CONTRACTOR SHALL CONFIRM ALL EXISTING STORM SEWER PIPE SIZES AND INVERTS PRIOR TO ORDERING STRUCTURES. ANY MODIFICATIONS OF STRUCTURES DUE TO THE FAILURE OF THE CONTRACTOR TO PERFORM THIS TASK MAY LEAD TO THE REJECTION OF THE STRUCTURE IN THE FIELD.

STRUCTURE ADJUSTMENTS: PRECAST CONCRETE ADJUSTMENT RINGS ARE NOT TO EXCEED 12 INCHES IN OVERALL HEIGHT AND SHALL BE USED IF AN ADJUSTMENT TO THE FINISHED GRADE ESTABLISHED BY THE ENGINEER IS NECESSARY. MAXIMUM TWO RINGS.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION.

DIMENSIONS: THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

LIMITS OF PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, STRUCTURES TO BE ADJUSTED AND STRUCTURES TO BE REPLACED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS SHOWN ON THE PLANS.

DEPRESSED CURB: RAMPS AND DEPRESSED CURBS ACCESSIBLE TO THE HANDICAPPED SHALL BE PROVIDED AT CROSSWALK LOCATIONS SHOWN ON THE PLANS.

ALL UNDERGROUND, DRIVEWAY, CONCRETE, AND LANDSCAPE RESTORATION WORK IS TO BE COMPLETED BEFORE THE SURFACE COURSE CAN BE INSTALLED.

FOR HOT-MIX ASPHALT SURFACE REMOVAL, THE EXISTING ASPHALT SURFACE SHALL BE REMOVED TO THE DEPTH SPECIFIED. THE GRINDINGS SHALL BE REMOVED FROM THE SITE AND THE SURFACE MECHANICALLY BROOMED UNTIL THE SURFACE IS COMPLETELY FREE OF ANY LOOSE MATERIAL AND DEBRIS.

NO CONSTRUCTION SHALL BEGIN UNTIL ALL PROPER TEMPORARY SIGNS AND BARRICADES HAVE BEEN INSTALLED.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR JOB SITE SAFETY AS WELL AS SUPERVISION/DIRECTION AND MEANS/METHODS OF CONSTRUCTION.

SOIL EROSION PROTECTION SHALL BE IN ACCORDANCE WITH IEPA STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL. ALL DISTURBED AREAS (NOT IMPERVIOUS IN NATURE) SHALL BE FINE GRADED, TOP SOIL RESTORED (MIN 4 INCHES) AND SOD APPLIED UNLESS OTHERWISE SPECIFIED ON THE PLANS.

ANY DEFACED WORK SHALL BE CORRECTED OR REPLACED BY THE CONTRACTOR. THE VILLAGE WILL COOPERATE WITH THE CONTRACTOR TO MINIMIZE VANDALISM, BUT THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE TO CORRECT ANY DAMAGE PER CONDITIONS OF ARTICLE 107.30 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR SHALL KEEP ONE LANE IN EACH DIRECTION OPEN TO TRAFFIC AT ALL TIMES DURING CONSTRUCTION. THE CONTRACTOR SHALL COMPLETE ALL MILLING OPERATIONS BEFORE INSTALLING ANY HOT-MIX ASPHALT PAVEMENT.

THE CONTRACTOR MUST PROVIDE AND MAINTAIN SUFFICIENT WASHROOMS FOR THE PROJECT.

ADVANCE WARNING SIGNS NEED TO BE ERECTED AT LEAST 7 DAYS PRIOR TO START OF CONSTRUCTION.

PRIOR TO INSTALLING SURFACE COURSE AND AT THE ENGINEER'S DISCRETION, THE CONTRACTOR SHALL SAWCUT THE EXISTING PAVEMENT TO PROVIDE A NEW BUTT JOINT TO PAVE AGAINST.

ALL REMOVAL OR EXCAVATION ITEMS BEING DISPOSED OF AT AN UNCONTAMINATED SOIL FILL OPERATION OR CLEAN CONSTRUCTION AND DEMOLITION DEBRIS (CCDD) FILL SITE SHALL MEET THE REQUIREMENTS OF PUBLIC ACT 96-1416.

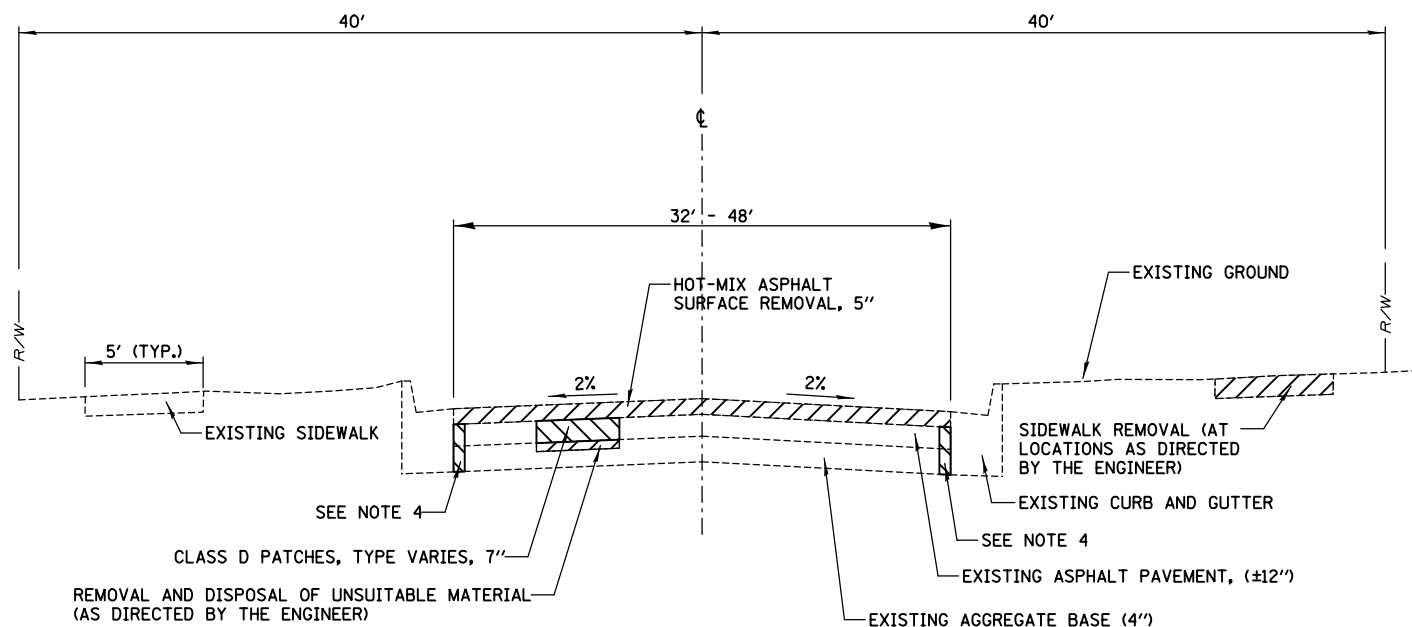
FILE NAME = N:\CHICAGO\RIDGE\9061\098\Civil\NOT_900061\098_01.SHT	USER NAME = aschaefer	DESIGNED - AJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	AUSTIN AVENUE RESURFACING GENERAL NOTES, INDEX OF SHEETS AND HIGHWAY STANDARDS			MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 48'	CHECKED - AMP	REVISED -					1070	16-00055-00-R5	COOK	19	2	
PLOT DATE = 12/19/2017	DATE - 12/19/2017	REVISED -	SCALE: 48'		SHEET NO.	OF	SHEETS	STA.	TO	STA.	ILLINOIS FED. AID PROJECT CONTRACT NO. 61E41		

SUMMARY OF QUANTITIES

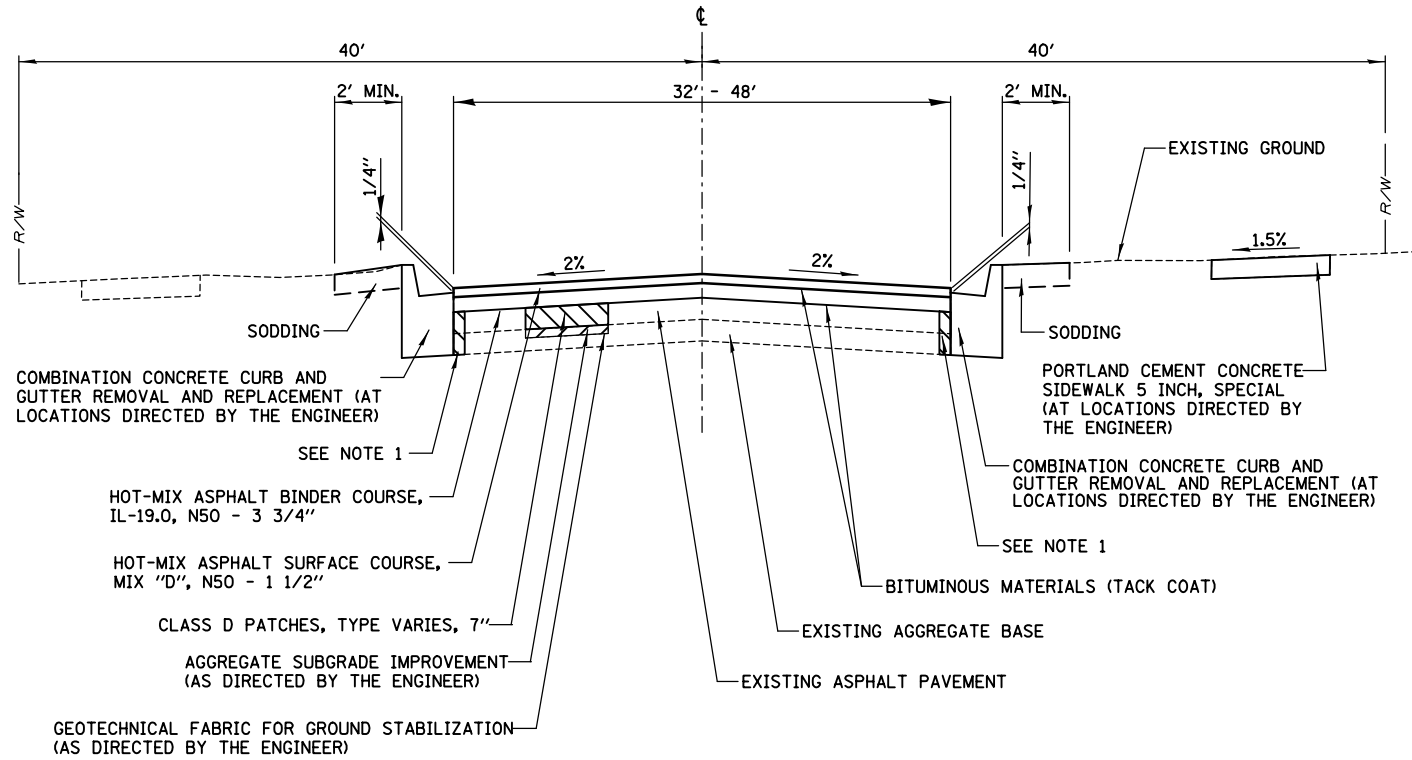
FUNDING SOURCE				TOTAL QUANTITY	
				STP	80% FED. 20% LOCAL
LOCATION OF WORK				AUSTIN AVENUE	
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE	
CODE NO.	ITEM	UNIT		0005	
20101100	TREE TRUNK PROTECTION	EACH		4	
20101200	TREE ROOT PRUNING	EACH		4	
20101300	TREE PRUNING (1 TO 10 INCH DIAMETER)	EACH		2	
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH		2	
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD		665	
21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD		1980	
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD		1570	
25000400	NITROGEN FERTILIZER NUTRIENT	POUND		20	
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND		20	
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND		20	
25200100	SODDING	SQ YD		1570	
25200200	SUPPLEMENTAL WATERING	UNIT		10	
28000510	INLET FILTERS	EACH		22	
* 30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD		665	
35800100	PREPARATION OF BASE	SQ YD		9770	
35800200	AGGREGATE BASE REPAIR	TON		535	
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND		3020	
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON		2	
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD		130	
40603080	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON		2375	
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON		950	
* 42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SQ YD		340	
* 42400800	DETECTABLE WARNINGS	SQ FT		160	
* 44000169	HOT-MIX ASPHALT SURFACE REMOVAL, 5"	SQ YD		10772	
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD		710	
44000300	CURB REMOVAL	FOOT		100	
44000600	SIDEWALK REMOVAL	SQ FT		5485	
44201725	CLASS D PATCHES, TYPE I, 7 INCH	SQ YD		25	
44201729	CLASS D PATCHES, TYPE II, 7 INCH	SQ YD		25	
44201733	CLASS D PATCHES, TYPE III, 7 INCH	SQ YD		25	

FUNDING SOURCE				TOTAL QUANTITY	
				STP	80% FED. 20% LOCAL
LOCATION OF WORK				AUSTIN AVENUE	
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE	
CODE NO.	ITEM	UNIT		0005	
44201735	CLASS D PATCHES, TYPE IV, 7 INCH	SQ YD		25	
60206905	CATCH BASINS, TYPE C, TYPE 1 FRAME, OPEN LID	EACH		6	
60266600	VALVE BOXES TO BE ADJUSTED	EACH		3	
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH		3	
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH		3	
60500050	REMOVING CATCH BASINS	EACH		6	
60600605	CONCRETE CURB, TYPE B	FOOT		100	
67100100	MOBILIZATION	L SUM		1	
* 70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM		1	
* 70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM		1	
* 70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM		1	
~ 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT		15	
~ 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT		400	
~ 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT		570	
~ 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT		145	
* ~ 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH		1	
* ~ 88600600	DETECTOR LOOP REPLACEMENT	FOOT		110	
* X4021000	TEMPORARY ACCESS (PRIVATE ENTRANCE)	EACH		61	
* X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH		5	
* X4023000	TEMPORARY ACCESS (ROAD)	EACH		7	
* X4240430	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH, SPECIAL	SQ FT		5720	
* X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH		6	
* XX004774	BRICK DRIVEWAY REMOVAL AND REPLACEMENT	SQ FT		50	
* XX008130	DRAINAGE STRUCTURES TO BE CLEANED AND GROUTED	EACH		13	
* Z0004510	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 3"	SQ YD		370	
* Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT		1630	
* Z0013798	CONSTRUCTION LAYOUT	L SUM		1	
* Z0017400	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED	EACH		10	
* Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT		52	

* INDICATES SPECIAL PROVISION
~ INDICATES SPECIALTY ITEM



EXISTING TYPICAL SECTION
STA. 10+50.00 TO STA. 12+50.00, AUSTIN AVENUE



PROPOSED TYPICAL SECTION
STA. 10+50.00 TO STA. 12+50.00, AUSTIN AVENUE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE ITEM	AIR VOIDS @ Ndes
ROADWAY RESURFACING	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 1 1/2"	4% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 3 3/4"	4% @ 50 GYR
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5 MM), 3"	4% @ 50 GYR
PATCHING	
CLASS D PATCHES, TYPE VARIES, 7 INCH (2 LIFTS) (HOT-MIX ASPHALT BINDER COURSE, IL-19.0) NOTE: SAW CUT PATCHES PRIOR TO REMOVAL	4% @ 70 GYR

- NOTES:
- THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 - THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
FOR HMA FULL DEPTH "AC TYPE" SEE SPECIAL PROVISIONS.
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

- NOTES:
- CLASS SI PCC, VIBRATED IN PLACE SHALL BE POURED WITHIN 48 HOURS AND INCLUDED IN THE COST OF COMBINATION CONCRETE CURB AND GUTTER.
 - CONTRACTOR SHALL SAWCUT PAVEMENT PRIOR TO REMOVING THE CURB.
 - ADDITIONAL AGGREGATE REQUIRED BETWEEN THE TOP OF THE EXISTING BASE AND BOTTOM OF PROPOSED ASPHALT SHALL BE PAID FOR AS AGGREGATE BASE REPAIR.
 - PAVEMENT REMOVAL ADJACENT TO PROPOSED CURB AND GUTTER SHALL BE INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER.
 - FOR ROADWAYS WITH DIFFERING ELEVATIONS AT THE EDGE OF PAVEMENT, CONTRACTOR SHALL PAVE THE SIDE WITH THE HIGHER ELEVATION FIRST. WHEN GRADING THE AGGREGATE BASE AT THESE LOCATIONS, CONTRACTOR SHALL PROVIDE GRADE STAKES SHOWING FINISHED PAVEMENT GRADES AT 2% (MIN.) OFF HIGH SIDE.
 - ENGINEER TO VERIFY MINIMUM 2% CROSS SLOPE PRIOR TO PLACING SURFACE COURSE. ANY CORRECTIONS MUST BE MADE BY THE CONTRACTOR AT HIS OWN EXPENSE.
 - HOT-MIX ASPHALT SURFACE REMOVAL INCLUDES REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL OR AGGREGATE BASE NECESSARY TO OBTAIN THE REQUIRED ELEVATION FOR INSTALLATION OF BINDER COURSE. THIS SHALL INCLUDE RESIDUAL LAYERS OF ASPHALT THAT ARE NOT OF SUFFICIENT THICKNESS TO BE PAVED OVER.
 - NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR PETROMAT (FABRIC) ENCOUNTERED DURING GRINDING OPERATIONS. REMOVAL AND DISPOSAL OF PETROMAT SHALL BE INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL.
 - CONTRACTOR SHALL MILL PRIOR TO PATCHING.

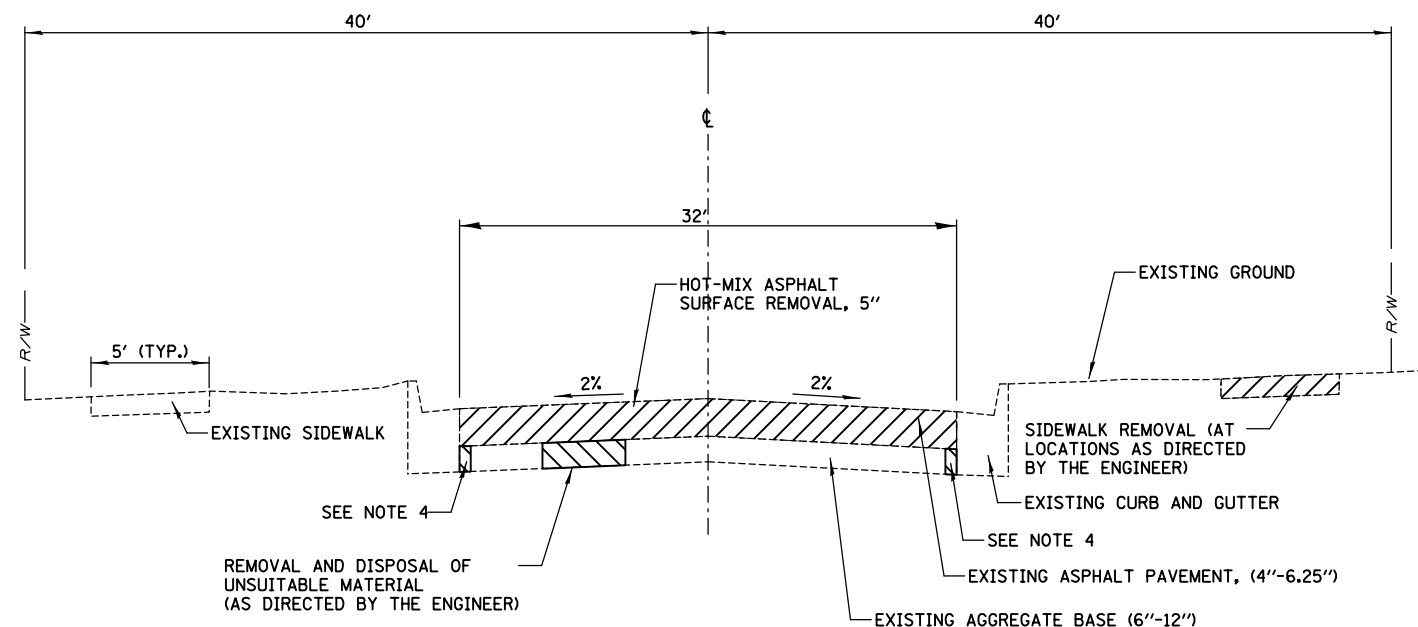
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	PLOT SCALE = 48"	CHECKED -	AMP	REVISED -	
	PLOT DATE = 12/19/2017	DATE -	12/19/2017	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

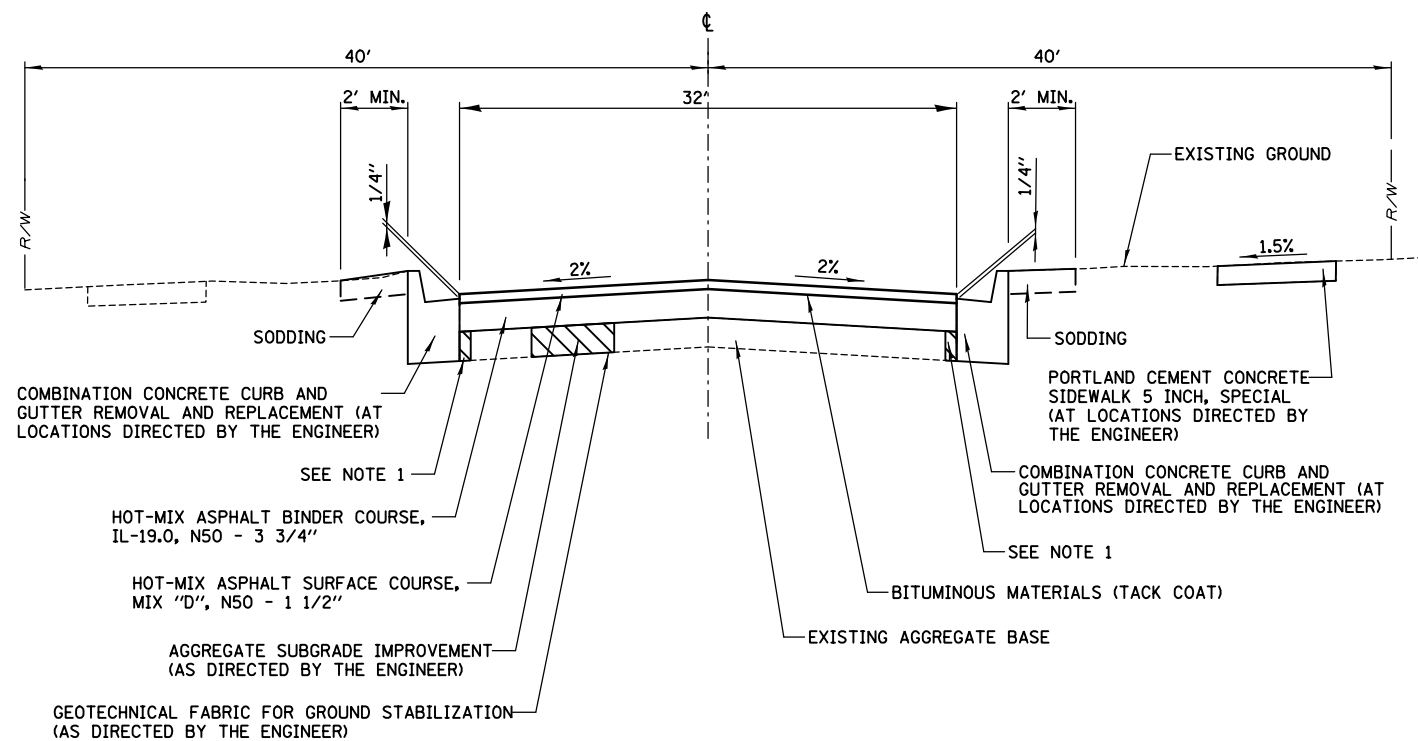
AUSTIN AVENUE RESURFACING
TYPICAL SECTIONS

SCALE: 48' SHEET NO. OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-R5	COOK	19	4
CONTRACT NO. 61E41				
ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION
STA. 12+50.00 TO STA. 36+19.17, AUSTIN AVENUE



PROPOSED TYPICAL SECTION
STA. 12+50.00 TO STA. 36+19.17, AUSTIN AVENUE

NOTES:

1. ADDITIONAL STONE BACK FILLING SHALL BE INCLUDED IN THE COST FOR COMBINATION CONCRETE CURB AND GUTTER.
2. CONTRACTOR SHALL SAWCUT PAVEMENT PRIOR TO REMOVING THE CURB.
3. ADDITIONAL AGGREGATE REQUIRED BETWEEN THE TOP OF THE EXISTING BASE AND BOTTOM OF PROPOSED ASPHALT SHALL BE PAID FOR AS AGGREGATE BASE REPAIR.
4. PAVEMENT REMOVAL ADJACENT TO PROPOSED CURB AND GUTTER SHALL BE INCLUDED IN THE COST OF THE PROPOSED CURB AND GUTTER.
5. FOR ROADWAYS WITH DIFFERING ELEVATIONS AT THE EDGE OF PAVEMENT, CONTRACTOR SHALL PAVE THE SIDE WITH THE HIGHER ELEVATION FIRST. WHEN GRADING THE AGGREGATE BASE AT THESE LOCATIONS, CONTRACTOR SHALL PROVIDE GRADE STAKES SHOWING FINISHED PAVEMENT GRADES AT 2% (MIN.) OFF HIGH SIDE.
6. ENGINEER TO VERIFY MINIMUM 2% CROSS SLOPE PRIOR TO PLACING SURFACE COURSE. ANY CORRECTIONS MUST BE MADE BY THE CONTRACTOR AT HIS OWN EXPENSE.
7. HOT-MIX ASPHALT SURFACE REMOVAL INCLUDES REMOVAL AND DISPOSAL OF ANY ADDITIONAL MATERIAL OR AGGREGATE BASE NECESSARY TO OBTAIN THE REQUIRED ELEVATION FOR INSTALLATION OF BINDER COURSE. THIS SHALL INCLUDE RESIDUAL LAYERS OF ASPHALT THAT ARE NOT OF SUFFICIENT THICKNESS TO BE PAVED OVER.
8. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR PETROMAT (FABRIC) ENCOUNTERED DURING GRINDING OPERATIONS. REMOVAL AND DISPOSAL OF PETROMAT SHALL BE INCLUDED IN THE COST OF HOT-MIX ASPHALT SURFACE REMOVAL.

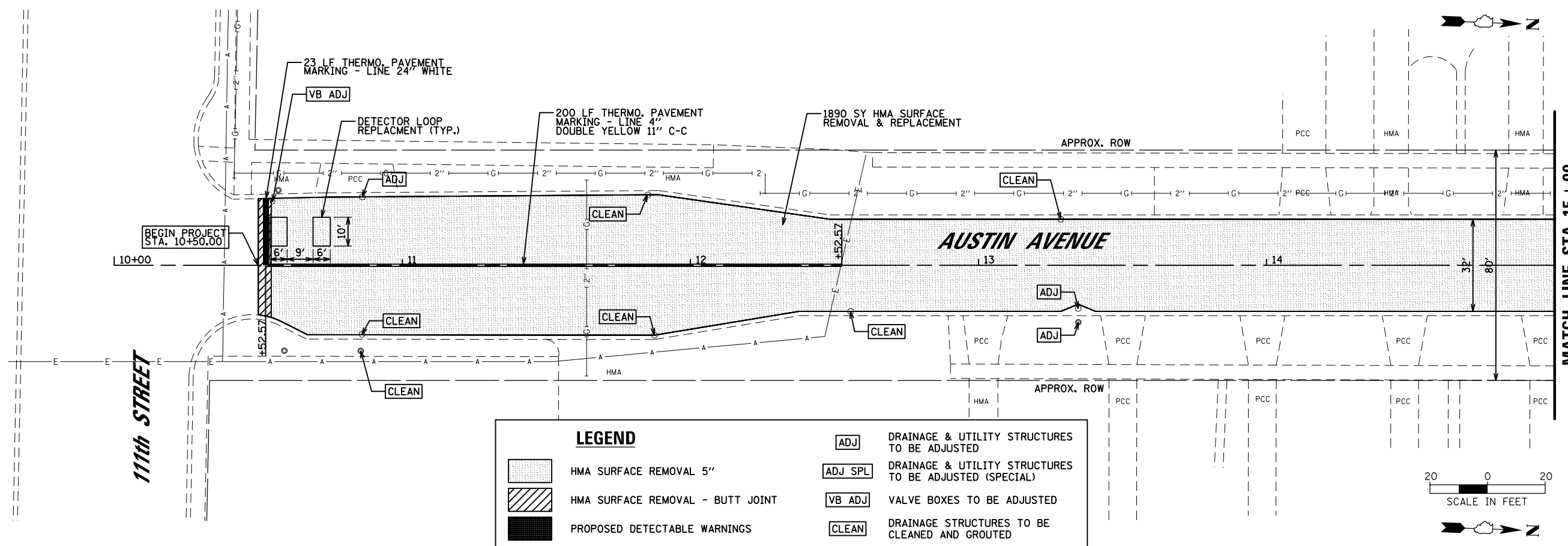
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	PLOT DATE = 12/19/2017	DATE -	12/19/2017	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

AUSTIN AVENUE RESURFACING
TYPICAL SECTIONS

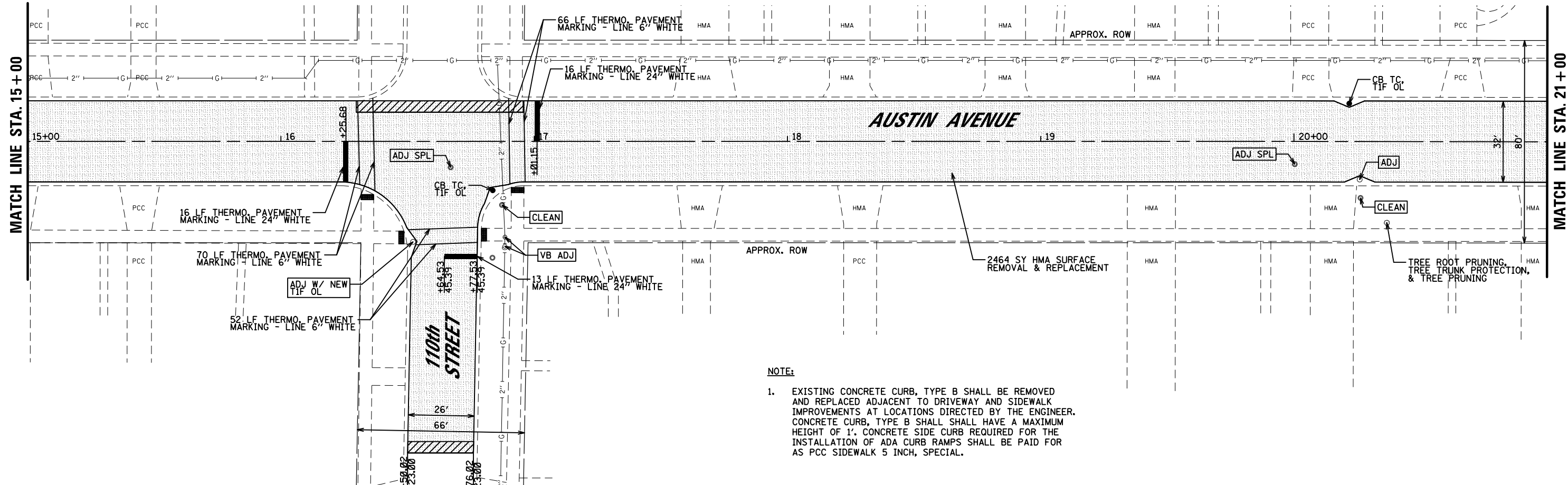
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MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-R5	COOK	19	5
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61E41	



LEGEND

	HMA SURFACE REMOVAL 5"		DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
	HMA SURFACE REMOVAL - BUTT JOINT		DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)
	PROPOSED DETECTABLE WARNINGS		VALVE BOXES TO BE ADJUSTED
			DRAINAGE STRUCTURES TO BE CLEANED AND GROUTED



NOTE:

- EXISTING CONCRETE CURB, TYPE B SHALL BE REMOVED AND REPLACED ADJACENT TO DRIVEWAY AND SIDEWALK IMPROVEMENTS AT LOCATIONS DIRECTED BY THE ENGINEER. CONCRETE CURB, TYPE B SHALL HAVE A MAXIMUM HEIGHT OF 1'. CONCRETE SIDE CURB REQUIRED FOR THE INSTALLATION OF ADA CURB RAMPS SHALL BE PAID FOR AS PCC SIDEWALK 5 INCH, SPECIAL.

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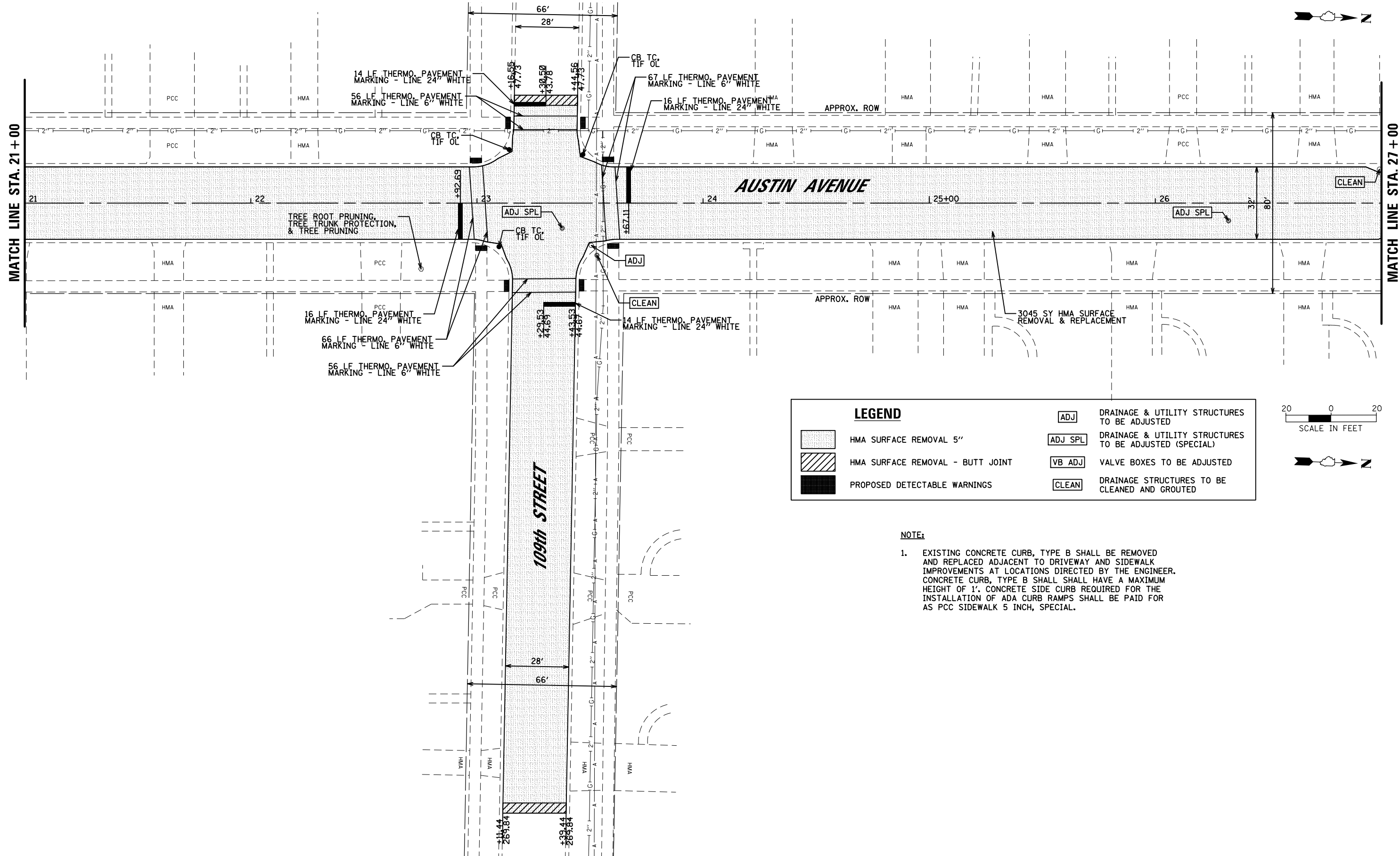
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**AUSTIN AVENUE RESURFACING
PROPOSED PLAN**

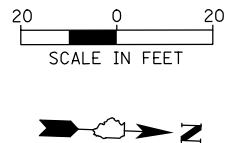
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MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-R5	COOK	19	6
CONTRACT NO. 61E41				

ILLINOIS FED. AID PROJECT



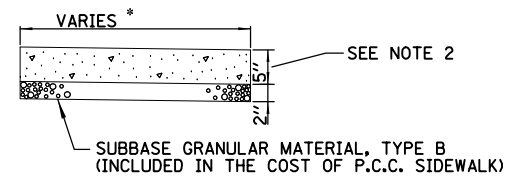
LEGEND	
	HMA SURFACE REMOVAL 5"
	HMA SURFACE REMOVAL - BUTT JOINT
	PROPOSED DETECTABLE WARNINGS
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED (SPECIAL)
	VALVE BOXES TO BE ADJUSTED
	DRAINAGE STRUCTURES TO BE CLEANED AND GROUTED



NOTE:

- EXISTING CONCRETE CURB, TYPE B SHALL BE REMOVED AND REPLACED ADJACENT TO DRIVEWAY AND SIDEWALK IMPROVEMENTS AT LOCATIONS DIRECTED BY THE ENGINEER. CONCRETE CURB, TYPE B SHALL HAVE A MAXIMUM HEIGHT OF 1'. CONCRETE SIDE CURB REQUIRED FOR THE INSTALLATION OF ADA CURB RAMPS SHALL BE PAID FOR AS PCC SIDEWALK 5 INCH, SPECIAL.

FILE NAME = N:\CHICAGO\RIDG\90611\098\Civil\PLN_9000011098_02.SHT	USER NAME = aschaefer	DESIGNED - AJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	AUSTIN AVENUE RESURFACING PROPOSED PLAN			MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PLOT SCALE = 48'	PLotted DATE = 12/19/2017	DRAWN - AJS	REVISED -					1070	16-00055-00-R5	COOK	19	7
CHECKED - AMP	DATE - 12/19/2017	REVISIED -	REVISED -					CONTRACT NO. 61E41			ILLINOIS FED. AID PROJECT	
SCALE: 48'	SHEET NO.	OF SHEETS	STA.					TO STA.				

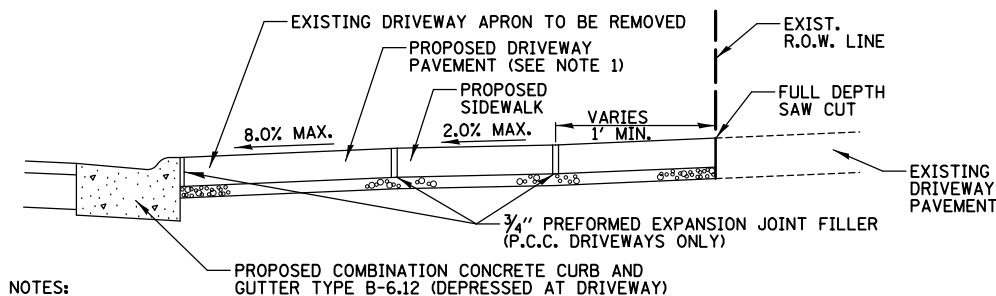


• CROSS SLOPE 0.5% (MIN.) TO 2% (MAX.)

NOTES:

1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCLUDED IN THE COST OF THE P.C.C. SIDEWALK 5 INCH, SPECIAL.
2. THICKNESS SHALL BE INCREASED TO 7" WHERE SIDEWALK IS ADJACENT TO A DRIVEWAY (NO WIRE MESH). (INCLUDED IN THE COST OF P.C.C. SIDEWALK).
3. IN LOCATIONS WHERE SIDEWALK IS REMOVED AND REPLACED THROUGH DRIVEWAYS, DRIVEWAYS SHALL BE SAWCUT AND PATCHED A MINIMUM OF 1' ON EITHER SIDE OF THE WALK. THIS WORK SHALL BE PAID FOR PER SQUARE YARD AT THE CONTRACT UNIT PRICE FOR DRIVEWAY REMOVAL AND P.C.C. CONCRETE DRIVEWAY PAVEMENT OR HOT-MIX ASPHALT DRIVEWAY PAVEMENT.
4. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.

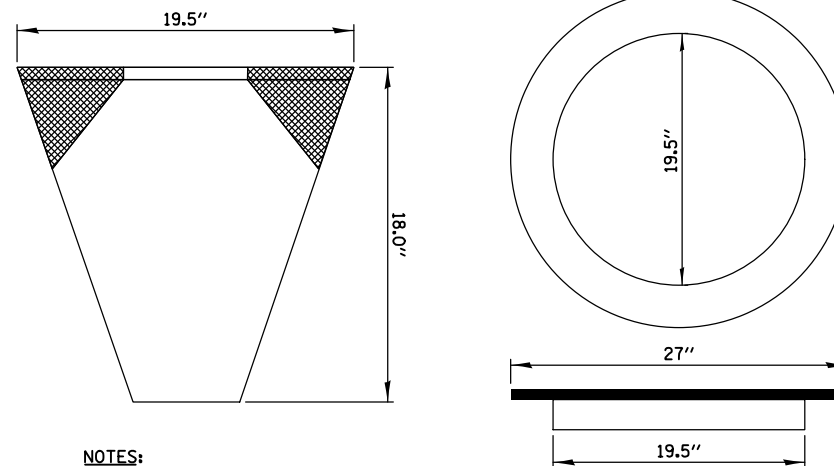
**P.C.C. SIDEWALK, 5 INCH SPECIAL
DETAIL**



NOTES:

1. THE PROPOSED DRIVEWAY PAVEMENT SHALL CONSIST OF:
 - A. 7" PORTLAND CEMENT CONCRETE AND 3" OF AGGREGATE BASE COURSE TYPE B IF THE EXISTING DRIVEWAY IS P.C. CONCRETE, OR
 - B. 3" HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50, AND 6" AGGREGATE BASE COURSE TYPE B IF THE EXISTING DRIVEWAY IS HMA OR AGGREGATE.
2. PREFORMED EXPANSION JOINT FILLER SHALL BE INCLUDED IN THE COST OF P.C.C. DRIVEWAY PAVEMENT.
3. ALL REQUIRED EARTH EXCAVATION SHALL BE INCLUDED IN THE COST OF DRIVEWAY REMOVAL.

DRIVEWAY REMOVAL AND REPLACEMENT DETAIL



NOTES:

FRAME:
 FLANGE SHALL BE FABRICATED FROM 1/8" FLAT STACK.
 BASE RING SHALL BE FABRICATED FROM 1/2" x 1/2" x 1/8" CHANNEL.
 DOMESTIC STEEL CONFORMING TO ASTM-A36.

SEDIMENT BAG: SHALL BE FABRICATED FROM 4 OZ/SQ YD NON-WAVEN POLYPROPYLENE GEOTEXTILE AND SHALL BE REINFORCED WITH POLYESTER MESH. THE BAG SHALL BE SECURED TO THE BASE RING WITH STAINLESS STEEL STRAP AND LOCK.

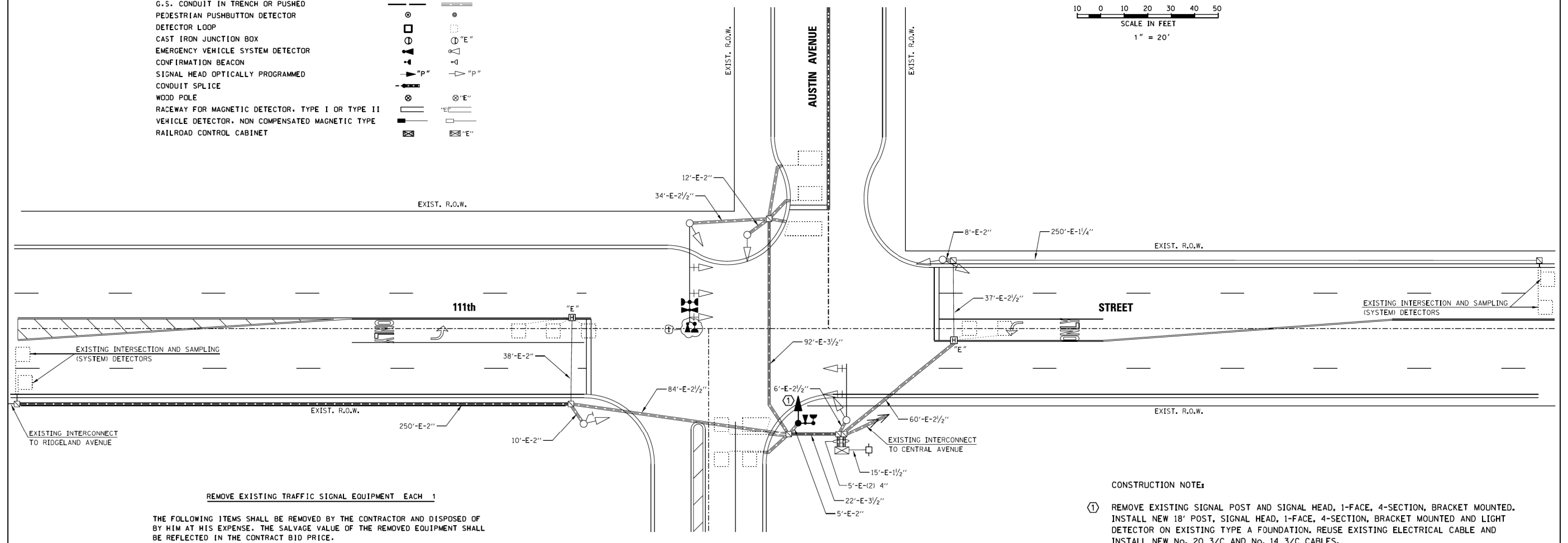
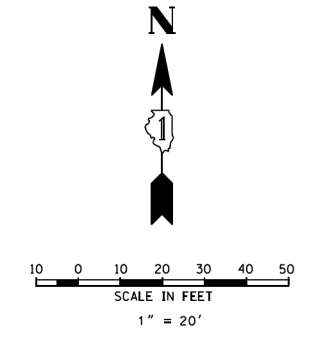
CATCH BASIN AND INLET FILTER
 N.T.S.

FILE NAME =	USER NAME = aschaefer	DESIGNED - AJS	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	AUSTIN AVENUE RESURFACING CONSTRUCTION DETAILS	MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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PLOT SCALE = 48'		CHECKED - AMP	REVISED -			CONTRACT NO. 61E41					
PLOT DATE = 12/19/2017		DATE - 12/19/2017	REVISED -			ILLINOIS FED. AID PROJECT					
						SCALE: 48'	SHEET NO.	OF SHEETS	STA.	TO STA.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
98-00036-00-TL		COOK	44	30
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT			

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
CONTROLLER		
SERVICE INSTALLATION		
SIGNAL HEAD		
SIGNAL HEAD WITH BACKPLATE		
SIGNAL HEAD, PEDESTRIAN		
SIGNAL POST		
MAST ARM ASSEMBLY AND POLE, STEEL		
MAST ARM ASSEMBLY AND POLE, ALUMINUM		
COMMON TRENCH		
UNIT DUCT		
HANDHOLE		
HEAVY DUTY HANDHOLE		
DOUBLE HANDHOLE		
G.S. CONDUIT IN TRENCH OR PUSHED		
PEDESTRIAN PUSHBUTTON DETECTOR		
DETECTOR LOOP		
CAST IRON JUNCTION BOX		
EMERGENCY VEHICLE SYSTEM DETECTOR		
CONFIRMATION BEACON		
SIGNAL HEAD OPTICALLY PROGRAMMED		
CONDUIT SPLICE		
WOOD POLE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR TYPE II		
VEHICLE DETECTOR, NON COMPENSATED MAGNETIC TYPE		
RAILROAD CONTROL CABINET		



REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT EACH 1

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND DISPOSED OF BY HIM AT HIS EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH SIGNAL HEAD, 1-FACE, 4-SECTION
- 1 EACH TRAFFIC SIGNAL POST

CONSTRUCTION NOTE:

- ① REMOVE EXISTING SIGNAL POST AND SIGNAL HEAD, 1-FACE, 4-SECTION, BRACKET MOUNTED. INSTALL NEW 18' POST, SIGNAL HEAD, 1-FACE, 4-SECTION, BRACKET MOUNTED AND LIGHT DETECTOR ON EXISTING TYPE A FOUNDATION. REUSE EXISTING ELECTRICAL CABLE AND INSTALL NEW No. 20 3/C AND No. 14 3/C CABLES.

HOLY SEPULCHRE CATHOLIC CEMETERY

RECORD DRAWING FOR INFORMATIONAL PURPOSES ONLY

NOTE: THE INTERSECTION AND TRAFFIC SIGNAL INSTALLATION ARE MAINTAINED BY IDOT.

REVISIONS	
NAME	DATE
NB EVP	6/9/99

ILLINOIS DEPARTMENT OF TRANSPORTATION
TRAFFIC SIGNAL MODIFICATION PLAN
 111th STREET AND AUSTIN AVENUE / HOLY SEPULCHRE CATHOLIC CEMETERY
 SCALE 1" = 20'
 DATE 9-25-98
 DRAWN BY FPB / FCP
 CHECKED BY ABR / GMZ

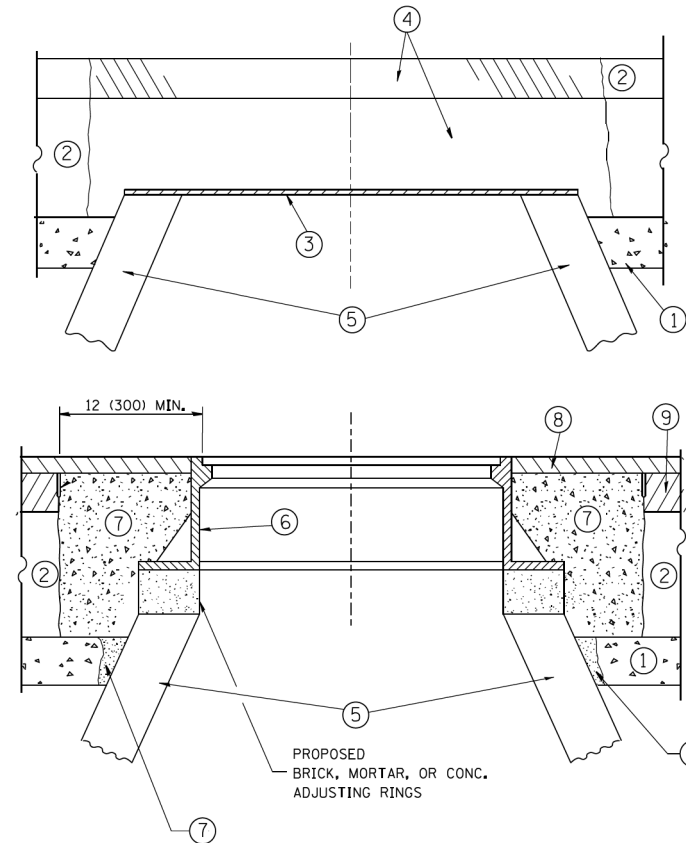
CHRISTOPHER B. BURKE ENGINEERING LTD.
 9575 West Higgins Road, Suite 600
 Rosemont, Illinois 60018
 (847) 823-0500

FILE NAME = N:\CHICAGO\RIDGELAND\9861T\098\Civil\SIGNAL	USER NAME = aschaefer	DESIGNED - AJ5	REVISED -
	ANS_900061T098_01.SHT	DRAWN - AJ5	REVISED -
	PLOT SCALE = 48'	CHECKED - AMP	REVISED -
	PLOT DATE = 12/19/2017	DATE - 12/19/2017	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

AUSTIN AVENUE RESURFACING EXISTING TRAFFIC SIGNAL PLANS (FOR REFERENCE ONLY)
 SCALE: 48' SHEET NO. OF SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-R5	COOK	19	10
CONTRACT NO. 61E41				
ILLINOIS FED. AID PROJECT				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

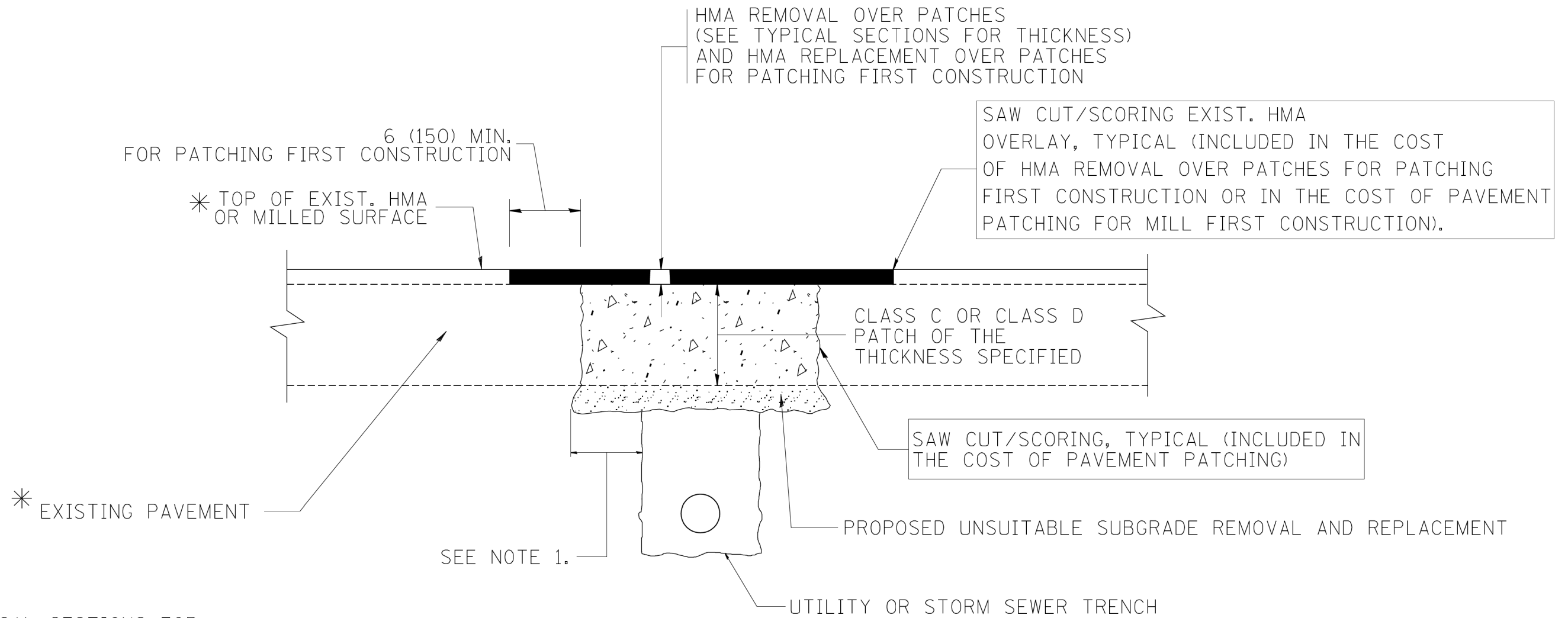
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	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-R5	COOK	19	12
BD600-03 (BD-8)		CONTRACT NO.	61E41	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	1070	16-00055-00-R5	COOK	19	13
		PLOT SCALE = 50.000' / IN.	REVISED - R. BORO 09-04-07					BD400-04 (BD-22)		CONTRACT NO. 61E41			
		PLOT DATE = 10/27/2008	REVISED - K. ENG 10-27-08					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

18" (450) MAX.

T/2 *

3" (75) MIN.

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, ~~SALT TOLERANT~~ AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

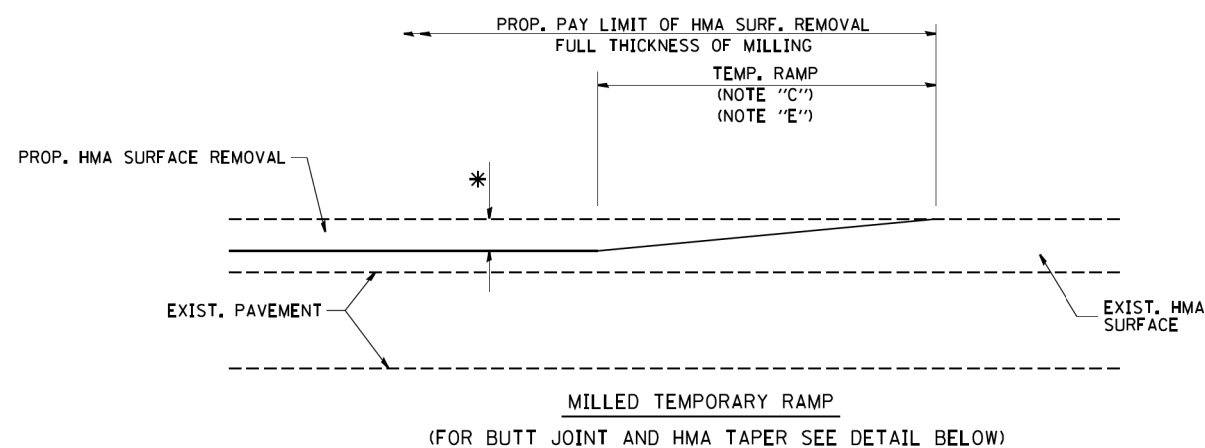
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

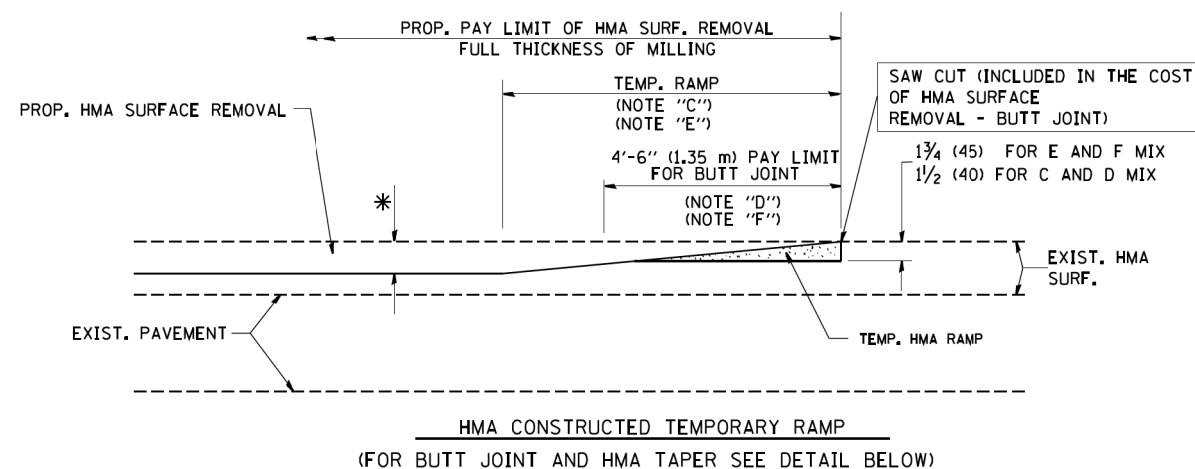
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 12-15-09				BD600-06 (BD-24)		CONTRACT NO. 61E41		
PLOT DATE = 12/15/2009	DATE - 03-11-94					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

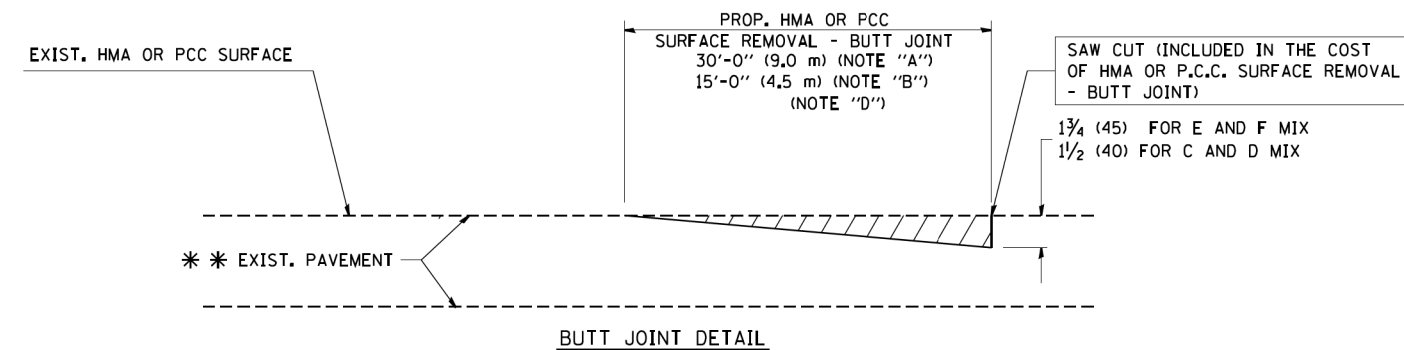


OPTION 1

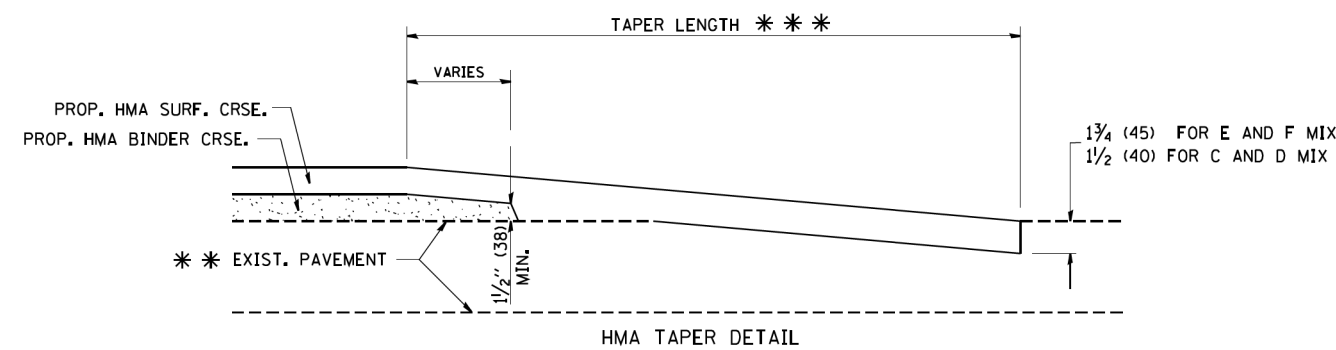


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT

G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

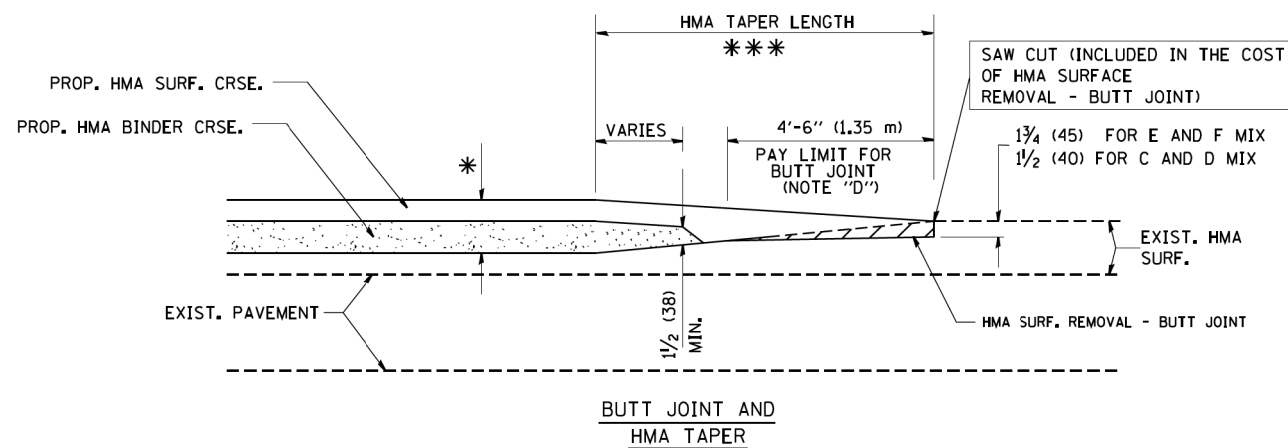
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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USER NAME = gaglianobt
PLOT SCALE = 50.0000' / IN.
PLOT DATE = 1/4/2008

DESIGNED - M. DE YONG
DRAWN -
CHECKED -
DATE - 06-13-90

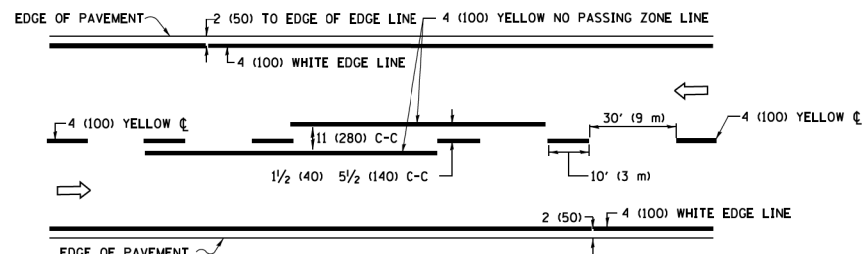
REVISED - R. SHAH 10-25-94
REVISED - A. ABBAS 03-21-97
REVISED - M. GOMEZ 04-06-01
REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

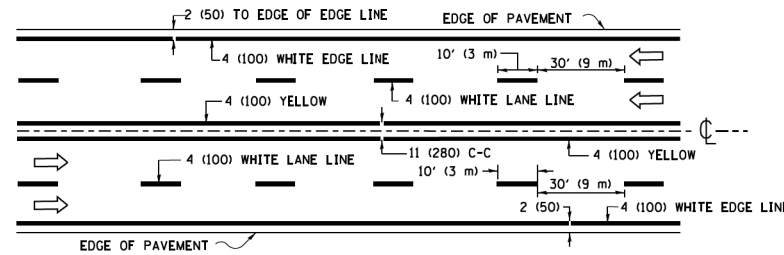
**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

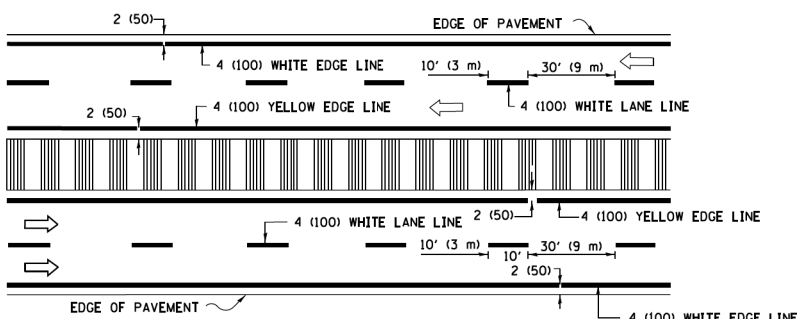
MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-R5	COOK	19	15
BD400-05 BD32		CONTRACT NO. 61E41		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



2-LANE ROADWAY

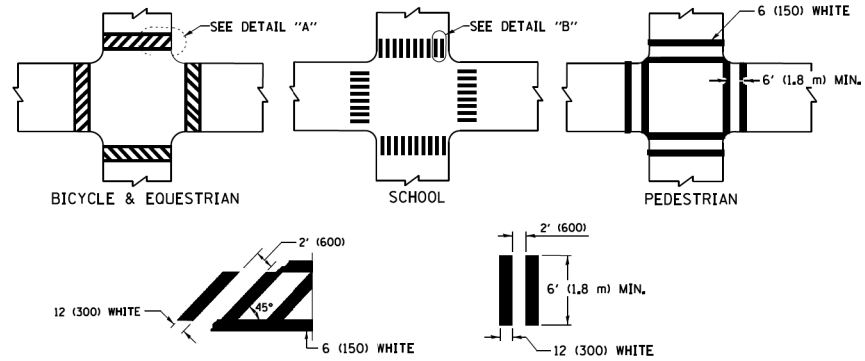


MULTI-LANE UNDIVIDED



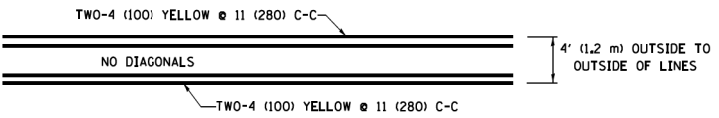
MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

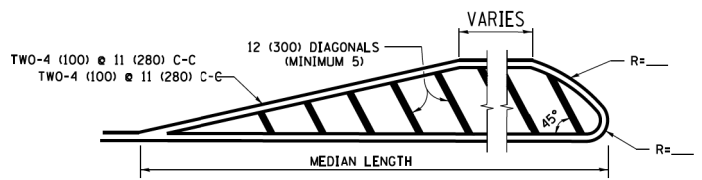


TYPICAL CROSSWALK MARKING

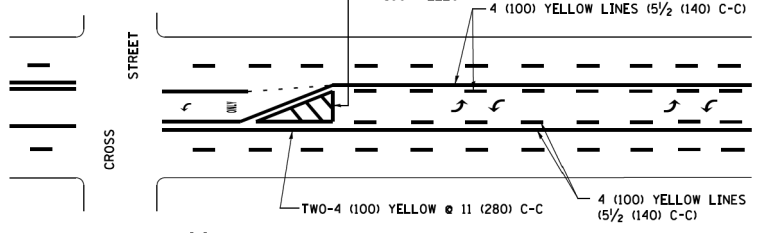
* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



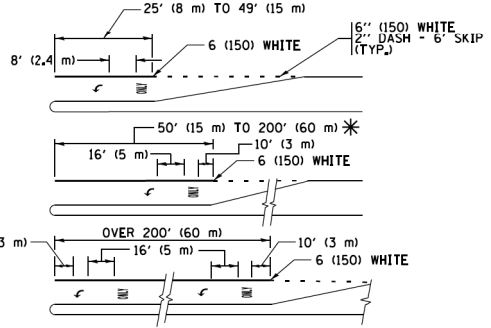
4' (1.2 m) WIDE MEDIANS ONLY



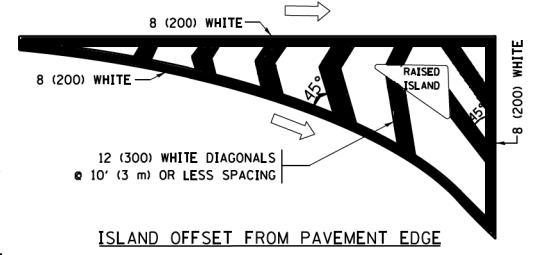
MEDIANS OVER 4' (1.2 m) WIDE



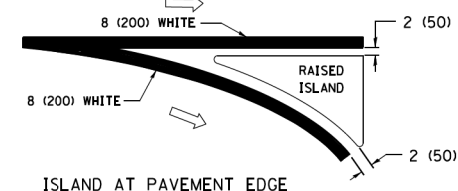
MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING



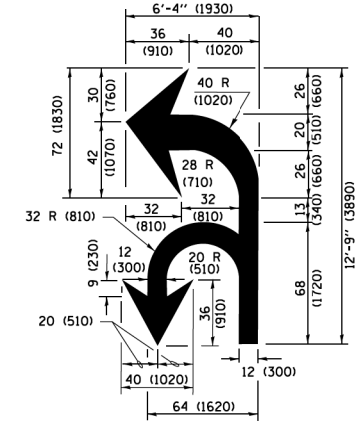
TYPICAL LEFT (OR RIGHT) TURN LANE TYPICAL TURN LANE MARKING



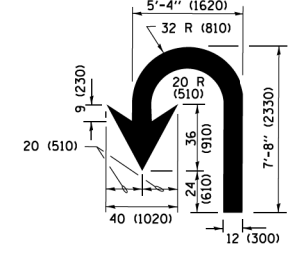
ISLAND OFFSET FROM PAVEMENT EDGE



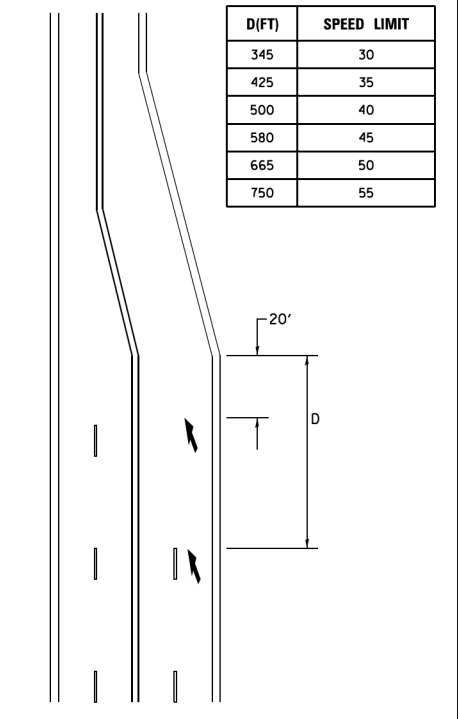
ISLAND AT PAVEMENT EDGE TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN



LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE AREA OF "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

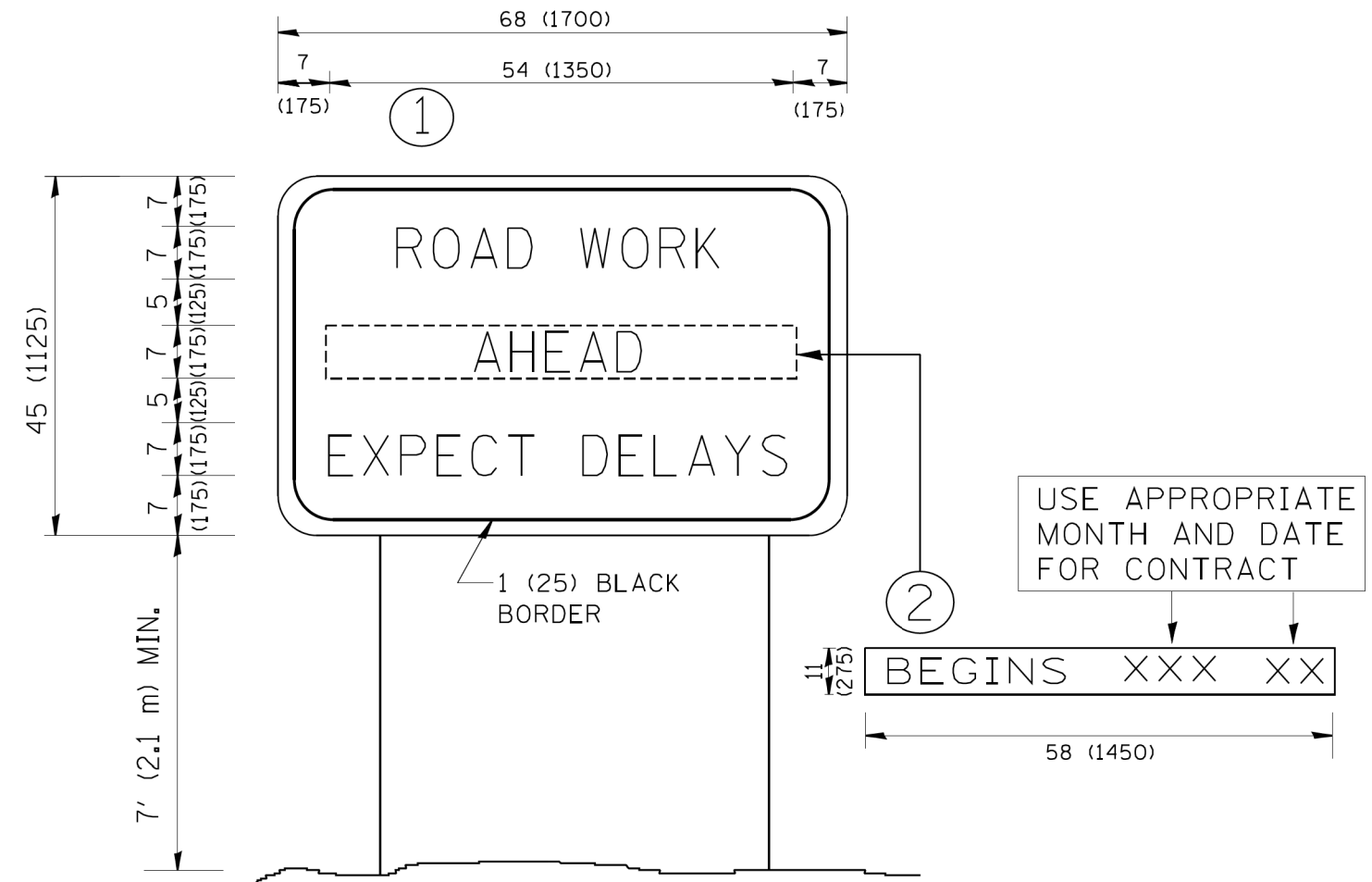
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = footemj	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084E8ID\INTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\CAD\CADData\CADsheets\to13.dgn		DRAWN -	REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 50,000' / in	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 4/13/2016	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-RS	COOK	19	17
TC-13		CONTRACT NO.	61E41	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = geglanoht	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

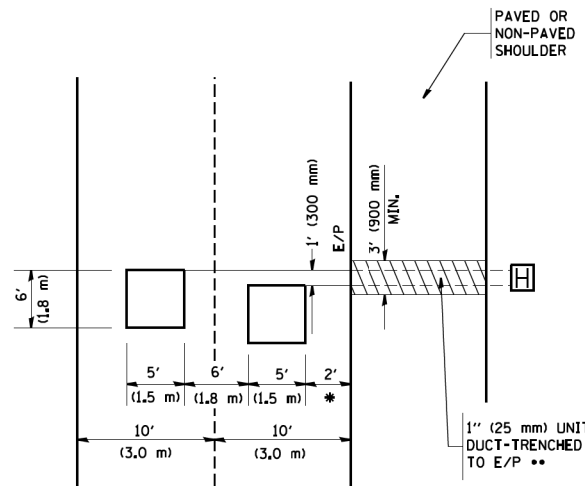
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-R5	COOK	19	18
TC-22		CONTRACT NO. 61E41		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



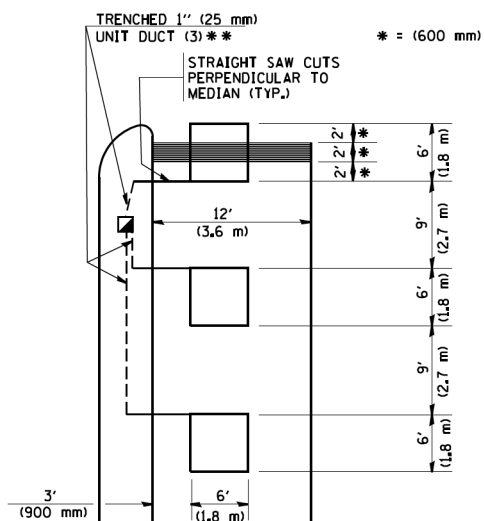
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



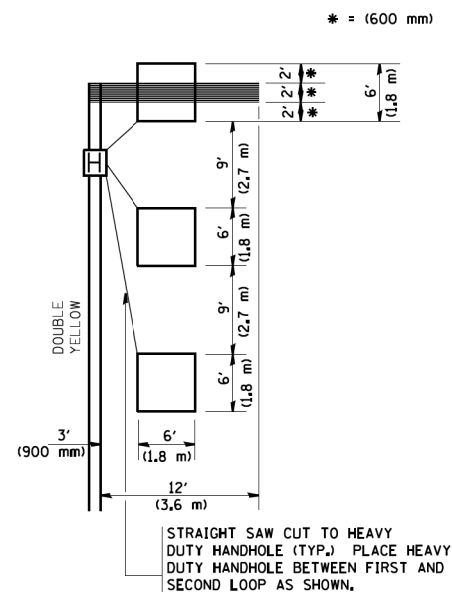
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH**

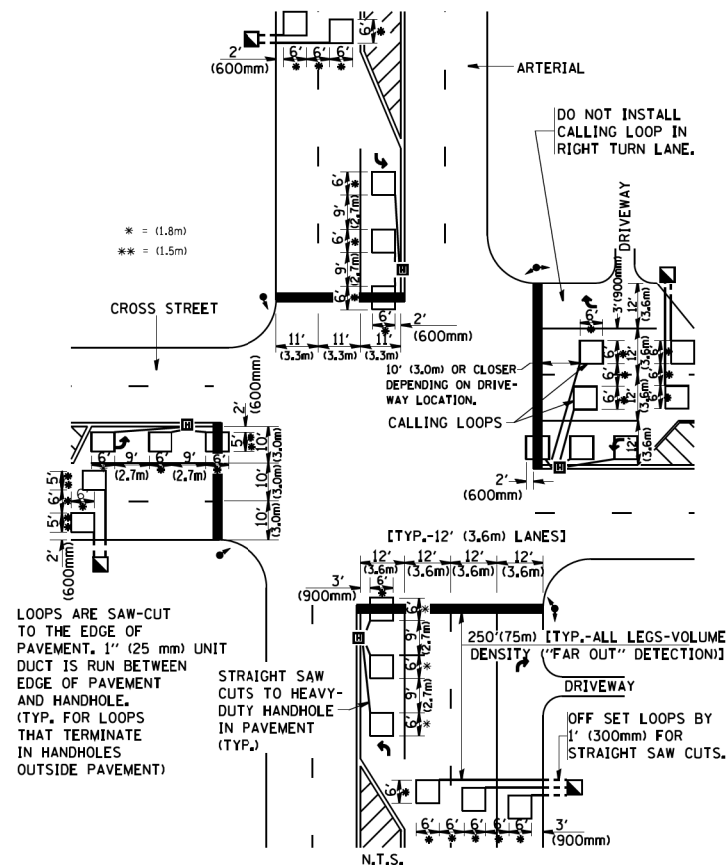
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

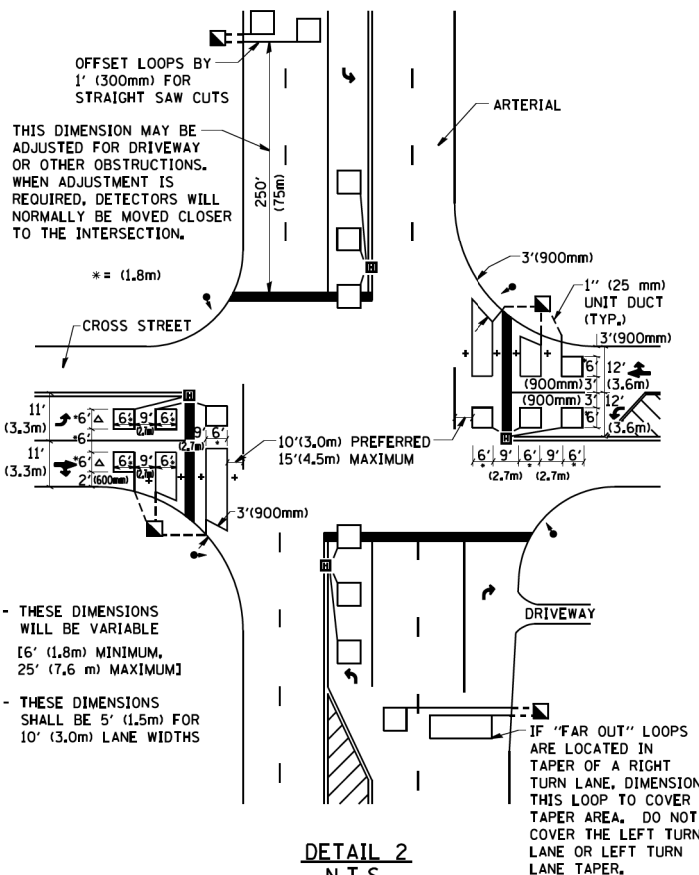
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



DETAIL 1
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME = W:\diststa\22x34\ts07.dgn

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PLOT SCALE = 50.0000 / IN.
PLOT DATE = 1/4/2008

DESIGNED -
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CHECKED - R.K.F.
DATE -

REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

MUN. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1070	16-00055-00-R5	COOK	19	19
TS-07		CONTRACT NO.	61E41	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				